(MAINTENANCE PROJECT)

(MAINTENANCE PROJECT) ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A ACRES (MAINTENANCE PROJECT) NOTICE OF INTENT EARTH DISTURBED AREA: N/A ACRES

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PLANS PREPARED BY: Z Ш

STATE OF OHIO DEPARTMENT OF TRANSPORTATION

ERI-2-0.14

CITY OF HURON CITY OF SANDUSKY **HURON TOWNSHIP MARGARETTA TOWNSHIP** PERKINS TOWNSHIP **ERIE COUNTY**

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STANDARD CONSTRUCTION DRAWINGS

LIMITED ACCESS

EARTH DISTURBED AREA:

PROJECT DESCRIPTION

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2013 SPECIFICATIONS

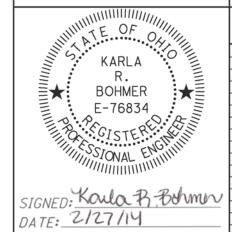
THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO. DEPARTMENT OF TRANSPORTATION. INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT DETOURS WILL BE PROVIDED AS INDICATED ON SHEET 23.

APPROVED DATE 2-27-14

APPROVED DIRECTOR, DEPARTMENT OF TRANSPORTATION

ENGINEER'S SEAL:



	STANDAN	D CONSTR	OCTION D	NAWINOS	SPECIF1	CATIONS	PROVISIONS
BP-3.1	4/20/12	MT-101.90	7/19/13		SS800	4/18/14	
		MT-105.10	7/19/13		SS821	4/20/12	
DM-4.3	7/19/13				SS832	1/17/14	
DM-4.4	7/20/12	TC-41.20	10/18/13		SS921	4/20/12	
		TC-42.20	10/18/13				
MT-95.30	7/19/13	TC-52.10	10/18/13				
MT-95.31	7/19/13	TC-52.20	1/17/14				
MT-95.32	7/19/13	TC-65.10	1/17/14				
MT-95.50	7/19/13	TC-65.11	1/17/14				
MT-98.10		TC-71.10	1/17/14				
MT-98.11	7/19/13	TC-72.20	7/20/12				
MT-98.20	7/19/13						
MT-98.22	7/19/13						
MT-98.28	7/19/13						
MT-98.29	7/19/13						
MT-99.20	7/19/13						

SUPPLEMENTAL

SPECIAL

UNDERGROUND UTILITIES

CONTACT BOTH SERVICES CALL TWO WORKING DAYS BEFORE YOU DIG



1-800-362-2764 (TOLL FREE) OHIO UTILITIES PROTECTION SERVICE

MUST BE CALLED DIRECTLY

OIL & GAS PRODUCERS UNDERGROUND PROTECTION SERVICE CALL: 1-800-925-0988



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BEGIN PROJECT

LOCATION MAP

SCALE IN MILES

LATITUDE: N 41° 24' 18" LONGITUDE: W 82° 44' 01"

