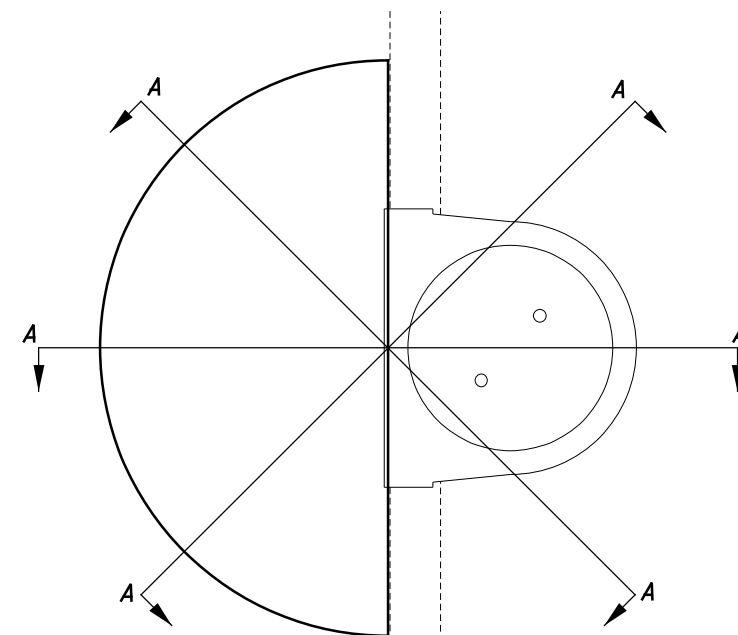


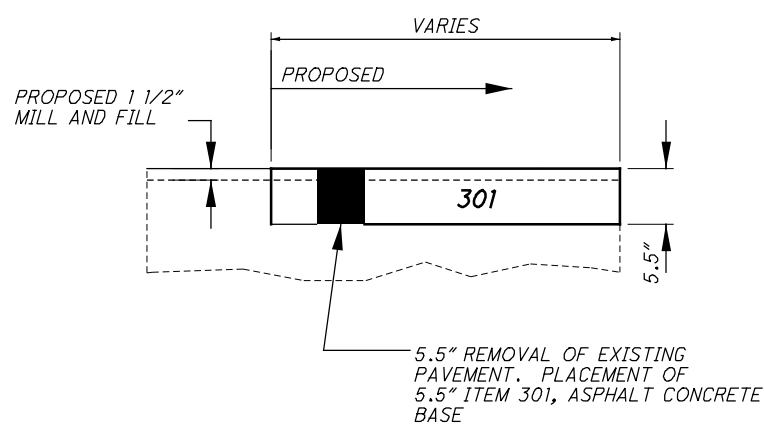
TYPICAL INTERSECTION DETAIL

■ MATCH PROPOSED PAVEMENT PLANING AND SURFACE TREATMENT DEPTH. TRANSITION TO BUTT JOINT. (BP 3.1)

LOC 1	FRA	256	SLM	00.05		BROAD ALLEY	LT & RT
				00.09		BROADWYN DRIVE	LT & RT
				00.14		RICKLY STREET	LT & RT
				00.18	TR 1899	SOUTH STREET	LT & RT
				00.27	TR 2050	JACKSON STREET	LT
				00.39	TR 1899	JOHN STREET	LT & RT
				00.74	CR 0106	LIVINGSTON AVE	RT
				00.74	CR 0106	SLATE BOULEVARD	LT
				00.88		STONETRAIL WAY	RT
				01.04		STONETRAIL WAY	RT
				01.16		FARMSBURY ROAD	LT

