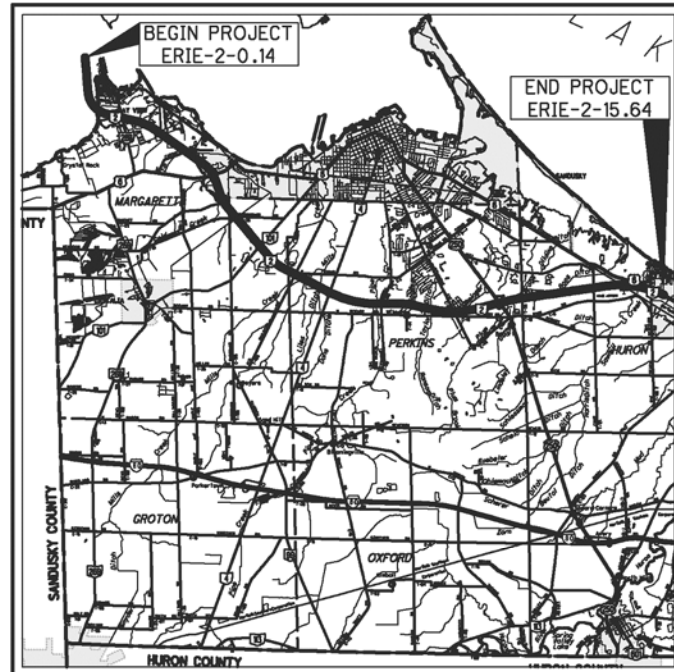
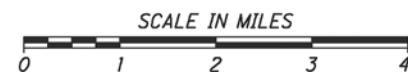


DESIGN FILE: \\projects\79851\roadway\sheets\79851GT001.dgn
MODELNAME: Design



LOCATION MAP

LATITUDE: N 41° 24' 18" LONGITUDE: W 82° 44' 01"



PORTION TO BE IMPROVED -----
INTERSTATE HIGHWAY -----
FEDERAL ROUTES -----
STATE ROUTES -----
COUNTY & TOWNSHIP ROADS -----
OTHER ROADS -----

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

ERI-2-0.14

**CITY OF HURON
CITY OF SANDUSKY
HURON TOWNSHIP
MARGARETTA TOWNSHIP
PERKINS TOWNSHIP
ERIE COUNTY**

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PROJECT DESCRIPTION

THIS PROJECT CONSISTS OF PAVEMENT REPAIRS,
OVERLAYING WITH MICROSURFACING AND PLACING
PAVEMENT MARKINGS ON THE PAVEMENT.

EARTH DISTURBED AREAS

EARTH DISTURBED AREA: N/A ACRES
(MAINTENANCE PROJECT)
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A ACRES
(MAINTENANCE PROJECT)
NOTICE OF INTENT EARTH DISTURBED AREA: N/A ACRES
(MAINTENANCE PROJECT)

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR
THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED
ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE
DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF
SECTION 5511.02 OF THE OHIO REVISED CODE.

2013 SPECIFICATIONS

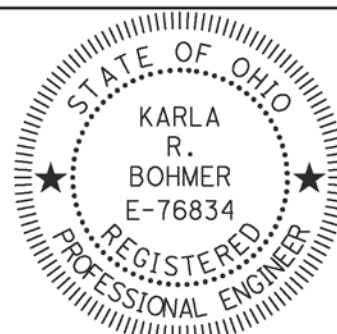
THE STANDARD SPECIFICATIONS OF THE STATE OF
OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING
CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED
IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT
THE MAKING OF THIS IMPROVEMENT WILL REQUIRE
THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT
DETOURS WILL BE PROVIDED AS INDICATED ON SHEET 23.

APPROVED _____
DATE 2-27-14 DISTRICT DEPUTY DIRECTOR

APPROVED _____
DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION

ENGINEER'S SEAL:



SIGNED: Karla R. Bohmer
DATE: 2/27/14

STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
BP-3.1	4/20/12	MT-101.90	7/19/13	SS800	4/18/14
		MT-105.10	7/19/13	SS821	4/20/12
DM-4.3	7/19/13			SS832	1/17/14
DM-4.4	7/20/12	TC-41.20	10/18/13	SS921	4/20/12
		TC-42.20	10/18/13		
MT-95.30	7/19/13	TC-52.10	10/18/13		
MT-95.31	7/19/13	TC-52.20	1/17/14		
MT-95.32	7/19/13	TC-65.10	1/17/14		
MT-95.50	7/19/13	TC-65.11	1/17/14		
MT-98.10	7/19/13	TC-71.10	1/17/14		
MT-98.11	7/19/13	TC-72.20	7/20/12		
MT-98.20	7/19/13				
MT-98.22	7/19/13				
MT-98.28	7/19/13				
MT-98.29	7/19/13				
MT-99.20	7/19/13				

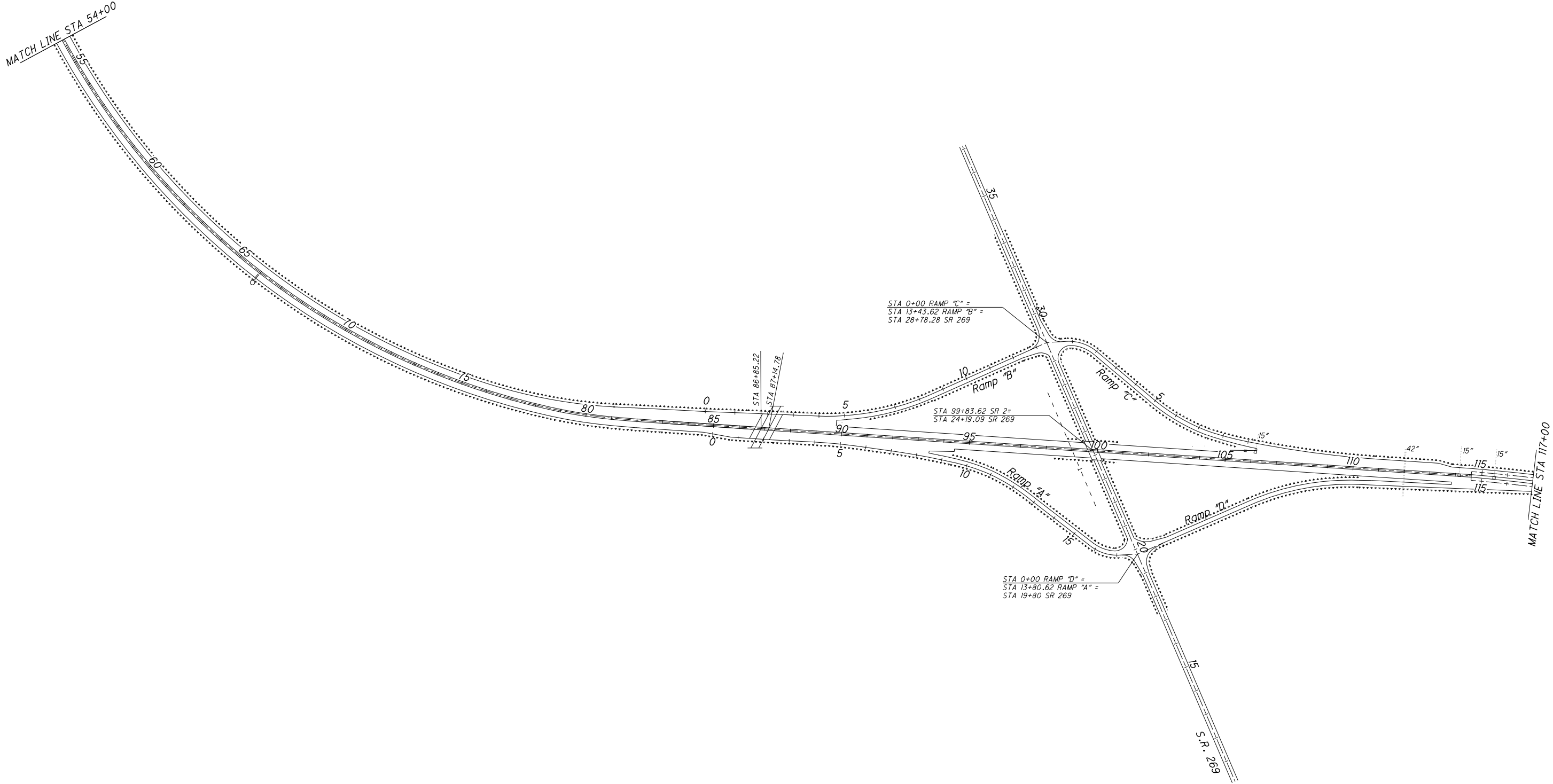
UNDERGROUND UTILITIES

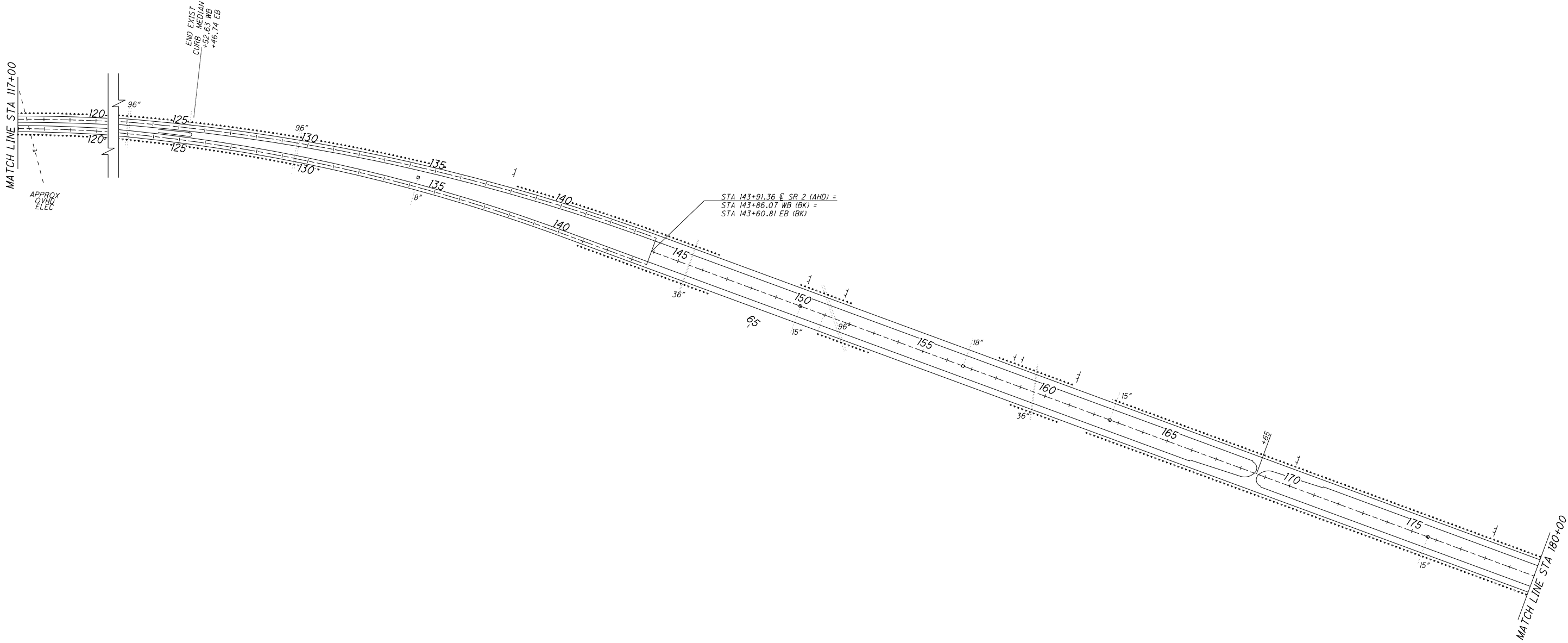
CONTACT BOTH SERVICES
CALL TWO WORKING DAYS
BEFORE YOU DIG
CALL
1-800-362-2764
(TOLL FREE)
OHIO UTILITIES PROTECTION SERVICE
NON-MEMBERS
MUST BE CALLED DIRECTLY
OIL & GAS PRODUCERS UNDERGROUND
PROTECTION SERVICE CALL: 1-800-925-0988

