

SUMMARY OF BIG BUILD 4A PART 1 / 4H PART 2 / 6A PART 3 / 4B PART 4 / 1301R PART 5 MOT SEQUENCING			
STEP	MOT PHASE	*COORDINATION OF OVERLAP WORK	MOT SCHEMATIC PLAN SHEET # (SEE PART # PLANS FOR DETAILS)
1	4A PART 1 PHASE 1		67/1151
2	4A PART 1 PHASE 2	STRUCTURE 1405C (PART 2)	68/1151
3	4A PART 1 PHASE 3		69/1151
4	6A PART 3 PHASE 1		102/702 - 103/702
5	6A PART 3 PHASE 2	STRUCTURES 1322L (PART 3), 1323C (PART 3), 1301L (PART 5), 4B PART 4 PHASE 1	125/702 - 126/702
	1301 PART 5 (1301L)		12/137
6	6A PART 3 PHASE 3	STRUCTURE 1301R (PART 5), 4B PART 4 PHASE 1, 4B PART 4 PHASE 2	168/702 - 169/702
	1301 PART 5 (1301R)		12/137
7	4B PART 4 PHASE 1		41/855 , 78/855 , 79/855
8	4B PART 4 PHASE 2		41/855 , 80/855
9	4B PART 4 PHASE 3		41/855 , 81/855 , 82/855 , 83/855
10	4B PART 4 PHASE 4		41/855 , 84/855 , 85/855
11	4B PART 4 PHASE 5		41/855

I-70 EB AVAILABILITY CLAUSE

PHYSICAL CONSTRUCTION WORK FOR THE PART 1 MAINTENANCE OF TRAFFIC PHASE 1 PLAN PAGES 67 SCHEMATIC AND PAGES 98-112 WHERE I-70 EB AND I-71 NB TRAFFIC ARE MAINTAINED ONTO RAMP C5/C6 CANNOT START UNTIL JUNE 1, 2024. IN ADDITION, I-70 EB STRUCTURES 1321 R, 1358 R, AND 1373 R ALSO CANNOT START UNTIL JUNE 1, 2024. AFTER JUNE 1, 2024, THE CURRENT ONGOING PROJECT PID 105523 WILL HAVE TRAFFIC INTO THAT PROJECT'S PHASE 4 MOT SCHEME AS DESIGNED ON PID 105523'S PH. 4R PLAN PAGES 175-183. FURTHER, STRUCTURE FRA-70-1405C REAR ABUTMENT CONSTRUCTION AND THE CLOSURE OF THE EXISTING I-70 EB RAMP TO LIVINGSTON/4TH CANNOT START UNTIL RAMP C5 IS FULLY CONSTRUCTED IN PROJECT PID 105523 AND OPEN TO TRAFFIC TO FULTON STREET. STRUCTURE FRA-70-1405C ABUTMENT CONSTRUCTION CANNOT START UNTIL RAMP C5/1390C/1395C IS FULLY CONSTRUCTED WITH THE NEW CITY OF COLUMBUS DOP DUCT BANK CONSTRUCTED AND ACTIVE ON 1395C IN PROJECT PID 105523 AND THE 4R 105523 CONTRACTOR REMOVES THE TEMPORARY CITY OF COLUMBUS DOP ELECTRICAL POLES/LINES CROSSING I-70 ADJACENT TO THE 1405C HIGH STREET CROSSING. THESE RESTRICTIONS INCLUDE MAINTENANCE OF TRAFFIC INSTALLATIONS IN EXCESS OF 24 HOURS. DATA COLLECTION INCLUDING BUT NOT LIMITED TO FIELD SURVEYS AND GEOTECHNICAL INVESTIGATIONS ARE PERMITTED UPON SIGNED CONTRACT SUBJECT TO ENVIRONMENTAL AND THIRD-PARTY RESTRICTIONS.

I-70 WB AVAILABILITY CLAUSE

PHYSICAL CONSTRUCTION WORK FOR THE PART 3 MAINTENANCE OF TRAFFIC PHASE 2 I-70 WB BRIDGES CONSTRUCTION PLAN PAGES 125 SCHEMATIC AND PAGES 133-151 CANNOT START UNTIL NOVEMBER 1, 2025. AFTER NOVEMBER 1, 2025, THE CURRENT ONGOING PROJECT PID 105523 WILL HAVE TRAFFIC INTO THAT PROJECT'S PHASE 3B MOT SCHEME AS DESIGNED ON PID 105523'S PH. 6R PLAN PAGES 236-246 WHERE THE I-70 WB MOVEMENT TO I-71 SB WILL BE ON THE 15.03L STRUCTURE. THESE RESTRICTIONS INCLUDE MAINTENANCE OF TRAFFIC INSTALLATIONS IN EXCESS OF 24 HOURS. DATA COLLECTION INCLUDING BUT NOT LIMITED TO FIELD SURVEYS AND GEOTECHNICAL INVESTIGATIONS ARE PERMITTED UPON SIGNED CONTRACT SUBJECT TO ENVIRONMENTAL AND THIRD-PARTY RESTRICTIONS.

MOT CLOSURE NOTES, REFERENCES AND TABLES

PARTS 1 AND 2: SEE SHEETS 54/1151 - 63/1151
PART 3: SEE SHEETS 44/702 - 54/702
PART 4: SEE SHEETS 41/855 - 48/855
PART 5: SEE SHEETS 12/137

* ORIGINAL MOT PHASING BASED ON FOLLOWING PROJECT ORDER - PROJECT 4A-4H / 6A / 1301 / 4B - OVERLAP AREAS IDENTIFIED IN TABLE

SB-315 TO I-70 EB RAMP CLOSURE

THE CONTRACTOR HAS THE NUMBER OF CALENDAR DAYS DESIGNATED IN THE SR-315 SB TO I-70 EB RAMP CLOSURE TABLE IN WHICH TO COMPLETE ALL IDENTIFIED ITEMS OF CRITICAL WORK FOR THE CLOSURE. THE CONTRACTOR MAY BEGIN ANY TIME AFTER THE IDENTIFIED START DATE, AS IDENTIFIED IN THE TABLE, AND MUST COMPLETE THE CRITICAL WORK WITHIN THE NUMBER OF CALENDAR DAYS DESIGNATED IN THE TABLE. THE CLOSURE OF THE SR-315 SB TO I-70 EB RAMP SHALL BE A CONSECUTIVE CLOSURE; THE CALENDAR DAYS TO COMPLETE SHALL NOT BE SPLIT INTO MULTIPLE CLOSURES.

COMPLETION OF CRITICAL WORK IS DEFINED AS HAVING SR-315 SB TO I-70 EB OPEN TO TRAFFIC. LANES MUST BE AVAILABLE FOR USE IN THEIR FINAL DESIGN, FINAL WIDTH, AND WITH ALL MARKINGS, RPM'S, AND SAFETY FEATURES INSTALLED.

IF THE WORK IS NOT COMPLETED WITHIN THE CALENDAR DAYS DESIGNATED IN THE TABLE, THE CONTRACTOR WILL BE SUBJECT TO DISINCENTIVES AS IDENTIFIED IN THE TABLE. EXTENSION OF TIME FOR THE CALENDAR DAYS TO COMPLETE SHALL BE PER CMS 108.06C; TABLE 108.06-1 SHALL APPLY.

SR-315 SB TO I-70 EB RAMP CLOSURE				
DESCRIPTION OF CRITICAL WORK	CALENDAR DAYS TO COMPLETE	DISINCENTIVE \$ PER DAY	WORK WINDOW	
			START	END
SR-315 SB TO I-70 EB RAMP CLOSURE STEPS 1 THROUGH 5 ALL PHASES OF PART 1 (4A) PART 3 (6A) PHASE 1 & 2	1080	\$5000 PER DAY	6/1/2024	PRIOR TO STARTING PART 3 (6A) PHASE 3

NO.	DESCRIPTION	REV. BY	DATE
1	ADDED NOTES	CWL	10-2-23
7	ADDED NOTES	CWL	11-17-23
8	ADDED NOTES	CWL	11-22-23
14	ADDED NOTES & TABLE	CWL	12-28-23

SUMMARY OF RAMP/ROAD CLOSURES					
MOT PHASE	ESTIMATED PHASE DURATION	STREET/RAMP	LOCATION	MAXIMUM DURATION	DISINCENTIVE
1	10 Months	315 Ramp	315S to I-70E Ramp	SEE SHEET 53	
		Scioto Trail	Bike Trail under 70/71	None	None
2	6 Months	315 Ramp	315S to I-70E Ramp	SEE SHEET 53	
		I-70/71	Under High St (EB and WB Closed)	Weekend	*
		Fulton Street	West of High Street	30 Days	\$8,500
		Livingston Ave	West of High Street	30 Days	\$6,000
		Fulton Street	East of High Street	30 Days	\$8,500
Livingston Ave	East of High Street	30 Days	\$6,000		
3	2 Months	315 Ramp	315S to I-70E Ramp	SEE SHEET 53	

Notes:
 1. Length and duration of lane closures and restrictions shall be at the approval of the Engineer. It is the intent to minimize the impact to the traveling public. Lane closures or restrictions over segments of the project in which no work is anticipated within a reasonable time frame, as determined by the Engineer, shall not be permitted. The level of utilization of maintenance of traffic devices shall be commensurate with the work in progress.
 2. The closure durations listed are maximums and shall be consecutive days. Closure, reopening and closing again shall not be permitted.
 3. The weekend closures are 10:00PM Friday - 5:00AM Monday.
 4. Night or weekend closures only. Night time closures are 10:00PM - 5:00AM.
 Weekend closures are 10:00PM Friday - 5:00AM Monday.
 * Refer to the Lane Value Contract Table.

ITEM 614 SPECIAL - WORK ZONE TRAFFIC SIGNAL

UNDER THIS ITEM OF WORK, THE CONTRACTOR SHALL FURNISH, INSTALL, RELOCATE, MODIFY AND SUBSEQUENTLY REMOVE: TEMPORARY SIGNAL SUPPORTS, DOWN GUYS, GROUND RODS, SIGNAL CABLE, POWER CABLE, SERVICE CABLE, CONDUIT RISERS, MESSENGER WIRE, SIGNAL HEADS, COVERING OF VEHICULAR SIGNAL HEADS AND A TEMPORARY CONTROLLER AS NEEDED TO RENDER A FULLY FUNCTIONAL TEMPORARY SIGNALIZED INTERSECTION.

AS DETAILED WITHIN, TEMPORARY TRAFFIC SIGNALS OR TRAFFIC SIGNAL MODIFICATIONS TO ACCOMMODATE INDIVIDUAL MAINTENANCE OF TRAFFIC PHASES SHALL BE INSTALLED AT THE INTERSECTIONS LISTED BELOW.

ALL TEMPORARY TRAFFIC SIGNAL EQUIPMENT SHALL COMPLY WITH THE SPECIFICATIONS OUTLINED FOR THE PERMANENT SIGNAL INSTALLATION INCLUDING GROUNDING AND BONDING AND TRAFFIC SIGNAL PLAN AND SPECIFICATION COMPLIANCE. ALL METHODS OF TRAFFIC CONTROL SHALL BE APPROVED BY THE ENGINEER AND SHALL BE IN PLACE AND OPERATING PRIOR TO THE DEACTIVATION AND REMOVAL AND/OR RELOCATION OF ANY EXISTING SIGNAL EQUIPMENT. REFERENCE IS MADE TO THE REQUIREMENTS OF ITEM 614. ALL MODIFICATIONS TO SIGNALIZATION SHALL BE DONE UNDER THE PROTECTION OF A LAW ENFORCEMENT OFFICER. REFERENCE IS MADE TO ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

ANY VEHICULAR TRAFFIC SIGNAL HEAD THAT WILL BE OUT OF OPERATION SHALL BE COVERED IN ACCORDANCE WITH 632.25. ANY EXISTING VEHICULAR OR PEDESTRIAN HEAD THAT IS NOT FUNCTIONAL SHALL BE REMOVED IMMEDIATELY OR COVERED. ANY PEDESTRIAN BUTTONS NOT IN USE SHALL ALSO BE COVERED.

EACH TEMPORARY SIGNAL POLE LOCATION SHALL BE STAKED AND THE LOCATION APPROVED BY THE CITY OF COLUMBUS. THE CONTRACTOR MAY REUSE EXISTING SPAN AND PIGTAILS OR INSTALL NEW AS REQUIRED. THE CONTRACTOR SHALL TRANSFER EXISTING SIGNAL ITEMS AND EXTEND EXISTING CABLE AS NEEDED. WEATHERPROOF CABLE SPLICING IS PERMITTED. DOWN GUYS SHALL BE SPECIFIED FOR ALL TEMPORARY WOOD POLES. ONE DOWN GUY PER POLE SHALL BE USED FOR A LAYOUT THAT CONTAINS A MAXIMUM OF 2 VEHICULAR SIGNAL HEADS PER SPAN. TWO DOWN GUYS PER POLE SHALL BE SPECIFIED FOR 3 OR MORE VEHICULAR SIGNAL HEADS PER SPAN. DOWN GUYS SHALL BE POSITIONED TO COUNTERACT THE MOMENT CREATED BY THE SPAN CONFIGURATION. ANY CHANGE TO THE PLANNED POLE LOCATION OR SPAN CONFIGURATION AS DETAILED IN THE PLAN SHALL BE APPROVED BY THE CITY OF COLUMBUS. THE CONTRACTOR SHALL SUBMIT A DIAGRAM TO THE CITY DOCUMENTING PROPOSED CHANGES.

ITEM 614 SPECIAL - WORK ZONE TRAFFIC SIGNAL (CONTINUED)

INSTALL THE SPAN TO PROVIDE FOR A 5 TO 6 PERCENT SAG FOR WOOD POLES. ATTACH THE SPAN NO CLOSER THAN 2 FT. FROM THE TOP OF THE POLE. THE LOWEST VEHICULAR HEAD IN EACH DIRECTION SHALL BE 16.5 FT. ABOVE PAVEMENT SURFACE WITH THE REMAINING VEHICULAR HEADS MEETING THE REQUIREMENTS OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

THE CONTRACTOR SHALL SHIFT EXISTING SIGNAL HEADS TO ALIGN WITH LANES IN THE INDIVIDUAL MAINTENANCE OF TRAFFIC PHASES. DETAILED HEAD PLACEMENT HAS BEEN PROVIDED FOR EACH PHASE OF WORK IN THE MAINTENANCE OF TRAFFIC PLAN. THIS ITEM SHALL CONSIST OF ADJUSTING THE LOCATION OF TEMPORARY TRAFFIC SIGNAL HEADS FOR EACH PHASE OF CONSTRUCTION INCLUDING UNLASHING AND RELASHING ALL WIRING. ALL TEMPORARY AERIAL WIRING SHALL BE A MINIMUM OF 21 FT. ABOVE THE ROADWAY SURFACE.

VEHICULAR DETECTION SHALL BE MAINTAINED AT ALL TIMES AND DURING ALL PHASES OF CONSTRUCTION USING EITHER EXISTING LOOP DETECTORS OR TEMPORARY VIDEO OR RADAR DETECTION.

LOCATE THE NON-FUSED POWER SUPPLY VOLTAGE (120 VOLT) IN A SEPARATE CONDUIT. IN ADDITION, LOCATE THE LOOP DETECTOR, PUSH BUTTON, AND VIDEO DETECTION CABLES IN A SEPARATE CONDUIT FROM ALL OTHER CABLES.

THIS ITEM OF WORK SHALL INCLUDE ALL LABOR, EQUIPMENT AND MATERIAL NECESSARY TO PROVIDE POWER TO THE TRAFFIC SIGNAL CONTROLLER FROM THE PROPOSED OR EXISTING POWER SOURCES AS DETERMINED BY CONSTRUCTION SEQUENCING.

THIS ITEM OF WORK SHALL INCLUDE ALL LABOR, EQUIPMENT AND MATERIALS NECESSARY TO FURNISH, INSTALL, MODIFY, REMOVE, STORE, ERECT, RELOCATE, ADJUST AND REPAIR TEMPORARY TRAFFIC SIGNAL ITEMS AS DESCRIBED ABOVE.

ALL COSTS FOR THE ABOVE WORK SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 614 WORK ZONE TRAFFIC SIGNAL, AS PER PLAN AND SHALL BE PER EACH INTERSECTION.

NO.	DESCRIPTION	REV. BY	DATE
9	UPDATED NOTES	RPD	12-04-2023
10	ADDED TABLE	RPD	12-06-2023
10	UPDATED TABLE	ENR	12-22-2023

LANE VALUE CONTRACT TABLE

THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE AS DESIGNATED IN THE LANE VALUE CONTRACT TABLE FOR EACH UNIT OF TIME A LANE/SHOULDER/RAMP IS CLOSED BY THE CONTRACTOR'S ACTION WHILE NOT OTHERWISE PERMITTED BY THE LANE VALUE CONTRACT TABLE.

LANE VALUE CONTRACT TABLE						
FRA-70						
Section (SLM)	Existing Number of Lanes per Direction	Lane closures are NOT permitted:				Disincentive Amounts per minute per lane
		Lane Reduction	Mon to Fri	Sat	Sun	
Glenwood Avenue (12.41) to Sounder Ave (12.82)	3	3 to 2	5AM-9PM	7AM-9AM & 1PM-7PM	7AM-9AM & 1PM-7PM	\$370
		3 to 1	5AM-10PM	6AM-8PM	6AM-8PM	\$370
Sounder Ave (12.82) to Scioto River (13.41)	2	2 to 1	5AM-11PM	6AM-11PM	6AM-11PM	\$555
Scioto River (13.41) to Short Street (13.73)	3	3 to 2	5AM-9PM	7AM-9AM & 1PM-7PM	7AM-9AM & 1PM-7PM	\$370
		3 to 1	5AM-11PM	6AM-10PM	6AM-10PM	\$370
Short Street (13.73) to Grant Avenue (14.56)	3	3 to 2	5AM-9PM	6AM-10PM	6AM-10PM	\$360
		3 to 1	5AM-11PM	5AM-10PM	5AM-10PM	\$360
Grant Avenue (14.56) to Champion Street (15.60) (WB)	2	2 to 1	5AM-11PM	6AM-10PM	6AM-10PM	\$540
Grant Avenue (14.56) to 18th Street (15.24) (EB)	2	2 to 1	5AM-11PM	6AM-10PM	6AM-10PM	\$540
18th Street (15.24) to Alum Creek Drive (17.00) (EB)	4	4 to 3	5AM-9AM & 2PM-7PM	No Restriction	No Restriction	\$270
		4 to 2	8AM-8PM	11AM-7PM	11AM-7PM	\$270
		4 to 1	5AM-Midnight	7AM-Midnight	7AM-Midnight	\$270
Champion Street (15.60) to Alum Creek Drive (17.00) (WB)	4	4 to 3	5AM-9PM	No Restriction	No Restriction	\$265
		4 to 2	5AM-8PM	9AM-7PM	9AM-7PM	\$265
		4 to 1	5AM-11PM	6AM-11PM	6AM-11PM	\$265
Alum Creek Drive (17.00) to College Avenue (18.67) (EB)	4	4 to 3	5AM-9AM & 2PM-7PM	No Restriction	No Restriction	\$250
		4 to 2	8AM-8PM	11AM-7PM	11AM-7PM	\$250
		4 to 1	5AM-Midnight	7AM-Midnight	7AM-Midnight	\$250
Alum Creek Drive (17.00) to College Avenue (18.67) (WB)	3	3 to 2	5AM-8PM	9AM-7PM	9AM-7PM	\$335
		3 to 1	5AM-11PM	6AM-11PM	6AM-11PM	\$335
Short term shoulder closures are NOT permitted 5AM-9AM and 3PM-6PM Monday-Friday.						
FRA-71						
Section (SLM)	Existing Number of Lanes per Direction	Lane closures are NOT permitted:				Disincentive Amounts per minute per lane
		Lane Reduction	Mon to Fri	Sat	Sun	
Frank Road (12.79) to I-70 (15.26)	4	4 to 3	5AM-9AM & 3PM-6PM	No Restriction	No Restriction	\$335
		4 to 2	5AM-7PM	7AM-9AM & 2PM-7PM	7AM-9AM & 2PM-7PM	\$335
		4 to 1	5AM-11PM	6AM-11PM	6AM-10PM	\$335
I-70-West Split (15.26) to I-70-East Split (16.83)	See corresponding section on I-70 (SLM 13.43 to 14.78)					
I-70-East Split (16.83) to Main Street (17.13)	2	2 to 1	5AM-10PM	6AM-10PM	6AM-10PM	\$455

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WORKSITE TRAFFIC SUPERVISOR

SUBJECT TO APPROVAL OF THE ENGINEER, THE CONTRACTOR SHALL EMPLOY AND IDENTIFY (SOMEONE OTHER THAN THE SUPERINTENDENT) A PREQUALIFIED WORKSITE TRAFFIC SUPERVISOR (WTS) BEFORE STARTING WORK IN THE FIELD. THE WTS SHALL BE TRAINED IN ACCORDANCE WITH CMS 614.03, SHALL HAVE SUCCESSFULLY COMPLETED ODOT ADMINISTERED WTS TESTING (AND RE-TESTING WHEN APPLICABLE) AND BE LISTED ON THE ODOT PREQUALIFIED WTS ROSTER. PREQUALIFICATION EXPIRES EVERY 5 YEARS. RE-TESTING SHALL BE SUCCESSFULLY REPEATED EVERY 5 YEARS TO REMAIN PREQUALIFIED.

THE NAME OF THE PREQUALIFIED WTS AND RELATED 24-HOUR CONTACT INFORMATION SHALL BE PROVIDED TO THE ENGINEER AT THE PRECONSTRUCTION CONFERENCE. IF THE DESIGNATED WTS WILL NOT BE AVAILABLE FULL TIME (24/7), THE CONTRACTOR MAY DESIGNATE AN ALTERNATE (SECONDARY) WTS TO BE AVAILABLE WHEN THE PRIMARY IS OFF DUTY; HOWEVER THE PRIMARY WTS SHALL REMAIN THE POINT OF CONTACT AT ALL TIMES. ANY ALTERNATE (SECONDARY) WTS IS SUBJECT TO THE SAME TRAINING, PREQUALIFICATION AND OTHER REQUIREMENTS OUTLINED WITHIN THIS PLAN NOTE. AT ALL TIMES THE ENGINEER, OR ENGINEER'S REPRESENTATIVES, MUST BE INFORMED OF WHO THE PRIMARY WTS (AND SECONDARY WTS, IF APPLICABLE) IS AT THE CURRENT TIME.

THE WTS POSITION HAS THE PRIMARY RESPONSIBILITY OF IMPLEMENTING THE TRAFFIC MANAGEMENT PLAN (TMP), MONITORING THE SAFETY AND MOBILITY OF THE ENTIRE WORK ZONE, AND CORRECTING TEMPORARY TRAFFIC CONTROL (TTC) DEFICIENCIES FOR THE ENTIRE WORK ZONE. THE WTS, AND ALTERNATE WTS WHEN ON DUTY, SHALL HAVE SUFFICIENT AUTHORITY TO EFFECTIVELY CARRY OUT THE IDENTIFIED WTS RESPONSIBILITIES AND DUTIES. THE DUTIES OF THE WTS ARE AS FOLLOWS:

1. BE AVAILABLE ON A 24-HOUR PER DAY BASIS.
2. BE ON SITE FOR ALL EMERGENCY TTC NEEDS WITHIN ONE HOUR OF NOTIFICATION BY POLICE OR PROJECT STAFF, AND EFFECT CORRECTIVE MEASURES IMMEDIATELY ON EXISTING WORK ZONE TTC DEVICES.
3. ATTEND PRECONSTRUCTION MEETING AND ALL PROJECT MEETINGS WHERE TTC MANAGEMENT IS DISCUSSED.
4. BE AVAILABLE ON SITE FOR OTHER MEETINGS OR DISCUSSIONS WITH THE ENGINEER UPON REQUEST.
5. BE AWARE OF ALL EXISTING AND PROPOSED TTC OPERATIONS OF THE CONTRACTOR, SUBCONTRACTORS AND SUPPLIERS, AND ENSURE COORDINATION OCCURS BETWEEN THEM TO ELIMINATE CONFLICTING TEMPORARY AND/OR PERMANENT TRAFFIC CONTROL.
6. COORDINATE PROJECT ACTIVITIES WITH ALL LAW ENFORCEMENT OFFICERS (LEOS). THE WTS SHALL ALSO BE THE MAIN CONTACT PERSON WITH THE LEOS WHILE LEOS ARE ON THE PROJECT.
7. COORDINATE AND FACILITATE MEETINGS WITH ODOT PERSONNEL, LEOS AND OTHER APPLICABLE ENTITIES BEFORE EACH PLAN PHASE SWITCH TO DISCUSS THE WORK ZONE TTC FOR IMPLEMENTING THE PHASE SWITCH. SUBMIT A WRITTEN DETAIL OF MOT OPERATIONS AND SCHEDULE OF EVENTS TO IMPLEMENT THE SWITCH BETWEEN PHASE PLANS TO THE ENGINEER 5 CALENDAR DAYS PRIOR TO THIS MEETING.
8. BE PRESENT, ON SITE FOR, AND INVOLVED WITH, EACH TTC SET UP/TAKE DOWN AND EACH PHASE CHANGE IN ACCORDANCE WITH CMS 614.03.
9. ON A CONTINUAL BASIS ENSURE THAT THE TTC ZONE AND ALL RELATED DEVICES ARE INSTALLED, MAINTAINED AND REMOVED IN COMPLIANCE WITH THE CONTRACT DOCUMENTS.
10. ON A CONTINUAL BASIS FACILITATE CORRECTIVE ACTION(S) NECESSARY TO BRING DEFICIENT TTC ZONES AND ALL RELATED DEVICES INTO COMPLIANCE WITH CONTRACT DOCUMENTS IN THE TIMEFRAME DETERMINED BY THE ENGINEER.
11. INSPECT, EVALUATE, PROPOSE NECESSARY MODIFICATIONS TO, AND DOCUMENT THE EFFECTIVENESS OF, THE TTC DEVICES AND TRAFFIC OPERATIONS ON A DAILY BASIS (7 DAYS A WEEK). IN ADDITION, PERFORM ONE WEEKLY NIGHT INSPECTION OF THE WORK ZONE SETUP FOR DAYTIME WORK OPERATIONS; AND ONE DAYTIME INSPECTION PER WEEK FOR NIGHTTIME PROJECTS. THIS SHALL INCLUDE (BUT NOT BE LIMITED TO) DOCUMENTATION ON THE FOLLOWING PROJECT EVENTS:
 - A. INITIAL TTC SETUP (DAY AND NIGHT REVIEW).
 - B. DAILY TTC SETUP AND REMOVAL.
 - C. WHEN CONSTRUCTION STAGING CAUSES A CHANGE IN THE TTC SETUP.

- D. CRASH OCCURRENCES WITHIN THE CONSTRUCTION AREA AND WITHIN THE INFLUENCE AREA(S) APPROACHING THE WORK ZONE.
- E. REMOVAL OF TTC DEVICES AT THE END OF A PHASE OR PROJECT.
- F. ALL OTHER EMERGENCY TTC NEEDS.

12. COMPLETE THE DEPARTMENT APPROVED LONG TERM INSPECTION FORM (CA-D-8) AFTER EACH INSPECTION AS REQUIRED IN #1) AND SUBMIT IT TO THE ENGINEER THE FOLLOWING WORKDAY. THESE REPORTS SHALL INCLUDE A CHECKLIST OF ALL TTC MAINTENANCE ITEMS TO BE REVIEWED. A COPY OF THE FORM WILL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING. ANY DEFICIENCIES OBSERVED SHALL BE NOTED, ALONG WITH RECOMMENDED OR COMPLETED CORRECTIVE ACTIONS AND THE DATES BY WHICH SUCH CORRECTIONS WERE, OR WILL BE, COMPLETED. A COPY OF THE CURRENT CA-D-8 DOCUMENT CAN BE FOUND ON THE OFFICE OF CONSTRUCTION ADMINISTRATION'S INSPECTION FORMS WEBSITE.

13. HAVE COPIES OF THE ODOT TEMPORARY TRAFFIC CONTROL MANUAL AND CONTRACT DOCUMENTS AVAILABLE AT ALL TIMES ON THE PROJECT.

THE DEPARTMENT WILL DEDUCT:

A. THE PRORATED DAILY AMOUNT OF ITEM 614 MAINTAINING TRAFFIC FOR ANY DAY IN WHICH THE WTS FAILS TO PERFORM THE DUTIES SET FORTH ABOVE. THE PRORATED DAILY AMOUNT WILL BE EQUAL TO THE ORIGINAL BID AMOUNT FOR ITEM 614 MAINTAINING TRAFFIC DIVIDED BY THE DIFFERENCE BETWEEN THE ORIGINAL COMPLETION DATE AND THE FIRST DAY OF WORK, IN CALENDAR DAYS.

B. 1% OF THE ORIGINAL BID AMOUNT FOR ITEM 614 MAINTAINING TRAFFIC FOR ANY DAY THAT A TTC ISSUE IS IDENTIFIED IN THE FIELD AND IS NOT CORRECTED IN THE GIVEN TIMEFRAME PER THE ENGINEER. DEDUCTION B SHALL NOT APPLY TO SITUATIONS COVERED BY DEDUCTION C.

C. 1% OF THE ORIGINAL BID AMOUNT FOR ITEM 614 MAINTAINING TRAFFIC FOR ANY DAY THAT A LANE OR RAMP IS BLOCKED (FULLY OR PARTIALLY) WITHOUT TTC, AS DETERMINED BY THE ENGINEER. THIS DEDUCTION SHALL BE IN ADDITION TO ANY OTHER DISINCENTIVES ESTABLISHED FOR UNAUTHORIZED LANE USE.

FOR DAYS IN WHICH MORE THAN ONE DEDUCTION LISTED ABOVE OCCUR, THE HIGHEST DEDUCTION AMOUNT WILL APPLY.

IF THREE OR MORE TOTAL DAYS RESULT IN TTC ISSUES DESCRIBED IN DEDUCTION B OR C ABOVE, THE PRIMARY WTS SHALL BE IMMEDIATELY REMOVED FROM THE WORK IN ACCORDANCE WITH C&MS 108.05. UPON REMOVAL THE ENGINEER SHALL NOTIFY ODOT CENTRAL OFFICE (WTSPREQUALIFICATION@DOT.OHIO.GOV) TO REGISTER A REMOVAL AGAINST THE STATEWIDE PREQUALIFICATION FOR THE PRIMARY WTS. THREE REMOVALS SHALL CAUSE STATEWIDE DISQUALIFICATION FOR ANY PREVIOUSLY PREQUALIFIED WTS.

