



LOCATION MAP

LATITUDE: 41°24'28.48"N LONGITUDE: 81°38'13.19"W



PORTION TO BE IMPROVED	
INTERSTATE HIGHWAY	
FEDERAL ROUTES	
STATE ROUTES	
COUNTY & TOWNSHIP ROADS	
OTHER ROADS	

DESIGN DESIGNATION

	IR-480 LOCAL LANES	IR-480 EXPRESS LANES
CURRENT ADT (2026)	120,000	52,300
DESIGN YEAR ADT (2046)	125,280	54,600
DESIGN HOURLY VOLUME (2046)	15,033	6,500
DIRECTIONAL DISTRIBUTION	0.64	0.64
TRUCKS (24 HOUR B&C)	0.05	0.05
DESIGN SPEED	65	65
LEGAL SPEED	60	60
DESIGN FUNCTIONAL CLASSIFICATION:	PRINCIPAL ARTERIAL INTERSTATE	PRINCIPAL ARTERIAL INTERSTATE
NHS PROJECT	YES	YES

DESIGN EXCEPTIONS

NONE

ADA DESIGN WAIVERS

NONE

UNDERGROUND UTILITIES

Contact Two Working Days Before You Dig

OHIO811.org
Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
ODOT DISTRICT 12
PLANNING AND ENGINEERING
5500 TRANSPORTATION BLVD
GARFIELD HEIGHTS, OHIO 44125

STATE OF OHIO

DEPARTMENT OF TRANSPORTATION

CUY-480-18.42

DECK SEALING

CUYAHOGA COUNTY

CITIES OF VALLEY VIEW, INDEPENDENCE AND GARFIELD HEIGHTS

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SFN 1812521	18-19
SFN 1812522	20-24
SFN 1812548	25-27

STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
MT-95.30	7/18/25					800-2023 7/18/25	
MT-95.45	7/21/23					832 7/18/25	
MT-98.11	1/17/20						
MT-98.20	4/19/19						
MT-98.22	1/17/20						
MT-98.29	1/17/20						
MT-99.20	4/19/19						
MT-99.60	7/19/24						
MT-102.20	4/19/19						
MT-104.10	1/19/24						
MT-105.10	1/17/20						

FEDERAL PROJECT NUMBER

E250868

RAILROAD INVOLVEMENT

CUYAHOGA VALLEY SCENIC RAILWAY

PROJECT DESCRIPTION

DECK SEALING OF THE IR-480 BRIDGES OVER THE CUYAHOGA RIVER VALLEY IN GARFIELD HEIGHTS, VALLEY VIEW AND INDEPENDENCE LOCATED IN CUYAHOGA COUNTY.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	N/A
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	N/A
NOTICE OF INTENT EARTH DISTURBED AREA:	N/A (NOI NOT REQUIRED)

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS, CHANGES LISTED IN THE PROPOSAL, AND THE SUPPLEMENTAL SPECIFICATION 800 VERSION INDICATED ON THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

John Picuri, P.E., P.S.
District 12 Deputy Director

Pamela Boratyn
Director, Department of Transportation

ENGINEER'S SEAL

BRIDGE

ENGINEER'S SEAL

MAINTENANCE OF TRAFFIC (P.3-P.13) (P.15-P.17)



UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER, OR ADJACENT TO, THE WORK AREA.

EXISTING PLANS

EXISTING PLANS MAY BE INSPECTED IN THE ODOT DISTRICT 12 OFFICE AT 5500 TRANSPORTATION BLVD., GARFIELD HEIGHTS, OH 44125.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PROTECTION OF RIGHT-OF-WAY LANDSCAPING

PRIOR TO BEGINNING WORK, THE CONTRACTOR, THE PROJECT ENGINEER AND A REPRESENTATIVE OF THE MAINTAINING AGENCY WILL REVIEW AND RECORD ALL LANDSCAPING ITEMS WITHIN THE RIGHT-OF-WAY (BOTH WITHIN AND OUTSIDE THE CONSTRUCTION LIMITS). A RECORD OF THIS REVIEW WILL BE KEPT IN THE PROJECT ENGINEER'S FILES. PRIOR TO FINAL ACCEPTANCE, A FINAL REVIEW OF LANDSCAPING ITEMS WILL BE MADE.

CONSTRUCT ALL ACTIVITIES, EQUIPMENT STORAGE AND STAGING TO WITHIN THE CONSTRUCTION LIMITS. UNLESS OTHERWISE IDENTIFIED IN THE PLANS OR PROPOSAL, THE CONSTRUCTION LIMITS ARE IDENTIFIED AS 30 FEET FROM THE EDGE OF PAVEMENT.

SUBMIT A WRITTEN REQUEST TO THE PROJECT ENGINEER TO USE ANY AREA OUTSIDE THESE LIMITS. THE DOCUMENT SUBMITTED MUST CLEARLY IDENTIFY THE AREA AND EXPLAIN THE PROPOSED USE AND RESTORATION OF THE AREA. USE OF THESE AREAS FOR OF WASTE MATERIAL AND CONSTRUCTION DEBRIS, EXCAVATION OF BORROW MATERIAL AND PLACEMENT OF PORTABLE PLANTS IS PROHIBITED. THE REQUEST MUST BE APPROVED, IN WRITING, BEFORE THE CONTRACTOR HAS PERMISSION TO USE THE AREA.

ANY ITEMS DAMAGED BEYOND THE CONSTRUCTION LIMITS, AS DEFINED ABOVE, WILL BE REPLACED IN KIND OR AS APPROVED BY THE PROJECT ENGINEER.

CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED “AS DIRECTED BY THE ENGINEER” UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

RIGHT OF WAY

ALL WORK SHALL BE PERFORMED WITHIN THE EXISTING RIGHT OF WAY OR EASEMENTS.

COOPERATION BETWEEN CONTRACTORS

THE CONTRACTOR SHALL COOPERATE AND COORDINATE OPERATIONS WITH THE CONTRACTORS ON OTHER PROJECTS THAT MAY BE IN FORCE DURING THE LIFE OF THE CONTRACT.

EQUIPMENT AND MATERIAL STORAGE

IN ORDER TO PROVIDE FOR THE SAFETY OF THE TRAVELING PUBLIC, THE CONTRACTOR'S ATTENTION IS DIRECTED TO 614.035. IN ADDITION, NO STORAGE OF EQUIPMENT MATERIALS AND VEHICLES WITH THE HIGHWAY RIGHT-OF-WAY WILL BE PERMITTED WITHOUR PRIOP APPROVAL FROM THE ENGINEER AND OBTAINING AN ODOT R/W PERMIT FROM THE D12 ROADWAY SERVICES. ALL RESTORATION WILL BE AT NO COST TO STATE

STAGING AREAS

THERE ARE NO SPECIFIC AREAS GIVEN IN THE PLANS FOR THE CONTRACTOR TO USE AS A STAGING AREA(S). IF THE CONTRACTOR WANTS TO USE AN AREA(S) FOR STAGING, REGARDLESS IF IT FALLS WITHIN THE PROJECT LIMITS OR NOT, THE CONTRACTOR IS TO USE THE RIGHT OF WAY E-PERMITTING SYSTEM AT [HTTPS://ODHCP.BEMCORP.NET/ACCOUNTS/ACCOUNT/ACCOUNT](https://odhcp.bemcorp.net/accounts/account/account) IN ORDER TO APPLY FOR A PERMIT PER SECTION 107.02 OF THE CMS. FOR SPECIFIC PERMITTING QUESTIONS, THE CONTRACTOR CAN CONTACT THE DISTRICT PERMITTING OFFICE, (MELVIN SAFFORD) AT 216-584-2137, (ANDREW TOMKO) AT 216-584-2195 OR AT DISTRICT12PERMITS@DOT.OHIO.GOV.

IF A PERMIT IS GRANTED, ALL CONDITIONS OF THE PERMIT SHALL BE MET IN ADDITION TO THE REQUIREMENTS OF 104.04 OF THE CMS, AT NO ADDITIONAL COST TO THE STATE. IF THE PROJECT ENGINEER DEEMS THAT ALL THE CONDITIONS OF THE PERMIT WERE NOT MET, THEN 10% OF THE CONTRACT BID AMOUNT FOR MOBILIZATION SHALL BE WITHHELD UNTIL ALL THE CONDITIONS OF THE PERMIT ARE SATISFIED.

RAISED PAVEMENT MARKERS (RPM)

DURING ALL DECK CLEANING AND DECK SEALING OPERATIONS, THE CONTRACTOR SHALL PROTECT ALL EXISTING RAISED PAVEMENT MARKERS (RPM) WITHIN THE PROJECT LIMITS. PROTECTION METHODS SHALL PREVENT DAMAGE OR DISLODGEMENT DUE TO HIGH-PRESSURE WATER, MECHANICAL EQUIPMENT, RESIN COATINGS, OR OTHER CONSTRUCTION ACTIVITIES. PRIOR TO DECK SEALING, ALL RPMS SHALL BE INDIVIDUALLY MASKED USING A DURABLE, NON-ABSORBENT, AND REMOVABLE MATERIAL TO PREVENT RESIN FROM COVERING OR BONDING TO THE MARKERS. ANY RPMS DAMAGED OR OBSCURED DURING THE COURSE OF WORK SHALL BE REPLACED IN KIND AND LOCATION AT NO ADDITIONAL COST TO THE DEPARTMENT, SUBJECT TO THE APPROVAL OF THE ENGINEER.

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURES HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURES AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURES AND THE PROPOSED WORK, BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO C&MS SECTIONS 102.05 AND 105.02.

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURES. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS WHICH HAVE BEEN VERIFIED IN THE FIELD.

ITEM 518 - STRUCTURE DRAINAGE, MISC.: SCUPPER CLEANOUT:

DESCRIPTION: THIS WORK CONSISTS OF REMOVING DIRT AND DEBRIS FROM THE EXISTING DECK SCUPPERS. ALL MATERIAL REMOVED SHALL BE DISPOSED OF AS PER C&MS 105.16 AND 105.17. ALL SCUPPERS SHALL BE CLEANED OUT TO THE SATISFACTION OF THE ENGINEER.

EXECUTION: REMOVE ACCUMULATED DIRT AND DEBRIS FROM SCUPPERS BY METHODS THAT DO NOT FLUSH THE MATERIALS INTO THE DOWNSPOUTS. REMOVE AND REINSTALL THE EXISTING BOLTED SCUPPER GRATES AS REQUIRED TO COMPLETE THE WORK. ALL SCUPPERS SHALL BE FREE OF SEDIMENT AND DEBRIS AT THE COMPLETION OF THE PROJECT. INCLUDE THE COST OF ANY FOLLOW-UP CLEANING REQUIRED TO SATISFY THIS REQUIREMENT IN THE UNIT COST BID.

MEASUREMENT AND PAYMENT: THE DEPARTMENT WILL MEASURE THIS WORK BY THE NUMBER OF EACH ACCEPTED IN PLACE. THE BID PRICE SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THE SCUPPER CLEANOUT. THE DEPARTMENT WILL PAY FOR THE ACCEPTED QUANTITIES AT THE CONTRACT UNIT PRICES BID FOR ITEM 518 - STRUCTURE DRAINAGE, MISC.: SCUPPER CLEANOUT.

THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY:
STRUCTURE REPAIR (CUY-480-18.723L): 30 EACH
STRUCTURE REPAIR (CUY-480-18.723C): 30 EACH
STRUCTURE REPAIR (CUY-480-18.723R): 30 EACH

ITEM 512 – SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)

THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER TO REPAIR CONCRETE SEALING ON THE INTERIOR SURFACE OF EXISTING BRIDGE RAILINGS:

STRUCTURE REPAIR (CUY-480-18.723L): 150 SY
STRUCTURE REPAIR (CUY-480-18.723C): 150 SY
STRUCTURE REPAIR (CUY-480-18.723R): 150 SY

PERFORM WORK IN CONJUNCTION WITH THE DECK SEALING OPERATIONS OR WITH SHOULDER CLOSURES IN ACCORDANCE WITH STANDARD DRAWING MT-95.45.

ITEM 512 – REMOVAL OF EXISTING COATINGS CONCRETE SURFACE

THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER TO REPAIR CONCRETE SEALING ON THE INTERIOR SURFACE OF EXISTING BRIDGE RAILINGS:

STRUCTURE REPAIR (CUY-480-18.723L): 150 SY
STRUCTURE REPAIR (CUY-480-18.723C): 150 SY
STRUCTURE REPAIR (CUY-480-18.723R): 150 SY

PERFORM WORK IN CONJUNCTION WITH THE DECK SEALING OPERATIONS OR WITH SHOULDER CLOSURES IN ACCORDANCE WITH STANDARD DRAWING MT-95.45.

ITEM 621 – RPM REFLECTOR, AS PER PLAN

THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER TO REPLACE MISSING OR DAMAGED RPM REFLECTORS ON THE EXISTING BRIDGE DECKS:

TRAFFIC CONTROL: 50 EACH

PERFORM WORK IN CONJUNCTION WITH THE DECK SEALING OPERATIONS OR WITH SHOULDER CLOSURES IN ACCORDANCE WITH STANDARD DRAWING MT-95.45.

ITEM 626 – BARRIER REFLECTOR, TYPE 1 (1WAY)

THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER TO REPLACE MISSING OR DAMAGED BARRIER REFLECTORS ON THE EXISTING BRIDGE RAILINGS:

TRAFFIC CONTROL: 100 EACH

PERFORM WORK IN CONJUNCTION WITH THE DECK SEALING OPERATIONS OR WITH SHOULDER CLOSURES IN ACCORDANCE WITH STANDARD DRAWING MT-95.45.

ITEM 646 - LANE LINE, 6", AS PER PLAN

THE MAINLINE LANE LINE PAVEMENT MARKING ACROSS BRIDGES AND APPROACH SLABS SHALL INCLUDE SHADOW MARKING USING A SPACING CONSISTING OF A 10 FOOT WHITE DASH STRIPE, 10 FOOT BLACK DASH STRIPE, AND 20 FOOT GAP SPACING.

DESIGN AGENCY



DESIGNER

TSR

REVIEWER

PAT 08/01/25

PROJECT ID

120625

SHEET

P.2

TOTAL

27

MAINTAINING TRAFFIC

GENERALLY, THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS AS TO MAKE THE PROPOSED REPAIR WITH A MINIMUM OF HAZARD, DELAY AND INCONVENIENCE TO THE MOTORISTS USING THE HIGHWAY AFFECTED BY THE WORK DONE UNDER THIS CONTRACT. IN ADDITION TO THE CONSTRUCTION AND MATERIAL SPECIFICATIONS, THE FOLLOWING SPECIFIC PROVISIONS ARE MANDATORY.

I. NOTIFICATION

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE (SEE NEXT COLUMN) TO INFORM THE FOLLOWING:

DISTRICT PUBLIC INFORMATION OFFICER BY EMAIL AT (D12.PUBLIC.INFROMATION@DOT.OHIO.GOV)

DISTRICT PERMIT SECTION BY EMAIL AT (DISTRICT12.PERMITS@DOT.OHIO.GOV)

CENTRAL OFFICE SPECIAL HAUL PERMITS SECTION BY EMAIL AT (HAULING.PERMITS@DOT.OHIO.GOV)

VILLAGE OF VALLEY VIEW
PAUL DEY &
DONALD G. BOHNING
VILLAGE OF CUYAHOGA HEIGHTS
4863 EAST 71ST STREET
CUYAHOGA HEIGHTS, OHIO 44125
PHONE: (216) 642-1130 X104
CELL: (330) 421-7250
PDEY@DBOHNING.COM

VILLAGE OF VALLEY VIEW
TODD SCIANO
DONALD G. BOHNING &
ASSOCIATES, INC.
7979 HUB PARKWAY
VALLEY VIEW, OHIO 44125
PHONE: (216) 642-1130 X 123
TSCIANO@DBOHNING.COM

CITY OF GARFIELD HEIGHTS
JAMES SICKELS-CITY ENGINEER
DIRECT 330-913-1050
CELL 330-338-5349
JAMES.SICKELS@OHM-ADVISORS.COM

CITY OF GARFIELD HEIGHTS
ALLISON HULL, PERMITS
ADMINISTRATIVE ASSISTANT TO
THE BLDG. COMMISSIONER
THE CITY OF GARFIELD HEIGHTS
5407 TURNEY ROAD
GARFIELD HEIGHTS, OH 44125
PHONE: 216-475-3835
AHULL@GARFIELDHTS.ORG

CITY OF INDEPENDENCE
DONALD RAMM
CITY ENGINEER
6335 SELIG DRIVE
INDEPENDENCE, OH 44131
OFFICE: 216-524-1374
RAMMDO@INDEPENDENCEOHIO.ORG

THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE			
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS AND PIO	SIGN DISPLAYED TO PUBLIC
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENADAR DAYS PRIOR TO CLOSURE	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS AND < 2 WEEKS	14 CALENADAR DAYS PRIOR TO CLOSURE	7 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE	2 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES AND RESTRICTIONS	>= 2 WEEKS	14 CALENADAR DAYS PRIOR TO CLOSURE	
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE	
START OF CONSTRUCTION AND TRAFFIC PATTERN CHANGES	N/A	14 CALENADAR DAYS PRIOR TO IMPLEMENTATION	

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

II. LANE CLOSURE RESTRICTIONS

LANE CLOSURE(S) SHALL CONFORM TO THE PLCS. PUBLISHED PLCS INFORMATION CAN BE FOUND ON THE ODOT WEBSITE.

THE MONTHLY PUBLISHED SCHEDULES REQUIRED TO BE USED, FOR EACH PLCS SEGMENT WITHIN THE PROJECT AREA, ARE THOSE THAT COMPRISE THE CONSECUTIVE 12-MONTH PERIOD BEGINNING 15 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE AND ENDING 4 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE. THESE SAME 12 MONTHS APPLY FOR THE LIFE OF THE PROJECT AND SHALL BE APPLIED TO EACH RESPECTIVE MONTH OF CONSTRUCTION (MONTH OF LANE CLOSURE(S) SHALL MATCH MONTH OF PLCS USED). LANE CLOSURE(S) IN PLACE FOR MULTIPLE MONTHS SHALL ALWAYS COMPLY WITH THE CURRENT RESPECTIVE MONTH.

(FOR EXAMPLE: IF THE SALE DATE FOR THE PROJECT WAS MARCH OF 2021, THE MONTHLY PUBLISHED SCHEDULES FOR EACH APPLICABLE PLCS SEGMENT WOULD BE DECEMBER 2019 TO NOVEMBER 2020. IF THIS WAS A THREE-YEAR PROJECT, YEAR THREE WOULD STILL BE USING THE DECEMBER 2019 TO NOVEMBER 2020 MONTHLY SCHEDULES. IF THE PROJECT DESIRED TO CLOSE TWO LANES IN JUNE 2021, REFERENCE WOULD BE MADE TO THE JUNE 2020 SCHEDULE(S) FOR THE RESPECTIVE PLCS SEGMENT(S). IF THE SAME TWO LANES WERE DESIRED TO BE CLOSED AGAIN IN JULY 2021, REFERENCE WOULD BE MADE TO THE JULY 2020 SCHEDULE(S) FOR THE RESPECTIVE PLCS SEGMENT(S).)

MORE RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE AT THE DISCRETION OF THE ENGINEER IN ORDER TO COMPLY WITH THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

LESS RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE SUBJECT TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)) AND SHALL NOT BE IMPLEMENTED UNTIL, AND UNLESS, APPROVED BY THE PROPER ODOT AUTHORITY. [EXISTING MOT EXCEPTIONS THAT HAVE ALREADY BEEN APPROVED IN ACCORDANCE TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY AND STANDARD PROCEDURE ARE DETAILED IN THE APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION(S) PLAN NOTE.]

ALLOWABLE LANE CLOSURE HOURS FOR FACILITIES NOT COVERED BY THE PLCS, IF ANY, SHALL BE AS SPECIFIED ELSEWHERE IN THE PLANS.

III. MAINTENANCE OF TRAFFIC SYSTEMS

1. WHEN REQUIRED
WHENEVER ANY PART OF THE TRAVELED SURFACE IS BEING WORKED UPON OR IS OTHERWISE NOT SUITABLE FOR SAFE AND CONVENIENT USE BY VEHICLES, TRAFFIC CONTROL DEVICES SUFFICIENT TO PROTECT SUCH AREAS TO ASSURE THE SAFE AND CONVENIENT PASSAGE OF VEHICULAR TRAFFIC SHALL BE INSTALLED AND MAINTAINED. SUCH TRAFFIC CONTROL DEVICES AND THE MANNER IN WHICH THEY ARE USED SHALL BE CONSISTENT WITH THESE PLANS AND THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, HEREINAFTER REFERRED TO AS THE “MANUAL”. THE TRAFFIC CONTROL DEVICE SYSTEM SHALL CONSTITUTE THE MINIMUM PROVISIONS FOR TRAFFIC CONTROL FOR EACH PARTICULAR SITUATION. WHENEVER THE ENGINEER DEEMS IT NECESSARY ESPECIALLY WHERE A GRADE, CURVE, OR MERGE CONDITIONS EXISTS, HE MAY DIRECT THAT ADDITIONAL OR ALTERNATIVE DEVICES BE USED.

2. CONDITION
DURING ALL PARTS OF THIS PROJECT FLAGGERS, SIGNING, BARRICADES, FLASHING ARROWS, ETC. SHALL BE LOCATED AS INDICATED IN THE “MANUAL” OR AS SHOWN IN THE STANDARD DRAWINGS.

3. ADVANCE WARNING SIGNS
ALL ADVANCE WARNING SIGNS FOR ANY CONDITION WHICH RESTRICTS TRAFFIC SHALL BE ERECTED BEFORE ANY SUCH RESTRICTION IS PUT INTO EFFECT. ALL SUCH SIGNS SHALL BE COVERED OR REMOVED FROM THE VIEW OF TRAFFIC WHENEVER THEY ARE NOT APPLICABLE.

4. FLAGGERS
AT LEAST ONE FLAGGER IS REQUIRED FOR CLOSURE. THE CONTRACTOR SHALL FURNISH ADDITIONAL FLAGGERS AS DIRECTED BY THE ENGINEER.

5. PROTECTION OF PUBLIC
PERSONAL CARS SHALL NOT BE PARKED WITHIN THE RIGHT OF WAY.

6. FAILURE TO COMPLY
IF THERE IS ANY FAILURE TO COMPLY WITH PROVISIONS FOR TRAFFIC CONTROL SET OUT IN THESE PLANS AND NOTES, OR WITH THE PROVISIONS OF THE “MANUAL”, THE HIGHWAY IN THE VICINITY OF THE WORK AREA SHALL NOT BE CONSIDERED IN A CONDITION FOR THE SAFE AND CONVENIENT USE BY THE TRAVELING PUBLIC. ANY FAILURE TO KEEP THE HIGHWAY, IN THE VICINITY OF THE WORK AREA, IN A CONDITION FOR THE SAFE AND CONVENIENT USE BY THE TRAVELING PUBLIC SHALL BE CONSIDERED A BREACH OF THIS CONTRACT. WORK SHALL BE SUSPENDED UNTIL THE CONTRACTOR COMPLIES WITH THE PROVISIONS OF THE AFOREMENTIONED ITEMS.

IV. MAINTENANCE OF TRAFFIC MATERIALS

1. SIGNS
SIGN DIMENSIONS AND SPECIFICATIONS, INCLUDING LETTER SIZES ARE TO BE AS PROVIDED IN THE “MANUAL”, OR IN DESIGN DRAWINGS PROVIDED BY THE DEPARTMENT OF TRANSPORTATION. THE SIGNS SHALL BE SUBJECT TO APPROVAL OF THE ENGINEER PRIOR TO THE START OF THE PROJECT.

2. SIGN SUPPORT
SIGN SUPPORTS SHALL BE OF SUFFICIENT SIZE AND MASS AS TO SUPPORT THE SIGNS AT THE APPROPRIATE HEIGHT. SUPPORTS SHALL BE AS SHOWN ON THE STANDARD DRAWINGS.

IV. MAINTENANCE OF TRAFFIC MATERIALS (CONT.)

3. FLASHING ARROW REQUIREMENT
WHENEVER ANY PART OF THE TRAVELED SURFACE IS CLOSED, THE MOTORISTS SHALL BE WARNED AND DIRECTED BY THE CONTRACTOR THROUGH THE USE OF ONE FLASHING ARROW PANEL FOR EACH LANE CLOSED. THE CONTRACTOR SHALL REFER TO SUPPLEMENT SPECIFICATION 821 AND THE PROVISIONS SET FORTH IN THE “MANUAL” FOR ALL INFORMATION REGARDING FURNISHING, MAINTAINING, AND USE OF FLASHING ARROW PANELS.