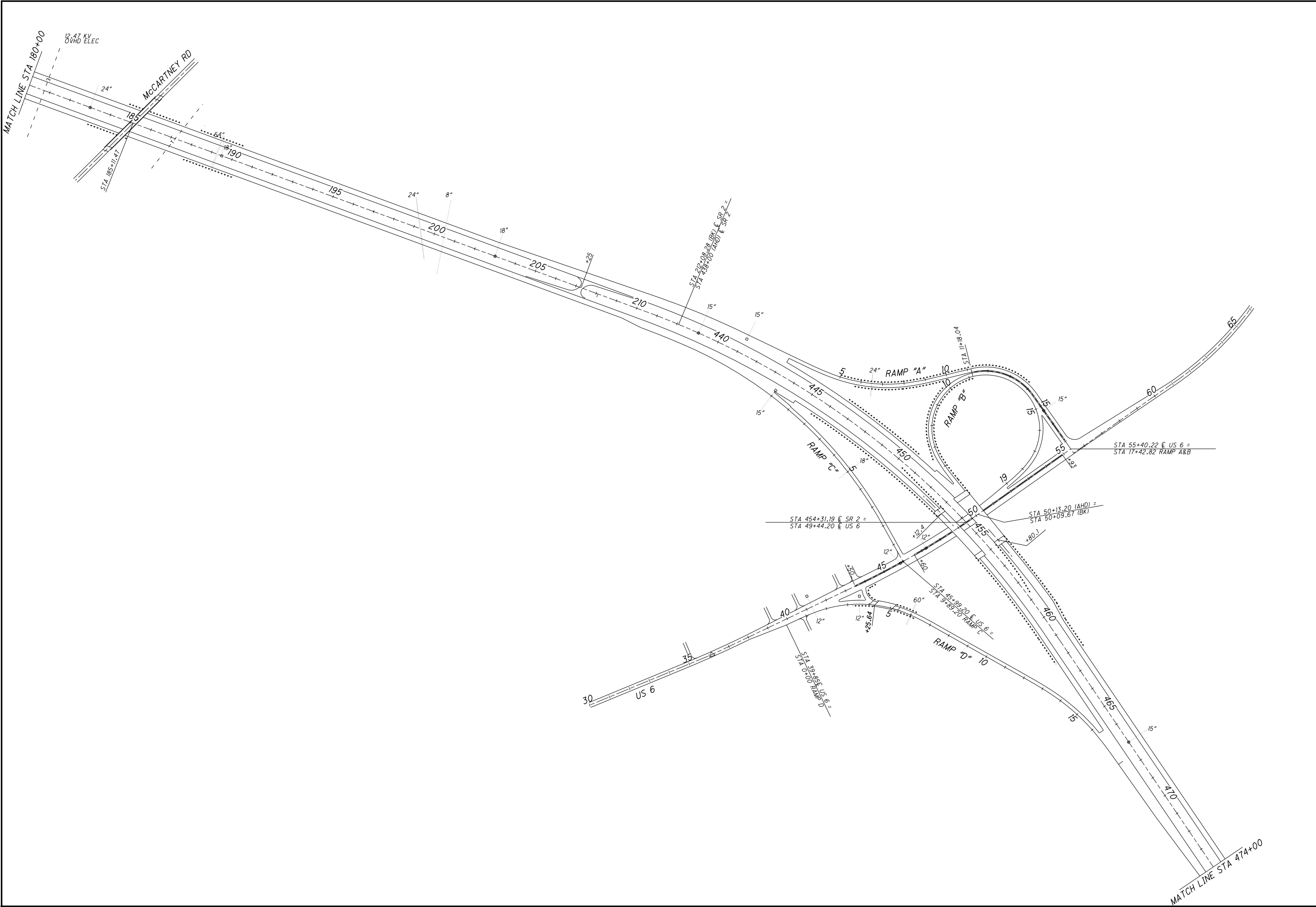
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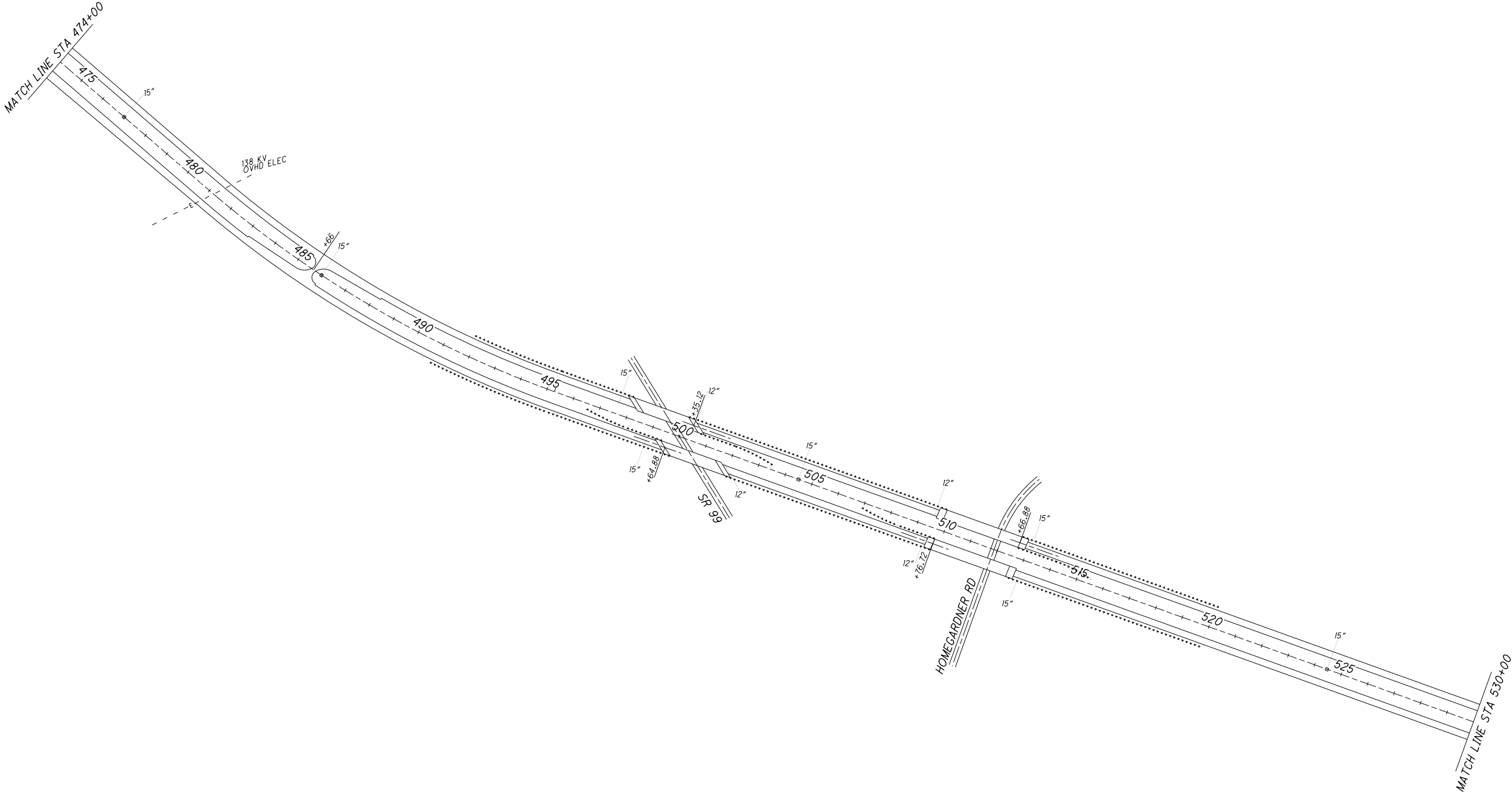


