

APPROVED
APR 30 1966

APPROVED
MAY 23 1966

LINE DATA

WAY-3-14.72 & WAY-76-12.04	
BEGIN PROJECT STA 773+72.81 TO EQUATION STA 798+24.13 BACK	2,451.32 LIN. FT.
EQUATION STA 798+17.97 AHEAD TO END PROJECT STA 981+00	18,282.03 LIN. FT.
TOTAL FOR PROJECT	=20,733.35 L.F. OR 3.926 MILES
ADD FOR APPROACH	
STA 769+05.86 TO STA 773+72.81	466.95 LIN. FT.
STA 981+00 TO STA 984+00	300.00 LIN. FT.
FROM SIDE ROADS (SEE SHEET 2)	16,913.66 LIN. FT.
TOTAL LENGTH OF WORK	38,413.96 L.F. OR 7.275 MILES

COUNTY ROAD 19 S-1511-(1)	
BEGIN PROJECT STA 74+75 TO END PROJECT STA 93+25	1,850.00 LIN. FT.
TOTAL FOR PROJECT & LENGTH OF WORK S-1511-(1)	1,850 L.F. OR .350 MILES

TOTAL LENGTH OF PROJECTS WAY-3-14.72 & WAY-76-12.04 & S-1511-(1) 22,583.35 L.F. OR 4.277 MI.
 TOTAL LENGTH OF WORK WAY-3-14.72 & WAY-76-12.04 & S-1511-(1) 40,263.96 L.F. OR 7.625 MI.

CONVENTIONAL SIGNS

COUNTY LINE	----
TOWNSHIP LINE	----
SECTION LINE	----
CORPORATION LINE	----
CENTER LINE	----
FENCE LINE	-x-x-x-
POLE LINE	-----
RAILROAD	====
GUARD RAIL	-----
DRAIN PIPE	-----
RIGHT OF WAY LINE	-----
LIMITED ACCESS LINE	-----
TREE REMOVAL	⊗ ⊗ ⊗ ⊗ ⊗

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STATE OF OHIO
 DEPARTMENT OF HIGHWAYS
WAY-3-14.72
WAY-76-12.04
 CITY OF WOOSTER
 WOOSTER & WAYNE TOWNSHIPS
 WAYNE COUNTY
 GRADE SEPARATION WITH THE PENN. R.R. CO.

FED. RD. DIVISION	STATE	PROJECT
2	OHIO	S-1511-(1)

1/623

WAYNE COUNTY
 WAY-3-14.72
 WAY-76-12.04

S-1511 (1)
 ON COUNTY ROAD 19

1965 SPECIFICATIONS

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR OF HIGHWAYS IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE REVISED CODE OF OHIO.

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF HIGHWAYS, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

THE RIGHT-OF-WAY FOR THIS IMPROVEMENT WILL BE PROVIDED BY THE STATE OF OHIO.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING OF THE HIGHWAY TO TRAFFIC AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED: D.H. Ginner
 DATE: 3-16-66 DIVISION DEPUTY DIRECTOR.

APPROVED: C.H. Altrater
 DATE: 4-25-66 ENGINEER OF BRIDGES

APPROVED: R.D. Ricketts
 DATE: 5-2-66 ENGINEER OF LOCATION & DESIGN

APPROVED: P.E. Shultz
 DATE: 5-2-66 DEPUTY DIRECTOR OF DESIGN & CONSTRUCTION

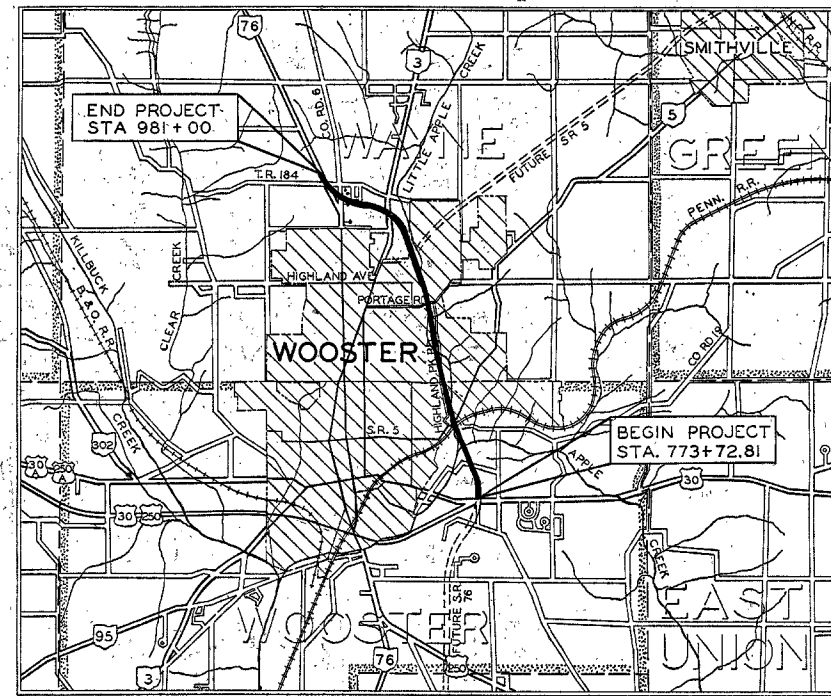
APPROVED: T.H. Brand
 DATE: 5-5-66 DEPUTY DIRECTOR OF RIGHT OF WAY

APPROVED: R.W. Wilson
 DATE: 5-6-66 DEPUTY DIRECTOR OF PLANNING & PROGRAMMING

APPROVED: _____
 DATE: _____ FIRST ASSISTANT DIRECTOR

APPROVED: P.E. Moshko
 DATE: 5/16/66 DIRECTOR OF HIGHWAYS

APPROVED: J. C. ...
 DATE: 5/16/66 MAYOR, CITY OF WOOSTER



DELIVERY POINT AVERAGE HAUL FROM SIDING PENNSYLVANIA R.R. WOOSTER 1.5 MILES

LOCATION MAP



PORTION TO BE IMPROVED
 STATE & FEDERAL HIGHWAYS
 OTHER ROADS
 PROPOSED HIGHWAYS

SCALES

PLAN 1" = 50'
 PROFILE - HORIZONTAL 1" = 50'
 PROFILE - VERTICAL 1" = 10'
 CROSS SECTIONS 1" = 10'

STANDARD DRAWINGS

GR-1	6-1-65	SP-53	6-30-61	MH-1A	6-1-65	BP-3	6-1-65	SD-2-64	11-25-64	801	9-2-65	816	8-6-65
BP-1	6-1-65	MC-4	6-1-65	MH-1	6-1-65	BP-4	6-1-65	BR-1-65	2-1-65	808	7-14-65	825	4-22-65
BP-2	6-1-65	CB-2-3&2-4	6-1-65	BP-7	1-1-66	BP-5	6-1-65	AS-1-54	8-10-65	809	4-30-65	1001	9-2-65
BP-6	6-1-65	CB-2-2A & B	6-1-65	MC-5	6-1-65	MH-2	6-1-65			810	5-10-65		
F-1	6-1-65	CB-3A	6-1-65	GR-2A	9-1-65	FACI-1	6-1-65			811	3-29-65		
F-2	6-1-65	CB-6	6-1-65	GR-6	6-1-65	FACI-2	6-1-65						
F-3	6-1-65	I-1	6-1-65	L-1	6-1-65	FSB-1-62	1-15-63			814	3-16-66		
MC-3	6-1-65	I-2	6-1-65	MC-1	6-1-65	SD-1-63	11-12-63			815	8-6-65		
HW-E	6-1-65	I-2A	6-1-65	MC-6	6-1-65								

FILE NO.	WAY-3-14.72 WAY-76-12.04
DATE OF LETTING	_____
CONTRACT NO.	_____

PREPARED AND RECOMMENDED BY
SHAFFER PARRETT & ASSOCIATES
 CONSULTING ENGINEERS
 MANSFIELD OHIO WOOSTER

DEPARTMENT OF COMMERCE
 BUREAU OF PUBLIC ROADS

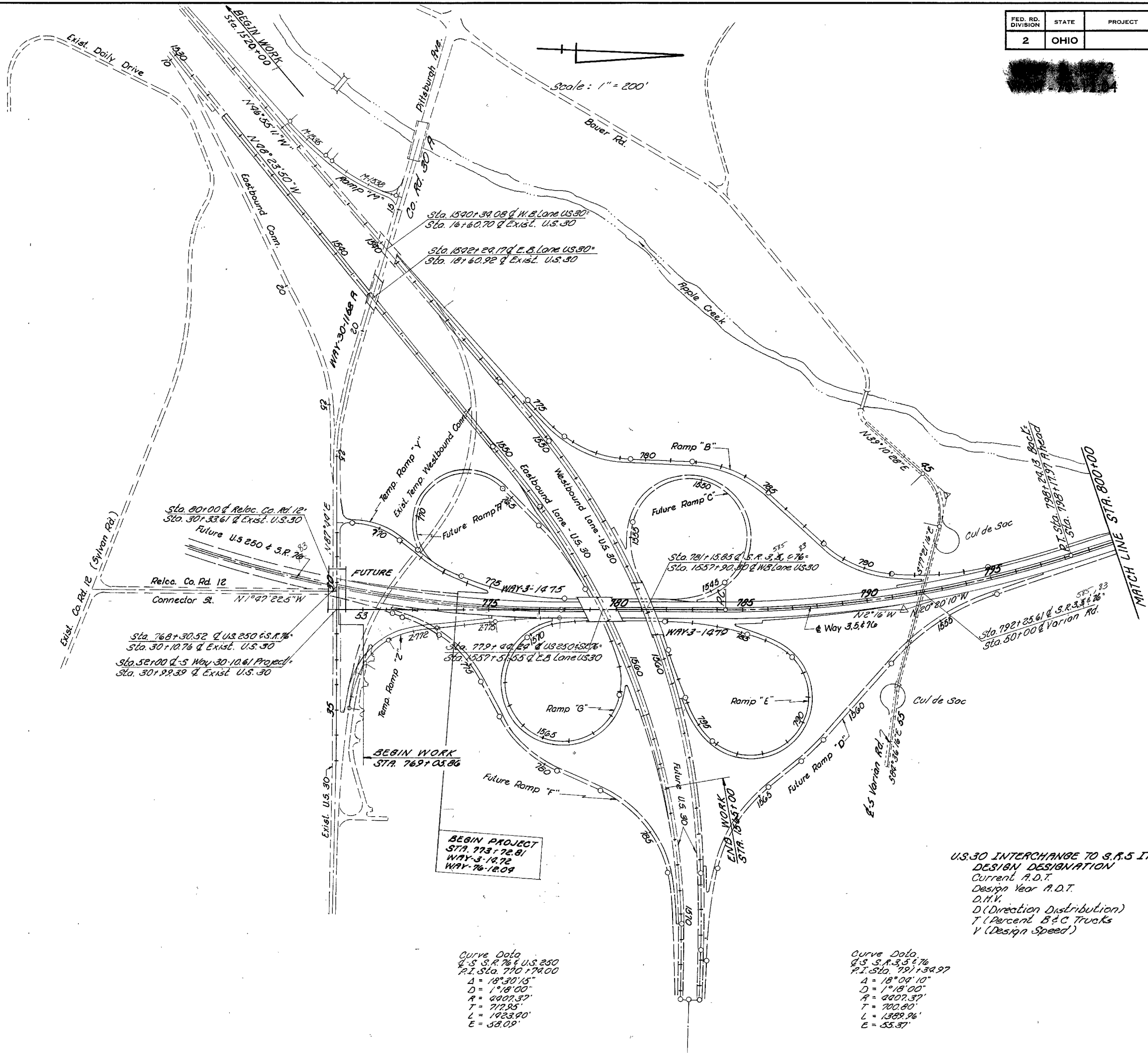
APPROVED _____ DATE _____

DIVISION ENGINEER _____ DATE _____

MICROFILMED
APR 25 1979

FED. RD. DIVISION	STATE	PROJECT
2	OHIO	

2
693



LINE DATA FOR APPROACHES

U.S. 30
Begin Work Sta. 1520+00
End Work Sta. 1565+00
Length of Work 4,500.00 Lin. Ft.

COUNTY ROAD 19
Begin Work Sta. 23+25
End Work Sta. 29+66
Length of Work 641.00 Lin. Ft.

RELOCATED S.R. 5
Begin Work Sta. 5+50
Equation Sta. 28+92.84 = Sta. 32+33.37
End Work Sta. 39+00
Length of Work 2,809.97 Lin. Ft.

PORTAGE ROAD
Begin Work Sta. 40+25
Equation Sta. 40+78.36 = 40+79.38 Ahd
End Work Sta. 61+75
Length of Work 2,148.98 Lin. Ft.

HIGHLAND AVENUE
Begin Work Sta. 110+00
End Work Sta. 117+19.38
Length of Work 719.38 Lin. Ft.

