

**ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN**

THE CONTRACTOR SHALL MAINTAIN TRAFFIC AT ALL TIMES IN ACCORDANCE WITH THE REQUIREMENTS OF ITEM 614. THE MINIMUM LANE WIDTH FOR TRAFFIC CONTROL SHALL BE 11 FEET (EDGE LINE TO EDGE LINE) AT ALL TIMES. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ORGANIZE HIS WORK IN SUCH A MANNER TO PROVIDE THE MOST SAFETY WITH THE LEAST INCONVENIENCE TO THE TRAVELING PUBLIC.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

THE CONTRACTOR IS RESPONSIBLE FOR DESIGNING THE MAINTENANCE OF TRAFFIC SCHEME. THE CONTRACTOR SHALL SUBMIT, IN WRITING, THIS MAINTENANCE OF TRAFFIC SCHEME AND A SCHEDULE OF OPERATIONS TO THE ENGINEER AND RECEIVE APPROVAL BEFORE WORK IS STARTED ON THE PROJECT.

ANY OPEN PAVEMENT TRENCH OR DROPOFF SHALL BE ADEQUATELY MAINTAINED AND PROTECTED. THE PROTECTION USED SHALL MEET THE REQUIREMENTS OF STD DWG MT-101.90

UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE WORK ZONES WHICH ALTERNATELY CLOSE BOTH THE PASSING AND TRAVEL LANE UNLESS THE DISTANCE BETWEEN THE LANE RESTRICTIONS EXCEEDS 2 MILES.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SMOOTH AND ORDERLY FLOW OF TRAFFIC THROUGH THE PROJECT AREA 24 HOURS PER DAY FOR THE DURATION OF THE PROJECT. THIS CONSISTS OF NOTIFYING THE OHIO STATE PATROL AFTER ENCOUNTERING ANY ACCIDENTS OR **CRASHED VEHICLES OR OBJECTS ENDANGERING THE FLOW OF TRAFFIC. THE RAMP MUST BE BOTH PART WITH AND CANNOT BE SHUT-DOWN AT ANY TIME.**

THE CONTRACTOR SHALL DESIGNATE TO THE ENGINEER A PERSON RESPONSIBLE FOR MAINTENANCE OF TRAFFIC CONTROL DURING NON-WORK HOURS WHO SHALL BE AVAILABLE WITHIN 30 MINUTES AFTER NOTIFICATION.

PAYMENT FOR PROVIDING WATCHMEN, FURNISHING, ERECTING, MAINTAINING AND REMOVING SIGNS, CONES, DRUMS, MARKERS, SPECIAL LIGHTING, FLOODLIGHTING, WORK ZONE PAVEMENT MARKINGS, WORK ZONE RAISED PAVEMENT MARKERS, ETC., SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN.

ALL MAINTENANCE OF TRAFFIC SIGNS SHALL HAVE TYPE "A" WARNING LIGHTS. SIGNS PLACED ON THE MEDIAN BARRIER SHALL BE REDUCED TO 24" AND WILL REQUIRE SPECIAL MOUNTING HARDWARE PER STD MT-105.10. THE CONTRACTOR SHALL VERIFY BARRIER WIDTH PER SIGN LOCATION BEFORE PLACEMENT.

THE CONTRACTOR SHALL FURNISH, INSTALL, AND MAINTAIN ALL ADDITIONAL SIGNS OR OTHER TRAFFIC CONTROL DEVICES AS REQUIRED ABOVE. ALL COSTS INVOLVED IN FURNISHING, INSTALLING AND MAINTAINING THESE DEVICES SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN.

UNLESS THE ENGINEER DEEMS IT PHYSICALLY IMPOSSIBLE, ALL CONSTRUCTION EQUIPMENT SHALL EXIT ALL WORK ZONES FROM THE DOWNSTREAM END OF THE WORK ZONE OR BY INTERCHANGE RAMPS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO DIRECTLY TRANSPORT OR OPERATE ANY EQUIPMENT ACROSS THE OPEN LANES OF THE ROADWAY.

**ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN (CONTINUED)**

WHEN RAISED PAVEMENT MARKERS ARE TO BE INSTALLED, THE REQUIRED LANE CLOSURE SHALL REMAIN IN EFFECT UNTIL THE EPOXY IS DRY AND ALL FOREIGN MATTER OR DEBRIS CREATED BY THE INSTALLATION OF THE RPM CASTING IS REMOVED FROM THE ROADWAY.

