

STATE OF OHIO  
DEPARTMENT OF TRANSPORTATION

MICROSURFACING ON IR 70 AND SR 56  
IN MADISON COUNTY WITH CRACK SEALING AND  
MINOR PAVEMENT REPAIR.



SCALE IN MILES



PORTION TO BE IMPROVED	
INTERSTATE HIGHWAY	_____
FEDERAL ROUTES	_____
STATE ROUTES	_____
COUNTY & TOWNSHIP ROADS	_____
OTHER ROADS	_____

## DESIGN DESIGNATION

MAD-70  
(0.00-8.96)  
CURRENT ADT (2017).....48,000  
DESIGN YEAR ADT (2029).....52,000  
TRUCKS (24 HR B&C).....27%  
DESIGN SPEED.....70  
LEGAL SPEED.....70  
DESIGN FUNCTIONAL CLASSIFICATION:  
0) INTERSTATE (RURAL)  
NHS.....YES

MAD-56	
(0.00-6.19)	
CURRENT ADT (2017).....	7,200
DESIGN YEAR ADT (2029).....	7,200
TRUCKS (24 HR B&C).....	5%
DESIGN SPEED.....	60
LEGAL SPEED.....	60 (45 IN SOMERFORD)
DESIGN FUNCTIONAL CLASSIFICATION:	
04 MINOR ARTERIAL (RURAL)	
NHS.....	NO

## UNDERGROUND UTILITIES

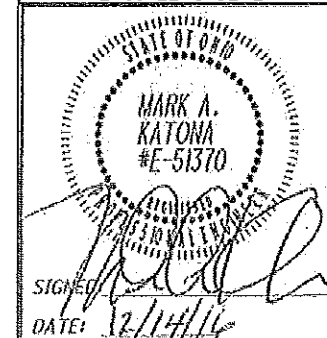
CONTACT BOTH SERVICES TWO WORKING DAYS  
BEFORE YOU DIG.



PLAN PREPARED BY:



**ENGINEERS SEAL:**



LOC	COUNTY	ROUTE	BEGIN	END	LENGTH	LATITUDE	LONGITUDE	TOWNSHIP	CITY/VILLAGE
1	MAD	70	0.00	8.96	8.96	N39°56'30"	W83°21'04"	SOMERFORD, DEER CREEK, JEFFERSON	
2	MAD	56	0.00	6.19	6.19	N39°56'14"	W83°29'39"	SOMERFORD, UNION	

## INDEX OF SHEETS

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[illegible]

**EARTH DISTURBED AREA:**

PROJECT EARTH DISTURBED AREA	N/A*
EST. CONTRACTOR EARTH DISTURBED AREA	N/A*
NOTICE OF INTENT EARTH DISTURBED AREA	N/A*
* MAINTENANCE PROJECT	

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

## 2016 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVED THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED John R. [Signature]  
DISTRICT DEPUTY DIRECTOR

DATE 12-15-16

APPROVED *James H. Gray, Jr.*  
DIRECTOR, DEPARTMENT OF TRANSPORTATION

DATE 1-10-17

MAD-70/56-0.00/0.00

# RAILROAD INVOLVEMENT!

CONSTRUCTION PROJECT NO.

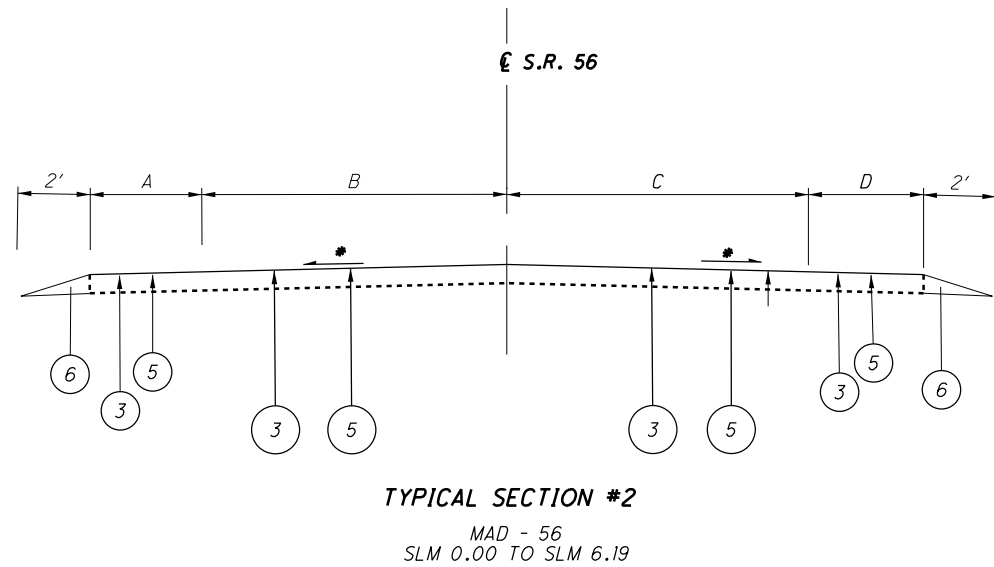
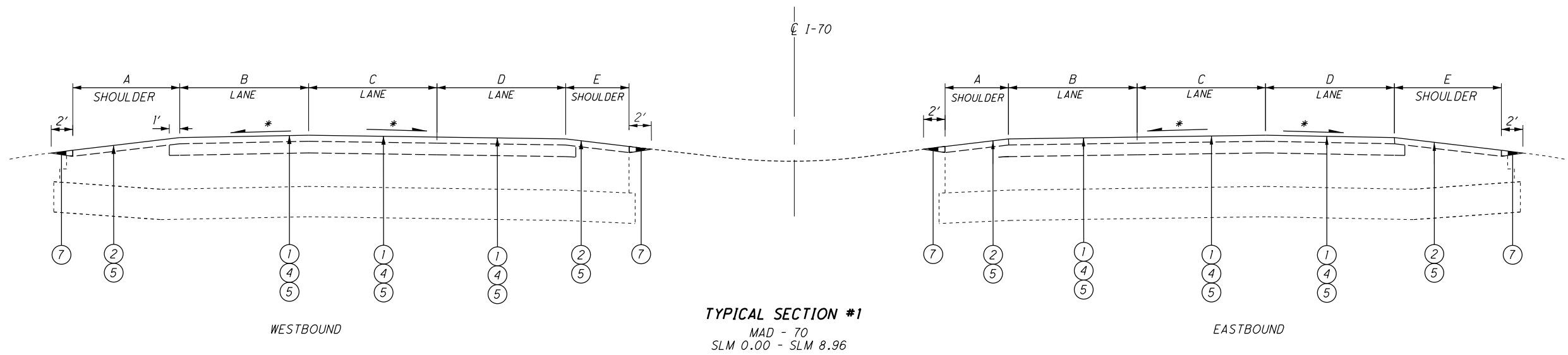
FILE NO. 101616

FEDERAL PROJECT NO.  
E161 (021)



Category	Group	Responsible Entity	Plan Split Code	Description
INTERSTATE	Pavement	STATE	01/IMS/PV	MICROSURFACING ON MAD-70-0.00 TO 8.96
STP RURAL	Pavement	STATE	02/STR/PV	MICROSURFACING ON MAD-56-0.00 TO 4.69
STP<200000	Pavement	STATE	03/5K2/PV	MICROSURFACING MAD-56-4.69 TO 6.19

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FOR PAVEMENT WIDTHS, SEE SHEET NO. 15

\* MAINTAIN EXISTING CROSS SLOPES

**LEGEND:**

- ① ITEM 421 - MICROSURFACING, SURFACE COURSE, AS PER PLAN (16 LB/SY)
- ② ITEM 421 - MICROSURFACING, SURFACE COURSE, AS PER PLAN (18 LB/SY)
- ③ ITEM 421 - MICROSURFACING, LEVELING COURSE, AS PER PLAN (22 LB/SY)
- ④ ITEM 421 - MICROSURFACING, LEVELING COURSE, AS PER PLAN (14 LB/SY)
- ⑤ ITEM 423 - CRACK SEALING, TYPE II OR III
- ⑥ ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN (1" DEPTH)
- ⑦ ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN (2" DEPTH)

