



UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER, OR ADJACENT TO, THE WORK AREA.

EXISTING PLANS

EXISTING PLANS MAY BE INSPECTED IN THE ODOT DISTRICT 12 OFFICE AT 5500 TRANSPORTATION BLVD., GARFIELD HEIGHTS, OH 44125.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PROTECTION OF RIGHT-OF-WAY LANDSCAPING

PRIOR TO BEGINNING WORK, THE CONTRACTOR, THE PROJECT ENGINEER AND A REPRESENTATIVE OF THE MAINTAINING AGENCY WILL REVIEW AND RECORD ALL LANDSCAPING ITEMS WITHIN THE RIGHT-OF-WAY (BOTH WITHIN AND OUTSIDE THE CONSTRUCTION LIMITS). A RECORD OF THIS REVIEW WILL BE KEPT IN THE PROJECT ENGINEER'S FILES. PRIOR TO FINAL ACCEPTANCE, A FINAL REVIEW OF LANDSCAPING ITEMS WILL BE MADE.

CONSTRIC ALL ACTIVITIES, EQUIPMENT STORAGE AND STAGING TO WITHIN THE CONSTRUCTION LIMITS. UNLESS OTHERWISE IDENTIFIED IN THE PLANS OR PROPOSAL, THE CONSTRUCTION LIMITS ARE IDENTIFIED AS 30 FEET FROM THE EDGE OF PAVEMENT.

SUBMIT A WRITTEN REQUEST TO THE PROJECT ENGINEER TO USE ANY AREA OUTSIDE THESE LIMITS. THE DOCUMENT SUBMITTED MUST CLEARLY IDENTIFY THE AREA AND EXPLAIN THE PROPOSED USE AND RESTORATION OF THE AREA. USE OF THESE AREAS FOR OF WASTE MATERIAL AND CONSTRUCTION DEBRIS, EXCAVATION OF BORROW MATERIAL AND PLACEMENT OF PORTABLE PLANTS IS PROHIBITED. THE REQUEST MUST BE APPROVED, IN WRITING, BEFORE THE CONTRACTOR HAS PERMISSION TO USE THE AREA.

ANY ITEMS DAMAGED BEYOND THE CONSTRUCTION LIMITS, AS DEFINED ABOVE, WILL BE REPLACED IN KIND OR AS APPROVED BY THE PROJECT ENGINEER.

CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

RIGHT OF WAY

ALL WORK SHALL BE PERFORMED WITHIN THE EXISTING RIGHT OF WAY OR EASEMENTS.

COOPERATION BETWEEN CONTRACTORS

THE CONTRACTOR SHALL COOPERATE AND COORDINATE OPERATIONS WITH THE CONTRACTORS ON OTHER PROJECTS THAT MAY BE IN FORCE DURING THE LIFE OF THE CONTRACT.

EQUIPMENT AND MATERIAL STORAGE

IN ORDER TO PROVIDE FOR THE SAFETY OF THE TRAVELING PUBLIC, THE CONTRACTOR'S ATTENTION IS DIRECTED TO 614.035. IN ADDITION, NO STORAGE OF EQUIPMENT MATERIALS AND VEHICLES WITH THE HIGHWAY RIGHT-OF-WAY WILL BE PERMITTED WITHOUT PRIOP APPROVAL FROM THE ENGINEER AND OBTAINING AN ODOT R/W PERMIT FROM THE D12 ROADWAY SERVICES. ALL RESTORATION WILL BE AT NO COST TO STATE

STAGING AREAS

THERE ARE NO SPECIFIC AREAS GIVEN IN THE PLANS FOR THE CONTRACTOR TO USE AS A STAGING AREA(S). IF THE CONTRACTOR WANTS TO USE AN AREA(S) FOR STAGING, REGARDLESS IF IT FALLS WITHIN THE PROJECT LIMITS OR NOT, THE CONTRACTOR IS TO USE THE RIGHT OF WAY E-PERMITTING SYSTEM AT [HTTPS://ODHCP.BEMCORP.NET/ACCOUNTS/ACCOUNT/ACCOUNT](https://ODHCP.BEMCORP.NET/ACCOUNTS/ACCOUNT/ACCOUNT) IN ORDER TO APPLY FOR A PERMIT PER SECTION 107.02 OF THE CMS. FOR SPECIFIC PERMITTING QUESTIONS, THE CONTRACTOR CAN CONTACT THE DISTRICT PERMITTING OFFICE, (MELVIN SAFFORD) AT 216-584-2137, (ANDREW TOMKO) AT 216-584-2195 OR AT [DISTRICT12PERMITS@DOT.OHIO.GOV](mailto:DISTRICT12PERMITS@DOT.OHIO.GOV).

IF A PERMIT IS GRANTED, ALL CONDITIONS OF THE PERMIT SHALL BE MET IN ADDITION TO THE REQUIREMENTS OF 104.04 OF THE CMS, AT NO ADDITIONAL COST TO THE STATE. IF THE PROJECT ENGINEER DEEMS THAT ALL THE CONDITIONS OF THE PERMIT WERE NOT MET, THEN 10% OF THE CONTRACT BID AMOUNT FOR MOBILIZATION SHALL BE WITHHELD UNTIL ALL THE CONDITIONS OF THE PERMIT ARE SATISFIED.

RAISED PAVEMENT MARKERS (RPM)

DURING ALL DECK CLEANING AND DECK SEALING OPERATIONS, THE CONTRACTOR SHALL PROTECT ALL EXISTING RAISED PAVEMENT MARKERS (RPM) WITHIN THE PROJECT LIMITS. PROTECTION METHODS SHALL PREVENT DAMAGE OR DISLODGEMENT DUE TO HIGH-PRESSURE WATER, MECHANICAL EQUIPMENT, RESIN COATINGS, OR OTHER CONSTRUCTION ACTIVITIES. PRIOR TO DECK SEALING, ALL RPM'S SHALL BE INDIVIDUALLY MASKED USING A DURABLE, NON-ABSORBENT, AND REMOVABLE MATERIAL TO PREVENT RESIN FROM COVERING OR BONDING TO THE MARKERS. ANY RPM'S DAMAGED OR OBSCURED DURING THE COURSE OF WORK SHALL BE REPLACED IN KIND AND LOCATION AT NO ADDITIONAL COST TO THE DEPARTMENT, SUBJECT TO THE APPROVAL OF THE ENGINEER.

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURES HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURES AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURES AND THE PROPOSED WORK, BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO C&MS SECTIONS 102.05 AND 105.02.

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURES. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS WHICH HAVE BEEN VERIFIED IN THE FIELD.

ITEM 518 - STRUCTURE DRAINAGE, MISC.: SCUPPER CLEANOUT:

DESCRIPTION: THIS WORK CONSISTS OF REMOVING DIRT AND DEBRIS FROM THE EXISTING DECK SCUPPERS. ALL MATERIAL REMOVED SHALL BE DISPOSED OF AS PER C&MS 105.16 AND 105.17. ALL SCUPPERS SHALL BE CLEANED OUT TO THE SATISFACTION OF THE ENGINEER.

EXECUTION: REMOVE ACCUMULATED DIRT AND DEBRIS FROM SCUPPERS BY METHODS THAT DO NOT FLUSH THE MATERIALS INTO THE DOWNSPOUTS. REMOVE AND REINSTALL THE EXISTING BOLTED SCUPPER GRATES AS REQUIRED TO COMPLETE THE WORK. ALL SCUPPERS SHALL BE FREE OF SEDIMENT AND DEBRIS AT THE COMPLETION OF THE PROJECT. INCLUDE THE COST OF ANY FOLLOW-UP CLEANING REQUIRED TO SATISFY THIS REQUIREMENT IN THE UNIT COST BID.

MEASUREMENT AND PAYMENT: THE DEPARTMENT WILL MEASURE THIS WORK BY THE NUMBER OF EACH ACCEPTED IN PLACE. THE BID PRICE SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THE SCUPPER CLEANOUT. THE DEPARTMENT WILL PAY FOR THE ACCEPTED QUANTITIES AT THE CONTRACT UNIT PRICES BID FOR ITEM 518 - STRUCTURE DRAINAGE, MISC.: SCUPPER CLEANOUT.

THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY:

STRUCTURE REPAIR (CUY-480-18.723L): 30 EACH  
STRUCTURE REPAIR (CUY-480-18.723C): 30 EACH  
STRUCTURE REPAIR (CUY-480-18.723R): 30 EACH

ITEM 512 - SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)

THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER TO REPAIR CONCRETE SEALING ON THE INTERIOR SURFACE OF EXISTING BRIDGE RAILINGS:

STRUCTURE REPAIR (CUY-480-18.723L): 150 SY  
STRUCTURE REPAIR (CUY-480-18.723C): 150 SY  
STRUCTURE REPAIR (CUY-480-18.723R): 150 SY

PERFORM WORK IN CONJUNCTION WITH THE DECK SEALING OPERATIONS OR WITH SHOULDER CLOSURES IN ACCORDANCE WITH STANDARD DRAWING MT-95.45.

ITEM 512 - REMOVAL OF EXISTING COATINGS CONCRETE SURFACE

THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER TO REPAIR CONCRETE SEALING ON THE INTERIOR SURFACE OF EXISTING BRIDGE RAILINGS:

STRUCTURE REPAIR (CUY-480-18.723L): 150 SY  
STRUCTURE REPAIR (CUY-480-18.723C): 150 SY  
STRUCTURE REPAIR (CUY-480-18.723R): 150 SY

PERFORM WORK IN CONJUNCTION WITH THE DECK SEALING OPERATIONS OR WITH SHOULDER CLOSURES IN ACCORDANCE WITH STANDARD DRAWING MT-95.45.

ITEM 621 – RPM REFLECTOR, AS PER PLAN

THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER TO REPLACE MISSING OR DAMAGED RPM REFLECTORS ON THE EXISTING BRIDGE DECKS:

TRAFFIC CONTROL: 50 EACH

PERFORM WORK IN CONJUNCTION WITH THE DECK SEALING OPERATIONS OR WITH SHOULDER CLOSURES IN ACCORDANCE WITH STANDARD DRAWING MT-95.45.

ITEM 626 – BARRIER REFLECTOR, TYPE 1 (1WAY)

THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER TO REPLACE MISSING OR DAMAGED BARRIER REFLECTORS ON THE EXISTING BRIDGE RAILINGS:

TRAFFIC CONTROL: 100 EACH

PERFORM WORK IN CONJUNCTION WITH THE DECK SEALING OPERATIONS OR WITH SHOULDER CLOSURES IN ACCORDANCE WITH STANDARD DRAWING MT-95.45.

ITEM 646 - LANE LINE, 6", AS PER PLAN

THE MAINLINE LANE LINE PAVEMENT MARKING ACROSS BRIDGES AND APPROACH SLABS SHALL INCLUDE SHADOW MARKING USING A SPACING CONSISTING OF A 10 FOOT WHITE DASH STRIPE, 10 FOOT BLACK DASH STRIPE, AND 20 FOOT GAP SPACING.



DESIGNER

TSR

REVIEWER

PAT

08/01/25

PROJECT ID

120625

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TOTAL

P.2

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ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTIFICATION TIME TABLE			
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS AND PIO	SIGN DISPLAYED TO PUBLIC
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS AND < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE	7 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

CONTINUOUS ACCESS

THE CONTRACTOR SHALL MAINTAIN SAFE AND ADEQUATE DRIVEWAYS AND WALKWAYS IN ORDER TO PROVIDE CONTINUOUS ACCESS FOR PEDESTRIANS, PASSENGER VEHICLES, TRUCKS, AND SAFETY EQUIPMENT TO ALL ADJOINING PROPERTIES. THE COST FOR ALL MATERIALS, EQUIPMENT AND LABOR NECESSARY TO PROVIDE CONTINUOUS ACCESS SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 - MAINTAINING TRAFFIC.

CONSTRUCTION TRAFFIC

ALL CONSTRUCTION TRAFFIC SHALL USE ACCEPTABLE TRUCK ROUTES TO ACCESS THE CONSTRUCTION AREA. USE OF LOCAL RESIDENTIAL STREETS IS STRICTLY PROHIBITED UNLESS ALLOWED IN WRITING BY THE LOCAL ENFORCEMENT AUTHORITY.

LANE CLOSURE DISINCENTIVE

A LANE CLOSURE IS DEFINED AS ANY RESTRICTION OF A LANE OF TRAFFIC INCLUDING, BUT NOT LIMITED TO, SET-UP AND TEAR- DOWN OF TRAFFIC CONTROL ZONES. THE CONTRACTOR WILL BE ASSESSED A DISINCENTIVE FEE AS PER THE LANE VALUE CONTRACT TABLE FOR LANES THAT ARE CLOSED TO TRAFFIC DURING TIMES DESIGNATED AS "LANE CLOSURE NOT PERMITTED" AS STATED IN THESE PLANS AND ON THE ODOT PLCS WEBSITE.

LANE VALUE CONTRACT TABLE

DESCRIPTION OF CRITICAL LANE/RAMP TO BE MAINTAINED	RESTRICTION TIME PERIOD	TIME UNIT	DISINCENTIVE \$ PER TIME UNIT PER LANE
EB I-480	5:00 AM TO 9:00 PM	EACH MINUTE	\$245
WB I-480	5:00 AM TO 9:00 PM	EACH MINUTE	\$245
RAMP I-480 W TO I-77 N/S	5:00 AM TO 9:00 PM	EACH MINUTE	\$230
RAMP I-77 N/S TO I-480 E	5:00 AM TO 9:00 PM	EACH MINUTE	\$205

THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE EQUAL TO THE LARGEST DISINCENTIVE WITHIN ALL SECTIONS IMPACTED BY THE PHYSICAL LANE RESTRICTION, INCLUDING THE TRANSITION AREA, ACTIVITY AREA, AND TERMINATION AREA AS DEFINED BY THE OMUTCD. HOLIDAY DISINCENTIVES SHALL BE APPLIED PER SECTION PER LANE PER TIME UNIT.

HOLIDAY CLOSURES

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

NEW YEARS	GENERAL/REGULAR ELECTION DAY (NOV)
MEMORIAL DAY	THANKSGIVING
FOURTH OF JULY (OBSERVED)	CHRISTMAS
LABOR DAY	(OTHER HOLIDAY OR SPECIAL EVENT)

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

HOLIDAY CLOSURES

DAY OF HOLIDAY OR SPECIAL EVENT	TIMES ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00AM WEDNESDAY
TUESDAY (GEN. / REG. ELECTION)	5:00AM TUESDAY THROUGH 12:00AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00AM THURSDAY
THURSDAY (THANKSGIVING DAY)	6:00AM WEDNESDAY THROUGH 6:00AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

SUSPENSION OF WORK

IF THE CONTRACTOR FAILS TO COMPLY WITH THE PROVISIONS FOR TRAFFIC CONTROL AS SET FORTH IN THESE PLANS OR WITH PROVISIONS OF THE OMUTCD, THE ENGINEER SHALL SUSPEND WORK UNTIL THE CONTRACTOR COMPLIES WITH THE NECESSARY REQUIREMENTS.

MAINTENANCE OF TRAFFIC CONTROL ZONES

THE CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN THE SIGNS, DRUMS OR CONES SPECIFIED IN THE STANDARD CONSTRUCTION DRAWINGS. WHEN THE CONTRACTOR IS NOTIFIED OF DEFICIENCIES, HE SHALL CORRECT THE DEFICIENCIES AS SOON AS POSSIBLE, PREFERABLY WITHIN 12 HOURS AND NO LATER THAN 24 HOURS. IF ANY NOTED DEFICIENCIES ARE NOT CORRECTED WITHIN 24 HOURS THE ENGINEER SHALL DEDUCT ONE DAY PAY FOR ITEM 614 - MAINTAINING TRAFFIC, NOT AS A PENALTY BUT AS ROAD USER COSTS. THE CONTRACTOR SHALL BE SUBJECT TO THESE ROAD USER COSTS FOR EACH AND EVERY DAY THAT THESE PROVISIONS ARE NOT MET. ALL COSTS FOR MAINTAINING THE WORK ZONES AS DESCRIBED ABOVE SHALL BE INCLUDED UNDER ITEM 614 - MAINTAINING TRAFFIC.

CONSTRUCTION EQUIPMENT MAINTENANCE CROSSING

CONSTRUCTION EQUIPMENT IS PERMITTED TO ONLY CROSS THE MEDIAN AT THE EXISTING INTERCHANGES, INTERSECTIONS, AND U-TURN CROSSOVERS.

COVERING OF GROUND-MOUNTED SIGNS – GENERAL

WHEN REQUIRED BY OTHER ITEMS OR INCIDENTALLY TO ITEM 614 - MAINTAINING TRAFFIC, COVER EXISTING GROUND-MOUNTED SIGNS WITH PLYWOOD OR OSB BLANKS (1/2" MINIMUM THICKNESS) COVERING 80% OF THE SIGN AREA AND ALL OF THE SIGN LEGEND. THE USE OF LOW QUALITY MATERIALS SUCH AS DUCT TAPE AND BLACK PLASTIC IS NOT PERMITTED.

CONTRACTOR'S EQUIPMENT – OPERATION AND STORAGE

VEHICLES AND EQUIPMENT SHALL ALWAYS MOVE WITH, AND NOT ACROSS OR AGAINST THE FLOW OF TRAFFIC. VEHICLES AND OTHER EQUIPMENT SHALL NOT PARK OR STOP EXCEPT WITHIN DESIGNATED WORK AREAS; AND SHALL NOT ENTER AND LEAVE WORK AREAS IN A MANNER WHICH WILL BE HAZARDOUS TO OR INTERFERE WITH THE NORMAL TRAFFIC FLOW. PERSONAL VEHICLES WILL NOT BE PERMITTED TO PARK WITHIN THE RIGHT OF WAY EXCEPT IN SPECIFIC AREAS DESIGNATED BY THE ENGINEER.

EQUIPMENT, VEHICLES AND MATERIALS SHALL NOT BE STORED OR PARKED WITHIN 30 FEET OF THE TRAVELED WAY UNLESS 6 FEET BEHIND PCB OR GUARDRAIL.

ALL WORK VEHICLES AND EQUIPMENT THAT ENTERS THE WORK ZONE MORE THAN ONCE A DAY MUST BE EQUIPPED WITH AT LEAST ONE FLASHING, ROTATING, OR OSCILLATING AMBER LIGHT THAT IS VISIBLE IN ALL DIRECTIONS OF TRAFFIC FOR AT LEAST ONE QUARTER OF A MILE, DAY OR NIGHT.

MAJOR WORK ITEMS

THE FOLLOWING MAJOR WORK ITEMS WILL REQUIRE TRAFFIC MAINTENANCE WHICH SHALL BE INCORPORATED INTO THE CONTRACTOR'S SEQUENCE OF OPERATIONS.

- A. REMOVAL OF EXISTING PAVEMENT MARKINGS ON BRIDGE DECKS.
- B. CLEANING OF DECK SURFACE.
- C. SEALING OF DECK SURFACE GRAVITY FED RESIN.

SEQUENCE OF CONSTRUCTIONEASTBOUND LOCAL PHASE 1

ALL I-480 EB TRAFFIC WILL BE DIRECTED INTO THE EXPRESS LANE. TRAFFIC FROM THE I-77 RAMPS WILL BE DIRECTED INTO THE LEFT LANE ON THE EB LOCAL DECK. THE EXIT TO TRANSPORTATION WILL BE CLOSED AND THE DETOUR WILL FOLLOW THE ROUTE SHOWN ON P.13/27.

EASTBOUND LOCAL PHASE 2

ALL I-480 EB TRAFFIC WILL BE DIRECTED INTO THE EXPRESS LANE. TRAFFIC FROM THE I-77 RAMPS WILL BE DIRECTED INTO THE RIGHT LANE ON THE EB LOCAL DECK. TRANSPORTATION EXIT WILL BE OPEN FOR TRAFFIC COMING FROM I-77. THE DETOUR ROUTE FOR TRAFFIC FROM I-480 TO TRANSPORTATION WILL FOLLOW THE ROUTE SHOWN ON P.13/27.

WESTBOUND LOCAL PHASE 1

THE RIGHT LANE ON THE WB LOCAL DECK SHALL REMAIN OPEN TO ALLOW TRAFFIC TO EXIT TO THE I-77 RAMPS. I-480 WB TRAFFIC WILL BE DIRECTED TO TAKE THE EXPRESS LANES. TRANSPORTATION TO I-480 WB RAMP WILL BE CLOSED. THE DETOUR ROUTE WILL FOLLOW THE ROUTE SHOWN ON P.12/27.

WESTBOUND LOCAL PHASE 2

THE RIGHT LANE OF I-480 WB WILL BE CLOSED AFTER THE I-480 WB EXIT TO TRANSPORTATION BLVD. TRANSPORTATION TO I-480 WB RAMP WILL BE CLOSED. THE DETOUR ROUTE WILL FOLLOW THE ROUTE SHOWN ON P.12/27.

EASTBOUND EXPRESS LANES

A SINGLE LEFT LANE CLOSURE WILL BE IMPLEMENTED PRIOR TO THE EB EXPRESS LANES. ALL I-480 EB TRAFFIC WILL BE DIRECTED TO THE LOCAL LANES.

WESTBOUND EXPRESS LANES

A SINGLE LEFT LANE CLOSURE WILL BE IMPLEMENTED PRIOR TO THE WB EXPRESS LANES. ALL I-480 WB TRAFFIC WILL BE DIRECTED TO THE LOCAL LANES.



DESIGNER  
TSR

REVIEWER  
TLO 09/26/25

PROJECT ID

120625

SHEET TOTAL  
P.4 27



**ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS**

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

- DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.
- DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).
- FOR OPERATIONS WITHOUT POSITIVE PROTECTION OCCURRING WITHIN 10 FEET OF AN OPEN TRAVELED LANE THAT MEET ALL OF THE FOLLOWING CRITERIA:
  - ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND
  - AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND,
  - AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

"WITHOUT POSITIVE PROTECTION" MEANS USE OF DRUMS, CONES, SHADOW VEHICLE, ETC., WITHOUT PROTECTION FROM PORTABLE BARRIER OR OTHER RIGID BARRIER ALONG THE WORK AREA. THIS PHRASE DOES NOT APPLY TO CASES WHERE POSITIVE PROTECTION IS REQUIRED. MOBILE OPERATIONS ARE REGARDED AS "WITHOUT POSITIVE PROTECTION". FOR WORK ZONES USING A COMBINATION OF BARRIER AND TEMPORARY TRAFFIC CONTROL DEVICES (CONES, DRUMS, ETC.), THE DESIGNATION SHALL BE BASED UPON THE TYPE OF DEVICES USED IN THE AREA THAT WORKERS ARE LOCATED.

IF MULTIPLE ACTIVE LOCALIZED QUALIFYING WORK AREAS OCCUR WITHOUT POSITIVE PROTECTION, PER MAINLINE TRAFFIC DIRECTION, PROVIDE A UNIFORMED LEO AND OFFICIAL PATROL CAR IN ADVANCE OF:

- THE FIRST ACTIVE WORK AREA THAT DRIVERS WILL ENCOUNTER; OR
- THE ACTIVE WORK AREA LATERALLY CLOSEST TO THE OPEN TRAVELED LANE; OR
- OTHER LOCATION AS APPROVED BY THE ENGINEER.