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

THE PLANING AND RESURFACING SHALL PROCEED CONTINUOUSLY A MINIMUM OF 5 DAYS PER WEEK, WEATHER PERMITTING, EXCEPT DURING HOLIDAYS, AND EVENTS LISTED BELOW.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS:

- NEW YEAR'S MEMORIAL DAY
- FOURTH OF JULY
- LABOR DAY
- GENERAL/REGULAR ELECTION DAY (NOV)
- THANKSGIVING
- CHRISTMAS

THE PERIOD OF TIME THAT LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVEN FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR SPECIAL EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY (GEN.REG. ELECTION)	5:00AM TUESDAY THROUGH 12:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	6:00AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

DURING THE SAME PERIODS, MAINTAIN PEDESTRIAN ACCESS IF PEDESTRIAN ACCESS WAS PRESENT PRIOR TO CONSTRUCTION SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$3,300 FOR EACH HOUR THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&M'S 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

WINDOW CONTRACT TABLE: USE THE FOLLOWING TABLE AS REFERRED TO IN THE PROPOSAL

DESCRIPTION OF CRITICAL WORK	CALENDAR DAYS TO COMPLETE	DISINCENTIVE \$/PER DAY	WORK WINDOW	
			CONTRACT EXECUTION DATE	PROJECT COMPLETION DATE
ALL WORK ON S.R. 7 IN BELMONT COUNTY	121 DAYS	\$3,300 PER DAY	04-01-2024	09-30-2024

**WORK ZONE SPEED ZONES (WZ-60722)**

THE FOLLOWING WORK ZONE SPEED ZONE (WZSZ) SPEED LIMIT REVISION(S) HAVE BEEN APPROVED FOR USE ON THIS PROJECT WHEN WORK ZONE CONDITIONS AND FACTORS ARE MET AS DESCRIBED BELOW:

WZSZ REVISION NUMBER(S)	CNTY-RT-SECT(S)	DIRECTION(S)
WZ-60722	BEL-7-11.87	NB & SB

POTENTIAL WZSZ LOCATIONS SHALL HAVE AN ORIGINAL (PRE-CONSTRUCTION) POSTED SPEED LIMIT OF 55 MPH OR GREATER, A QUALIFYING WORK ZONE CONDITION OF AT LEAST 0.5 MILE IN LENGTH, AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS, AND A WORK ZONE CONDITION IN PLACE THAT REDUCES THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS (I.E. LANE CLOSURE, LANE SHIFT, CROSSEVER, CONTRAFLOW AND/OR SHOULDER CLOSURE). THE LENGTH OF THE WORK ZONE CONDITION IS MEASURED FROM THE BEGINNING OF THE TAPER FOR THE SUBJECT WORK ZONE CONDITION IMPACTING THE TRAVEL LANES AND/OR SHOULDER TO THE END OF THE DOWNSTREAM TAPER, WHERE DRIVERS ARE RETURNED TO TYPICAL ALIGNMENT. AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS IS REQUIRED TO BALANCE THE ADDITIONAL EXPOSURE CREATED BY INSTALLING AND REMOVING WZSZ SIGNING WITH THE TIME NEEDED TO COMPLETE THE WORK.

IF THE WORK ZONE MEETS THESE MINIMUM CRITERIA, IT SHALL BE ANALYZED FURTHER USING TABLE 1 BELOW TO DETERMINE IF AND WHEN IT QUALIFIES FOR A SPEED LIMIT REDUCTION. DEPENDING ON THE ORIGINAL POSTED SPEED LIMIT, THE TYPE OF TEMPORARY TRAFFIC CONTROL USED, AND WHETHER OR NOT WORKERS ARE PRESENT, A WARRANTED WZSZ WILL VARY IN THE APPROVED SPEED LIMIT TO BE POSTED OVER TIME.

C&M'S ITEM 614, PARAGRAPH 614.02(B), INDICATES THAT TWO DIRECTIONS OF A DIVIDED HIGHWAY ARE CONSIDERED SEPARATE HIGHWAY SECTIONS. THEREFORE, IF THE WORK ON A MULTI-LANE DIVIDED HIGHWAY IS LIMITED TO ONLY ONE DIRECTION, A SPEED LIMIT REDUCTION IN THE DIRECTION OF THE WORK DOES NOT AUTOMATICALLY CONSTITUTE A SPEED LIMIT REDUCTION IN THE OPPOSITE DIRECTION. EACH DIRECTION SHALL BE ANALYZED INDEPENDENTLY FROM EACH OTHER.