INTERSTATE HIGHWAY._____

FEDERAL ROUTES _____

STATE ROUTES._____

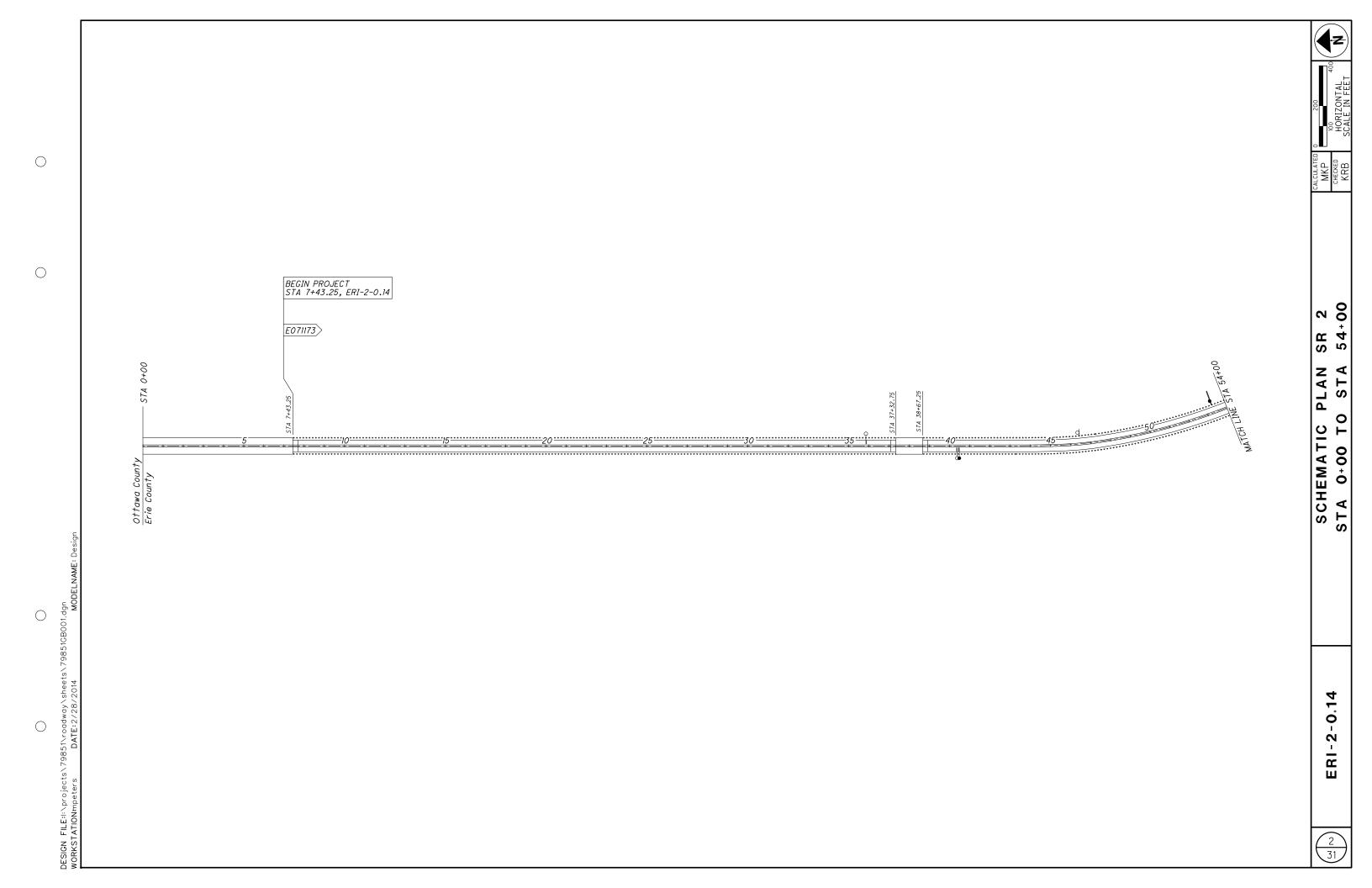
COUNTY & TOWNSHIP ROADS ______

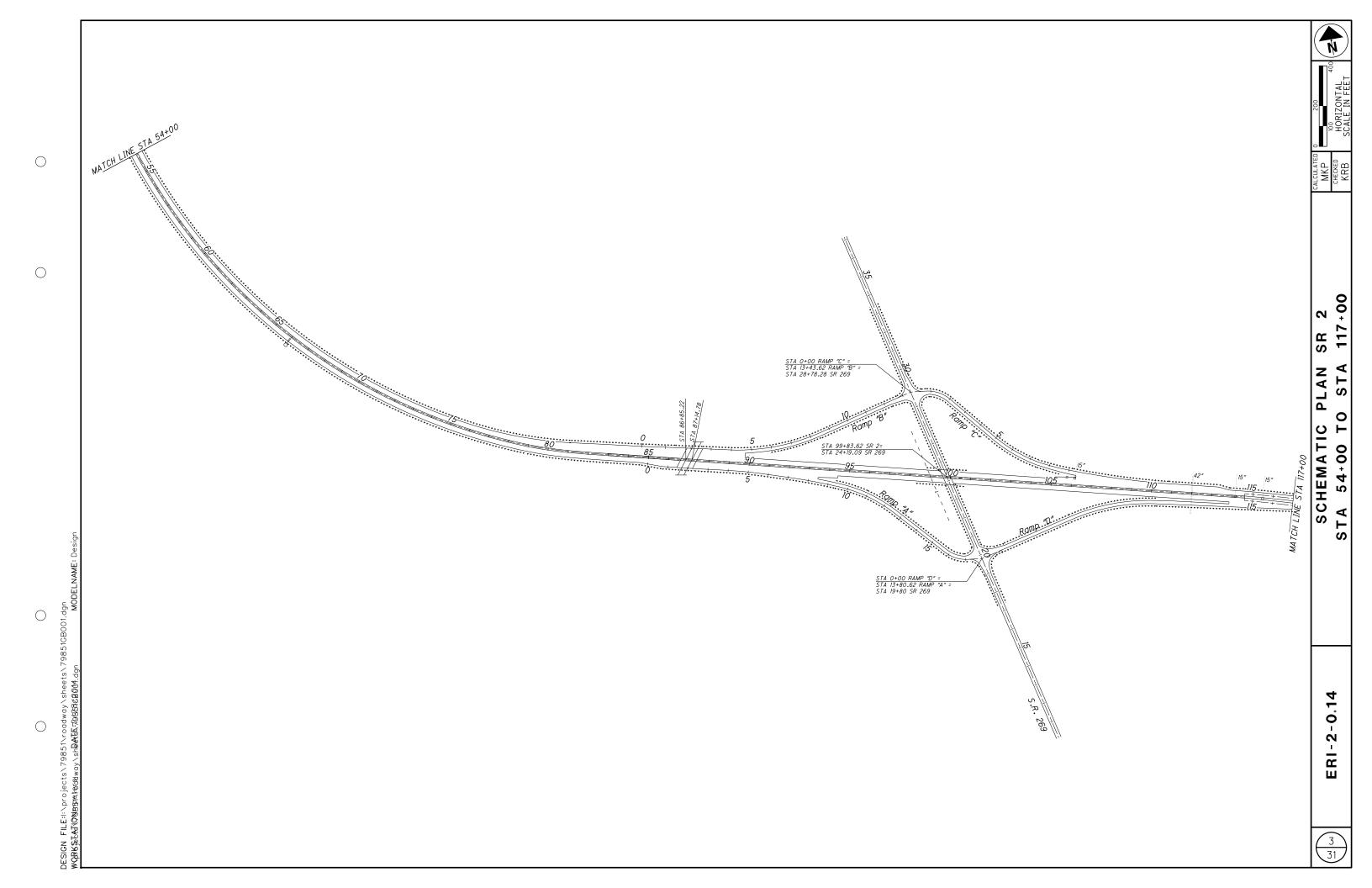
OTHER ROADS _____

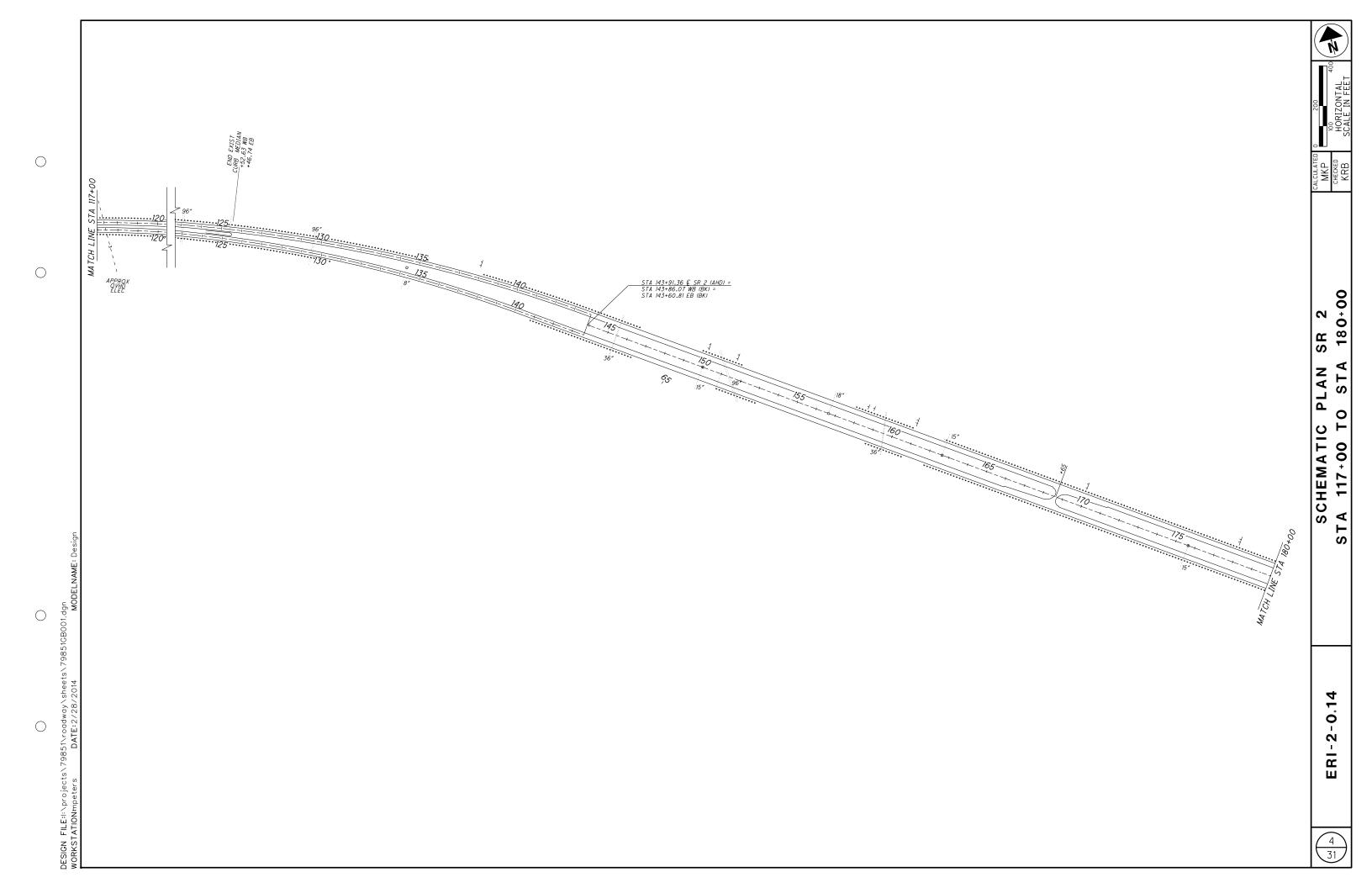
PORTION TO BE IMPROVED _ _ _

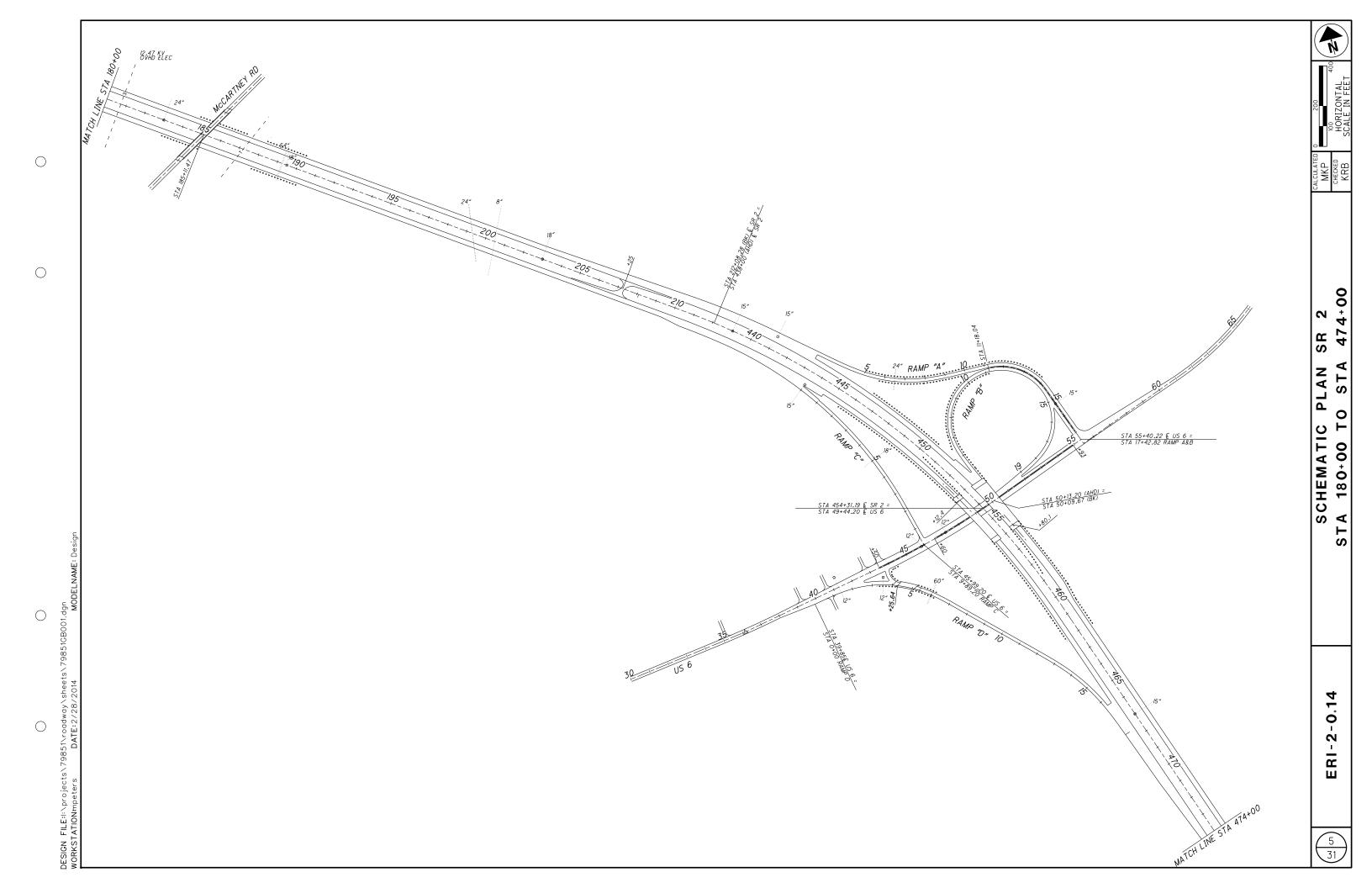
END PROJECT

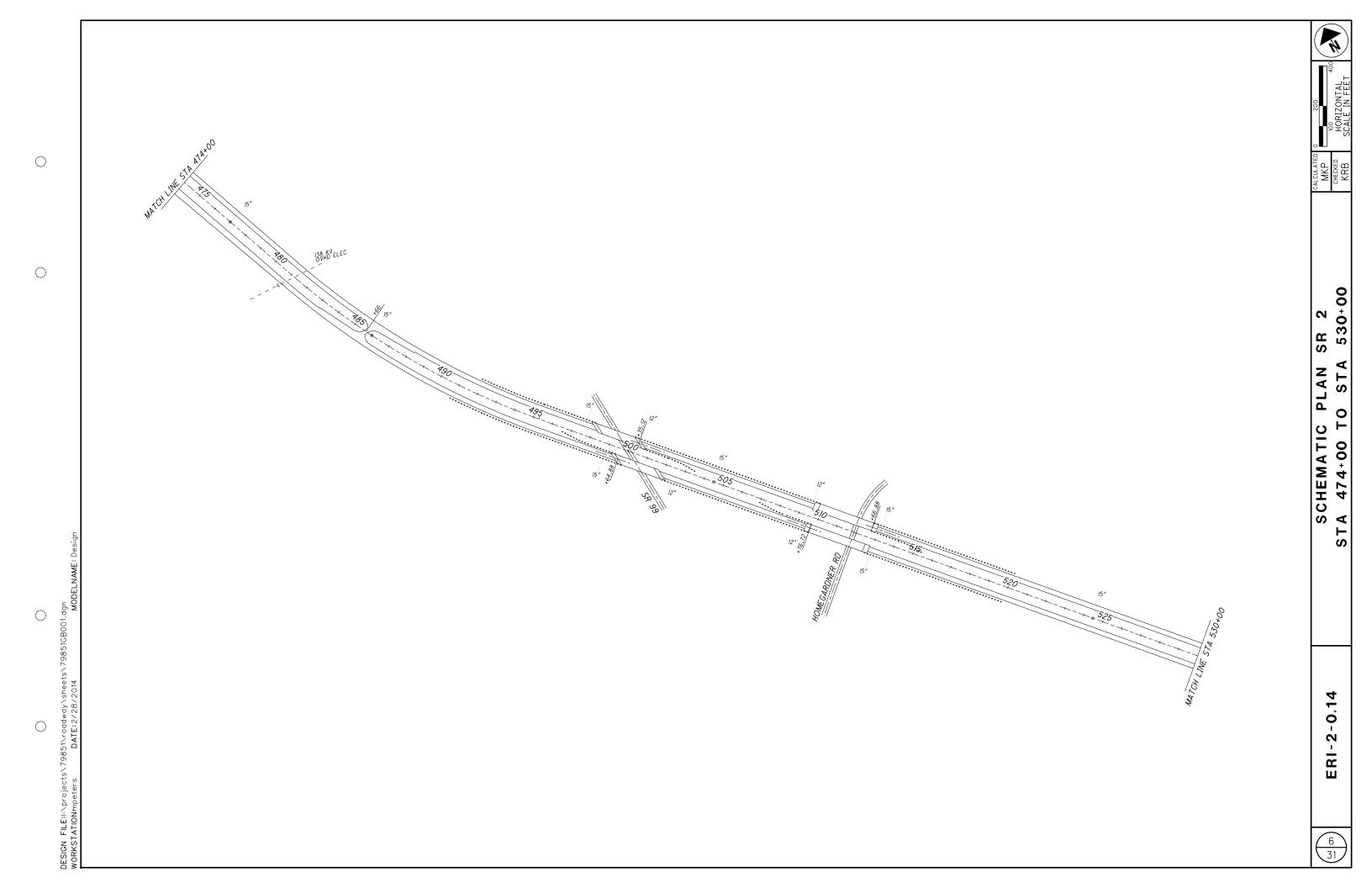
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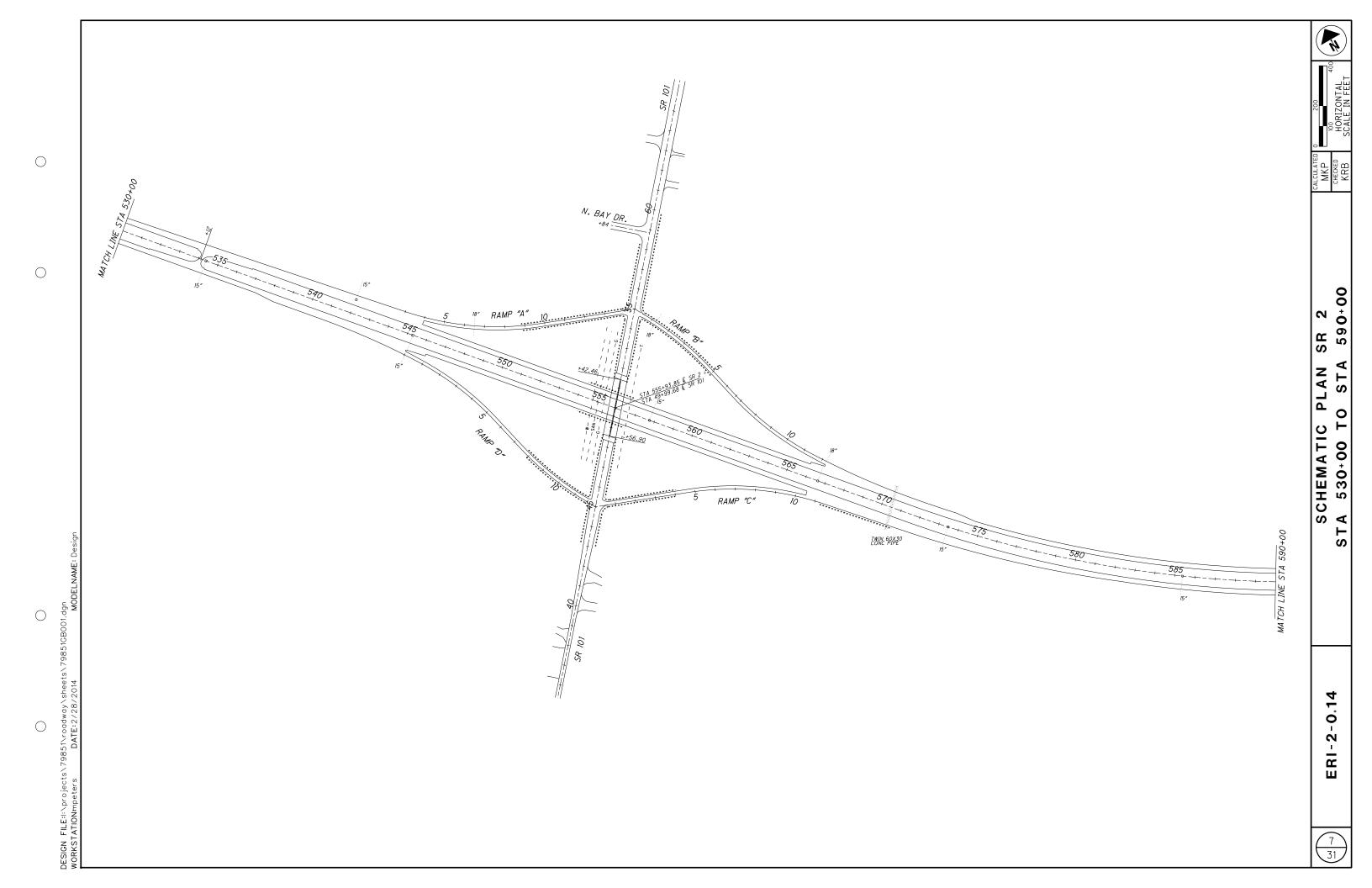












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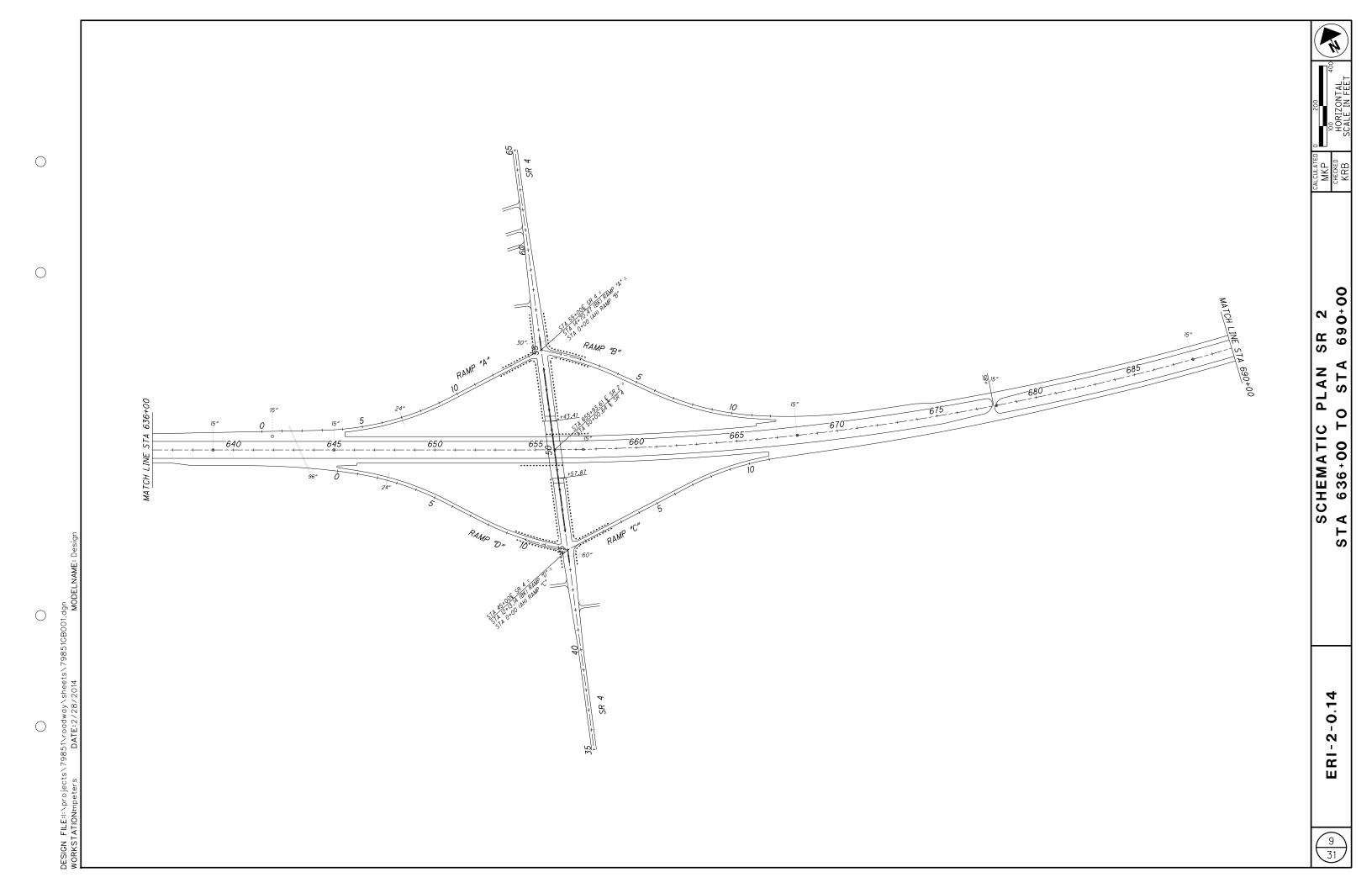
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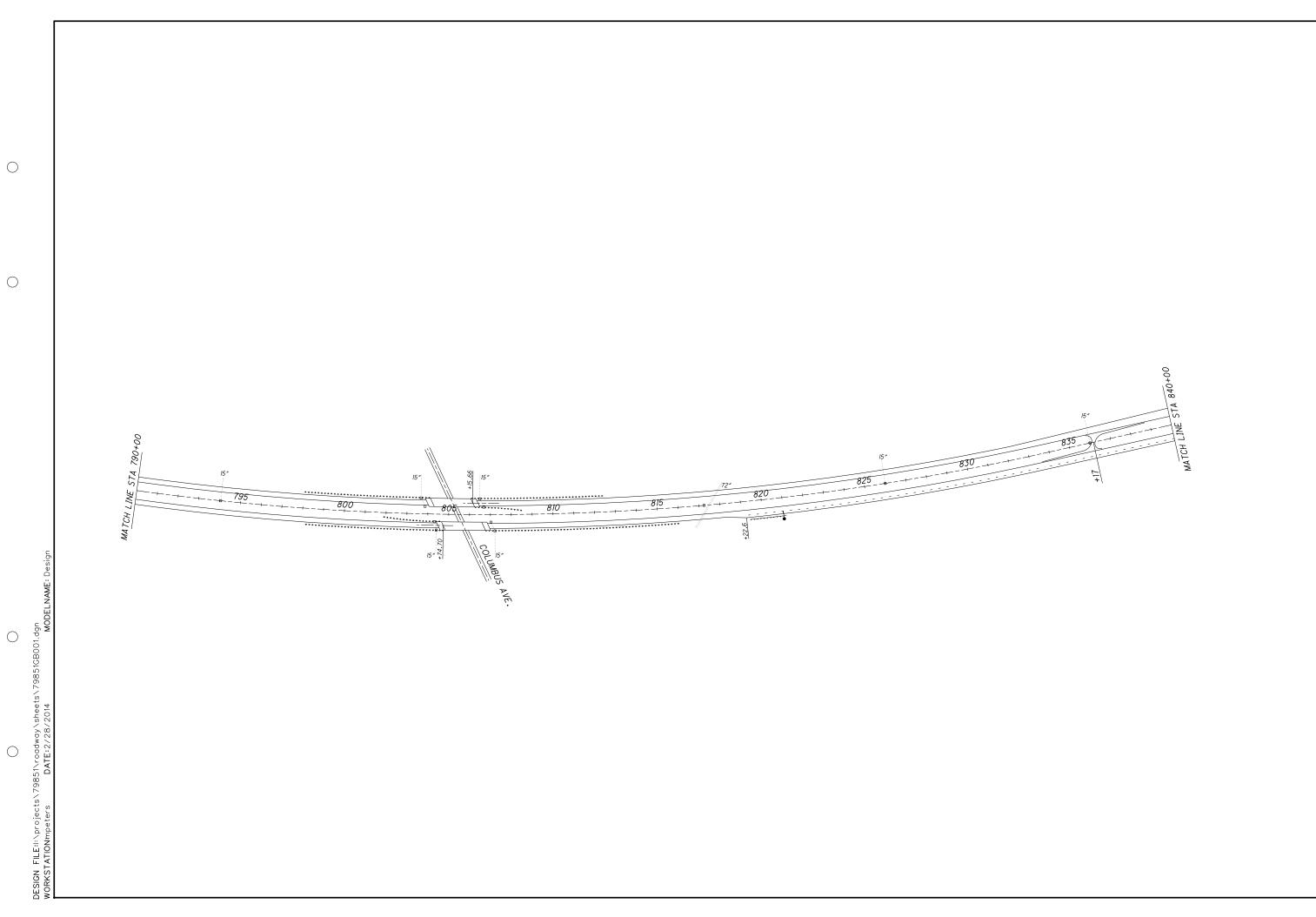
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SCHEMATIC PLAN STA 690+00 TO STA