THE UNIFORMED LEO AND OFFICIAL PATROL CAR MAY RELOCATE AMONG THE LISTED LOCATIONS AS APPROPRIATE AS THE OPERATIONS PROCEED IN THE LOCALIZED QUALIFYING WORK AREAS.

**ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONT.)**

IN GENERAL, LEO'S SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE **500 HOURS**

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

**ITEM 614 – PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN**

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEY'S DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

**ITEM 614 – PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN (CONT.)**

THE PORTABLE CHANGEABLE MESSAGE SIGN SHALL HAVE A WEB BASED COMMUNICATION SYSTEM THAT WILL ALLOW THE CONTRACTOR OR ODOT TO CHANGE OR PROGRAM THE MESSAGE BOARD REMOTELY. THIS SYSTEM SHALL BE PASSWORD PROTECTED AND MAY BE OPERATED FROM A COMPUTER OR HAVE AN APPLICATION THAT CAN BE OPENED FROM A CELL PHONE, ANDROID OR I PHONE. THE WEB BASED COMMUNICATION SYSTEM WILL SHOW THE LOCATION OF EACH MESSAGE BOARD ON A MAP. ALL CHARGES FOR THE WEB BASED COMMUNICATION SYSTEM WILL BE INCLUDED IN THE COST OF THIS ITEM, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614 – PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN **16 SIGN MONTH(S)**  
ASSUMING 2 PCMS SIGNS FOR 8 MONTHS AVERAGE.



DESIGNER

TSR

REVIEWER

TLO

09/26/25

PROJECT ID

120625

SHEET

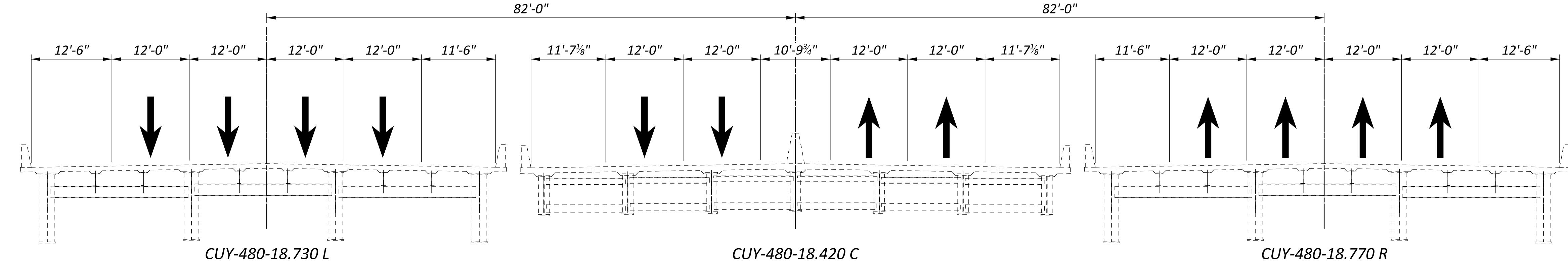
TOTAL

P.6

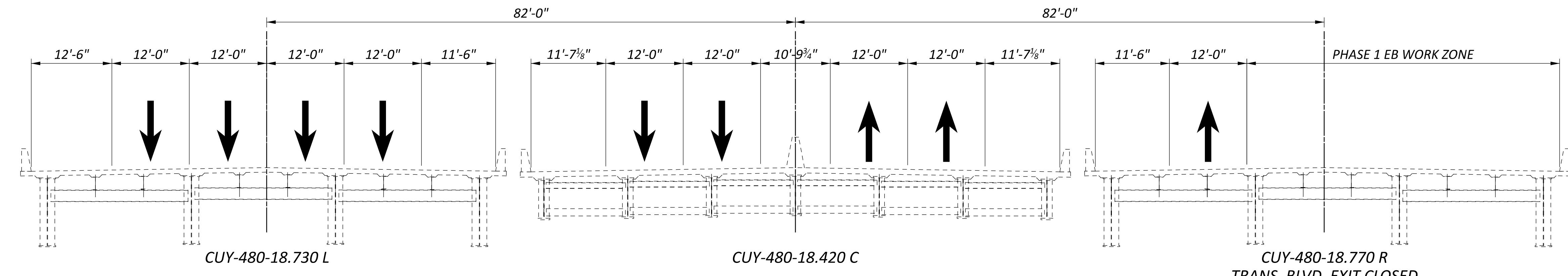
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# CUY-480-18.42 DECK SEALING

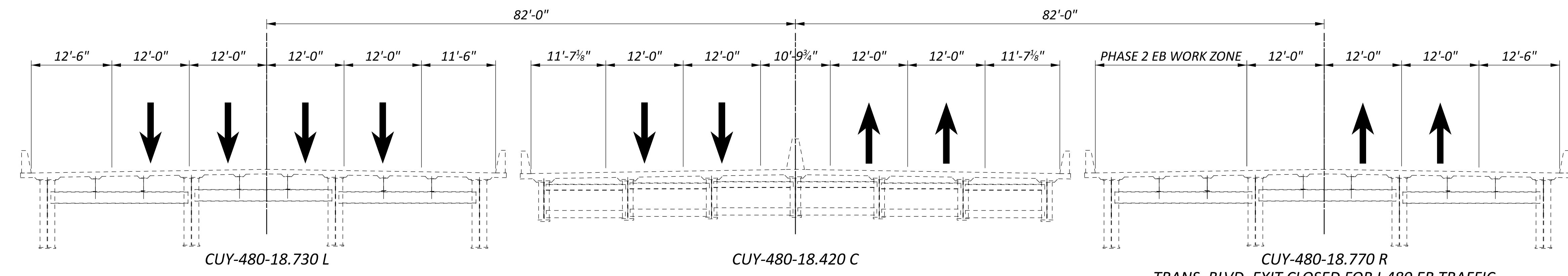
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## EXISTING MAINTENANCE OF TRAFFIC TYPICAL SECTIONS

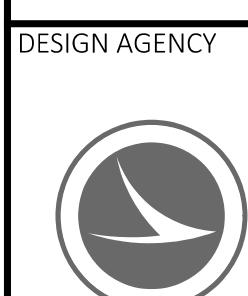


## MAINTENANCE OF TRAFFIC TYPICAL SECTIONS- PHASE 1



## MAINTENANCE OF TRAFFIC TYPICAL SECTIONS- PHASE 2

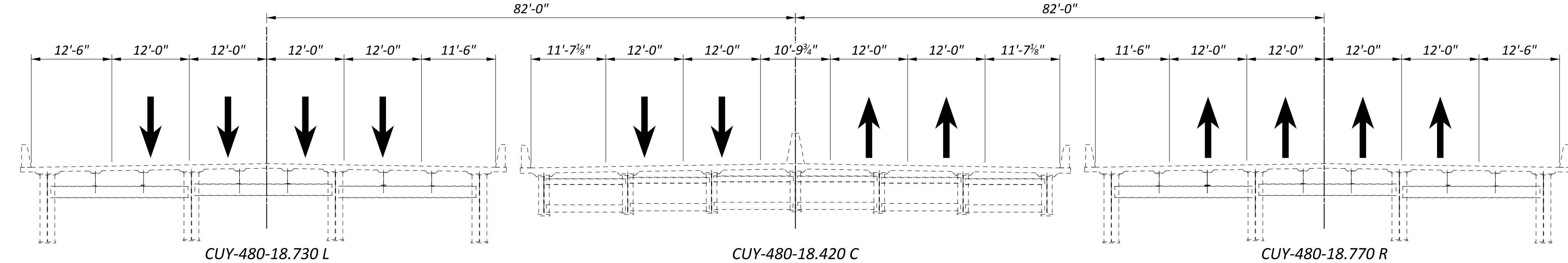
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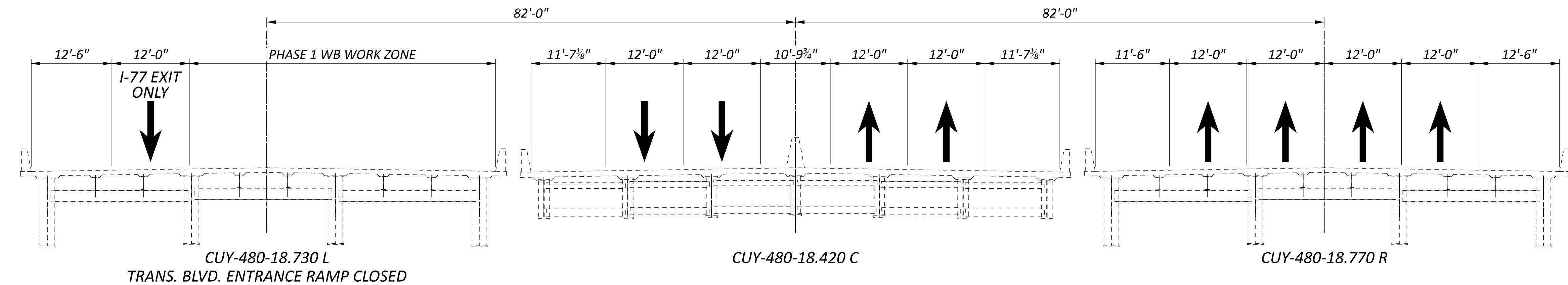
DESIGNER  
TSR  
REVIEWER  
TLO 09/26/25  
PROJECT ID  
120625  
SHEET TOTAL  
P.7 27

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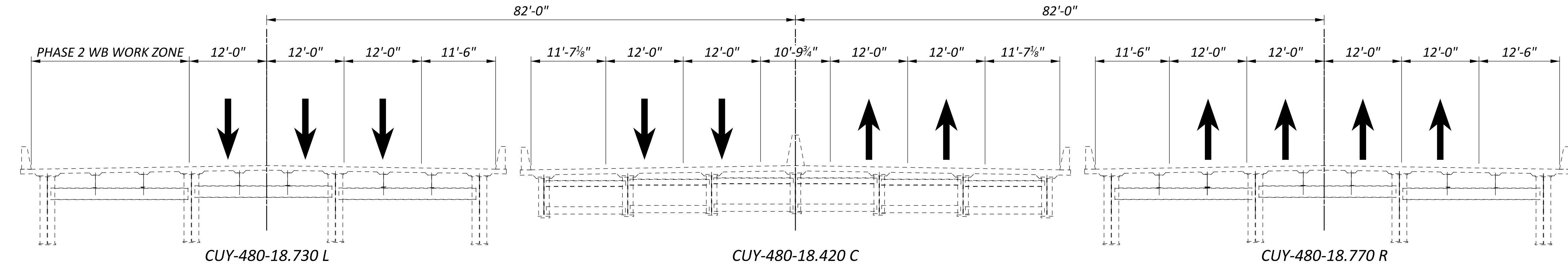
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## EXISTING MAINTENANCE OF TRAFFIC TYPICAL SECTIONS



## MAINTENANCE OF TRAFFIC TYPICAL SECTIONS- PHASE 1



## MAINTENANCE OF TRAFFIC TYPICAL SECTIONS- PHASE 2

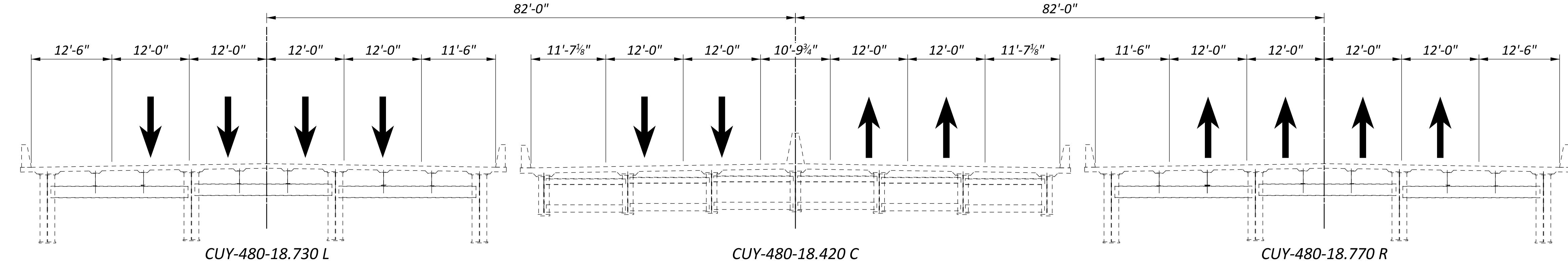
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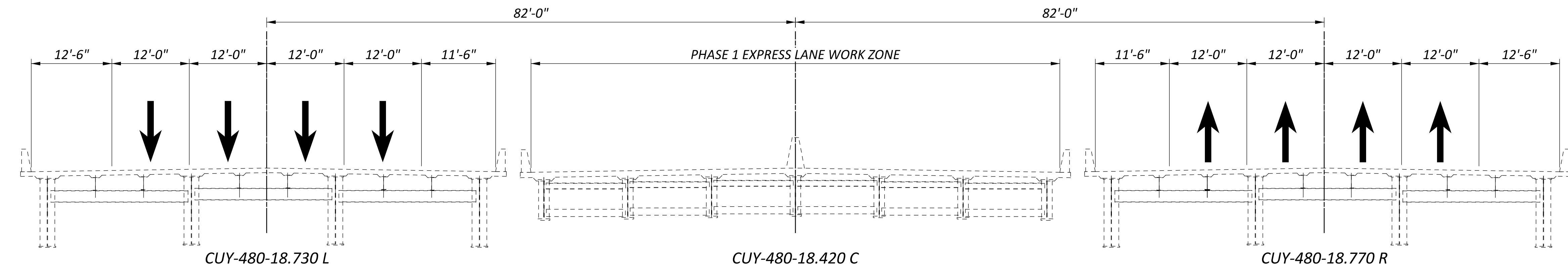
DESIGNER  
TSR  
REVIEWER  
TLO 09/26/25  
PROJECT ID  
120625  
SHEET TOTAL  
P.8 27

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EXISTING MAINTENANCE OF TRAFFIC TYPICAL SECTIONS



MAINTENANCE OF TRAFFIC TYPICAL SECTIONS- PHASE 1

## MAINTENANCE OF TRAFFIC - I-480 EXPRESS LANES TYPICAL SECTIONS

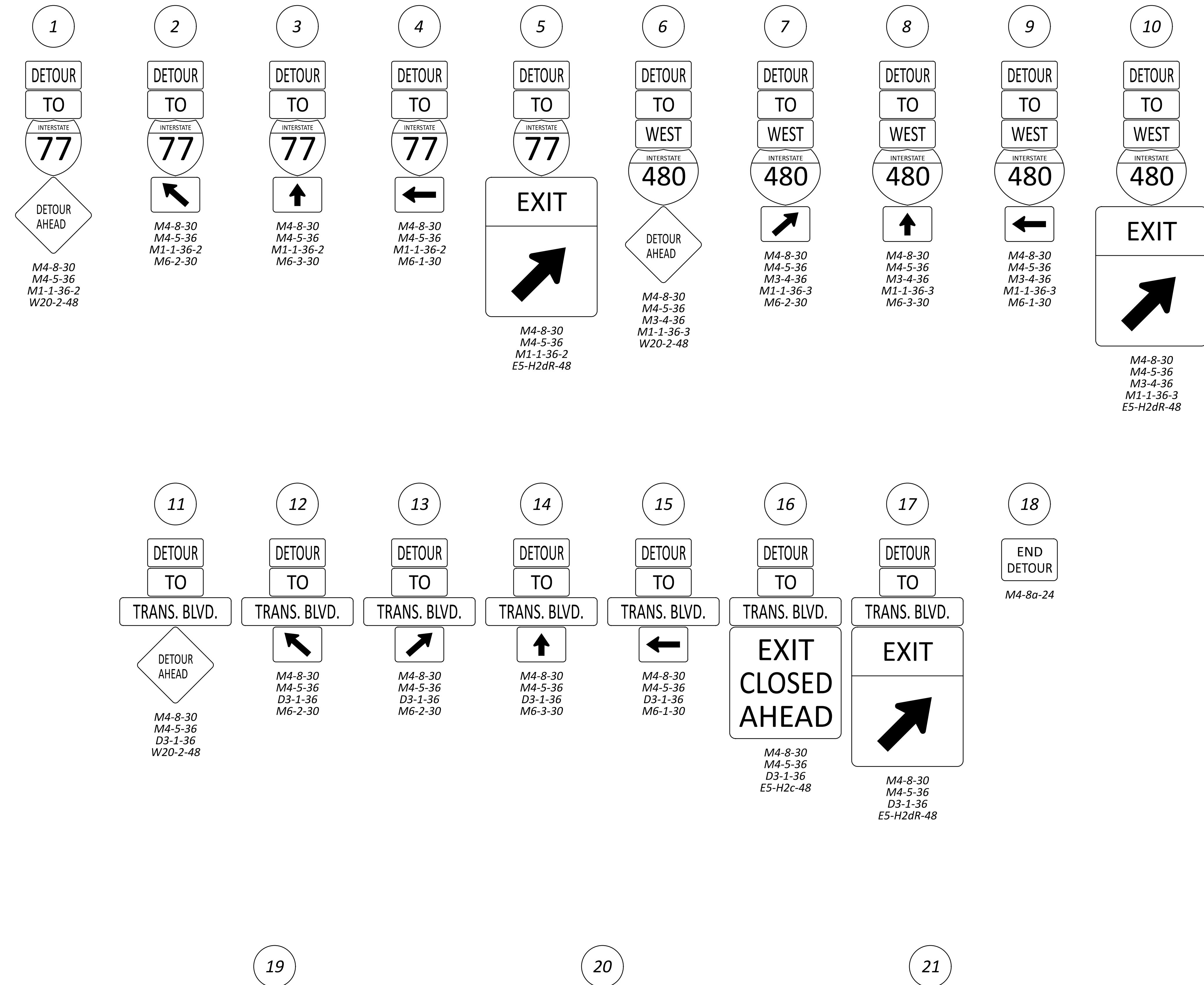


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TSR  
REVIEWER  
TLO 09/26/25  
PROJECT ID  
120625

SHEET TOTAL  
P.9 27

CUY-480-18.42 DECK SEALING

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	PRIOR TO CLOSURE	DURING CLOSURE
MESSAGE 1	I-77 RAMPS CLOSED	I-77 RAMPS CLOSED
MESSAGE 2	XX/XX - XX/XX 9P-5A	USE DETOUR

	PRIOR TO CLOSURE	DURING CLOSURE
<i>MESSAGE 1</i>	TRANS. BLVD. TO I-480 WEST CLOSED	TRANS. BLVD. TO I-480 WEST CLOSED
<i>MESSAGE 2</i>	XX/XX - XX/XX 9P-5A	USE DETOUR

	PRIOR TO CLOSURE	DURING CLOSURE
<i>MESSAGE 1</i>	EXIT TO TRANS. BLVD. CLOSED	EXIT TO TRANS. BLVD. CLOSED
<i>MESSAGE 2</i>	XX/XX - XX/XX 9P-5A	USE DETOUR

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# DESIGN AGENCY

DESIGNER	TSR
REVIEWER	
TLO	09/26
PROJECT ID	120625
SHEET	TOTAL

CUY-480-18.42 DECK SEALING

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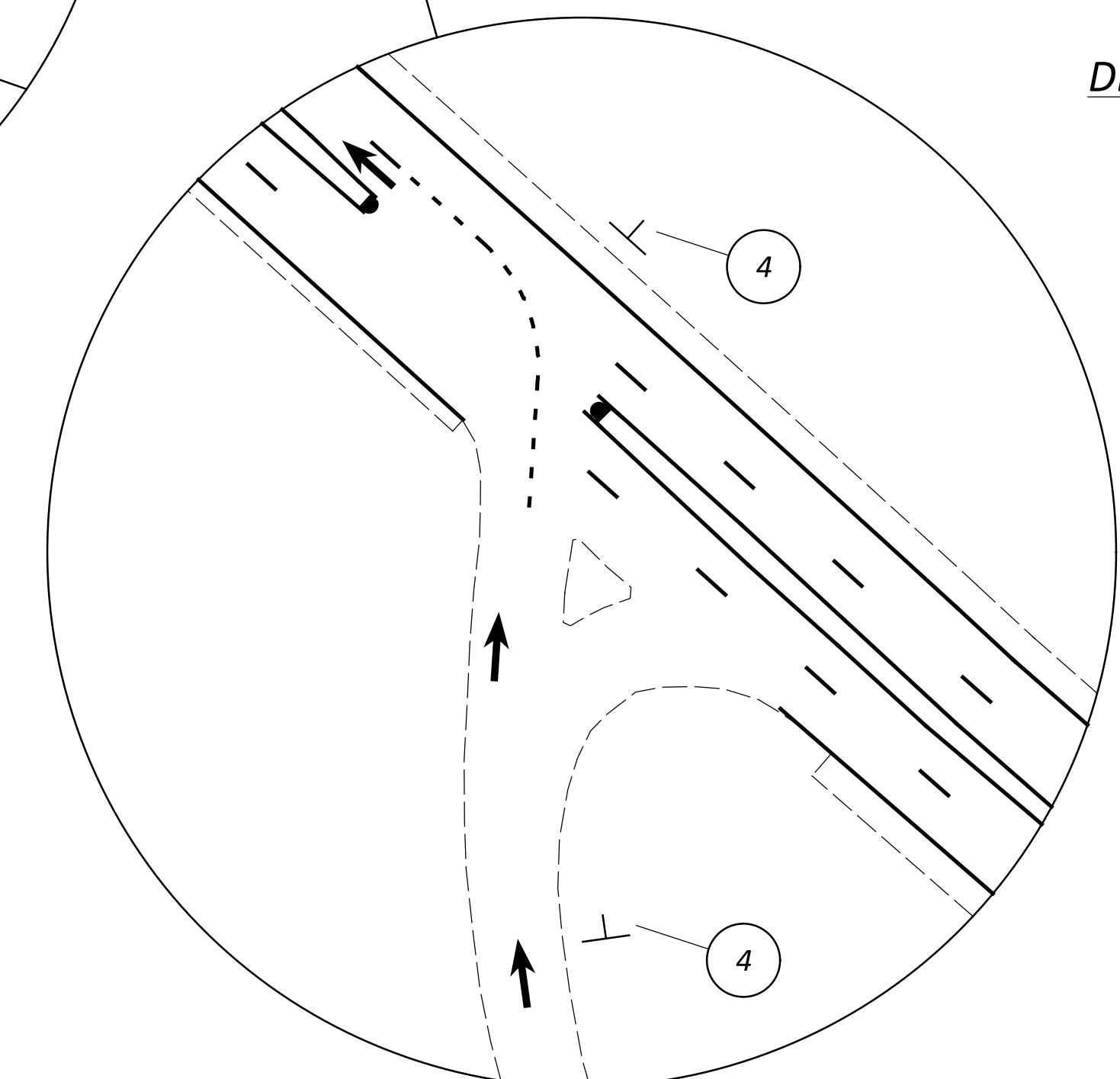
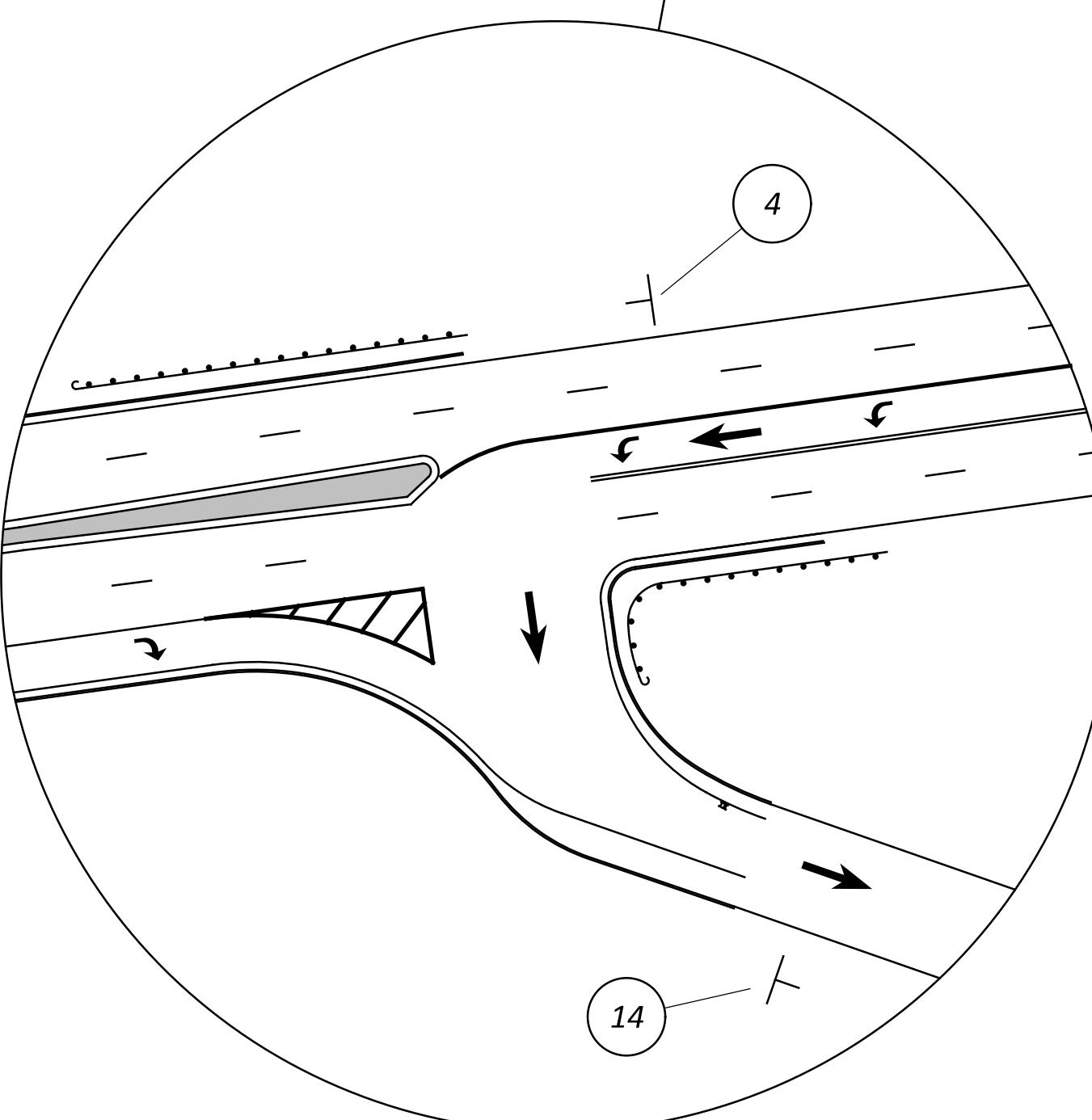
- ROAD CLOSURE

