

CONCRETE COLLAR DETAIL

ALL WORK IDENTIFIED IN THIS DETAIL SHALL BE INCLUDED IN THE BID PRICE FOR ITEM 611 - MANHOLE ADJUSTED TO GRADE, AS PER PLAN

NOTE: CONCRETE COLLAR IS TO BE WITHIN 1/8 INCH OF MANHOLE LID AFTER ADJUSTMENT TO GRADE.



GENERAL:
THE CONTRACTOR SHALL SUBMIT IN WRITING A SCHEDULE OF OPERATIONS TO THE ENGINEER (SEE 108.02) AND RECEIVE APPROVAL IN WRITING BEFORE WORK IS STARTED ON THIS PROJECT. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

ALIGNMENT AND PROFILE:
THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED, AND THE PROFILE OF THE PROPOSED SURFACE WILL BE THE SAME AS EXISTING.

CONTRACTORS EQUIPMENT - OPERATION AND STORAGE:
THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL. EQUIPMENT SHALL HAVE AT LEAST ONE AMBER FLASHING LIGHT. WHEN PARKED ALONG THE HIGHWAY, THE EQUIPMENT SHALL BE LOCATED EITHER A MINIMUM OF THIRTY FEET FROM THE EDGE OF PAVEMENT OR SIX FEET BEHIND GUARDRAIL WITH A MINIMUM OF 125 FEET OF GUARDRAIL PRECEDING THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT AN APPROVED CONTRACTOR'S STORAGE AREA.

CONTINGENCY QUANTITIES:
THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

REMOVAL ITEMS:
ASPHALT AND MISCELLANEOUS ITEMS DESIGNATED FOR REMOVAL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF. PAYMENT FOR THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE REMOVED ITEM.

WORK LIMITS:
THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

CENTERLINE CONSTRUCTION / RIGHT OF WAY:
THE INTENT OF THIS PROJECT IS THAT ALL WORK IS TO BE COMPLETED WITHIN THE EXISTING RIGHT OF WAY. THE CENTERLINE SHOWN IN THIS PLAN IS TO BE CONSIDERED A CENTERLINE OF CONSTRUCTION ONLY AND NOT TO BE CONSTRUED AS THE ACTUAL GEOMETRIC ALIGNMENT OF THE ROADWAY. THIS CENTERLINE PROVIDED IS TO BE USED AS A REFERENCE OF PROJECT LENGTH ONLY AND SHALL NOT BE USED TO ESTABLISH PRECISE LOCATIONS OF ANY OTHER FEATURES SUCH AS/NOT LIMITED TO THE EXISTING RIGHTS OF WAY. ANY RIGHT OF WAY LOCATION SHOWN IN THE PLAN IS A GRAPHICAL REPRESENTATION (OF SAID RIGHT OF WAY) CONFIRMING THAT THE PLANNED WORK HAS BEEN DETERMINED TO BE IN ODOT RIGHT OF WAY. IN THE EVENT THAT ANY ACTIVITIES DEVIATE FROM THE PLAN, THE CONTRACTOR MAY BE REQUIRED, PER THE ENGINEER, TO VERIFY THE RIGHT OF WAY LIMITS IN THE FIELD. PAYMENT FOR ANY RIGHT OF WAY VERIFICATION WILL BE INCLUDED UNDER THE LUMP SUM BID ITEM 623 - CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN.

PROPOSED PAVEMENT MARKINGS:
IT IS THE INTENT OF THE PROPOSED PAVEMENT MARKINGS TO BE THE SAME AS EXISTING. ANY DEVIATION FROM EXISTING WILL BE IDENTIFIED WITHIN THIS PLAN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE AND SHAPE OF THESE EXISTING PAVEMENT MARKINGS BEFORE THE WORK OBLITERATES THEM. ANY PAVEMENT MARKING WHICH IS PLACED AT THE WRONG LOCATION SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

UTILITIES:
NO UTILITY IMPACT IS ANTICIPATED DUE TO THE SCOPE OF WORK. THE ODOT CONTRACTOR IS REQUIRED TO CONTACT OHIO811 A MINIMUM OF 48 HOURS EXCLUDING WEEKENDS AND HOLIDAYS TO PERMIT ALL UNDERGROUND UTILITIES AN OPPORTUNITY TO MARK THEIR LINES AND TO ENSURE ALL UTILITIES ARE MARKED PRIOR TO BEGINNING WORK. IT IS ALSO THE ODOT CONTRACTOR'S RESPONSIBILITY TO CONTACT ALL NON-MEMBERS OF OHIO811 DIRECTLY A MINIMUM OF 48 HOURS NOTICE EXCLUDING WEEKENDS AND HOLIDAYS PRIOR TO EXCAVATION OCCURRING AT ANY LOCATIONS TO PROVIDE THEM WITH THE SAME OPPORTUNITY.