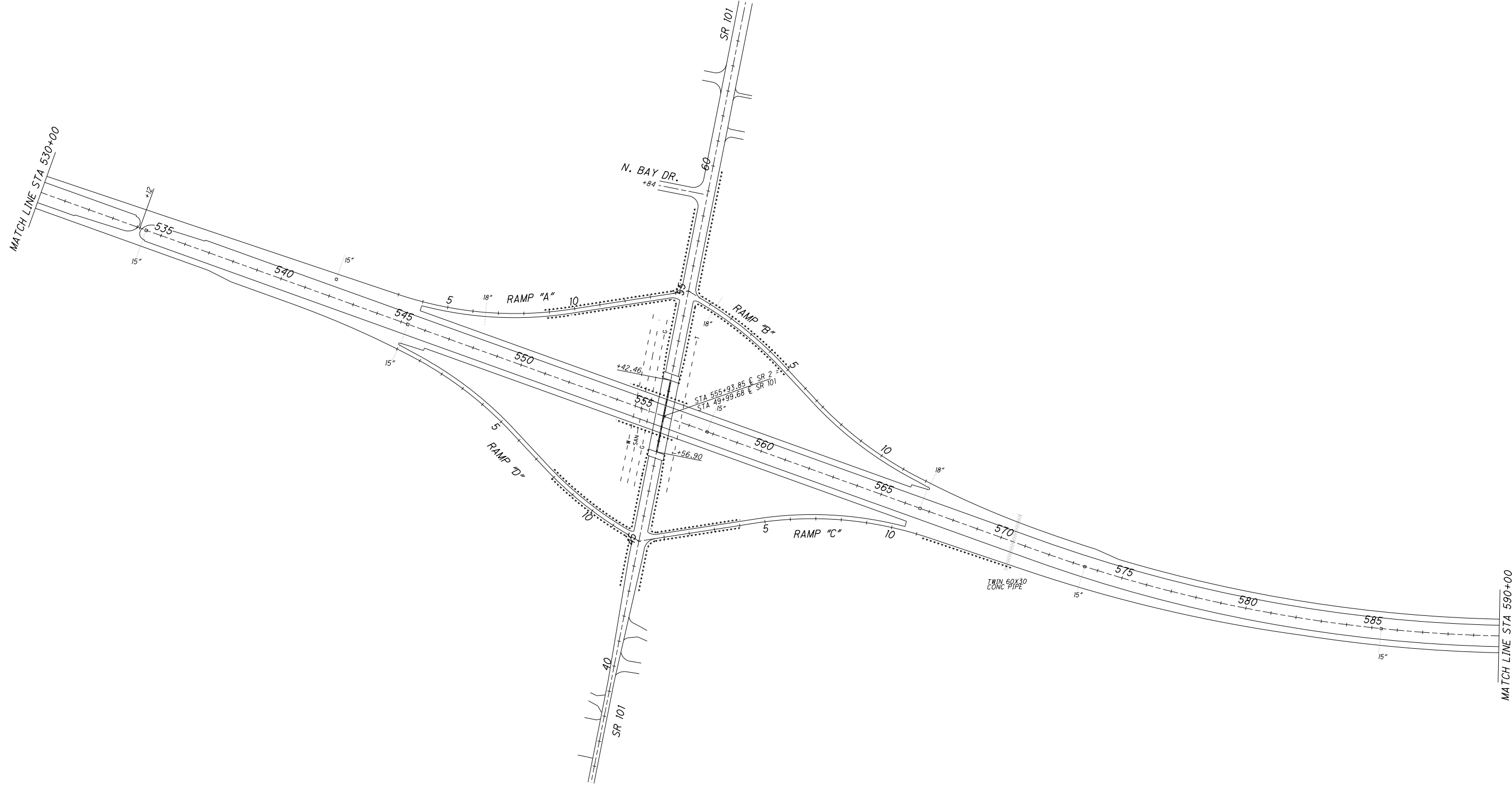


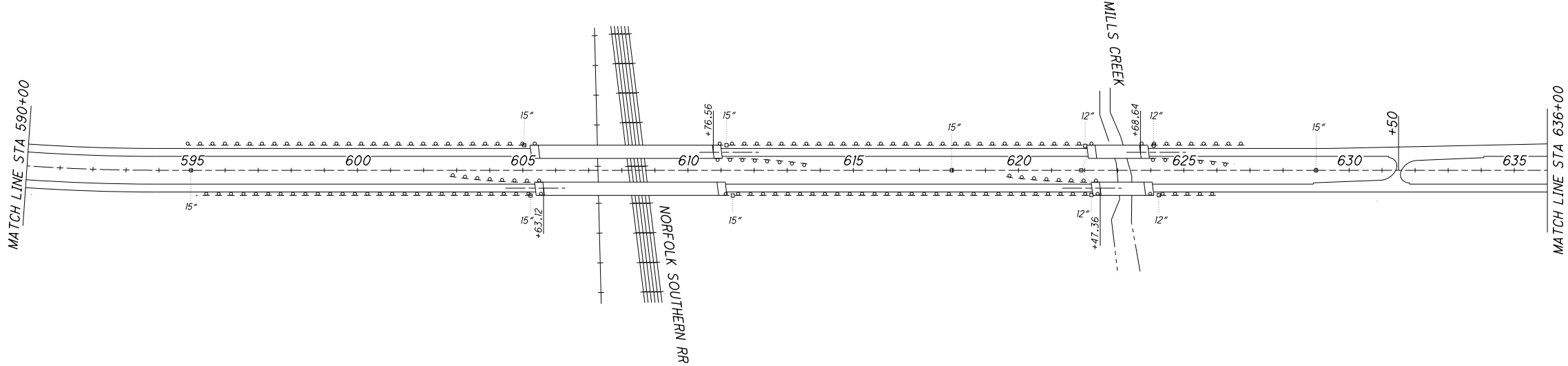


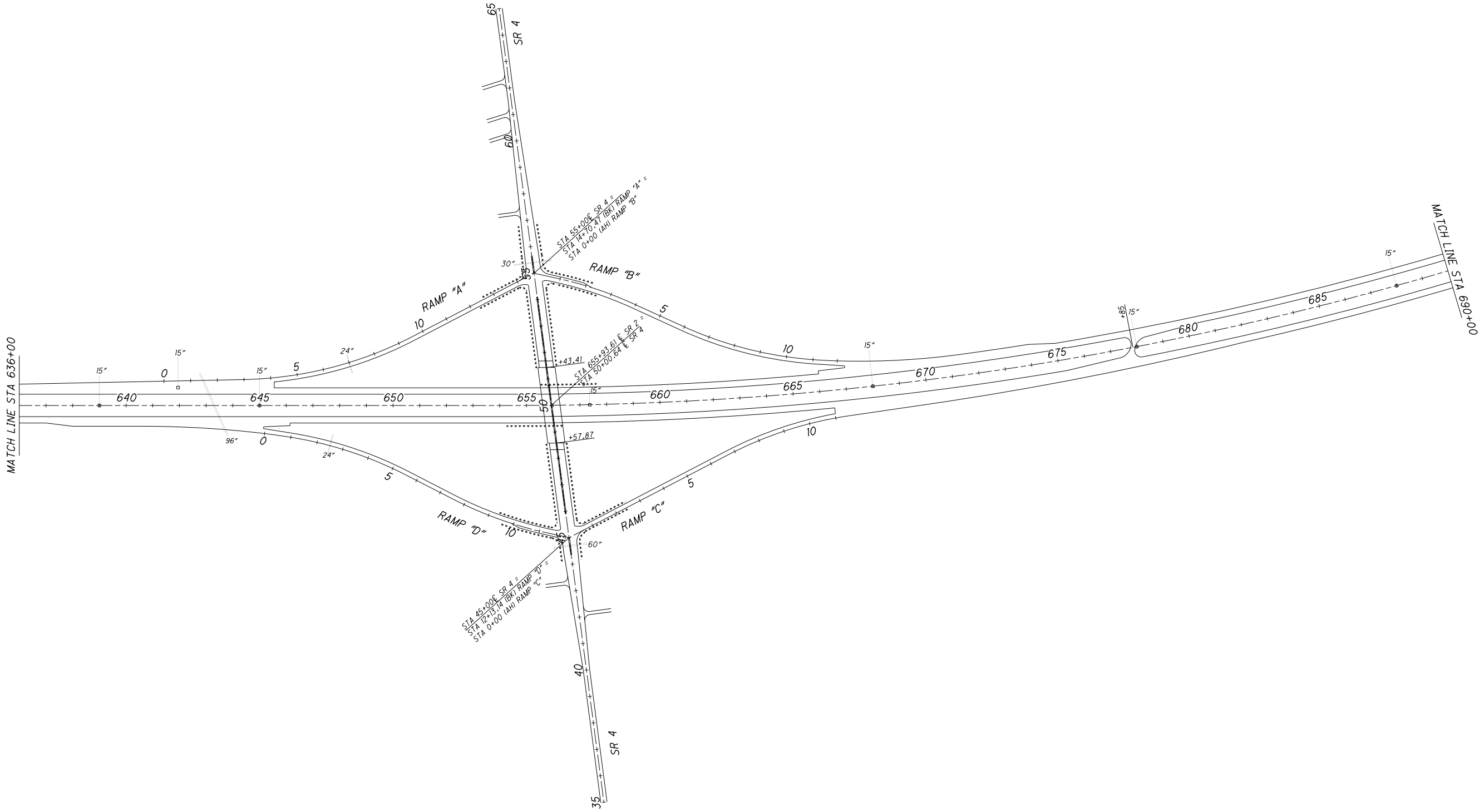


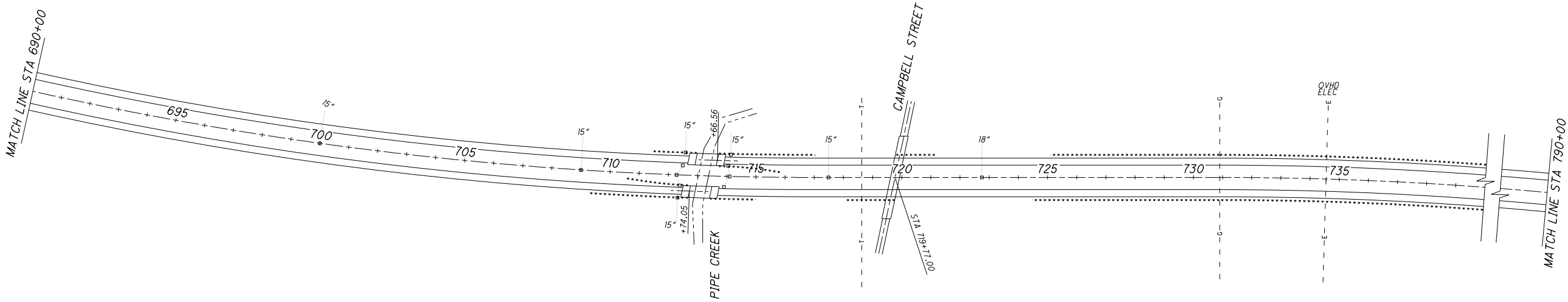


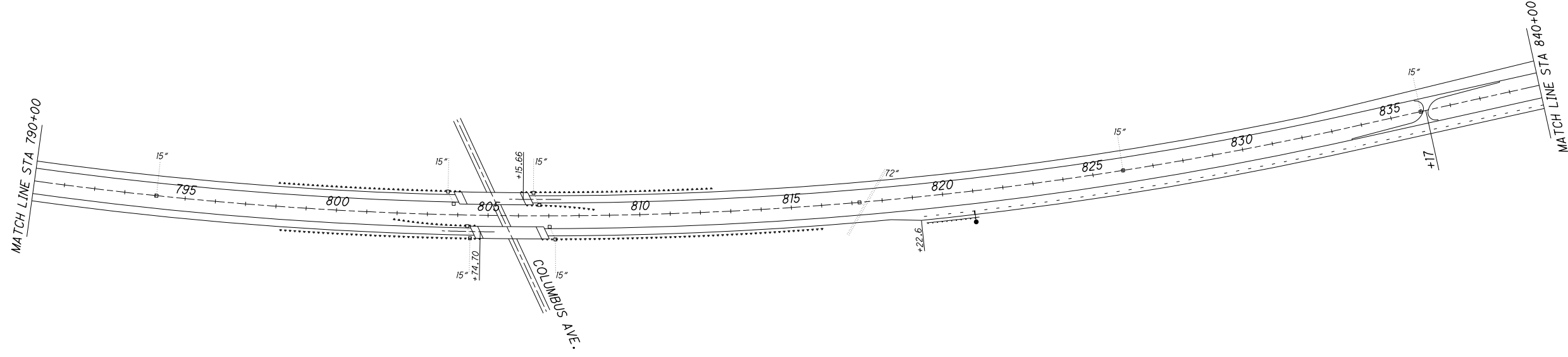


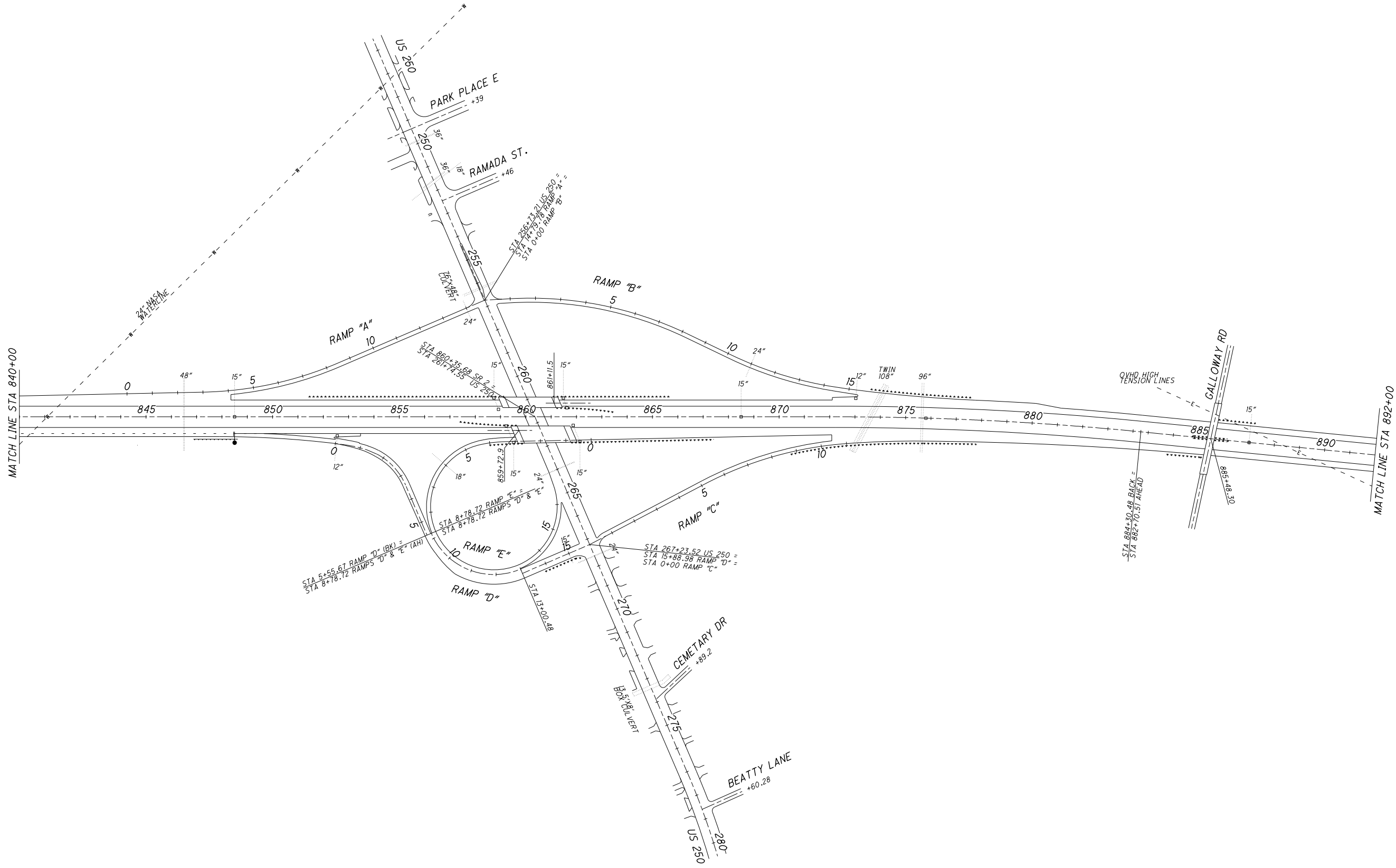




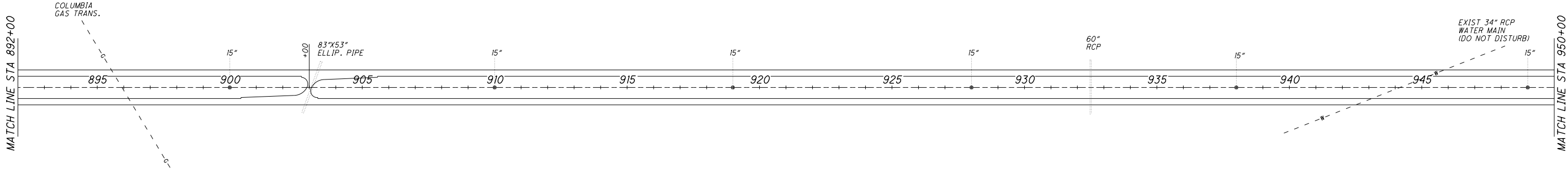


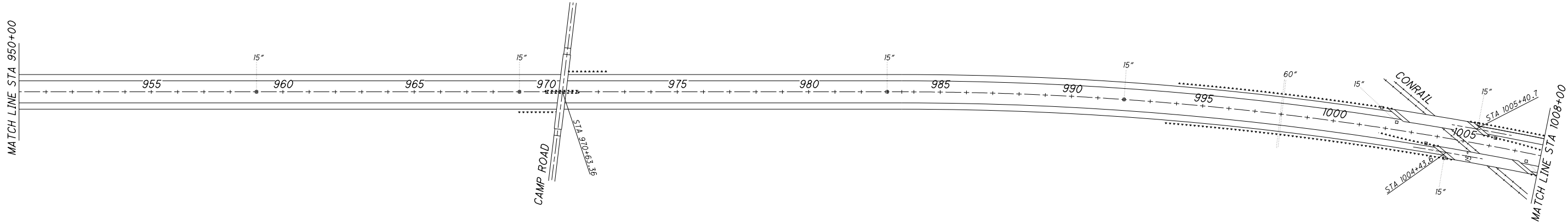


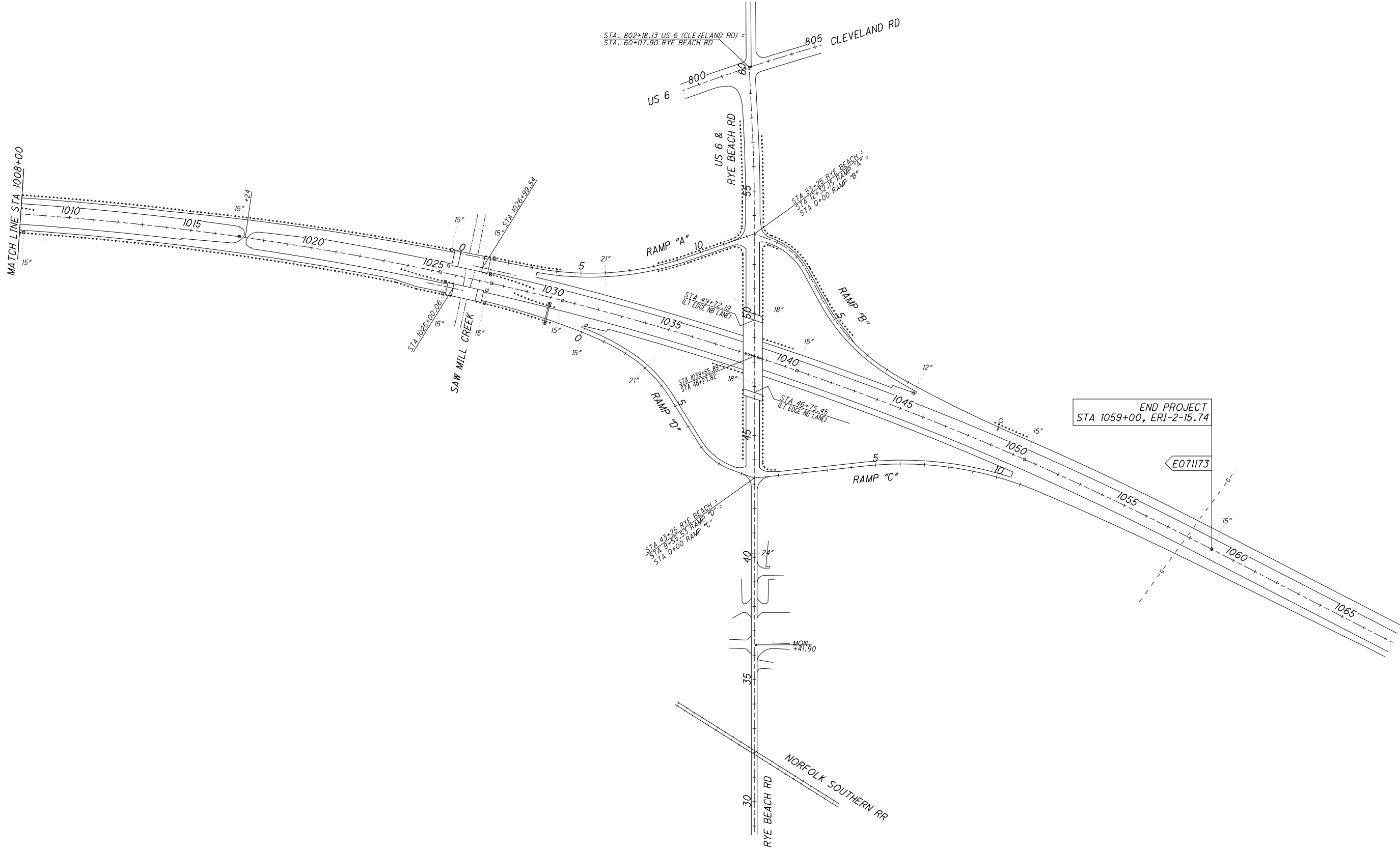




<div>12</div> <div>31</div>	ERI-2-0.14	SCHEMATIC PLAN SR 2		N 0 100 200 400 HORIZONTAL SCALE IN FEET
		STA 840+00 TO STA 892+00		
CALCULATED MKP		CHECKED KRB		







DESIGN DESIGNATION (ERI-2-0.14 TO 1.82)

CURRENT ADT (2014)-----24,000
DESIGN YEAR ADT (2022)-----25,000
DESIGN HOURLY VOLUME (2022)-----2,200
DIRECTIONAL DISTRIBUTION-----0.53
TRUCK (24 HOUR B&C)-----0.10
DESIGN SPEED-----70 MPH
LEGAL SPEED-----70 MPH
DESIGN FUNCTIONAL CLASSIFICATION:
RURAL PRINCIPLE ARTERIAL

NHS PROJECT-----YES

DESIGN EXCEPTIONS
NONE

DESIGN DESIGNATION (ERI-2-1.82 TO 3.71)

CURRENT ADT (2014)-----23,000
DESIGN YEAR ADT (2022)-----23,000
DESIGN HOURLY VOLUME (2022)-----2,100
DIRECTIONAL DISTRIBUTION-----0.53
TRUCK (24 HOUR B&C)-----0.11
DESIGN SPEED-----70 MPH
LEGAL SPEED-----70 MPH
DESIGN FUNCTIONAL CLASSIFICATION:
URBAN FREEWAY & EXPRESSWAY

NHS PROJECT-----YES

DESIGN EXCEPTIONS
NONE

DESIGN DESIGNATION (ERI-2-3.71 TO 4.23)

CURRENT ADT (2014)-----23,000
DESIGN YEAR ADT (2022)-----23,000
DESIGN HOURLY VOLUME (2022)-----2,100
DIRECTIONAL DISTRIBUTION-----0.53
TRUCK (24 HOUR B&C)-----0.11
DESIGN SPEED-----70 MPH
LEGAL SPEED-----70 MPH
DESIGN FUNCTIONAL CLASSIFICATION:
URBAN FREEWAY & EXPRESSWAY

NHS PROJECT-----YES

DESIGN EXCEPTIONS
NONE

DESIGN DESIGNATION (ERI-2-4.23 TO 4.76)

CURRENT ADT (2014)-----25,000
DESIGN YEAR ADT (2022)-----26,000
DESIGN HOURLY VOLUME (2022)-----2,300