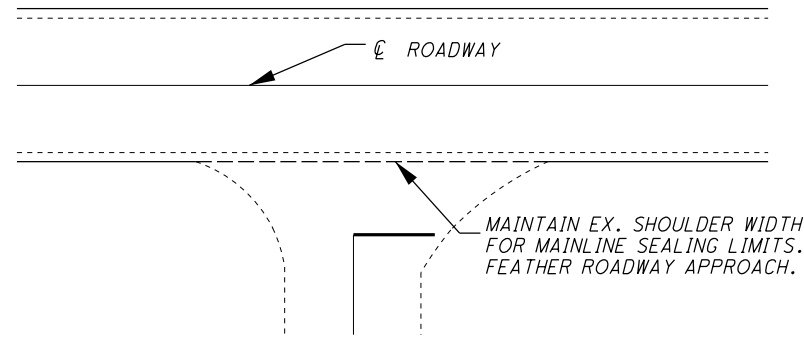
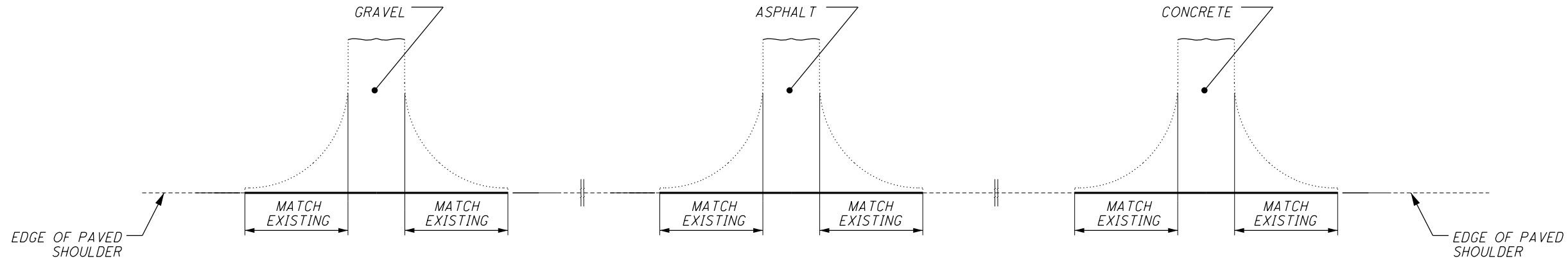
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**TYPICAL SECTIONS**

**MAD-70 / 56-0.00 / 0.00**

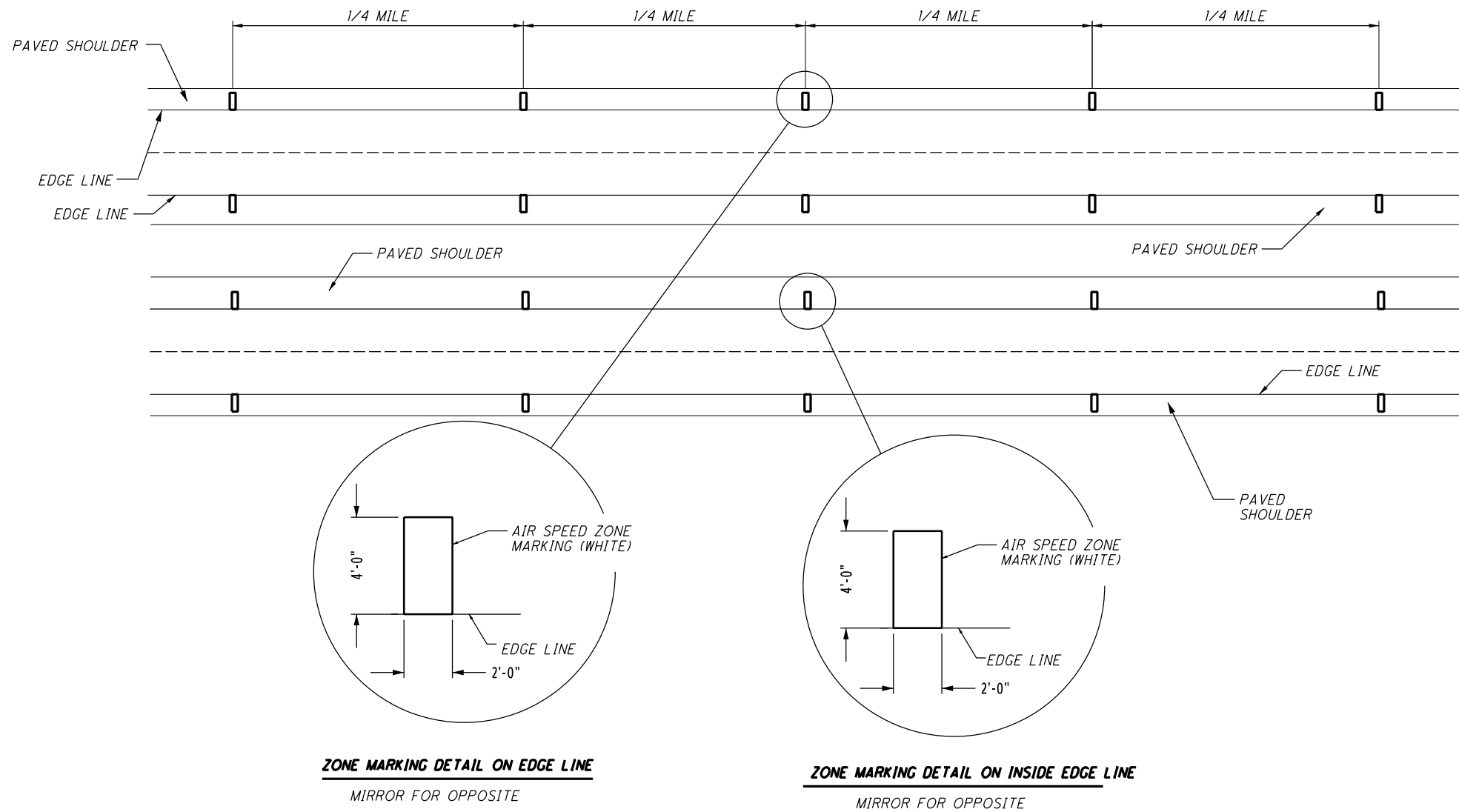
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36

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TYPICAL INTERSECTION DETAIL

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#### NOTES:

- ON 4-LANE ROADWAYS, THE MARKINGS ARE AS DETAILED.
- EACH AIR SPEED ZONE SHALL CONSIST OF WHITE 24" X 48" TRANSVERSE PAVEMENT MARKINGS.
- THE MARKINGS SHALL BE LAID OUT BY THE DISTRICT 6 REGISTERED SURVEYOR.
- THE QUARTER MILE SURFACE MEASUREMENTS (NOT HORIZONTAL PROJECTION) SHALL BE VERIFIED BY THE REGISTERED SURVEYOR.
- A LETTER OF VERIFICATION SHALL BE FILED WITH THE LOCAL OR NEAREST STATE HIGHWAY PATROL OFFICE WHICH OVERSEES SAID ZONES. A COPY SHALL BE FILED WITH THE DISTRICT SIX SURVEYOR. A SAMPLE LETTER WILL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING.
- ON I-70, THE MARKINGS ARE NOT SYMMETRIC. THE EAST BOUND MARKINGS ARE AT MM 6.37 - 7.37. THE WEST BOUND MARKINGS ARE AT MM 3.35 - 4.35
- SEE GENERAL NOTES ON SHEET 8 FOR QUANTITIES.

TYPICAL DETAILS

MAD-70 / 56 - 0.00 / 0.00

5  
36

CALCULATED  
MAK  
CHECKED  
XXX

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NOTIFICATION OF CONSTRUCTION INITIATION:

AT LEAST FOURTEEN DAYS PRIOR TO ANY CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL ADVISE THE DISTRICT OFFICE OF COMMUNICATIONS VIA EMAIL AT d06.pio@dot.ohio.gov AND THE DISTRICT WORK ZONE TRAFFIC MANAGER VIA EMAIL AT d06.mot@dot.ohio.gov OF THE ANTICIPATED START DATE OF ANY CONSTRUCTION ACTIVITIES, INCLUDING BUT NOT LIMITED TO THE PLACING OF WORK ZONE SIGNS. THE NOTIFICATION SHALL ALSO INCLUDE THE PROJECT NUMBER, PID, NAME AND PHONE NUMBER OF THE CONTRACTOR, A POINT OF CONTACT AND THE ANTICIPATED IMPACT ON TRAFFIC. THE CONTRACTOR WILL IMMEDIATELY INFORM THE DISTRICT OFFICE OF COMMUNICATIONS AND THE DISTRICT WORK ZONE TRAFFIC MANAGER OF ANY AND ALL DELAYS AND/OR CHANGES REGARDING THE CONSTRUCTION INITIATION DATE.

GENERAL:

THE CONTRACTOR SHALL SUBMIT IN WRITING A SCHEDULE OF OPERATIONS TO THE ENGINEER (SEE 108.02) AND RECEIVE APPROVAL IN WRITING BEFORE WORK IS STARTED ON THIS PROJECT. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

CONTRACTORS EQUIPMENT - OPERATION AND STORAGE:

THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL. EQUIPMENT SHALL HAVE AT LEAST ONE AMBER FLASHING LIGHT. WHEN PARKED ALONG THE HIGHWAY, THE EQUIPMENT SHALL BE LOCATED EITHER A MINIMUM OF THIRTY FEET FROM THE EDGE OF PAVEMENT OR SIX FEET BEHIND GUARDRAIL WITH A MINIMUM OF 125 FEET OF GUARDRAIL PRECEDING THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT AN APPROVED CONTRACTOR'S STORAGE AREA.