**← - DETOUR ROUTE**

1 - DEL 100

- PCMS

1. *THIS DETOUR SHALL BE ACTIVE DURING I-480 EB PHASE 3.*
2. *SEE SHEET P.10 / 27 FOR SIGNING LEGEND.*



## DETOUR PLAN - I-480 WB TO I-77 RAMPS

SEE SHEET P.12 / 27  
FOR TRANSPORTATION  
BLVD. RAMP DETOUR

## DETOUR PLAN - I-480 WB TO I-77 INTERCHANGE

# DESIGN AGENCY

## DESIGNER

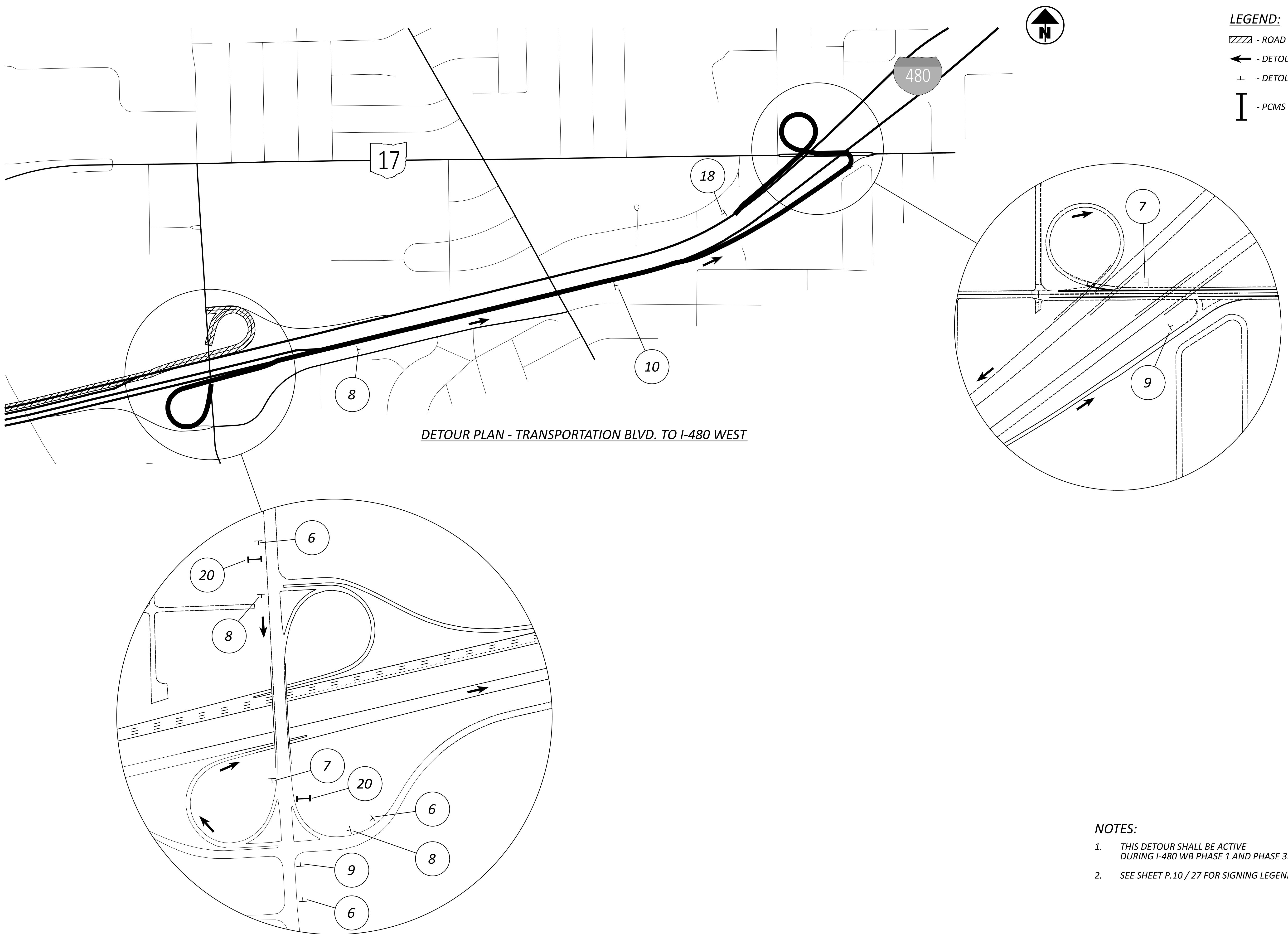
TSR  
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TLO 09/26/

PROJECT ID

SHEET TOTAL  
P 11 | 27

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## DETOUR PLAN - TRANSPORTATION BLVD. TO I-480 WEST

## **NOTES:**

1. *THIS DETOUR SHALL BE ACTIVE DURING I-480 WB PHASE 1 AND PHASE 3.*
2. *SEE SHEET P.10 / 27 FOR SIGNING LEGEND.*

DESIGN AGENCY

DESIGNER  
TOP

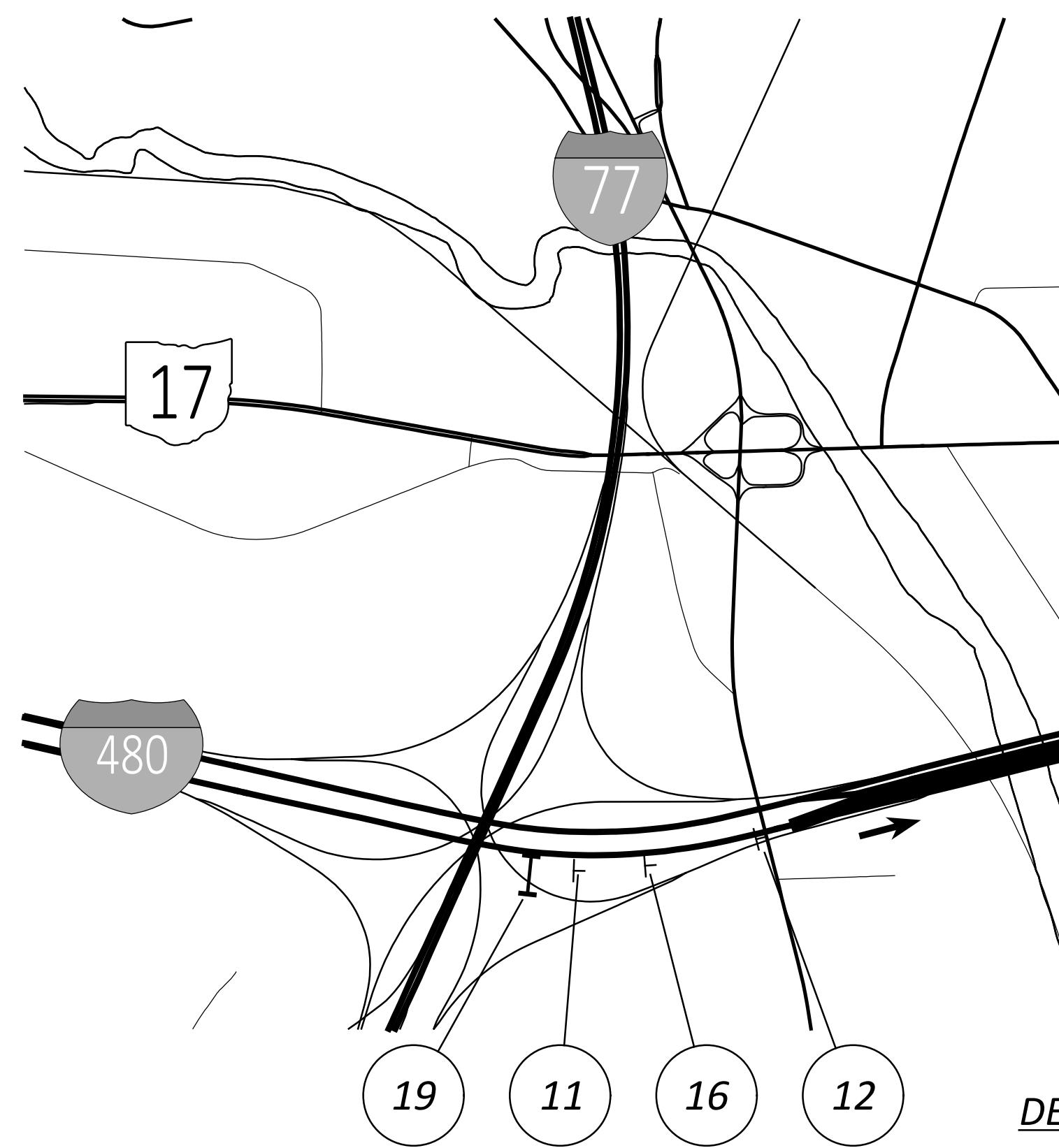
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TLO 09/26/

PROJECT ID

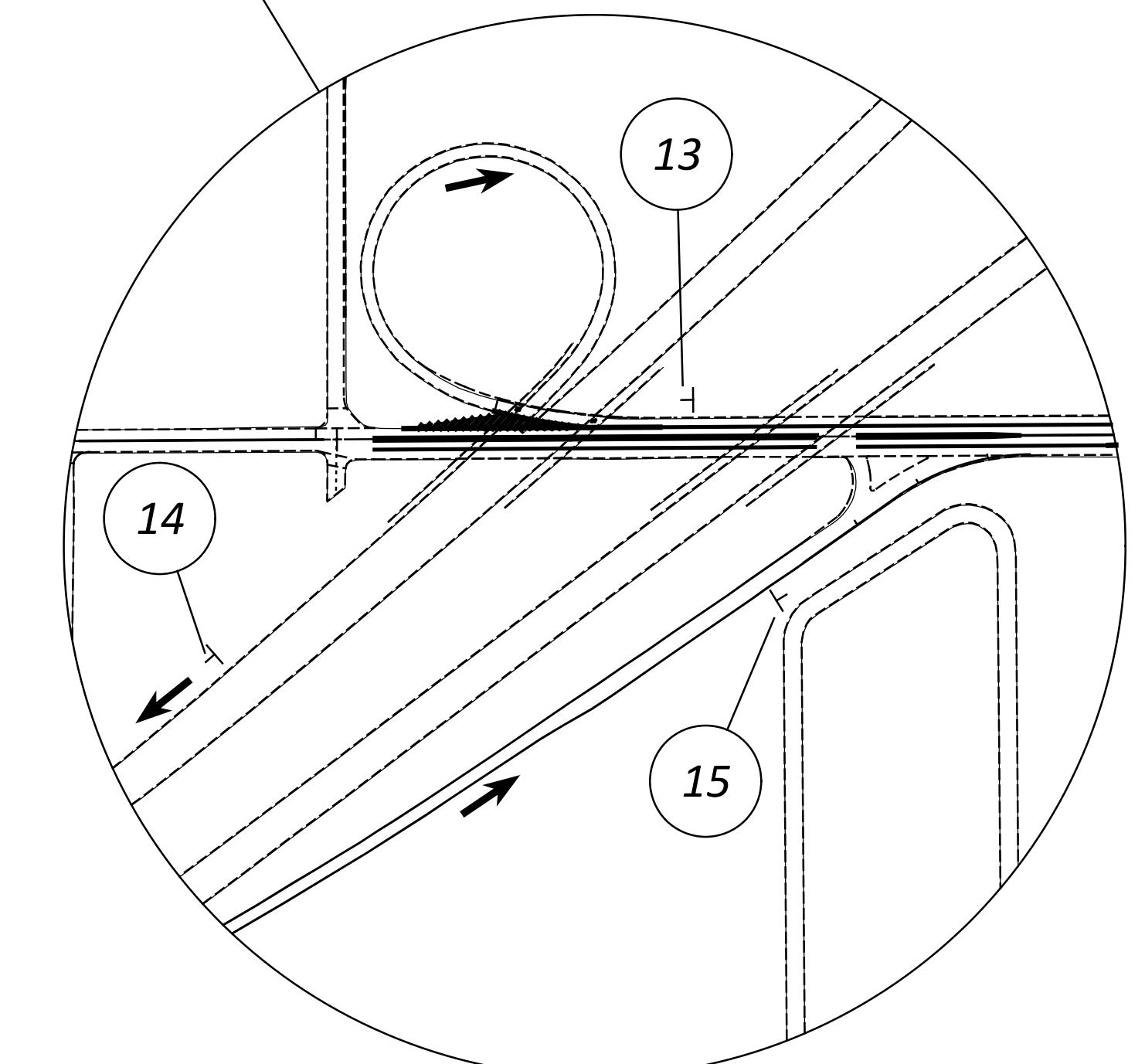
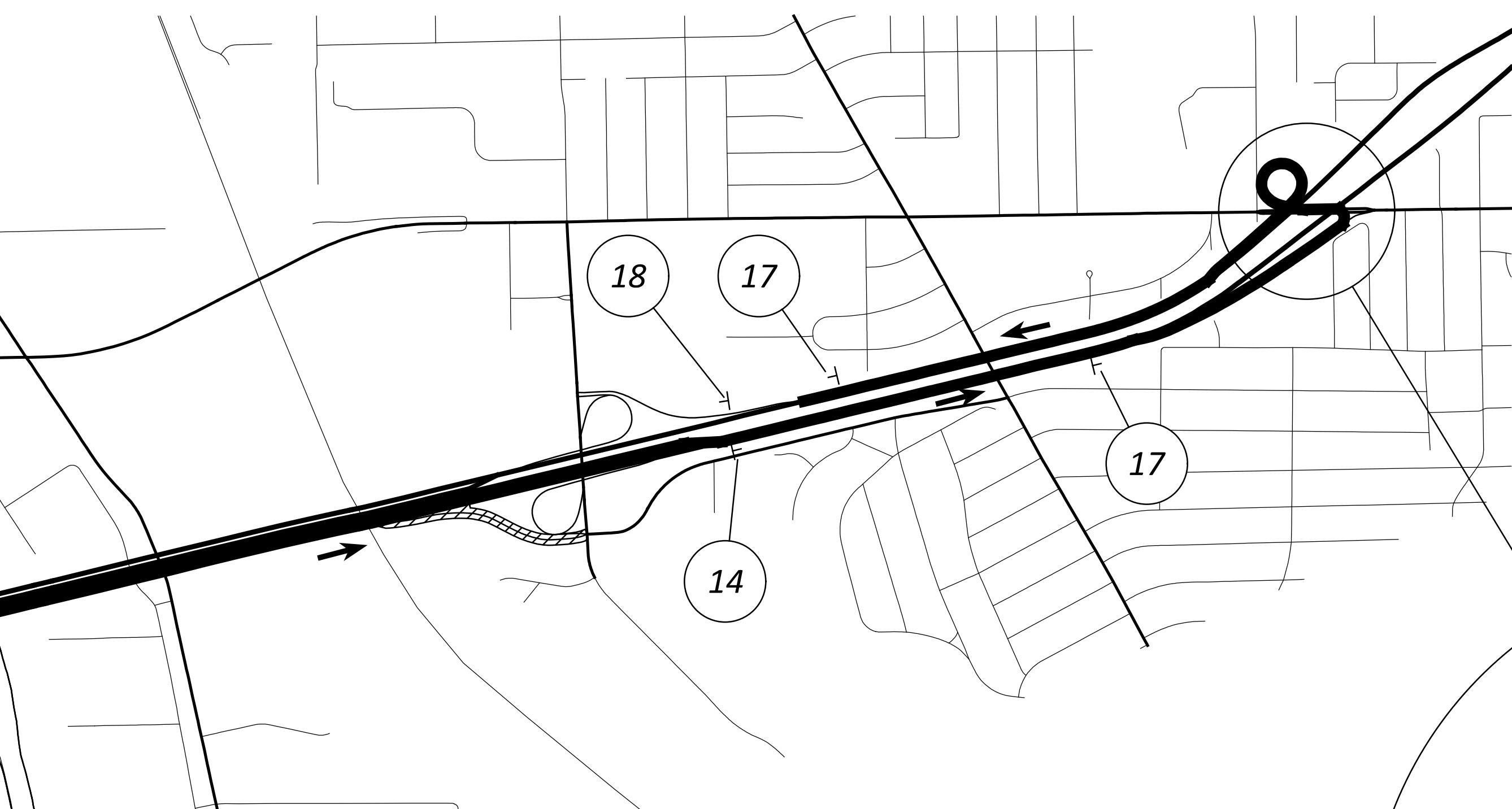
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P 12 | 27

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DETOUR PLAN - I-480 EAST TO TRANSPORTATION BLVD.

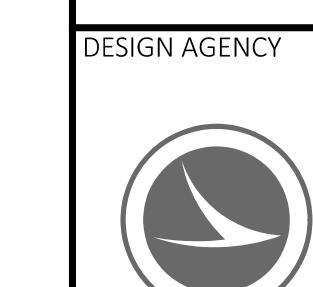


## LEGEND:

- |||| - ROAD CLOSURE
- ← - DETOUR ROUTE
- - DETOUR SIGN
- - PCMS

## NOTES:

1. THIS DETOUR SHALL BE ACTIVE DURING I-480 EB PHASE 1 & PHASE 2.
2. SEE SHEET P.10 / 27 FOR SIGNING LEGEND.



DESIGN AGENCY



DESIGNER

TSR



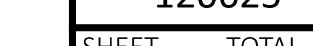
REVIEWER

TLO 09/26/25



PROJECT ID

120625



Sheet Total

P.13 27

DETOUR PLAN - I-480 EAST TO TRANSPORTATION BLVD.



