

SEE SHEET 2 FOR LOCATION MAP

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

AUG/MER-PAVE-FY26

ST. MARYS & NOBLE TOWNSHIPS (AUG)
WASHINGTON, RECOVERY, LIBERTY & BUTLER TOWNSHIPS (MER)
JEFFERSON, MARION, BLACK CREEK & DUBLIN TOWNSHIPS (MER)
VILLAGES OF COLDWATER & ROCKFORD

AUGLAIZE COUNTY
MERCER COUNTY

FEDERAL PROJECT NUMBER

E170 (181)

RAILROAD INVOLVEMENT

R J CORMAN RAILROAD

PROJECT DESCRIPTION

THIS PROJECT CONSISTS OF RESURFACING WITH ASPHALT CONCRETE OF SR 29, SR 49, SR 118, SR 274 & SR 707 IN MERCER COUNTY AND SR 364 AND SR 703 IN AUGLAIZE COUNTY. THE PROJECT ALSO INCLUDES SOME FULL DEPTH RECONSTRUCTION ON USR 127 IN MERCER COUNTY.

PROJECT LOCATIONS

PARTICIPATION 1 = 01/STR
AUG-364-SLM 15.24 TO 15.42
AUG-364I-SLM 0.00 TO 0.19
MER-127-SLM 7.55 TO 7.60
MER-29-SLM 0.00 TO 5.99
MER-49-SLM 5.05 TO 15.43
MER-118-SLM 10.39 TO 13.42
MER-274-SLM 0.00 TO 3.79
MER-707-SLM 0.00 TO 8.21

PARTICIPATION 3 = 03/NHS
MER-33-SLM 6.45

PARTICIPATION 2 = 02/S-2
AUG-364-SLM 15.06 TO 15.24
AUG-703-SLM 0.00 TO 0.59
MER-118-SLM 8.35 TO 10.39

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: NA*
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: NA*
NOTICE OF INTENT EARTH DISTURBED AREA: NA*

* MAINTENANCE PROJECT

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS, CHANGES LISTED IN THE PROPOSAL, AND THE SUPPLEMENTAL SPECIFICATION 800 VERSION INDICATED ON THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

DISTRICT DEPUTY DIRECTOR

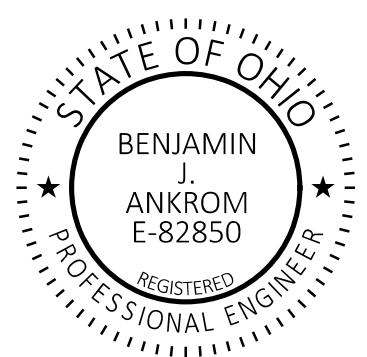
John W. O'Brien
John W. O'Brien
07

DIRECTOR, DEPARTMENT OF TRANSPORTATION

Pamela Boratyn
Pamela Boratyn

ENGINEER'S SEAL

ROADWAY



ROUTE	LOCATION	ADT	TRUCKS
AUG-364	SLM 15.06 TO 15.42	5960	70
AUG-703	SLM 0.00 TO 0.59	3425	115
MER-29	SLM 0.00 TO 5.99	3040	820
MER-49	SLM 5.05 TO 15.43	1695	535
MER-118	SLM 8.35 TO 13.42	4710	370
MER-127	SLM 7.55 TO 7.60	5330	945
MER-274	SLM 0.00 TO 3.79	3515	485
MER-707	SLM 0.00 TO 8.21	1080	280
MER-33	SLM 6.45	3353	974

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DESIGN EXCEPTIONS

NONE

ADA DESIGN WAIVERS

NONE

UNDERGROUND UTILITIES

Contact Two Working Days Before You Dig

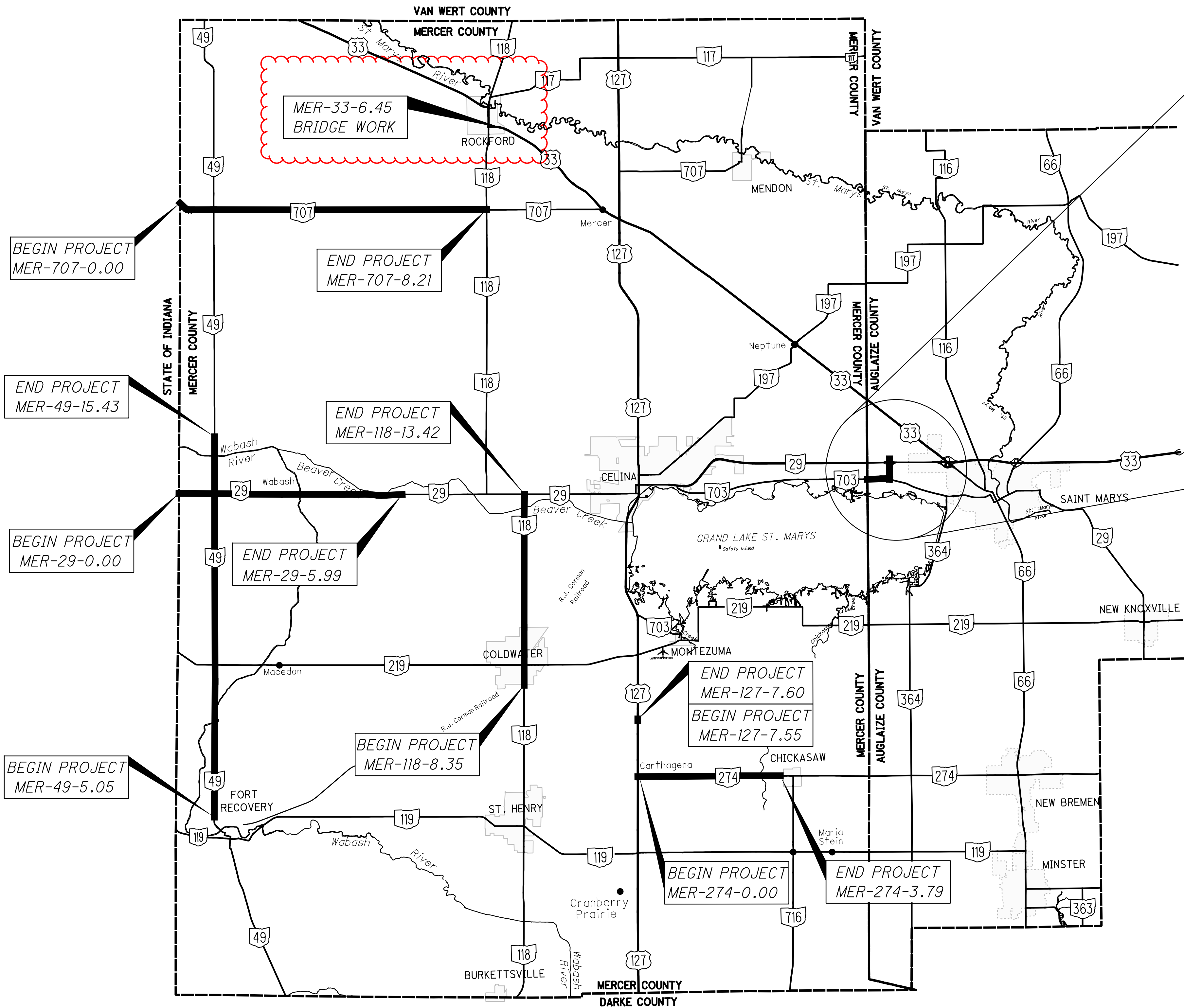
OHIO811.org
Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
ODOT DISTRICT 7 - ENGINEERING
1001 ST. MARYS AVE.
SIDNEY, OHIO

STANDARD CONSTRUCTION DRAWINGS								SUPPLEMENTAL SPECIFICATIONS		SPECIAL PROVISIONS	
BP-3.1	1/19/24	MT-96.11	7/18/25	TC-41.20	10/18/13			800	1/17/25		
BP-3.2	1/18/19	MT-96.20	7/18/25	TC-42.20	10/18/13						
		MT-96.26	1/17/25	TC-52.10	10/18/13						
		MT-97.10	4/19/19	TC-52.20	1/15/21			832	7/21/23		
		MT-97.12	1/20/17	TC-64.10	7/21/23						
DM-4.3	1/15/16	MT-99.20	4/19/19	TC-65.10	1/17/14			872	1/21/22		
DM-4.4	1/15/16	MT-101.90	7/17/20	TC-65.11	1/17/25			874	4/17/20		
		MT-105.10	1/17/20	TC-71.10	4/21/23			875	1/18/19		
				TC-74.10	7/21/23						
				DS-1-92	7/15/22						





AUGLAIZE/MERCER COUNTY

PROJECT LOCATIONS

PARTICIPATION 1 = 01/STR

AUG-364-SLM 15.24 TO 15.42
AUG-364I-SLM 0.00 TO 0.19
MER-127-SLM 7.55 TO 7.60
MER-29-SLM 0.00 TO 5.99
MER-49-SLM 5.05 TO 15.43
MER-118-SLM 10.39 TO 13.42
MER-274-SLM 0.00 TO 3.79
MER-707-SLM 0.00 TO 8.21

