

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER, OR ADJACENT TO, THE WORK AREA.

EXISTING PLANS

EXISTING PLANS ENTITLED SCI-52-33.84 MAY BE INSPECTED IN THE ODOT DISTRICT 9 OFFICE IN CHILLICOTHE.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

WINDOW CONTRACT TABLE

DESCRIPTION OF CRITICAL WORK	CALENDAR DAYS TO COMPLETE
ALL WORK ON PROJECT	60

GENERAL NOTES

DESIGN AGENCY



DESIGNER

SS

REVIEWER

SRB 12-14-22

PROJECT ID

117629

SHEET TOTAL

2 20

ITEM 614, MAINTAINING TRAFFIC

A MINIMUM OF 1 LANE OF TRAFFIC IN THE WESTBOUND AND 2 LANES IN THE EASTBOUND DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, ITEM 615 ROADS FOR MAINTAINING TRAFFIC, AND TEMPORARY SURFACES USING ITEMS 410 AND 614. CR 7 IS CURRENTLY BEING CONTROLLED BY TEMPORARY SIGNALS OWNED BY THE SCIOTO COUNTY ENGINEER. THE SCIOTO COUNTY ENGINEER WILL CONTINUE TO MAINTAIN/OPERATE THE TEMPORARY SIGNAL DURING CONSTRUCTION OF THE PROJECT. THE CONTRACTOR SHALL NOT DISTURB THE TIMING OR OPERATION OF THE SIGNALS DURING CONSTRUCTION OF THE PROJECT AND SHALL COORDINATE ANY CHANGE NEEDED WITH THE SCIOTO COUNTY ENGINEER. NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	FOURTH OF JULY
NEW YEAR'S	LABOR DAY
MEMORIAL DAY	THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY	
OR EVENT TIME	ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00AM FRIDAY
THANKSGIVING	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$50 PER MINUTE FOR THE TIME THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

MAINTAINING TRAFFIC SEQUENCE

THE MAINTAINING TRAFFIC ON THE BRIDGE TO STANDARD DRAWING MT-96.11 WITH A CLOSURE OF THE RIGHT WESTBOUND LANE IS ALREADY IN PLACE BY THE SCIOTO COUNTY ENGINEER'S OFFICE. OVERNIGHT CLOSURES MAY BE REQUIRED FOR EPOXY INJECTION ABOVE THE BEAM. THE OVERNIGHT CLOSURE SHALL BE FROM 7 PM TO 5 AM. THE CONTRACTOR SHALL COORDINATE WITH ODOT AND THE SCIOTO COUNTY ENGINEER'S OFFICE TWO (2) WEEKS PRIOR TO PERFORMING THIS WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING AND MAINTAINING TYPE III BARRICADES AS DIRECTED BY THE COUNTY ENGINEER.

ONE LANE OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES ON US-52 WESTBOUND. ALL LANES WILL BE 11' TYPICAL WITH 2'-0" SHOULDERS AND 2' PORTABLE BARRIERS UNLESS OTHERWISE NOTED ON THE PLANS. BELOW IS THE CONSTRUCTION SEQUENCE FOR THE PROJECT:

PHASE 1

PLACE PORTABLE BARRIERS REQUIRED TO FACILITATE NECESSARY REPAIRS TO THE STRUCTURE OVER THE LEFT PASSING LANE OF US-52 WESTBOUND. CONTRACTOR SHALL CLOSE THE LEFT WESTBOUND PASSING LANE, AND THEN SHIFT TRAFFIC ONTO A PORTION OF THE RIGHT DRIVE LANE AND SHOULDER.

PHASE 2

PLACE PORTABLE BARRIERS REQUIRED TO FACILITATE NECESSARY REPAIRS TO THE STRUCTURE OVER THE RIGHT DRIVE LANE OF US-52 WESTBOUND. CONTRACTOR SHALL CLOSE THE RIGHT WESTBOUND DRIVE LANE, AND THEN SHIFT TRAFFIC ONTO A PORTION OF THE LEFT PASSING LANE AND SHOULDER.

PHASE 3

PLACE PORTABLE BARRIERS REQUIRED TO FACILITATE NECESSARY REPAIRS TO THE STRUCTURE OVER THE LEFT PASSING LANE OF US-52 WESTBOUND. CONTRACTOR SHALL CLOSE THE LEFT WESTBOUND PASSING LANE, AND THEN SHIFT TRAFFIC ONTO A PORTION OF THE RIGHT DRIVE LANE AND SHOULDER.

DO NOT EXCEED THE PERMITTED LANE CLOSURE TIMES POSTED ON THE DEPARTMENT'S PERMITTED LANE CLOSURE WEB SITE OR THE LANE VALUE CONTRACT TABLE UNLESS OTHERWISE DIRECTED BY THE ENGINEER. SUBMIT A LANE CLOSURE APPLICATION REQUEST FORM TO THE ENGINEER IN WRITING, THREE (3) WORKING DAYS IN ADVANCE OF THE LANE(S) CLOSURE UNLESS OTHERWISE DIRECTED BY THE ENGINEER. NO LANE CLOSURES WILL BE PERMITTED DURING HOLIDAYS OR SPECIAL EVENTS, SEE SPECIAL EVENTS / HOLIDAY NOTE. MONITOR TRAFFIC FLOW AND RE-OPEN LANES IF ANY BACKUP BEGINS TO OCCUR.

THE MAINTAINING TRAFFIC ON THE BRIDGE TO STANDARD DRAWING MT-96.11 WITH A CLOSURE OF THE RIGHT WESTBOUND LANE IS ALREADY IN PLACE BY THE SCIOTO COUNTY ENGINEER'S OFFICE. OVERNIGHT CLOSURES MAY BE REQUIRED FOR EPOXY INJECTION ABOVE THE BEAM. THE OVERNIGHT CLOSURE SHALL BE FROM 7 PM TO 5 AM. THE CONTRACTOR SHALL COORDINATE WITH ODOT AND THE SCIOTO COUNTY ENGINEER'S OFFICE TWO (2) WEEKS PRIOR TO PERFORMING THIS WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING AND MAINTAINING TYPE III BARRICADES AS DIRECTED BY THE COUNTY ENGINEER.

ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS UNIDIRECTIONAL

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS UNIDIRECTIONAL 3 EACH

SHOULDER RUMBLE STRIPS

TRAFFIC SHALL NOT BE REQUIRED TO USE ANY PAVED BERMS FOR MAINTENANCE OF TRAFFIC OPERATIONS UNTIL THE EXISTING RUMBLE STRIPS HAVE BEEN REMOVED AND REPLACED WITH ITEM , ASPHALT CONCRETE SURFACE COURSE, TYPE , PG64-22. QUANTITIES HAVE BEEN PROVIDED BELOW TO PLANE THE EXISTING SHOULDERS 2 FEET WIDE AND 1 INCH DEEP IN THE AREA WHERE THE EXISTING RUMBLE STRIPS OCCUR. THIS AREA SHALL RECEIVE AN APPLICATION OF ITEM 407 TACK COAT PRIOR TO REPLACEMENT OF THE ASPHALT CONCRETE SURFACE COURSE. ACCEPTANCE OF THE ASPHALT SURFACE COURSE SHALL IN ACCORDANCE WHIT C&MS 449.

THE QUANTIITES BELOW ARE BASED ON THE FOLLOWING:
 STA. 1882+05 TO STA. 1899+80 (NORTHBOUND RIGHT LANE)

STA. 1883+80 TO STA. 1898+45 (NORTHBOUND LEFT LANE)

ITEM 254, PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A, (1" DEEP) (3240 FT. x 2 FT. / 9) = 720 SY

ITEM 441, ASPHALT CONCRETE SURFACE COURSE, TYPE 1 (449), PG64-22 (1" THICK) (3240 FT. x 0.0833 FT. x 2 FT. / 27) = 20 CY

ITEM 407, TACK COAT (@ 0.085 GAL PER SQ. YD.) (780 FT. x 0.085) = 67 GAL

ITEM 618, RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE) (1775 FT. + 1465 FT.) = 3240 FT.

DELINEATION OF PORTABLE AND PERMANENT BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL; AND, ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70. WHEN THE PB CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

ITEM 614, BARRIER REFLECTOR, TYPE 1, ONE-WAY	24 EACH
ITEM 614, OBJECT MARKER, ONE-WAY	24 EACH

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE ABOVE ITEMS.

ALONG RUNS OF INCREASED BARRIER DELINEATION WHERE THIS ITEM IS PROVIDED, THE QUANTITY SHALL BE MEASURED AS THE ENTIRE LENGTH OF THE RUN OF INCREASED BARRIER DELINEATION, INCLUDING THE SPACES BETWEEN THE INDIVIDUAL DELINEATION PANELS OR STACKS OF BARRIER REFLECTORS.

DESIGN AGENCY



DESIGNER
SS

REVIEWER
SRB 12-14-22

PROJECT ID
117629

SHEET	TOTAL
3	20

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

FOR OPERATIONS WITHOUT POSITIVE PROTECTION OCCURRING WITHIN 10 FEET OF AN OPEN TRAVELED LANE THAT MEET ALL OF THE FOLLOWING CRITERIA:
 ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE OF THE OPERATION; AND, AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

"WITHOUT POSITIVE PROTECTION" MEANS USE OF DRUMS, CONES, SHADOW VEHICLE, ETC, WITHOUT PROTECTION FROM PORTABLE BARRIER OR OTHER RIGID BARRIER ALONG THE WORK AREA. THIS PHRASE DOES NOT APPLY TO CASES WHERE POSITIVE PROTECTION IS REQUIRED. MOBILE OPERATIONS ARE REGARDED AS "WITHOUT POSITIVE PROTECTION". FOR WORK ZONES USING A COMBINATION OF BARRIER AND TEMPORARY TRAFFIC CONTROL DEVICES (CONES, DRUMS, ETC), THE DESIGNATION SHALL BE BASED UPON THE TYPE OF DEVICES USED IN THE AREA THAT WORKERS ARE LOCATED.

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS, CONT.

IF MULTIPLE ACTIVE LOCALIZED QUALIFYING WORK AREAS OCCUR WITHOUT POSITIVE PROTECTION, PER MAINLINE TRAFFIC DIRECTION, PROVIDE A UNIFORMED LEO AND OFFICIAL PATROL CAR IN ADVANCE OF:
 THE FIRST ACTIVE WORK AREA THAT DRIVERS WILL ENCOUNTER; OR
 THE ACTIVE WORK AREA Laterally CLOSEst TO THE OPEN TRAVELED LANE; OR
 OTHER LOCATION AS APPROVED BY THE ENGINEER.
 THE UNIFORMED LEO AND OFFICIAL PATROL CAR MAY RELOCATE AMONG THE LISTED LOCATIONS AS APPROPRIATE AS THE OPERATIONS PROCEED IN THE LOCALIZED QUALIFYING WORK AREAS.

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 40 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTE A MINIMUM VERTICAL CLEARANCE OF 14'-6" SHALL BE MAINTAINED AT ALL TIMES. NOTIFY THE ENGINEER IF THE MINIMUM VERTICAL CLEARANCE IS LESS THAN 14'-6" AT ANY POINT DUE TO THE EXISTING CONDITIONS OR CONTRACTOR OPERATIONS. PRIOR TO OPENING ANY TEMPORARY LANE CONFIGURATION TO TRAFFIC THE CONTRACTOR SHALL VERIFY THE ACTUAL VERTICAL CLEARANCE AND PROVIDE DOCUMENTATION OF THIS CLEARANCE TO THE ENGINEER. INCLUDE THE COSTS FOR THESE MEASUREMENTS AND SUBMITTALS FOR PAYMENT WITH ITEM 614.

NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE

ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	4 CALENDAR DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