## CUY-480-18.42 DECK SEALING

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TRAFFIC CONTROL													
50							50	621	00301	50	EACH	RPM REFLECTOR, AS PER PLAN	2
100							100	626	00102	100	EACH	BARRIER REFLECTOR, TYPE 1, (1WAY)	
			68,270				68,270	645	30000	68,270	FT	REMOVAL OF PAVEMENT MARKINGS	
			6.4				6.4	646	10010	6.4	MILE	EDGE LINE, 6"	
			6				6	646	10111	6	MILE	LANE LINE, 6", AS PER PLAN	2
			1,804				1,804	646	10310	1,804	FT	CHANNELIZING LINE, 12"	
			502				502	646	10620	502	FT	CHEVRON MARKING	
			5,018				5,018	646	20510	5,018	FT	DOTTED LINE, 12"	
STRUCTURE REPAIR (CUY-480-17.723R)													
150							150	512	10100	150	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)	
			34,375				34,375	512	73500	34,375	SY	TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN	
150							150	512	74000	150	SY	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES	
30							30	518	62200	30	EACH	STRUCTURE DRAINAGE, MISC.: SCUPPER CLEANOUT	2
STRUCTURE REPAIR (CUY-480-18.723C)													
150							150	512	10100	150	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)	
			37,109				37,109	512	73500	37,109	SY	TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN	
150							150	512	74000	150	SY	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES	
30							30	518	62200	30	EACH	STRUCTURE DRAINAGE, MISC.: SCUPPER CLEANOUT	2
STRUCTURE REPAIR (CUY-480-18.723L)													
150							150	512	10100	150	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)	
			33,660				33,660	512	73500	33,660	SY	TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN	
150							150	512	74000	150	SY	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES	
30							30	518	62200	30	EACH	STRUCTURE DRAINAGE, MISC.: SCUPPER CLEANOUT	2
MAINTENANCE OF TRAFFIC													
500							500	614	11110	500	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
16							16	614	18601	16	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	6
			6				6	614	20560	6	MILE	WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT	
			6.4				6.4	614	22360	6.4	MILE	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT	
			1,804				1,804	614	23690	1,804	FT	WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT	
			5,018				5,018	614	24618	5,018	FT	WORK ZONE DOTTED LINE, CLASS III, 12", 642 PAINT	
300							300	630	97800	300	SF	SIGNING, MISC.:ADDITIONAL SIGNS, GROUND MOUNTED, AS DIRECTED BY ENGINEER	5
INCIDENTALS													
							LS	614	11000	LS		MAINTAINING TRAFFIC	
							LS	623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING	
							LS	624	10000	LS		MOBILIZATION	



DESIGNER

TSR

REVIEWER

PAT 08/01/25

PROJECT ID

120625

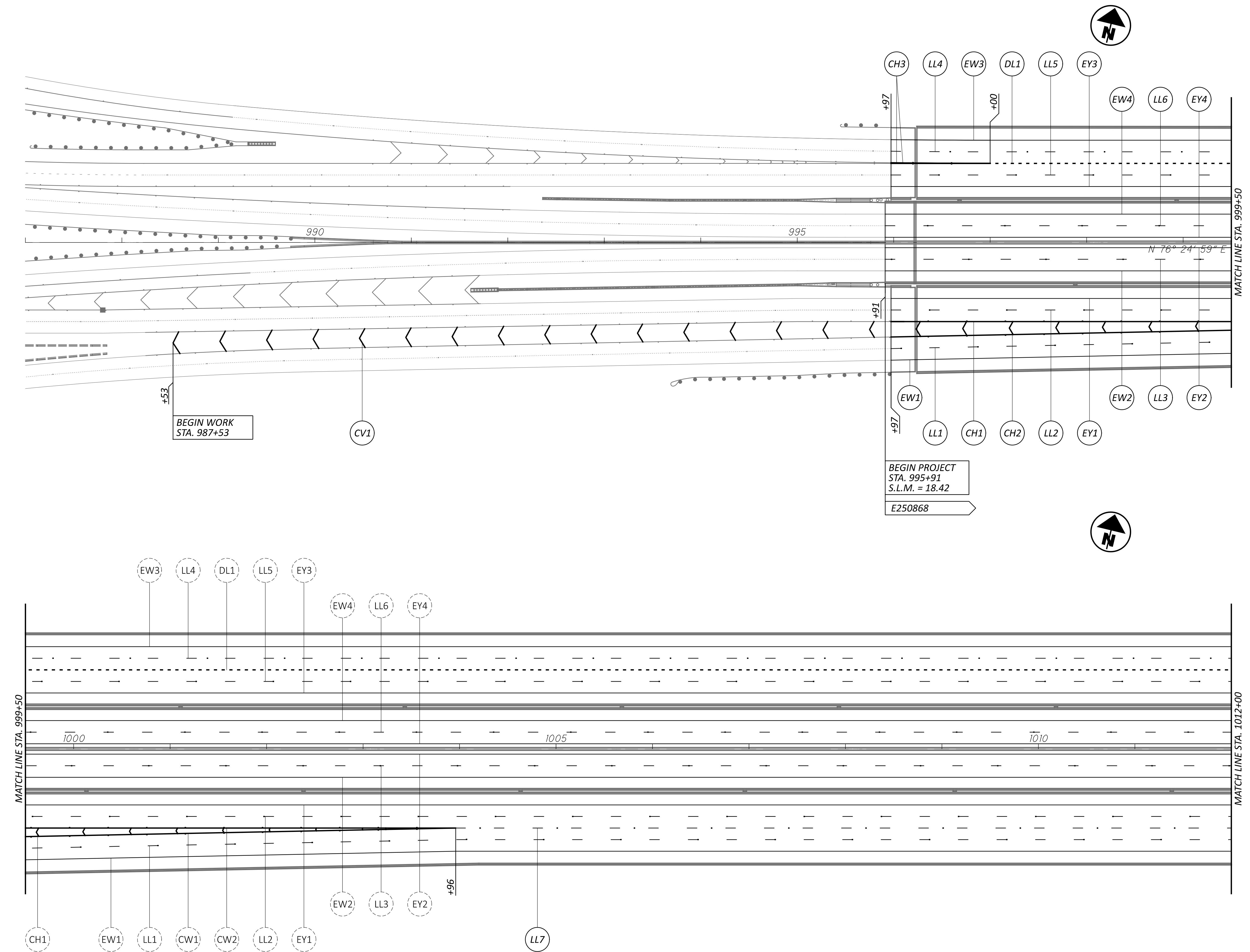
SHEET TOTAL

P.14 27

GENERAL SUMMARY

# CUY-480-18.42 DECK SEALING

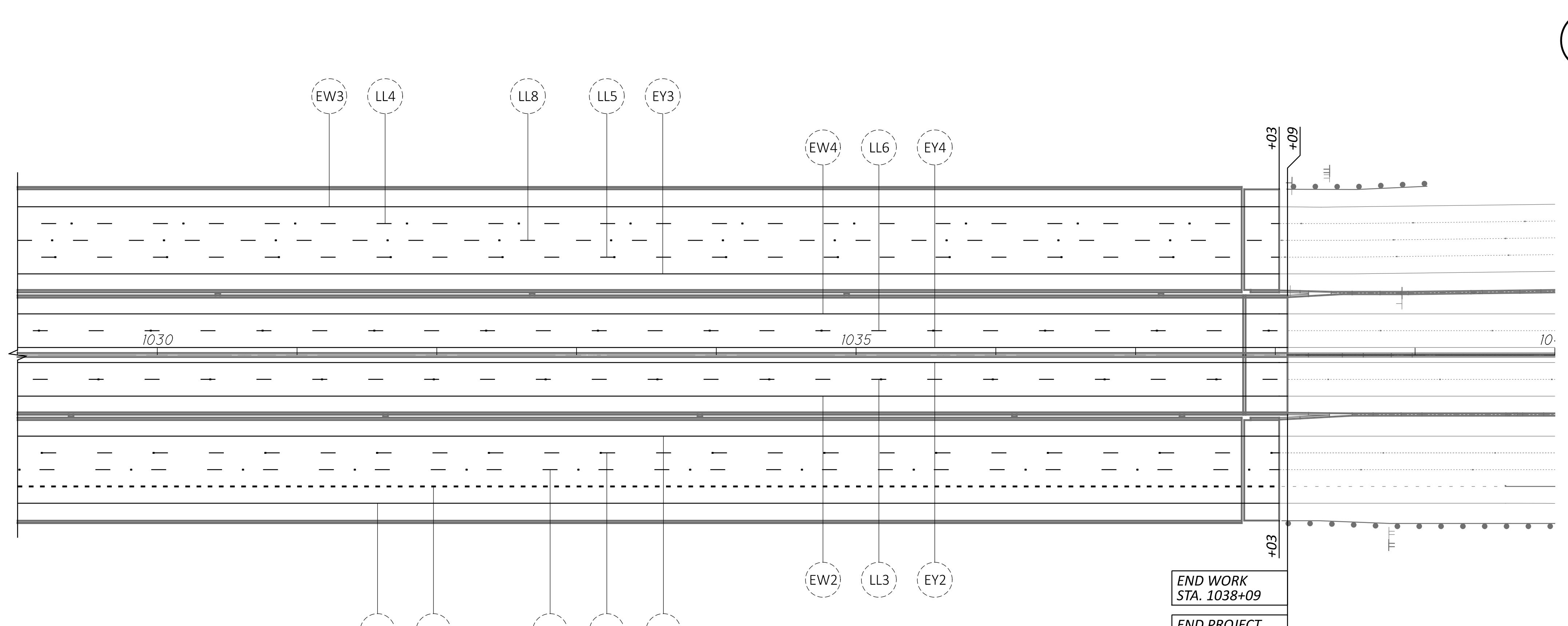
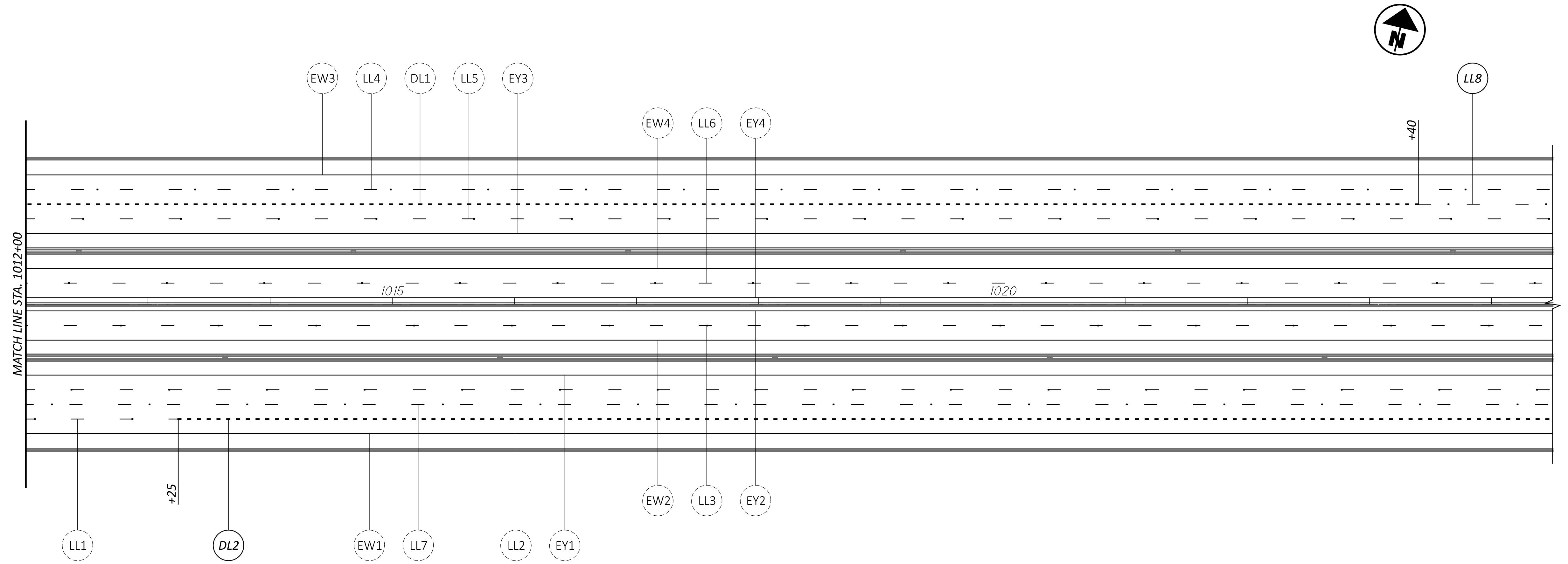
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DESIGN AGENCY	
DESIGNER	TSR
REVIEWER	TLO 09/26/25
PROJECT ID	120625
SHEET	TOTAL
P.15	27

CUY-480-18.42 DECK SEALING

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END WORK  
STA. 1038+09

END PROJECT  
STA. 1038+09  
S.L.M. = 19.22

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## DESIGN AGENCY

DESIGNER

REVIEWER  
TLO 09/26/1

PROJECT ID  
120625

HEET P.16 | TOTAL 27

HORIZONTAL  
SCALE IN FEET



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2UY-480-18.42 DECK SEAHING

PLTDRV: OHDOT\_PDF.pltcfg PENTBL: OHDOT\_Pen.tbl USER: Thomas.Ritter@dot.ohio.gov WORKSPACE: OHDOTCEv02 WORKSET: 120625 PRODUCT: OpenRoadsDesigner 24.00.00.205  
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pwv:\ohio\dot-pwv\bentley.com\ohiodot-pw\02\Documents\01 Active Projects\District 12\Cuahoga\120625\400-Engineering\Structures\1810521\Sheets\120625\_SFN\_1810521\_SF001.dgn

PROPOSED STRUCTURE CUY-480-1842 L	
TYPE:	NEW COMPOSITE REINFORCED CONCRETE DECK ON EXISTING STEEL SUPERSTRUCTURE.
SPANS:	SAME AS EXISTING
ROADWAY:	UNIT 1 - VARIES 72'-0" TO 72'-4" T/T PARAPETS UNITS 2, 3, 4 AND 5 - 72'-0" T/T PARAPETS
LOADING:	HS20-44, ALTERNATE MILITARY LOADING AND 15 PSF FUTURE WEARING SURFACE
SKEW:	SAME AS EXISTING
WEARING SURFACE:	1" MONOLITHIC CONCRETE
APPROACH SLABS:	AS-1-15 (25'-0" LONG) & AS-2-15
ALIGNMENT:	TANGENT
CROWN:	0.0156
COORDINATES:	WESTBOUND BRIDGE LATITUDE 41° 24' 29.27" N LONGITUDE 81° 38' 13.44" W

EXISTING STRUCTURE CUY-480-1842 R/L	
TYPE:	CONTINUOUS WELDED STEEL GIRDERS WITH FLOOR SYSTEM AND REINFORCED CONCRETE DECK AND SUBSTRUCTURE.
SPANS:	UNIT 1 - 220±, 300±, AND 275± UNIT 2, 3 AND 4 - 25' CANTILEVER, 2 @ 300± AND 275± UNIT 5 - 25' CANTILEVER, 2 @ 225± AND 180±
ROADWAY:	UNIT 1 L - VARIES 69'-6"± TO 69'-10"± FACE TO FACE OF PARAPETS UNIT 1 R - VARIES 69'-6"± TO 85'-0"± FACE TO FACE OF PARAPETS UNITS 2, 3, 4 AND 5 - 69'-6"± FACE TO FACE OF PARAPETS
LOADING:	HS 20-44 AND ALTERNATE MILITARY LOADING
SKEW:	NONE
WEARING SURFACE:	2½" SUPERPLASTICIZED DENSED CONCRETE (1990)
APPROACH SLABS:	AS-1-67 (25' LONG)
ALIGNMENT:	TANGENT
CROWN:	0.0156±
DATE BUILT:	1975
DISPOSITION:	NEW DECK (LEFT BRIDGE)

UNIT	LENGTH	START WIDTH	END WIDTH	512	
				FT	SY
REAR APPROACH START	25.0	72.3	72.3	1808	201
UNIT 1	797.8	72.3	72.0	57571	6397
UNIT 2	900.0	72.0	72.0	64800	7200
UNIT 3	900.0	72.0	72.0	64800	7200
UNIT 4	900.0	72.0	72.0	64800	7200
UNIT 5	657.8	72.0	72.0	47358	5262
FWD APPROACH END	25.0	72.0	72.0	1800	200
TOTAL CARRIED TO GENERAL SUMMARY				33,660	
CALCULATED:				TSR	11/6/2024
CHECKED:				NDG	12/31/2024

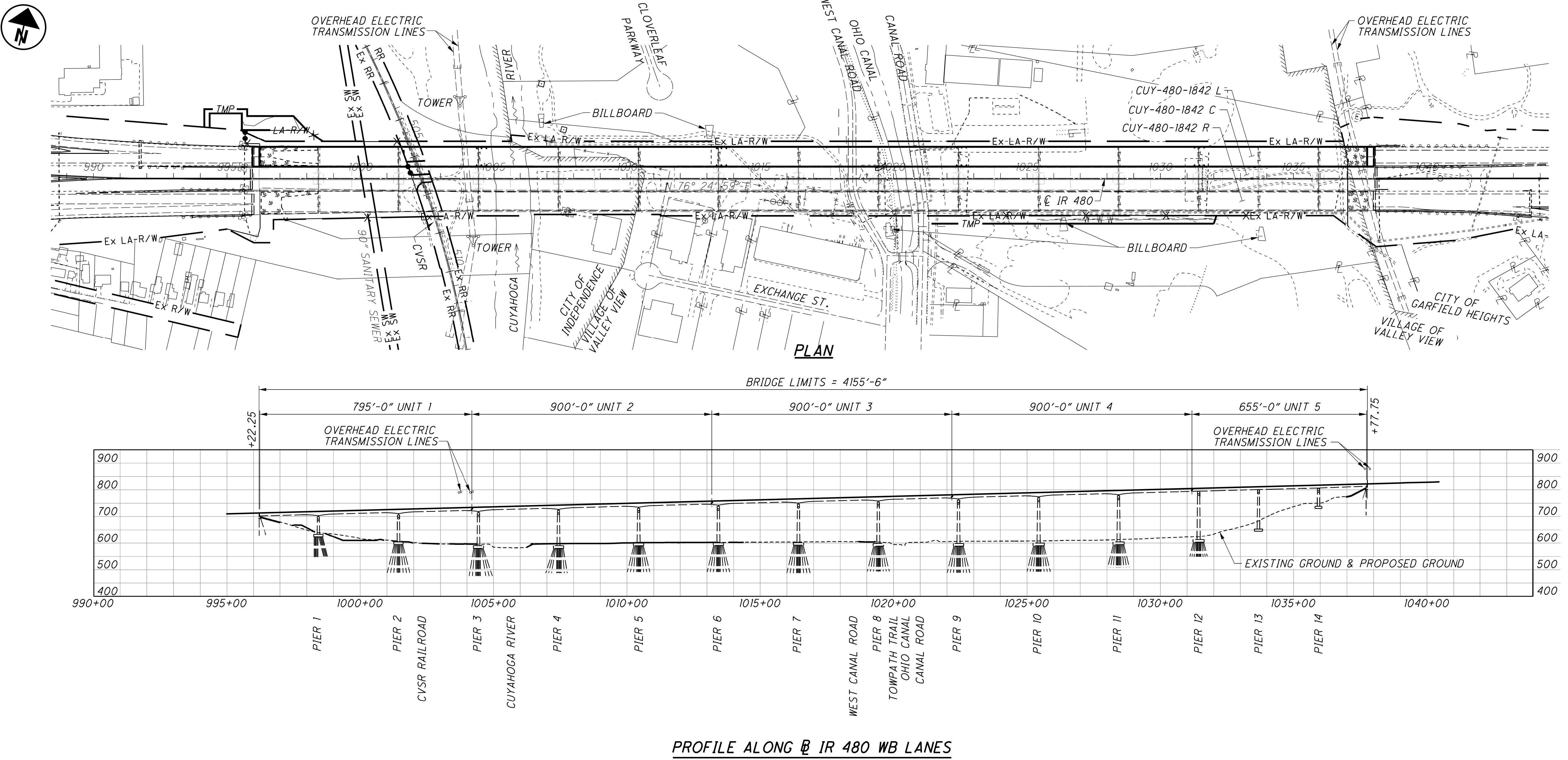
## NOTES

- DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.
- PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT, ESTIMATED QUANTITIES AND/OR AS DESCRIBED IN THE GENERAL NOTES.

DESIGNER	CHECKER
TSR	RHC
REVIEWER	
PAT 08/01/25	
PROJECT ID	
120625	
SUBSET TOTAL	
1 10	
SHEET TOTAL	
P.18 27	

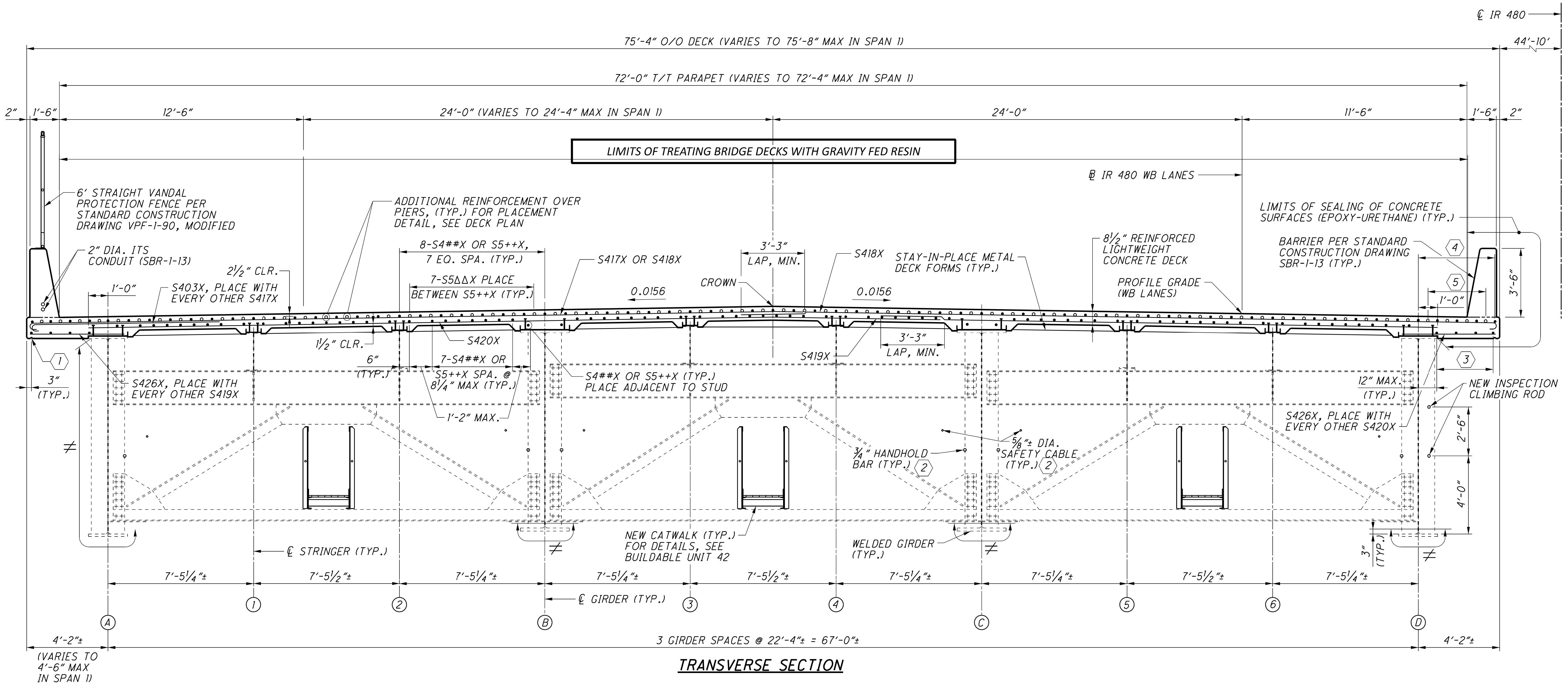
## GENERAL PLAN

BRIDGE NO. CUY-480-18.723 L  
OVER CUYAHOGA RIVER VALLEY



TRANSVERSE SECTION  
BRIDGE NO. CUY-480-18.723 L  
OVER CUYAHOGA RIVER VALLEY

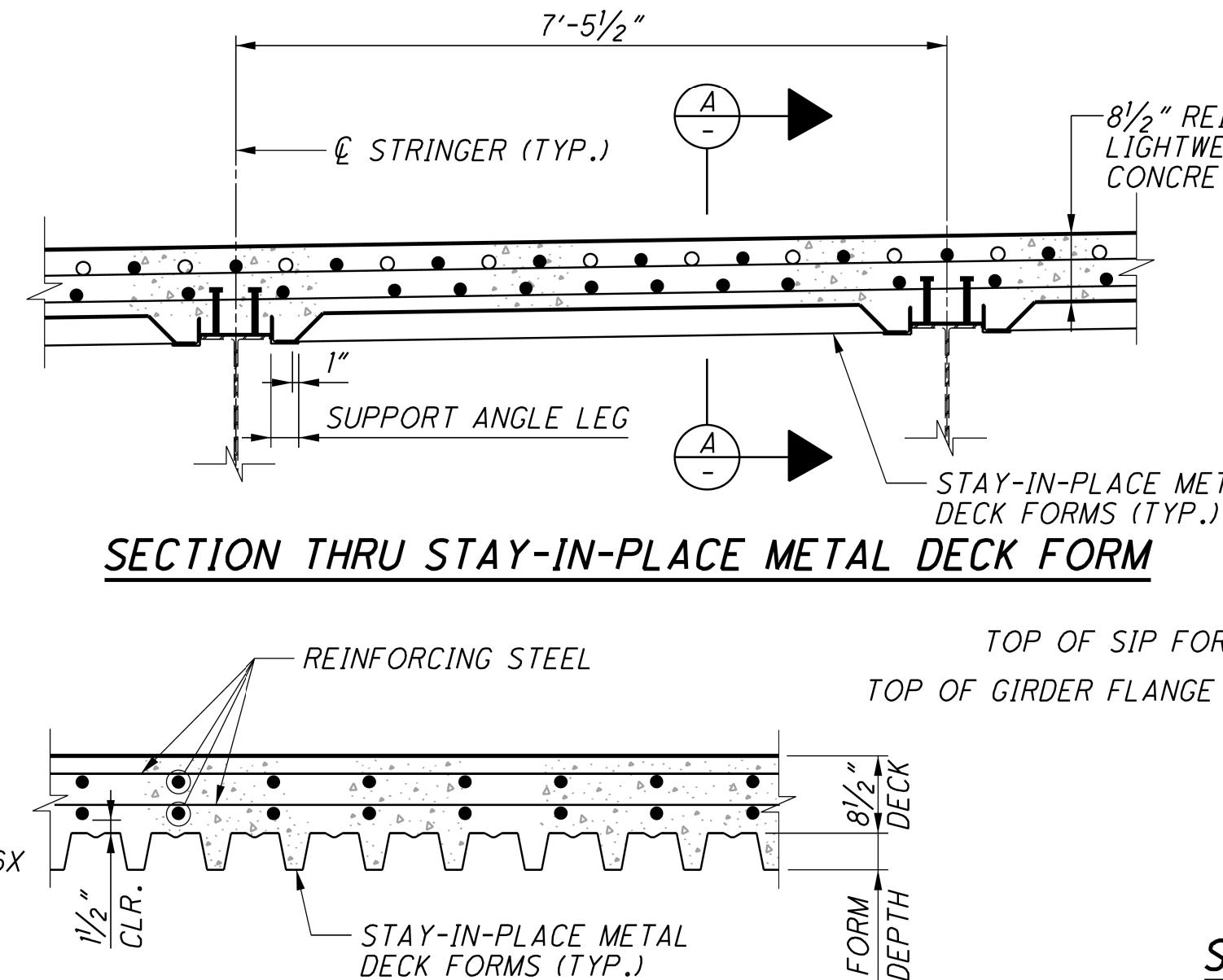
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## LEGEND

≠ INDICATES LIMITS FOR COMPLETE ABRASIVE BLAST CLEANING & PAINTING FOR THE FULL LENGTH OF THE BRIDGE.