PAYMENT FOR THE ABOVE REQUIREMENTS, RESPONSIBILITIES AND DUTIES SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC.

TRAFFIC INCIDENT MANAGEMENT (TIM) DURING MOT

OHIO TIM IS OHIO'S TRAFFIC INCIDENT MANAGEMENT PROGRAM WHICH IS COMMITTED TO MAINTAINING THE SAFE AND EFFECTIVE FLOW OF TRAFFIC DURING EMERGENCIES AS TO PREVENT FURTHER DAMAGE, INJURY OR UNDUE DELAY OF THE MOTORING PUBLIC. IN ADDITION TO COMPLYING WITH THE PROVISION OF OMUTCD CHAPTER 6I, CONTROL OF TRAFFIC THROUGH TRAFFIC INCIDENT MANAGEMENT AREAS, THE CONTRACTOR SHALL ACTIVELY PARTICIPATE IN TIM PLANNING AND IMPLEMENTATION AS OUTLINED BELOW.

1. SUPERINTENDENT SHALL IDENTIFY THE INDIVIDUAL PERSONS ON THE PROJECT WHO WILL, OR MAY NEED TO, PERFORM THE DUTIES HEREIN. AT A MINIMUM, INCLUDE THE SUPERINTENDENT, FOREMEN AND SUPERVISORS (OR EQUIVALENT) AS WELL AS THE WORKSITE TRAFFIC SUPERVISOR (WTS; IF APPLICABLE TO THE PROJECT). THESE INDIVIDUALLY IDENTIFIED PERSONS SHALL COLLECTIVELY BE KNOWN AS CONTRACTOR TRAFFIC INCIDENT MANAGEMENT (TIM) CONTACTS. NOTIFY THE PROJECT ENGINEER OF THE CONTRACTOR TIM CONTACTS (ALONG WITH CONTACT INFORMATION FOR EACH) AT OR BEFORE THE PRECONSTRUCTION MEETING.

2. SUPERINTENDENT SHALL NOTIFY THE ENGINEER IMMEDIATELY IF ANY CONTRACTOR TIM CONTACT IS ADDED, REMOVED OR THE CONTACT INFORMATION CHANGES OVER THE COURSE OF THE PROJECT.

3. PRIOR THE FIRST DAY OF WORK IN THE FIELD, EACH CONTRACTOR TIM CONTACT ON THE PROJECT SHALL HAVE ATTENDED AND SUCCESSFULLY COMPLETED OHIO TIM TRAINING PROVIDED BY THE DEPARTMENT OR DESIGNEE. TRAINING INFORMATION CAN BE FOUND AT WWW.OHIOTIM.COM.

4. SUPERINTENDENT, AT A MINIMUM, SHALL ATTEND AND ACTIVELY PARTICIPATE IN A DEPARTMENT SCHEDULED TIM MEETING BEFORE CONSTRUCTION WORK BEGINS AND BEFORE EACH PHASE CHANGE. THESE MEETINGS WILL RESULT IN A DEPARTMENT ISSUED PROJECT SPECIFIC TRAFFIC INCIDENT MANAGEMENT PLAN (TIMP). AT THE TIM MEETINGS THE ATTENDING CONTRACTOR TIM CONTACTS SHALL:

- A. COLLABORATE WITH ODOT AND SAFETY FORCES;
- B. SHARE PROJECT SPECIFIC DETAILS THAT IMPACT TIM RESPONDERS; AND
- C. RECOMMEND WAYS TO INCORPORATE NECESSARY EMERGENCY ACCESS AND OTHER TIM ELEMENTS FOR TIM RESPONDERS GIVEN PROJECT SPECIFIC WORK BEING COMPLETED AND PROJECT SPECIFIC PHASING.

5. CONTRACTOR TIM CONTACTS SHALL IMPLEMENT COMPONENTS OF THE RESULTING TIMP (SUCH AS APPROVED EMERGENCY INGRESS/EGRESS POINTS, ETC), AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH 109.05.

6. CONTRACTOR TIM CONTACTS SHALL PERFORM, AT A MINIMUM, THE FOLLOWING FUNCTIONS WHEN AN INCIDENT/CRASH OCCURS:

- A. IF OBSERVED OR PRESENT WHEN OCCURS, CALL 911 AND THEN NOTIFY THE TRAFFIC MANAGEMENT CENTER (TMC) TO PROVIDE THE FOLLOWING:
 - I. LOCATION, INCLUDING MILEPOST NUMBER AND DIRECTION OF TRAVEL
 - II. NUMBER AND TYPE OF VEHICLES INVOLVED, IF KNOWN
 - III. ESTIMATED EXTENT OF DAMAGE OR INJURY, IF KNOWN
 - IV. ESTIMATED NUMBER OF PATIENTS INVOLVED, IF KNOWN
 - V. ANY POTENTIAL HAZARDOUS CONDITIONS, IF KNOWN
 - VI. THE PLACARD NUMBER ON ANY HAZARDOUS MATERIALS PLACARD FROM A SAFE DISTANCE, IF APPLICABLE AND VISIBLE
- B. FOLLOWING AN INCIDENT/CRASH:
 - I. INITIATE TRAFFIC MANAGEMENT/PROVIDE TEMPORARY TRAFFIC CONTROL AS INDICATED IN THE TIMP, AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH 109.05.
 - II. RECOMMEND ROADWAY REPAIR NEEDS.
 - III. PROVIDE REPAIR RESOURCES AND INITIATE REPAIRS, AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH 109.05.
 - IV. ATTEND AND PARTICIPATE IN AN AFTER ACTION REVIEW (AAR).

THE TRAFFIC MANAGEMENT CENTER (TMC) CONTACT PERSONNEL ARE THE AM SUPERVISOR TODD SEITER AND PM SUPERVISOR DOMINIC DELCOL. THEY CAN BE REACHED AT 614-387-2438 OR 800-884-4030.

ALL COSTS, UNLESS OTHERWISE SPECIFIED, RESULTING FROM THE ABOVE REQUIREMENTS SHALL BE CONSIDERED TO BE INCLUDED IN THE LUMP SUM PRICE FOR ITEM 614, MAINTAINING TRAFFIC. FAILURE TO PERFORM THE REQUIREMENTS OF THIS PLAN NOTE WILL RESULT IN A DAILY FINE OF 2% OF ITEM 614, MAINTAINING TRAFFIC AND MAY RESULT IN ONE OR MORE CONTRACTOR TIM CONTACTS BEING REMOVED FROM THE LIST OF OHIO TIM TRAINED INDIVIDUALS (AT THE SOLE DISCRETION OF THE OHIO TIM EXECUTIVE COMMITTEE). IN THE EVENT AN INDIVIDUAL IS REMOVED FROM THE OHIO TIM TRAINED LIST, THE INDIVIDUAL WILL BE REMOVED FROM CONTRACTOR TIM CONTACT RESPONSIBILITIES ON ALL PROJECTS.

APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION(S)

PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE APPROVED MOT EXCEPTION(S) PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

APPROVED MOT EXCEPTION(S) INCLUDE:
 - CLOSURE OF 315SB TO 70EB FOR 3 YEARS FOR 77372
 - CLOSURE OF POWB TO 315NB FOR 6 MONTHS IN TOTAL
 - MONITOR TRAFFIC CONDITIONS FOR POSSIBLE CONFIGURATION ADJUSTMENTS AT THE 670EB TO 71SB DETOUR RAMP

A MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF 30 CALENDAR DAYS PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER AND CITY OF COLUMBUS AS WELL AS THE CONTRACTOR, WORKSITE TRAFFIC SUPERVISOR (WTS) AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.

IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO THE OFFICE OF ROADWAY ENGINEERING, STATEWIDE TMC, DWZTM AND SPECIAL HAULING PERMITS AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE. REFERENCE "EXCEPTION REQUEST APPROVAL DATED 01/24/2023 FOR PID 77372" IN THE NOTIFICATION AND OTHER CORRESPONDENCE.

ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTION(S) LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE MOT EXCEPTION COMMITTEE (MOTEC). IN THE EVENT THAT SUCH CHANGES ARE PROPOSED, THE REQUEST SHALL BE COORDINATED THROUGH THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM) A MINIMUM OF 30 CALENDAR DAYS PRIOR TO THE DESIRED IMPLEMENTATION DATE. IF THE DISTRICT AGREES WITH THE PROPOSED CHANGES THE DWZTM SHALL SEEK APPROVAL FROM THE MOTEC. IN THE EVENT THE PROPOSED CHANGES ARE APPROVED IN WRITING, THE CLOSURES ARE STILL SUBJECT TO NOTIFICATION REQUIREMENTS WITHIN THIS NOTE PRIOR TO IMPLEMENTATION.

NOTIFICATIONS DURING CLOSURE REQUIRED
 A DESIGNATED ON-SITE POINT OF CONTACT SHOULD COMMUNICATE WITH THE TMC AS THE STATUS OF THE CLOSURE CHANGES.
 CONTACT THE TMC:
 - IF THE CLOSURE IS POSTPONED OR CANCELLED
 - AT THE TIME THE CLOSURE IS IMPLEMENTED
 - AT THE TIME THE CLOSURE IS REMOVED AND ALL LANES RESTORED
 - IF THE CLOSURE WILL NOT BE OPENING ON TIME

CONTACT CAN BE MADE WITH THE TMC IN THE FOLLOWING WAYS:
 - PHONE: 1-614-387-2438 OR 1-800-884-4030
 - EMAIL: STATEWIDETMC@DOT.OHIO.GOV
 - RADIO: XDOT MAIN

NO.	DESCRIPTION	REV. BY	DATE
9	UPDATED NOTES	RPD	12-04-2023
14	UPDATED NOTES	RPD	12-20-2023

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SHEET NUMBER								PARTICIPATION				ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	CALCULATED CJC	CHECKED CWL
P1/65	P1/163	P2/40	P3/197B	P4/49	P4/158			01/IMS/04	02/IMS/11	03/NHS/10	04/NHS/10								
MAINTENANCE OF TRAFFIC (CONTINUED)																			
LS				LS 4600				LS 4600					615	10000	LS				ROADS FOR MAINTAINING TRAFFIC
								4032					615	20000	4600	SY			PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A
								4032					615	20001	4032	SY			PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN
1545		695	4032	629				2869					615	25000	2869	SY			PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B
100		100		200				400					615	25001	400	SY			PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN, TYPE 1
50		50		200				300					615	25001	300	SY			PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN, TYPE 2
20			20	200				240					615	25001	240	SY			PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN, TYPE 3
20				200				220					615	25001	220	SY			PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN, TYPE 4
325				550				875					616	10000	875	MGAL			WATER
			4					4					622	10201	4	EACH			BARRIER TRANSITION, AS PER PLAN
			7279					7279					622	41011	7279	FT			PORTABLE BARRIER, 50", AS PER PLAN
1				2				3					622	41050	3	EACH			PORTABLE BARRIER, "Y" CONNECTOR
16790			11575					28365					622	41100	28365	FT			PORTABLE BARRIER, UNANCHORED
				29484				29484					622	41101	29484	FT			PORTABLE BARRIER, UNANCHORED, AS PER PLAN
1030								1030					622	41110	1030	FT			PORTABLE BARRIER, ANCHORED
72			288	48				408					808	18700	408	SNMT			DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY
32								32					829	00100	32	SNMT			WORK ZONE EGRESS WARNING SYSTEM
108				48				156					896	00010	156	SNMT			PORTABLE NON-INTRUSIVE TRAFFIC SENSOR, CLASS I
				48				48					896	00020	48	SNMT			PORTABLE CHANGEABLE MESSAGE SIGN
36								36					896	00021	36	SNMT			PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN
INCIDENTALS																			
143000								143000					100	51100	143000	EACH			DEPARTMENT'S SHARE OF THE DISPUTE RESOLUTION BOARD
LS								LS					108	10000	LS				CPM PROGRESS SCHEDULE
32000								32000					SPECIAL	1110100	32000	EACH			DEPARTMENTS SHARE FACILITATED PARTNERING COSTS
LS			LS	LS				LS					614	11000	LS				MAINTAINING TRAFFIC
56			LS	LS				56					619	16021	56	MNTH			FIELD OFFICE, TYPE C, AS PER PLAN
LS		LS	LS	LS				LS					623	10000	LS				CONSTRUCTION LAYOUT STAKES AND SURVEYING
LS		LS	LS	LS				LS					624	10000	LS				MOBILIZATION
675000								225000	225000	225000			900	00100	675000	EACH			RAILROAD FLAGGING SERVICES

NO.	DESCRIPTION	REV. BY	DATE
1	ADDED RR FLAGGING	CWL	10-2-23
2	BRICK X-WALK/REV. RR FLAGGING	CWL	10-16-23
5	REVISED FB	CWL	11-6-23
6	REVISED ME10100	CWL	11-9-23
11	ADDED 619E16021	CWL	12-7-23
14	REVISED PART 4	CWL	12-28-23

BIG BUILD MASTER GENERAL SUMMARY

FRA-70-13.11

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12/28/2023
9:14:06 AM
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SPECIFICATIONS

UNLESS NOTED OTHERWISE, CONSTRUCTION SHALL CONFORM TO THE OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS, 2016 EDITION OR LATEST VERSION, WHICHEVER IS MORE RECENT.

PADLOCKS AND KEYS

PADLOCKS FURNISHED SHALL BE EITHER BRASS OR BRONZE, EQUAL TO MASTER NO. 4BKA OR WILSON BOHANNAN 660A, AND SHALL BE KEYED IN ACCORDANCE WITH CMS 631.06. PAYMENT SHALL BE INCLUDED IN THE BID FOR THE ITEM(S) BEING LOCKED.

CONDUIT EXPANSION AND DEFLECTION

EXPANSION FITTINGS SHALL BE OZ TYPE AX, CROUSE HINDS TYPE XJG, APPLETON TYPE AX, OR EQUAL APPROVED BY THE ENGINEER. EACH EXPANSION FITTING SHALL PROVIDE EITHER 4 OR 8 INCHES (100 OR 200 MILLIMETERS) TOTAL MOVEMENT AS SPECIFIED BY THE PLAN DETAILS AND SHALL HAVE AN EXTERNAL COPPER BONDING JUMPER, UNLESS SPECIFIED OTHERWISE BY THE PLAN DETAILS. DEFLECTION COUPLINGS SHALL BE OZ TYPE DX, CROUSE HINDS TYPE XD, APPLETON TYPE DF, OR EQUAL APPROVED BY THE ENGINEER. EACH DEFLECTION COUPLING SHALL HAVE AN EXTERNAL COPPER BONDING JUMPER, UNLESS SPECIFIED OTHERWISE BY THE PLAN DETAILS.

ITEM 625 - LIGHTING MISC: DECORATIVE LIGHTING CONTROL CABINET

ITEMS "LIGHTING MISC" SHALL CONSIST OF PROVIDING A CONTROL CABINET ENCLOSURE, LIGHTING CONTROL SERVER, DMX LIGHTING CONTROL DEVICES, POWER RELAY MODULE AND FIBER COMMUNICATION DEVICES AS DETAILS ON SHEET 185 TO 187

THE CENTRAL LIGHTING CONTROLLER:
IT SHALL BE CAPABLE TO PROCESS LIGHT OUTPUT DATA FOR UP TO 15,000 LED NODES OR 50,000 PIXEL. WITH WINDOW DOWN 10 OR NEWER OPERATING SYSTEM.
POWER SUPPLY VOLTAGE: 100-240VAC 50/60HZ

THE CENTRAL CONTROLLER SHALL UTILIZE THE DMX-512 PROTOCOL.
THE FOLLOWING CENTRAL PROGRAMMABLE CONTROLLERS ARE APPROVED BY CITY OF COLUMBUS:

OSRAM E:CUE LCE3
COLOR KINETICS LSM GEN6
LOCATE THE CENTRAL LIGHTING CONTROLLER IN A CLIMATE-CONTROLLED ROOM AS DETAILED IN THE PLANS AND EASILY ACCESSIBLE TO BOTH LIGHTING AND INTERNET/MANAGEMENT NETWORKS.

BE ADVISED THAT SUBMITTAL, PURCHASE AND INSTALLATION OF THE LIGHTING CONTROL SERVER UNIT AT COLUMBUS CITY HALL SHALL BE WITHIN THE FIRST 6 MONTHS AFTER THE START OF THE PROJECT.

DMX CONTROL DEVICES:
IT SHALL BE CAPABLE OF PROCESSING UP TO 8 CHANNEL FOR DMX512 WITH POWER OVER INTERNET OPTION. FLEXIBLE MOUNTED ON THE DIN RAIL.

POWER RELAY DEVICES:
IT SHALL BE COMPITABLE WITH LIGHTING CONTROL SOFTWARE INCLUDING THE AUTO SWITCHING INTERFACE-ASTRONOMICAL TIME CLOCK PROGRAMING FUNCTION. SUPPLY POWER SHALL BE VOLTAGE VIA AN EXTERNAL ADPATER OR THROUGH POE. EASILY MOUNTED ON THE DIN RAIL.

FIBER COMMUNICATION DEVICES:
WEB SERVER ACCESS (WHEN REQUIRED) SHALL BE SECURE HTTP (HTTPS) MEETING ALL APPLICABLE DOT NETWORK SPECIFICATIONS FOR INTELLIGENT TRANSPORTATION SYSTEMS (ITS).

PAYMENT WILL BE MADE AT UNIT BID UNDER SPECIAL ITEM"
"LIGHTING MISC: DECORATIVE LIGHTING CONTROL CABINET SHALL BE FULL COMPENSATION FOR ALL LABOR, MATERIALS AND INCIDENTALS REQUIRED TO COMPLETE THIS ITEM IN A SATISFACTORY AND WORKMANLIKE MANNER.

ITEM 625 - LIGHTING MISC: DECORATIVE LIGHTING POWER SERVICE

ITEMS "LIGHTING MISC" SHALL CONSIST OF A POWER SERVICE CENTER CABINET AND POWER SERVICE HARDWARE AS DETAILS ON SHEET 185 TO 187

THE 100AMP 240V SINLE PHASE METER IS FM2S BY ITRON PART#SSISID ATTACHED TO SINGLE POSITION METER SOCKET BY ANCHOR PART#URS1304-E. ALL ASSEMBLY SHALL BE INSTALLED ON HINGE SIDE OF CONTROLLER. THE METER AND METER ASSEMBLY SHALL BE INSTALLED PRIOR TO ENERGIZING THE CONTROLLER.

METER SOCKET AND COVER SHALL BE GROUNDED PER NEC SPECIFICATIONS.

THE REQUIRED ARC FLASH LABEL SHALL BE AFFIXED TO THE FACE OF THE METER SOCKET.

PAYMENT WILL BE MADE AT UNIT BID UNDER SPECIAL ITEM"
"LIGHTING MISC: DECORATIVE LIGHTING POWER SERVICE SHALL BE FULL COMPENSATION FOR ALL LABOR, MATERIALS AND INCIDENTALS REQUIRED TO COMPLETE THIS ITEM IN A SATISFACTORY AND WORKMANLIKE MANNER.

DECORATIVE LIGHTING LEGEND	
L1 	LUMINAIRE, DECORATIVE, RECESSED WALL LIGHT, 3.8W, 329 LUMENS, 240V, APP RECESSED WALL LIGHT WITH HOUSING CAST INTO PRECAST PLANTER WALLS, WITH INTEGRAL DRIVER. BEGA MODEL #33 166, 3.8W, 240V
L2 	LUMINAIRE, DECORATIVE, LED SEAT WALL LIGHT, 3W/FT, 121 LUMENS PER FEET, 24V DC, APP LED LIGHT FIXTURE, MOUNTED TO UNDERSIDE OF PRECAST SEATWALL, WITH REMOTE DRIVER IN QUAZITE PULL BOX (LOCATION AS NOTED). KENDO M WET, MODEL #KMW-XX-30K-SO-F-FC-BK/PDCU-W-3X96W-24 OR PDCU-W-96W-24 (AS NOTED)
J 	8" X 8" X 12" PLANTER PULL BOX, 725.06, UNLESS NOTED OTHERWISE,
P 	11"X18"X12" PLANTER PULL BOX, 725.06, WITH SEAT WALL LIGHTING FIXTURE EXTERIOR POWER PDCU-W
W 	WET RATED 8"X6"X4" WALL JUNCTION BOX
HA NO.4 	PROPOSED LIGHTING CONDUIT, 725.04, (SIZE AS NOTED) WITH LIGHTING CIRCUIT CONDUCTORS (SIZE AS NOTED). LABEL INDICATES CIRCUIT NAME AND CONDUCTOR SIZE.
CAT6 	CAT 6 CABLES IN 1" CONDUIT OR 2" CONDUIT, 725.04 (AS NOTED)

ITEM 625 - RGBW AESTHETIC SCREEN WALL LIGHTING

THIS ITEM CONSISTS OF SUPPLYING, INSTALLING, TESTING, AND PROVIDING TRAINING FOR AN AESTHETIC LIGHTING , ACCORDING TO THE DETAILS SHOWN IN THE PLANS.

ITEM 625 RGBW AESTHETIC LIGHTING SYSTEM IS PAID FOR BY EACH INSTANCE (TYPICALLY EACH SIDE OF SCREENWALL STRUCTURE TO BE LIGHTED), AND INCLUDES THE FOLLOWING ITEMS: CAT6 WIRING, LEADER CABLES AND JUMPER CABLES WIRING, CONDUIT AND FITTINGS, DATA INJECTORS, COMMUNICATION AND WIRELESS LINKS.

TRAINING:

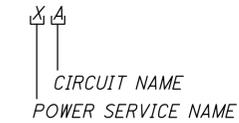
THE CONTRACTOR SHALL ARRANGE A MINIMUM ONE-DAY (4-7 CONTACT HOURS) TRAINING SESSION ON THE OPERATION OF THE SYSTEM. COMPLEX SYSTEMS MAY REQUIRE MORE THAN ONE DAY.

TESTING:

1. MAXIMUM LUMINANCE TEST:
USING A PHOTOMETER MEASURING IN UNITS OF CD/M², DEMONSTRATE TO THE ENGINEER DURING NIGHT TESTING THAT THE PROGRAMMED, OPERATIONAL LIGHTING SYSTEM MEETS THE MAXIMUM SURFACE LUMINANCE CRITERIA:
AESTHETIC LIGHTING SYSTEM WITH A SOFTWARE OR HARDWARE LIMIT TO THE WHITE-LIGHT SURFACE LUMINANCE OF NO MORE THAN 100 CD/M² IN URBAN/SUBURBAN AREAS AREAS AT ANY POINT OF AN ILLUMINATED SURFACE OVER OR DIRECTLY ADJACENT TO THE ROADWAY.
2. BURN-IN TEST:
FOLLOWING THE MAXIMUM LUMINANCE TEST, OPERATE THE SYSTEM FOR AT LEAST FOURTEEN DAYS WITHOUT ANY MAINTENANCE INTERVENTION.

SEE ODOT TEM 1142-26 FOR MORE INFORMATION & REQUIREMENT.

POWER SERVICE INFORMATION



ITEM 625 MISC.: PULL BOX 13"x24"

PULL BOX SHALL BE SIMILAR IN MATERIAL AND SPECIFICATION TO ODOT 725.06, EXCEPT THE DIMENSIONS SHALL BE 13"x24"x18".

ITEM 625 - LIGHTING MISC.: SERVICE TO FRONT STREET BRIDGE TRELIS LIGHTING

THIS ITEM SHALL CONTINUE TO PROVIDE A COMPLETE ELECTRICAL WORK THAT WAS NON-PERFORMED ON 4R/6R FOR THE FRONT STREET BRIDGE TRELIS DECORATIVE LED LIGHTING SYSTEM, EXCEPT FOR LUMINAIRES.

ALL LABOR AND COORDINATION REQUIRED TO INSTALL THIS DECORATIVE LED LIGHTING SYSTEM, INCLUDING BUT NOT LIMITED TO CONCEALING CONDUIT BEHIND OR IN STRUCTURAL ELEMENTS, SECURING CONDUIT, BOXES, DISCONNECT SWITCHES TO WALLS OR STRUCTURES AND COORDINATING FOR THE CASTING OF JUNCTION BOXES IN WALLS OR OTHER STRUCTURES SHALL BE THE RESPONSIBILITY OF THIS CONTRACTOR AND SHALL BE INCLUDED AS PART OF THE SERVICE TO DECORATIVE LIGHTING.

PAYMENT WILL BE MADE AT THE UNIT BID PRICE UNDER CMS ITEM 625 "LIGHTING MISC.: SERVICE TO FRONT STREET BRIDGE TRELIS LIGHTING" FOR EACH TRELIS INSTALLED, WHICH SHALL BE FULL COMPENSATION FOR ALL LABOR, MATERIALS AND INCIDENTALS REQUIRED TO COMPLETE THIS ITEM IN A SATISFACTORY AND WORKMANLIKE MANNER.

ITEM 625 MISC.: PULL BOX 11"x18"

PULL BOX SHALL BE SIMILAR IN MATERIAL AND SPECIFICATION TO ODOT 725.06, EXCEPT THE DIMENSIONS SHALL BE 11"x18"x12".

ITEM 625 MISC.: FIBER OPTIC CABLE, 24 STRAND

THE CONTRACTOR SHALL PROVIDE ALL MATERIALS REQUIRED FOR THE INSTALLATION, CONNECTORIZATION, AND SPLICING OF THE SPECIFIED COMMUNICATIONS CABLES. THE 24-STRAND CABLE SHALL BE CORNING PART NUMBER 024EU4-T4101D20 OR APPROVED BY CITY OF COLUMBUS DOT

ITEM 625 - LIGHTING MISC.: SERVICE TO DECORATIVE LIGHTING

LIGHTING MISC.: SERVICE TO DECORATIVE LIGHTING, AS PER PLAN SHALL PROVIDE A COMPLETE ELECTRICAL SYSTEM FOR THE DECORATIVE LED LIGHTING SYSTEM, EXCEPT FOR LUMINAIRES. ALL POWER CONTROL ENCLOSURES, POWER CONTROL HARDWARE, 24VDC POWER DRIVERS, CONDUIT, CONDUIT GROUNDING, MOUNTINGS, FITTINGS, JUNCTION BOXES, DISCONNECT SWITCHES, POWER CABLES AND WIRING AND ANY OTHER EQUIPMENT OR INCIDENTALS NECESSARY TO COMPLETE THE INSTALLATION ARE INCLUDED AS PART OF THE LIGHTING MISC.: SERVICE TO DECORATIVE LIGHTING.

ALL LABOR AND COORDINATION REQUIRED TO INSTALL THE DECORATIVE LED LIGHTING SYSTEM, INCLUDING BUT NOT LIMITED TO CONCEALING CONDUIT BEHIND OR IN STRUCTURAL ELEMENTS, SECURING CONDUIT, BOXES, DISCONNECT SWITCHES TO WALLS OR STRUCTURES AND COORDINATING FOR THE CASTING OF JUNCTION BOXES IN WALLS OR OTHER STRUCTURES SHALL BE THE RESPONSIBILITY OF THIS CONTRACTOR AND SHALL BE INCLUDED AS PART OF THE SERVICE TO DECORATIVE LIGHTING.

PULL BOXES AND STRUCTURE JUNCTION BOXES UTILIZED IN SERVICE OF THE DECORATIVE LIGHTING SYSTEM SHALL BE LABELED WITH 'LIGHTING' ON THE COVER OF THE PULL BOX.

PAYMENT WILL BE MADE AT THE UNIT BID PRICE UNDER CMS ITEM 625 "LIGHTING MISC.: SERVICE TO DECORATIVE LIGHTING" FOR EACH DECORATIVE LIGHTING SYSTEM INSTALLED, WHICH SHALL BE FULL COMPENSATION FOR ALL LABOR, MATERIALS AND INCIDENTALS REQUIRED TO COMPLETE THIS ITEM IN A SATISFACTORY AND WORKMANLIKE MANNER.