ALL WZSZS FLUCTUATE BETWEEN TWO APPROVED REDUCED SPEED LIMITS OR BETWEEN AN APPROVED REDUCED SPEED LIMIT AND THE ORIGINAL POSTED SPEED LIMIT. ONLY ONE OF TWO SIGNING STRATEGIES SHALL BE USED TO IMPLEMENT A WZSZ.

WZSZS USING DSL SIGN ASSEMBLIES SHALL BE IN ACCORDANCE WITH THIS NOTE, APPROVED LIST, SUPPLEMENTAL SPECIFICATIONS (SS) 808 AND 908, AND TRAFFIC SCD MT-104.10

ONLY ONE WARRANTED SPEED LIMIT APPLIES AT ANY ONE TIME; SPEED LIMIT REDUCTIONS ARE NOT CUMULATIVE. WZSZS SHALL NOT BE USED FOR MOVING/MOBILE ACTIVITIES, AS DEFINED IN OMTUCD PART 6.

(CONTINUED...)

**WORK ZONE SPEED ZONES (WZ-60722)**

(CONTINUED...)

WHEN LOOKING UP THE WARRANTED WORK ZONE SPEED LIMITS, ALWAYS USE THE ORIGINAL, PRECONSTRUCTION, POSTED SPEED LIMIT. DO NOT USE A PRIOR OR CURRENT WORK ZONE SPEED LIMIT AS A LOOK UP VALUE IN THE TABLE. POSITIVE PROTECTION IS GENERALLY REGARDED AS PORTABLE BARRIER OR OTHER RIGID BARRIER IN USE ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WITHOUT POSITIVE PROTECTION IS GENERALLY REGARDED AS USING DRUMS, CONES, SHADOW VEHICLE, ETC., ALONG THE WORK ZONE CONDITION. WORKERS ARE CONSIDERED AS BEING PRESENT WHEN ON-SITE, WORKING WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WHEN THE WORK ZONE CONDITION IS REDUCING THE FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS IS REMOVED, THE SPEED LIMIT DISPLAYED SHALL RETURN TO THE ORIGINAL POSTED SPEED LIMIT.

**TABLE 1: WARRANTED WORK ZONE SPEED LIMITS (MPH) FOR WORK ZONES ON HIGH-SPEED (55 MPH OR GREATER) MULTI-LANE HIGHWAYS**

ORIGINAL POSTED SPEED LIMIT	WITH POSITIVE PROTECTION		WITHOUT POSITIVE PROTECTION	
	WORKERS PRESENT	WORKERS NOT PRESENT	WORKERS PRESENT	WORKERS NOT PRESENT
70	60	65	55	65
65	55	60	50	60
60	55	60	50	60
55	50	55	45	55

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 808, DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY - - **28 SIGN MNTH** (ASSUMING 18 DSL SIGN ASSEMBLIES FOR 3 MONTHS.)

**ITEM 614, REPLACEMENT DRUM**

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL

THE FOLLOWING QUANTITY HAS BEEN PROVIDED IN THE GENERAL SUMMARY:

PART 1 (01)/NHS/05)  
ITEM 614 - REPLACEMENT DRUM **10** EACH

**MOVEMENT OF DRUMS**

THE ROW OF DRUMS ALONG A CLOSED LANE SHALL BE MOVED OUT OF THE OPEN LANE ONTO THE NEW PAVEMENT AS SOON AS PAVING OPERATIONS PERMIT.