DIRECTIONAL DISTRIBUTION-----0.53
TRUCK (24 HOUR B&C)-----0.12
DESIGN SPEED-----70 MPH
LEGAL SPEED-----70 MPH
DESIGN FUNCTIONAL CLASSIFICATION:
URBAN FREEWAY & EXPRESSWAY

NHS PROJECT-----YES

DESIGN EXCEPTIONS
NONE

DESIGN DESIGNATION (ERI-2-4.76 TO 6.17)

CURRENT ADT (2014)-----25,000
DESIGN YEAR ADT (2022)-----26,000
DESIGN HOURLY VOLUME (2022)-----2,300
DIRECTIONAL DISTRIBUTION-----0.53
TRUCK (24 HOUR B&C)-----0.12
DESIGN SPEED-----70 MPH
LEGAL SPEED-----70 MPH
DESIGN FUNCTIONAL CLASSIFICATION:
URBAN FREEWAY & EXPRESSWAY

NHS PROJECT-----YES

DESIGN EXCEPTIONS
NONE

DESIGN DESIGNATION (ERI-2-6.17 TO 8.07)

CURRENT ADT (2014)-----28,000
DESIGN YEAR ADT (2022)-----29,000
DESIGN HOURLY VOLUME (2022)-----2,600
DIRECTIONAL DISTRIBUTION-----0.53
TRUCK (24 HOUR B&C)-----0.11
DESIGN SPEED-----70 MPH
LEGAL SPEED-----70 MPH
DESIGN FUNCTIONAL CLASSIFICATION:
RURAL PRINCIPLE ARTERIAL

NHS PROJECT-----YES

DESIGN EXCEPTIONS
NONE

DESIGN DESIGNATION (ERI-2-8.07 TO 11.93)

CURRENT ADT (2014)-----29,000
DESIGN YEAR ADT (2022)-----29,000
DESIGN HOURLY VOLUME (2022)-----2,600
DIRECTIONAL DISTRIBUTION-----0.53
TRUCK (24 HOUR B&C)-----0.12
DESIGN SPEED-----70 MPH
LEGAL SPEED-----70 MPH
DESIGN FUNCTIONAL CLASSIFICATION:
RURAL PRINCIPLE ARTERIAL 8.07-9.27
URBAN FREEWAY & EXPRESSWAY 9.27-11.93

NHS PROJECT-----YES

DESIGN EXCEPTIONS
NONE

DESIGN DESIGNATION (ERI-2-11.93 TO 15.35)

CURRENT ADT (2014)-----27,000
DESIGN YEAR ADT (2022)-----28,000
DESIGN HOURLY VOLUME (2022)-----2,500
DIRECTIONAL DISTRIBUTION-----0.53
TRUCK (24 HOUR B&C)-----0.14
DESIGN SPEED-----70 MPH
LEGAL SPEED-----70 MPH
DESIGN FUNCTIONAL CLASSIFICATION:
URBAN FREEWAY & EXPRESSWAY

NHS PROJECT-----YES

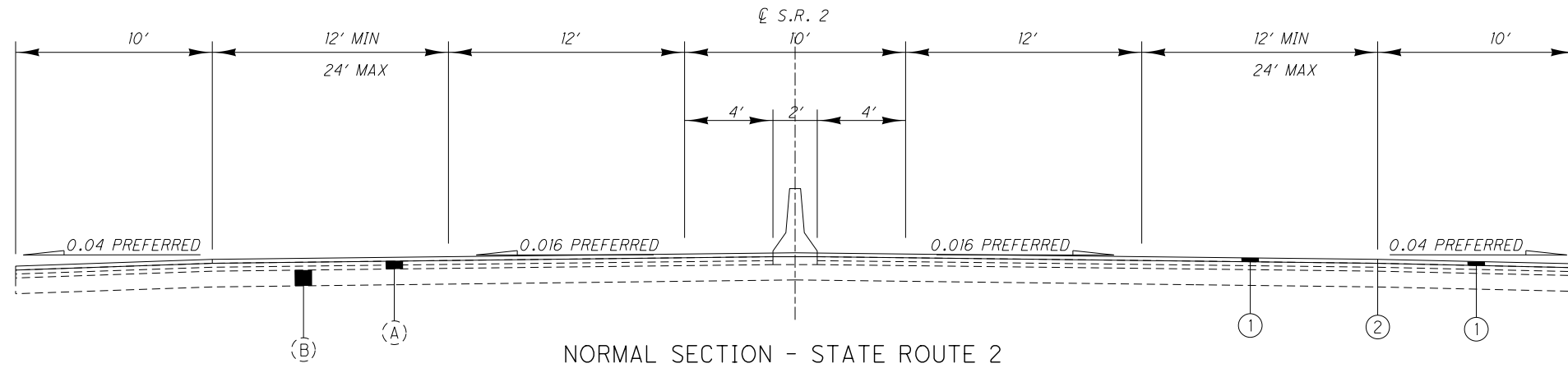
DESIGN EXCEPTIONS
NONE

DESIGN DESIGNATION (ERI-2-15.35 TO 15.64)