CALCULATED
JCS
CHECKED
LH

ODOT - DECORATIVE LIGHTING GENERAL NOTES

FRA-70-14.05C

LANDSCAPE AND SOIL SUMMARY - HIGH STREET BRIDGE

PLAN SHEET NUMBER				PARTICIPATION 06/MPO/OT	ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	DETAIL SHEET NO.
325	326	332	333							
		226	316	542	660	E30001	542	SY	SODDING, AS PER PLAN	334
		3	3	6	661	E99900	6	EACH	PLANTING, MISC.: DECIDUOUS TREE, 12' HT - 3-5 STEMS, MAGNOLIA VIRGINIANA - SWEETBAY MAGNOLIA, AS PER PLAN	334-335
		11	11	22	661	E40141	22	EACH	DECIDUOUS TREE, 4" CAL, ACER RUBRUM 'ARMSTRONG GOLD' - ARMSTRONG GOLD MAPLE, AS PER PLAN	334-335
		10	10	20	661	E40121	20	EACH	DECIDUOUS TREE, 3" CAL, AMELANCHIER X GRANDILORA 'AUTUMN BRILLIANCE' - AUTUMN BRILLIANCE SERVICEBERRY, AS PER PLAN	334-335
		6	6	12	661	E40121	12	EACH	DECIDUOUS TREE, 3" CAL, CRATAEGUS VIRIDIS 'WINTER KING' - WINTER KING HAWTHORN, AS PER PLAN	334-335
		98	93	191	661	E30061	191	EACH	EVERGREEN SHRUB, 2' HT, JUNIPERUS CHINENSIS 'GOLD COAST' - GOLD COAST JUNIPER, #3 CONT.	334-335
		24	24	48	661	E20041	48	EACH	DECIDUOUS SHRUB, 2' HT, HYDRANGEA PANICULATA 'JANE' - LITTLE LIME HARDY HYDRANGEA, #3 CONT.	334-335
		145	160	305	661	E14001	305	EACH	PERENNIALS, AS PER PLAN: PENNISETUM ALOPECUROIDES 'HAMLIN' - FOUNTAIN GRASS, #2 CONT.	334-335
		80	80	160	661	E14001	160	EACH	PERENNIALS, AS PER PLAN: SCHIZACHYRIUM SCOPARIUM 'THE BLUES' - LITTLE BLUESTEM, #2 CONT.	334-335
		439	480	919	661	E14001	919	EACH	PERENNIALS, AS PER PLAN: FESTUCA GLAUCA 'ELIJAH BLUE' - BLUE FESCUE, #1 CONT.	334-335
		69	46	115	661	E14001	115	EACH	PERENNIALS, AS PER PLAN: LAVANDULA AUGUSTIFOLIA 'HIDCOTE' - HIDCOTE ENGLISH LAVANDER, #1 CONT.	334-335
		143	176	319	661	E14001	319	EACH	PERENNIALS, AS PER PLAN: AMSONIA HUBRICHTII - HUBRICHT'S AMSONIA, #2 CONT.	334-335
		225	227	452	661	E14001	452	EACH	PERENNIALS, AS PER PLAN: LIRIOPE MUSCARI 'MONROE'S WHITE' - MONROE'S WHITE LILYTURF, #1 CONT.	334-335
		220	220	440	661	E11001	440	EACH	GROUNDCOVER AND VINES, #1 CONT, LYSIMACHIA NUMMULARIA 'AUREA' - CREEPING JENNY	334-335
75	105			180	SPECIAL	690E98700	180	CY	TURFGRASS SOIL MIX FURNISHED & PLACED, AS PER PLAN (12" DEPTH UPPER HORIZON)	334
28	39			67	SPECIAL	690E98700	67	CY	TURFGRASS SOIL MIX FURNISHED & PLACED, AS PER PLAN (4.5" DEPTH LOWER HORIZON)	334
394	402			796	SPECIAL	690E98700	796	CY	TREE AND PLANTS SOIL MIX FURNISHED & PLACED, AS PER PLAN (24" DEPTH UPPER HORIZON)	334-335
148	151			299	SPECIAL	690E98700	299	CY	TREE AND PLANTS SOIL MIX FURNISHED & PLACED, AS PER PLAN (9" DEPTH LOWER HORIZON)	334-335

SITE FURNISHINGS AND UTILITIES SUMMARY - HIGH STREET BRIDGE

PLAN SHEET NO.		PARTICIPATION 06/MPO/OT	PARTICIPATION 07/NHS/04/COL	ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	DETAIL SHEET NO.
316	317								
		9		518	E62200	9	EACH	STRUCTURE DRAIN, MISC.: YARD DRAIN	324
		259		605	E98000	259	FT	UNDERDRAINS, MISC.: 6" PERFORATED AND SOLID PLANTER UNDERDRAIN	324
		1442		605	E98000	1442	FT	UNDERDRAINS, MISC.: 4" PERFORATED AND SOLID PLANTER UNDERDRAIN	324
		13		611	E97200	13	EACH	CONDUIT, MISC.: "T" CONNECTION WITH CLEANOUT RISER AND ATRIUM GRATE	324
		42		611	E97200	42	EACH	CONDUIT, MISC.: 90 DEGREE ELBOW WITH CLEANOUT RISER AND ATRIUM GRATE	324
8	7	18		625	E98000	18	EACH	LIGHTING, MISC.: RECESSED WALL LIGHT TYPE 1	362
28	23	51		625	E98000	51	EACH	LIGHTING, MISC.: SEATWALL LIGHT TYPE 2 (8' SEGMENTS)	362
2	2	4		630	630E97700	4	EACH	SIGNING, MISC.: BRIDGE PYLON ALUMINUM LETTERS	360
19	18	37		SPECIAL	517E76300	37	LF	RAILING, MISC.: STAINLESS STEEL HANDRAIL	370
5	4	9		SPECIAL	690E98000	9	EACH	TABLES WITH SEATS	
		1		SPECIAL	680E43100	1	EACH	COMPLETE IRRIGATION SYSTEM (HIGH ST. BRIDGE, FULTON ST., LIVINGSTON AVE.)	330
10	10	20		SPECIAL	690E98000	20	EACH	ALUMINUM PLANTER POT TYPE A (HIGH STREET BRIDGE)	351
40	40	80		SPECIAL	690E98000	80	EACH	SKATE DETERRENTS - TYPE A- PLANTER POTS	351
101	107	208		SPECIAL	690E98000	208	EACH	SKATE DETERRENTS - TYPE B- WALLS	355
57	45	102		SPECIAL	690E98000	102	EACH	SKATE DETERRENTS - TYPE C- WOOD SEAT WALL	349
		1		SPECIAL	690E98400	1	LS	ARCHITECTURAL PRECAST CONCRETE PLANTERS, WALLS, STEPS, AND PLINTHS	356
33	43	76		SPECIAL	690E98700	76	CY	CAST IN PLACE CONCRETE FOR PRECAST VENEER RETAINING WALL TYPE "C"	351
		1		SPECIAL	690E98400	1	LS	WOOD CLAD SEATWALL	361-362
		1		SPECIAL	690E98400	1	LS	COMPLETE PLANTER WATERPROOFING SYSTEM (HIGH STREET BRIDGE)	353-354
		1		SPECIAL	690E98000	1	LS	ARCHITECTURAL TRELIS & PLASTIC FABRICATIONS - EAST AND WEST CAPS, HIGH ST. BRIDGE	357-360
		1		SPECIAL	690E98400	1	LS	ILLUMINATED SCREEN WALL (HIGH ST BRIDGE)	365-369
690	508	1198		SPECIAL	690E98200	1198	SF	UNIT PAVER (SETTING METHOD A- PEDESTAL SYSTEM)	352
71	124	195		SPECIAL	690E98200	195	SF	UNIT PAVER (SETTING METHOD B- BITUMINOUS SET WITH SAND JOINT)	352
2	2			630	630E97700	4	EACH	CAST ALUMINUM WALL MOUNTED BRIDGE PLAQUE	349
315	293	608		511	511E53100	608	SY	CLASS QC2 CONCRETE, MISC: ARC WALK SPECIAL FINISH CONCRETE PAVING TYPES 1&2 (5.5" DEPTH UNREINFORCED SLAB, INCLUDES 6" AVERAGE DEPTH OF LIGHTWEIGHT AGGREGATE)	347-350, 355

NO.	DESCRIPTION	REV. BY	DATE
11	ADDED PAY ITEM	LW/JB	12.08.23
14	ADDED PAY ITEM	LW/JB	1.02.24

MKSK
LANDSCAPE ARCHITECTURE + URBAN PLANNING

P/W DESIGNER
LW/ADH
P/W REVIEWER
JFB

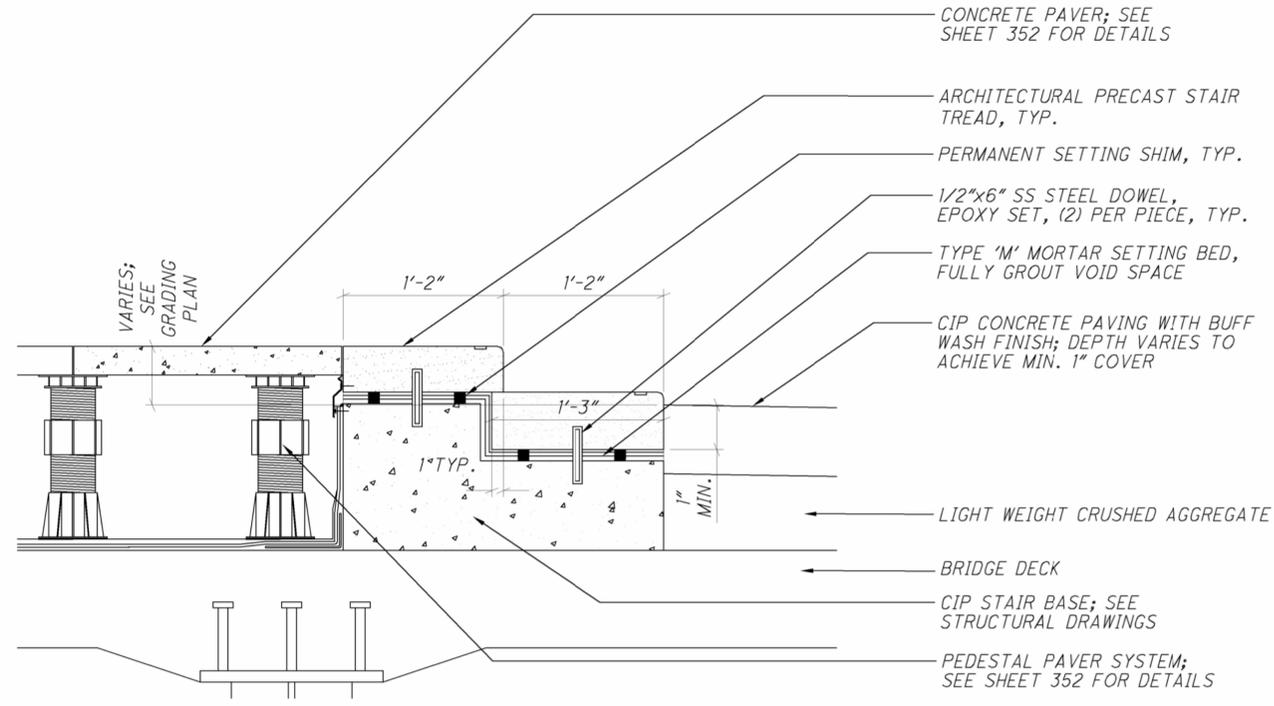
AESTHETIC ENHANCEMENTS SUMMARY
HIGH STREET BRIDGE

FRA-70-14.05C

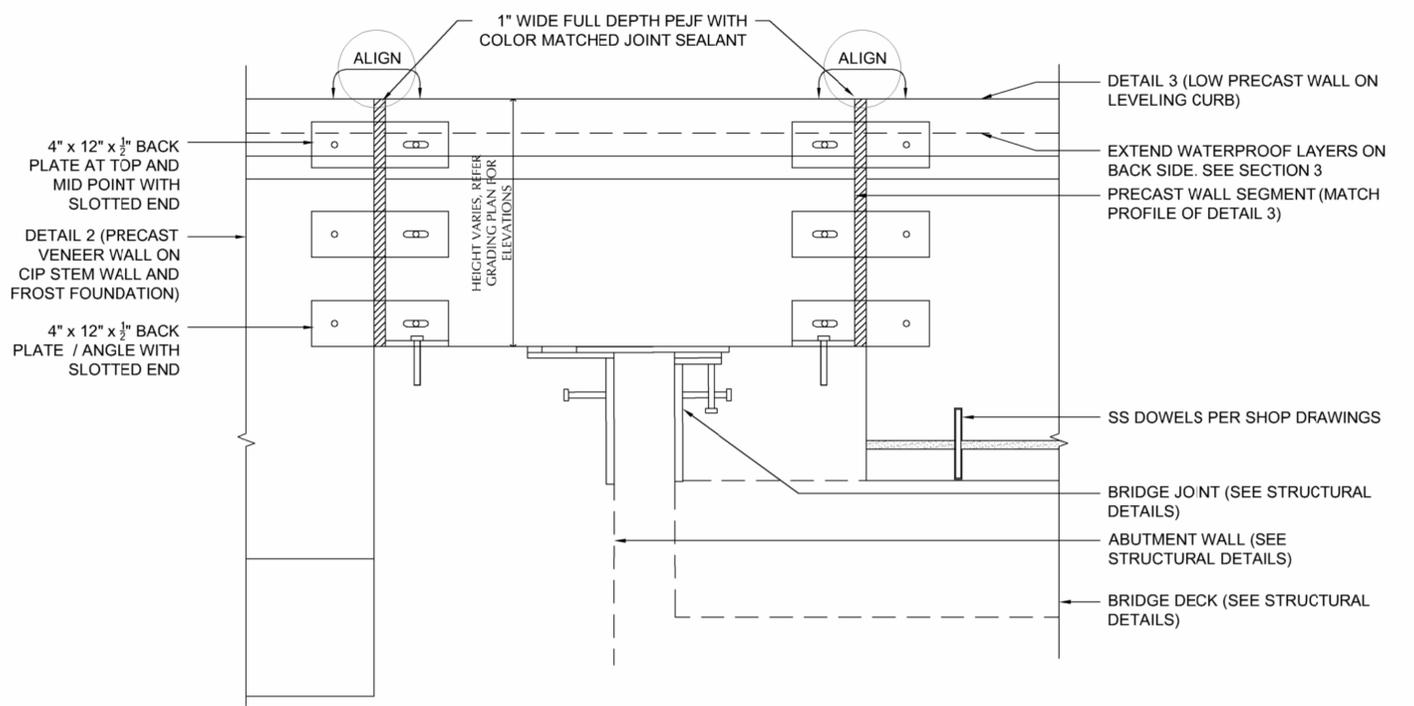
314
395

3524-E

P:\2013\c:\3811-06-70-71 4R & 4H\CAD\Current\Sheets\055596pm100.dwg

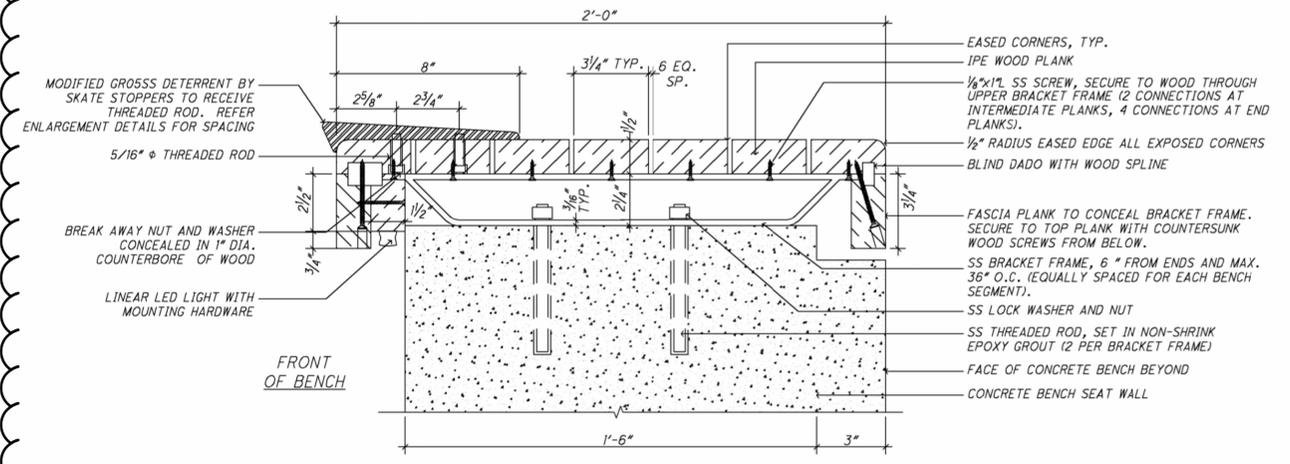


1 HIGH STREET BRIDGE TYP SECTION - PRECAST STEPS AT PAVER PLAZA
SCALE: 1 1/2" = 1'-0"

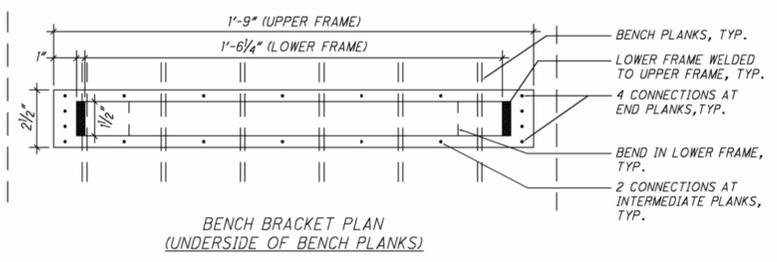


2 PRECAST WALL AT BRIDGE JOINT
SCALE: 1 1/2" = 1'-0"

- NOTES:
- 1.) WOOD PLANKS TO BE IPE, SUBMIT SAMPLE TO PROJECT ENGINEER FOR APPROVAL.
 - 2.) WOOD TO RECEIVE AN OIL BASED PENETRATING SEALER, APPLIED PER MANUFACTURER'S SPECIFICATIONS. SEALER TO BE APPLIED PRIOR TO ATTACHMENT TO CONCRETE BENCH.
 - 3.) CONTRACTOR TO SUBMIT SHOP DRAWINGS OF SS BRACKET FRAME FOR APPROVAL PRIOR TO FABRICATION.
 - 4.) CONTRACTOR TO COORDINATE REQUIRED RECESS IN CONCRETE BENCH TO ACCOMMODATE WOOD TOP. TOP OF WOOD PLANKS TO BE FLUSH WITH TOP OF ADJACENT CONCRETE BENCH.



3 WOOD SEAT TOP - CONCRETE BENCH
SCALE: 3" = 1'-0"

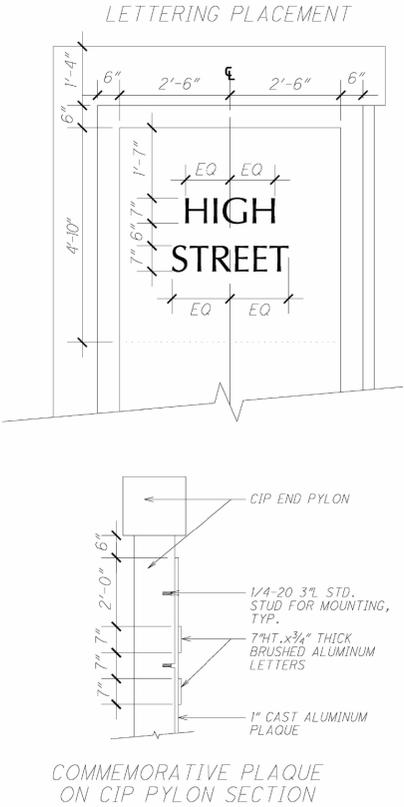
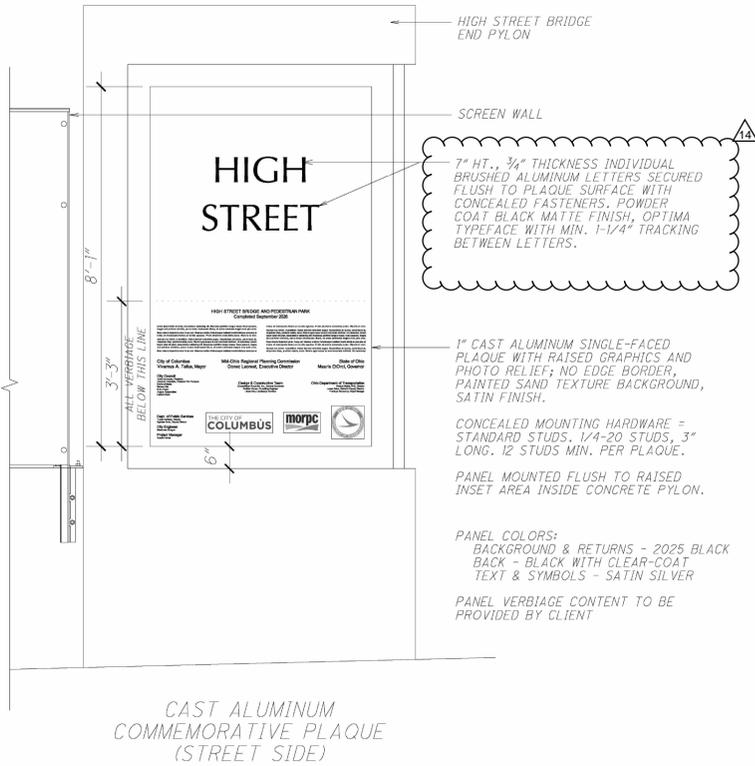


NO.	DESCRIPTION	REV. BY	DATE
14	DETAIL UPDATE	LW/JB	1.02.24

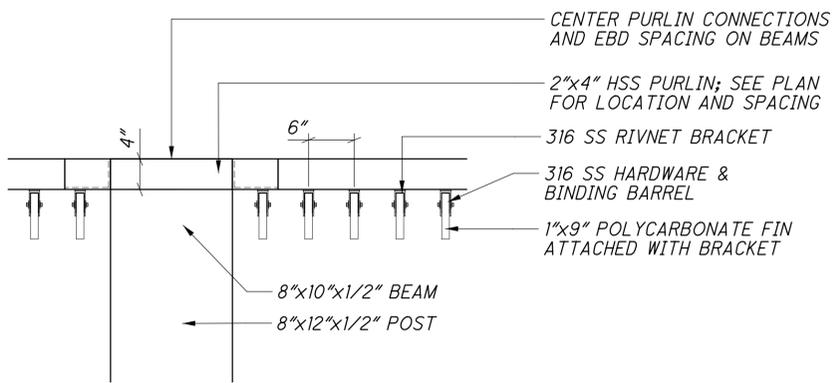


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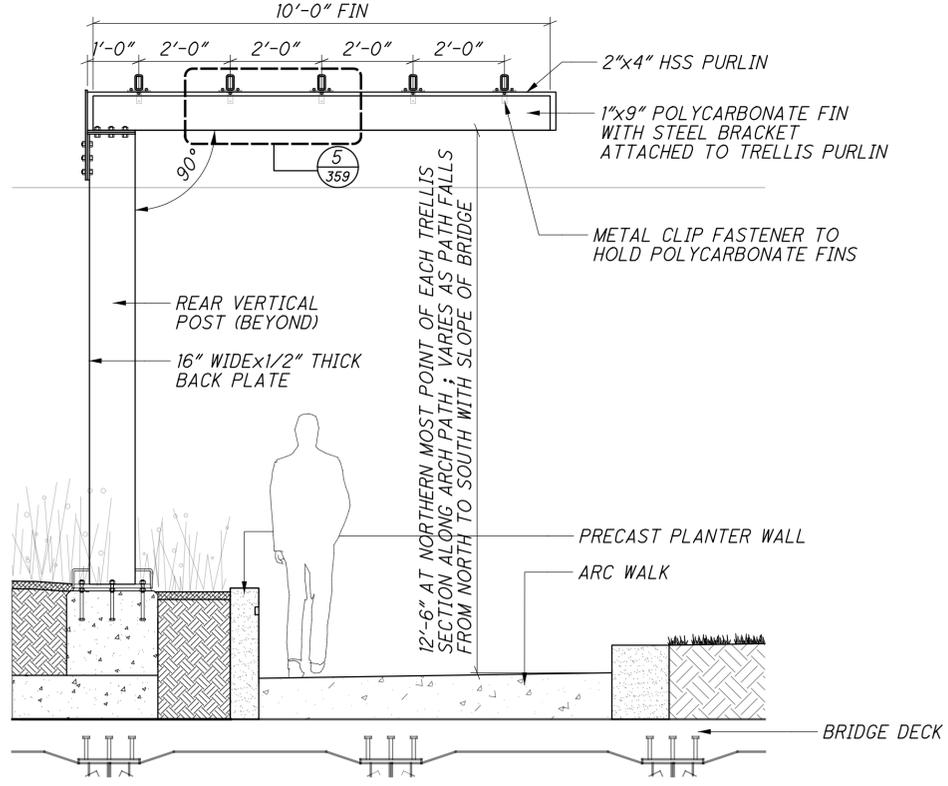
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3 HIGH ST BRIDGE PYLON LETTERING
SCALE: 1/2" = 1'-0"



1 HIGH ST BRIDGE TRELLIS SUB-FRAME
SCALE: 1" = 1'-0"



2 HIGH ST BRIDGE TRELLIS SECTION THROUGH SLAT AND SUB-FRAME
SCALE: 1/2" = 1'-0"

NO.	DESCRIPTION	REV. BY	DATE
14	DETAIL UPDATE	LW/JB	1.02.24

PLOT.CEL
 ms consultants, inc.
 mscconsultants.com
 Ohio DOT Workspace
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 Columbus
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 34" x 22"
 www.mscconsultants.com

TRAFFIC INCIDENT MANAGEMENT (TIM) DURING MOT

OHIO TIM IS OHIO'S TRAFFIC INCIDENT MANAGEMENT PROGRAM WHICH IS COMMITTED TO MAINTAINING THE SAFE AND EFFECTIVE FLOW OF TRAFFIC DURING EMERGENCIES AS TO PREVENT FURTHER DAMAGE, INJURY OR UNDUE DELAY OF THE MOTORING PUBLIC. IN ADDITION TO COMPLYING WITH THE PROVISION OF OMUTCD CHAPTER 6I, CONTROL OF TRAFFIC THROUGH TRAFFIC INCIDENT MANAGEMENT AREAS, THE CONTRACTOR SHALL ACTIVELY PARTICIPATE IN TIM PLANNING AND IMPLEMENTATION AS OUTLINED BELOW.

1. SUPERINTENDENT SHALL IDENTIFY THE INDIVIDUAL PERSONS ON THE PROJECT WHO WILL, OR MAY NEED TO, PERFORM THE DUTIES HEREIN. AT A MINIMUM, INCLUDE THE SUPERINTENDENT, FOREMEN AND SUPERVISORS (OR EQUIVALENT) AS WELL AS THE WORKSITE TRAFFIC SUPERVISOR (WTS; IF APPLICABLE TO THE PROJECT). THESE INDIVIDUALLY IDENTIFIED PERSONS SHALL COLLECTIVELY BE KNOWN AS CONTRACTOR TRAFFIC INCIDENT MANAGEMENT (TIM) CONTACTS. NOTIFY THE PROJECT ENGINEER OF THE CONTRACTOR TIM CONTACTS (ALONG WITH CONTACT INFORMATION FOR EACH) AT OR BEFORE THE PRECONSTRUCTION MEETING.

2. SUPERINTENDENT SHALL NOTIFY THE ENGINEER IMMEDIATELY IF ANY CONTRACTOR TIM CONTACT IS ADDED, REMOVED OR THE CONTACT INFORMATION CHANGES OVER THE COURSE OF THE PROJECT.

3. PRIOR THE FIRST DAY OF WORK IN THE FIELD, EACH CONTRACTOR TIM CONTACT ON THE PROJECT SHALL HAVE ATTENDED AND SUCCESSFULLY COMPLETED OHIO TIM TRAINING PROVIDED BY THE DEPARTMENT OR DESIGNEE. TRAINING INFORMATION CAN BE FOUND AT WWW.OHIOTIM.COM.

4. SUPERINTENDENT, AT A MINIMUM, SHALL ATTEND AND ACTIVELY PARTICIPATE IN A DEPARTMENT SCHEDULED TIM MEETING BEFORE CONSTRUCTION WORK BEGINS AND BEFORE EACH PHASE CHANGE. THESE MEETINGS WILL RESULT IN A DEPARTMENT ISSUED PROJECT SPECIFIC TRAFFIC INCIDENT MANAGEMENT PLAN (TIMP). AT THE TIM MEETINGS THE ATTENDING CONTRACTOR TIM CONTACTS SHALL:

- A. COLLABORATE WITH ODOT AND SAFETY FORCES;
- B. SHARE PROJECT SPECIFIC DETAILS THAT IMPACT TIM RESPONDERS; AND
- C. RECOMMEND WAYS TO INCORPORATE NECESSARY EMERGENCY ACCESS AND OTHER TIM ELEMENTS FOR TIM RESPONDERS GIVEN PROJECT SPECIFIC WORK BEING COMPLETED AND PROJECT SPECIFIC PHASING.

5. CONTRACTOR TIM CONTACTS SHALL IMPLEMENT COMPONENTS OF THE RESULTING TIMP (SUCH AS APPROVED EMERGENCY INGRESS/EGRESS POINTS, ETC), AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH 109.05.

6. CONTRACTOR TIM CONTACTS SHALL PERFORM, AT A MINIMUM, THE FOLLOWING FUNCTIONS WHEN AN INCIDENT/CRASH OCCURS:

- A. IF OBSERVED OR PRESENT WHEN OCCURS, CALL 911 AND THEN NOTIFY THE TRAFFIC MANAGEMENT CENTER (TMC) TO PROVIDE THE FOLLOWING:
 - I. LOCATION, INCLUDING MILEPOST NUMBER AND DIRECTION OF TRAVEL
 - II. NUMBER AND TYPE OF VEHICLES INVOLVED, IF KNOWN
 - III. ESTIMATED EXTENT OF DAMAGE OR INJURY, IF KNOWN
 - IV. ESTIMATED NUMBER OF PATIENTS INVOLVED, IF KNOWN

V. ANY POTENTIAL HAZARDOUS CONDITIONS, IF KNOWN

VI. THE PLACARD NUMBER ON ANY HAZARDOUS MATERIALS PLACARD FROM A SAFE DISTANCE, IF APPLICABLE AND VISIBLE

B. FOLLOWING AN INCIDENT/CRASH:

- I. INITIATE TRAFFIC MANAGEMENT/PROVIDE TEMPORARY TRAFFIC CONTROL AS INDICATED IN THE TIMP, AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH 109.05.
- II. RECOMMEND ROADWAY REPAIR NEEDS.
- III. PROVIDE REPAIR RESOURCES AND INITIATE REPAIRS, AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH 109.05.
- IV. ATTEND AND PARTICIPATE IN AN AFTER ACTION REVIEW (AAR).

ALL COSTS, UNLESS OTHERWISE SPECIFIED, RESULTING FROM THE ABOVE REQUIREMENTS SHALL BE CONSIDERED TO BE INCLUDED IN THE LUMP SUM PRICE FOR ITEM 614, MAINTAINING TRAFFIC. FAILURE TO PERFORM THE REQUIREMENTS OF THIS PLAN NOTE WILL RESULT IN A DAILY FINE OF 2% OF ITEM 614, MAINTAINING TRAFFIC AND MAY RESULT IN ONE OR MORE CONTRACTOR TIM CONTACTS BEING REMOVED FROM THE LIST OF OHIO TIM TRAINED INDIVIDUALS (AT THE SOLE DISCRETION OF THE OHIO TIM EXECUTIVE COMMITTEE). IN THE EVENT AN INDIVIDUAL IS REMOVED FROM THE OHIO TIM TRAINED LIST, THE INDIVIDUAL WILL BE REMOVED FROM CONTRACTOR TIM CONTACT RESPONSIBILITIES ON ALL PROJECTS.

ITEM 614 - WORK ZONE RAISED PAVEMENT MARKER, AS PER PLAN

WORK ZONE RAISED PAVEMENT MARKERS, AS PER PLAN, AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614 OR C&MS 621 AS SPECIFIED HEREIN.

- RAISED PAVEMENT MARKERS IN USE DURING THE SNOW-PLOWING SEASON SHALL CONFORM TO 621.

- RAISED PAVEMENT MARKERS IN USE DURING THE NON-SNOW-PLOW SEASON SHALL CONFORM TO EITHER 614 OR 621.

THE SNOW-PLOWING SEASON SHALL RUN FROM OCTOBER 15 THROUGH APRIL 1.