CONTINGENCY QUANTITIES:

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

WORK LIMITS:

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

ITEM 202 - GUARDRAIL REMOVED, AS PER PLAN:

ITEM 202 - ANCHOR ASSEMBLY REMOVED, TYPE A, AS PER PLAN: IN ADDITION TO THE REQUIREMENTS OF ITEM 202, REMOVAL OF SPECIFIED GUARDRAIL ITEMS SHALL INCLUDE BUT NOT BE LIMITED TO ANY ATTACHED POSTS, SIGNS AND DELINEATORS (NOT OTHERWISE SPECIFIED). THIS REMOVAL WILL INCLUDE ALL POSTS, ANCHORS AND HARDWARE UNDER GROUND.

THE CONTRACTOR SHALL EXPECT TO REMOVE ALL CONCRETE FOUNDATIONS COMPLETELY AT ALL LOCATIONS UNLESS OTHERWISE INSTRUCTED OR APPROVED BY THE ENGINEER. REMOVING EXISTING CONCRETE FOUNDATION TO A MINIMUM OF 1 FOOT BELOW THE GRADE OF THE SURROUNDING AREA MAY ONLY BE PERMITTED IF THE EXISTING CONCRETE DOES NOT FALL WITHIN 6 FEET OF THE PROPOSED GUARDRAIL, SO AS TO NOT COMPROMISE THE PERFORMANCE OF THE PROPOSED GUARDRAIL SYSTEM(S).

ALL HOLES AND VOIDS REMAINING AFTER REMOVAL OF GUARDRAIL POSTS AND FOUNDATIONS SHALL BE FILLED WITH GRANULAR MATERIAL CONFORMING TO CMS 203.02R. FILL MATERIAL CONTAINING SOD SHALL NOT BE USED. ALL FILL MATERIAL SHALL BE APPROVED BY THE ENGINEER. MATERIAL PLACED IN HOLES SHALL BE THOROUGHLY COMPACTED AND LEVELED OFF AS DIRECTED BY THE ENGINEER. PAYMENT FOR THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE APPLICABLE GUARDRAIL REMOVAL ITEM.

ITEM 202 - GUARDRAIL REMOVED, AS PER PLAN (CONT.):

ITEM 202 - ANCHOR ASSEMBLY REMOVED, TYPE A, AS PER PLAN (CONT.):

NO HAZARD SHALL BE LEFT UNPROTECTED EXCEPT FOR THE ACTUAL TIME NECESSARY TO REMOVE THE EXISTING GUARDRAIL, PREPARE THE SITE AND INSTALL NEW GUARDRAIL IN A CONTINUOUS OPERATION. ALL GUARDRAIL DESIGNATED FOR REMOVAL ARE TO BECOME PROPERTY OF THE CONTRACTOR AND DISPOSED OF.

ITEM 203 - EMBANKMENT, AS PER PLAN:

QUANTITIES FOR ITEM 203 - EMBANKMENT HAVE BEEN PROVIDED THROUGHOUT THIS PLAN TO BUILD UP FORE-SLOPES AND ENSURE PROPER GRADING FOR THE PROPOSED ANCHOR ASSEMBLIES. THIS ITEM OF WORK INCLUDES ANY CLEARING AND GRUBBING NECESSARY TO PLACE THE EMBANKMENT AT THE LOCATIONS SPECIFIED OR DIRECTED. THE CONTRACTOR SHALL BE PREPARED TO USE EMBANKMENT AT THE LOCATIONS SPECIFIED IN THE PLANS.

ITEM 606 - GUARDRAIL, TYPE MGS:

THE LOCATION OF GUARDRAIL RUNS AS SHOWN IN THESE PLANS, ARE SUBJECT TO ADJUSTMENT TO ASSURE THAT THE PLANNED INSTALLATION WILL AFFORD THE MAXIMUM PROTECTION FOR TRAFFIC.

ITEM 606 - ANCHOR ASSEMBLY, MGS TYPE E

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY OF THE GUARDRAIL END TERMINALS FOR TYPE MGS GUARDRAIL AS LISTED ON ROADWAY ENGINEERING'S WEB PAGE UNDER ROADSIDE SAFETY DEVICES FOR APPROVED GUARDRAIL END TREATMENTS.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS. THE FACE OF THE TYPE E IMPACT HEAD SHALL BE COVERED WITH A SHEET OF TYPE G REFLECTIVE SHEETING, PER CMS 730.19.

REFER TO THE MANUFACTURER'S INSTRUCTIONS REGARDING THE INSTALLATION OF, AND THE GRADING AROUND THE FOUNDATION TUBES AND GROUND STRUT. THE TOP OF ANY FOUNDATION TUBE SHOULD BE LESS THAN 4 INCHES ABOVE THE GROUND. THE PLACEMENT OF THE FOUNDATION TUBES SHOULD BE AN APPROPRIATE DEPTH BELOW THE LEVEL LINE IN ORDER TO MAINTAIN THE FINISHED GUARDRAIL HEIGHT OF 31 INCHES FROM THE EDGE OF THE SHOULDER.

ON-SITE GRADING IS REQUIRED IF THE TOP OF THE FOUNDATION TUBES OR TOP OF THE GROUND STRUT DOES PROJECT MORE THAN 4 INCHES ABOVE THE GROUND LINE.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, ANCHOR ASSEMBLY, MGS TYPE E, EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL ANCHOR ASSEMBLY SYSTEM, INCLUDING ALL RELATED TRANSITIONS, REFLECTIVE SHEETING, HARDWARE, GRADING, EMBANKMENT AND EXCAVATION NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

ITEM 606 - CURVED RAIL ELEMENTS:

ALL RADII OF CURVED RAIL ARE ESTIMATED AND ACTUAL RADII OF PROPOSED RAIL SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR PRIOR TO ORDERING. LENGTH OF CURVED RAIL ELEMENTS, WHERE CALLED FOR IN A RUN, SHALL BE INCLUDED IN THE TOTAL LENGTH OF RUN SHOWN IN THE GUARDRAIL COLUMN AND THE CURVED RAIL ELEMENT TOTAL ARE INCLUDED WITH THE GUARDRAIL TOTALS ON THE GENERAL SUMMARY SHEET.

ITEM 606 - GUARDRAIL, MISC.: ALTERNATIVE GUARDRAIL PLACEMENT:

THIS ITEM SHALL BE USED WHEN THE CONTRACTOR IS REQUIRED TO USE AN ALTERNATE METHOD TO SET POSTS TO PREVENT DAMAGE TO AN UNDERGROUND OBSTACLE, SUCH AS A UTILITY. THE USE OF THIS ITEM WILL BE AS DEEMED NECESSARY BY THE ENGINEER. THIS ITEM SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIAL NEEDED TO SET AND BACKFILL POSTS WHILE MEETING THE REQUIREMENTS OF THE APPLICABLE GUARDRAIL ITEM BEING PERFORMED. APPLICABLE GUARDRAIL ITEMS INCLUDE BUT ARE NOT LIMITED TO SETTING POSTS

ITEM 606 - GUARDRAIL, MISC.: ALTERNATIVE GUARDRAIL PLACEMENT (CONT.):

(AND SLEEVES) FOR MGS, BARRIER DESIGN, ANCHOR ASSEMBLIES, AND BRIDGE TERMINAL ASSEMBLIES. PAYMENT SHALL BE AT THE UNIT BID PRICE OF EACH AND SHALL BE PAID FOR IN ADDITION TO THE APPLICABLE GUARDRAIL PLACEMENT ITEM LISTED ABOVE.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN PROVIDED:  
ITEM 606 - GUARDRAIL, MISC.: ALTERNATIVE GUARDRAIL PLACEMENT: = 75 FT

TYPE 5 GUARDRAIL TO TYPE MGS GUARDRAIL TRANSITION:

THIS TRANSITION SHALL BE AS PER STANDARD CONSTRUCTION DRAWING MGS-4.3.

CONNECTION BETWEEN EXISTING AND PROPOSED GUARDRAIL:

WHEN IT IS NECESSARY TO SPLICE PROPOSED GUARDRAIL TO EXISTING GUARDRAIL, ONLY THE EXISTING GUARDRAIL SHALL BE CUT, DRILLED, OR PUNCHED. THE CONNECTION SHALL BE MADE USING A W-BEAM, BEAM SPLICE AS SHOWN IN AASHTO M 180-12, EXCEPT THE BEAM WASHERS ARE NOT TO BE USED. PAYMENT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE RESPECTIVE GUARDRAIL ITEMS. USE STANDARD DRAWING MGS-4.3 GUARDRAIL TRANSITIONS WHEN CONNECTING MGS TO TYPE 5 GUARDRAIL.