4. DRUMS
DRUMS SHALL BE IN ACCORDANCE WITH PERTINENT SECTIONS OF THE “MANUAL”. ALL COSTS FOR INSTALLING, MAINTAINING, AND SUBSEQUENT REMOVAL OF SAID DRUMS IS TO BE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

5. CONES
CONES, IF UTILIZED, ARE TO BE LOCATED AS SHOWN IN THE “MANUAL” AND THE STANDARD DRAWINGS.

6. BARRIER
PORTABLE BARRIER IF NECESSARY IS TO BE LOCATED AS SHOWN IN THE "MANUAL" AND THE STANDARD DRAWINGS.

7. FLASHERS
FLASHERS SHALL BE 12 VOLT BATTERY-OPERATED MODELS WITH 7 INCH DIAMETER YELLOW LENSES ILLUMINATED BY RAPID INTERMITTENT FLASHERS OF SHORT DURATION AND ARE TO BE PLACED ON ALL SIGNS AT ALL TIMES AS REQUIRED BY THE “MANUAL” AND THE STANDARD CONSTRUCTION DRAWINGS.

8. FLOODLIGHTS
FLOODLIGHTING OF THE WORKSITE FOR OPERATIONS CONDUCTED DURING THE NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND ENGINEER SHALL DRIVE THROUGH THE WORKSITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

9. WORK VEHICLES
ALL WORK VEHICLES LICENSED TO OPERATE ON THE HIGHWAY, SHALL BE EQUIPPED WITH A FLASHING, ROTATING, OR OSCILLATING AMBER LIGHT VISIBLE TO ALL DIRECTIONS OF TRAFFIC FOR A MINIMUM OF ONE-QUARTER MILE IN BRIGHT SUNLIGHT AND SHALL BE OPERATED WITH LIGHTED HEAD AND TAIL LAMPS. THE AMBER LIGHT SHALL BE IN OPERATION AT ALL TIMES WITHIN THE WORK ZONE AND WHILE TRAVELING TO AND FROM THE WORK ZONE WHENEVER THE VEHICLE SPEED IS BELOW THE POSTED LEGAL LIMIT. VEHICLE HAZARD LIGHTS DO NOT SATISFY THIS REQUIREMENT. ALL OTHER EQUIPMENT SHALL BE EQUIPPED WITH A FLASHING, ROTATING, OR OSCILLATING AMBER LIGHT VISIBLE TO ALL DIRECTIONS OF TRAFFIC FOR A MINIMUM OF ONE-QUARTER MILE IN BRIGHT SUNLIGHT. THE AMBER LIGHT SHALL BE IN OPERATION WHILE THE EQUIPMENT IS WITHIN THE WORK ZONE.

V. PAYMENT

PAYMENT FOR PROVIDING, ERECTING, MAINTAINING AND REMOVING TEMPORARY MAINTENANCE OF TRAFFIC CONTROL DEVICES SHALL BE MADE UNDER THE LUMP SUM PRICE BID FOR ITEM 614 - MAINTAINING TRAFFIC.

ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTIFICATION TIME TABLE			
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS AND PIO	SIGN DISPLAYED TO PUBLIC
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENADAR DAYS PRIOR TO CLOSURE	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS AND < 2 WEEKS	14 CALENADAR DAYS PRIOR TO CLOSURE	7 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

CONTINUOUS ACCESS

THE CONTRACTOR SHALL MAINTAIN SAFE AND ADEQUATE DRIVEWAYS AND WALKWAYS IN ORDER TO PROVIDE CONTINUOUS ACCESS FOR PEDESTRIANS, PASSENGER VEHICLES, TRUCKS, AND SAFETY EQUIPMENT TO ALL ADJOINING PROPERTIES. THE COST FOR ALL MATERIALS, EQUIPMENT AND LABOR NECESSARY TO PROVIDE CONTINUOUS ACCESS SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 - MAINTAINING TRAFFIC.

CONSTRUCTION TRAFFIC

ALL CONSTRUCTION TRAFFIC SHALL USE ACCEPTABLE TRUCK ROUTES TO ACCESS THE CONSTRUCTION AREA. USE OF LOCAL RESIDENTIAL STREETS IS STRICTLY PROHIBITED UNLESS ALLOWED IN WRITING BY THE LOCAL ENFORCEMENT AUTHORITY.

LANE CLOSURE DISINCENTIVE

A LANE CLOSURE IS DEFINED AS ANY RESTRICTION OF A LANE OF TRAFFIC INCLUDING, BUT NOT LIMITED TO, SET-UP AND TEAR- DOWN OF TRAFFIC CONTROL ZONES. THE CONTRACTOR WILL BE ASSESSED A DISINCENTIVE FEE AS PER THE LANE VALUE CONTRACT TABLE FOR LANES THAT ARE CLOSED TO TRAFFIC DURING TIMES DESIGNATED AS "LANE CLOSURE NOT PERMITTED" AS STATED IN THESE PLANS AND ON THE ODOT PLCS WEBSITE.

LANE VALUE CONTRACT TABLE

DESCRIPTION OF CRITICAL LANE/RAMP TO BE MAINTAINED	RESTRICTION TIME PERIOD	TIME UNIT	DISINCENTIVE \$ PER TIME UNIT PER LANE
EB I-480	5:00 AM TO 9:00 PM	EACH MINUTE	\$245
WB I-480	5:00 AM TO 9:00 PM	EACH MINUTE	\$245
RAMP I-480 W TO I-77 N/S	5:00 AM TO 9:00 PM	EACH MINUTE	\$230
RAMP I-77 N/S TO I-480 E	5:00 AM TO 9:00 PM	EACH MINUTE	\$205

THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE EQUAL TO THE LARGEST DISINCENTIVE WITHIN ALL SECTIONS IMPACTED BY THE PHYSICAL LANE RESTRICTION, INCLUDING THE TRANSITION AREA, ACTIVITY AREA, AND TERMINATION AREA AS DEFINED BY THE OMTCD. HOLIDAY DISINCENTIVES SHALL BE APPLIED PER SECTION PER LANE PER TIME UNIT.

HOLIDAY CLOSURES

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

NEW YEARS	GENERAL/REGULAR ELECTION DAY (NOV)
MEMORIAL DAY	THANKSGIVING
FOURTH OF JULY (OBSERVED)	CHRISTMAS
LABOR DAY	(OTHER HOLIDAY OR SPECIAL EVENT)

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

HOLIDAY CLOSURES

<u>DAY OF HOLIDAY OR SPECIAL EVENT</u>	<u>TIMES ALL LANES MUST BE OPEN TO TRAFFIC</u>
SUNDAY	12:00N FRIDAY THROUGH 6:00AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00AM WEDNESDAY
TUESDAY (GEN. / REG. ELECTION	5:00AM TUESDAY THROUGH 12:00AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00AM THURSDAY
THURSDAY (THANKSGIVING DAY)	6:00AM WEDNESDAY THROUGH 6:00AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

SUSPENSION OF WORK

IF THE CONTRACTOR FAILS TO COMPLY WITH THE PROVISIONS FOR TRAFFIC CONTROL AS SET FORTH IN THESE PLANS OR WITH PROVISIONS OF THE OMTCD, THE ENGINEER SHALL SUSPEND WORK UNTIL THE CONTRACTOR COMPLIES WITH THE NECESSARY REQUIREMENTS.

MAINTENANCE OF TRAFFIC CONTROL ZONES

THE CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN THE SIGNS, DRUMS OR CONES SPECIFIED IN THE STANDARD CONSTRUCTION DRAWINGS. WHEN THE CONTRACTOR IS NOTIFIED OF DEFICIENCIES, HE SHALL CORRECT THE DEFICIENCIES AS SOON AS POSSIBLE, PREFERABLY WITHIN 12 HOURS AND NO LATER THAN 24 HOURS. IF ANY NOTED DEFICIENCIES ARE NOT CORRECTED WITHIN 24 HOURS THE ENGINEER SHALL DEDUCT ONE DAY PAY FOR ITEM 614 – MAINTAINING TRAFFIC, NOT AS A PENALTY BUT AS ROAD USER COSTS. THE CONTRACTOR SHALL BE SUBJECT TO THESE ROAD USER COSTS FOR EACH AND EVERY DAY THAT THESE PROVISIONS ARE NOT MET. ALL COSTS FOR MAINTAINING THE WORK ZONES AS DESCRIBED ABOVE SHALL BE INCLUDED UNDER ITEM 614 – MAINTAINING TRAFFIC.

CONSTRUCTION EQUIPMENT MAINTENANCE CROSSING

CONSTRUCTION EQUIPMENT IS PERMITTED TO ONLY CROSS THE MEDIAN AT THE EXISTING INTERCHANGES, INTERSECTIONS, AND U-TURN CROSSEOVERS.

COVERING OF GROUND-MOUNTED SIGNS – GENERAL

WHEN REQUIRED BY OTHER ITEMS OR INCIDENTALLY TO ITEM 614 – MAINTAINING TRAFFIC, COVER EXISTING GROUND-MOUNTED SIGNS WITH PLYWOOD OR OSB BLANKS (1/2" MINIMUM THICKNESS) COVERING 80% OF THE SIGN AREA AND ALL OF THE SIGN LEGEND. THE USE OF LOW QUALITY MATERIALS SUCH AS DUCT TAPE AND BLACK PLASTIC IS NOT PERMITTED.

CONTRACTOR'S EQUIPMENT – OPERATION AND STORAGE

VEHICLES AND EQUIPMENT SHALL ALWAYS MOVE WITH, AND NOT ACROSS OR AGAINST THE FLOW OF TRAFFIC. VEHICLES AND OTHER EQUIPMENT SHALL NOT PARK OR STOP EXCEPT WITHIN DESIGNATED WORK AREAS; AND SHALL NOT ENTER AND LEAVE WORK AREAS IN A MANNER WHICH WILL BE HAZARDOUS TO OR INTERFERE WITH THE NORMAL TRAFFIC FLOW. PERSONAL VEHICLES WILL NOT BE PERMITTED TO PARK WITHIN THE RIGHT OF WAY EXCEPT IN SPECIFIC AREAS DESIGNATED BY THE ENGINEER.

EQUIPMENT, VEHICLES AND MATERIALS SHALL NOT BE STORED OR PARKED WITHIN 30 FEET OF THE TRAVELED WAY UNLESS 6 FEET BEHIND PCB OR GUARDRAIL.

ALL WORK VEHICLES AND EQUIPMENT THAT ENTERS THE WORK ZONE MORE THAN ONCE A DAY MUST BE EQUIPPED WITH AT LEAST ONE FLASHING, ROTATING, OR OSCILLATING AMBER LIGHT THAT IS VISIBLE IN ALL DIRECTIONS OF TRAFFIC FOR AT LEAST ONE QUARTER OF A MILE, DAY OR NIGHT.

MAJOR WORK ITEMS

THE FOLLOWING MAJOR WORK ITEMS WILL REQUIRE TRAFFIC MAINTENANCE WHICH SHALL BE INCORPORATED INTO THE CONTRACTOR'S SEQUENCE OF OPERATIONS.

- A. REMOVAL OF EXISTING PAVEMENT MARKINGS ON BRIDGE DECKS.
- B. CLEANING OF DECK SURFACE.
- C. SEALING OF DECK SURFACE GRAVITY FED RESIN.

SEQUENCE OF CONSTRUCTION

EASTBOUND LOCAL PHASE 1
ALL I-480 EB TRAFFIC WILL BE DIRECTED INTO THE EXPRESS LANE. TRAFFIC FROM THE I-77 RAMPS WILL BE DIRECTED INTO THE LEFT LANE ON THE EB LOCAL DECK. THE EXIT TO TRANSPORTATION WILL BE CLOSED AND THE DETOUR WILL FOLLOW THE ROUTE SHOWN ON P.13/27.

EASTBOUND LOCAL PHASE 2
ALL I-480 EB TRAFFIC WILL BE DIRECTED INTO THE EXPRESS LANE. TRAFFIC FROM THE I-77 RAMPS WILL BE DIRECTED INTO THE RIGHT LANE ON THE EB LOCAL DECK. TRANSPORTATION EXIT WILL BE OPEN FOR TRAFFIC COMING FROM I-77. THE DETOUR ROUTE FOR TRAFFIC FROM I-480 TO TRANSPORTATION WILL FOLLOW THE ROUTE SHOWN ON P.13/27.

WESTBOUND LOCAL PHASE 1
THE RIGHT LANE ON THE WB LOCAL DECK SHALL REMAIN OPEN TO ALLOW TRAFFIC TO EXIT TO THE I-77 RAMPS. I-480 WB TRAFFIC WILL BE DIRECTED TO TAKE THE EXPRESS LANES. TRANSPORTATION TO I-480 WB RAMP WILL BE CLOSED. THE DETOUR ROUTE WILL FOLLOW THE ROUTE SHOWN ON P.12/27.

WESTBOUND LOCAL PHASE 2
THE RIGHT LANE OF I-480 WB WILL BE CLOSED AFTER THE I-480 WB EXIT TO TRANSPORTATION BLVD. TRANSPORTATION TO I-480 WB RAMP WILL BE CLOSED. THE DETOUR ROUTE WILL FOLLOW THE ROUTE SHOWN ON P.12/27.

EASTBOUND EXPRESS LANES
A SINGLE LEFT LANE CLOSURE WILL BE IMPLEMENTED PRIOR TO THE EB EXPRESS LANES. ALL I-480 EB TRAFFIC WILL BE DIRECTED TO THE LOCAL LANES.

WESTBOUND EXPRESS LANES
A SINGLE LEFT LANE CLOSURE WILL BE IMPLEMENTED PRIOR TO THE WB EXPRESS LANES. ALL I-480 WB TRAFFIC WILL BE DIRECTED TO THE LOCAL LANES.

DESIGN AGENCY



DESIGNER

TSR

REVIEWER

TLO 09/26/25

PROJECT ID

120625

SHEET

P.4

TOTAL

27

ITEM 630 – SIGNING MISC.: ADDITIONAL SIGNS, GROUND MOUNTED, AS DIRECTED BY THE ENGINEER

WHEN ADDITIONAL SIGNING IS NEEDED TO MAINTAIN TRAFFIC, THE CONTRACTOR SHALL FURNISH THE SIGN OR SIGNS AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE GROUND MOUNTED AND MEET ALL THE SPECIFICATIONS OF THE PLAN, PROPOSAL AND CURRENT YEAR CMS.

PAYMENT FOR THIS ITEM SHALL INCLUDE, BUT NOT BE LIMITED TO, THE COST TO FURNISH AND ERECT THE SIGN, INCLUDING DRIVING POSTS OR OTHER APPROVED METHODS OF SIGN SUPPORT, MAINTAINING THE SIGN AND REMOVAL OF THE SIGN.

THIS ITEM OF WORK SHALL BE USED TO PROVIDE SIGNS THAT ARE BEYOND THE REQUIREMENTS OF THE SIGNAGE DETAILED IN THE STANDARD CONSTRUCTION DRAWINGS AND THE OMUTCD.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

ITEM 630 – SIGNING MISC.: ADDITIONAL SIGNS, GROUND MOUNTED, AS DIRECTED BY THE ENGINEER **300 SQ FT**

ITEM 614 – MAINTAINING TRAFFIC LANE CLOSURE-REDUCTION REQUIRED

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT BID PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

WORKSITE TRAFFIC SUPERVISOR

SUBJECT TO APPROVAL OF THE ENGINEER, THE CONTRACTOR SHALL EMPLOY AND IDENTIFY (SOMEONE OTHER THAN THE SUPERINTENDENT) A PREQUALIFIED WORKSITE TRAFFIC SUPERVISOR (WTS) BEFORE STARTING WORK IN THE FIELD. THE WTS SHALL BE TRAINED IN ACCORDANCE WITH CMS 614.03, SHALL HAVE SUCCESSFULLY COMPLETED ODOT ADMINISTERED WTS TESTING (AND RE-TESTING WHEN APPLICABLE) AND BE LISTED ON THE ODOT PREQUALIFIED WTS ROSTER. PREQUALIFICATION EXPIRES EVERY 5 YEARS. RE-TESTING SHALL BE SUCCESSFULLY REPEATED EVERY 5 YEARS TO REMAIN PREQUALIFIED.

THE NAME OF THE PREQUALIFIED WTS AND RELATED 24-HOUR CONTACT INFORMATION SHALL BE PROVIDED TO THE ENGINEER AT THE PRECONSTRUCTION CONFERENCE. IF THE DESIGNATED WTS WILL NOT BE AVAILABLE FULL TIME (24/7), THE CONTRACTOR MAY DESIGNATE AN ALTERNATE (SECONDARY) WTS TO BE AVAILABLE WHEN THE PRIMARY IS OFF DUTY; HOWEVER THE PRIMARY WTS SHALL REMAIN THE POINT OF CONTACT AT ALL TIMES. ANY ALTERNATE (SECONDARY) WTS IS SUBJECT TO THE SAME TRAINING, PREQUALIFICATION AND OTHER REQUIREMENTS OUTLINED WITHIN THIS PLAN NOTE. AT ALL TIMES THE ENGINEER, OR ENGINEER’S REPRESENTATIVES, MUST BE INFORMED OF WHO THE PRIMARY WTS (AND SECONDARY WTS, IF APPLICABLE) IS AT THE CURRENT TIME.

THE WTS POSITION HAS THE PRIMARY RESPONSIBILITY OF IMPLEMENTING THE TRAFFIC MANAGEMENT PLAN (TMP), MONITORING THE SAFETY AND MOBILITY OF THE ENTIRE WORK ZONE, AND CORRECTING TEMPORARY TRAFFIC CONTROL (TTC) DEFICIENCIES FOR THE ENTIRE WORK ZONE. THE WTS, AND ALTERNATE WTS WHEN ON DUTY, SHALL HAVE SUFFICIENT AUTHORITY TO EFFECTIVELY CARRY OUT THE IDENTIFIED WTS RESPONSIBILITIES AND DUTIES. THE DUTIES OF THE WTS ARE AS FOLLOWS:

- BE AVAILABLE ON A 24-HOUR PER DAY BASIS.
- BE ON SITE FOR ALL EMERGENCY TTC NEEDS WITHIN ONE HOUR OF NOTIFICATION BY POLICE OR PROJECT STAFF, AND EFFECT CORRECTIVE MEASURES IMMEDIATELY ON EXISTING WORK ZONE TTC DEVICES.
- ATTEND PRECONSTRUCTION MEETING AND ALL PROJECT MEETINGS WHERE TTC MANAGEMENT IS DISCUSSED.
- BE AVAILABLE ON SITE FOR OTHER MEETINGS OR DISCUSSIONS WITH THE ENGINEER UPON REQUEST.
- BE AWARE OF ALL EXISTING AND PROPOSED TTC OPERATIONS OF THE CONTRACTOR, SUBCONTRACTORS AND SUPPLIERS, AND ENSURE COORDINATION OCCURS BETWEEN THEM TO ELIMINATE CONFLICTING TEMPORARY AND/OR PERMANENT TRAFFIC CONTROL.
- COORDINATE PROJECT ACTIVITIES WITH ALL LAW ENFORCEMENT OFFICERS (LEOS). THE WTS SHALL ALSO BE THE MAIN CONTACT PERSON WITH THE LEOS WHILE LEOS ARE ON THE PROJECT.
- COORDINATE AND FACILITATE MEETINGS WITH ODOT PERSONNEL, LEOS AND OTHER APPLICABLE ENTITIES BEFORE EACH PLAN PHASE SWITCH TO DISCUSS THE WORK ZONE TTC FOR IMPLEMENTING THE PHASE SWITCH. SUBMIT A WRITTEN DETAIL OF MOT OPERATIONS AND SCHEDULE OF EVENTS TO IMPLEMENT THE SWITCH BETWEEN PHASE PLANS TO THE ENGINEER 5 CALENDAR DAYS PRIOR TO THIS MEETING.
- BE PRESENT, ON SITE FOR, AND INVOLVED WITH, EACH TTC SET UP/TAKE DOWN AND EACH PHASE CHANGE IN ACCORDANCE WITH CMS 614.03.
- ON A CONTINUAL BASIS ENSURE THAT THE TTC ZONE AND ALL RELATED DEVICES ARE INSTALLED, MAINTAINED AND REMOVED IN COMPLIANCE WITH THE CONTRACT DOCUMENTS.

WORKSITE TRAFFIC SUPERVISOR (CONT.)

10. ON A CONTINUAL BASIS FACILITATE CORRECTIVE ACTION(S) NECESSARY TO BRING DEFICIENT TTC ZONES AND ALL RELATED DEVICES INTO COMPLIANCE WITH CONTRACT DOCUMENTS IN THE TIMEFRAME DETERMINED BY THE ENGINEER.

11. INSPECT, EVALUATE, PROPOSE NECESSARY MODIFICATIONS TO, AND DOCUMENT THE EFFECTIVENESS OF, THE TTC DEVICES AND TRAFFIC OPERATIONS ON A DAILY BASIS (7 DAYS A WEEK). IN ADDITION, PERFORM ONE WEEKLY NIGHT INSPECTION OF THE WORK ZONE SETUP FOR DAYTIME WORK OPERATIONS; AND ONE DAYTIME INSPECTION PER WEEK FOR NIGHTTIME PROJECTS. THIS SHALL INCLUDE (BUT NOT BE LIMITED TO) DOCUMENTATION ON THE FOLLOWING PROJECT EVENTS:

- INITIAL TTC SETUP (DAY AND NIGHT REVIEW).
- DAILY TTC SETUP AND REMOVAL.
- WHEN CONSTRUCTION STAGING CAUSES A CHANGE IN THE TTC SETUP.
- CRASH OCCURRENCES WITHIN THE CONSTRUCTION AREA AND WITHIN THE INFLUENCE AREA(S) APPROACHING THE WORK ZONE.
- REMOVAL OF TTC DEVICES AT THE END OF A PHASE OR PROJECT.
- ALL OTHER EMERGENCY TTC NEEDS.

12. COMPLETE THE DEPARTMENT APPROVED LONG TERM INSPECTION FORM (CA-D-8) AFTER EACH INSPECTION AS REQUIRED IN # 11 AND SUBMIT IT TO THE ENGINEER THE FOLLOWING WORKDAY. THESE REPORTS SHALL INCLUDE A CHECKLIST OF ALL TTC MAINTENANCE ITEMS TO BE REVIEWED. A COPY OF THE FORM WILL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING. ANY DEFICIENCIES OBSERVED SHALL BE NOTED, ALONG WITH RECOMMENDED OR COMPLETED CORRECTIVE ACTIONS AND THE DATES BY WHICH SUCH CORRECTIONS WERE, OR WILL BE, COMPLETED. A COPY OF THE CURRENT CA-D-8 DOCUMENT CAN BE FOUND ON THE OFFICE OF CONSTRUCTION ADMINISTRATION’S INSPECTION FORMS WEBSITE.

13. HAVE COPIES OF THE ODOT TEMPORARY TRAFFIC CONTROL MANUAL AND CONTRACT DOCUMENTS AVAILABLE AT ALL TIMES ON THE PROJECT.

THE DEPARTMENT WILL DEDUCT:

- THE PRORATED DAILY AMOUNT OF ITEM 614 MAINTAINING TRAFFIC FOR ANY DAY IN WHICH THE WTS FAILS TO PERFORM THE DUTIES SET FORTH ABOVE. THE PRORATED DAILY AMOUNT WILL BE EQUAL TO THE ORIGINAL BID AMOUNT FOR ITEM 614 MAINTAINING TRAFFIC DIVIDED BY THE DIFFERENCE BETWEEN THE ORIGINAL COMPLETION DATE AND THE FIRST DAY OF WORK, IN CALENDAR DAYS.
- 1% OF THE ORIGINAL BID AMOUNT FOR ITEM 614 MAINTAINING TRAFFIC FOR ANY DAY THAT A TTC ISSUE IS IDENTIFIED IN THE FIELD AND IS NOT CORRECTED IN THE GIVEN TIMEFRAME PER THE ENGINEER. DEDUCTION B SHALL NOT APPLY TO SITUATIONS COVERED BY DEDUCTION C.
- 1% OF THE ORIGINAL BID AMOUNT FOR ITEM 614 MAINTAINING TRAFFIC FOR ANY DAY THAT A LANE OR RAMP IS BLOCKED (FULLY OR PARTIALLY) WITHOUT TTC, AS DETERMINED BY THE ENGINEER. THIS DEDUCTION SHALL BE IN ADDITION TO ANY OTHER DISINCENTIVES ESTABLISHED FOR UNAUTHORIZED LANE USE.

FOR DAYS IN WHICH MORE THAN ONE DEDUCTION LISTED ABOVE OCCUR, THE HIGHEST DEDUCTION AMOUNT WILL APPLY.