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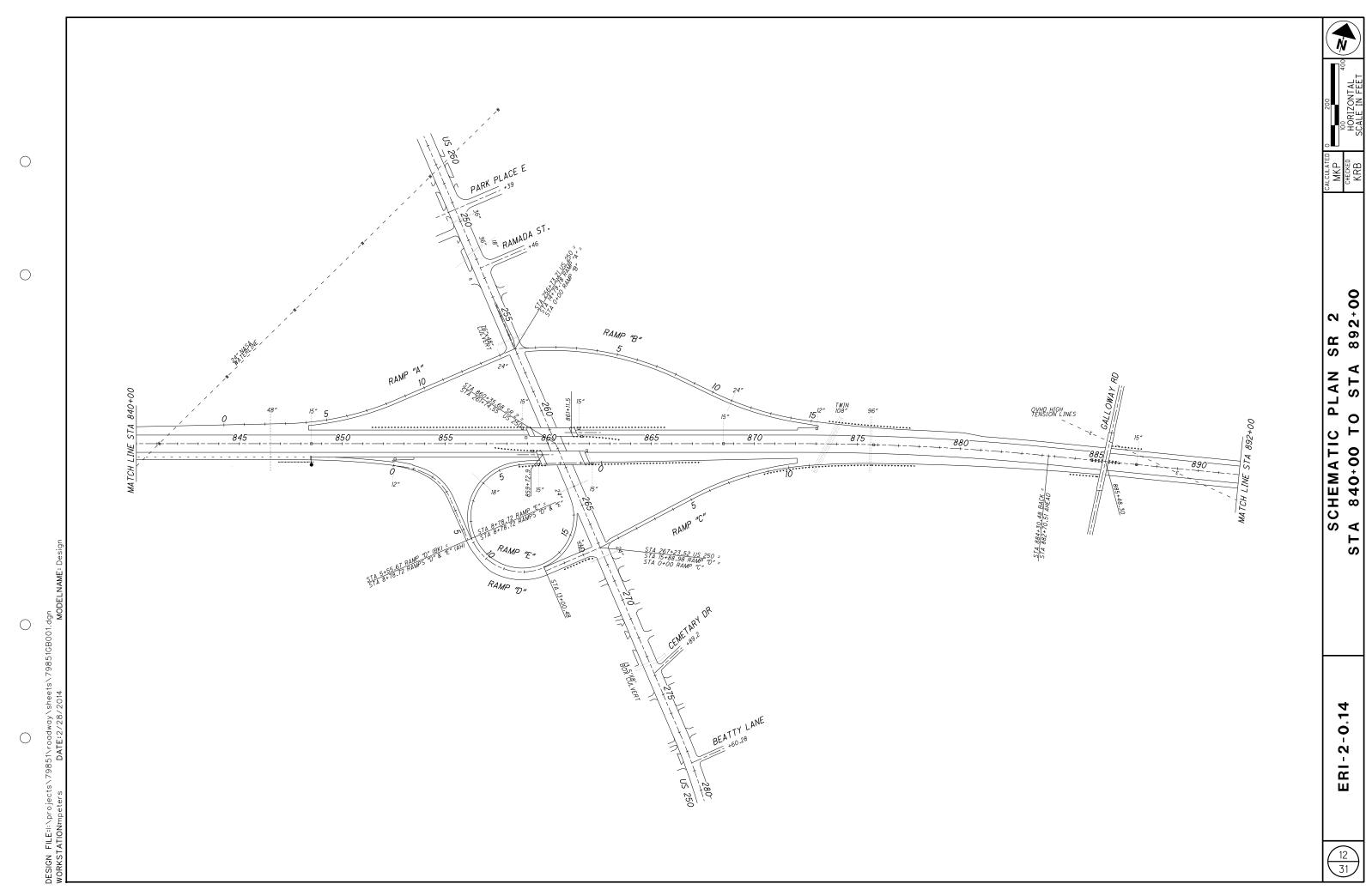
840+00 7

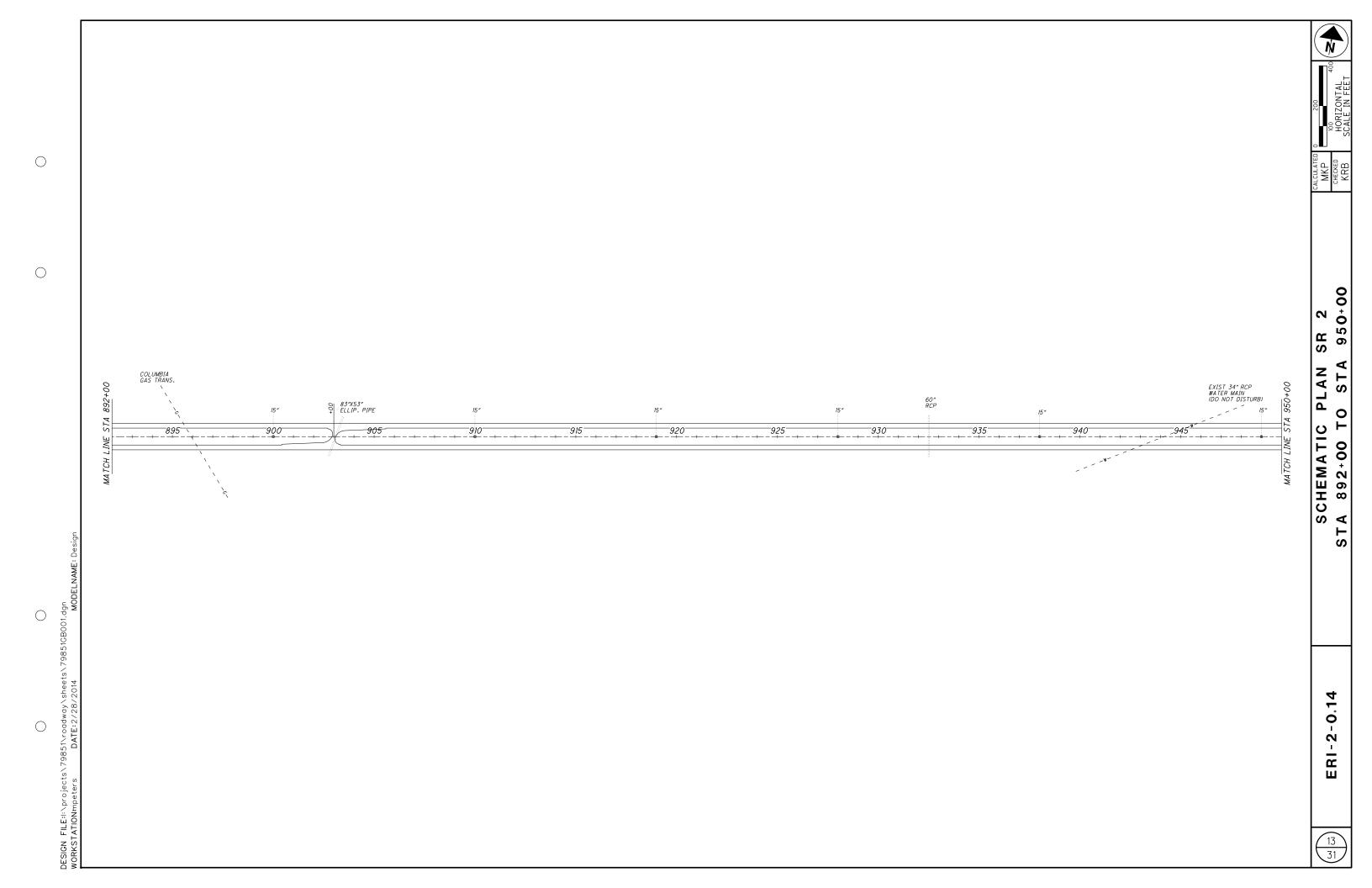
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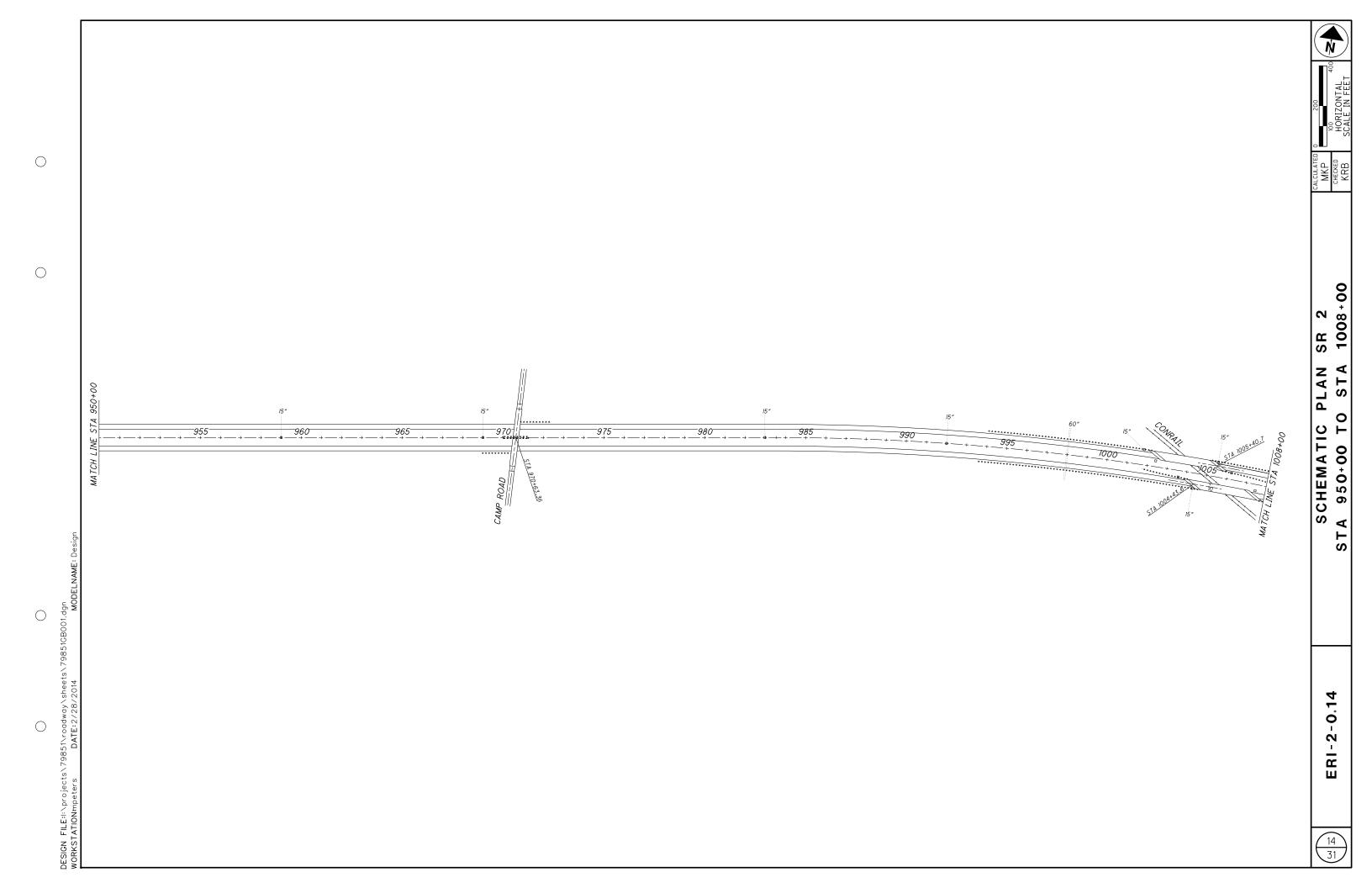
PLAN O STA

