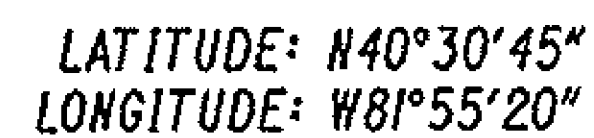


PLAN NO.

FEDERAL PROJECT NO.

NON-FEDERAL

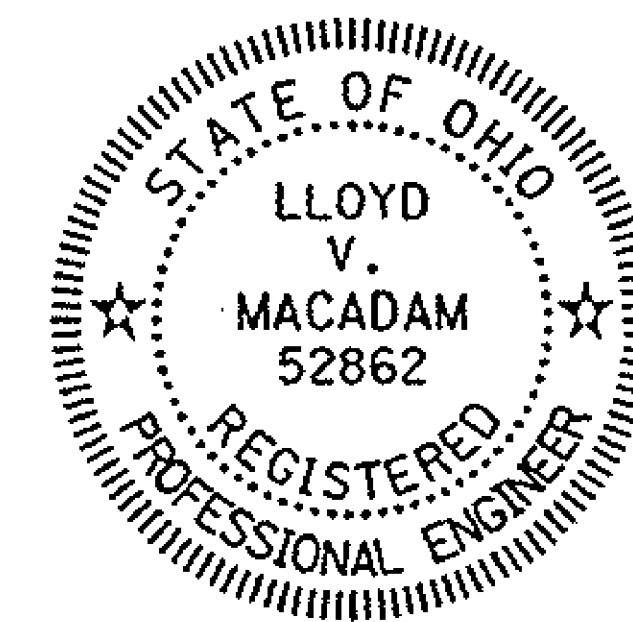


PORTIONS TO BE IMPROVED

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ENGINEERS SEAL:



SIGNED: [Signature]
DATE: 9/17/09

Project Earth Disturbed Area - N/A (Maintenance Project)
Estimated Contractor Earth Disturbed Area - N/A (Maintenance Project)
Notice of Intent Earth Disturbed Area - N/A (Maintenance Project)

The Standard 2008 Specifications of the State of Ohio, Department of Transportation, including changes and Supplemental Specifications listed in the plans and proposal govern these improvements.

I hereby approve these plans and declare that the making of these improvements will require the closing of the highways to traffic on Parts No. none and that detours will be provided by State forces. The closing to traffic of the highways will not be required on Part Nos. 1, 2, 3, & 4 and provisions for the maintenance and safety of traffic will be as indicated in the proposal.

APPROVED
DATE 9/12/09

Richard A. Bible DE
DISTRICT DEPUTY DIRECTOR

APPROVED
DATE 9-24-01

Salerno M. Molitoris
DIRECTOR, DEPARTMENT OF TRANSPORTATION

FILE NO. 84166

CONSTRUCTION PROJECT NO.

OHIO CENTRAL RAILROAD
RAILROAD INVOLVEMENT

HOL-93-0.00

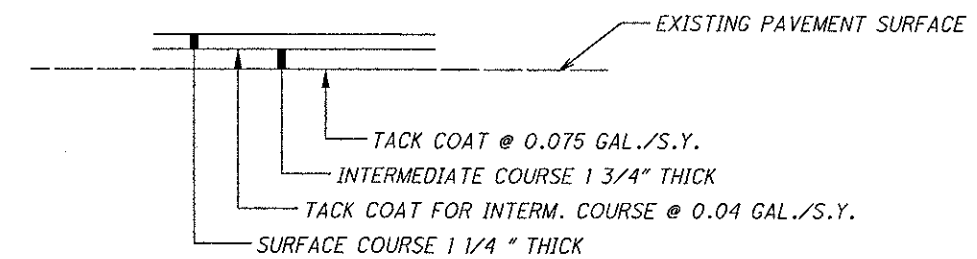
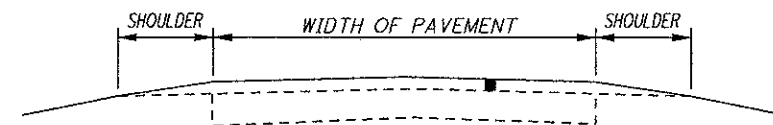
1/17

HOL - SR-93-0.00
090543 PID - 84166
Dist 11 12/16/2009

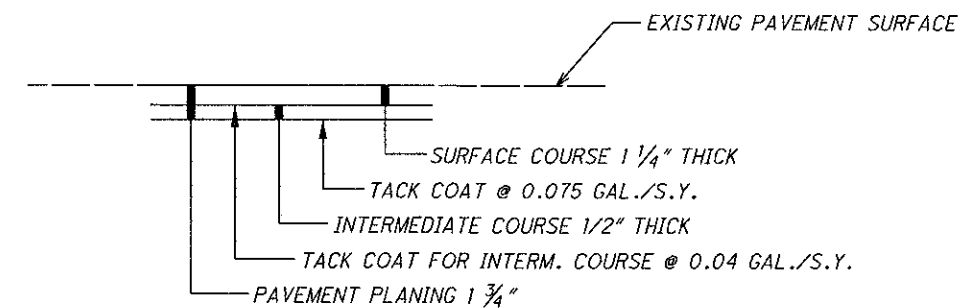
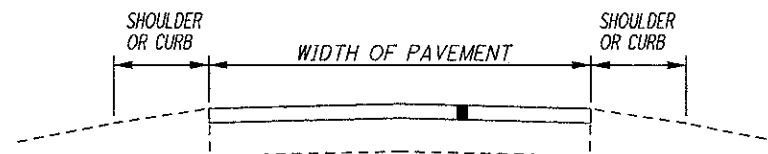
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FOR PAVEMENT AND SHOULDER WIDTHS SEE SHEETS 6-10

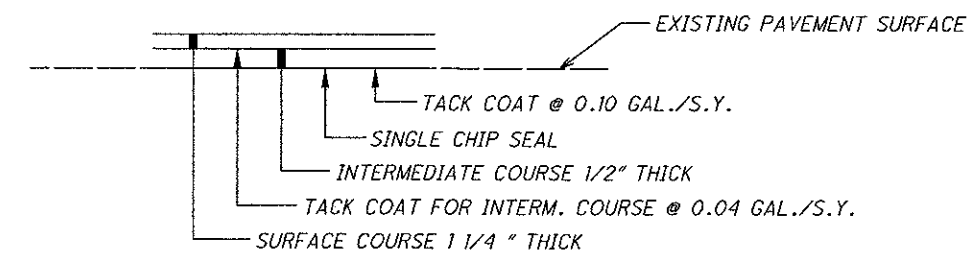
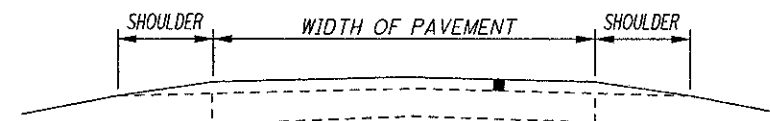
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1	HOL-93, 0.00-4.18
2	HOL-514, 0.00-5.73
3	TUS-93, 0.00-1.08
4	TUS-651, 0.00-0.45



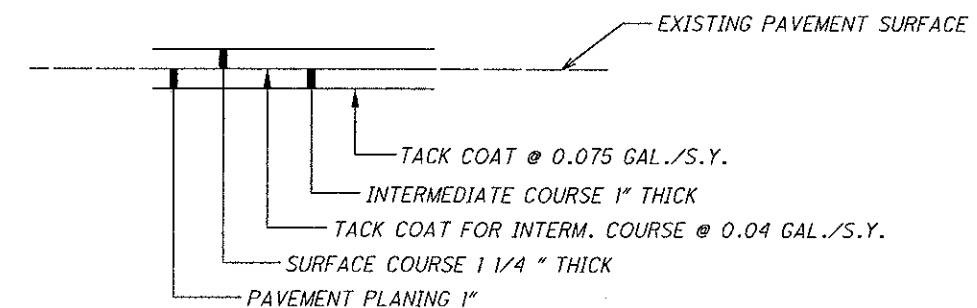
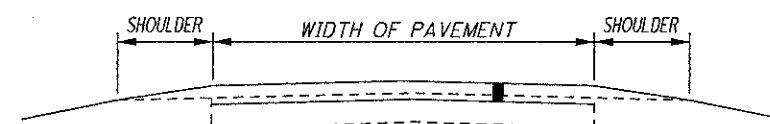
TYPICAL SECTION 1 -- Part 1 SLM 0.17-4.18
Part 3 SLM 0.00-0.49



TYPICAL SECTION 2 -- Part 1 SLM 0.00-0.17
Part 2 SLM 5.46-5.73
Part 3 SLM 0.49-1.08
Part 4 SLM 0.30-0.45



TYPICAL SECTION 3 -- Part 2 SLM 0.00-5.46



TYPICAL SECTION 4 -- Part 4 SLM 0.00-0.30

TYPICAL SECTIONS

HOL-93-0.00

MAINTENANCE OF TRAFFIC

A MINIMUM OF ONE LANE OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES PER ITEM 614 AND AS PER SCD MT-97.11. THE LENGTH OF RESTRICTED TRAFFIC LANES SHALL BE KEPT TO A MINIMUM CONSISTENT WITH THE SPECIFICATION REQUIREMENTS FOR THE PROTECTION OF WORK ITEMS WHICH NECESSITATE THE RESTRICTION. THE LIMITS AND DURATION OF LANE CLOSURES SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.