IT IS ODOT'S EXPECTATION THAT THERE WILL BE NO DISRUPTION TO UNDERGROUND UTILITIES. IF THERE IS A UTILITY MARKING WITHIN THE TOLERANCE ZONE OF A UTILITY LOCATE FROM ANY PROPOSED WORK/EXCAVATION IT IS THE ODOT CONTRACTORS RESPONSIBILITY TO DIRECTLY CONTACT THE IMPACTED UTILITY AND WORK WITH THEM TO FIND A SOLUTION THAT DOES NOT CHANGE THE PROPOSED WORK OR DAMAGE THE EXISTING UTILITY. NO UTILITY RELOCATION WILL BE REIMBURSED NOR WILL DELAY CLAIMS BE PERMISSIBLE BASED ON LACK OF COORDINATION BETWEEN THE ODOT CONTRACTOR AND THE IMPACTED UTILITY.

ITEM 202 - GUARDRAIL REMOVED, AS PER PLAN:
ITEM 202 - ANCHOR ASSEMBLY REMOVED, TYPE A, AS PER PLAN:
ITEM 202 - ANCHOR ASSEMBLY REMOVED, TYPE E, AS PER PLAN:
ITEM 202 - BRIDGE TERMINAL ASSEMBLY REMOVED, AS PER PLAN:
IN ADDITION TO THE REQUIREMENTS OF ITEM 202, REMOVAL OF SPECIFIED GUARDRAIL ITEMS SHALL INCLUDE BUT NOT BE LIMITED TO ANY ATTACHED POSTS, SIGNS AND DELINEATORS (NOT OTHERWISE SPECIFIED). THIS REMOVAL WILL INCLUDE ALL POSTS, ANCHORS AND HARDWARE UNDER GROUND.

THE CONTRACTOR SHALL EXPECT TO REMOVE ALL CONCRETE FOUNDATIONS COMPLETELY AT ALL LOCATIONS UNLESS OTHERWISE INSTRUCTED OR APPROVED BY THE ENGINEER. REMOVING EXISTING CONCRETE FOUNDATION TO A MINIMUM OF 1 FOOT BELOW THE GRADE OF THE SURROUNDING AREA MAY ONLY BE PERMITTED IF THE EXISTING CONCRETE DOES NOT FALL WITHIN 6 FEET OF THE PROPOSED AS TO NOT COMPROMISE THE PERFORMANCE OF THE PROPOSED GUARDRAIL SYSTEM(S).

ALL HOLES AND VOIDS REMAINING AFTER REMOVAL OF GUARDRAIL POSTS AND FOUNDATIONS SHALL BE FILLED WITH GRANULAR MATERIAL CONFORMING TO CMS 203.02R. FILL MATERIAL CONTAINING SOD SHALL NOT BE USED. ALL FILL MATERIAL SHALL BE APPROVED BY THE ENGINEER. MATERIAL PLACED IN HOLES SHALL BE THOROUGHLY COMPACTED AND LEVELED OFF AS DIRECTED BY THE ENGINEER. PAYMENT FOR THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE APPLICABLE GUARDRAIL REMOVAL ITEM. GUARDRAIL DESIGNATED FOR REMOVAL SHALL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF.

NO HAZARD SHALL BE LEFT UNPROTECTED EXCEPT FOR THE ACTUAL TIME NECESSARY TO REMOVE THE EXISTING GUARDRAIL, PREPARE THE SITE, AND INSTALL NEW GUARDRAIL IN A CONTINUOUS OPERATION.

ITEM 203 - EMBANKMENT, AS PER PLAN:
QUANTITY FOR ITEM 203 - EMBANKMENT, AS PER PLAN HAS BEEN PROVIDED THROUGHOUT THIS PLAN TO BUILD UP FORE-SLOPES AND ENSURE PROPER GRADING FOR THE PROPOSED GUARDRAIL AND ANCHOR ASSEMBLIES. THIS ITEM OF WORK INCLUDES ANY CLEARING AND GRUBBING NECESSARY TO PLACE THE EMBANKMENT AT THE LOCATIONS SPECIFIED OR DIRECTED. SEE THE GUARDRAIL EMBANKMENT DETAIL ON SHEET P.9 FOR MORE INFORMATION.

ITEM 606 - BRIDGE TERMINAL ASSEMBLY, TYPE 4, AS PER PLAN:
BRIDGE TERMINAL ASSEMBLY, TYPE 4, AS PER PLAN SHALL BE CONSTRUCTED AS PER THE GUARDRAIL DETAILS ON SHEET P.8. PAYMENT FOR THIS ITEM SHALL BE MADE AT THE UNIT PRICE BID OF EACH AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT, MATERIALS, AND ALL TYPE 5

GUARDRAIL COMPONENTS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL BRIDGE TERMINAL ASSEMBLY, TYPE 4.