WORKSITE TRAFFIC SUPERVISOR (CONT.)

IF THREE OR MORE TOTAL DAYS RESULT IN TTC ISSUES DESCRIBED IN DEDUCTION B OR C ABOVE, THE PRIMARY WTS SHALL BE IMMEDIATELY REMOVED FROM THE WORK IN ACCORDANCE WITH C&MS 108.05. UPON REMOVAL THE ENGINEER SHALL NOTIFY ODOT CENTRAL OFFICE (WTSPREQUALIFICATION@DOT.OHIO.GOV) TO REGISTER A REMOVAL AGAINST THE STATEWIDE PREQUALIFICATION FOR THE PRIMARY WTS. THREE REMOVALS SHALL CAUSE STATEWIDE DISQUALIFICATION FOR ANY PREVIOUSLY PREQUALIFIED WTS.

PAYMENT FOR THE ABOVE REQUIREMENTS, RESPONSIBILITIES AND DUTIES SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC.



DESIGN AGENCY	
DESIGNER	TSR
REVIEWER	
TLO PROJECT ID	09/26/25
120625	
SHEET	TOTAL
P.5	27

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

- DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.
- DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).
- FOR OPERATIONS WITHOUT POSITIVE PROTECTION OCCURRING WITHIN 10 FEET OF AN OPEN TRAVELED LANE THAT MEET ALL OF THE FOLLOWING CRITERIA:
 - ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND
 - AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND,
 - AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

“WITHOUT POSITIVE PROTECTION” MEANS USE OF DRUMS, CONES, SHADOW VEHICLE, ETC., WITHOUT PROTECTION FROM PORTABLE BARRIER OR OTHER RIGID BARRIER ALONG THE WORK AREA. THIS PHRASE DOES NOT APPLY TO CASES WHERE POSITIVE PROTECTION IS REQUIRED. MOBILE OPERATIONS ARE REGARDED AS “WITHOUT POSITIVE PROTECTION”. FOR WORK ZONES USING A COMBINATION OF BARRIER AND TEMPORARY TRAFFIC CONTROL DEVICES (CONES, DRUMS, ETC.), THE DESIGNATION SHALL BE BASED UPON THE TYPE OF DEVICES USED IN THE AREA THAT WORKERS ARE LOCATED.

IF MULTIPLE ACTIVE LOCALIZED QUALIFYING WORK AREAS OCCUR WITHOUT POSITIVE PROTECTION, PER MAINLINE TRAFFIC DIRECTION, PROVIDE A UNIFORMED LEO AND OFFICIAL PATROL CAR IN ADVANCE OF:

- THE FIRST ACTIVE WORK AREA THAT DRIVERS WILL ENCOUNTER; OR
- THE ACTIVE WORK AREA Laterally CLOSEST TO THE OPEN TRAVELED LANE; OR
- OTHER LOCATION AS APPROVED BY THE ENGINEER.

THE UNIFORMED LEO AND OFFICIAL PATROL CAR MAY RELOCATE AMONG THE LISTED LOCATIONS AS APPROPRIATE AS THE OPERATIONS PROCEED IN THE LOCALIZED QUALIFYING WORK AREAS.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONT.)

IN GENERAL, LEO’S SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST’S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS’ DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE **500 HOURS**

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

ITEM 614 – PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR’S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

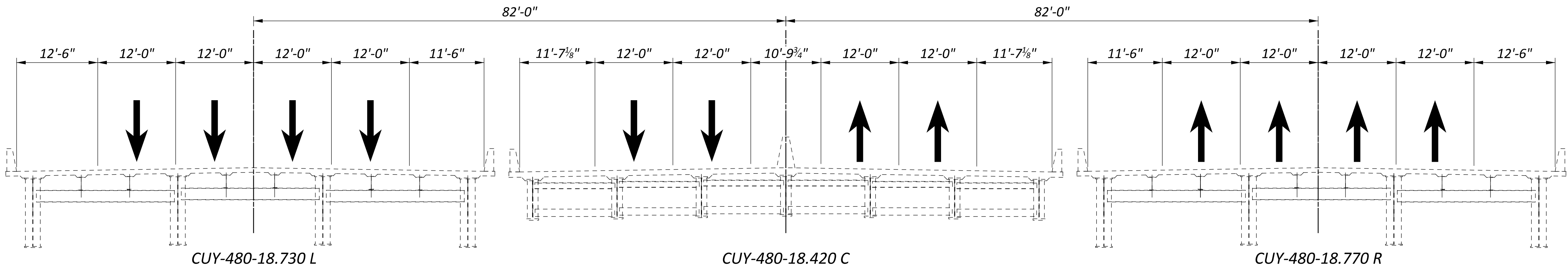
THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

ITEM 614 – PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN (CONT.)

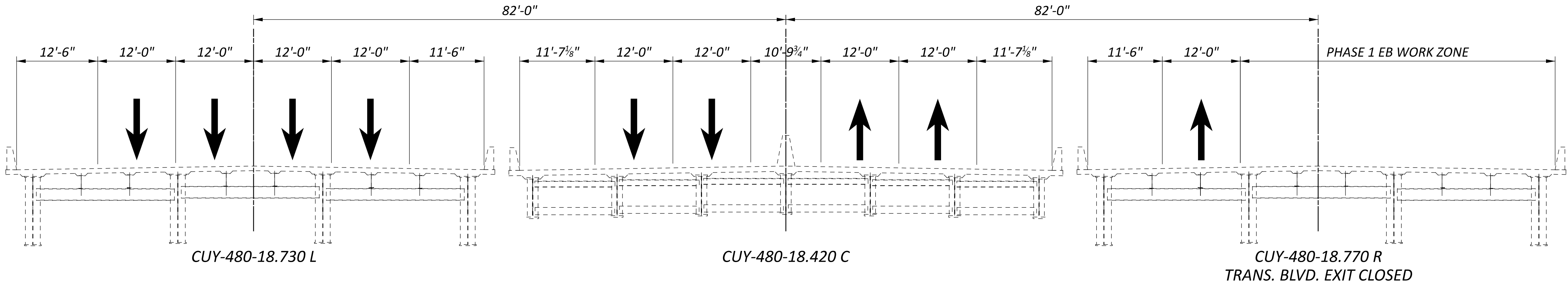
THE PORTABLE CHANGEABLE MESSAGE SIGN SHALL HAVE A WEB BASED COMMUNICATION SYSTEM THAT WILL ALLOW THE CONTRACTOR OR ODOT TO CHANGE OR PROGRAM THE MESSAGE BOARD REMOTELY. THIS SYSTEM SHALL BE PASSWORD PROTECTED AND MAY BE OPERATED FROM A COMPUTER OR HAVE AN APPLICATION THAT CAN BE OPENED FROM A CELL PHONE, ANDROID OR I PHONE. THE WEB BASED COMMUNICATION SYSTEM WILL SHOW THE LOCATION OF EACH MESSAGE BOARD ON A MAP. ALL CHARGES FOR THE WEB BASED COMMUNICATION SYSTEM WILL BE INCLUDED IN THE COST OF THIS ITEM, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

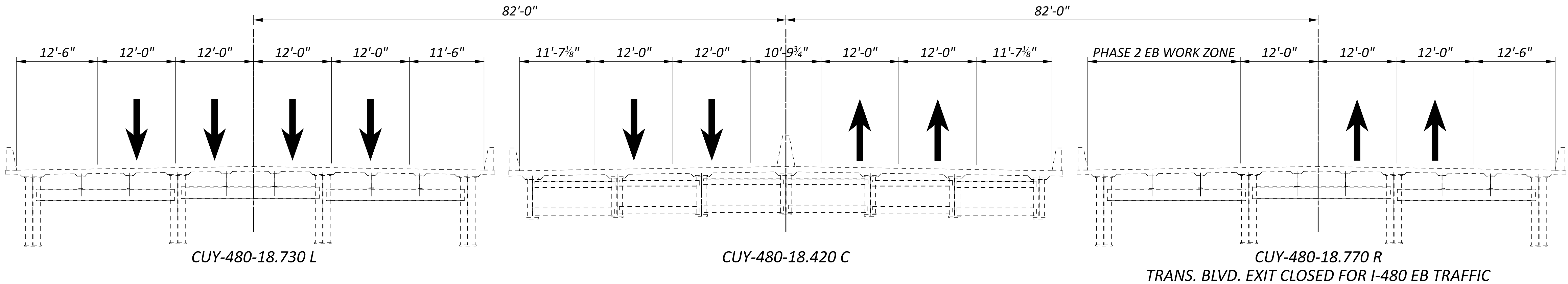
ITEM 614 – PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN **16 SIGN MONTH(S)** ASSUMING 2 PCMS SIGNS FOR 8 MONTHS AVERAGE.



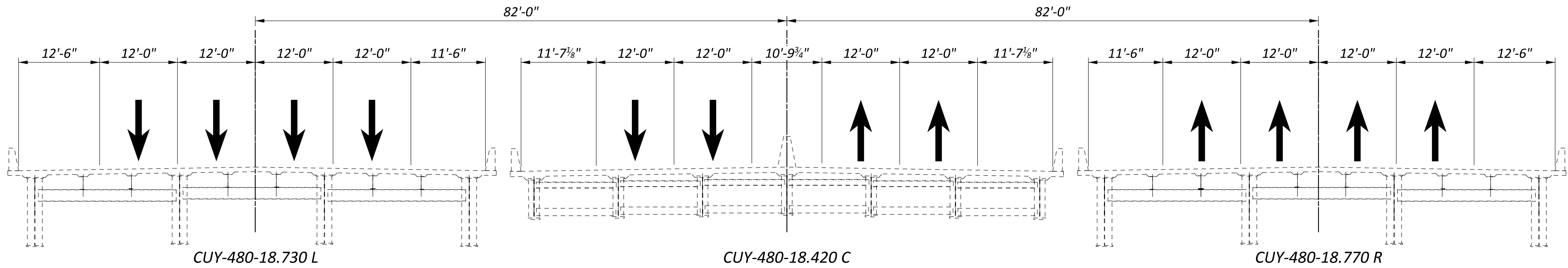
EXISTING MAINTENANCE OF TRAFFIC TYPICAL SECTIONS



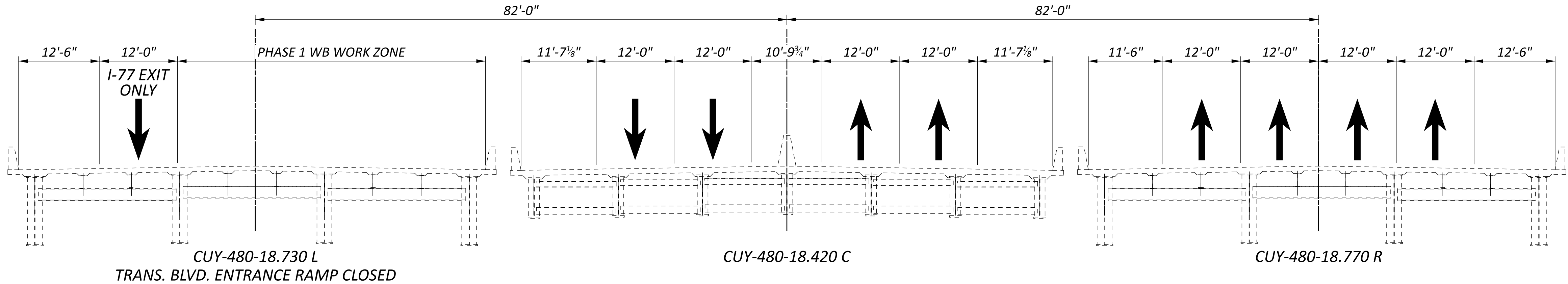
MAINTENANCE OF TRAFFIC TYPICAL SECTIONS- PHASE 1



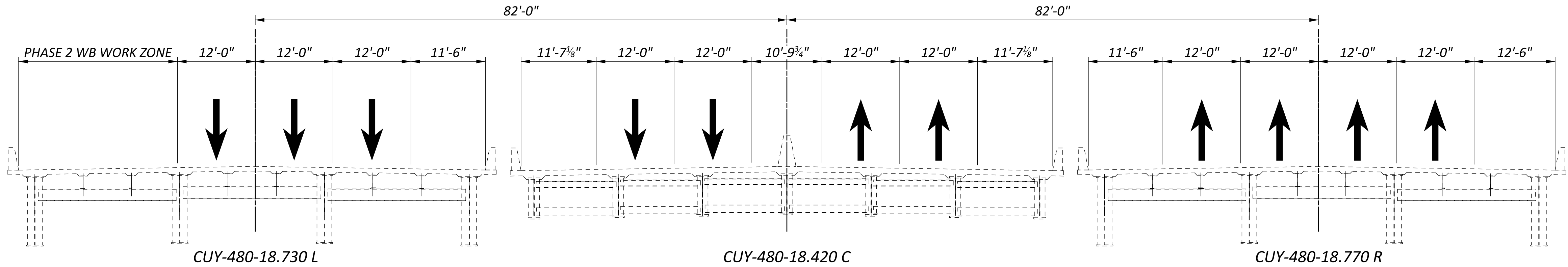
MAINTENANCE OF TRAFFIC TYPICAL SECTIONS- PHASE 2



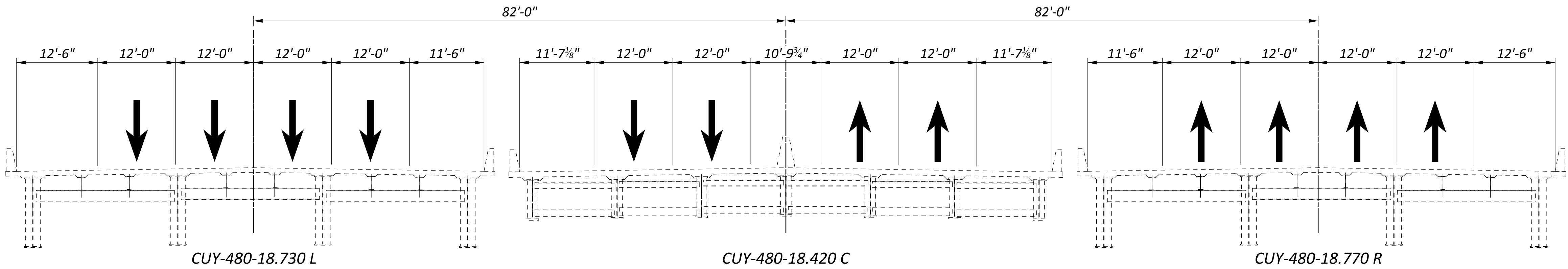
EXISTING MAINTENANCE OF TRAFFIC TYPICAL SECTIONS



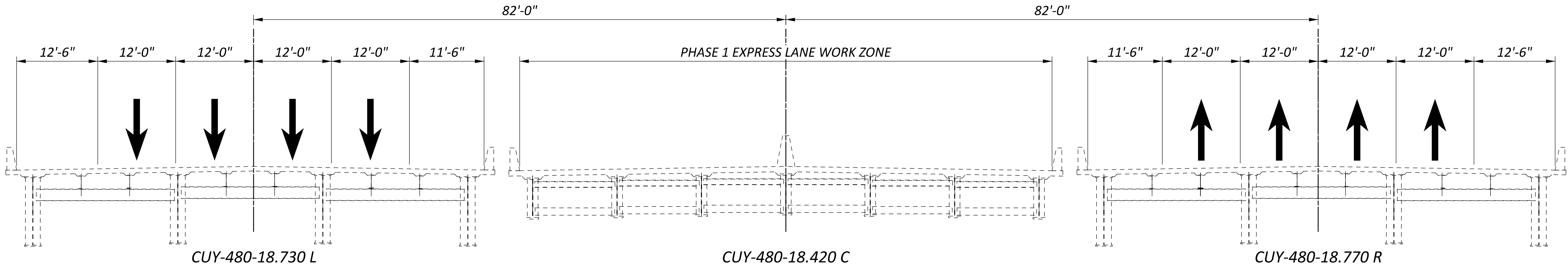
MAINTENANCE OF TRAFFIC TYPICAL SECTIONS- PHASE 1



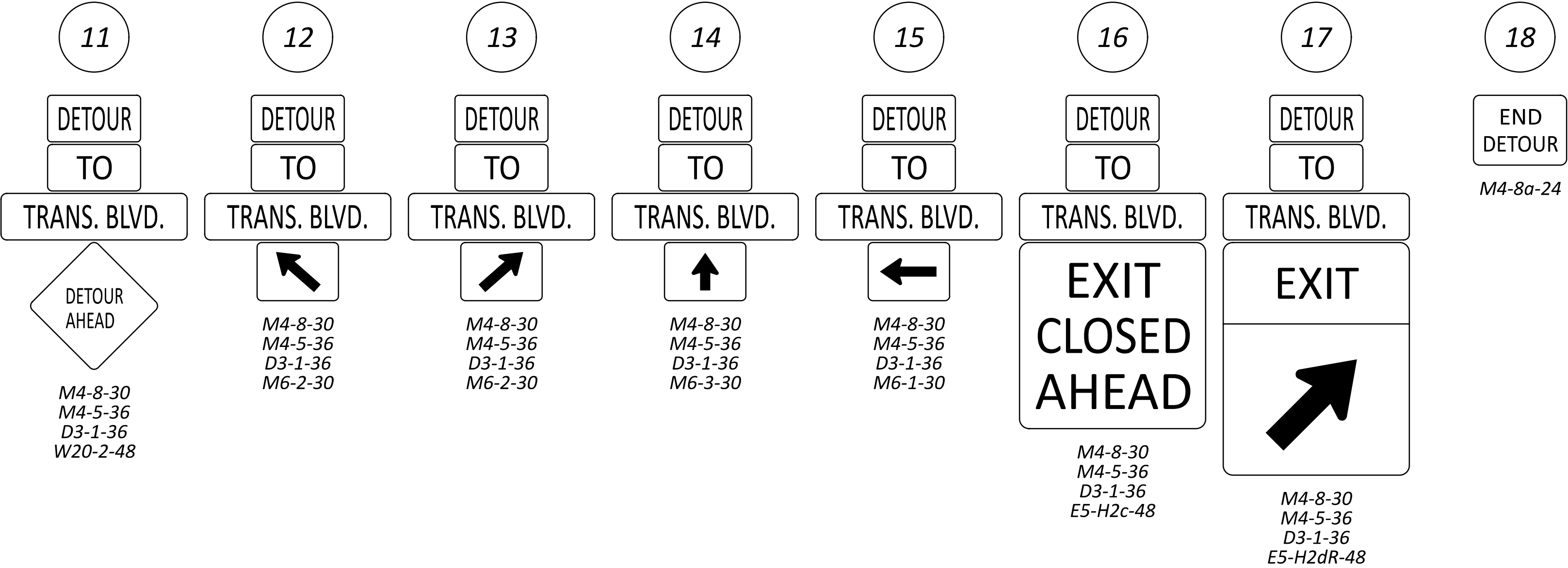
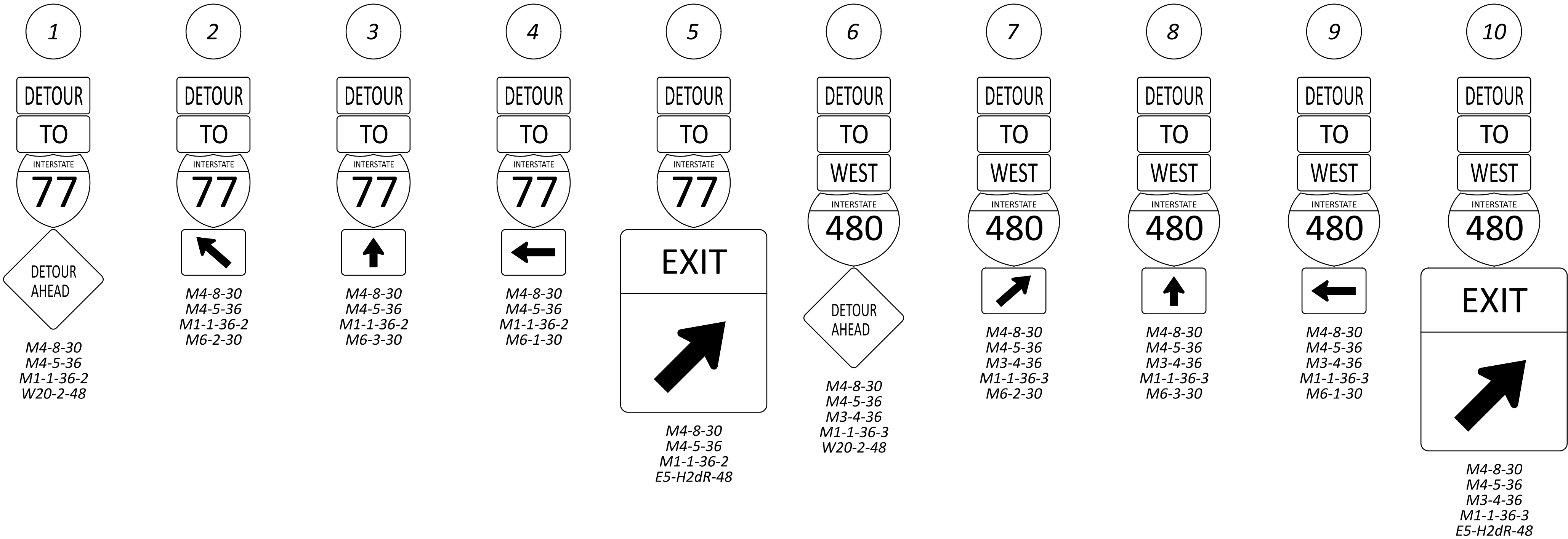
MAINTENANCE OF TRAFFIC TYPICAL SECTIONS- PHASE 2



EXISTING MAINTENANCE OF TRAFFIC TYPICAL SECTIONS



MAINTENANCE OF TRAFFIC TYPICAL SECTIONS- PHASE 1



19

PORTABLE CHANGEABLE MESSAGE SIGN

	PRIOR TO CLOSURE	DURING CLOSURE
MESSAGE 1	I-77 RAMP CLOSED	I-77 RAMP CLOSED
MESSAGE 2	XX/XX - XX/XX 9P-5A	USE DETOUR

20

PORTABLE CHANGEABLE MESSAGE SIGN

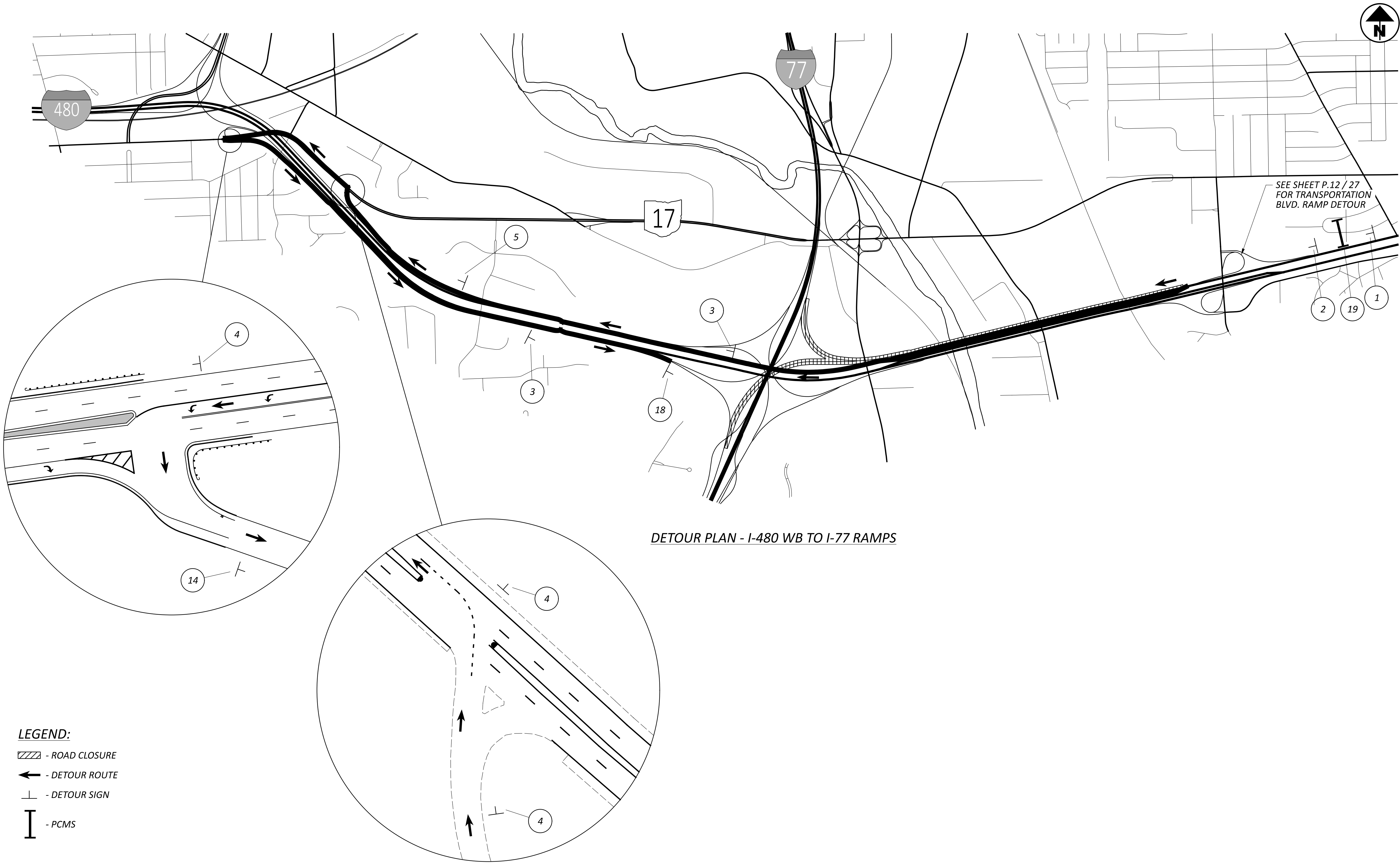
	PRIOR TO CLOSURE	DURING CLOSURE
MESSAGE 1	TRANS. BLVD. TO I-480 WEST CLOSED	TRANS. BLVD. TO I-480 WEST CLOSED
MESSAGE 2	XX/XX - XX/XX 9P-5A	USE DETOUR

21

PORTABLE CHANGEABLE MESSAGE SIGN

	PRIOR TO CLOSURE	DURING CLOSURE
MESSAGE 1	EXIT TO TRANS. BLVD. CLOSED	EXIT TO TRANS. BLVD. CLOSED
MESSAGE 2	XX/XX - XX/XX 9P-5A	USE DETOUR





DETOUR PLAN - I-480 WB TO I-77 RAMPS

LEGEND:

- ROAD CLOSURE
- DETOUR ROUTE
- DETOUR SIGN
- PCMS

NOTES:

- THIS DETOUR SHALL BE ACTIVE DURING I-480 EB PHASE 3.
- SEE SHEET P.10 / 27 FOR SIGNING LEGEND.