PARTICIPATION 3 = 03/NHS

MER-33-SLM 6.45

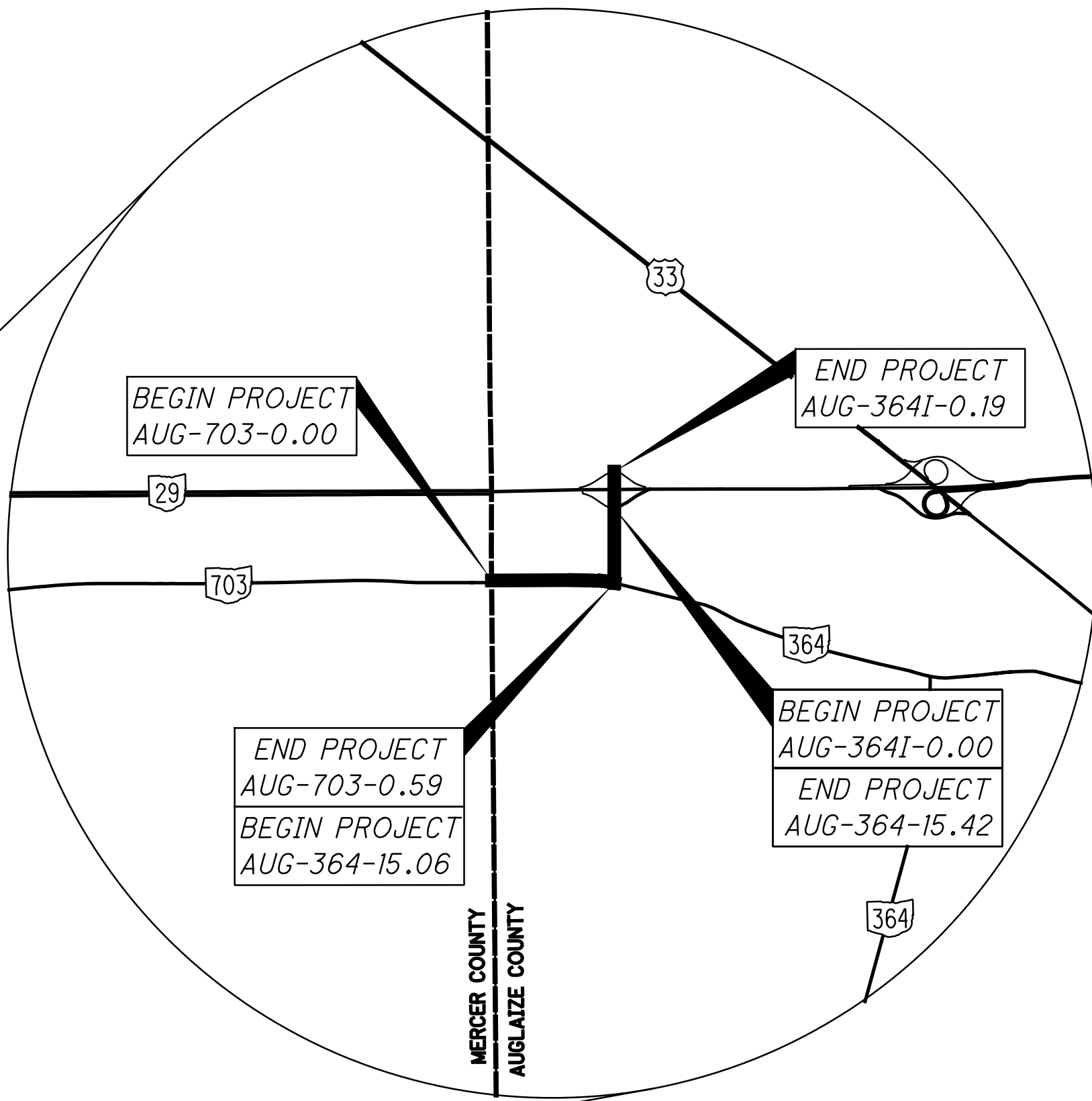
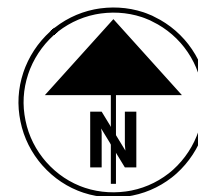
PARTICIPATION 2 = 02/S<2

AUG-364-SLM 15.06 TO 15.24
AUG-703-SLM 0.00 TO 0.59
MER-118-SLM 8.35 TO 10.39

LATITUDE: 40° 32' 30" N LONGITUDE: 84° 37' 44" W

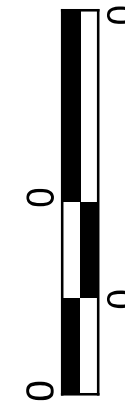


PORTION TO BE IMPROVED



SCHEMATIC PLAN

HORIZONTAL
SCALE IN FEET



DESIGN AGENCY



DESIGNER

TMK

REVIEWER

BJA 6-11-25

PROJECT ID

101178

SHEET

P.2

TOTAL

20

ALIGNMENT AND PROFILE

THE WORK PROPOSED BY THIS PROJECT CONSISTS OF PLANING AND RESURFACING OF THE EXISTING PAVEMENT. THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED AND THE PROFILE OF THE PROPOSED SURFACE WILL BE SIMILAR TO THAT OF THE EXISTING PAVEMENT.

ITEM 253 - PAVEMENT REPAIR, AS PER PLAN

PAVEMENT REPAIR SHALL BE IN ACCORDANCE WITH ITEM 253 - PAVEMENT REPAIR, WITH THE FOLLOWING ADDITIONS:

THE ENGINEER SHALL DESIGNATE THE LOCATIONS AND LIMITS OF THE AREAS TO BE REPAIRED. THE AREAS SHALL BE ROUGHLY RECTANGULAR IN SHAPE AND SAWED OR MILLED TO A NEAT LINE. THE DEPTH OF REMOVAL, AS DIRECTED BY THE ENGINEER, SHALL BE SUFFICIENT TO REMOVE ALL OF THE DETERIORATED PAVEMENT. THE ESTIMATED PAVEMENT REMOVAL DEPTH IS 9". THE ENTIRE AREA INCLUDING VERTICAL FACES SHALL BE COATED PRIOR TO PLACING THE REPLACEMENT MATERIAL AS PER 253.03. THE REPLACEMENT MATERIAL SHALL BE 9" OF ITEM 301 - ASPHALT CONCRETE BASE, (449), PG64-22.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

ITEM 253 - PAVEMENT REPAIR, AS PER PLAN = 100 SQ YD

FULL DEPTH PAVEMENT REPAIR LOCATIONS

LISTED BELOW ARE POSSIBLE REPAIR LOCATIONS THAT HAVE BEEN IDENTIFIED BY FIELD OBSERVATIONS. THESE LOCATIONS ARE FOR REFERENCE ONLY. ALL REPAIR LOCATIONS SHALL BE APPROVED BY THE ENGINEER.

ROUTE	DIR.	SLM	SIZE
SR 118	NB	8.43	85' X 6'

ITEM 202 PAVEMENT REMOVED, AS PER PLAN

THIS PAY ITEM SHALL INCLUDE REMOVING ALL ASPHALT CONCRETE, CONCRETE BASE, AND ANY OTHER MATERIALS ENCOUNTERED WHEN EXCAVATING FOR THE FULL DEPTH PAVEMENT REPLACEMENT NEAR THE MER-127-0757 STRUCTURE.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN

THE PAVEMENT REPAIRS SHALL BE IN ACCORDANCE WITH ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR, WITH THE FOLLOWING ADDITIONS:

THE ENGINEER SHALL DESIGNATE THE LOCATIONS AND LIMITS OF THE AREAS TO BE REPAIRED. THE AREAS SHALL BE ROUGHLY RECTANGULAR IN SHAPE AND SAWED OR MILLED TO A NEAT LINE. THE DEPTH OF REMOVAL, AS DIRECTED BY THE ENGINEER, SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT. THE REPLACEMENT MATERIAL SHALL BE ITEM 301 - ASPHALT CONCRETE BASE, (449), PG64-22.

THE ESTIMATED PAVEMENT REPAIR AREAS SHALL BE A MINIMUM OF 4 FEET IN WIDTH. THE DEPTH SHALL BE 4 INCHES MEASURED FROM THE MILLED SURFACE OR AS DIRECTED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

PARTICIPATION 1 = 01/STR
AUG-364-SLM 15.24 TO 15.42 = 50 SY
AUG-364I-SLM 0.00 TO 0.19 = 50 SY
MER-29-SLM 0.00 TO 5.99 = 500 SY
MER-49-SLM 5.05 TO 15.43 = 750 SY
MER-118-SLM 10.39 TO 13.42 = 250 SY
MER-274-SLM 0.00 TO 3.79 = 250 SY
MER-707-SLM 0.00 TO 8.21 = 500 SY
PARTICIPATION TOTAL 1 = 2350 SY

PARTICIPATION 2 = 02/SK2
AUG-364-SLM 15.06 TO 15.24 = 50 SY
AUG-703-SLM 0.00 TO 0.59 = 350 SY
MER-118-SLM 8.35 TO 10.39 = 350 SY
PARTICIPATION 2 TOTAL = 750 SY

PARTIAL DEPTH PAVEMENT REPAIR LOCATIONS

LISTED BELOW ARE POSSIBLE REPAIR LOCATIONS THAT HAVE BEEN IDENTIFIED BY FIELD OBSERVATIONS. THESE LOCATIONS ARE FOR REFERENCE ONLY. ALL REPAIR LOCATIONS SHALL BE APPROVED BY THE ENGINEER.