ITEM 606 - ANCHOR ASSEMBLY, MGS TYPE E, AS PER PLAN (MASH 2016):
THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY OF THE GUARDRAIL END TERMINALS FOR TYPE MGS GUARDRAIL AS LISTED ON ROADWAY ENGINEERING'S WEB PAGE UNDER ROADSIDE SAFETY DEVICES FOR APPROVED GUARDRAIL END TREATMENTS. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

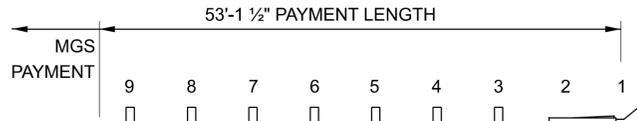
WHEN THE FACE OF THE ADJACENT (ATTACHED) GUARDRAIL IS LESS THAN 4' OFFSET FROM THE PROPOSED EDGE LINE, THE PROPOSED TYPE E ANCHOR ASSEMBLY SHALL BE INSTALLED USING A 50:1 FLARE RATE AS DETAILED IN THE SHOP DRAWINGS AND AS DIRECTED BY THE ENGINEER.

THE FACE OF THE TYPE E IMPACT HEAD SHALL BE COVERED WITH REBOUNDABLE RETROREFLECTIVE SHEETING, PER CMS 730.191.

REFER TO THE MANUFACTURER'S INSTRUCTIONS REGARDING THE INSTALLATION OF, AND THE GRADING AROUND THE FOUNDATION TUBES AND GROUND STRUT. THE TOP OF ANY FOUNDATION TUBE SHOULD BE LESS THAN 4 INCHES ABOVE THE GROUND. THE PLACEMENT OF THE FOUNDATION TUBES SHOULD BE AN APPROPRIATE DEPTH BELOW THE LEVEL LINE IN ORDER TO MAINTAIN THE FINISHED GUARDRAIL HEIGHT OF 31 INCHES FROM THE EDGE OF THE SHOULDER.

ON-SITE GRADING IS REQUIRED IF THE TOP OF THE FOUNDATION TUBES OR TOP OF THE GROUND STRUT DOES PROJECT MORE THAN 4 INCHES ABOVE THE GROUND LINE.

THE PAYMENT LIMIT (LENGTH) FOR THE PROPOSED ANCHOR ASSEMBLY, (MGS) TYPE E, AS PER PLAN SHALL BE 53'- 1 1/2" (TO THE STANDARD MGS CONNECTION) AS DETAILED BELOW.



PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, ANCHOR ASSEMBLY, MGS TYPE E, EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL ANCHOR ASSEMBLY SYSTEM, INCLUDING ALL RELATED TRANSITIONS, REFLECTIVE SHEETING, HARDWARE, GRADING, EMBANKMENT AND EXCAVATION NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

ITEM 606 - GUARDRAIL, MISC.: ALTERNATIVE GUARDRAIL PLACEMENT:
THIS ITEM SHALL BE USED WHEN THE CONTRACTOR IS REQUIRED TO USE AN ALTERNATE METHOD TO SET POSTS TO PREVENT DAMAGE TO AN UNDERGROUND OBSTACLE, SUCH AS A UTILITY. THE USE OF THIS ITEM WILL BE AS DEEMED NECESSARY BY THE ENGINEER. THIS ITEM SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIAL NEEDED TO SET AND BACKFILL POSTS WHILE MEETING THE REQUIREMENTS OF THE APPLICABLE GUARDRAIL ITEM BEING PERFORMED. APPLICABLE GUARDRAIL ITEMS INCLUDE BUT ARE NOT LIMITED TO SETTING POSTS (AND SLEEVES) FOR TYPE 5, TYPE MGS, BARRIER DESIGN, ANCHOR ASSEMBLIES, AND BRIDGE TERMINAL ASSEMBLIES. PAYMENT SHALL BE AT THE UNIT BID PRICE OF EACH AND SHALL BE PAID FOR IN ADDITION TO THE APPLICABLE GUARDRAIL PLACEMENT ITEM LISTED ABOVE.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN PROVIDED AND CARRIED TO THE GENERAL SUMMARY:

ITEM 606 - GUARDRAIL, MISC.: ALTERNATIVE GUARDRAIL PLACEMENT = 25 FT

CONNECTION BETWEEN EXISTING AND PROPOSED GUARDRAIL:
WHEN IT IS NECESSARY TO SPLICE PROPOSED GUARDRAIL TO EXISTING GUARDRAIL, ONLY THE EXISTING GUARDRAIL SHALL BE CUT, DRILLED, OR PUNCHED. THE CONNECTION SHALL BE MADE USING A W-BEAM, BEAM SPLICE AS SHOWN IN AASHTO M 180-12, EXCEPT THE BEAM WASHERS ARE NOT TO BE USED. PAYMENT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE RESPECTIVE GUARDRAIL ITEMS.