THE PLANING AND RESURFACING WILL PROCEED CONTINUOUSLY A MINIMUM OF FIVE (5) DAYS PER WEEK, WEATHER PERMITTING, EXCEPT THE HOLIDAYS AND EVENTS LISTED BELOW. ANY OPEN PAVEMENT TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH BARRICADES, DRUMS, VERTICAL PANELS OR PORTABLE CONCRETE BARRIER.

THE CONTRACTOR SHALL FURNISH, INSTALL, AND MAINTAIN W8-H15 "GROOVED PAVEMENT" SIGNS FOR EACH STATE ROUTE AND COUNTY ROAD APPROACH TO A PLANED SURFACE AT A LOCATION DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL FURNISH, INSTALL, AND MAINTAIN SIGNS W8-1 (48"x48") "BUMP" AND W8-2 (48"x48") "DIP" WITH W13-1 (24"x24") ADVISORY SPEED PLAQUE WITH SPEEDS APPROVED BY THE ENGINEER FOR ALL BUTT JOINT LOCATIONS, WHILE THE BUMP OR DIP CONDITION EXISTS.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

MEMORIAL DAY
FOURTH OF JULY
LABOR DAY

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 12:00N MONDAY
MONDAY	12:00N FRIDAY THROUGH 12:00N TUESDAY
TUESDAY	12:00N MONDAY THROUGH 12:00N WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 12:00N THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 12:00N MONDAY
FRIDAY	12:00N THURSDAY THROUGH 12:00N MONDAY
SATURDAY	12:00N FRIDAY THROUGH 12:00N MONDAY

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES IN ACCORDANCE WITH CMS 108.07.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

WHEN RAISED PAVEMENT MARKERS ARE TO BE INSTALLED, THE REQUIRED LANE CLOSURE SHALL REMAIN IN EFFECT UNTIL THE EPOXY IS DRY AND ALL FOREIGN MATTER OR DEBRIS CREATED BY THE INSTALLATION OF THE RPM CASTING IS REMOVED FROM THE ROADWAY.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614 - WORK ZONE PAVEMENT MARKINGS AND SIGNS

THE CONTRACTOR SHALL INSTALL ITEM 614 - WORK ZONE CENTER LINE, CLASS 1, 642 PAINT PRIOR TO OPENING THE LANE TO TRAFFIC, OR WHEN THE EXISTING MARKINGS HAVE BEEN COVERED OR DAMAGED, AS PER CMS 614.11.

IN THE EVENT THE CONTRACTOR CANNOT INSTALL THE WORK ZONE CENTER LINE, CLASS 1, 642 PAINT DUE TO CONDITIONS BEYOND HIS CONTROL, AN ESTIMATED CONTINGENCY QUANTITY OF "DO NOT PASS" (R4-1) AND "PASS WITH CARE" (R4-2) SIGNS HAVE BEEN PROVIDED BELOW.

WORK ZONE CENTER LINE, CLASS 1, 642 PAINT, MARKINGS SHALL BE PLACED, AND THE ABOVE SIGNS REMOVED BY THE END OF THE CONTRACTOR'S NEXT WORK DAY. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

(PART 1) 614, WORK ZONE CENTER LINE, CLASS 1, 642 PAINT - 8.53 MILE
(PART 2) 614, WORK ZONE CENTER LINE, CLASS 1, 642 PAINT - 11.78 MILE
(PART 3) 614, WORK ZONE CENTER LINE, CLASS 1, 642 PAINT - 2.75 MILE
(PART 4) 614, WORK ZONE CENTER LINE, CLASS 1, 642 PAINT - 1.34 MILE

24.40 MILE

THE CONTRACTOR SHALL ERECT "NO EDGE LINE" (W8-H12a) SIGNS IN ADVANCE OF ANY SECTION OF ROADWAY LACKING CMS STANDARD EDGE LINE MARKINGS, AS PER CMS 614.04.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR WORK ZONE MARKING SIGNS PER THE REQUIREMENTS ABOVE AND ITEM 614 OF THE SPECIFICATIONS.

614, WORK ZONE MARKING SIGN "DO NOT PASS" - 20 EACH
614, WORK ZONE MARKING SIGN "PASS WITH CARE" - 20 EACH
614, WORK ZONE MARKING SIGN "NO EDGE LINE" - 40 EACH

80 EACH

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

- 1) FOR DIRECTING TRAFFIC IN CARROLLTON AT THE INTERSECTIONS OF SR 39 AND SR 43, ALSO MAIN STREET AND LISBON STREETS.
- 2) FOR PATROLLING ANY OR ALL OF THE PROJECT AREAS WITHIN THE VILLAGE OF CARROLLTON OR AS DEEMED APPROPRIATE BY THE ENGINEER.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE ----- 80 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED. ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE

TEMPORARY ORANGE PLASTIC/NYLON CONSTRUCTION FENCE SHALL BE PLACED FOR THE PROTECTION OF PEDESTRIAN TRAFFIC. IT SHALL BE SECURELY FASTENED TO WOOD OR METAL POST AT NOT MORE THAN 6' SPACING. IT SHALL BE NOMINALLY 42" HIGH AND THE TOP EDGE SHALL NOT SAG BELOW 30". IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSURE THAT THE FENCE IS IN GOOD CONDITION AND PROPERLY PLACED AND MAINTAINED.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 607 - FENCE MISC.: ORANGE PLASTIC CONSTRUCTION FENCE - 736 FT

CALCULATED
JAG
CHECKED
LVM

GENERAL NOTES

HOL-93-0.00

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SHOULDER PREPARATION

THIS WORK WILL BE IN ACCORDANCE WITH CMS ITEM 617, WITH SPECIAL ATTENTION GIVEN TO SECTION 617.04. THE WORK DONE WILL BE IN REASONABLY CLOSE CONFORMITY WITH THE LINES AND TYPICAL SECTIONS SHOWN ON THE PLANS OR AS ESTABLISHED BY THE ENGINEER.

ITEM 408 - PRIME COAT, AS PER PLAN

THE CONTRACTOR WILL APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED COMPACTED AGGREGATE SHOULDER, AS PER PLAN.

SHIELD

THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID BITUMINOUS MATERIAL ONTO THE EDGE OF THE PAVEMENT OR EDGELINE. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO 107.10 OF THE SPECIFICATIONS.

ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN

THE CONTRACTOR WILL UTILIZE MATERIAL (I.E. GRINDINGS) OBTAINED FROM THE PAVEMENT PLANING, ASPHALT CONCRETE, OPERATION. THIS MATERIAL WILL BE PLACED IN LIEU OF THE COMPACTED AGGREGATE. IF THE AMOUNT OF GRINDINGS MATERIAL IS NOT SUFFICIENT TO COVER THE COMPACTED AGGREGATE QUANTITY IN THIS PLAN, THEN ADDITIONAL MATERIAL MEETING SPECIFICATION 617 SHALL BE USED. ALL SPECIFICATIONS FOR ITEM 617 APPLY.

ITEM 642 - TRAFFIC PAINT

THE CONTRACTOR SHALL REPLACE THE EXISTING PAVEMENT MARKINGS WITHIN THE PROJECT LIMITS WITH NEW PAVEMENT MARKINGS AT THE SAME LOCATIONS AS PER CMS 641.06. SEE STANDARD DRAWINGS TC-71.10 AND TC-73.10 FOR PAVEMENT MARKING DETAILS.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

SURFACE COURSE COMPLETION REQUIREMENTS

ANY GIVEN LENGTH OF WORK ON WHICH RESURFACING OPERATIONS HAVE BEEN STARTED IN A CONSTRUCTION SEASON SHALL HAVE THE SURFACE COURSE PLACED THAT SAME SEASON.

NOTIFICATION OF WORK ZONE LANE RESTRICTIONS

THE CONTRACTOR SHALL NOTIFY THE ENGINEER AT LEAST EIGHTEEN (18) DAYS PRIOR TO IMPLEMENTING ANY WORK ZONE RESTRICTIONS THAT WILL REDUCE THE WIDTH OR VERTICAL CLEARANCE OF ANY LANE ON WHICH TRAFFIC WILL BE MAINTAINED DURING CONSTRUCTION.