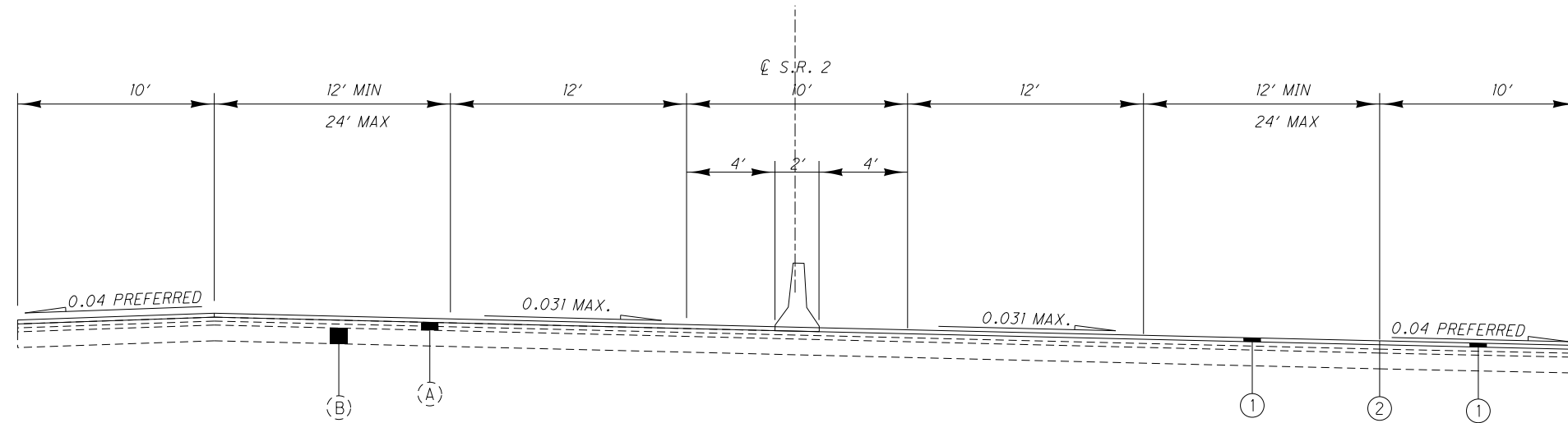
CURRENT ADT (2014)-----24,000
DESIGN YEAR ADT (2022)-----24,000
DESIGN HOURLY VOLUME (2022)-----2,200
DIRECTIONAL DISTRIBUTION-----0.53
TRUCK (24 HOUR B&C)-----0.15
DESIGN SPEED-----70 MPH
LEGAL SPEED-----70 MPH
DESIGN FUNCTIONAL CLASSIFICATION:
URBAN FREEWAY & EXPRESSWAY

NHS PROJECT-----YES

DESIGN EXCEPTIONS
NONE



NORMAL SECTION - STATE ROUTE 2



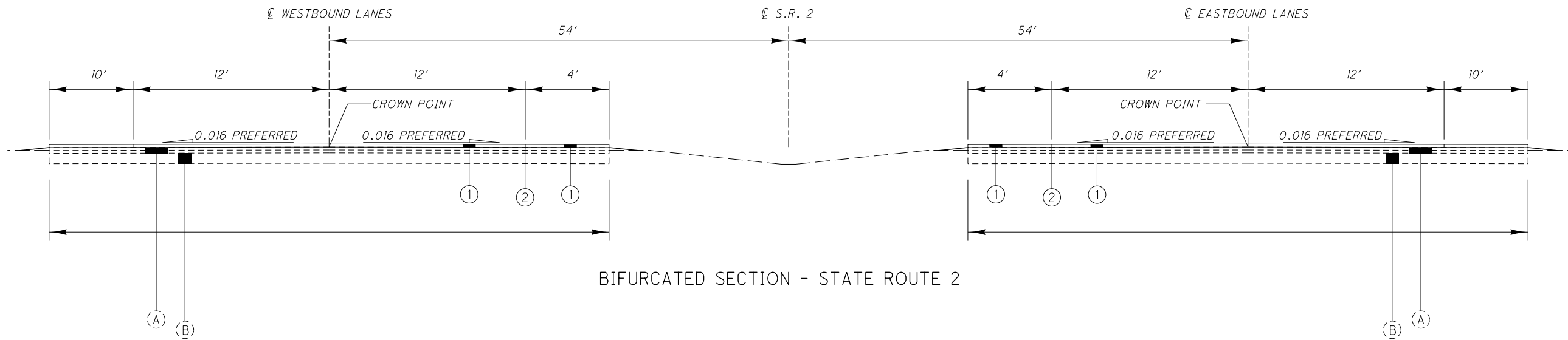
SUPERELEVATION SECTION - STATE ROUTE 2

EXISTING LEGEND

- (A) 3.25"± ASPHALT CONCRETE
- (B) 9"± REINFORCED CONCRETE PAVEMENT

PROPOSED LEGEND

- ① ITEM 421 MICROSURFACING, SURFACE COURSE, AS PER PLAN (22 LBS/SY)
- ② ITEM 423 CRACK SEALING, TYPE II OR TYPE III

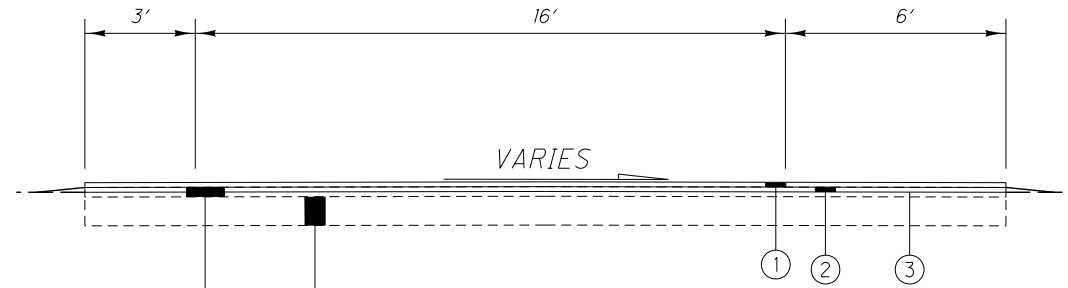


EXISTING LEGEND

- (A) 3.25"± ASPHALT CONCRETE
- (B) 9"± REINFORCED CONCRETE PAVEMENT

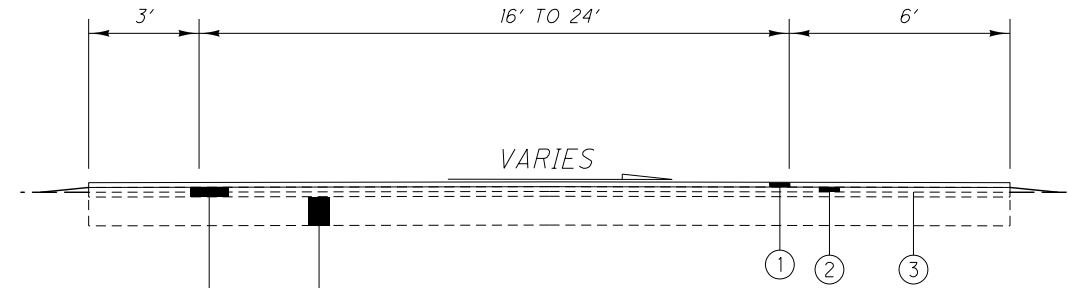
PROPOSED LEGEND

- (1) ITEM 421 MICROSURFACING, SURFACE COURSE, AS PER PLAN (22 LBS/SY)
- (2) ITEM 423 CRACK SEALING, TYPE II OR TYPE III



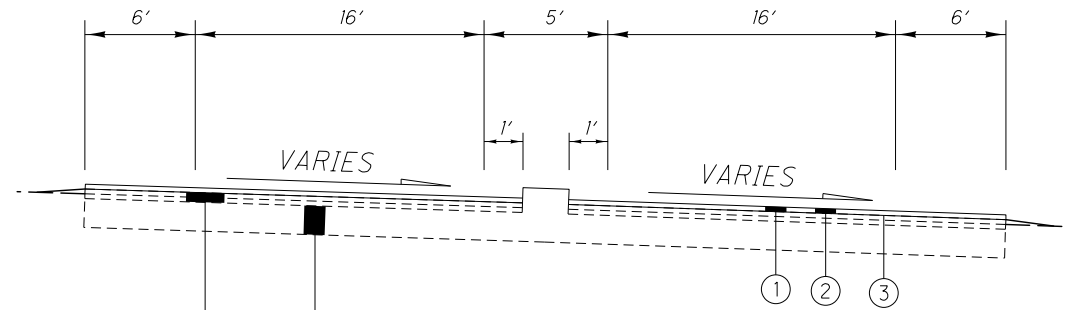
RAMP SECTION

SR 269 RAMPS A,B,C,D
US 6 RAMPS C,D
SR 101 RAMPS A,B,C,D
SR 4 RAMPS A,B,C,D
US 250 RAMPS A,C
RYE BEACH RD RAMPS A,C,D



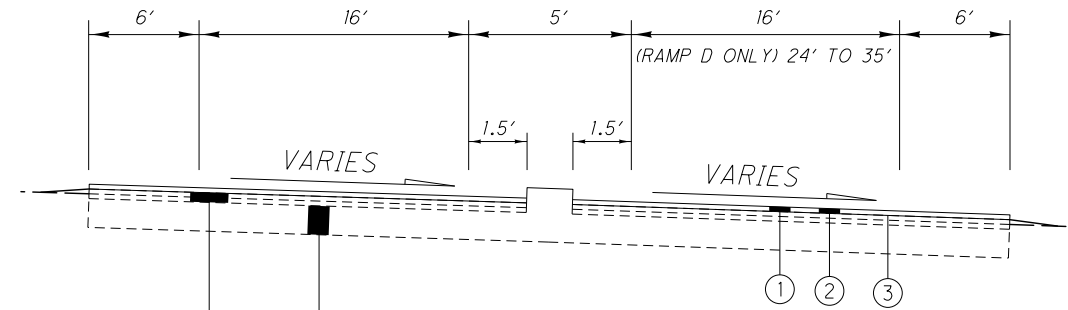
RAMP SECTION

US 250 RAMP B
RYE BEACH RAMP B



RAMP TWO-WAY SECTION

US 6 RAMPS A,B



RAMP TWO-WAY SECTION

US 250 RAMPS D,E

EXISTING LEGEND

- (A) 3.25"± ASPHALT CONCRETE
(B) 9"± REINFORCED CONCRETE PAVEMENT

PROPOSED LEGEND

- ① ITEM 421 MICROSURFACING, SURFACE COURSE, AS PER PLAN
② ITEM 421 MICROSURFACING, LEVELING COURSE, AS PER PLAN
③ ITEM 423 CRACK SEALING, TYPE II OR TYPE III

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WORKSPACE: \\projects\79851\roadway\shp\DATE: 2868661004.dgn
MODELNAME: Design

ITEM 614 - WORK ZONE MARKING SIGN

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR TEMPORARY WORK ZONE MARKING SIGNS PER THE REQUIREMENTS OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS, 614.04.

WORK ZONE MARKING SIGN: (W8-HI2A-36) NO EDGE LINE = 22 EACH

ITEM 614 - MAINTAINING TRAFFIC
(LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	FOURTH OF JULY
NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE FEE OF \$1500 PER DAY.

ITEM 614 - MAINTAINING TRAFFIC: GENERAL

ONE 11' LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES. ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH ITEM 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, PLAN DETAILS, STANDARD DRAWINGS, AND AS OUTLINED IN THE CONSTRUCTION AND MAINTENANCE SECTION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES CURRENT EDITION WITH THE LATEST REVISIONS. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED ON THIS PLAN.

THE FOLLOWING REQUIREMENTS SHALL ALSO APPLY:
THE CONTRACTOR SHALL SUBMIT, IN WRITING, A SCHEDULE OF OPERATIONS TO THE ENGINEER AND RECEIVE APPROVAL BEFORE WORK IS STARTED ON THE PROJECT. PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COORDINATE THE MAINTENANCE OF TRAFFIC OPERATIONS WITH THE LOCAL STATE HIGHWAY PATROL.

NIGHT WORK IS PERMITTED.

THE CONTRACTOR IS REQUIRED TO MAINTAIN ALL PAVEMENT THROUGHOUT THE PROJECT UNDER ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC DURING THE PERIOD FROM THE START OF WORK TO THE COMPLETION OF ALL WORK.

ITEM 614 - MAINTAINING TRAFFIC

ALL ADVANCE WARNING SIGNS FOR ANY CONDITION WHICH RESTRICTS TRAFFIC SHALL BE ERECTED BEFORE ANY SUCH RESTRICTION IS PUT INTO EFFECT. ALL SUCH SIGNS SHALL BE COVERED OR REMOVED FROM THE VIEW OF TRAFFIC WHEN THEY ARE NOT APPLICABLE, WITH THE APPROVAL OF THE ENGINEER.

IF THE CONTRACTOR FAILS TO COMPLY WITH THE PROVISIONS FOR TRAFFIC CONTROL AS SET FORTH IN THESE PLANS OR WITH PROVISIONS OF THE OMUTCD, AND SUCH FAILURE RESULTS IN A CONDITION AT THE WORK SITE WHICH IS UNSAFE FOR TRAFFIC, THE ENGINEER SHALL SUSPEND WORK UNTIL THE CONTRACTOR COMPLIES WITH THE NECESSARY REQUIREMENTS.

ALL MAINTENANCE OF TRAFFIC SIGNS ARE PAID UNDER ITEM 614 - MAINTAINING TRAFFIC.

PLACEMENT OF WORK ZONE PAVEMENT MARKINGS

THE CONTRACTOR SHALL PLACE EDGE LINES AND LANE LINES AT THE END OF EACH WORK SHIFT WHEN EXISTING LINES HAVE BEEN OBLITERATED.