EXISTING CONCRETE CURB AND GUTTER:
THE CONTRACTOR SHALL USE EXTREME CARE AND CAUTION WHEN WORKING AROUND EXISTING CONCRETE CURB AND GUTTER. EXISTING CONCRETE CURB AND GUTTER SHALL IN NO WAY BE EXPOSED TO TACK COAT AT ANY TIME. IN THE OCCURRENCE THAT CONCRETE CURB AND/OR GUTTER IS EXPOSED TO TACK COAT, THE CONTRACTOR SHALL RECTIFY IT AT THE CONTRACTOR'S EXPENSE. RECTIFICATION MAY INCLUDE (BUT IS NOT LIMITED TO) CLEANING, PATCHING, OR FULLY REPLACING THE EXPOSED/DAMAGED CONCRETE. THE TYPE AND MEANS OF RECTIFICATION SHALL BE DETERMINED BY THE PROJECT ENGINEER ON A CASE BY CASE BASIS.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 4.5":
ALL REPAIR AREAS DETAILED IN THE PLAN SHALL BE VERIFIED BY THE PROJECT ENGINEER BEFORE THE BEGINNING OF WORK. THE REPAIR AREAS SHALL CONSIST OF REMOVING 4.5 INCHES OF PAVEMENT AND PLACING 4.5 INCHES OF ITEM 301 - ASPHALT CONCRETE BASE, PG64-22, (449). FOR MORE INFORMATION SEE DETAIL ON SHEET P.4. WORK SHALL BE PERFORMED PRIOR TO PLANING. NO MORE PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE) SHALL BE STARTED AND PERFORMED THAN CAN BE COMPLETED IN THE SAME WORKING DAY.

IN ADDITION TO THE LOCATIONS LISTED ON SHEET P.21, THE FOLLOWING CONTINGENCY QUANTITY HAS BEEN PROVIDED AND BEEN CARRIED TO THE GENERAL SUMMARY:
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 4.5" = 100 SY

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 9.0" :
ALL REPAIR AREAS DETAILED IN THE PLAN SHALL BE VERIFIED BY THE PROJECT ENGINEER BEFORE THE BEGINNING OF WORK. THE REPAIR AREAS SHALL CONSIST OF REMOVING 9.0 INCHES OF CONCRETE, BRICK, OR ASPHALT PAVEMENT (SAWCUTTING OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL), AND PLACING TWO LIFTS OF ITEM 301 - ASPHALT CONCRETE BASE, PG64-22, (449). FOR MORE INFORMATION SEE DETAIL ON SHEET P.4. WORK SHALL BE PERFORMED PRIOR TO PLANING. NO MORE PAVEMENT REPAIR, AS PER PLAN, 9.0" SHALL BE STARTED AND PERFORMED THAN CAN BE COMPLETED IN THE SAME WORKING DAY.

IN ADDITION TO THE LOCATIONS LISTED ON SHEET P.21, THE FOLLOWING CONTINGENCY QUANTITY HAS BEEN PROVIDED AND BEEN CARRIED TO THE GENERAL SUMMARY:
ITEM 251 - PARTIAL PAVEMENT DEPTH REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 9.0" = 50 CY

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 9.0", INSIDE CURVE REPAIR:
ALL REPAIR AREAS DETAILED IN THE PLAN SHALL BE VERIFIED BY THE PROJECT ENGINEER BEFORE THE BEGINNING OF WORK. THE REPAIR AREAS SHALL CONSIST OF REMOVING 9.0 INCHES OF PAVEMENT AND PLACING 9.0 INCHES OF ITEM 301 - ASPHALT CONCRETE BASE, PG64-22, (449). THE AVERAGE WIDTH SHALL NOT BE LESS THAN 5 FEET. FOR MORE INFORMATION, SEE DETAILS ON SHEETS P.10 AND P.11. WORK SHALL BE PERFORMED PRIOR TO PLANING. NO MORE PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE) SHALL BE STARTED AND PERFORMED THAN CAN BE COMPLETED IN THE SAME WORKING DAY.

FOR LOCATIONS AND QUANTITIES, SEE SHEET P.21.



ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, 1.50":
 THE CONTRACTOR SHALL BE TOTALLY RESPONSIBLE FOR ANY AND ALL DAMAGE TO THE CONTRACTORS EQUIPMENT THAT MAY RESULT FROM THE PLANING OPERATION, INCLUDING DAMAGE CAUSED BY CONCRETE PAVEMENT, CASTINGS, AND LOOP DETECTORS. THE DEPTH OF PLANING CLOSE TO THE CASTINGS SHALL BE AS DIRECTED; TO ACHIEVE A SMOOTH RIDING FINISHED PAVEMENT. GREAT CARE SHALL BE TAKEN TO PREVENT THE REMOVAL OF THE EXISTING PAVEMENT CROSS-SLOPE (CROWN) DURING THE PLANING OPERATIONS.