DETOUR PLAN - I-480 WB TO I-77 INTERCHANGE

DESIGN AGENCY



DESIGNER

TSR

REVIEWER

TLO 09/26/25

PROJECT ID

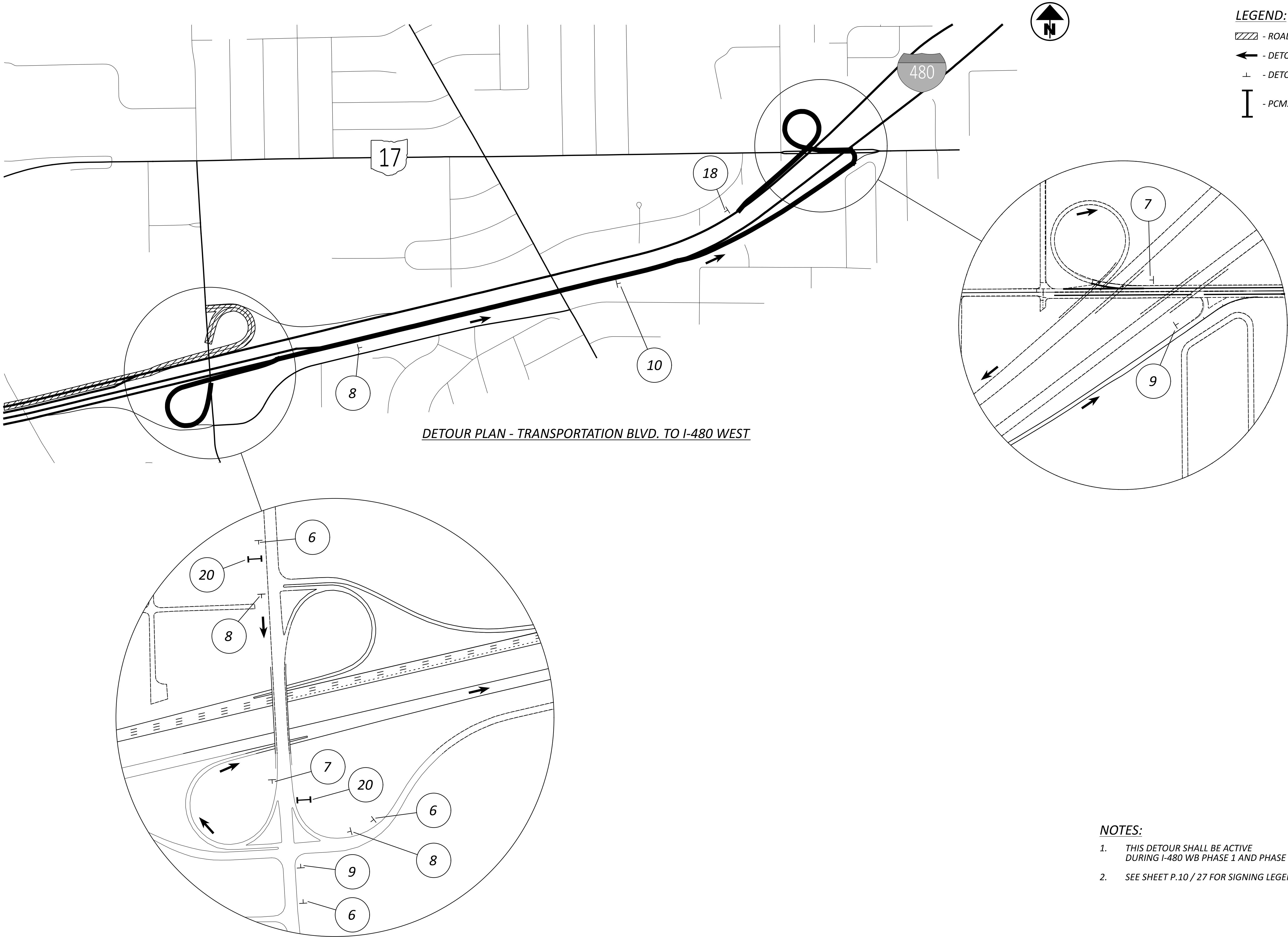
120625

SHEET

P.11

TOTAL

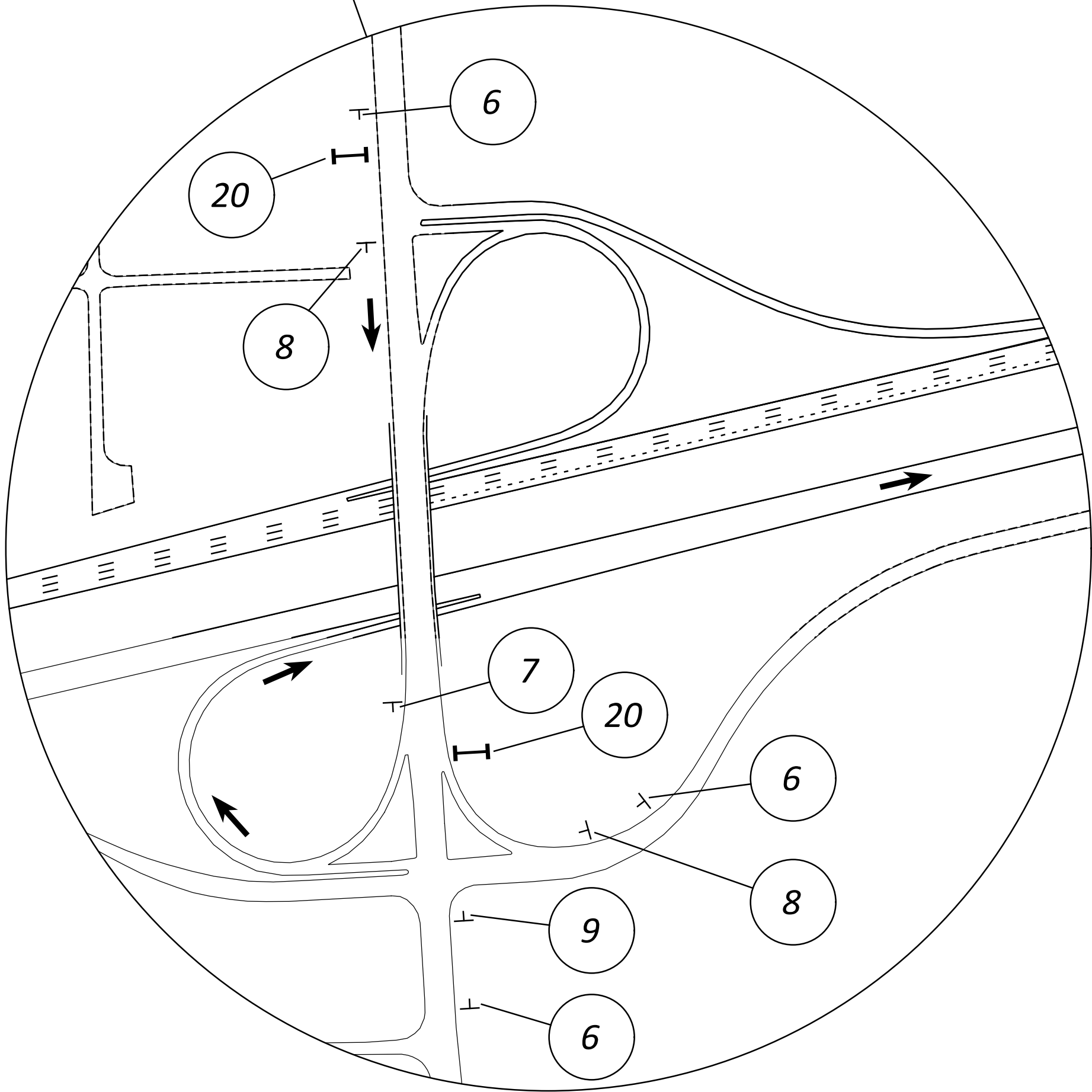
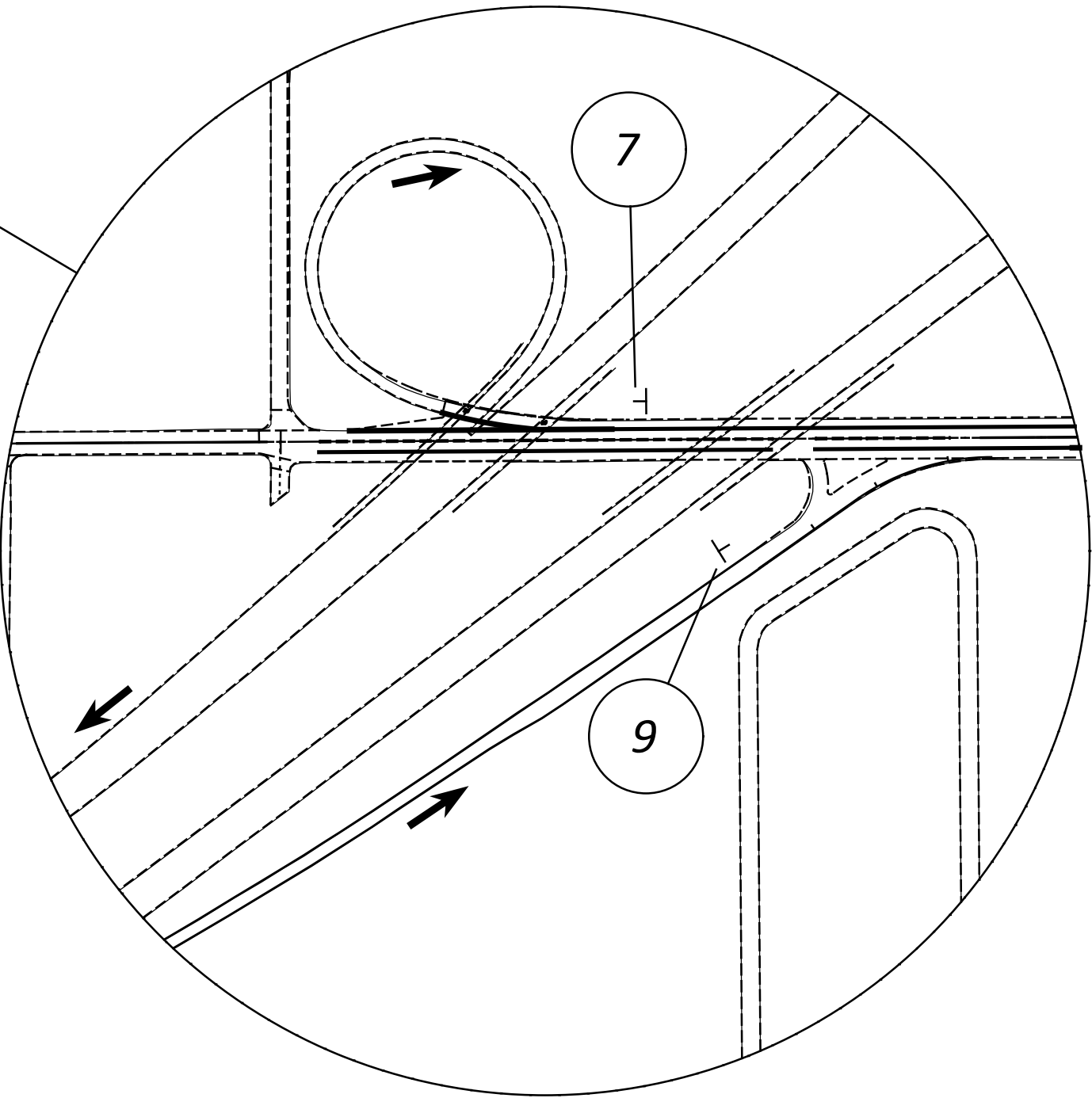
27



DETOUR PLAN - TRANSPORTATION BLVD. TO I-480 WEST

LEGEND:

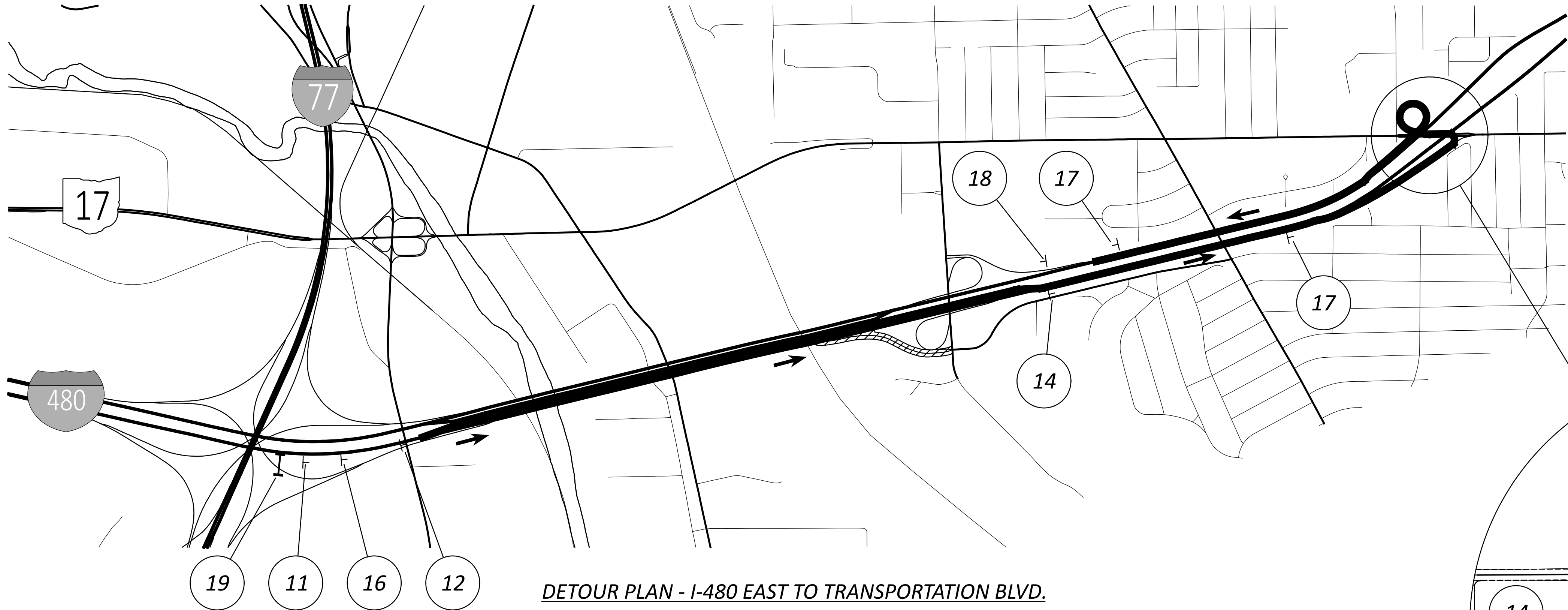
- ROAD CLOSURE
- DETOUR ROUTE
- DETOUR SIGN
- PCMS



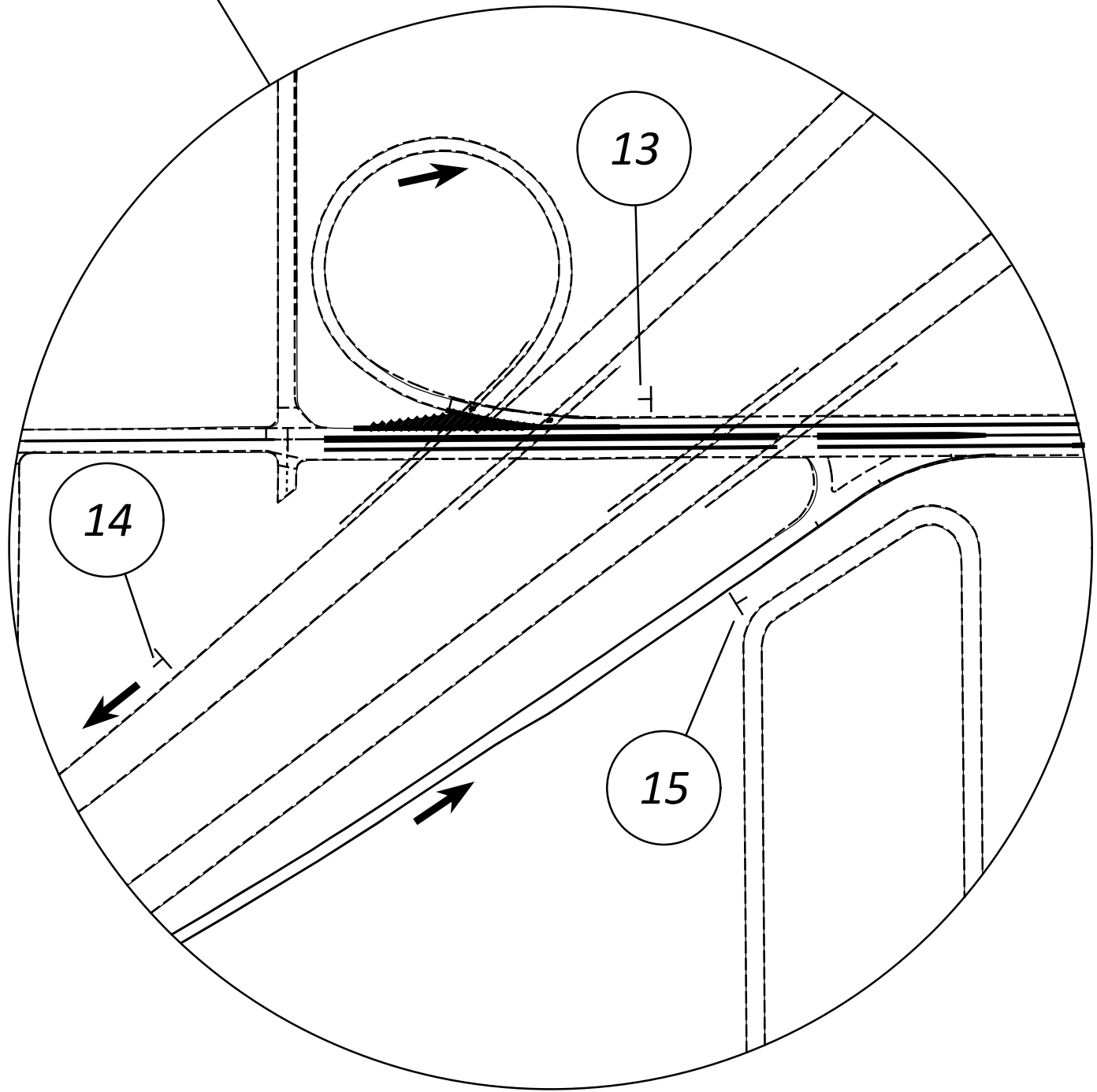
NOTES:

- THIS DETOUR SHALL BE ACTIVE DURING I-480 WB PHASE 1 AND PHASE 3.
- SEE SHEET P.10 / 27 FOR SIGNING LEGEND.





DETOUR PLAN - I-480 EAST TO TRANSPORTATION BLVD.




LEGEND:

- ROAD CLOSURE
- DETOUR ROUTE
- DETOUR SIGN
- PCMS

NOTES:

- THIS DETOUR SHALL BE ACTIVE DURING I-480 EB PHASE 1 & PHASE 2.
- SEE SHEET P.10 / 27 FOR SIGNING LEGEND.