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN, ON SITE, FOR THE DURATION OF THE PROJECT. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS MAINTAINED BY THE DIRECTOR (OFFICE OF MATERIALS MANAGEMENT). THE APPROVED LIST OF PORTABLE CHANGEABLE MESSAGE SIGNS CAN BE FOUND ON THE ODOT WEBSITE BY CLICKING ON THE SERVICES MENU, THEN CLICKING ON MATERIALS MANAGEMENT. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 650 FT. AND 475 FT., RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. PCMS TRAILERS SHALL BE DELINEATED ON A PERMANENT BASIS BY AFFIXING CONSPICUITY TAPE CONFORMING TO CMS 614.03, IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER AS SEEN BY ONCOMING ROAD USERS.

THE PROBABLE PCMS LOCATIONS WILL BE DETERMINED BY THE ENGINEER PRIOR TO BEGINNING WORK ON THIS PROJECT. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED, FACING AWAY FROM ALL TRAFFIC, AND SHALL DISPLAY ONE OR MORE YELLOW RETROREFLECTIVE SHEETING SURFACES OF 9-INCH BY 15-INCH MINIMUM SIZE FACING TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

(THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN __ HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.)

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PREPROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PREPROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.)

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF CMS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK. THE CONTRACTOR SHALL ONLY BE PAID FOR PCMS UNITS WHEN THEY ARE IN OPERATION ON THE PROJECT AS SPECIFIED IN THE PLANS OR BY THE ENGINEER.

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 120 DAY

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE LATEST EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED LAW ENFORCEMENT OFFICER (AND OFFICIAL PATROL CAR WITH MOUNTED EMERGENCY FLASHING LIGHTS) SHALL BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS AS DIRECTED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED.

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION.

LAW ENFORCEMENT OFFICERS (LEO'S) SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED. THE LEO'S ARE CONSIDERED TO BE EMPLOYED BY THE CONTRACTOR AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR ACTIONS. ALTHOUGH THEY ARE EMPLOYED BY THE CONTRACTOR, THE PROJECT ENGINEER SHALL HAVE CONTROL OVER THEIR PLACEMENT. THE OFFICIAL PATROL CAR SHALL BE A PUBLIC SAFETY VEHICLE AS REQUIRED BY THE OHIO REVISED CODE. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEO'S SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THESE SERVICES AND PROVIDE 72 HOURS ADVANCE NOTICE AS REQUIRED BY THE HIGHWAY PATROL LISTED BELOW:

STATE HIGHWAY PATROL
300 SOUTH NORWALK ROAD
NORWALK, OHIO 44857
419-668-4087

LAW ENFORCEMENT OFFICERS WITH PATROL CAR REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 120 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

IF THE CONTRACTOR WISHES TO UTILIZE LEO'S FOR FLAGGING AND TRAFFIC CONTROL OTHER THAN FOR THAT REQUIRED IN THESE PLANS, THEY MAY DO SO AT THEIR OWN EXPENSE.

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC

TEMPORARY WEDGES AT END OF RAMPS, PAVEMENT LAYER ENDS, APPROACH SLABS OR BRIDGE DECKS ARE TO BE CONSTRUCTED AS PER STANDARD DRAWING BP-3.1.

THIS ITEM SHALL ALSO BE USED TO REPAIR EXISTING HOLES, LEFT BEHIND BY PREVIOUS WORK TO REMOVE RPMS, AND OTHER PAVEMENT DEFECTS AS DIRECTED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR MAINTENANCE OF TRAFFIC.

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC 75 CU YD

BUTT JOINTS

BUTT JOINTS SHALL NOT BE CUT AND LEFT OPEN TO TRAFFIC. THEY SHALL BE FILLED IN WITH A TEMPORARY ASPHALT CONCRETE WEDGE USING ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC.

CONSTRUCTION "BUMP" (W8-1-36) AND "ADVISORY SPEED" (W13-1-24) SIGNS SHALL BE ERECTED AND MAINTAINED DURING THE PERIOD THE BUTT JOINT IS LEFT OPEN. THESE SIGNS SHALL BE PAID FOR UNDER THE LUMP SUM ITEM FOR ITEM 614 MAINTAINING TRAFFIC.

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MODELNAME: Design

ITEM 614 - WORKSITE TRAFFIC SUPERVISOR

SUBJECT TO APPROVAL OF THE ENGINEER, THE CONTRACTOR SHALL EMPLOY AND IDENTIFY (SOMEONE OTHER THAN THE SUPERINTENDENT) A CERTIFIED WORKSITE TRAFFIC SUPERVISOR (WTS) BEFORE STARTING WORK IN THE FIELD. THE WTS MAY BE CERTIFIED FROM ONE OF THE FOLLOWING ORGANIZATIONS:

1. AMERICAN TRAFFIC SAFETY SERVICE ASSOCIATION (ATSSA), PHONE NUMBER 1-800-272-8772, CERTIFIED TRAFFIC CONTROL SUPERVISOR (TCS).
2. NATIONAL HIGHWAY INSTITUTE, DESIGN AND OPERATION OF WORK ZONE TRAFFIC CONTROL, PHONE NUMBER 1-703- 235-0528.
3. THE OHIO CONTRACTORS ASSOCIATION, TRAFFIC CONTROL SUPERVISOR (OCA/TCS) WORK ZONE CLASS, ONLY IF TAKEN AFTER MAY 5, 2004, PHONE NUMBER 1-614-599-7915.
4. OHIO LABORERS TRAINING, TRAFFIC CONTROL SUPERVISORS CLASS, PHONE NUMBER 1-740-599-7915.

A COPY OF EACH WTS'S CERTIFICATION AND 24-HOUR CONTACT INFORMATION SHALL BE PROVIDED TO THE ENGINEER AT THE PRECONSTRUCTION CONFERENCE. IF THE DESIGNATED WTS WILL NOT BE AVAILABLE FULL TIME (24/7) THE CONTRACTOR MAY DESIGNATE AN ALTERNATE WTS TO BE AVAILABLE WHEN THE PRIMARY IS OFF DUTY. EACH WTS SHALL HAVE A CURRENT WTS CERTIFICATION (WITH AN EXPIRATION DATE NO MORE THAN 5 YEARS FROM THE DATE OF ISSUE) FROM ANY OF THE APPROVED ORGANIZATIONS.

THE WTS POSITION HAS THE RESPONSIBILITY OF MONITORING TRAFFIC CONTROL DEFICIENCIES FOR THE ENTIRE WORK ZONE. THE DUTIES OF THE WTS ARE AS FOLLOWS:

1. BE AVAILABLE ON A 24-HOUR PER DAY BASIS, AND BE ABLE TO BE ON SITE FOR ALL EMERGENCY TRAFFIC CONTROL NEEDS WITHIN ONE HOUR OF NOTIFICATION BY POLICE OR PROJECT STAFF AND BE PREPARED TO EFFECT CORRECTIVE MEASURES IMMEDIATELY ON EXISTING WORK ZONE TRAFFIC CONTROL DEVICES.
2. ATTEND PRECONSTRUCTION MEETING AND ALL PROJECT MEETINGS WHERE TRAFFIC CONTROL MANAGEMENT IS DISCUSSED.
3. BE AVAILABLE FOR MEETINGS OR DISCUSSIONS WITH THE ENGINEER UPON REQUEST OR WITHIN 36 HOURS.
4. BE AWARE OF, AND COORDINATE IF NECESSARY, ALL TRAFFIC CONTROL OPERATIONS, INCLUDING THOSE OF SUBCONTRACTORS AND SUPPLIERS.
5. COORDINATE PROJECT ACTIVITIES WITH ALL LAW ENFORCEMENT OFFICERS (LEOS). A WTS SHALL ALSO BE THE MAIN CONTACT PERSON WITH THE LEO'S WHILE THEY ARE ON THE PROJECT.
6. COORDINATE MEETINGS WITH ODOT PERSONNEL, LEO'S AND OTHER APPLICABLE ENTITIES BEFORE EACH PLAN PHASE SWITCH TO DISCUSS WORK ZONE TRAFFIC CONTROL.
7. ENSURE COMPLIANCE WITH THE CONTRACT DOCUMENTS FOR SIGNS, BARRICADES, TEMPORARY CONCRETE BARRIER, PAVEMENT MARKINGS, PORTABLE MESSAGE SIGNS, AND OTHER TRAFFIC CONTROL DEVICES ON A DAILY BASIS; AND FACILITATE ANY CORRECTIVE ACTION NECESSARY.
8. NOTIFY THE CONTRACTOR OF THE NEED FOR CLEANING AND MAINTENANCE OF ALL TRAFFIC CONTROL DEVICES, INCLUDING THE COVERING AND REMOVAL OF INAPPLICABLE SIGNS.
9. INSPECT, EVALUATE, PROPOSE NECESSARY MODIFICATIONS TO, AND DOCUMENT THE EFFECTIVENESS OF, THE TRAFFIC CONTROL DEVICES AND/OR TRAFFIC OPERATIONS ON A DAILY BASIS (7 DAYS A WEEK). IN ADDITION, A WEEKLY NIGHT INSPECTION OF THE WORK ZONE SETUP FOR DAYTIME WORK OPERATIONS; AND ONE DAYTIME INSPECTION PER WEEK FOR NIGHTTIME PROJECTS. THIS SHALL INCLUDE (BUT NOT BE LIMITED TO) DOCUMENTATION ON THE FOLLOWING PROJECT EVENTS:
 - A. INITIAL TRAFFIC CONTROL SETUP (DAY AND NIGHT REVIEW).
 - B. DAILY TRAFFIC CONTROL SETUP AND REMOVAL.
 - C. WHEN CONSTRUCTION STAGING CAUSES A CHANGE IN THE TRAFFIC CONTROL SETUP.
 - D. CRASH OCCURRENCES WITHIN THE CONSTRUCTION AREA.
 - E. REMOVAL OF TRAFFIC CONTROL DEVICES AT THE END OF A PHASE OR PROJECT.
 - F. ALL OTHER EMERGENCY TRAFFIC CONTROL NEEDS.
10. COMPLETE THE DEPARTMENT APPROVED LONG TERM INSPECTION FORM (CA-D-8) AFTER EACH INSPECTION AS REQUIRED IN # 9 AND SUBMIT IT TO THE ENGINEER THE FOLLOWING WORK DAY. THESE REPORTS SHALL INCLUDE A CHECKLIST OF ALL TRAFFIC CONTROL MAINTENANCE ITEMS TO BE REVIEWED. A COPY OF THE FORM WILL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING. ANY DEFICIENCIES OBSERVED SHALL BE NOTED, ALONG WITH RECOMMENDED CORRECTIVE ACTIONS AND THE DATES BY WHICH SUCH CORRECTIONS WERE, OR WILL BE, COMPLETED. A COPY OF THIS DOCUMENT CAN BE FOUND IN THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION INSPECTION FORMS MANUAL DATED 10/15/06 OR CURRENT REVISION.
11. VERIFY THAT ALL FLAGGING OPERATIONS ARE BEING CONDUCTED PER THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
12. HAVE COPIES OF THE ODOT TEMPORARY TRAFFIC CONTROL MANUAL AND APPLICABLE STANDARDS AND SPECIFICATIONS INCLUDED IN THE CONTRACT DOCUMENTS AVAILABLE AT ALL TIMES ON THE PROJECT.

THE DEPARTMENT WILL NOT PAY THE UNIT PRICE BID FOR THE WTS FOR ANY DAY ON WHICH THE CONTRACTOR FAILS TO PERFORM THE DUTIES SET FORTH ABOVE. SHOULD THE CONTRACTOR'S FAILURE TO PERFORM ANY OF THE DUTIES DESCRIBED ABOVE RESULT IN A MAINTENANCE OF TRAFFIC SAFETY ISSUE, THE DEPARTMENT WILL DEDUCT THE PRORATED DAILY AMOUNT FOR ITEM 614 MAINTENANCE OF TRAFFIC FROM THE CONTRACTOR'S NEXT SCHEDULED ESTIMATE.

IF THREE OR MORE FAILURES TO PERFORM THE DUTIES SET FORTH ABOVE OCCUR, THE WTS SHALL BE IMMEDIATELY REMOVED FROM THE WORK IN ACCORDANCE WITH C&MS 108.05.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED FOR THE WORKSITE TRAFFIC SUPERVISOR:

ITEM 614 - WORKSITE TRAFFIC SUPERVISOR 3 MONTHS

ITEM 614 - REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614 - REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 25 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT UNIT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

WORK OPERATIONS

IN ADDITION TO THE REQUIREMENTS OF SECTION 614 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS THE FOLLOWING SHALL APPLY:

THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAVEL WHERE PRACTICAL. A FLAGGER SHALL BE USED WHERE THE CONTRACTOR'S EQUIPMENT MUST MERGE WITH THE TRAFFIC STREAM.