ITEM 606 - REPLACE EXISTING GUARDRAIL BLOCKOUT, AS PER PLAN:

THIS CONTINGENCY ITEM SHALL CONSIST OF REMOVING AND REPLACING EXISTING GUARDRAIL BLOCKOUTS AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL INCLUDE THE PROPER INSTALLATION AND TIGHTENING OF THE 18" POST ANCHOR BOLT AFTER THE BLOCKOUT IS REPLACED. THE CONTRACTOR MAY REUSE THE EXISTING POST ANCHOR BOLT UNLESS IT IS DEEMED DAMAGED OR DEFECTED. IN THE EVENT A NEW POST ANCHOR BOLT IS NEEDED, THE BOLT (AND OTHER MISC, HARDWARE) WILL BE CONSIDERED INCIDENTAL TO THIS ITEM.

THE FOLLOWING CONTINGENCY QUANTITY HAS BEEN PROVIDED AND CARRIED TO THE GENERAL SUMMARY:

ITEM 606 - REPLACE EXISTING GUARDRAIL BLOCKOUT = 20 EACH

ITEM 623 - MONUMENT BOX ADJUSTED TO GRADE:

THIS ITEM OF WORK WILL PROVIDE ALL MATERIAL, LABOR, EQUIPMENT, AND HARDWARE NECESSARY TO ADJUST TO TAPER THE MICROSURFACE TO THE PROPOSED ASPHALT ELEVATION AT THE FOLLOWING LOCATIONS.

SLM	INTERSECTION	QUANTITY	UNIT
1.989	SR 56 AT SR 187	1	EACH
4.45	MAD56 @ SLM 4.45	1	EACH
TOTAL		2	EACH

THIS ITEM IS A CONTINGENCY QUANTITY TO BE USED AS DIRECTED BY THE PROJECT ENGINEER AT VARIOUS LOCATIONS. THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 623 - MONUMENT BOX ADJUSTED TO GRADE: = 2 EACH

GRADING AND EROSION CONTROL:

AREAS DISTURBED BY GUARDRAIL ACTIVITIES, AND AREAS WHERE EMBANKMENT HAVE BEEN PLACED SHALL BE REPAIRED WITH THE FOLLOWING QUANTITIES, AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN PROVIDED:

ITEM	QUANTITY	UNIT	DESCRIPTION
659	89	CU YD	TOPSOIL
659	807	SQ YD	SEEDING AND MULCHING
659	40	SQ YD	REPAIR SEEDING AND MULCHING
659	40	SQ YD	INTER-SEEDING
659	0.11	TON	COMMERCIAL FERTILIZER
659	0.17	ACRE	LIME
659	4	M.GAL	WATER

THE CONTRACTOR SHALL MAINTAIN THE AREAS UNTIL FINAL INSPECTION, INCLUDING MOWING AND WATERING AS NECESSARY.

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ITEM 421 - MICROSURFACING, SURFACE COURSE, AS PER PLAN (16 LB/SY RATE):  
ITEM 421 - MICROSURFACING, SURFACE COURSE, AS PER PLAN (18 LB/SY RATE):  
ITEM 421 - MICROSURFACING, SURFACE COURSE, AS PER PLAN (22 LB/SY RATE):  
ITEM 421 - MICROSURFACING, LEVELING COURSE, AS PER PLAN (14 LB/SY RATE):

IN ADDITION TO THE REQUIREMENTS OF CMS 421, THE FINAL AGGREGATE BLEND SHALL BE APPROVED BY THE ENGINEER AND MEET THE FOLLOWING REQUIREMENTS:

- THE FINE AGGREGATE SHALL BE A BLEND OF DOLOMITIC LIMESTONE AND TRAP ROACK, A DARKER COLORED (GRAY/BLACK) NON-POLISHING AGGREGATE. THE AGGREGATE MIX SHALL BE 50/50 BLEND - ONTARIO TRAP ROCK AND LIMESTONE.

THE FOLLOWING APPLICATION RATES FOR MICROSURFACING SHALL SUPERCEED ANY RATES GIVEN IN CMS 421.10 OR SS800:

LOCATION 1 - MAD 70:  
A LEVELING COURSE AND A SURFACE COURSE ARE SPECIFIED AT THIS LOCATION ON THE MAINLINE (DRIVING LANES). APPLY THE PAVING MIXTURE AT 14 +/- 2 POUNDS PER SQUARE YARD FOR THE LEVELING COURSE AND 16 +/- 1 POUNDS PER SQUARE YARD FOR THE SURFACE COURSE. APPLY THE TWO COURSES AT A MINIMUM COMBINED

RATE OF 30 POUNDS PER SQUARE YARD REGARDLESS OF THE ABOVE TOLERANCES.

A SURFACE COURSE (ONLY) IS SPECIFIED FOR I-70 MAINLINE SHOULDERS (INSIDE AND OUTSIDE). APPLY THE PAVING MIXTURE ON THE SHOULDERS AT 18 +/- 1 POUNDS PER SQUARE YARD.

EXISTING RUMBLE STRIPS ENCOUNTERED ON THIS PROJECT SHALL BE "PAVED" OVER WITH THE PROPOSED MICROSURFACING TREATMENT. IT IS NOT THE INTENT OF THIS PROJECT TO REPAIR OR COMPLETELY FILL IN THE EXISTING VOIDS LEFT BEHIND.

SEE TYPICAL SECTION ON SHEET 3.

LOCATION 2 - MAD 56:  
A SURFACE COURSE (ONLY) IS SPECIFIED FOR THESE LOCATIONS. APPLY THE PAVING MIXTURE AT A RATE OF 21 - 24 POUNDS PER SQUARE YARD.

SEE TYPICAL SECTIONS ON SHEET 3.

THE MICROSURFACING SHALL BE FEATHERED AT BEGIN AND END OF PROJECT, AND AT ANY PERTINANT AT GRADE ITEMS SUCH AS EXISTING STRUCTURES, MONUMENTS, CATCH BASINS, WATER VALVES AND ROAD SIDE INTERSECTIONS. DO NOT PAVE BACK ON ANY SIDE ROADS OR DRIVEWAYS.

ITEM 407 - TACK COAT:  
THE ITEM 407 - TACK COAT IS INCIDENTAL TO THE ITEM 421 - MICROSURFACING, PER CMS-421.14

ITEM 423 - CRACK SEALING, TYPE II OR III:  
THE ITEM 423 - CRACK SEALING, TYPE II OR III IS INCIDENTAL TO THE ITEM 421 - MICROSURFACING, PER CMS-421.14

ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN:  
THE AGGREGATE IS REQUIRED TO BE LIMESTONE.

ITEM 617 - WATER:  
THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 617 - WATER = 4 MGAL

ITEM 626 - BARRIER REFLECTOR:  
AN ESTIMATED QUANTITY OF TYPE A AND B BARRIER REFLECTORS HAVE BEEN PROVIDED FOR LOCATIONS ON I-70 THAT DO NOT HAVE BARRIER REFLECTORS ON THE EXISTING GUARDRAIL AND CONCRETE BARRIERS.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 626 - BARRIER REFLECTOR, TYPE A = 40 EACH  
ITEM 626 - BARRIER REFLECTOR, TYPE B = 40 EACH  
TOTAL = 80 EACH

ITEM 611 - MANHOLE ADJUSTED TO GRADE:  
THE CASTING TOPS OF MANHOLES, VALVE BOXES, AND OTHER STRUCTURES OWNED BY PUBLIC SERVICE CORPORATIONS MAY BE ADJUSTED TO GRADE BY THEIR RESPECTIVE OWNERS OR GIVE AUTHORIZATION TO ODOT TO ADJUST AS PART OF THIS CONTRACT. THIS WORK NEEDS TO BE COMPLETED PRIOR TO THE CONSTRUCTION OF THE SURFACE COURSE. THE CONTRACTOR SHALL NOTIFY SUCH PUBLIC SERVICE CORPORATIONS A MINIMUM OF 7 CALENDAR DAYS IN ADVANCE OF WORK OPERATIONS SO THAT WORK MAY BE PROPERLY SCHEDULED.

THE CASTING TOPS OF MANHOLES, VALVE BOXES, AND OTHER STRUCTURES REQUIRING ADJUSTMENT THAT ARE OWNED BY PRIVATE UTILITES NEED TO BE ADJUSTED TO GRADE BY THEIR RESPECTIVE OWNERS. THE ODOT CONTRACTOR SHALL NOTIFY THE PRIVATE OWNER A MINIMUM OF 7 CALENDAR DAYS IN ADVANCE OF WORK OPERATIONS SO THE WORK MAY BE PROPERLY SCHEDULED.

IF ADJUSTMENTS HAVE NOT BEEN COMPLETED 14 CALENDAR DAYS AFTER NOTIFICATION, THE ODOT CONTRACTOR WILL NOTIFY THE ODOT PROJECT ENGINEER AND PROVIDE SPECIFIC STATION LOCATIONS AND OWNER INFORMATION. THE ODOT PROJECT ENGINEER WILL WORK WITH THE DISTRICT UTILITY COORDINATOR TO ISSUE AND OBSTRUCTION REMOVAL NOTICE WITHIN 5 DAYS OF RECEIPT WHICH WILL INFORM THE PRIVATE UTILITY TO ADJUST THE STRUCTRUES AS NECESSARY OR ODOT WILL AUTHORIZE THE ODOT CONTRACTOR TO ADJUST AS NEEDED AND BILL THE OWNER OF THE FACILITY FOR THE ADJUSTMENT TO THE STRUCTURE.