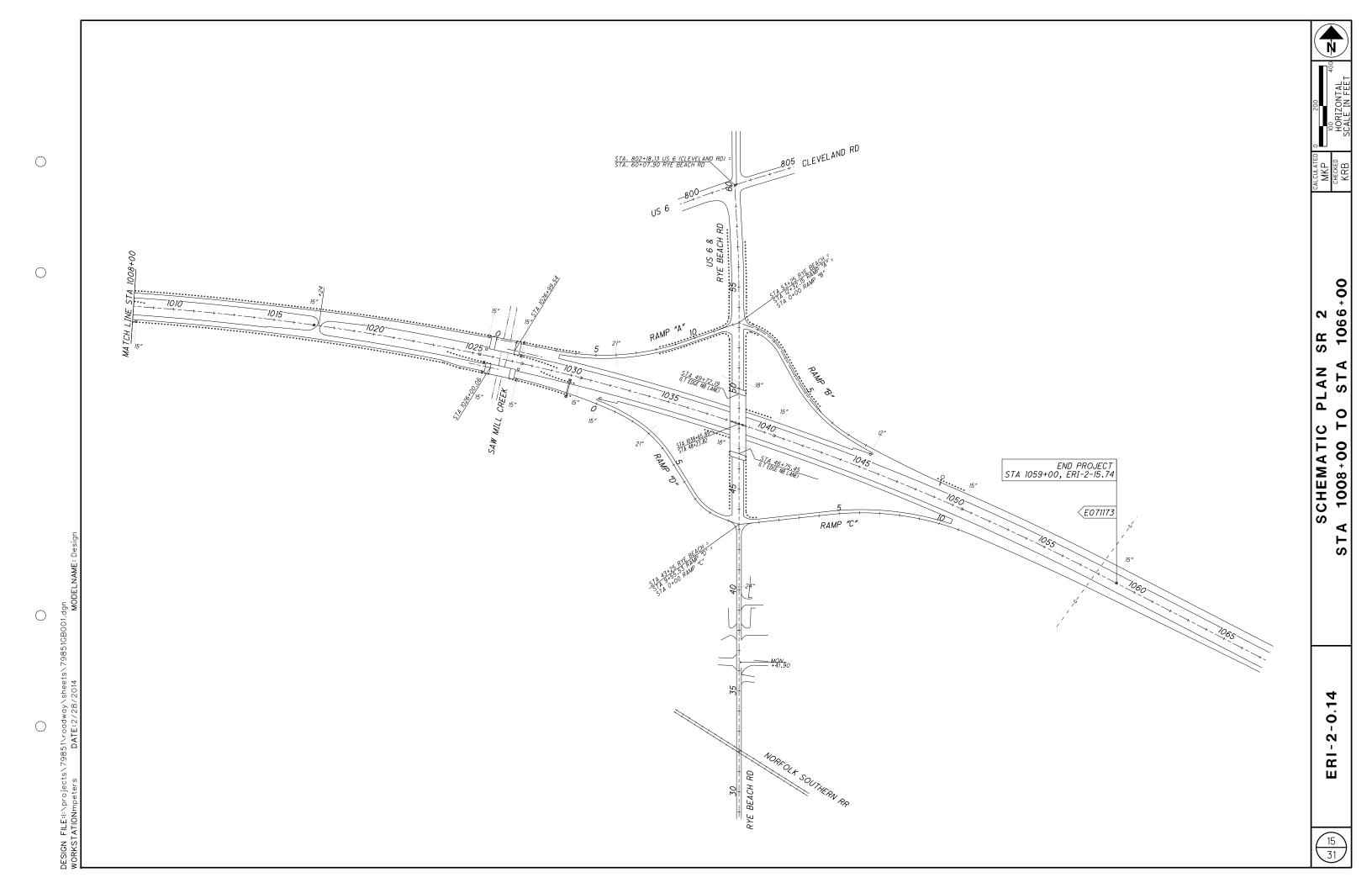
SCHEMATIC P STA 790+00 TO

ERI-2-0.14









DESIGN DESIGNATION (ERI-2-0.14 TO 1.82)	DESIGN DESIGNATION (ERI-2-4.23 TO 4.76)	DESIGN DESIGNATION (ERI-2-8.07 TO 11.93)
CURRENT ADT (2014)24,000 DESIGN YEAR ADT (2022)25,000 DESIGN HOURLY VOLUME (2022)2,200 DIRECTIONAL DISTRIBUTION0.53 TRUCK (24 HOUR B&C)0.10 DESIGN SPEED70 MPH LEGAL SPEED70 MPH DESIGN FUNCTIONAL CLASSIFICATION: RURAL PRINCIPLE ARTERIAL	CURRENT ADT (2014)	CURRENT ADT (2014)
NHS PROJECTYES	NHS PROJECTYES	NHS PROJECTYES
DESIGN EXCEPTIONS NONE	DESIGN EXCEPTIONS NONE	DESIGN EXCEPTIONS NONE
DESIGN DESIGNATION (ERI-2-1.82 TO 3.71)	DESIGN DESIGNATION (ERI-2-4.76 TO 6.17)	DESIGN DESIGNATION (ERI-2-11.93 TO 15.35)
CURRENT ADT (2014)	CURRENT ADT (2014)	CURRENT ADT (2014)
NHS PROJECTYES	NHS PROJECTYES	NHS PROJECTYES
DESIGN EXCEPTIONS NONE	DESIGN EXCEPTIONS NONE	DESIGN EXCEPTIONS NONE
DESIGN DESIGNATION (ERI-2-3.71 TO 4.23)	DESIGN DESIGNATION (ERI-2-6.17 TO 8.07)	DESIGN DESIGNATION (ERI-2-15.35 TO 15.64)
CURRENT ADT (2014)	CURRENT ADT (2014)	CURRENT ADT (2014)24,000 DESIGN YEAR ADT (2022)24,000 DESIGN HOURLY VOLUME (2022)2,200 DIRECTIONAL DISTRIBUTION0.53 TRUCK (24 HOUR B&C)0.15 DESIGN SPEED70 MPH LEGAL SPEED70 MPH DESIGN FUNCTIONAL CLASSIFICATION: URBAN FREEWAY & EXPRESSWAY
NHS PROJECTYES	NHS PROJECTYES	NHS PROJECTYES
DESIGN EXCEPTIONS NONE	DESIGN EXCEPTIONS NONE	DESIGN EXCEPTIONS NONE

DESIGN FILE:I:\projects\79851\roadway\sheets\79851GB001.dgn WQRK\$EAIYOVBBRRt@GBway\sh<mark>PA</mark>IE*T:298E*86\$8001.dgn MODELNAME:

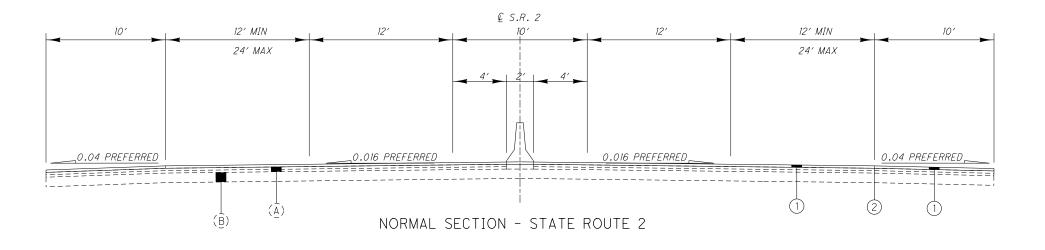
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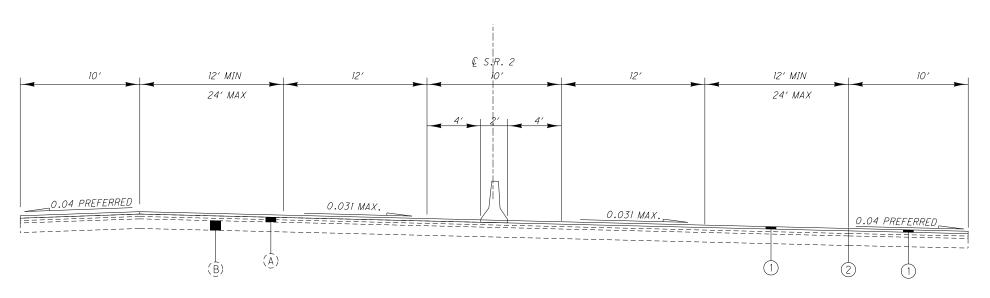
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SUPERELEVATION SECTION - STATE ROUTE 2

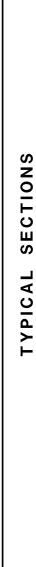
EXISTING LEGEND

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- (a) 3.25"± ASPHALT CONCRETE
- (B) 9"± REINFORCED CONCRETE PAVEMENT

PROPOSED LEGEND

- 1 ITEM 421 MICROSURFACING, SURFACE COURSE, AS PER PLAN (22 LBS/SY)
- ② ITEM 423 CRACK SEALING, TYPE II OR TYPE III



BIFURCATED SECTION - STATE ROUTE 2

€ S.R. 2

EXISTING LEGEND

€ WESTBOUND LANES

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- (Â) 3.25"± ASPHALT CONCRETE
- (B) 9"± REINFORCED CONCRETE PAVEMENT

54′

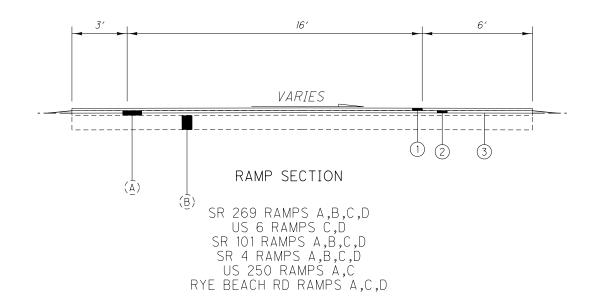
PROPOSED LEGEND

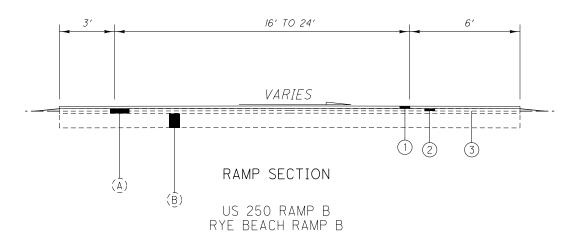
1 ITEM 421 MICROSURFACING, SURFACE COURSE, AS PER PLAN (22 LBS/SY)

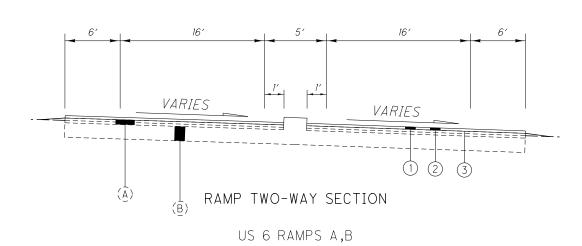
€ EASTBOUND LANES

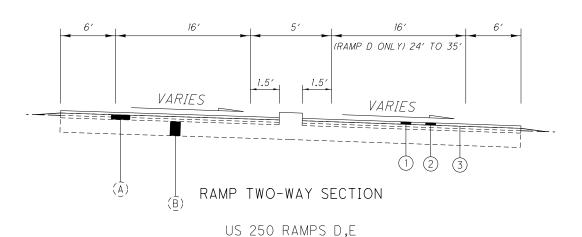
② ITEM 423 CRACK SEALING, TYPE II OR TYPE III











EXISTING LEGEND

- (a) 3.25"± ASPHALT CONCRETE
- (B) 9"± REINFORCED CONCRETE PAVEMENT

PROPOSED LEGEND

- 1 ITEM 421 MICROSURFACING, SURFACE COURSE, AS PER PLAN
- 2 ITEM 421 MICROSURFACING, LEVELING COURSE, AS PER PLAN
- 3 ITEM 423 CRACK SEALING, TYPE II OR TYPE III

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ROUTINE MAINTENANCE

BETWEEN THE TIME THAT BIDS ARE TAKEN AND THE START OF CONSTRUCTION, THE MAINTAINING AGENCY MAY ENTER UPON THE PROJECT AND PERFORM ROUTINE MAINTENANCE SUCH AS CRACK SEALING, PATCHING, AND BERM AND SHOULDER REPAIR. THE EFFECTS, IF ANY, OF THE PERFORMANCE OF ROUTINE MAINTENANCE SHALL BE CONSIDERED AS INHERENT IN WORK OF THE CHARACTER PROVIDED FOR IN THE PLAN AND THE RESULTING CONDITIONS SHALL NOT BE CONSIDERED AS DIFFERING MATERIALLY FROM THOSE EXISTING AT THE TIME

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS.

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

BUCKEYE CABLE SYSTEM 409 EAST MARKET STREET P.O. BOX 5800 SANDUSKY, OHIO 44870 419-627-1371

CARL F TIME WARNER CABLE 1575 LEXINGTON AVENUE MANSFIELD, OHIO 44901 419-756-6091

ELECTRIC OHIO EDISON COMPANY 2508 WEST PERKINS AVE. SANDUSKY, OHIO 44870 419-627-6881

COLUMBIA GAS OF OHIO 3101 NORTH RIDGE ROAD E LORAIN, OHIO 44055 440-240-6107

TELEPHONE ONE COMMUNITY 800 W. SAINT CLAIR 2ND FLOOR CLEVELAND, OHIO, 44113 216-633-5591

FRONTIER COMMUNICATIONS 83 TOWNSEND AVENUE NORWALK, OHIO 44857 419-744-3613

TELEPHONE VERIZON BUSINESS 120 RAVINE STREET AKRON, OHIO 44303 *330-253-8267*

TELEPHONE 13002 ECKEL JUNCTION ROAD PERRYSBURG, OHIO 43551 419 872-8534

TELEPHONE AT&T OF OHIO 130 N ERIE ST ROOM 714 TOLEDO OHIO 43604 419-245-7244

COUNTY ERIE COUNTY ENGINEER'S OFFICE 2700 COLUMBUS AVENUE SANDUSKY, OHIO 44870 419-627-7710

ERIE COUNTY SEWER 554 RIVER ROAD P.O. BOX 469 HURON, OHIO 44839 419-433-7303

CITY OF HURON 417 MAIN STREET HURON, OHIO 44839 *419-43*3-5000

CITY OF SANDUSKY 222 MEIGS AVENUE SANDUSKY, OHIO 44870 419-627-5844

STATE ODOT DISTRICT 3 TRAFFIC 906 CLARK AVENUE ASHLAND, OHIO 44805 419-207-7045

THE AFOREMENTIONED UTILITY COMPANIES AND AGENCIES HAVE VARIOUS FACILITIES IN THE AREA THAT WILL REMAIN IN PLACE DURING CONSTRUCTION.

WORK LIMITS

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THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

ITEM 423 - CRACK SEALING, TYPE II OR TYPE III

THE CONTRACTOR SHALL SEAL ALL VISABLE JOINTS AND CRACKS OVER TWO (2) FEET IN LENGTH ACCORDING TO ITEM 423 PRIOR TO MICROSURFACING.

PAYMENT WILL BE MADE AT THE CONTRACT UNIT BID PRICE PER SQUARE YARD.