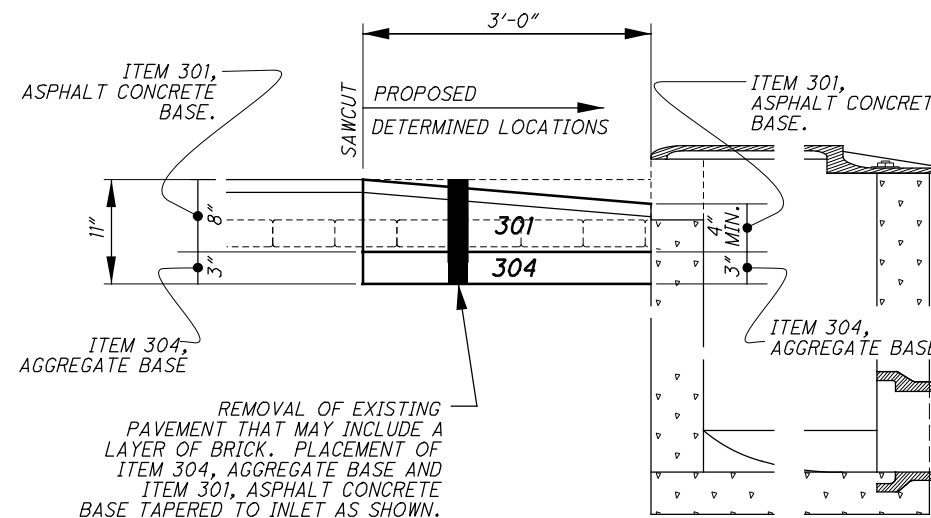


TYPICAL DETAILS



PARTIAL DEPTH PAVEMENT REPAIR DETAIL

SEE GENERAL NOTES FOR MORE INFORMATION REGARDING ITEM 251-PARTIAL DEPTH PAVEMENT REPAIR, (ASPHALT CONCRETE BASE) AS PER PLAN.



REMOVAL OF EXISTING PAVEMENT THAT MAY INCLUDE A LAYER OF BRICK. PLACEMENT OF ITEM 304, AGGREGATE BASE AND ITEM 301, ASPHALT CONCRETE BASE TAPERED TO INLET AS SHOWN.

SECTION A-A
ITEM 253 - PAVEMENT REPAIR, AS PER PLAN, (CATCH BASIN) 11"
 SEE GENERAL NOTES SHEET 7/20 FOR MORE INFORMATION REGARDING ITEM 253- PAVEMENT REPAIRS, AS PER PLAN, (CATCH BASIN) 11".

FRA-256-0.00

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DESIGN AGENCY	
DESIGNER	RAM
REVIEWER	
PROJECT ID	106264
SHEET	P.04
TOTAL	20

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 5.5" :

ALL REPAIR AREAS SHALL BE VERIFIED BY THE PROJECT ENGINEER BEFORE THE BEGINNING OF WORK. THE THE REPAIR AREAS SHALL CONSIST OF REMOVING 5.5 INCHES OF PAVEMENT AND PLACING 5.5 INCHES OF ITEM 301 - ASPHALT CONCRETE BASE, PG64-22 AND THE MINIMUM WIDTH SHALL BE 4' WIDE. FOR MORE INFORMATION SEE DETAIL ON SHEET 4. WORK SHALL BE PERFORMED PRIOR TO PLANING. NO MORE PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE) SHALL BE STARTED AND PERFORMED THAN CAN BE COMPLETED IN THE SAME WORKING DAY.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AS A CONTINGENCY AND HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN, 6" = 150 SQ YD

ITEM 253 - PAVEMENT REPAIR, AS PER PLAN, (CATCH BASIN) 11" :

THESE PAVEMENT REPAIRS SHALL CONSIST OF FULL DEPTH PAVEMENT SAWING AND COMPLETE REMOVAL OF ANY EXISTING PAVEMENT (AND BRICK) TO A DEPTH OF 11" ADJACENT TO CITY OF REYNOLDSBURG INLETS. THE REPAIR AREA SHALL BE REFILLED WITH ITEM 304 - AGGREGATE BASE AND ITEM 301 - ASPHALT CONCRETE BASE TO CREATE A "BOWL" OPENING AS DETAILED ON SHEET 4/20. SPECIFIC LOCATIONS OF THIS WORK CAN BE FOUND ON PLAN SHEETS 12-18. TACK COAT SHALL BE APPLIED TO ALL VERTICAL SURFACES AT A RATE OF 0.075 PER SQ YD PRIOR TO THE PLACING OF ITEM 301 - ASPHALT CONCRETE BASE. THESE REPAIRS SHALL BE PERFORMED PRIOR TO MILLING.

THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED AS A CONTINGENCY AND HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 253 - PAVEMENT REPAIR, AS PER PLAN, (CATCH BASIN) 11" = 5 SQ YD

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, 1.5" :

THE CONTRACTOR SHALL BE TOTALLY RESPONSIBLE FOR ANY AND ALL DAMAGE TO THE CONTRACTORS EQUIPMENT THAT MAY RESULT FROM THE PLANING OPERATION, INCLUDING DAMAGE CAUSED BY CASTINGS AND LOOP DETECTORS. THE DEPTH OF PLANING CLOSE TO THE CASTINGS SHALL BE AS DIRECTED; TO ACHIEVE A SMOOTH RIDING FINISHED PAVEMENT. GREAT CARE SHALL BE TAKEN TO PREVENT THE REMOVAL OF THE EXISTING PAVEMENT CROSS SLOPE (CROWN) DURING THE PLANING OPERATIONS.

THE CONTRACTOR SHALL LIMIT THE PLANING OPERATION TO ONE LANE AT A TIME AS TO ENSURE THAT THE PROPOSED SURFACE COURSE IS BUTTING UP TO EITHER PROPOSED OR EXISTING ASPHALT.

THE MAJORITY OF PLANED PAVEMENT WILL BE ASPHALT CONCRETE, HOWEVER THE CONTRACTOR MAY ENCOUNTER SMALL PATCHES OF CONCRETE PAVEMENT (SUCH AS OVER TOP OF EXISTING CULVERTS). THIS CONCRETE IS TO BE PLANED AT THE SAME DEPTH AS THE ASPHALT CONCRETE AND IS TO BE INCLUDED IN THE PAYMENT OF ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE.

FAILURE TO MEET THIS REQUIREMENT WILL SUBJECT THE CONTRACTOR TO A DISINCENTIVE OF \$900/DAY FOR EACH DAY THE PLANED SURFACE IS NOT RESURFACED.

ITEM 617 - WATER:

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 617 - WATER = 2 MGAL

ITEM 644 - THERMOPLASTIC PAVEMENT MARKING:

THE LOCATIONS, SIZES AND SHAPES OF PROPOSED PAVEMENT MARKINGS WILL BE THE SAME AS EXISTING ON THE MAJORITY OF THIS PROJECT. ANY DEVIATION FROM EXISTING WILL BE IDENTIFIED WITHIN THIS PLAN.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE AND SHAPE OF THESE EXISTING PAVEMENT MARKINGS BEFORE THE PAVEMENT PLANING AND RESURFACING OBLITERATES THEM. ANY PAVEMENT MARKING WHICH IS PLACED AT THE WRONG LOCATION SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

QUANTITIES FOR ITEM 644 - THERMOPLASTIC PAVEMENT MARKINGS HAVE BEEN PROVIDED IN THE TRAFFIC CONTROL SUBSUMMARY ON SHEET 20.

ITEM 632 - LOOP DETECTOR, AS PER PLAN:

THE LOCATIONS, SIZES AND SHAPES OF PROPOSED LOOP DETECTORS WILL BE THE SAME AS EXISTING. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE AND SHAPE OF THE EXISTING LOOP DETECTORS LISTED IN THE PLAN BEFORE THE PAVEMENT PLANING DESTROYS THEM. ALL LOOP WIRE SHALL BE IDENTIFIED WITH A PLASTIC TAG (WBLT, EBRT, ETC.) AT THE SPLICE POINT OR AT ENTRANCE TO THE CONTROL CABINET. WHEN A PULLBOX IS NOT USED, THE SOLDERED SPLICE SHALL BE MADE IN AN ANCHOR BASE, STRAIN POLE OR A CONDUIT RISER SPECIFIED BY THE PROJECT ENGINEER, EXCEPT WHERE A CONTROLLER CABINET IS MOUNTED ON THAT POLE IN WHICH CASE THE LOOP WIRE SHALL BE ROUTED DIRECTLY INTO THE CABINET. THE CONTRACTOR SHALL PROVIDE THE PROJECT ENGINEER PRIOR TO THE COMMENCEMENT OF WORK, A COPY OF THE IMSA CERTIFICATION PAPERS FOR ALL SIGNAL TECHNICIANS WORKING ON THIS PROJECT. THE CONTRACTOR SHALL ALSO CONTACT KEITH KUNDTZ WITH CITY OF REYNOLDSBURG AT LEAST TWO WEEKS (EXCLUDING SAT & SUN) IN ADVANCE OF THIS ITEM OF WORK. ALL DETECTOR LOOPS SHALL BE REPLACED AND FUNCTIONAL WITHIN 48 HOURS OF BEING REMOVED. THE CONTRACTOR SHALL ALSO PROTECT ANY INLET OR CATCH BASIN FROM FOREIGN MATERIAL OR CONSTRUCTION DEBRIS ENTERING THE INLET OR CATCH BASIN WHILE CUTTING DETECTOR LOOPS.