THE CONTRACTOR SHALL ARRANGE CONSTRUCTION OPERATIONS SO AS TO PREVENT ANY INTERFERENCE TO THE CONTINUOUS FLOW OF TRAFFIC. ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO THE CLOSED LANES UNLESS OTHERWISE APPROVED BY THE ENGINEER.

ITEM 614 - MAINTAINING TRAFFIC LANE CLOSURE/REDUCTION REQUIRED

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

GENERAL LANE CLOSURE LIMITATIONS

THE INTENT OF THE LANE CLOSURE LIMITATIONS IN THIS PLAN NOTE IS TO SUPPLEMENT OTHER TIME LIMITATIONS WHICH APPEAR IN THIS CONTRACT.

- THE FOLLOWING LANE CLOSURE RESTRICTIONS APPLY:
1. NO WORK SHALL BE DONE ON ANY WEEKENDS IN THE MONTH OF AUGUST

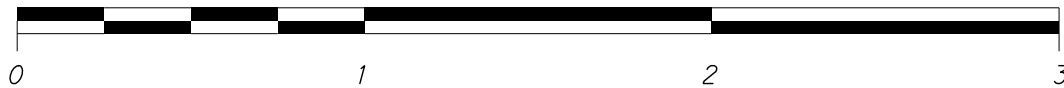
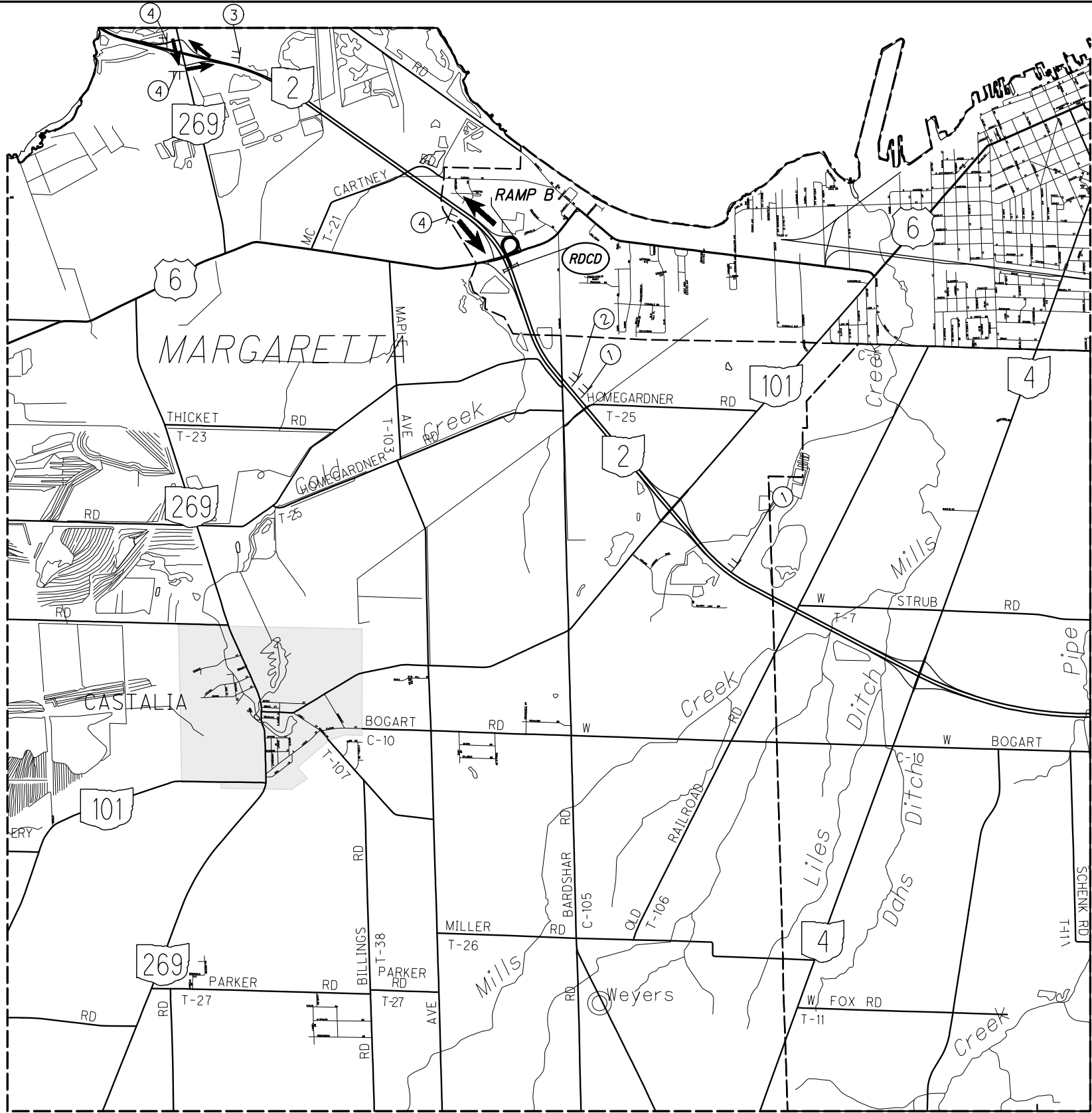
FAILURE OF THE CONTRACTOR TO MEET ANY OF THE ABOVE REQUIREMENTS ARE SUBJECT TO LIQUIDATED DAMAGES AS PER CMS 108.07.

RAMP CLOSURE LIMITATIONS

THE INTENT OF THE RAMP CLOSURE LIMITATIONS IN THIS PLAN NOTE IS TO SUPPLEMENT OTHER TIME LIMITATIONS WHICH APPEAR IN THIS CONTRACT.

- THE FOLLOWING LANE CLOSURE RESTRICTIONS APPLY:
1. NO WORK SHALL BE DONE ON THE US 250 RAMPS ON ANY WEEKENDS

FAILURE OF THE CONTRACTOR TO MEET ANY OF THE ABOVE REQUIREMENTS ARE SUBJECT TO LIQUIDATED DAMAGES AS PER CMS 108.07.



MAP LEGEND

- OFFICIAL STATE SIGNED DETOUR
- GATES AND BARRICADES, AS PER MT-98.29

SIGN LEGEND

① ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN

② DETOUR AHEAD
W20-2-36

③ DETOUR
M4-8-12
M1-4-36-2
 M6-1-12

④ DETOUR
M4-8-12
M1-4-36-2
 M6-1-12

RDCD
 DETOUR
M4-8-24
M1-5-24
 M6-1L-21

ROAD CLOSED TO THRU TRAFFIC
R11-4

DETOUR
M4-10L

TYPE III BARRICADES

DETOUR SIGNING

THE FOLLOWING QUANTITY IS INCLUDED FOR THE CONTRACTOR TO PROVIDE THE DETOUR SIGNING AS SHOWN AS PER 614.06 (B):

ITEM 614, DETOUR SIGNING LUMP

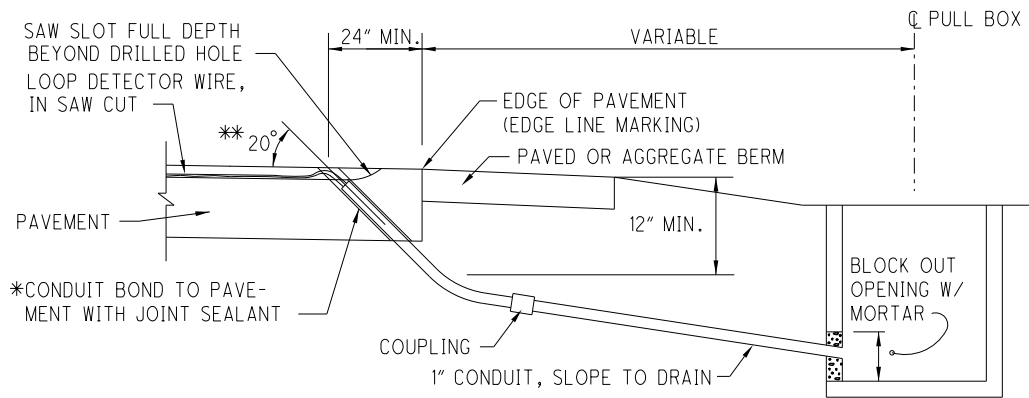
DETOUR FOR CLOSURE OF THE SR 2 WESTBOUND EXIT RAMP TO US 6 (RAMP B)

A TWO NIGHT CLOSURE WILL BE PERMITTED FOR THE PLANING AND PAVING OF THE RAMP. THE CONTRACTOR MAY ONLY WORK FROM 9 PM TO 6 AM. THE RAMP MUST REMAIN OPEN AT ALL TIMES DURING THE DAY.

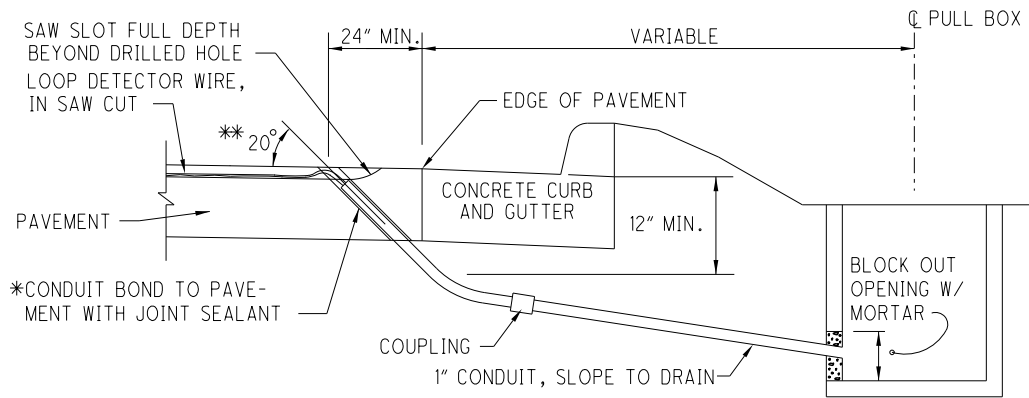
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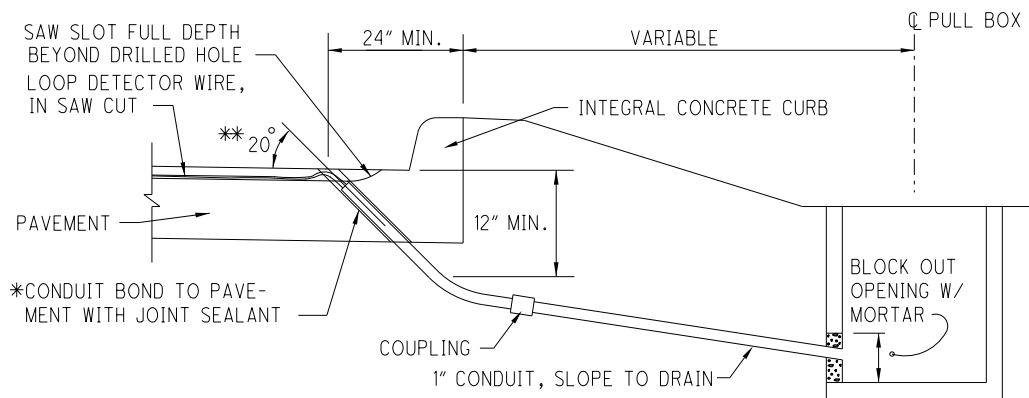
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WORKSTATIONmpeters DATE:2/28/2014 MODELNAME:Default



DRILLED HOLE LOCATION DETAIL WITH PAVED OR AGGREGATE BERM



DRILLED HOLE LOCATION DETAIL WITH CONCRETE CURB AND GUTTER

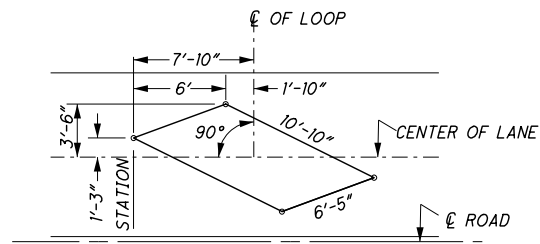


DRILLED HOLE LOCATION DETAIL WITH INTEGRAL CONCRETE CURB

* CONDUIT SHALL BE 1" DIAMETER 725.04.