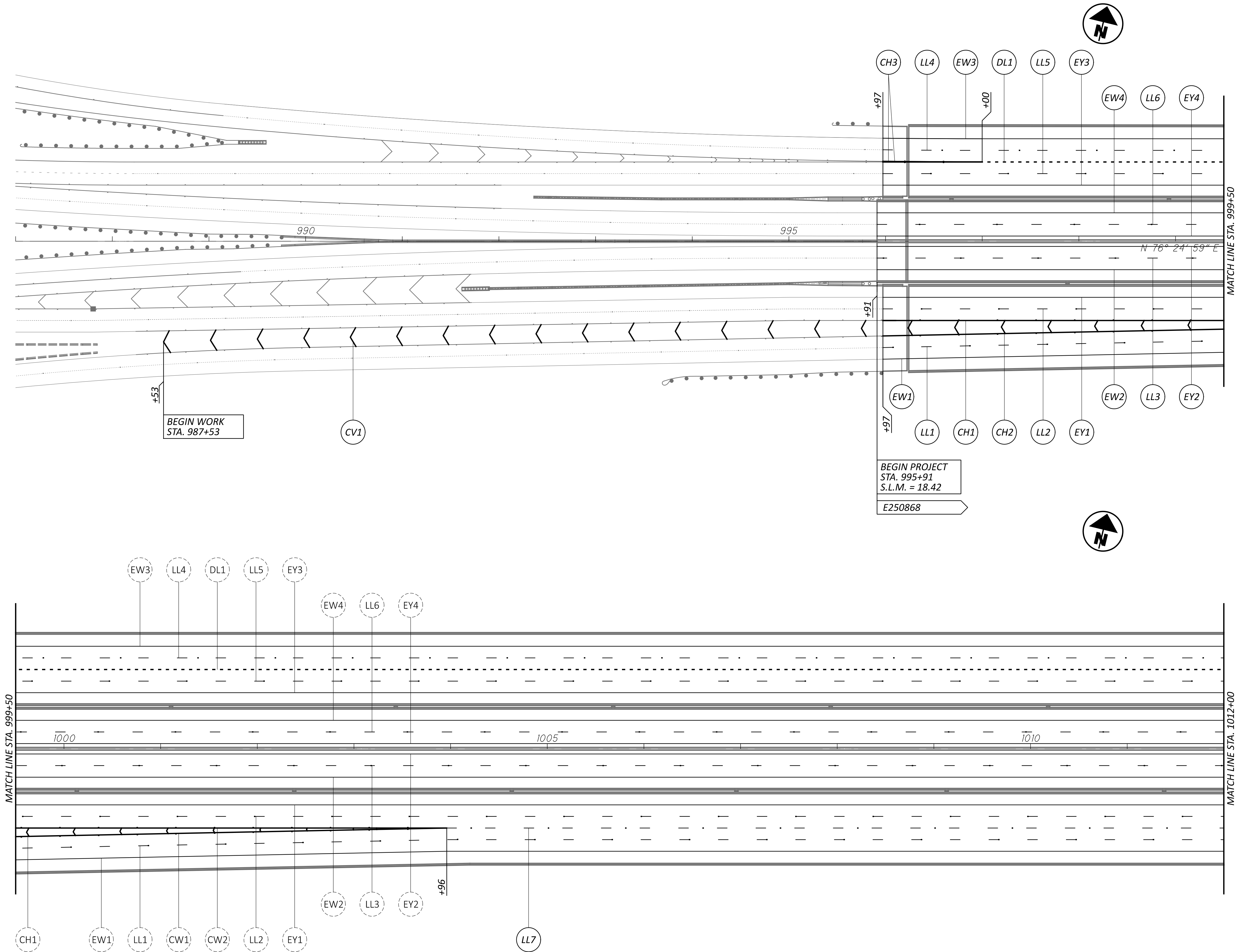


GENERAL SUMMARY	
DESIGN AGENCY	
	
DESIGNER	TSR
REVIEWER	PAT 08/01/25
PROJECT ID	120625
SHEET	TOTAL
P.14	27

CUY-480-18.42 DECK SEALING

MODEL: 120625_TP001.PAPER: 34x22 (in.) DATE: 1/8/2026 TIME: 10:17:36 AM PLTDRV: OHDOT_PDF.plt PENTBL: OHDOT_Pen.tbl USER: Thomas.Ritter@dot.ohio.gov WORKSPACE: OHDOTCEV02 WORKSET: 120625_PRODUCT: OpenRoadsDesigner 24.00.00.205

pw:\ohiodot-pw.bentley.com\ohiodot-pw-02\Documents\01 Active Projects\District 12\Cuyahoga\120625\400-Engineering\Traffic\Sheets\120625_TP001.dgn



TRAFFIC CONTROL
STA. 987+00 TO STA. 1012+00

DESIGN AGENCY



DESIGNER

TSR

REVIEWER

TLO 09/26/25

PROJECT ID

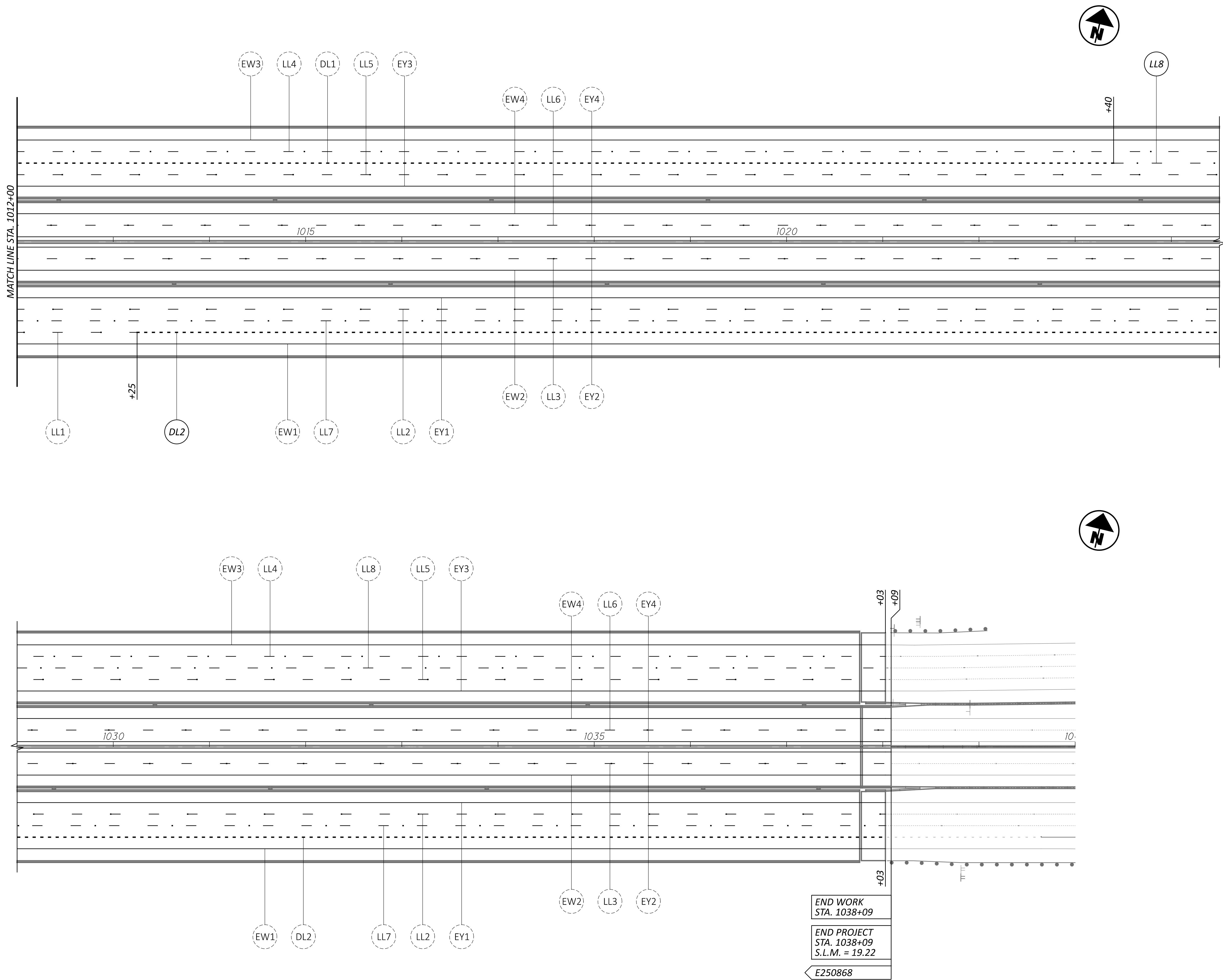
120625

SHEET TOTAL

P.15 27

CUY-480-18.42 DECK SEALING

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DESIGN AGENCY



DESIGNER

TSR

REVIEWER

TLO 09/26/25

PROJECT ID
120625

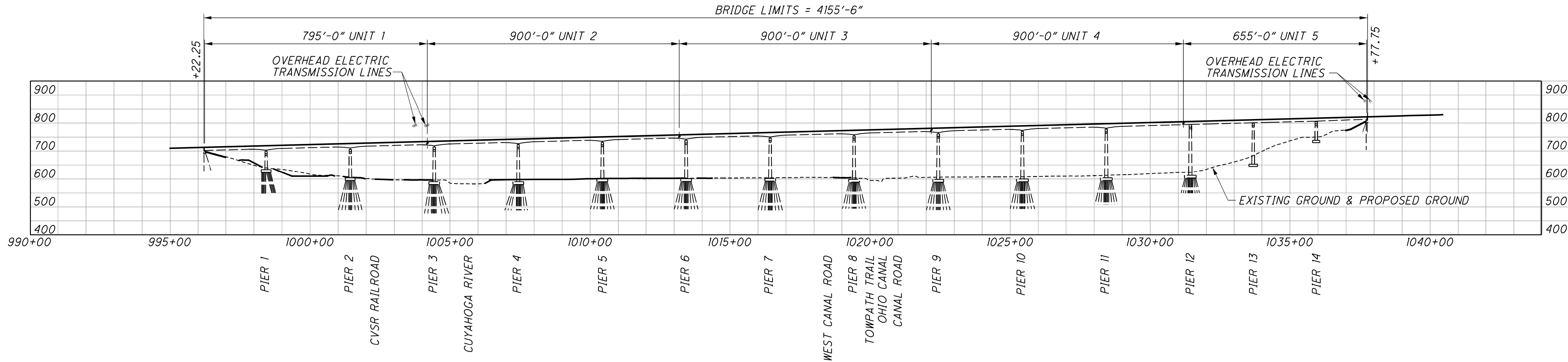
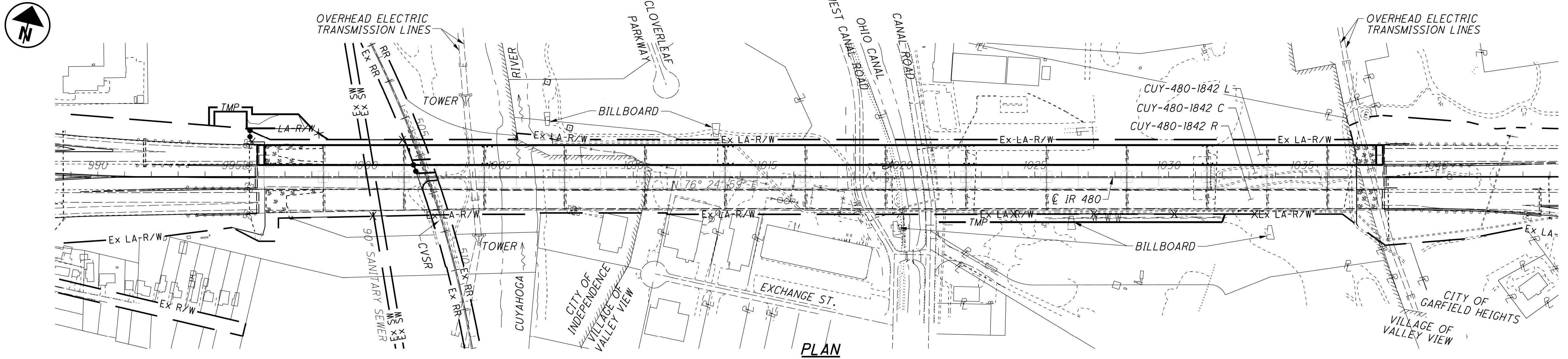
SHEET	TOTAL
P.16	2

TRAFFIC CONTROL
STA. 1012+00 TO 1040+00

HORIZONTAL
SCALE IN FEET

A horizontal scale bar with a black and white checkerboard pattern. The bar is divided into four equal segments. The first segment is black, the second is white, the third is black, and the fourth is white. The segments are labeled 0, 25, 50, and 100 from left to right.

TRAFFIC CONTROL SUBSUMMARY	
DESIGN AGENCY	
	
DESIGNER	
TSR	
REVIEWER	
TLO	09/26/25
PROJECT ID	
120625	
SHEET	TOTAL
P.17	27



PROFILE ALONG @ IR 480 WB LANES

PROPOSED STRUCTURE CUY-480-1842 L

TYPE: NEW COMPOSITE REINFORCED CONCRETE DECK ON EXISTING STEEL SUPERSTRUCTURE.

SPANS: SAME AS EXISTING

ROADWAY: UNIT 1 - VARIES 72'-0" TO 72'-4" T/T PARAPETS
UNITS 2, 3, 4 AND 5 - 72'-0" T/T PARAPETS

LOADING: HS20-44, ALTERNATE MILITARY LOADING AND 15 PSF FUTURE WEARING SURFACE

SKEW: SAME AS EXISTING

WEARING SURFACE: 1" MONOLITHIC CONCRETE

APPROACH SLABS: AS-1-15 (25'-0" LONG) & AS-2-15

ALIGNMENT: TANGENT

CROWN: 0.0156

COORDINATES: WESTBOUND BRIDGE

LATITUDE 41° 24' 29.27" N
LONGITUDE 81° 38' 13.44" W

EXISTING STRUCTURE CUY-480-1842 R/L

TYPE: CONTINUOUS WELDED STEEL GIRDERS WITH FLOOR SYSTEM AND REINFORCED CONCRETE DECK AND SUBSTRUCTURE.

SPANS: UNIT 1 - 220'±, 300'±, AND 275'±
UNIT 2, 3 AND 4 - 25'± CANTILEVER, 2 @ 300'± AND 275'±
UNIT 5 - 25'± CANTILEVER, 2 @ 225'± AND 180'±

ROADWAY: UNIT 1 L - VARIES 69'-6"± TO 69'-10"± FACE TO FACE OF PARAPETS
UNIT 1 R - VARIES 69'-6"± TO 85'-0"± FACE TO FACE OF PARAPETS
UNITS 2, 3, 4 AND 5 - 69'-6"± FACE TO FACE OF PARAPETS

LOADING: HS 20-44 AND ALTERNATE MILITARY LOADING

SKEW: NONE

WEARING SURFACE: 2½"± SUPERPLASTICIZED DENSED CONCRETE (1990)

APPROACH SLABS: AS-1-67 (25' LONG)

ALIGNMENT: TANGENT

CROWN: 0.0156±

DATE BUILT: 1975

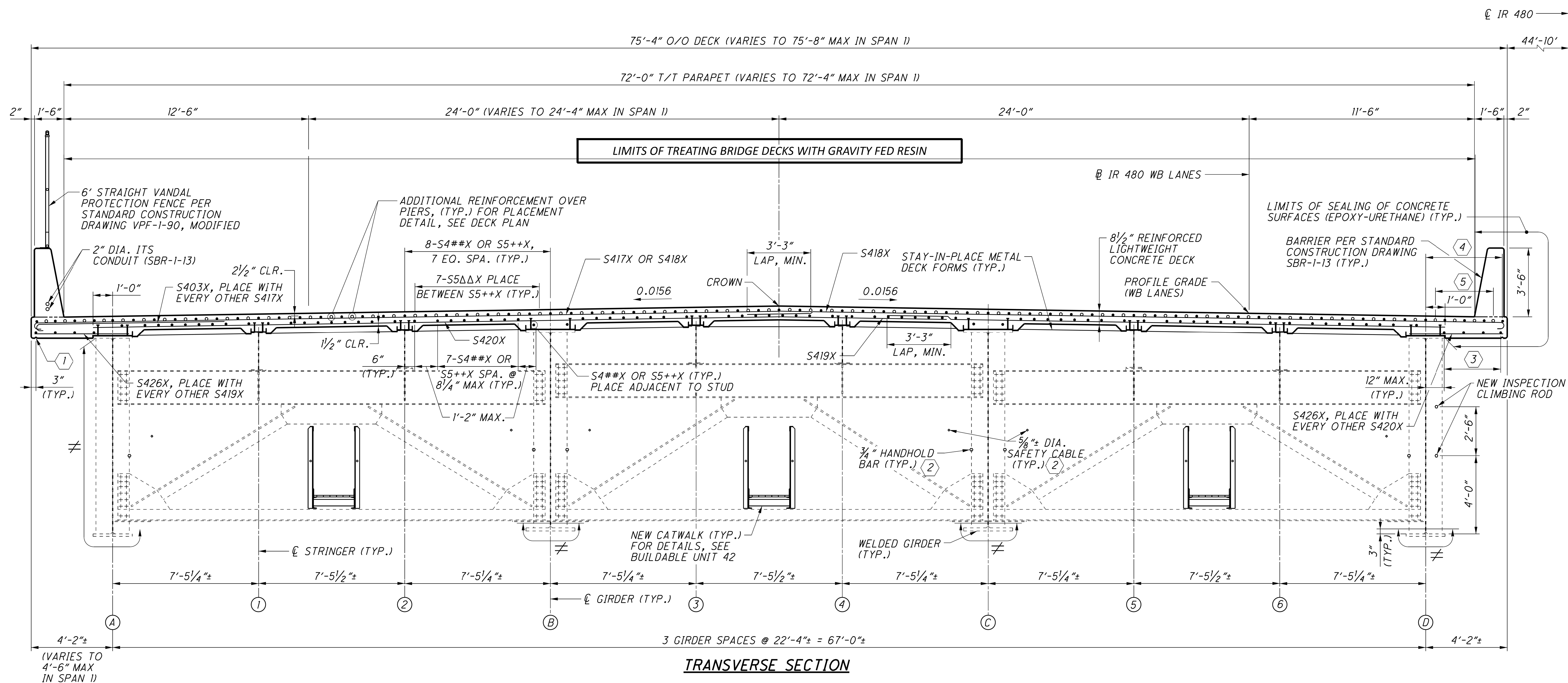
DISPOSITION: NEW DECK (LEFT BRIDGE)

UNIT	LENGTH	START WIDTH	END WIDTH	AREA	512
					TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN
REAR APPROACH START	25.0	72.3	72.3	1808	201
UNIT 1	797.8	72.3	72.0	57571	6397
UNIT 2	900.0	72.0	72.0	64800	7200
UNIT 3	900.0	72.0	72.0	64800	7200
UNIT 4	900.0	72.0	72.0	64800	7200
UNIT 5	657.8	72.0	72.0	47358	5262
FWD APPROACH END	25.0	72.0	72.0	1800	200
TOTAL CARRIED TO GENERAL SUMMARY					33,660
CALCULATED:				TSR	11/6/2024
CHECKED:				NDG	12/31/2024

NOTES

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- PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT, ESTIMATED QUANTITIES AND/OR AS DESCRIBED IN THE GENERAL NOTES.





TRANSVERSE SECTION

LEGEND

≠ INDICATES LIMITS FOR COMPLETE ABRASIVE BLAST CLEANING & PAINTING FOR THE FULL LENGTH OF THE BRIDGE.

STRINGER DESIGNATION

L GIRDER DESIGNATION

① 1" DIA. HALF ROUND DRIP GROOVE, (TYP.)

② SEE CONTRACT DOCUMENT ATTACHMENT ST-01 PLANS FOR REPAIR DETAILS

③ 4 - S4#X OR S5++X, SPA. @ 12" MAX. (TYP.)

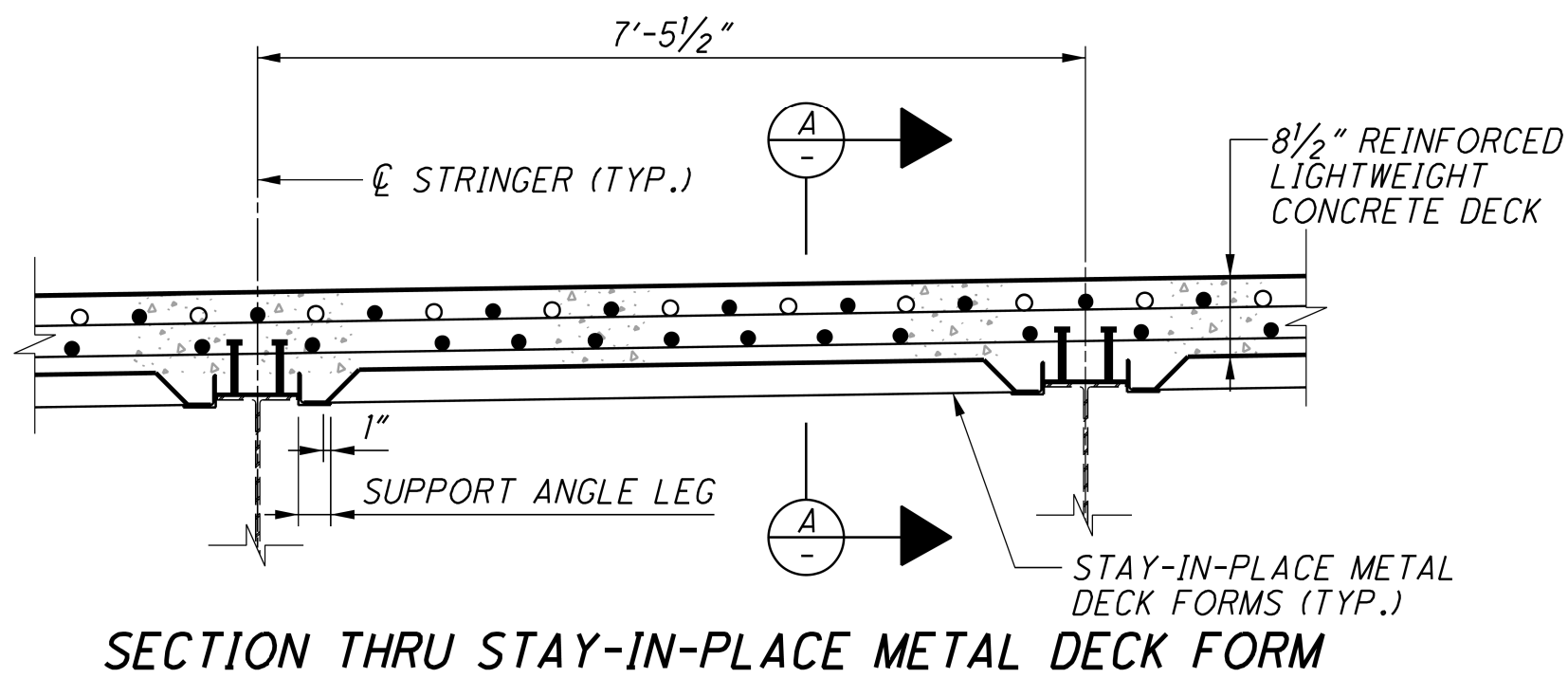
④ 5 - S4#X OR S5++X, SPA. @ 12" MAX. (TYP.)

⑤ 4 - S5ΔΔX, PLACE BETWEEN S5++X (TYP.)

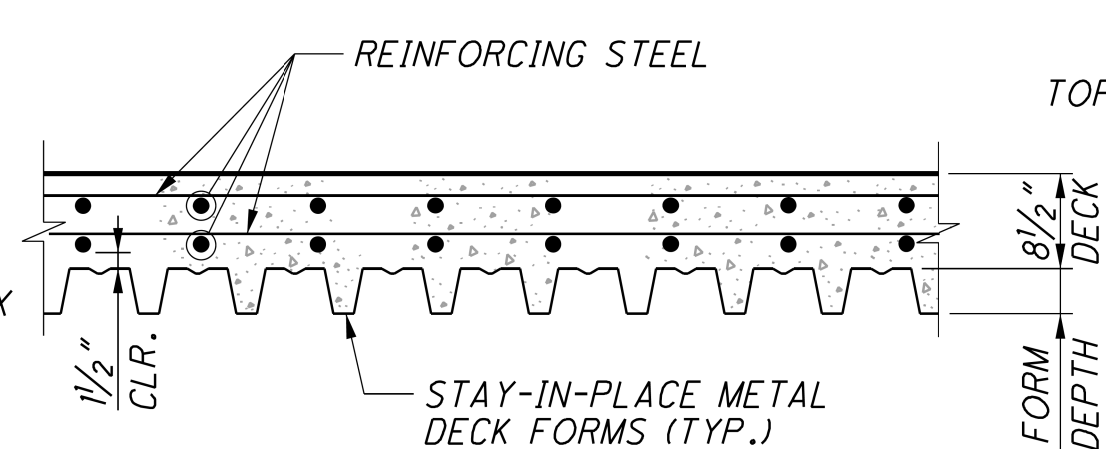
S401X, S402X, S404X, S405X, S406X, S407X, S408X, S409X, S410X, S411X, S412X, S413X, S414X, S415X, S438X, S416X

++ S501X, S502X, S503X, S504X, S505X, S506X, S507X, S508X, S509X, S510X, S511X, S512X, S526X

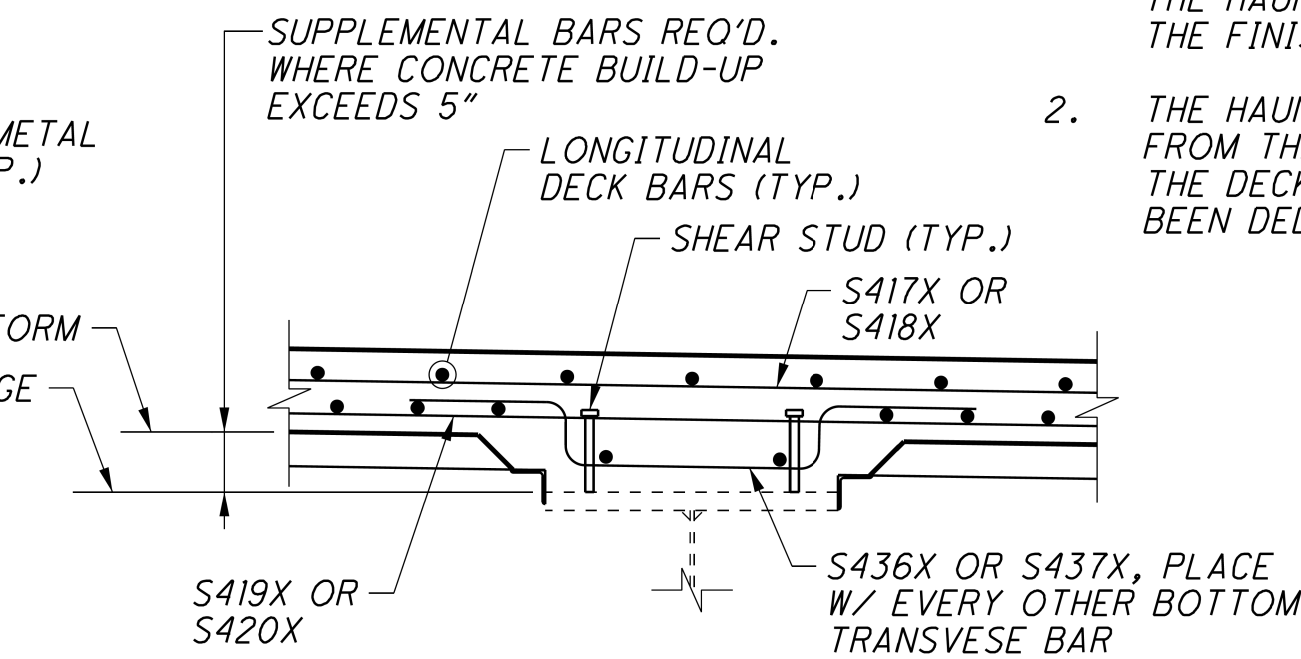
ΔΔ S501X, S513X, S514X, S515X, S516X, S517X, S518X, S519X, S520X, S521X, S522X, S524X, S525X, S527X, S528X



SECTION THRU STAY-IN-PLACE METAL DECK FORM



SECTION A-A



SUPPLEMENTAL HAUNCH REINFORCEMENT DETAIL

FOR GIRDERS B & C WHERE CONCRETE BUILD-UP >5" ONLY. ESTIMATED LOCATIONS SHOWN ON DECK PLAN SHEETS, TO BE REVISED AFTER MEASURING GIRDER REBOUND AND SETTING SCREED ELEVATIONS PER SHEET 36B3-44.