IF PROJECT DELAYS, NOT THE FAULT OF ODOT, CAUSE THE WORK TO EXTEND INTO THE SNOW-PLOWING SEASON, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACING WORK ZONE RAISED PAVEMENT MARKERS (WZRPMS) CONFORMING TO C&MS 614, WITH RAISED PAVEMENT MARKERS CONFORMING TO 621, AS DETERMINED BY THE ENGINEER, AT THE CONTRACTOR'S EXPENSE.

THIS ITEM SHALL INCLUDE PURCHASE, INSTALLATION AND REMOVAL OF ITEM 614 WORK ZONE RAISED PAVEMENT MARKER, AS PER PLAN, INCLUDING FILLING ANY DEPRESSIONS CREATED IN THE PAVEMENT AS PER C&MS 621.08.

RESURFACING OF THE TRANSITION AREAS SHALL BE PERFORMED AT THE TIME THAT THE SURFACE COURSE IS BEING APPLIED TO THE ENTIRE PROJECT. PRIOR TO APPLICATION OF THE SURFACE COURSE ON THE PROJECT, THE EXISTING PAVEMENT WITHIN THE TRANSITION AREA SHALL BE REMOVED TO A DEPTH NECESSARY TO REACH THE LEVEL OF THE INTERMEDIATE COURSE OF THE PAVEMENT, AS DETERMINED BY THE ENGINEER.

AN ESTIMATED QUANTITY HAS BEEN PROVIDED IN THE MAINTENANCE OF TRAFFIC SUBSUMMARY.

PAYMENT FOR RESURFACING WITHIN THE TRANSITION AREA SHALL BE PAID FOR UNDER THE APPROPRIATE BID ITEMS FOR THE WORK REQUIRED, AS PROVIDED FOR IN THE PLANS.

ITEM 614 - WORK ZONE RAISED PAVEMENT MARKERS ON CONCRETE SURFACES

RAISED PAVEMENT MARKERS IN WORK ZONES, INSTALLED ON TO CONCRETE SURFACES, SHALL BE ITEM 614 WORK ZONE RAISED PAVEMENT MARKERS. WZRPMS ARE INTENDED FOR USE ONLY DURING THE NON-SNOW-PLOWING SEASON. WZRPMS SHALL NOT BE PROVIDED DURING THE SNOW-PLOWING SEASON.

THE SNOW-PLOWING SEASON SHALL RUN FROM OCTOBER 15 THROUGH APRIL 1.

WHERE A TEMPORARY ALIGNMENT WILL REMAIN IN USE THROUGH THE WINTER, THE WZRPMS SHALL BE REMOVED PRIOR TO THE BEGINNING OF THE SNOW-PLOWING SEASON AND REPLACED APPROXIMATELY APRIL 1, OR AS OTHERWISE DETERMINED BY THE ENGINEER.

THIS ITEM SHALL INCLUDE PURCHASE, INSTALLATION AND REMOVAL OF ITEM 614 WORK ZONE RAISED PAVEMENT MARKERS.

ESTIMATED QUANTITIES OF ITEM 614 WORK ZONE RAISED PAVEMENT MARKER HAVE BEEN PROVIDED IN THE MAINTENANCE OF TRAFFIC SUBSUMMARIES AND CARRIED TO THE GENERAL SUMMARY.

ITEM 614 - WORK ZONE PAVEMENT MARKING, MISC.: ROUTE SHIELDS

THIS ITEM SHALL COMPLY WITH ODOT SUPPLEMENTAL SPECIFICATION 814 AND SHALL INCLUDE THE REMOVAL OF THE ROUTE SHIELD MARKINGS UPON COMPLETION OF THE PROJECT, IF APPLICABLE.



APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION(S)

PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE APPROVED MOT EXCEPTION(S) PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

APPROVED MOTEC EXCEPTION(S) INCLUDE:
 - CLOSURE OF SR 315SB TO 70EB FOR 3 YEARS FOR 77372
 - CLOSURE OF 70WB TO 315NB FOR 6 MONTHS IN TOTAL
 - MONITOR TRAFFIC CONDITIONS FOR POSSIBLE CONFIGURATION ADJUSTMENTS AT THE 670EB TO 71SB DETOUR RAMP

A MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF 30 CALENDAR DAYS PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER AND CITY OF COLUMBUS WORK ZONE TRAFFIC MANAGER AS WELL AS THE CONTRACTOR, WORKSITE TRAFFIC SUPERVISOR (WTS) AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.

IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO THE OFFICE OF ROADWAY ENGINEERING, STATEWIDE TMC, DWZTM AND SPECIAL HAULING PERMITS AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE. REFERENCE "EXCEPTION REQUEST APPROVAL DATED 01/24/2023 FOR PID 77372" IN THE NOTIFICATION AND OTHER CORRESPONDENCE.

ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTION(S) LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE MOT EXCEPTION COMMITTEE (MOTEC). IN THE EVENT THAT SUCH CHANGES ARE PROPOSED, THE REQUEST SHALL BE COORDINATED THROUGH THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM) A MINIMUM OF 30 CALENDAR DAYS PRIOR TO THE DESIRED IMPLEMENTATION DATE. IF THE DISTRICT AGREES WITH THE PROPOSED CHANGES THE DWZTM SHALL SEEK APPROVAL FROM THE MOTEC. IN THE EVENT THE PROPOSED CHANGES ARE APPROVED IN WRITING, THE CLOSURES ARE STILL SUBJECT TO NOTIFICATION REQUIREMENTS WITHIN THIS NOTE PRIOR TO IMPLEMENTATION.

NOTIFICATIONS DURING CLOSURE REQUIRED

A DESIGNATED ON-SITE POINT OF CONTACT SHOULD COMMUNICATE WITH THE TMC AS THE STATUS OF THE CLOSURE CHANGES.

- CONTACT THE TMC:
- IF THE CLOSURE IS POSTPONED OR CANCELLED
 - AT THE TIME THE CLOSURE IS IMPLEMENTED
 - AT THE TIME THE CLOSURE IS REMOVED AND ALL LANES RESTORED
 - IF THE CLOSURE WILL NOT BE OPENING ON TIME

CONTACT CAN BE MADE WITH THE TMC IN THE FOLLOWING WAYS:

- PHONE: 1-614-387-2438 OR 1-800-884-4030
- EMAIL: STATEWIDETMC@DOT.OHIO.GOV
- RADIO: XDOT MAIN

NO.	DESCRIPTION	REV. BY	DATE
5	NOTE REVISED	KWR	11/6/23
9	NOTE REVISED	KJF	11/30/23
14	NOTE REVISED	JML	12/20/23

CALCULATED TAG CHECKED JML
 MAINTENANCE OF TRAFFIC - GENERAL NOTES
 FRA-70-13.10
 51
 702
 ms consultants, inc.



PHASE 2 SEQUENCE OF CONSTRUCTION

PHASE 2 (SHEETS 127 - 152)

1) CLOSE THE RAMP FROM 315 SB TO I-70 EB AND DETOUR TRAFFIC. DETOUR TRAFFIC USING I-670 EB TO I-71 SB TO I-70 EB TO COMPLETE THE DETOUR.

2) CLOSE RAMP TO I-70 EB FROM W. MOUND STREET, AND DETOUR TRAFFIC. TRAFFIC SHALL BE DETOURED USING CENTRAL AVE. TO SULLIVANT AVE.

3) CREATE MERGE CONDITION FROM W. BROAD STREET TO I-70 EB, AND SHIFT EB TRAFFIC TO THE TEMPORARY TRAFFIC PATTERN AND PLACE PORTABLE BARRIER.

4) CLOSE RAMP FROM I-70 WB TO SR 315 NB AND DETOUR TRAFFIC USING I-71 NB TO I-670 WB TO S.R. 315 NB TO COMPLETE THE DETOUR. MAXIMUM DURATION OF THE CLOSURE SHALL BE 360 DAYS.

5) CLOSE RAMP FROM I-70 WB TO RICH/TOWN ST AND DETOUR TRAFFIC USING EXIT 98A TOWARDS CENTRAL AVE./SULLIVANT AVE., THEN LEFT ON CENTRAL AVE. TO RICH ST. TO COMPLETE THE DETOUR. MAXIMUM DURATION OF THE CLOSURE SHALL BE 360 DAYS.

6) CLOSE THE RIGHT LANE OF I-71 SB. SHIFT I-71 SB ONTO THE SHOULDER. PLACE PORTABLE BARRIER AND SHIFT I-70 WB TRAFFIC TO CROSSOVERS AND ONTO THE EXISTING I-70 EB ROADWAY

7) BEGIN CONSTRUCTION ON I-70 WB BRIDGES AND APPROACHES.

8) CONSTRUCT THE I-70 WB TO SR-315 NB CONTRAFLOW CROSSOVER.

9) CLOSE AND DETOUR SHORT ST. FOR CONSTRUCTION.

10) PRIOR TO PROCEEDING TO PHASE 2A, COMPLETE THE I-70 WB MOT ROADWAY TIE-IN WITH A NIGHT TIME CLOSURE AND DETOUR OF I-71 SB ACCORDING TO THE TYPICAL DETOUR ON SHEET 99.

PHASE 2A (SHEETS 153 - 156)

1) MAINTAIN THE TRAFFIC PATTERNS FROM PHASE 2 EXCEPT AS DETAILED BELOW.

2) SHIFT I-71 SB ONTO THE PORTION OF THE ROADWAY COMPLETED IN PHASE 2 AND CONSTRUCT THE REMAINING HALF OF I-71 SB.

PHASE 2B (SHEETS 156A - 156F)

1) MAINTAIN THE TRAFFIC PATTERNS FROM PHASE 2 EXCEPT AS DETAILED BELOW.

2) OPEN RAMP BC IN THE CONTRAFLOW DIRECTION (I-70 WB TO SR-315 NB) UTILIZING THE SR-315 CROSSOVER CONSTRUCTED IN PHASE 2.

PHASE 2C (SHEETS 156G - 156L)

1) MAINTAIN THE TRAFFIC PATTERNS FROM PHASE 2B EXCEPT AS DETAILED BELOW.

2) WHEN I-70 WB IS COMPLETE AND READY TO BE OPENED, CLOSE THE CONTRAFLOW RAMP BC IN ORDER TO REMOVE THE CROSSOVER INSTALLED ON SR-315.

DISINCENTIVE AMOUNTS FOR TYPICAL ROAD CLOSURES AND LANE RESTRICTIONS						
ACTIVITY	AFFECTED ROADWAY(S)	RESTRICTION TYPE	SHEET	RESTRICTION TIME	TIMES	DISINCENTIVE
RAMP BC BRIDGE REMOVAL AND RAMP CONSTRUCTION. SR 315 CROSSOVER CONSTRUCTION	SR 315 SB TO I-70 EB RAMP*	ROAD CLOSURE	127	SEE SHEET 53 IN PART 14A		
SR 315 CROSSOVER REMOVAL						
I-70 WB CONSTRUCTION	W. MOUND ST. TO I-70 EB RAMP	ROAD CLOSURE	128	360 CALENDAR DAYS	1	\$9,000 PER DAY
I-70 WB CONSTRUCTION	I-70 WB TO SR 315 NB RAMP	ROAD CLOSURE	132	360 CALENDAR DAYS	1	\$5,000 PER DAY
I-70 WB CONSTRUCTION	W. MOUND ST. TO I-70 WB RAMP	ROAD CLOSURE	132A	360 CALENDAR DAYS	1	\$5,000 PER DAY
SHORT STREET CONSTRUCTION	SHORT STREET FROM MOUND ST. TO LIBERTY ST.	ROAD CLOSURE	152	60 CALENDAR DAYS	1	\$1,500 PER DAY

* RAMP WILL BE UTILIZED FOR I-70 WB TO SR 315 NB TRAFFIC BETWEEN CLOSURES.

NOTE: SEE SHEET 53 FOR DISINCENTIVE AMOUNTS ASSOCIATED WITH ANY MAINLINE ROADWAY OR SYSTEM RAMP OVERNIGHT CLOSURE REQUIRED IN THIS PHASE.

SPECIAL HAUL NOTIFICATIONS FOR PHASE RESTRICTIONS

I-70 EB TO I-71 SB
LANE WIDTH: 11'
AVAILABLE PAVEMENT WIDTH: 13.0'

I-70 WB TO I-71 SB
LANE WIDTH: 11'
AVAILABLE PAVEMENT WIDTH: 12.6'

I-71 SB MAINLINE
LANE WIDTH: 11'
AVAILABLE PAVEMENT WIDTH: 13.0'

SEE SHEET 45 FOR NOTIFICATION OF TRAFFIC RESTRICTIONS NOTE AND TABLE.

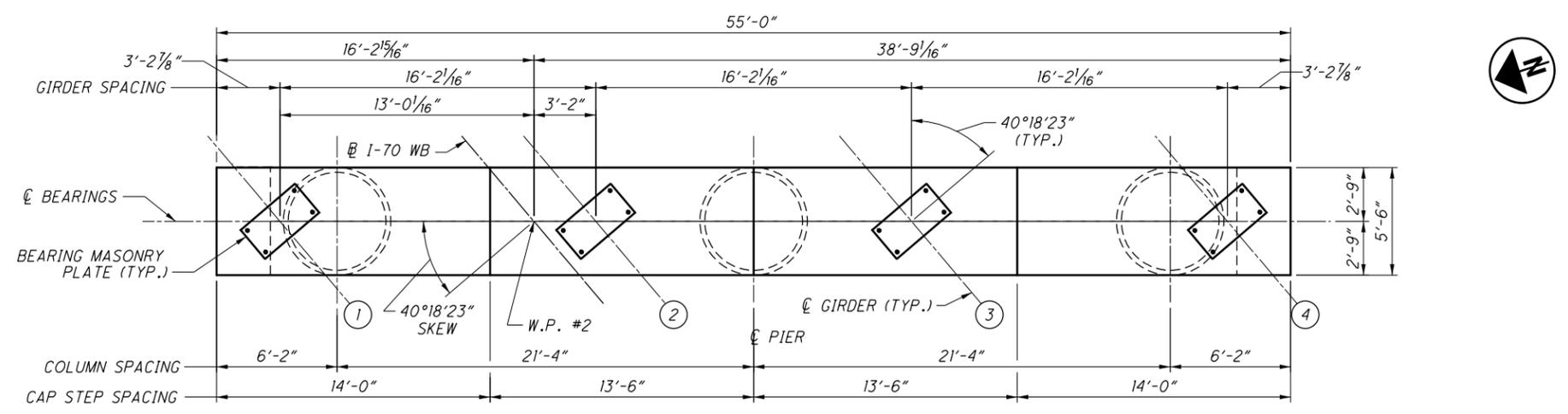
PROPOSED ROADWAY DESCRIPTIONS	
ROAD NAME	DESCRIPTION
TRANS RAMP D3 N	I-71 SB TO RICH & TOWN
TRANS RAMP D3 W	I-70 WB (WEST SIDE)
TRANS I-70 WB (WEST)	I-70 WB TO SR 315 NB
TRANS I-70 WB (EAST)	I-70 WB (EAST SIDE)
RAMP D7	W. MOUND STREET TO I-70 WB

TEMPORARY ROADWAY DESCRIPTIONS	
PLAN VIEW LABEL	DESCRIPTION
TR-ID	TEMP CROSSOVER (WEST END)
TR-IE	TEMP CROSSOVER (EAST END)

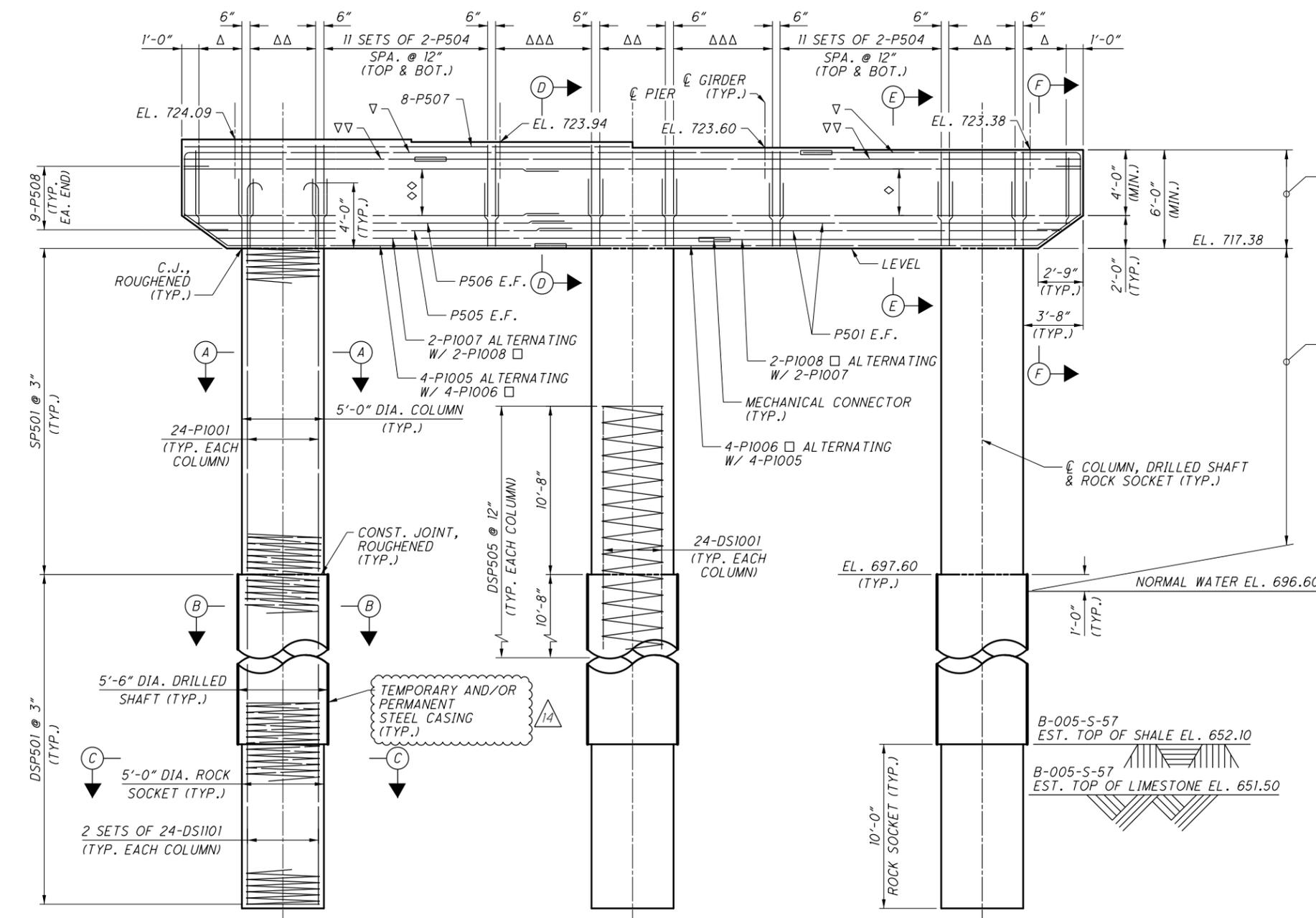
14

NO.	DESCRIPTION	REV. BY	DATE
14	NOTE AND TABLE REVISED	JML	12/20/23

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 ms consultants, inc.
 msconsultants.com
 Ohio DOT Workspace
 70171 East Interchange 6A
 Columbus, OH 43229
 www.msconsultants.com
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 1/31/2024
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PLAN - PIER 1



ELEVATION - PIER 1
(LOOKING UPSTATION)



LEGEND:

- ① - DENOTES PROPOSED GIRDER NUMBER
- △ 6 SETS OF 3-P503 SPA. @ 6" (TOP AND BOTTOM)
- △△ 3 SETS OF 2-P504 SPA. @ 24" (TOP AND BOTTOM)
- △△△ 13 SETS OF 2-P504 SPA. @ 6" (TOP AND BOTTOM)
- ▽ 4-P1011 ALTERNATING WITH 4-P1012 □ IN ALL DIRECTIONS
- ▽▽ 3-P1009 ALTERNATING WITH 3-P1010 □ IN ALL DIRECTIONS
- ◇ 7-P501 (E.F.)
- ◇◇ 7-P502 (E.F.)
- DENOTES BAR REQUIRING MECHANICAL CONNECTOR

LIMITS OF SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) (TYP.)

LIMITS OF SEALING OF CONCRETE SURFACES SEAL ENTIRE SURFACE AREA OF COLUMNS (EPOXY-URETHANE) (TYP. ALL COLUMNS)

MIN. REBAR LAP	
#5 = 2'-5"	
#10 = 10'-3"	
#11 = 12'-7"	

NOTES:

1. FOR BEARING DETAILS, SEE SHEETS 37/58 AND 38/58. ADJUST BEAM SEAT ELEVATIONS IF ACTUAL BEARING HEIGHTS VARY FROM TABULATED VALUE ON SHEET 38/58.
2. FOR SECTIONS A-A TO F-F, SEE SHEET 20/58.
3. FOR FOUNDATION PLAN, SEE SHEETS 9/58 AND 10/58.
4. MECHANICAL CONNECTORS TO BE STAGGERED IN ALL DIRECTIONS IN ORDER TO OBTAIN AMPLE SPACE TO MAKE THE CONNECTION. MECHANICAL CONNECTORS IN THE TOP MAT ARE TO BE STAGGERED AT 11'-6" ABOUT CENTER COLUMN. MECHANICAL CONNECTORS IN THE BOTTOM MAT ARE TO BE STAGGERED AT 5'-0" ABOUT THE CENTER COLUMN. MECHANICAL CONNECTORS SHALL BE INCIDENTAL TO THE COST OF ITEM 509, EPOXY COATED REINFORCING STEEL, AS PER PLAN.
5. REINFORCING STEEL LABELED "DS" SHALL BE INCLUDED WITH ITEM 524 - DRILLED SHAFTS FOR PAYMENT.
6. FOR WORKPOINTS (W.P.) STATIONS, SEE SHEET 4/58.
7. ROUGHEN THE SURFACE OF ALL CONSTRUCTION JOINTS.
8. SPIRAL REINFORCEMENT AT TOP OF COLUMN SHALL BE EMBEDDED A MINIMUM OF 2" INTO PIER CAP.

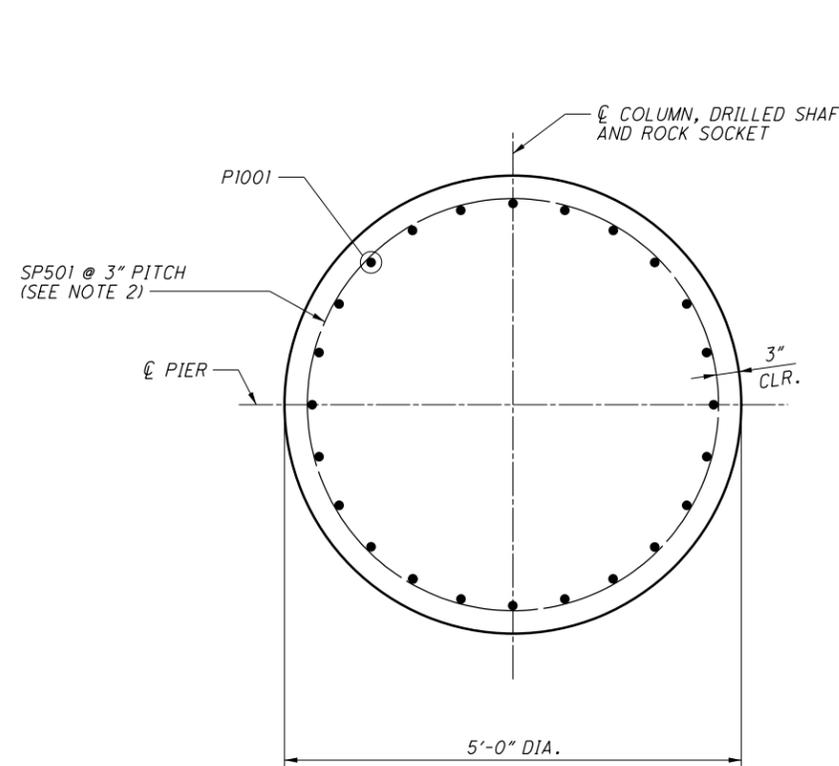
LEGEND:

- △ - LIMITS OF SEALING OF CONCRETE SURFACES, AS PER PLAN (PERMANENT GRAFFITI PROTECTION)

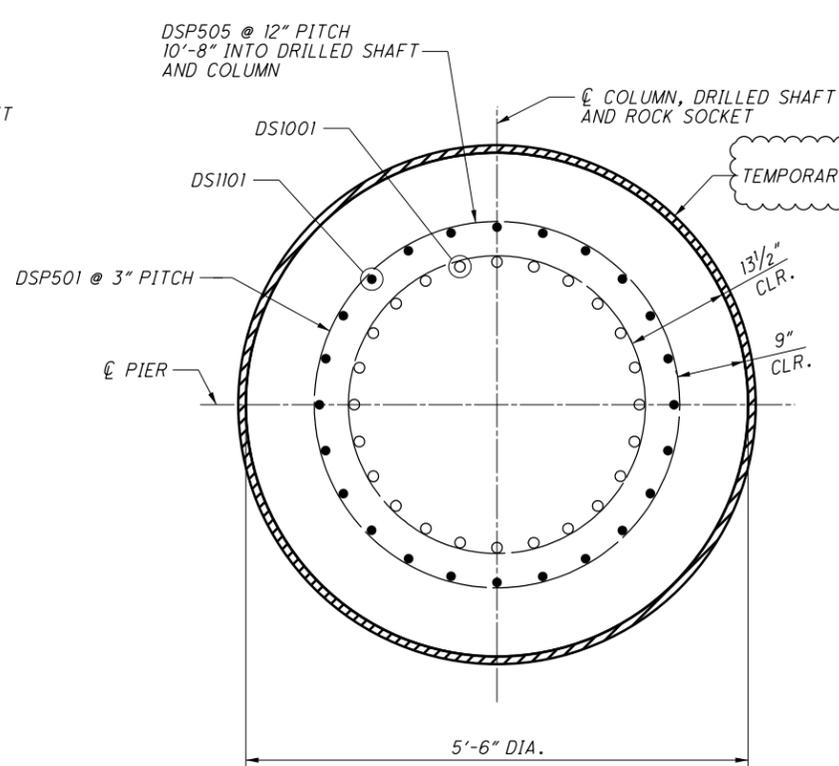
DESIGN AGENCY: ms consultants, inc.
 2221 Schrock Road
 Columbus, Ohio 43229
 DATE: 3/06/23
 REVIEWED: WER
 STRUCTURE FILE NUMBER: 2510027
 DRAWN: KRM
 CHECKED: SUR
 DESIGNED: LAW
 REVISIONS: ---
PIER 1 PLAN AND ELEVATION
 BRIDGE NO. FRA-70-1322L
 I-70 WB OVER SCIOTO RIVER
FRA-70-13-10
 PID No. 77372
 19/58
 491
 702

NO.	DESCRIPTION	REV. BY	DATE
14	EDITED CALLOUT	ACW	1/3/24

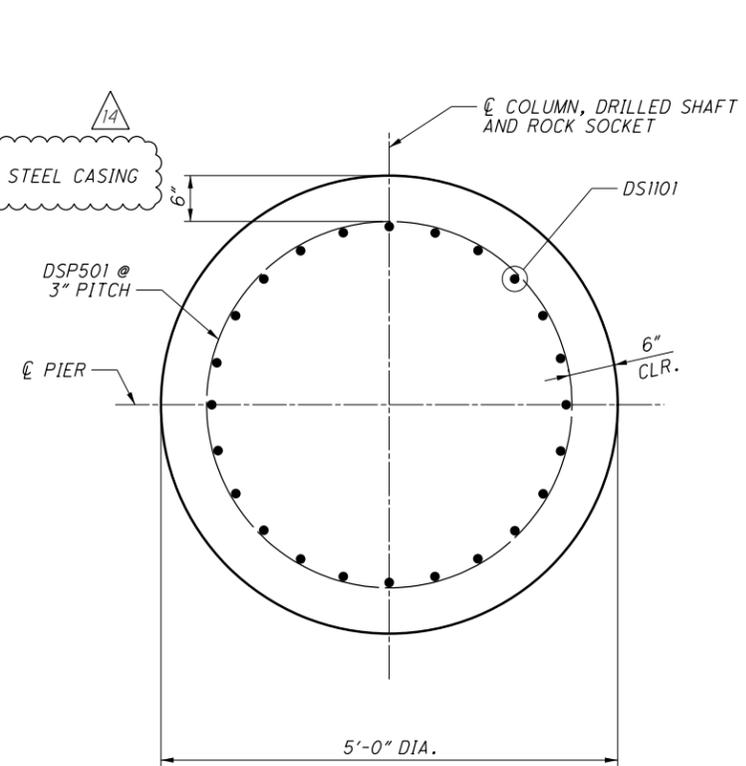
Model: Sheet
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 Ohio DOT Workspace
 70171 East Interchange 6A
 Columbus
 60-06634-6A
 www.msconsultants.com



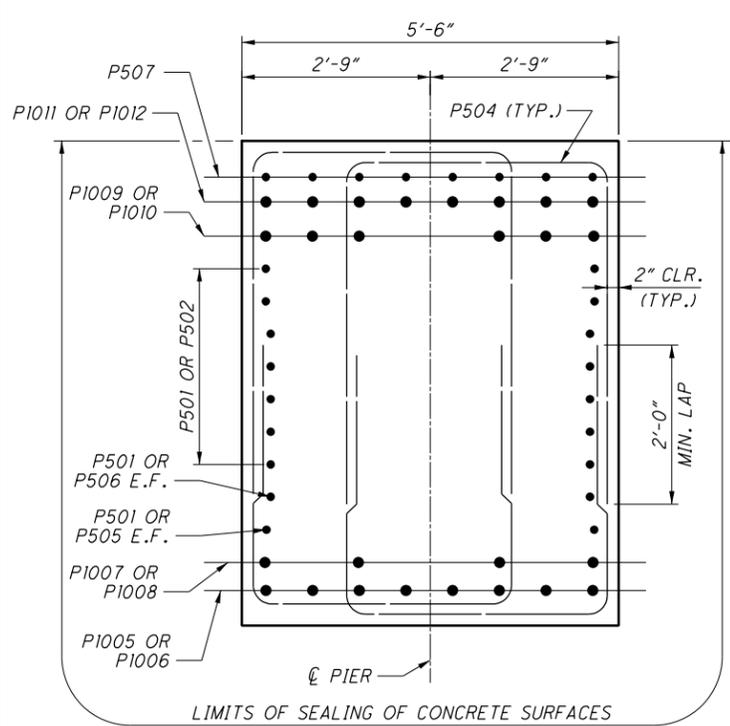
SECTION A-A
(COLUMN)



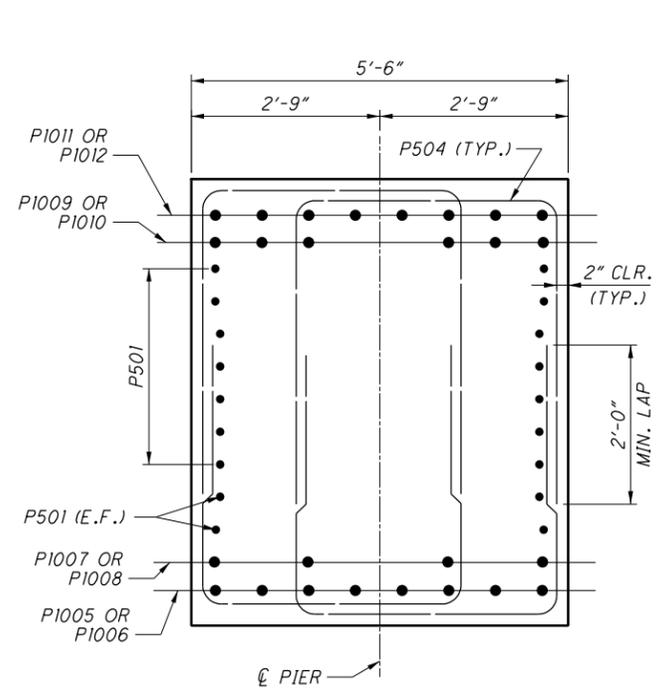
SECTION B-B
(DRILLED SHAFT)



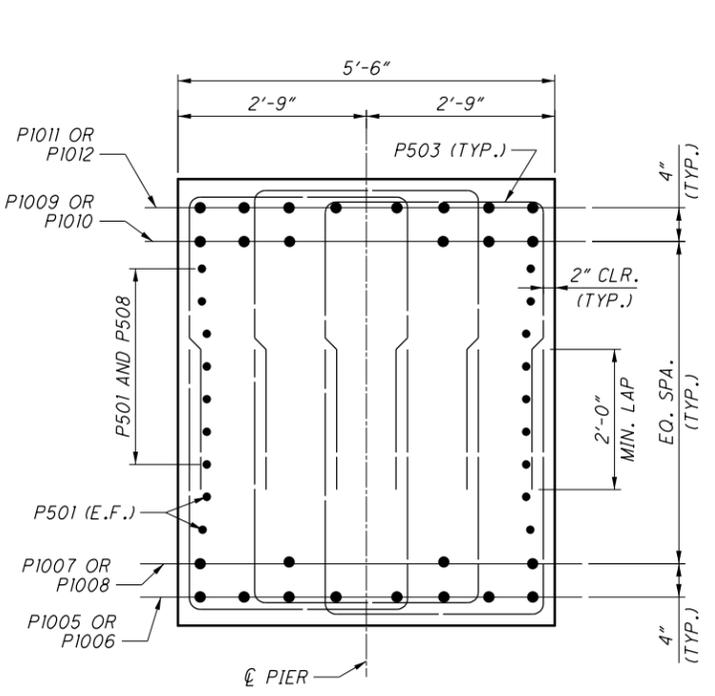
SECTION C-C
(ROCK SOCKET)



SECTION D-D



SECTION E-E



SECTION F-F

NOTES:

- SECTIONS A-A, B-B AND C-C TYPICAL FOR ALL COLUMNS. FOR LOCATION OF SECTIONS A-A TO F-F AND ADDITIONAL NOTES, SEE SHEET [19/58].
- FOR TRANSVERSE SPACING OF BARS IN TOP OF CAP, SEE SHEET [25/58].