THE ENGINEER SHALL IMMEDIATELY NOTIFY THE DISTRICT ROADWAY SERVICES MANAGER TO ADVISE THE OFFICE OF HIGHWAY MANAGEMENT OF THE RESTRICTIONS.

COORDINATION OF RESURFACING AND PLANING OPERATIONS

ONCE THE PAVEMENT PLANING OPERATIONS HAVE BEGUN, IT SHALL PROCEED CONTINUOUSLY UNTIL ALL ELEMENTS OF THE WORK ASSOCIATED WITH THE PAVEMENT PLANING OPERATIONS ARE COMPLETED. THE PAVEMENT PLANING OPERATION SHALL BE COMPLETED IN A TIMELY MANNER AS DIRECTED BY THE ENGINEER. IF PAVING THE ASPHALT CONCRETE DIRECTLY ONTO PORTLAND CEMENT, CONCRETE OR BRICK PAVEMENT, TACK THE PAVEMENT WITH RUBBERIZED ASPHALT EMULSION CONFORMING TO CMS 702.13.

ALL GRINDINGS SHALL BECOME THE PROPERTY OF THE CONTRACTOR EXCEPT WHAT IS REQUIRED TO BE USED FOR SHOULDER MATERIAL.

EXTRA FOR WIDENING (PAVEMENT AREA)

AN ADDITIONAL QUANTITY HAS BEEN ADDED TO THE PAVEMENT DATA SHEETS TO BE USED AS DIRECTED BY THE ENGINEER, TO COVER AREAS THAT HAVE BEEN WIDENED ON CURVES OR ON PREVIOUS MAINTENANCE ACTIVITIES BEYOND THE AVERAGE PAVEMENT WIDTH SHOWN.

ITEM 448 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-22 (SPOT LEVELING)

LONGITUDINAL AND TRANSVERSE IRREGULARITIES ARE INTERMITTENTLY PRESENT THROUGHOUT THE EXISTING PAVEMENT SURFACE, BUT THE PAVEMENT DOES NOT REQUIRE A FULL-WIDTH LEVELING COURSE. IRREGULARITIES SHALL BE FILLED WITH 448 IN A MANNER THAT WILL RESULT IN SURROUNDING PORTIONS OF THE EXISTING SURFACE REMAINING EXPOSED AFTER THE SPOT LEVELING COURSE IS PLACED. THE SPOT LEVELING COURSE SHALL BE A VARIABLE DEPTH COURSE WITH A MINIMUM THICKNESS OF 0". THE MATERIAL SHALL BE PLACED IN A SEPARATE OPERATION DIRECTED BY THE ENGINEER.

ITEM 448 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22 (DRIVEWAYS), AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF PAVING ALL EXISTING DRIVEWAYS AND INTERSECTING PUBLIC ROADS NOT OTHERWISE INDICATED. A 2 INCH AVERAGE THICKNESS SHALL BE PLACED ON EXISTING AGGEGATE DRIVES AND APPROACHES OR A 1 1/4 INCH AVERAGE THICKNESS PLACED ON THE EXISTING PAVED DRIVES AND APPROACHES, FOR AN APPROXIMATE DISTANCE OF 10 FEET FOR DRIVEWAYS AND 20 FEET FOR PUBLIC ROADS FROM THE EDGE OF PAVEMENT OR PAVED SHOULDERS, WHICHEVER IS APPLICABLE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. UP GRADE DRIVEWAY PAVING SHALL BE PLACED TO THE BEGINNING OF THE UPSLOPE OF THE DRIVEWAY AS DIRECTED BY THE ENGINEER. ALL GRADING, TACK COAT, PRIME COAT, TOOLS, EQUIPMENT AND INCIDENTALS REQUIRED TO LAYOUT AND PAVE THE DRIVEWAYS AND INTERSECTING PUBLIC ROADS SHALL BE INCLUDED IN THE CU. YD. PRICE BID FOR ITEM 448 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22 (DRIVEWAYS), AS PER PLAN. THE CONTRACTOR'S ATTENTION IS DIRECTED TO CMS 107.10. ALL DRIVEWAYS SHALL BE PAVED WITHIN (5) WORKING DAYS AFTER PLACING OF THE ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M, AS PER PLAN. MATERIALS FURNISHED FOR FINE AND COARSE AGGREGATES USED IN THIS ITEM SHALL EXCLUDE ALL STONE AND CRUSHED CARBONATE STONE.

ITEM 448 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M, AS PER PLAN

MATERIALS FURNISHED FOR FINE AND COARSE AGGREGATES USED IN THIS ITEM SHALL EXCLUDE ALL STONE AND CRUSHED CARBONATE STONE.

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

CALCULATED
JAG
CHECKED
LVM

GENERAL NOTES

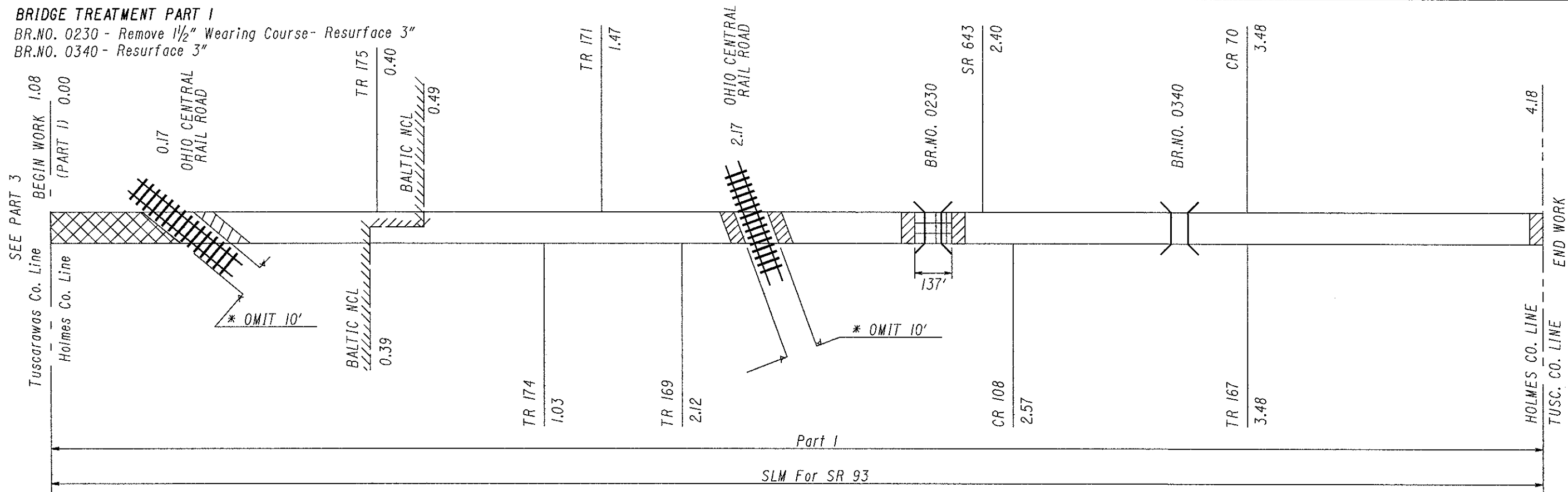
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BRIDGE TREATMENT PART 1

BR.NO. 0230 - Remove 1 1/2" Wearing Course- Resurface 3"

BR.NO. 0340 - Resurface 3"



ITEM 202 - WEARING COURSE REMOVED
BUTT JOINT OR FEATHER AS PER STD. DWG. BP-3.1

ITEM 254 - PAVEMENT PLANING

ITEM 202 - WEARING COURSE REMOVED 1.5"

FOR TYPICAL SECTION DETAILS SEE SHEET 2

PAVEMENT DATA

PROPOSED PAVEMENT

Part	Route	Log Point to Log Point (Straight Line Mileage)		Mile	Feet	Width of Pavement	Typical	Existing Type Pavement	Pavement Area	407		448						202		254		REMARKS		
										Tack Coat @ 0.075 gal/s.y.	Tack Coat For Intermediate Course @ 0.04 gal/s.y.	In.	Asphalt Concrete Intermediate Course, Type 1, PG64-22, (Spot Leveling)	In.	Asphalt Concrete Intermediate Course, Type 2, PG64-22, (Spot Leveling)	In.	Asphalt Concrete Surface Course, Type 1, PG 70-22M, As Per Plan	In.	Asphalt Concrete Surface Course Type 1, PG64-22 (Driveways), As Per Plan	Wearing Course Removed	Pavement Planing Asphalt Concrete 1-3/4"			
		From	To			Feet			Sq. Yards	Gal	Gal		Cu. Yards		Cu. Yards		Cu. Yards		Cu. Yards		Sq. Yards		Sq. Yards	
1	SR 93	0.00 *	0.17 *	0.17	893	25	2	404	2,481	186	99	1/2	34			1 1/4	86					2,481		
1	SR 93	0.17 *	0.49 *	0.32	1,685	25	1	404	4,681	351	187			1 3/4	228	1 1/4	163				292			
1	SR 93	0.49 *	4.18 *	3.69	19,473	25	1	404	54,092	4,057	2,164			1 3/4	2,629	1 1/4	1,878				1,839			
		Extra for Aggregate Drives			550	10			611								2	34						
		Extra for Paved Drives			1,230	10			1,367								1 1/4	47						
		Extra for Paved Public Roads			1,880	20			4,178								1 1/4	145						
		Extra for Mailbox Turnouts (Part 1) 32 Ea x 20 Sq. Yd.							640	48	26			1 3/4	31	1 1/4	22							
		Extra for Widening (10%)							6,125	459	245	1/2	3	1 3/4	286	1 1/4	213							
SUB-TOTAL																								
TOTAL PART 1 (CARRIED TO GENERAL SUMMARY)										5,101	2,721		37		3,174		2,362		226		2,131		2,481	