** THE RANGE OF THIS ANGLE SHALL BE FROM 15 TO 30 DEGREES.

NOTE: SEE STANDARD DRAWING TC-82.10 FOR ADDITIONAL NOTES AND DETAILS



ANGULAR DESIGN DETECTION (ADD) LOOP DETAIL FOR LANE WIDTH 11' & LARGER

ITEM 632- DETECTOR LOOP, AS PER PLAN

AN ESTIMATED QUANTITY OF ITEM 632, DETECTOR LOOP, AS PER PLAN, HAS BEEN PROVIDED FOR THE PURPOSE OF REPLACING DAMAGED DETECTOR LOOPS AND/OR UPGRADING DETECTOR LOOPS TO IMPROVE MOTORCYCLE DETECTION. IT IS IMPERATIVE THAT REPLACEMENT OF DETECTOR LOOPS BE INSTALLED AND FULLY FUNCTIONAL IN THE SHORTEST POSSIBLE TIME. THE CONTRACTOR SHALL HAVE REPLACEMENT DETECTOR LOOPS INSTALLED AND FULLY FUNCTIONAL WITHIN 7 CALENDAR DAYS OF DESTRUCTION OF THE EXISTING DETECTOR LOOPS.

THE CONTRACTOR SHALL NOTIFY MATT BLANKENSHIP, ODOT DISTRICT 3 ROADWAY SERVICES MANAGER, (PHONE 419-207-7045) 5 WORKING DAYS IN ADVANCE OF ANY PLANING OPERATIONS OR PAVEMENT REPAIR WORK. THIS NOTIFICATION IS NEEDED FOR DISTRICT 3 TO SCHEDULE TEMPORARY SIGNAL TIMING MODIFICATIONS FOR THE TIME PERIOD WHEN THE DETECTOR LOOPS ARE OUT OF OPERATION. THE CONTRACTOR SHALL THEN RENOTIFY MR. BLANKENSHIP WITHIN 2 WORKING DAYS AFTER THE NEW DETECTOR LOOPS ARE REPLACED SO THAT HE CAN RESCHEDULE DISTRICT CREWS TO RESTORE SIGNAL TIMINGS TO THE ORIGINAL SETTINGS.

FAILURE TO COMPLY WITH THE ABOVE STATED REQUIREMENTS WILL RESULT IN THE ASSESSMENT OF A DISINCENTIVE FEE OF \$500.00 PER DAY TO THE CONTRACTOR FOR EACH CALENDAR DAY BEYOND THE SPECIFIED LIMIT.

THE NEW DETECTOR LOOPS SHALL BE PLACED PER THE PLAN DETAILS AFTER THE PLANING AND PAVEMENT REPAIR OPERATIONS ARE COMPLETED WITHIN THE AFFECTED AREAS. THE DETECTOR LOOPS SHALL NOT BE CUT INTO THE SURFACE COURSE.

IN ADDITION TO THE REQUIREMENTS OF CMS 632.11, THE CONTRACTOR SHALL PROVIDE A POSITIVE AND EFFECTIVE MEANS FOR REMOVAL OF SOLID RESIDUE RESULTING FROM THE DRY SAW BLADE CUTTING OF LOOP DETECTOR SLOTS IN THE PAVEMENT. THE RESIDUE SHALL BE REMOVED BY VACUUM OR OTHER EFFECTIVE MEANS, BEFORE IT IS BLOWN BY TRAFFIC ACTION OR WIND. RESIDUE FROM DRY CUTTING SHALL NOT BE REMOVED BY COMPRESSED AIR. AS AN ALTERNATE, THE CONTRACTOR MAY USE WET CUTTING.

LOOP DETECTOR WIRE TO LEAD-IN CABLE SPLICES WITHIN EPOXY ENCAPSULATED SPLICE ENCLOSURES SHALL BE JOINED BY AN APPROVED CONNECTOR AND SOLDERED PER CMS 632.23 & 725.15. ALL COSTS ASSOCIATED WITH THE SOLDERED SPLICE CONNECTION AND EPOXY SPLICE KIT SHALL BE INCLUDED WITH THE DETECTOR LOOP.

IF THE PULL BOX IS NOT SPECIFIED IN THE PLANS, THE SPLICE SHALL BE MADE IN THE FIRST ENTERED POLE OR PEDESTAL, EXCEPT WHERE THE CONTROLLER CABINET IS MOUNTED ON THE POLE OR PEDESTAL, IN WHICH CASE THE LOOP WIRES SHALL BE ROUTED DIRECTLY INTO THE CABINET UNLESS SPECIFIED DIFFERENTLY IN THE PLANS. LOOP DETECTOR WIRE ROUTED THROUGH CONDUIT, PULL BOXES, POLES, AND PEDESTALS SHALL BE TWISTED PER CMS 632.23.

FURNISH ALL MATERIALS ACCORDING TO THE DEPARTMENT'S QUALIFIED PRODUCTS LIST (QPL).

SEE DETAILS ON THIS SHEET FOR ADDITIONAL REQUIREMENTS.

PAYMENT FOR ALL OF THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH FOR ITEM 632, DETECTOR LOOP, AS PER PLAN.

ITEM	QTY.	UNITS	DESCRIPTION
632	2	EACH	DETECTOR LOOP, AS PER PLAN

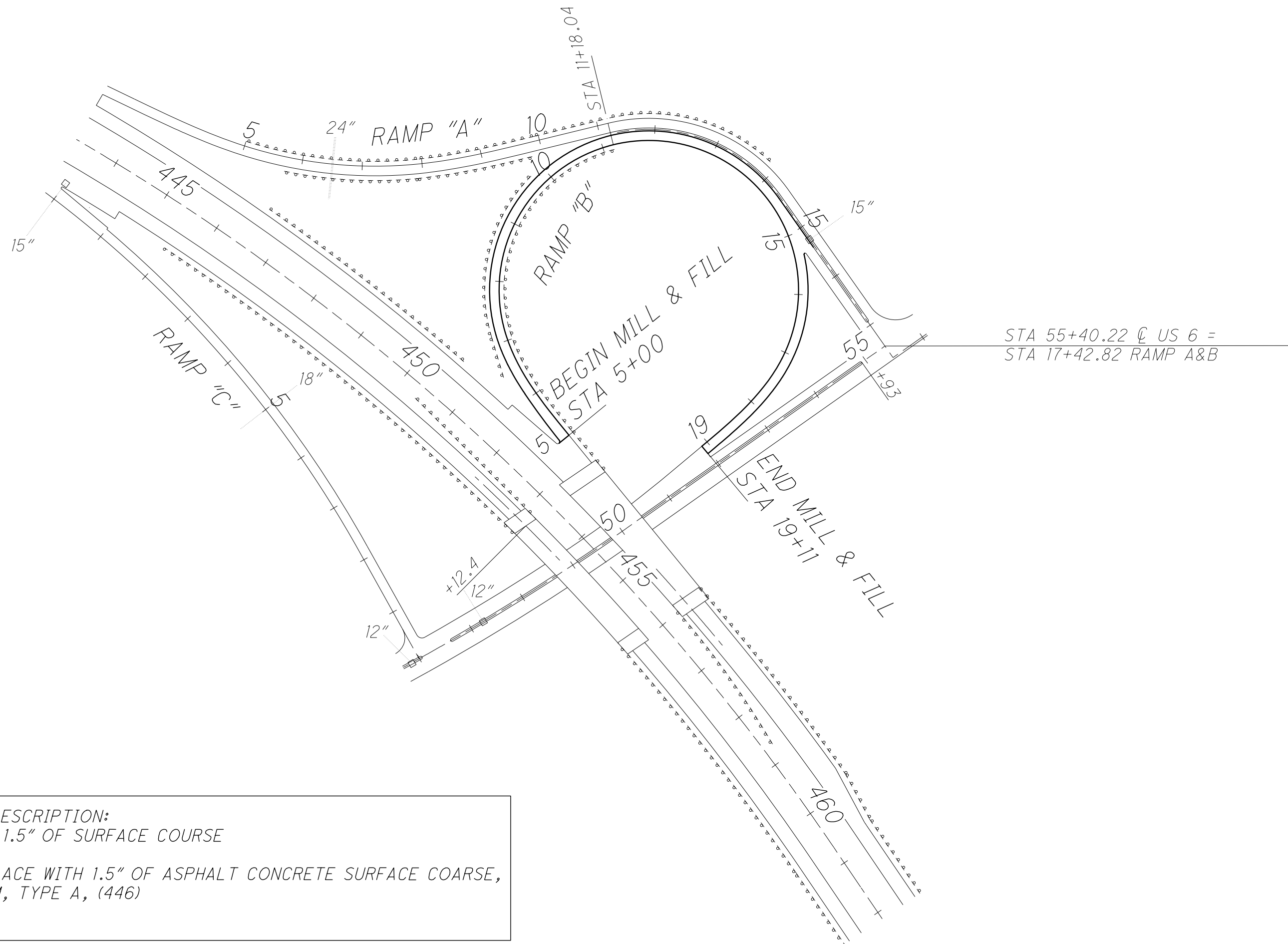
REPLACE LOOPS L-3 AND L-4.

ALL QUANTITIES CARRIED TO GENERAL SUMMARY



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WORKSTATION: ksalay DATE: 3/1/2014

2) REPLACE WITH 1.5" OF ASPHALT CONCRETE SURFACE COARSE,
12.5 MM, TYPE A, (446)



[illegible]

FOR REFERENCE ONLY

Mileage	Repair Type	Outside Lane (EB)				Repair Type	Inside Lane (EB)				Repair Type	Inside Lane (WB)				Repair Type	Outside Lane (WB)			
SR-2 SLM	TRANSVERSE/ LONGITUDINAL	Length (Ft)	Width (Ft)	Depth (in)	Volume (CY)	TRANSVERSE/ LONGITUDINAL	Length (Ft)	Width (Ft)	Depth (in)	Volume (CY)	TRANSVERSE/ LONGITUDINAL	Length (Ft)	Width (Ft)	Depth (in)	Volume (CY)	TRANSVERSE/ LONGITUDINAL	Length (Ft)	Width (Ft)	Depth (in)	Volume (CY)
2.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
	LONGITUDINAL	12	12	4	1.78															
	LONGITUDINAL	6	6	4	0.44															
	LONGITUDINAL	50	6	4	3.70															
3.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	40	12	4	5.93	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
4.0																				
	LONGITUDINAL	25	12	4	3.70	LONGITUDINAL	25	12	4	3.70	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
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	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
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9.0																				
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10.0																				
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											LONGITUDINAL	25	12	4	3.70					
13.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44
14.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	25	12	4	3.70	LONGITUDINAL	6	6	4	0.44
											LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	25	2	4	0.62
																LONGITUDINAL	6	6	4	0.44
15.0																				
	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	6	6	4	0.44	LONGITUDINAL	25	12	4	3.70	LONGITUDINAL	6	6	4	0.44
16.0																				

Ramp Repairs	Repair Type	Length (Ft)	Width (Ft)	Depth (in)	Volume (CY)
SR 269 - RAMP A	LONGITUDINAL	20	6	4	1.48
SR 269 - RAMP B	LONGITUDINAL	25	6	4	1.85
SR 269 - RAMP C	LONGITUDINAL	20	6	4	1.48
SR 269 - RAMP D	LONGITUDINAL	25	6	4	1.85
US 6 - RAMP A	LONGITUDINAL	20	6	4	1.48
US 6 - RAMP B	LONGITUDINAL	20	6	4	1.48
US 6 - RAMP C	LONGITUDINAL	15	6	4	1.11
US 6 - RAMP D	LONGITUDINAL	15	6	4	1.11
SR 101 - RAMP A	LONGITUDINAL	15	6	4	1.11
SR 101 - RAMP B	LONGITUDINAL	15	6	4	1.11
SR 101 - RAMP C	LONGITUDINAL	15	6	4	1.11
SR 101 - RAMP D	LONGITUDINAL	20	6	4	1.48
SR 4 - RAMP A	LONGITUDINAL	20	6	4	1.48
SR 4 - RAMP B	LONGITUDINAL	15	6	4	1.11
SR 4 - RAMP C	LONGITUDINAL	15	6	4	1.11
SR 4 - RAMP D	LONGITUDINAL	15	6	4	1.11
US 250 - RAMP A	LONGITUDINAL	15	6	4	1.11
US 250 - RAMP B	LONGITUDINAL	10	6	4	0.74
US 250 - RAMP C	LONGITUDINAL	10	6	4	0.74
US 250 - RAMP D	LONGITUDINAL	40	6	4	2.96
US 250 - RAMP E	LONGITUDINAL	10	6	4	0.74
RYE BEACH - RAMP A	LONGITUDINAL	10	6	4	0.74
RYE BEACH - RAMP B	LONGITUDINAL	25	6	4	1.85
RYE BEACH - RAMP C	LONGITUDINAL	15	6	4	1.11
RYE BEACH - RAMP D	LONGITUDINAL	10	6	4	0.74