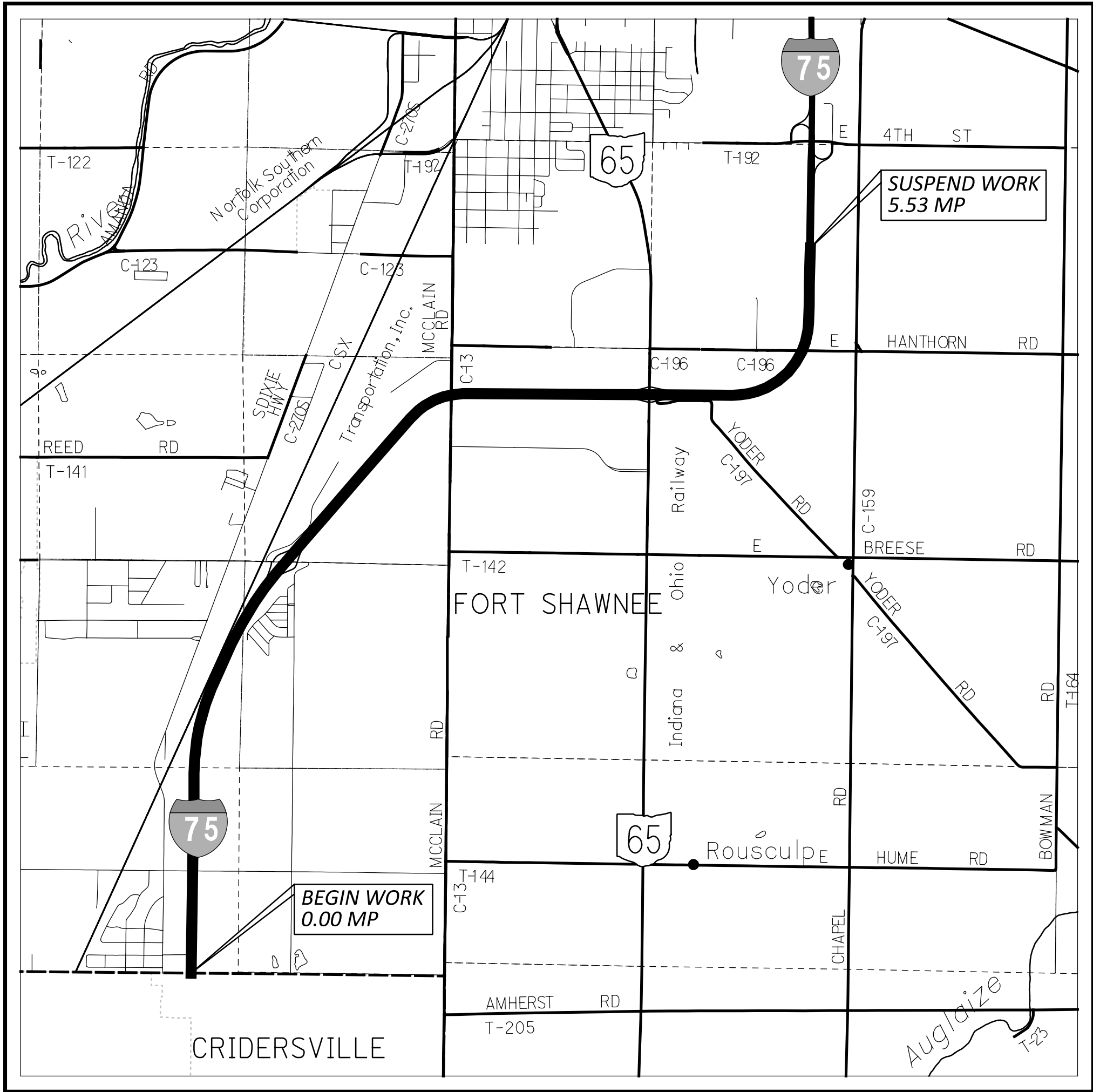


Allen & Hancock Counties

SHEET	TOTAL
1	2

ALL-75 0.00 to 5.53



LOCATION MAP

LATITUDE: N 40°41'38" LONGITUDE: W 84°6'55"

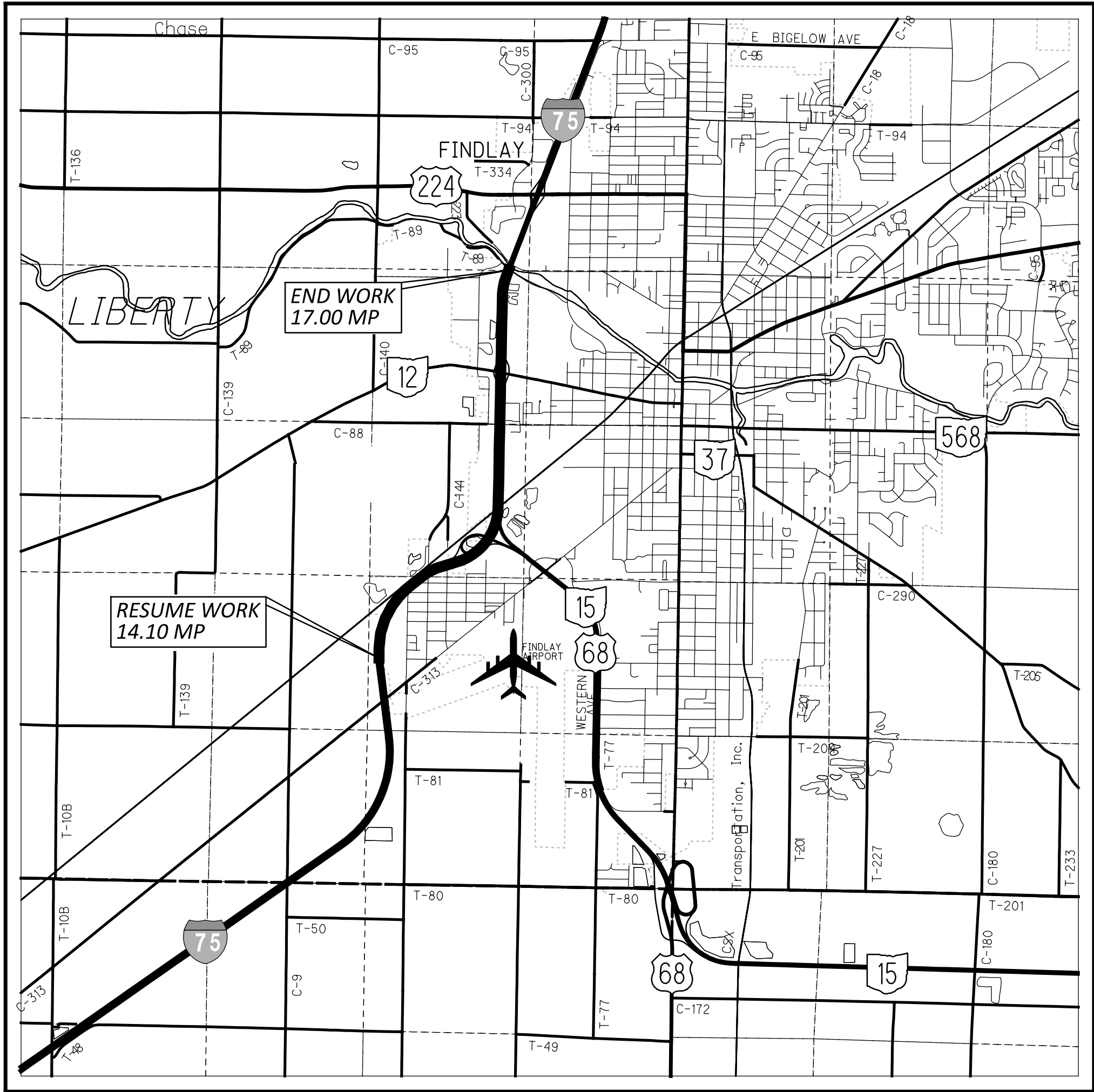


PORTION TO BE IMPROVED	
INTERSTATE HIGHWAY	
FEDERAL ROUTES	
STATE ROUTES	
COUNTY & TOWNSHIP ROADS	
OTHER ROADS	

DESIGN DESIGNATION

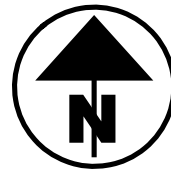
CURRENT ADT (2023)	41,387
DESIGN YEAR ADT (2046)	47,000
DESIGN HOURLY VOLUME (2046)	5300
DIRECTIONAL DISTRIBUTION	55.0
TRUCKS (24 HOUR B&C)	34.0
DESIGN SPEED	70 mph
LEGAL SPEED	70 mph
DESIGN FUNCTIONAL CLASSIFICATION:	
INTERSTATE	
NHS PROJECT	YES

HAN-75 14.10 to 17.00



LOCATION MAP

LATITUDE: N 41°1'50" LONGITUDE: W 83°40'22"



PORTION TO BE IMPROVED	
INTERSTATE HIGHWAY	
FEDERAL ROUTES	
STATE ROUTES	
COUNTY & TOWNSHIP ROADS	
OTHER ROADS	

DESIGN DESIGNATION

CURRENT ADT (2023)	56,329
DESIGN YEAR ADT (2046)	66,500
DESIGN HOURLY VOLUME (2046)	7400
DIRECTIONAL DISTRIBUTION	51.0
TRUCKS (24 HOUR B&C)	33.0
DESIGN SPEED	70 mph
LEGAL SPEED	70 mph
DESIGN FUNCTIONAL CLASSIFICATION:	
INTERSTATE	
NHS PROJECT	YES



UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER, OR ADJACENT TO, THE WORK AREA.

EXISTING PLANS

EXISTING PLANS ENTITLED ALL-75-0.21 PID 89029, AND HAN-75-14.39 PID 87005 MAY BE INSPECTED IN THE ODOT DISTRICT ONE OFFICE IN LIMA, OHIO.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

PIPE UNDERDRAINS

ANY PIPE UNDERDRAINS BROKEN OR DAMAGED AS A RESULT OF CONSTRUCTION OPERATIONS SHALL BE REPLACED BY THE CONTRACTOR AT NO COST TO THE STATE.

CONTRACTOR'S EQUIPMENT - OPERATION AND STORAGE

IN ADDITION TO THE REQUIREMENTS OF SECTION 614.03(A) OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS THE FOLLOWING SHALL APPLY. THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL. EXTREME CAUTION SHALL BE USED WHERE THE CONTRACTOR'S VEHICLES AND EQUIPMENT MERGE WITH THE TRAFFIC STREAM. THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE EQUIPPED WITH AT LEAST ONE AMBER FLASHING LIGHT. CONSTRUCTION EQUIPMENT MAY BE PARKED IN AREA ALONG THE HIGHWAY, THIRTY FEET (30') FROM THE EDGE OF THE TRAVELED HIGHWAY UNLESS BEHIND GUARDRAIL, WHEN VARIOUS OPERATIONS ARE SCHEDULED TO CONTINUE THE NEXT WORKDAY. EQUIPMENT PARKED BEHIND THE GUARDRAIL SHALL BE AT LEAST 6 FEET FROM THE FACE OF THE GUARDRAIL. NO EQUIPMENT SHALL BE PARKED BEHIND A GUARDRAIL ATTENUATOR. ON WEEKENDS OR AT OTHER TIMES OF SUSPENSION OF WORK, EQUIPMENT SHALL BE STORED AT A STORAGE AREA REMOVED FROM THE INTERSTATE RIGHT OF WAY. NO EQUIPMENT SHALL BE PARKED IN THE MEDIAN OF THE HIGHWAY EXCEPT WHEN TRAFFIC IS MAINTAINED ON THE OUTSIDE LANES. ADEQUATE BARRICADES AND LIGHT SHALL BE PLACED ON THE PAVEMENT SIDE OF THE EQUIPMENT TO IDENTIFY THE LIMITS OF THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT THE APPROVED CONTACTOR'S STORAGE AREA.

ITEM 421 - MICROSURFACING, SURFACE COURSE, AS PER PLAN & ITEM 421 - MICROSURFACING, LEVELING COURSE, AS PER PLAN

ALL REQUIREMENTS OF ITEM 421 APPLY , IN ADDITION, SUPPLY A BLEND OF A MINIMUM OF 50% IGNEOUS DIABASE TRAP ROCK AND A MAXIMUM OF 50% LIMESTONE AGGREGATE FROM APPROVED SOURCES FOR USE AS AGGREGATE IN ITEM 421. DO NOT USE OTHER AGGREGATES.

MICROSURFACING, SURFACE COURSE, AS PER PLAN AND MICROSURFACING, LEVELING COURSE, AS PER PLAN SHALL BE IN ACCORDANCE WITH ITEM 421 - MICROSURFACING WITH THE FOLLOWING ADDITIONS: TRUCK MOUNTED MACHINES AS PER 421.06 MAY BE USED FOR THE ENTIRE PROJECT.

ALL LABOR, EQUIPMENT AND MATERIAL REQUIRED TO PERFORM THE ABOVE WORK SHALL BE INCLUDED IN THE COST PER SQUARE YARD OF ITEM 421 - MICROSURFACING, SURFACE COURSE, AS PER PLAN AND ITEM 421 - MICROSURFACING, LEVELING COURSE, AS PER PLAN.

PERSONAL PROTECTIVE EQUIPMENT (PPE)

THE CONTRACTOR SHALL FOLLOW ALL REQUIREMENTS OF SECTIONS XXIV AND XXXIV OF THE OHIO DEPARTMENT OF TRANSPORTATION SAFETY AND HEALTH STANDARD OPERATING PROCEDURE 220-006(SP) EFFECTIVE: NOVEMBER 1, 2018 (EXCEPT AS AMENDED BELOW) AND ALL SUBSEQUENT UPDATES POSTED AT THE FOLLOWING WEBSITE:

<https://www.transportation.ohio.gov/about-us/policies-and-procedures/procedures/220-006-sp>

AMENDMENTS TO THE REQUIREMENTS OF THIS DOCUMENT ARE:

XXIV. HEAD PROTECTION (HARD HATS)
ALL PERSONS WITHIN THE RIGHT-OF-WAY OF ANY HIGHWAY OR ANY OTHER TYPE OF ROADWAY OR CONSTRUCTION SITE WHO ARE EXPOSED TO EITHER TRAFFIC (VEHICLES USING THE HIGHWAY FOR PURPOSES OF TRAVEL) OR CONSTRUCTION EQUIPMENT WITHIN THE WORK AREA, REGARDLESS OF JOB TYPE, SHALL WEAR APPROPRIATE HEAD PROTECTION. ALL HARD HATS MUST MEET OR EXCEED ANSI Z89.1-2009 TYPE 1, CLASS E-G REQUIREMENTS.

XXXIV. SAFETY APPAREL AND VEST (HIGH VISIBILITY)
ALL PERSONS WITHIN THE RIGHT-OF-WAY OF ANY HIGHWAY OR ANY OTHER TYPE OF ROADWAY OR CONSTRUCTION SITE WHO ARE EXPOSED TO EITHER TRAFFIC (VEHICLES USING THE HIGHWAY FOR PURPOSES OF TRAVEL) OR CONSTRUCTION EQUIPMENT WITHIN THE WORK AREA, REGARDLESS OF JOB TYPE, SHALL WEAR A HIGH-VISIBILTY SAFETY VEST THAT MEETS THE PERFORMANCE CLASS II OR CLASS III REQUIREMENTS OF THE ANSI/ISEA 107-2015 PUBLICATION ENTITLED "AMERICAN NATIONAL STANDARD FOR HIGH-VISIBILITY SAFETY APPAREL AND ACCESSORIES."

WORKERS MAY WEAR AN ANSI CLASS II OR ANSI CLASS III APPROVED RAIN SUIT, JACKET OR OTHER APPAREL WITHOUT A SAFETY VEST OVER IT.

ITEM 850 - GROOVING FOR RECESSED PAVEMENT MARKINGS, AS PER PLAN

ALL SPECIFICATIONS OF SUPPLEMENTAL SPECIFICATION 850 SHALL APPLY WITH THE ADDITION OF: ALL COSTS ASSOCIATED WITH STRIPING A GUIDELINE FOR THE GROOVING SHALL BE INCIDENTAL TO THIS ITEM. THE CONTRACTOR MAY USE ANY PAINT OF THEIR CHOOSING TO BE USED AS A GUIDELINE.

PAVEMENT PREPARATION

IN ADDITION TO THE REQUIREMENTS OF SPEC. 641.05, THE ITEM SHALL CONSIST OF REMOVING AND DISPOSING OF EXCESSIVE AMOUNTS OF LOOSE OR FOREIGN MATERIAL FROM THE SURFACE TO BE MARKED TO THE SATISFACTION OF THE ENGINEER. THIS SHALL INCLUDE THE AREA AROUND THE SURFACE TO BE MARKED. THE ENTIRE AREA SHALL BE CLEANED, NOT JUST THE LINES TO BE MARKED. ALL WORK SHALL BE INCIDENTAL TO 641.

DAMAGE CLAIMS

IN ADDITION TO 107.10 AND 107.12 OF THE C&MS BOOK, THE FOLLOWING SHALL APPLY.

COPIES OF ALL DAMAGE CLAIMS OR WRITTEN COMPLAINTS MADE AGAINST THE CONTRACTOR OR SUBCONTRACTOR RESULTING FROM FIELD PAINTING OF PAVEMENT MARKINGS SHALL BE PROVIDED TO THE ODOT DISTRICT CONSTRUCTION ENGINEER WITHIN SEVEN (7) DAYS OF RECEIPT. THE CONTRACTOR SHALL RESPOND TO EACH CLAIM OR COMPLAINT IN A TIMELY MANNER, BUT IN NO CASE SHALL RESPONSE TAKE MORE THAN FIFTEEN (15) DAYS. A COPY OF THE RESPONSE SHALL BE PROVIDED TO THE ODOT DISTRICT CONSTRUCTION ENGINEER AT THE TIME IT IS SENT TO THE CLAIMANT OR COMPLAINANT.

GENERAL NOTES

DESIGN AGENCY



DESIGNER

GLI

REVIEWER

MPB 06-23-25

PROJECT ID

120987

SHEET

3

TOTALY

23

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

WHEN CONSTRUCTION VEHICLES ARE ENTERING/EXITING THE ZONE DIRECTLY FROM/INTO AN OPEN LANE OF TRAFFIC. IF A LANE HAS BEEN CLOSED TO PROVIDE AN ACCELERATION/ DECELERATION LANE FOR THE VEHICLE, THE LEO WILL NOT BE REQUIRED.

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 120 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONTIN.)

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

PAVEMENT MARKING

PRIOR TO PLACEMENT OF ANY WORK ZONE PAVEMENT MARKINGS, THE CONTRACTOR SHALL COMPLETELY OBLITERATE, AS PER SPEC. 641.10, ALL EXISTING PAVEMENT MARKINGS THAT WOULD CREATE CONFUSION OR CONFLICT WITH THE WORK ZONE PAVEMENT MARKINGS. PAYMENT FOR THIS COMPLETE REMOVAL SHALL BE INCLUDED IN ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHEILDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN.

ITEM 614, REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 50 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

WORK ZONE MARKINGS AND SIGNS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

ITEM 614 - WORK ZONE MARKING SIGNS
NO EDGE LINE SIGNS 18 EACH

ITEM 614 - WORK ZONE LANE LINE, CLASS I,
6" 50.58 MI

ITEM 614 - WORK ZONE EDGE LINE, CLASS I,
6", 101.16 MI

ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I,
12", 53,745 FT

MAINTAINING TRAFFIC NEAR RUMBLE STRIPS

TRAFFIC IS NOT PERMITTED TO RUN OR CROSS OVER ANY RUMBLE STRIPS AT ANY TIME. RUMBLE STRIPS MUST BE FILLED WHEN THEY CONFLICT WITH THE MAINTENANCE OF TRAFFIC LANE CONFIGURATION. THIS INCLUDES LOCATIONS OF LANE SHIFTS ENTERING AND EXITING A WORK ZONE, AS WELL AS, CONFLICTING RUMBLE STRIPS AT THE ENTRANCE AND EXIT RAMPS. THE RUMBLE STRIPS SHALL BE FILLED TO PROVIDE A SMOOTH RIDE TO THE SATISFACTION OF THE PROJECT ENGINEER.

ONCE TRAFFIC IS RETURNED TO ITS FINAL LANE CONFIGURATION, RUMBLE STRIPS THAT WERE REMOVED IN ANY EXISTING PAVEMENT NOT BEING RESURFACED, SHALL BE RESTORED TO THE PRE-CONSTRUCTION CONDITION TO THE SATISFACTION OF THE ENGINEER.

PAYMENT FOR ALL MATERIALS, LABOR, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK DESCRIBED ABOVE SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES AND RESTRICTIONS	> = 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

DESIGN AGENCY



DESIGNER

GLI

REVIEWER

MPB 06-23-25

PROJECT ID

120987

SHEET

4

TOTAL

23

ITEM 614, REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 5 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

MAINTENANCE OF TRAFFIC DESIGN SPEED LIMIT

THE EXISTING FUNCTIONALITY OF I.R. 75 WILL BE REDUCED AT THE BEGINNING OF THE LANE CLOSURE TAPER. THIS IS CONSIDERED THE BEGINNING OF THE WORK ZONE. THE LANE CLOSURE SHALL BE SET UP PER CONSTRUCTION DRAWING MT-95.30. USE A SPEED LIMIT OF 65 MPH TO DETERMINE THE TAPER FOR THIS STANDARD DRAWING.

ONCE INSIDE THE CONSTRUCTION ZONE, THE MAINTENANCE OF TRAFFIC MUST BE DESIGNED FOR THE HIGHEST SPEED LIMIT THAT WILL BE PRESENT.

LANE CLOSURES

THE CONTRACTOR SHALL PROVIDE AN ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN IN ADVANCE WARNING OF ANY LANE CLOSURES. SEE NOTE ON THIS SHEET.

ITEM 850 GROOVING FOR RECESSED PAVEMENT MARKINGS, AS PER PLAN

ALL SPECIFICATIONS OF SUPPLEMENTAL SPECIFICATION 850 SHALL APPLY WITH THE ADDITION OF: ALL COSTS ASSOCIATED WITH STRIPING A GUIDELINE FOR THE GROOVING SHALL BE INCIDENTAL TO THIS ITEM. THE CONTRACTOR MAY USE ANY PAINT OF THEIR CHOOSING TO BE USED AS A GUIDELINE.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY WHEN NOT IN USE, FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 6 SIGN MONTH

ASSUMING 2 PCMS SIGNS FOR 3 MONTHS (LANE CLOSURES)

WORK ZONE INCREASED PENALTIES SIGN (R11-H5A)

R11-H5A-48 SIGNS SHALL BE FURNISHED, ERECTED, AND MAINTAINED IN GOOD CONDITION AND/OR REPLACED AS NECESSARY AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. SIGNS SHALL BE MOUNTED AT THE APPROPRIATE OFFSETS AND ELEVATIONS AS PRESCRIBED BY THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THEY SHALL BE MAINTAINED ON SUPPORTS MEETING CURRENT SAFETY CRITERIA.

THE SIGNS MAY BE ERECTED OR UNCOVERED NO MORE THAN FOUR HOURS BEFORE THE ACTUAL START OF WORK. THE SIGNS SHALL BE REMOVED OR COVERED NO LATER THAN FOUR HOURS FOLLOWING RESTORATION OF ALL LANES TO TRAFFIC WITH NO RESTRICTIONS, OR SOONER AS DIRECTED BY THE ENGINEER. TEMPORARY SIGN COVERING AND UNCOVERING DUE TO TEMPORARY LANE RESTORATIONS SHALL BE GUIDED BY THE FOUR-HOUR LIMITATIONS STATED ABOVE. SUCH LANE RESTORATIONS SHOULD BE EXPECTED TO REMAIN IN EFFECT FOR 30 OR MORE CONSECUTIVE CALENDAR DAYS, SUCH AS DURING WINTER SHUT-DOWNS.