DESIGN AGENCY



DESIGNER

SS

REVIEWER

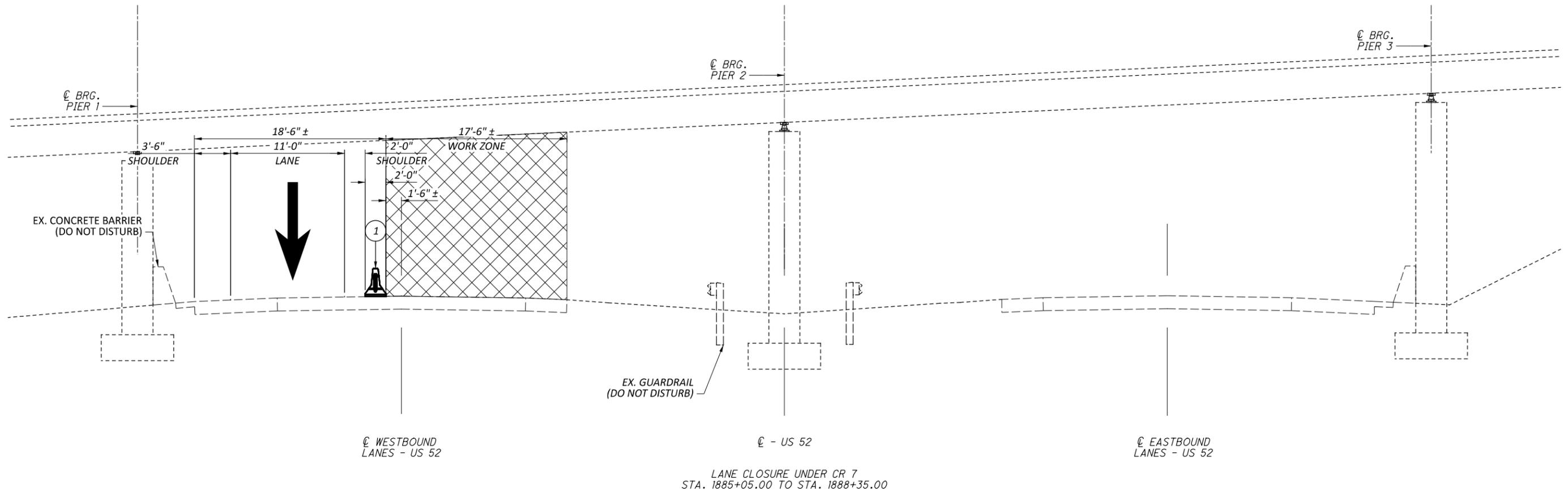
SRB 12-14-22

PROJECT ID

117629

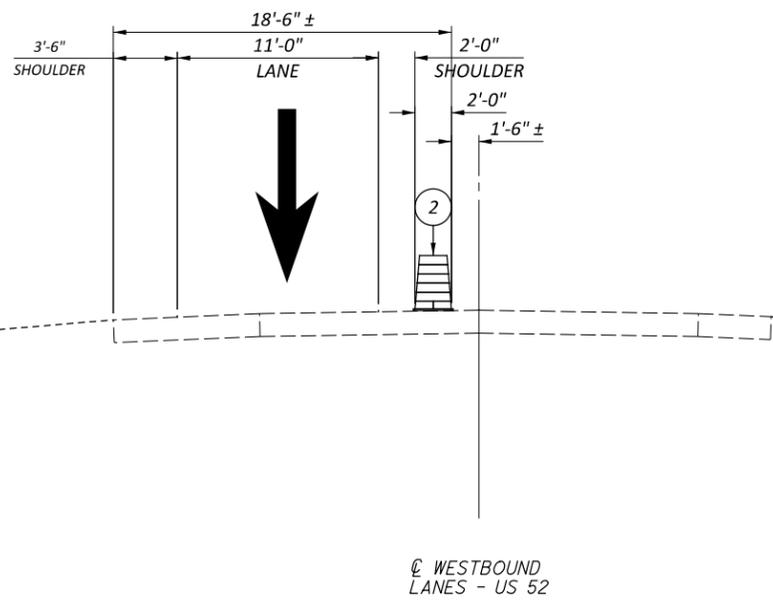
SHEET TOTAL

4 | 20



LEGEND

- ① PORTABLE BARRIER
- ② DRUMS FOR MAINTAINING TRAFFIC



DESIGN AGENCY



DESIGNER

SS

REVIEWER

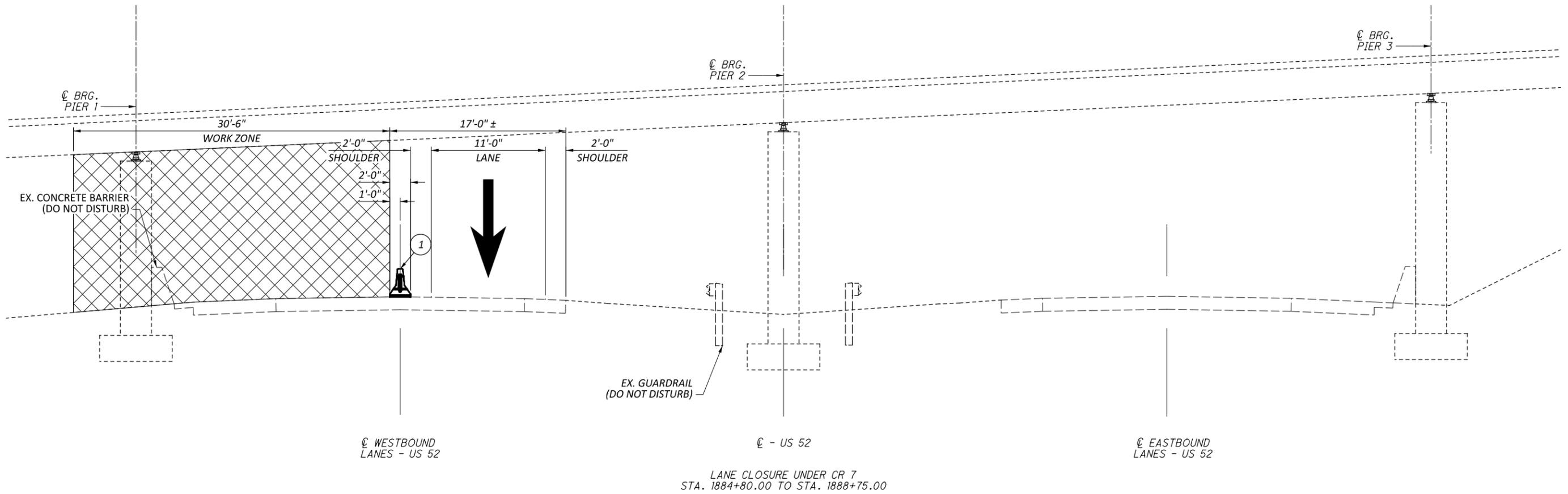
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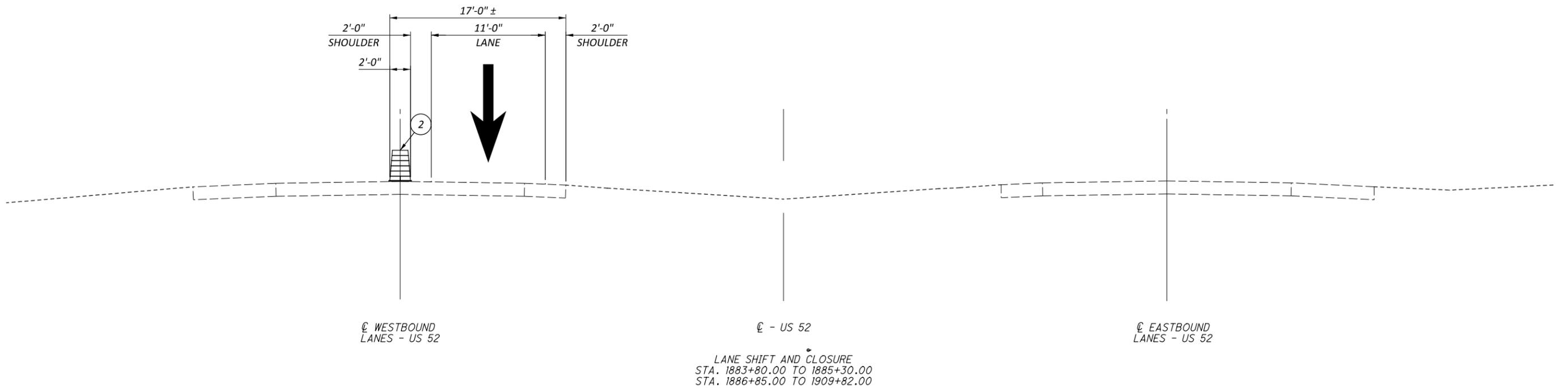
SHEET TOTAL

5 20



LEGEND

- ① PORTABLE CONCRETE BARRIER
- ② DRUMS FOR MAINTAINING TRAFFIC



DESIGN AGENCY



DESIGNER

SS

REVIEWER

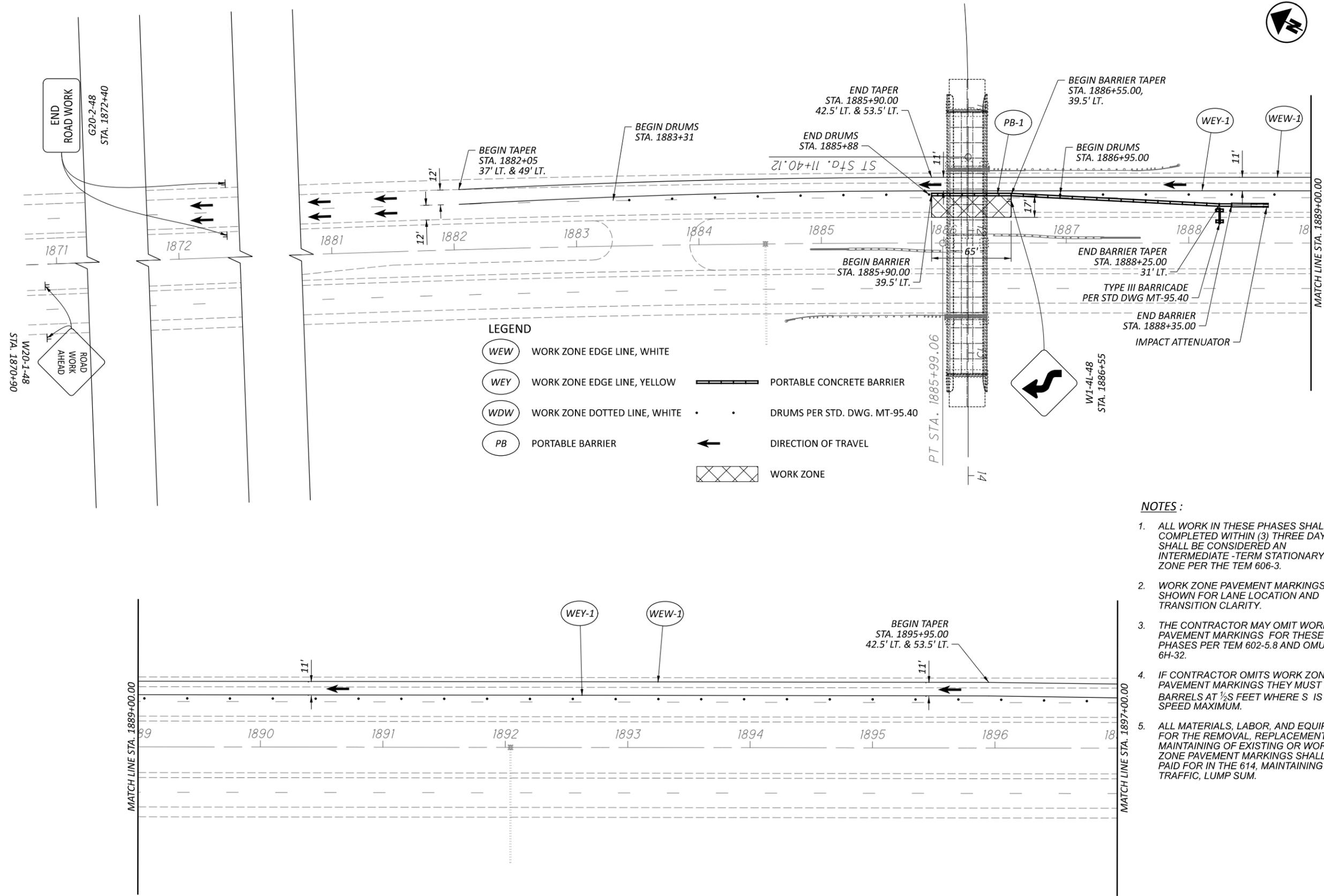
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PROJECT ID

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SHEET TOTAL

6 20



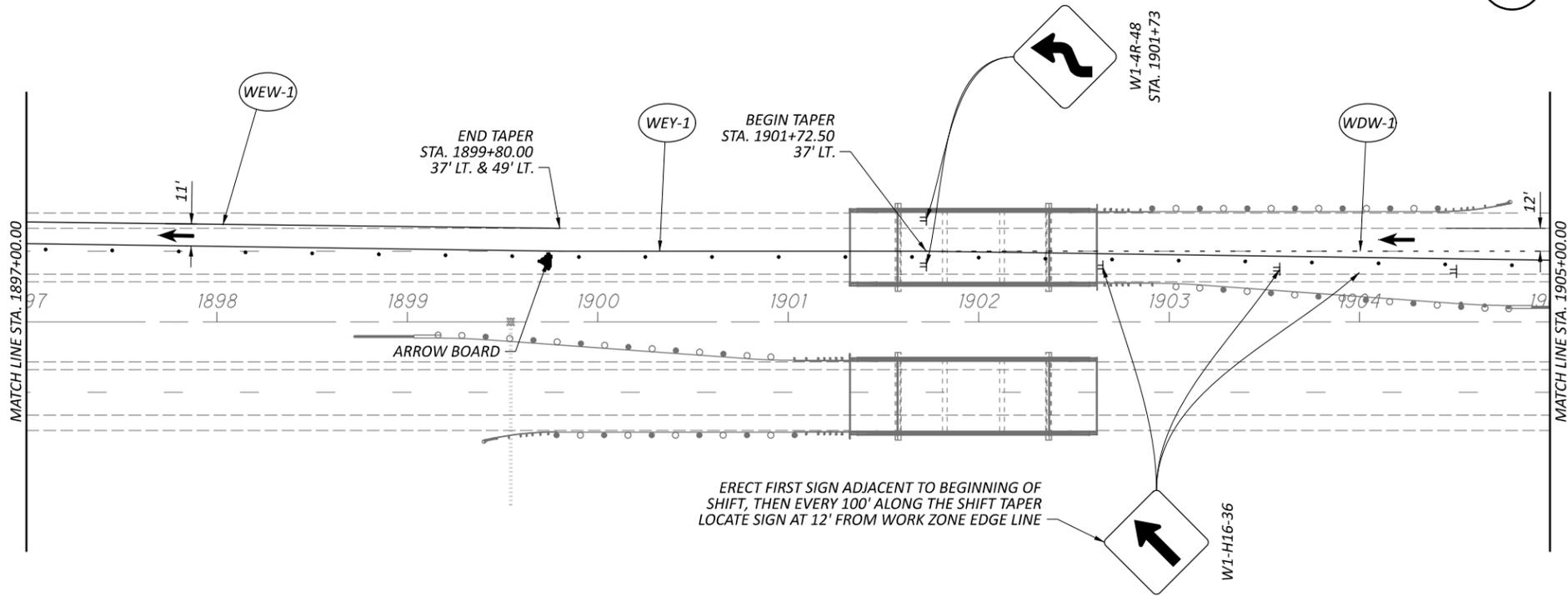
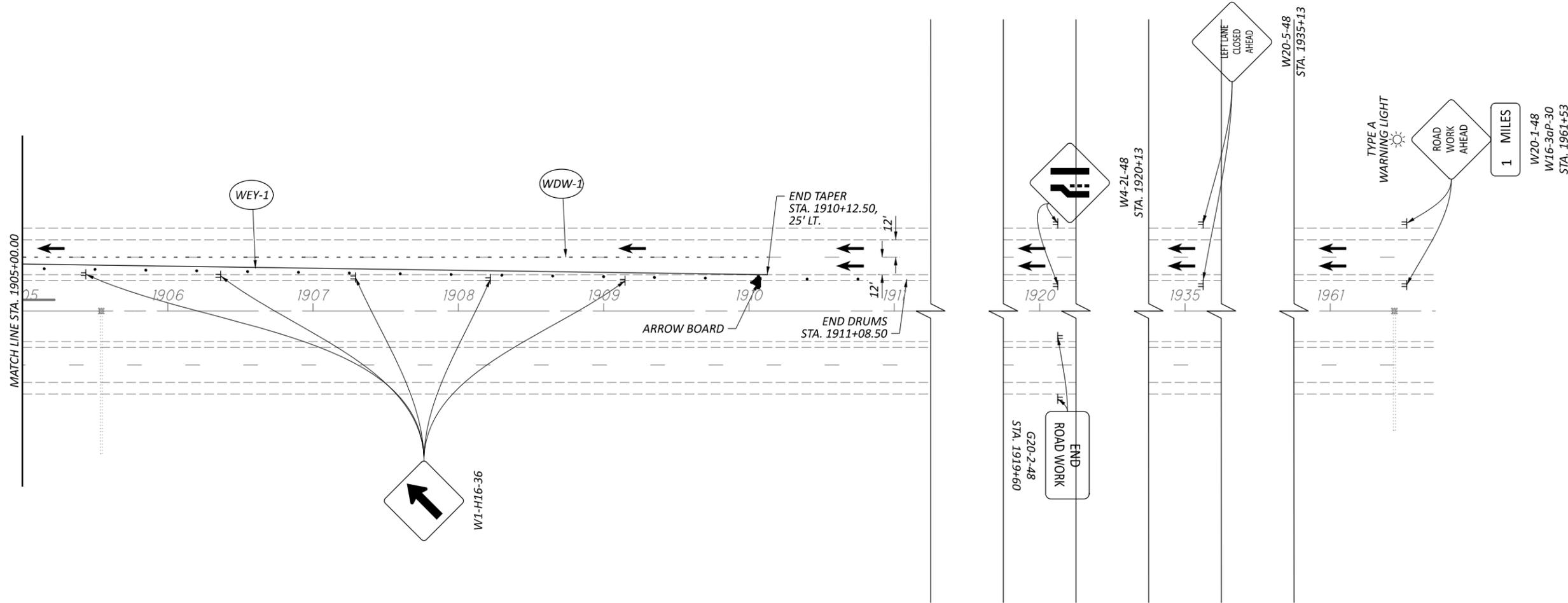
LEGEND