ROUTE	DIR.	SLM	SIZE
SR 118	NB	8.56	120' X 4'
SR 118	NB	8.60	20' X 4'
SR 118	SB	9.10	5' X 5'
SR 118	SB	9.23	15' X 8'
SR 118	SB	9.32	20' X 4'
SR 118	NB	9.41	20' X 4'
SR 118	SB	9.44	45' X 4'
SR 118	NB	9.48	135' X 4'
SR 118	SB	9.59	45' X 4'
SR 118	SB	9.72	40' X 4'
SR 118	SB	9.97	70' X 4'

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE

AN ESTIMATED QUANTITY OF ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE HAS BEEN CARRIED TO THE GENERAL SUMMARY AND INCLUDED IN THE PLANS TO BE USED AS DIRECTED BY THE ENGINEER.

THE APPROXIMATE DEPTH OF PAVEMENT PLANING SHALL BE ONE AND ONE HALF INCH (1 1/2") AS SHOWN ON THE TYPICAL SECTIONS.

THE APPROXIMATE WIDTH OF THE PAVEMENT PLANING WILL VARY FROM 24.5' TO 49.0'.

NO MILLED SURFACE SHALL BE EXPOSED FOR MORE THAN 72 HOURS UNLESS APPROVED BY THE ENGINEER.

THERE ARE SIXTY FOUR (64) INTERSECTIONS WITHIN THE LIMITS OF THE PAVEMENT PLANING. THERE ARE NINETY SEVEN (97) STREETS THAT THE PROJECT WILL TIE INTO AT THESE SIXTY FOUR (64) INTERSECTIONS.

THERE ARE APPROXIMATELY SIXTY SIX (66) CASTINGS WITHIN THE PLANING AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING ALL LABOR AND EQUIPMENT NECESSARY TO MAINTAIN TRAFFIC AROUND THE CASTINGS AND SHALL INCLUDE THE COST IN THE UNIT PRICE BID FOR: ITEM 614 - MAINTAINING TRAFFIC

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

ITEM 611 - MANHOLE ADJUSTED TO GRADE = 20 EACH
ITEM 638 - VALVE BOX ADJUSTED TO GRADE = 10 EACH

ANY CASTINGS THAT REQUIRE ADJUSTING DURING CONSTRUCTION ARE THE RESPONSIBILITY OF THE CONTRACTOR. ALL LABOR, MATERIAL AND EQUIPMENT NECESSARY FOR THE ADJUSTMENTS OF CASTINGS SHALL BE INCLUDED IN ITEMS 611 & 638. ANY NEW CASTINGS REQUIRED SHALL BE FURNISHED TO THE CONTRACTOR BY THE VILLAGE OF COLDWATER. ADJUSTING DEVICES ON MANHOLE COVERS ARE PROHIBITED.

ITEM 254 - PATCHING PLANED SURFACE, AS PER PLAN

PAVEMENT AREAS DESIGNATED FOR PATCHING AFTER PAVEMENT PLANING OPERATION SHALL BE MILLED 2 INCHES (2") IN DEPTH AND PATCHED PER 254.04.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

ITEM 254 - PATCHING PLANED SURFACE, AS PER PLAN = 600 SY
(PARTICIPATION 1 = 500 SY) (PARTICIPATION 2 = 100 SY)



ITEM 301 - ASPHALT CONCRETE BASE, PG 64-22, (449), AS PER PLAN

THE CONTRACTOR SHALL BE RESPONSIBLE FOR EXCAVATING OF MATERIAL FROM ALL EXISTING STONE DRIVEWAYS AND MAILBOX APPROACHES TO A DEPTH OF FOUR INCHES (4") BELOW THE EXISTING PAVEMENT ELEVATION. ALL DRIVEWAY AND MAILBOX APPROACHES SHALL BE CONSTRUCTED AS PER TYPICAL SHOWN IN THE PLAN. EXCAVATED MATERIAL SHALL BE USED TO BACKUP THE NEW PAVEMENT OR REMOVED FROM THE PROJECT SITE AS DIRECTED BY THE PROJECT ENGINEER.

ANY PREVIOUSLY SEEDED AND MULCHED AREAS WITH ESTABLISHED GRASS DISTURBED BY THE REMOVAL AND REPLACEMENT OF THE MAILBOX APPROACHES THROUGHOUT THE LIMITS OF THE PROJECT WILL NEED REPAIRED ACCORDING TO ITEM 659 AND ARE CONSIDERED INCIDENTAL TO ITEM 301 - ASPHALT CONCRETE BASE, AS PER PLAN.

A QUANTITY OF ITEM 301 - ASPHALT CONCRETE BASE, PG 64-22 (449), AS PER PLAN HAS BEEN INCLUDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED IN THOSE AREAS EXCAVATED FOR DRIVEWAY AND MAILBOX APPROACHES AND THE CURVE LOCATIONS SPECIFIED.

ALL WORK, MATERIALS, LABOR AND EQUIPMENT NECESSARY TO COMPLETE THE ABOVE DESCRIBED WORK SHALL BE INCIDENTAL TO THE PLACEMENT AND INCLUDED IN THE COST OF ITEM 301 - ASPHALT CONCRETE BASE, (449), PG 64-22, AS PER PLAN.

ITEM 301 - ASPHALT CONCRETE BASE, PG 64-22, (449), AS PER PLAN TOTAL = 13 CY (PART 1)

ESTIMATED APPROACHES:

- 1 COMB. MB/DRIVE APPROACH - MER-49 SLM 10.94 RT = 3 CY
- 1 COMB. MB/DRIVE APPROACH - MER-118 SLM 10.75 LT = 5 CY
- 1 COMB. MB/DRIVE APPROACH - MER-118 SLM 10.78 LT = 5 CY

ITEM 690 - SPECIAL, MAILBOX REMOVED AND RESET

THIS WORK SHALL CONSIST OF REMOVING AND RESETTNG EXISTING MAILBOX SUPPORTS. THE CONTRACTOR SHALL TAKE GREAT CARE IN REMOVING AND RESETTNG MAILBOX SUPPORTS TO THE CORRECT DISTANCE FROM THE EDGE OF PAVEMENT AS SHOWN ON THE APPROACH TYPICAL SHEET OR AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OR REPLACEMENT FOR DAMAGED OR IMPROPER HANDLING ON THEIR PART AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE LOCAL POSTMASTER REGARDING THE TIMING OF THE MOVEMENT OF ANY MAILBOX TO BE REMOVED AND RESET. TEMPORARY INSTALLATIONS SHALL BE IN ACCORDANCE WITH 707.10.

MAILBOXES REMOVED AND RESET SOLELY FOR THE CONVENIENCE OF THE CONTRACTOR'S OPERATION OF CONSTRUCTING THE ASPHALT CONCRETE APPROACH IN JUDGMENT OF THE ENGINEER SHALL NOT BE PAID FOR.

PAYMENT SHALL BE FOR FINAL PERMANENT INSTALLATIONS ONLY. ALL LABOR, MATERIAL AND EQUIPMENT NEEDED TO COMPLETE THEIR WORK SHALL BE INCLUDED IN ITEM 690 - SPECIAL, MAILBOX REMOVED AND RESET.

A QUANTITY OF THREE (3) EACH OF ITEM SPECIAL, MAILBOX REMOVED AND RESET HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 690 - SPECIAL - MAILBOX REMOVED AND RESET, 3 EACH (PART 1)

ASPHALT SAFETY EDGES

ASPHALT SAFETY EDGES SHALL BE CONSTRUCTED AS PER STANDARD CONSTRUCTION DRAWING BP-3.2 AT THE FOLLOWING LOCATIONS:

PARTICIPATION 1 = 01/STP
MER-29-SLM 0.00 TO 5.99 = 5.99 MILE
MER-49-SLM 5.05 TO 15.43 = 10.38 MILE
MER-118-SLM 10.39 TO 13.42 = 3.03 MILE
MER-274-SLM 0.00 TO 3.79 = 3.79 MILE
MER-707-SLM 0.00 TO 8.21 = 8.21 MILE
PARTICIPATION 1 TOTAL = 31.41 MILE
PARTICIPATION 2 = 02/SC2
MER-118-SLM 9.89 TO 10.39 = 0.50 MILE

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY TO CONSTRUCT THE SAFETY EDGES:

PARTICIPATION 1
ITEM 209 PREPARING SUBGRADE FOR SHOULDER PAVING = 62.80 MILE

ITEM 441 ASPHALT CONCRETE SURFACE COURSE,
TYPE 1 (448), PG70-22M = 167 CY

PARTICIPATION 2
ITEM 209 PREPARING SUBGRADE FOR SHOULDER PAVING = 1.00 MILE

ITEM 441 ASPHALT CONCRETE SURFACE COURSE,
TYPE 1 (448), PG70-22M = 3 CY

PAVEMENT MARKINGS

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DOCUMENT THE LAYOUT OF THE EXISTING PAVEMENT MARKINGS INCLUDING EXISTING LANE AND SHOULDER WIDTHS IN A LOG AND SUBMIT TO THE DEPARTMENT FOR ACCEPTANCE. THE DEPARTMENT WILL NOT ALLOW THE CONTRACTOR TO PERFORM ANY PAVEMENT WORK FUNCTIONS (MILLING, OVERLAY, ETC.) UNTIL ACCEPTANCE OF THE SUBMITTED EXISTING MARKING LOG.

MARKINGS SHALL BE REPLACED IN KIND EXCEPT WHERE EXISTING MARKINGS DO NOT MEET THE CURRENT STANDARD CONSTRUCTION DRAWINGS. THE CONTRACTOR SHALL COORDINATE AND CORRABORATE THE PROPOSED LAYOUT OF ALL PAVEMENT MARKINGS PER APPLICABLE STANDARD CONSTRUCTION DRAWINGS WITH ODOT.