- # STRINGER DESIGNATION
- L GIRDER DESIGNATION
- 1 1" DIA. HALF ROUND DRIP GROOVE, (TYP.)
- 2 SEE CONTRACT DOCUMENT ATTACHMENT ST-01 PLANS FOR REPAIR DETAILS
- 3 4 - S4##X OR S5++X, SPA. @ 12" MAX. (TYP.)
- 4 5 - S4##X OR S5++X, SPA. @ 12" MAX. (TYP.)
- 5 4 - S5ΔΔX, PLACE BETWEEN S5++X (TYP.)
- ## S401X, S402X, S404X, S405X, S406X, S407X, S408X, S409X, S410X, S411X, S412X, S413X, S414X, S415X, S416X
- ++ S501X, S502X, S503X, S504X, S505X, S506X, S507X, S508X, S509X, S510X, S511X, S512X, S526X
- ΔΔ S501X, S513X, S514X, S515X, S516X, S517X, S518X, S519X, S520X, S521X, S522X, S524X, S525X, S527X



## SECTION THRU STAY-IN-PLACE METAL DECK FORMS

## **SECTION A-**

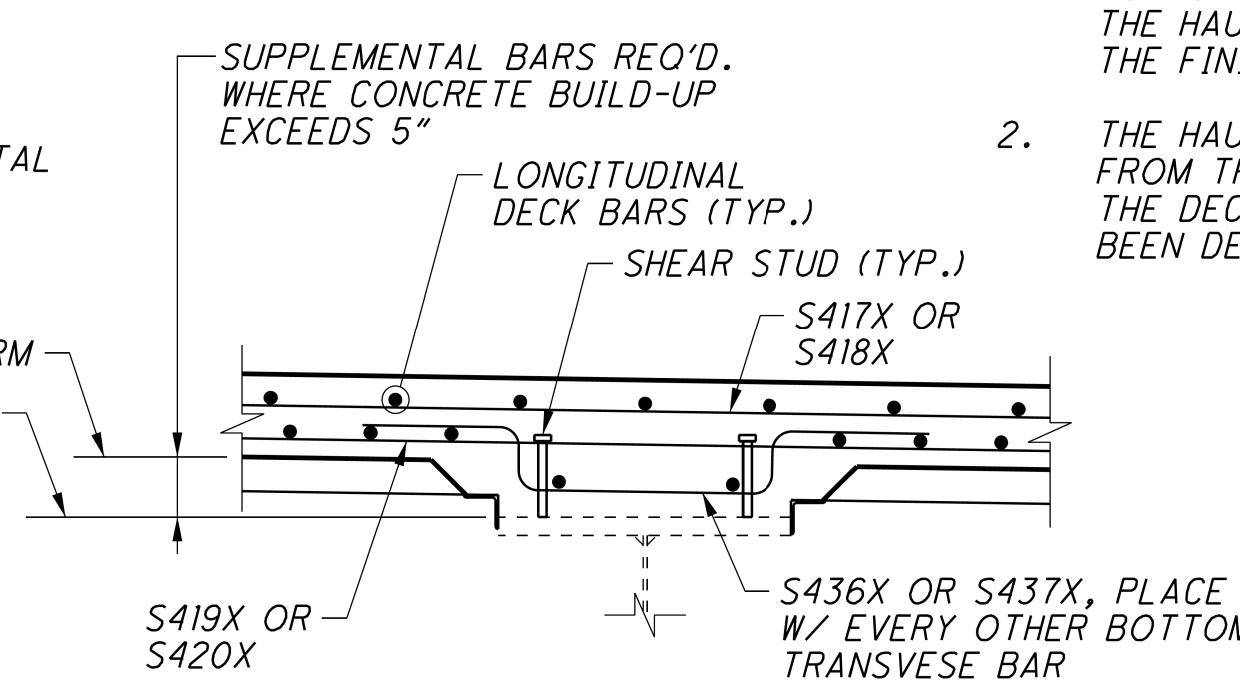
FOR GIRDERS B & C WHERE CONCRETE BUILD-UP >5" ONLY. ESTIMATED LOCATIONS SHOWN ON DECK PLAN SHEETS, TO BE REVISED AFTER MEASURING GIRDER REBOUND AND SETTING SCREED ELEVATIONS PER SHEET 36B3-44.

## *SUPPLEMENTAL HAUNCH REINFORCEMENT DETAIL*

FOR GIRDERS B & C WHERE CONCRETE BUILD-UP >5" ONLY. ESTIMATED LOCATIONS SHOWN ON DECK PLAN SHEETS, TO BE REVISED AFTER MEASURING GIRDER REBOUND AND SETTING SCREED ELEVATIONS PER SHEET 36B3-44.

## NOTES:

1. **DECK SLAB CONCRETE QUANTITY:** THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH GIRDER OR STRINGER HAUNCH, PLUS 1 INCH OF ADDITIONAL DECK THICKNESS TO ACCOUNT FOR CONCRETE IN THE FLUTES OF THE STAY-IN-PLACE METAL DECK FORMS. THE ESTIMATE ASSUMES A CONSTANT GIRDER HAUNCH THICKNESS OF 7" AS MEASURED FROM TOP OF GIRDER WEB TO UNDERSIDE OF THE 8½" DECK AND A CONSTANT STRINGER HAUNCH THICKNESS OF 4½" AS MEASURED FROM TOP OF TOP FLANGE TO UNDERSIDE OF THE 8½" DECK, DEVIATE FROM THE HAUNCH THICKNESSES AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.
2. THE HAUNCH THICKNESS WAS MEASURED AT THE CENTERLINE OF THE GIRDER, FROM THE SURFACE OF THE DECK TO THE BOTTOM OF THE TOP FLANGE MINUS THE DECK SLAB THICKNESS. THE AREA OF ALL EMBEDDED STEEL PLATES HAS BEEN DEDUCTED FROM THE HAUNCH QUANTITY IN ACCORDANCE WITH 511.23.



## NOTES

1. *DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.*
2. *PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT, ESTIMATED QUANTITIES AND/OR AS DESCRIBED IN THE GENERAL NOTES.*

SFN	1812521
DESIGN AGENCY	
	
DESIGNER	CHECKER
TSR	RHC
REVIEWER	
PAT	08/01/25
PROJECT ID	
120625	
SUBSET	TOTAL
2	10
SHEET	
P.19	
TOTAL	
27	

# CUY-480-18.42 DECK SEALING

MODEL: Sheet: PAPERSIZE: 34x22 (in.) DATE: 1/8/2026 TIME: 10:20:44 AM PTDRV: OHDOT\_PDF.pdf PENTBL: OHDOT\_Pen.tbl USER: Thomas.Ritter@dot.ohio.gov WORKSPACE: OHDOTCE02 WORKSET: OHDOTCE02 WORKSN: 1812522\Sheets\120625\400-Engineering\Structures\SN\_1812522\_SG001.dgn

## PROPOSED STRUCTURE CUY-480-1842C

TYPE: CONTINUOUS STEEL GIRDERS WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURES

SPANS: UNIT 1 - 220'-0", 300'-0", 300'-0", 232'-0"  
 UNIT 2 - 68'-0" CANTILEVER, 300'-0", 300'-0", 300'-0", 232'-0"  
 UNIT 3 - 68'-0" CANTILEVER, 300'-0", 300'-0", 300'-0", 255'-0"  
 UNIT 4 - 45'-0" CANTILEVER, 225'-0", 225'-0", 180'-0"  
 C/C BEARINGS ALONG & IR 480

ROADWAY: TWO AT 39'-7 1/8" T/T PARAPETS

LOADING: HL-93 AND 60 PSF FUTURE WEARING SURFACE

SKew: NONE

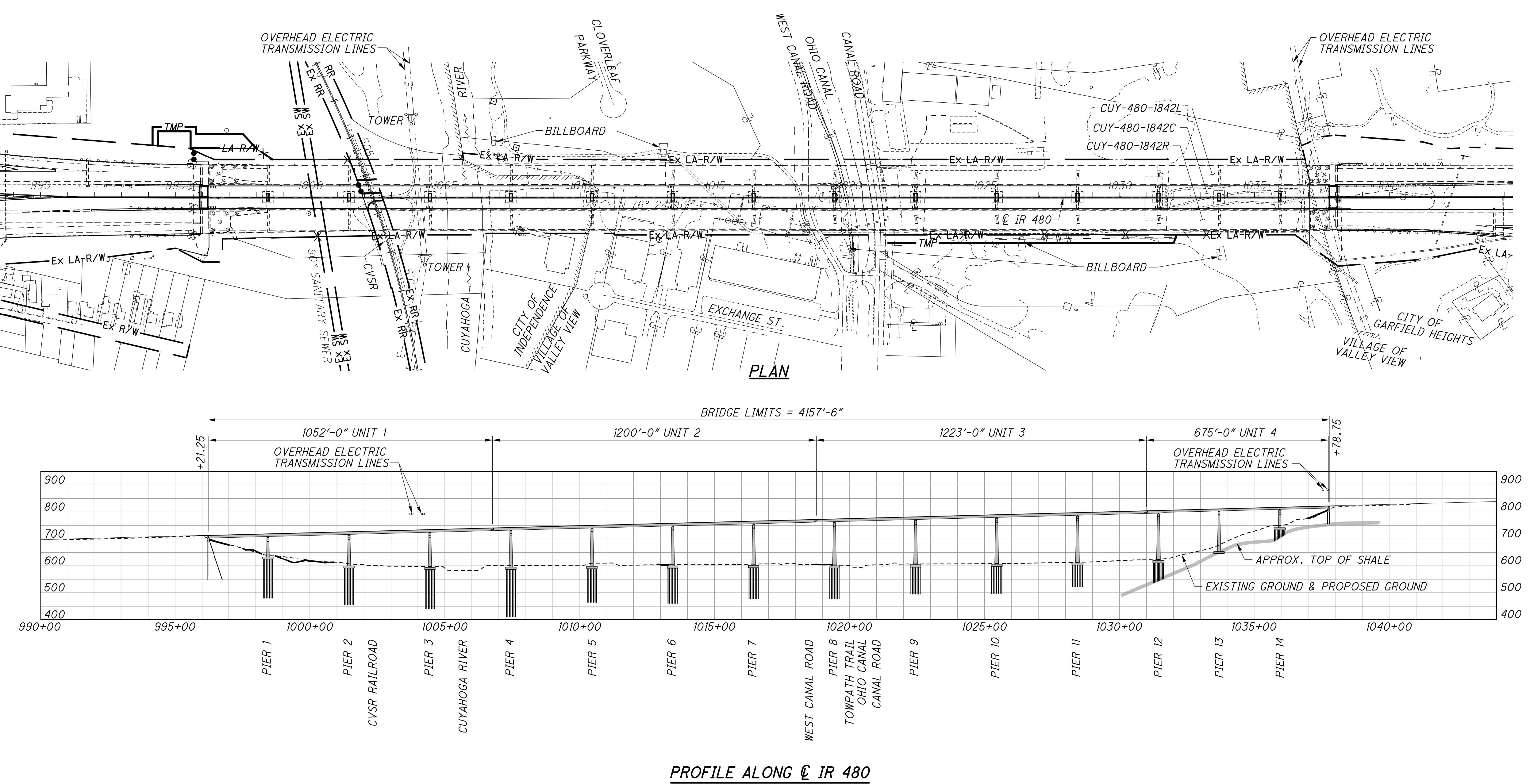
WEARING SURFACE: 1" MONOLITHIC CONCRETE

APPROACH SLABS: AS-1-15 (30'-0" LONG MINIMUM) & AS-2-15

ALIGNMENT: TANGENT

CROWN: 0.016

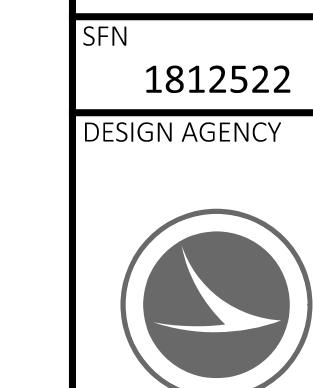
COORDINATES: LATITUDE 41° 24' 28.48" N  
 LONGITUDE 81° 38' 13.19" W



UNIT	LENGTH	START WIDTH	END WIDTH	AREA	512
					FT
REAR APPROACH	30.0	79.2	79.2	2376	264
UNIT 1	1055.8	79.2	79.2	83602	9289
UNIT 2	1200.0	79.2	79.2	95025	10558
UNIT 3	1223.0	79.2	79.2	96846	10761
UNIT 4	678.8	79.2	79.2	53749	5972
FWD APPROACH	30.0	79.2	79.2	2376	264
TOTAL CARRIED TO GENERAL SUMMARY					37,109
CALCULATED: TSR 11/6/2024					
CHECKED: NDG 12/31/2024					

## NOTES

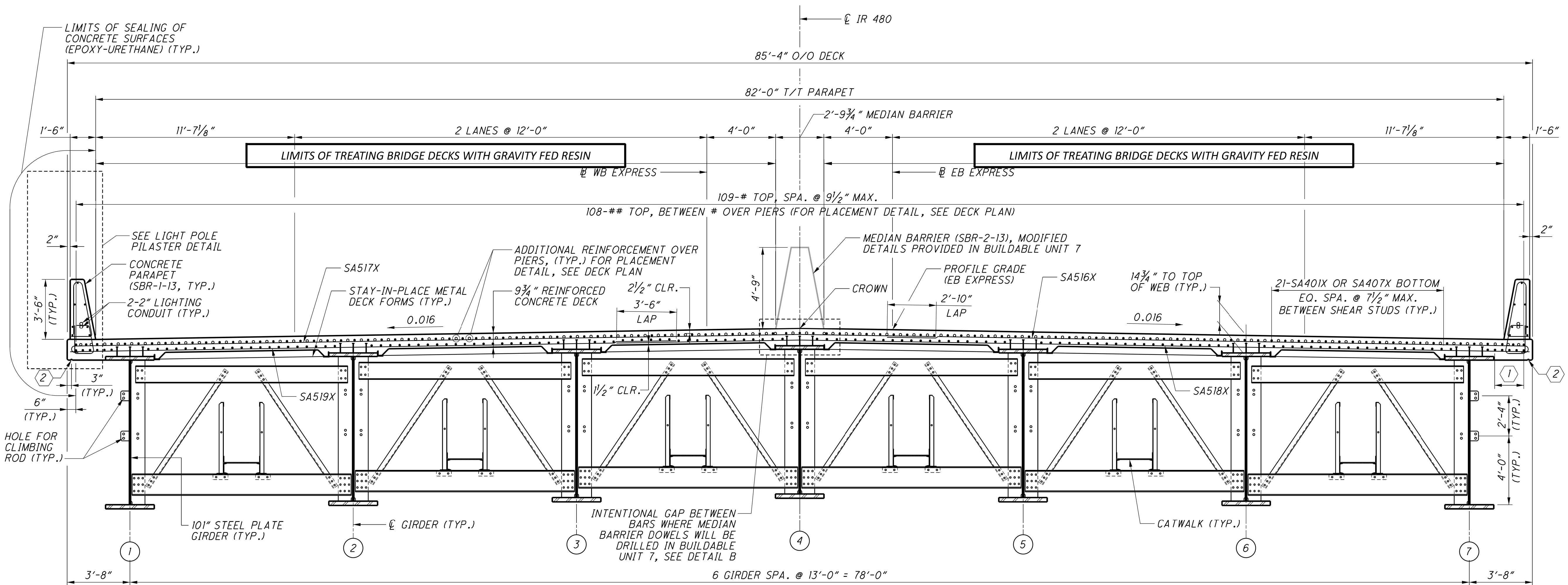
- DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.
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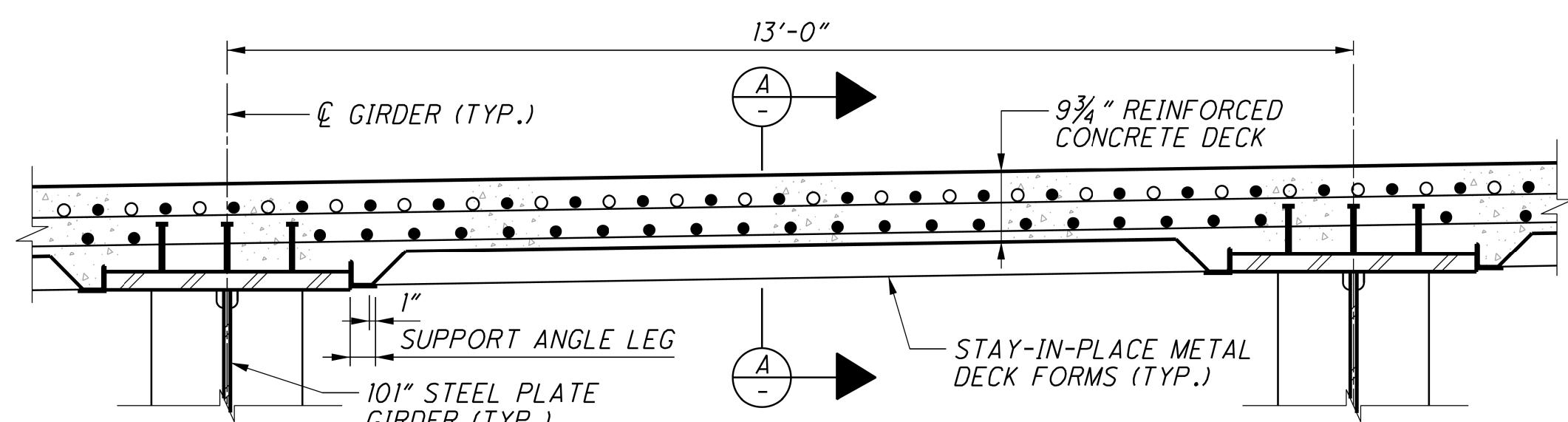
DESIGNER TSR	CHECKER RHC
REVIEWER PAT 08/01/25	
PROJECT ID 120625	
SUBSET 3	TOTAL 10
SHEET P.20	TOTAL 27

TRANSVERSE SECTION  
BRIDGE NO. CUY-480-18.723 C  
OVER CUYAHOGA RIVER VALLEY

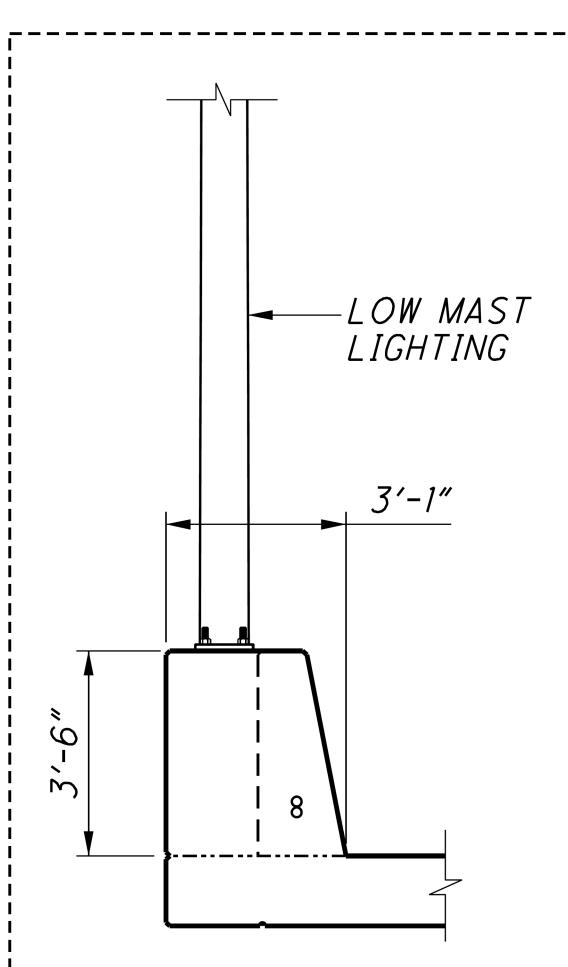
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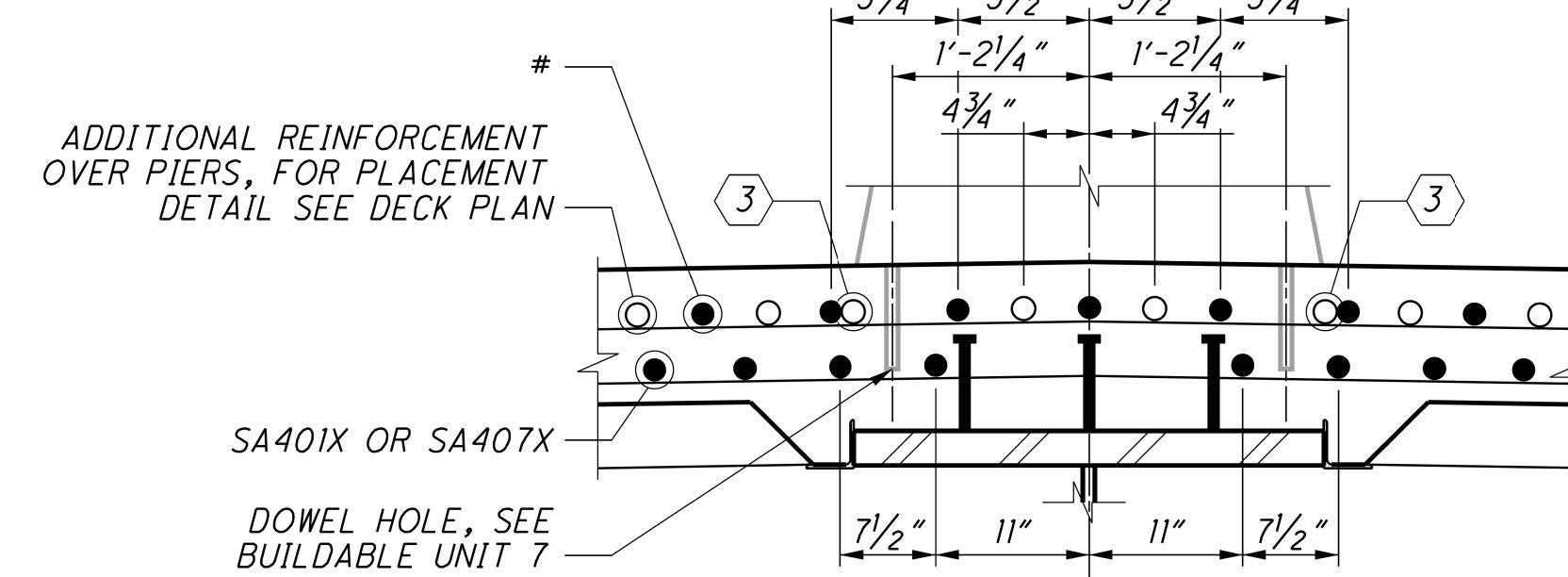
## TRANSVERSE SECTION - UNIT 1