S.R. 3
Begin Work Sta. 58+75
End Work Sta. 86+00
Length of Work 2,725.00 Lin. Ft.

FRIENDSVILLE ROAD
Begin Work Sta. 12+00
Suspend Work Sta. 13+57.84
Resume Work Sta. 13+97.08
End Work Sta. 17+50
Length of Work 510.76 Lin. Ft.

MILLTOWN ROAD WEST
Begin Work Sta. 18+00
Suspend Work Sta. 19+88.73
Resume Work Sta. 20+29.66
End Work Sta. 26+00
Length of Work 759.07 Lin. Ft.

PENNSYLVANIA RAILROAD RELOCATION
Begin Work Sta. 7062+00
End Work Sta. 7083+00
Length of Work 2100.00 Lin. Ft.

Total length 16,913.66 Lin. Ft.
or 3.203 Miles

BEGIN PROJECT
STA. 773+72.81
WAY-3-14.72
WAY-76-12.09

Curve Data
Q-S S.R. 76 & U.S. 250
P.I. Sta. 770+74.00
Δ = 18°30'15"
D = 1°18'00"
R = 4007.37'
T = 712.95'
L = 1923.90'
E = 58.09'

Curve Data
Q-S S.R. 3, 5 & 76
P.I. Sta. 791+34.97
Δ = 18°04'10"
D = 1°18'00"
R = 4007.37'
T = 700.80'
L = 1389.96'
E = 55.37'

U.S. 30 INTERCHANGE TO S.R. 5 INTERCHANGE

DESIGN DESIGNATION	6102
Current A.D.T.	13,750
Design Year A.D.T.	2,060
D.H.V.	1,236
D (Direction Distribution)	12.6%
T (Percent B & C Trucks)	60 M.P.H.
V (Design Speed)	

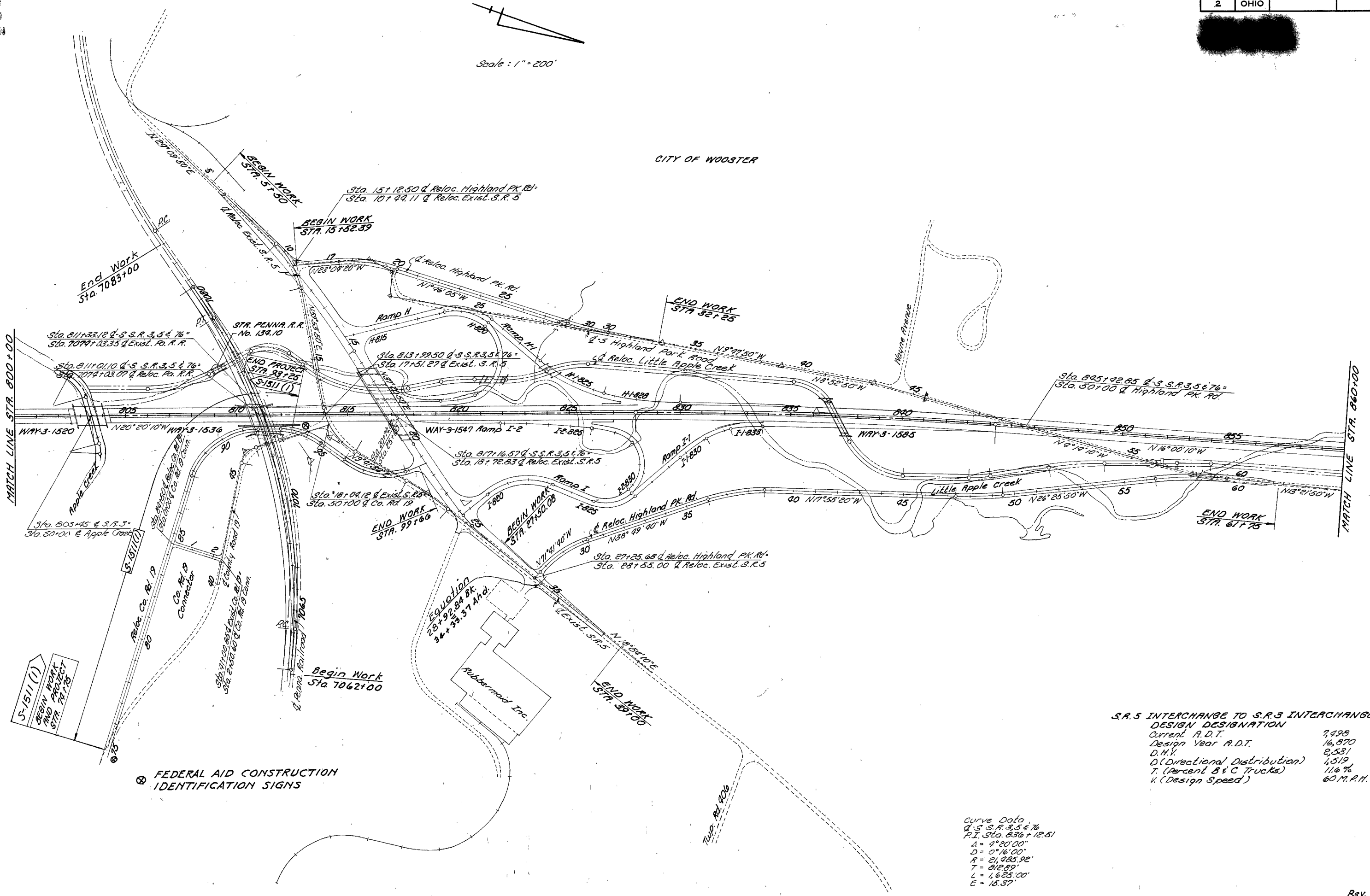
MODIFIED
APR 25 1970
REVISION

FED. RD. DIVISION	STATE	PROJECT
2	OHIO	

3
623

Scale: 1" = 200'

CITY OF WOOSTER



FEDERAL AID CONSTRUCTION IDENTIFICATION SIGNS

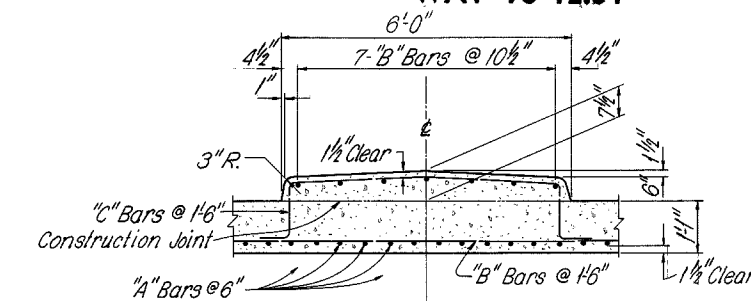
S.R. 5 INTERCHANGE TO S.R. 3 INTERCHANGE

DESIGN DESIGNATION	
Current A.D.T.	7,498
Design Year A.D.T.	16,870
D.H.V.	2,531
D (Directional Distribution)	4,519
T (Percent B+C Trucks)	11.6%
V (Design Speed)	60 M.P.H.

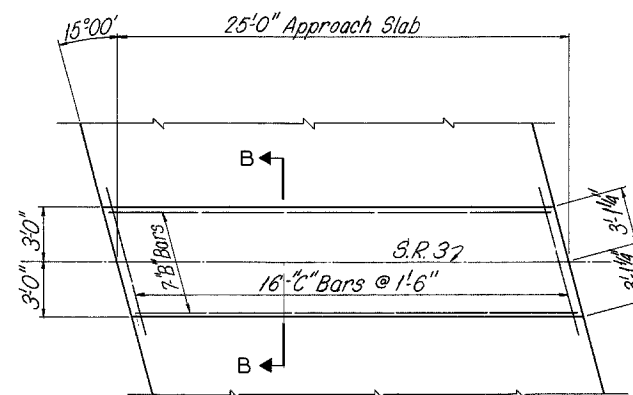
Curve Data
 d-s S.R. 3, 5 & 76
 P.I. Sta. 836+12.51
 Δ = 4° 20' 00"
 D = 0° 16' 00"
 R = 21,285.92'
 T = 812.89'
 L = 1,625.00'
 E = 15.37'

Rev. 5-10-66

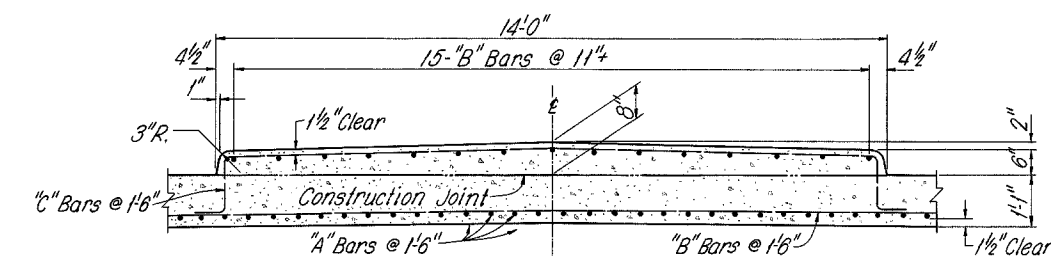
WAY-3-14.72
WAY-76-12.04



SECTION B-B



APPROACH SLAB MEDIAN DETAIL
BRIDGE NO. WAY-3-1520
OVER APPLE CREEK

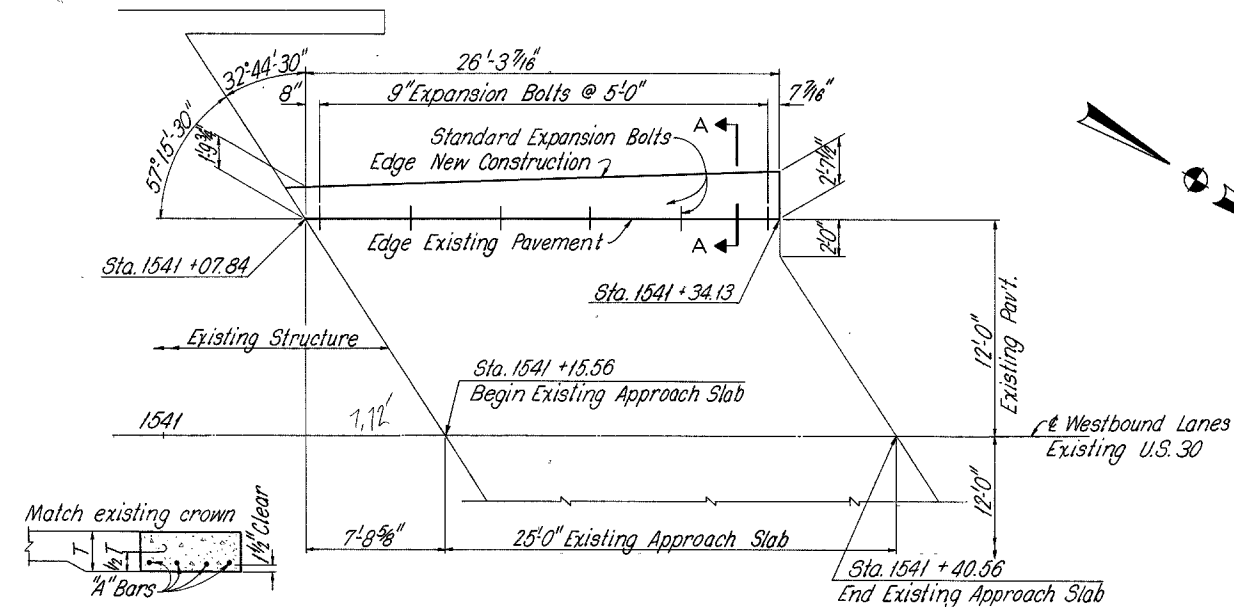


SECTION C-C

NOTES

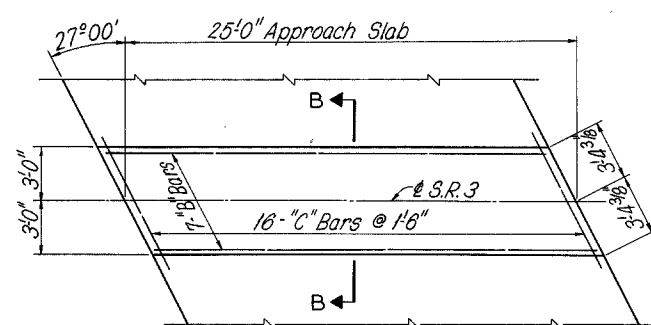
NOTES AND DETAILS See Standard Drawing AS-1-54 (dated 8-10-65).