NO PERMANENT PAVEMENT MARKINGS OR RAISED PAVEMENT MARKERS SHALL BE PLACED UNTIL THE ODOT PROJECT ENGINEER HAS APPROVED THE LOCATION AND/OR LAYOUT OF THE WORK ZONE PAVEMENT MARKINGS.

PAVEMENT MARKINGS ON INTERSECTING ROADWAYS

THE EDGE LINE SHALL BE CONTINUED AROUND ALL RADII TO MATCH THE EXISTING EDGE LINE OF THE INTERSECTING ROUTE. IF THE INTERSECTING ROUTE HAS A CENTER LINE MARKING, THE CENTER LINE SHALL BE RE-STRIPED OVER THE AREA OF RESURFACING.

COOPERATION WITH RAILROAD (R.J. CORMAN RAILROAD)

THE CONTRACTOR SHALL COORDINATE ALL WORK ON, OVER OR ADJACENT TO THE RAILROAD WITHIN PROJECT LIMITS. THE CONTRACTOR SHALL CONTACT THE FOLLOWING, AT LEAST 30 DAYS IN ADVANCE, IN ORDER TO COORDINATE THE NECESSARY WORK. UNDER NO CIRCUMSTANCES SHALL THERE BE ANY WORK PERFORMED WITHIN THE RAILROAD RIGHT-OF-WAY WITHOUT THE PROPER WRITTEN AUTHORIZATION AND/OR FLAGGING PROTECTION FROM THE RAILROAD. SEE "PAVING AT RAILROAD CROSSINGS" NOTE FOR ADDITIONAL DETAILS.

R.J. CORMAN RAILROAD
CROSSING LOCATIONS:
AUG-364-15.24
MER-118-8.98

PAVING AT RAILROAD CROSSINGS

THE RAILROAD CROSSINGS AT AUG-364-15.24 & MER-118-8.98: SUSPEND AND RESUME RESURFACING AT THE EXISTING RAILROAD CROSSING MATERIALS, APPROXIMATELY 3 FEET FROM NEAREST RAIL. THE CONTRACTOR SHALL VERIFY THE ACTUAL JOINT LOCATIONS WITH THE ENGINEER BEFORE PERFORMING WORK.

ITEM 618 - EDGE LINE RUMBLE STRIPE (ASPHALT CONCRETE)
ITEM 618 - CENTER LINE RUMBLE STRIPE (ASPHALT CONCRETE)

DESCRIPTION: THIS WORK CONSISTS OF GRINDING DEPRESSIONS (RUMBLE STRIPES) IN THE PAVEMENT.

CONSTRUCT RUMBLE STRIPES ACCORDING TO TRAFFIC STANDARD DRAWING TC-64.10.

LOCATION AND PLACEMENT RESTRICTIONS OF RUMBLE STRIPES: RUMBLE STRIPES ARE NOT TO BE USED AT LOCATIONS WHERE THE LANE WIDTH IS LESS THAN 11 FEET AND THE POSTED SPEED LIMIT IS LESS THAN 50 MPH.

LOCATE RUMBLE STRIPES ON THE PAVEMENT PER THE SPECIFICATIONS OF STANDARD DRAWING TC-64.10.

METHOD OF MEASUREMENT: THE DEPARTMENT WILL MEASURE RUMBLE STRIPES IN THE UNIT OF MILES COMPLETED IN PLACE. THE DEPARTMENT WILL MEASURE QUANTITIES AS THE LENGTH OF COMPLETED GRINDING, INCLUDING THE GAPS FOR INTERSECTIONS AND OTHER SECTIONS OF PAVEMENT NOT BEING GROUND BETWEEN THE BEGINNING STRAIGHT LINE MILEAGE AND THE ENDING STRAIGHT LINE MILEAGE FOR THE SEGMENTS LISTED IN THE PLANS.

BASIS OF PAYMENT: THE DEPARTMENT WILL NOT PAY FOR REPAIRING SURFACE DAMAGE AND EXTRANEIOUS MARKS CAUSED BY THE CONTRACTOR'S OPERATIONS. THE DEPARTMENT WILL PAY FOR THE PAVEMENT MARKINGS ITEM 642 TRAFFIC PAINT, SEPARATELY.

A QUANTITY OF 17.76 MILES OF ITEM 618 EDGE LINE, RUMBLE STRIPE (ASPHALT CONCRETE) HAS BEEN CARRIED TO THE GENERAL SUMMARY. THE SEGMENT LOCATIONS ARE:

- (PART. 1) MER-29 FROM SLM 0.00 TO SLM 2.42
- (PART. 1) MER-29 FROM SLM 2.92 TO SLM 5.99
- (PART. 1) MER-274 FROM SLM 0.00 TO SLM 0.33
- (PART. 1) MER-274 FROM SLM 0.60 TO SLM 3.66

A QUANTITY OF 30.41 MILES OF ITEM 618 CENTER LINE, RUMBLE STRIPE (ASPHALT CONCRETE) HAS BEEN CARRIED TO THE GENERAL SUMMARY. THE SEGMENT LOCATIONS ARE:

- (PART. 1) MER-29 FROM SLM 0.00 TO SLM 2.42
- (PART. 1) MER-29 FROM SLM 2.92 TO SLM 5.99
- (PART. 1) MER-49 FROM SLM 5.16 TO SLM 11.24
- (PART. 1) MER-49 FROM SLM 11.60 TO SLM 15.43
- (PART. 2) MER-118 FROM SLM 10.01 TO SLM 10.39
- (PART. 1) MER-118 FROM SLM 10.39 TO SLM 13.42
- (PART. 1) MER-274 FROM SLM 0.00 TO SLM 0.33
- (PART. 1) MER-274 FROM SLM 0.60 TO SLM 3.66
- (PART. 1) MER-707 FROM SLM 0.00 TO SLM 8.21

A QUANTITY OF 31.78 MILES OF ITEM 874 LONGITUDINAL JOINT PREPARATION HAS BEEN CARRIED TO THE GENERAL SUMMARY. THE SEGMENT LOCATION IS:

- (PART. 1) MER-29 FROM SLM 0.00 TO SLM 5.99
- (PART. 1) MER-49 FROM SLM 5.05 TO SLM 15.43
- (PART. 2) MER-118 FROM SLM 10.01 TO SLM 10.39
- (PART. 1) MER-118 FROM SLM 10.39 TO SLM 13.42
- (PART. 1) MER-274 FROM SLM 0.00 TO SLM 3.79
- (PART. 1) MER-707 FROM SLM 0.00 TO SLM 8.21

PAVEMENT MARKINGS: THE RUMBLE STRIPE SHALL BE DUST-FREE AND DRY WHEN THE PAVEMENT MARKINGS ARE APPLIED.

DESIGN AGENCY



DESIGNER

TMK

REVIEWER

BJA 6-11-25

PROJECT ID

101178

SHEET

P.6

TOTAL

20

ITEM 614 – MAINTAINING TRAFFIC

TRAFFIC SHALL BE MAINTAINED AT ALL TIMES. THE LENGTH OF RESTRICTED TRAFFIC ZONES SHALL BE KEPT TO A MINIMUM CONSISTENT WITH THE SPECIFICATION REQUIREMENTS FOR PROTECTION OF COMPLETED COURSES.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

THE CONTRACTOR SHALL BE REQUIRED TO FURNISH AND ERECT “GROOVED PAVEMENT” SIGNS FOR ANY PLANED SURFACE OF THE ROADWAY REMAINING OPENED AT THE CONCLUSION OF A WORK DAY.

WORK ZONE MARKINGS SHALL BE COMPLETE AND IN PLACE ON ALL PAVEMENT, INCLUDING RAMPS, BEFORE EXPOSING THE PAVEMENT TO TRAFFIC.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVEABLE PAVEMENT DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO D7 PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HRS & < 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HRS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES AND RESTRICTIONS	>= 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

PROJECT SCHEDULE

COLDWATER:

ALL MILLING AND RESURFACING IN COLDWATER MUST BE DONE FROM JUNE 1, 2026 TO AUGUST 20, 2026. THIS WILL BE AFTER THE VILLAGE OF COLDWATER COMPLETES THEIR STORM SEWER AND FULL DEPTH PAVEMENT REPLACEMENT WORK AND DURING COLDWATER SCHOOLS SUMMER BREAK.

NO WORK IS PERMITTED IN COLDWATER FROM JULY 29, 2026 TO AUGUST 3, 2026 WHICH IS DURING AND ONE DAY BEFORE AND AFTER THE COLDWATER COMMUNITY PICNIC.

NO MILLED SURFACE ON SR 118 SHALL BE EXPOSED IN THE VILLAGE OF COLDWATER DURING THIS TIME.

WORK HOURS

ALL WORK WITHIN THE VILLAGES OF CARTHAGENA, COLDWATER, ST. ANTHONY AND WABASH SHALL BE DONE BETWEEN THE HOURS OF 7:00AM AND 9:00PM.

FULLY-ACTUATED OPERATION OF WORK ZONE TRAFFIC SIGNAL (MER-33-6.45)

THE WORK ZONE SIGNAL CONTROL REQUIRED FOR THIS PROJECT AND SHOWN ON SHEETS 7A AND 7B AND TRAFFIC SCDS MT– 96.11, 96.20 AND 96.26 SHALL BE FULLY TRAFFIC-ACTUATED AND OPERATE IN A MANNER SIMILAR TO THAT DESCRIBED IN SECTION 733.02 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS.