**PROTECTION OF TRAFFIC MONITORING EQUIPMENT**

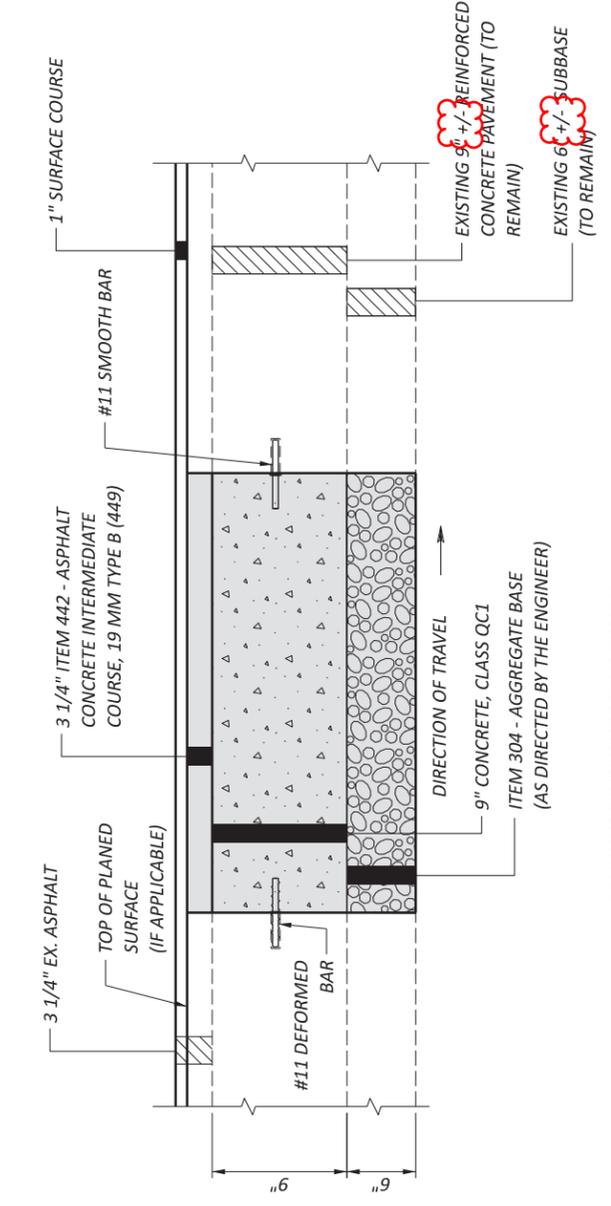
PRIOR TO BEGINNING ANY PAVEMENT ACTIVITIES OR ANY EXCAVATION ACTIVITIES AT: STA 697+40 (SLM13.15) THE CONTRACTOR, THE PROJECT ENGINEER, AND A REPRESENTATIVE FROM THE OWNER WILL COORDINATE A TIME FOR THE OWNER/MAINTAINING AGENCY TO DISCONNECT THE EQUIPMENT. FOLLOWING THE DISCONNECTION BY THE OWNER, THE CONTRACTOR WILL BE ALLOWED TO PERFORM THEIR PAVEMENT ACTIVITIES. THE REMOVED LOOPS AND SENSORS BECOME PROPERTY OF THE CONTRACTOR.

CONTACT ED NEWMAYER (614-204-0914) BEFORE PAVING OPERATIONS BEGIN.

DESIGN AGENCY



DESIGNER: TLC  
REVIEWER: JPB  
PROJECT ID: 10-24-23  
SHEET: 102410  
TOTAL SHEETS: 16  
P.03



**MAINLINE PAVEMENT REPAIR TYPICAL**

FOR DETAILS NOT SHOWN SEE STANDARD CONSTRUCTION DRAWING BP-2.5 FOR TRANSVERSE JOINT REPAIR DETAILS AND BP-2.1 FOR LONGITUDINAL JOINT DETAILS

**ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC1, AS PER PLAN**

THE ESTIMATED QUANTITIES ARE TO BE CONSIDERED APPROXIMATE. A FINAL FIELD REVIEW WILL BE PERFORMED BY ODOT PRIOR TO CONSTRUCTION AND FINAL LOCATIONS WILL BE GIVEN TO THE CONTRACTOR PRIOR TO CONSTRUCTION.

THIS WORK CONSISTS OF REMOVING THE EXISTING ASPHALT CONCRETE, REINFORCED CONCRETE, AND THE AGGREGATE BASE COURSES; SHAPING AND COMPACTING THE EXPOSED MATERIAL; PLACING ITEM 304 AGGREGATE BASE; THEN INSTALLING DOWEL RODS FOLLOWED BY CONCRETE PAVEMENT, CLASS QC1. FINALLY, PLACE ITEM 442, ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE B (449) UP TO THE LEVEL OF THE EXISTING ASPHALT CONCRETE SURFACE.