EXISTING LOOP DETECTORS ARE SHOWN ON THE PLAN VIEWS SHEETS 12-18 AND ARE TO BE REPLACED WITH THIS PROJECT.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 632 - DETECTOR LOOP, AS PER PLAN = 18 EACH

ITEM 632 - LOOP DETECTOR TIE-IN:

THIS ITEM SHALL BE USED TO CONNECT AND SPLICE THE PROPOSED LOOP DETECTORS TO THE APPROPRIATE EXISTING LEAD IN CABLE INSIDE THE PULL BOX.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 632 - LOOP DETECTOR TIE IN = 18 EACH

ITEM 623 - CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN:

THIS ITEM SHALL CONSIST OF STATIONING USING 3 FT LATH STAKES. THE STAKES SHALL BE SPACED AT 200 FT INTERVALS AND SHALL EXTEND THROUGHOUT THE LENGTH OF EACH PROJECT LOCATION AND THROUGHOUT THE LENGTH OF ANY RAMPS.

PLACEMENT OF THE STAKES SHALL BE AS DIRECTED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY DAMAGED OR MISSING STAKES. THIS ITEM OF WORK SHALL ALSO INCLUDE ANY WORK NECESSARY FOR THE CONTRACTOR TO VERIFY EXISTING RIGHT OF WAY, AS DIRECTED.

CONSTRUCTION LAYOUT STAKES, AS PER PLAN WILL BE PAID FOR AT THE CONTRACT LUMP SUM BID, WHICH SHALL BE FULL COMPENSATION FOR ALL SERVICES, MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS, INCLUDING THE REMOVAL, NECESSARY TO COMPLETE THIS ITEM.

CENTERLINE CONSTRUCTION / RIGHT OF WAY:

THE INTENT OF THIS PROJECT IS THAT ALL WORK IS TO BE COMPLETED WITHIN THE EXISTING RIGHT OF WAY. THE CENTERLINE SHOWN IN THIS PLAN IS TO BE CONSIDERED A CENTERLINE OF CONSTRUCTION ONLY AND NOT TO BE CONSTRUED AS THE ACTUAL GEOMETRIC ALIGNMENT OF THE ROADWAY. THIS CENTERLINE PROVIDED IS TO BE USED AS A REFERENCE OF PROJECT LENGTH ONLY AND SHALL NOT BE USED TO ESTABLISH PRECISE LOCATIONS OF ANY OTHER FEATURES SUCH AS/NOT LIMITED TO THE EXISTING RIGHTS OF WAY. ANY RIGHT OF WAY LOCATION SHOWN IN THE PLAN IS A GRAPHICAL REPRESENTATION (OF SAID RIGHT OF WAY) CONFIRMING THAT THE PLANNED WORK HAS BEEN DETERMINED TO BE IN ODOT RIGHT OF WAY. IN THE EVENT THAT ANY ACTIVITIES DEVIATE FROM THE PLAN, THE CONTRACTOR MAY BE REQUIRED, PER THE ENGINEER, TO VERIFY THE RIGHT OF WAY LIMITS IN THE FIELD. PAYMENT FOR ANY RIGHT OF WAY VERIFICATION WILL BE INCLUDED UNDER THE LUMP SUM BID ITEM 623 - CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN.