NOTES:

- DECK SLAB CONCRETE QUANTITY: THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH GIRDER OR STRINGER HAUNCH, PLUS 1 INCH OF ADDITIONAL DECK THICKNESS TO ACCOUNT FOR CONCRETE IN THE FLUTES OF THE STAY-IN-PLACE METAL DECK FORMS. THE ESTIMATE ASSUMES A CONSTANT GIRDER HAUNCH THICKNESS OF 7" AS MEASURED FROM TOP OF GIRDER WEB TO UNDERSIDE OF THE 8 1/2" DECK AND A CONSTANT STRINGER HAUNCH THICKNESS OF 4 1/2" AS MEASURED FROM TOP OF TOP FLANGE TO UNDERSIDE OF THE 8 1/2" DECK, DEVIATE FROM THE HAUNCH THICKNESSES AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.
- THE HAUNCH THICKNESS WAS MEASURED AT THE CENTERLINE OF THE GIRDER, FROM THE SURFACE OF THE DECK TO THE BOTTOM OF THE TOP FLANGE MINUS THE DECK SLAB THICKNESS. THE AREA OF ALL EMBEDDED STEEL PLATES HAS BEEN DEDUCTED FROM THE HAUNCH QUANTITY IN ACCORDANCE WITH 511.23.

NOTES

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TRANSVERSE SECTION

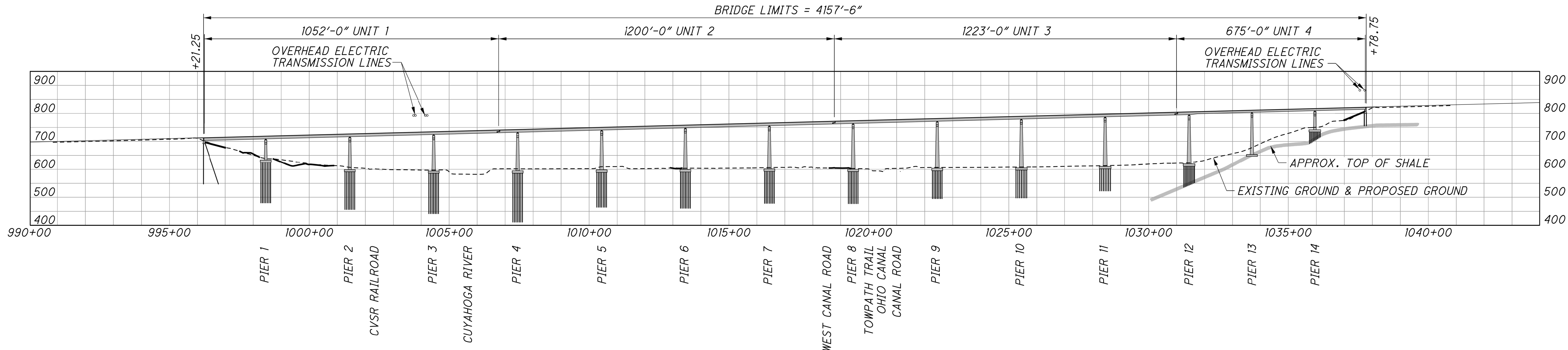
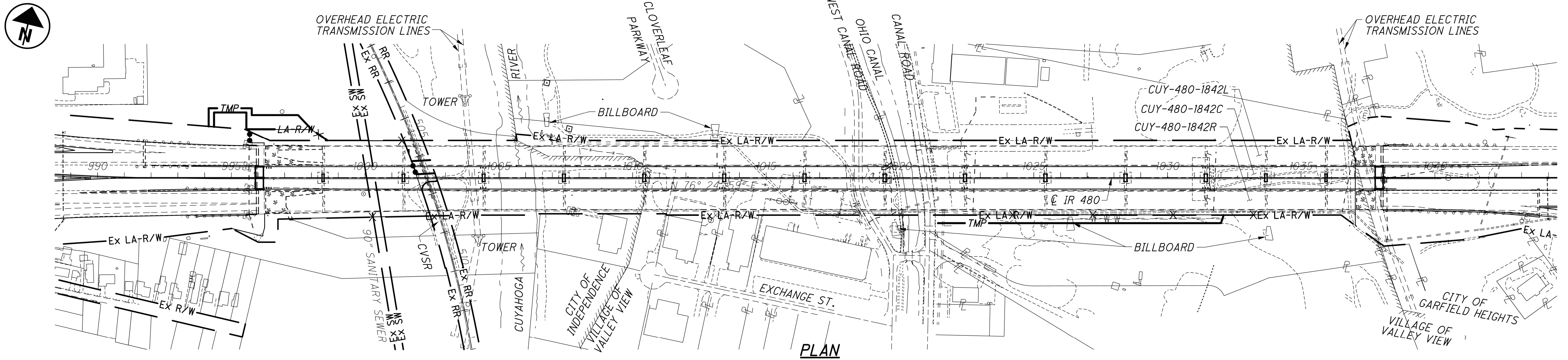
BRIDGE NO. CUY-480-18.723 L
OVER CUYAHOGA RIVER VALLEY

SFN

1812521

DESIGN AGENCY

DESIGNER
TSRCHECKER
RHCREVIEWER
PAT 08/01/25PROJECT ID
120625SUBSET TOTAL
2 10SHEET TOTAL
P.19 27



PROFILE ALONG \varnothing IR 480

PROPOSED STRUCTURE CUY-480-1842C

TYPE: CONTINUOUS STEEL GIRDERS WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURES

SPANS: UNIT 1 - 220'-0", 300'-0", 300'-0", 232'-0"
UNIT 2 - 68'-0" CANTILEVER, 300'-0", 300'-0", 300'-0", 232'-0"
UNIT 3 - 68'-0" CANTILEVER, 300'-0", 300'-0", 300'-0", 255'-0"
UNIT 4 - 45'-0" CANTILEVER, 225'-0", 225'-0", 180'-0"

C/C BEARINGS ALONG \varnothing IR 480

ROADWAY: TWO AT 39'-7 1/8" T/T PARAPETS

LOADING: HL-93 AND 60 PSF FUTURE WEARING SURFACE

SKEW: NONE

WEARING SURFACE: 1" MONOLITHIC CONCRETE

APPROACH SLABS: AS-1-15 (30'-0" LONG MINIMUM) & AS-2-15

ALIGNMENT: TANGENT

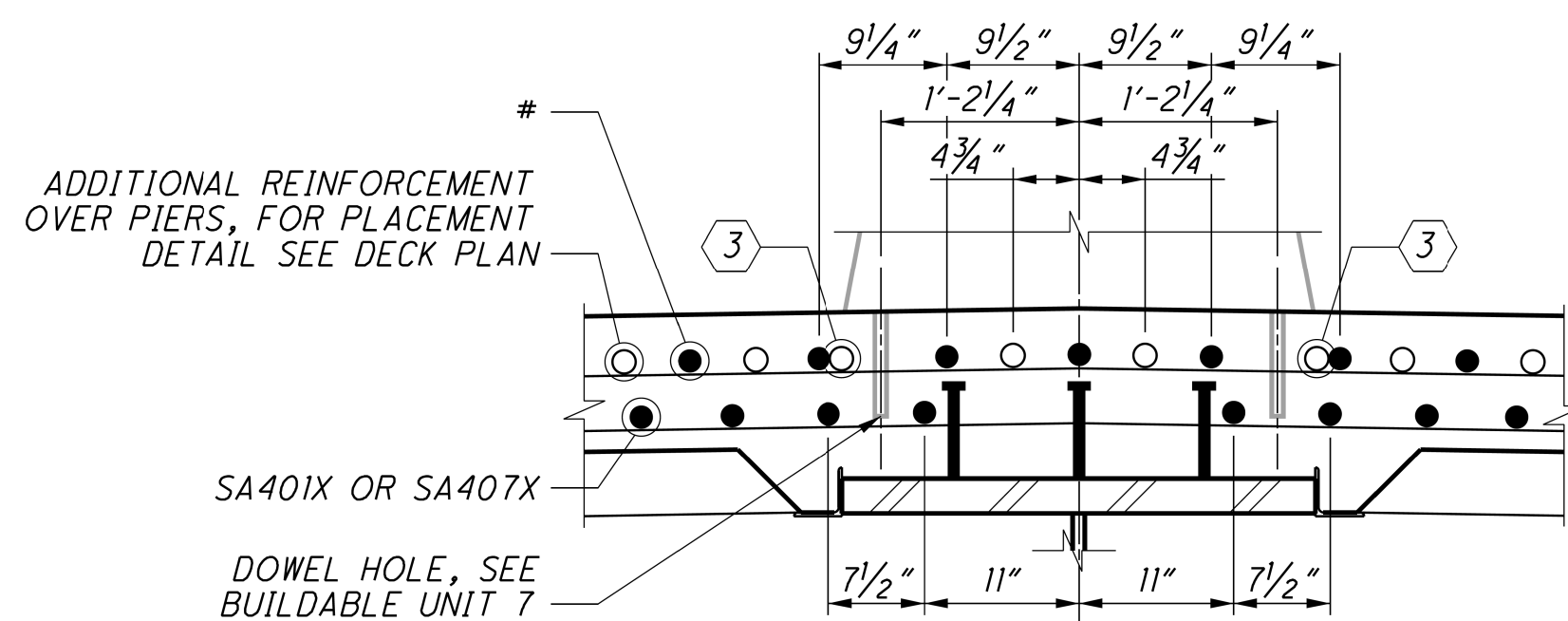
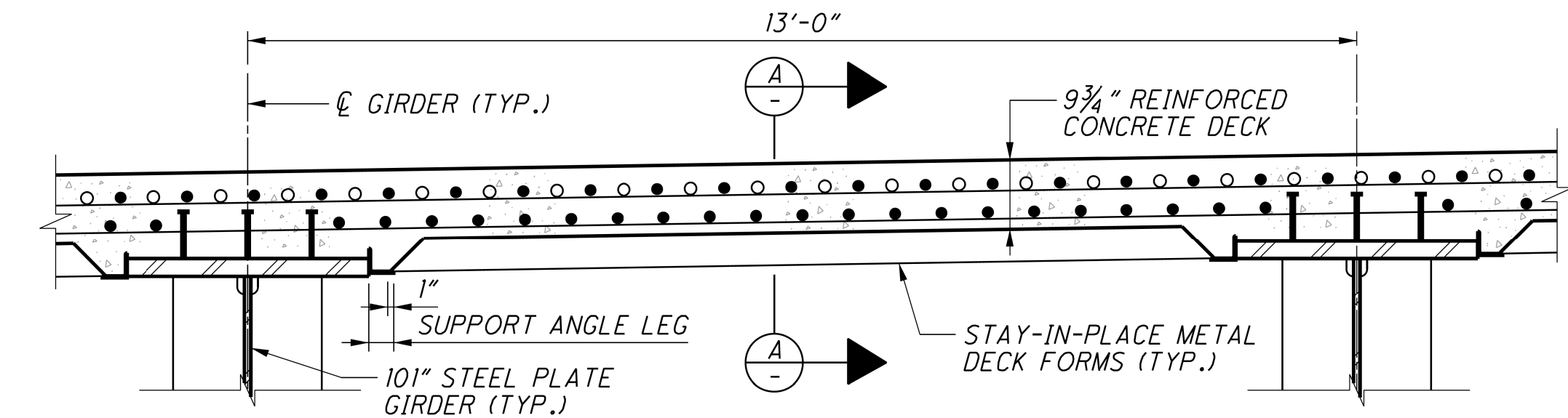
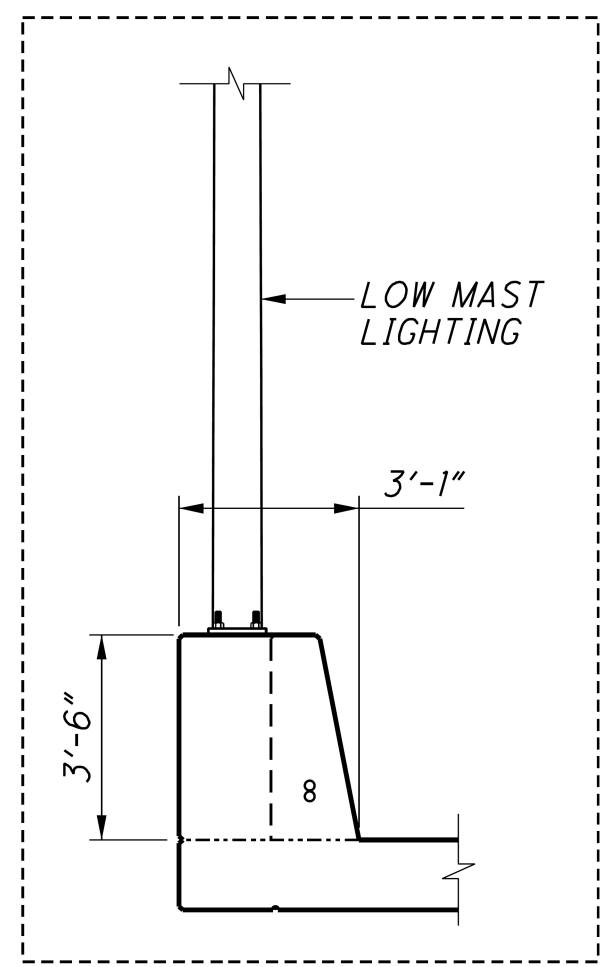
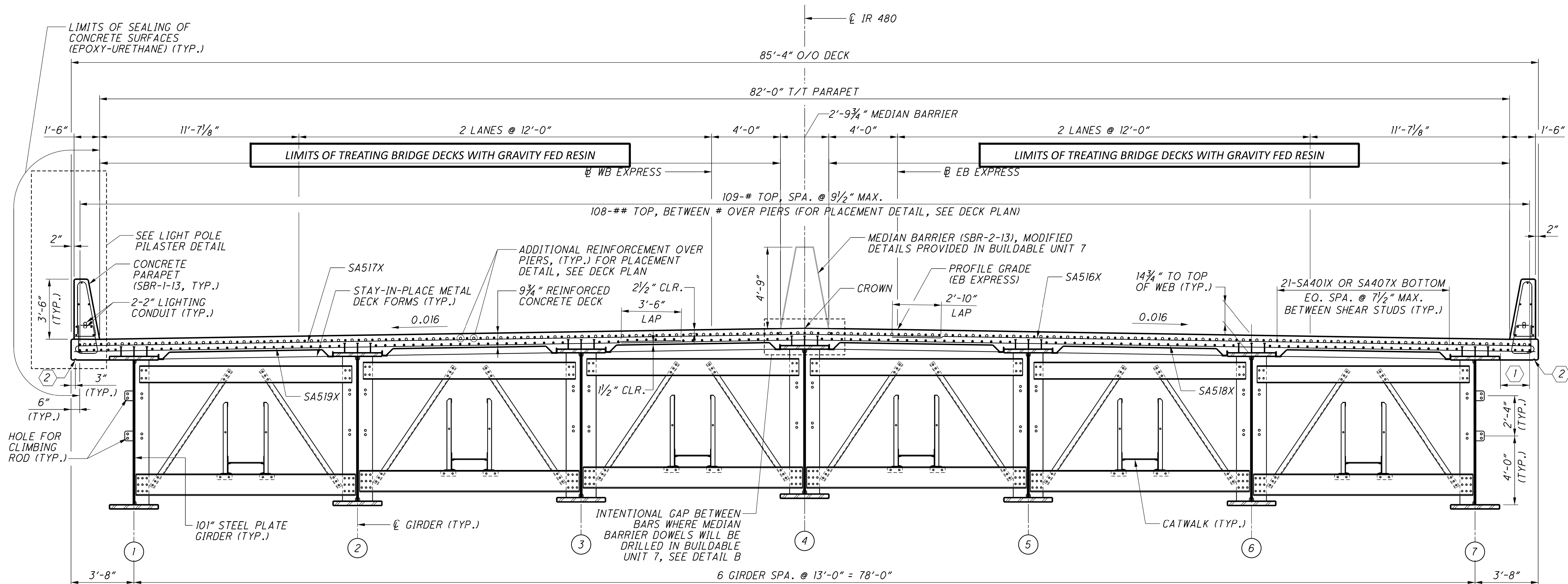
CROWN: 0.016

COORDINATES: LATITUDE 41° 24' 28.48" N
LONGITUDE 81° 38' 13.19" W

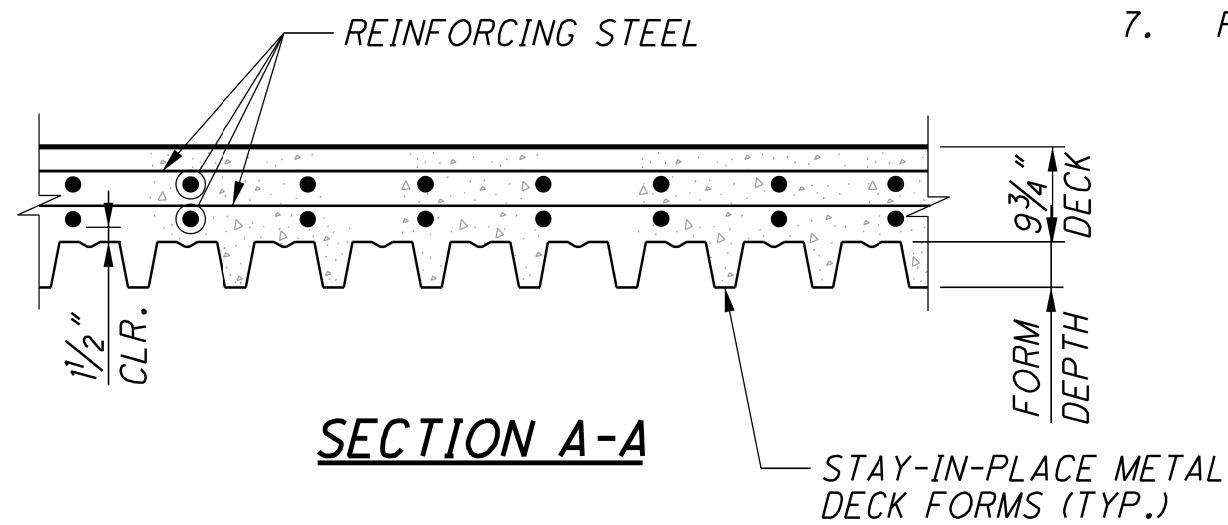
UNIT	LENGTH	START WIDTH	END WIDTH	AREA	512
					TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN
	FT	FT	FT	SF	SY
REAR APPROACH	30.0	79.2	79.2	2376	264
UNIT 1	1055.8	79.2	79.2	83602	9289
UNIT 2	1200.0	79.2	79.2	95025	10558
UNIT 3	1223.0	79.2	79.2	96846	10761
UNIT 4	678.8	79.2	79.2	53749	5972
FWD APPROACH	30.0	79.2	79.2	2376	264
TOTAL CARRIED TO GENERAL SUMMARY					37,109
CALCULATED:				TSR	11/6/2024
CHECKED:				NDG	12/31/2024

NOTES

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- PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT, ESTIMATED QUANTITIES AND/OR AS DESCRIBED IN THE GENERAL NOTES.

**LEGEND:**

- # GIRDER DESIGNATION
- 1 6-SA401X OR 6-SA407X @ 7 1/2" MAX. (TYP. EA. SIDE)
- 2 1" DIA. HALF ROUND DRIP GROOVE
- 3 ADDITIONAL REINFORCEMENT OVER PIER BAR INDICATED SHALL BE PLACED ADJACENT TO LONGITUDINAL TOP BAR AS SHOWN
- # SA404X, SA503X, SA501X, SA504X, SA401X, SA402X, SA502X, SA403X OR SA405X
- ## SA501X, SA506X, SA505X OR SA507X

**NOTES:**

- DECK SLAB CONCRETE QUANTITY: THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH GIRDER HAUNCH, PLUS 1 INCH OF ADDITIONAL DECK THICKNESS TO ACCOUNT FOR CONCRETE IN THE FLUTES OF THE STAY-IN-PLACE METAL DECK FORMS. THE ESTIMATE ASSUMES A CONSTANT HAUNCH THICKNESS OF 5 INCHES, AS MEASURED FROM TOP OF GIRDER WEB TO UNDERSIDE OF THE 9 3/4" DECK. DEVIATE FROM THIS HAUNCH THICKNESS AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.
- THE HAUNCH THICKNESS WAS MEASURED AT THE CENTERLINE OF THE GIRDER, FROM THE SURFACE OF THE DECK TO THE BOTTOM OF THE TOP FLANGE MINUS THE DECK SLAB THICKNESS. THE AREA OF ALL EMBEDDED STEEL PLATES HAS BEEN DEDUCTED FROM THE HAUNCH QUANTITY IN ACCORDANCE WITH 511.23.
- FOR PARAPET ELEVATIONS AND DETAILS, SEE SHEET 29B1-9.
- FOR DECK PLAN, SEE SHEET 29B1-4.
- FOR SCREED ELEVATIONS, TOP OF HAUNCH ELEVATIONS, AND FINAL DECK SURFACE ELEVATIONS, SEE SHEETS 29B1-7 AND 29B1-8.
- FOR MODULAR EXPANSION JOINT DETAILS, SEE SHEETS 29B1-18 AND 29B1-20.
- FOR REINFORCEMENT STEEL LIST, SEE SHEET 29B1-13.