THE SIGNS ON THE MAINLINE SHALL BE DUAL MOUNTED UNLESS NOT PHYSICALLY POSSIBLE. THE FIRST SIGN SHALL BE PLACED BETWEEN THE ROAD WORK AHEAD (W20-1) SIGN AND THE NEXT SIGN IN THE SEQUENCE. SIGNS SHALL BE ERECTED ON EACH ENTRANCE RAMP AND EVERY 2 MILES THROUGH THE CONSTRUCTION WORK LIMITS. SIGNS ON THE MAINLINE SHALL BE R11-H5A-48. SIGNS USED ON THE RAMPS SHALL BE R11-H5A-24. R11-H5A-24 SIGNS MAY BE USED IN THE MEDIAN IN LIEU OF R11-H5A-48 SIGNS IF IT IS NOT PHYSICALLY POSSIBLE TO PROVIDE R11-H5A-48 SIGNS IN THE MEDIAN.

THE R11-H5A-48 SIGNS SHALL BE MOUNTED ON 2 NO. 3 POSTS WHEN LOCATED WITHIN CLEAR ZONES.

THE CONTRACTOR MAY USE SIGNS AND SUPPORTS IN USED, BUT GOOD, CONDITION PROVIDED THE SIGNS MEET CURRENT ODOT SPECIFICATIONS. SIGN FACES SHALL BE RETROREFLECTORIZED WITH TYPE G SHEETING COMPLYING WITH THE REQUIREMENTS OF C&MS 730.19.

WORK ZONE INCREASED PENALTIES SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS, INCLUDING THE SIGN AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION AS DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.

PAYMENT FOR ACCEPTED QUANTITIES, COMPLETE, IN PLACE WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, ERECTING, MAINTAINING, COVERING DURING SUSPENSION OF WORK, AND REMOVAL OF THE SIGN AND SUPPORT.

ITEM 614, WORK ZONE INCREASED PENALTIES SIGN 14 EACH

DESIGN AGENCY



DESIGNER
GLI

REVIEWER
MPB 06-23-25

PROJECT ID
120987

SHEET 5 TOTAL 23

WORK ZONE SPEED ZONES (WZSZS)

THE FOLLOWING WORK ZONE SPEED ZONE (WZSZ) SPEED LIMIT REVISION(S) HAVE BEEN APPROVED FOR USE ON THIS PROJECT WHEN WORK ZONE CONDITIONS AND FACTORS ARE MET AS DESCRIBED BELOW:

WZSZ REVISION NUMBER(S)	C-R-S	DIRECTION(S)
WZ-10171	ALL/HAN-75-0.00/14.10	NB & SB

POTENTIAL WZSZ LOCATIONS SHALL HAVE AN ORIGINAL (PRE-CONSTRUCTION) POSTED SPEED LIMIT OF 55 MPH OR GREATER, A QUALIFYING WORK ZONE CONDITION OF AT LEAST 0.5 MILE IN LENGTH, AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS, AND A WORK ZONE CONDITION IN PLACE THAT REDUCES THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS (I.E., LANE CLOSURE, LANE SHIFT, Crossover, CONTRAFLOW AND/OR SHOULDER CLOSURE). THE LENGTH OF THE WORK ZONE CONDITION IS MEASURED FROM THE BEGINNING OF THE TAPER FOR THE SUBJECT WORK ZONE CONDITION IMPACTING THE TRAVEL LANES AND/OR SHOULDER TO THE END OF THE DOWNSTREAM TAPER, WHERE DRIVERS ARE RETURNED TO TYPICAL ALIGNMENT. AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS IS REQUIRED TO BALANCE THE ADDITIONAL EXPOSURE CREATED BY INSTALLING AND REMOVING WZSZ SIGNING WITH THE TIME NEEDED TO COMPLETE THE WORK.

IF THE WORK ZONE MEETS THESE MINIMUM CRITERIA, IT SHALL BE ANALYZED FURTHER USING TABLE 1 BELOW TO DETERMINE IF AND WHEN IT QUALIFIES FOR A SPEED LIMIT REDUCTION. DEPENDING ON THE ORIGINAL POSTED SPEED LIMIT, THE TYPE OF TEMPORARY TRAFFIC CONTROL USED, AND WHETHER OR NOT WORKERS ARE PRESENT, A WARRANTED WZSZ WILL VARY IN THE APPROVED SPEED LIMIT TO BE POSTED OVER TIME.

C&MS ITEM 614, PARAGRAPH 614.02(B), INDICATES THAT TWO DIRECTIONS OF A DIVIDED HIGHWAY ARE CONSIDERED SEPARATE HIGHWAY SECTIONS. THEREFORE, IF THE WORK ON A MULTI-LANE DIVIDED HIGHWAY IS LIMITED TO ONLY ONE DIRECTION, A SPEED LIMIT REDUCTION IN THE DIRECTION OF THE WORK DOES NOT AUTOMATICALLY CONSTITUTE A SPEED LIMIT REDUCTION IN THE OPPOSITE DIRECTION. EACH DIRECTION SHALL BE ANALYZED INDEPENDENTLY FROM EACH OTHER.

ALL WZSZS FLUCTUATE BETWEEN TWO APPROVED REDUCED SPEED LIMITS OR BETWEEN AN APPROVED REDUCED SPEED LIMIT AND THE ORIGINAL POSTED SPEED LIMIT. ONLY ONE OF TWO SIGNING STRATEGIES SHALL BE USED TO IMPLEMENT A WZSZ.

WZSZS USING DSL SIGN ASSEMBLIES SHALL BE IN ACCORDANCE WITH THIS NOTE, APPROVED LIST, SUPPLEMENTAL SPECIFICATIONS (SS) 808 AND 908, AND TRAFFIC SCD MT-104.10.

WZSZS USING TEMPORARY FLATSHEET SPEED LIMIT SIGNS SHALL BE IN ACCORDANCE WITH THIS NOTE AND SCD MT-104.10. ADDITIONALLY PAYMENT MAY BE REMOVED, OR A DISINCENTIVE APPLIED, FOR WZSZS USING TEMPORARY FLATSHEET SPEED LIMIT SIGNS THE SAME AS DESCRIBED IN THE MOST RECENT PUBLICATION OF SS 808 IN REGARDS TO WZSZS USING DSL SIGN ASSEMBLIES (SEE SS 808.06 PARAGRAPHS 4 THROUGH 7, INCLUDING TABLE 1).

ONLY ONE WARRANTED SPEED LIMIT APPLIES AT ANY ONE TIME; SPEED LIMIT REDUCTIONS ARE NOT CUMULATIVE. WZSZS SHALL NOT BE USED FOR MOVING/MOBILE ACTIVITIES, AS DEFINED IN OMUTCD PART 6.

WHEN LOOKING UP THE WARRANTED WORK ZONE SPEED LIMITS, ALWAYS USE THE ORIGINAL, PRECONSTRUCTION, POSTED SPEED LIMIT. DO NOT USE A PRIOR OR CURRENT WORK ZONE SPEED LIMIT AS A LOOK UP VALUE IN THE TABLE. POSITIVE PROTECTION IS GENERALLY REGARDED AS PORTABLE BARRIER OR OTHER RIGID BARRIER IN USE ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WITHOUT POSITIVE PROTECTION IS GENERALLY REGARDED AS USING DRUMS, CONES, SHADOW VEHICLE, ETC., ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WORKERS ARE CONSIDERED AS BEING PRESENT WHEN ON-SITE, WORKING WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WHEN THE WORK ZONE CONDITION REDUCING THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS IS REMOVED, THE SPEED LIMIT DISPLAYED SHALL RETURN TO THE ORIGINAL POSTED SPEED LIMIT.

TABLE 1: WARRANTED WORK ZONE SPEED LIMITS (MPH)
FOR WORK ZONES ON HIGH-SPEED (55 MPH
OR GREATER) MULTI-LANE HIGHWAYS

	WITH POSITIVE PROTECTION		WITHOUT POSITIVE PROTECTION	
	ORGINAL POSTED SPEED LIMIT	WORKERS PRESENT	WORKERS NOT PRESENT	WORKERS PRESENT
70	60	65	55	65
65	55	60	50	60
60	55	60	50	60
55	50	55	45	55

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 808, DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY
36 SIGN MONTHS
ASSUMING 12 DSL SIGN ASSEMBLY(IES) FOR
3 MONTH(S)

ITEM 614, MAINTAINING TRAFFIC (TIME LIMITATION ON RAMP
DETOURS)

A MINIMUM OF ONE LANE OF TRAFFIC ON THE RAMPS SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED 1 TO 2 CONSECUTIVE CALENDAR DAYS PER RAMP, WHEN THROUGH TRAFFIC MAY BE DETOURED AS SHOWN IN THESE PLANS OR AS APPROVED BY THE ENGINEER. RAMPS WITH 1 DAY CLOSURES ARE THE RAMPS AT BREESE RD., SR 65 AND SR 12 INTERCHANGES. RAMPS WITH 2 DAY CLOSURES ARE THE RAMPS AT US 68/SR 15 INTERCHANGE. US 68/ SR 15 INTERCHANGE RAMPS SHALL BE WEEKEND CLOSURES ONLY FROM 8:00 PM FRIDAY THRU 6:00 AM MONDAY DURING THE MONTHS OF MAY THRU JULY. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$3,500 PER DAY FOR BREESE RD., SR 65 AND SR 12 RAMPS FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMITS. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$15,000 PER DAY FOR US 68/SR 15 RAMPS FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMITS. NO CONCURRENT CLOSURES ARE PERMITTED THAT IMPACT A DETOUR ROUTE FOR ANOTHER CLOSURE.

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD AND RAMP CLOSURES IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIMETABLE LISTED IN THESE PLANS. THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS AND SIGN SUPPORTS, AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AND TYPE III BARRICADES OF THE TYPE AND LOCATION AS SHOWN IN THESE PLANS.

THE CONTRACTOR SHALL FURNISH, ERECT, MAINTAIN AND SUBSEQUENTLY REMOVE THE DETOUR ROUTING SIGNS AND SUPPORTS ALONG THE DESIGNATED DETOUR ROUTES NOTED IN THE PLANS.

ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN

THE CONTRACTOR SHALL MAINTAIN AT LEAST ONE ELEVEN (11) FOOT LANE OF TRAFFIC IN EACH DIRECTION AT ALL TIMES ON MAINLINE I.R. 75 IN ACCORDANCE WITH THE REQUIREMENTS OF SPEC. 614 AND THESE MAINTENANCE OF TRAFFIC NOTES. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ORGANIZE THEIR WORK IN SUCH A MANNER TO PROVIDE THE MOST SAFETY WITH THE LEAST INCONVENIENCE TO THE TRAVELING PUBLIC.

ONE LANE CLOSURES ON ALL I.R. 75 AND ONE OR TWO LANE CLOSURES ON HAN I.R. 75 (IN AREAS THAT HAVE THREE LANES) WILL ONLY BE PERMITTED BETWEEN 8:00 P.M. AND 6:00 A.M.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS ON I.R. 75 SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT OF THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASON-ABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. A CLOSED LANE WITH NO FURTHER PATCHING REQUIRED AND NO PAVING/PLANING OPERATIONS SCHEDULED WITHIN FOUR (4) CALENDAR DAYS SHALL BE OPENED FOR THE CONVENIENCE OF THE TRAVELING PUBLIC. THE LEVEL OF UTILI-ZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COM-MENSURATED WITHIN THE WORK IN PROGRESS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ERECTING AND MAINTAINING SAFE AND EFFECTIVE TRAFFIC CONTROL 24 HOURS A DAY FOR THE DURATION OF THIS PROJECT. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, AND MAIN-TAINED, AND REMOVED BY THE CONTRACTOR. THIS CONSISTS OF NOTIFYING THE OHIO STATE PATROL AFTER ENCOUNTERING ANY ACCIDENTS OR DISABLED VEHICLES OR OBJECTS HINDERING THE FLOW OF TRAFFIC.

THE CONTRACTOR SHALL DESIGNATE TO THE ENGINEER, A PERSON RESPONSIBLE FOR MAINTENANCE OF TRAFFIC CONTROL DURING NON-WORK HOURS WHO SHALL BE AVAILABLE ON SITE WITHIN THIRTY (30) MINUTES AFTER NOTIFICATION.

UNLESS PHYSICALLY IMPOSSIBLE, ALL CONSTRUCTION EQUIPMENT SHALL EXIT ALL WORK ZONES FROM THE DOWNSTREAM END OF THE WORK ZONE OR BY INTERCHANGE RAMPS.

UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PER-MITTED TO DIRECTLY TRANSPORT OR OPERATE EQUIPMENT ACROSS THE OPEN LANES OF I.R. 75. THE CONTRACTOR WILL NOT BE PERMITTED TO UTILIZE THE EMERGENCY TURN-AROUNDS AS PART OF THE HAUL ROUTE.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ALTERNATE METHODS

IF THE CONTRACTOR SO ELECTS, THEY MAY SUBMIT ALTERNATE METHODS FOR MAINTENANCE OF TRAFFIC PROVIDED THE INTENT OF THE ABOVE PROVISIONS ARE FOLLOWED AND NO ADDITIONAL INCONVENIENCE TO THE TRAVELING PUBLIC RESULTS THERE FROM. NO ALTERNATE PLAN SHALL BE PLACED INTO EFFECT UNTIL APPROVAL HAS BEEN GRANTED, IN WRITING, BY THE DISTRICT CONSTRUCTION ENGINEER.

ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS
OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES AND RAMPS SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS:

NEW YEAR'S (OBSERVED)	GENERAL/REGULAR ELECTION DAY ((NOV)
THANKSGIVING	(OTHER HOLIDAY OR SPECIAL EVENT)
MEMORIAL DAY	CHRISTMAS (OBSERVED)
FOURTH OF JULY (OBSERVED)	
LABOR DAY	

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR SPECIAL EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY	(GEN./REG. ELECTION) 5:00 AM TUESDAY THROUGH 12:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY	(THANKSGIVING ONLY) 6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

DURING THE SAME PERIODS, MAINTAIN PEDESTRAIN ACCESS IF PEDESTRIAN ACCESS WAS PRESENT PRIOR TO CONSTRUCTION.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

LANE VALUE CONTRACT TABLE

DESCRIPTION OF CRITICAL LANE TO BE MAINTAINED	RESTRICTED TIME PERIOD	TIME UNIT	DISINCENTIVE \$ PER TIME UNIT
ANY LANES ON ALL IR 75	#	EACH HOUR	\$15,000
ANY LANES ON HAN IR 75	#	EACH HOUR	\$15,000 *

- SEE MAINTAINING TRAFFIC, HOLIDAY/SPECIAL EVENT NOTE AND MAINTAINING TRAFFIC, AS PER PLAN NOTE ON THIS SHEET.

* - PER EACH LANE

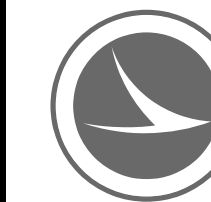
MICROSURFACING ON RAMPS

ON RAMPS LISTED BELOW ONLY MICROSURFACE ASPHALT AREAS. DO NOT MICROSURFACE ON CONCRETE SECTION OF RAMPS.

BREESE ROAD RAMPS
SR 65 RAMPS
SR 12 RAMPS

ON US 68/SR 15 RAMPS ONLY MICROSURFACE ASPHALT AREAS. DO NOT MICROSURFACE ON CONCRETE SECTIONS OF RAMPS AND ALSO EXCLUDE ALL CONCRETE STRUCTURES ON RAMPS.

DESIGN AGENCY

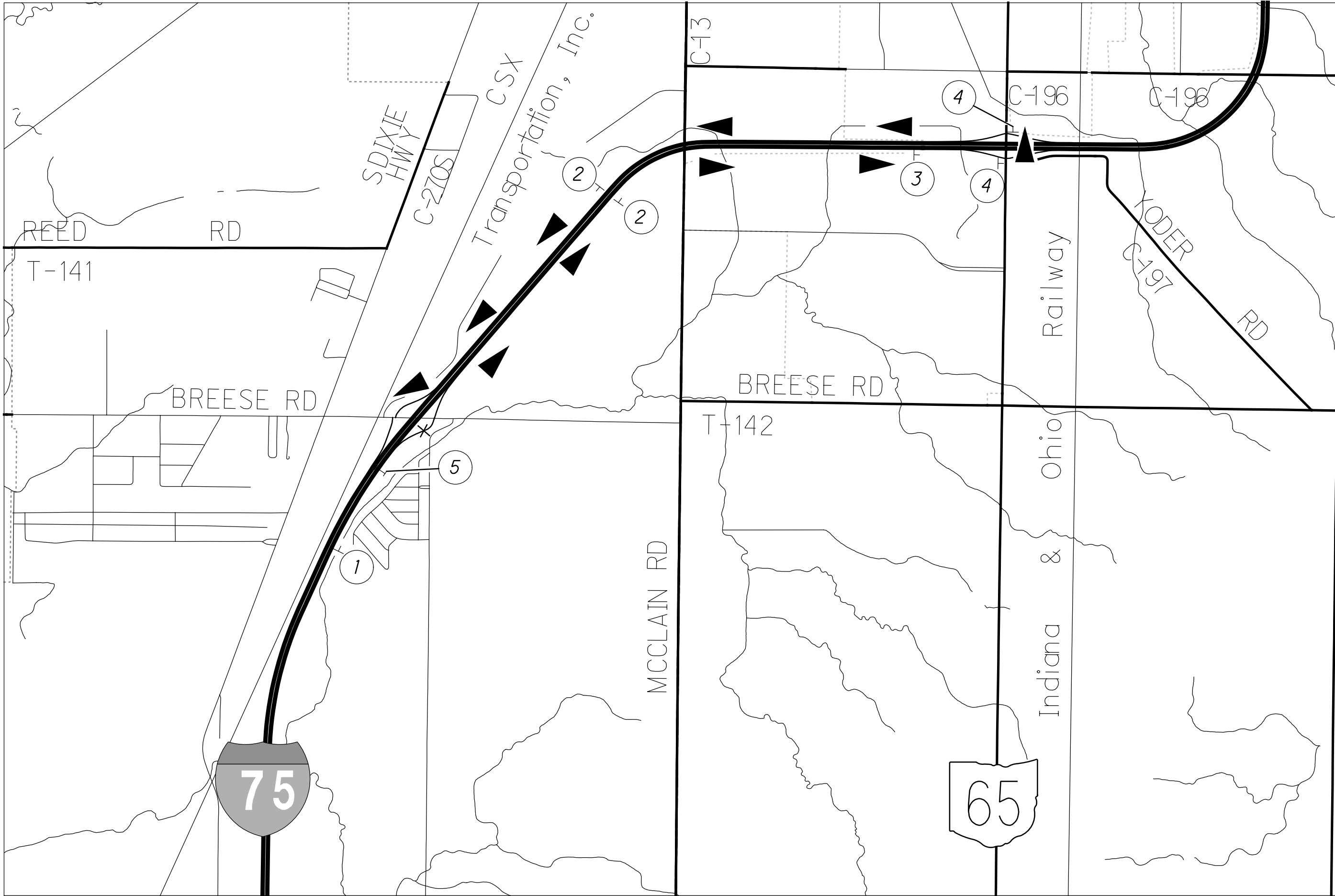


DESIGNER
GLI

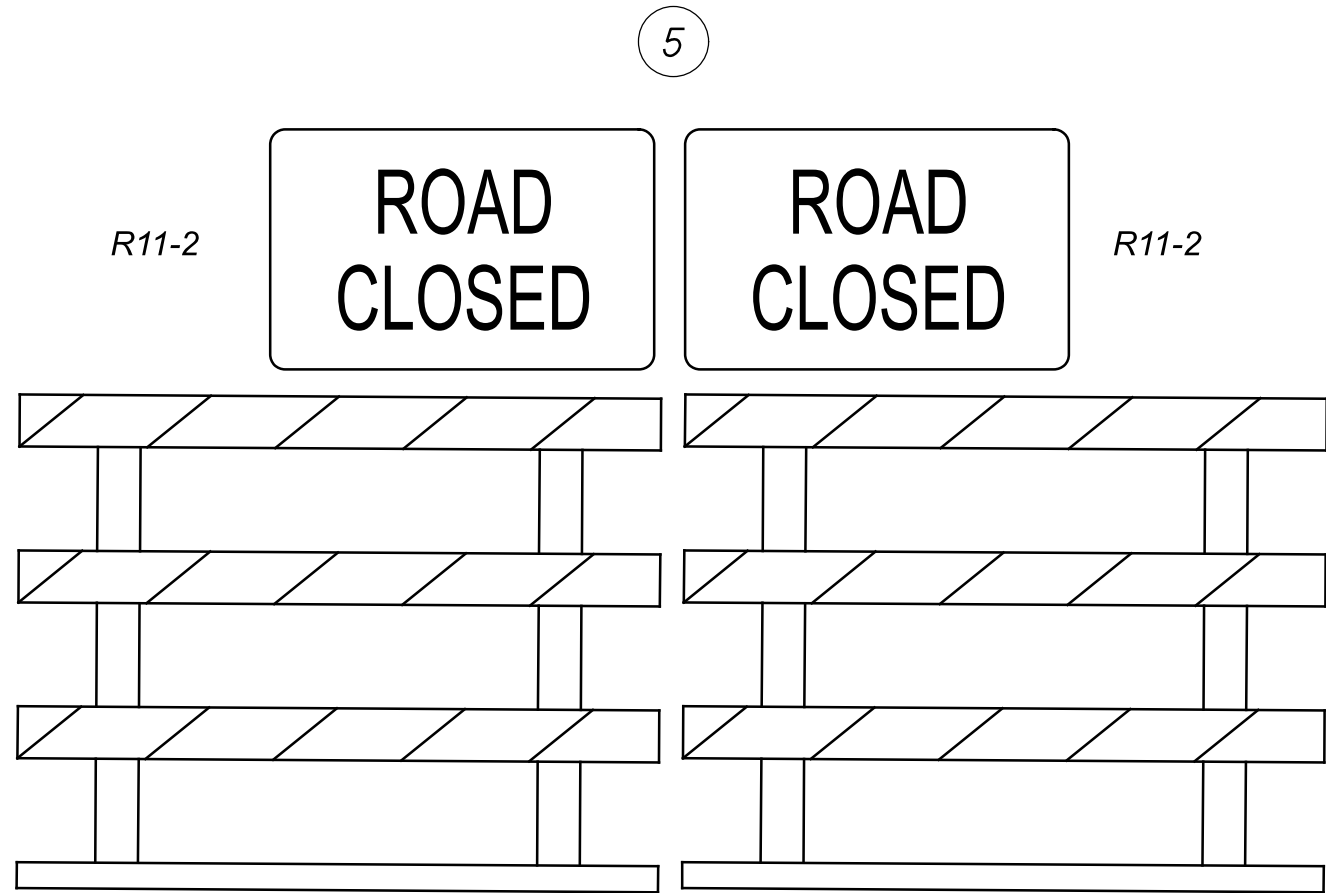
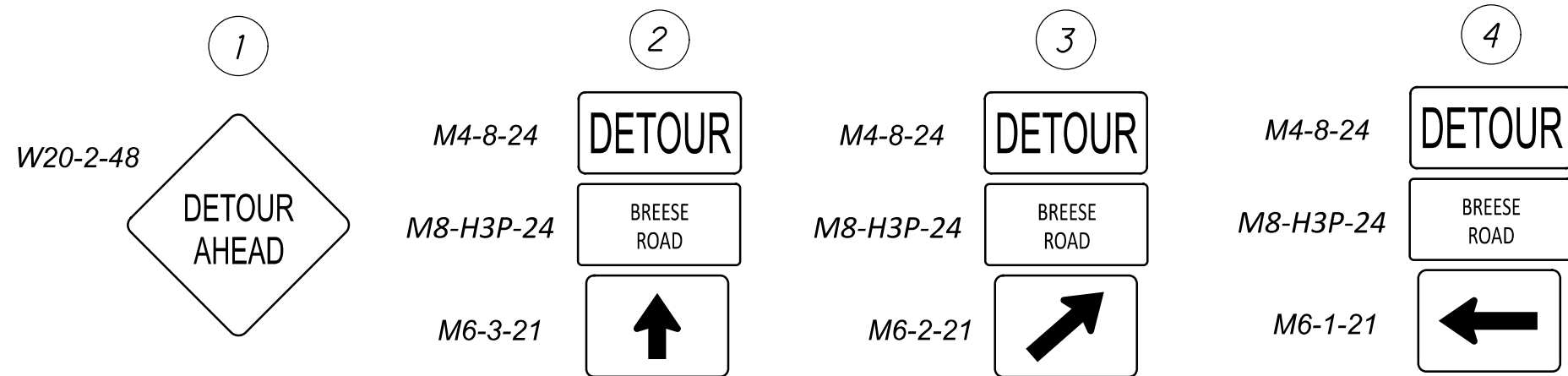
REVIEWER
MPB 06-23-25