SHOULD THE CONTRACTOR FAIL TO NOTIFY PUBLIC SERVICE CORPORATIONS OR PRIVATE UTILITIES OF EXISTING MANHOLES, VALVE BOXES, AND OTHER STRUCTURES THAT REQUIRE ADJUSTMENTS TO GRADE, AND COVER THESE WITH THE PROPOSED ASPHALT TREATMENT, THE CONTRACTOR WILL BE REQUIRED TO UNCOVER THE MANHOLES, VALVE BOXES, AND OTHER STRUCTURES AT THEIR OWN EXPENSE SO THAT THE NECESSARY ADJUSTMENTS CAN BE MADE. THE METHOD OF REMOVAL AND REPAIR OF THE ASPHALT SHALL MEET ALL REQUIREMENTS OF THE ODOT ENGINEER AND SHALL BE AT THE CONTRACTORS EXPENSE.

THIS ITEM IS A CONTINGENCY QUANTITY TO BE USED AS DIRECTED BY THE PROJECT ENGINEER AT VARIOUS LOCATIONS. ANY ADDITIONAL COST FOR ALL MATERIAL, LABOR, EQUIPMENT AND HARDWARE NECESSARY TO ADJUST THE EXISTING MANHOLE TO THE PROPOSED ASPHALT .

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 611 - MANHOLE ADJUSTED TO GRADE = 4 EACH

ITEM 621 - RAISED PAVEMENT MARKER REMOVED (PRIOR TO MICRO):  
RAISED PAVEMENT MARKERS ARE TO BE REMOVED PER CMS 621.08. THE CONTRACTOR HAS THE OPTION TO FILL THE DEPRESSIONS WITH EITHER ASPHALT CONCRETE OR VIRGIN MICROSURFACING MATERIAL. IF MICROSURFACING MATERIAL IS USED, THE CONTRACTOR WILL BE REQUIRED TO HAND-FILL AND COMPACT THE MATERIAL PRIOR TO THE APPLICATION OF THE PROPOSED MICROSURFACING COURSE. ALL OTHER REQUIREMENTS OF 621.01 WILL STILL APPLY.

ITEM 623 - CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN:  
THIS ITEM SHALL CONSIST OF STATIONING USING 3 FT LATH STAKES. THE STAKES SHALL BE SPACED AT 200 FT INTERVALS AND SHALL EXTEND THROUGHOUT THE LENGTH OF THE PROJECT AND THROUGHOUT THE LENGTH OF ALL RAMPS. PLACEMENT OF THE STAKES SHALL BE AS DIRECTED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY DAMAGED OR MISSING STAKES.

CONSTRUCTION LAYOUT STAKES, AS PER PLAN WILL BE PAID FOR AT THE CONTRACT LUMP SUM BID, WHICH SHALL BE FULL COMPENSATION FOR ALL SERVICES, MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS, INCLUDING THE REMOVAL, NECESSARY TO COMPLETE THIS ITEM.

ITEM 630 - SIGN, FLAT SHEET:  
ITEM 630 - GROUND MOUNTED SUPPORT, NO. 2 POST:  
THESE ITEMS SHALL BE USED TO PLACE NEW STRUCTURE IDENTIFICATION SIGNS AT THE FOLLOWING STRUCTURES:

MAD-38-1744	MAD-70-0188R
MAD-56-0262	MAD-70-0332
MAD-70-0060	MAD-70-0630L
MAD-70-0155L	MAD-70-0630R
MAD-70-0155R	MAD-70-0643
MAD-70-0188L	MAD-70-0715

SEE DETAIL SHEET 8.

EACH SIGN SHALL BE ATTACHED TO THE CONCRETE PARAPET WITH CONCRETE ANCHORS AT THE RIGHT REAR LOCATION. IF THE BRIDGE DOES NOT HAVE A CONCRETE PARAPET, THE SIGN SHALL BE POST MOUNTED TO ONE NEW NO. 2 POST AS PER STANDARD CONSTRUCTION DRAWING TC-41.20 (MOST CURRENT REVISION) USING TWO 5/16" ALUMINUM BOLTS 2 1/2" IN LENGTH. THE POST SHALL BE 7'-0" LONG.

SIGNS SHALL BE SIZED AS PER SIGN I-H25a AS GIVEN IN THE ODOT SIGN DESIGN MANUAL (MOST CURRENT VERSION). ALL INCIDENTALS WILL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 630 - SIGN, FLAT SHEET.

THE FOLLOWING ITEMS HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 630 - SIGN, FLAT SHEET = 12.0 SQ. FT.  
ITEM 630 - GROUND MOUNTED SUPPORT, NO. 2 POST = 84 FT.

ITEM 642 - EDGE AND LANE LINE:  
ITEM 646 - EDGE AND LANE LINE (STRUCTURES):  
ALL EDGE AND LANE LINE MARKINGS SHALL HAVE A 6" WIDTH.

ITEM 642 - CENTER LINE:  
ITEM 646 - CENTER LINE (STRUCTURES):  
ALL CENTER LINE MARKINGS SHALL HAVE A 4" WIDTH.

ITEM 642 - CHANNELIZING LINE:  
ALL CHANNELIZING LINES ON I.R. 70 SHALL HAVE A 12" WIDTH.  
ALL CHANNELIZING LINES ON S.R. 56 SHALL HAVE AN 8" WIDTH.

ITEM 644 - REMOVAL OF PAVEMENT MARKING, AS PER PLAN:  
THE FIRST LINE OF THE SECOND PARAGRAPH OF ITEM 421.08 SURFACE PREPARATION IS TO BE REMOVED FROM THE SPECIFICATION. ALL PAVEMENT MARKINGS, INCLUDING AUXILIARY MARKINGS, ARE TO BE REMOVED, INCLUDING EPOXY, THERMO AND PAINT. THE METHOD IS TO CONFORM TO 641.10. THE DEPARTMENT WILL PAY FOR REMOVAL OF ALL TYPES OF PAVEMENT MARKINGS UNDER THIS ITEM.



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ITEM 642 - AIR SPEED ZONE MARKING:

AIR SPEED ZONE MARKINGS SHALL BE WHITE AND 24 INCHES WIDE MEASURED IN THE DIRECTION OF TRAVEL AND 4 FEET IN LENGTH. ON TWO-LANE ROADWAYS WITH PAVED SHOULDERS LESS THAN 4 FEET IN WIDTH, THE AIR SPEED ZONE MARKINGS SHALL BE PLACED WITH 2 FEET ON EACH SIDE OF THE CENTER LINE OR EDGE LINE MARKINGS. WHEN PAVED SHOULDERS OF SUFFICIENT WIDTH ARE AVAILABLE, THE AIR SPEED ZONE MARKINGS SHALL BE PLACED ON THE EDGE LINE. PLACE THE MARKINGS AT 0.25 MILE INTERVALS OVER A 1 MILE LENGTH OF ROADWAY.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO CALL DISTRICT 6 SURVEYORS TO HAVE THE MARKINGS LAID OUT BEFORE BEING MARKED. A RECORD IS TO BE KEPT AND ONE ORIGINAL SIGNED AND SEALED DOCUMENT IS TO BE SENT TO THE DISTRICT TRAFFIC ENGINEER AND ONE COPY IS TO BE SENT TO THE DISTRICT CONSTRUCTION ENGINEER.

MATERIALS, EQUIPMENT AND APPLICATION SHALL BE ACCORDING TO THE TYPE OF PAVEMENT MARKING MATERIAL USED.

PAYMENT SHALL BE ACCORDING TO THE PAVEMENT MARKING MATERIAL USED AND SHALL INCLUDE THE SURVEYING WORK. THE FIVE MARKINGS PLACED IN EACH 1 MILE OF ROADWAY IN EACH DIRECTION SHALL EQUAL ONE ZONE. THERE WILL BE FIVE MARKINGS IN 1 MILE.

SEE DETAIL SHEET 5.

A QUANTITY OF 160 SQUARE FEET OF:

ITEM 642 - SPECIAL - AIR SPEED ZONE MARKING HAS BEEN CARRIED TO THE GENERAL SUMMARY.

PROPOSED NO PASSING ZONES:

THE PROPER PLACEMENT OF THE PASSING AND NO PASSING ZONES AS SHOWN ON PLAN SHEETS 21 THROUGH 24 SHALL BE CONFIRMED BY THE CONTRACTOR AND PLACED BY USING THE CONTROL POINTS SHOWN ON THE PLAN SHEETS.

SLM'S ON THE PASSING ZONE SHEETS COULD BE DIFFERENT THAN THE SLM'S SHOWN ON THE PAVING PLAN. ALL START AND STOP SLM LOCATIONS SHALL BE WITHIN 0.005 MILES OF THE LOCATIONS SHOWN ON THE PLAN SHEETS LISTED ABOVE. A LETTER OF VERIFICATION OF ALL PASSING AND NO PASSING ZONES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR PLACEMENT IN THE PROJECT RECORDS. ANY IMPROPERLY PLACED PASSING OR NO PASSING ZONES SHALL BE IMMEDIATELY CORRECTED.