THIS WORK SHALL BE COMPLETED BEFORE MILLING AND RESURFACING BEGINS.

ALL OTHER PROVISIONS OF STANDARD CONSTRUCTION DRAWINGS BP-2.1 AND BP-2.5 APPLY.

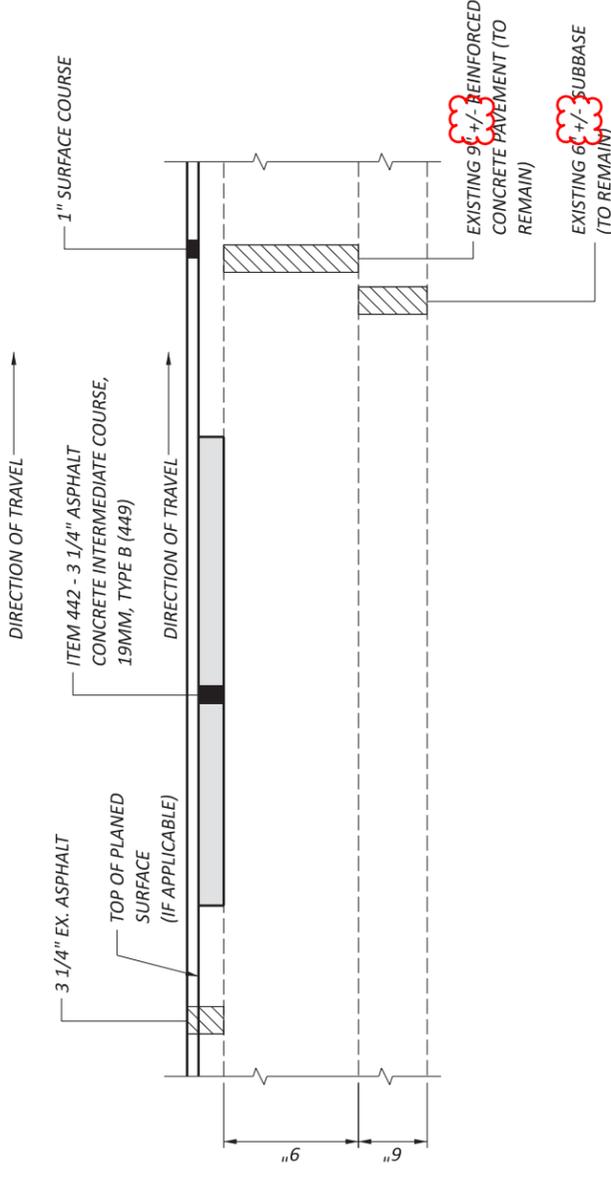
THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN PROVIDED FOR INFORMATION ONLY.

9" CONCRETE, CLASS QC1	125 CU. YD.
ITEM 304 - AGGREGATE BASE	83 CU. YD.
ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE B (449)	44 CU. YD.
ITEM 509 - EPOXY COATED REINFORCING	5826 ROUNDS
ITEM 510 - DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT	940 EACH

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER. FINAL PAYMENT FOR THESE ITEMS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

**ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC1, AS PER PLAN - 500 SQ. YD.**

**ITEM 255 - FULL DEPTH PAVEMENT SAWING ----- 1900 FT.**



**PARTIAL DEPTH PAVEMENT REPAIR TYPICAL**

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (442)**

PARTIAL DEPTH PAVEMENT REPAIRS SHALL BE 3 1/4" INCHES DEEP AND FILLED WITH ITEM 442, ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE B, (449). THE ESTIMATED QUANTITY IS TO BE CONSIDERED APPROXIMATE. A FINAL FIELD REVIEW WILL BE PERFORMED BY ODOT AND FINAL LOCATIONS WILL BE GIVEN TO THE CONTRACTOR PRIOR TO CONSTRUCTION.

ALL PARTIAL DEPTH REPAIRS ARE TO BE COMPLETED AFTER MILLING AND PRIOR TO THE SURFACE COURSE PAVING.

THE ESTIMATED QUANTITY IS TO BE USED AS DIRECTED BY THE ENGINEER. THE ENGINEER WILL DETERMINE THE SIZE AND LOCATION OF EACH PAVEMENT REPAIR. FINAL PAYMENT FOR THE ABOVE ITEMS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (442) - 100 CU. YD.**