REINFORCING STEEL: "A" Bars are number 8
"B" Bars are number 5
"C" Bars are number 5
"D" Bars are number 5

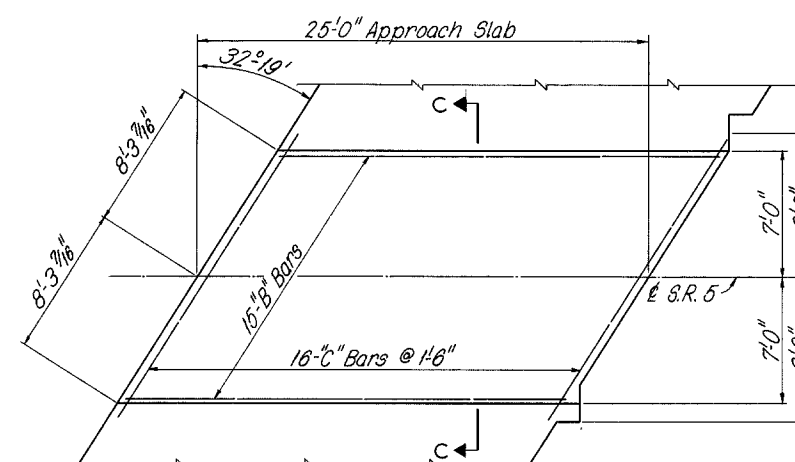


SECTION A-A

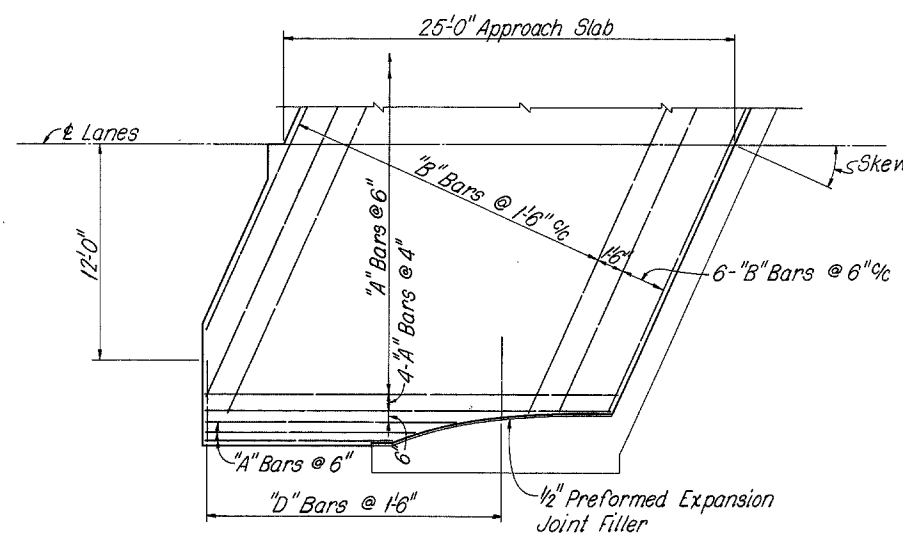
APPROACH SLAB MODIFICATION
EXISTING STRUCTURE
WAY-30-1163-L



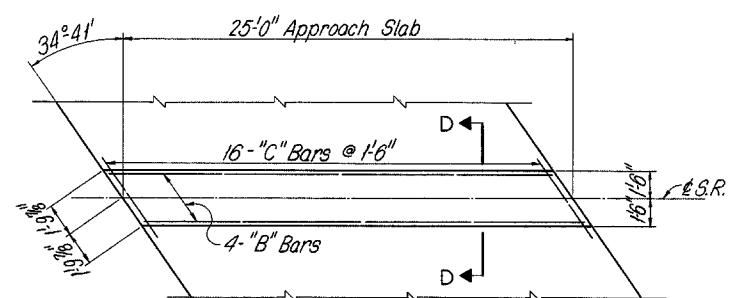
APPROACH SLAB MEDIAN DETAIL
BRIDGE NO. WAY-3-1475
E.B. LANES UNDER S.R.3



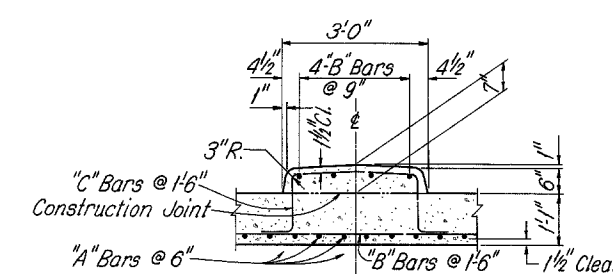
APPROACH SLAB MEDIAN DETAIL
BRIDGE NO. WAY-3-1547
S.R.5 OVER LITTLE APPLE CREEK & S.R.3



APPROACH SLAB DETAIL
BRIDGE NO. WAY-3-1479 (AS SHOWN) 1766 (OPP. HAND)
OVER S.R.3
BRIDGE NO. WAY-3-1547
S.R.5 OVER LITTLE APPLE CREEK & S.R.3



APPROACH SLAB MEDIAN DETAIL
BRIDGE NO. WAY-3-1766
OVER EXISTING U.S.3



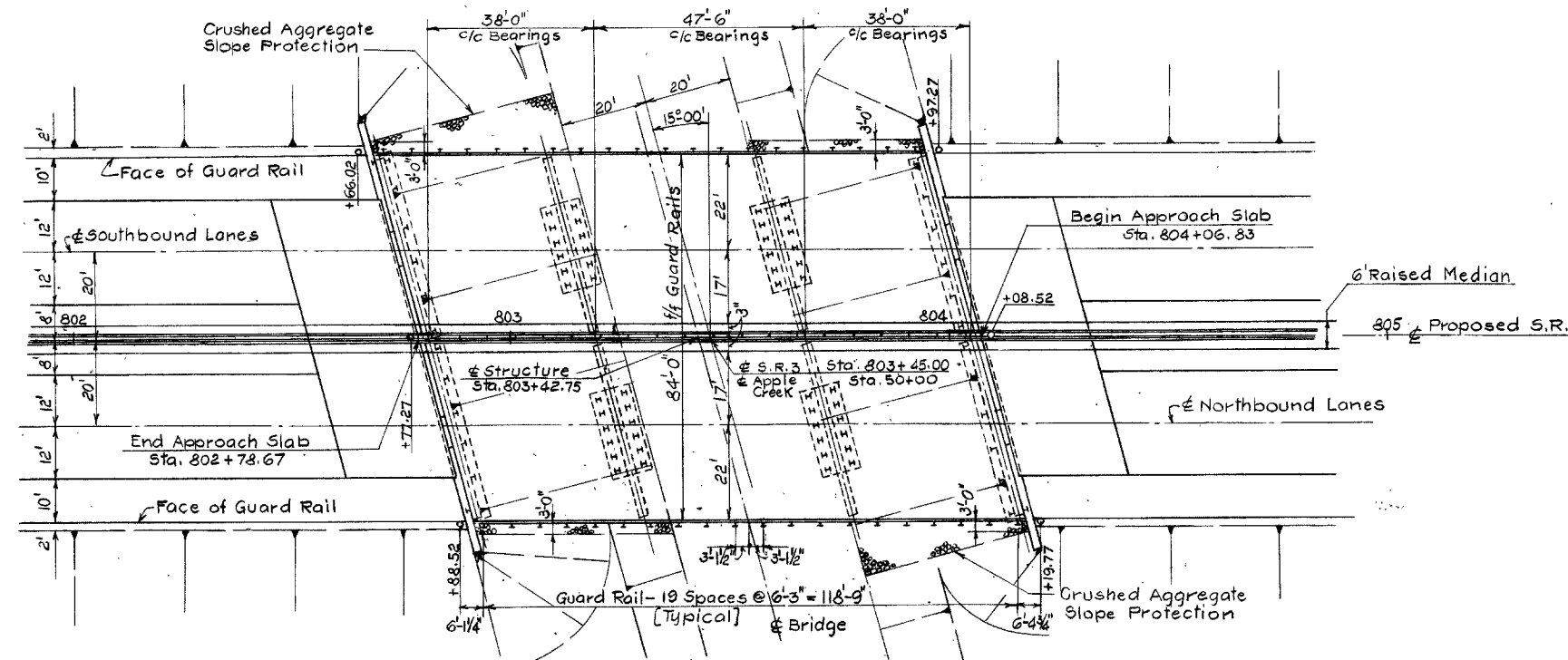
SECTION D-D

SHAFFER, PARRETT AND ASSOCIATES
Consulting Engineers
MANSFIELD, OHIO.

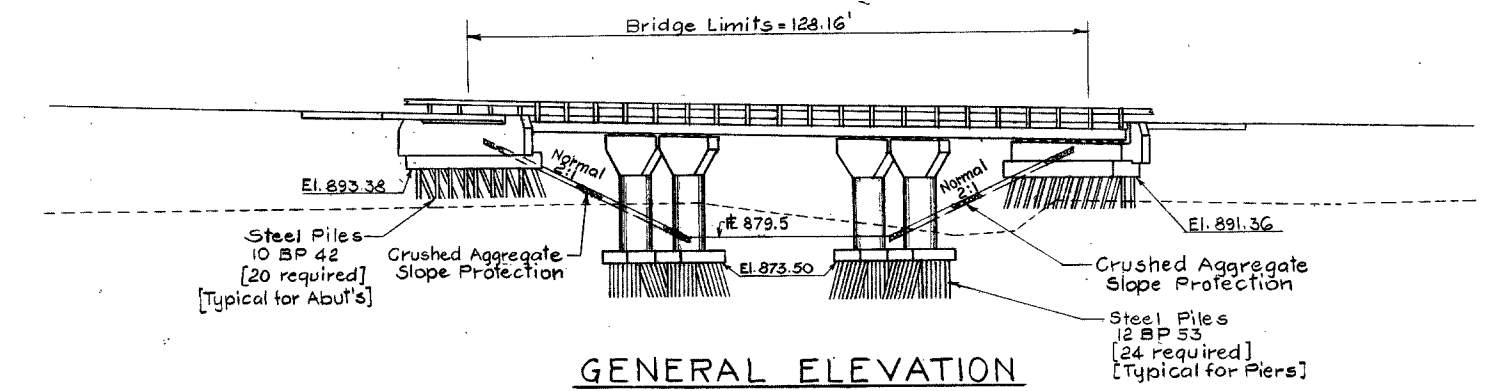
APPROACH SLAB DETAILS
BRIDGE NO. WAY-30-1163-L (EXISTING),
WAY-3-1475, 1479, 1520, 1547 & 1766
WAYNE COUNTY SR.3

DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE	REVISED
JPG	JPG	JS				

WAY-3-14.72
WAY-76-1204



GENERAL PLAN



GENERAL ELEVATION

GENERAL NOTES:

REFERENCE shall be made to Standard Drawings FSB-1-62 revised 1-15-63, SD-1-63 [Sheets 2 and 3] dated 11-12-63, SD-2-64 dated 11-25-64, AS-1-54 revised 8-10-65, and Supplemental Specifications 808 dated 7-14-65, 811 dated 3-29-65 and 825 dated 4-22-65.

DESIGN SPECIFICATIONS: This structure conforms to the requirements of "Design Specifications for Highway Structures" of the State of Ohio, Department of Highways, dated 9-1-57, together with current revisions thereof.

Design Loading : CF-400 [57]
Concrete Class "C" : basic unit stress 1,333 p.s.i.
Concrete Class "E" : basic unit stress 1,133 p.s.i.
Structural Steel : ASTM A36, basic unit stress - 20,000 p.s.i.

Reinforcing Steel : ASTM A15, A16, A160, Deformed, Intermediate or Hard Grade. Basic unit stress - 20,000 p.s.i.

PROCEDURE: The embankment shall be placed and compacted up to the finished spill-thru slope and to the level of the subgrade for a distance of 200 feet back of the abutments, after which excavation shall be made for the abutments and piers and piles driven.

EXCAVATION QUANTITY for the abutments includes the removal of fill material required for construction of the abutments. Excavation quantity for the piers includes the amount of material between the surface of the embankment slope and the bottom of the pier footings.

PILES shall be driven to a minimum bearing capacity of 33 tons per pile for the abutments and 40 tons per pile for the piers.

MACHINE FINISH: The concrete bridge deck shall be finished by the use of a finishing machine.