WATER QUALITY PROTECTION:

NO TOXIC OR HAZARDOUS MATERIALS SUCH AS SEALANTS, PAINT, SOLVENTS, CLEANING AGENTS, EARTHEN MATERIALS, WASTE-WATER FUELS OR DEBRIS OF ANY KIND SHALL BE DISCHARGED TO ANY STREAMS, DRAINAGE COURSES OR BODIES OF WATER. ALL ASPHALT OR CONCRETE GRINDINGS, EXCESS ASPHALT OR CONCRETE MATERIALS OR ANY OTHER DEBRIS GENERATED DURING RESURFACING OR OTHER SIMILAR ACTIVITIES SHALL NOT BE DISPOSED OF WITHIN A FLOODPLAIN BELOW THE 100-YEAR FLOOD ELEVATION. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO PREVENT LIQUIDS USED TO REPAIR, CLEAN, SEAL, OR TREAT ANY BRIDGE STRUCTURE (E.G. PAINT, SEALER, SOLVENT) FROM ENTERING STREAMS, WETLANDS OR OTHER "WATERS OF THE UNITED STATES" AND TAKE THE APPROPRIATE ACTIONS IN THE EVENT OF A RELEASE.

WATERSHED PROTECTION:

THE PROJECT IS LOCATED WITHIN VARIOUS SOURCE WATER PROTECTION AREAS. IT IS ESSENTIAL THAT ALL ACTIVITIES ASSOCIATED WITH THIS WORK BE PERFORMED IN A MANNER CONSISTENT WITH BEST WATERSHED MANAGEMENT PRACTICES INCLUDING, BUT NOT LIMITED TO: AREAS OF DISTURBED GROUND SHALL HAVE APPROPRIATE EROSION AND SEDIMENT CONTROLS. IF HAZARDOUS/TOXIC MATERIALS INCLUDING BUT NOT LIMITED TO FUELS, OILS, BITUMEN'S PAINTS, SEALANTS, OR OTHER CHEMICALS, ARE STORED ON SITE, THEY SHALL BE STORED IN A DOUBLE-CONTAINMENT MANNER. ALL EQUIPMENT REPAIRS, MAINTENANCE, AND MECHANICAL WORK THAT COULD RESULT IN THE

WATERSHED PROTECTION (CONT.):

RELEASE OF HAZARDOUS/TOXIC MATERIALS SHALL BE PERFORMED IN AN APPROPRIATELY CONTAINED AREA, PREFERABLY OFF SITE OR AN APPROPRIATE OFF-SITE FACILITY. IN THE EVENT THAT ANY HAZARDOUS/TOXIC MATERIALS INCLUDING, BUT NOT LIMITED TO FUELS, OILS, BITUMEN'S PAINTS, SEALANTS, OR OTHER CHEMICALS ARE SPILLED, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY EMERGENCY SERVICES BY CALLING 911 AND THE OHIO EPA @ (800) 282-9378. THE CONTRACTOR SHOULD BE PREPARED TO PROVIDE DETAILED INFORMATION RELATIVE TO THE TYPE AND QUANTITY OF MATERIAL THAT HAS BEEN SPILLED AS WELL AS THE EXACT LOCATION AND THE EXACT TIME AT WHICH THE SPILL OCCURRED. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR INFORMING ALL SUBCONTRACTORS AND OTHER AGENTS OF THESE RESPONSIBILITIES, PRECAUTIONS, AND PROHIBITIONS.

PN 129 - WINDOW CONTRACT:

THE CONTRACTOR IS TO PICK THE ORDER OF CONSTRUCTION, BUT ONLY ONE ROUTE IS TO BE WORKED ON AT A TIME. THE CONTRACTOR MUST FINISH THE ROUTE BEFORE BEGINNING WORK ON ANOTHER ROUTE.

DESCRIPTION OF CRITICAL WORK	CALENDAR DAYS TO COMPLETE
LOC 1 - MAD 70	90 DAYS
LOC 2 - MAD 56	60 DAYS

STARTING DATE:

THE CONTRACTOR MAY NOT START PAVING ON MAD-70 OR MAD-56 UNTIL AFTER JUNE 1, 2017. THIS IS TO ALLOW ODOT CREWS TO COMPLETE PAVEMENT REPAIRS BEFORE THE OVERLAY WORK.

<div>MAD 38 1744</div> <div>BRIDGE ID SIGN SIGN CODE I-H25a</div>	<div>MAD 56 0262</div> <div>BRIDGE ID SIGN SIGN CODE I-H25a</div>	<div>MAD 70 0188R</div> <div>BRIDGE ID SIGN SIGN CODE I-H25a</div>	<div>MAD 70 0332</div> <div>BRIDGE ID SIGN SIGN CODE I-H25a</div>
<div>MAD 70 0060</div> <div>BRIDGE ID SIGN SIGN CODE I-H25a</div>	<div>MAD 70 0155L</div> <div>BRIDGE ID SIGN SIGN CODE I-H25a</div>	<div>MAD 70 0630L</div> <div>BRIDGE ID SIGN SIGN CODE I-H25a</div>	<div>MAD 70 0630R</div> <div>BRIDGE ID SIGN SIGN CODE I-H25a</div>
<div>MAD 70 0155R</div> <div>BRIDGE ID SIGN SIGN CODE I-H25a</div>	<div>MAD 70 0188L</div> <div>BRIDGE ID SIGN SIGN CODE I-H25a</div>	<div>MAD 70 0643</div> <div>BRIDGE ID SIGN SIGN CODE I-H25a</div>	<div>MAD 70 0715</div> <div>BRIDGE ID SIGN SIGN CODE I-H25a</div>



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UNDERGROUND UTILITIES:

THE IDENTITY OF SOME OF THE EXISTING UNDERGROUND FACILITIES KNOWN TO BE LOCATED IN THE CONSTRUCTION AREA HAVE BEEN IDENTIFIED. THE CONTRACTOR SHALL GIVE NOTICE OF INTENT TO CONSTRUCT TO THE OHIO UTILITIES PROTECTION SERVICE, PRODUCERS UNDERGROUND PROTECTION SERVICE, AND OWNERS OF UNDERGROUND FACILITIES THAT ARE NOT MEMBERS OF A REGISTERED PROTECTION SERVICE IN ACCORDANCE WITH SECTION 153.64 OF THE OHIO REVISED CODE. THE ABOVE, MENTIONED NOTICE SHALL BE GIVEN AT LEAST TWO WORKING DAYS PRIOR TO THE START OF CONSTRUCTION THE FOLLOWING UTILITIES ARE LOCATED WITHIN THE WORK LIMITS OF THE PROJECT AND THE OWNERS SUBSCRIBE TO REGISTERED UNDERGROUND PROTECTION SERVICE.

OHIO UTILITY PROTECTION SERVICE 1-800-362-2764  
PRODUCERS UNDERGROUND PROTECTION SERVICE 1-614-587-0486  
NON-MEMBERS MUST BE CALLED DIRECTLY.

UTILITIES:

THE ODOT CONTRACTOR IS REQUIRED TO CONTACT OUPS A MINIMUM OF 48 HOURS EXCLUDING WEEKENDS AND HOLIDAYS TO PERMIT ALL UNDERGROUND UTILITIES AN OPPORTUNITY TO MARK THEIR LINES. IT IS ALSO THE ODOT CONTRACTOR’S RESPONSIBILITY TO CONTACT ALL NON-MEMBERS OF OUPS DIRECTLY A MINIMUM OF 48 HOURS’ NOTICE EXCLUDING WEEKENDS AND HOLIDAYS TO PROVIDE THEM WITH THE SAME OPPORTUNITY.

IT IS ODOT’S EXPECTATION THAT ALL GUARD RAIL POSTS WILL BE INSTALLED IN THE SAME LOCATIONS AND THERE WILL BE NO DISRUPTION TO UNDERGROUND UTILITIES. IF THERE IS A UTILITY MARKING WITHIN THE TOLERANCE ZONE OF A UTILITY LOCATE FROM THE PROPOSED GUARDRAIL PLACEMENT IT IS THE ODOT CONTRACTORS RESPONSIBILITY TO DIRECTLY CONTACT THE IMPACTED UTILITY AND WORK WITH THEM TO FIND A SOLUTION THAT DOES NOT CHANGE THE GUARDRAIL PLACEMENT OR DAMAGE THE EXISTING UTILITY. NO UTILITY RELOCATION WILL BE REIMBURSED NOR WILL DELAY CLAIMS BE PERMISSIBLE BASED ON LACK OF COORDINATION BETWEEN THE ODOT CONTRACTOR AND THE IMPACTED UTILITY.