ITEM 611 - MANHOLE ADJUSTED TO GRADE, AS PER PLAN:
 THE CONTRACTOR SHALL PERFORM ALL NECESSARY DEMOLITION AND CONSTRUCTION WORK IN ORDER TO ADJUST EXISTING MANHOLE COVERS TO MATCH THE FINAL GRADE OF THE ROADWAY. ALL WORK IDENTIFIED ON DETAIL SHEET P.7, INCLUDING PLACEMENT OF ITEM 452 - NON-REINFORCED CONCRETE, SHALL BE INCLUDED IN THIS PAY ITEM.

MANHOLES AND OTHER CASTING STRUCTURES:
 THE ITEMS PROVIDED BELOW ARE CONTINGENCY QUANTITIES TO BE USED AS DIRECTED BY THE PROJECT ENGINEER AT VARIOUS LOCATIONS. EACH ITEM SHALL INCLUDE THE COST OF ALL LABOR, EQUIPMENT, MATERIALS, AND HARDWARE NECESSARY TO ADJUST CASTINGS TO GRADE TO THE PROPOSED ASPHALT ELEVATION AS DIRECTED.

THE FOLLOWING CONTINGENCY QUANTITIES HAVE BEEN PROVIDED AND THE TOTALS HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 611 - CATCH BASIN ADJUSTED TO GRADE	=	4 EACH
ITEM 611 - MANHOLE ADJUSTED TO GRADE, AS PER PLAN	=	4 EACH
ITEM 638 - VALVE BOX ADJUSTED TO GRADE	=	4 EACH

ITEM 623 - CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN:
 THIS ITEM SHALL CONSIST OF STATIONING USING 3 FT LATH STAKES. THE STAKES SHALL BE SPACED AT 200 FT INTERVALS AND SHALL EXTEND THROUGHOUT THE LENGTH OF EACH PROJECT LOCATION AND THROUGHOUT THE LENGTH OF ANY RAMPS.

PLACEMENT OF THE STAKES SHALL BE AS DIRECTED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY DAMAGED OR MISSING STAKES.

THIS ITEM SHALL ALSO INCLUDE ANY WORK NECESSARY FOR THE CONTRACTOR TO VERIFY EXISTING RIGHT OF WAY, AS DIRECTED.

CONSTRUCTION LAYOUT STAKES, AS PER PLAN WILL BE PAID FOR AT THE CONTRACT LUMP SUM BID, WHICH SHALL BE FULL COMPENSATION FOR ALL SERVICES, MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS, INCLUDING THE REMOVAL, NECESSARY TO COMPLETE THIS ITEM.

DRAINAGE AT INTERSECTING STREETS:
 AT INTERSECTING STREETS WHERE THE DRAINAGE IS TOWARD OR INTO THE PROJECT, SPECIAL CARE SHALL BE TAKEN BY THE CONTRACTOR TO MAINTAIN PROPER GRADE ALONG THE EDGE OF PAVEMENT SO THAT WATER WILL NOT POND. AT INTERSECTING STREETS, WHERE THE EDGE OF PAVEMENT CONTINUES ACROSS THE STREET, CARE SHALL BE TAKEN TO FEATHER DOWN AND FORM A NEAT SEAM WITH THE PROPER GRADE.

FIRE HYDRANTS:
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ANY PERMITS AND FEES THAT ARE REQUIRED FOR THE USE OF ANY FIRE HYDRANTS. A SIAMESE VALVE IS TO BE USED ON THE HYDRANT OUTLET IF A HOSE IS TO BE LEFT CONNECTED AND UNATTENDED. THIS PERMIT CAN BE OBTAINED FROM THE VILLAGE OF JEFFERSONVILLE.

ITEM 202 - WALK REMOVED, AS PER PLAN:
 THIS ITEM HAS BEEN PROVIDED AND SHALL BE USED IN CONJUNCTION WITH THE CONSTRUCTION OF THE PROPOSED CURB RAMPS. THE CONTRACTOR SHALL PROVIDE ALL MATERIAL, LABOR, EQUIPMENT, AND HARDWARE NECESSARY TO REMOVE EXISTING SIDEWALK (INCLUDING ANY EXISTING CURB RAMPS). SPECIFIC LOCATIONS AND LIMITS OF THIS WORK CAN BE FOUND ON PLAN SHEETS P.48 - P.55.