THE INITIAL CONTROLLER TIMING SHALL BE AS FOLLOWS:

	PHASE			
	1	2	3	4
	(ALL RED) DUMMY PHASE	MAINLINE (DIRECTION)	(ALL RED) DUMMY PHASE	MAINLINE (DIRECTION)
MIN.GREEN		10		10
EXTENSION		4		4
MAX.GREEN		18		18
YELLOW		3.5		3.5
ALL RED	13		13	
RECALL	ON	OFF	OFF	OFF

PROVIDE TIMING APPROPRIATE FOR THE SIGNAL LOCATION UNDER CONSIDERATION. TYPICAL FLOW RATES ARE DISPLAYED IN TABLE 697-2 IN THE ODOT TRAFFIC ENGINEERING MANUAL (TEM).

THE CONTRACTOR SHALL ALSO DESIGN, FURNISH, INSTALL AND MAINTAIN A TRAFFIC DETECTOR ON EACH TRAFFIC APPROACH WHICH WILL RELIABLY DETECT ALL LEGAL TRAFFIC APPROACHING (BUT NOT LEAVING) THE SIGNAL AS IT PASSES OR WAITS IN THE DESIGNATED DETECTOR ZONE SHOWN IN THE PLANS. DETECTOR DESIGNS WHICH DO NOT PROVIDE RELIABLE DETECTION, FREE FROM FALSE CALLS, SHALL BE IMMEDIATELY REPLACED BY THE CONTRACTOR.

OVERHEAD-MOUNTED WORK ZONE SIGNALS

SIGNALS SHALL BE OVERHEAD MOUNTED IN ACCORDANCE WITH THE DETAILS SHOWN ON TRAFFIC SCD MT-96.20.

COORDINATION OF WORK:

AUG/MER-PAVE-FY26 (PID 101178)
MER-CR 80-1.10 BH (PID 117990)
MER-219-13.80 (PID 114949)

THE CONTRACTOR IS ADVISED THAT ADJACENT CONSTRUCTION PROJECTS WITHIN OR NEAR THE WORK LIMITS OF THIS PLAN MAY IMPACT THE PROJECT SCHEDULE, SEQUENCE OF CONSTRUCTION AND/OR TRAFFIC CONTROL BETWEEN ADJACENT ZONES. THE CONTRACTOR IS REQUIRED TO COORDINATE ALL MAINTENANCE OF TRAFFIC OPERATIONS WITH ADJACENT CONSTRUCTION PROJECTS. COOPERATION WITH THE ENGINEER, INSPECTORS AND ALL OTHER CONTRACTORS ON OR ADJACENT TO THE PROJECT IS REQUIRED PER CMS 105.08.

ITEM 614 – LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE Omutcd INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE Omutcd, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

* DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

* DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE Omutcd, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

* FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST’S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS’ DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 80 HOURS.

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

ENVIRONMENTAL NOTES

PROTECTION OF DRINKING WATER RESOURCES: BEST CONSTRUCTION PRACTICES ARE TO BE IMPLEMENTED TO MINIMIZE WATER QUALITY IMPACTS. IDLE EQUIPMENT, PETROCHEMICALS, AND TOXIC/HAZARDOUS MATERIALS SHALL NOT BE STORED NEAR DRAINAGE WAYS, DITCHES OR STREAMS. REFUELING SHALL NOT BE UNDERTAKEN NEAR DRAINAGE WAYS, DITCHES OR STREAMS. A SPILL CONTAINMENT KIT IS TO BE MAINTAINED ON-SITE THROUGHOUT CONSTRUCTION ACTIVITIES. SPILLS OF FUELS, OILS, CHEMICALS, OR OTHER MATERIALS WHICH COULD POSE A THREAT TO GROUNDWATER SHALL BE CLEANED UP IMMEDIATELY. IF THE SPILL IS A REPORTABLE AMOUNT, THE LOCAL FIRE DEPARTMENT (911), LOCAL EMERGENCY COORDINATOR (AUGLAIZE: 419-733-3857; MERCER: 419-586-6455) AND THE OEPA (1-800-282-9378) MUST BE CONTACTED WITHIN 30 MINUTES OF KNOWLEDGE OF THE RELEASE.

BICYCLE TRAFFIC ON STATE ROUTE 703: STATE ROUTE 703 IS A DESIGNATED ON-ROAD BIKE ROUTE. CONSTRUCTION PERSONNEL SHALL BE ALERT TO CYCLISTS AND ACCOMMODATE CYCLISTS’ SAFE TRAVEL THROUGH THE WORK ZONE WITHIN THIS SEGMENT.

DESIGN AGENCY



DESIGNER

TMK

REVIEWER

BJA 6-11-25

PROJECT ID

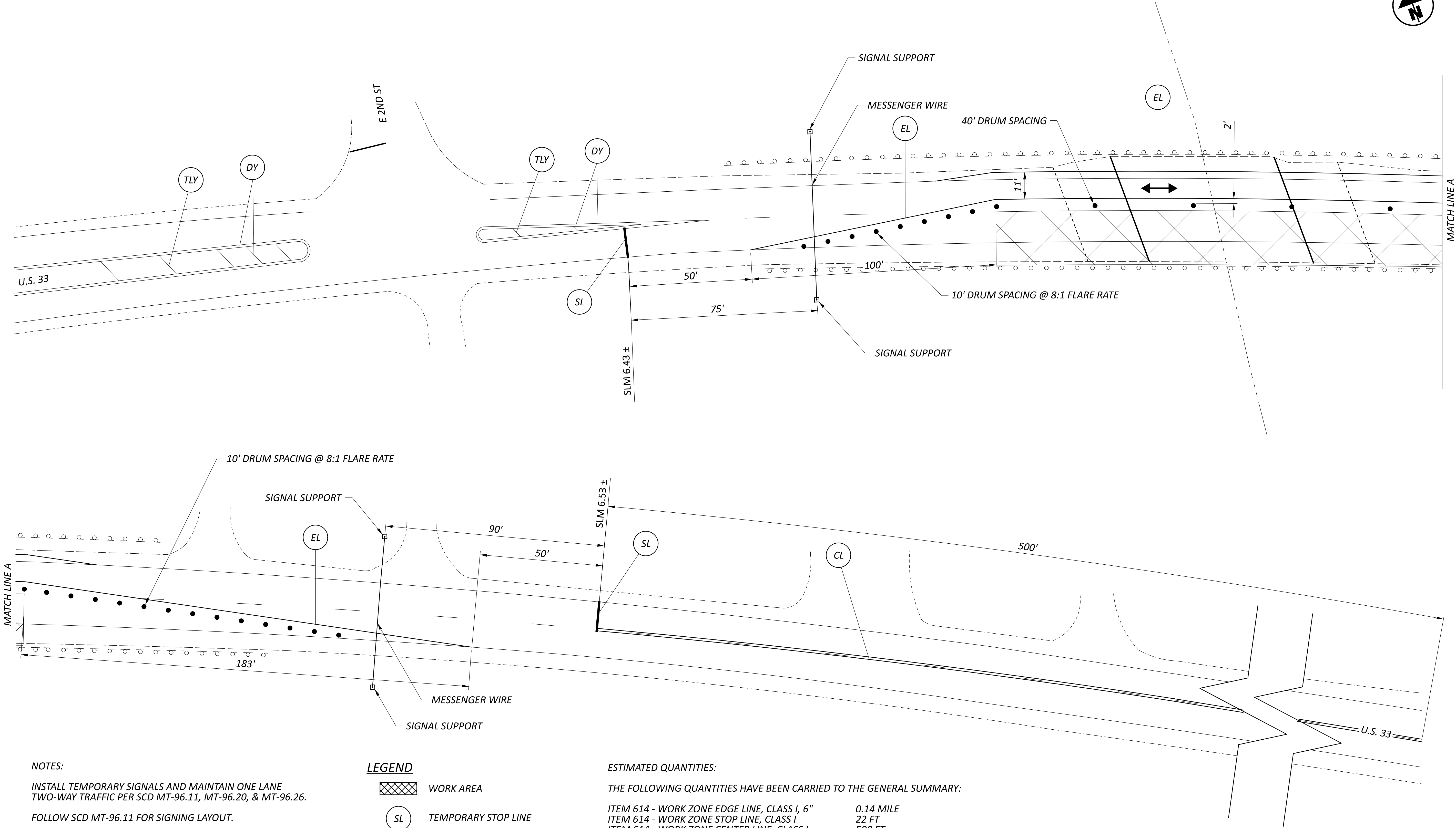
101178

SHEET

P.7

TOTAL

20






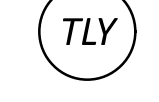


NOTES:

INSTALL TEMPORARY SIGNALS AND MAINTAIN ONE LANE TWO-WAY TRAFFIC PER SCD MT-96.11, MT-96.20, & MT-96.26.

FOLLOW SCD MT-96.11 FOR SIGNING LAYOUT.

LEGEND

-  WORK AREA
-  TEMPORARY STOP LINE
-  TEMPORARY EDGE LINE
-  TEMPORARY CENTERLINE
-  EX. DOUBLE YELLOW
-  EX. TRANSVERSE LINE YELLOW

ESTIMATED QUANTITIES:

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 6" 0.14 MILE
ITEM 614 - WORK ZONE STOP LINE, CLASS I 22 FT
ITEM 614 - WORK ZONE CENTER LINE, CLASS I 500 FT

ALL OTHER LABOR, EQUIPMENT, AND MATERIALS ASSOCIATED WITH THE WORK ZONE ARE INCLUDED IN ITEM 614, MAINTAINING TRAFFIC.