NOTES

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TRANSVERSE SECTION
BRIDGE NO. CUY-480-18.723 C
OVER CUYAHOGA RIVER VALLEY

SFN
1812522

DESIGN AGENCY



DESIGNER
TSR

CHECKER
RHC

REVIEWER
PAT

08/01/25

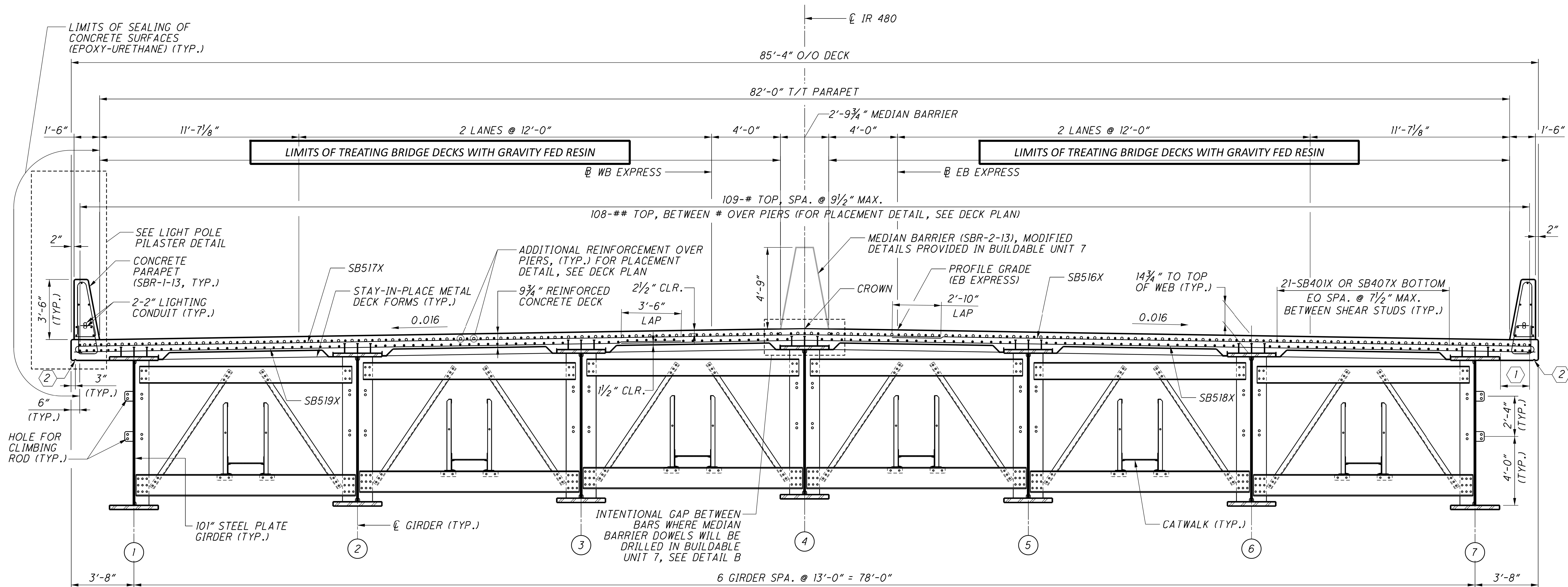
PROJECT ID
120625

SUBSET
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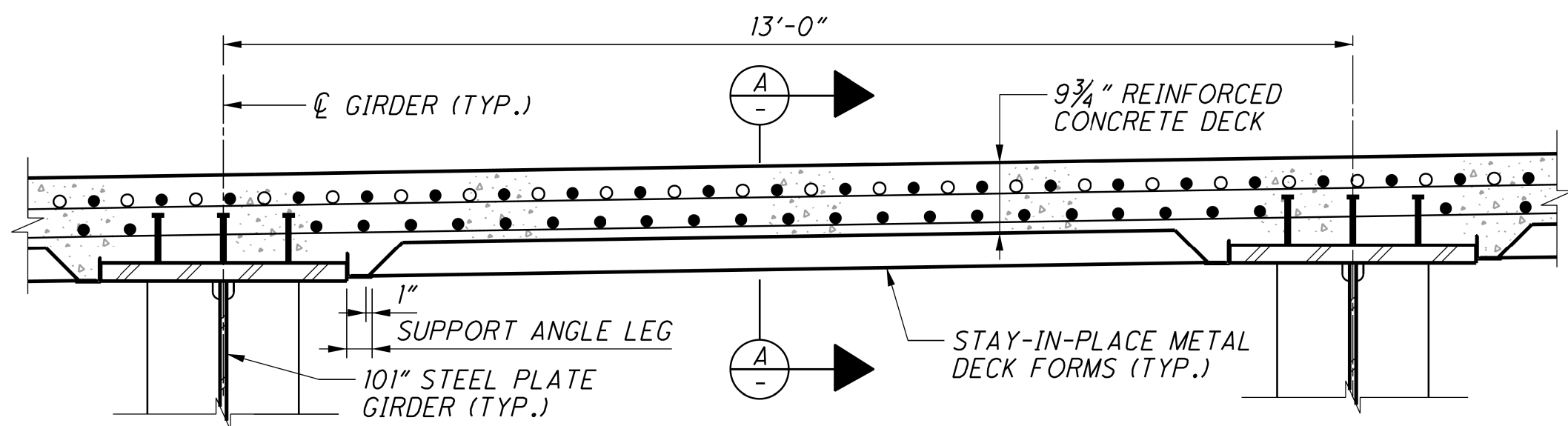
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SHEET
P.21

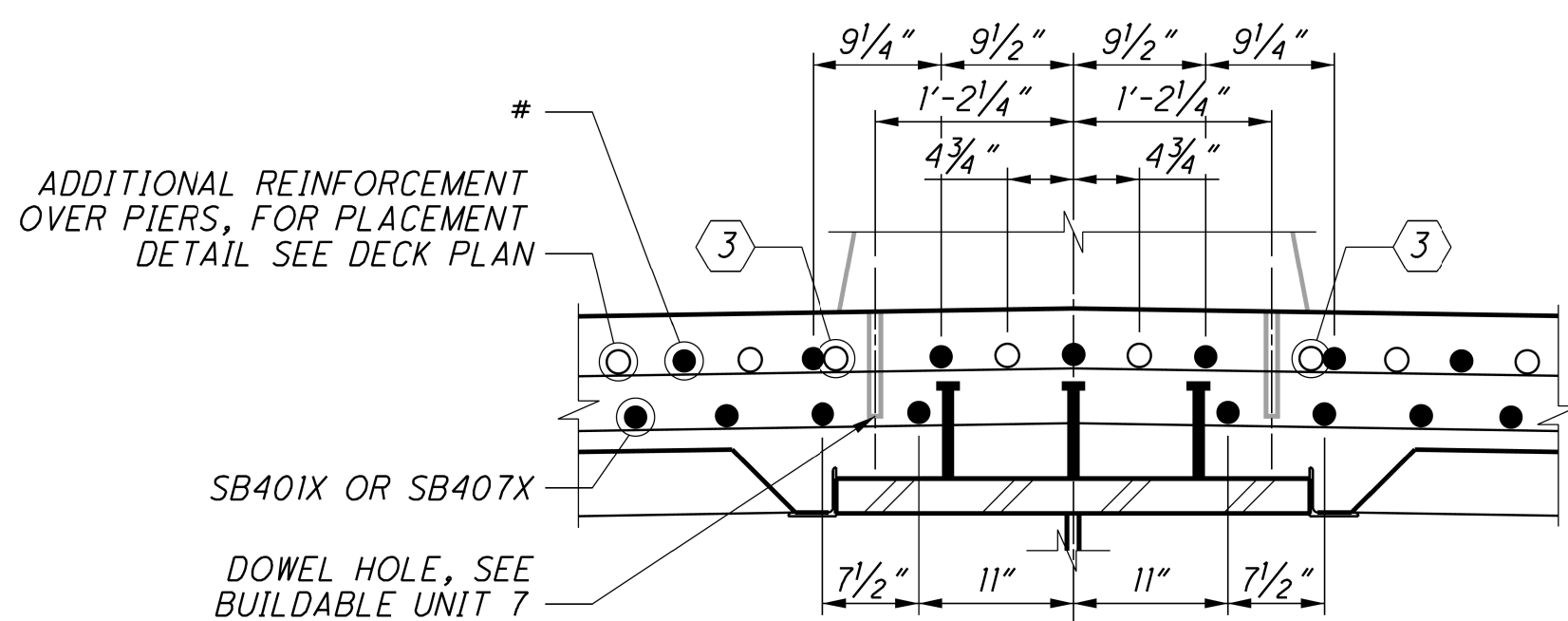
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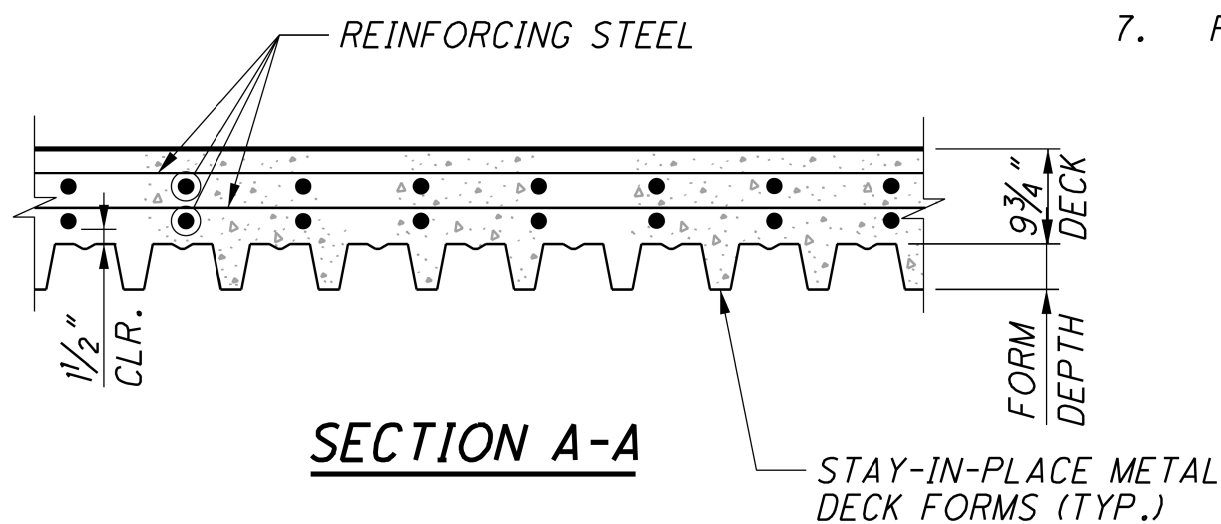
TRANSVERSE SECTION - UNIT 2



SECTION THRU STAY-IN-PLACE METAL DECK FORM



DETAIL B



SECTION A-A

LEGEND:

- # GIRDER DESIGNATION
- 1 6-SB401X OR 6-SB407X @ 7 1/2" MAX. (TYP. EA. SIDE)
- 2 1" DIA. HALF ROUND DRIP GROOVE
- 3 ADDITIONAL REINFORCEMENT OVER PIER BAR INDICATED SHALL BE PLACED ADJACENT TO LONGITUDINAL TOP BAR AS SHOWN
- # SB404X, SB503X, SB501X, SB504X, SB401X, SB408X, SB402X, SB502X, SB403X OR SB405X
- ** SB501X, SB505X, SB506X, SB507X OR SB508X

NOTES:

- DECK SLAB CONCRETE QUANTITY: THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH GIRDER HAUNCH, PLUS 1 INCH OF ADDITIONAL DECK THICKNESS TO ACCOUNT FOR CONCRETE IN THE FLUTES OF THE STAY-IN-PLACE METAL DECK FORMS. THE ESTIMATE ASSUMES A CONSTANT HAUNCH THICKNESS OF 5 INCHES, AS MEASURED FROM TOP OF GIRDER WEB TO UNDERSIDE OF THE 9 3/4" DECK. DEVIATE FROM THIS HAUNCH THICKNESS AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.
- THE HAUNCH THICKNESS WAS MEASURED AT THE CENTERLINE OF THE GIRDER, FROM THE SURFACE OF THE DECK TO THE BOTTOM OF THE TOP FLANGE MINUS THE DECK SLAB THICKNESS. THE AREA OF ALL EMBEDDED STEEL PLATES HAS BEEN DEDUCTED FROM THE HAUNCH QUANTITY IN ACCORDANCE WITH 511.23.
- FOR PARAPET ELEVATIONS AND DETAILS, SEE SHEET 23B1-9.
- FOR DECK PLAN, SEE SHEET 23B1-4.
- FOR SCREED ELEVATIONS, TOP OF HAUNCH ELEVATIONS, AND FINAL DECK SURFACE ELEVATIONS, SEE SHEETS 23B1-6 THRU 23B1-8.
- FOR MODULAR EXPANSION JOINT DETAILS, SEE SHEET 23B1-16.
- FOR REINFORCEMENT STEEL LIST, SEE SHEET 23B1-11.

NOTES

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TRANSVERSE SECTION
BRIDGE NO. CUY-480-18.723 C
OVER CUYAHOGA RIVER VALLEY

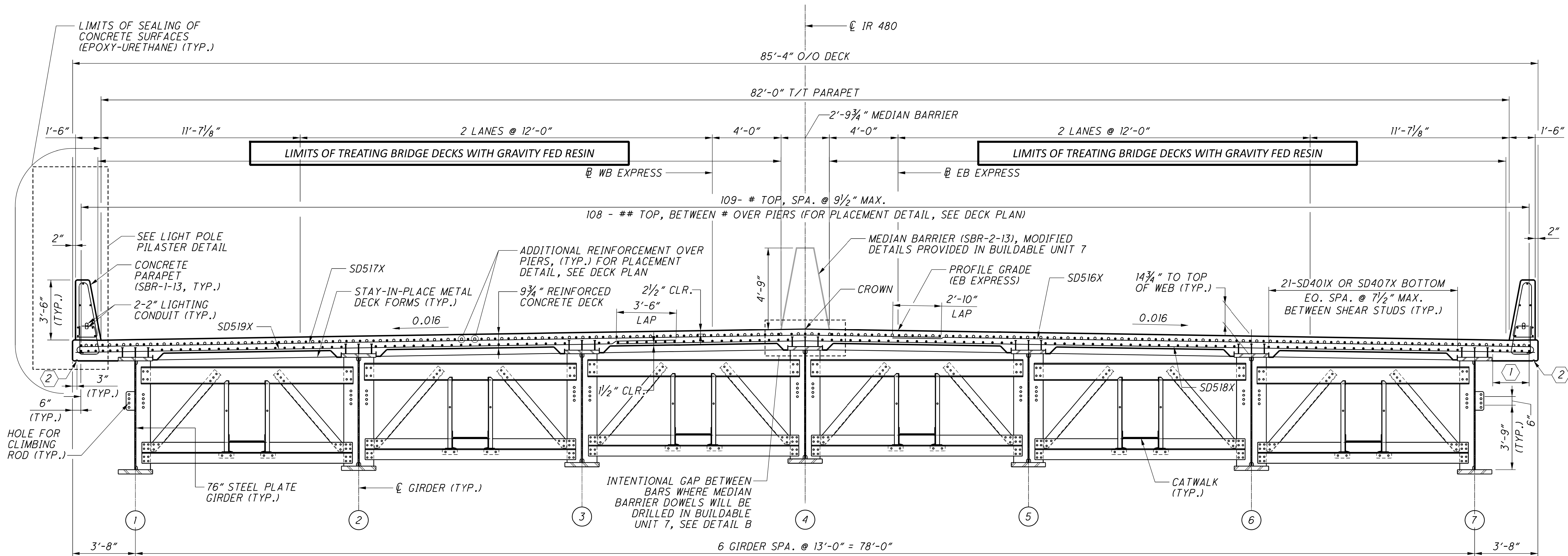
SFN
1812522
DESIGN AGENCY



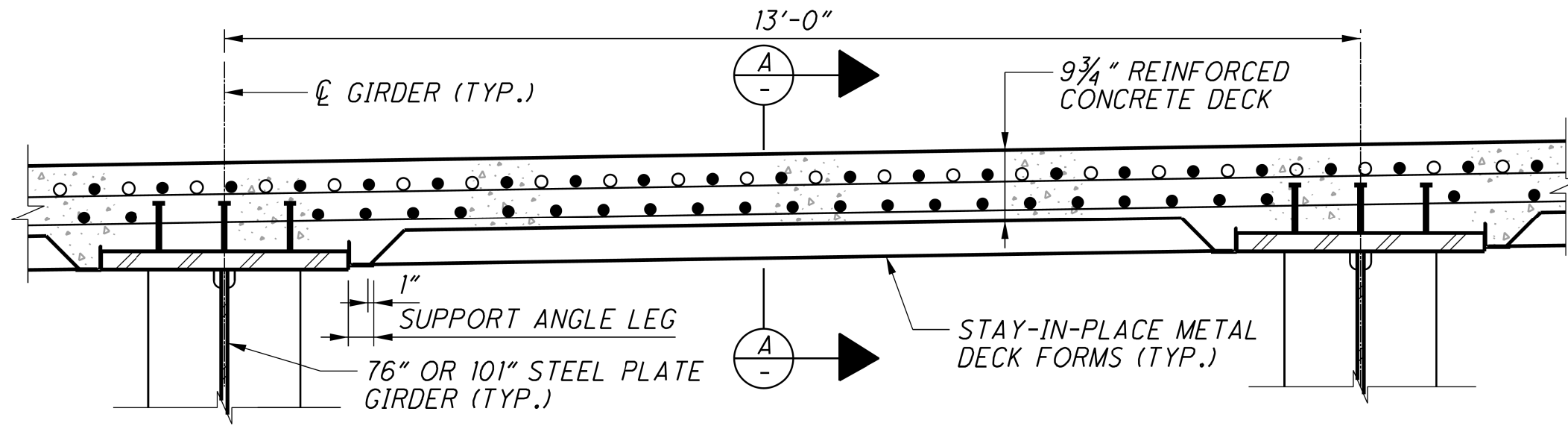
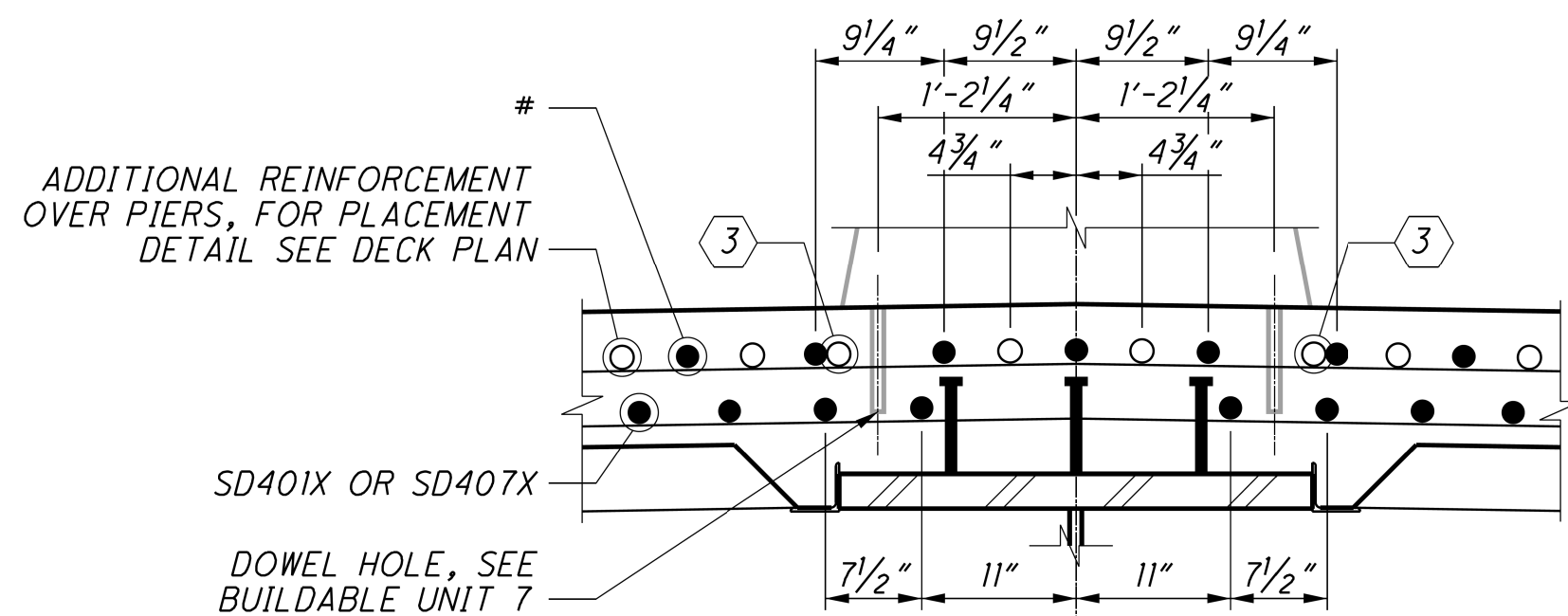
DESIGNER	CHECKER
TSR	RHC
REVIEWER	
PAT 08/01/25	
PROJECT ID	
120625	
SUBSET	TOTAL
5	10
SHEET	TOTAL
P.22	27



2. *PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT, ESTIMATED QUANTITIES AND/OR AS DESCRIBED IN THE GENERAL NOTES.*

**TRANSVERSE SECTION - UNIT 4**

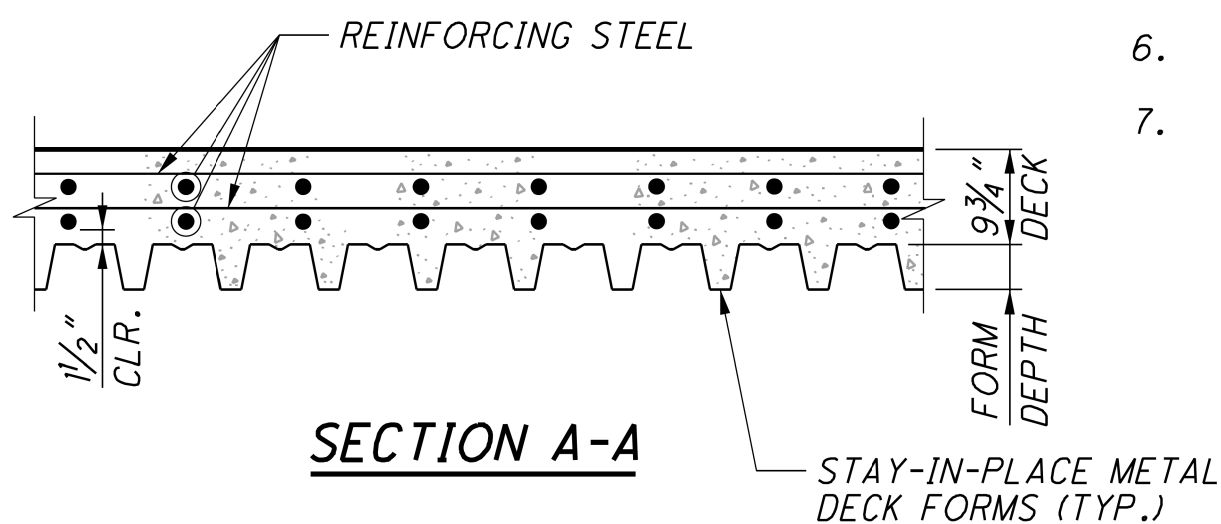
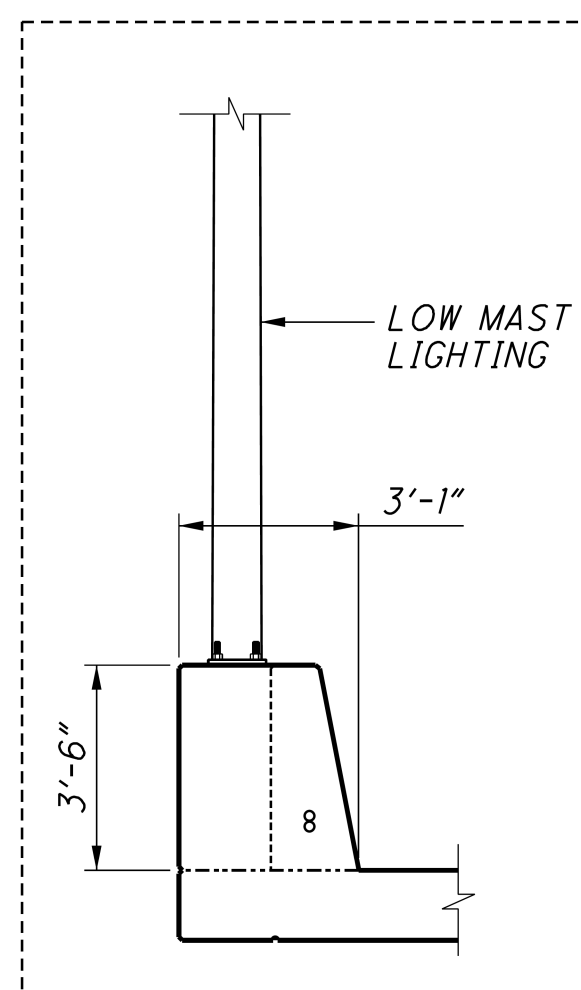
76" STEEL PLATE GIRDER SHOWN, 101" STEEL PLATE GIRDER SIMILAR
STA. 1031+00.00 TO STA. 1031+49.00 (101" STEEL PLATE GIRDER)
STA. 1031+49.00 TO STA. 1031+72.00 (STEEL PLATE GIRDER TRANSITION)
STA. 1031+72.00 TO STA. 1037+76.33 (76" STEEL PLATE GIRDER)

**SECTION THRU STAY-IN-PLACE METAL DECK FORM****DETAIL B****LEGEND:**

- # GIRDER DESIGNATION
- 1 6-SD401X OR SD407X @ 7 1/2" MAX. (TYP. EA. SIDE)
- 2 1" DIA. HALF ROUND DRIP GROOVE
- 3 ADDITIONAL REINFORCEMENT OVER PIER BAR INDICATED SHALL BE PLACED ADJACENT TO LONGITUDINAL TOP BAR AS SHOWN
- # SD409X, SD501X, SD510X, SD401X, SD410X, SD511X, SD411X, SD512X OR SD412X
- ** SD501X, SD514X, SD515X OR SD506X

NOTES:

1. DECK SLAB CONCRETE QUANTITY: THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH GIRDER HAUNCH, PLUS 1 INCH OF ADDITIONAL DECK THICKNESS TO ACCOUNT FOR CONCRETE IN THE FLUTES OF THE STAY-IN-PLACE METAL DECK FORMS. THE ESTIMATE ASSUMES A CONSTANT HAUNCH THICKNESS OF 5 INCHES, AS MEASURED FROM TOP OF GIRDER WEB TO UNDERSIDE OF THE 9 3/4" DECK. DEVIATE FROM THIS HAUNCH THICKNESS AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.
2. THE HAUNCH THICKNESS WAS MEASURED AT THE CENTERLINE OF THE GIRDER, FROM THE SURFACE OF THE DECK TO THE BOTTOM OF THE TOP FLANGE MINUS THE DECK SLAB THICKNESS. THE AREA OF ALL EMBEDDED STEEL PLATES HAS BEEN DEDUCTED FROM THE HAUNCH QUANTITY IN ACCORDANCE WITH 511.23.
3. FOR PARAPET ELEVATIONS AND DETAILS, SEE SHEET 12B1-14.
4. FOR DECK PLAN, SEE SHEET 12B1-7.
5. FOR SCREED ELEVATIONS, TOP OF HAUNCH ELEVATIONS, AND FINAL DECK SURFACE ELEVATIONS, SEE SHEETS 12B1-11 AND 12B1-12.
6. FOR MODULAR EXPANSION JOINT DETAILS, SEE SHEETS 12B1-24 THRU 12B1-27.
7. FOR REINFORCEMENT STEEL LIST, SEE SHEET 12B1-18.