PAVING AT RAILROAD CROSSING:
 WORK THE CROWN OUT OF THE PROPOSED PAVEMENT ON EACH SIDE OF THE RAILROAD CROSSING, BEGINNING 50 FEET FROM THE NEAREST RAIL, BY RAISING THE EDGES OF THE NEW PAVEMENT TO MEET THE PLATFORM ELEVATION.

ITEM 642 - TYPE 1 TRAFFIC PAINT, AS PER PLAN, POTTERS P18+2 BEADS:
 ALL ITEMS BELOW SHALL COMPLY WITH ITEM 642 EXCEPT AS FOLLOWS:

IN LIEU OF ODOT TYPE A GLASS BEADS, THE CONTRACTOR SHALL APPLY POTTERS P18+2 DUAL COATED GLASS BEADS TO THE WET PAINT SO THAT THE BEADS ARE EMBEDDED AND RETAINED IN THE PAINT AND PROVIDE UNIFORM RETROREFLECTIVITY IN THE PAINT SURFACE. APPLY AT A MINIMUM RATE OF 10 POUNDS PER 100 SQUARE FEET.

POTTERS P18+2 BEADS SHALL BE USED FOR THE FOLLOWING ITEMS:
 ITEM 642 - EDGE LINE, 6", TYPE 1, AS PER PLAN
 ITEM 642 - CENTER LINE, TYPE 1, AS PER PLAN

ITEM 644 - THERMOPLASTIC, AS PER PLAN, POTTERS VISIBLEND BEADS:
 ALL ITEMS BELOW SHALL COMPLY WITH ITEM 644 EXCEPT AS FOLLOWS:

IN LIEU OF ODOT TYPE C GLASS BEADS FOR INTERMIX AND DROP-ONS, THE CONTRACTOR SHALL USE AND APPLY POTTERS VISIBLEND GLASS BEADS. FURNISH DROP-ON VISIBLEND BEADS THAT HAVE, AT A MINIMUM, A MOISTURE-PROOF COATING. APPLY DROP-ON VISIBLEND BEADS AT A MINIMUM RATE OF 12 POUNDS PER 100 SQUARE FEET OF THERMOPLASTIC SURFACE AREA.

POTTERS VISIBLEND BEADS SHALL BE USED FOR THE FOLLOWING ITEMS:
 ITEM 644 - EDGE LINE, 6", AS PER PLAN
 ITEM 644 - LANE LINE, 6", AS PER PLAN
 ITEM 644 - CENTER LINE, AS PER PLAN
 ITEM 644 - CHANNELIZING LINE, 8", AS PER PLAN
 ITEM 644 - STOP LINE, AS PER PLAN
 ITEM 644 - CROSSWALK, 12", AS PER PLAN
 ITEM 644 - TRANSVERSE/DIAGONAL LINE, AS PER PLAN
 ITEM 644 - ISLAND MARKING, AS PER PLAN
 ITEM 644 - RAILROAD SYMBOL MARKING, AS PER PLAN
 ITEM 644 - LANE ARROW, AS PER PLAN
 ITEM 644 - DOTTED LINE, 6", AS PER PLAN

ENVIRONMENTAL COMMITMENTS:
 1) ANY AND ALL CONSTRUCTION DEBRIS, EARTHEN DEBRIS, EXCESS ASPHALT OR CONCRETE, WOOD DEBRIS FROM CLEARING, EXCESS FILL MATERIAL, AND TRASH SHALL BE REMOVED AND DISPOSED OF AT AN APPROVED UPLAND SITE OR LAND FILL ABOVE FEMA 100-YEAR FLOOD ELEVATIONS.
 2) NO TREE CLEARING IS REQUIRED OR PERMITTED ON THIS PROJECT.
 3) ENSURE IMPACTS TO THE FEDERALLY LISTED AND PROTECTED BAT SPECIES ARE AVOIDED AND MINIMIZED. DO NOT REMOVE TREES FROM APRIL 1 THROUGH SEPTEMBER 30. PERFORM ALL NECESSARY TREE REMOVAL FROM OCTOBER 1 THROUGH MARCH 31. DEMARCATATE CLEARING LIMITS IN THE FIELD TO AVOID ANY UNAUTHORIZED TREE CLEARING. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS A LIVE, DYING, OR DEAD WOODY PLANT, WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE THE GROUND SURFACE, AND WITH A MINIMUM HEIGHT OF 13 FEET.
 4) WATERS OF THE UNITED STATES SHALL NOT BE IMPACTED AS PART OF THIS PROJECT.