SEQUENCE OF WORK

1) PERFORM PAVEMENT REPAIRS 2) CRACK SEAL PAVEMENT 3) MICROSURFACE MAINLINE, SHOULDERS AND RAMPS 4) APPLY PERMANENT STRIPING

ITEM 421 - MICROSURFACING, SURFACE COURSE, AS PER PLAN

ALL REQUIREMENTS OF ITEM 421 APPLY. IN ADDITION, SUPPLY A BLEND OF A MINIMUM OF 50% IGNEOUS DIABASE TRAP ROCK AND A MAXIMUM OF 50% LIMESTONE AGGREGATE FROM APPROVED SOURCES FOR USE AS AGGREGATE IN ITEM 421. DO NOT USE OTHER AGGREGATES.

OMIT ITEM 421 ON STRUCTURES WITH CONCRETE WEARING SURFACE.

THE CONTRACTOR IS RESPONSIBLE FOR COVERING ANY CASTINGS SO THE MICROSURFACING WILL NOT COVER THE CASTINGS (MONUMENT BOXES,

<u> ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR</u> <u>ITEM 253 - PAVEMENT REPAIR</u>

THESE ITEMS OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE.

PAVEMENT REPAIR SHALL BE PERFORMED BEFORE MICROSURFACING. THE DEPTH OF REMOVAL SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT WITH AN AVERAGE DEPTH OF 4" FOR ESTIMATING PURPOSES.

THE CONTRACTOR SHALL BE CAPABLE OF PERFORMING PAVEMENT REPAIRS 2

REPLACEMENT MATERIAL SHALL BE ITEM 301, ITEM 448 TYPE 2, OR ITEM 442 19MM MATERIAL AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE. ITEM 301 ASPHALT CONCRETE, PG64-22 CAN BE USED WHEN THE DEPTH OF THE REPAIR IS BETWEEN 3" AND 12" WITH A MAXIMUM PAVEMENT LIFT OF 6". ITEM 448 TYPE 2 OR ITEM 442 19MM CAN BE USED WHEN THE DEPTH OF THE REPAIR IS BETWEEN 1.5" AND 5" WITH A MAXIMUM PAVEMENT LIFT OF 3". THE CONTRACTOR HAS THE OPTION OF USING EITHER ITEM 301, ITEM 448 TYPE 2, OR ITEM 442 19MM MATERIAL WHEN THE PAVEMENT REPAIR IS BETWEEN 3" AND 5" DEEP. ITEM 448 TYPE 2 OR ITEM 442 19MM MATERIAL SHALL BE PG64-22 FOR MEDIUM MIX DESIGN PAVEMENTS AND PG64-28 FOR HEAVY MIX DESIGN PAVEMENTS.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR IS TO BE A MAXIMUM OF 4" DEEP AND ITEM 253 PAVEMENT REPAIR IS FOR DEPTHS GREATER THAN 4". PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER CUBIC YARD, (BY TICKET WEIGHT CONVERSION), OF ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR OR ITEM 253 - PAVEMENT REPAIR. THE FOLLOWING ESTIMATED QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

SR 2 ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR 420 CU. YD. SR 2 ITEM 253 - PAVEMENT REPAIR 50 CU. YD.

ITEM SPECIAL - AIR SPEED ZONE MARKINGS

THIS ITEM IS TO MEET CMS 646. THE SPEED MEASUREMENT MARKINGS ARE TO BE WHITE AND 24 INCHES WIDE (MEASURED IN THE DIRECTION OF TRAVEL) AND FOUR (4) FEET IN LENGTH.

PLACE THE MARKINGS AT 0.25 MILE INTERVALS OVER A ONE (1) MILE LENGTH OF ROADWAY ENTIRELY ON THE PAVED SHOULDERS. THE ZONES ARE FROM STA 757+00 TO STA 809+80 AND STA 907+50 TO STA 960+30, EAST AND WESTBOUND RESPECTIVELY.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO HAVE THE MARKINGS LAID OUT BY A REGISTERED SURVEYOR. A RECORD IS TO BE KEPT AND ONE ORIGINAL SIGNED AND SEALED DOCUMENT IS TO BE SENT TO THE DISTRICT 3 TRAFFIC ENGINEER AND ONE COPY FOR THE DISTRICT CONSTRUCTION ENGINEER.

MEASUREMENT AND PAYMENT: THE FIVE (5) MARKINGS PLACED EQUAL ONE ZONE. ONE ZONE WILL BE MEASURED
AS ONE (1) EACH. PAYMENT FOR ALL MATERIALS, LABOR, EQUIPMENT AND SURVEYING FOR ACCEPTED WORK IS TO BE INCLUDED PER EACH IN ITEM SPECIAL - AIR SPEED ZONE MARKINGS.

ITEM 644 THERMOPLASTIC PAVEMENT MARKINGS

ITEM 644 IS TO BE PLACED, AT MINIMUM, 14 CALENDAR DAYS AFTER ITEM 421 MICROSURFACING HAS BEEN PLACED

CONSTRUCTION NOTIFICATION

THE CONTRACTOR SHALL ADVISE THE PROJECT ENGINEER A MINIMUM OF FOURTEEN (14) DAYS PRIOR TO THE FOLLOWING: THE START OF CONSTRUCTION ACTIVITIES, LANE RESTRICTIONS, LANE CLOSURES, AND OR ROAD CLOSURES. THE PROJECT ENGINEER WILL FORWARD THIS INFORMATION TO THE FOLLOWING:

DISTRICT PUBLIC INFORMATION OFFICER (PIO) BY FAX AT (614) 887-4305 OR EMAIL AT DO3.PIO@DOT.STATE.OH.US

DISTRICT PERMIT SECTION BY FAX AT (419) 281-5925 OR EMAIL AT ERNIE.ROGGE@DOT.STATE.OH.US

CENTRAL OFFICE SPECIAL HAUL PERMITS SECTION BY FAX AT (614) 728-4099 OR EMAIL AT HAULING.PERMITS@DOT.STATE.OH.US

THE PIO WILL, IN TURN, NOTIFY THE PUBLIC, THE LOCAL EMERGENCY SERVICES, AFFECTED SCHOOLS AND BUSINESSES, AND ANY OTHER IMPACTED LOCAL PÚBLIC AGENCY OF ANY OF THE ABOVE MENTIONED ITEMS, VIA MEDIA

AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT, AT MAXIMUM OPERATING HEIGHT, SHALL EXCEED A HEIGHT OF 25 FEET. IF ANY TEMPORARY STRUCTURE OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA) AND ODOT OFFICE OF AVIATION BETWEEN THE PROPERTY OF THE PROPERTY WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. THE CONTRACTOR WILL BE REQUIRED TO FILE A NEW FAA FORM 7460-1 ADVISING THE FAA THAT AERONAUTICAL STUDY NUMBER(S) [SEE BELOW LIST] IS (ARE) BEING RESUBMITTED AND THAT AN ALTERATION TO THE ORIGINAL SUBMISSION(S) IS (ARF) REQUESTED

NOTIFY THE ODOT OFFICE OF AVIATION WHEN RESUBMITTING AN FAA FORM 7460-1. NO TEMPORARY STRUCTURES OF CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT UNTIL A COPY OF THE FAA APPORVAL AND THE OFFICE OF AVIATION PERMIT HAS BEEN FURNISHED TO THE PROJECT FNGINFFR.

FAA APROVAL MAY TAKEUP TO 45 DAYS. ALL SUBMISSIONS SHALL BE DIRECTED TO THESE OFFICES:

EXPRESS PROCESSING CENTER THE FEDERAL AVIATION ADMINISTRATION SOUTHWEST REGIONAL OFFICE AIR TRAFFIC AIRSPACE BRANCH ASW-520 2601 MEACHAN BLVD. FORT WORTH, TX 76137-4298 PREFERRED METHOD: WEBSITE: oeaaa.faa.gov

OHIO DEPARTMENT OF TRANSPORTATION OFFICE OF AVIATION 2829 WEST DUBLIN-GRANVILLE ROAD COLUMBUS, OH 43235 614.387.2346

	ERI-2-0.14			
LAT-LONG				
DEG-MIN-SEC	AERC	NAUTICAL S	TUDY NUME	BER
41° 23' 50.28"N, 82° 40' 29.72"W	2014-	AGL-	2682	-OE
41° 23' 51''N, 82° 40' 15.88''W	2014-	AGL-	2683	-OE
41° 23' 56.4"N, 82° 39' 41.95"W	2014-	AGL-	2684	-OE
41° 24' 2.88"N, 82° 39' 8.47"W	2014-	AGL-	2685	-OE
41° 24' 7.2"N, 82° 38' 34.43"W	2014-	AGL-	2686	-OE
41° 24' 11.16"N, 82° 38' 0.09"W	2014-	AGL-	2687	-OE
41° 24' 14.76"N, 82° 37' 25.74"W	2014-	AGL-	2688	-OE
41° 24' 18.72"N, 82° 36' 51.43"W	2014-	AGL-	2689	-OE
41° 24' 21.24"N, 82° 36' 17.23"W	2014-	AGL-	2690	-OE

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ITEM 614 - WORK ZONE MARKING SIGN

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR TEMPORARY WORK ZONE MARKING SIGNS PER THE REQUIREMENTS OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS, 614.04.

WORK ZONE MARKING SIGN: (W8-H12A-36) NO EDGE LINE

= 22 EACH

<u>ITEM 614 - MAINTAINING TRAFFIC</u> (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS FOURTH OF JULY NEW YEARS LABOR DAY MEMORIAL DAY THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE TIME ALL LANES MUST BE OPEN TO TRAFFIC

SUNDAY 12:00N FRIDAY THROUGH 6:00 AM MONDAY MONDAY 12:00N FRIDAY THROUGH 6:00 AM TUESDAY TUESDAY 12:00N MONDAY THROUGH 6:00 AM TUESDAY THROUGH 6:00 AM TUESDAY THROUGH 6:00 AM THURSDAY 12:00N WEDNESDAY THROUGH 6:00 AM MONDAY FRIDAY 12:00N THURSDAY THROUGH 6:00 AM MONDAY SATURDAY 12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE FEE OF \$1500 PER DAY.

ITEM 614 - MAINTAINING TRAFFIC: GENERAL

ONE 11' LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES. ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH ITEM 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, PLAN DETAILS, STANDARD DRAWINGS, AND AS OUTLINED IN THE CONSTRUCTION AND MAINTENANCE SECTION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES CURRENT EDITION WITH THE LATEST REVISIONS. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED ON THIS PLAN.

THE FOLLOWING REQUIREMENTS SHALL ALSO APPLY:
THE CONTRACTOR SHALL SUBMIT, IN WRITING, A SCHEDULE OF OPERATIONS TO
THE ENGINEER AND RECEIVE APPROVAL BEFORE WORK IS STARTED ON THE
PROJECT. PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COORDINATE THE
MAINTENANCE OF TRAFFIC OPERATIONS WITH THE LOCAL STATE HIGHWAY
PATROL.

NIGHT WORK IS PERMITTED.

THE CONTRACTOR IS REQUIRED TO MAINTAIN ALL PAVEMENT THROUGHOUT THE PROJECT UNDER ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC DURING THE PERIOD FROM THE START OF WORK TO THE COMPLETION OF ALL WORK.

ITEM 614 - MAINTAINING TRAFFIC

ALL ADVANCE WARNING SIGNS FOR ANY CONDITION WHICH RESTRICTS TRAFFIC SHALL BE ERECTED BEFORE ANY SUCH RESTRICTION IS PUT INTO EFFECT. ALL SUCH SIGNS SHALL BE COVERED OR REMOVED FROM THE VIEW OF TRAFFIC WHEN THEY ARE NOT APPLICABLE, WITH THE APPROVAL OF THE ENGINEER.

IF THE CONTRACTOR FAILS TO COMPLY WITH THE PROVISIONS FOR TRAFFIC CONTROL AS SET FORTH IN THESE PLANS OR WITH PROVISIONS OF THE OMUTCD, AND SUCH FAILURE RESULTS IN A CONDITION AT THE WORK SITE WHICH IS UNSAFE FOR TRAFFIC, THE ENGINEER SHALL SUSPEND WORK UNTIL THE CONTRACTOR COMPLIES WITH THE NECESSARY REQUIREMENTS.

ALL MAINTENANCE OF TRAFFIC SIGNS ARE PAID UNDER ITEM 614 - MAINTAINING TRAFFIC.

PLACEMENT OF WORK ZONE PAVEMENT MARKINGS

THE CONTRACTOR SHALL PLACE EDGE LINES AND LANE LINES AT THE END OF EACH WORK SHIFT WHEN EXISTING LINES HAVE BEEN OBLITERATED.

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN, ON SITE, FOR THE DURATION OF THE PROJECT. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS MAINTAINED BY THE DIRECTOR (OFFICE OF MATERIALS MANAGEMENT). THE APPROVED LIST OF PORTABLE CHANGEABLE MESSAGE SIGNS CAN BE FOUND ON THE ODOT WEBSITE BY CLICKING ON THE SERVICES MENU, THEN CLICKING ON MATERIALS MANAGEMENT. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 650 FT. AND 475 FT., RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. PCMS TRAILERS SHALL BE DELINEATED ON A PERMANENT BASIS BY AFFIXING CONSPICUITY TAPE CONFORMING TO CMS 614.03, IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER AS SEEN BY ONCOMING ROAD USERS.

THE PROBABLE PCMS LOCATIONS WILL BE DETERMINED BY THE ENGINEER PRIOR TO BEGINNING WORK ON THIS PROJECT. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED, FACING AWAY FROM ALL TRAFFIC, AND SHALL DISPLAY ONE OR MORE YELLOW RETROREFLECTIVE SHEETING SURFACES OF 9-INCH BY 15-INCH MINIMUM SIZE FACING TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

(THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN __ HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.)

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PREPROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PREPROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.)

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF CMS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK. THE CONTRACTOR SHALL ONLY BE PAID FOR PCMS UNITS WHEN THEY ARE IN OPERATION ON THE PROJECT AS SPECIFIED IN THE PLANS OR BY THE ENGINEER.

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 120 DAY

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE LATEST EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED LAW ENFORCEMENT OFFICER (AND OFFICIAL PATROL CAR WITH MOUNTED EMERGENCY FLASHING LIGHTS) SHALL BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS AS DIRECTED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED.

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION.

LAW ENFORCEMENT OFFICERS (LEO'S) SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED. THE LEO'S ARE CONSIDERED TO BE EMPLOYED BY THE CONTRACTOR AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR ACTIONS. ALTHOUGH THEY ARE EMPLOYED BY THE CONTRACTOR, THE PROJECT ENGINEER SHALL HAVE CONTROL OVER THEIR PLACEMENT. THE OFFICIAL PATROL CAR SHALL BE A PUBLIC SAFETY VEHICLE AS REQUIRED BY THE OHIO REVISED CODE. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEO'S SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THESE SERVICES AND PROVIDE 72 HOURS ADVANCE NOTICE AS REQUIRED BY THE HIGHWAY PATROL LISTED BELOW:

STATE HIGHWAY PATROL 300 SOUTH NORWALK ROAD NORWALK, OHIO 44857 419-668-4087

LAW ENFORCEMENT OFFICERS WITH PATROL CAR REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 120 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

IF THE CONTRACTOR WISHES TO UTILIZE LEO'S FOR FLAGGING AND TRAFFIC CONTROL OTHER THAN FOR THAT REQUIRED IN THESE PLANS, THEY MAY DO SO AT THEIR OWN EXPENSE.