ESTIMATED QUANTITIES [TWO STRUCTURES]						
ITEM	QUANTITY	UNIT	DESCRIPTION	ABUT'S	PIERS	SUPER GENERAL AS BUILT
503	Lump	Sum	Cofferdams, Cribbs and Sheeting			Lump
503	691	Cu.Yds.	Unclassified Excavation	397	294	
505	Lump	Sum	First Test Pile			Lump
507	1200	Lin. Ft.	Steel Piles, 10 BP 42	1200		
507	960	Lin. Ft.	Steel Piles, 12 BP 53		960	
509	130,015	Lbs.	Reinforcing Steel	18,602	22,428	88,985
511	302	Cu.Yds.	Class "C" Concrete, Superstructure			302
511	158	Cu.Yds.	Class "C" Concrete, Pier Caps and Stems		158	
511	156	Cu.Yds.	Class "E" Concrete, Abutments above Footings	156		
511	197	Cu.Yds.	Class "E" Concrete, Footings	128	69	
512	13	Lin. Ft.	Waterproofing, Premolded Sealing Strip		13	
513	199,800	Lbs.	Structural Steel			199,800
514	199,800	Lbs.	Field Painting of Structural Steel			199,800
516	45	Sq. Ft.	Preformed Expansion Joint Filler [AASHTO, M-153]	45		
517	256.32	Lin. Ft.	Railing [Deep Beam Rails, with steel posts and bolts]			256.32
517	128.16	Lin. Ft.	Railing, Barrier [Deep Beam Rails, with steel posts and bolts]			128.16
518	68	Cu.Yds.	Porous Backfill	68		
518	166	Lin. Ft.	6" Helical Perforated C.M.P., Sec. 707.06, including Specials	166		
518	96	Lin. Ft.	6" Helical C.M.P., Sec. 707.06, Non-perforated	96		
518	10	Each	Scuppers, including supports			10
601	940	Sq.Yds.	Crushed Aggregate Slope Protection			940
808	302	Units	Water-Reducing, Set-Retarding Admixture			302
825	1270	Sq.Yds.	Concrete Surface Treatment			1270

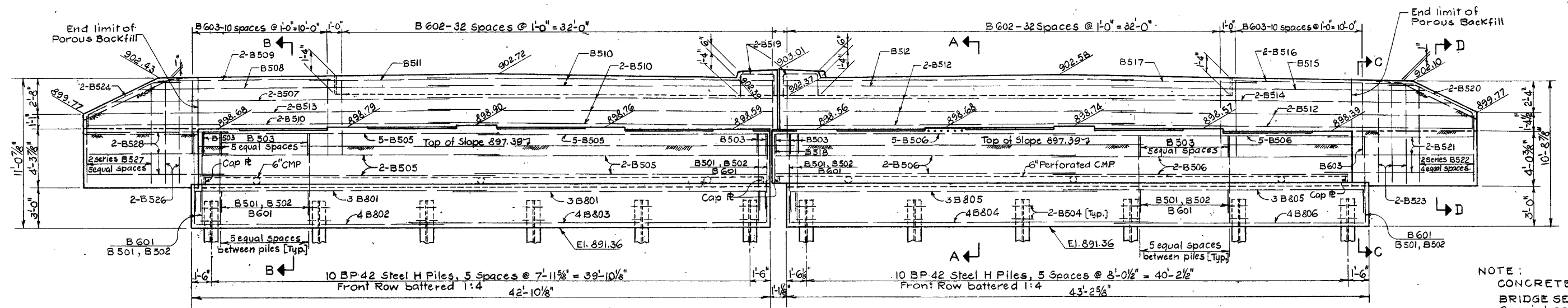
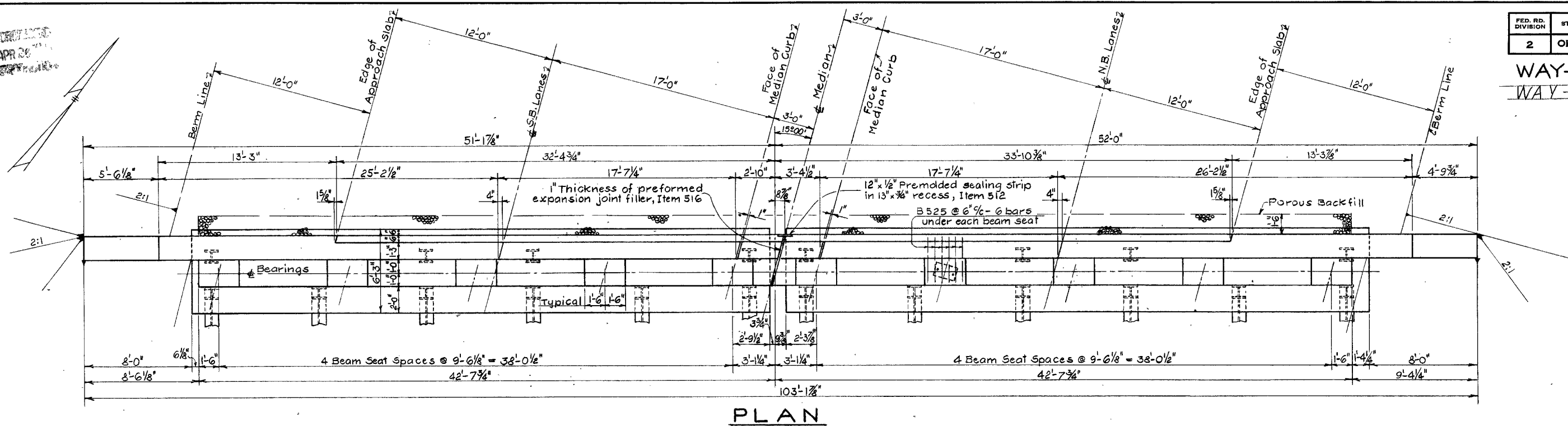
SHAFFER, PARRETT AND ASSOCIATES
Consulting Engineers
MANSFIELD, OHIO.

GENERAL PLAN AND ELEVATION
EST. QUANTITIES AND GEN. NOTES
BRIDGE NO. WAY-3-1520
OVER APPLE CREEK
WAYNE COUNTY S.R.3
STA. 802+78.67 TO STA. 804+06.83

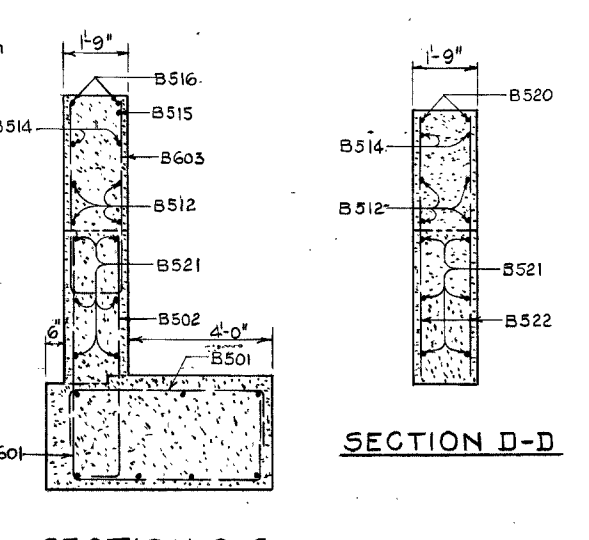
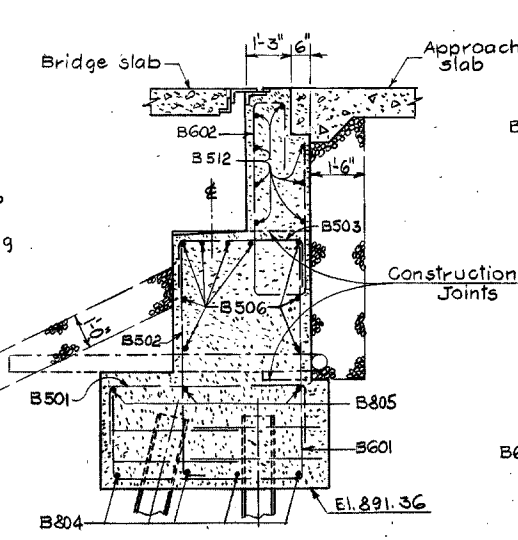
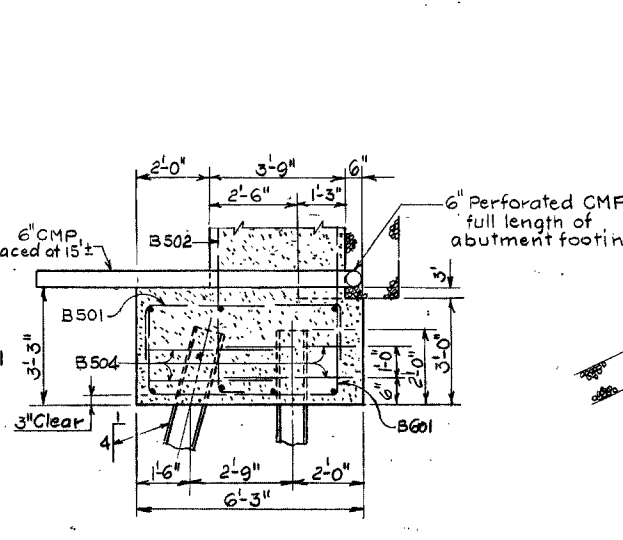
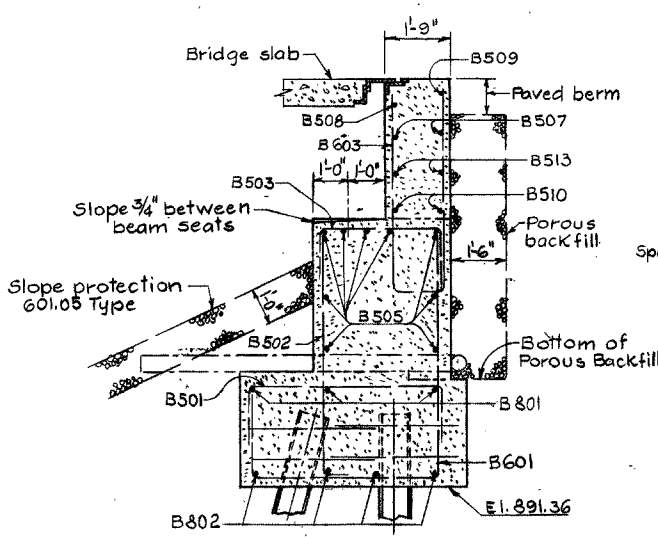
DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE	REVISED
F.H.S.	L.P.				12/7/65	

FED. RD. DIVISION	STATE	PROJECT
2	OHIO	

WAY-3-14.72
WAY-76-1204



NOTE:
CONCRETE shall be Class "E".
BRIDGE SEAT REINFORCING: Special care shall be taken in placing reinforcing steel in the vicinity of the bridge seat so as to avoid interference with the drilling of the anchor bar holes.
POROUS BACKFILL shall extend upward to the approach slab, paved berm and to the surface of the earth shoulders, and outward to the limits as shown on elevation views. Excavation therefor in excess of that required for construction of abutment, shall be considered as paid for in the bid price per Cu. Yd. paid for porous backfill.



SHAFFER, PARRETT AND ASSOCIATES
Consulting Engineers
MANSFIELD, OHIO.