PAVEMENT DATA

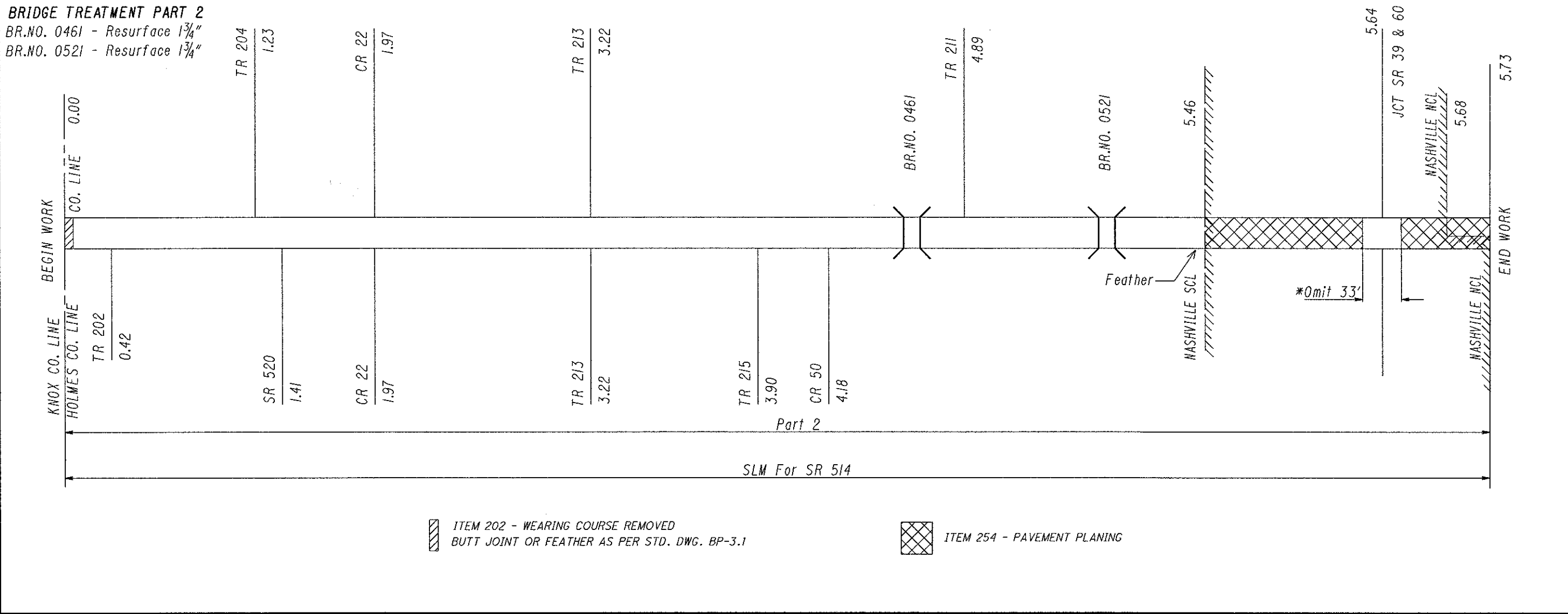
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BRIDGE TREATMENT PART 2

BR.NO. 0461 - Resurface 1 3/4"

BR.NO. 0521 - Resurface 1 3/4"



FOR TYPICAL SECTION DETAILS SEE SHEET 2

PAVEMENT DATA

										PROPOSED PAVEMENT															REMARKS	
										407			422	448						202	254					
Part	Route	Log Point to Log Point (Straight Line Mileage)		Mile	Feet	Width of Pavement	Typical	Existing Type Pavement	Pavement Area	Tack Coat @ 0.10 or 0.075* * gal/s.y.	Tack Coat For Intermediate Course @ 0.04 gal/s.y.		Single Chip Seal	In.	Asphalt Concrete Surface Course, Type 1, PG 70-22M, As Per Plan	In.	Asphalt Concrete Intermediate Course, Type 1, PG64-22, (Spot Leveling)	In.	Asphalt Concrete Surface Course Type 1, PG64-22 (Driveways), As Per Plan	Wearing Course Removed	Pavement Planing Asphalt Concrete 1-3/4"					
		From	To			Feet			Sq. Yards	Gal	Gal		Sq. Yards		Cu. Yards		Cu. Yards		Cu. Yards	Sq. Yards	Sq. Yards					
2	SR 514	0.00	5.46	5.46	28,829	22	3	448	70,471	7,047	2,819		70,471	1 1/4	2,447	1/2	979			150						
2	SR 514	5.46 *	5.73 *	0.27	1,396	22	2	448	3,412	256 * *	136			1 1/4	118	1/2	47				3,412					
		Extra for Aggregate Drives			1,705	10			1,894									2		105						
		Extra for Paved Drives			730	10			811									1 1/4		28						
		Extra for Paved Public Roads			835	20			1,856									1 1/4		64						
		Extra for Mailbox Turnouts			44 Ea x 20 Sq. Yd.				880	88	35			1 1/4	31	1/2	12									
		Extra for Widening (10%)							7,388	739	296			1 1/4	257	1/2	103									
SUB-TOTAL																										
TOTAL PART 2 (CARRIED TO GENERAL SUMMARY)											8,130	3,286		70,471		2,853		1,141		197		150	3,412			



SEE PART 1

CALCULATED JAG	CHECKED LVM
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PAVEMENT DATA

HOL 93-0.00

$$\frac{8}{17}$$

PAVEMENT DATA

[illegible]



PAVEMENT DATA

PAVEMENT DATA

HOL 93-0.00

$$\frac{9}{17}$$

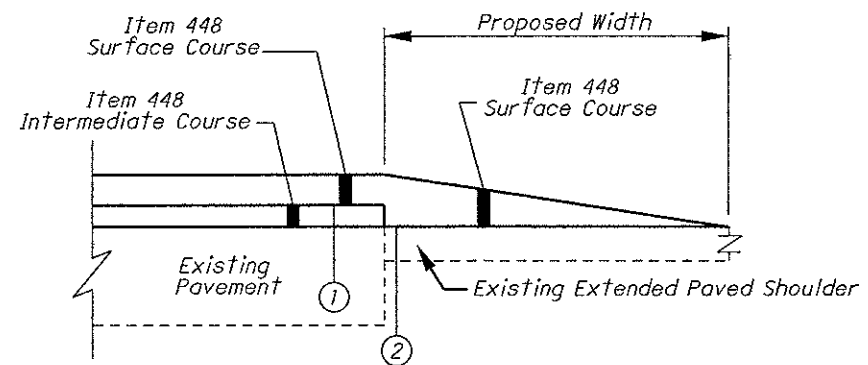


- ①— Item 407 - Tack Coat for Intermediate Course
- ②— Item 407 - Tack Coat
- ③— Item 408 - Prime Coat, As Per Plan

[illegible]