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC

TEMPORARY WEDGES AT END OF RAMPS, PAVEMENT LAYER ENDS, APPROACH SLABS OR BRIDGE DECKS ARE TO BE CONSTRUCTED AS PER STANDARD DRAWING BP-3.1.

THIS ITEM SHALL ALSO BE USED TO REPAIR EXISTING HOLES, LEFT BEHIND BY PREVIOUS WORK TO REMOVE RPMS, AND OTHER PAVEMENT DEFECTS AS DIRECTED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR MAINTENANCE OF TRAFFIC.

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC 75 CU YD

BUTT JOINTS

BUTT JOINTS SHALL NOT BE CUT AND LEFT OPEN TO TRAFFIC. THEY SHALL BE FILLED IN WITH A TEMPORARY ASPHALT CONCRETE WEDGE USING ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC.

CONSTRUCTION "BUMP" (W8-1-36) AND "ADVISORY SPEED" (W13-1-24) SIGNS SHALL BE ERECTED AND MAINTAINED DURING THE PERIOD THE BUTT JOINT IS LEFT OPEN. THESE SIGNS SHALL BE PAID FOR UNDER THE LUMP SUM ITEM FOR ITEM 614 MAINTAINING TRAFFIC.

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ITEM 614 - WORKSITE TRAFFIC SUPERVISOR

SUBJECT TO APPROVAL OF THE ENGINEER, THE CONTRACTOR SHALL EMPLOY AND IDENTIFY (SOMEONE OTHER THAN THE SUPERINTENDENT) A CERTIFIED WORKSITE TRAFFIC SUPERVISOR (WTS) BEFORE STARTING WORK IN THE FIELD. THE WTS MAY BE CERTIFIED FROM ONE OF THE FOLLOWING ORGANIZATIONS:

AMERICAN TRAFFIC SAFETY SERVICE ASSOCIATION (ATSSA), PHONE NUMBER 1-800-272-8772. CERTIFIED TRAFFIC CONTROL SUPERVISOR (TCS).

- 2. NATIONAL HIGHWAY INSTITUTE, DESIGN AND OPERATION OF WORK ZONE TRAFFIC CONTROL, PHONE NUMBER 1-703- 235-0528.
- 3. THE OHIO CONTRACTORS ASSOCIATION, TRAFFIC CONTROL SUPERVISOR (OCA/TCS) WORK ZONE CLASS, ONLY IF TAKEN AFTER MAY 5, 2004, PHONE NUMBER 1-614-599-
- 4. OHIO LABORERS TRAINING, TRAFFIC CONTROL SUPERVISORS CLASS, PHONE NUMBER

A COPY OF EACH WTS'S CERTIFICATION AND 24-HOUR CONTACT INFORMATION SHALL BE PROVIDED TO THE ENGINEER AT THE PRECONSTRUCTION CONFERENCE. IF THE DESIGNATED WTS WILL NOT BE AVAILABLE FULL TIME (24/7) THE CONTRACTOR MAY DESIGNATE AN ALTERNATE WTS TO BE AVAILABLE WHEN THE PRIMARY IS OFF DUTY. EACH WTS SHALL HAVE A CURRENT WTS CERTIFICATION (WITH AN EXPIRATION DATE NO MORE THAN 5 YEARS FROM THE DATE OF ISSUE) FROM ANY OF THE APPROVED

THE WTS POSITION HAS THE RESPONSIBILITY OF MONITORING TRAFFIC CONTROL DEFICIENCIES FOR THE ENTIRE WORK ZONE. THE DUTIES OF THE WTS ARE AS FOLLOWS:

- 1. BE AVAILABLE ON A 24-HOUR PER DAY BASIS, AND BE ABLE TO BE ON SITE FOR ALL EMERGENCY TRAFFIC CONTROL NEEDS WITHIN ONE HOUR OF NOTIFICATION BY POLICE OR PROJECT STAFF AND BE PREPARED TO EFFECT CORRECTIVE MEASURES IMMEDIATELY ON EXISTING WORK ZONE TRAFFIC CONTROL DEVICES.
- 2. ATTEND PRECONSTRUCTION MEETING AND ALL PROJECT MEETINGS WHERE TRAFFIC CONTROL MANAGEMENT IS DISCUSSED.
- 3. BE AVAILABLE FOR MEETINGS OR DISCUSSIONS WITH THE ENGINEER UPON REQUEST
- BE AWARE OF, AND COORDINATE IF NECESSARY, ALL TRAFFIC CONTROL OPERATIONS. INCLUDING THOSE OF SUBCONTRACTORS AND SUPPLIERS.
- 5. COORDINATE PROJECT ACTIVITIES WITH ALL LAW ENFORCEMENT OFFICERS (LEOS). A WTS SHALL ALSO BE THE MAIN CONTACT PERSON WITH THE LEO'S WHILE THEY ARE
- 6. COORDINATE MEETINGS WITH ODOT PERSONNEL, LEO'S AND OTHER APPLICABLE ENTITIES BEFORE EACH PLAN PHASE SWITCH TO DISCUSS WORK ZONE TRAFFIC
- ENSURE COMPLIANCE WITH THE CONTRACT DOCUMENTS FOR SIGNS, BARRICADES, TEMPORARY CONCRETE BARRIER, PAVEMENT MARKINGS, PORTABLE MESSAGE SIGNS, AND OTHER TRAFFIC CONTROL DEVICES ON A DAILY BASIS; AND FACILITATE ANY CORRECTIVE ACTION NECESSARY.
- 8. NOTIFY THE CONTRACTOR OF THE NEED FOR CLEANING AND MAINTENANCE OF ALL TRAFFIC CONTROL DEVICES, INCLUDING THE COVERING AND REMOVAL OF INAPPLICABLE SIGNS.
- 9. INSPECT, EVALUATE, PROPOSE NECESSARY MODIFICATIONS TO, AND DOCUMENT THE EFFECTIVENESS OF, THE TRAFFIC CONTROL DEVICES AND/OR TRAFFIC OPERATIONS ON A DAILY BASIS (7 DAYS A WEEK). IN ADDITION, A WEEKLY NIGHT INSPECTION OF THE WORK ZONE SETUP FOR DAYTIME WORK OPERATIONS; AND ONE DAYTIME INSPECTION PER WEEK FOR NIGHTTIME PROJECTS. THIS SHALL INCLUDE (BUT NOT BE LIMITED TO) DOCUMENTATION ON THE FOLLOWING PROJECT EVENTS:
- A. INITIAL TRAFFIC CONTROL SETUP (DAY AND NIGHT REVIEW). B. DAILY TRAFFIC CONTROL SETUP AND REMOVAL. C. WHEN CONSTRUCTION STAGING CAUSES A CHANGE IN THE TRAFFIC CONTROL SETUP. D. CRASH OCCURRENCES WITHIN THE CONSTRUCTION AREA.
- REMOVAL OF TRAFFIC CONTROL DEVICES AT THE END OF A PHASE OR PROJECT. F. ALL OTHER EMERGENCY TRAFFIC CONTROL NEEDS.
- 10. COMPLETE THE DEPARTMENT APPROVED LONG TERM INSPECTION FORM (CA-D-8)
 AFTER EACH INSPECTION AS REQUIRED IN # 9 AND SUBMIT IT TO THE ENGINEER THE
 FOLLOWING WORK DAY. THESE REPORTS SHALL INCLUDE A CHECKLIST OF ALL TRAFFIC
 CONTROL MAINTENANCE ITEMS TO BE REVIEWED. A COPY OF THE FORM WILL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING. ANY DEFICIENCIES OBSERVED SHALL BE NOTED, ALONG WITH RECOMMENDED CORRECTIVE ACTIONS AND THE DATES BY WHICH SUCH CORRECTIONS WERE, OR WILL BE, COMPLETED. A COPY OF THIS DOCUMENT CAN BE FOUND IN THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION INSPECTION FORMS MANUAL DATED 10/15/06 OR CURRENT REVISION.
- 11. VERIFY THAT ALL FLAGGING OPERATIONS ARE BEING CONDUCTED PER THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
- 12. HAVE COPIES OF THE ODOT TEMPORARY TRAFFIC CONTROL MANUAL AND APPLICABLE STANDARDS AND SPECIFICATIONS INCLUDED IN THE CONTRACT DOCUMENTS AVAILABLE AT ALL TIMES ON THE PROJECT.

THE DEPARTMENT WILL NOT PAY THE UNIT PRICE BID FOR THE WTS FOR ANY DAY ON WHICH THE CONTRACTOR FAILS TO PERFORM THE DUTIES SET FORTH ABOVE. SHOULD THE CONTRACTOR'S FAILURE TO PERFORM ANY OF THE DUTIES DESCRIBED ABOVE RESULT IN A MAINTENANCE OF TRAFFIC SAFETY ISSUE, THE DEPARTMENT WILL DEDUCT THE PRORATED DAILY AMOUNT FOR ITEM 614 MAINTENANCE OF TRAFFIC FROM THE CONTRACTOR'S NEXT SCHEDULED ESTIMATE.

IF THREE OR MORE FAILURES TO PERFORM THE DUTIES SET FORTH ABOVE OCCUR, THE WTS SHALL BE IMMEDIATELY REMOVED FROM THE WORK IN ACCORDANCE WITH C&MS

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED FOR THE WORKSITE

ITEM 614 - WORKSITE TRAFFIC SUPERVISOR 3 MONTHS

ITEM 614 - REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614 - REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 25 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT UNIT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

WORK OPERATIONS

IN ADDITION TO THE REQUIREMENTS OF SECTION 614 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS THE FOLLOWING SHALL APPLY:

THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAVEL WHERE PRACTICAL. A FLAGGER SHALL BE USED WHERE THE CONTRACTOR'S EQUIPMENT MUST MERGE WITH THE TRAFFIC STREAM.

THE CONTRACTOR SHALL ARRANGE CONSTRUCTION OPERATIONS SO AS TO PREVENT ANY INTERFERENCE TO THE CONTINUOUS FLOW OF TRAFFIC. ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO THE CLOSED LANES UNLESS OTHERWISE APPROVED BY THE ENGINEER.

<u> ITEM 614 - MAINTAINING TRAFFIC LANE CLOSURE/REDUCTION</u> REQUIRED

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

GENERAL LANE CLOSURE LIMITATIONS

THE INTENT OF THE LANE CLOSURE LIMITATIONS IN THIS PLAN NOTE IS TO SUPPLEMENT OTHER TIME LIMITATIONS WHICH APPEAR IN THIS CONTRACT.

THE FOLLOWING LANE CLOSURE RESTRICTIONS APPLY: 1. NO WORK SHALL BE DONE ON ANY WEEKENDS IN THE MONTH OF AUGUST

FAILURE OF THE CONTRACTOR TO MEET ANY OF THE ABOVE REQUIREMENTS ARE SUBJECT TO LIQUIDATED DAMAGES AS PER CMS 108.07.

RAMP CLOSURE LIMITATIONS

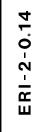
THE INTENT OF THE RAMP CLOSURE LIMITATIONS IN THIS PLAN NOTE IS TO SUPPLEMENT OTHER TIME LIMITATIONS WHICH APPEAR IN THIS CONTRACT.

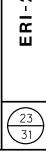
THE FOLLOWING LANE CLOSURE RESTRICTIONS APPLY: 1. NO WORK SHALL BE DONE ON THE US 250 RAMPS ON ANY WEEKENDS

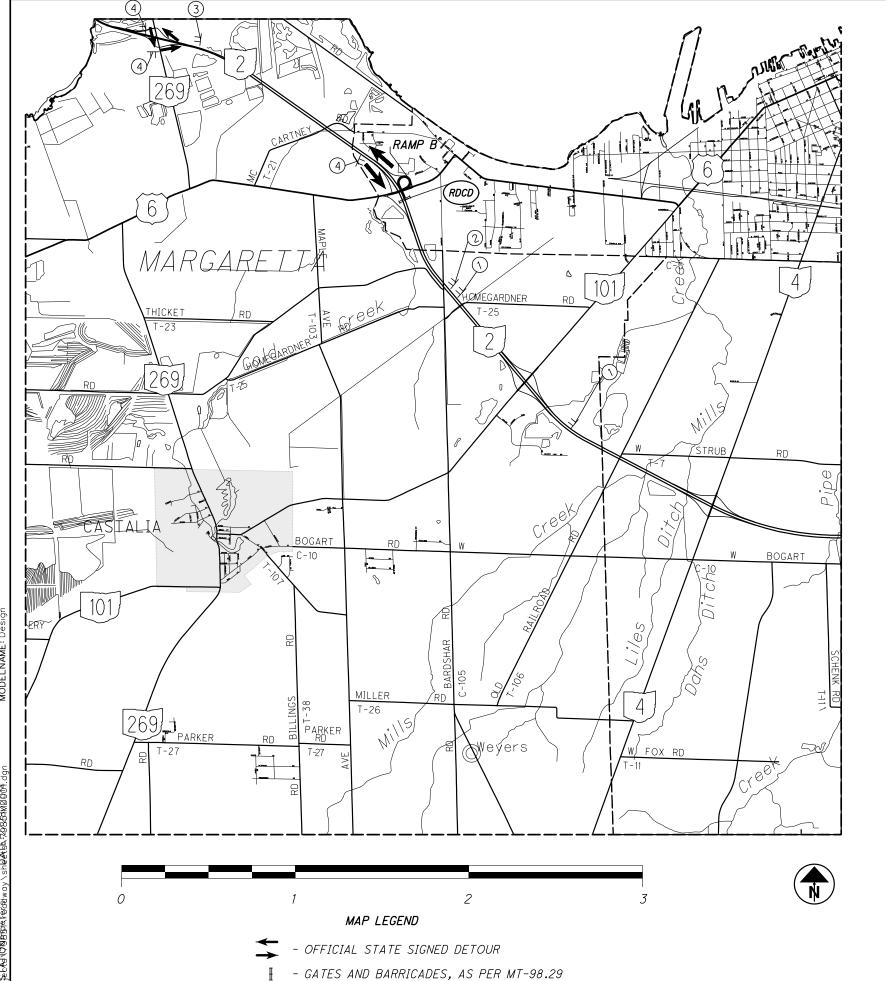
FAILURE OF THE CONTRACTOR TO MEET ANY OF THE ABOVE REQUIREMENTS ARE SUBJECT TO LIQUIDATED DAMAGES AS PER CMS 108.07.