PROJECT ID
120987

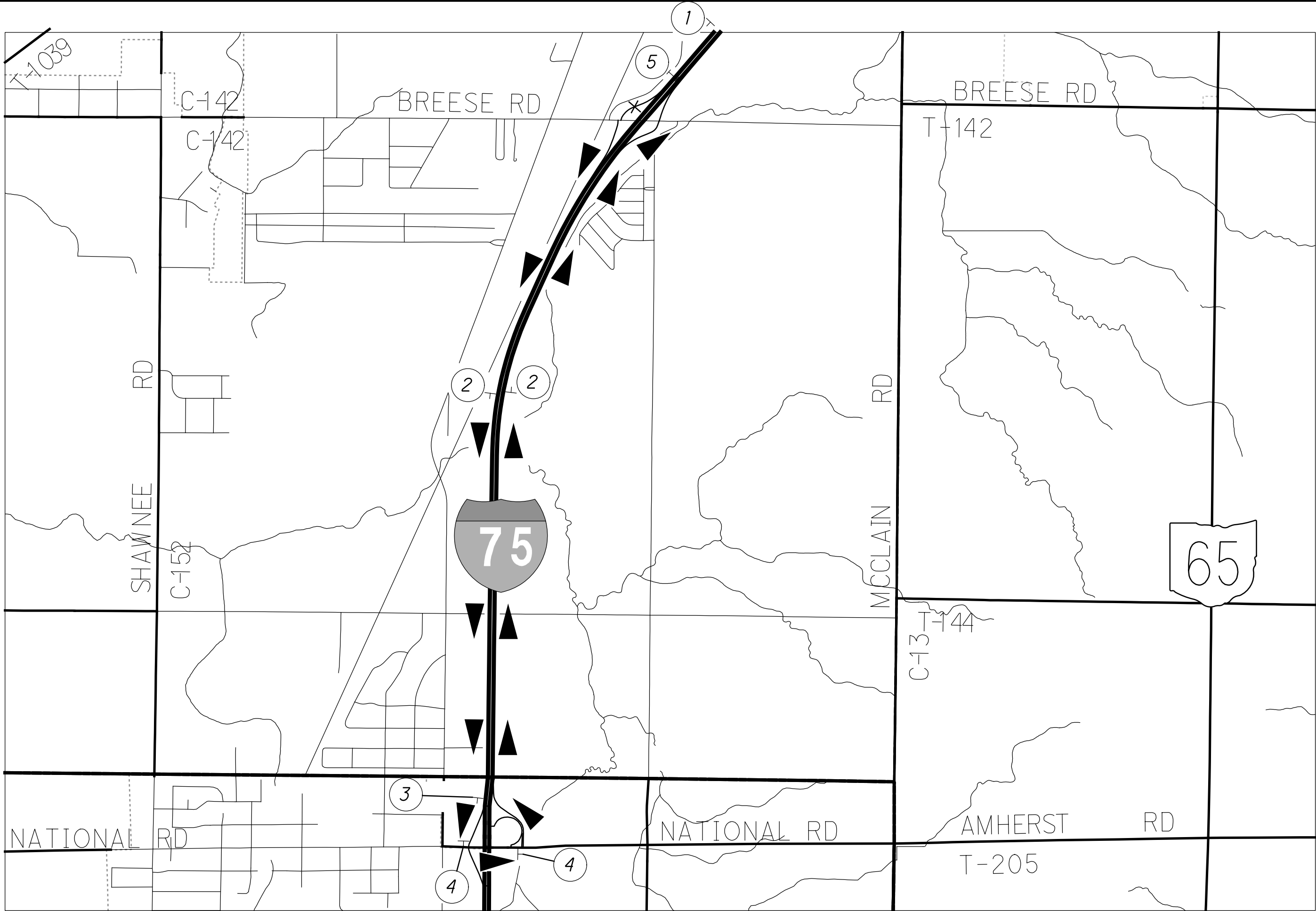
SHEET 6 TOTAL 23



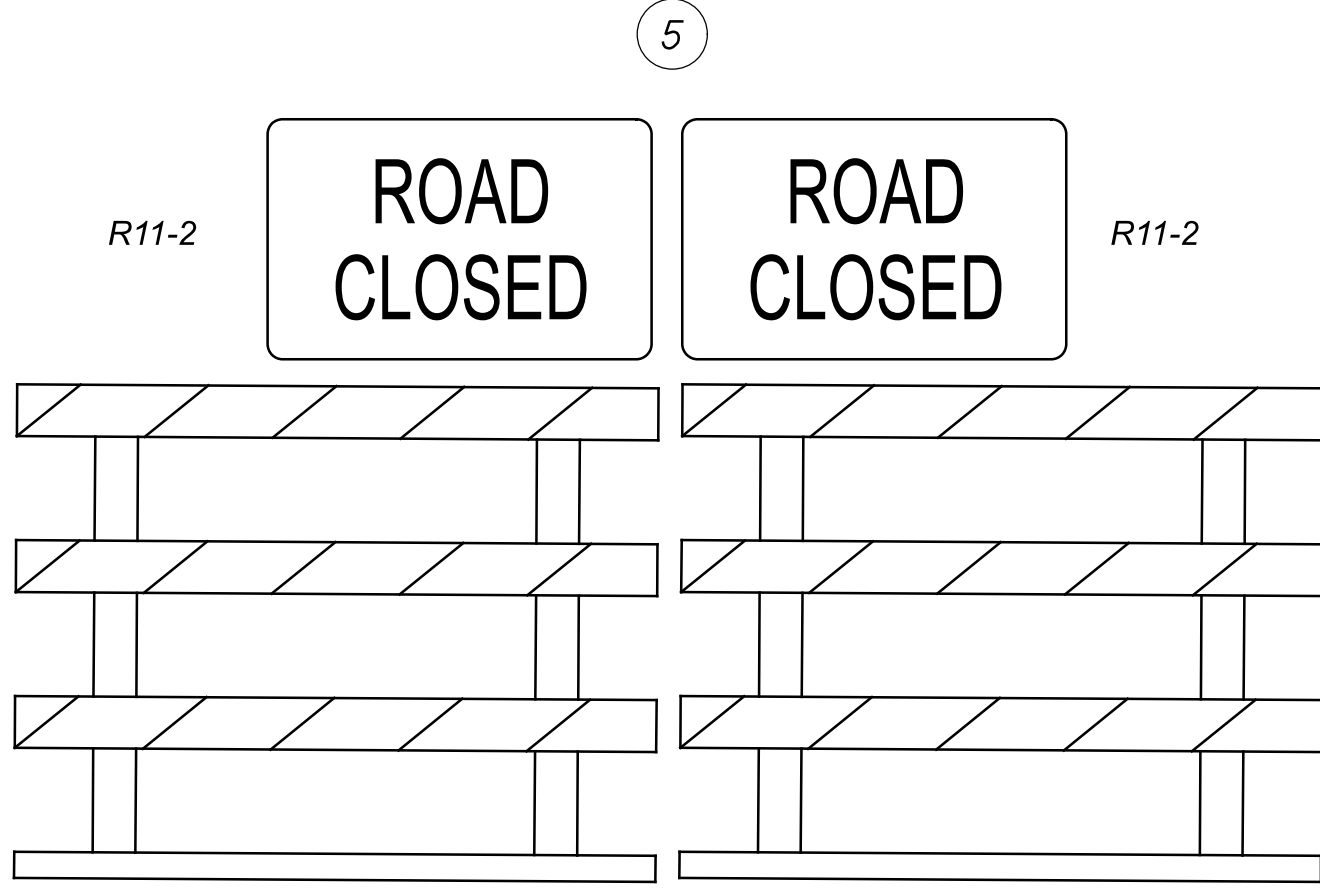
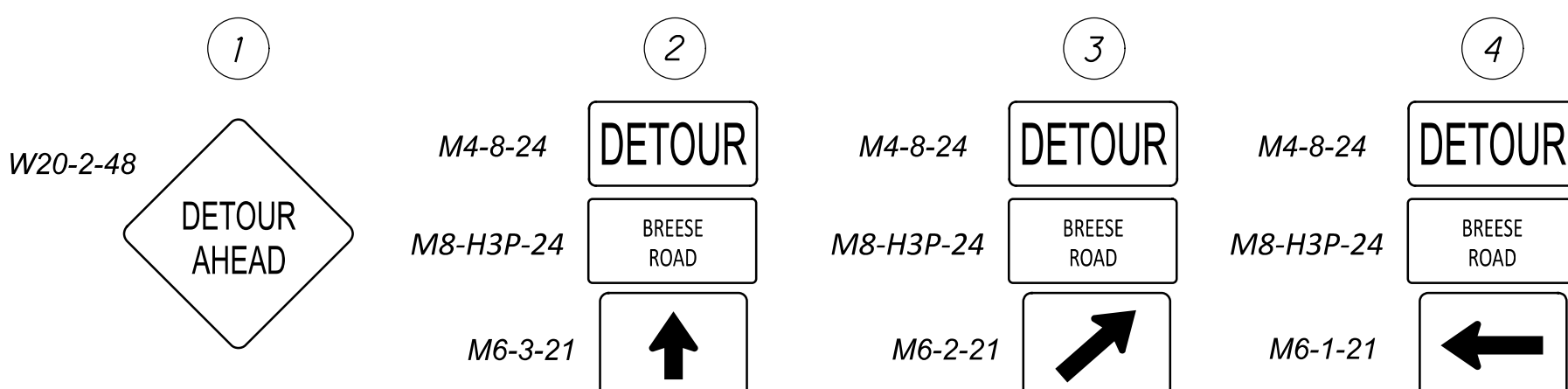
IR 75 NB Exit Ramp to Breese Rd. Detour



Type III Barricades Per MT-101.60

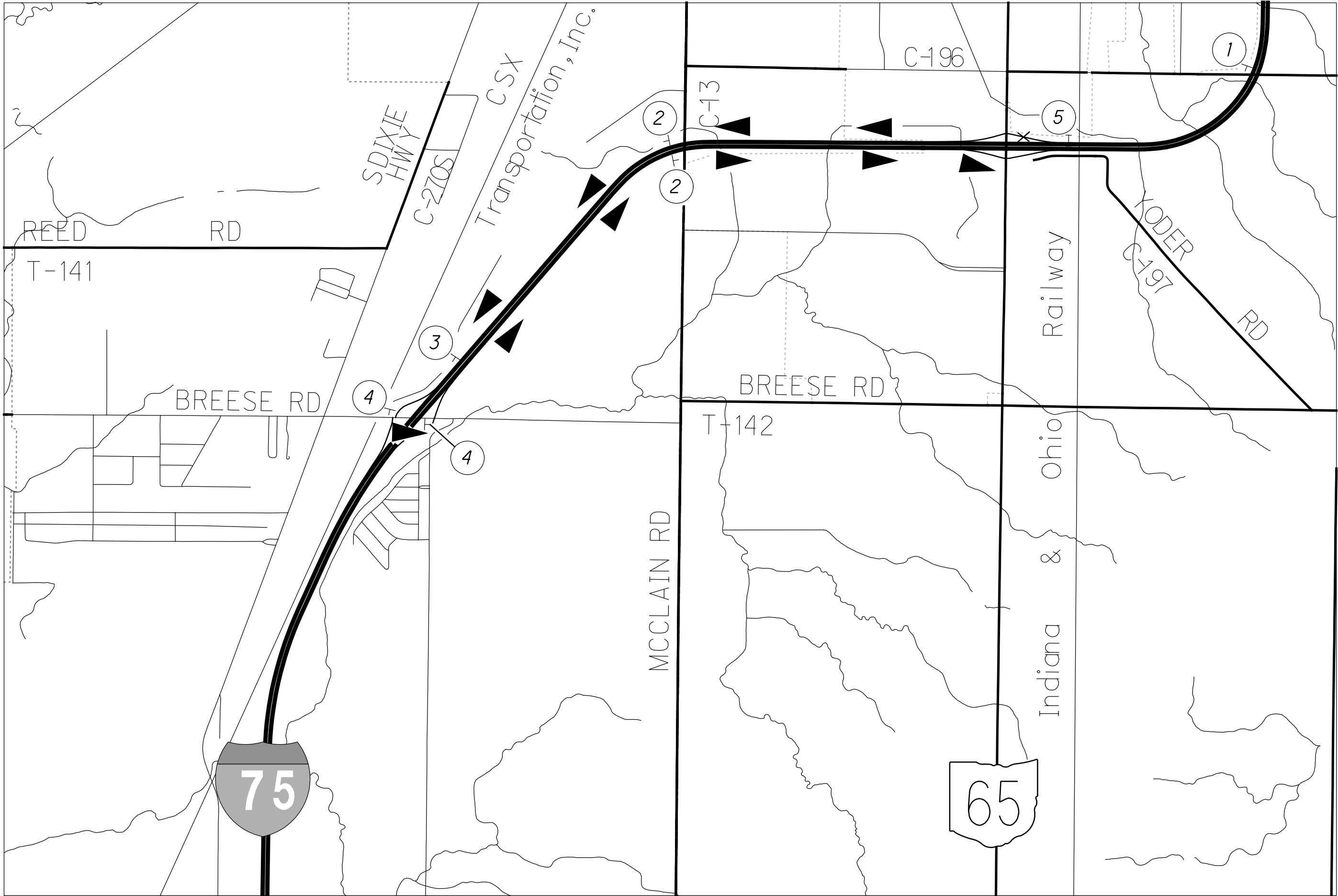


IR 75 SB Exit Ramp to Breese Rd. Detour

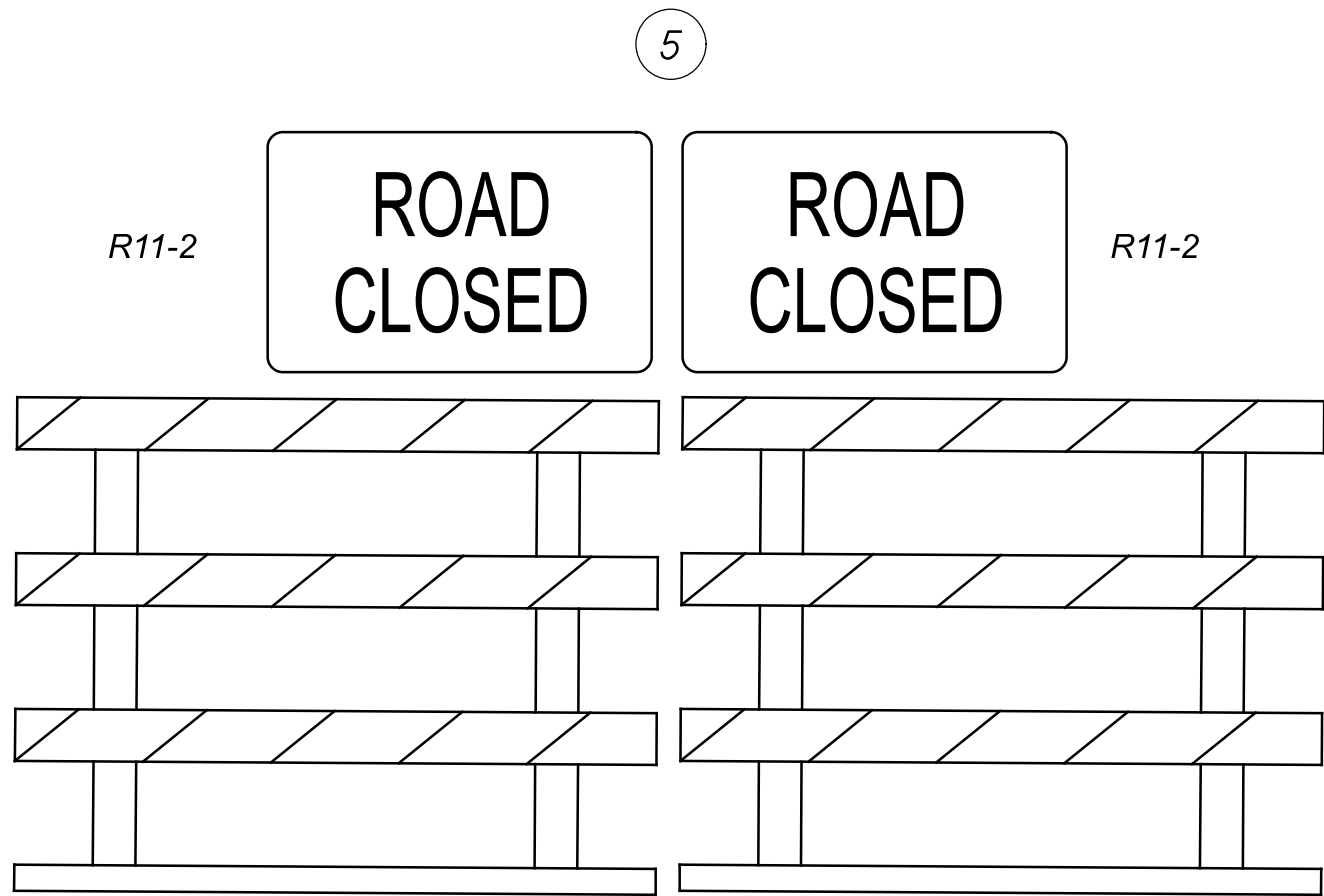
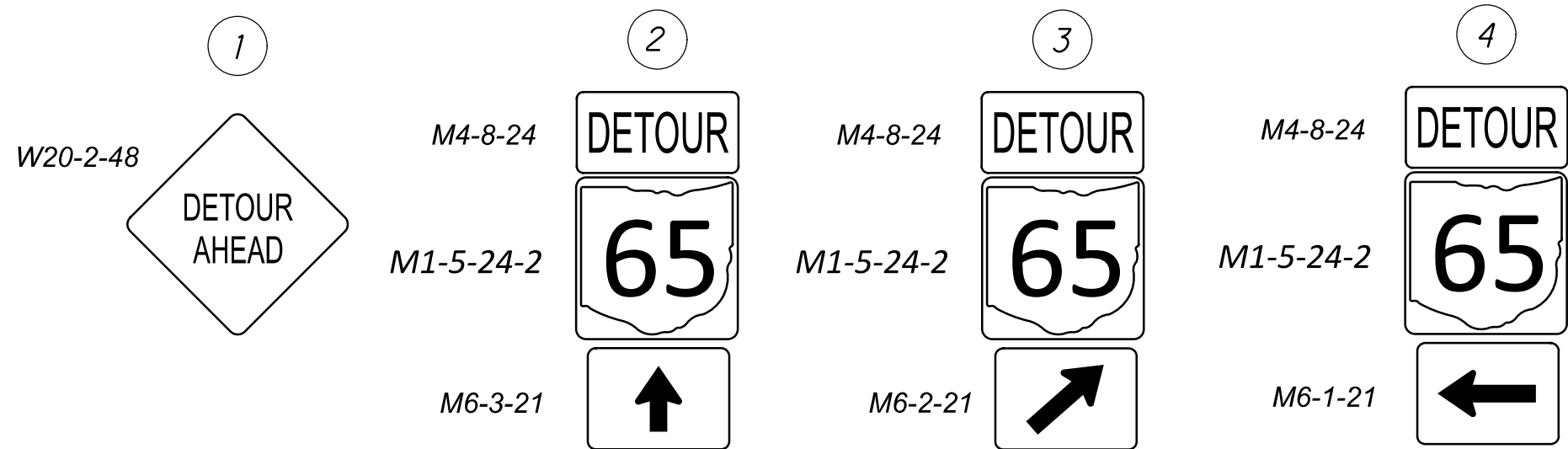