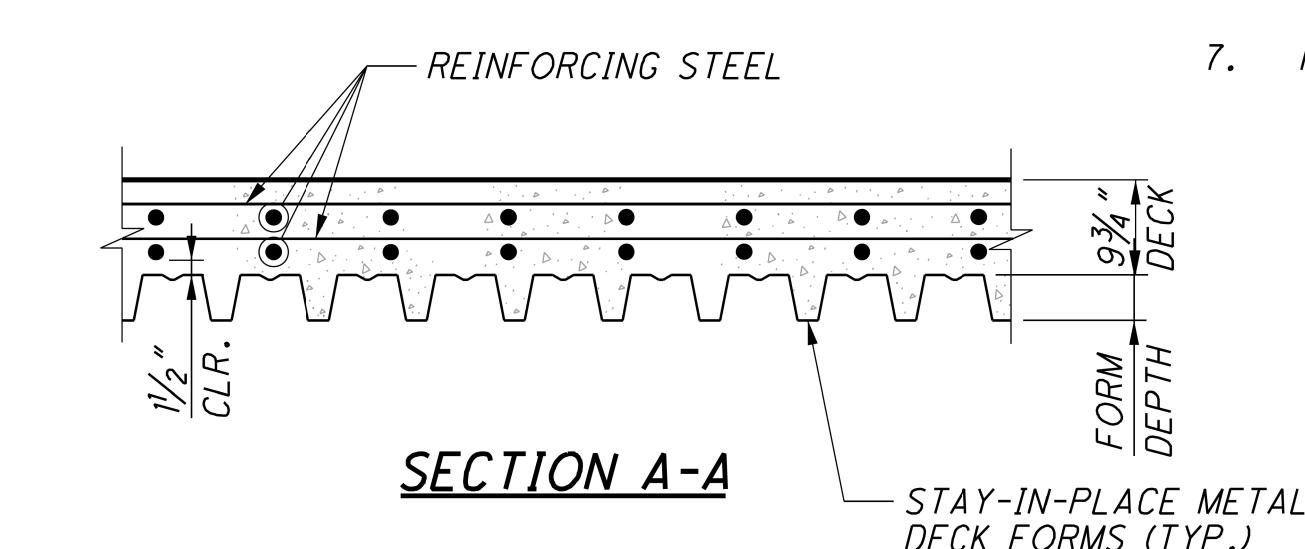
## *SECTION THRU STAY-IN-PLACE METAL DECK FORM*



PARTIAL SECTION AT  
LIGHT POLE PILASTER



## DETAIL B



## *SECTION A-A*

## NOTES:

1. **DECK SLAB CONCRETE QUANTITY:** THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH GIRDER HAUNCH, PLUS 1 INCH OF ADDITIONAL DECK THICKNESS TO ACCOUNT FOR CONCRETE IN THE FLUTES OF THE STAY-IN-PLACE METAL DECK FORMS. THE ESTIMATE ASSUMES A CONSTANT HAUNCH THICKNESS OF 5 INCHES, AS MEASURED FROM TOP OF GIRDER WEB TO UNDERSIDE OF THE  $9\frac{3}{4}$ " DECK. DEVIATE FROM THIS HAUNCH THICKNESS AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.
2. THE HAUNCH THICKNESS WAS MEASURED AT THE CENTERLINE OF THE GIRDER, FROM THE SURFACE OF THE DECK TO THE BOTTOM OF THE TOP FLANGE MINUS THE DECK SLAB THICKNESS. THE AREA OF ALL EMBEDDED STEEL PLATES HAS BEEN DEDUCTED FROM THE HAUNCH QUANTITY IN ACCORDANCE WITH 511.23.
3. FOR PARAPET ELEVATIONS AND DETAILS, SEE SHEET 29B1-9.
4. FOR DECK PLAN, SEE SHEET 29B1-4.
5. FOR SCREED ELEVATIONS, TOP OF HAUNCH ELEVATIONS, AND FINAL DECK SURFACE ELEVATIONS, SEE SHEETS 29B1-7 AND 29B1-8.
6. FOR MODULAR EXPANSION JOINT DETAILS, SEE SHEETS 29B1-18 AND 29B1-20.
7. FOR REINFORCEMENT STEEL LIST SEE SHEET 29B1-13

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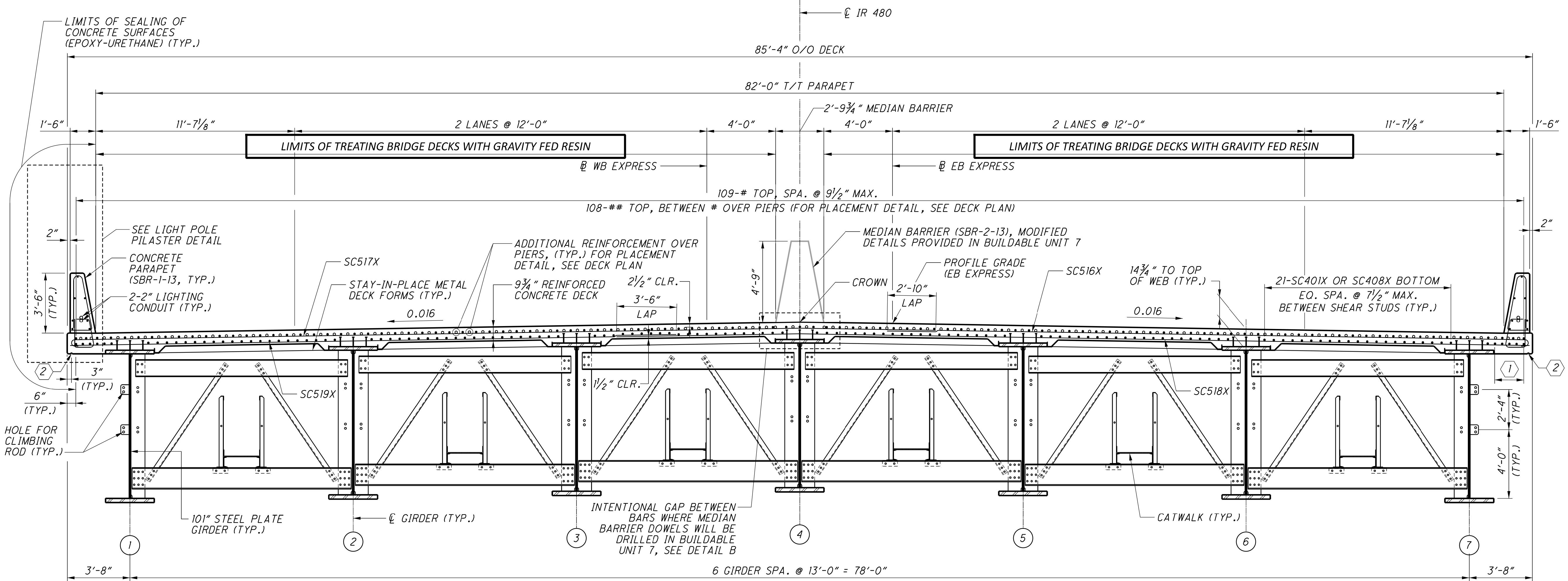
## NOTES

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- 2. PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT, ESTIMATED QUANTITIES AND/OR AS DESCRIBED IN THE GENERAL NOTES.**

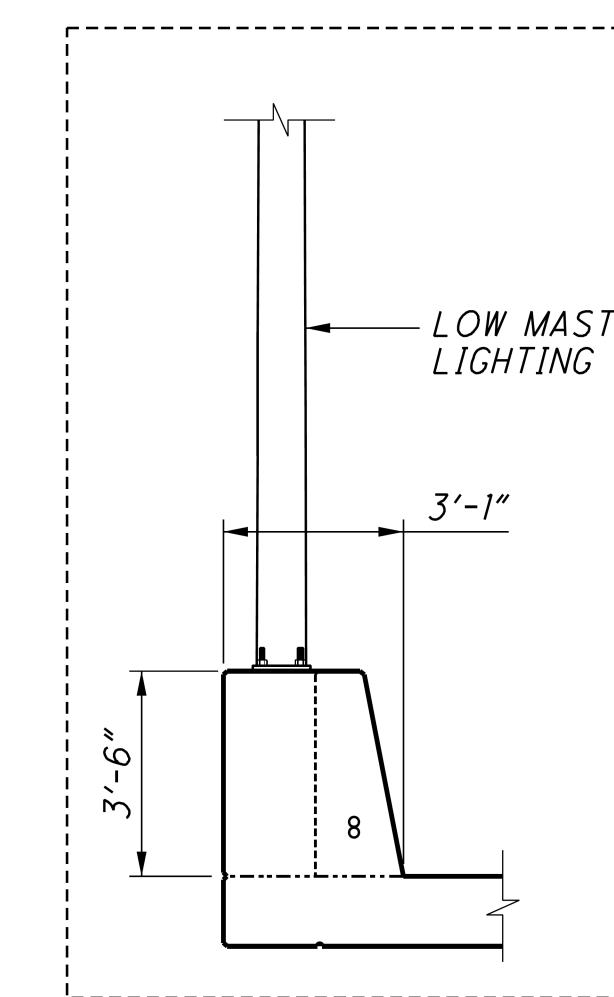
SFN  
**1812522**  
DESIGN AGENCY

DESIGNER	CHECKER
TSR	RHC
REVIEWER	
PAT	08/01/25
PROJECT ID	
120625	
SUBSET	TOTAL
4	10
SHEET	TOTAL
P.21	27

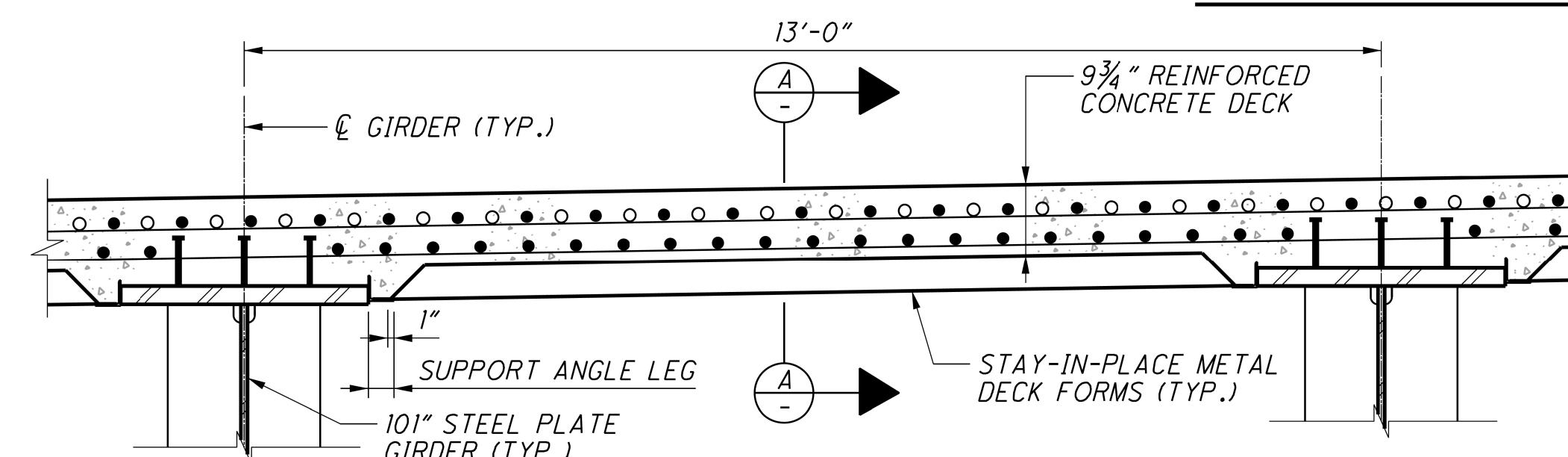




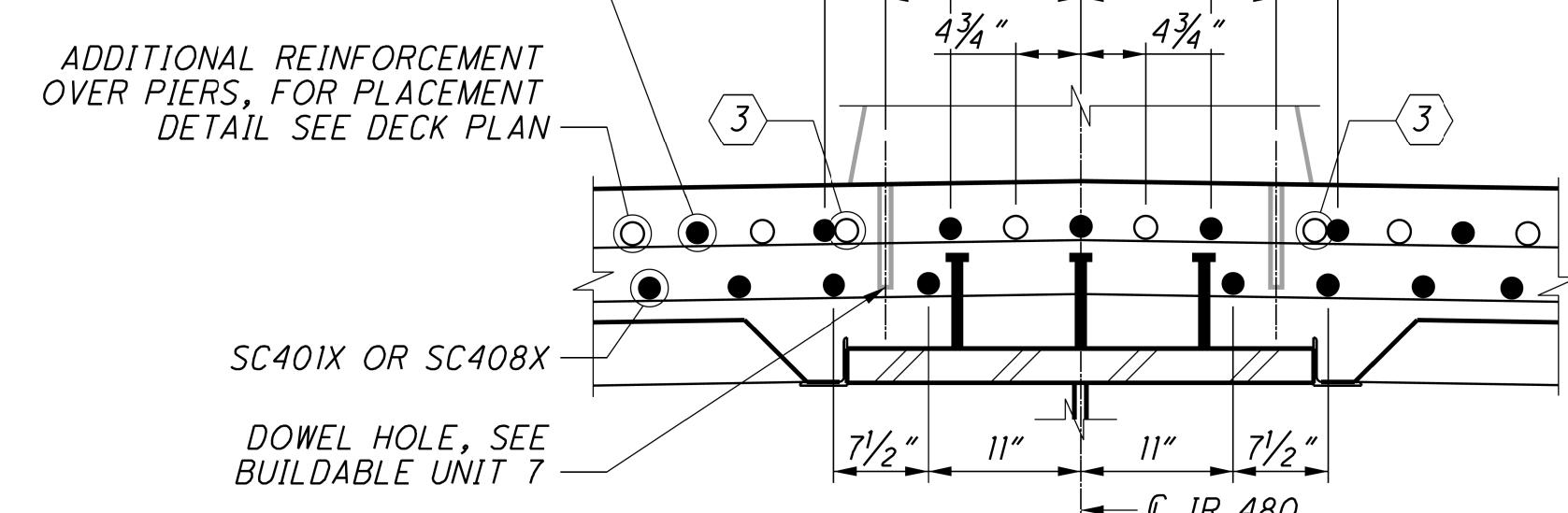
TRANSVERSE SECTION - UNIT 3



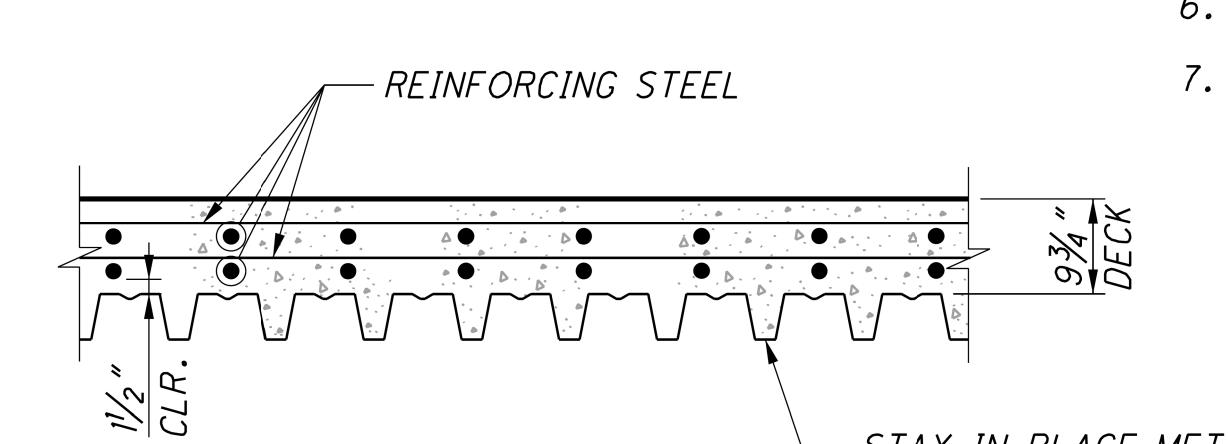
PARTIAL SECTION AT LIGHT POLE PILASTER



SECTION THRU STAY-IN-PLACE METAL DECK FORM



DETAIL B



SECTION A-A

## LEGEND:

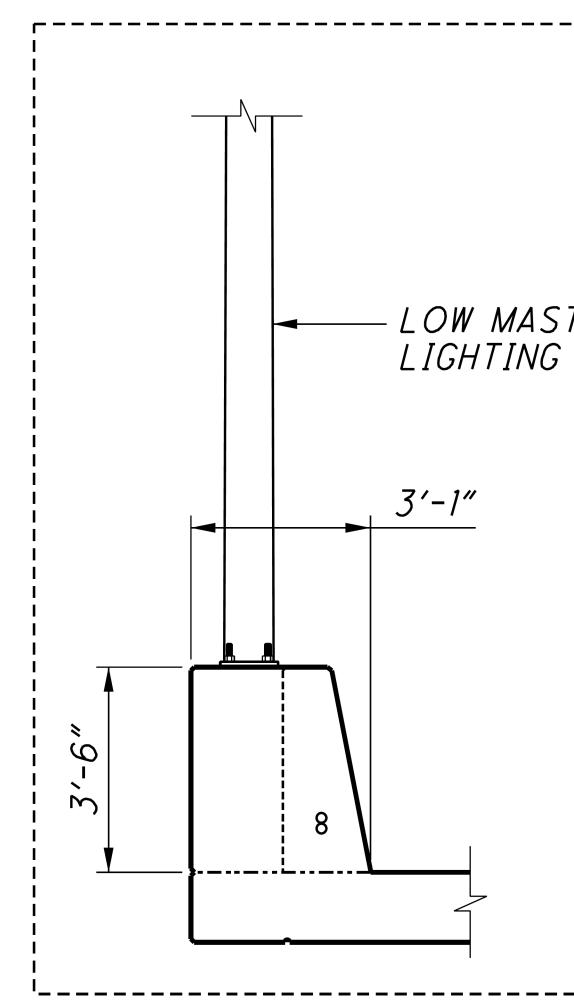
## NOTES:

- ① GIRDER DESIGNATION
- ② 6-SC401X OR SC408X @ 7 1/2" MAX. (TYP. EA. SIDE)
- ③ 1" DIA. HALF ROUND DRIP GROOVE
- ④ ADDITIONAL REINFORCEMENT OVER PIER BAR INDICATED SHALL BE PLACED ADJACENT TO LONGITUDINAL TOP BAR AS SHOWN
- # SC404X, SC503X, SC501X, SC507X, SC401X, SC414X, SC402X, SC502X, SC403X, SC509X OR SC405X
- ## SC501X, SC504X, SC505X, SC508X OR SC513X

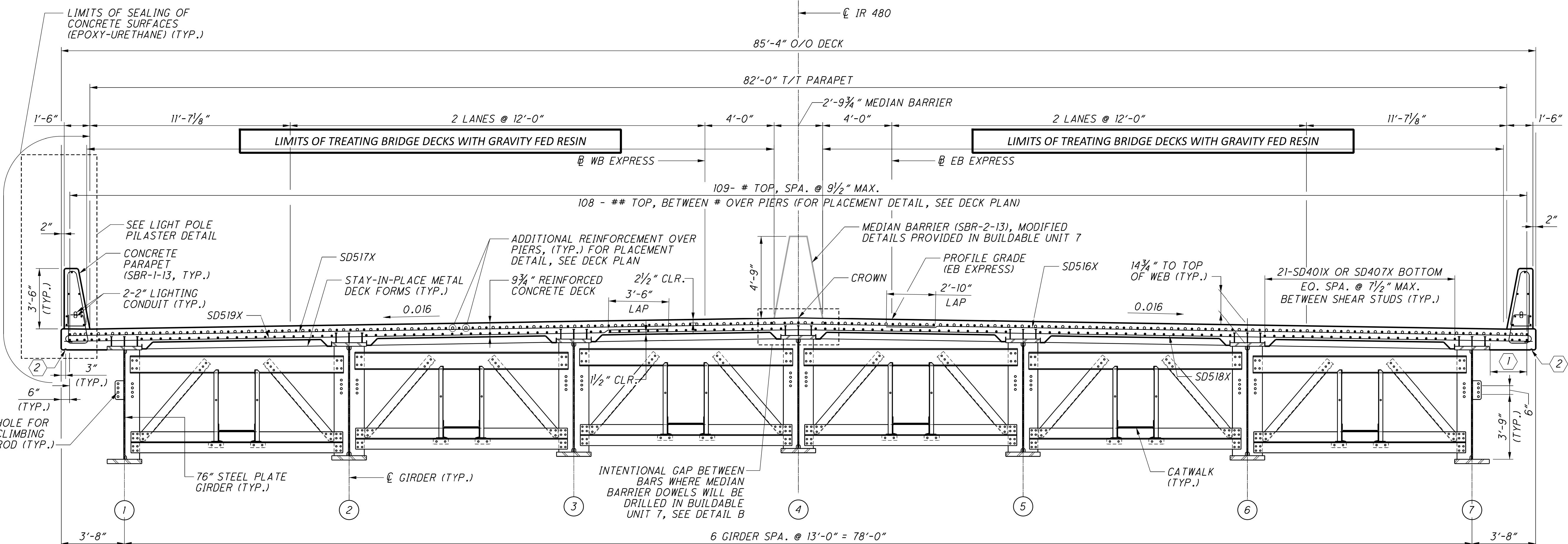
1. DECK SLAB CONCRETE QUANTITY: THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH GIRDER HAUNCH, PLUS 1 INCH OF ADDITIONAL DECK THICKNESS TO ACCOUNT FOR CONCRETE IN THE FLUTES OF THE STAY-IN-PLACE METAL DECK FORMS. THE ESTIMATE ASSUMES A CONSTANT HAUNCH THICKNESS OF 5 INCHES, AS MEASURED FROM TOP OF GIRDER WEB TO UNDERSIDE OF THE 9 3/4" DECK. DEVIATE FROM THIS HAUNCH THICKNESS AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.
2. THE HAUNCH THICKNESS WAS MEASURED AT THE CENTERLINE OF THE GIRDER, FROM THE SURFACE OF THE DECK TO THE BOTTOM OF THE TOP FLANGE MINUS THE DECK SLAB THICKNESS. THE AREA OF ALL EMBEDDED STEEL PLATES HAS BEEN DEDUCTED FROM THE HAUNCH QUANTITY IN ACCORDANCE WITH 511.23.
3. FOR PARAPET ELEVATIONS AND DETAILS, SEE SHEET 12B1-13.
4. FOR DECK PLAN, SEE SHEET 12B1-6.
5. FOR SCREED ELEVATIONS, TOP OF HAUNCH ELEVATIONS, AND FINAL DECK SURFACE ELEVATIONS, SEE SHEETS 12B1-9 AND 12B1-10.
6. FOR MODULAR EXPANSION JOINT DETAILS, SEE SHEETS 12B1-24 AND 12B1-25.
7. FOR REINFORCEMENT STEEL LIST, SEE SHEET 12B1-17.