MAINTAINANCE OF TRAFFIC - PHASE 1
MER-33-6.45 BRIDGE

DESIGN AGENCY



DESIGNER

DAV

REVIEWER

BJA 9-04-25

PROJECT ID

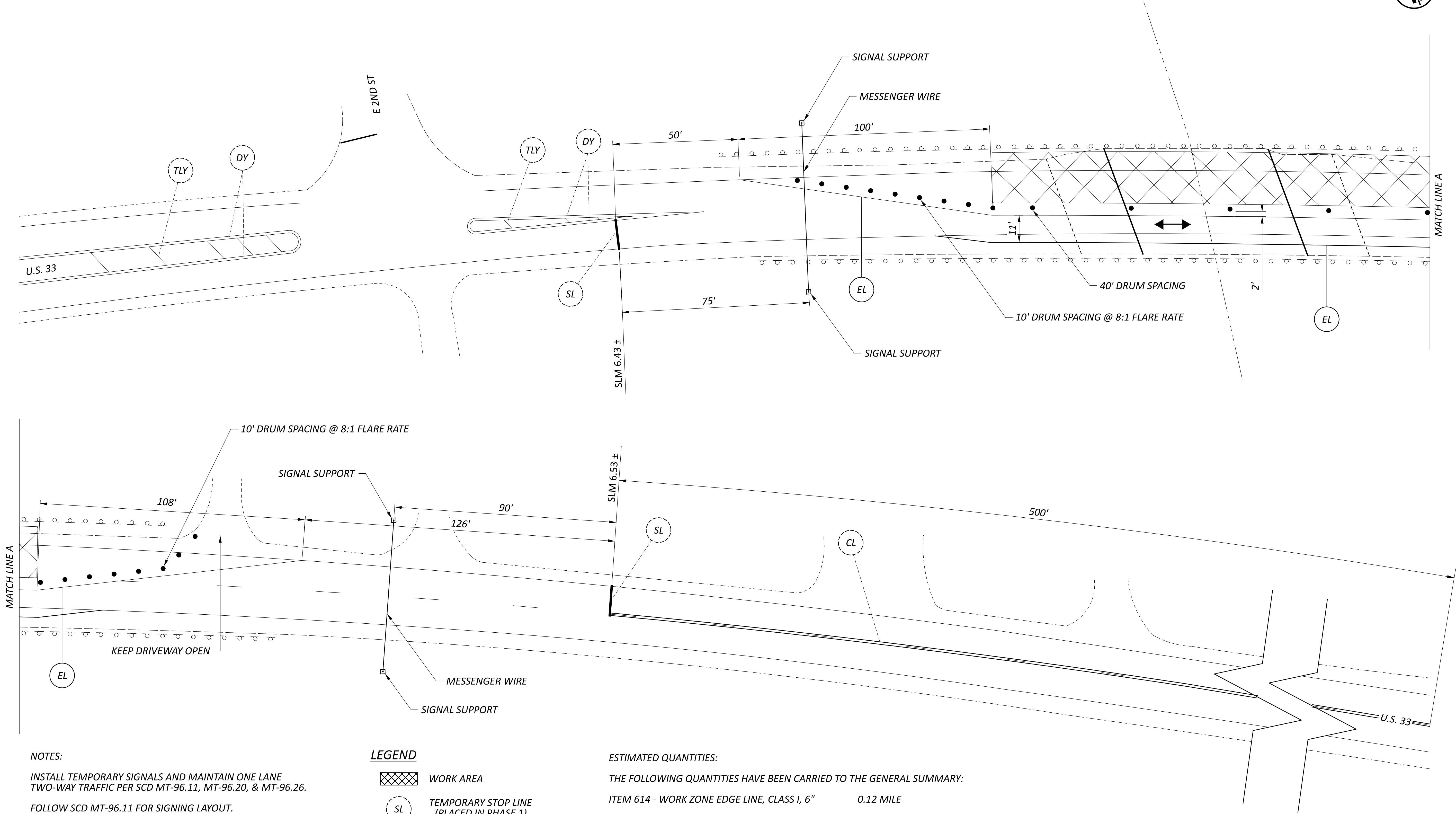
101178

SHEET

P.7A

TOTAL

20



NOTES:

INSTALL TEMPORARY SIGNALS AND MAINTAIN ONE LANE TWO-WAY TRAFFIC PER SCD MT-96.11, MT-96.20, & MT-96.26.

FOLLOW SCD MT-96.11 FOR SIGNING LAYOUT.

- LEGEND**
- WORK AREA
 - SL TEMPORARY STOP LINE (PLACED IN PHASE 1)
 - EL TEMPORARY EDGE LINE
 - CL TEMPORARY CENTERLINE (PLACED IN PHASE 1)
 - DY EX. DOUBLE YELLOW
 - TLY EX. TRANSVERSE LINE YELLOW

ESTIMATED QUANTITIES:

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 6" 0.12 MILE

ALL OTHER LABOR, EQUIPMENT, AND MATERIALS ASSOCIATED WITH THE WORK ZONE ARE INCLUDED IN ITEM 614, MAINTAINING TRAFFIC.

MAINTAINANCE OF TRAFFIC - PHASE 2
MER-33-6.45 BRIDGE

DESIGN AGENCY



DESIGNER

DAV

REVIEWER

BJA 9-09-25

PROJECT ID

101178

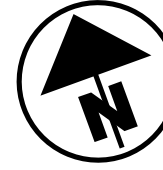
SHEET

P.7B

TOTAL

20

HORIZONTAL
SCALE IN FEET



SHEET NUM.												PART.			ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
5	6	7	7A	7B	10	11	12	13	15	16	20	01/STR	02/S<2	03/NHS						
																			ROADWAY	
								712				712			202	23001	712	SY	PAVEMENT REMOVED, AS PER PLAN	5
											815	815			202	23500	815	SY	WEARING COURSE REMOVED	
								778				778			204	10000	778	SY	SUBGRADE COMPACTION	
	63.8											62.8	1		209	72050	63.8	MILE	PREPARING SUBGRADE FOR SHOULDER PAVING	
	3											3			SPECIAL	69050350	3	EACH	MAILBOX REMOVED AND RESET	5
																			DRAINAGE	
20													20		611	99654	20	EACH	MANHOLE ADJUSTED TO GRADE	
												2,350	750		251	01041	3,100	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2" Depth	5
3,100													100		253	01001	100	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 2 1/2" Depth	5
100					287,828	162,850	162,465					555,573	57,570		254	01000	613,143	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 2" Depth	
								106				375		375	254	01000	375	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 2 1/2" Depth	
												106			254	01000	106	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 2" Depth	
												465		465	254	01000	465	SY	PAVEMENT PLANING, ASPHALT CONCRETE, VARIABLE DEPTH (1 1/2" TO 2 1/2")	
												1,012			254	01000	1,012	SY	PAVEMENT PLANING, ASPHALT CONCRETE, Variable Depth (2" TO 5")	
600												500	100		254	01601	600	SY	PATCHING PLANED SURFACE, AS PER PLAN	5
								184				184			301	56000	184	CY	ASPHALT CONCRETE BASE, PG64-22, (449)	
	13											13			301	56001	13	CY	ASPHALT CONCRETE BASE, (449), AS PER PLAN, PG64-22	6
								144				144			304	20000	144	CY	AGGREGATE BASE	
					24,470	14,163	13,812	196				227	47,581	5,215	407	20000	52,868	GAL	NON-TRACKING TACK COAT	
		170			11,995	6,944	6,771	46				124	23,491	2,559	441	50100	26,050	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M	
								34				34			441	50300	34	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)	
												61		61	442	20001	61	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (448), AS PER PLAN, PG 76-22M	20
					1,062	474	605					2,058	83		617	10100	2,141	CY	COMPACTED AGGREGATE	
	17.76											17.76			618	41000	17.76	MILE	RUMBLE STRIPES, EDGE LINE (ASPHALT CONCRETE)	
	30.41											30.03	0.38		618	43000	30.41	MILE	RUMBLE STRIPES, CENTER LINE (ASPHALT CONCRETE)	
	31.78											31.4	0.38		874	21000	31.78	MILE	LONGITUDINAL JOINT PREPARATION	
																			WATER WORK	
10													10		638	10800	10	EACH	VALVE BOX ADJUSTED TO GRADE	
																			TRAFFIC CONTROL	
										2,300		2,199	100	1	621	00100	2,300	EACH	RPM	
										2,300		2,199	100	1	621	54000	2,300	EACH	RAISED PAVEMENT MARKER REMOVED	
								66.28				63.66	2.54	0.08	642	00104	66.28	MILE	EDGE LINE, 6", TYPE 1	
								34.68				31.83	2.81	0.04	642	00300	34.68	MILE	CENTER LINE, TYPE 1	
								418					418		644	00400	418	FT	CHANNELIZING LINE, 8"	
								454				296	158		644	00500	454	FT	STOP LINE	
								2,008					2,008		644	00620	2,008	FT	CROSSWALK LINE, 12"	
								84					84		644	00630	84	FT	CROSSWALK LINE, 24", HIGH VISIBILITY	
								78					78		644	00700	78	FT	TRANSVERSE/DIAGONAL LINE	
								4				1	3		644	01000	4	EACH	RAILROAD SYMBOL MARKING	
													2		644	01110	2	EACH	SCHOOL SYMBOL MARKING, 96"	
								246					246		644	01200	246	FT	PARKING LOT STALL MARKING	
								8					8		644	01300	8	EACH	LANE ARROW	
								1					1		644	01600	1	EACH	HANDICAP SYMBOL MARKING	
																			STRUCTURE OVER 20 FOOT SPAN (MER-118-1302)	
											88	88			516	31001	88	FT	JOINT SEALER, AS PER PLAN	20
																			STRUCTURE OVER 20 FOOT SPAN (MER-274-0029)	
											238	238			512	33010	238	SY	TYPE 3 WATERPROOFING	
											72	72			516	31001	72	FT	JOINT SEALER, AS PER PLAN	20
											107	107			SPECIAL	51822300	107	FT	STEEL DRIP STRIP	20
																			STRUCTURE OVER 20 FOOT SPAN (MER-29-0486)	
											93	93			516	31001	93	FT	JOINT SEALER, AS PER PLAN	20
																			STRUCTURE OVER 20 FOOT SPAN (MER-33-0645)	
											375			375	512	33010	375	SY	TYPE 3 WATERPROOFING	
											88			88	516	31001	88	FT	JOINT SEALER, AS PER PLAN	20
											25			25	519	12300	25	SY	PATCHING CONCRETE BRIDGE DECK - TYPE B	