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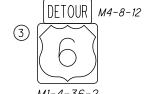
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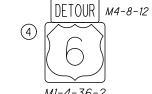
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ITEM 614 -PORTABLE CHANGEABLE MESSAGE SIGN

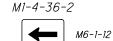


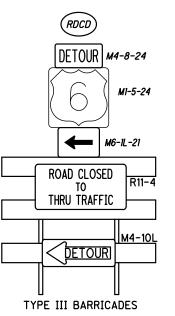
SIGN LEGEND





M1-4-36-2





DETOUR SIGNING

THE FOLLOWING QUANTITY IS INCLUDED FOR THE CONTRACTOR TO PROVIDE THE DETOUR SIGNING AS SHOWN AS PER 614.06 (B):

ITEM 614, DETOUR SIGNING LUMP

DETOUR FOR CLOSURE OF THE SR 2 WESTBOUND FXIT RAMP TO US 6 (RAMP B)

A TWO NIGHT CLOSURE WILL BE PERMITTED FOR THE PLANING AND PAVING OF THE RAMP. THE CONTRACTOR MAY ONLY WORK FROM 9 PM TO 6 AM. THE RAMP MUST REMAIN OPEN AT ALL TIMES DURING THE DAY.

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* - FOR TYPICALS, SEE SHEETS 17-19 254 254 407 421 421 421 423 442 LENGTH CHKD BY LOG POINT KRB ASHALT CONCRETE SURFACE TO MICROSURFACING, LEVELII COURSE, AS PER PLAN MICROSURFACING, SURFA COURSE, AS PER PLAN FEET PAVEMENT AREA PLANING ASPHALT CONCRETE (1.5") PATCHING PLANED SURFACE COURSE, 12.5 MM, TYPE A, (446), AS PER PLAN DO NOT PLACE MICROSURFACING ON EXPOSED CONCRETE BRIDGE DECKS & APPROACH SLABS ROUTE LOG POINT TACK COAT @ MILE FEET 0.08 GAL/SY 22 LBS/SY 1.5 INCHES STRAIGHT LINE MILEAGE SQ YD SQ YD SQ YD GALLON SQ YD SQ YD SQ YD SQ YD 0.14 15.63 81787 25.0 227,186 227,186 15.49 227,186 0.14 15.63 81787 25.0 227,186 227,186 227,186 15.49 ⋖ SHOULDERS ER OUTSIDE 0.14 15.63 15.49 81787 10.0 90,874 90,874 90,874 SHOULD WB 0.14 15.63 15.49 81787 10.0 90,874 90,874 90,874 81787 4.0 36,350 ERI 2 EB 0.14 15.63 15.49 36,350 36,350 ∞ಶ 0.14 15.49 81787 4.0 36,350 36,350 36,350 EMENT ACEL/DECEL LANES 0.09 0.09 500 25.0 SR 269 - RAMP A 0.00 1,389 1,389 1,389 SR 269 - RAMP B 0.00 0.20 0.20 1050 25.0 2,917 2,917 2,917 A V SR 269 - RAMP 2,250 2,250 2,250 25.0 3,403 3,403 3,403 SR 269 - RAMP D 0.00 0.23 0.23 1225 US 6 - RAMP A 0.16 0.16 2,389 2,389 2,389 0.00 US 6 - RAMP B 0.16 0.16 860 25.0 2,389 2,389 2,389 0.16 0.16 820 25.0 2,278 2,278 2,278 US 6 - RAMP (25.0 0.19 1025 2,847 2,847 2,847 US 6 - RAMP D 0.00 0.19 SR 101 - RAMP A 0.17 0.17 875 2,431 2,431 2,431 SR 101 - RAMP B 0.00 0.16 0.16 850 25.0 2,361 2,361 2,361 0.19 995 25.0 2,764 2,764 2,764 SR 101 - RAMP C 0.00 0.19 SR 101 - RAMP D 0.16 820 25.0 2,278 2,278 2,278 0.00 0.16 0.24 1250 25.0 3,472 3,472 3,472 SR 4 - RAMP A 0.24 0.16 850 25.0 0.00 0.16 2,361 2,361 2,361 SR 4 - RAMP B 1020 25.0 SR 4 - RAMP C 0.00 0.19 0.19 2,833 2,833 2,833 SR 4 - RAMP D 0.00 0.17 0.17 900 25.0 2,500 2,500 2,500 25.0 US 250 - RAMP A 0.21 0.21 1100 3,056 3,056 3.056 25.0 US 250 - RAMP B 0.00 0.17 0.17 900 2,500 2,500 2,500 ERI-2-0 US 250 - RAMP C 0.00 0.17 895 25.0 2,486 2,486 2,486 US 250 - RAMP D 0.65 3450 25.0 9,583 9,583 9,583 25.0 0.27 1400 3,889 3,889 3,889 US 250 - RAMP E 0.00 0.27 RYE BEACH - RAMP A 0.00 0.22 0.22 1150 3,194 3,194 3,194 RYE BEACH - RAMP B 0.40 0.40 2100 25.0 5,833 5,833 5,833 0.27 0.27 1400 25.0 3,889 3,889 3,889 RYE BEACH - RAMP C 25.0 1,667 1,667 RYE BEACH - RAMP D 0.00 0.11 0.11 600 1,667 XTRA AREA GORE AREAS 8809 8,809 8,809 EXTRA AREA FOR U-TURNS 4200 4,200 4,200 DEDUCTION FOR CONCRETE STRUCTURES MAINLINE 2284 20810 -20,810 -20,810 25 TOTALS 98.19 518428 777,978 777,978 31

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* - FOR TYPICALS, SEE SHEETS 17-19 254 254 407 421 421 421 423 442 CHKD BY LOG POINT TO CONCRETE FEET PAVEMENT DO NOT PLACE MICROSURFACING ON EXPOSED CONCRETE BRIDGE DECKS & APPROACH SLABS PLANING ASPHALT CONCRETE PATCHING ROUTE LOG POINT TACK COAT @ 0.08 GAL/SY FEET COURSE, 12.5 MM, TYPE A, MILE AVG. PLANED SURFACE MICROSURFACING, COURSE, AS PER P (446) 22 LBS/SY 1.5 INCHES STRAIGHT LINE MILEAGE SQ YD SQ YD SQ YD GALLON SQ YD SQ YD SQ YD SQ YD RAMPS SR 269 - RAMP A 0.13 0.13 700 25.0 1,944 0.17 890 25.0 2,472 SR 269 - RAMP B 0.17 2,472 2,472 2,472 ⋖ Δ 25.0 SR 269 - RAMP C 0.00 0.18 0.18 925 2,569 2,569 2,569 2,569 SR 269 - RAMP D 0.00 0.16 0.16 840 25.0 2,333 2,333 2,333 2,333 ш SHOULD US 6 - RAMP A 0.26 0.26 1350 3,750 3,750 3,750 200 44.0 978 978 US 6 - RAMP A 0.29 0.04 978 978 1500 25.0 4,167 4.167 417 333 174 US 6 - RAMP B 0.00 0.28 0.28 US 6 - RAMP C 0.00 0.18 0.18 950 2,639 2,639 2,639 2,639 US 6 - RAMP D 0.00 0.29 0.29 1525 25.0 4,236 4,236 4,236 4,236 જ ENT SR 101 - RAMP A 0.00 0.19 0.19 1020 25.0 2,833 2,833 2,833 2,833 1140 25.0 3,167 3,167 3,167 0.00 0.22 0.22 3.167 SR 101 - RAMP B Ε 1375 3,819 SR 101 - RAMP C 0.00 0.26 0.26 3,819 3,819 3,819 A < SR 101 - RAMP D 0.00 0.21 0.21 1120 25.0 3,111 3,111 3,111 3,111 Δ 1030 25.0 SR 4 - RAMP A 0.00 0.20 0.20 2,861 2,861 2,861 2,861 SR 4 - RAMP B 0.00 0.12 0.12 650 25.0 1,806 1,806 1.806 1.806 SR 4 - RAMP C 0.21 0.21 1100 3,056 3,056 0.00 3,056 3,056 SR 4 - RAMP D 0.00 0.22 0.22 1140 25.0 3,167 3,167 3,167 3,167 US 250 - RAMP A 1300 25.0 3,611 3,611 0.00 0.25 0.25 3,611 3,611 1010 25.0 US 250 - RAMP B 0.04 0.23 0.19 2,806 2,806 2,806 2,806 0.00 0.04 0.04 200 733 US 250 - RAMP B 1000 25.0 US 250 - RAMP C 0.19 0.19 2,778 2,778 2,778 2,778 0.23 0.23 1230 44.0 6.013 6.013 6,013 US 250 - RAMP D US 250 - RAMP E 25.0 0.00 0.24 0.24 1250 3,472 3,472 3,472 3,472 0.00 0.16 870 2,417 2,417 2,417 0.08 400 25.0 1,111 RYE BEACH - RAMP B 0.00 0.08 1,111 1,111 33.0 RYE BEACH - RAMP B 0.08 0.17 0.09 500 1,833 1.833 1.833 1.833 RYE BEACH - RAMP C 0.00 0.20 0.20 1050 25.0 2,917 2,917 2,917 2,917 RYE BEACH - RAMP D 0.00 0.17 0.17 875 25.0 2,431 2,431 2,431 2,431 o ERI-2-(6241 EXTRA AREA FOR RAMP INTERSECTIONS 6,241 6,241 6,241 DEDUCTION FOR CONCRETE STRUCTURES RAMP D (SR-6) 129 358 -358 -358 -358 TOTALS 26 5.14 27140 4,900 490 392 80.013 80.013 80.013 205 31

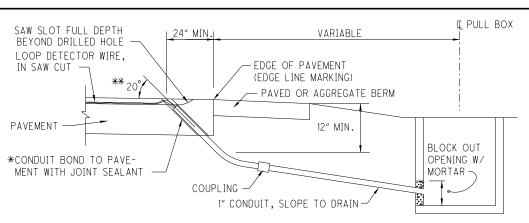
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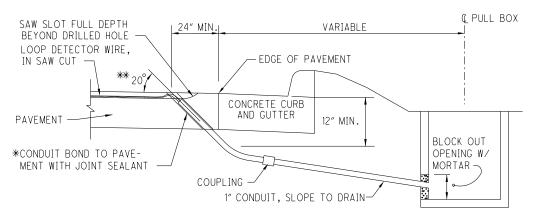
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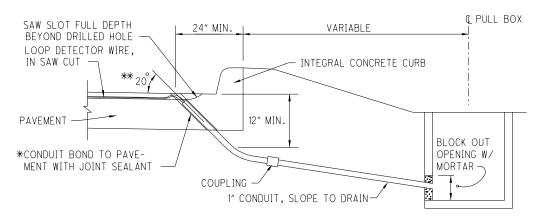
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DRILLED HOLE LOCATION DETAIL WITH PAVED OR AGGREGATE BERM



DRILLED HOLE LOCATION DETAIL WITH CONCRETE CURB AND GUTTER



DRILLED HOLE LOCATION DETAIL WITH INTEGRAL CONCRETE CURB

- * CONDUIT SHALL BE I" DIAMETER 725.04.
- ** THE RANGE OF THIS ANGLE SHALL BE FROM 15 TO 30 DEGREES.

NOTE: SEE STANDARD DRAWING TC-82.10 FOR ADDITIONAL NOTES AND DETAILS

ITEM 632- DETECTOR LOOP, AS PER PLAN

AN ESTIMATED QUANTITY OF ITEM 632, DETECTOR LOOP, AS PER PLAN, HAS BEEN PROVIDED FOR THE PURPOSE OF REPLACING DAMAGED DETECTOR LOOPS AND/OR UPGRADING DETECTOR LOOPS TO IMPROVE MOTORCYCLE DETECTION. IT IS IMPERA-TIVE THAT REPLACEMENT OF DETECTOR LOOPS BE INSTALLED AND FULLY FUNCTIONAL IN THE SHORTEST POSSIBLE TIME. THE CONTRACTOR SHALL HAVE REPLACEMENT DETECTOR LOOPS INSTALLED AND FULLY FUNCTIONAL WITHIN 7 CALENDAR DAYS OF DESTRUCTION OF THE EXISTING DETECTOR LOOPS.

THE CONTRACTOR SHALL NOTIFY MATT BLANKENSHIP, ODOT DISTRICT 3 ROADWAY SERVICES MANAGER. (PHONE 419-207-7045) 5 WORKING DAYS IN ADVANCE OF ANY PLANING OPERATIONS OR PAVEMENT REPAIR WORK. THIS NOTIFICATION IS NEEDED FOR DISTRICT 3 TO SCHEDULE TEMPORARY SIGNAL TIMING MODIFICATIONS FOR THE TIME PERIOD WHEN THE DETECTOR LOOPS ARE OUT OF OPERATION. THE CONTRACTOR SHALL THEN RENOTIFY MR. BLANKENSHIP WITHIN 2 WORKING DAYS AFTER THE NEW DETECTOR LOOPS ARE REPLACED SO THAT HE CAN RESCHEDULE DISTRICT CREWS TO RESTORE SIGNAL TIMINGS TO THE ORIGINAL SETTINGS.

FAILURE TO COMPLY WITH THE ABOVE STATED REQUIREMENTS WILL RESULT IN THE ASSESSMENT OF A DISINCENTIVE FEE OF \$500.00 PER DAY TO THE CONTRACTOR FOR EACH CALENDAR DAY BEYOND THE SPECIFIED LIMIT.

THE NEW DETECTOR LOOPS SHALL BE PLACED PER THE PLAN DETAILS AFTER THE PLANING AND PAVEMENT REPAIR OPERATIONS ARE COMPLETED WITHIN THE AFFECTED AREAS. THE DETECTOR LOOPS SHALL NOT BE CUT INTO THE SURFACE COURSE.

IN ADDITION TO THE REQUIREMENTS OF CMS 632.11, THE CONTRACTOR SHALL PROVIDE A POSITIVE AND EFFECTIVE MEANS FOR REMOVAL OF SOLID RESIDUE RESULTING FROM THE DRY SAW BLADE CUTTING OF LOOP DETECTOR SLOTS IN THE PAVEMENT. THE RESIDUE SHALL BE REMOVED BY VACUUM OR OTHER EFFECTIVE MEANS. BEFORE IT IS BLOWN BY TRAFFIC ACTION OR WIND. RESIDUE FROM DRY CUTTING SHALL NOT BE REMOVED BY COMPRESSED AIR. AS AN ALTERNATE, THE CONTRACTOR MAY USE WET

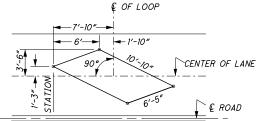
LOOP DETECTOR WIRE TO LEAD-IN CABLE SPLICES WITHIN EPOXY ENCAPSULATED SPLICE ENCLOSURES SHALL BE JOINED BY AN APPROVED CONNECTOR AND SOLDERED PER CMS 632.23 & 725.15. ALL COSTS ASSOCIATED WITH THE SOLDERED SPLICE CONNECTION AND EPOXY SPLICE KIT SHALL BE INCLUDED WITH THE DETECTOR LOOP.

IF THE PULL BOX IS NOT SPECIFIED IN THE PLANS, THE SPLICE SHALL BE MADE IN THE FIRST ENTERED POLE OR PEDESTAL, EXCEPT WHERE THE CONTROLLER CABINET IS MOUNTED ON THE POLE OR PEDESTAL. IN WHICH CASE THE LOOP WIRES SHALL BE ROUTED DIRECTLY INTO THE CABINET UNLESS SPECIFIED DIFFERENTLY IN THE PLANS. LOOP DETECTOR WIRE ROUTED THROUGH CONDUIT, PULL BOXES, POLES, AND PEDESTALS SHALL BE TWISTED PER CMS 632.23.