DESIGN AGENCY



DESIGNER

JDM

REVIEWER

KLM 12/02/24

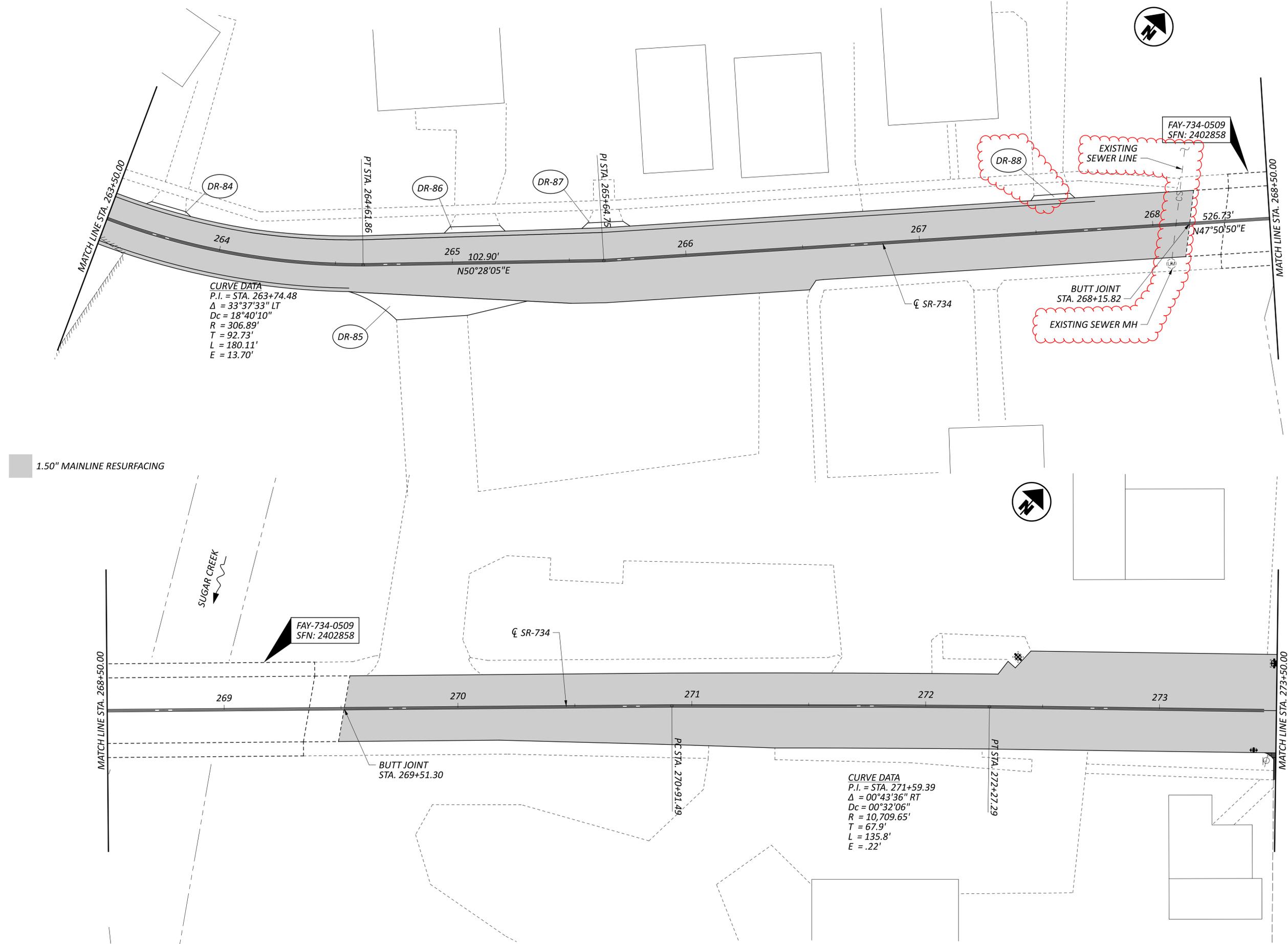
PROJECT ID

110584

SHEET TOTAL

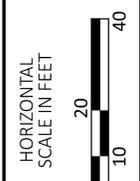
P.13 P.58

LOCATION								DESIGN			QUANTITIES			REMARKS
LOCATION	COUNTY	ROUTE	DIRECTION	PLAN SPLIT #	PRIORITY	APPROX. BEGIN SLM	APPROX. END SLM	LENGTH	REPAIR WIDTH	TOTAL PAVEMENT AREA	251E01041	251E01043	251E01043	
											PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 4.5"	PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 9.0"	PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 9.0" INSIDE CURVE REPAIR	
											SY	CY	CY	
1	FAY	41	EB	3	1	24.480	24.390	475	12	634	634			Full lane width
1	FAY	41	EB	3	1	24.186		20	8	18	18			Center on lane
1	FAY	41	EB	3	1	24.151		30	10	33	33			Center on lane, pot hole formed.
1	FAY	41	EB	3	1	24.103		20	6	13		3		Passenger wheel path to center of the lane
1	FAY	41	EB	3	1	24.005		50	8	44		11		Rutting / Shoving
1	FAY	41	EB/WB	3	1	23.669		30	26	87	87			West Bridge Approach
1	FAY	41	EB	3	1	23.683		6	6	4		1		Previous utility repair
1	FAY	41	EB	3	1	23.672		6	6	4		1		Previous utility repair
1	FAY	41	EB	3	1	23.668		25	12	33		8		Adjacent to previous utility repair
1	FAY	41	EB	3	1	23.587		6	6	4		1		Located in passenger wheel path
1	FAY	41	EB	3	1	23.478		10	15	17		4		Previous utility repair
1	FAY	41	EB	3	1	23.280		20	13	29		7		Signs of pumping, could be subgrade issue
1	FAY	41	EB	3	1	23.153	22.998	818	6	546		137		
1	FAY	41	EB	3	1	23.058		50	6	33		8		Shoulder repair
1	FAY	41	WB	3	1	23.234		10	6	7		2		Previous utility repair
1	FAY	41	WB	3	1	23.277		10	10	11		3		Previous utility repair
1	FAY	41	WB	3	1	23.392		15	6	10		3		Previous utility repair
1	FAY	41	WB	3	1	23.346		50	6	33	33			Passenger wheel path
1	FAY	41	WB	3	1	23.511	23.516	26	6	18		5		Gravel to surface from curb repair
1	FAY	41	WB	3	1	23.560	23.659	523	10	581		145		In parking area gravel to surface multiple repairs
1	FAY	41	WB	3	2	23.184		6	6	4		1		Shoulder repair
1	FAY	41	WB	3	2	23.208		6	6	4		1		Shoulder repair, NE corner of Maple and SR 41