	WORK ZONE EDGE LINE, WHITE		PORTABLE CONCRETE BARRIER
	WORK ZONE EDGE LINE, YELLOW		DRUMS PER STD. DWG. MT-95.40
	WORK ZONE DOTTED LINE, WHITE		DIRECTION OF TRAVEL
	PORTABLE BARRIER		WORK ZONE

- NOTES :**
- ALL WORK IN THESE PHASES SHALL BE COMPLETED WITHIN (3) THREE DAYS AND SHALL BE CONSIDERED AN INTERMEDIATE - TERM STATIONARY WORK ZONE PER THE TEM 606-3.
 - WORK ZONE PAVEMENT MARKINGS SHOWN FOR LANE LOCATION AND TRANSITION CLARITY.
 - THE CONTRACTOR MAY OMIT WORK ZONE PAVEMENT MARKINGS FOR THESE PHASES PER TEM 602-5.8 AND OMUTCD 6H-32.
 - IF CONTRACTOR OMITTS WORK ZONE PAVEMENT MARKINGS THEY MUST PLACE BARRELS AT 1/2 S FEET WHERE S IS THE SPEED MAXIMUM.
 - ALL MATERIALS, LABOR, AND EQUIPMENT FOR THE REMOVAL, REPLACEMENT, AND MAINTAINING OF EXISTING OR WORK ZONE PAVEMENT MARKINGS SHALL BE PAID FOR IN THE 614, MAINTAINING TRAFFIC, LUMP SUM.

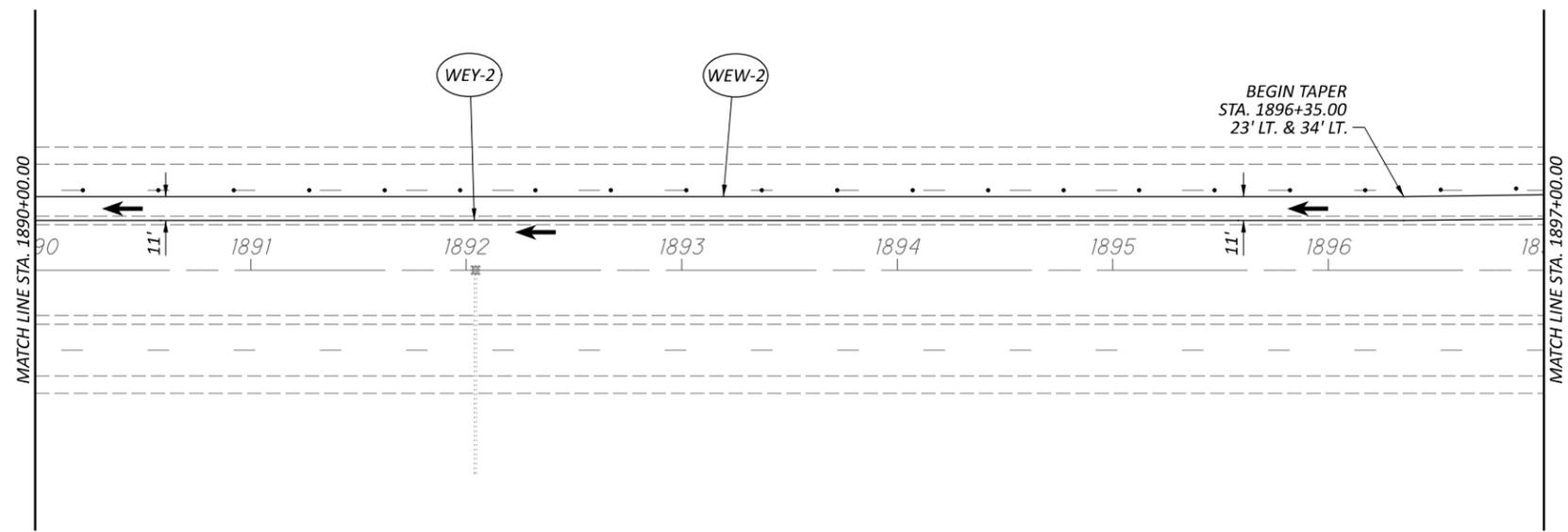
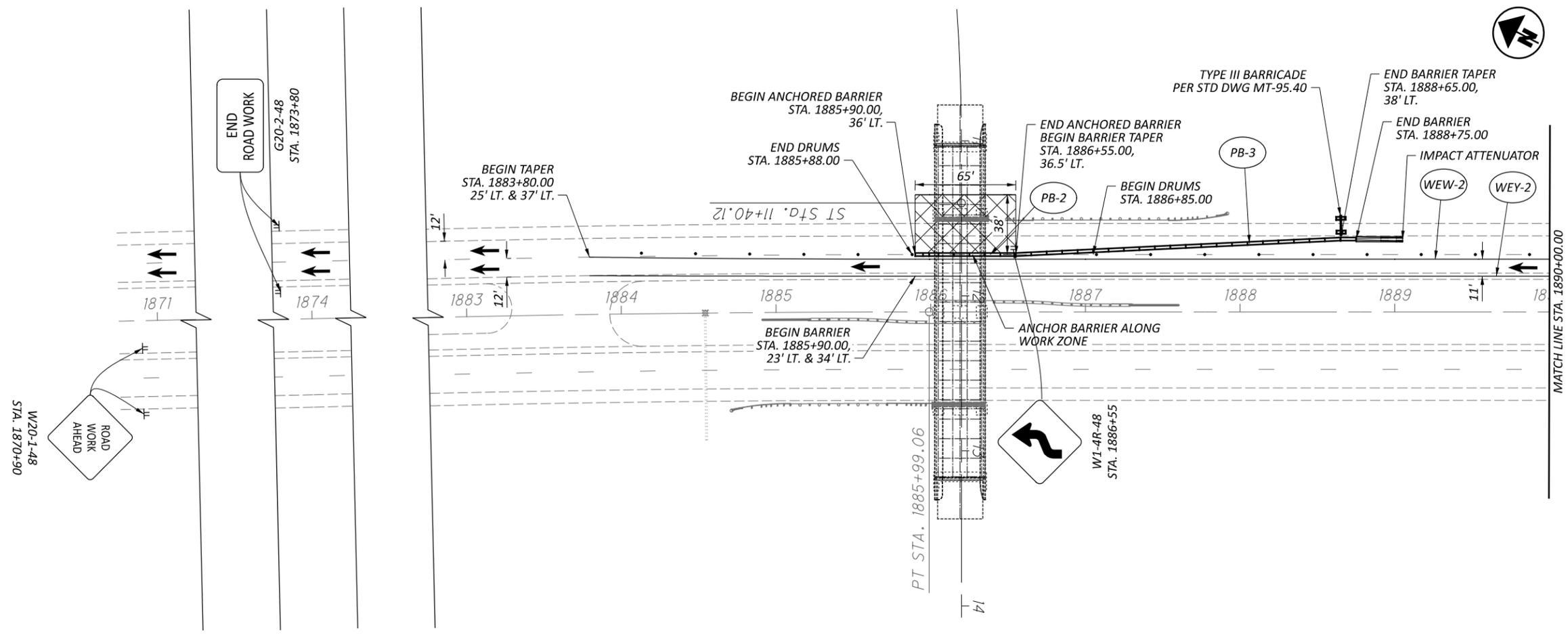
**MAINTENANCE OF TRAFFIC PHASE 1 & 3 PLAN
 STA. 1870+75.00 TO STA. 1897+00.00**

DESIGN AGENCY	
DESIGNER	SS
REVIEWER	SRB
PROJECT ID	12-14-22
SHEET	117629
TOTAL	7
	20



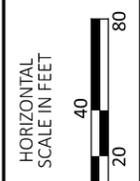
MAINTENANCE OF TRAFFIC PHASE 1 & 3 PLAN
 STA. 1897+00.00 TO STA. 1961+75.00

DESIGN AGENCY	
[B]	
DESIGNER	SS
REVIEWER	SRB
PROJECT ID	12-14-22
SHEET	117629
TOTAL	8
	20



NOTES :

1. ALL WORK IN THIS PHASES SHALL BE CONSIDERED AN LONG-TERM STATIONARY WORK ZONE PER THE TEM 606-3.
3. ALL MATERIALS, LABOR, AND EQUIPMENT FOR THE REMOVAL, REPLACEMENT, AND MAINTAINING OF EXISTING OR WORK ZONE PAVEMENT MARKINGS SHALL BE PAID FOR IN THE 614, MAINTAINING TRAFFIC, LUMP SUM, UNLESS NOTED OTHERWISE.