FURNISH ALL MATERIALS ACCORDING TO THE DEPARTMENT'S QUALIFIED PRODUCTS LIST (QPL).

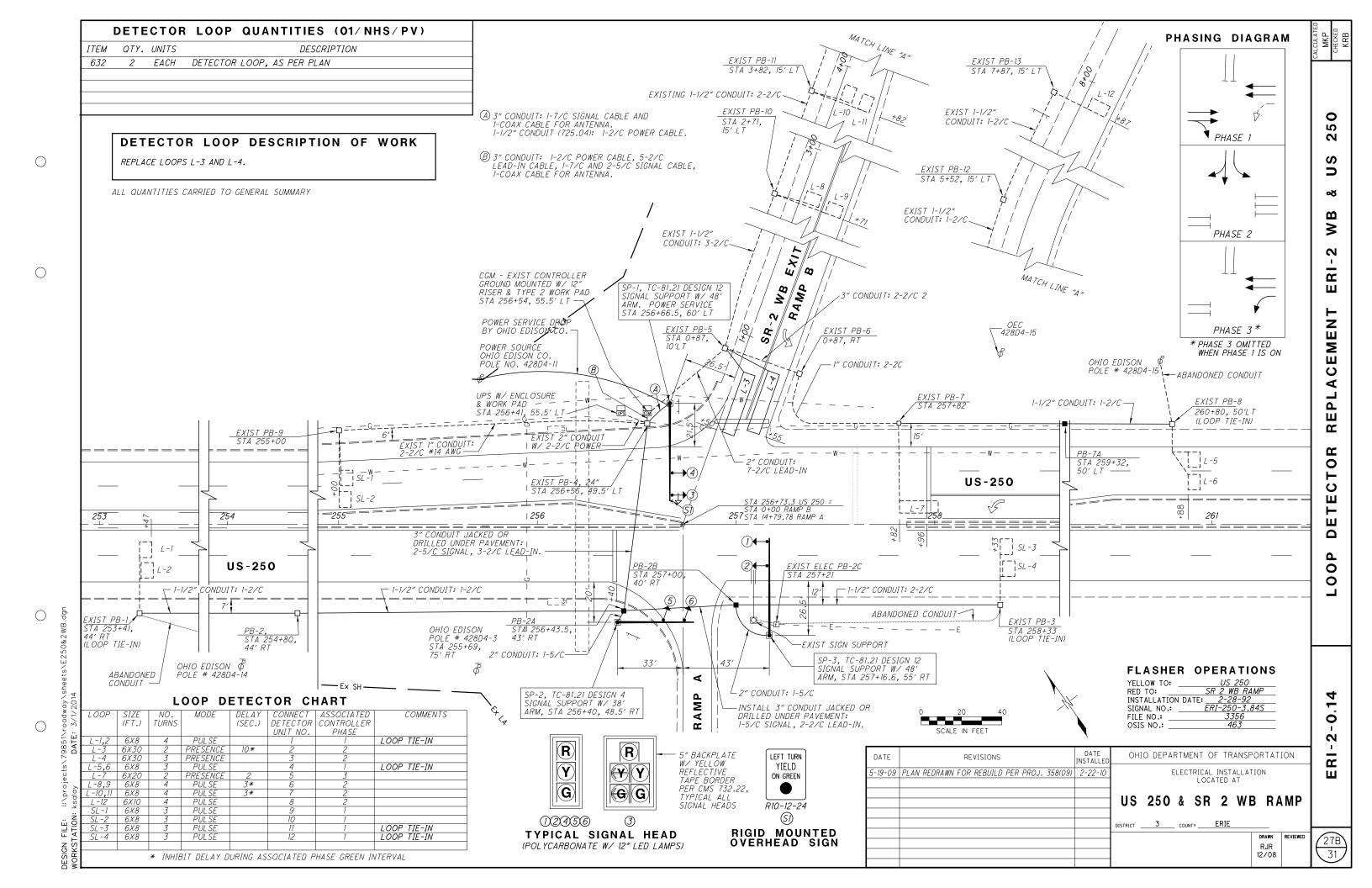
SEE DETAILS ON THIS SHEET FOR ADDITIONAL REQUIREMENTS.

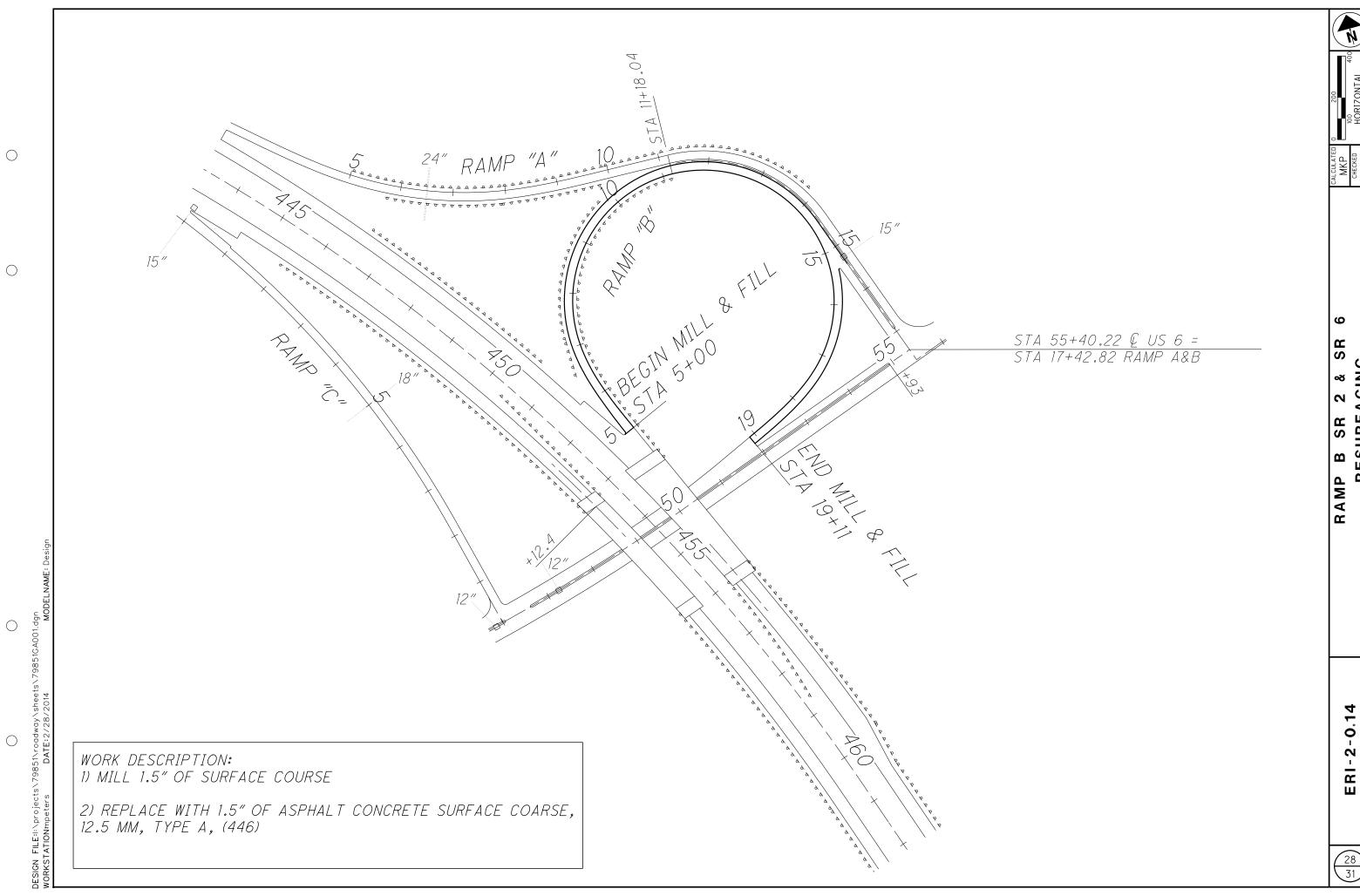
PAYMENT FOR ALL OF THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH FOR ITEM 632, DETECTOR LOOP, AS PER PLAN.



ANGULAR DESIGN DETECTION (ADD) LOOP DETAIL FOR LANE WIDTH 11' & LARGER

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2 & S ACING P B SR RESURF/

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	TRANSVERSE TRANSVERSE	3	12	4 0.44 4 0.44	TRANSVERSE TRANSVERSE	3	12 4 12 4	0.44											5
	TRANSVERSE TRANSVERSE TRANSVERSE	3 3 3	12 12 12	4 0.44 4 0.44 4 0.44	TRANSVERSE TRANSVERSE TRANSVERSE	3	12 4 12 4 12 4	0.44 0.44 0.44											[]
	TRANSVERSE TRANSVERSE TRANSVERSE	3	12	4 0.44	TRANSVERSE TRANSVERSE TRANSVERSE	3	12 4 12 4 12 4	0.44											"
	TRANSVERSE TRANSVERSE	3 3	12		TRANSVERSE TRANSVERSE	3	12 4 12 4												
	TRANSVERSE TRANSVERSE TRANSVERSE	3 3 3	12 12 12	4 0.44 4 0.44 4 0.44	TRANSVERSE TRANSVERSE TRANSVERSE	3	12 4 12 4 12 4	0.44 0.44 0.44						+					(20
	TRANSVERSE ONGITUDINAL	3 200	12	4 0.44	TRANSVERSE		12 4	0.44										ı	$\frac{2}{3}$

 \bigcirc \bigcirc Mileage SR-2 SLM 0.0

CALCULATED MKP	CHECKED	
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CALCULATIONS REPAIR

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Mileage	Repair Type			Lane (Repair Type	I	nside I	Lane (E	EB)	Repair Type	Ir	nside L	ane (V	VB)	Repair Type			Lane (
SR-2 SLM	TRANSVERSE/ Longitudinal	Lengt (Ft)		1 -	Volume (CY)	TRANSVERSE/ LONGITUDINAL	Lengti (Ft)	h Width (Ft)	Depth (in)	Volume (CY)	TRANSVERSE/ LONGITUDINAL	Length (Ft)	Width (Ft)	Depth (in)	Volume (CY)	TRANSVERSE/ LONGITUDINAL	Length (Ft)		Depth (in)	(CY)
1.0	TDANGVEDGE	-	10	1	0.44	TRANSVERSE		10	4	0.44	LONGITUDINAL	-	-	4	0.44	LONGITUDINAL	-		4	- 11
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
	TRANSVERSE TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4 4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
		3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12 12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12 12	4	0.44
	TRANSVERSE TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
		+ -		4		TRANSVERSE	3			1	TRANSVERSE	3	12	4		TRANSVERSE		12	4	
	TRANSVERSE	3	12	4	0.44	TRANSVERSE		12	4	0.44	TRANSVERSE			4	0.44	TRANSVERSE	3		4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4 4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE		12	4		TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3		4	_
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	+ -	0.44	TRANSVERSE	3		4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12		0.44	TRANSVERSE	— <u> </u>	12	4	0.44	TRANSVERSE		12	<u> </u>	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	TRANSVERSE	3	12	4	0.44	TRANSVERSE	3	12	4	0.44
	LONGITUDINAL	6	6	4	0.44															
	LONGITUDINAL	200	6	4	14.81								1							

FOR REFERENCE ONLY

Mileage	Repair Type			Repair Type	Ir	side L	ane (E	В)	Repair Type	air Type Inside Lane (WB)				Repair Type	Ou	tside l	ane (WB)		
SR-2 SLM	TRANSVERSE/ Longitudinal	Length (Ft)			Volume (CY)	TRANSVERSE/ Longitudinal	Length (Ft)		Depth (in)	Volume (CY)	TRANSVERSE/ Longitudinal	Length (Ft)	Width (Ft)	Depth (in)	Volume (CY)	TRANSVERSE/ Longitudinal	Length (Ft)	Width (Ft)	Depth (in)	Volume (CY)
2.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
	LONGITUDINAL	12	12	4	1.78															
	LONGITUDINAL	6	6	4	0.44															
	LONGITUDINAL	50	6	4	3.70															
3.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	40	12	4	5.93	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
4.0																				
	LONGITUDINAL	25	12	4	3.70	LONGITUDINAL	25	12	4	3.70	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
5.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
						LONGITUDINAL	6	6	4	0.44										
6.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
7.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
8.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
						LONGITUDINAL	6	6	4	0.44										
						LONGITUDINAL	6	6	4	0.44										
						LONGITUDINAL	6	6	4	0.44										
						LONGITUDINAL	6	6	4	0.44										
9.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	25	12	4	3.70	LONGITUDINAL	6	6	4	0.44
																LONGITUDINAL	50	6	4	3.70
10.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
11.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
12.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
											LONGITUDINAL	25	12	4	3.70					
13.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
14.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	25	12	4	3.70	LONGITUDINAL	6	6	4	0.44
											LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	25	2	4	0.62
																LONGITUDINAL	6	6	4	0.44
15.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	25	12	4	3.70	LONGITUDINAL	6	6	4	0.44
16.0																				

Ramp Repairs	Repair Type	Length (Ft)	Width (Ft)	Depth (in)	Volume (CY)
SR 269 - RAMP A	LONGITUDINAL	20	6	4	1.48
SR 269 - RAMP B	LONGITUDINAL	25	6	4	1.85
SR 269 - RAMP C	LONGITUDINAL	20	6	4	1.48
SR 269 - RAMP D	LONGITUDINAL	25	6	4	1.85
US 6 - RAMP A	LONGITUDINAL	20	6	4	1.48
US 6 - RAMP B	LONGITUDINAL	20	6	4	1.48
US 6 - RAMP C	LONGITUDINAL	15	6	4	1.11
US 6 - RAMP D	LONGITUDINAL	15	6	4	1.11
SR 101 - RAMP A	LONGITUDINAL	15	6	4	1,11
SR 101 - RAMP B	LONGITUDINAL	15	6	4	1,11
SR 101 - RAMP C	LONGITUDINAL	15	6	4	1.11
SR 101 - RAMP D	LONGITUDINAL	20	6	4	1.48
SR 4 - RAMP A	LONGITUDINAL	20	6	4	1.48
SR 4 - RAMP B	LONGITUDINAL	15	6	4	1.11
SR 4 - RAMP C	LONGITUDINAL	15	6	4	1.11
SR 4 - RAMP D	LONGITUDINAL	15	6	4	1,11
<u> </u>					
US 250 - RAMP A	LONGITUDINAL	15	6	4	1.11
US 250 - RAMP B	LONGITUDINAL	10	6	4	0.74
US 250 - RAMP C	LONGITUDINAL	10	6	4	0.74
US 250 - RAMP D	LONGITUDINAL	40	6	4	2.96
US 250 - RAMP E	LONGITUDINAL	10	6	4	0.74
RYE BEACH - RAMP A	LONGITUDINAL	10	6	4	0.74
RYE BEACH - RAMP B	LONGITUDINAL	25	6	4	1.85
RYE BEACH - RAMP C	LONGITUDINAL	15	6	4	1,11
RYE BEACH - RAMP D	LONGITUDINAL	10	6	4	0.74