GENERAL SUMMARY

DESIGN AGENCY



DESIGNER

TMK

REVIEWER

BJA 6-11-25

PROJECT ID


101178

SHEET


P.8

TOTAL

20

GENERAL SUMMARY	
DESIGN AGENCY	
	
DESIGNER	
TMK	
REVIEWER	
BJA	6-11-25
PROJECT ID	
101178	
SHEET	TOTAL
P.9	20

PAVEMENT MARKING SUBSUMMARY

DESIGN AGENCY	
	
DESIGNER	
TMK	
REVIEWER	
BJA	6-11-2
PROJECT ID	
101178	
SHEET	TOTAL
P.15	20

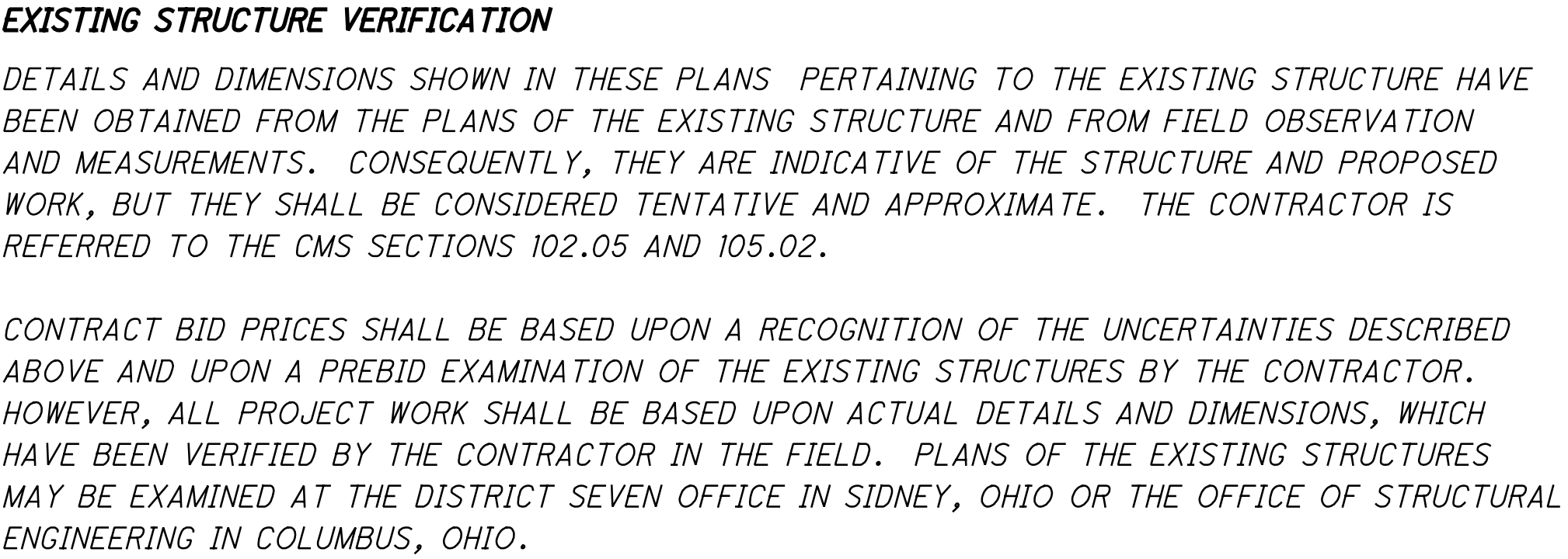
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RAISE PAVEMENT MARKER DETAIL No.:

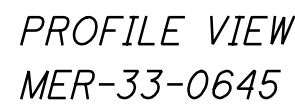
- 1 - TYPICAL SPACING/MULTILANE DIVIDED
- 2 - TAPERED ACCELERATION LANE
- 3 - DECELERATION LANE
- 4 - PARALLEL ACCELERATION LANE
- 5 - MULTILANE DIVIDED/EXPRESSWAY
- 6 - STOP APPROACH
- 7 - 1-LANE APPROACH W/ LEFT TURN LANE
- 8 - THRU APPROACH
- 9 - 2-LANE APPROACH W/ LEFT TURN LANE
- 10 - 4-LANE DIVIDED TO 2 LANE TRANSITION
- 11 - 4-LANE UNDIVIDED TO 2 LANE TRANSITION
- 12 - 2-LANE NARROW BRIDGE
- 13 - TWO-WAY LEFT TURN LANE
- 14 - 1-LANE BRIDGE
- 15 - HORIZONTAL CURVE
- 16 - HORIZONTAL CURVE, ALT.
- 17 - STOP APPROACH, ALT.

GAP - CENTERLINE AT 80 FT (TYPICAL)

RPM SUBSUMMARY



A 1 1/2" DEEP X 1/2" WIDE STRIP SHALL BE SAWCUT OUT OF THE THE PROPOSED OVERLAY. CENTER THE SAWCUT OVER THE CENTER OF THE EXISTING POLYMER MODIFIED ASPHALT EXPANSION JOINT THAT WAS COVERED UP BY THE PROPOSED OVERLAY. JOINT SEALER AS PER 705.04 SHALL BE USED TO SEAL THE JOINT CREATED. THE CONTRACTOR SHALL MARK AND VERIFY THE LOCATIONS WITH THE ENGINEER PRIOR TO ANY MILLING OR PAVING. A QUANTITY HAS BEEN INCLUDED IN THE ESTIMATED QUANTITIES.



- 1) MARK AND VERIFY LOCATIONS OF THE EXPANSION JOINTS WITH THE ENGINEER PRIOR TO PLANING THE BRIDGE DECK. REMOVE 2 1/2" OF ASPHALT FROM THE BRIDGE DECK. TRANSITION THE PLANING TO 1 1/2" DEPTH AS SHOWN IN THE PROFILE VIEW.
- 2) PATCH CONCRETE BRIDGE DECK
- 3) REMOVE AND REPLACE WATERPROOFING ON BRIDGE DECK. THE NEW WATERPROOFING SHOULD EXTEND 3' ON TO THE APPROACH SLABS.
- 4) OVERLAY THE BRIDGE DECK, THE APPROACH SLABS, AND THE POLYMER MODIFIED ASPHALT EXPANSION JOINTS WITH ITEM 442, ASPHALT CONCRETE AS SHOWN IN THE PROFILE VIEW.
- 5) SAW CUT PER ITEM 516, JOINT SEALER, AS PER PLAN. THE SAW CUTS SHALL BE LOCATED OVER THE CENTER OF THE EXISTING POLYMER MODIFIED ASPHALT EXPANSION JOINT. SEAL THE JOINTS WITH 705.04 JOINT SEALER.

- ① - ITEM 442 - 3" ASPHALT CONCRETE, SURFACE COURSE, 12.5 MM, TYPE A (448), AS PER PLAN
- ② - ITEM 442 - VARIABLE THICKNESS (3" TO 1 1/2") ASPHALT CONCRETE, SURFACE COURSE, 12.5 MM, TYPE A (448), AS PER PLAN
- ③ - ITEM 407 - NON-TRACKING TACK COAT @ 0.085 GAL/SY
- ④ - ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, 2 1/2" DEPTH
- ⑤ - ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, VAR. DEPTH (1 1/2" TO 2 1/2")
- ⑥ - ITEM 516 - JOINT SEALER, AS PER PLAN (1 1/2" DEEP)
- ⑦ - ITEM 512 - TYPE 3 WATERPROOFING
- ⑧ - ITEM 519 - PATCHING CONCRETE BRIDGE DECK, TYPE B
- ④ - EXISTING PAVEMENT
- ⑤ - EXISTING POLYMER MODIFIED ASPHALT EXPANSION JOINT