**MAINTENANCE OF TRAFFIC PHASE 2 PLAN
 STA. 1870+75.00 TO STA. 1897+00.00**

DESIGN AGENCY



DESIGNER

SS

REVIEWER

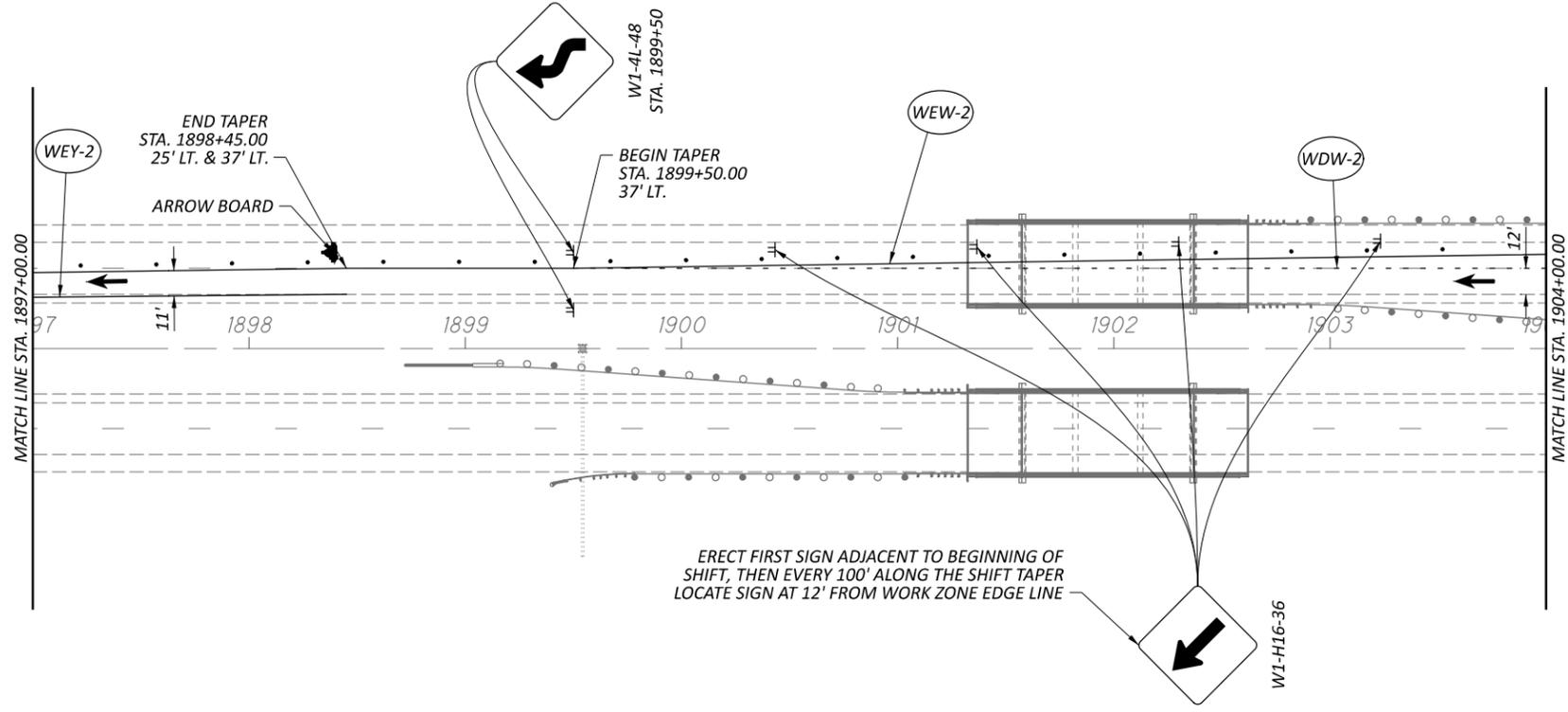
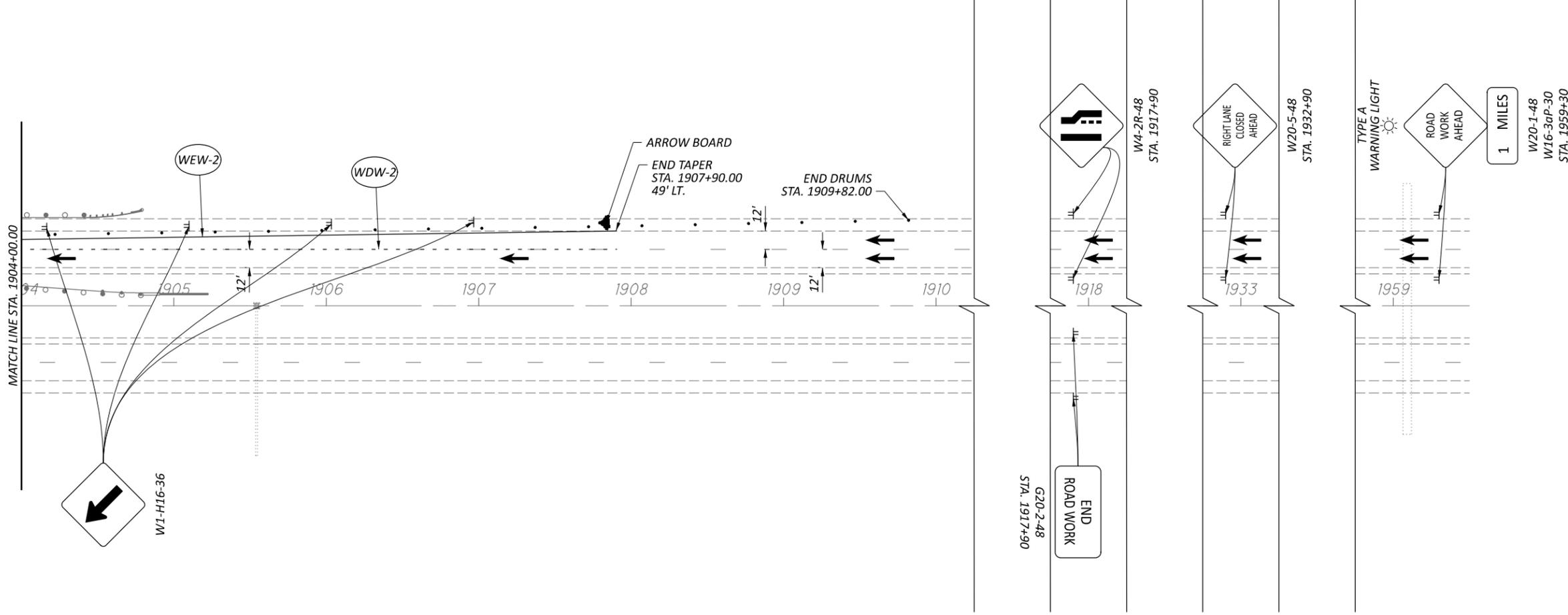
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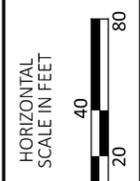
SHEET TOTAL

9 20



DESIGN AGENCY	[B]
DESIGNER	SS
REVIEWER	SRB
PROJECT ID	12-14-22
SHEET	10
TOTAL	20

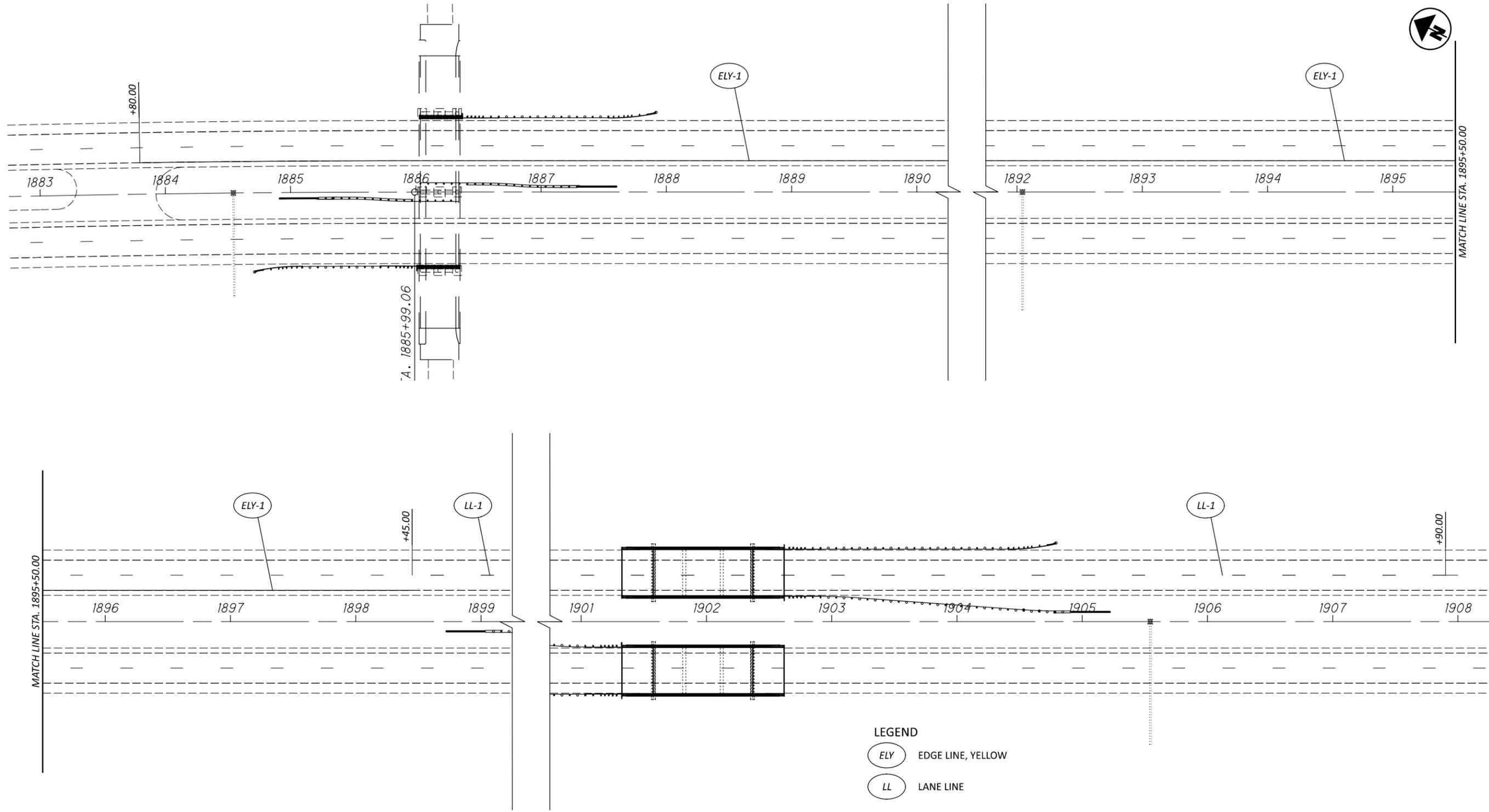
MAINTENANCE OF TRAFFIC PHASE 2 PLAN
 STA. 1897+00.00 TO STA. 1959+50.00



TRAFFIC CONTROL						
REF. NO.	SHEET NO.	STATION TO STATION	621		642	
			RPM EACH	RAISED PAVEMENT MARKER REMOVED EACH	EDGE LINE, 4", TYPE 1 MILE	LANE LINE, 4", TYPE 1 MILE
ELY-1	13	1883+80.00			0.28	
LL-1	13	1898+45.00	8	8		0.18
TOTALS CARRIED TO GENERAL SUMMARY			8	8	0.28	0.18

MAINTENANCE OF TRAFFIC							
REF. NO.	SHEET NO.	STATION TO STATION	614			622	
			WORK ZONE EDGE LINE, CLASS I, 4", 740.06, TYPE I (YELLOW) MILE	WORK ZONE EDGE LINE, CLASS I, 4", 740.06, TYPE I (WHITE) MILE	WORKZONE DOTTED LINE, CLASS I, 4", 740.06, 740.06, TYPE I FT	PORTABLE BARRIER, UNANCHORED FT	PORTABLE BARRIER, ANCHORED FT
PB-1	7	1885+90.00				735	
PB-1	7	1885+90.00				735	
PB-2	9	1885+90.00					195
PB-3	9	1886+55.00				660	
WEY-1	7-8	1882+05.00	0.54				
WEW-1	7-8	1882+05.00		0.34			
WDW-1	8	1901+72.50			840		
WEY-2	9 - 10	1883+80.00	0.28				
WEW-2	9 - 10	1883+80.00		0.46			
WDW-2	10	1899+50.00			840		
TOTALS CARRIED TO GENERAL SUMMARY			1.62		1680	2130	195

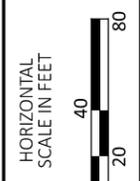




LEGEND

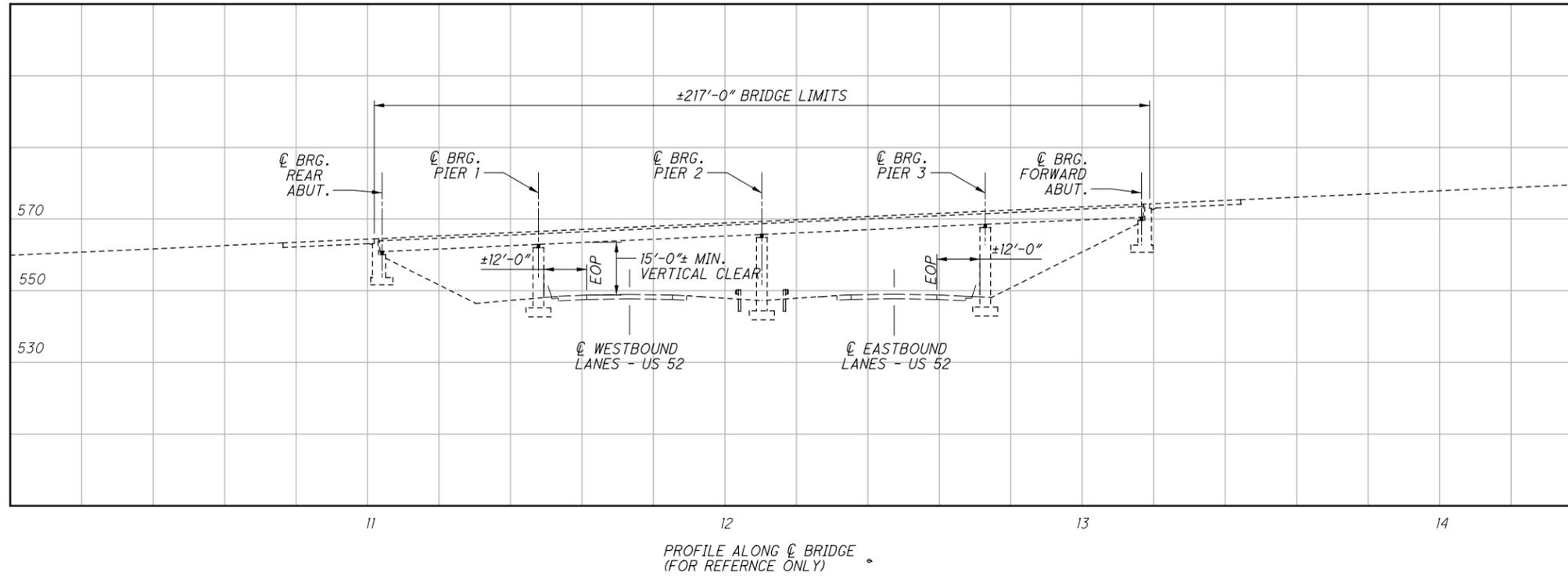
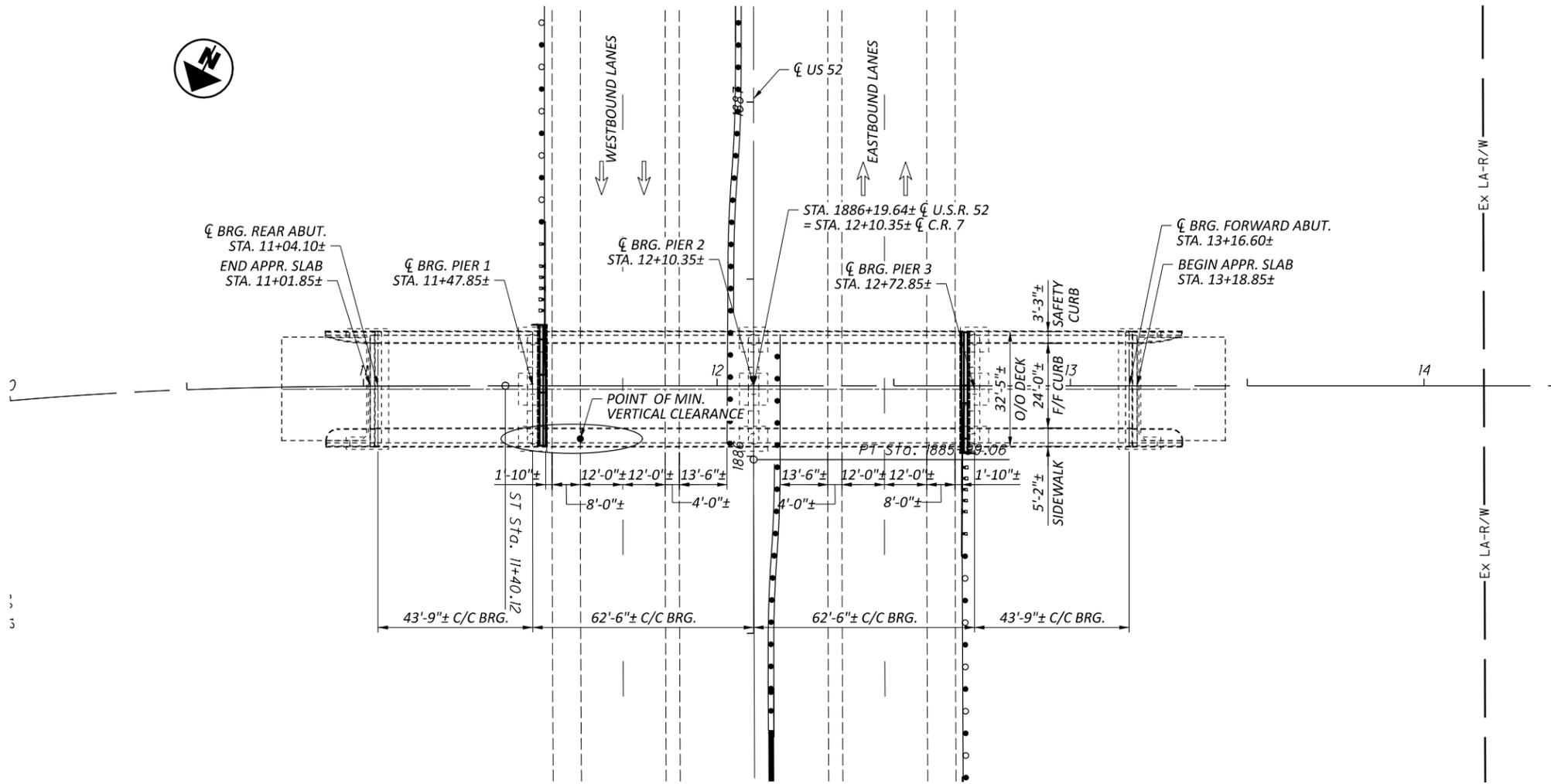
ELY EDGE LINE, YELLOW