FORWARD ABUTMENT DETAILS
BRIDGE No. WAY-3-1520
OVER APPLE CREEK
WAYNE COUNTY S.R.3
STA. 802+78.67 TO STA. 804+06.83

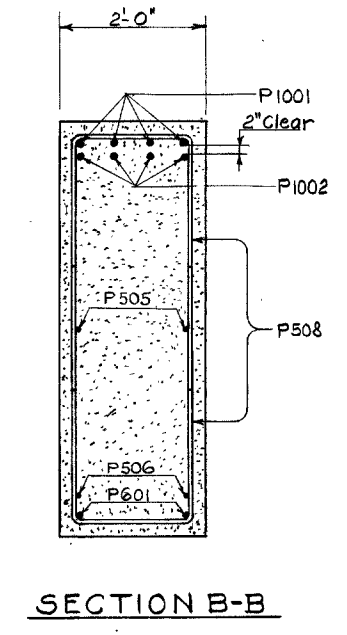
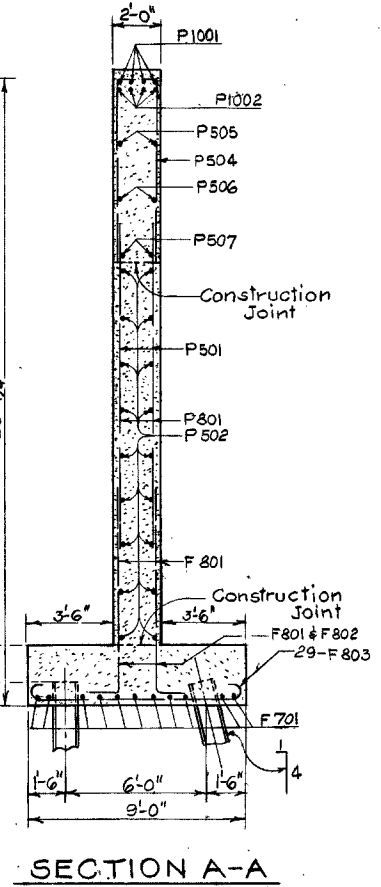
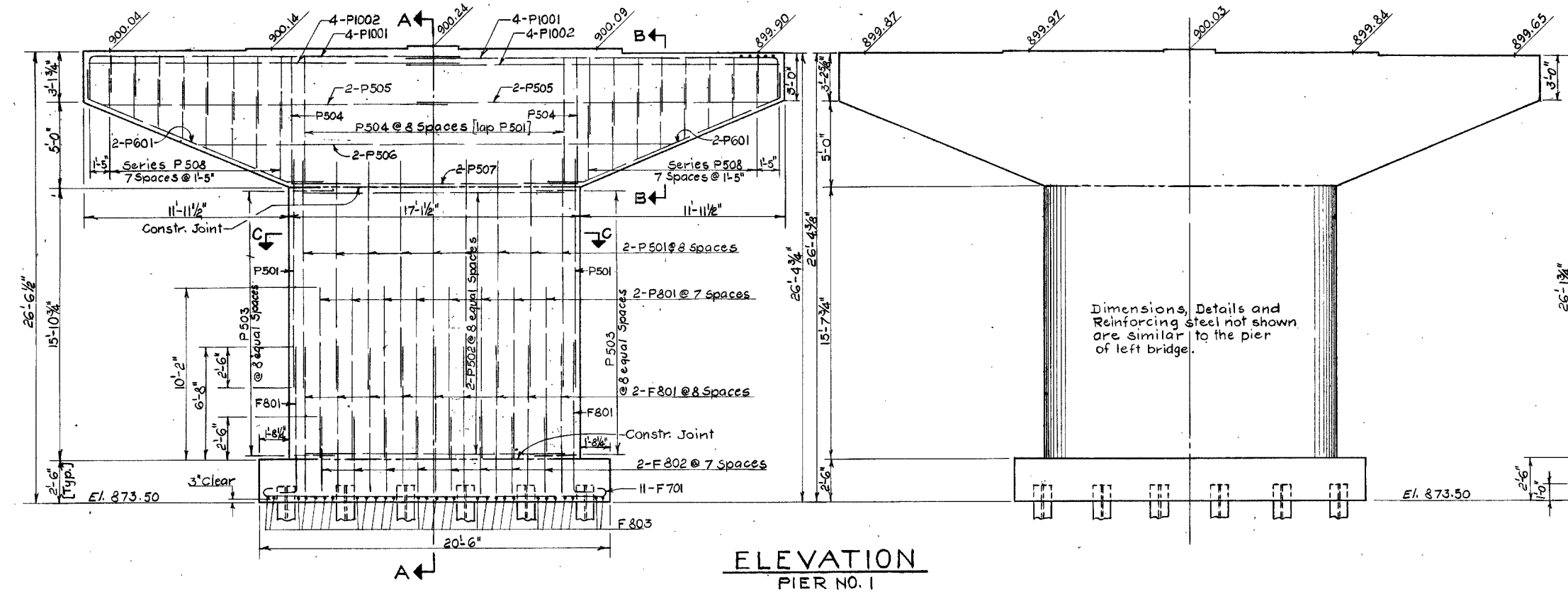
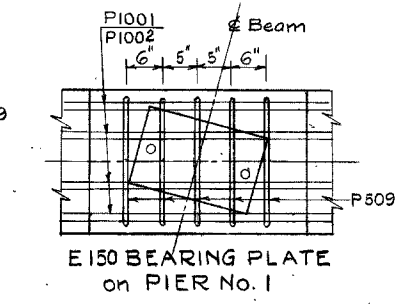
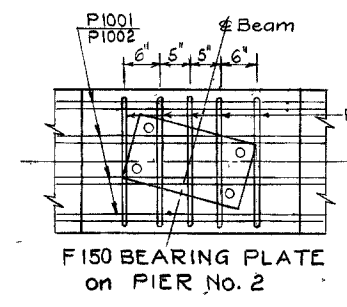
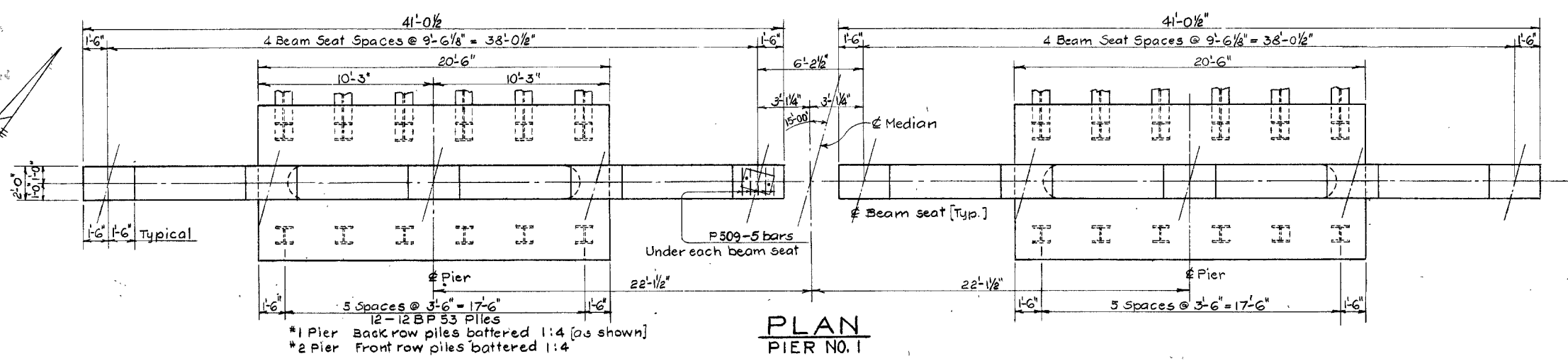
DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE	REVISED
F.H.S.	F.H.S.	L.P.	PH	TK	12/7/65	

APR 26

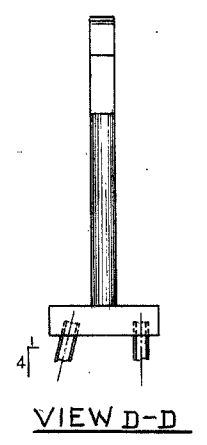
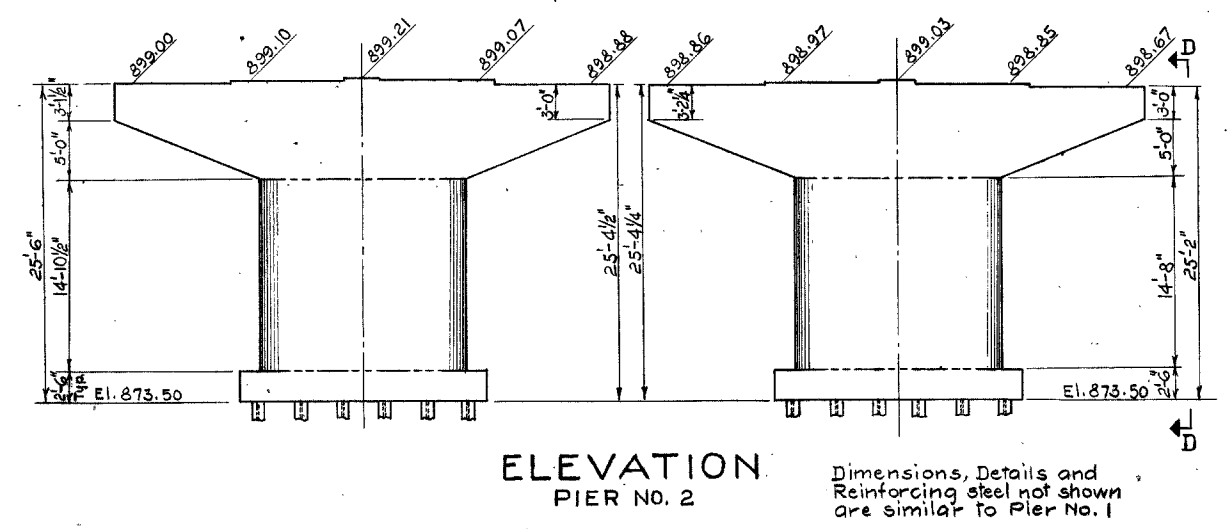
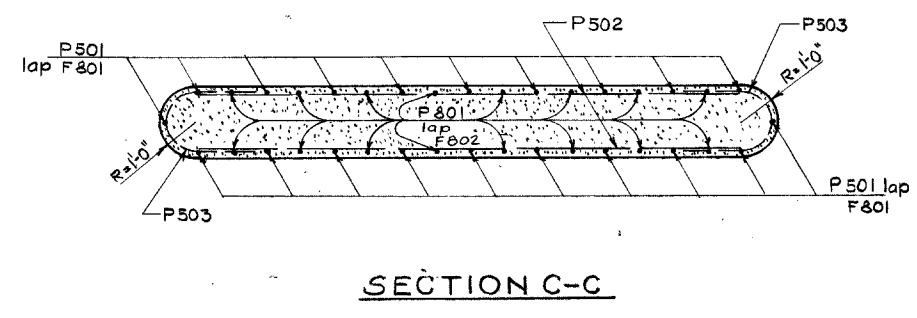
FED. RD. DIVISION	STATE	PROJECT
2	OHIO	

559
693

WAY-3-14.72
WAY-76-12.04



NOTE:
CONCRETE shall be Class "C" for caps and stems and Class "E" for footing.
BRIDGE SEAT REINFORCING: Special care shall be taken in placing reinforcing steel in the vicinity of the bridge seat so as to avoid interference with the drilling of anchor bar holes.