**SECTION A-A****PARTIAL SECTION AT LIGHT POLE PILASTER****NOTES**

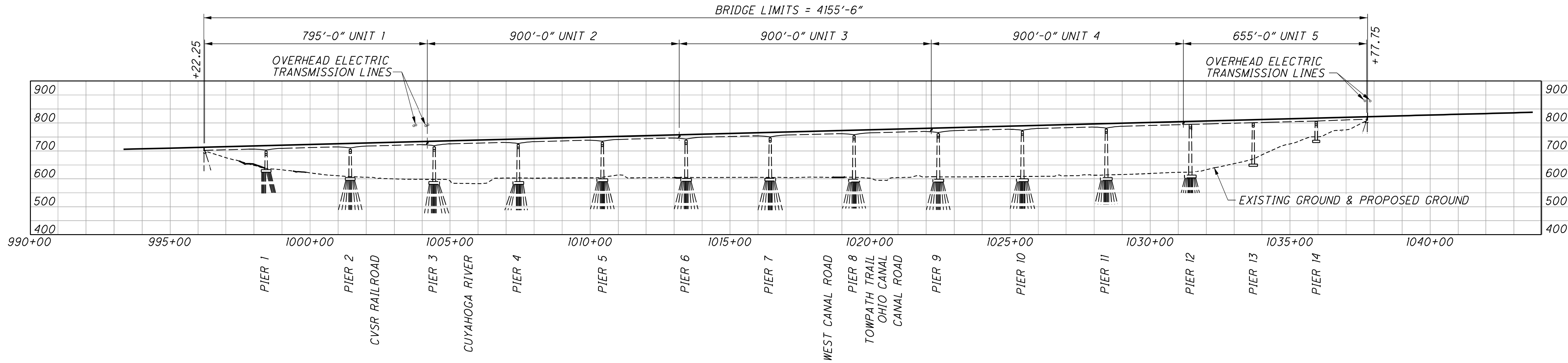
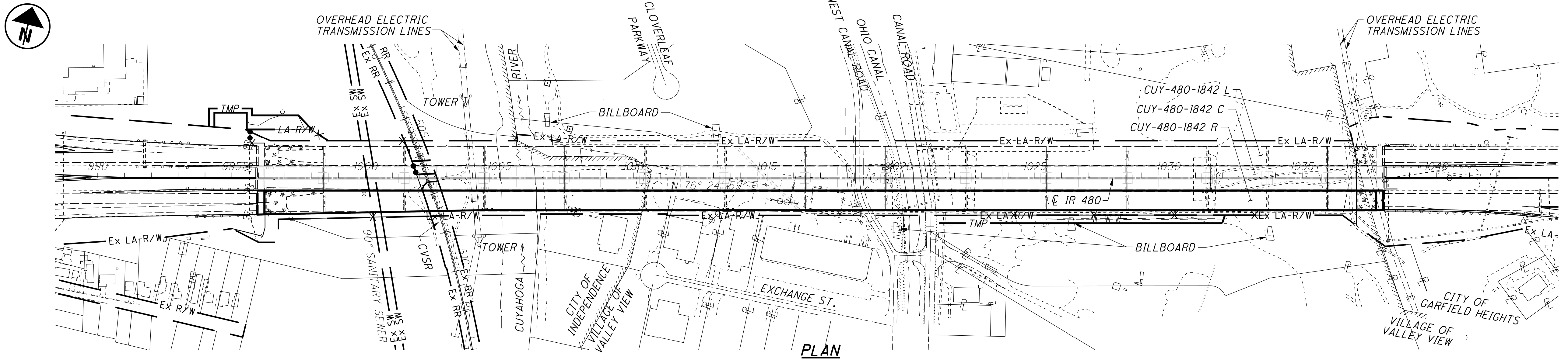
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TRANSVERSE SECTION
BRIDGE NO. CUY-480-18.723 C
OVER CUYAHOGA RIVER VALLEY

SFN
1812522
DESIGN AGENCY



DESIGNER	CHECKER
TSR	RHC
REVIEWER	
PAT 08/01/25	
PROJECT ID	
120625	
SUBSET	TOTAL
7	10
SHEET	
P.24	
TOTAL	
27	



PROFILE ALONG @ IR 480 EB LANES

PROPOSED STRUCTURE CUY-480-1842 R

TYPE: NEW COMPOSITE REINFORCED CONCRETE DECK ON EXISTING STEEL SUPERSTRUCTURE.

SPANS: SAME AS EXISTING

ROADWAY: UNIT 1 - VARIES 72'-0" TO 87'-6" T/T PARAPETS
UNITS 2, 3, 4 AND 5 - 72'-0" T/T PARAPETS

LOADING: HS20-44, ALTERNATE MILITARY LOADING AND 15 PSF FUTURE WEARING SURFACE

SKEW: SAME AS EXISTING

WEARING SURFACE: 1" MONOLITHIC CONCRETE

APPROACH SLABS: AS-1-15 (25'-0" LONG) & AS-2-15

ALIGNMENT: TANGENT

CROWN: 0.0156

COORDINATES: EASTBOUND BRIDGE
LATITUDE 41° 24' 27.69" N
LONGITUDE 81° 38' 12.95" W

EXISTING STRUCTURE CUY-480-1842 R/L

TYPE: CONTINUOUS WELDED STEEL GIRDERS WITH FLOOR SYSTEM AND REINFORCED CONCRETE DECK AND SUBSTRUCTURE.

SPANS: UNIT 1 - 220'±, 300'±, AND 275'±
UNIT 2, 3 AND 4 - 25'± CANTILEVER, 2 @ 300'± AND 275'±
UNIT 5 - 25'± CANTILEVER, 2 @ 225'± AND 180'±

ROADWAY: UNIT 1 L - VARIES 69'-6"± TO 69'-10"± FACE TO FACE OF PARAPETS
UNIT 1 R - VARIES 69'-6"± TO 85'-0"± FACE TO FACE OF PARAPETS
UNITS 2, 3, 4 AND 5 - 69'-6"± FACE TO FACE OF PARAPETS

LOADING: HS 20-44 AND ALTERNATE MILITARY LOADING

SKEW: NONE

WEARING SURFACE: 2½"± SUPERPLASTICIZED DENSED CONCRETE (1990)

APPROACH SLABS: AS-1-67 (25' LONG)

ALIGNMENT: TANGENT

CROWN: 0.0156±

DATE BUILT: 1975

DISPOSITION: NEW DECK (RIGHT BRIDGE)

UNIT	LENGTH	START WIDTH	END WIDTH	AREA	512
					TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN
	FT	FT	FT	SF	SY
REAR APPROACH START	25.0	87.5	87.5	2188	243
UNIT 1	797.8	87.5	72.0	63621	7069
UNIT 2	900.0	72.0	72.0	64800	7200
UNIT 3	900.0	72.0	72.0	64800	7200
UNIT 4	900.0	72.0	72.0	64800	7200
UNIT 5	657.8	72.0	72.0	47358	5262
FWD APPROACH END	25.0	72.0	72.0	1800	200
TOTAL CARRIED TO GENERAL SUMMARY					34,375
CALCULATED:				TSR	11/6/2024
CHECKED:				NDG	12/31/2024

NOTES

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≠ INDICATES LIMITS FOR COMPLETE ABRASIVE BLAST
CLEANING & PAINTING FOR THE FULL LENGTH
OF THE BRIDGE.

(#) STRINGER DESIGNATION

(L) GIRDER DESIGNATION

(1) 1" DIA. HALF ROUND DRIP GROOVE, (TYP.)

(2) SEE CONTRACT DOCUMENT ATTACHMENT ST-01 PLANS
FOR REPAIR DETAILS

(3) 4 - S4##X OR S5++X, SPA. @ 12" MAX. (TYP.)

(4) 5 - S4##X OR S5++X, SPA. @ 12" MAX. (TYP.)

(5) 4 - S5ΔΔX, PLACE BETWEEN S5++X (TYP.)

S401X, S402X, S404X, S415X

++ S501X, S502X, S503X

ΔΔ S501X, S513X, S514X

** S417X, S439X, S440X, S441X, S442X, S443X, S444X, S445X,
S446X, S447X, S448X, S449X, S450X, S451X, S452X, S453X

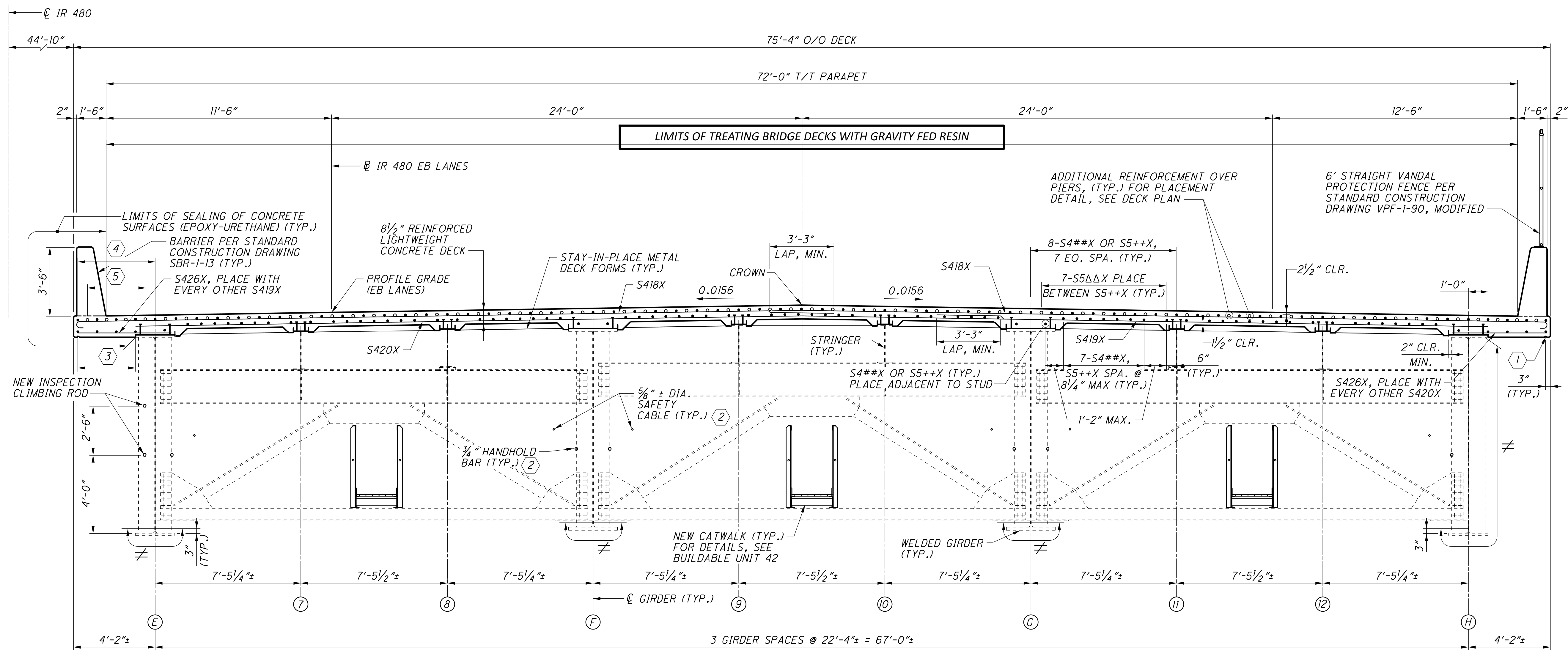
*** S454X, S455X, S456X, S457X, S458X, S459X, S460X, S461X,
S462X, S463X, S464X, S465X, S466X, S467X, S468X, S469X



FOR GIRDERS F & G WHERE CONCRETE BUILD-UP >5" ONLY. ESTIMATED LOCATIONS SHOWN ON DECK PLAN SHEETS, TO BE REVISED AFTER MEASURING GIRDER REBOUND AND SETTING SCREED ELEVATIONS PER SHEET 38B2-42.

1. DECK SLAB CONCRETE QUANTITY: THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH GIRDER OR STRINGER HAUNCH, PLUS 1 INCH OF ADDITIONAL DECK THICKNESS TO ACCOUNT FOR CONCRETE IN THE FLUTES OF THE STAY-IN-PLACE METAL DECK FORMS. THE ESTIMATE ASSUMES A CONSTANT GIRDER HAUNCH THICKNESS OF 7" AS MEASURED FROM TOP OF GIRDER WEB TO UNDERSIDE OF THE $\frac{3}{4}$ " DECK AND A CONSTANT STRINGER HAUNCH THICKNESS OF $\frac{1}{2}$ " AS MEASURED FROM TOP OF TOP FLANGE TO UNDERSIDE OF THE $\frac{3}{4}$ " DECK. DEVIATE FROM THE HAUNCH THICKNESSES AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.
2. THE HAUNCH THICKNESS WAS MEASURED AT THE CENTERLINE OF THE GIRDER, FROM THE SURFACE OF THE DECK TO THE BOTTOM OF THE TOP FLANGE MINUS THE DECK SLAB THICKNESS. THE AREA OF ALL EMBEDDED STEEL PLATES HAS BEEN DEDUCTED FROM THE HAUNCH QUANTITY IN ACCORDANCE WITH 511.23.

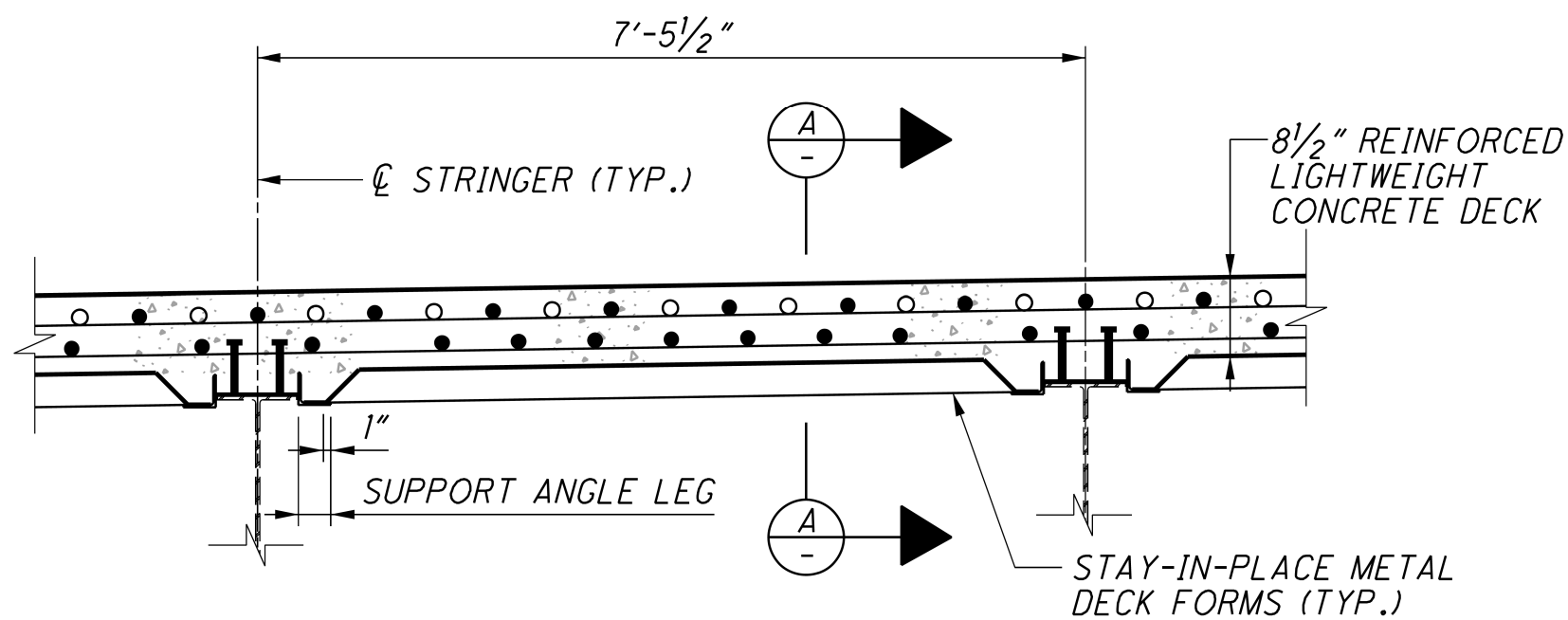
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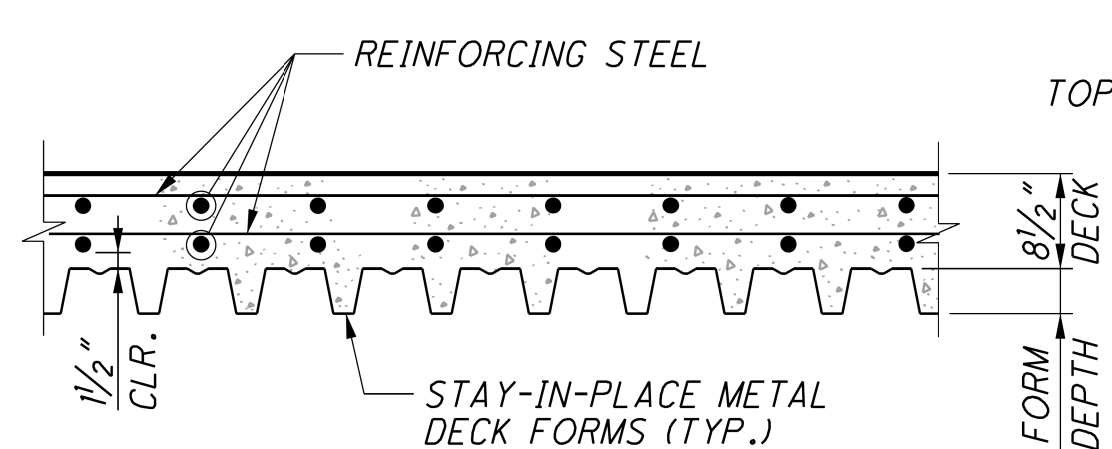
TRANSVERSE SECTION - UNIT 2, 3, 4 & 5

LEGEND

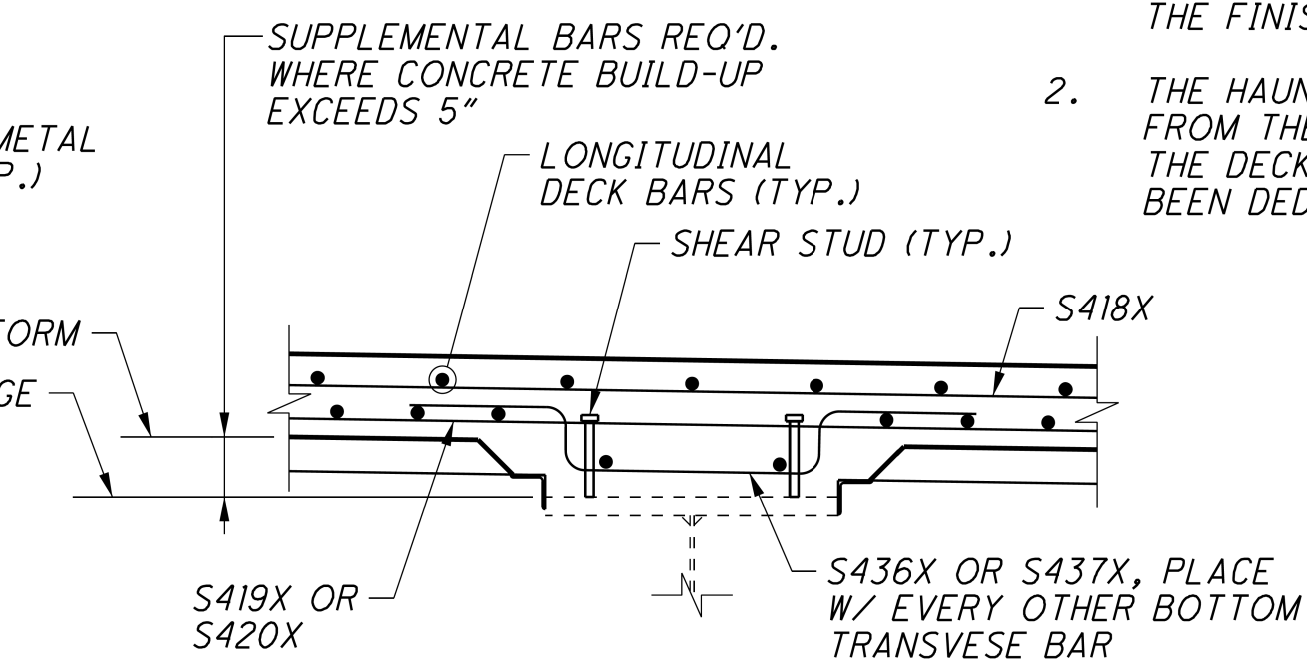
- ≠ INDICATES LIMITS FOR COMPLETE ABRASIVE BLAST CLEANING & PAINTING FOR THE FULL LENGTH OF THE BRIDGE.
- Ⓢ STRINGER DESIGNATION
- Ⓛ GIRDER DESIGNATION
- ① 1" DIA. HALF ROUND DRIP GROOVE, (TYP.)
- ② SEE CONTRACT DOCUMENT ATTACHMENT ST-01 PLANS FOR REPAIR DETAILS
- ③ 4 - S4##X OR S5++X, SPA. @ 12" MAX. (TYP.)
- ④ 5 - S4##X OR S5++X, SPA. @ 12" MAX. (TYP.)
- ⑤ 4 - S5ΔX, PLACE BETWEEN S5++X (TYP.)
- ## S401X, S405X, S406X, S407X, S408X, S409X, S410X, S411X, S412X, S413X, S414X, S416X, S438X
- ++ S501X, S504X, S505X, S506X, S507X, S508X, S509X, S510X, S511X, S512X, S526X
- ΔΔ S501X, S515X, S516X, S517X, S518X, S519X, S520X, S521X, S522X, S524X, S525X, S527X, S528X



SECTION THRU STAY-IN-PLACE METAL DECK FORM



SECTION A-A



SUPPLEMENTAL HAUNCH REINFORCEMENT DETAIL

FOR GIRDERS F & G WHERE CONCRETE BUILD-UP >5" ONLY. ESTIMATED LOCATIONS SHOWN ON DECK PLAN SHEETS, TO BE REVISED AFTER MEASURING GIRDER REBOUND AND SETTING SCREED ELEVATIONS PER SHEET 38B2-42.

NOTES:

- DECK SLAB CONCRETE QUANTITY: THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH GIRDER OR STRINGER HAUNCH, PLUS 1 INCH OF ADDITIONAL DECK THICKNESS TO ACCOUNT FOR CONCRETE IN THE FLUTES OF THE STAY-IN-PLACE METAL DECK FORMS. THE ESTIMATE ASSUMES A CONSTANT GIRDER HAUNCH THICKNESS OF 7" AS MEASURED FROM TOP OF GIRDER WEB TO UNDERSIDE OF THE 8 1/2" DECK AND A CONSTANT STRINGER HAUNCH THICKNESS OF 4 1/2" AS MEASURED FROM TOP OF TOP FLANGE TO UNDERSIDE OF THE 8 1/2" DECK, DEViate FROM THE HAUNCH THICKNESSES AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.
- THE HAUNCH THICKNESS WAS MEASURED AT THE CENTERLINE OF THE GIRDER, FROM THE SURFACE OF THE DECK TO THE BOTTOM OF THE TOP FLANGE MINUS THE DECK SLAB THICKNESS. THE AREA OF ALL EMBEDDED STEEL PLATES HAS BEEN DEDUCTED FROM THE HAUNCH QUANTITY IN ACCORDANCE WITH 511.23.

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TRANSVERSE SECTION
BRIDGE NO. CUY-480-18.723 R
OVER CUYAHOGA RIVER VALLEY

SFN
1812548

DESIGN AGENCY



DESIGNER
TSR

CHECKER
RHC

REVIEWER
PAT 08/01/25

PROJECT ID
120625

SUBSET TOTAL
10 10

SHEET TOTAL
P.27 27