Type III Barricades Per MT-101.60

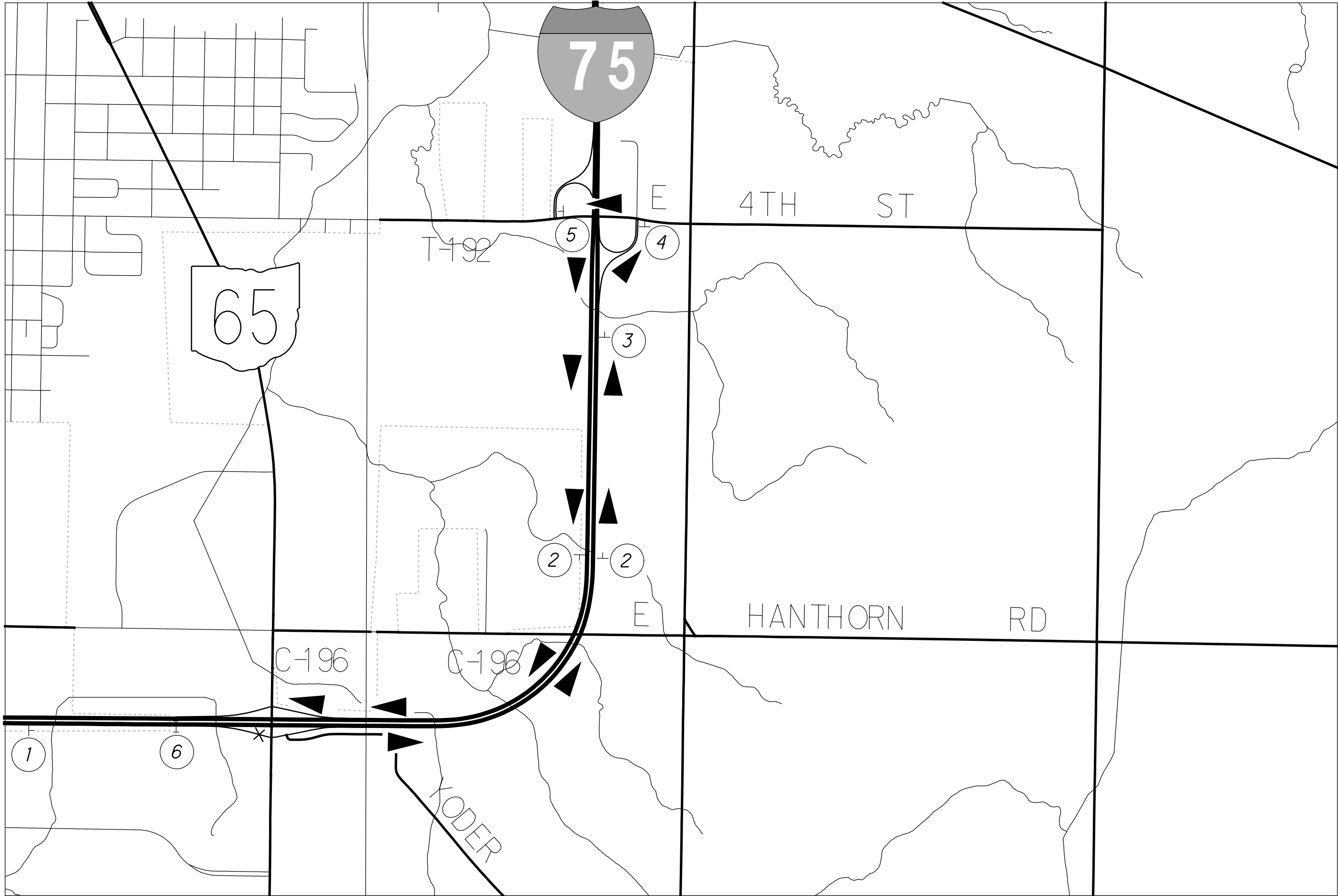




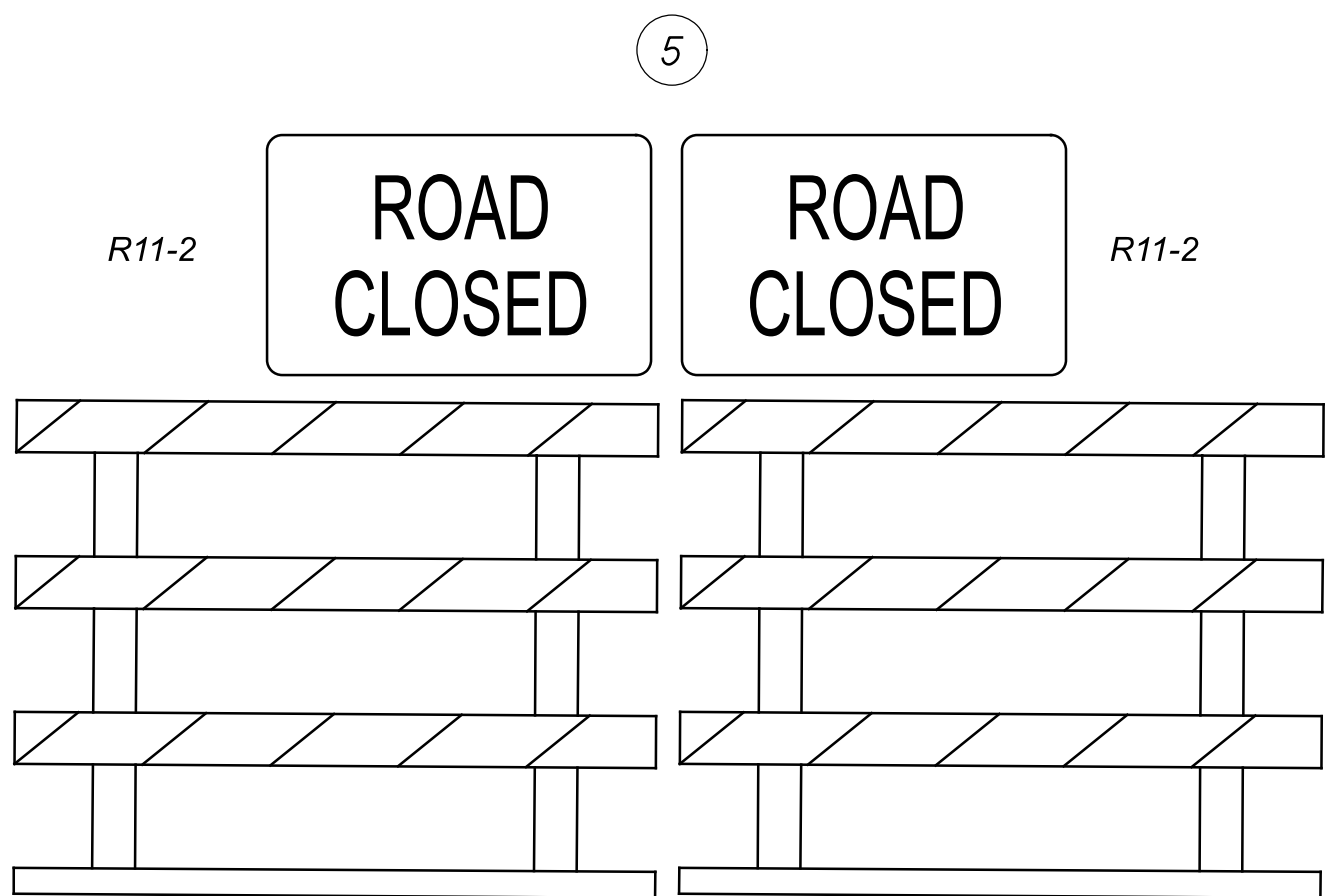
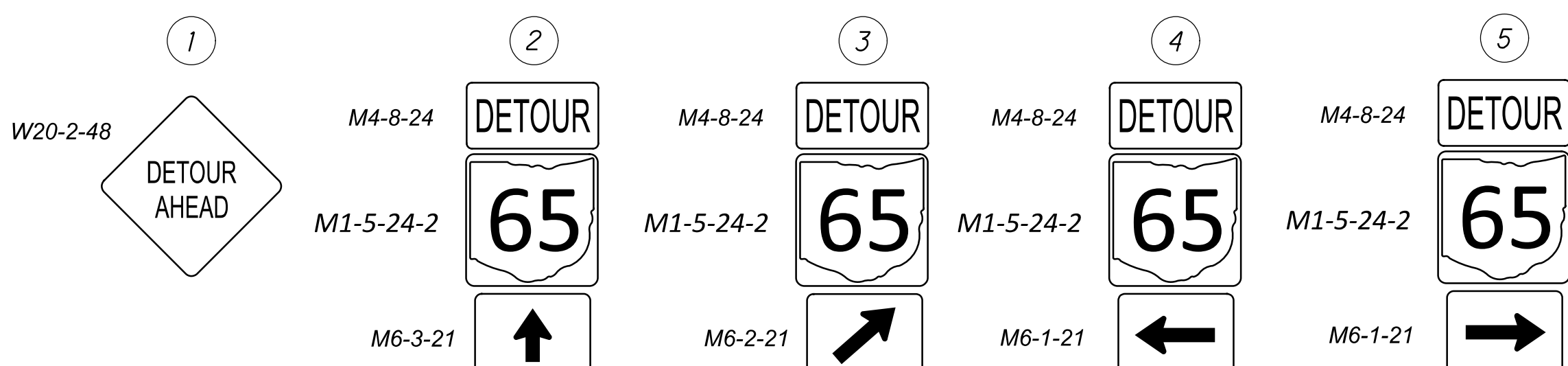
IR 75 SB Exit Ramp to SR 65 Detour



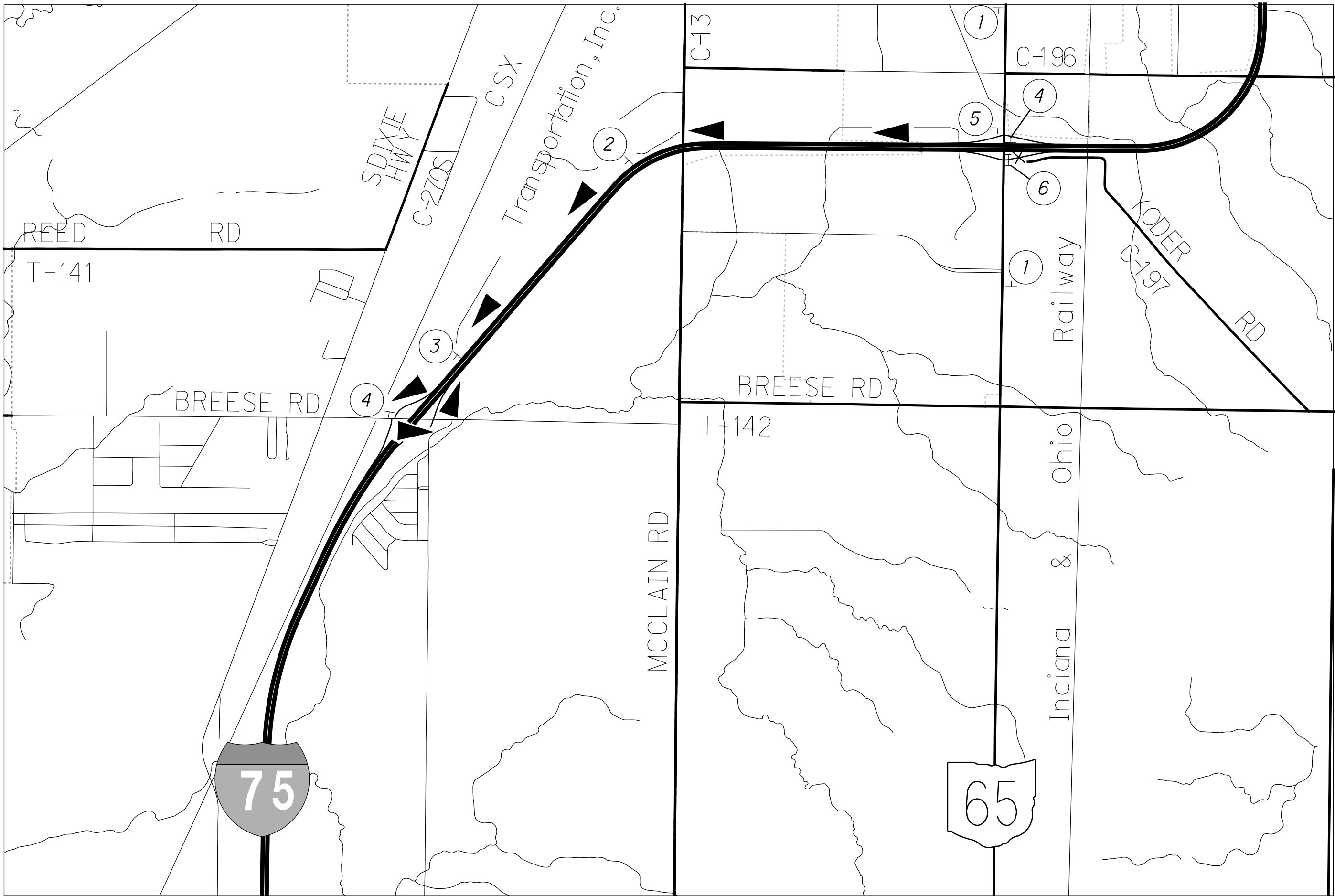
Type III Barricades Per MT-101.60



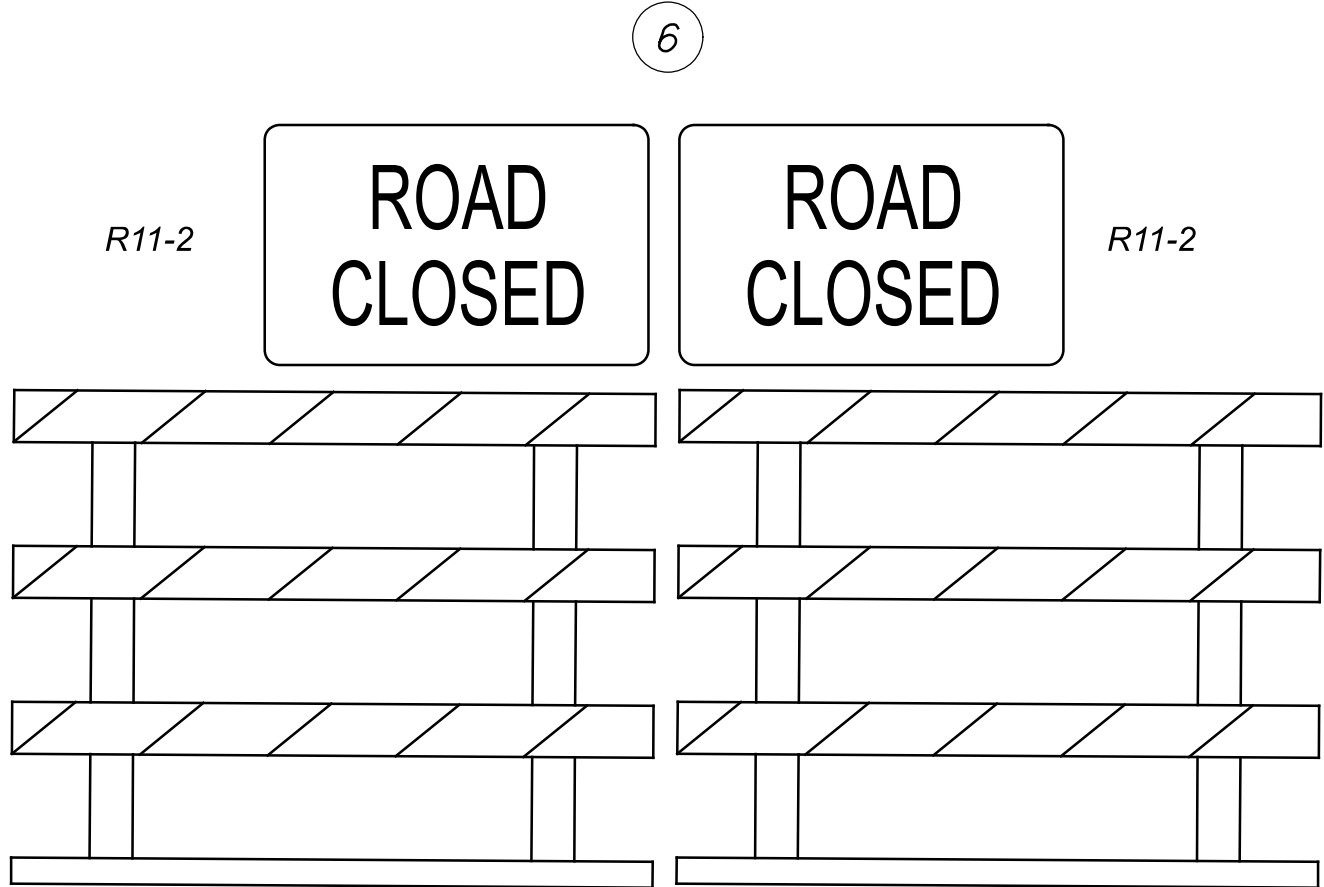
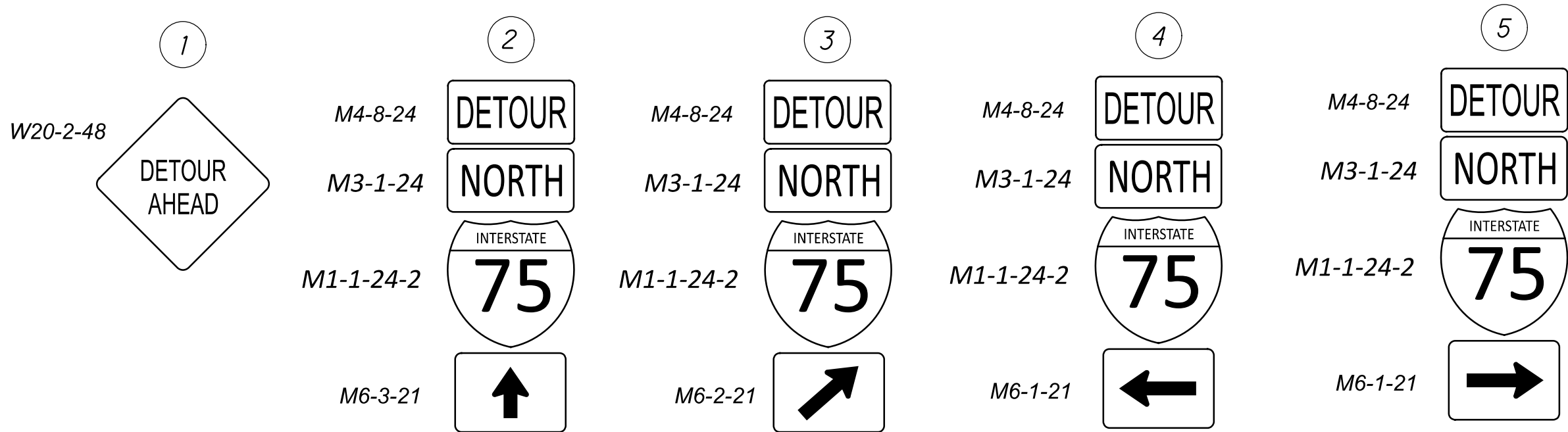
IR 75 NB Exit Ramp to SR 65 Detour



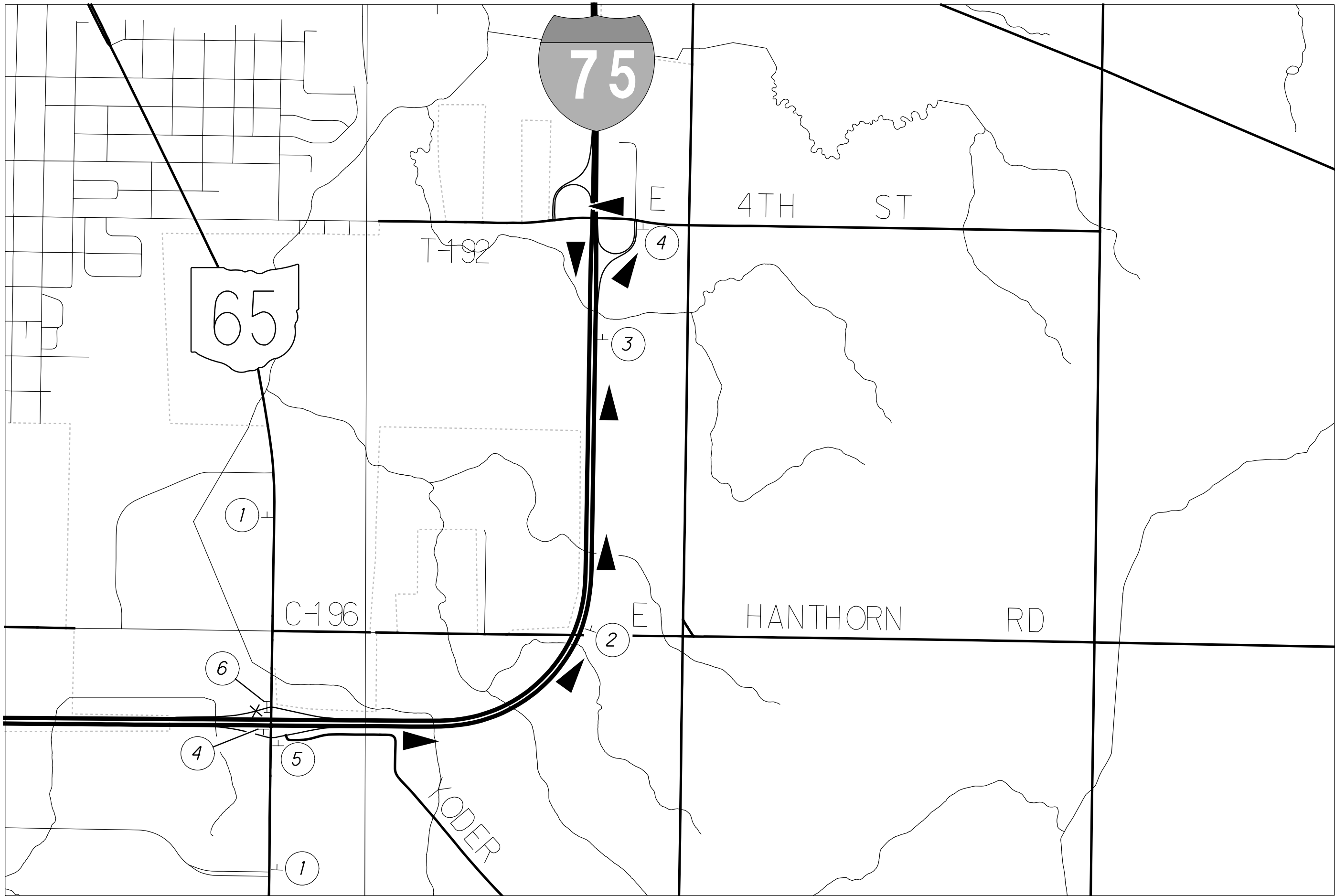
Type III Barricades Per MT-101.60



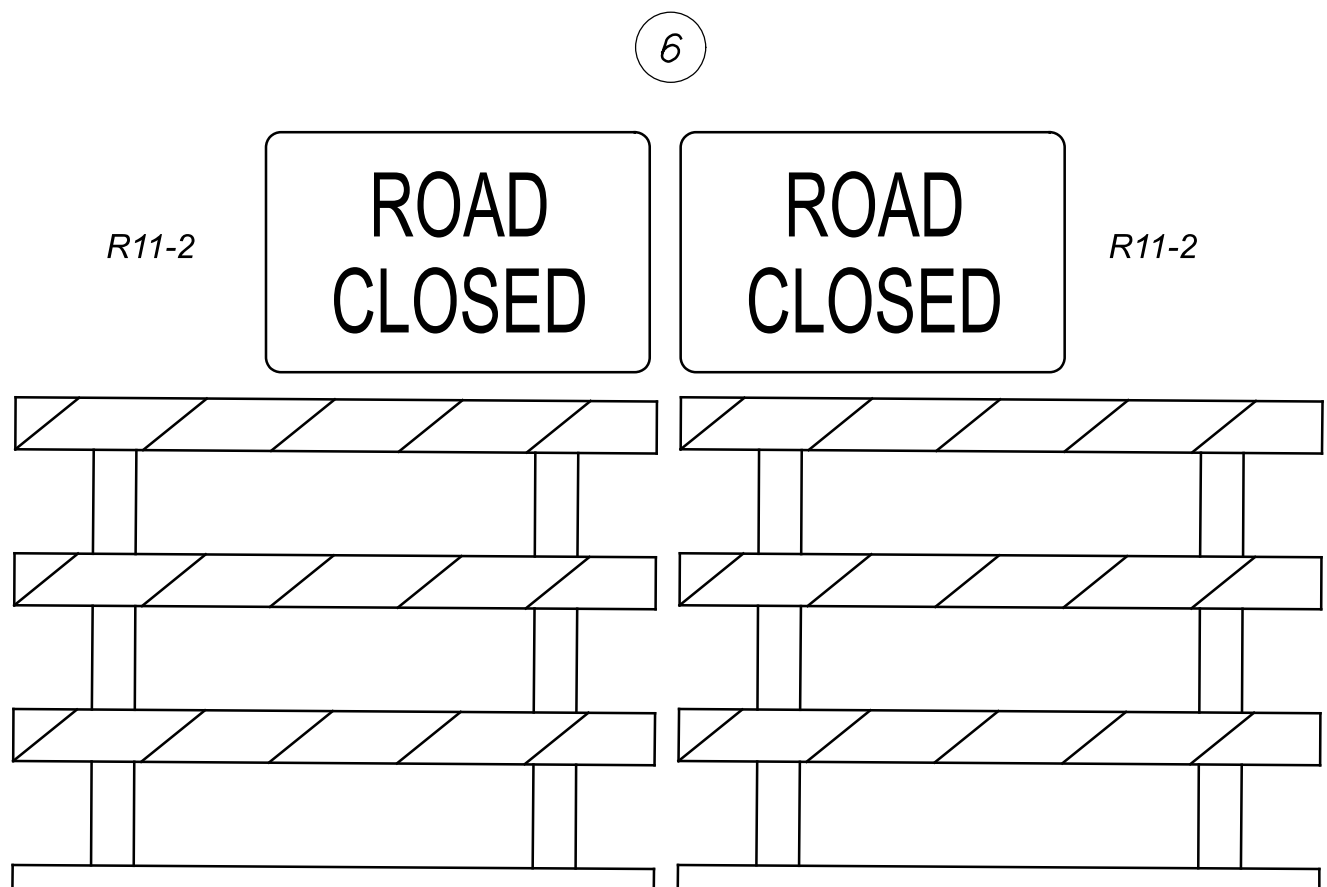
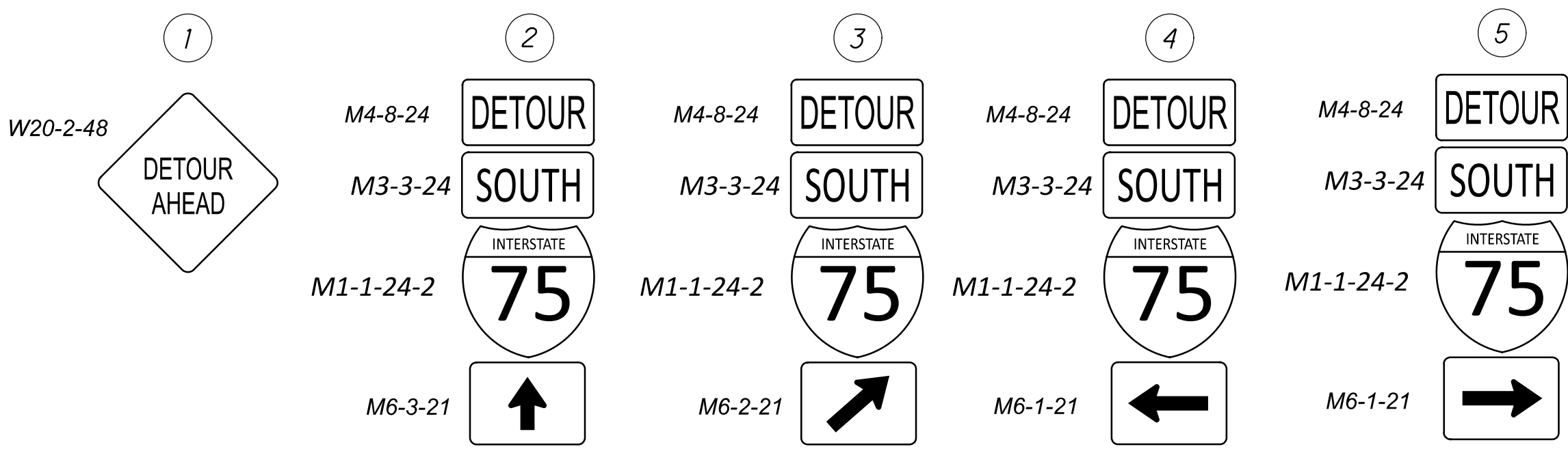
SR 65 Entrance Ramp to IR 75 NB Detour