LL LANE LINE



TRAFFIC CONTROL PLAN
STA. 1903+00.00 TO STA. 1923+00.00

DESIGN AGENCY	
[BI]	
DESIGNER	SS
REVIEWER	SRB
PROJECT ID	12-14-22
SHEET	117629
TOTAL	13
	20

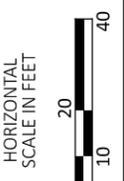


PROFILE ALONG C BRIDGE
(FOR REFERENCE ONLY)

LEGEND
 INDICATES AREA TO BE HEAT STRAIGHTENED

EXISTING STRUCTURE	
TYPE:	CONTINUOUS STEEL BEAM BRIDGE WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE
SPANS:	±43'-9", ±62'-6", ±62'-6", ±43'-9" c/c BEARING
ROADWAY:	±24'-0" F/F SAFETY CURB
LOADING:	CF 130 (57)
SKEW:	NONE
WEARING SURFACE:	LOW SLUMP CONCRETE (1.2"± THICK)
APPROACH SLABS:	AS-1-54 (±25'-0" LONG)
ALIGNMENT:	SPIRAL TANGENT
CROWN:	VARIES
STRUCTURE FILE NUMBER:	7302045
DATE BUILT:	1963
DISPOSITION:	TO BE REPAIRED
COORDINATES:	LATITUDE 38°37'20.23" N LONGITUDE 82°50'13.55" W

PROPOSED WORK	
1. HEAT STRAIGHTEN BEAM 1 2. REPLACE SECTION OF BEAM 1 3. REPLACE PORTION OF BEAM 1 FIELD SPLICE AT PIER 1 4. REPLACE BEAM 1 ROCKER FACE PLATE AT PIER 1 5. REPAIR/REPLACED DAMAGED CROSSFRAMES 6. REPAIR GAP BETWEEN BEAM 1 TOP FLANGE AND THE CONCRETE DECK USING EPOXY INJECTION. 7. FIELD PAINT REPAIRED/REPLACED STRUCTURAL STEEL 8. COMPLETE OTHER ITEMS OF WORK SHOWN ON THE PLANS. WORK LISTED IS NOT ALL INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED FOLLOWING THE MAINTENANCE OF TRAFFIC PHASE CONSTRUCTIONS SHOWN ON THE PLANS.	



SITE PLAN
 BRIDGE NO.: SCI-00007-00.420
 COUNTY ROUTE 7 OVER US ROUTE 52

SFN	7302045
DESIGN AGENCY	
DESIGNER	SS
CHECKER	DEB
REVIEWER	
SRB	12-14-22
PROJECT ID	117629
SUBSET	TOTAL
1	7
SHEET	TOTAL
14	20

STANDARD DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS:

REFER TO THE FOLLOWING STANDARD BRIDGE DRAWINGS:

RB-1-55 REVISED 7-19-13

AND TO THE FOLLOWING SUPPLEMENTAL SPECIFICATIONS:

849 DATED 1/18/13

DESIGN DATA:

STRUCTURAL STEEL - ASTM A709, GRADE 50, MINIMUM YIELD STRENGTH 50 KSI

STEEL RESTRAINT OR PRELOAD LIMITS:

EXISTING STRUCTURAL STEEL - ASTM A373, YIELD STRENGTH 33 KSI, DO NOT SUBJECT ANY PART OF THE STRUCTURAL STEEL TO A JACKING, PULLING, OR RESTRAINING UNIT STRESS EXCEEDING 16 KSI.

EXISTING STRUCTURE VERIFICATION:

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO C&MS SECTIONS 102.05, 105.02 AND 513.04.

BASE CONTRACT BID PRICES UPON RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PRE-BID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

EXISTING STRUCTURE VERIFICATION:

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CM&S SECTIONS 102.05, 105.02 AND 513.04.

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

EXISTING PLANS:

EXISTING PLANS CAN BE INSPECTED IN THE LOCAL ODOT DISTRICT OFFICE.

ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN (MAIN MEMBERS):

AN ESTIMATED QUANTITY FOR EACH LOCATION IS PROVIDED IN TABLE 3, FOR REMOVAL OF PORTIONS OF MAIN MEMBERS AS DETERMINED BY FIELD INSPECTION ACCORDING TO ITEM 849, DAMAGE ASSESSMENT OR AS DIRECTED BY THE ENGINEER. SUPPORT THE EXISTING MAIN MEMBERS ACCORDING TO ITEM 849, STRAIGHTENING WORK PLAN. FLAME OR SAW CUT PORTIONS OF THE EXISTING MEMBERS USING A MECHANICAL GUIDE ACCORDING TO C&MS 513.12. PROVIDE SHIELDING AS NECESSARY TO PREVENT DAMAGE TO MAIN OR SECONDARY MATERIALS THAT REMAIN. GRIND THE REMAINING CUT SURFACES OF THE EXISTING MEMBER SMOOTH IN PREPARATION FOR COMPLETE PENETRATION OR FILLET WELDING. PROVIDE A SURFACE FINISH ACCORDING TO ANSI B46.1 OF 250 MIL (TO ACCOMMODATE THE PROPOSED REPLACEMENT MATERIAL). DETERMINE FINAL QUANTITIES BY FIELD MEASUREMENTS. THE DEPARTMENT WILL INCLUDE ALL MATERIALS, TOOLS, LABOR, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK FOR PAYMENT WITH THE UNIT PRICE BID FOR ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN (MAIN MEMBERS).

ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN (SECONDARY MEMBERS):

AN ESTIMATED QUANTITY FOR EACH LOCATION (SECONDARY MEMBERS): IS PROVIDED IN TABLE 2, FOR REMOVAL OF SECONDARY MEMBERS AS DETERMINED BY FIELD INSPECTION ACCORDING TO ITEM 849, DAMAGE ASSESSMENT OR AS DIRECTED BY THE ENGINEER. SUPPORT THE EXISTING SECONDARY MEMBERS ACCORDING TO ITEM 849, STRAIGHTENING WORK PLAN. FLAME OR SAW CUT THE EXISTING MEMBERS TO WITHIN 1/8 INCH OF THE EXISTING MAIN MATERIAL USING A MECHANICAL GUIDE ACCORDING TO C&MS 513.12 PROVIDE SHIELDING AS NECESSARY TO PREVENT DAMAGE TO MAIN OR SECONDARY MATERIALS THAT REMAIN. GRIND THE EXISTING MAIN OR SECONDARY MEMBER SMOOTH IN PREPARATION FOR COMPLETE PENETRATION OR FILLET WELDING. PROVIDE A SURFACE FINISH ACCORDING TO ANSI B46.1 OF 250 MIL (TO ACCOMMODATE THE PROPOSED REPLACEMENT MATERIALS). DETERMINE FINAL QUANTITIES BY FIELD MEASUREMENTS. THE DEPARTMENT WILL INCLUDE ALL MATERIALS, TOOLS, LABOR, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK FOR PAYMENT WITH THE UNIT PRICE BID FOR ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN (SECONDARY MEMBERS).

ITEM 513 - STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN:

ALL REQUIREMENTS OF 513 APPLY TO SHOP FABRICATED MEMBERS. PERFORM WORK FOR FIELD FABRICATED MEMBERS ACCORDING TO ITEM 513, EXCEPT AS MODIFIED HEREIN. THE DEPARTMENT WILL NOT REQUIRE THE CONTRACTOR PERFORMING FIELD FABRICATION TO BE PRE-QUALIFIED AS SPECIFIED IN SUPPLEMENT 1078. SUBMIT A WRITTEN LETTER OF MATERIAL ACCEPTANCE, 501.06, TO THE ENGINEER. PROVIDE SHOP DRAWINGS ACCORDING TO 513.06 OR SUPPLY THE ENGINEER WITH "AS BUILT" DRAWINGS MEETING 513.06 AFTER COMPLETION OF FIELD FABRICATION. THE ENGINEER WILL REVIEW THE SUBMITTED DRAWINGS FOR CONCURRENCE WITH THE FINAL AS-BUILT CONDITION. THE ENGINEER MAY CONTACT THE OFFICE OF STRUCTURAL ENGINEERING FOR TECHNICAL ASSISTANCE. IF THE ENGINEER IS SATISFIED WITH THE "AS-BUILT" DRAWINGS AND THE DELIVERED MATERIALS. SUPPLY A COPY OF THE DRAWINGS, STAMPED, SEALED AND DATED, ACCORDING S1002, TO THE STRUCTURAL, WELDING AND METALS SECTION OF THE OFFICE OF MATERIAL MANAGEMENT FOR RECORD PURPOSES. THE MEMBERS INCLUDED IN THIS ITEM ARE PROVIDED IN TABLE 2 AND 3. THE DEPARTMENT WILL INCLUDE ALL MATERIALS, TOOLS, LABOR, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK FOR PAYMENT WITH ITEM 513 - STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN: POUND.

ITEM 513 - STRUCTURAL STEEL MISC., REPAIR OF DAMAGED MAIN MEMBER, COMPLETE PENETRATION WELDING:

AFTER DAMAGED AREAS HAVE BEEN INSPECTED ACCORDING TO ITEM 849 DAMAGE ASSESSMENT. PREPARE THE DAMAGED MATERIAL FOR WELDING, PROVIDE RUNOFF TABS FOR ALL COMPLETE PENETRATION WELDS. PERFORMING COMPLETE PENETRATION WELDS ACCORDING TO C&MS 513 USING APPROVED ELECTRODES, PROCEDURES AND WELDERS. REMOVE RUNOFF TABS AND GRIND THE COMPLETED EDGES SMOOTH. GRIND THE COMPLETED WELDS SMOOTH AND FLUSH WITH THE ADJACENT SURFACES TO PROVIDE A SURFACE FINISH ACCORDING TO ANSI B46.1 OF 250 MIL. DO NOT OVER GRIND AS TO REDUCE THE MATERIAL THICKNESS OR WIDTH OF THE NEW OR EXISTING MATERIALS. PREPARE ALL REENRANT CORNERS WITH A ONE INCH RADIUS. REMOVE WELDING, START AND STOP DISCONTINUITIES. RADIOGRAPHIC TEST THE FINISHED WELDS ACCORDING TO C&MS 513.25A AND SUBMIT COPIES OF THE REPORTS TO THE ENGINEER. THE ENGINEER MAY OBTAIN TECHNICAL ASSISTANCE FROM THE OFFICE OF MATERIALS MANAGEMENT. THE DEPARTMENT WILL INCLUDE ALL MATERIALS; TOOLS; LABOR; EQUIPMENT; AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK FOR PAYMENT WITH ITEM 513 - STRUCTURAL STEEL MISC., REPAIR OF DAMAGED MAIN MEMBERS, COMPLETE PENETRATION WELDING. FOOT.

ITEM 513 - STRUCTURAL STEEL MISC., REPAIR OF DAMAGED MAIN OR SECONDARY MEMBERS, FILLET WELDING:

AFTER DAMAGED AREAS HAVE BEEN INSPECTED ACCORDING TO ITEM 849 DAMAGE ASSESSMENT. PREPARE THE DAMAGED MATERIAL FOR WELDING, PERFORMING 3/16 INCH FILLET WELDS ACCORDING TO ITEM 513 USING APPROVED ELECTRODES, PROCEDURES AND WELDERS. WELD EACH SECONDARY MEMBER ACCORDING TO PLAN DETAILS. MAGNETIC PARTICLE INSPECT ALL FILLET WELDS ACCORDING TO C&MS 513.25B. THE ENGINEER MAY OBTAIN TECHNICAL ASSISTANCE FROM THE OFFICE OF MATERIALS MANAGEMENT. THE DEPARTMENT WILL INCLUDE ALL MATERIALS; TOOLS; LABOR; EQUIPMENT; AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK FOR PAYMENT WITH ITEM 513 - STRUCTURAL STEEL MISC., REPAIR OF DAMAGED MAIN OR SECONDARY MEMBERS: FILLET WELDING. FOOT.