COLUMBIA GAS OF OHIO  
2101 WEST MAIN ST.  
SPRINGFIELD, OH 45504  
MR. DAVE GREENWELL  
937.327.7102  
[dgreenwell@nisource.com](mailto:dgreenwell@nisource.com)

MADISON COUNTY ENGINEER  
825 U.S. 42  
LONDON, OH 43140  
KEN KOPPES  
740.852.9404  
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MADISON ENERGY COOPERATIVE  
4100 HOLIDAY STREET NW, SUITE 201  
CANTON, OH 44718  
KYLE UNDERWOOD  
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AT&T-OHIO  
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ROOM 802  
COLUMBUS, OH 43215  
MR. GARY VAN ALMSICK  
614.223.7276  
[GV2758@ATT.COM](mailto:GV2758@ATT.COM)

OHIO EDISON  
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MARATHON PIPELINE, LLC  
20-C INDUSTRIAL DRIVE  
LEXINGTON, OH 44904  
GREG NEWMAN  
419.884.0800  
[gnewman@maratonpetroleum.com](mailto:gnewman@maratonpetroleum.com)

FRONTIER COMMUNICATIONS  
1300 COLUMBUS - SANDUSKY ROAD  
MARION, OH 43302  
MR. CHRIS AVERY  
740.383.0551  
[ira.avery@ftr.com](mailto:ira.avery@ftr.com)

CENTURYLINK  
441 WEST BROAD STREET  
PATASKALA, OH 43062  
MS.DEE REED  
740.927.8282  
[DELORES.A.REED@CENTURYLINK.COM](mailto:DELORES.A.REED@CENTURYLINK.COM)

ODOT TRAFFIC (DISTRICT 6)  
400 EAST WILLIAM STREET  
DELAWARE, OH 43015  
MR. KRAIG SHREWSBERRY  
740.833.8198  
[Kraig.Shrewsberry@dot.ohio.gov](mailto:Kraig.Shrewsberry@dot.ohio.gov)

INDEPENDENTS FIBER NETWORK  
13888 COUNTY ROAD 25A  
WAPAKONETA, OH 45895  
800.634.4032

COLUMBIA GAS OF OHIO  
3550 JOHNNY APPLESEED COURT  
COLUMBUS, OHIO 43231  
MR. NATE LODEN  
614.818.2118  
[NLODEN@NISOURCE.COM](mailto:NLODEN@NISOURCE.COM)

CHARTER COMMUNICATIONS  
(FORMERLY TWC)  
3760 INTERCHANGE DRIVE  
COLUMBUS, OH 43204  
MR. RAY MAURER  
614.481.5262  
[ray.maurer@charter.com](mailto:ray.maurer@charter.com)

AT&T TRANSMISSION  
THAYER POWER AND COMMUNICATION  
950 FREEWAY DRIVE NORTH  
COLUMBUS, OH 43229  
CHRISTOPHER McCLOSKEY  
614.431.9292  
[ChrisMcCloskey@thayerpc.com](mailto:ChrisMcCloskey@thayerpc.com)

CITY OF LONDON  
6 EAST 2<sup>ND</sup> STREET  
LONDON, OH 43140  
740.852.3243

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GENERAL:

ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CURRENT EDITION). COPIES ARE AVAILABLE FROM:

THE OHIO DEPARTMENT OF TRANSPORTATION  
BUREAU OF TRAFFIC,  
1980 WEST BROAD STREET  
COLUMBUS, OHIO 43223.

USE OF STANDARD DRAWINGS:

FOR THE PURPOSE OF THIS PROJECT, "MOVING OPERATION" SHALL BE LIMITED TO PAVEMENT MARKING STRIPING.

IT MAY BE NECESSARY TO EXTEND THE ADVANCE WARNING AND BUFFER ZONES BEYOND THE MINIMUM DISTANCES SHOWN ON THE STANDARD DRAWINGS. THIS MAY BE DUE TO HORIZONTAL ALIGNMENT, VERTICAL ALIGNMENT, RAMP LOCATIONS, OR OTHER SIGHT OBSTRUCTIONS. LOCATIONS OF THE TAPER ZONES MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER, BUT TAPER LENGTHS MUST MEET THE MINIMUM STANDARDS. TAPERS SHOULD BE PLACED IN TANGENT SECTIONS WHENEVER POSSIBLE. ADDITIONAL YIELD SIGNS MAY BE REQUIRED FOR RAMPS WITHIN 1,000 FEET OF A WORK ZONE. PAYMENT SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

FOR ANY MULTILANE HIGHWAY, DEVICE SPACING SHALL BE A MAXIMUM OF 40' CENTER ON CENTER IN THE TAPERS AND 80' CENTER ON CENTER IN THE TANGENT SECTIONS.

NOTIFICATION OF TRAFFIC RESTRICTIONS:

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW. THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE BUT IS NOT LIMITED TO ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHOULD LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, DETOUR ROUTES IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

ITEM	DURATION OF CLOSURE	NOTIFICATION DUE TO DISTRICT 6 COMMUNICATIONS OFFICE
RAMP AND ROAD CLOSURES	>= 2 WEEKS	14 BUSINESS DAYS PRIOR TO CLOSURE
	> 12 HOURS AND < 2 WEEKS	7 BUSINESS DAYS PRIOR TO CLOSURE
	< 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

LANE CLOSURES/ RESTRICTIONS	>= 2 WEEKS	7 BUSINESS DAYS PRIOR TO CLOSURE
	< 2 WEEKS	2 BUSINESS DAYS PRIOR TO CLOSURE

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME FRAME TABLE.

WORK SITE LIGHTING:

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR, AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

USE OF WEIGHTED CHANNELIZERS:

THE WEIGHTED CHANNELIZERS MAY BE USED IN ACCORDANCE WITH THIS SECTION. THE WEIGHTED CHANNELIZERS SHALL BE PREDOMINANTLY ORANGE IN COLOR AND SHALL BE MADE OF LIGHTWEIGHT, FLEXIBLE, AND DEFORMABLE MATERIAL. THEY SHALL BE AT LEAST 42 INCHES IN HEIGHT WITH A WEIGHTED BASE. THEY MAY HAVE A HANDLE OR LIFTING DEVICE, WHICH EXTENDS ABOVE THE 42" MINIMUM HEIGHT.

THE MARKINGS ON THE WEIGHTED CHANNELIZERS SHALL BE HORIZONTAL, CIRCUMFERENTIAL, ALTERNATING ORANGE AND WHITE RETRO REFLECTIVE STRIPES 6 INCHES WIDE. EACH WEIGHTED CHANNELIZERS SHALL HAVE A MINIMUM OF TWO ORANGE AND TWO WHITE STRIPES. ANY NON-RETRO REFLECTIVE SPACES BETWEEN THE HORIZONTAL ORANGE AND WHITE STRIPES SHALL NOT EXCEED 2 INCHES WIDE. THE WEIGHTED CHANNELIZERS SHALL HAVE A 4-INCH MINIMUM WIDTH, REGARDLESS OF ORIENTATION.

USE OF WEIGHTED CHANNELIZERS ON FREEWAYS AND MULTILANE HIGHWAYS SHALL BE LIMITED TO SHORT-TERM OPERATION FOR EITHER DAY OR NIGHT. UPON COMPLETION OF WORK, THE WEIGHTED CHANNELIZERS SHALL BE REMOVED. THE WEIGHTED CHANNELIZERS MAY AGAIN BE PLACED ON THE HIGHWAY WHEN THE WORK IS TO RESUME ON THE FOLLOWING DAY OR NIGHT. ANY LANE CLOSURE USING CHANNELIZATION DEVICES, EXPECTED TO REMAIN FOR MORE THAN TWELVE HOURS, SHALL REQUIRE THE USE OF DRUMS OR BARRIERS. WORK IS TO RESUME ON THE FOLLOWING DAY OR NIGHT.

WHEN USED AT NIGHT, WEIGHTED CHANNELIZERS SHALL ONLY BE PLACED IN THE TANGENT AREA. THE TANGENT AREA IS DEFINED AS THE AREA AFTER THE TRANSITION TAPER WHERE THE WORK TAKES PLACE. DRUMS SHALL BE USED IN THE TRANSITION TAPERS FOR NIGHT OPERATIONS. MAXIMUM SPACING OF THE WEIGHTED CHANNELIZERS SHALL BE 40 FEET.