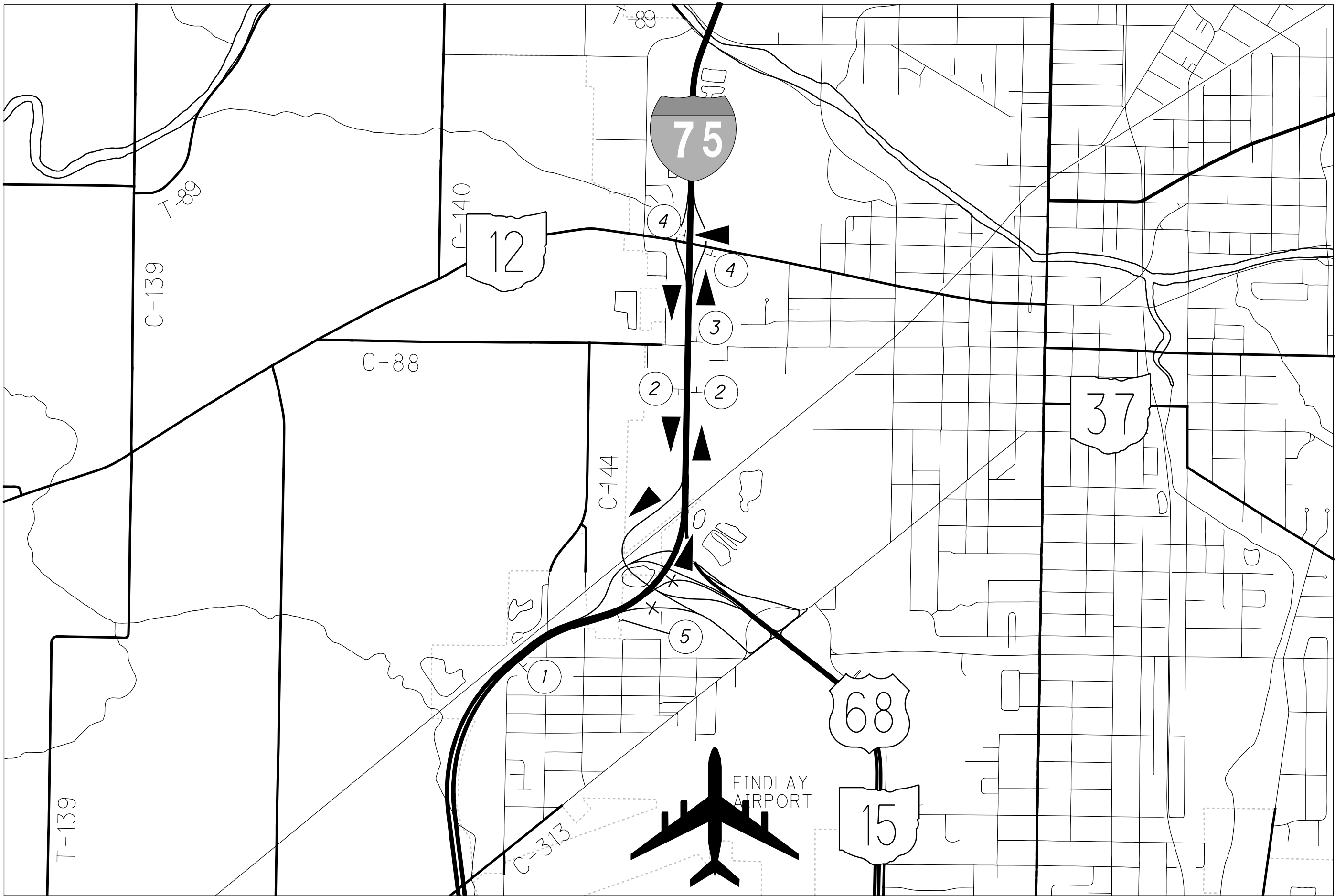
Type III Barricades Per MT-101.60



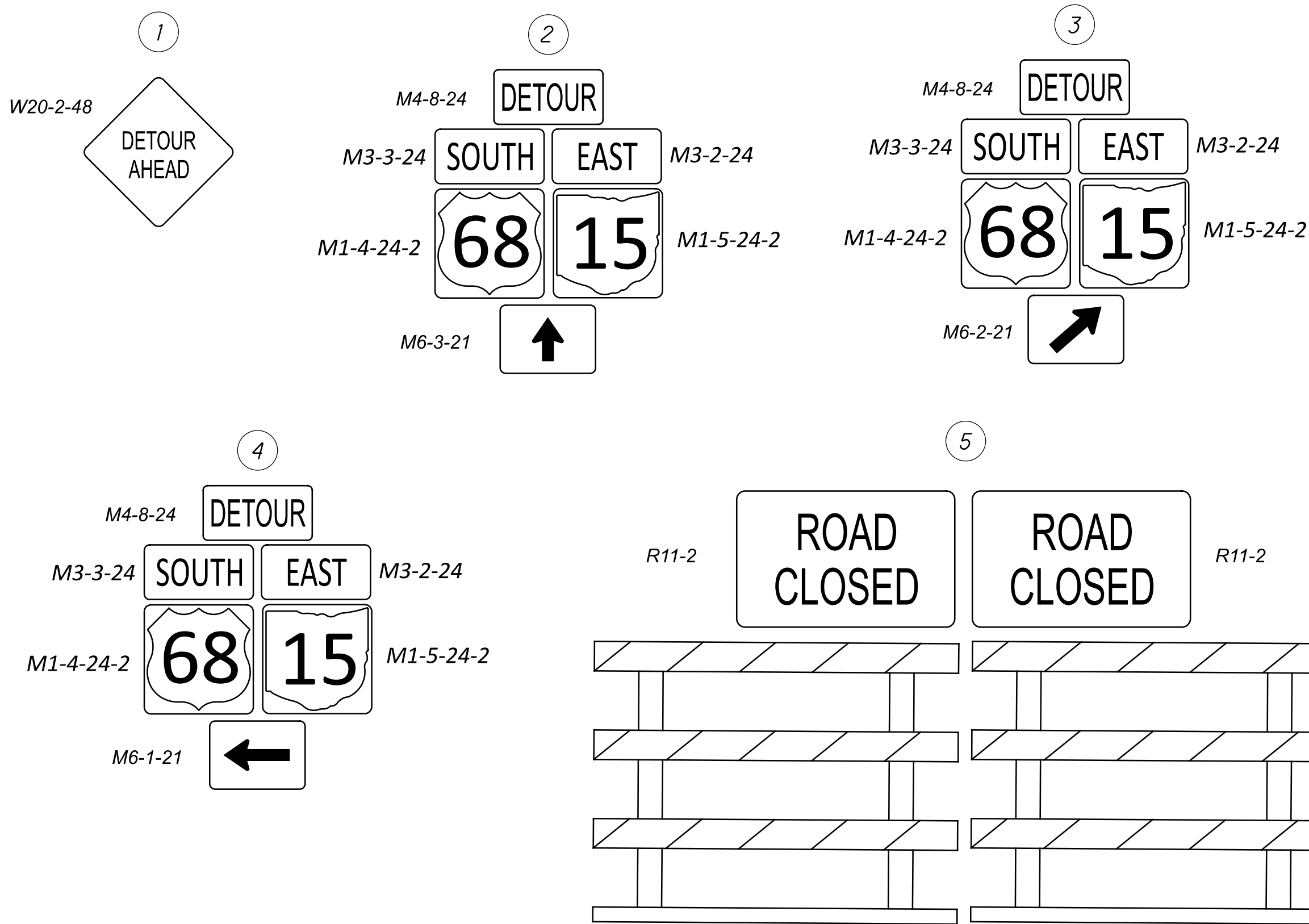
SR 65 Entrance Ramp to IR 75 SB Detour



Type III Barricades Per MT-101.60



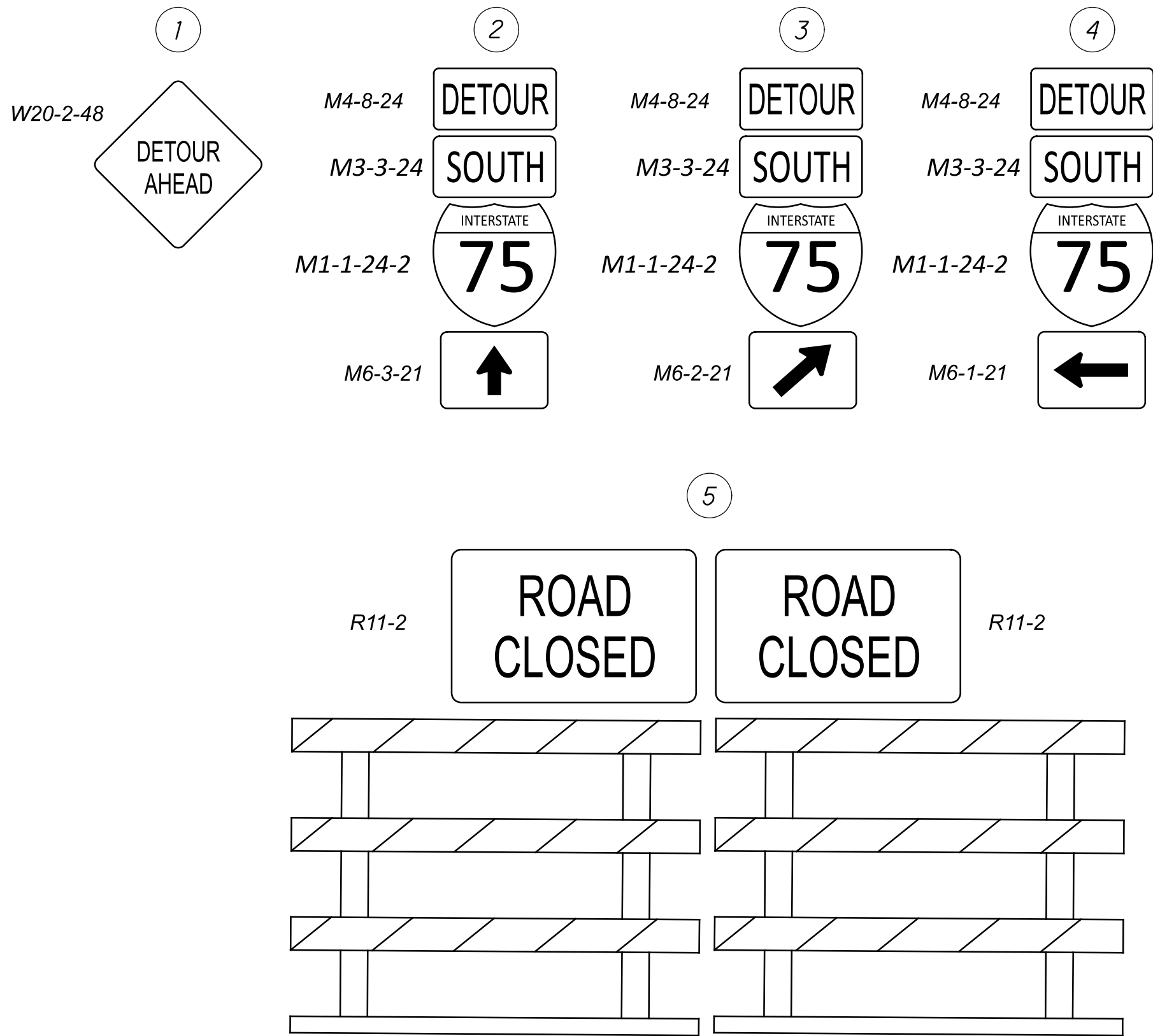
IR 75 NB Exit Ramp to US 68 SB and SR 15 EB and Lima Ave. Detour



Type III Barricades Per MT-101.60

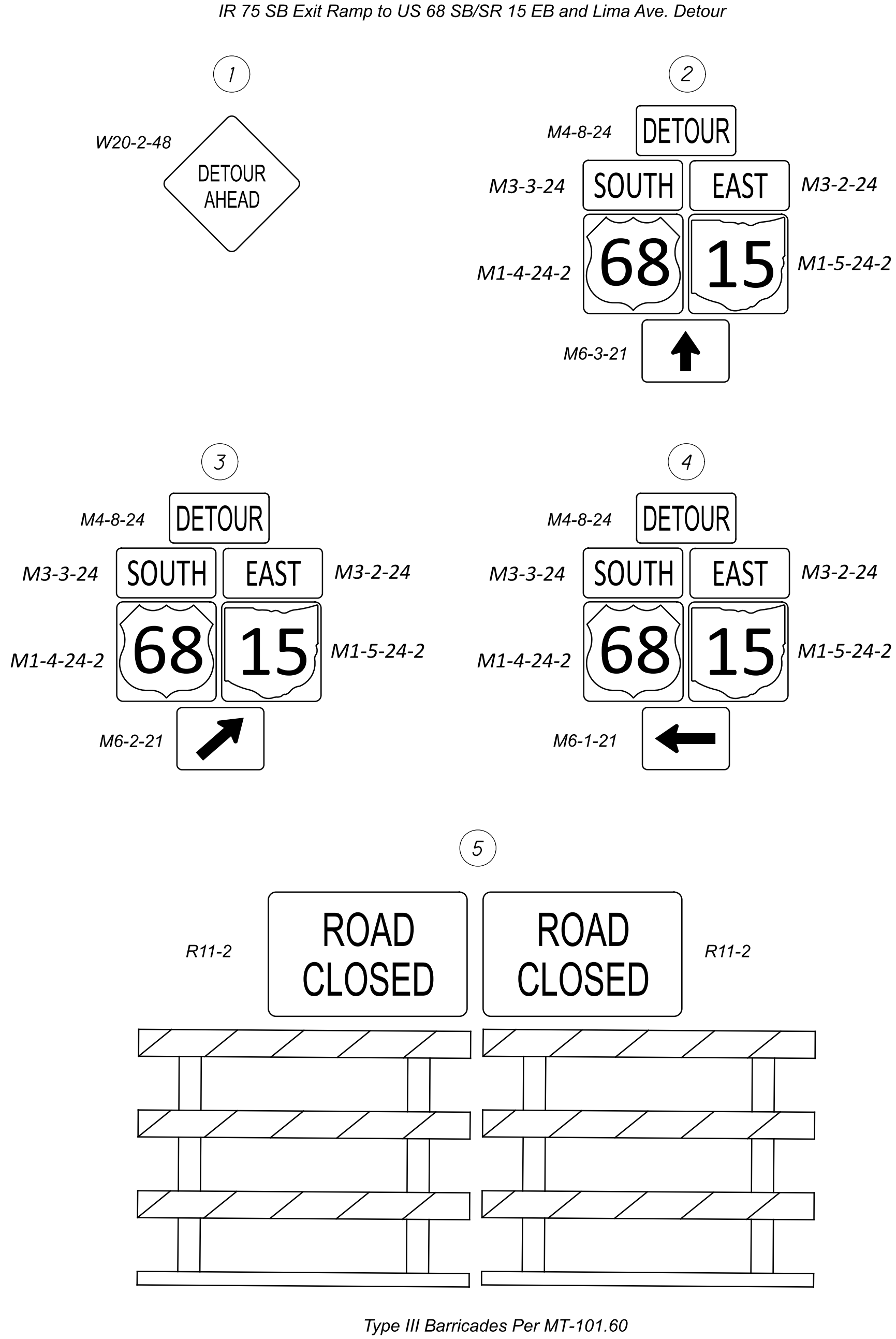
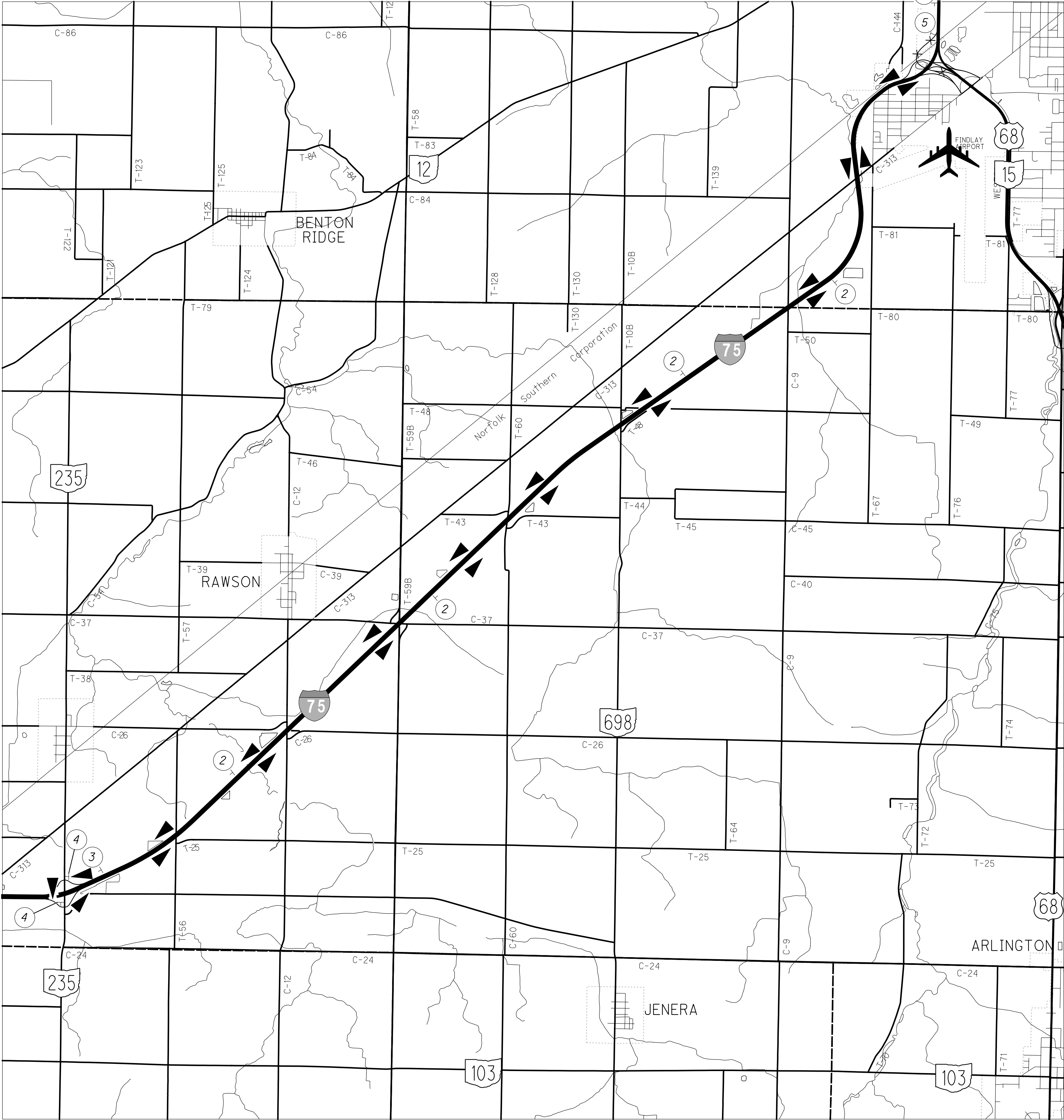


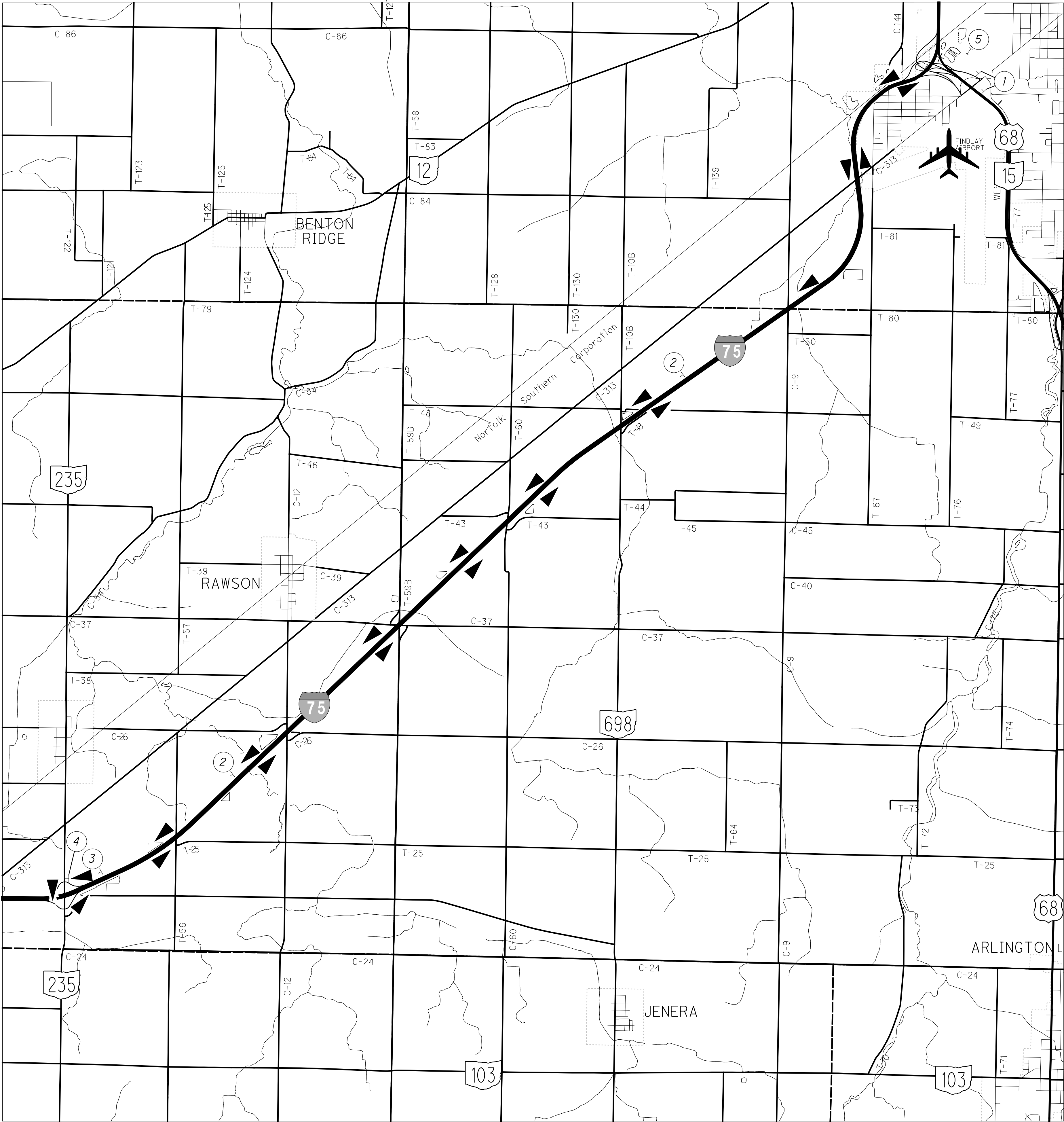
US 68 NB/SR 15 WB and Lima Ave. Exit Ramp to IR 75 SB Detour