NO.	DESCRIPTION	REV. BY	DATE
14	EDITED CALLOUT	ACW	1/3/24

DESIGN AGENCY: **ms consultants, inc.**

DATE: 3/06/23

REVIEWED: WER 2510027

DRAWN: KRM

DESIGNED: LAW

CHECKED: SUR

DESIGNED BY: SUR

REVISIONS: ---

PROJECT: BRIDGE NO. FRA-70-1322L

LOCATION: I-70 WB OVER SCIOTO RIVER

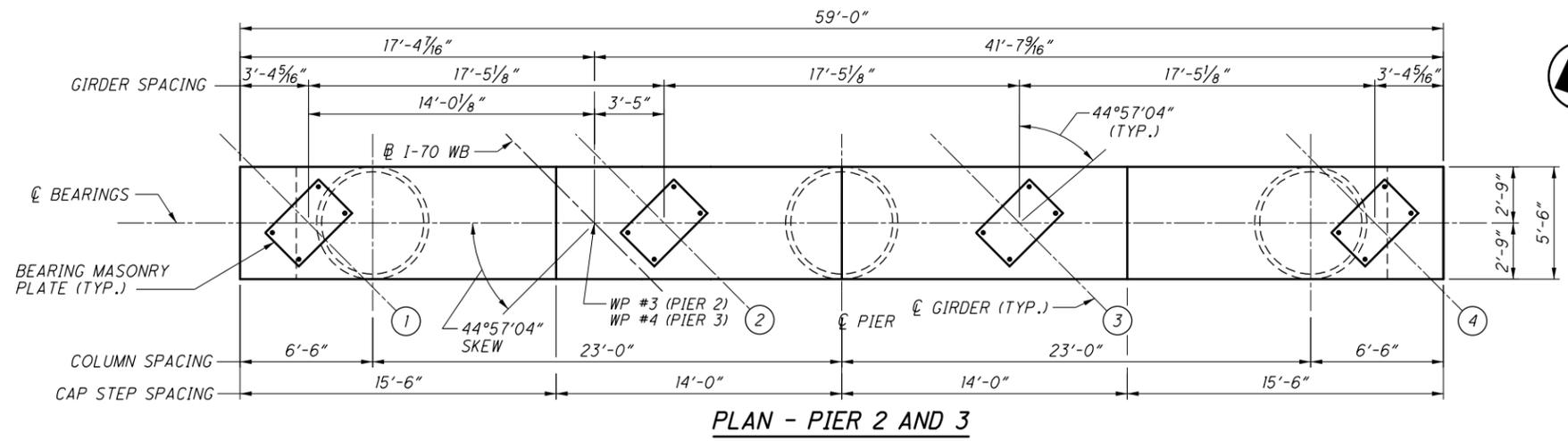
SHEET NO: 20/58

PID NO: 77372

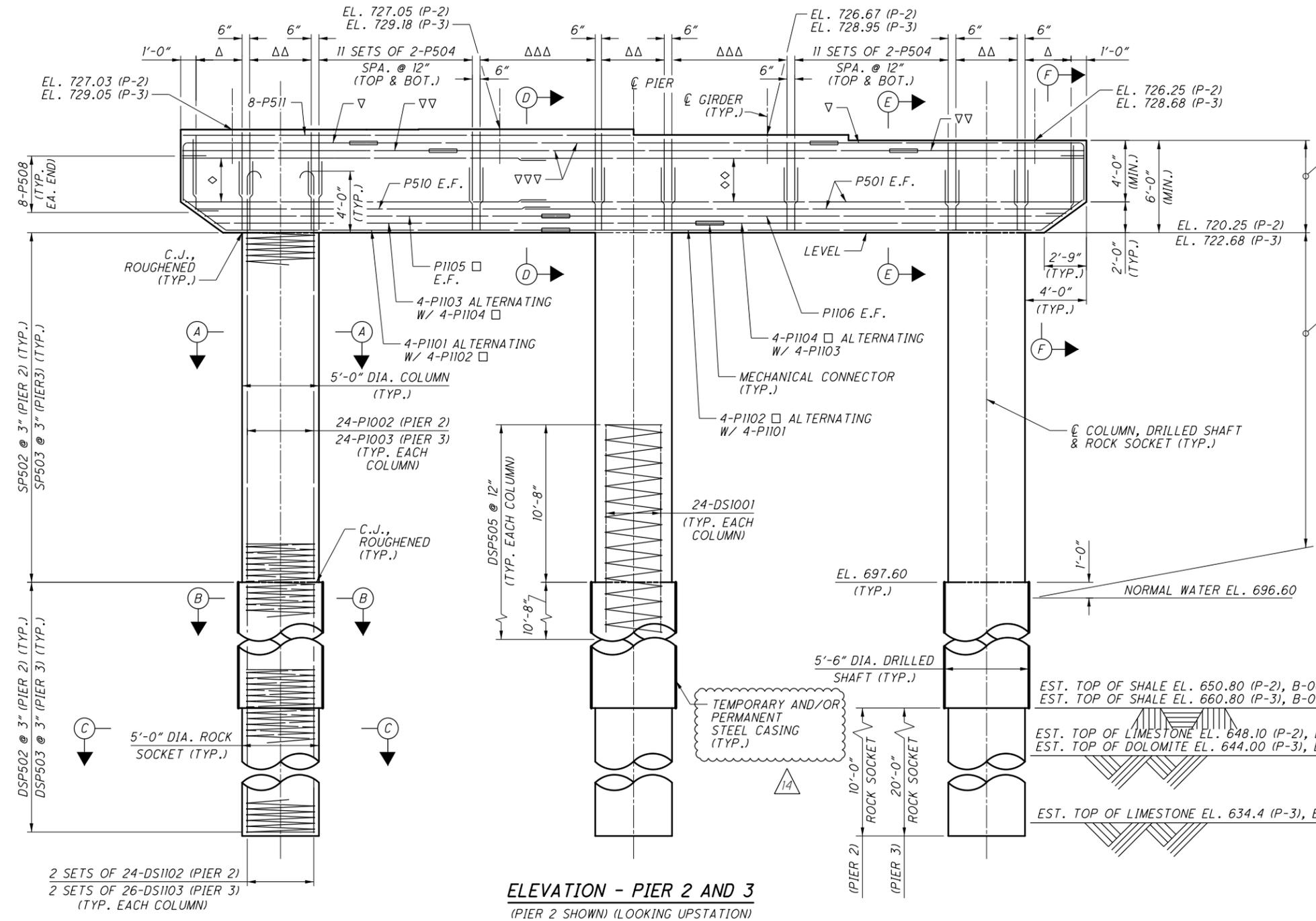
NO. 492

NO. 702

ms consultants, inc.



- LEGEND:**
- ① - DENOTES PROPOSED GIRDER NUMBER
 - △ 7 SETS OF 3-P503 SPA. @ 6" (TOP AND BOTTOM)
 - △△ 3 SETS OF 2-P504 SPA. @ 24" (TOP AND BOTTOM)
 - △△△ 16 SETS OF 2-P504 SPA. @ 6" (TOP AND BOTTOM)
 - ▽ 4-P1109 ALTERNATING WITH 4-P1110 □ IN ALL DIRECTIONS
 - ▽▽ 4-P1107 ALTERNATING WITH 4-P1108 □ IN ALL DIRECTIONS
 - ▽▽▽ 4-P1106
 - ◇ 7-P509 E.F.
 - ◇◇ 7-P501 E.F.



LIMITS OF SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) (TYP.)

LIMITS OF SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) (TYP. ALL COLUMNS)

- NOTES:**
- FOR ADDITIONAL NOTES, SEE SHEET 19/58.
 - FOR SECTIONS A-A TO F-F, SEE SHEET 22/58.

DESIGN AGENCY: ms consultants, inc. 2221 Schrock Road Columbus, Ohio 43229

DATE: 3/06/23

REVIEWED: WER 2510027

STRUCTURE FILE NUMBER: 2510027

DESIGNED: LAW SUR

CHECKED: KRM

DRAWN: KRM

REVISED: ---

PIER 2 AND 3 PLAN AND ELEVATION

BRIDGE NO. FRA-70-1322L

1-70 WB OVER SCIOTO RIVER

FRA-70-13.10

PID No. 77372

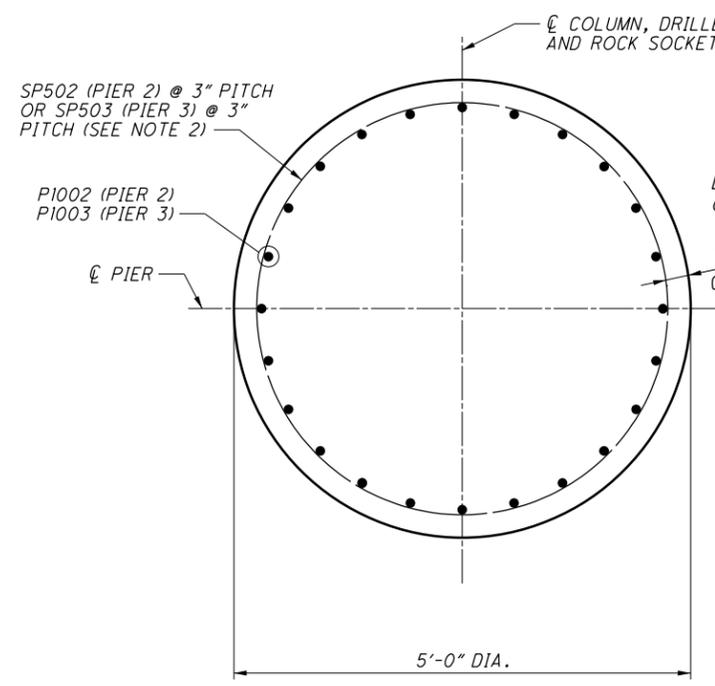
21/58

493

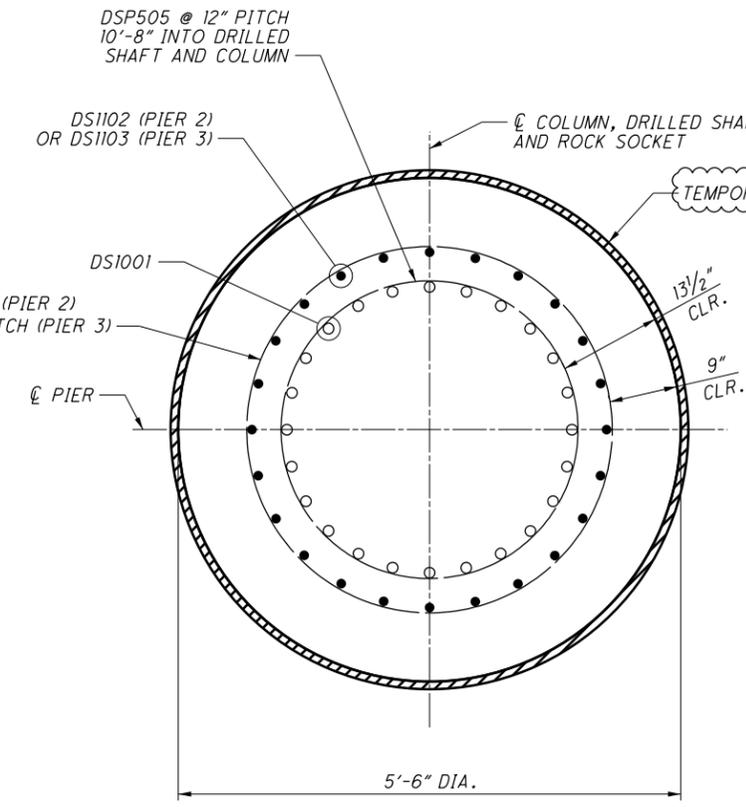
702

NO.	DESCRIPTION	REV. BY	DATE
14	EDITED CALLOUT	ACW	1/3/24

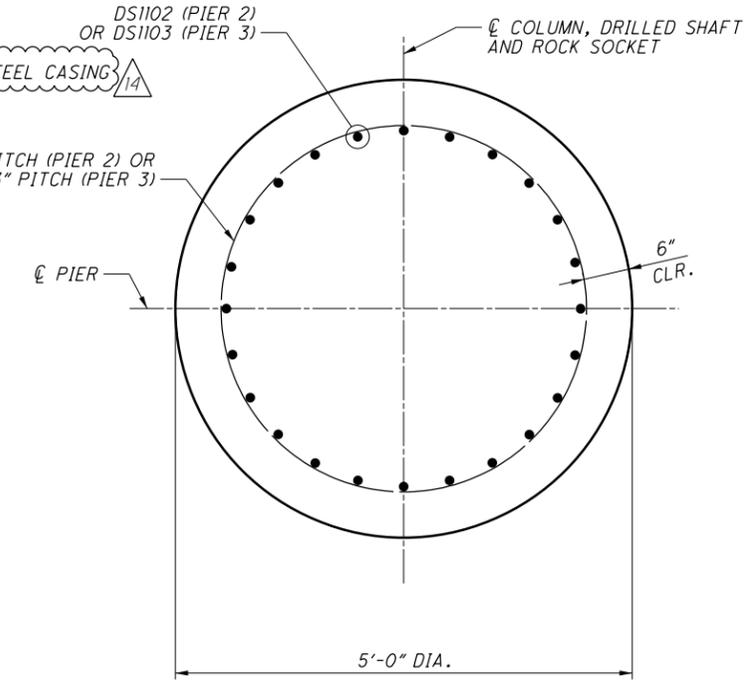
ms consultants, inc.



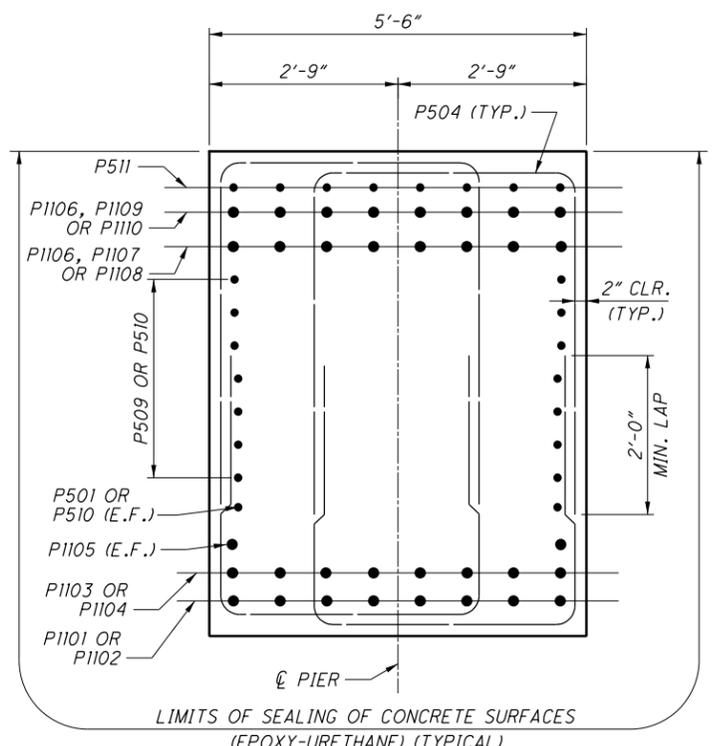
SECTION A-A
(COLUMN)



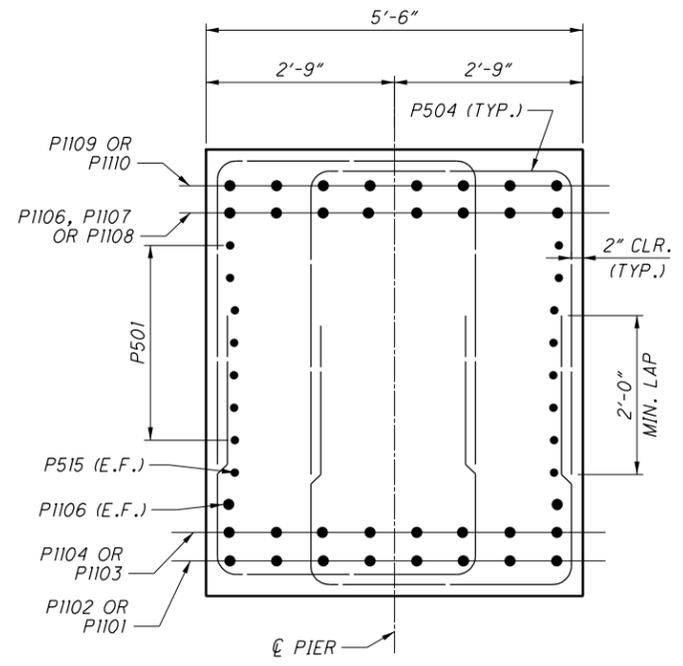
SECTION B-B
(DRILLED SHAFT)



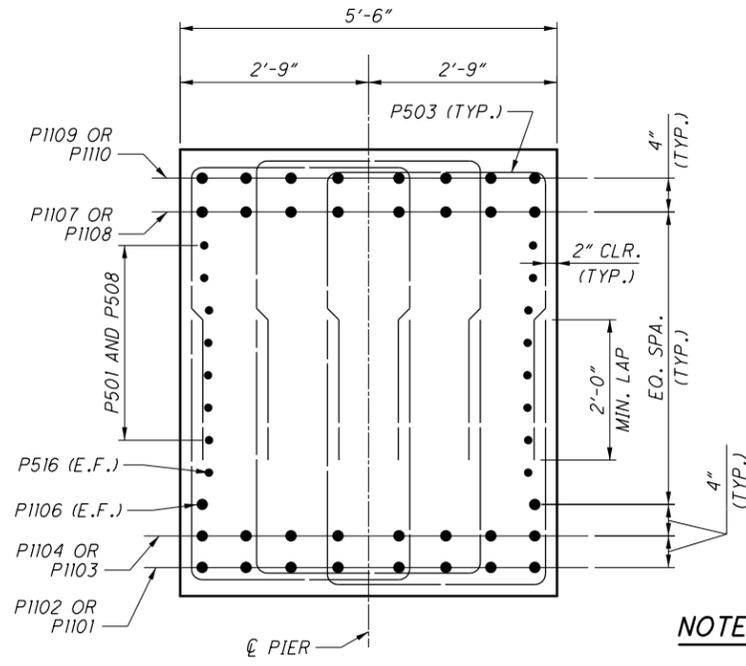
SECTION C-C
(ROCK SOCKET)



SECTION D-D



SECTION E-E

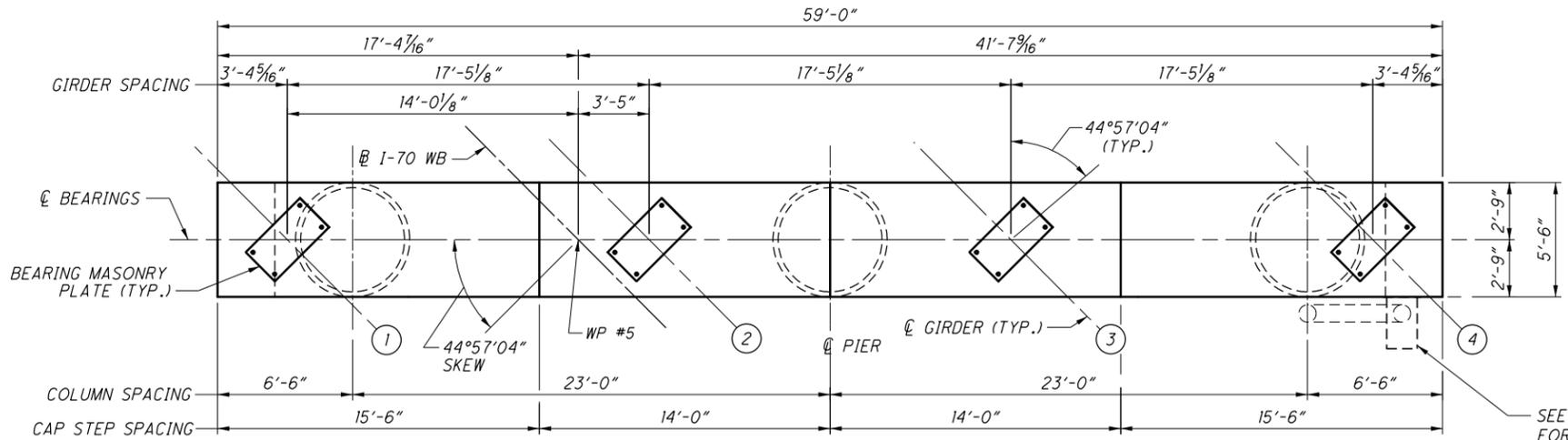


SECTION F-F

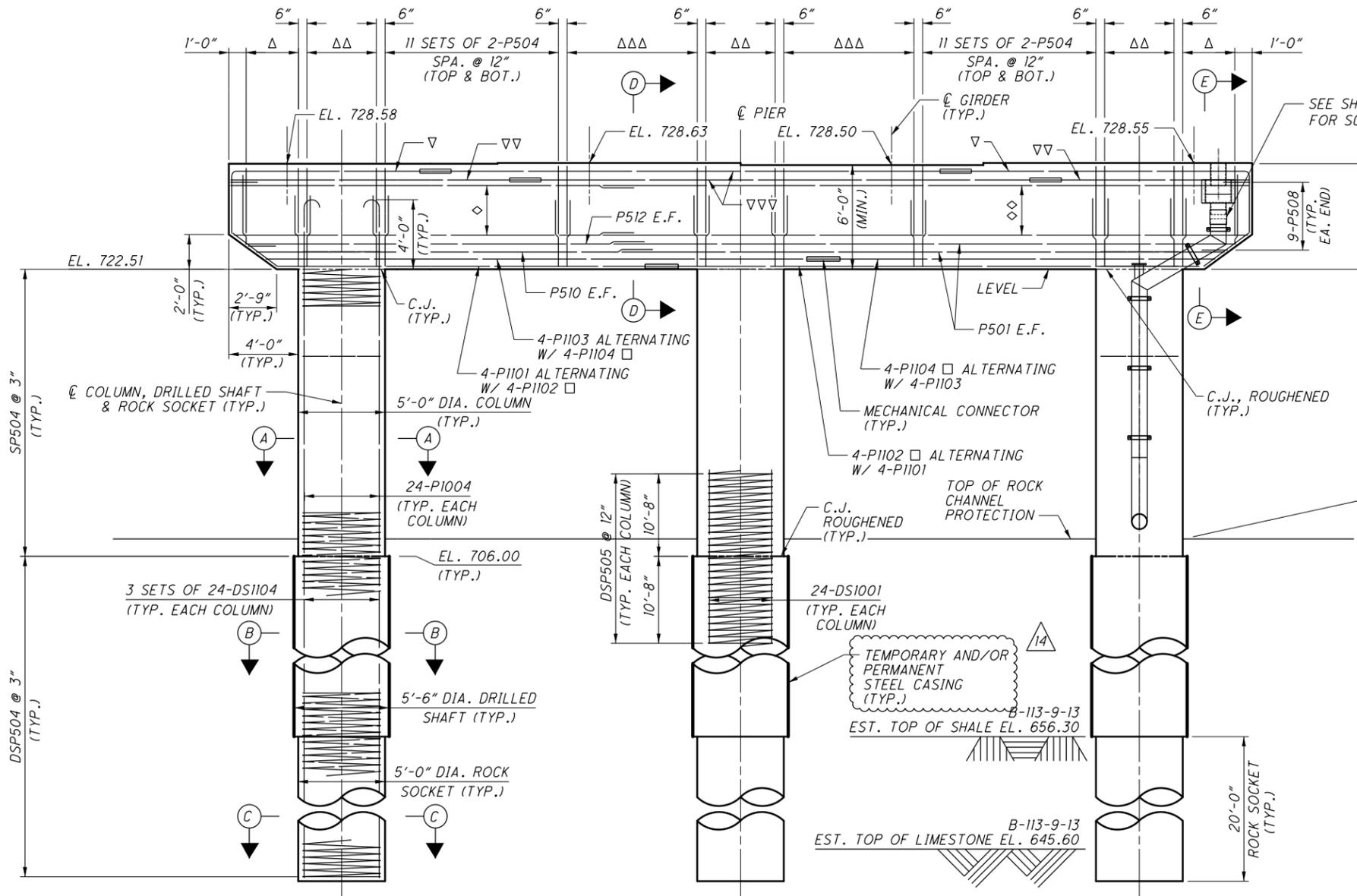
NOTES:

1. SECTIONS A-A, B-B AND C-C TYPICAL FOR ALL COLUMNS. FOR LOCATION OF SECTIONS A-A TO F-F AND ADDITIONAL NOTES, SEE SHEET [21/58].
2. FOR TRANSVERSE SPACING OF BARS IN TOP OF CAP, SEE SHEET [25/58].

NO.	DESCRIPTION	REV. BY	DATE
14	EDITED CALLOUT	ACW	1/3/24



- LEGEND:**
- ① - DENOTES PROPOSED GIRDER NUMBER
 - △ 7 SETS OF 3-P503 SPA. @ 6" (TOP AND BOTTOM)
 - △△ 3 SETS OF 2-P504 SPA. @ 24" (TOP AND BOTTOM)
 - △△△ 16 SETS OF 2-P504 SPA. @ 6" (TOP AND BOTTOM)
 - ▽ 4-P1109 ALTERNATING WITH 4-P1110 □ IN ALL DIRECTIONS
 - ▽▽ 4-P1107 ALTERNATING WITH 4-P1108 □ IN ALL DIRECTIONS
 - ▽▽▽ 4-P1106
 - ◇ 7-P509 E.F.
 - ◇◇ 7-P501 E.F.



LIMITS OF SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) (TYP.)

LIMITS OF SEALING OF CONCRETE SURFACES SEAL ENTIRE SURFACE AREA OF COLUMNS (EPOXY-URETHANE), (TYP. ALL COLUMNS), SEALING OF CONCRETE SURFACES, AS PER PLAN (PERMANENT GRAFFITI PROTECTION) (TYP.)

MIN. REBAR LAP	
#5	= 2'-5"
#10	= 10'-3"
#11	= 12'-7"

- NOTES:**
- FOR ADDITIONAL NOTES, SEE SHEET [19/58].
 - FOR SECTIONS A-A TO E-E, SEE SHEET [24/58].