P A R T	C O U N T Y	R O U T E	LENGTH S.L.M.		S I D E	T Y P I C A L	L E N G T H	PROPOSED WIDTH	PAVEMENT AREA		254	ITEM 448 - ASPHALT CONCRETE SURFACE COURSE, TYPE I, PG70-22M, AS PER PLAN		407	R E M A R K S
											PAVEMENT PLANING ASP HALT CONCRETE 1-3/4"			TACK COAT @ 0.075 Gal./SQ YD	
			FROM	TO							FT	FT	SQ YD		
1	HOL	SR 93	0.00	0.135	Lt.	1	713	3	238		238	1	7	18	Ted's Trailer Sales
			2.408	2.451	Rt.	1	227	3	76			1	2	6	Houses #2484-2510
				Sub-Total Part 1							238		9	24	
2	HOL	SR 514	5.640	5.730	Lt.	1	475	9	475		475	1	13	36	Nashville VFD
			5.630	5.649	Rt.	1	100	10	111		111	1	3	8	Library
			5.650	5.667	Rt.	1	90	9	90		90	1	3	7	Horse & Buggy Pulloff
				Sub-Total Part 2							676		19	51	
3	TUS	SR 93	0.326	0.388	Lt.	1	327	3	109			1	3	8	Flex
			0.357	0.385	Rt.	1	148	3	49			1	1	4	Buckeye Storage
			0.664	0.728	Lt.	1	338	3	113		113	1	3	9	Gerber&Sons
			0.664	0.692	Rt.	1	148	3	49		49	1	1	4	Workshops
			0.699	0.728	Rt.	1	153	3	51		51	1	1	4	Spirit Gas Station
			0.740	0.790	Lt.	1	264	3	88		88	1	3	7	Gerber&Sons--Post Office
			0.740	0.752	Rt.	1	63	3	21		21	1	1	2	Gray Building (NE corner)
			0.991	1.047	Lt.	1	296	3	99		99	1	3	7	American Legion
			1.063	1.113	Rt.	1	264	3	88		88	1	3	7	Marathon/T-bone
				Sub-Total Part 3							509		19	52	
4	TUS	SR 651	0.270	0.278	Lt.	1	42	3	14		14	1	1	1	White House #204
			0.278	0.298	Rt.	1	106	3	35		35	1	1	3	Tan House
				Sub-Total Part 4							49		2	4	
				Total (Carried to General Summary)							1472		49	131	

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TYPICAL 1 - EXTENDED PAVED SHOULDER
(Without Existing Paved Shoulder)

ITEM 407

① Tack Coat for Intermediate Course

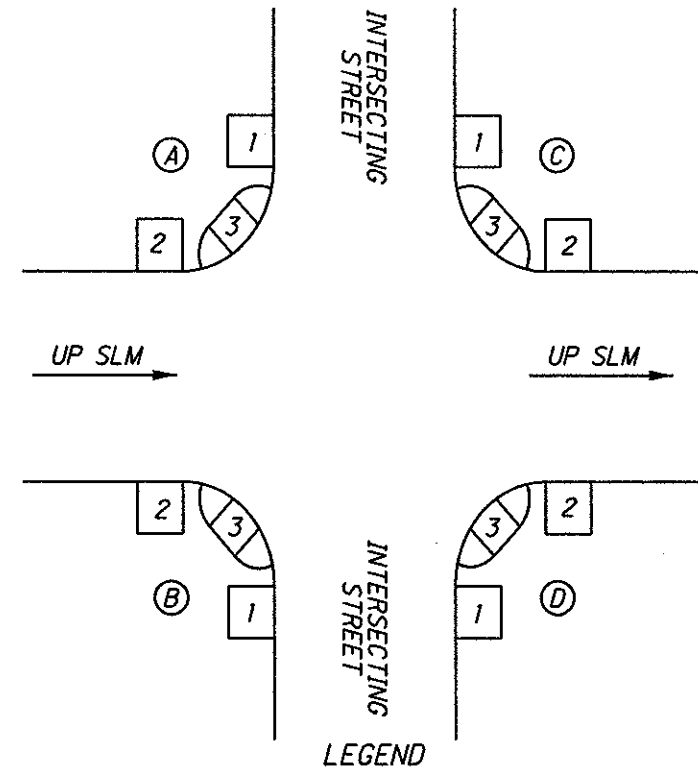
② Tack Coat

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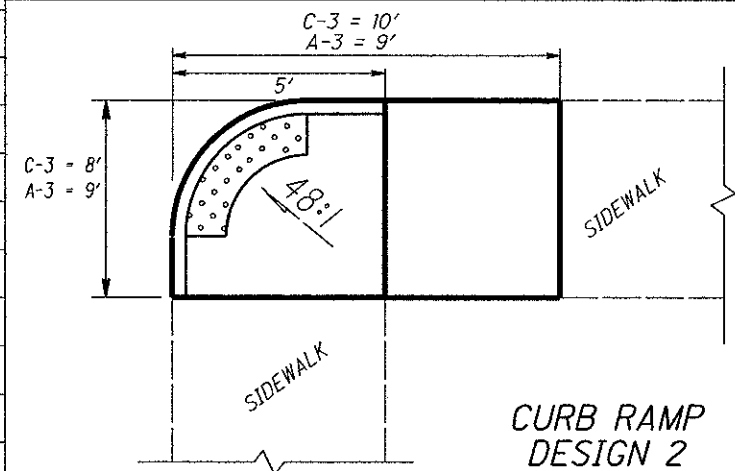
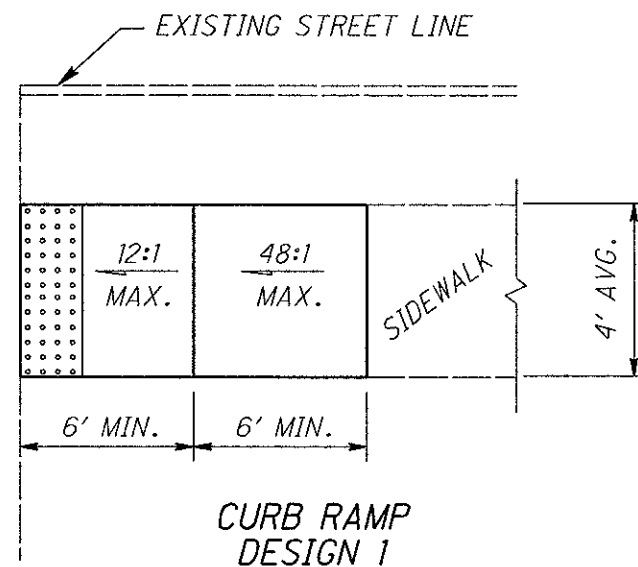
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P A R T	R O U T E	INTERSECTION	LOCATION (SEE LEGEND)	202		608				609	TRUNCATED DOMES, FOR INFORMATION ONLY (SQ. FT.)
				CURB REMOVED	WALK REMOVED	CURB RAMP		TRUNCATED DOMES	6" CONCRETE WALK	CURB, TYPE 6	
				LIN FT	SQ FT	SQ FT	DESIGN	EACH	SQ FT	LIN FT	
1	93	Short St.	B-1		40			1	40		8
			D-1		48			1	48		8
1	93	Maple St.	D-1					1			8
1	93	Fairview St.	D-1					1			8
2	514	S.R. 39	A-2					1			8
			B-2		60		1	1	60		8
			C-2					1			8
			D-2					1			8
3	93	South St.	B-1					1			8
			D-1		56		1	1	56		8
3	93	High St.	B-1					1			8
			D-1					1			8
3	93	E.Main St. (651)	A-3	20	75	39	2		36	4	
			B-1		40			1	40		8
			B-2					1			8
			C-3	17	80	40	2		40		
			D-1					1			8
			D-2					1			8
SUB-TOTALS CARRIED TO SHEET 13				37	399	79		16	320	4	128

FOR DETAILS SEE SCD BP-7.1 AND BP-7.2.



S.R. 93/514 AND INTERSECTION STREETS



CURB RAMP SUB-SUMMARY

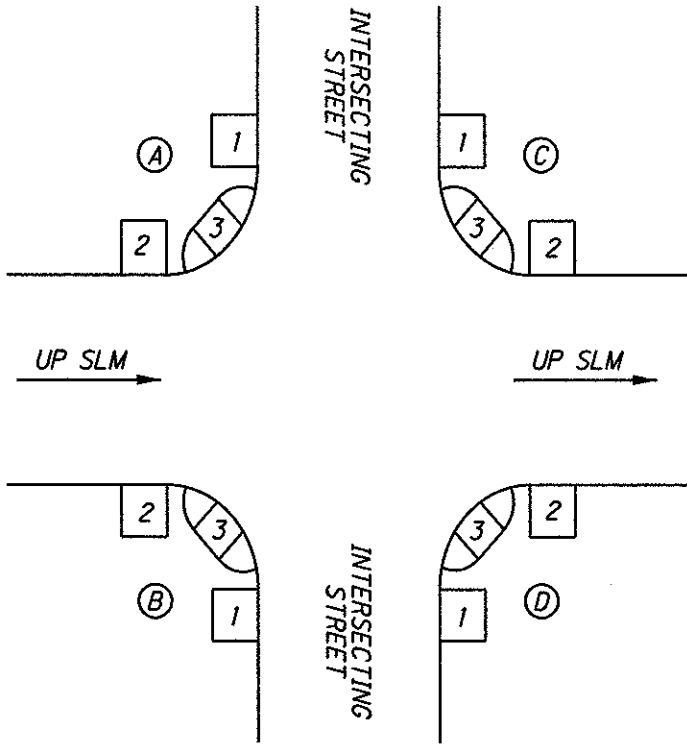
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17