Type III Barricades Per MT-101.60

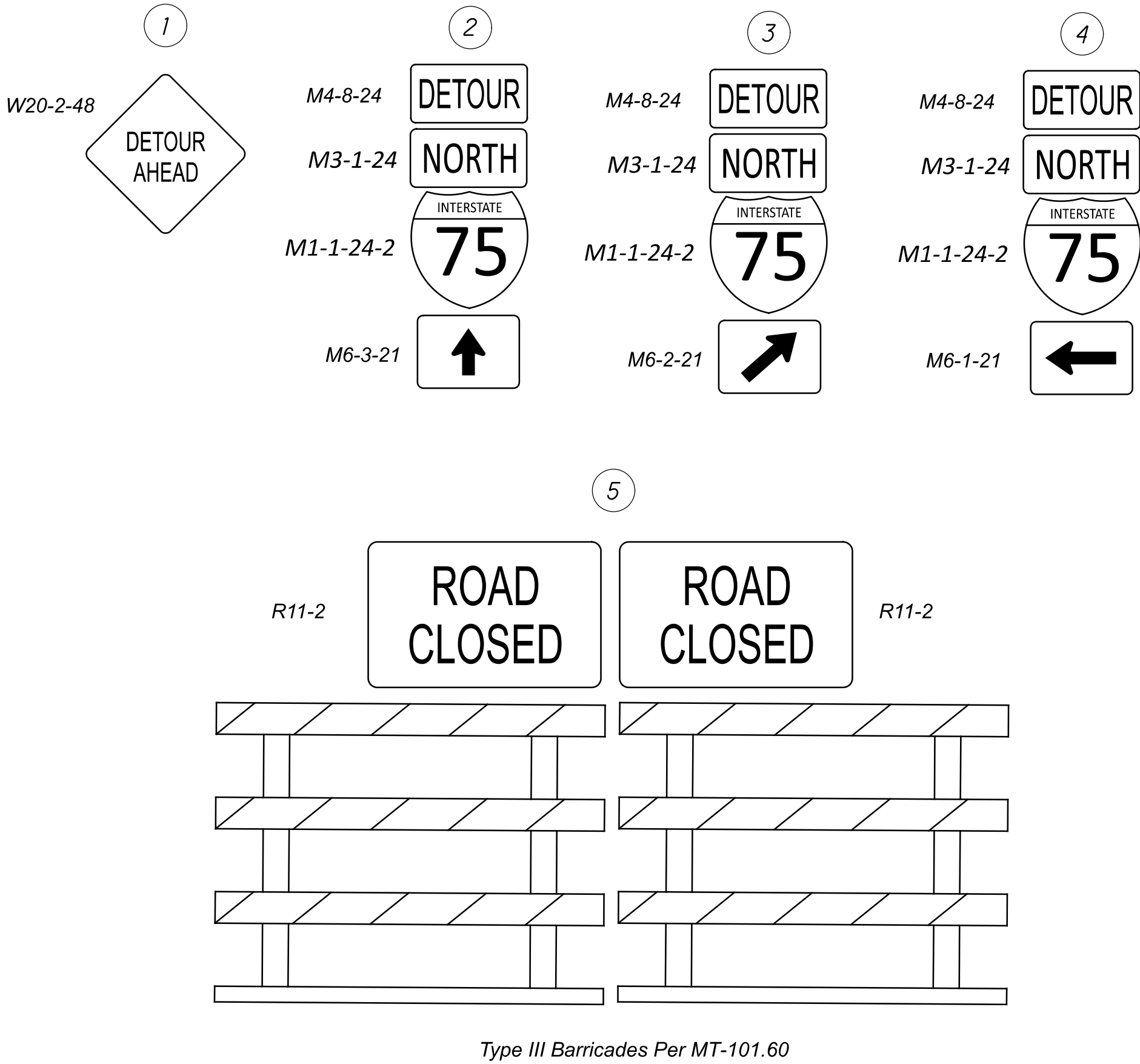


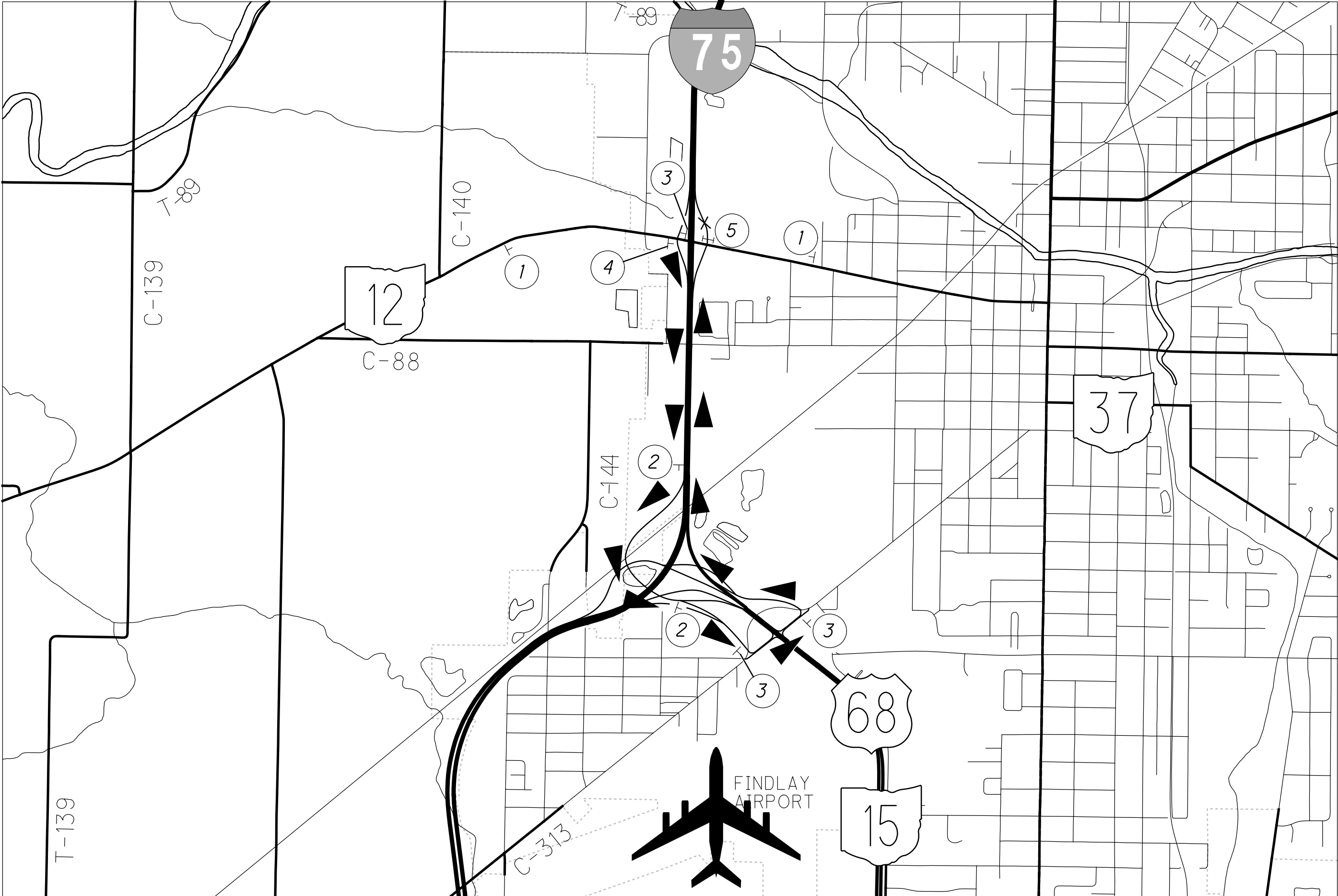




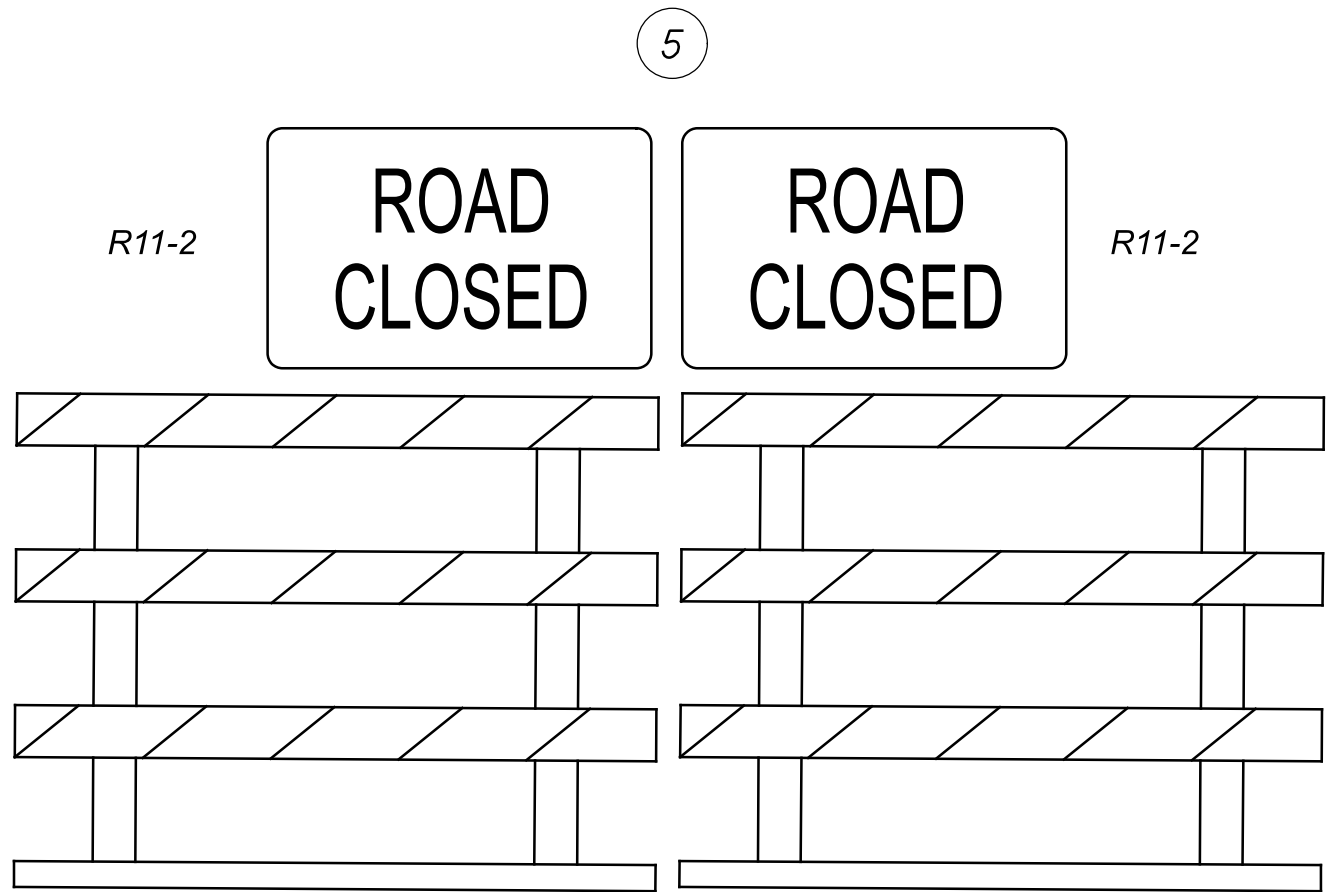
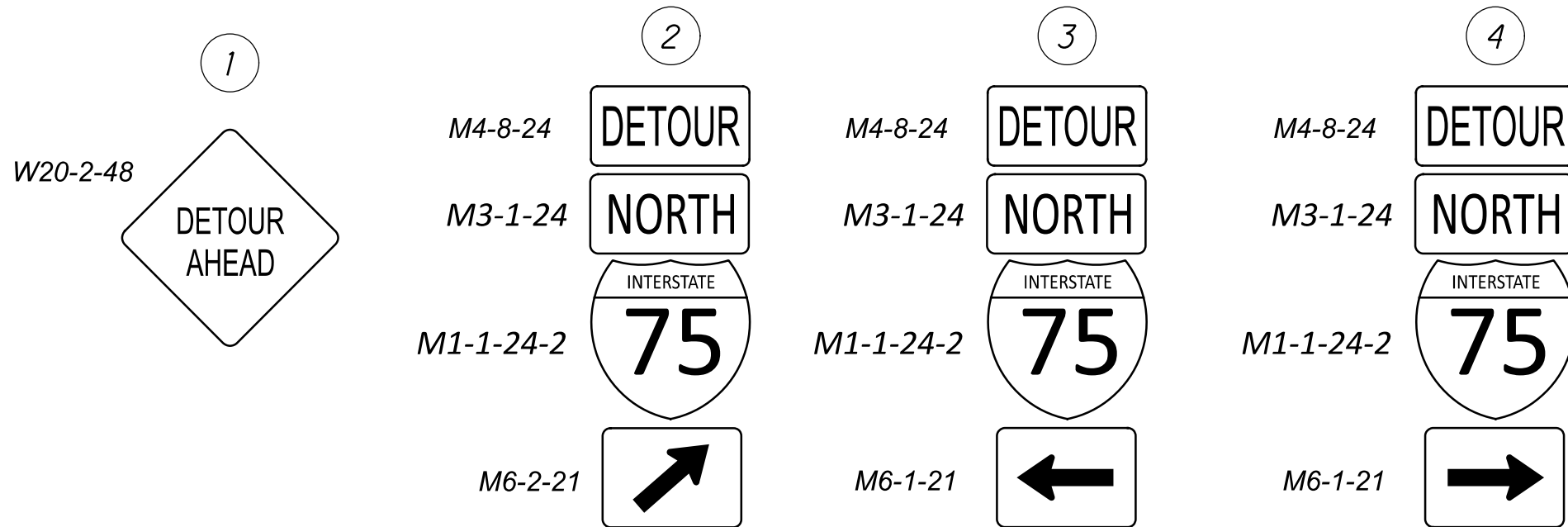
Note: Place barricade (Item number 5) at US 68/SR 15 NB ramp and Lima Ave. NB ramp to IR 75.

US 68 NB/SR 15 WB and Lima Ave. Entrance Ramp to IR 75 NB Detour

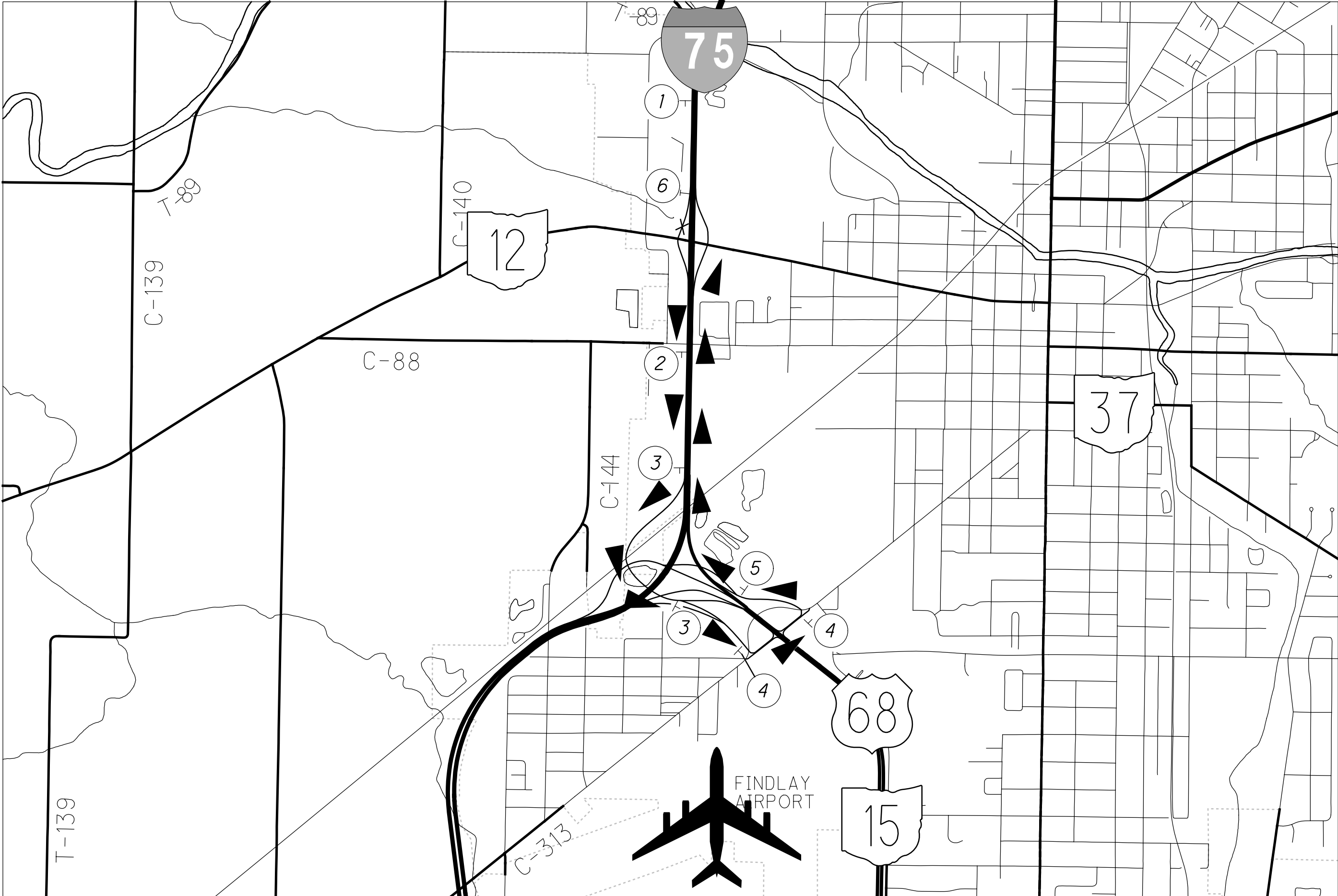




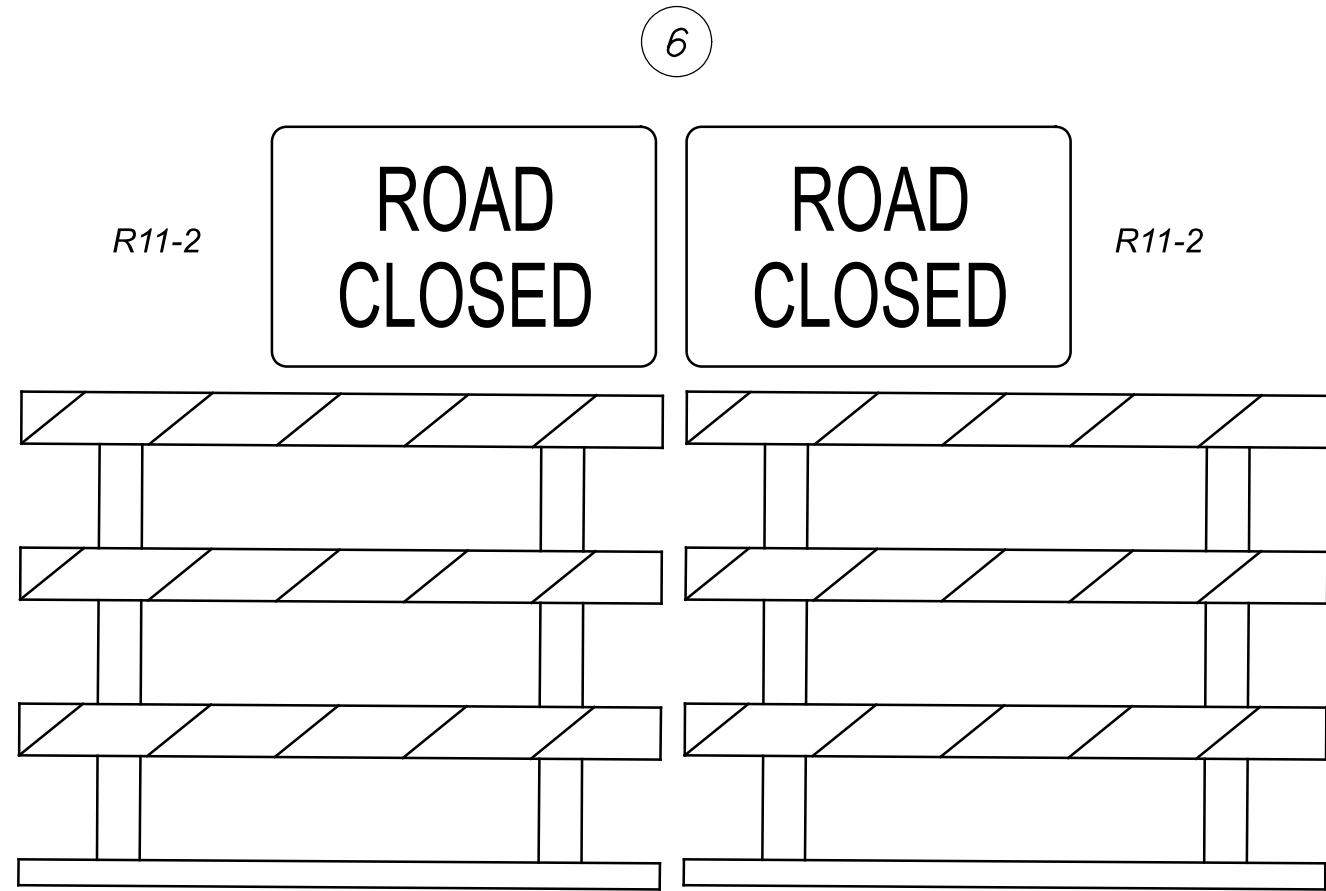
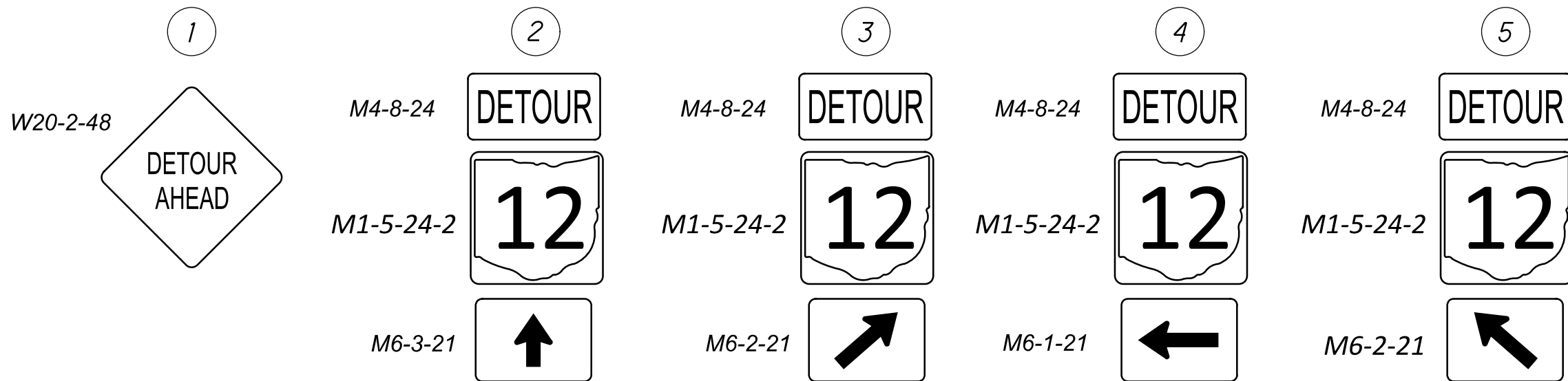
SR 12 Entrance Ramp to IR 75 NB Detour