DESIGN AGENCY: **ms consultants, inc.**
 2221 Schrock Road
 Columbus, Ohio 43229

DATE: 3/06/23
 WER: [redacted]
 STRUCTURE FILE NUMBER: 2510027

DRAWN: KRM
 CHECKED: SUR

DESIGNED: LAW

PIER 4 PLAN AND ELEVATION
 BRIDGE NO. FRA-70-1322L
 I-70 WB OVER SCIOTO RIVER

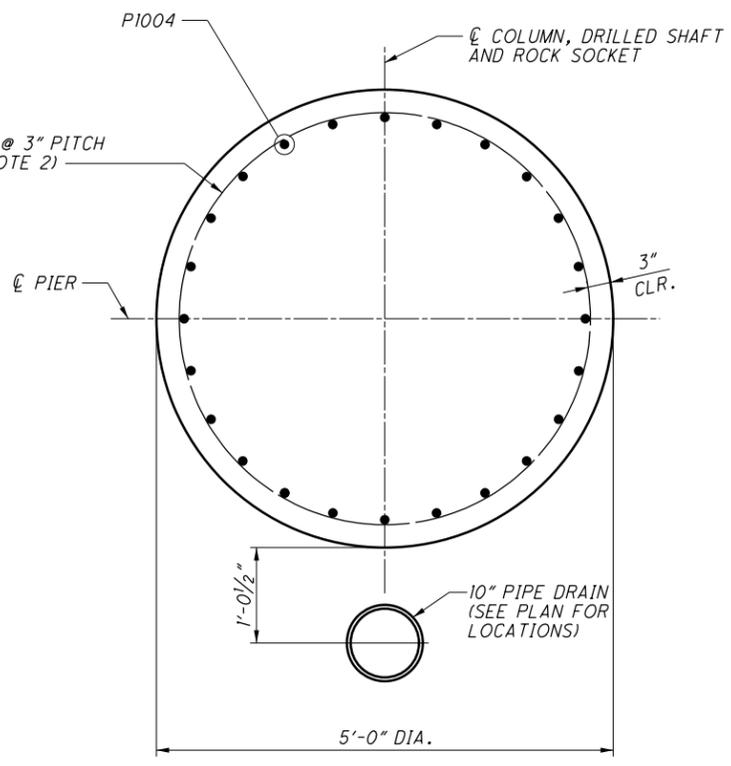
FRA-70-13.10
PID No. 77372

23/58

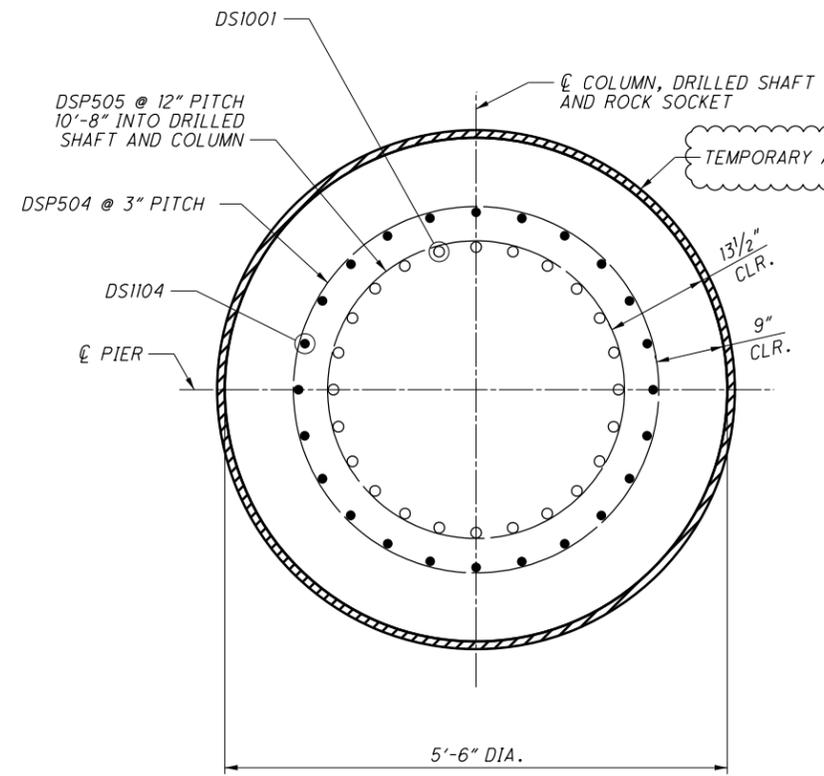
NO.	DESCRIPTION	REV. BY	DATE
14	EDITED CALLOUT	ACW	1/3/24

495
702

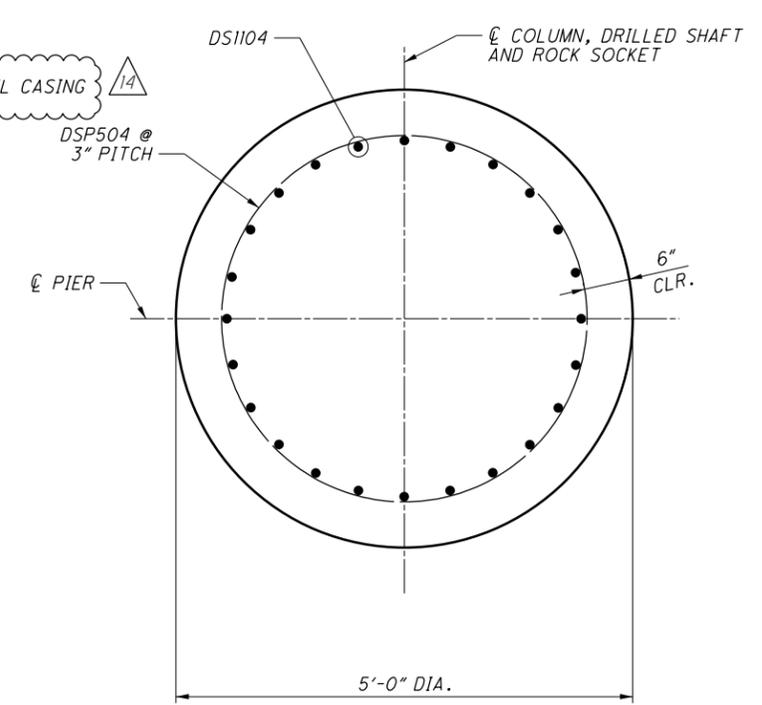
ms consultants, inc.



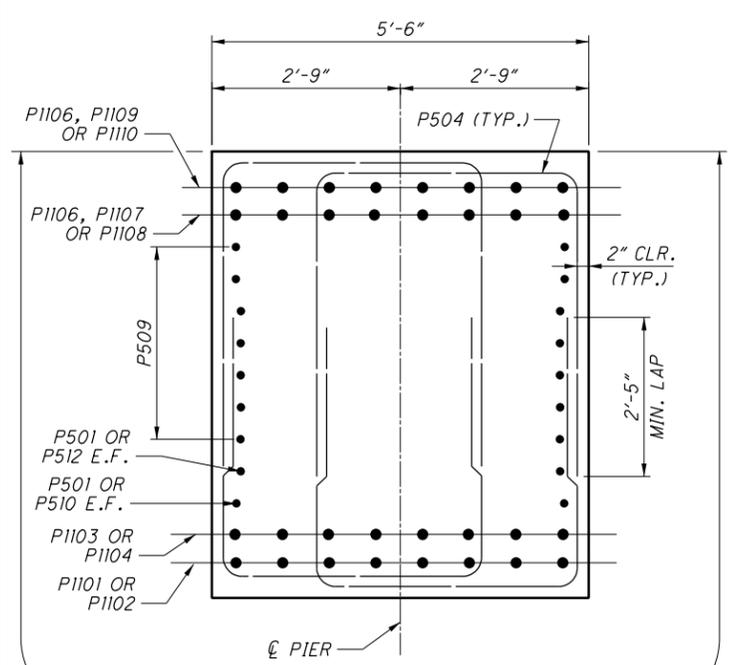
SECTION A-A
(COLUMN)



SECTION B-B
(DRILLED SHAFT)

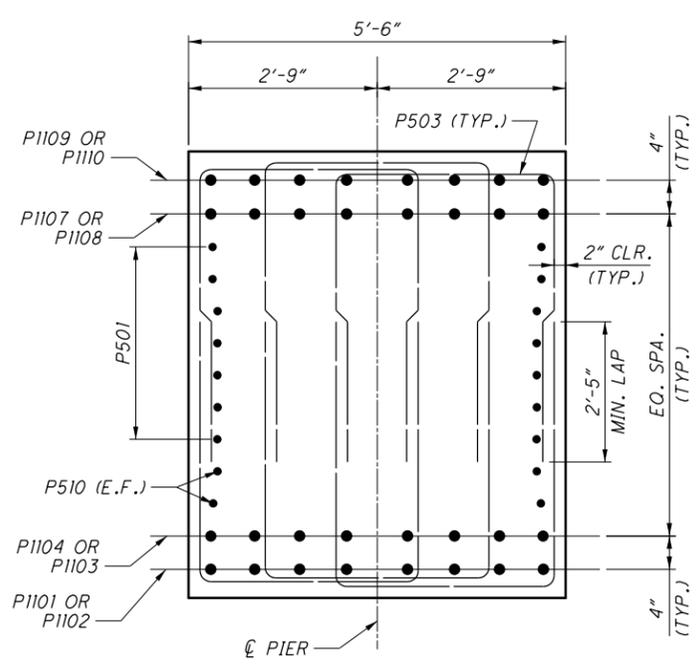


SECTION C-C
(ROCK SOCKET)



LIMITS OF SEALING OF CONCRETE SURFACES
(EPOXY-URETHANE) AND SEALING OF
CONCRETE SURFACES, AS PER PLAN
(PERMANENT GRAFFITI PROTECTION)

SECTION D-D



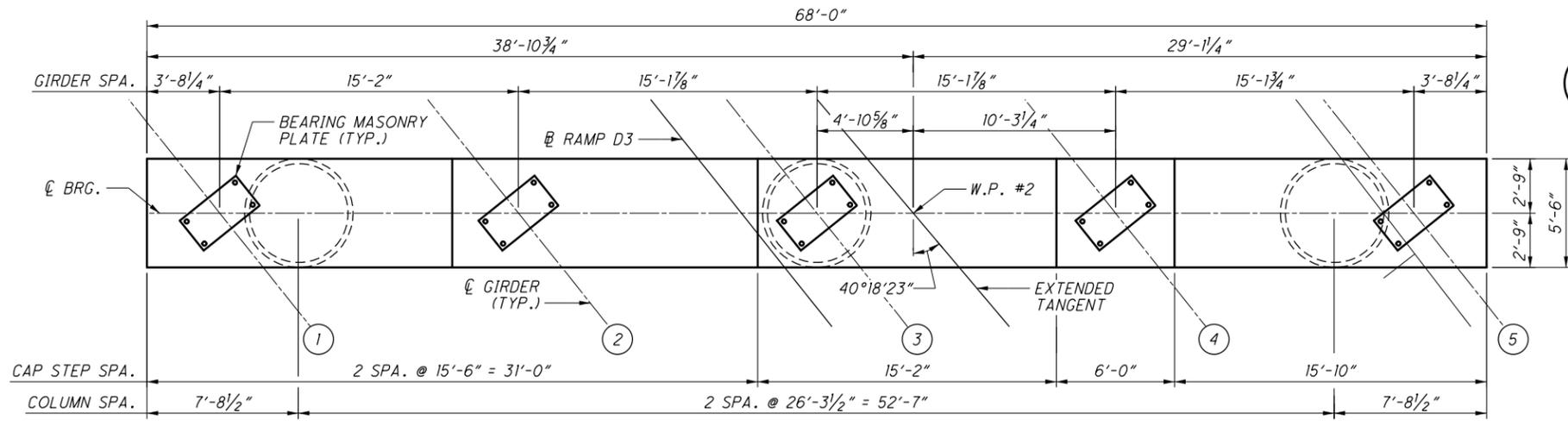
SECTION E-E

NOTES:

- SECTIONS A-A, B-B AND C-C TYPICAL FOR ALL COLUMNS. FOR LOCATION OF SECTIONS A-A TO E-E AND ADDITIONAL NOTES, SEE SHEET [23/58].
- FOR TRANSVERSE SPACING OF BARS IN TOP OF CAP, SEE SHEET [25/58].
- PIER DRILLED SHAFT BARS ARE INCIDENTAL TO ITEM 524.

MARK	NUMBER	LENGTH	WEIGHT	TYPE	DIMENSIONS					
					A	B	C	D	E	INC.
PIER DRILLED SHAFT BARS										
DSP501	3	55'-2"	8,773	27	0'-3"	4'-0"	55'-2"			
DSP502	3	56'-6"	8,985	27	0'-3"	4'-0"	56'-6"			
DSP503	3	56'-6"	8,985	27	0'-3"	4'-0"	56'-6"			
DSP504	3	69'-5"	11,039	27	0'-3"	4'-0"	69'-5"			
DSP505	12	21'-4"	2,769	27	1'-0"	3'-3"	21'-4"			
DS1001	288	21'-4"	26,446	ST						
DS1101	144	34'-0"	26,012	ST						
DS1102	144	34'-7"	26,459	ST						
DS1103	156	34'-7"	28,663	ST						
DS1104	216	27'-4"	31,368	ST						
TOTAL			179,499							

NO.	DESCRIPTION	REV. BY	DATE
14	EDITED CALLOUT	ACW	1/3/24

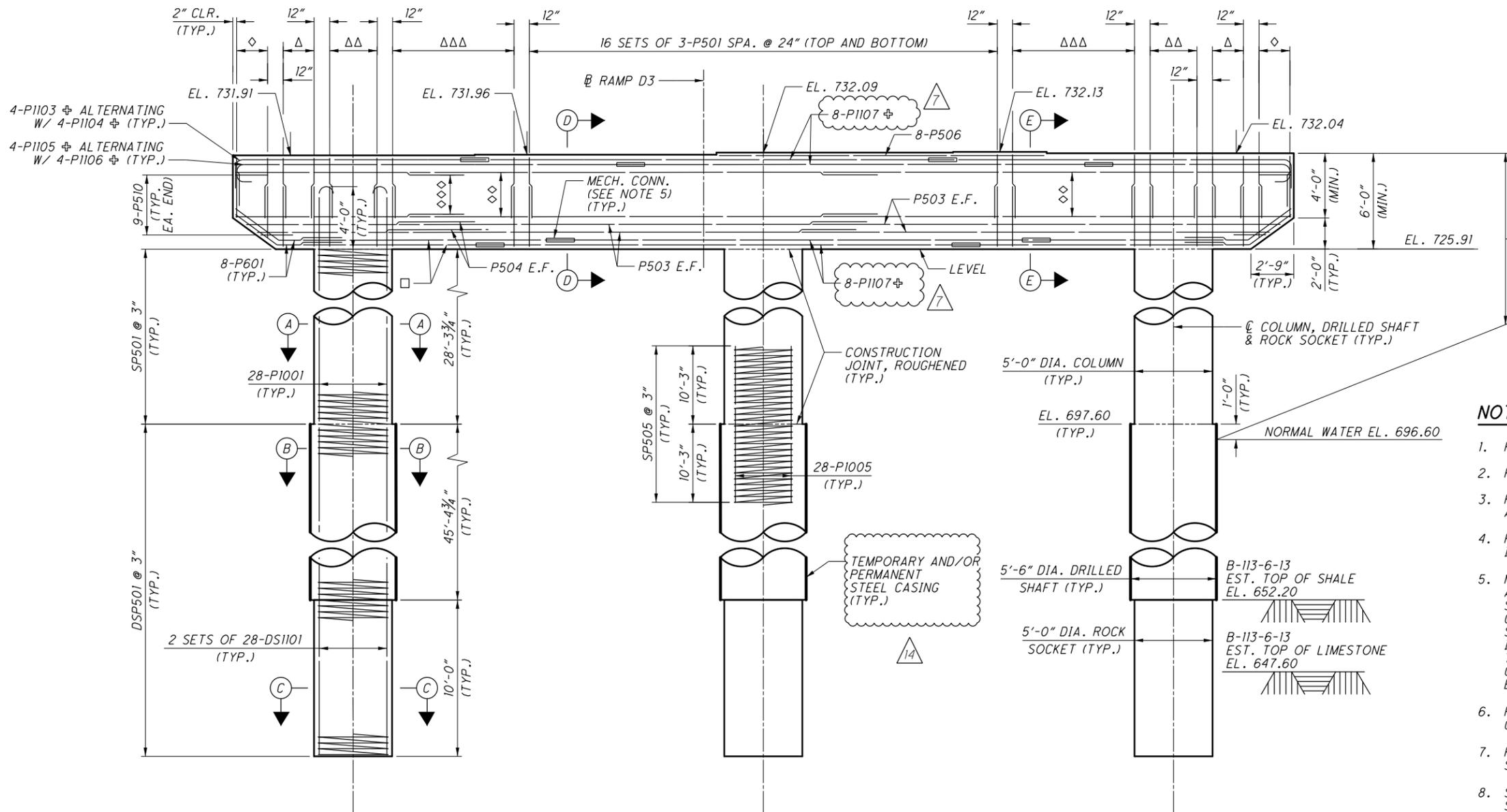


PLAN - PIER 1

LEGEND:

- # DENOTES PROPOSED GIRDER NUMBER
- △ 3 SETS OF 3-P501 SPA. @ 12" (TOP AND BOTTOM)
- △△ 4 SETS OF 2-P501 SPA. @ 24" (TOP AND BOTTOM)
- △△△ 10 SETS OF 3-P501 SPA. @ 12" (TOP AND BOTTOM)
- ◇ SERIES OF 4 SETS OF 3-P502 SPA. @ 12" (TOP AND BOTTOM)
- ◇◇ 7-P503 E.F.
- ◇◇◇ 7-P504 E.F.
- 4-P1101 ⇄ ALTERNATING W/ 4-P1102 ⇄ IN ALL DIRECTIONS (TYP.)
- ⇄ DENOTES BAR REQUIRING MECHANICAL CONNECTOR

MIN. REBAR LAP	
#5 VERT.	= 2'-5"
#5 HORIZ.	= 3'-5"
#6	= 2'-11"
#10	= 10'-3"
#11	= 12'-7"



PIER ELEVATION - PIER 1
LOOKING UPSTATION

NOTES:

1. FOR FOUNDATION PLAN, SEE SHEETS 9/70 AND 10/70.
2. FOR SECTIONS A-A TO E-E, SEE SHEET 26/70.
3. FOR BEARING DETAILS, SEE SHEETS 44/70 AND 45/70.
4. REINFORCING LABELED "DS" TO BE INCLUDED WITH ITEM 524 FOR PAYMENT.
5. MECHANICAL CONNECTORS TO BE STAGGERED IN ALL DIRECTIONS IN ORDER TO OBTAIN AMPLE SPACE TO MAKE THE CONNECTION. MECHANICAL CONNECTORS IN THE TOP MAT ARE TO BE STAGGERED AT 3'. MECHANICAL CONNECTORS IN THE BOTTOM MAT ARE TO BE STAGGERED AT 7'-6" ABOUT THE CENTER COLUMN. MECHANICAL CONNECTORS SHALL BE INCIDENTAL TO ITEM 509 EPOXY COATED REINFORCING STEEL.
6. ROUGHEN THE SURFACE OF ALL CONCRETE CONSTRUCTION JOINTS.
7. FOR WORK POINT (W.P.) LOCATIONS, SEE SHEET 4/70.
8. SPIRAL REINFORCEMENT AT TOP OF COLUMN SHALL BE EMBEDDED A MINIMUM OF 2" INTO CAP.

NO.	DESCRIPTION	REV. BY	DATE
7	LEGEND SYMBOL ADDED	ACW	11/20/23
14	EDITED CALLOUT	ACW	1/3/24

DESIGN AGENCY: ms consultants, inc. 2221 Schrock Road Columbus, Ohio 43229

DATE: 03/08/23

REVIEWED: GLG/YJS

DESIGNED: ABD

DRAWN: ABD

CHECKED: FBW

STRUCTURE FILE NUMBER: 2510026

PIER 1 PLAN AND ELEVATION

BRIDGE NO. FRA-70-1323C

PID No. 77372

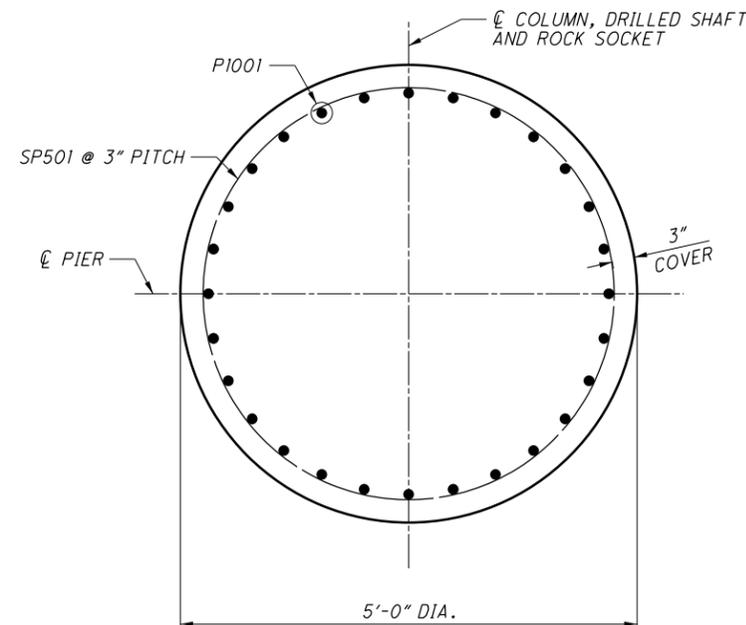
FRA-70-13.10

25/70

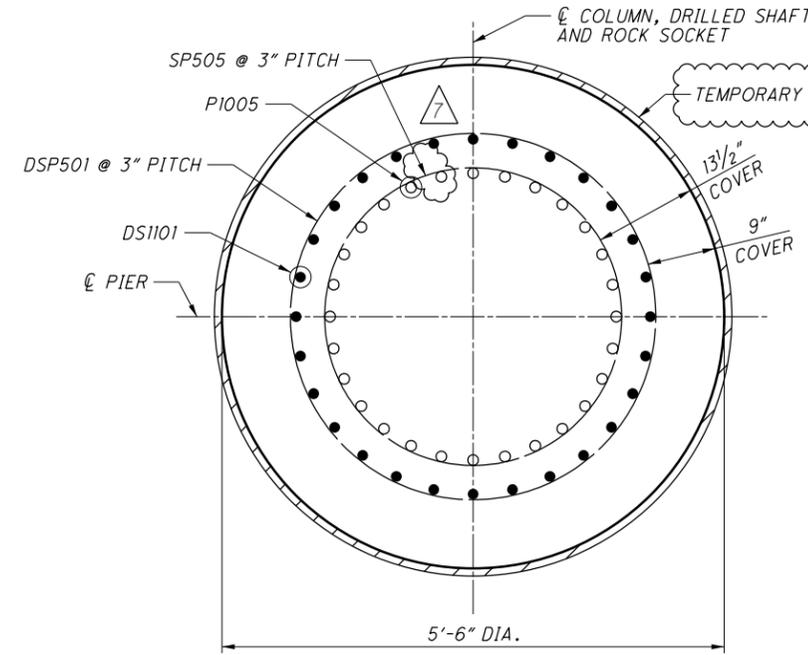
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702

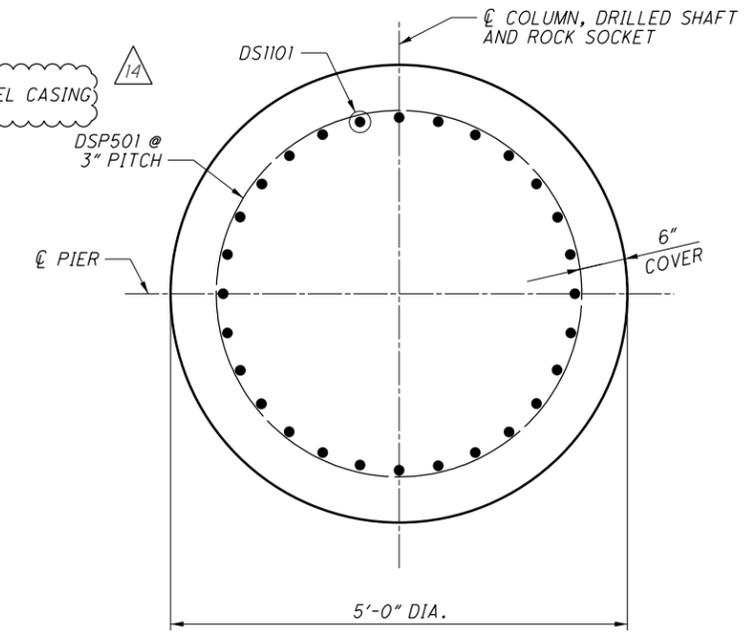
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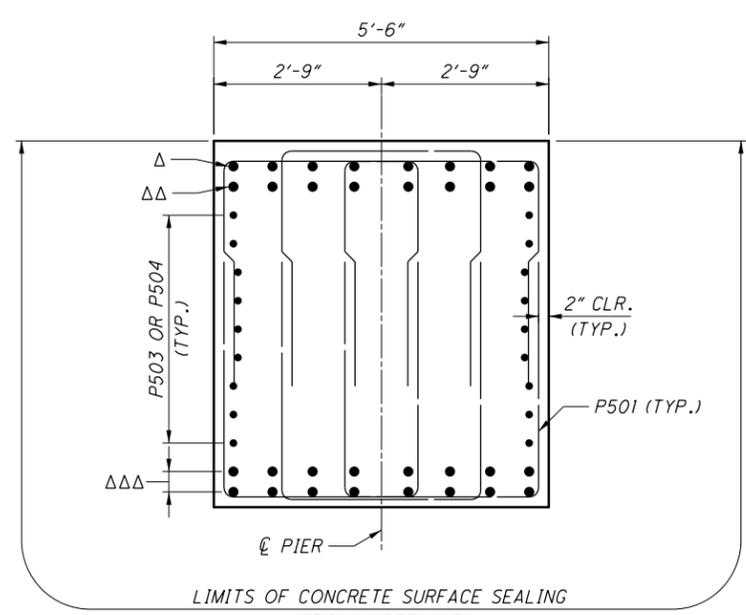
SECTION A-A
COLUMN



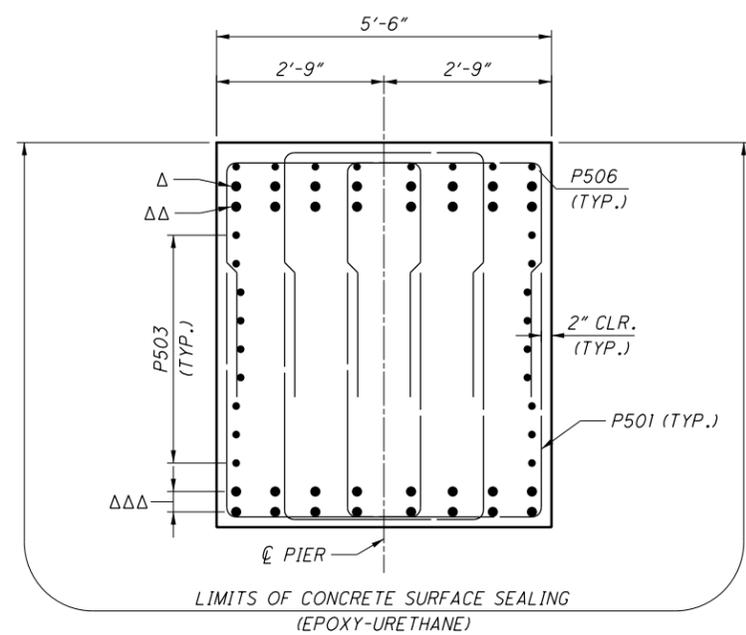
SECTION B-B
DRILLED SHAFT



SECTION C-C
ROCK SOCKET



SECTION D-D



SECTION E-E

MARK	NUMBER TOTAL	LENGTH	WEIGHT	TYPE	DIMENSIONS		
					A	B	C
PIER DRILLED SHAFT BARS							
DSP501	3	55'-0"	8,770	27	0'-3"	4'-0"	55'-0"
DSP502	3	48'-9"	7,669	27	0'-3"	4'-0"	48'-9"
DSP503	3	53'-7"	8,547	27	0'-3"	4'-0"	53'-7"
DSP504	3	58'-3"	9,281	27	0'-3"	4'-0"	58'-3"
DS1101	168	33'-9"	30,154	ST			
DS1102	168	30'-8"	27,387	ST			
DS1103	168	33'-1"	29,529	ST			
DS1104	168	35'-5"	31,627	ST			
		152,965		FOR INFORMATIONAL PURPOSES ONLY			

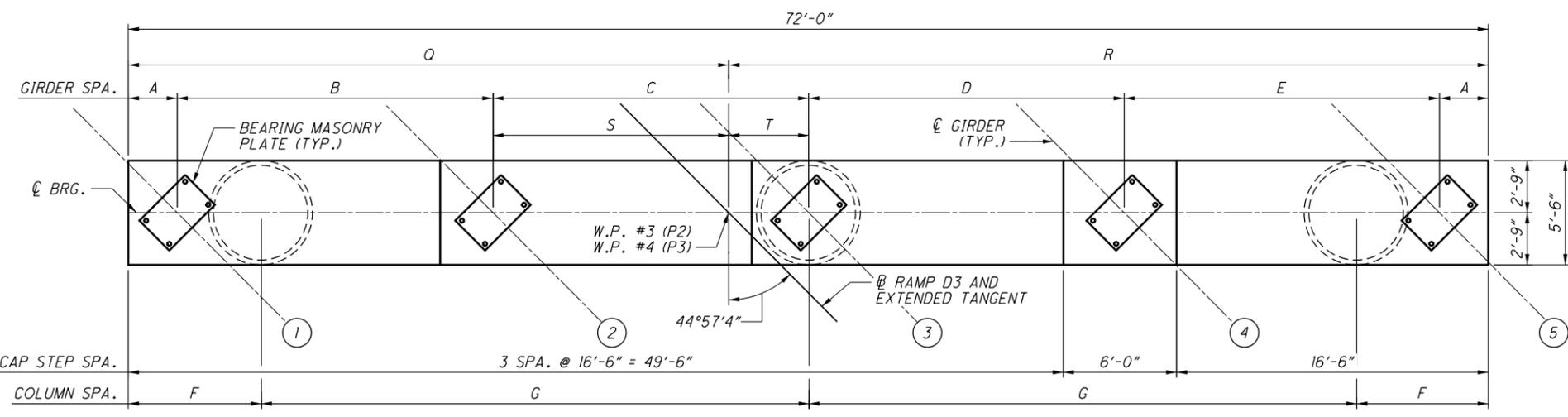
LEGEND:

- △ 4-P1103 ALTERNATING W/ 4-P1104 (EACH END), 8-P1107 STAGGERED
- △△ 4-P1105 ALTERNATING W/ 4-P1106 (EACH END), 8-P1107 STAGGERED
- △△△ 2 MATS OF 4-P1101 ALTERNATING W/ 4-P1102 IN ALL DIRECTIONS, 8-P1107 STAGGERED

NOTES:

1. SECTIONS A-A, B-B AND C-C TYPICAL FOR ALL COLUMNS. FOR LOCATION OF SECTIONS A-A TO E-E AND ADDITIONAL NOTES, SEE SHEET [25/70].
2. CAP TO BE PAID FOR UNDER ITEM 511 - CLASS QC4 MASS CONCRETE, SUBSTRUCTURE WITH QC/QA.
3. FOR TRANSVERSE SPACING OF BARS IN TOP OF CAP, SEE SHEET [31/70].
4. FOR REINFORCEMENT NOTES, SEE SHEET [67/70].