ITEM 513 - STRUCTURAL STEEL MISC., REPAIR OF DAMAGED CROSSFRAME MEMBERS:

AFTER DAMAGED AREAS HAVE BEEN INSPECTED ACCORDING TO ITEM 849 DAMAGE ASSESSMENT. PREPARE THE DAMAGED MATERIAL FOR WELDING, PERFORMING 3/16 INCH FILLET WELDS ACCORDING TO ITEM 513 USING APPROVED ELECTRODES, PROCEDURES AND WELDERS. WELD EACH COSSFRAME MEMBER ACCORDING TO PLAN DETAILS. MAGNETIC PARTICLE INSPECT ALL FILLET WELDS ACCORDING TO C&MS 513.25B. THE ENGINEER MAY OBTAIN TECHNICAL ASSISTANCE FROM THE OFFICE OF MATERIALS MANAGEMENT. THE DEPARTMENT WILL INCLUDE ALL MATERIALS; TOOLS; LABOR; EQUIPMENT; AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK FOR PAYMENT WITH ITEM 513 - STRUCTURAL STEEL MISC., REPAIR OF DAMAGED CROSSFRAME MEMBERS. EACH.

ITEM 513 - STRUCTURAL STEEL MISC., REPAIR OF DAMAGED MEMBERS, COPE HOLES:

AN ESTIMATED QUANTITY FOR EACH LOCATION IS PROVIDED IN TABLE 3, FOR COPING MAIN MEMBERS AS DETERMINED BY FIELD INSPECTION ACCORDING TO ITEM 849, DAMAGE ASSESSMENT OR AS DIRECTED BY THE ENGINEER. PROVIDE A, 2 INCH DIAMETER X 4 INCH LONG COPE ACCORDING TO PLAN DETAILS OR AS DIRECTED BY THE ENGINEER. GRIND THE HOLES SMOOTH ACCORDING TO C&MS 513.19. THE DEPARTMENT WILL INCLUDE ALL MATERIALS, TOOLS, LABOR, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK FOR PAYMENT WITH ITEM 513 -STRUCTURAL STEEL MISC., REPAIR OF DAMAGED MEMBERS, COPE HOLES. EACH.

SFN	
7302045	
DESIGN AGENCY	
	
DESIGNER	CHECKER
SS	DEB
REVIEWER	
SRB 12-14-22	
PROJECT ID	
117629	
SUBSET	TOTAL
2	7
SHEET	TOTAL
15	20

ITEM 514 - FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN (THREE COAT):

1.0 DESCRIPTION

STRUCTURAL STEEL PREVIOUSLY COATED WITH A NEWER EXISTING OZEU OR IZEU PAINT SYSTEM OR UNPAINTED WEATHERING STEEL TO CORRECT DAMAGE BY COLLISION OR CORROSION. THIS WORK CONSIST OF PERFORMING SURFACE PREPARATION AND APPLYING A THREE-COAT PAINT SYSTEM TO THE PREPARED STEEL AND FEATHERED REMOVAL AREAS OF EXISTING OZEU OR IZEU PAINT SYSTEMS OR UNPAINTED WEATHERING STEEL.

2.0 GENERAL

CMS 514.05 THROUGH 514.10 AND 514.13.D APPLY UNLESS MODIFIED BY THESE NOTES.

3.0 WASHING EXISTING OZEU OR IZEU PAINTED SURFACES OR UNPAINTED WEATHERING STEEL

COATED WITH LOW PRESSURE WATER CLEANING TO REMOVE ALL DIRT, DEBRIS, ANIMAL EXCREMENT, SALT CONTAMINANTS AND OTHER ACCUMULATED FOREIGN MATERIAL IN ACCORDANCE WITH SSPC-SP12 (LP WC), LOW PRESSURE WATER CLEANING. THE PRESSURE WASHER SHALL BE CAPABLE OF ACHIEVING AT LEAST 2000 POUNDS PER SQUARE INCH AT THE NOZZLE. WHEN USING THE POWER WASHING EQUIPMENT, THE NOZZLE SHALL BE MAINTAINED NO MORE THAN 10 INCHES FROM THE SURFACE. SUPPLY AND USE POTABLE WATER. PROVIDE TO THE ENGINEER A LETTER OF WRITTEN ACCEPTANCE FOR ANY BIODEGRADABLE DETERGENTS OR CLEANERS USED IN CONJUNCTION WITH THIS METHOD.

COLLECT AND CONTAIN WATER AND DEBRIS REMOVED DURING WASHING OPERATIONS ABOVE WATER FEATURES IN CONFORMANCE WITH C&MS 514.08 AND C&MS 514.13.D FOR ANY DEBRIS. CREATE SETTLEMENT COLLECTION BASINS AND STRAIN ALL WASH WATER ABOVE LAND FEATURES AS NECESSARY TO PRODUCE VISIBLY CLEAR WATER AND COMPLY WITH C&MS 514.08 AND C&MS 514.13.D FOR ANY DEBRIS.

4.0 SURFACE PREPARATION

AFTER THE PRESSURE WASHED SURFACE HAS DRIED, REMOVE EXISTING PAINT COATING TO CONTRACT LIMITS OR AS DIRECTED BY THE ENGINEER ACCORDING TO: SSPC-SP 11, POWER TOOL CLEANING TO BARE METAL, AS SHOWN ON THE PICTORIAL SURFACE PREPARATION STANDARDS FOR PAINTING STEEL SURFACES SHOWN IN SSPC-VIS 3; SSPC SP6, COMMERCIAL BLAST CLEANING, AS SHOWN ON THE PICTORIAL SURFACE PREPARATION STANDARDS FOR PAINTING STEEL SURFACES SHOWN IN SSPC-VIS 1; OR SSPC SP12 UHP WJ-4, ULTRAHIGH-PRESSURE WATER JETTING, AS SHOWN ON THE PICTORIAL SURFACE PREPARATION STANDARDS FOR PAINTING STEEL SURFACES SHOWN IN SSPC-VIS 4. SUPPLY BLAST WATER CONTAINING A COMMERCIALY AVAILABLE RUST INHIBITOR AT A DOSAGE THAT PREVENTS FLASH RUSTING FOR 12 HOURS AND DOCUMENTED AS ACCEPTABLE TO THE COATING'S MANUFACTURER. THE ENGINEER WILL USE THE SSPC-VIS 1, SSPC-VIS 3 OR SSPC-VIS 4 TO DETERMINE THE ACCEPTANCE OF THE SURFACE PREPARATION. FEATHER THE EXISTING PAINT TO EXPOSE A MINIMUM OF 1/2 INCH OF EACH COAT. CONTAIN AND DISPOSE OF WASTE GENERATED BY THE CLEANING ACCORDING TO C&MS 514.13.D.

ROUND ALL EXPOSED CORNERS OF MAIN MATERIAL TO BE PAINTED AS NECESSARY TO ACHIEVE A 1/4 INCH RADIUS OR EQUIVALENT FLAT SURFACE AT A 45 DEGREE ANGLE.

ITEM 514 - FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN (THREE COAT) CONTINUED:

5.0 FIELD PAINTING

APPLY THE PRIME, INTERMEDIATE, AND FINISH COATS OF THE THREE-COAT PAINT SYSTEM SPECIFIED IN C&MS 708.02, ACCORDING TO C&MS 514.15, 514.16, 514.17, 514.19 AND 514.20 TO CONTRACT LIMITS OR AS DIRECTED BY THE ENGINEER. TINT THE FINISH COAT TO APPROXIMATELY THE SAME COLOR AS THE EXISTING FINISH COLOR, UNPAINTED WEATHERING STEEL OR AS DESIGNATED IN THE CONTRACT. MATCH THE COLOR TO THE ENGINEERS SATISFACTION. THE ENGINEER WILL DETERMINE THE PRIME AND INTERMEDIATE COAT THICKNESS USING A TYPE 2 MAGNETIC GAGE AT SPOT LOCATIONS. THE PRIME, INTERMEDIATE AND FINISH COAT OF PAINT SHALL MEET THE MINIMUM DRY FILM THICKNESS REQUIREMENTS OF C&MS 514.20. APPLY PAINT AS FOLLOWS:

- A. APPLY THE PRIME COAT ONLY TO THE PREPARED SURFACE OF THE BARE STEEL AND THE EXISTING PRIME COAT EXPOSED BY FEATHERING. DO NOT APPLY THE PRIME COAT TO THE ADJACENT INTERMEDIATE COAT.
- B. APPLY CAULK AFTER PRIMING
- C. APPLY THE INTERMEDIATE COAT TO THE NEW PRIME COAT AND TO THE EXISTING INTERMEDIATE COATS THAT ARE EXPOSED BY FEATHERING.
- D. APPLY THE FINISH COAT TO THE NEW INTERMEDIATE COAT AND TO THE EXISTING FINISH COATS THAT ARE EXPOSED BY FEATHERING.

AT THE PERIMETER OF THE REPAIR AREA, APPLY THE PRIME, INTERMEDIATE AND FINISH COATS WITH A BRUSH. IN LIEU OF BRUSHING THE CONTRACTOR MAY DOUBLE MASK AREAS NOT TO BE COATED AND SPRAY TO FEATHERED REMOVAL LINES.

BLEND REPAIR AREAS WITH THE ADJACENT COATING TO PROVIDE A FINISHED SURFACE IN THE PATCHED AREAS THAT IS SMOOTH AND HAS AN EVEN PROFILE WITH THE ADJACENT SURFACE.

THE CONTRACTOR SHALL NOT PERFORM FIELD PAINTING UNTIL ALL WELDING, AND HEAT STRAIGHTENING ACTIVITIES HAVE BEEN COMPLETED.

6.0 MEASUREMENT

PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN BY THE NUMBER OF SQUARE FEET OF STRUCTURAL STEEL PAINTED. THE DEPARTMENT WILL DETERMINE THE SURFACE AREA BY TAKING EXACT FIELD MEASUREMENTS OF ALL PAINTED SURFACES AND CALCULATIONS.

7.0 BASIS OF PAYMENT

THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES AT THE CONTRACT PRICES AS FOLLOWS:

THE DEPARTMENT MAY CONSIDER PAINT AS ELIGIBLE FOR PAYMENT FOR MATERIAL ON-HAND AS SPECIFIED IN 109.10, HOWEVER, ONLY PAINT THAT THE CONTRACTOR CAN PROVE TO THE ENGINEER WILL BE USED DURING THE CONSTRUCTION SEASON IS ELIGIBLE FOR PAYMENT. THE CONTRACTOR SHALL PROVIDE THE ENGINEER CALCULATIONS INDICATING THE TOTAL SQUARE FEET OF STEEL TO BE PAINTED DURING THE CONSTRUCTION SEASON. THE CONTRACTOR SHALL ALSO PROVIDE CALCULATIONS SHOWING THE TOTAL NUMBER OF GALLONS REQUIRED.

IF THE CONTRACTOR CAUSES DAMAGE OR INJURY TO PUBLIC OR PRIVATE PROPERTY, THE DEPARTMENT WILL NOT PAY FOR RESTORING THE PROPERTY TO ITS ORIGINAL CONDITION.

THE DEPARTMENT WILL NOT PAY FOR REPAIRING ADJACENT COATINGS DAMAGED DURING THE WASHING, POWER TOOL CLEANING OR BLAST CLEANING OPERATION.

THE DEPARTMENT WILL NOT PAY FOR REMOVING AND REPLACING AN AREA OF COATING BECAUSE A SPOT OR MAXIMUM AVERAGE THICKNESS EXCEEDS THE MAXIMUM SPOT THICKNESS.

THE DEPARTMENT WILL NOT PAY FOR ADDITIONAL TESTING REQUIRED BY ANY HAULER, TREATMENT FACILITY, DISPOSAL FACILITY OR LANDFILL. THE DEPARTMENT WILL NOT PAY FOR ACCESSING, INSPECTING, AND REPAIRING AREAS THAT ARE NOT FOUND TO BE IN CONFORMANCE WITH THE SPECIFICATIONS AND PERTINENT CONTRACT DOCUMENTS.

ALL OTHER REQUIREMENTS OF THIS FIELD PAINTING SPECIFICATION ARE CONSIDERED INCIDENTAL TO THE WORK.

ITEM	UNIT	DESCRIPTION
514	SQUARE FEET	FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN (THREE COAT)

ITEM 516, JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN:

THIS WORK CONSISTS OF RAISING, SUPPORTING, OR RE-POSITIONING EXISTING STRUCTURES TO THE PERFORM REPAIRS DEFINED IN THE PLANS.

SUBMIT CONSTRUCTION PLANS IN ACCORDANCE WITH C&MS 501.05.

IF, DURING THE JACKING OPERATIONS, CRACKING OF THE CONCRETE SUPERSTRUCTURE, SEPARATION OF THE CONCRETE DECK FROM THE STEEL STRINGERS, OR OTHER DAMAGE TO THE STRUCTURE IS VISUALLY OBSERVED, IMMEDIATELY CEASE THE JACKING OPERATION AND INSTALL SUPPORTS TO THE SATISFACTION OF THE ENGINEER. ANALYZE THE DAMAGE AND SUBMIT A METHOD OF CORRECTION TO THE ENGINEER FOR APPROVAL. EPOXY INJECT ALL BEAMS THAT SEPARATE FROM THE DECK FOR THE DISTANCE OF THE SEPARATION IN ACCORDANCE WITH C&MS 512.07. THE DEPARTMENT WILL NOT PAY FOR THE COST OF THIS EPOXY INJECTION OR OTHER REQUIRED REPAIRS NECESSITATED BY THE JACKING OPERATION. THE BRIDGE BEARINGS SHALL BE FULLY SEATED AT ALL CONTACT AREAS. IF FULL SEATING IS NOT ATTAINED, SUBMIT A REPAIR PLAN TO THE ENGINEER. THE DEPARTMENT WILL NOT PAY FOR THE REPAIR COSTS TO ENSURE FULL SEATING ON BEARINGS. THE DEPARTMENT WILL MEASURE THIS WORK ON A LUMP SUM BASIS. THE DEPARTMENT WILL PAY FOR THE ACCEPTED QUANTITIES AT THE CONTRACT PRICE FOR ITEM 516, JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN.