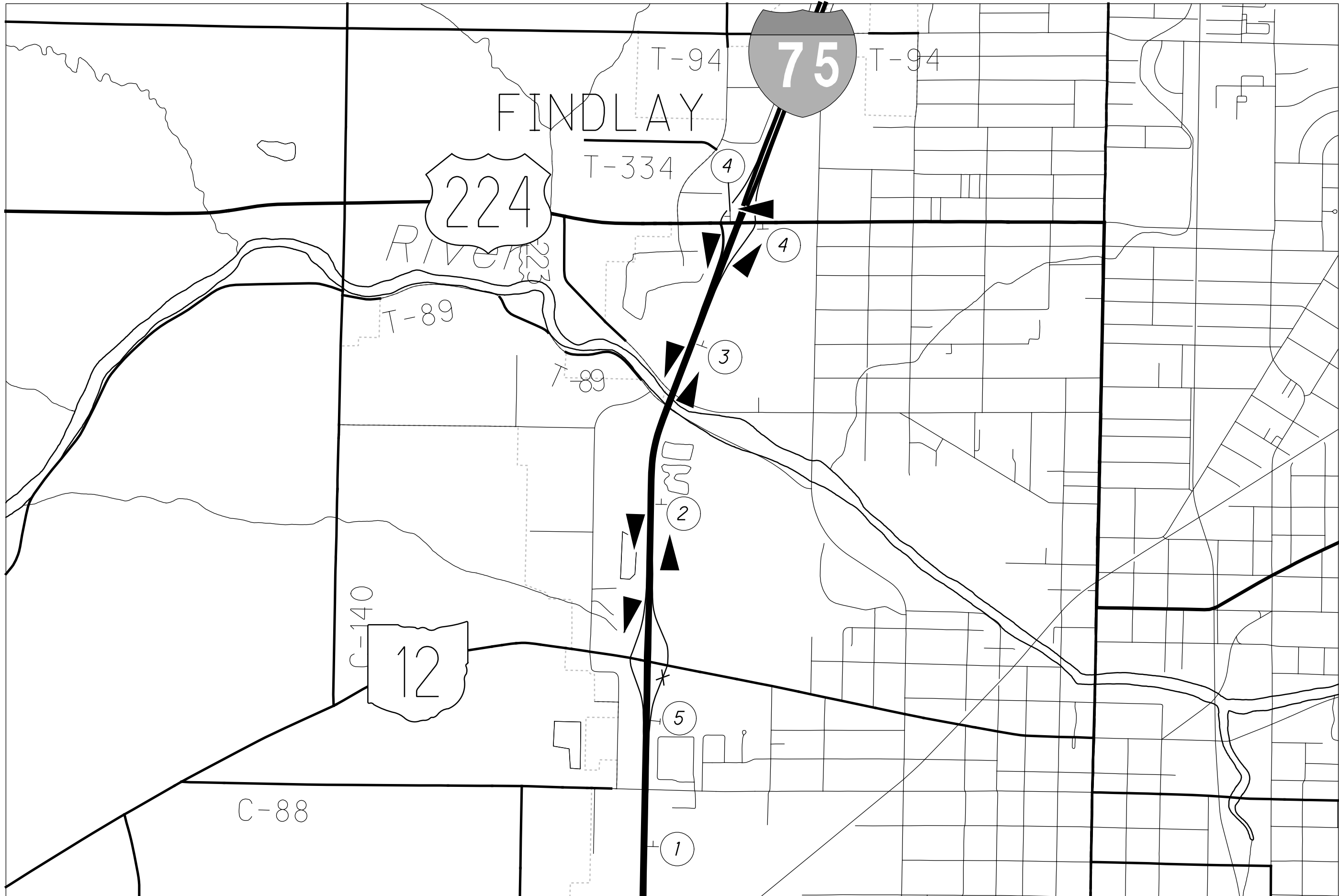
Type III Barricades Per MT-101.60



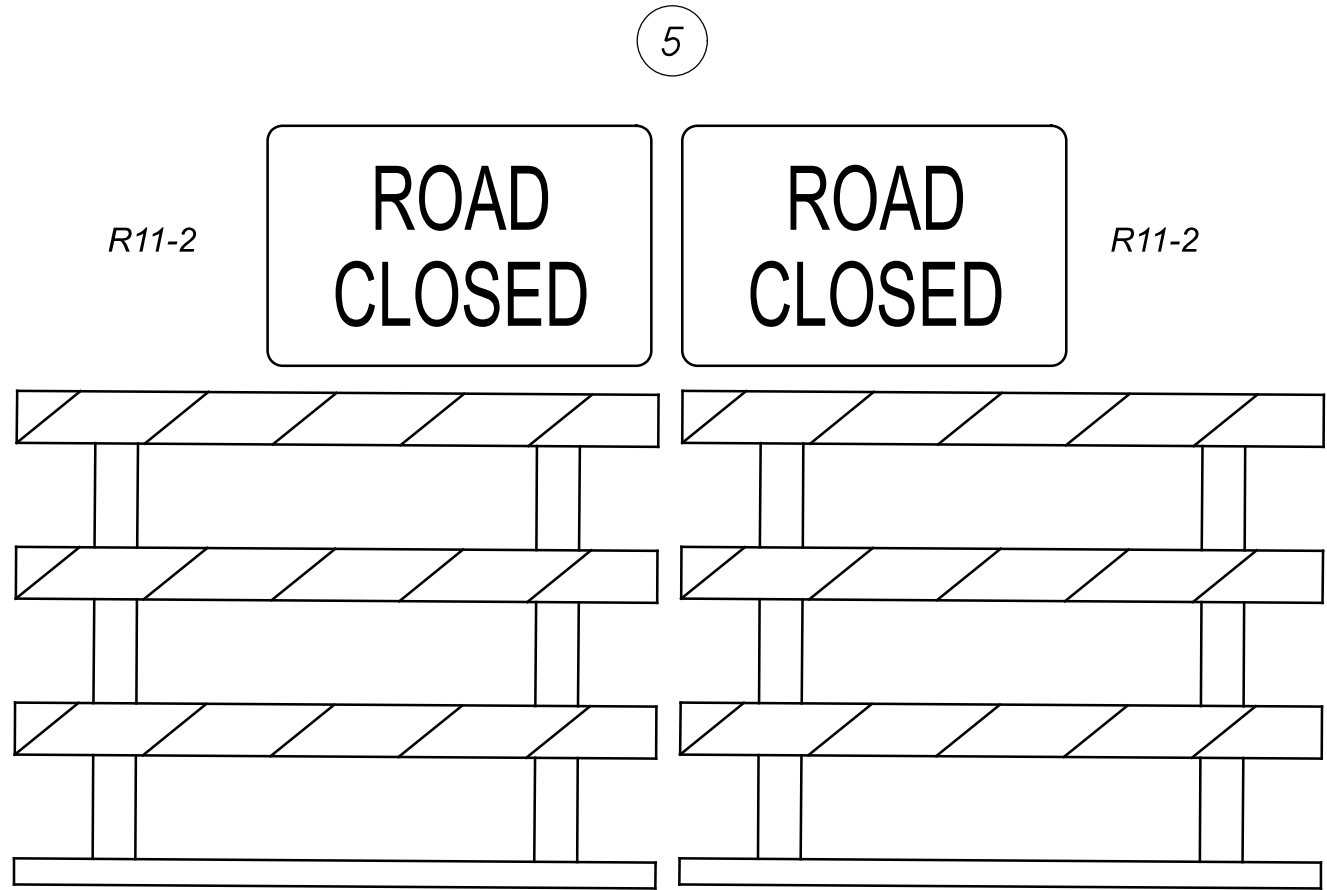
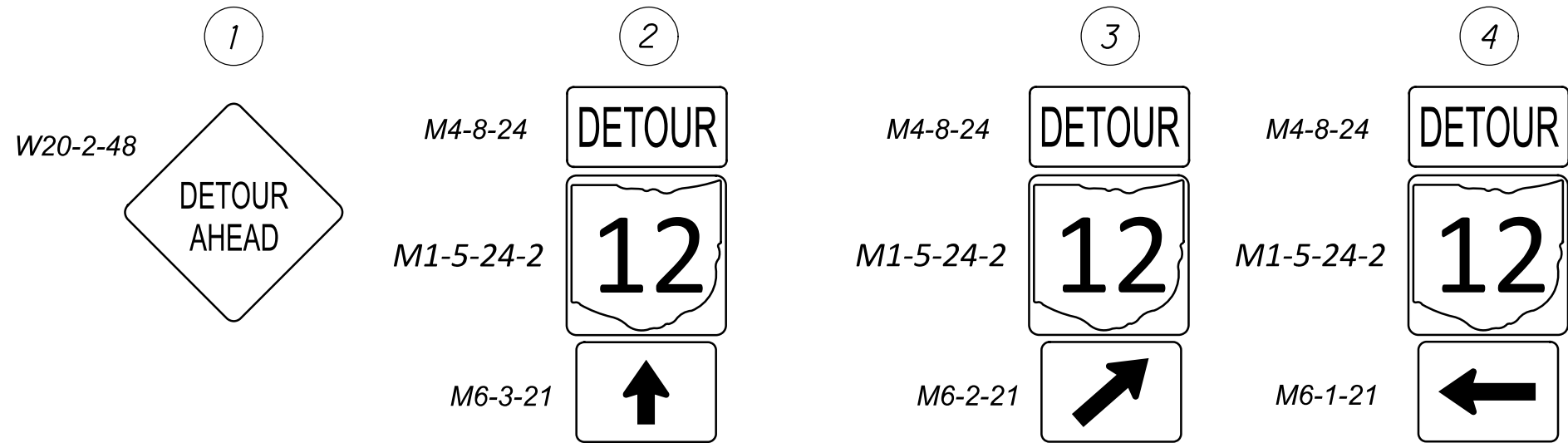
IR 75 SB Exit Ramp to SR 12 Detour



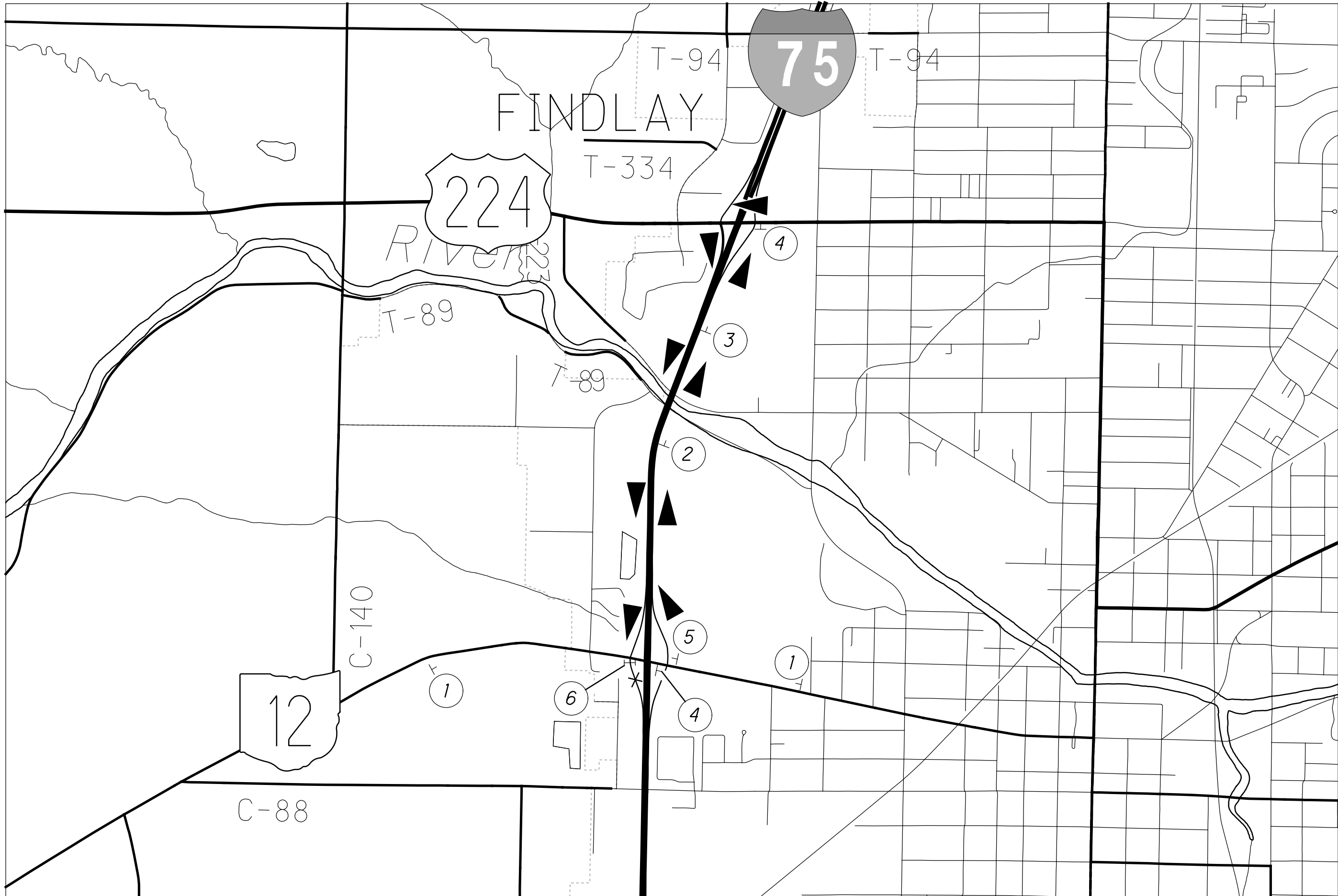
Type III Barricades Per MT-101.60



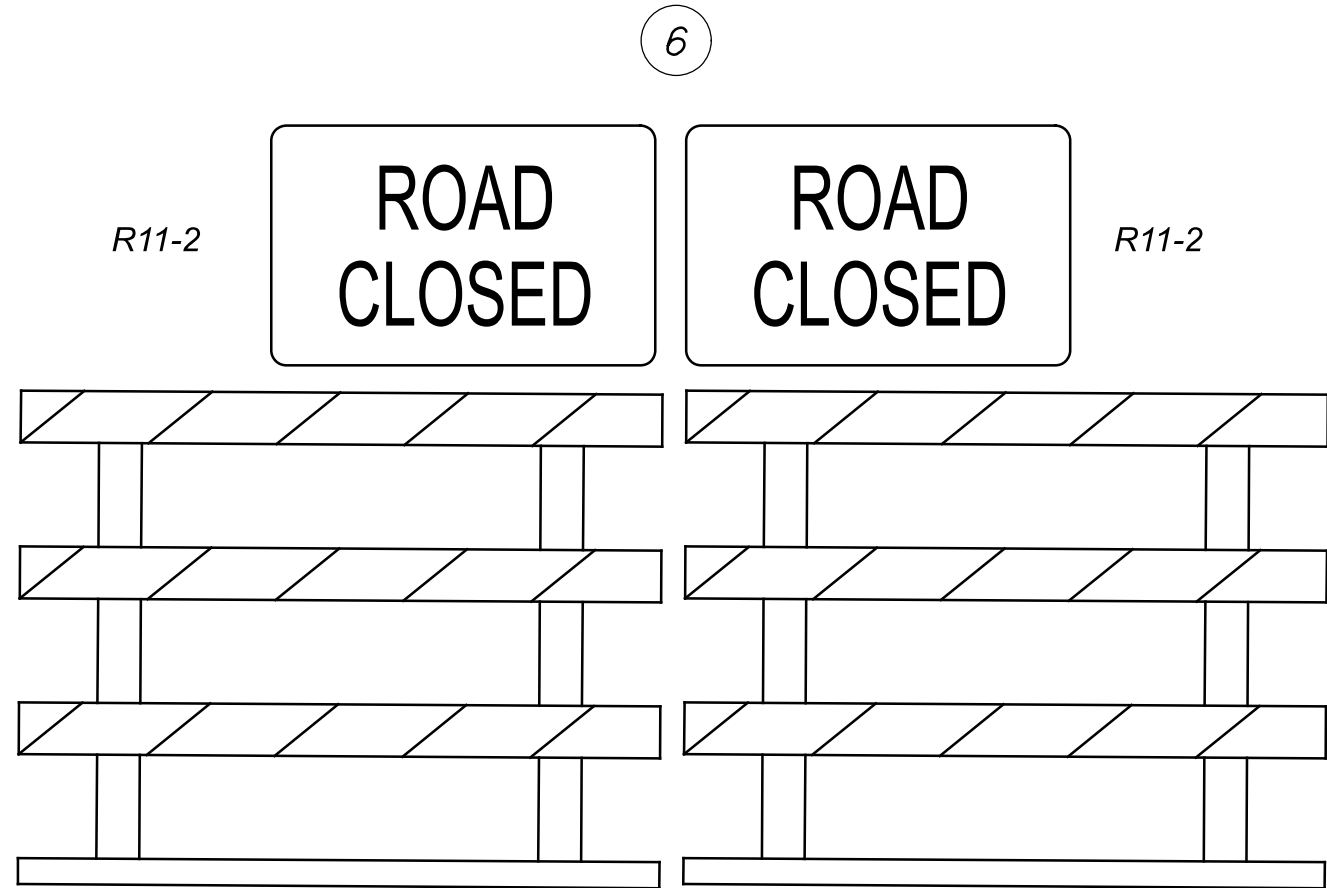
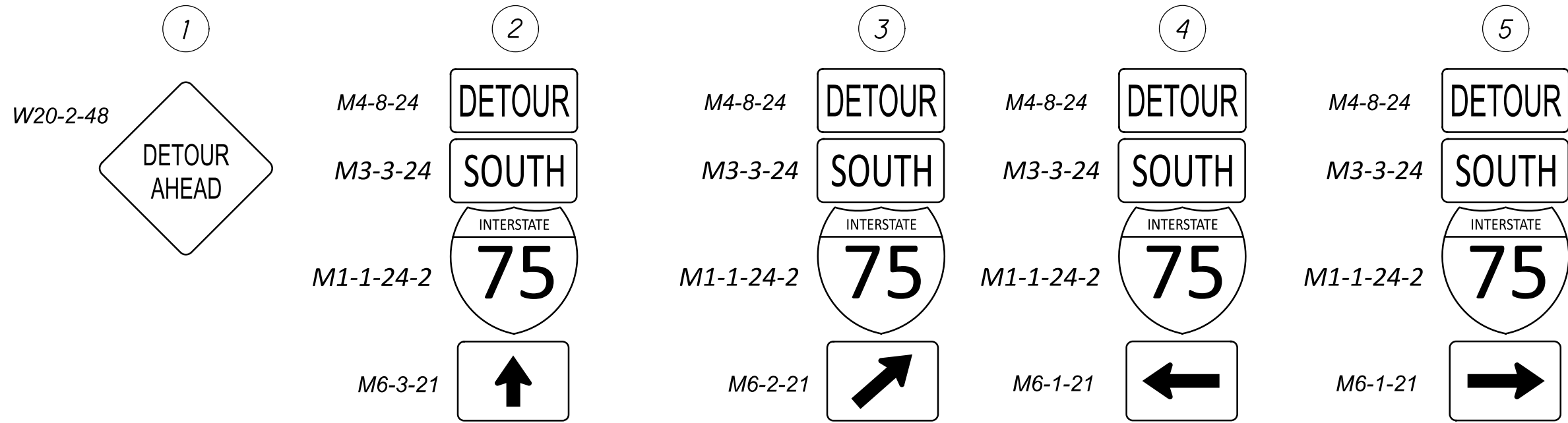
IR 75 NB Exit Ramp to SR 12 Detour



Type III Barricades Per MT-101.60



SR 12 Entrance Ramp to IR 75 SB Detour



Type III Barricades Per MT-101.60

ALL/HAN-75-0.00/14.10

MODEL: Sheet PAPERSIZE: 34x22 (in.) DATE: 8/28/2025 TIME: 3:55:54 PM PLTDRV: OHDOT_PDF.pltG USER: isaac.ricker@dot.ohio.gov WORKSPACE: OHDOTDev02 WORKSET: 120987 PRODUCT: OpenRoadsDesigner 24.00.00.205
 pw:\lohtodol-pw-bentley.com:ohiodot-pw-02\Documents\01 Active Projects\District 01\Allen\120987\7400-Engineering\Roadway\Sheets\120987 GG001.dgn

[illegible]

GENERAL SUMMARY

DESIGN AGENCY



DESIGNER

GLI

REVIEWER

MDR 06 23

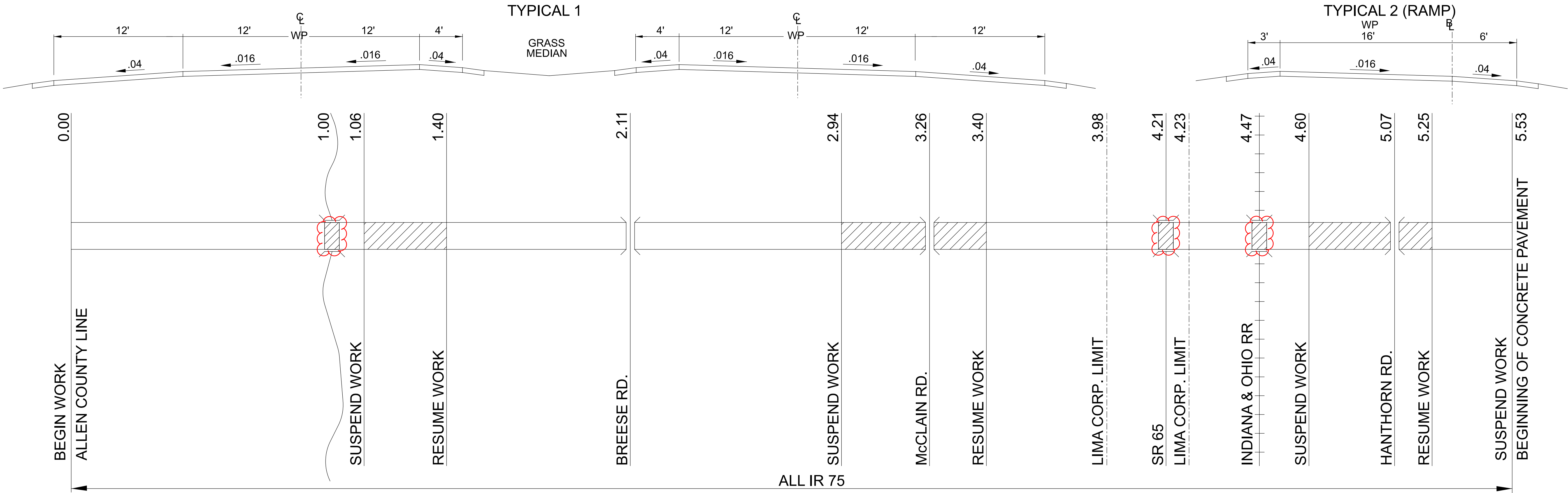
IVH D 00 23

PROJECT ID

120987

SHEET	TOTAL
-------	-------

16 | Page



NOTE: DRAWINGS NOT TO SCALE

NOTE: ALL TOTALS CARRIED TO GENERAL SUMMARY

PAVEMENT DATA

ROUTE	FROM	TO	DISTANCE		PAVT WIDTH	TYPICAL SECTION	PAVT AREA	407	421	
								TACK COAT **	MICRO-SURFACING, LEVELING COURSE	MICRO-SURFACING, SURFACE COURSE
	SLM	SLM	MILES	FEET	FEET		SQ YD	GAL	SQ YD	SQ YD
ALL IR 75	0.00	0.96	0.96	5069	40 * (2)	1	45056	4055	27034	45056
ALL IR 75	0.96	1.04	Bridge No Work							
ALL IR 75	1.04	1.06	0.02	106	40 * (2)	1	939	84	563	939
ALL IR 75	1.06	1.40	No Work							
ALL IR 75	1.40	2.94	1.54	8131	40 * (2)	1	72277	6505	43366	72277
ALL IR 75	Extra areas (Breese Rd. Ramps)						6934	624		6934
ALL IR 75	2.94	3.40	No Work							
ALL IR 75	3.40	4.19	0.79	4171	40 * (2)	1	37077	3337	22246	37077
ALL IR 75	4.19	4.27	Bridge No Work							
ALL IR 75	4.27	4.44	0.17	898	40 * (2)	1	7979	718	4787	7979
ALL IR 75	4.44	4.52	Bridge No Work							
ALL IR 75	4.52	4.60	0.08	422	40 * (2)	1	3755	338	2253	3755
ALL IR 75	Extra areas (SR 65 Ramps)						7636	687		7636
ALL IR 75	4.60	5.25	No Work							
ALL IR 75	5.25	5.53	0.28	1478	40 * (2)	1	13141	1183	7885	13141
Totals								17531	108134	194794

** Not carried to General Summary, item is incidental to Microsurfacing per CMS 421.13. The quantity is provided for informational purposes only and for Leveling Course application.

NOTE: FOR BRIDGE SUSPENSION OF WORK LIMITS, SEE SHEETS 21-23.