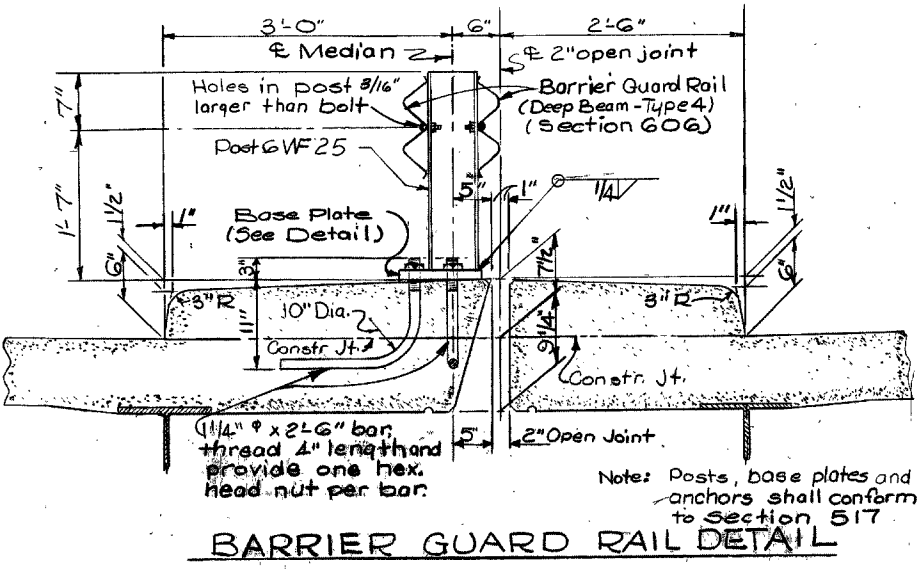
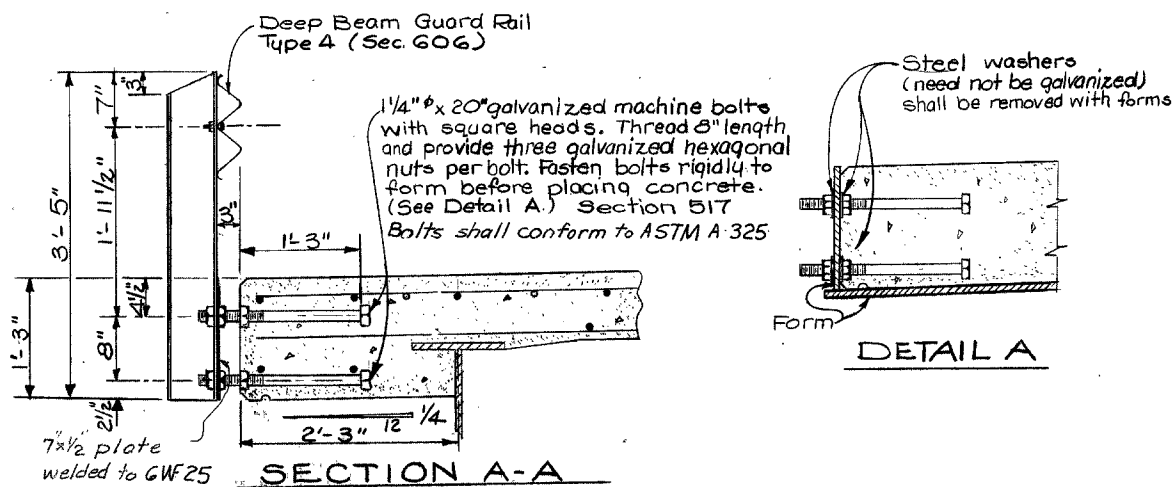
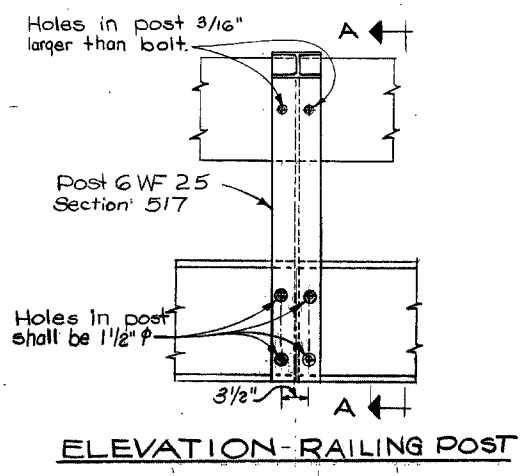
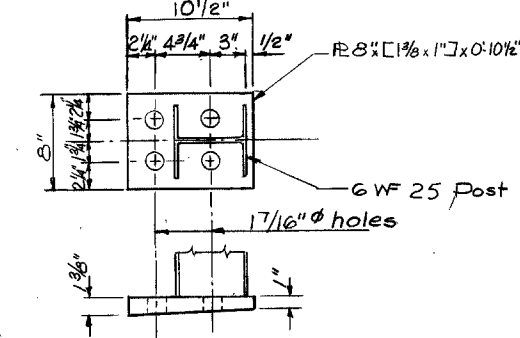
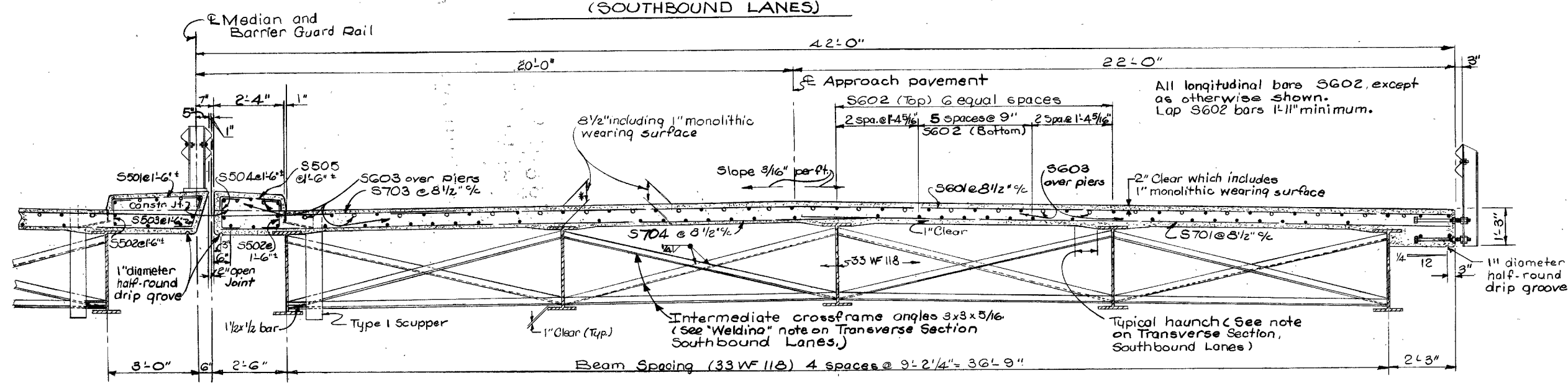
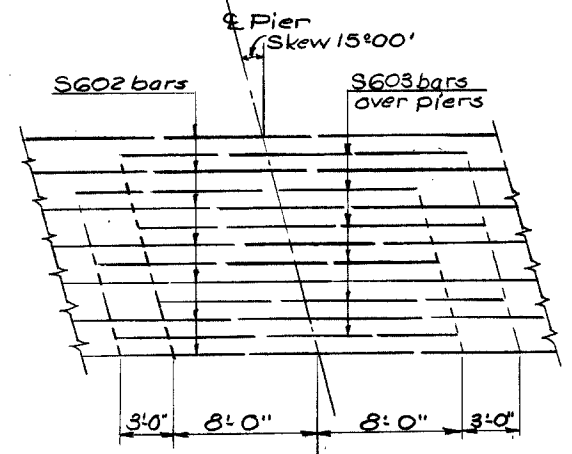
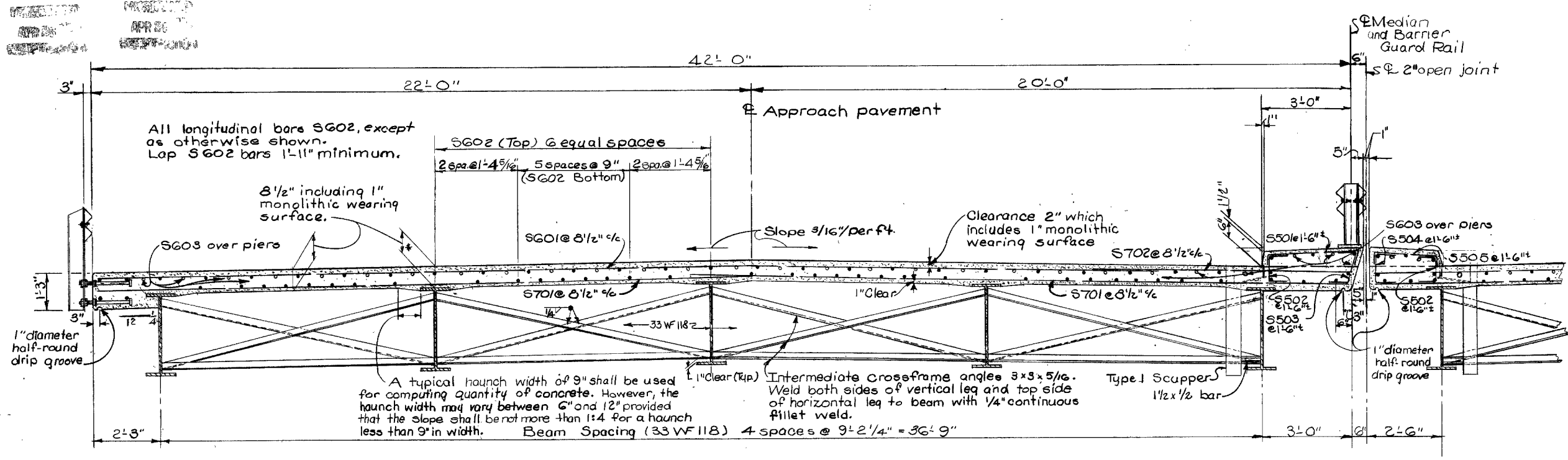
SHAFFER, PARRETT AND ASSOCIATES Consulting Engineers MANSFIELD, OHIO.						
PIER DETAILS BRIDGE NO. WAY-3-1520 OVER APPLE CREEK WAYNE COUNTY S.R. 3 STA. 802+78.67 To STA. 804+06.83						
SCALE	DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE
	F.H.S.	L.P.				12/1/65

WAY-3-14.72
WAY-76-1204

- NOTES -

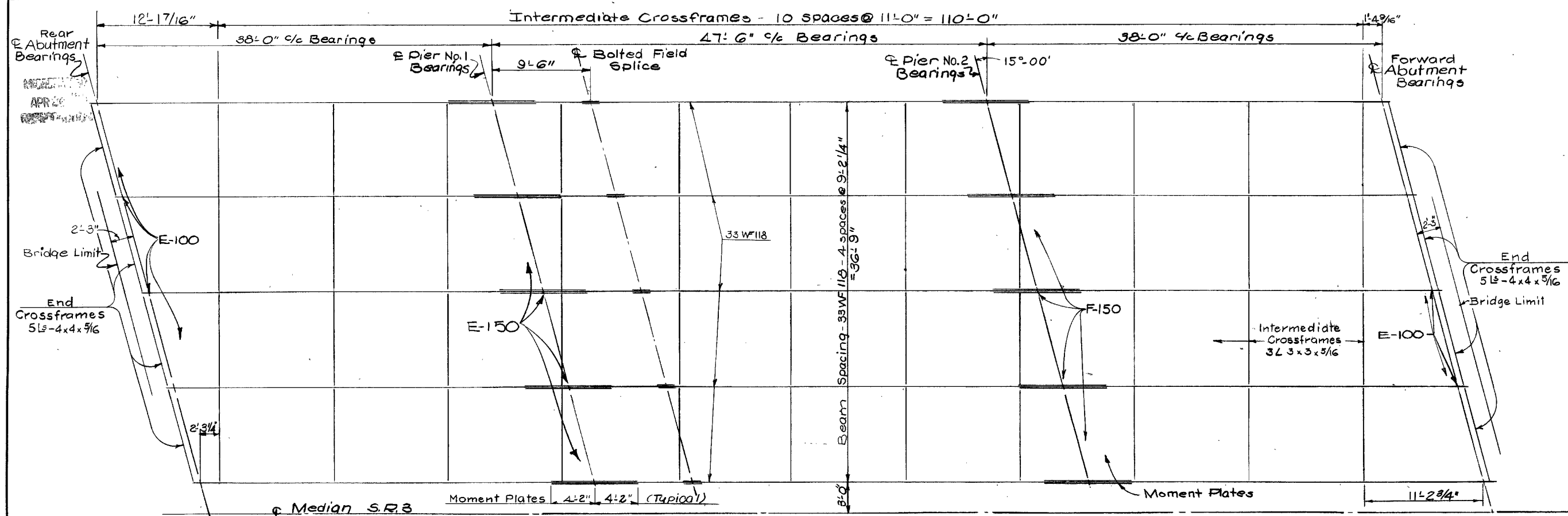
CONCRETE shall be Class "C". $f_c = 1333 \text{ p.s.i.}$

* This is the nominal dimension. The quantity of deck concrete to be paid for shall be based upon this dimension, even though deviation from it may be necessary because the top flange of the beam may not have the exact camber or conformation required to place it parallel to the finished grade. Deduction shall be made for volume of encased steel plates as per Sec. 511.19 of the Construction and Material Specifications.

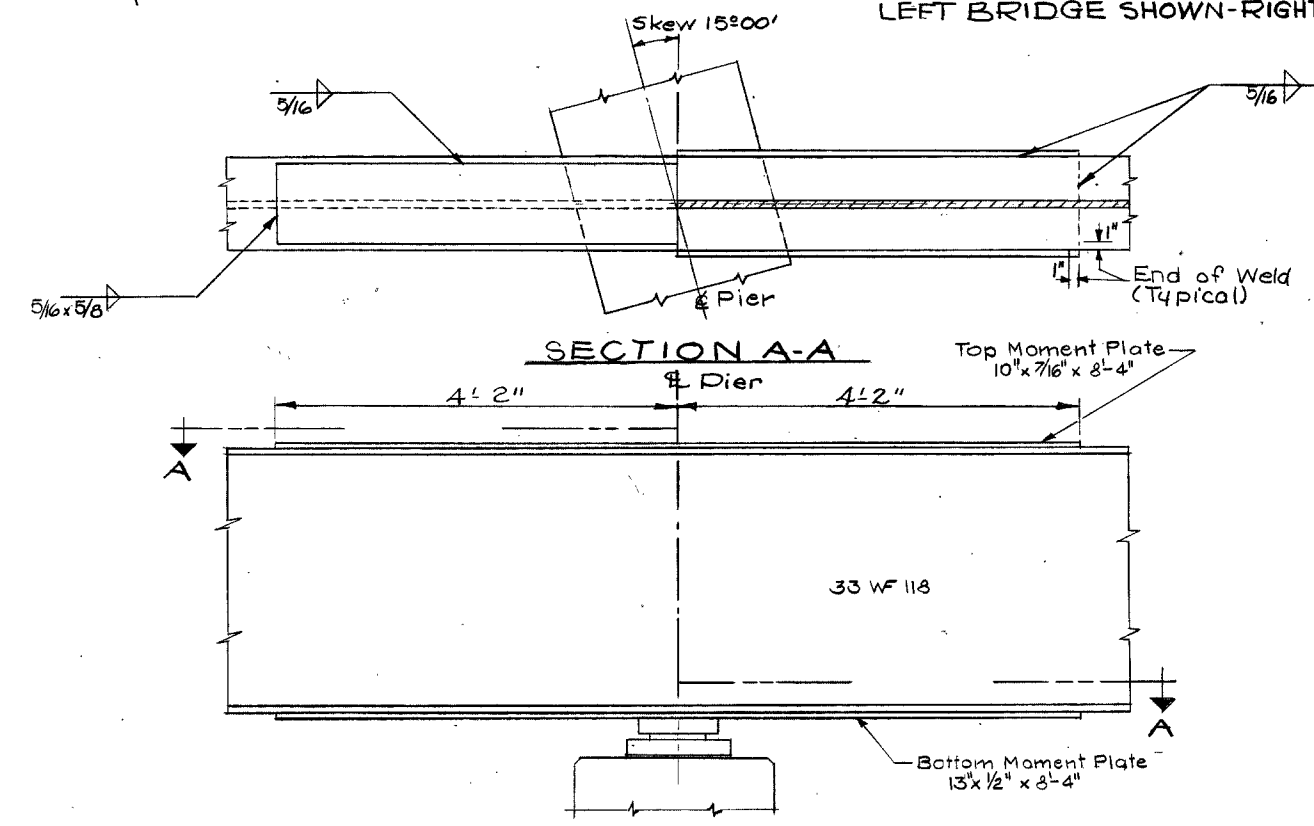


SHAFER, PARRETT AND ASSOCIATES Consulting Engineers MANSFIELD, OHIO.					
SUPERSTRUCTURE DETAILS BRIDGE NO. WAY-3-1520 OVER APPLE CREEK					
WAYNE COUNTY				S.R.3	
STA. 802+78.67 TO STA. 804+06.83					
DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE
F.H.S.	P.J.M.				12/1/5