2	FAY	729	NB	3	2	9.67		50	6	33		8		Shoulder Repair
2	FAY	729	NB	3	1	9.782	9.799	75	6	50			13	Shoulder repair around the curve
2	FAY	729	NB	3	2	9.775		50	10	56	56			Rutting / shoving at stop sign
2	FAY	729	NB	3	1	9.768		10	6	7		2		Shoulder repair
2	FAY	729	NB	3	1	9.782		50	6	33		8		Shoulder around int. radius of SR 729 and SR 41
2	FAY	729	NB	3	2	10.24		6	6	4	4			Start at Maple Street
2	FAY	729	NB	3	2	10.29		6	6	4	4			100' S. R/R tracks, pot hole has formed
2	FAY	729	NB	3	2	10.35		8	15	13	13			Intersection of Fawkes St. and SR 729
2	FAY	729	SB	3	1	9.711	9.695	65	6	43			11	Shoulder repair around the curve
2	FAY	729	SB	3	1	9.86		25	12	33		8		Shoulder around int. radius of SR 729 and SR 41
2	FAY	729	SB	3	1	10.26		60	6	40		10		30' N. Intersection of Maple St. and SR 729
2	FAY	729	SB	3	1	10.36		6	6	4		1		Near Fawkes Street, shoulder repair
2	FAY	729	SB	3	2	10.41		8	15	13	13			Intersection of George St. and SR 729
2	FAY	729	NB/SB	3	2	CONTINGENCY		-	-	220		55		Previous utility repair
3	FAY	734	NB	3	2	5.03		8	8	7		2		Previous utility repair
3	FAY	734	NB	3	2	5.04	5.06	74	10	82		21		Previous utility repair
3	FAY	734	NB/SB	3	1	5.08		7	28	23		6		Full width repair over existing sewer line
3	FAY	734	NB	3	2	5.14		30	10	33		8		Previous utility repair
3	FAY	734	NB	3	2	5.16		6	6	4		1		Hole in pavement partially filled with cold patch
TOTALS CARRIED TO PLAN SPLIT #1											0	0		
TOTALS CARRIED TO PLAN SPLIT #2											0	0		
TOTALS CARRIED TO PLAN SPLIT #3											895	471	24	
TOTALS CARRIED TO GENERAL SUMMARY											895	471	24	



CURVE DATA
 P.I. = STA. 263+74.48
 $\Delta = 33^{\circ}37'33''$ LT
 $Dc = 18^{\circ}40'10''$
 $R = 306.89'$
 $T = 92.73'$
 $L = 180.11'$
 $E = 13.70'$

CURVE DATA
 P.I. = STA. 271+59.39
 $\Delta = 00^{\circ}43'36''$ RT
 $Dc = 00^{\circ}32'06''$
 $R = 10,709.65'$
 $T = 67.9'$
 $L = 135.8'$
 $E = .22'$



ROADWAY PLAN - SR-734
 STA. 263+50.00 TO STA. 273+50.00

DESIGN AGENCY	
DESIGNER	JDM
REVIEWER	KLM 12/02/24
PROJECT ID	110584
SHEET	TOTAL
P.45	P.58