ITEM 516 - REFURBISHING BEARING DEVICES, AS PER PLAN:

THIS ITEM SHALL INCLUDE ALL WORK NECESSARY TO PROPERLY ALIGN BRIDGE BEARINGS AS WELL AS THEIR CLEANING AND PAINTING. INCLUDED SHALL BE THE DISASSEMBLY OF THE BEARINGS, HAND TOOL CLEANING (GRINDING IF NECESSARY), PAINTING ACCORDING TO ITEM 514, REPLACEMENT OF ANY DAMAGED SHEET LEAD WITH PREFORMED BEARING PADS (C&MS 711.21), INSTALLATION OF ANY NECESSARY STEEL SHIMS OF THE SAME SIZE AS THE BEARINGS TO PROVIDE A SNUG FIT, REALIGNMENT OF THE UPPER BEARING PLATE BY REMOVING EXISTING WELDS AND REWELDING SO THAT THE BEARINGS ARE VERTICALLY ALIGNED AT 60°F, REMOVING EXISTING WELDS AND REPLACEMENT OF THE DAMAGED BEARING FACE PLATE, LUBRICATING SLIDING SURFACES, AND REASSEMBLY OF THE BEARINGS. ASSURE THE BEARING IS SHIMMED ADEQUATELY AT NO ADDITIONAL COST TO THE STATE, THE CONTRACTOR MAY INSTALL NEW BEARINGS OF THE SAME TYPE AS THE EXISTING IN PLACE OF REFURBISHING THE BEARINGS. ALL WORK SHALL BE TO THE SATISFACTION OF THE ENGINEER. PAYMENT FOR ALL OF THE ABOVE DESCRIBED LABOR AND MATERIALS WILL BE MADE AT THE CONTRACT PRICE BID FOR ITEM 516 - REFURBISH BEARING DEVICES, AS PER PLAN.

SFN	7302045
DESIGN AGENCY	
	
DESIGNER	CHECKER
SS	DEB
REVIEWER	
SRB	12-14-22
PROJECT ID	
117629	
SUBSET	TOTAL
3	7
SHEET	TOTAL
16	20

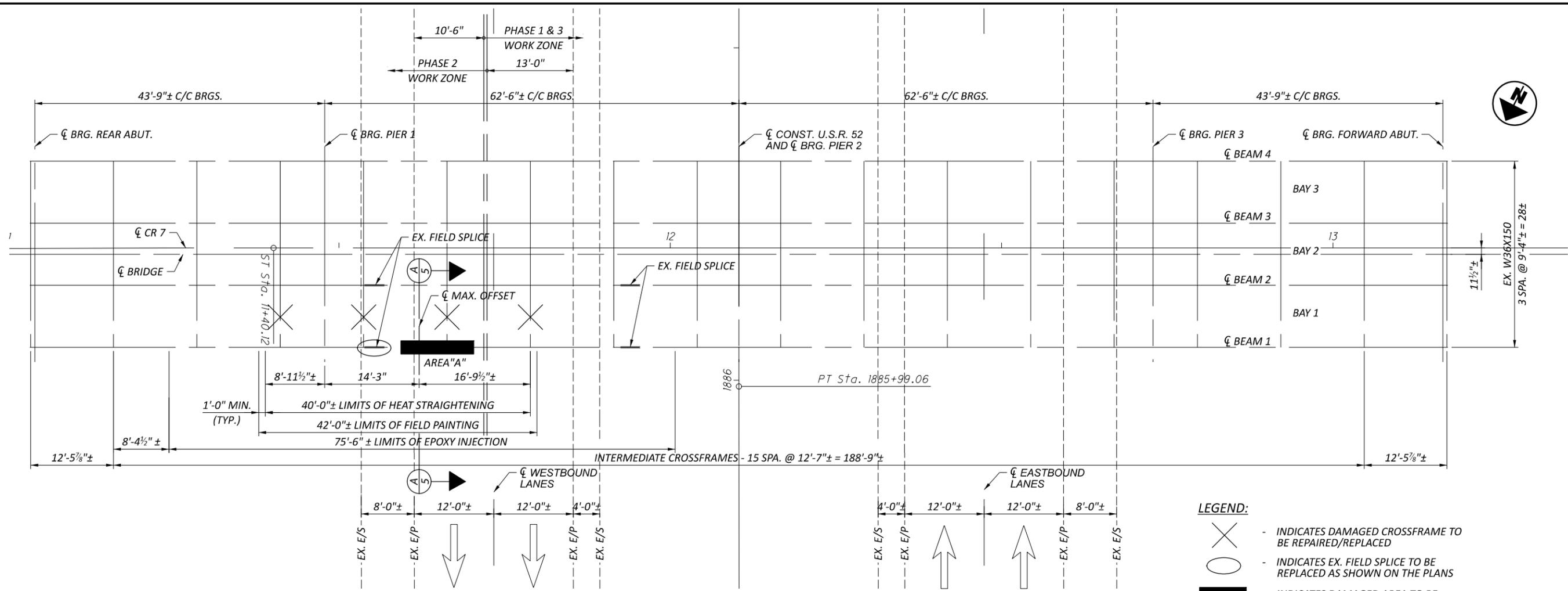


TABLE #2 DAMAGED SECONDARY MEMBER TO BE REPAIRED

CROSSFRAME BAY	PIER/ABUT.	NO.	1D	2D	3S
1	R.A.	4	1	1	1
1	PIER 1	1	1	1	1
1	PIER 1	2	1	1	1*
1	PIER 1	3	1	1	1

* CROSSFRAME MEMBER TO BE REPLACED, SEE NOTE 5

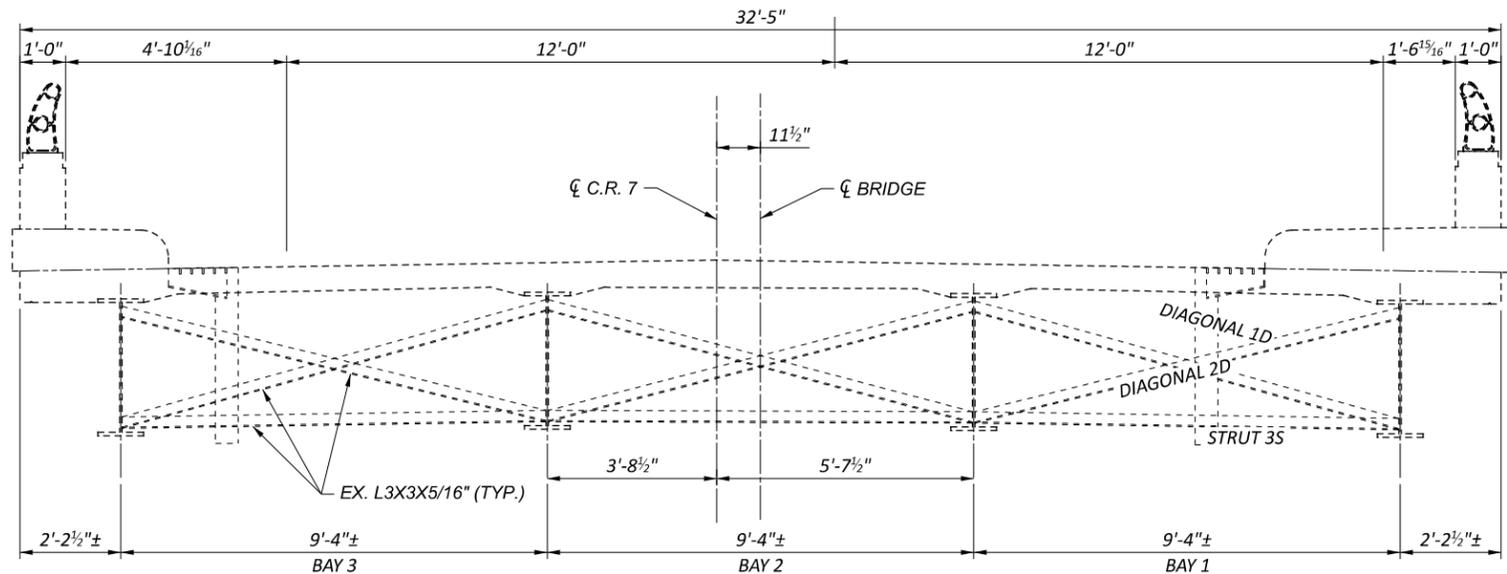
EX. FRAMING PLAN

LEGEND:

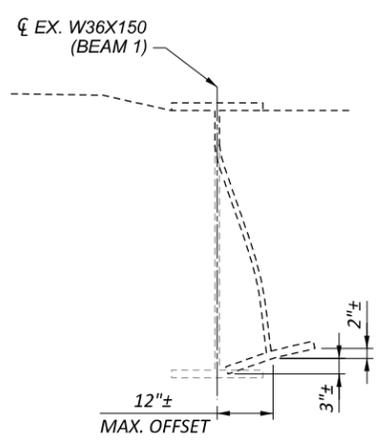
- INDICATES DAMAGED CROSSFRAME TO BE REPAIRED/REPLACED
- INDICATES EX. FIELD SPLICE TO BE REPLACED AS SHOWN ON THE PLANS
- INDICATES DAMAGED AREA TO BE REPLACED AS SHOWN ON THE PLANS AREA "A"

NOTES:

1. EX. STEEL TYPE IS A-373
 - YIELD STRENGTH IS 33 KSI
 - ALLOWABLE JACKING STRESS IS 16 KSI
2. EX. PAINT SYSTEM IS OZEU APPLIED IN 2000.
3. BEAM 1 HAS BEEN SHIFTED OFF THE ROCKER BY APPROXIMATELY 2 1/4" AT PIER 1. BEAM 1 TO BE HEAT STRAIGHTENED BACK OVER THE CENTER OF THE ROCKER AND THE ROCKER'S FACE PLATE TO BE REPLACED IN ACCORDANCE WITH STD BRG. DWG. RB-1-55.
4. THE COST ASSOCIATED WITH REWELDING CROSS FRAME MEMBERS (TOTAL OF 11) SHALL BE INCLUDED WITH ITEM 513, STRUCTURAL STEEL, MISC.: REPAIR OF DAMAGED CROSSFRAME MEMBERS FOR PAYMENT.
5. REPLACE MISSING CROSSFRAME MEMBERS AS SHOWN ON THE PLANS. THE COST ASSOCIATED WITH REPLACING CROSS FRAME MEMBER SHALL BE INCLUDED WITH ITEM 513, STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN FOR PAYMENT.



EX. TRANSVERSE SECTION

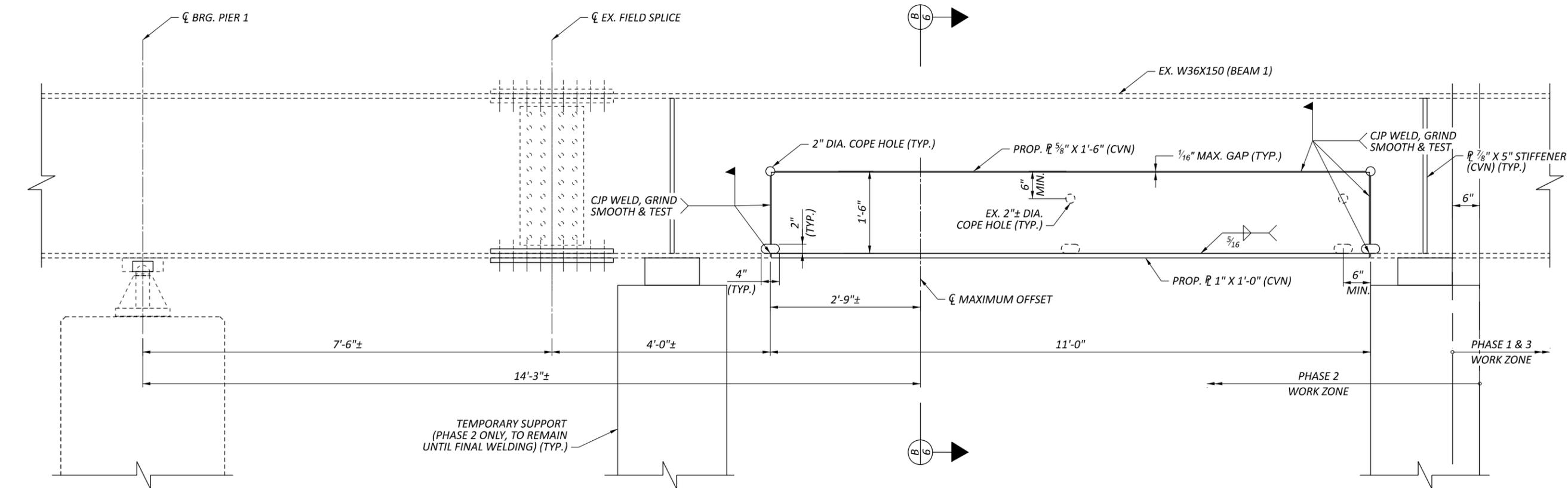


SECTION A-A

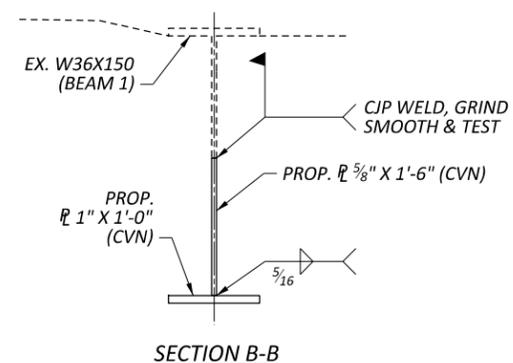
DIMENSIONS SHOWN ARE APPROXIMATE AND TO BE FIELD VERIFIED BY THE CONTRACTOR

SUPERSTRUCTURE DETAILS
 BRIDGE NO.: SCI-00007-00.420
 COUNTY ROUTE 7 OVER US ROUTE 52

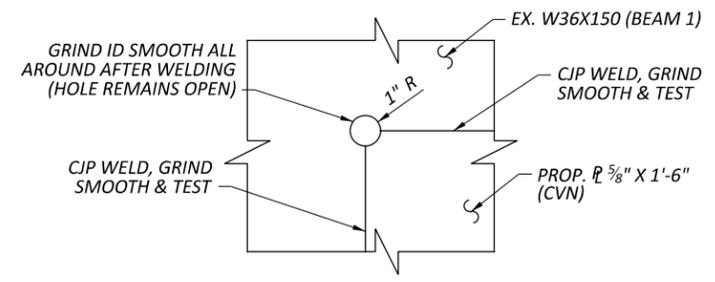
SFN	7302045
DESIGN AGENCY	[BI]
DESIGNER	SS
CHECKER	DEB
REVIEWER	
SRB	12-14-22
PROJECT ID	117629
SUBSET	5
TOTAL	7
SHEET	18
TOTAL	20