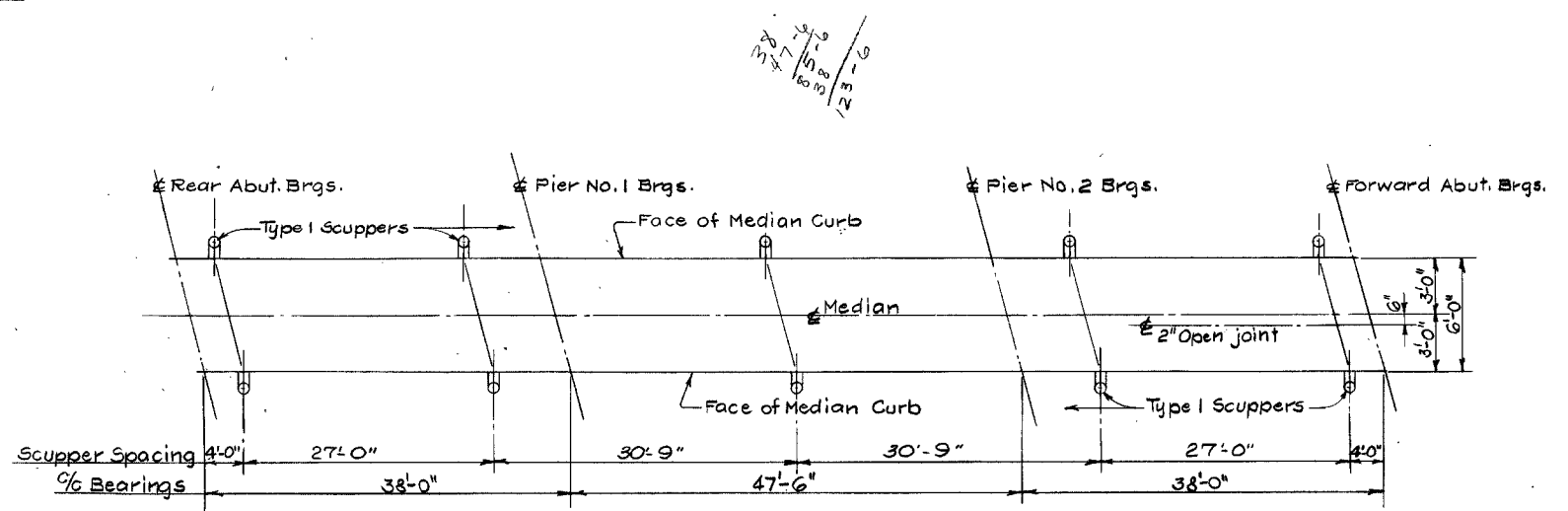
WAY-3-14.72
WAY-76-12.04



NOTE:
For beam splice details see Standard Drawing SD-2-64.
For scupper details see Type I Scupper Standard Drawing SD-1-63 [Sheet 3], a modification consisting of the elimination of the bulb angle gutter shown is intended to apply to this sheet.



ELEVATION MOMENT PLATE DETAILS



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SUPERSTRUCTURE DETAILS
BRIDGE NO. WAY-3-1520
over APPLE CREEK
WAYNE COUNTY S.R.3
STA. 802+78.67 To STA. 804+06.83

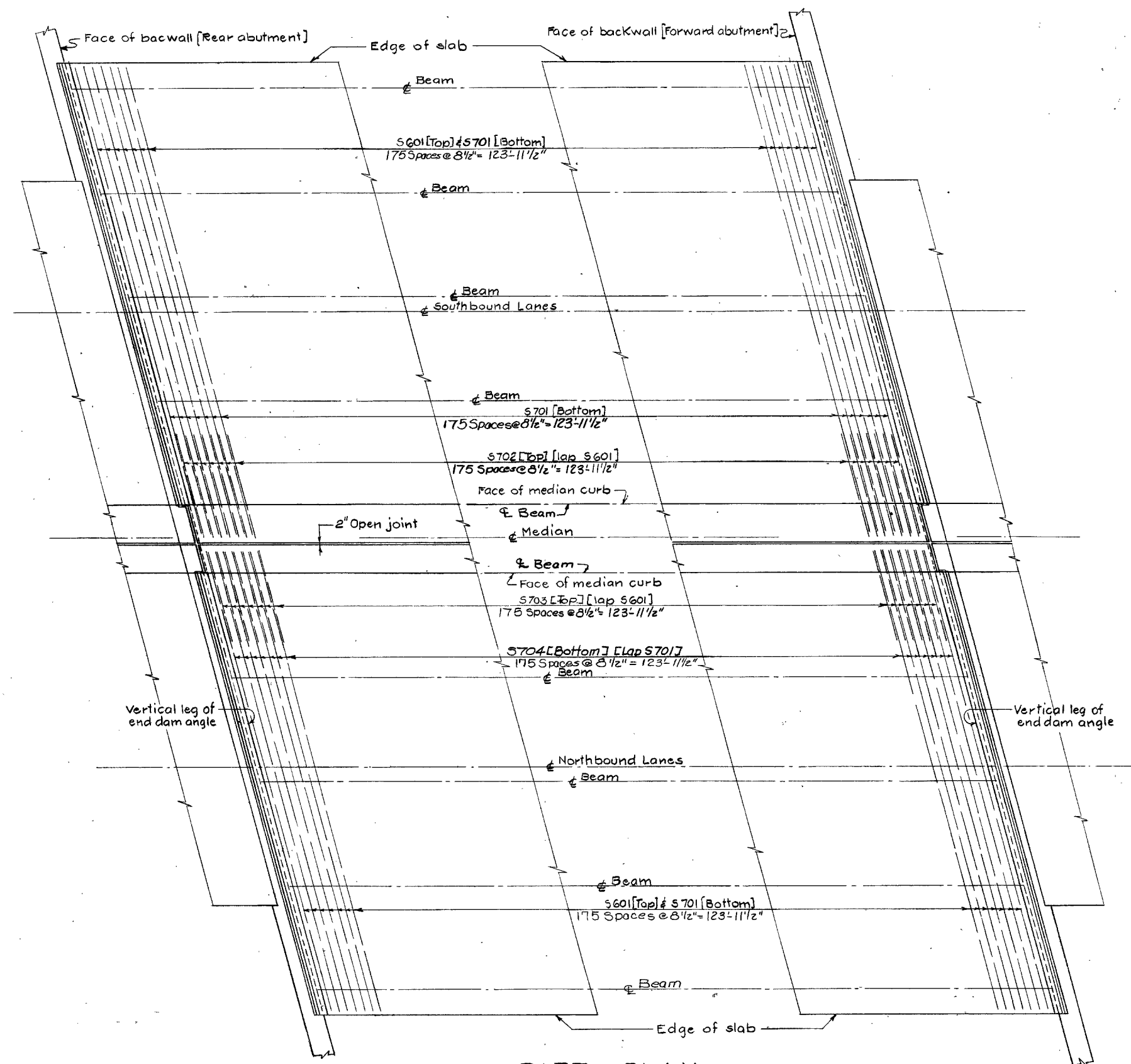
SCALE: DATE

DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE	REVISION
F.H.S.	P.J.M. L.R.		804	NV	12/7/65	

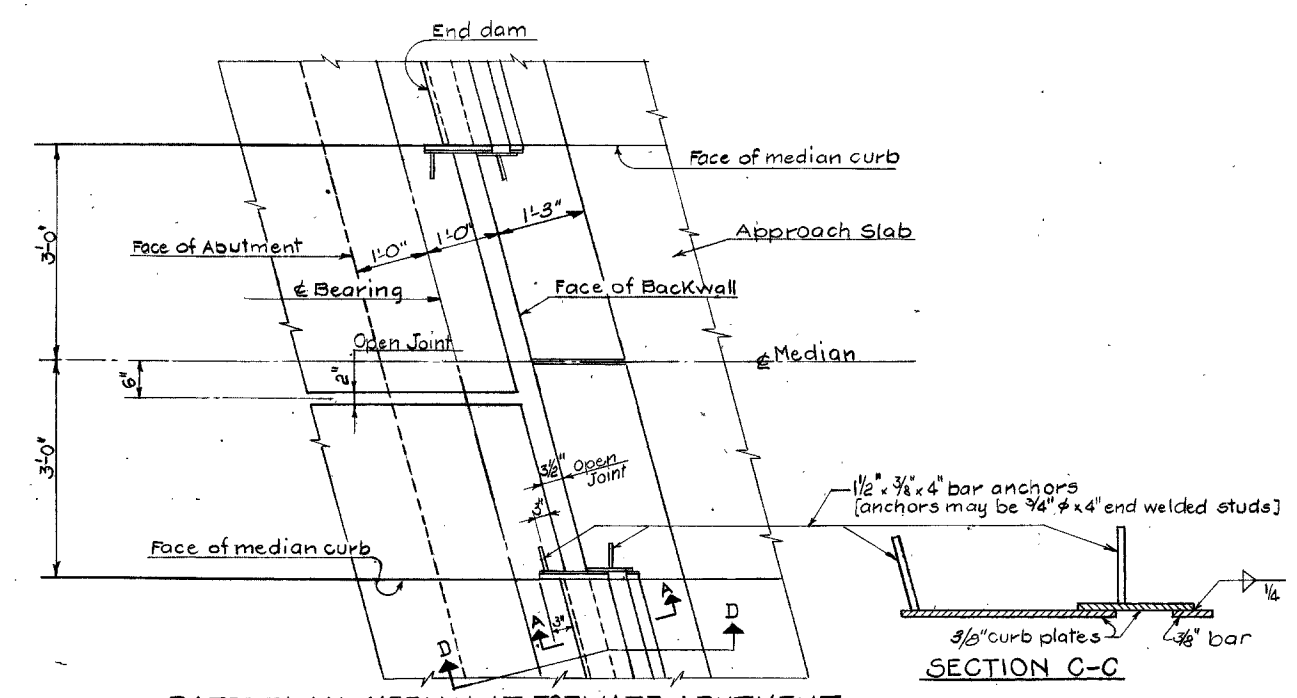
APR 22 1965

FED. RD. DIVISION	STATE	PROJECT	552 693
2	OHIO		

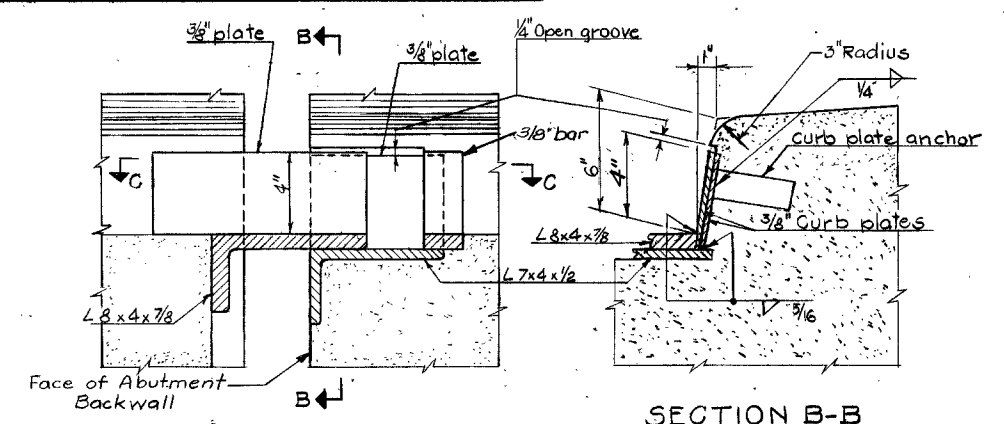
WAY-3-14.72
WAY-76-1204



PART PLAN
TRANSVERSE REINFORCING STEEL
IN RIGHT and LEFT BRIDGE

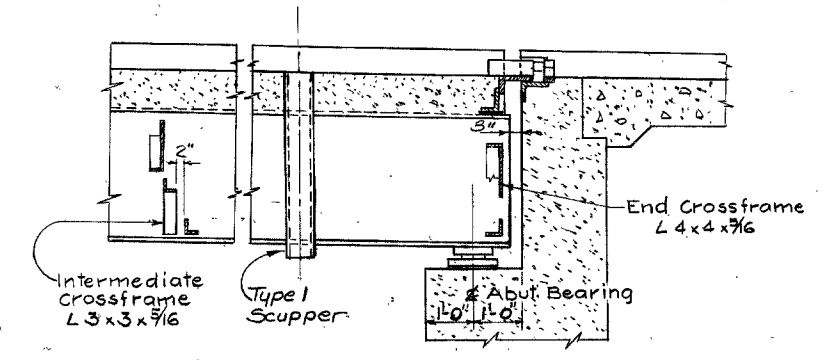


PART PLAN-MEDIAN AT FORWARD ABUTMENT.