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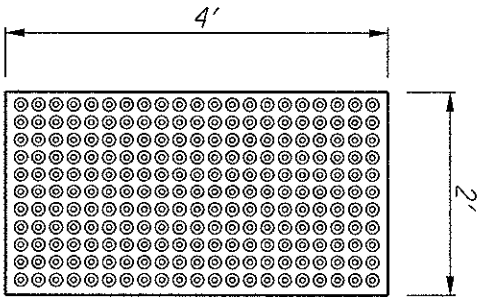
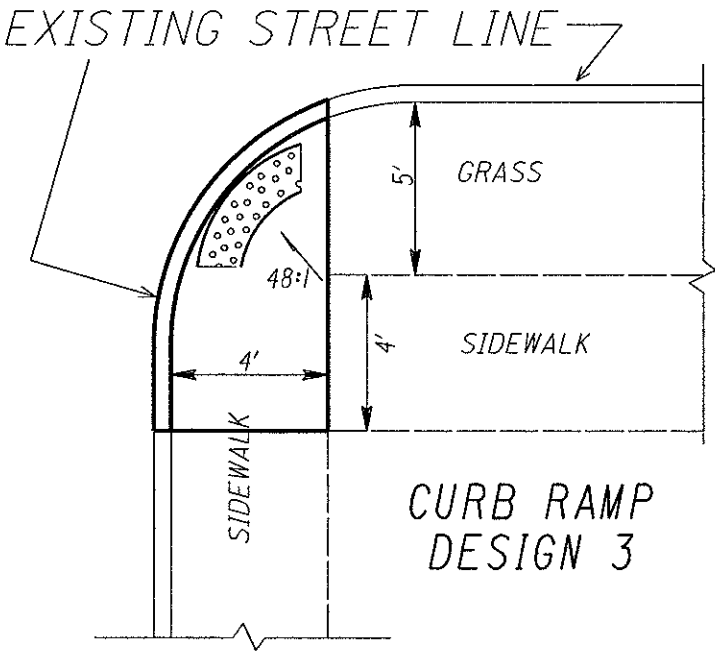
P A R T	R O U T E	INTERSECTION	LOCATION (SEE LEGEND)	202		608				609	TRUNCATED DOMES, FOR INFORMATION ONLY (SQ. FT.)
				CURB REMOVED	WALK REMOVED	CURB RAMP		TRUNCATED DOMES	6" CONCRETE WALK	CURB, TYPE 6	
				LIN FT	SQ FT	SQ FT	DESIGN	EACH	SQ FT	LIN FT	
4	651	S. Butler St.	A-3	10	44	44	3				
			B-3					1			8
			C-3					1			8
			D-3					1			8
4	651	S. Park Dr.	A-1					1			8
			B-1					1			8
			C-1					1			8
			C-2					1			8
			D-1					1			8
			D-2					1			8
4	651	Cross Walk (SLM 0.387)	A-2					1			8
		SHEET SUB-TOTALS		10	44	44		10	0	0	80

FOR DETAILS SEE SCD BP-7.1 AND BP-7.2.



LEGEND

S.R. 651 AND INTERSECTION STREETS



DETAIL

TRUNCATED DOME MAT

CURB RAMP SUB-SUMMARY

HOL-93-0.00

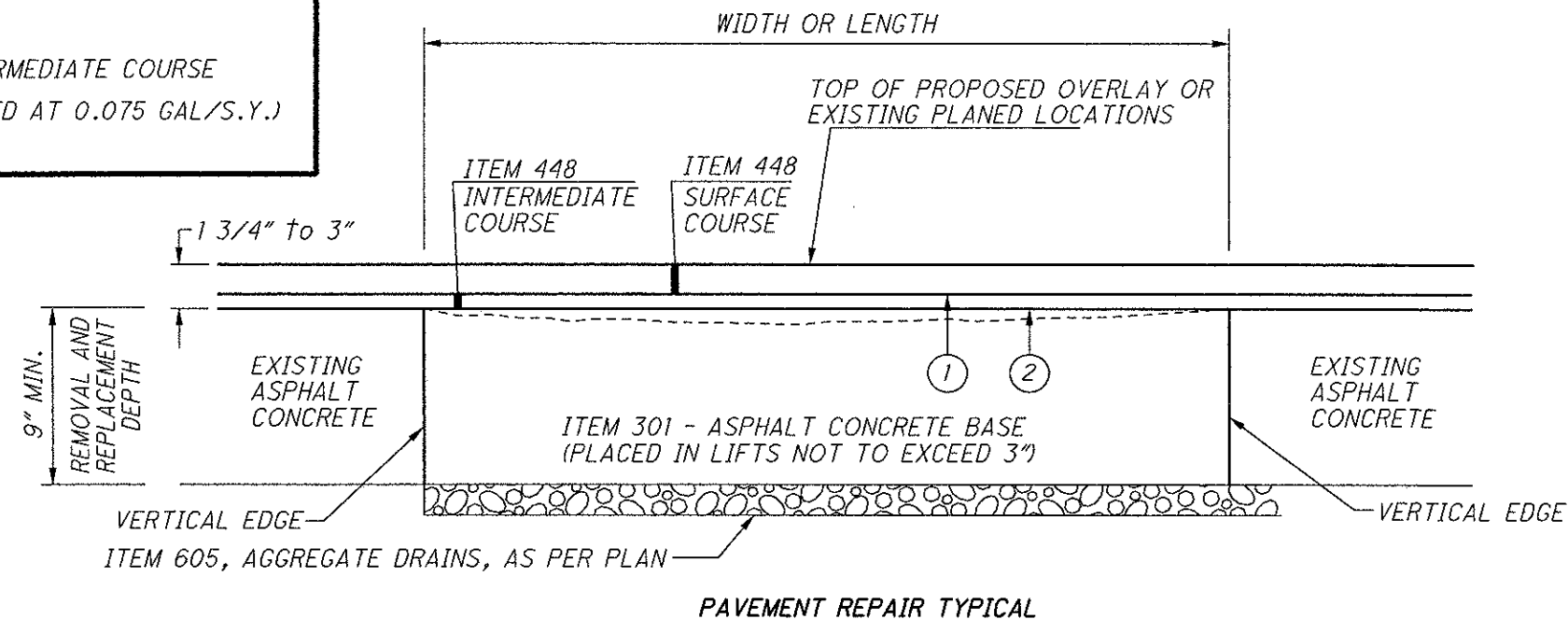
LEGEND

①

ITEM 407, TACK COAT FOR INTERMEDIATE COURSE

②

ITEM 407, TACK COAT (APPLIED AT 0.075 GAL/S.Y.)



ESTIMATED QUANTITIES

PART 1, ITEM 253 - - - PAVEMENT REPAIR - 185 CU YD
PART 2, ITEM 253 - - - PAVEMENT REPAIR - 382 CU YD
PART 3, ITEM 253 - - - PAVEMENT REPAIR - 35 CU YD
PART 4, ITEM 253 - - - PAVEMENT REPAIR - 0 CU YD

602 CU YD TOTAL

PART 1, ITEM 605 - - - AGGREGATE DRAINS, AS PER PLAN - 370 FT
PART 2, ITEM 605 - - - AGGREGATE DRAINS, AS PER PLAN - 764 FT
PART 3, ITEM 605 - - - AGGREGATE DRAINS, AS PER PLAN - 70 FT
PART 4, ITEM 605 - - - AGGREGATE DRAINS, AS PER PLAN - 0 FT

1204 FT TOTAL

(TOTALS CARRIED TO GENERAL SUMMARY)

ITEM 253 - PAVEMENT REPAIR

THE PAVEMENT REPAIR LOCATIONS AND ESTIMATED QUANTITIES WERE OBTAINED BY PRELIMINARY FIELD REVIEW AND SHALL BE CONSIDERED APPROXIMATE. A FINAL FIELD REVIEW WILL BE PERFORMED BY ODOT PRIOR TO CONSTRUCTION AND FINAL LOCATIONS WILL BE GIVEN TO THE CONTRACTOR AT THE PRE-CONSTRUCTION CONFERENCE.

THIS WORK CONSISTS OF REMOVING EXISTING ASPHALT CONCRETE, BRICK, PORTLAND CEMENT CONCRETE, OR AGGREGATE PAVEMENT COURSES; SHAPING AND COMPACTING THE EXPOSED MATERIAL; AND PLACING NEW ASPHALT CONCRETE PAVEMENT OR AGGREGATE AND ASPHALT CONCRETE PAVEMENT COURSES.

IF NEEDED AN AGGREGATE DRAIN, AS PER PLAN SHALL BE INSTALLED IN ACCORDANCE WITH CMS 605.07.