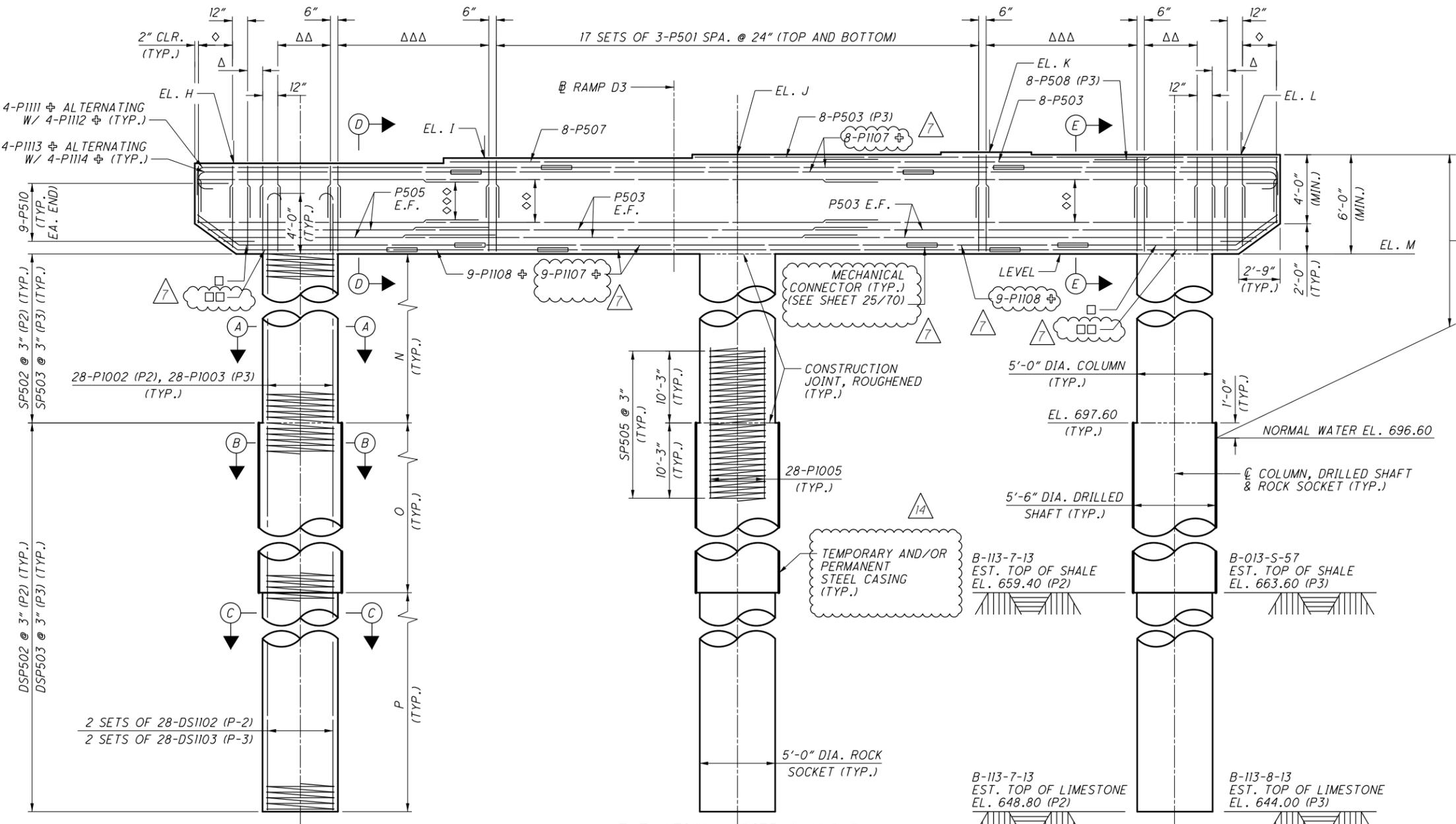
NO.	DESCRIPTION	REV. BY	DATE
7	ADJUSTED LEADER	ACW	11/20/23
14	EDITED CALLOUT	ACW	1/3/24



- LEGEND:**
- # DENOTES PROPOSED GIRDER NUMBER
 - Δ 2 SETS OF 3-P501 SPA. @ 12" (TOP AND BOTTOM)
 - ΔΔ 4 SETS OF 2-P501 SPA. @ 24" (TOP AND BOTTOM)
 - ΔΔΔ 22 SETS OF 3-P501 SPA. @ 6" (TOP AND BOTTOM)
 - ◇ 4 SETS OF 3 SERIES OF P502 SPA. @ 12" (TOP AND BOTTOM)
 - ◇◇ 7-P503 E.F.
 - ◇◇◇ 7-P505 E.F.
 - 5-P1109 + ALTERNATING W/ 4-P1110 +
 - ◇ 4-P1109 ALTERNATING W/ 5-P1110 (WITH CROSSES FOR MECH CONNECTORS)
 - + DENOTES BAR REQUIRING MECHANICAL CONNECTOR

MIN. REBAR LAP

#5 VERT. = 2'-5"
#5 HORIZ. = 3'-5"
#10 = 10'-3"
#11 = 12'-7"

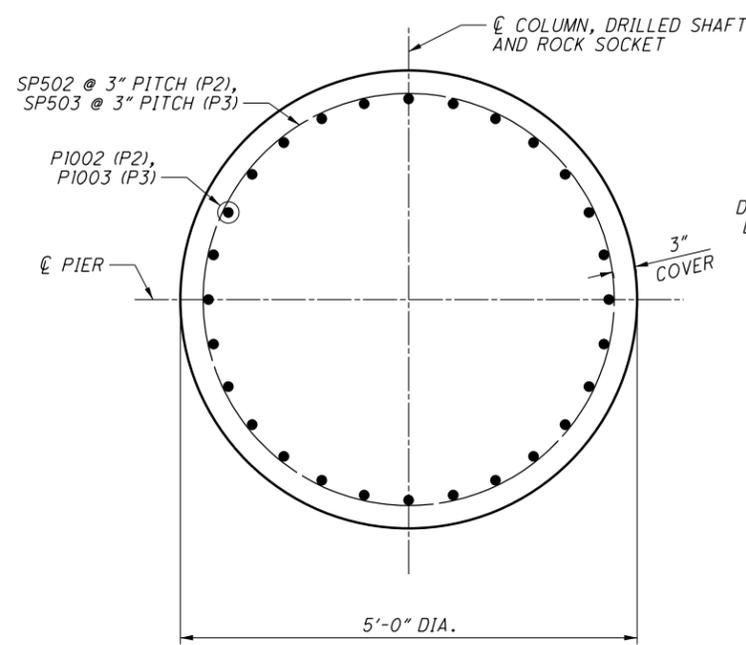


LIMITS OF CONCRETE SURFACE SEALING. SEAL ENTIRE SURFACE AREA WITH EPOXY-URETHANE

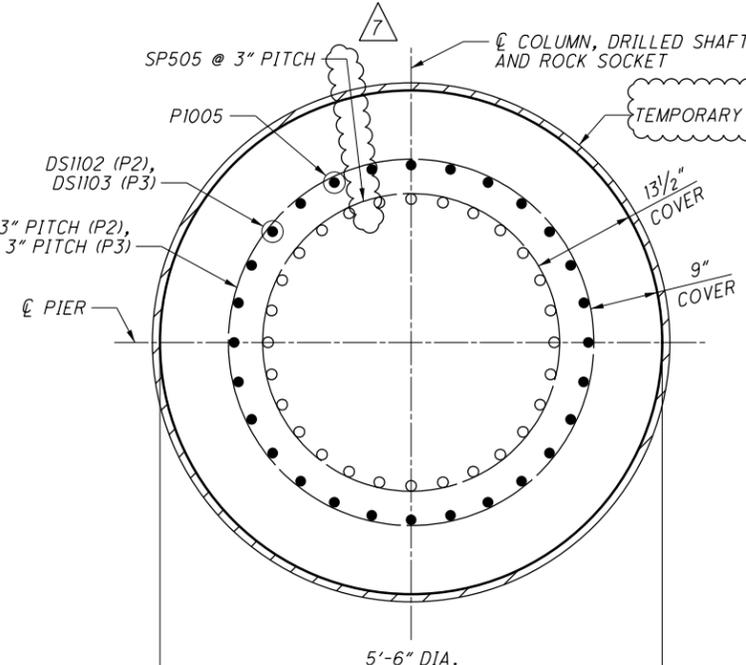
	PIER 2	PIER 3
A	3'-0 1/2"	2'-7"
B	16'-5 7/8"	16'-8 5/8"
C	16'-5 3/4"	16'-8 5/8"
D	16'-5 5/8"	16'-8 3/8"
E	16'-5 1/2"	16'-8 1/4"
F	7'-5"	7'-0"
G	28'-7"	29'-0"
H	732.82	732.53
I	733.08	732.87
J	733.24	733.11
K	733.30	733.26
L	733.07	733.10
M	726.82	726.53
N	29'-2 3/4"	28'-11 1/8"
O	38'-2 3/8"	34'-0"
P	11'-0"	20'-0"
Q	34'-4 1/8"	31'-9 1/2"
R	37'-7 7/8"	40'-2 1/2"
S	14'-9 3/4"	12'-5 3/4"
T	1'-8"	4'-2 7/8"

- NOTES:**
- FOR ADDITIONAL NOTES, SEE SHEET 25/70.
 - FOR SECTIONS A-A TO E-E, SEE SHEET 28/70.

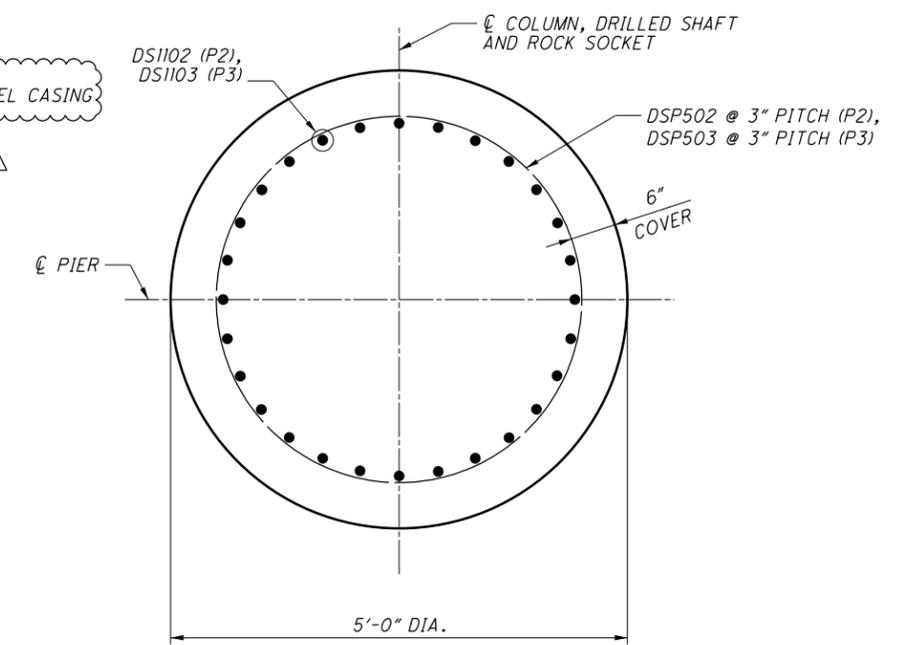
NO.	DESCRIPTION	REV. BY	DATE
7	CALLOUTS/SYMBOLS EDITED	ACW	11/20/23
14	EDITED CALLOUT	ACW	1/3/24



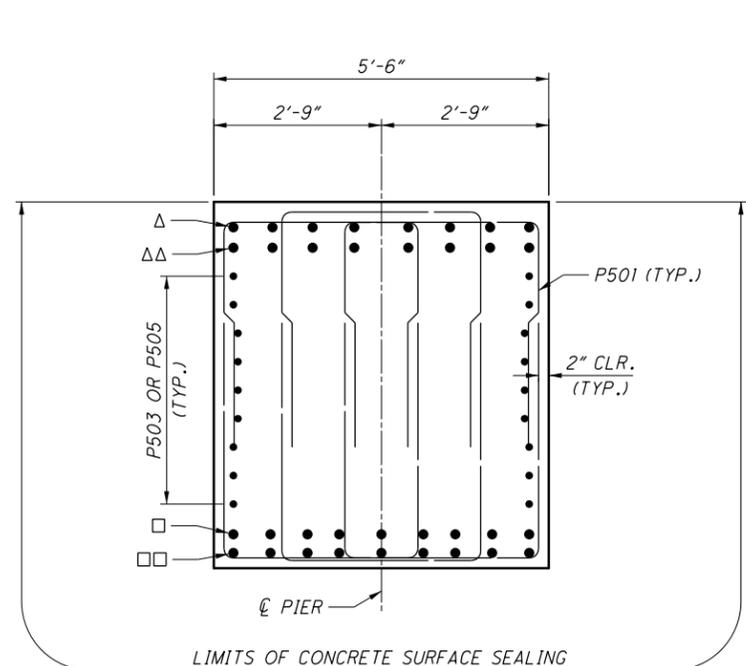
SECTION A-A
COLUMN



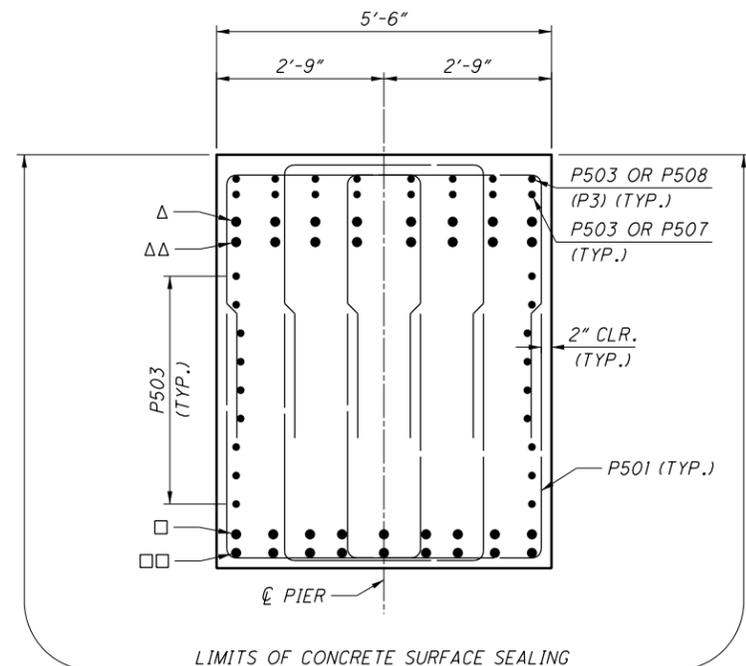
SECTION B-B
DRILLED SHAFT



SECTION C-C
ROCK SOCKET



SECTION D-D



SECTION E-E

LEGEND:

- △ 4-P1111 ALTERNATING W/ 4-P1112 (EACH END) OR 8-P1107 STAGGERED
- △△ 4-P1113 ALTERNATING W/ 4-P1114 (EACH END) OR 8-P1107 STAGGERED
- 5-P1109 ALTERNATING W/ 4-P1110, 9-P1107 STAGGERED OR 9-P1108 STAGGERED
- 4-P1109 ALTERNATING W/ 5-P1110, 9-P1107 STAGGERED OR 9-P1108 STAGGERED

NOTES:

1. SECTIONS A-A, B-B AND C-C TYPICAL FOR ALL COLUMNS. FOR LOCATION OF SECTIONS A-A TO E-E, SEE SHEET [27/70].
2. FOR ADDITIONAL NOTES AND DRILLED SHAFT REBAR DATA, SEE SHEET [26/70].

NO.	DESCRIPTION	REV. BY	DATE
7	LEADER ADJUSTED	ACW	11/20/23
14	EDITED CALLOUT	ACW	1/3/24

DESIGN AGENCY: **ms consultants, inc.**
2221 Schrock Road, Columbus, Ohio 43229

DESIGNED: ABD
CHECKED: FBW

DRAWN: ABD
REVISED:

REVIEWED: GLG/YSJ 03/08/23
STRUCTURE FILE NUMBER: 2510026

DATE:

DATE:

DATE:

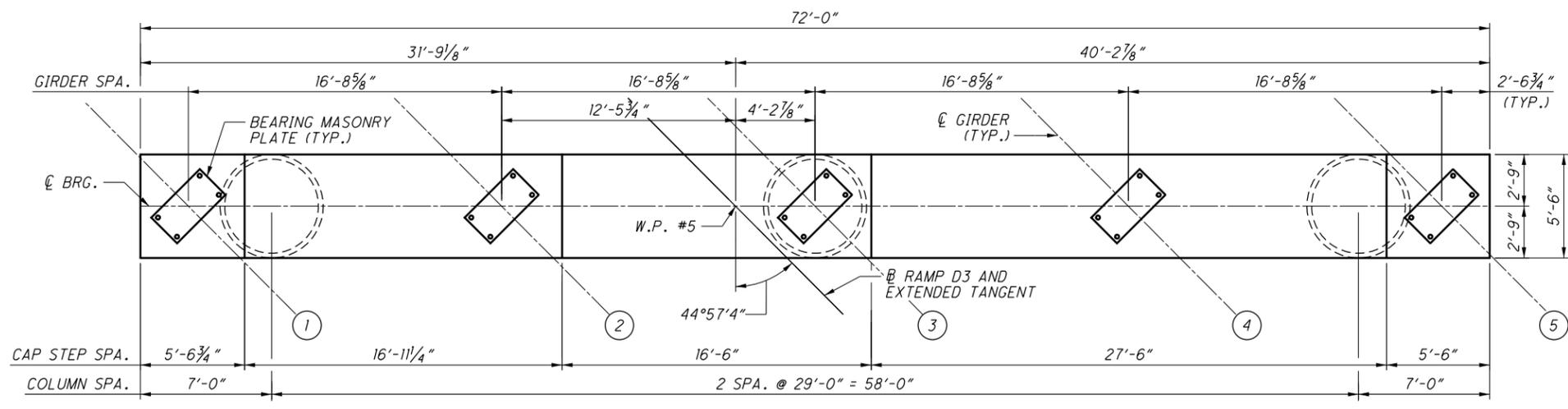
PIER 2 AND 3 SECTIONS
BRIDGE NO. FRA-70-1323C
RAMP D3 OVER SCIOTO RIVER

FRA-70-13.10
PID No. 77372

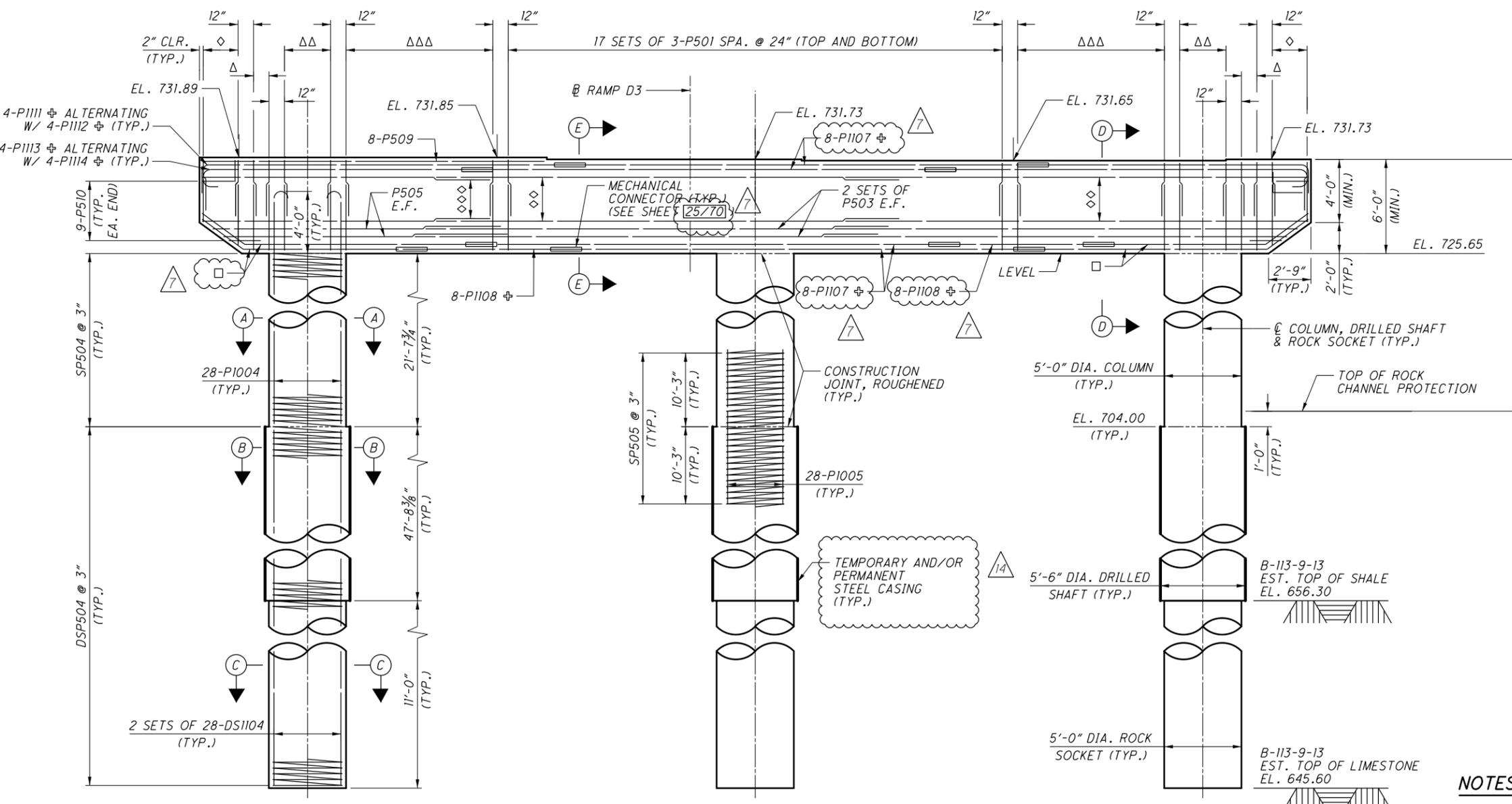
28/70

558
702

ms consultants, inc.



PLAN - PIER 4



ELEVATION - PIER 4
LOOKING UPSTATION

LEGEND:

- # DENOTES PROPOSED GIRDER NUMBER
- Δ 2 SETS OF 3-P501 SPA. @ 12\"/>
- ΔΔ 4 SETS OF 2-P501 SPA. @ 24\"/>
- ΔΔΔ 12 SETS OF 3-P501 SPA. @ 12\"/>
- ◇ 4 SETS OF 3 SERIES OF P502 SPA. @ 12\"/>
- ◇◇ 7-P503 E.F.
- ◇◇◇ 7-P505 E.F.
- 4-P1115 ⊕ ALTERNATING W/ 4-P1116 ⊕ IN ALL DIRECTIONS
- ⊕ DENOTES BAR REQUIRING MECHANICAL CONNECTOR

LIMITS OF CONCRETE SURFACE SEALING.
SEAL ENTIRE SURFACE AREA WITH
EPOXY-URETHANE

MIN. REBAR LAP	
#5 VERT.	= 2'-5"
#5 HORIZ.	= 3'-5"
#10	= 10'-3"
#11	= 12'-7"

- NOTES:**
1. FOR ADDITIONAL NOTES, SEE SHEET 25/70.
 2. FOR SECTIONS A-A TO E-E, SEE SHEET 30/70.

NO.	DESCRIPTION	REV. BY	DATE
7	CALLOUTS/LEGEND EDITED	ACW	11/20/23
14	EDITED CALLOUT	ACW	1/3/24

DESIGN AGENCY: ms consultants, inc. 2221 Schrock Road, Columbus, Ohio 43229

DESIGNED: ABD
CHECKED: FBW

DRAWN: ABD
REVISED:

REVIEWED: GLG/YSJ 03/08/23
STRUCTURE FILE NUMBER: 2510026

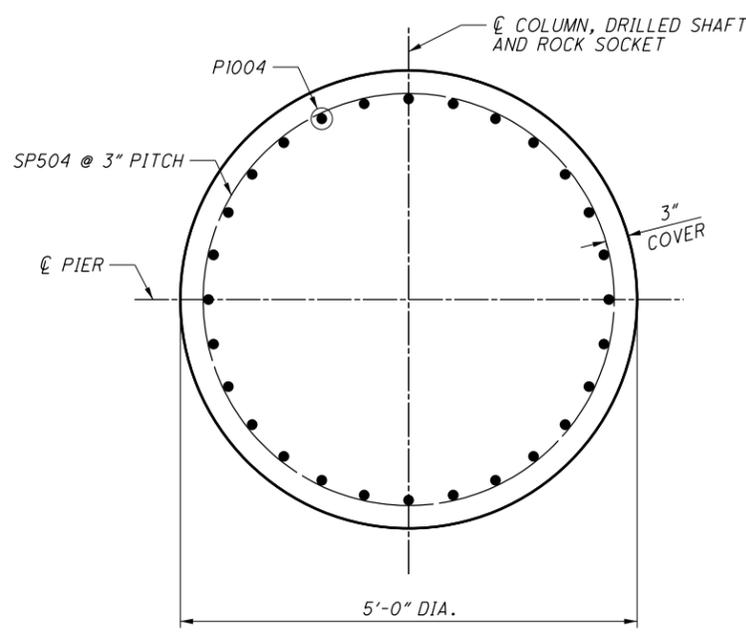
PIER 4 PLAN AND ELEVATION
BRIDGE NO. FRA-70-1323C
RAMP D3 OVER SCIOTO RIVER

FRA-70-13.10
PID No. 77372

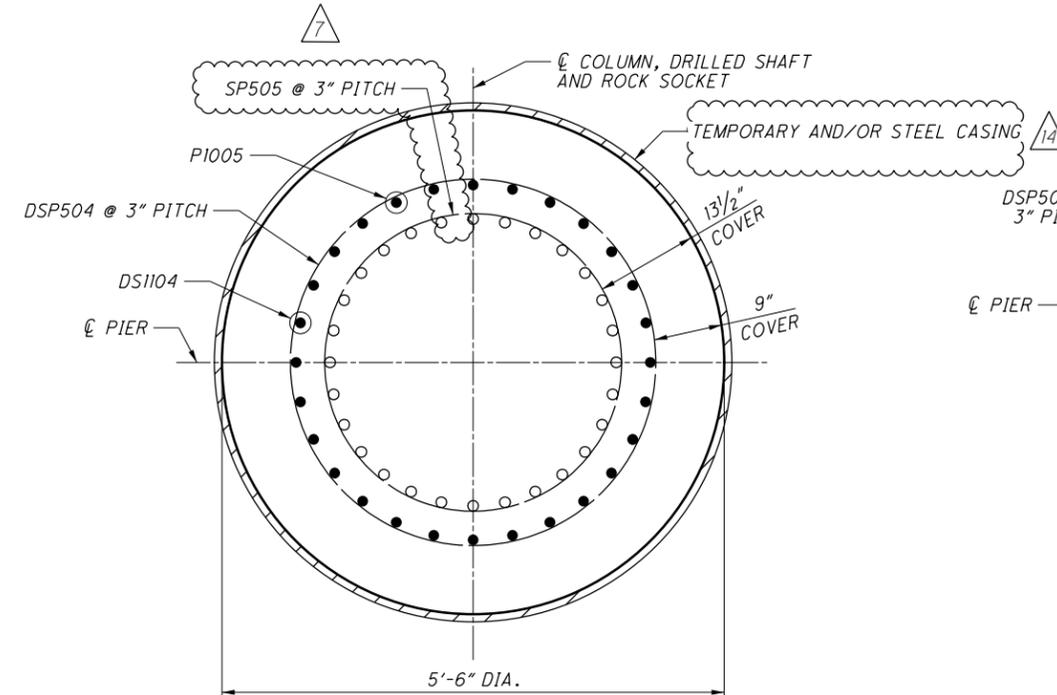
29 / 70

559
702

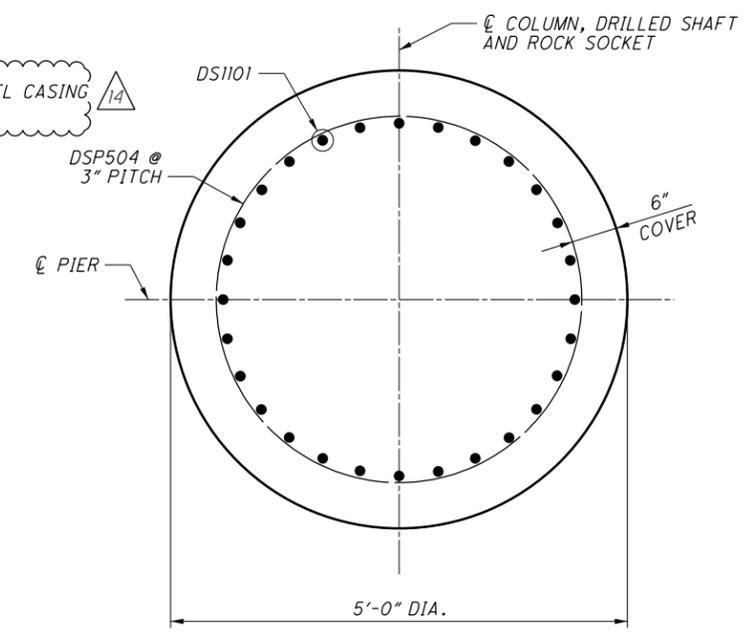
ms consultants, inc.