MICROSURFACING

DESIGN AGENCY



DESIGNER

GLI

REVIEWER

MPB 06-23-25

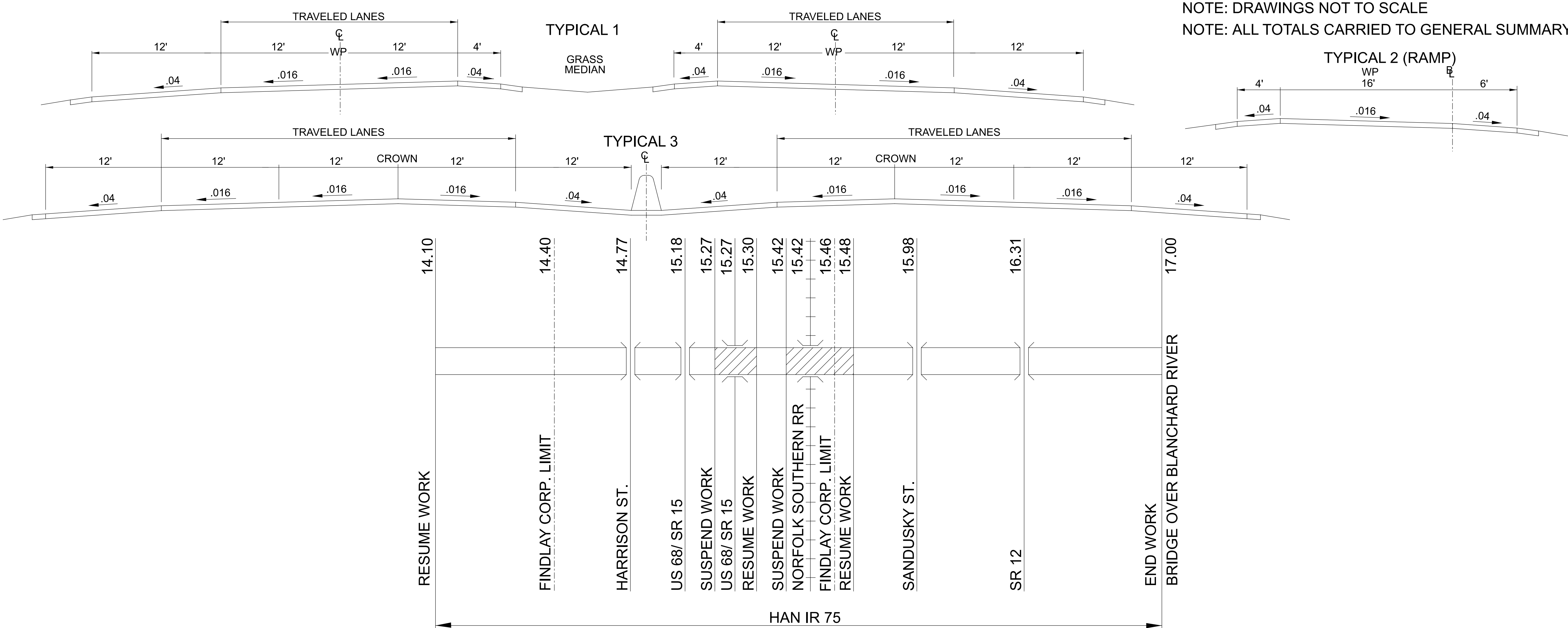
PROJECT NO.

120987

SHEET TOTAL

17

23



PAVEMENT DATA

ROUTE	FROM	TO	DISTANCE		PAVT WIDTH	TYPICAL SECTION	PAVT AREA	407	421	
								TACK COAT **	MICRO-SURFACING, LEVELING COURSE	MICRO-SURFACING, SURFACE COURSE
									24' TO 36' WIDE (PLACED ON TRAVELED LANES)	TOTAL WIDTH OF PAVEMENT
	SLM	SLM	MILES	FEET	FEET		SQ YD	GAL	SQ YD	SQ YD
HAN IR 75	14.10	14.45	0.35	1848	40 * (2)	1	16427	1478	9856	16427
HAN IR 75	Extra area (lane taper SB only)						2259	203	1825	2259
HAN IR 75	14.45	15.27	0.82	4330	60 * (2)	3	57728	5196	34637	57728
HAN IR 75	Extra areas (US 68 & SR 15 Ramps)						45043	4054		45043
HAN IR 75	15.27	15.30	No Work							
HAN IR 75	15.30	15.42	0.12	634	60 * (2)	3	8448	760	5069	8448
HAN IR 75	15.42	15.48	No Work							
HAN IR 75	15.48	17.00	1.52	8026	60 * (2)	3	107008	9631	64205	107008
HAN IR 75	Extra areas (SR 12 Ramps)						7874	709		7874
Totals								22031	115591	244787

** Not carried to General Summary, item is incidental to Microsurfacing per CMS 421.13. The quantity is provided for informational purposes only and for Leveling Course application.

MICROSURFACING

DESIGN AGENCY



DESIGNER

GLI

REVIEWER

MPB 06-23-25

PROJECT ID

120987

SHEET

18

TOTAL

23

ALL/HAN-75-0.00/14.10

MODEL: Sheet PAPERSIZE: 34x22 (in.) DATE: 8/28/2025 TIME: 3:56:49 PM PLTDRV: OHDOT_PDF.plt PENTBL: OHDOT Pen.tbl USER: Isaac.Ricker@dol.ohio.gov WORKSPACE: OHDOTCv02 WORKSET: 120987 PRODUCT: OpenRoadsDesigner 24.00.00.205
 pw://ohdotd-pw-bentley.com:ohdotd-pw-02/Documents/01 Active Projects/District 01/Vallen/120987400-Engineering/Roadway/Sheets/120987 GS003.dgn

S.L.M.			DESCRIPTION	DISTANCE		621			807					850				643			644	814
						RAISED PAVEMENT MARKER REMOVED	RPM	RPM	WET REFLECTIVE TRAFFIC PAINT, EDGE LINE, 6"	WET REFLECTIVE TRAFFIC PAINT, EDGE LINE, 6"	WET REFLECTIVE TRAFFIC PAINT, LANE LINE, 6"	WET REFLECTIVE TRAFFIC PAINT, CHANNELIZING LINE, 12"	WET REFLECTIVE TRAFFIC PAINT, DOTTED LINE, 6"	GROOVING FOR 6" RECESSED PAVEMENT MARKING, (ASPHALT)	GROOVING FOR 6" RECESSED PAVEMENT MARKING, (ASPHALT)	GROOVING FOR 6" RECESSED PAVEMENT MARKING, (CONCRETE)	GROOVING FOR 12" RECESSED PAVEMENT MARKING, (ASPHALT)	REMOVAL OF PAVEMENT MARKING	REMOVAL OF PAVEMENT MARKING	REMOVAL OF PAVEMENT MARKING	LANE REDUCTION ARROW	INTERSTATE ELONGATED ROUTE SHIELD MARKINGS, TYPE B125
				MILES	FT	EA	EA	EA	MILE	MILE	MILE	FT	FT	MILE	FT	MILE	FT	FT	EA	MILE	EA	EA
			MAINLINE ALL I.R. 75 (NB)																			
0.00	TO	0.96		0.96	5068.80	63	63		0.96	0.96	0.96			2.88						2.88		
0.96		1.04	MAINLINE STRUCTURE OVER LITTLE OTTAWA RIVER	0.08	422.40				0.08	0.08	0.08					0.24						
1.04		1.06		0.02	105.60	1	1		0.02	0.02	0.02			0.06						0.06		
1.06		1.40	NO MICRO-SURFACING WORK	0.34	1795.20				0.34	0.34	0.34			1.02								
1.40		2.94		1.54	8131.20	102	102		1.54	1.54	1.54			4.62						4.62		
2.94		3.40	NO MICRO-SURFACING WORK	0.46	2428.80				0.46	0.46	0.46			1.38								
3.40		4.19		0.79	4171.20	52	52		0.79	0.79	0.79			2.37						2.37		
4.19		4.27	MAINLINE STRUCTURE OVER SR 65	0.08	422.40				0.08	0.08	0.08					0.24						
4.27		4.43		0.16	844.80	11	11		0.16	0.16	0.16			0.48						0.48		
4.43		4.53	MAINLINE STRUCTURE OVER INDIANA & OHIO RR	0.10	528.00				0.10	0.10	0.10					0.30						
4.53		4.60		0.07	369.60	5	5		0.07	0.07	0.07			0.21						0.21		
4.60		5.25	NO MICRO-SURFACING WORK	0.65	3432.00				0.65	0.65	0.65			1.95								
5.25		5.53		0.28	1478.40	18	18		0.28	0.28	0.28			0.84						0.84		
			MAINLINE ALL I.R. 75 (SB)																			
0.00	TO	0.96		0.96	5068.80	63	63		0.96	0.96	0.96			2.88						2.88		
0.96		1.04	MAINLINE STRUCTURE OVER LITTLE OTTAWA RIVER	0.08	422.40				0.08	0.08	0.08					0.24						
1.04		1.06		0.02	105.60	1	1		0.02	0.02	0.02			0.06						0.06		
1.06		1.40	NO MICRO-SURFACING WORK	0.34	1795.20				0.34	0.34	0.34			1.02								
1.40		2.94		1.54	8131.20	102	102		1.54	1.54	1.54			4.62						4.62		
2.94		3.40	NO MICRO-SURFACING WORK	0.46	2428.80				0.46	0.46	0.46			1.38								
3.40		4.19		0.79	4171.20	52	52		0.79	0.79	0.79			2.37						2.37		
4.19		4.27	MAINLINE STRUCTURE OVER SR 65	0.08	422.40				0.08	0.08	0.08					0.24						
4.27		4.43		0.16	844.80	11	11		0.16	0.16	0.16			0.48						0.48		
4.43		4.53	MAINLINE STRUCTURE OVER INDIANA & OHIO RR	0.10	528.00				0.10	0.10	0.10					0.30						
4.53		4.60		0.07	369.60	5	5		0.07	0.07	0.07			0.21						0.21		
4.60		5.25	NO MICRO-SURFACING WORK	0.65	3432.00				0.65	0.65	0.65			1.95								
5.25		5.53		0.28	1478.40	18	18		0.28	0.28	0.28			0.84						0.84		
			MAINLINE HAN I.R. 75 (NB)																			
14.10		14.45		0.35	1848.00	23	23		0.35	0.35	0.35		112	1.05	112					1.05		
14.45		15.27		0.82	4329.60	108	108		0.82	0.82	1.64			3.28						3.28		4
15.27		15.30	MAINLINE STRUCTURE OVER US 68	0.03	158.40				0.03	0.03	0.06			0.48		0.12						
15.30		15.42		0.12	633.60	16	16		0.12	0.12	0.24			0.48						0.48		
15.42		15.48	MAINLINE STRUCTURE OVER NS RR	0.06	316.80				0.06	0.06	0.12			0.48		0.24						
15.48		17.00		1.52	8025.60	201	201		1.52	1.52	3.04			6.08						6.08		4
			MAINLINE HAN I.R. 75 (SB)																			
14.10		14.45		0.35	1848.00	23	23		0.35	0.35	0.53		835	1.23	835				2	1.23	2	
14.45		15.27		0.82	4329.60	108	108		0.82	0.82	1.64			3.28					1	3.28	1	
15.27		15.30	MAINLINE STRUCTURE OVER US 68	0.03	158.40				0.03	0.03	0.06			0.48		0.12						
15.30		15.42		0.12	633.60	16	16		0.12	0.12	0.24			0.48						0.48		
15.42		15.48	MAINLINE STRUCTURE OVER NS RR	0.06	316.80				0.06	0.06	0.12			0.48		0.24						
15.48		17.00		1.52	8025.60	201	201		1.52	1.52	3.04			6.08						6.08		
																</						

PAVEMENT MARKING SUBSUMMARY

DESIGN AGENCY



DESIGNER

GLI

REVIEWER

MDR 06 23

1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
---	---	---	---	---	---	---	---	---	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	----	-----

PROJECT ID
100005

120987

SHEET TOTAL

19 | 2

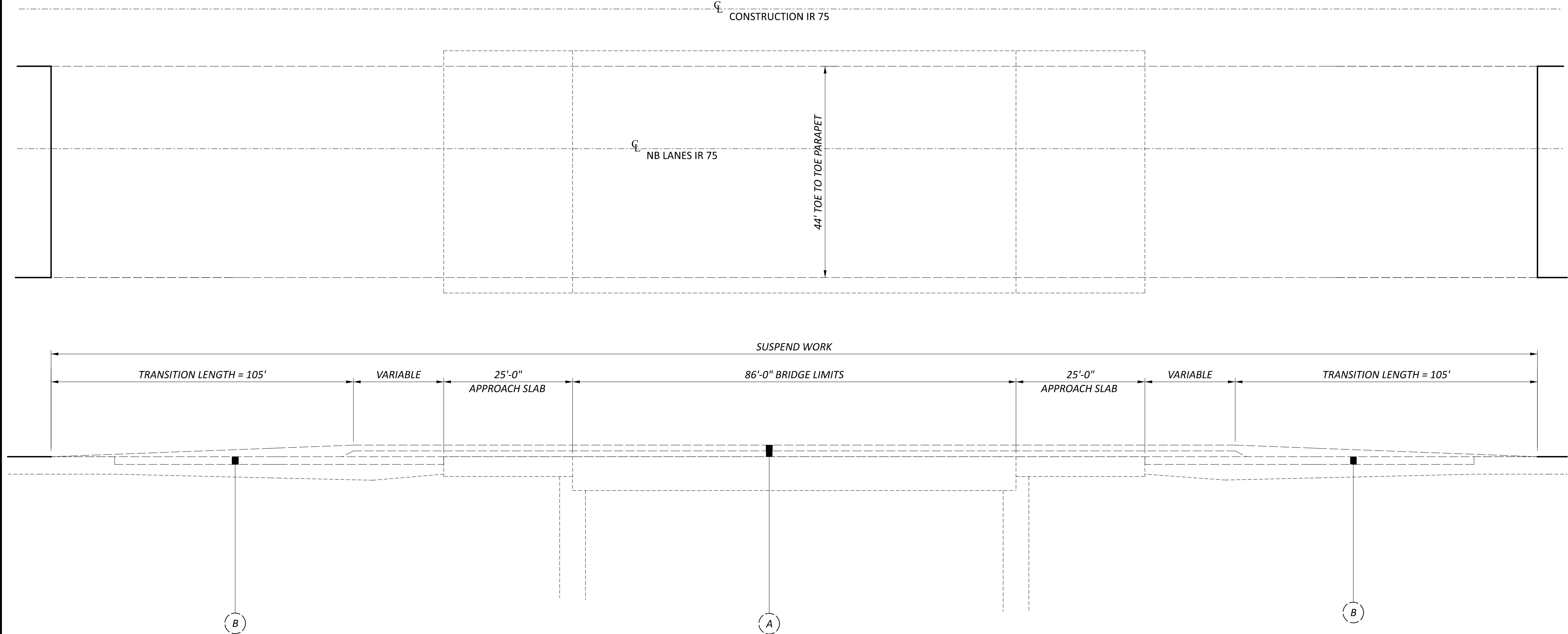
DESCRIPTION		DISTANCE		621			807					850				643			644	814
				RAISED PAVEMENT MARKER REMOVED	RPM	RPM	WET REFLECTIVE TRAFFIC PAINT, EDGE LINE, 6"	WET REFLECTIVE TRAFFIC PAINT, EDGE LINE, 6"	WET REFLECTIVE TRAFFIC PAINT, LANE LINE, 6"	WET REFLECTIVE TRAFFIC PAINT, CHANNELIZING LINE, 12"	WET REFLECTIVE TRAFFIC PAINT, DOTTED LINE, 6"	GROOVING FOR 6" RECESSED PAVEMENT MARKING, (ASPHALT)	GROOVING FOR 6" RECESSED PAVEMENT MARKING, (ASPHALT)	GROOVING FOR 6" RECESSED PAVEMENT MARKING, (CONCRETE)	GROOVING FOR 12" RECESSED PAVEMENT MARKING, (ASPHALT)	REMOVAL OF PAVEMENT MARKING	REMOVAL OF PAVEMENT MARKING	REMOVAL OF PAVEMENT MARKING	LANE REDUCTION ARROW	INTERSTATE ELONGATED ROUTE SHIELD MARKINGS, TYPE B125
MILES	FT	EA	EA	EA	MILE	MILE	MILE	FT	FT	MILE	FT	MILE	FT	FT	EA	MILE	EA	EA		
					W/R	Y/R	WHITE	YELLOW												
INTERCHANGE RAMPS																				
ALL IR 75 NB EXIT TO BREESE RD.				21	21				848	387		387		848	1235					
ALL IR 75 NB ENTRANCE FROM BREESE RD.				12	12				800	1033		1033		800	1833					
ALL IR 75 SB ENTRANCE FROM BREESE RD.				11	11				842	965		965		842	1807					
ALL IR 75 SB EXIT TO BREESE RD.				15	15				620	542		542		620	1162					
ALL IR 75 NB EXIT TO SR 65				19	19				707	448		448		707	1155					
ALL IR 75 NB ENTRANCE FROM SR 65				7	7				1052	956		956		1052	1484					
ALL IR 75 SB ENTRANCE FROM SR 65				12	12				910	975		975		910	1885					
ALL IR 75 SB EXIT TO SR 65				17	17				620	562		562		620	915					
HAN IR 75 NB EXIT TO US 68 SB				94	60	34	0.46	0.52	2193	1323		1323		2193	3516		0.98			
HAN IR 75 NB ENTRANCE FROM US 68 NB				53	32	21	0.44	0.31	2824	1419		1419		2824	3226		0.63			
HAN IR 75 SB ENTRANCE FROM US 68 NB				99	55	44	0.60	0.66	2600	1764		1764		2600	4364		1.26			
HAN IR 75 SB EXIT TO US 68 SB				76	34	42	0.69	0.63	1372	1287		1287		1372	2087		0.98			
HAN IR 75 NB EXIT TO SR 12				15	15				544	648		648		544	1192					
HAN IR 75 NB ENTRANCE FROM SR 12				10	10				733	1170		1170		733	1903					
HAN IR 75 SB ENTRANCE FROM SR 12				10	10				731	1132		1132		731	1863					
HAN IR 75 SB EXIT TO SR 12				15	15				519	637		637		519	1156					
	SUBTOTALS THIS SHEET			486	345	141	2.19	2.12	0.00	17915	15248	0.00	15248	0	17915	30783	0	3.85	0	0
	SUBTOTALS SHEET 19			1200	1200	0	16.86	16.86	22.14	0	947	53.58	947	2.28	0	0	3	44.88	3	8
TOTALS CARRIED TO GEN. SUMMARY				1686	1545	141	19.05	18.98	22.14	17915	16195	53.58	16195	2.28	17915	30783	3	48.73	3	8

PAVEMENT MARKING SUBSUMMARY

DESIGN AGENCY	
	
DESIGNER	
GLI	
REVIEWER	
MPB	06-23-25
PROJECT ID	
120987	
SHEET	TOTAL
20	23

LEGEND

- (A) - 3" EX. BRIDGE DECK WATERPROOFING ASPHALT CONCRETE
- (B) - EX. VARIABLE DEPTH ASPHALT CONCRETE (2" AVG.)

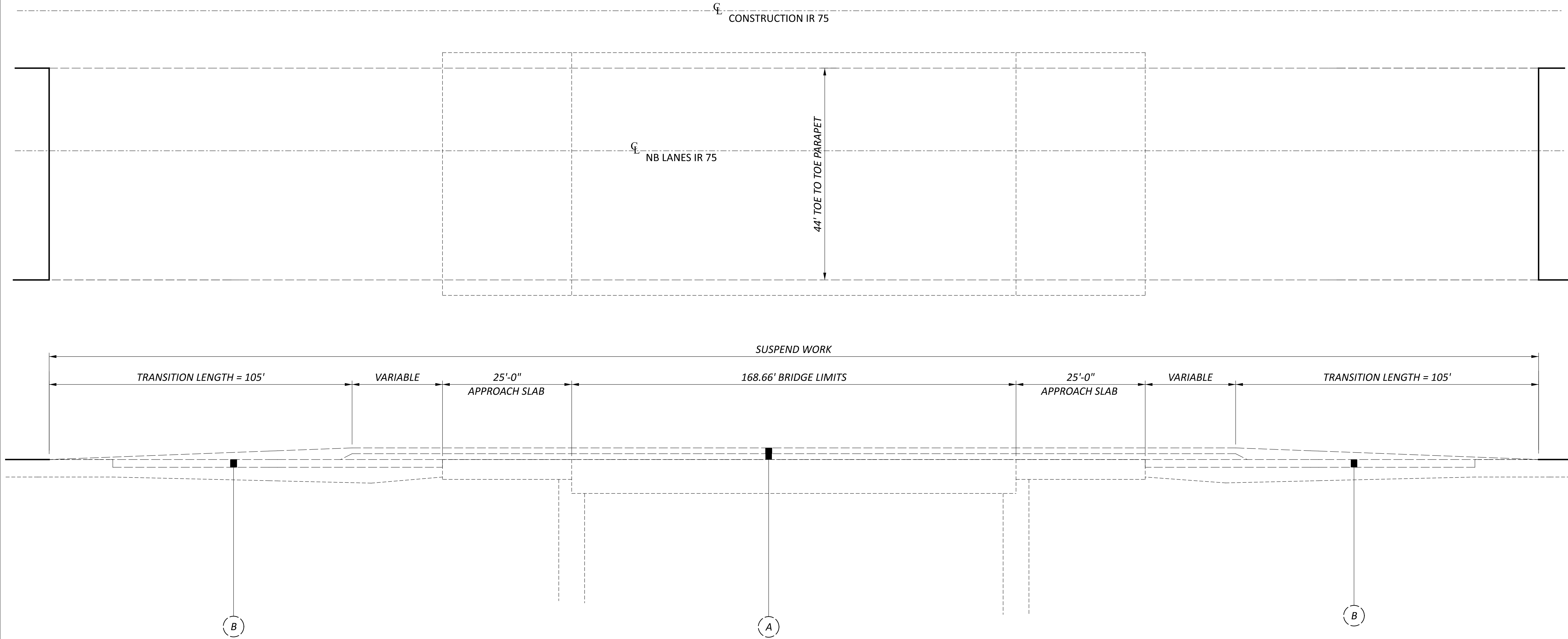


DETAIL TYPICAL FOR L & R STRUCTURE
STRUCTURE NO. ALL-75-1.00 L & R

NOTE: DRAWINGS NOT TO SCALE

STRUCTURE DETAILS
ALL-75-1.00 L & R
IR-75 OVER LITTLE OTTAWA RIVER

SFN	0201634
SFN	0201669
DESIGN AGENCY	
DESIGNER	CHECKER
ISR	TLI
REVIEWER	
TLI	09-22-25
PROJECT ID	120987
SUBSET	TOTAL
1	3
SHEET	TOTAL
21	23



DETAIL TYPICAL FOR L & R STRUCTURE
STRUCTURE NO. ALL-75-4.23 L & R

LEGEND

- (A) - 3" EX. BRIDGE DECK WATERPROOFING ASPHALT CONCRETE
- (B) - EX. VARIABLE DEPTH ASPHALT CONCRETE (2" AVG.)

NOTE: DRAWINGS NOT TO SCALE

STRUCTURE DETAILS
ALL-75-4.23 L & R
IR-75 OVER SR 65

SFN	0201812
SFN	0201820
DESIGN AGENCY	
DESIGNER	CHECKER
ISR	TLI
REVIEWER	
TLI	09-22-25
PROJECT ID	
120987	
SUBSET	TOTAL
2	3
SHEET	TOTAL
22	23

ALL/HAN-75-0.00/14.10

(A) - 3" EX. BRIDGE DECK WATERPROOFING ASPHALT CONCRETE

(B) - EX. VARIABLE DEPTH ASPHALT CONCRETE (2" AVG.)

DETAIL TYPICAL FOR L & R STRUCTURE
STRUCTURE NO. ALL-75-4.48 L & R

NOTE: DRAWINGS NOT TO SCALE

STRUCTURE DETAILS
ALL-75-4.48 L & R
IR-75 OVER INDIANA & OHIO RR

SFN
0201871

SFN	0201901
-----	---------



DESIGNER	CHECKER
ISR	TLI

REVIEWER	
TLI	09-22-25

PROJECT ID	120987
------------	--------

SUBSET	TOTAL
3	3

SHEET	TOTAL
23	23