LENGTH	WIDTH	BRIDGE DECK AREA	SKEW ANGLE	STRUCTURE NAME	STRUCTURE FILE NUMBER	COMMENTS (WEARING COURSE)	APPROACH SLAB LENGTH	APPROACH SLAB AREA	TYPE
FT.	FT.	SQ. YD.	DEGREE, MIN., SEC				FT	SQ. YD.	
58.00	44.00	284	0	MER-29-0047	5400058	SKIP CONCRETE BRIDGE DECK & APPROACH SLABS	15	133	SINGLE SPAN COMPOSITE PRESTRESSED CONCRETE BOX BEAM WITH INTREGAL STUB ABUTMENTS
-	-	-	0	MER-29-0162	5400066	PAVE OVER AS ROADWAY	-	-	14' X 7' REINFORCED CONCRETE BOX CULVERT
60.02	40.00	267	10 LF	MER-29-0285	5400090	SKIP CONCRETE BRIDGE DECK	15	133	CONTINUOUS CONCRETE SLAB WITH CAPPED PILE ABUTMENTS
119.30	44.00	583	0	MER-29-0321	5400139	SKIP CONCRETE BRIDGE DECK	25	245	PRESTRESSED CONCRETE BOX BEAM
79.74	40.00	354	30 RF	MER-29-0486	5400155	PAVE OVER AS ROADWAY SAW & SEAL JOINTS	15	133	CONTINUOUS REINFORCED CONCRETE SLAB WITH CAPPED PILE ABUTMENTS
-	-	-	30 RF	MER-29-0559	5400252	PAVE OVER AS ROADWAY	-	-	12' X 6' REINFORCED CONCRETE BOX CULVERT
99.46	40.00	442	40 RF	MER-29-0784	5400902	SKIP CONCRETE BRIDGE DECK & APPROACH SLABS	20	178	WIDENED SINGLE SPAN COMPOSITE STEEL BEAM WITH CONCRETE SUBSTRUCTURE
66.59	44.0	326	20 RF	MER-33-0645	5400392	2 1/2" MILL OVER BRIDGE DECK REPLACE WATERPROOFING & PAVE WITH 3" OF ASPHALT	25	245	CONTINUOUS REINFORCED CONCRETE SLAB WITH CAPPED PILE SUBSTRUCTURE
-	-	-	0	MER-49-1045	5400872	PAVE OVER AS ROADWAY	-	-	TWIN 60" REINFORCED CONCRETE PIPES
-	-	-	0	MER-49-1419	5400813	PAVE OVER AS ROADWAY	-	-	TWIN 8'7" X 5'11" SECTIONAL PLATE PIPE ARCH
69.00	36.00	276	0	MER-49-1456	5400856	PAVE OVER AS ROADWAY SAW & SEAL JOINTS	25	200	PRESTRESSED CONCRETE BOX BEAM
79.50	32.00	283	0	MER-49-1478	5400767	SKIP CONCRETE BRIDGE DECK	15	107	CONTINUOUS CONCRETE SLAB WITH CAPPED PILE ABUTMENTS
160.5	32.00	571	0	MER-49-1492	5400759	SKIP CONCRETE BRIDGE DECK & APPROACH SLABS	15	107	CONTINUOUS STEEL BEAMS WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE
-	-	-	18 RF	MER-118-0927	5401577	PAVE OVER AS ROADWAY	-	-	10' X 7' REINFORCED CONCRETE BOX CULVERT
-	-	-	21 LF	MER-118-1019	5401607	PAVE OVER AS ROADWAY	-	-	12' X 6' REINFORCED CONCRETE BOX CULVERT
48.00	44.00	235	0	MER-118-1302	5401623	PAVE OVER AS ROADWAY SAW & SEAL JOINTS	25	245	PRESTRESSED CONCRETE BOX BEAM
53.00	36.00	212	0	MER-274-0029	5403480	REMOVE EXISTING ASPHALT AND WATERPROOFING, REPLACE DRIP STRIP PLACE NEW WATERPROOFING & ASPHALT CONCRETE, SAW & SEAL JOINTS	25	200	CONTINUOUS REINFORCED CONCRETE SLAB WITH CAPPED PILE ABUTMENTS
-	-	-	0	MER-274-0061	5403537	PAVE OVER AS ROADWAY	-	-	12' X 8' REINFORCED CONCRETE BOX CULVERT
60.24	36.00	241	30 RF	MER-274-0187	5403545	SKIP CONCRETE BRIDGE DECK	25	200	CONTINUOUS REINFORCED CONCRETE SLAB WITH CAPPED PILE ABUTMENTS
-	-	-	10 LF	MER-274-0318	5403634	PAVE OVER AS ROADWAY	-	-	14'-3" X 8'-11" CORRUGATED STEEL PIPE ARCH CULVERT
-	-	-	10 LF	MER-707-0105	5403944	PAVE OVER AS ROADWAY	-	-	12' X 6' REINFORCED CONCRETE BOX CULVERT
86.24	36.00	345	30 LF	MER-707-0334	5403960	PAVE OVER AS ROADWAY SAW & SEAL JOINTS	25	200	CONTINUOUS REINFORCED CONCRETE SLAB WITH CAPPED PILE ABUTMENTS
-	-	-	0	MER-707-0476	5403987	PAVE OVER AS ROADWAY	-	-	14' X 7' REINFORCED CONCRETE BOX CULVERT
60.00	36.00	240	0	MER-707-0606	5403987	REMOVE EXISTING ASPHALT AND WATERPROOFING, REPLACE DRIP STRIP PLACE NEW WATERPROOFING & ASPHALT CONCRETE, SAW & SEAL JOINTS	25	200	CONTINUOUS REINFORCED CONCRETE SLAB WITH CAPPED PILE ABUTMENTS
-	-	-	5 14 LF	MER-707-0802	5404029	PAVE OVER AS ROADWAY	-	-	10' X 4' REINFORCED CONCRETE BOX CULVERT
234.00	40.00	1040	0	AUG-364-1546	0603643	SKIP CONCRETE BRIDGE DECK	25	222	CONTINUOUS STEEL BEAMS WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE
24.00	40.00	107	0	MER-127-0757	5402468	SKIP CONCRETE BRIDGE DECK, MILL AND FILL ASPHALT CONCRETE OVER APPROACH SLABS, REMOVE AND REPLACE WITH FULL DEPTH PAVEMENT 100' IN ADVANCE OF THE APPROACH SLABS	15	133	CONCRETE SLAB ON REINFORCED CONCRETE ABUTMENTS

STRUCTURES

DESIGN AGENCY



DESIGNER

TMK

REVIEWER

BJA 6-11-25

PROJECT ID

101178

SHEET

P.19

TOTAL

20

MER-29-0488	MER-33-0645	MER-49-1456	MER-118-1302	MER-274-0029	MER-707-0334	MER-707-0606		ITEM	ITEM EXTENSION	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET
				397		418		202	23500	815	SY	WEARING COURSE REMOVED	
	375							254	01000	375	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 2 1/2" DEPTH	
	465							254	01000	465	SY	PAVEMENT PLANING, ASPHALT CONCRETE, VARIABLE DEPTH (1 1/2" TO 2 1/2")	
			533			479		254	01000	1012	SY	PAVEMENT PLANING, ASPHALT CONCRETE, VARIABLE DEPTH (2" TO 5")	
	72			79		76		407	20000	227	GAL	NON-TRACKING TACK COAT	
			63			61		441	50100	124	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M	
	61							442	20001	61	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (448), AS PER PLAN, PG76-22M	
	375			238		264		512	33010	877	SY	TYPE 3 WATERPROOFING	
93	88	72	88	72	83	72		516	31001	568	FT	JOINT SEALER, AS PER PLAN	
	25							519	12300	25	SY	PATCHING CONCRETE BRIDGE DECK, TYPE B	
				107		120		SPECIAL	518E22300	227	FT	STEEL DRIP STRIP	
												QUANTITIES CARRIED TO GENERAL SUMMARY	

NOTE: THE PAVEMENT QUANTITIES AT STRUCTURE MER-127-0757 ARE INCLUDED WITH THE PAVEMENT CALCULATIONS ON SHEET 13.

ITEM 519 - PATCHING CONCRETE BRIDGE DECK, TYPE B

THIS ITEM OF WORK SHALL BE USED TO REPAIR DETERIORATED AREAS ON THE BRIDGE DECK AND APPROACH SLABS ACCORDANCE WITH PROPOSAL NOTE 512 - ITEM SPECIAL PATCHING CONCRETE BRIDGE DECKS. UNSOUND AREAS ARE TO BE MARKED OUT BY THE ENGINEER. THE ENTIRE DECK AND APPROACH SLABS ARE NOT TO BE SOUNDED. PAYMENT FOR THIS ITEM OF WORK SHALL INCLUDE ALL LABOR, MATERIALS, AND INCIDENTALS REQUIRED FOR ITEM 519 PATCHING BRIDGE DECK, TYPE B PER SQUARE YARD.

THIS ITEM APPLIES ON THE FOLLOWING STRUCTURE(S):

MER-33-0645

ITEM 516 - JOINT SEALER, AS PER PLAN (1 1/2" DEEP)

A 1 1/2" DEEP X 1/2" WIDE STRIP SHALL BE SAWCUT OUT OF THE THE PROPOSED OVERLAY. CENTER THE SAWCUT OVER THE CENTER OF THE EXISTING ASPHALT EXPANSION JOINTS THAT WERE COVERED UP BY THE PROPOSED OVERLAY. JOINT SEALER AS PER 705.04 SHALL USED TO SEAL THE JOINTS CREATED.

THE CONTRACTOR SHALL MARK AND VERIFY THE LOCATIONS PRIOR TO ANY MILLING OR PAVING. A QUANTITY HAS BEEN INCLUDED IN THE ESTIMATED QUANTITIES.