## NOTES

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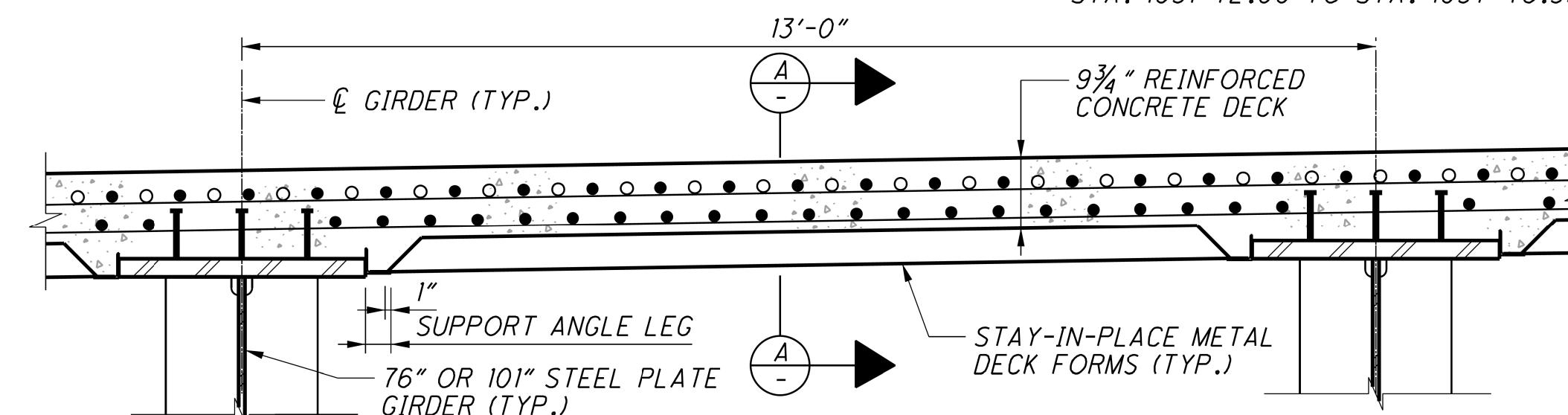


PARTIAL SECTION AT  
LIGHT POLE PILASTER

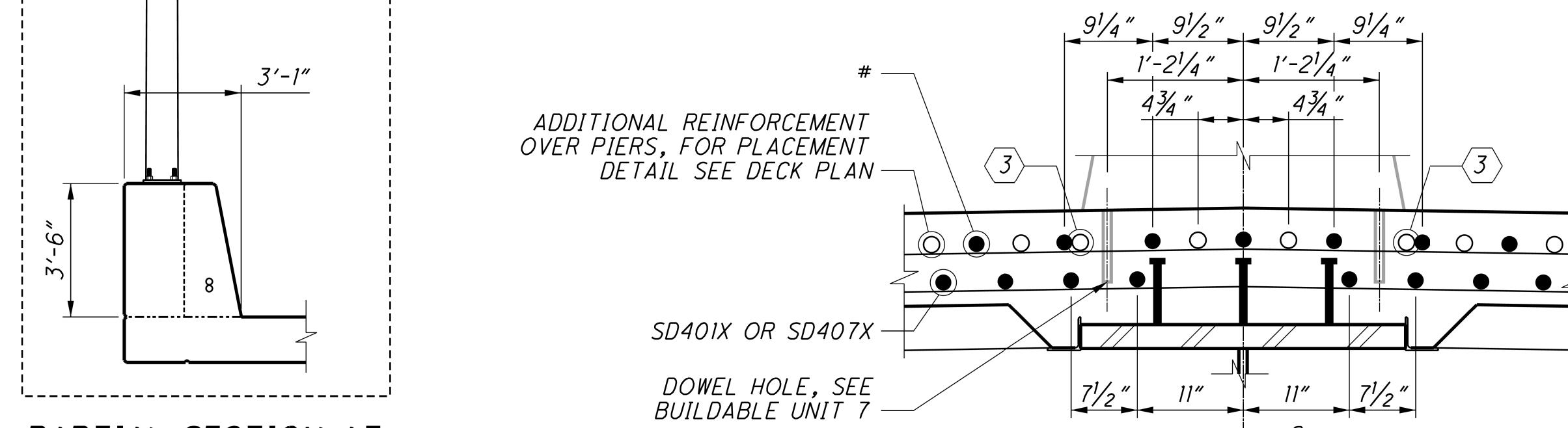


TRANSVERSE SECTION - UNIT 4

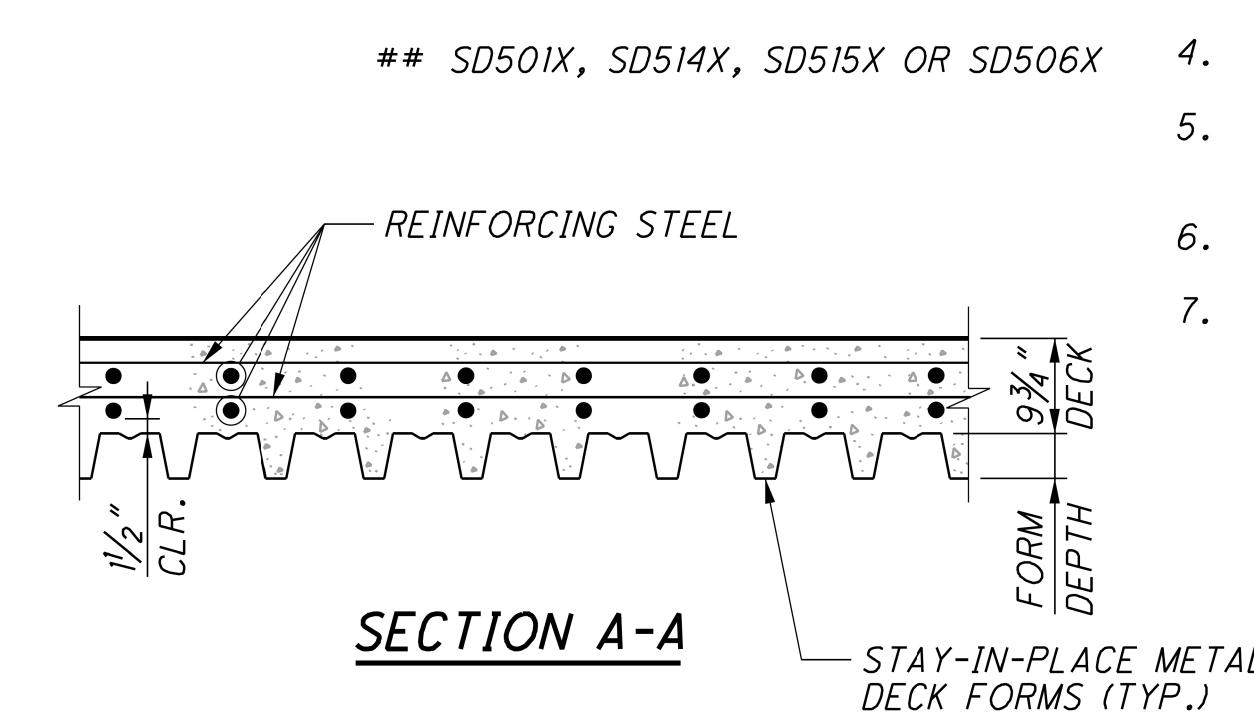
76" STEEL PLATE GIRDER SHOWN, 101" STEEL PLATE GIRDER SIMILAR  
STA. 1031+00.00 TO STA. 1031+49.00 (101" STEEL PLATE GIRDER)  
STA. 1031+49.00 TO STA. 1031+72.00 (STEEL PLATE GIRDER TRANSITION)  
STA. 1031+72.00 TO STA. 1037+76.33 (76" STEEL PLATE GIRDER)



SECTION THRU STAY-IN-PLACE METAL DECK FORM



DETAIL B



SECTION A-A

LEGEND:

- ① GIRDER DESIGNATION
- ② 6-SD401X OR SD407X @ 7 1/2" MAX. (TYP. EA. SIDE)
- ③ 1" DIA. HALF ROUND DRIP GROOVE
- ④ ADDITIONAL REINFORCEMENT OVER PIER BAR INDICATED SHALL BE PLACED ADJACENT TO LONGITUDINAL TOP BAR AS SHOWN
- # SD409X, SD501X, SD510X, SD401X, SD410X, SD511X, SD411X, SD512X OR SD412X
- ## SD501X, SD514X, SD515X OR SD506X

NOTES:

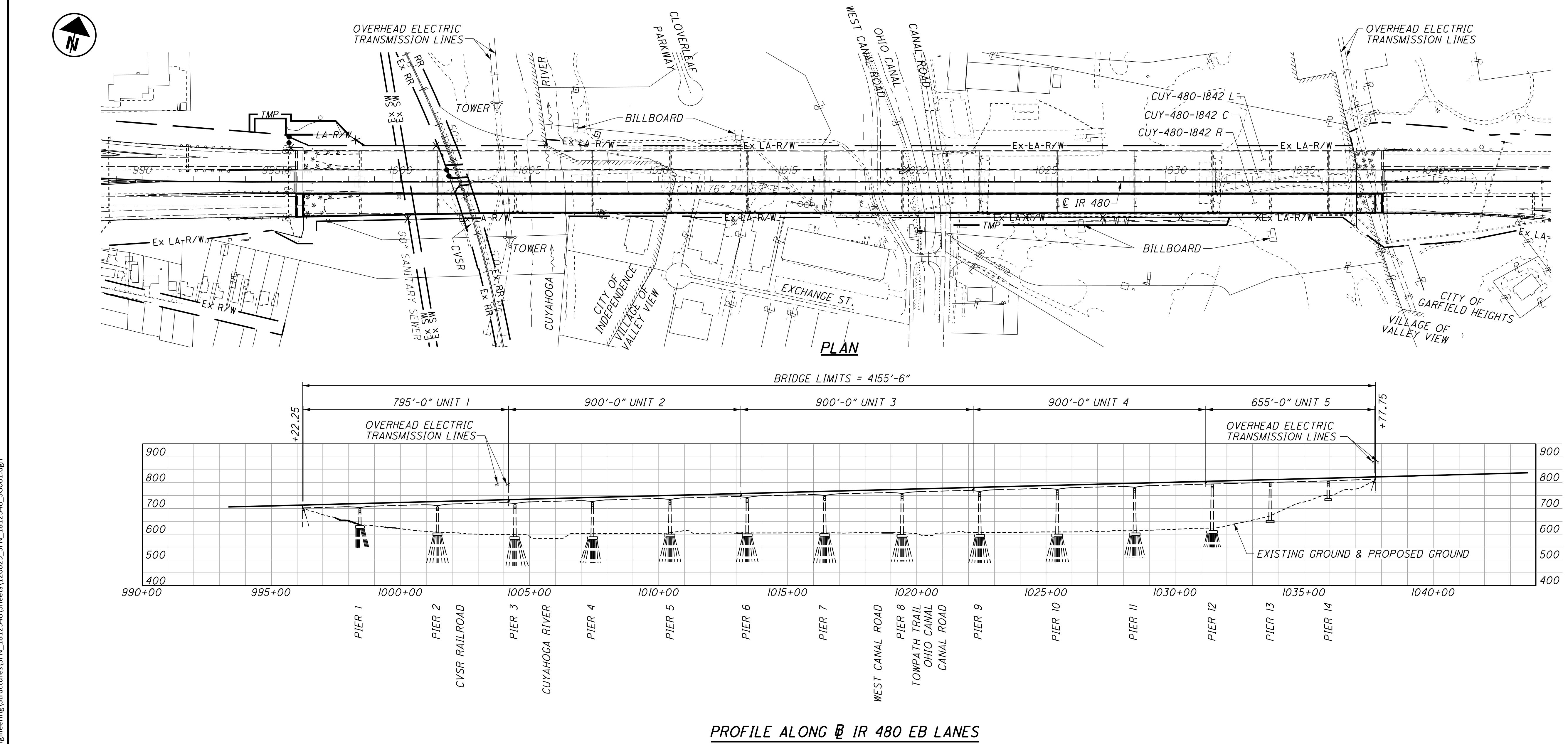
1. DECK SLAB CONCRETE QUANTITY: THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH GIRDER HAUNCH, PLUS 1 INCH OF ADDITIONAL DECK THICKNESS TO ACCOUNT FOR CONCRETE IN THE FLUTES OF THE STAY-IN-PLACE METAL DECK FORMS. THE ESTIMATE ASSUMES A CONSTANT HAUNCH THICKNESS OF 5 INCHES, AS MEASURED FROM TOP OF GIRDER WEB TO UNDERSIDE OF THE 9 3/4" DECK. DEVIATE FROM THIS HAUNCH THICKNESS AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.
2. THE HAUNCH THICKNESS WAS MEASURED AT THE CENTERLINE OF THE GIRDER, FROM THE SURFACE OF THE DECK TO THE BOTTOM OF THE TOP FLANGE MINUS THE DECK SLAB THICKNESS. THE AREA OF ALL EMBEDDED STEEL PLATES HAS BEEN DEDUCTED FROM THE HAUNCH QUANTITY IN ACCORDANCE WITH 511.23.
3. FOR PARAPET ELEVATIONS AND DETAILS, SEE SHEET 12B1-14.
4. FOR DECK PLAN, SEE SHEET 12B1-7.
5. FOR SCREED ELEVATIONS, TOP OF HAUNCH ELEVATIONS, AND FINAL DECK SURFACE ELEVATIONS, SEE SHEETS 12B1-11 AND 12B1-12.
6. FOR MODULAR EXPANSION JOINT DETAILS, SEE SHEETS 12B1-24 THRU 12B1-27.
7. FOR REINFORCEMENT STEEL LIST, SEE SHEET 12B1-18.

NOTES

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2. PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT, ESTIMATED QUANTITIES AND/OR AS DESCRIBED IN THE GENERAL NOTES.

# CUY-480-18.42 DECK SEALING

MODEL: Sheet\_PAPERSIZE: 34x22 (in.) DATE: 1/8/2026 TIME: 10:25:22 AM PNTDRV: OHDOT\_PDF.pdf CFG: PENTBL: OHDOT\_PDF.pdf USER: Thomas.Ritter@dot.ohio.gov WORKSPACE: OHDOTCE02 WORKSET: OHDOTCE02 WORKSET: OpenRoadsDesigner 24.00.00.dgn



## PROPOSED STRUCTURE CUY-480-1842 R

**TYPE:** NEW COMPOSITE REINFORCED CONCRETE DECK ON EXISTING STEEL SUPERSTRUCTURE.  
**SPANS:** SAME AS EXISTING  
**ROADWAY:** UNIT 1 - VARIES 72'-0" TO 87'-6" T/T PARAPETS  
 UNITS 2, 3, 4 AND 5 - 72'-0" T/T PARAPETS  
**LOADING:** HS20-44, ALTERNATE MILITARY LOADING AND 15 PSF FUTURE WEARING SURFACE  
**SKEW:** SAME AS EXISTING  
**WEARING SURFACE:** 1" MONOLITHIC CONCRETE  
**APPROACH SLABS:** AS-1-15 (25'-0" LONG) & AS-2-15  
**ALIGNMENT:** TANGENT  
**CROWN:** 0.0156  
**COORDINATES:** EASTBOUND BRIDGE  
 LATITUDE 41° 24' 27.69" N  
 LONGITUDE 81° 38' 12.95" W

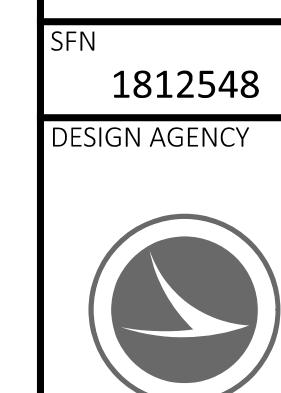
## EXISTING STRUCTURE CUY-480-1842 R/L

**TYPE:** CONTINUOUS WELDED STEEL GIRDERS WITH FLOOR SYSTEM AND REINFORCED CONCRETE DECK AND SUBSTRUCTURE.  
**SPANS:** UNIT 1 - 220'±, 300'±, AND 275'±  
 UNIT 2, 3 AND 4 - 25' CANTILEVER, 2 @ 300'± AND 275'±  
 UNIT 5 - 25' CANTILEVER, 2 @ 225'± AND 180'±  
**ROADWAY:** UNIT 1 L - VARIES 69'-6"± TO 69'-10"± FACE TO FACE OF PARAPETS  
 UNIT 1 R - VARIES 69'-6"± TO 85'-0"± FACE TO FACE OF PARAPETS  
 UNITS 2, 3, 4 AND 5 - 69'-6"± FACE TO FACE OF PARAPETS  
**LOADING:** HS 20-44 AND ALTERNATE MILITARY LOADING  
**SKEW:** NONE  
**WEARING SURFACE:** 2½" SUPERPLASTICIZED DENSED CONCRETE (1990)  
**APPROACH SLABS:** AS-1-67 (25' LONG)  
**ALIGNMENT:** TANGENT  
**CROWN:** 0.0156±  
**DATE BUILT:** 1975  
**DISPOSITION:** NEW DECK (RIGHT BRIDGE)

UNIT	LENGTH	START WIDTH	END WIDTH	AREA	512				
					FT	FT	FT	SF	SY
REAR APPROACH START	25.0	87.5	87.5	2188	243				
UNIT 1	797.8	87.5	72.0	63621	7069				
UNIT 2	900.0	72.0	72.0	64800	7200				
UNIT 3	900.0	72.0	72.0	64800	7200				
UNIT 4	900.0	72.0	72.0	64800	7200				
UNIT 5	657.8	72.0	72.0	47358	5262				
FWD APPROACH END	25.0	72.0	72.0	1800	200				
TOTAL CARRIED TO GENERAL SUMMARY					34,375				
CALCULATED:					TSR	11/6/2024			
CHECKED:					NDG	12/31/2024			

## NOTES

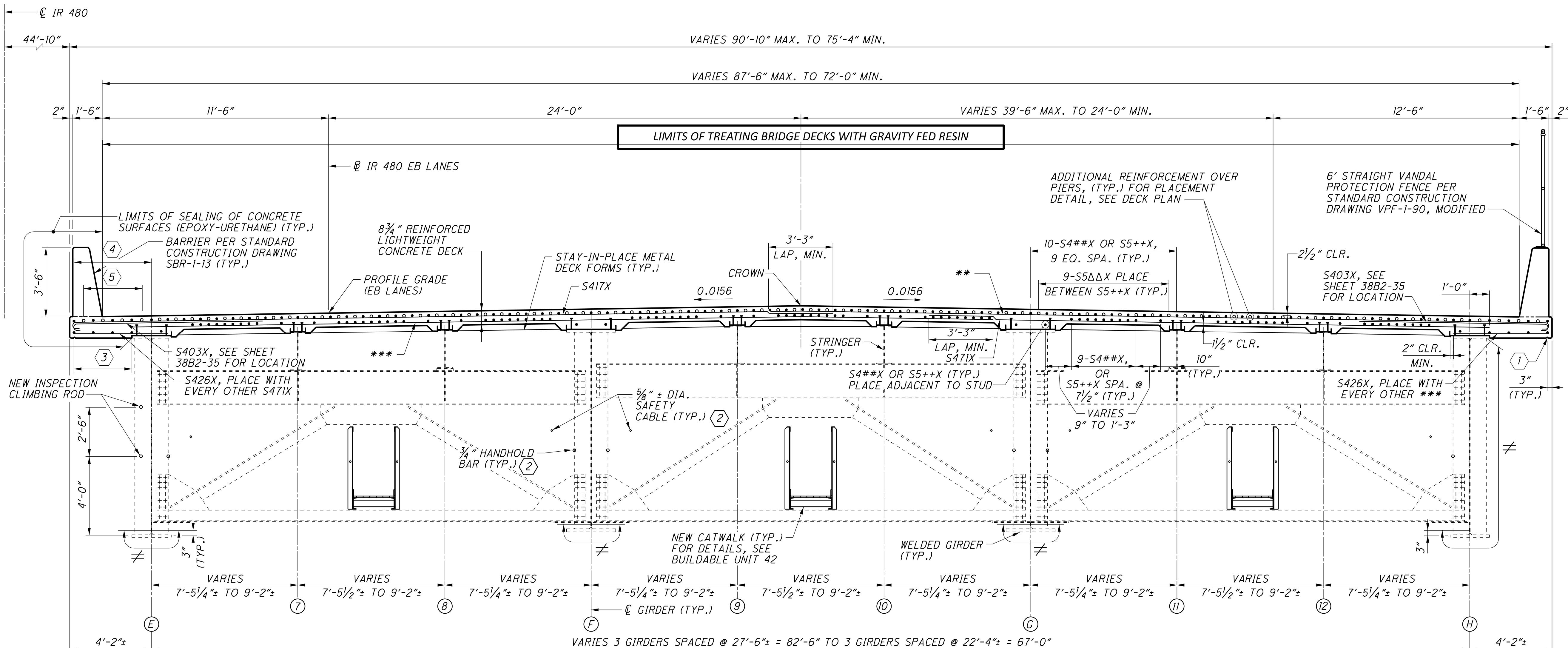
- DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.
- PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT, ESTIMATED QUANTITIES AND/OR AS DESCRIBED IN THE GENERAL NOTES.