THE ABOVE ESTIMATED QUANTITY IS TO BE USED AS DIRECTED BY THE ENGINEER. FINAL PAYMENT FOR THE ABOVE ITEMS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

ITEM 605 - AGGREGATE DRAINS, AS PER PLAN

THIS ITEM SHALL BE USED IN ACCORDANCE WITH ITEM 253, PAVEMENT REPAIR AND ITEM 605.07 OF THE CMS. THE AGGREGATE SHALL BE NO. 57 SIZE GRAVEL, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

THE ABOVE ESTIMATED QUANTITY IS TO BE USED AS DIRECTED BY THE ENGINEER. FINAL PAYMENT FOR THE ABOVE ITEMS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

COUNTY HOL
ROUTE SR 93
RailRoad Symbol Marking, Type I, 4 each
Part I

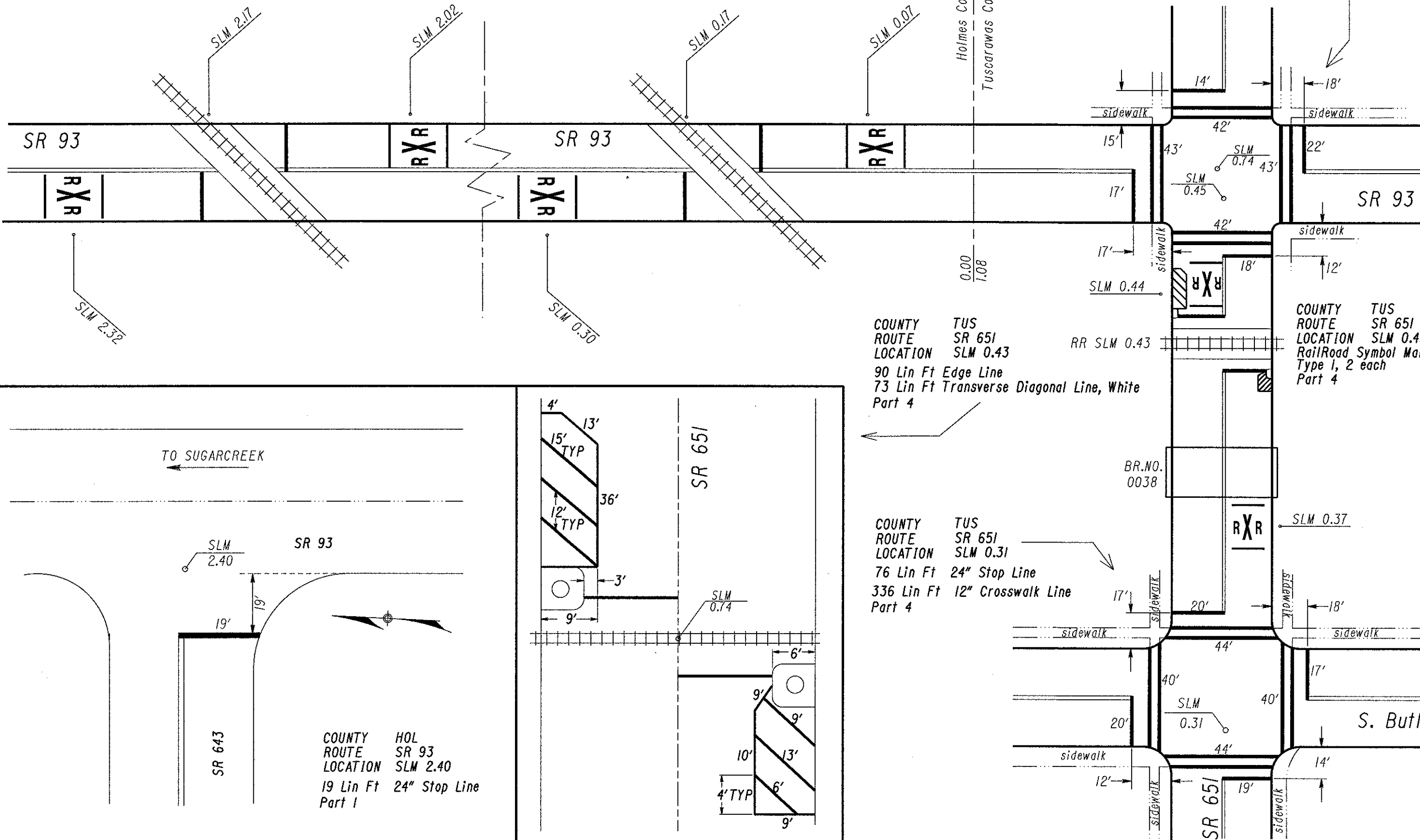
COUNTY TUS
ROUTE SR 93
LOCATION SLM 0.74
71 Lin Ft 24" Stop Line
340 Lin Ft 12" Crosswalk Line
Part 3

CALCULATED
JAG
CHECKED
LVM

PAVEMENT MARKING DETAIL

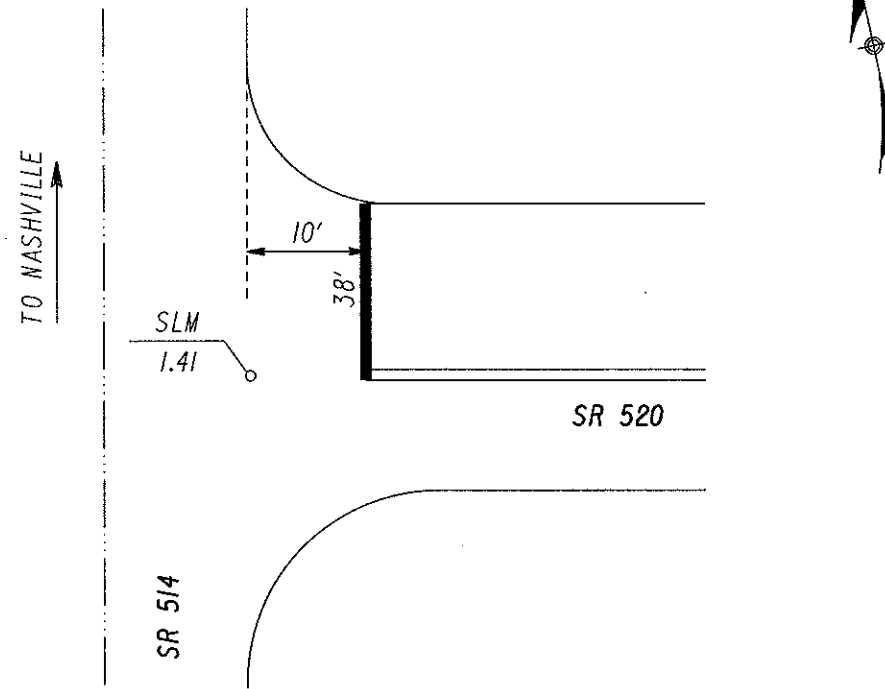
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17