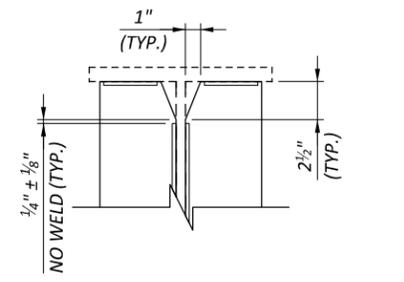
PARTIAL BEAM REPLACEMENT DETAIL



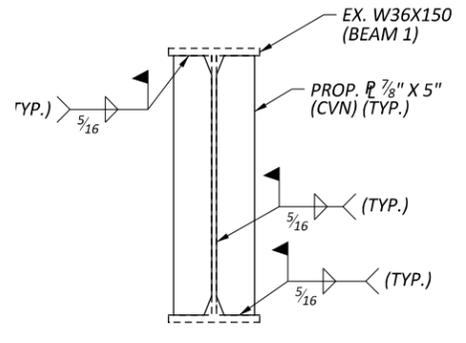
SECTION B-B



FIELD REPAIR TOP COPE DETAIL



STIFFENER CLIP AND WELD TERMINATION



STIFFENER PLATE DETAILS

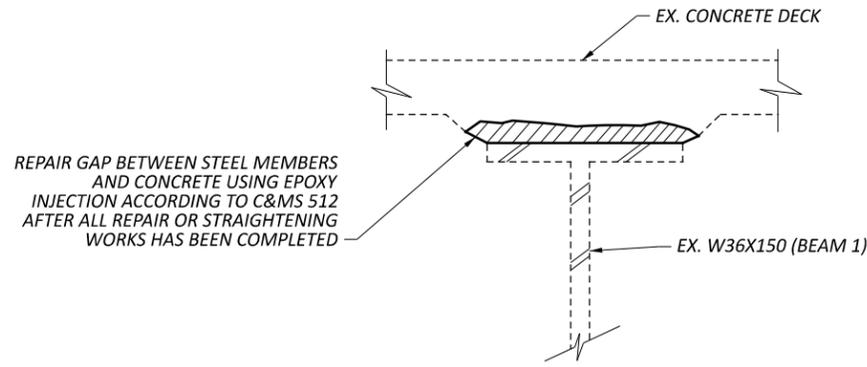
TABLE #3 513 REPAIRS									
DAMAGED AREA No.	MEMBER LINE No.	PIER OR ABUTMENT	DIM. C	REPAIR DETAIL TYPE	DRILLING HOLES (EACH)	COPE HOLES (EACH)	STEEL MEMBER LEVEL UF (POUNDS)	CP WELD (FEET)	FILLET WELD (FEET)
AREA A	BEAM 1	PIER 1				4	871	16	22

NOTES :

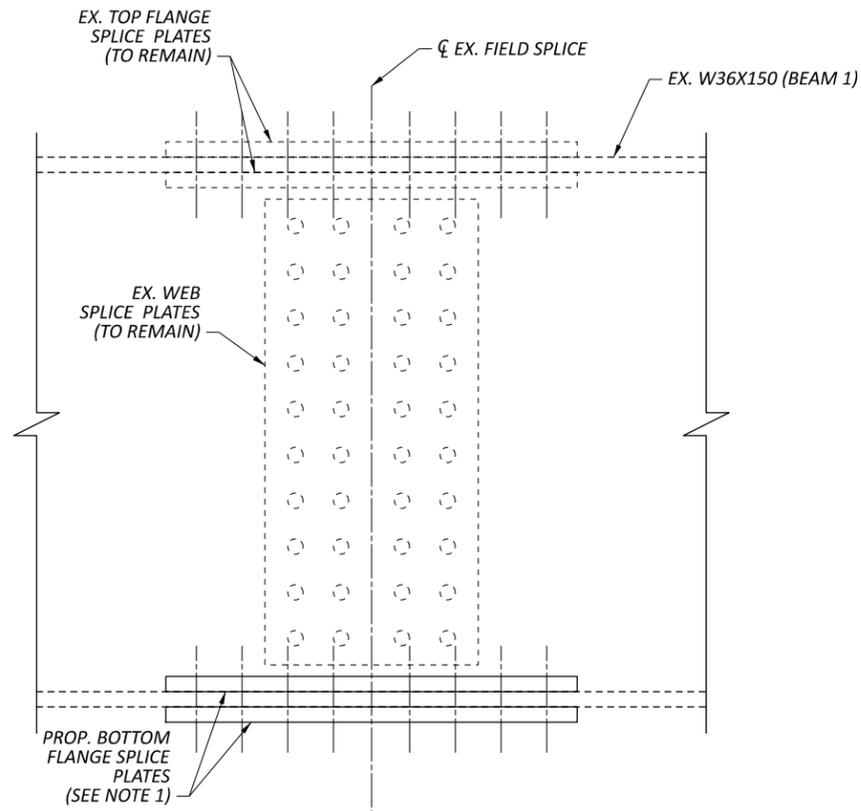
1. REMOVE SECONDARY MEMBERS AS NECESSARY AND AS SHOWN ON THE PLANS. CAREFULLY GRIND EXISTING WELDS FLUSH. DO NOT DAMAGE WEB OR FLANGE.
2. PERFORM ALL HEAT STRAIGHTENING ACTIVITIES.
3. INSTALL TEMPORARY SUPPORT AND PROVIDE TEMPORARY BRACING AS NEEDED. THE TEMPORARY SUPPORT SHALL REMAIN UNTIL ALL WELDING AND SPLICE PLATE REPLACEMENT ACTIVITIES HAVE BEEN COMPLETED.
4. INSTALL STIFFENER PLATES.
5. MARK REMOVAL AREA, DRILL CORNER HOLES, AND SAW OR FLAME CUT TO REMOVE DAMAGED BEAM USING A MECHANICAL GUIDE.
6. PREPARE EXISTING BEAM FOR COMPLETE PENETRATION WELDS BY EDGE BEVELING AND CREATING UNDERSIZED WEB COPE.
7. CUT NEW PLATES, AND BEVEL EDGES FOR COMPLETE PENETRATION AND FILLET WELDS.
8. CHECK FIT OF NEW PLATES, NO GAP EXCEEDING 1/16. TACK WELD PLATES INTO PLACE.
9. PERFORM FILLET WELDING ACCORDING TO C&MS 513.21 AND NDT TESTING ACCORDING TO C&MS 513.25B.
10. PERFORM COMPLETE PENETRATION WELDING ACCORDING TO C&MS 513.21 BY ATTACHING RUN OFF TABS, PERFORM WELDS, REMOVING TABS AND GRINDING ALL WELD SURFACES SMOOTH ACCORDING TO ANSI B46.1 OF 250 MIL. COMPLETE NDT TESTING ACCORDING TO C&MS 513.25A.
11. REPAIR/REPLACE SECONDARY MEMBERS AS SHOWN ON THE PLANS. MAKE CONNECTIONS TO MATCH EXISTING.
12. REPLACE PORTION OF EX. FIELD SPLICE AS SHOWN ON THE PLANS.
13. REPLACE EX. ROCKER FACE PLATE AS SHOWN ON THE PLANS.
14. WHERE A SHAPE OR PLATE IS DESIGNATED (CVN), FURNISH MATERIAL THAT MEET THE MINIMUM TOUGHNESS REQUIREMENTS AS SPECIFIED IN 711.01.
15. THE CONTRACTOR SHALL SEQUENCE WORK AS NEEDED FOLLOWING THE MAINTENANCE OF TRAFFIC PHASE CONSTRUCTIONS SHOWN ON THE PLANS.
16. THE CONTRACTOR SHALL NOT PERFORM FIELD PAINTING UNTIL ALL WELDING AND HEAT STRAIGHTENING ACTIVITIES HAVE BEEN COMPLETED.

SFN 7302045
 DESIGN AGENCY

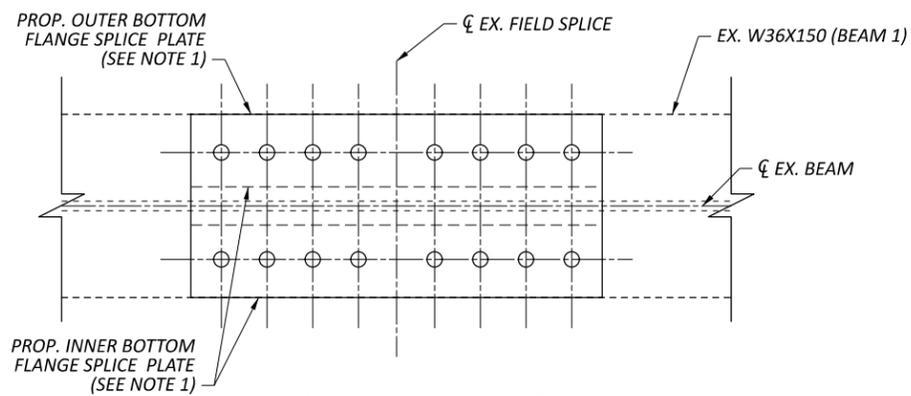
 DESIGNER SS CHECKER DEB
 REVIEWER
 SRB 12-14-22
 PROJECT ID 117629
 SUBSET 6 TOTAL 7
 SHEET 19 TOTAL 20



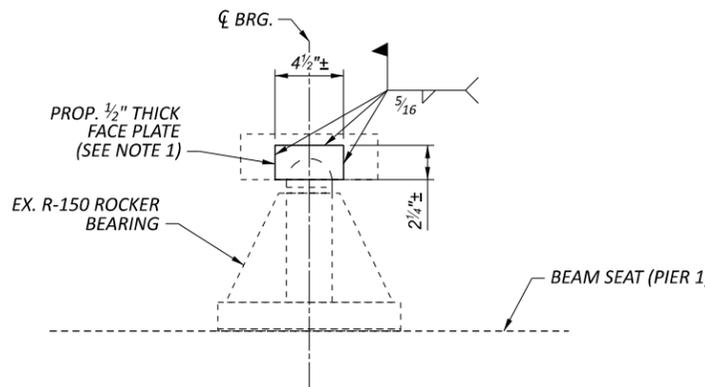
EPOXY INJECTION DETAILS



WEB SPLICE DETAILS



FLANGE FIELD SPLICE DETAILS



BEARING DETAILS (PIER 1)

TABLE 4 DAMAGED MEMBERS TO BE EPOXY INJECTED				
DAMAGED AREA	MEMBER LINE	PIER OR ABUTMENT	C	LINEAR FEET
AREA A	BEAM 1	PIER 1		40.00 FT

TABLE 5 DAMAGED MEMBER, BEARINGS TO BE RESET				
DAMAGED AREA	MEMBER LINE	PIER OR ABUTMENT	BEARING TYPE	QUANTITY
AREA A	BEAM 1	PIER 1	R-150 ROCKER	1

SEE PARTIAL FRAMING PLAN FOR DETAILS.

NOTES:

- ALL MATERIALS FOR THE PROP. STIFFENER AND FIELD SPLICE PLATES SHALL BE "CVN". FURNISH MATERIALS THAT MEETS THE MINIMUM NOTCH TOUGHNESS REQUIREMENTS AS SPECIFIED IN 711.01.
 HIGH STRENGTH BOLTS SHALL BE A325 TYPE 1, BOLT SIZE, DIAMETER AND SPACING SHALL MATCH EXISTING.
 FIELD SPLICE PLATE DIMENSIONS SHALL MATCH EXISTING.
 ALL STRUCTURAL STEEL MATERIALS SHALL BE ASTM A709 GRADE 50.
 THE COST FOR PROVIDING AND INSTALLING PROP. STIFFENER PLATES, AND FIELD SPLICE PLATES, INCLUDING ALL LABOR EQUIPMENT, MATERIALS AND INCIDENTALS NECESSARY TO COMPLETE ABOVE WORKS SHALL BE INCLUDED FOR PAYMENT WITH ITEM 513, STRUCTURAL STEEL MEMBERS LEVEL UF, AS PER PLAN UNLESS SEPARATELY ITEMIZED IN THE PLANS.
 THE COST ASSOCIATED WITH BEARING FACE PLATE REPLACEMENT SHALL BE INCLUDED WITH ITEM 516 - REFURBISH BEARING DEVICES, AS PER PLAN FOR PAYMENT.
 THE COST ASSOCIATED WITH TEMPORARY SUPPORT SHALL BE INCLUDED WITH ITEM 516 - JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN FOR PAYMENT.
 THE COST FOR REMOVING EXISTING MEMBERS SHALL BE INCLUDED WITH ITEM 202 FOR PAYMENT.
- REPAIR GAP BETWEEN STEEL MEMBERS AND CONCRETE USING EPOXY INJECTION. THE LIMITS FOR THE EPOXY INJECTION SHALL BE AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER.
 THIS WORK SHALL BE PERFORMED AT NIGHT TIME. THE CONTRACTOR SHALL CLOSE C.R 7 BRIDGE TO TRAFFIC, APPLY THE EPOXY INJECTION AND ALLOW THE EPOXY INJECTION TO CURE OVERNIGHT. APPLY AND CURE THE EPOXY INJECTION ACCORDING TO THE MANUFACTURER'S PUBLISHED RECOMMENDATION. SEE MAINTENANCE OF TRAFFIC PLANS FOR BRIDGE CLOSURE NOTES AND REQUIREMENTS.
 THE COST SHALL BE INCLUDED WITH ITEM 512, CONCRETE REPAIR BY EPOXY INJECTION FOR PAYMENT.

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