SECTION A-A
SECTION B-B
MEDIAN CURB PLATES

NOTE:
For additional Superstructure
details see Standard Drawing SD-1-63
(Sheet 2)



SECTION D-D

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SUPERSTRUCTURE DETAILS
BRIDGE NO. WAY-3-1520
OVER APPLE CREEK

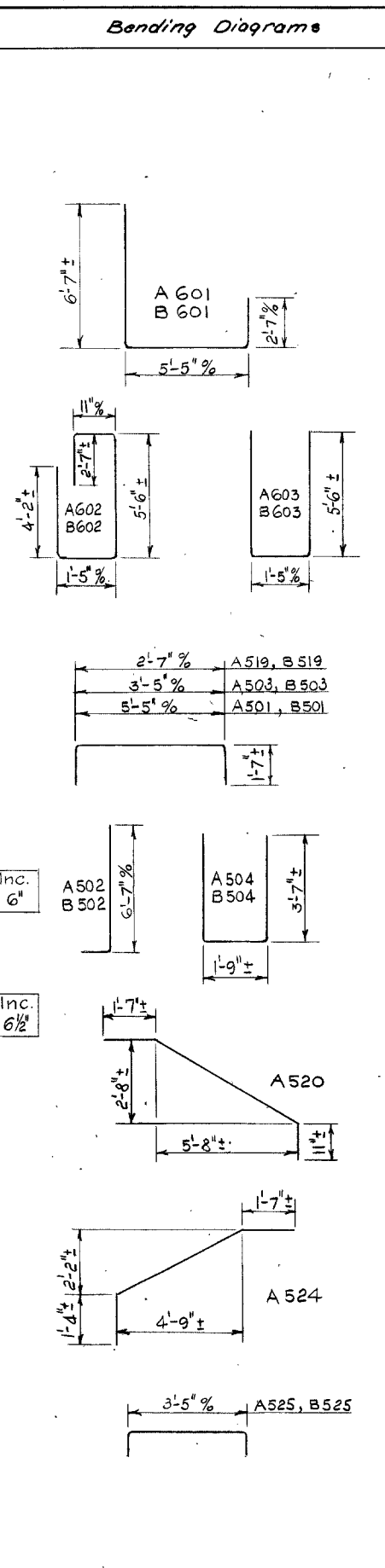
WAYNE COUNTY S.R.3
STA. 802+78.67 TO STA. 804+06.83

DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE	REVISED
F.H.S.	L.P.				12/7/65	

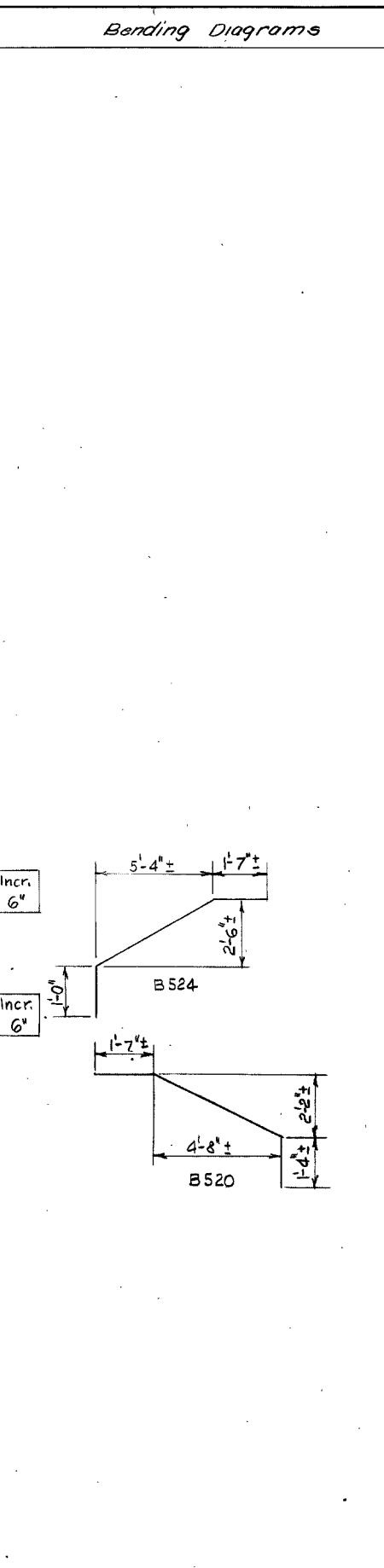
WAY-3-14.72
WAY-76-12.04

REINFORCING STEEL LIST

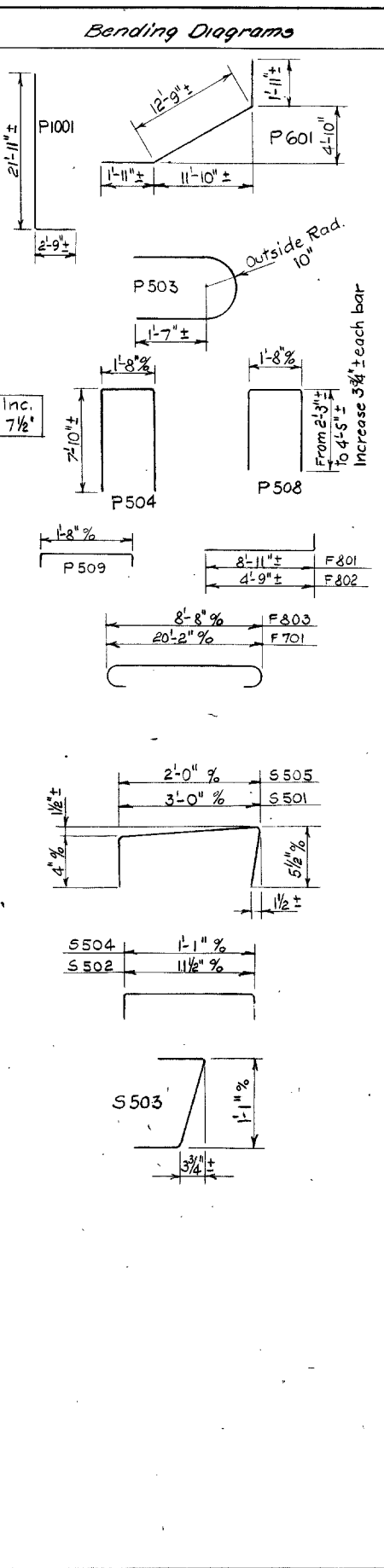
Bar Number	Number Required	Length	Shape	Weight
REAR ABUTMENT				
A 801	6	22'-6"	—	360
A 802	4	18'-6"	—	198
A 803	4	26'-6"	—	283
A 804	4	27'-5"	—	293
A 805	6	23'-3"	—	372
A 806	4	19'-2"	—	205
A 601	68	14'-3"	U	1455
A 602	66	14'-0"	U	1388
A 603	23	12'-0"	U	415
A 501	68	8'-4"	U	591
A 502	68	7'-1"	U	502
A 503	66	6'-4"	U	436
A 504	48	8'-8"	U	434
A 505	18	22'-4"	—	419
A 506	18	22'-1"	—	415
A 507	2	24'-8"	—	51
A 508	1	23'-0"	—	24
A 509	2	13'-1"	—	27
A 510	12	26'-4"	—	330
A 511	1	9'-11"	—	10
A 512	10	27'-1"	—	282
A 513	2	26'-2"	—	55
A 514	2	24'-6"	—	51
A 515	1	22'-10"	—	24
A 516	2	13'-2"	—	27
A 517	1	9'-8"	—	10
A 518	1	3'-8"	—	4
A 519	4	5'-6"	—	23
A 520	2	8'-7"	—	18
A 521	6	12'-2"	—	76
A 522	2 series of 6 bars	4'-11" to 7'-5"	—	77
A 523	4	7'-11"	—	33
A 524	2	8'-0"	—	17
A 525	60	4'-5"	—	276
A 526	4	7'-5"	—	31
A 527	2 series of 5 bars	5'-0" to 7'-2"	—	63
A 528	6	9'-7"	—	60
TOTAL REAR ABUTMENT				9,335



Bar Number	Number Required	Length	Shape	Weight
FORWARD ABUTMENT				
B 801	6	22'-6"	—	360
B 802	4	18'-6"	—	198
B 803	4	26'-6"	—	283
B 804	4	26'-8"	—	285
B 805	6	22'-8"	—	363
B 806	4	18'-8"	—	199
B 601	67	14'-3"	U	1434
B 602	66	14'-0"	U	1388
B 603	22	12'-0"	U	397
B 501	67	8'-4"	U	582
B 502	67	7'-1"	U	495
B 503	66	6'-4"	U	436
B 504	48	8'-8"	U	434
B 505	18	22'-4"	—	419
B 506	18	22'-1"	—	415
B 507	2	24'-9"	—	52
B 508	1	22'-11"	—	24
B 509	2	12'-11"	—	27
B 510	10	26'-7"	—	277
B 511	1	9'-8"	—	10
B 512	12	26'-6"	—	332
B 513	2	26'-5"	—	55
B 514	2	24'-11"	—	52
B 515	1	23'-4"	—	24
B 516	2	13'-0"	—	27
B 517	1	10'-3"	—	11
B 518	1	3'-7"	—	4
B 519	4	5'-6"	—	23
B 520	2	7'-11"	—	17
B 521	6	10'-11"	—	68
B 522	2 series of 5 bars	5'-1" to 7'-1"	—	63
B 523	6	7'-5"	—	46
B 524	2	8'-4"	—	17
B 525	60	4'-5"	—	276
B 526	4	7'-9"	—	32
B 527	2 series of 6 bars	5'-1" to 7'-7"	—	79
B 528	6	10'-1"	—	63
TOTAL FORWARD ABUTMENT				9,267
TOTAL TWO ABUTMENTS				18,602



Bar Number	Number Required	Length	Shape	Weight
ONE PIER				
P 1001	8	24'-5"	—	841
P 1002	8	21'-11"	—	754
P 801	16	10'-2"	—	434
P 601	4	16'-5"	—	99
P 501	20	13'-4"	—	278
P 502	18	15'-1"	—	283
P 503	18	5'-8"	—	106
P 504	11	17'-1"	—	196
P 505	4	20'-6"	—	86
P 506	2	27'-10"	—	58
P 507	2	16'-11"	—	35
P 508	4 series of 8 bars	5'-11" to 10'-3"	—	270
P 509	25	2'-8"	—	70
F 801	20	9'-10"	—	525
F 802	16	5'-8"	—	242
F 803	29	10'-10"	—	839
F 701	11	21'-10"	—	491
TOTAL ONE PIER				5,607
TOTAL FOUR PIERS				22,428
SUPERSTRUCTURE (S.B. & N.B.)				
S 701	528	22'-8"	—	24,463
S 702	176	9'-1"	—	3,268
S 703	176	8'-2"	—	2,938
S 704	176	22'-1"	—	7,944
S 601	352	36'-7"	—	19,342
S 602	544	32'-8"	—	26,692
S 603	108	19'-0"	—	3,082
S 501	84	3'-7"	—	314
S 502	168	2'-0"	—	350
S 503	84	2'-1"	—	183
S 504	84	2'-1"	—	183
S 505	84	2'-7"	—	226
TOTAL TWO SUPERSTRUCTURES				88,985
REPLACEMENT STEEL				
RE 1001	1	7'-2"	—	
RE 801	1	6'-6"	—	
RE 701	2	6'-2"	—	
RE 601	3	5'-11"	—	
RE 501	1	5'-7"	—	



NOTE:
BAR SIZE is indicated in the bar mark. The first digit where three digits are used, and the first two digits where four are used, indicate the bar size number. For example, A 501 is a No. 5 size bar and P 1001 is a No. 10 size.

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REINFORCING STEEL LIST
BRIDGE NO. WAY-3-1520
OVER APPLE CREEK
WAYNE COUNTY S.R.3
STA. 802+78.67 TO STA. 804+06.83

DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE	REVISION
F.H.S.	L.P.	L.P.	8/27	W.C.	12/1/65	