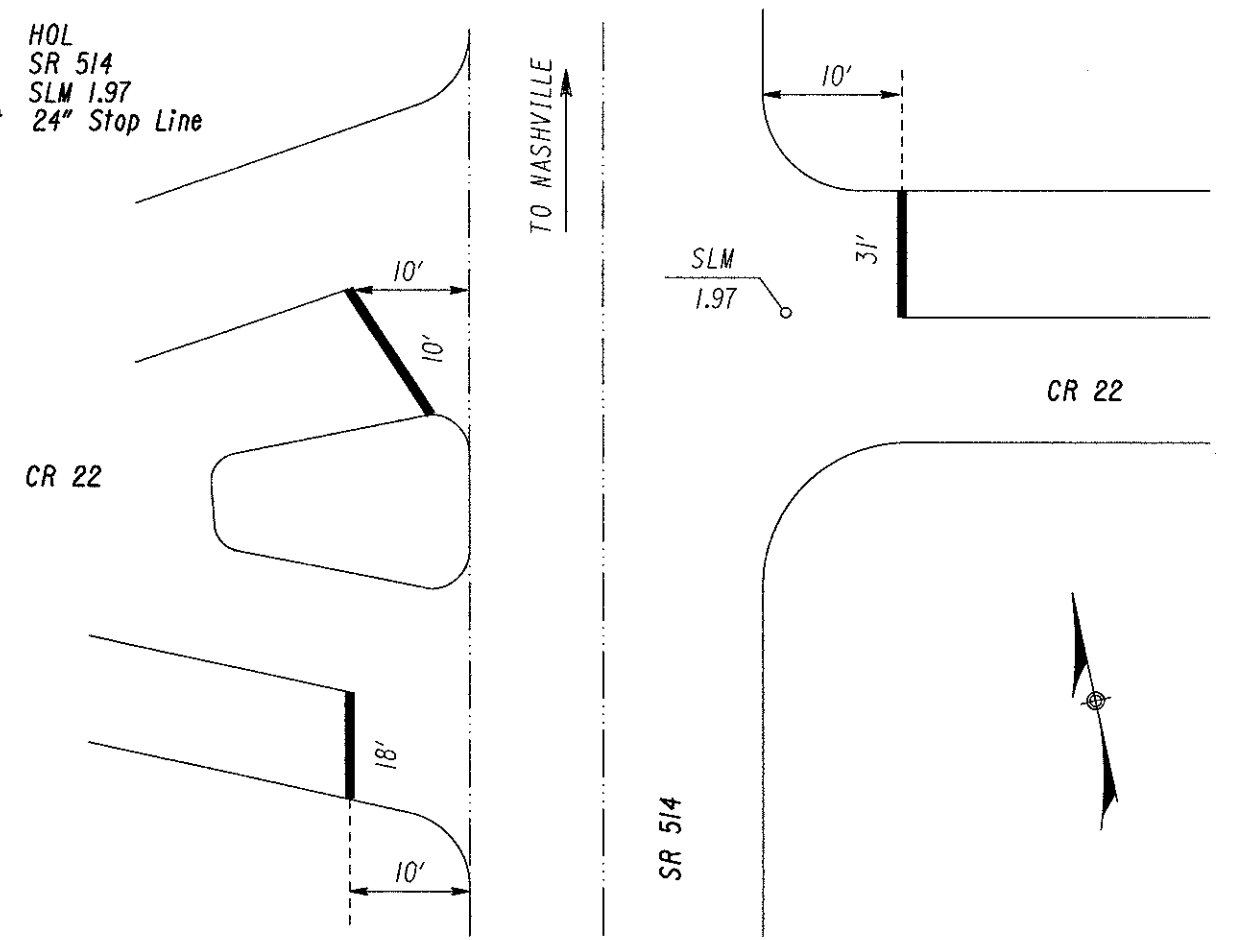


NOTES: 1. FOR PAVEMENT MARKING DETAILS NOT SHOWN, SEE STANDARD CONSTRUCTION DRAWING TC-71.10 AND TC-73.10. 2. ALL QUANTITIES CARRIED TO PAVEMENT MARKING SUB-SUMMARY, SHEET 17

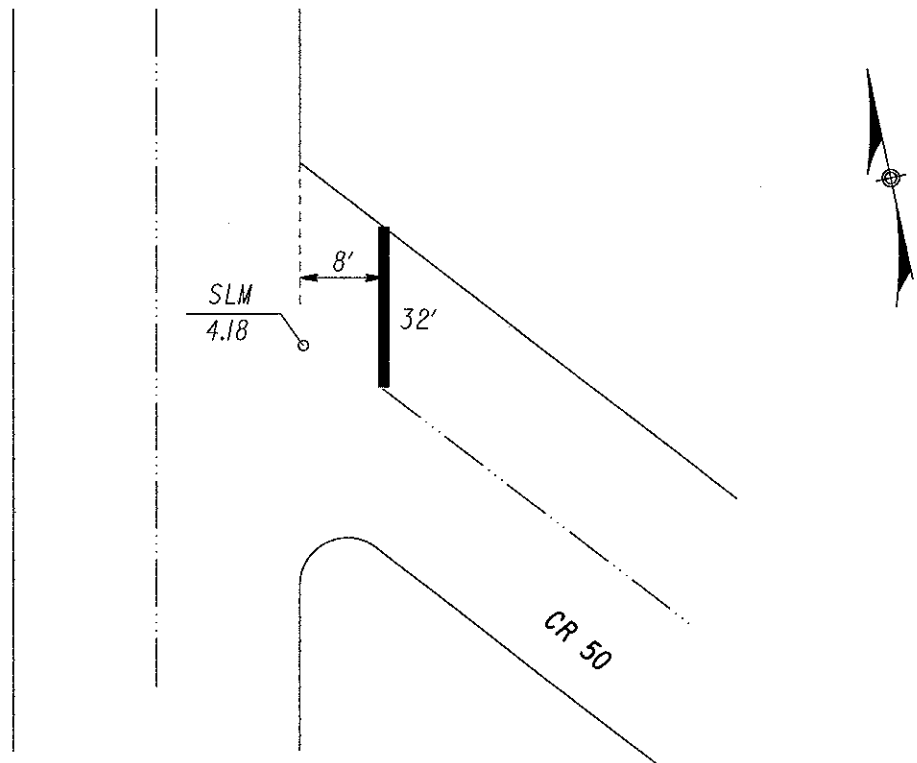
COUNTY HOL
 ROUTE SR 514
 LOCATION SLM 1.41
 38 Lin Ft 24" Stop Line
 PART 2



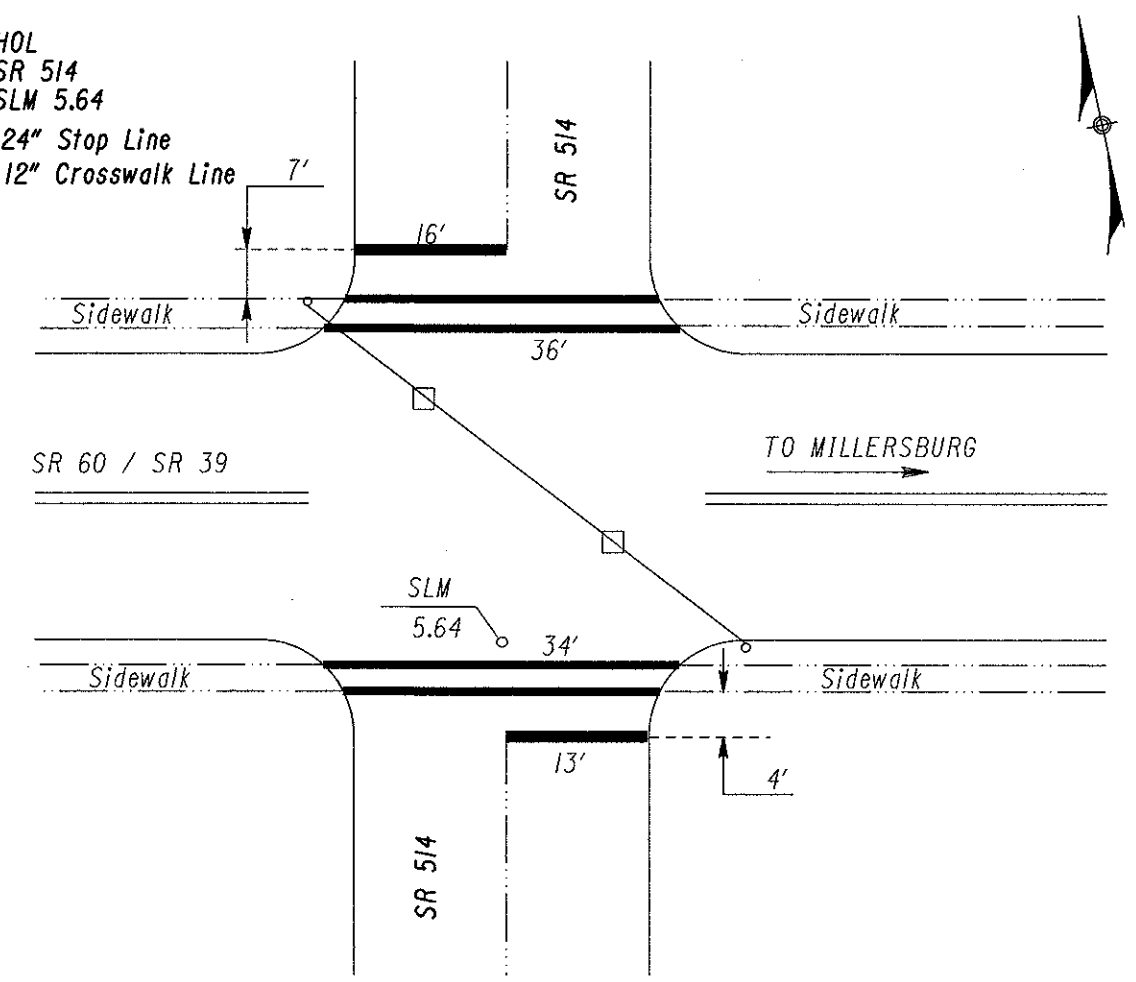
COUNTY HOL
 ROUTE SR 514
 LOCATION SLM 1.97
 59 Lin Ft 24" Stop Line
 PART 2



COUNTY HOL
 ROUTE SR 514
 LOCATION SLM 4.18
 32 Lin Ft 24" Stop Line
 PART 2



COUNTY HOL
 ROUTE SR 514
 LOCATION SLM 5.64
 29 Lin Ft 24" Stop Line
 140 Lin Ft 12" Crosswalk Line
 PART 2



NOTES: ALL QUANTITIES CARRIED TO PAVEMENT MARKING SUB-SUMMARY, SHEET 17

AUXILIARY PAVEMENT MARKING

HOL-93-0.00

16
17

[illegible]