DESIGN AGENCY



DESIGNER

RAM

REVIEWER

XXX MM-DD-YY

PROJECT ID

106264

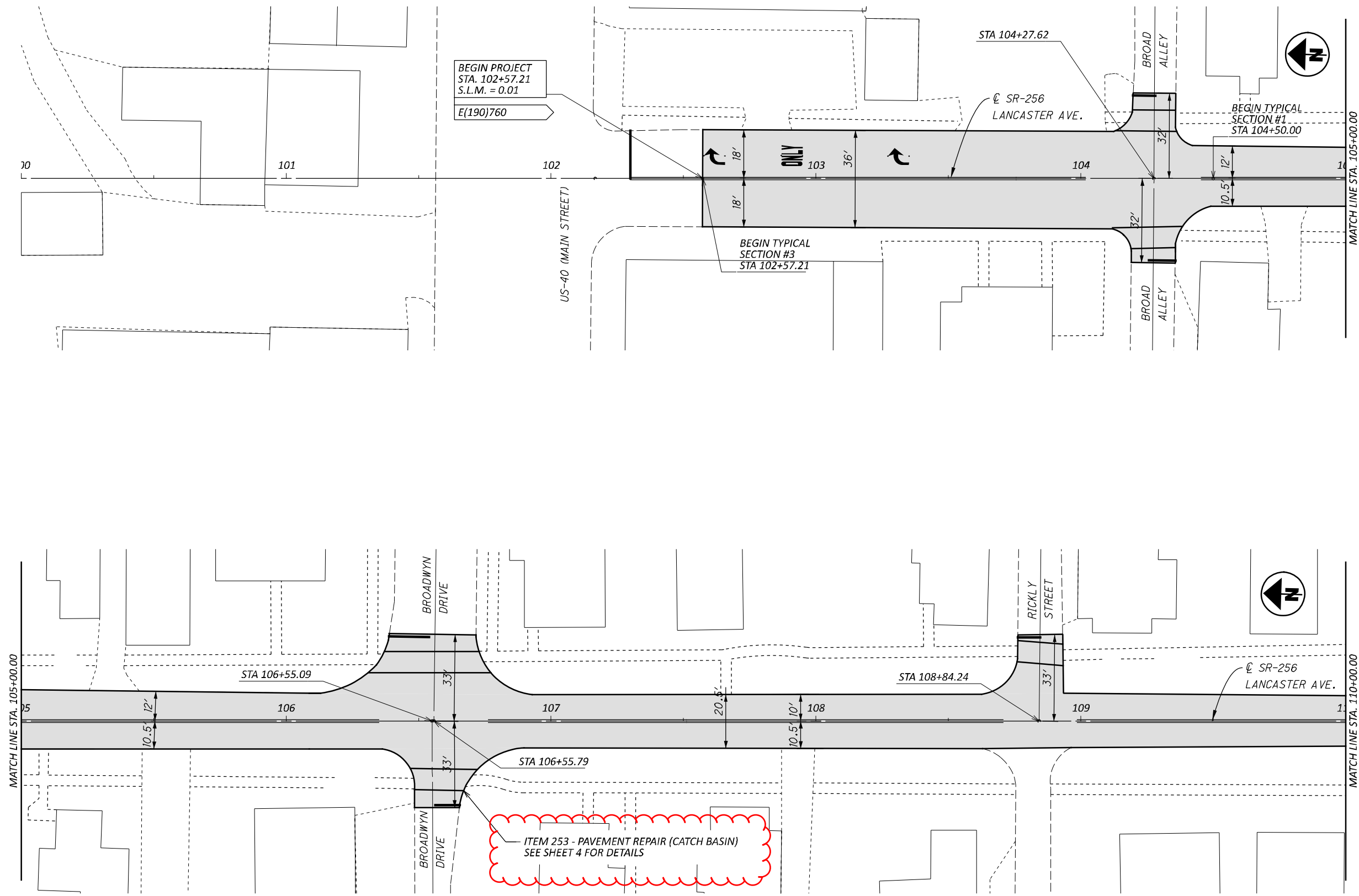
SHEET

P.07

TOTAL

20

FOR PAVEMENT QUANTITIES, SEE SHEETS 11
 FOR PAVEMENT MARKING QUANTITIES, SEE SHEET 20



PLAN VIEWS
 STA 100+00 TO STA 110+00

DESIGN AGENCY



DESIGNER
 RAM

REVIEWER
 XXX MM-DD-YY

PROJECT ID
 106264

SHEET	TOTAL
P.12	20