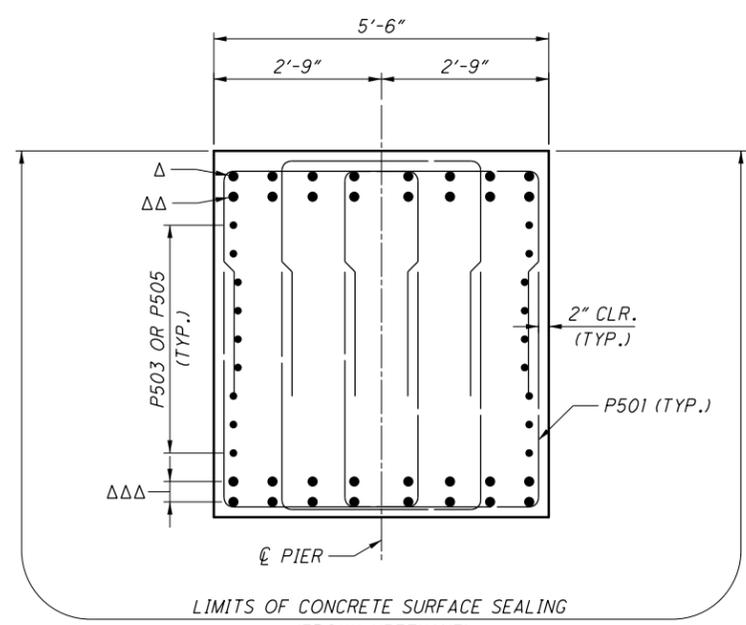
SECTION A-A
COLUMN



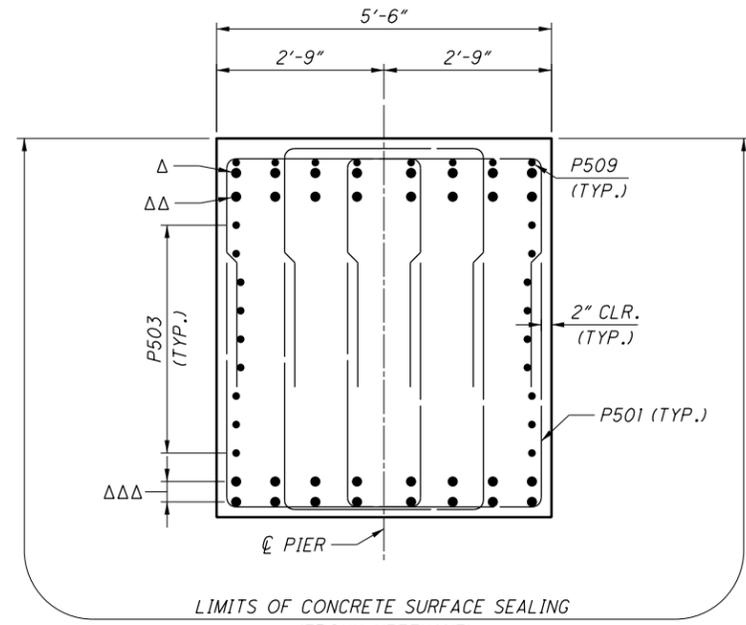
SECTION B-B
DRILLED SHAFT



SECTION C-C
ROCK SOCKET



SECTION D-D



SECTION E-E

LEGEND:

- △ 4-P1111 ALTERNATING W/ 4-P1112 (EACH END) OR 8-P1107 STAGGERED
- △△ 4-P1113 ALTERNATING W/ 4-P1114 (EACH END) OR 8-P1107 STAGGERED
- △△△ 2 MATS OF 4-P1115 ALTERNATING W/ 4-P1116 (IN ALL DIRECTIONS @ EACH END) OR 2 MATS OF 9-P1107 STAGGERED IN ALL DIRECTIONS W/ P1108 STAGGERED BETWEEN P1107 AND P1115/P1116 (SEE ELEVATION)

NOTES:

1. SECTIONS A-A, B-B AND C-C TYPICAL FOR ALL COLUMNS. FOR LOCATION OF SECTIONS A-A TO E-E, SEE SHEET [29/70].
2. FOR ADDITIONAL NOTES AND DRILLED SHAFT REBAR DATA, SEE SHEET [26/70].

NO.	DESCRIPTION	REV. BY	DATE
7	UPDATING CALLOUTS	ATM	11/20/23
14	EDITED CALLOUT	ACW	1/3/24

DESIGN AGENCY: **ms consultants, inc.**
2221 Schrock Road, Columbus, Ohio 43229

DESIGNED: ABD, CHECKED: FBW
DRAWN: ABD, REVISED:
REVIEWED: GLG/VSJ, DATE: 03/08/23
STRUCTURE FILE NUMBER: 2510026

PIER 4 SECTIONS
BRIDGE NO. FRA-70-1323C
RAMP D3 OVER SCIOTO RIVER

FRA-70-13.10
PID No. 77372

30/70

560
702

ITEM 615 - PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, APP, TYPE 1:
ITEM 615 - PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, APP, TYPE 2:
ITEM 615 - PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, APP, TYPE 3:
ITEM 615 - PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, APP, TYPE 4:

THIS ITEM SHALL BE UTILIZED FOR THE PAVEMENT REPAIRS NEEDED DURING THIS CONSTRUCTION PROCESS. ALL AREAS TO BE REPAIRED SHALL BE LOCATED BY THE ENGINEER. IT IS LIKELY THAT REPAIRS WILL BE NEEDED PRIOR TO EACH PHASE SWITCH. GREAT CARE SHALL BE TAKEN TO MAINTAIN THE EXISTING PAVEMENT CROSS SLOPE AS WELL AS ALL LONGITUDINAL SLOPES. THE TYPE OF REPAIR SHALL BE DETERMINED BY THE PROJECT ENGINEER. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS REQUIRED FOR MAINTENANCE OF TRAFFIC FOR PAVEMENT REPAIRS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

TYPE 1 - IS TO BE USED WHEN YOU NEED TO MILL & FILL AN AREA OF VARYING LENGTH AND HAVE AN AVERAGE WIDTH OF NOT LESS THAN 2 FEET.

TYPE 2 - IS TO BE USED FOR FIXING THE LONGITUDINAL JOINT ISSUES OF VARYING LENGTH AND HAVE A CONSISTENT WIDTH OF 2 FEET.

TYPE 3 - IS TO BE USED FOR DEEPER REPAIRS OF VARYING LENGTH AND WILL HAVE AN AVERAGE WIDTH OF NOT LESS THAN 4 FEET.

TYPE 4 - IS TO BE USED FOR COMPOSITE PAVEMENT REPAIRS OF VARYING LENGTH AND WILL HAVE AN AVERAGE WIDTH OF NOT LESS THAN 3 FEET.

ALL COSTS ASSOCIATED WITH REMOVING AND REPLACING PAVEMENT AND TACK COAT FOR THE REPAIRS SHALL BE INCIDENTAL TO ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN.

MAINTENANCE OF TRAFFIC FOR MAKING PAVEMENT REPAIRS

PROVIDE LANE CLOSURES PER ALL APPLICABLE MAINTENANCE OF TRAFFIC STANDARD CONSTRUCTION DRAWINGS A MINIMUM OF 24 HOURS PRIOR TO PERFORMING PAVEMENT REPAIRS TO ALLOW THE ENGINEER TO IDENTIFY AND MARK THE AREAS OF THE PAVEMENT IN NEED OF REPAIRS

PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS, LEO HOURS, AND INCIDENTALS NEEDED TO PERFORM THE ABOVE LISTED WORK IS CONSIDERED INCIDENTAL TO MAINTAINING TRAFFIC ON THE PROJECT AND WILL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 MAINTAINING TRAFFIC.

APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS:

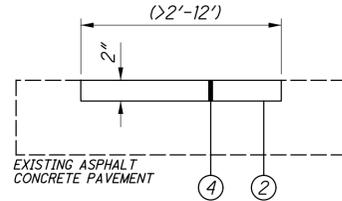
PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE APPROVED MOT EXCEPTION(S) PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

APPROVED MOT EXCEPTION(S) INCLUDE:
 - CLOSURE OF 315SB TO 70EB FOR 3 YEARS FOR 77372
 - CLOSURE OF 70WB TO 315NB FOR 6 MONTHS IN TOTAL
 - MONITOR TRAFFIC CONDITIONS FOR POSSIBLE CONFIGURATION ADJUSTMENTS AT THE 670EB TO 71SB DETOUR RAMP

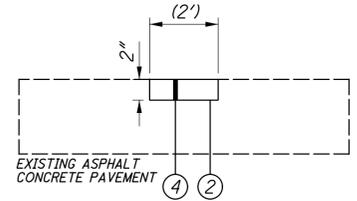
A MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF 30 CALENDAR DAYS PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER AND THE CITY OF COLUMBUS AS WELL AS THE CONTRACTOR, WORKSITE TRAFFIC SUPERVISOR (WTS) AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.

IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO THE OFFICE OF ROADWAY ENGINEERING, STATEWIDE TMC, DWZTM AND SPECIAL HAULING PERMITS AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE. REFERENCE "EXCEPTION REQUEST APPROVAL DATED 1/24/23 FOR PID 77372" IN THE NOTIFICATION AND OTHER CORRESPONDENCE.

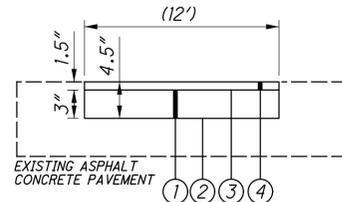
ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTION(S) LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE MOT EXCEPTION COMMITTEE (MOTEC). IN THE EVENT THAT SUCH CHANGES ARE PROPOSED, THE REQUEST SHALL BE COORDINATED THROUGH THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM) A MINIMUM OF 30 CALENDAR DAYS PRIOR TO THE DESIRED IMPLEMENTATION DATE. IF THE DISTRICT AGREES WITH THE PROPOSED CHANGES THE DWZTM SHALL SEEK APPROVAL FROM THE MOTEC. IN THE EVENT THE PROPOSED CHANGES ARE APPROVED IN WRITING, THE CLOSURES ARE STILL SUBJECT TO NOTIFICATION REQUIREMENTS WITHIN THIS NOTE PRIOR TO IMPLEMENTATION.



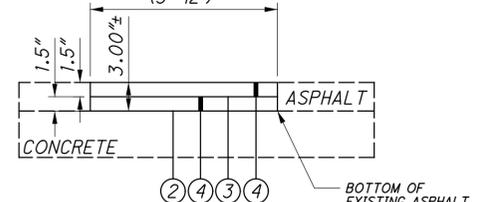
TYPE 1 DETAIL
PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN TYPE 1



TYPE 2 DETAIL
PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN TYPE 2



TYPE 3 DETAIL
PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN TYPE 3



TYPE 4 DETAIL
PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN TYPE 4

LEGEND:

- ① ITEM 301 - ASPHALT CONCRETE BASE, PG64-22
- ② ITEM 407 - TACK COAT @ 0.075 PER SQ. YD.
- ③ ITEM 407 - TACK COAT FOR INTERMEDIATE @ 0.05 PER SQ. YD.
- ④ ITEM 441 - TYPE 1 (AS DESCRIBED IN CMS 615.05)

ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, APP, TYPE 1 = 200 S.Y.
 ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, APP, TYPE 2 = 200 S.Y.
 ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, APP, TYPE 3 = 200 S.Y.
 ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, APP, TYPE 4 = 200 S.Y.

SUMMARY OF RAMP/ROAD CLOSURES				
MOT PHASE	STREET	LOCATION	MAX DURATION	DISINCENTIVE
1	FULTON/3RD ST RAMP	FULTON/3RD STREET INTERSECTION	PERMANENT	N/A
1 THRU 4B	3RD ST	3RD STREET BRIDGE	DURATION OF PROJECT	N/A
1 THRU 4B	4TH ST	4TH STREET BRIDGE	DURATION OF PROJECT	N/A
1, 4A, AND 4B	I-70/71	BETWEEN 315 & 70/71 INTERCHANGE AND EAST 70/71 INTERCHANGE	SEE TABLE ON THIS SHEET	***

BRIDGE DESCRIPTION	STRUCTURE #	WORK TYPE	DAYS	CLOSURE/DETOUR TIME***	# TIMES ALLOWED	DETOUR DETAILS ON SHEETS
3RD ST. BRIDGE	FRA-33-1747C	DEMOLITION	WEEKEND *	FRI 10PM - MON 5 AM	1**	69 - 72
		BEAM ERECTION	WEEKEND *	FRI 10PM - MON 5 AM	2**	
		DECK POUR	NIGHTTIME CLOSURE	FRI 10PM - MON 5 AM	1	
		DECK POUR	NIGHTTIME CLOSURE	FRI 10PM - MON 5 AM	1	
3RD ST. BRIDGE EAST CAP		DECK POUR	NIGHTTIME CLOSURE	FRI 10PM - MON 5 AM	1	
3RD ST. BRIDGE WEST CAP		DECK POUR	NIGHTTIME CLOSURE	FRI 10PM - MON 5 AM	1	
4TH ST. BRIDGE	FRA-23-1075C	DEMOLITION	WEEKEND *	FRI 10PM - MON 5 AM	1**	69 - 72
		BEAM ERECTION	WEEKEND *	FRI 10PM - MON 5 AM	2**	
		DECK POUR	NIGHTTIME CLOSURE	FRI 10PM - MON 5 AM	1	
*		THE CONTRACTOR MAY CHOOSE TO COMPLETE THIS WORK OVER THE COURSE OF NIGHTLY CLOSURES (MONDAY THRU SUNDAY) IN LIEU OF A WEEKEND CLOSURE. NIGHTLY CLOSURES SHALL TAKE PLACE BETWEEN 10PM AND 5AM.				
**		IF WORK IS PERFORMED VIA NIGHTLY CLOSURES, THE NUMBER OF CLOSURES REQUIRED SHALL BE APPROVED BY THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC.				
***		DISCINCENTIVES WILL BE ASSESSED PER LANE PER MINUTE AT THE RATES SHOWN IN THE LANE VALUE CONTRACT TABLE FOR ANY CLOSURE OUTSIDE OF THE CLOSURE/DETOUR TIMES				

SHEET NUMBER										PARTICIPATION		ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
42	43	44	45	46	47	48	50	260	01/IMS/04								
	1,000								1000		607	30001	1000	FT	FENCE, SNOW, AS PER PLAN	44	
	2000								2000		614	1110	2000	hour	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE		
2									2		SPECIAL	614E11300	2	EACH	WORK ZONE TRAFFIC SIGNAL	43	
							27976		27976		614	11630	27976	FT	INCREASED BARRIER DELINEATION		
							14		14		614	12380	14	EACH	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (UNIDIRECTIONAL)		
				LS					LS		614	12420	LS	EACH	DETOUR SIGNING		
					11				11		614	12470	11	EACH	WORK ZONE SPEED LIMIT SIGN		
					10				10		614	12484	10	EACH	WORK ZONE INCREASED PENALTIES SIGN		
					50				50		614	12500	50	EACH	REPLACEMENT SIGN		
2	300								300		614	12600	300	EACH	REPLACEMENT DRUM		
									2		614	12756	2	EACH	WORK ZONE CROSSOVER LIGHTING SYSTEM		
							3645		3645		614	12800	3645	EACH	WORK ZONE RAISED PAVEMENT MARKER		
							1471		1471		614	13310	1471	EACH	BARRIER REFLECTOR, TYPE I, BIDIRECTIONAL		
							48		48		614	18601	48	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	45	
							3.41		3.41		614	20056	3.41	MILE	WORK ZONE LANE LINE, CLASS I, 6", 807 PAINT		
							0.62		0.62		614	20200	0.62	MILE	WORK ZONE LANE LINE, CLASS I, 4", 740.06, TYPE I		
							6.24		6.24		614	20560	6.24	MILE	WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT		
							13.08		13.08		614	22056	13.08	MILE	WORK ZONE EDGE LINE, CLASS I, 6", 807 PAINT		
							1.42		1.42		614	22200	1.42	MILE	WORK ZONE EDGE LINE, CLASS I, 4", 740.06, TYPE I		
							4.46		4.46		614	22360	4.46	MILE	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT		
							30704		30704		614	23110	30704	FT	WORK ZONE CHANNELIZING LINE, CLASS I, 12", 807 PAINT		
							275		275		614	23400	275	FT	WORK ZONE CHANNELIZING LINE, CLASS I, 8", 740.06, TYPE I		
							5427		5427		614	23690	5427	FT	WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT		
							7974		7974		614	24100	7974	FT	WORK ZONE DOTTED LINE, CLASS I, 4", 807 PAINT		
							857		857		614	24400	857	FT	WORK ZONE DOTTED LINE, CLASS I, 740.06, TYPE I		
							8872		8872		614	24612	8872	FT	WORK ZONE DOTTED LINE, CLASS III, 6", 642 PAINT		
							1159		1159		614	25000	1159	FT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I		
							53		53		614	26400	53	FT	WORK ZONE STOP LINE, CLASS I, 740.06, TYPE I		
							80		80		614	26610	80	FT	WORK ZONE STOP LINE, CLASS III, 642 PAINT		
							466		466		614	27070	466	FT	WORK ZONE CROSSWALK LINE, CLASS I, 12", 740.06, TYPE I		
							12		12		614	30400	12	EACH	WORK ZONE ARROW, CLASS I, 740.06, TYPE I		
							2		2		614	30650	2	EACH	WORK ZONE ARROW, CLASS III, 642 PAINT		
LS							LS		LS		615	10000	LS		ROADS FOR MAINTAINING TRAFFIC		
							4600		4600		615	20000	4600	SY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A		
							255	374	629		615	25000	629	SY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B		
							200		200		615	25001	200	SY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN, TYPE 1	48	
							200		200		615	25001	200	SY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN, TYPE 2	48	
							200		200		615	25001	200	SY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN, TYPE 3	48	
							200		200		615	25001	200	SY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN, TYPE 4	48	
							550		550		616	10000	550	MGAL	WATER		
							2		2		622	41050	2	EACH	PORTABLE BARRIER, "Y" CONNECTOR		
							29484		29484		622	41101	29484	FT	PORTABLE BARRIER, UNANCHORED, AS PER PLAN	46	
							48		48		808	18700	48	SNMT	DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY		
							48		48		896	00010	48	SNMT	PORTABLE NON-INTRUSIVE TRAFFIC SENSOR, CLASS I		
							48		48		896	00020	48	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN		

MAINTENANCE OF TRAFFIC GENERAL SUMMARY

FRA - 70-14.05

NO.	DESCRIPTION	REV. BY	DATE
2	REV. NOTE/BRICK X-WALK TEMP.	CWL	10-13-23
5	ADDED WZ CLASS III	CWL	11-3-23
14	6A/4B OVERLAP	CWL	12-28-23

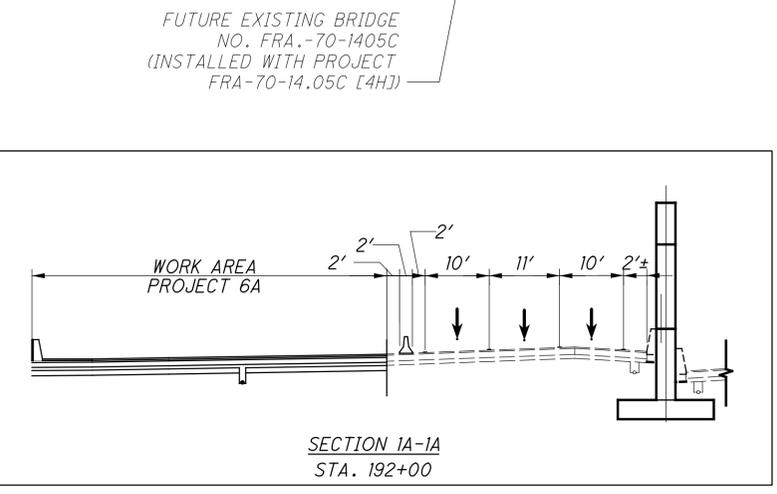
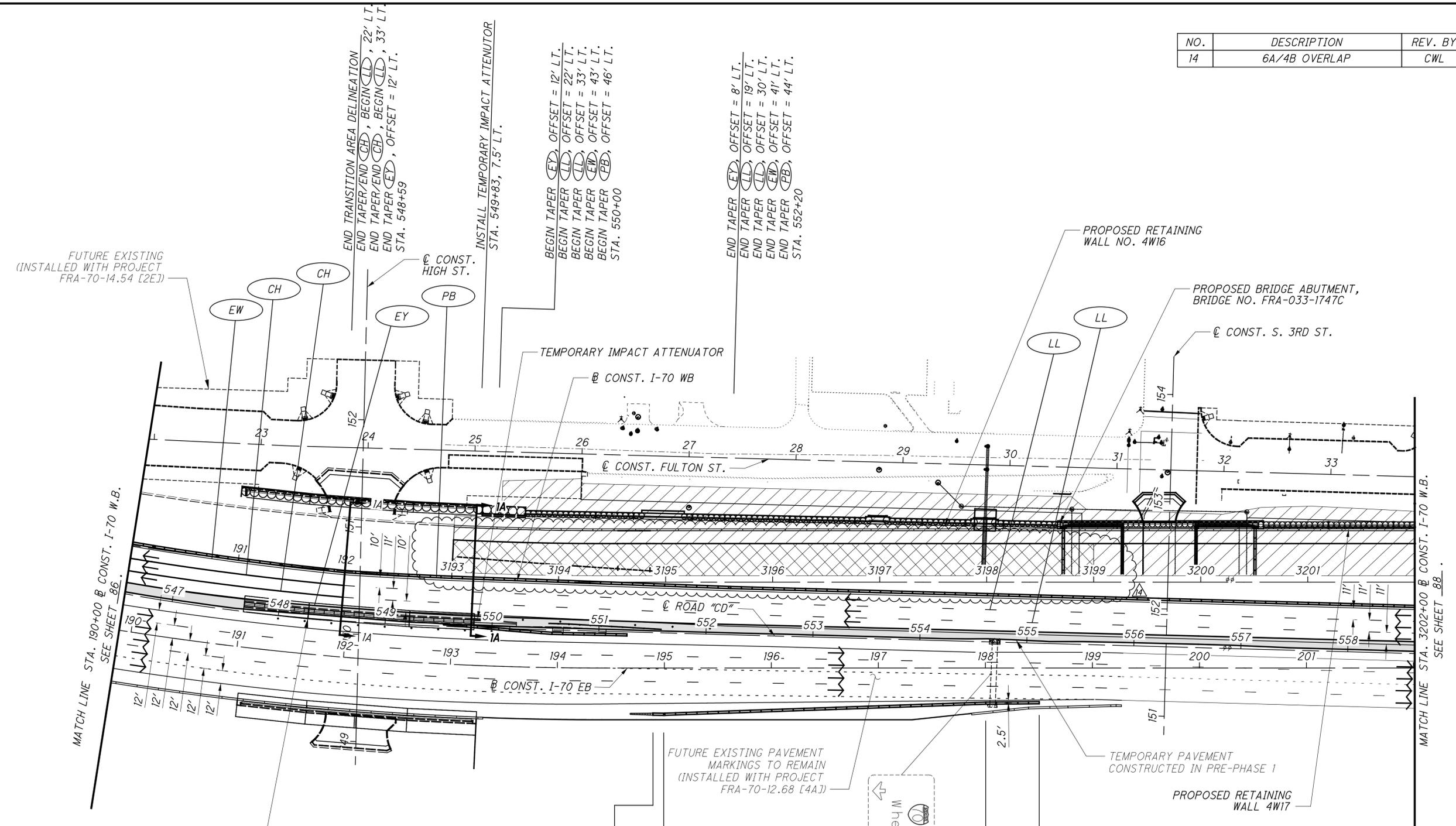
0:\2015\2015370\FRA\6063\NOT SHEETS\6063HP182.DGN
 12/28/2023
 7:17:17 AM
 0001\6151D.LIGER

NO.	DESCRIPTION	REV. BY	DATE
14	6A/4B OVERLAP	CWL	12-28-23



CALCULATED BY: MSS
 CHECKED BY: AKF
MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 190+00.00 TO STA. 3202+00.00

FRA-70-14.05



END TAPER/END (CH), BEGIN (LL), 22' LT.
 END TAPER/END (CH), BEGIN (LL), 33' LT.
 END TAPER (EY), OFFSET = 12' LT. STA. 548+59
 INSTALL TEMPORARY IMPACT ATTENUATOR STA. 549+83, 7.5' LT.
 BEGIN TAPER (EY), OFFSET = 12' LT.
 BEGIN TAPER (LL), OFFSET = 22' LT.
 BEGIN TAPER (LL), OFFSET = 33' LT.
 BEGIN TAPER (EW), OFFSET = 43' LT.
 BEGIN TAPER (PB), OFFSET = 46' LT. STA. 550+00
 END TAPER (EY), OFFSET = 8' LT.
 END TAPER (LL), OFFSET = 19' LT.
 END TAPER (LL), OFFSET = 30' LT.
 END TAPER (EW), OFFSET = 41' LT.
 END TAPER (PB), OFFSET = 44' LT. STA. 552+20

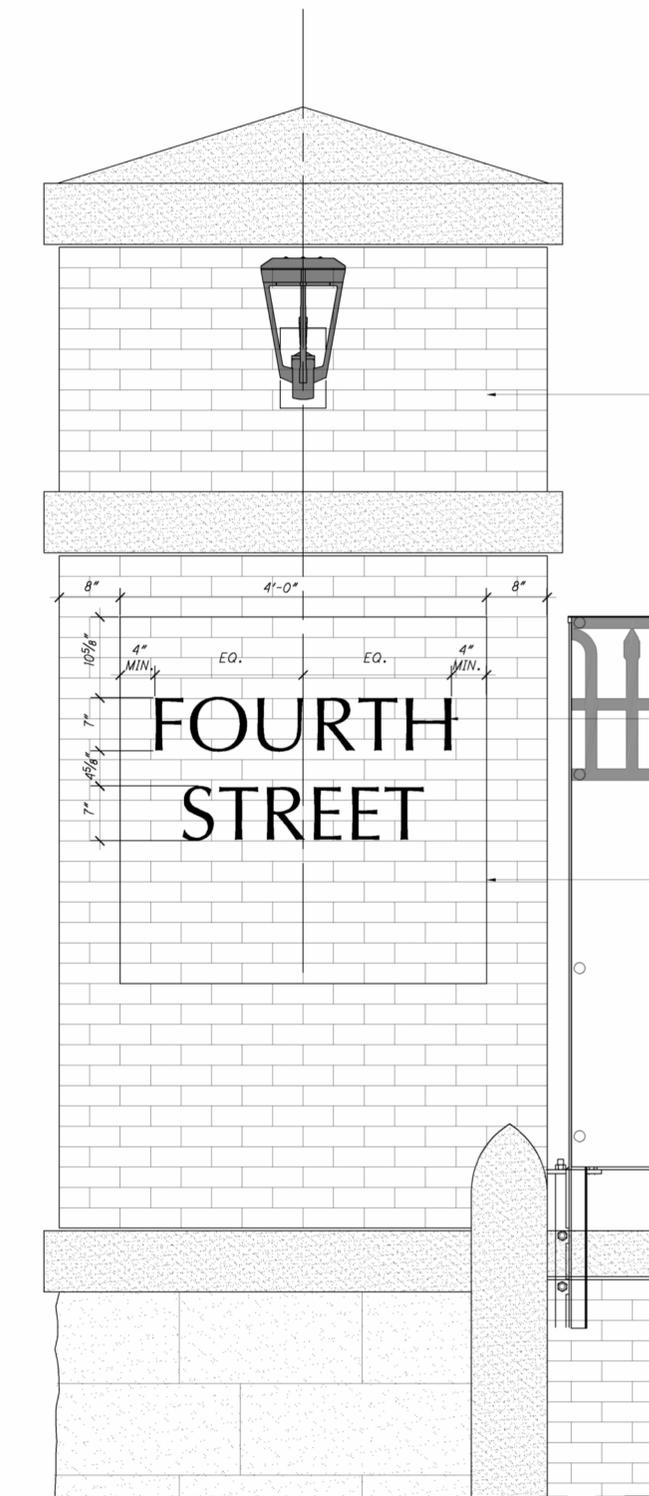
BEGIN TAPER (PB), OFFSET = 43.0' RT. INSTALL TEMPORARY IMPACT ATTENUATOR STA. 194+90
 BEGIN TAPER (PB), OFFSET = 43.0' RT. STA. 195+00

END TAPER (PB), OFFSET = 32.8' RT. STA. 198+00
 END (PB), OFFSET = 32.3' RT. STA. 198+50

POTENTIAL OVERLAP WITH PROJECT 6A 77372 PART 3 MOT PHASE 2 - SEE NOTE 3

- NOTES:
- THE CONTRACTOR SHALL COVER CONFLICTING SIGNING OR SHIFT SIGNS AS REQUIRED TO BE APPROPRIATELY OVER TRAFFIC AND COVER OR REMOVE CONFLICTING PAVEMENT MARKINGS PRIOR TO CONSTRUCTION.
 - FOR MAINTENANCE OF TRAFFIC LEGEND, SEE SHEET 60.
 - THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED FOR THE POTENTIAL OVERLAP WITH PROJECT 6A 77372 PART 3 MOT PHASE 2 AS DIRECTED BY THE ENGINEER:
 ITEM 614 - WORK ZONE IMPACT ATTENUATOR, (UNIDIRECTIONAL) 1 EACH
 ITEM 622 - PORTABLE BARRIER, UNANCHORED, AS PER PLAN 600 FT

P:\2016\c16805-170-71 Phase 4B\CAD\Current\Sheets\DeTail sheets\996053PD511.dwg



NOTE: PYLON STREET IDENTIFIER SIGNAGE TO BE PLACED ONLY ON THE STREET SIDE OF THE PYLON.

FOURTH STREET BRIDGE END PYLON

14

7" HT, 3/4" THICKNESS PIN SET ALUMINUM LETTERS WITH 1/2" OFFSET FROM WALL. POWDER COATED BLACK MATTE FINISH, OPTIMA TYPEFACE WITH MIN. 1.25" TRACKING BETWEEN LETTERS.

1" CORBEL RECESS IN BRICK PANEL FOR MOUNTING LETTERS, WITH LETTERING CENTERED HORIZ. WITHIN THE PANEL.

1 FOURTH ST BRIDGE PYLON STREET SIGNAGE
SCALE: 1" = 1'-0"

NO.	DESCRIPTION	REV. BY	DATE
14	DETAIL UPDATE	LW/JB	12.21.23

3555-E

732
855

FRA-70-14.05

FOURTH ST. BRIDGE
MISCELLANEOUS DETAILS

R/W DESIGNER
LHW
R/W REVIEWER
JRB

MKSK
LANDSCAPE ARCHITECTURE + URBAN PLANNING