DESIGNER	CHECKER
TSR	RHC
REVIEWER	
PAT	08/01/25
PROJECT ID	120625
SUBSET	TOTAL
8	10
SHEET	TOTAL
P.25	27

TRANSVERSE SECTION  
BRIDGE NO. CUY-480-18.723 R  
OVER CUYAHOGA RIVER VALLEY

MODEL: Sheet PAPER SIZE: 34x22 (in.) DATE: 1/8/2026 TIME: 10:26:16 AM PLTDRV: OHDOT\_PDF.pltcfg PENTBL: OHDOT\_Pen.tbl USER: Thomas.Ritter@dot.ohio.gov WORKSPACE: OHDOTCEv02 WORKSET: 120625 PRODUCT: OpenRoadsDesigner 24.00.00.205



## LEGEND

≠ INDICATES LIMITS FOR COMPLETE ABRASIVE BLAST  
CLEANING & PAINTING FOR THE FULL LENGTH  
OF THE BRIDGE.

# STRINGER DESIGNATION

L GIRDER DESIGNATION

1 1" DIA. HALF ROUND DRIP GROOVE, (TYP.)

2 SEE CONTRACT DOCUMENT ATTACHMENT ST-01 PLANS  
FOR REPAIR DETAILS

3 4 - S4##X OR S5++X, SPA. @ 12" MAX. (TYP.)

4 5 - S4##X OR S5++X, SPA. @ 12" MAX. (TYP.)

5 4 - S5ΔΔX, PLACE BETWEEN S5++X (TYP.)

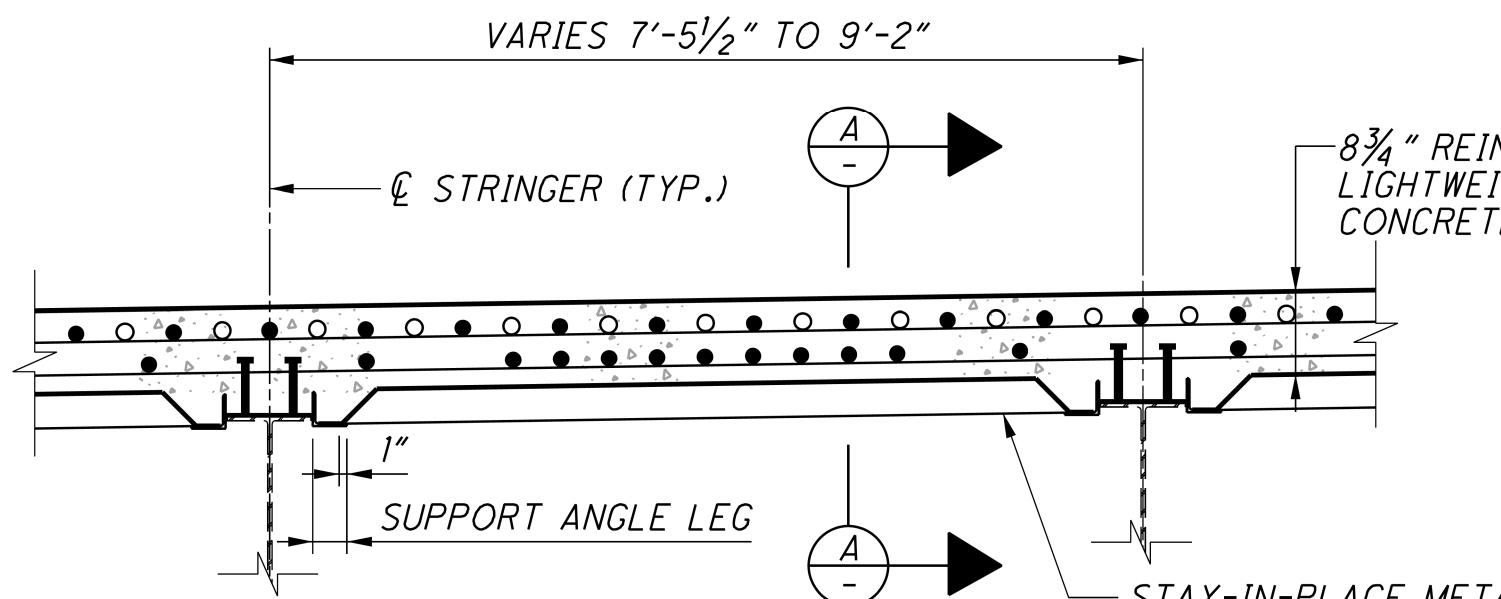
## S401X, S402X, S404X, S415X

++ S501X, S502X, S503X

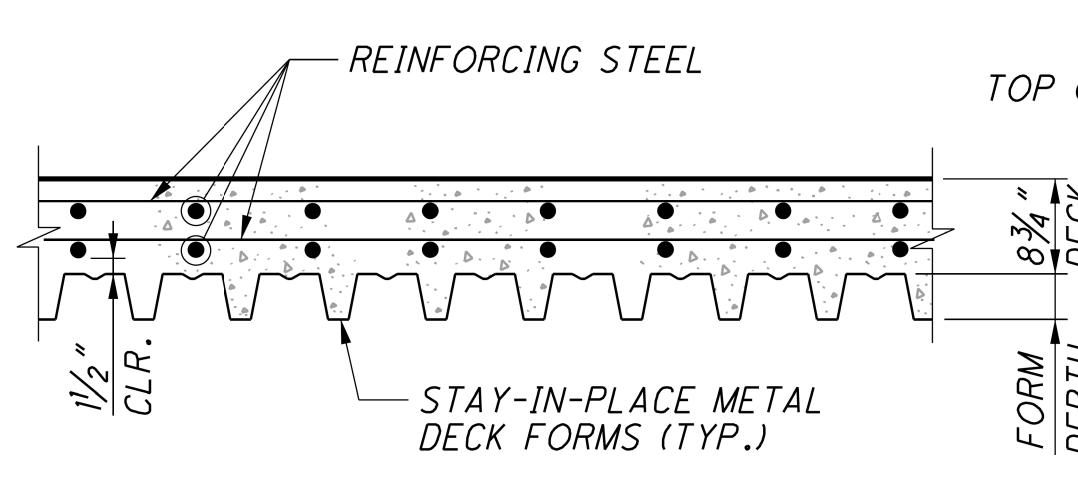
ΔΔ S501X, S513X, S514X

\*\* S417X, S439X, S440X, S441X, S442X, S443X, S444X, S445X, S446X, S447X, S448X, S449X, S450X, S451X, S452X, S453X

\*\*\* S454X, S455X, S456X, S457X, S458X, S459X, S460X, S461X, S462X, S463X, S464X, S465X, S466X, S467X, S468X, S469X



# SECTION THRU STAY-IN-PLACE METAL DECK FORM

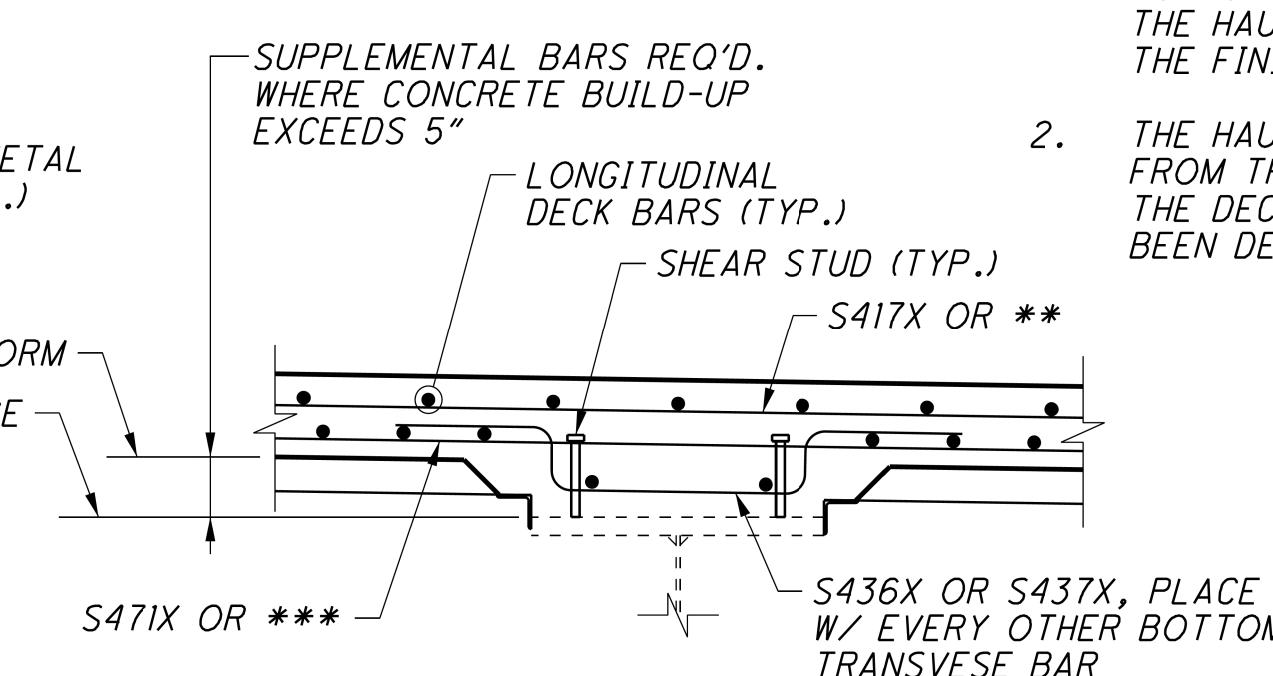


## SECTION A-

### TRANSVERSE SECTION - UNIT 4

## NOTES

1. **DECK SLAB CONCRETE QUANTITY:** THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH GIRDER OR STRINGER HAUNCH, PLUS 1 INCH OF ADDITIONAL DECK THICKNESS TO ACCOUNT FOR CONCRETE IN THE FLUTES OF THE STAY-IN-PLACE METAL DECK FORMS. THE ESTIMATE ASSUMES A CONSTANT GIRDER HAUNCH THICKNESS OF 7" AS MEASURED FROM TOP OF GIRDER WEB TO UNDERSIDE OF THE  $8\frac{3}{4}$ " DECK AND A CONSTANT STRINGER HAUNCH THICKNESS OF  $4\frac{1}{2}$ " AS MEASURED FROM TOP OF TOP FLANGE TO UNDERSIDE OF THE  $8\frac{3}{4}$ " DECK, DEVIATE FROM THE HAUNCH THICKNESSES AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.
2. THE HAUNCH THICKNESS WAS MEASURED AT THE CENTERLINE OF THE GIRDER, FROM THE SURFACE OF THE DECK TO THE BOTTOM OF THE TOP FLANGE MINUS THE DECK SLAB THICKNESS. THE AREA OF ALL EMBEDDED STEEL PLATES HAS BEEN DEDUCTED FROM THE HAUNCH QUANTITY IN ACCORDANCE WITH 511.23.



## *SUPPLEMENTAL HAUNCH REINFORCEMENT DETAIL*

FOR GIRDERS F & G WHERE CONCRETE BUILD-UP >5" ONLY. ESTIMATED LOCATIONS SHOWN ON DECK PLAN SHEETS, TO BE REVISED AFTER MEASURING GIRDER REBOUND AND SETTING SCREED ELEVATIONS PER SHEET 38B2-42.

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## NOTES

**NOTES**

1. **DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.**
2. **PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT, ESTIMATED QUANTITIES AND/OR AS DESCRIBED IN THE GENERAL NOTES.**

SFN  
**1812548**  
DESIGN AGENCY

DESIGNER	CHECKER
TSR	RHC
REVIEWER	
PAT	08/01/25
PROJECT ID	
120625	
SUBSET	TOTAL
9	10
SHEET	TOTAL
P.26	27

TRANSVERSE SECTION  
BRIDGE NO. CUY-480-18.723 R  
OVER CUYAHOGA RIVER VALLEY

IR 480

44'-10"

75'-4" O/O DECK

72'-0" T/T PARAPET

2"

1'-6"

11'-6"

24'-0"

24'-0"

12'-6"

1'-6" 2"

LIMITS OF TREATING BRIDGE DECKS WITH GRAVITY FED RESIN

IR 480 EB LANES

LIMITS OF SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) (TYP.)

BARRIER PER STANDARD CONSTRUCTION DRAWING SBR-1-13 (TYP.)

S426X, PLACE WITH EVERY OTHER S419X

8½" REINFORCED LIGHTWEIGHT CONCRETE DECK

PROFILE GRADE (EB LANES)

STAY-IN-PLACE METAL DECK FORMS (TYP.)

S418X

CROWN

3'-3" LAP, MIN.

0.0156

0.0156

ADDITIONAL REINFORCEMENT OVER PIERS, (TYP.) FOR PLACEMENT DETAIL, SEE DECK PLAN

8-S4#\*X OR S5++X, 7 EQ. SPA. (TYP.)

7-S5ΔX PLACE BETWEEN S5++X (TYP.)

2½" CLR.

1'-0"

6' STRAIGHT VANDAL PROTECTION FENCE PER STANDARD CONSTRUCTION DRAWING VPF-1-90, MODIFIED

2½" CLR.

1½" CLR.

2" CLR. MIN.

3" (TYP.)

NEW INSPECTION CLIMBING ROD

2'-6"

4'-0"

3" (TYP.)

5/8" ± DIA. SAFETY CABLE (TYP.)

3/4" HANDHOLD BAR (TYP.)

1'-2" MAX.

S4#\*X OR S5++X (TYP.) PLACE ADJACENT TO STUD

S5++X SPA. @ 8¼" MAX (TYP.)

S426X, PLACE WITH EVERY OTHER S420X

3" (TYP.)

WELDED GIRDER (TYP.)

NEW CATWALK (TYP.) FOR DETAILS, SEE BUILDABLE UNIT 42

GIRDER (TYP.)

E

7'-5½" ±

7'-5½" ±

7'-5½" ±

7'-5½" ±

7'-5½" ±

7'-5½" ±

7'-5½" ±

7'-5½" ±

7'-5½" ±

7'-5½" ±

F

G

H

3 GIRDER SPACES @ 22'-4" ± = 67'-0" ±

4'-2" ±

4"-2" ±

## LEGEND

≠ INDICATES LIMITS FOR COMPLETE ABRASIVE BLAST CLEANING & PAINTING FOR THE FULL LENGTH OF THE BRIDGE.

# STRINGER DESIGNATION

L GIRDER DESIGNATION

1 1" DIA. HALF ROUND DRIP GROOVE, (TYP.)

2 SEE CONTRACT DOCUMENT ATTACHMENT ST-01 PLANS FOR REPAIR DETAILS

3 4 - S4##X OR S5++X, SPA. @ 12" MAX. (TYP.)

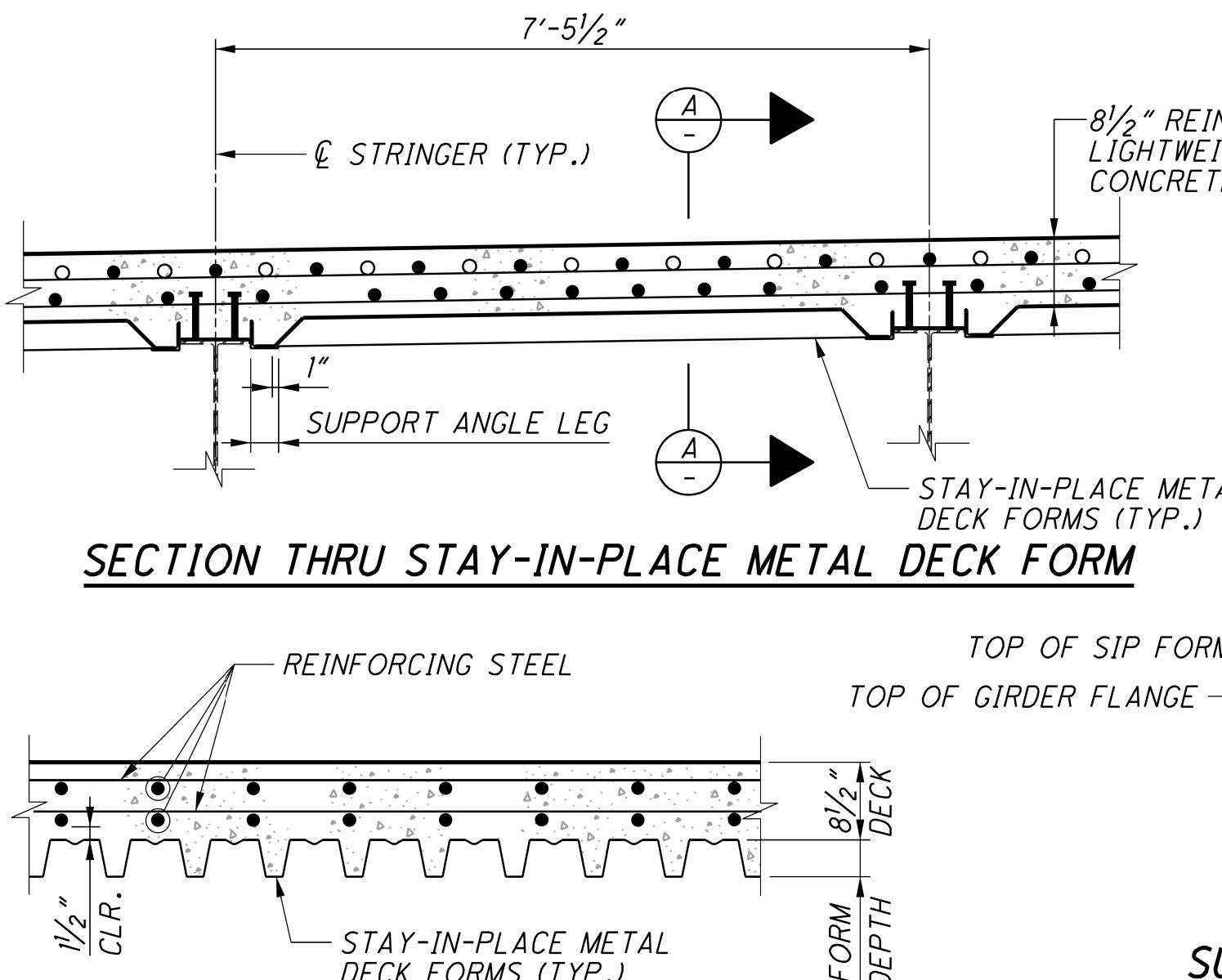
4 5 - S4##X OR S5++X, SPA. @ 12" MAX. (TYP.)

5 4 - S5ΔΔX, PLACE BETWEEN S5++X (TYP.)

## S401X, S405X, S406X, S407X, S408X, S409X, S410X, S411X, S412X, S413X, S414X, S416X, S438X

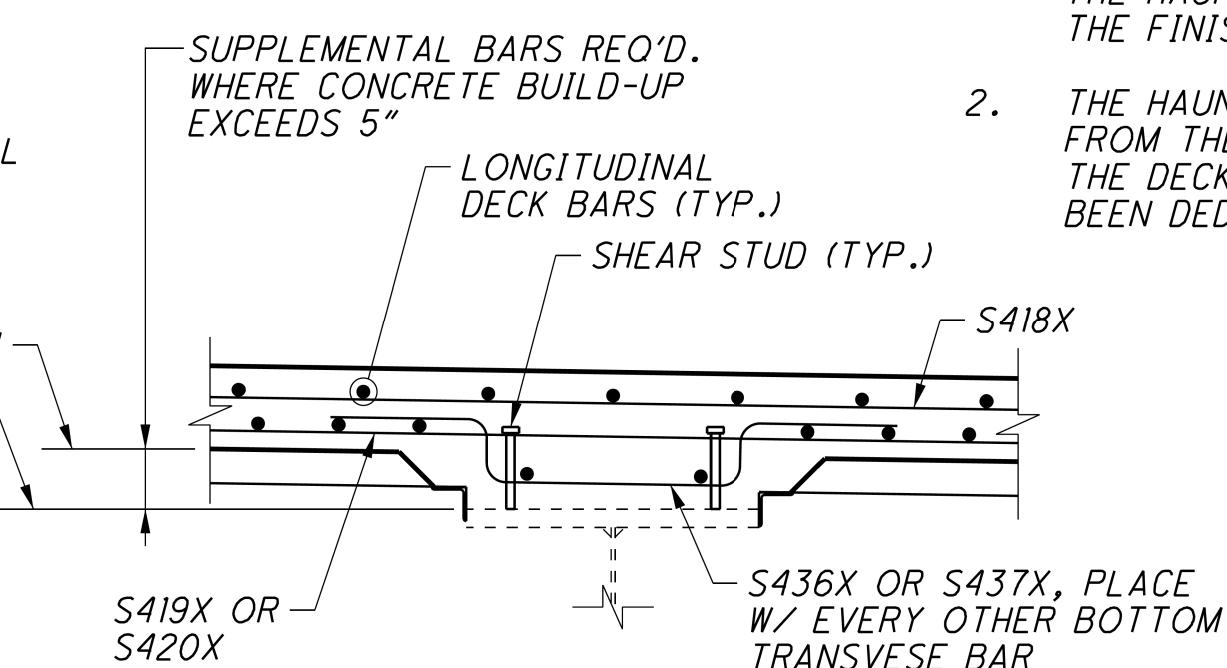
++ S501X, S504X, S505X, S506X, S507X, S508X, S509X, S510X, S511X, S512X, S526X

ΔΔ S501X, S515X, S516X, S517X, S518X, S519X, S520X, S521X, S522X, S524X, S525X, S527X, S528X



## *SECTION THRU STAY-IN-PLACE METAL DECK FORMS*

## SECTION A-A



## SUPPLEMENTAL HAUNCH REINFORCEMENT DETAIL

FOR GIRDERS F & G WHERE CONCRETE BUILD-UP >5" ONLY. ESTIMATED LOCATIONS SHOWN ON DECK PLAN SHEETS, TO BE REVISED AFTER MEASURING GIRDER REBOUND AND SETTING SCREED ELEVATIONS PER SHEET 38B2-42.

## NOTES

1. **DECK SLAB CONCRETE QUANTITY:** THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH GIRDER OR STRINGER HAUNCH, PLUS 1 INCH OF ADDITIONAL DECK THICKNESS TO ACCOUNT FOR CONCRETE IN THE FLUTES OF THE STAY-IN-PLACE METAL DECK FORMS. THE ESTIMATE ASSUMES A CONSTANT GIRDER HAUNCH THICKNESS OF 7" AS MEASURED FROM TOP OF GIRDER WEB TO UNDERSIDE OF THE  $8\frac{1}{2}$ " DECK AND A CONSTANT STRINGER HAUNCH THICKNESS OF  $4\frac{1}{2}$ " AS MEASURED FROM TOP OF TOP FLANGE TO UNDERSIDE OF THE  $8\frac{1}{2}$ " DECK, DEVIATE FROM THE HAUNCH THICKNESSES AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.
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SFN <b>1812548</b>	
DESIGN AGENCY	
	
DESIGNER <b>TSR</b>	CHECKER <b>RHC</b>
REVIEWER	
PAT	<b>08/01/25</b>
PROJECT ID	
<b>120625</b>	
SUBSET <b>10</b>	TOTAL <b>10</b>
SHEET <b>P.27</b>	TOTAL <b>27</b>