STEPS SHOULD BE TAKEN TO ENSURE THAT THE WEIGHTED CHANNELIZERS WILL NOT BE BLOWN OVER OR DISPLACED BY WIND OR MOVING TRAFFIC. BALLASTS SHOULD NOT PRESENT A HAZARD IF THE WEIGHTED CHANNELIZERS ARE INADVERTENTLY STRUCK, NOR SHOULD THEY AFFECT THE VISIBILITY OF THE WEIGHTED CHANNELIZERS. ALL BALLASTS USED SHOULD BE IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

LANES OPEN DURING HOLIDAYS AND SPECIAL EVENTS:

NO WORK SHALL BE PERFORMED AND THE SAME NUMBER OF LANES AS WERE AVAILABLE AT THE START OF THE PROJECT SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

HOLIDAYS  
CHRISTMAS FOURTH OF JULY  
NEW YEAR'S EVE LABOR DAY  
MEMORIAL DAY THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

LANES OPEN DURING HOLIDAYS AND SPECIAL EVENTS (CONT.):

DAY OF HOLIDAY	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00 NOON FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00 NOON FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00 NOON MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00 NOON TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00 NOON WEDNESDAY THROUGH 6:00 AM FRIDAY
THANKSGIVING	5:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00 NOON THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00 NOON FRIDAY THROUGH 6:00 AM MONDAY

SPECIAL EVENTS

FARM SCIENCE REVIEW - LANE OR SHOULDER CLOSURES ARE NOT PERMITTED DURING THE FARM SCIENCE REVIEW 5AM-10PM DAILY ON THE FOLLOWING ROUTES:  
I-70 BETWEEN SR 54 (CLARK COUNTY) AND SR 142

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA WIDE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN ACCORDANCE WITH THE UNAUTHORIZED LANE USE TABLE.

PERMITTED LANE CLOSURES:

THE EXISTING NUMBER OF LANES IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES EXCEPT DURING PERIODS OF WORK AT WHICH TIME LANES MAY BE CLOSED IN ACCORDANCE WITH THE UNAUTHORIZED LANE USE TABLE FOR EACH LOCATION UNLESS OTHERWISE SHOWN IN THE PLANS.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITME 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

DISINCENTIVE:

THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE AS DESIGNATED IN THE UNAUTHORIZED LANE USE TABLE FOR EACH UNIT OF TIME A CRITICAL LANE / RAMP IS CLOSED BY THE CONTRACTOR'S ACTION WHILE NOT OTHERWISE PERMITTED BY THE CONTRACT. THE DISINCENTIVE WILL BE FOR ANY LANE CLOSURES CAUSED BY THE CONTRACTOR DURING TIMES AND LOCATIONS NOT SPECIFICALLY PERMITTED BY THIS CONTRACT. THERE SHALL BE NO WORK PERFORMED, NO LANE REDUCTIONS OR NO LANE RESTRICTIONS OUTSIDE OF THE WORKING HOURS GIVEN WITH IN THIS PLAN. THE CONTRACTOR SHALL BE ASSESSED DISINCENTIVE AS SHOWN IN THE UNAUTHORIZED LANE USE TABLE.

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UNAUTHORIZED LANE USE:

THE PERMITTED LANE CLOSURE TIMES PREVIOUSLY LISTED SHALL BE SUBJECT TO DISINCENTIVES AS PER THE UNAUTHORIZED LANE USED TABLE BELOW FOR ANY TIME PERIOD OR PORTION THEREOF IN WHICH THE MINIMUM OF OPEN LANES PER DIRECTION IS VIOLATED.

UNAUTHORIZED LANE USE TABLE						
MAD-70						
Section	Existing Number of Lanes per Direction	Lane closures are NOT permitted:				Disincentive Amounts per minute per lane
		Lane Reduction	Mon to Fri	Sat	Sun	
Clark County Line (0.00) to Franklin County Line (15.58)	3	3 to 2	No Restriction	No Restriction	No Restriction	\$75
		3 to 1	6AM-7PM	7AM-8PM	7AM-10PM	\$100
Short term shoulder closures are permitted any time except 6AM-9AM and 3PM-6PM Monday-Friday.						

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS:

USE OF LEOS BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST UNLESS PRIOR APPROVAL HAS BEEN OBTAINED FROM THE PROJECT ENGINEER. LAW ENFORCEMENT OFFICERS (LEOS) SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED. IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED LAW ENFORCEMENT OFFICER WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

1. THE USE OF A LAW ENFORCEMENT OFFICER WITH PATROL CAR IS REQUIRED WHERE A COMPLETE BLOCKAGE OF APPROACH TRAFFIC IS REQUIRED
2. THE USE OF A LAW ENFORCEMENT OFFICER WITH PATROL CAR IS REQUIRED IF THE CONTRACTOR TURNS THE INTERSECTION SIGNALS OFF OR PUTS THE INTERSECTION IN FLASHING OPERATION, AND WHEN THE LEO DIRECTS TRAFFIC THROUGH THE INTERSECTION CONTRARY TO THE SIGNAL DISPLAY.

LAW ENFORCEMENT OFFICERS SHOULD NOT FORSAKE THEIR TRAFFIC CONTROL RESPONSIBILITIES TO CHASE MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF THE MOTORISTS ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST MAY BE ACCEPTABLE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEO'S AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEO. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THEIR RESPECTIVE DUTIES, PLACEMENT AND WILL RESOLVE ANY ISSUES BETWEEN THE TWO PARTIES THAT MAY ARISE. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THE SERVICES WITH:

THE OHIO HIGHWAY PATROL 1-614-466-2660

THE LEO SHOULD REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING THE SHIFT. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF THE SHIFT.

LAW ENFORCEMENT OFFICERS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A L.E.O. ARE INCLUDED WITHIN THE BID UNIT PRICE FOR

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONT.):

ITEM-614 LAW ENFORCEMENT OFFICER WITH PATROL CAR. THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN PROVIDED:

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE = 120 HOUR

ITEM 614 - WORK ZONE MARKING SIGN:

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE MARKING SIGNS PER THE REQUIREMENTS OF MT-101.90, 614.04 AND 614.11.

				R4-1-18	R4-2-18	W8-H12A-36
	BEGIN	END	SIDE	DO NOT PASS	PASS WITH CARE	NO EDGE LINE
LOC	SLM	SLM				
				EACH	EACH	EACH
1	0.00	8.96	ROTH			20
2	0.00	6.19	RT	26	26	6
			TOTAL	26	26	26

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 614 - WORK ZONE MARKING SIGN = 78 EACH

ITEM 614 - WORK ZONE LANE LINE, CLASS III, 642 PAINT:

WORK ZONE LANE LINE SHALL BE PLACED TO REFLECT THE PROPOSED LANE LINE AS DETERMINED FROM THE PROPOSED MARKINGS WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND TYPE OF WORK ZONE MARKINGS NEEDED MEETING THE REQUIREMENTS OF ITEM 614 BEFORE THE REMOVAL OR RESURFACING OBLITERATES THE EXISTING. TO BE USED ON INTERMEDIATE WZ MARKINGS.

LOCATION	BEGIN SLM	END SLM	SIDE	APP	TOTAL	UNIT
1	0.00	8.96	WB	4	71.68	MI
1	0.00	8.96	EB	4	71.68	MI
				TOTAL	143.36	MI

INTERSTATE ROUTE 70 SHALL NOT BE LEFT AT ANYTIME WITH NO LANE LINES. ONE APPLICATION OF THE TYPE III 642 PAINT SHALL BE APPLIED AS SOON AS THE PERMANENT MARKINGS HAVE BEEN REMOVED BEFORE THE LANE IS OPENED TO THROUGH TRAFFIC.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 614 - WORK ZONE LANE LINE, CLASS III, 642 PAINT = 143.36 MI

ITEM 614 - WORK ZONE LANE LINE, CLASS II, 642 PAINT:

WORK ZONE LANE LINE SHALL BE PLACED TO REFLECT THE PROPOSED LANE LINE AS DETERMINED FROM THE PROPOSED MARKINGS WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND TYPE OF WORK ZONE MARKINGS NEEDED MEETING THE REQUIREMENTS OF ITEM 614 BEFORE THE REMOVAL OR RESURFACING OBLITERATES THE EXISTING. TO BE USED FOR FINAL WZ MARKINGS.

LOCATION	BEGIN SLM	END SLM	SIDE	APP	TOTAL	UNIT
1	0.00	8.96	WB	1	17.92	MI
1	0.00	8.96	EB	1	17.92	MI
				TOTAL	35.84	MI

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 614 - WORK ZONE LANE LINE, CLASS II, 642 PAINT = 35.84 MI

ITEM 614 - WORK ZONE CENTER LINE, CLASS III, 642 PAINT:

WORK ZONE CENTER LINE SHALL BE PLACED TO REFLECT THE PROPOSED CENTER LINE AS DETERMINED FROM THE PROPOSED MARKINGS WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND TYPE OF WORK ZONE MARKINGS NEEDED MEETING THE REQUIREMENTS OF ITEM 614 BEFORE THE REMOVAL OR RESURFACING OBLITERATES THE EXISTING. TO BE USED ON INTERMEDIATE WZ MARKINGS.

LOCATION	BEGIN SLM	END SLM	SIDE	APP	TOTAL	UNIT
2	0.00	6.19		2	12.38	MI
					TOTAL	12.38 MI

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 614 - WORK ZONE CENTER LINE, CLASS III, 642 PAINT = 12.38 MI

ITEM 614 - WORK ZONE CENTER LINE, CLASS II, 642 PAINT:

WORK ZONE CENTER LINE SHALL BE PLACED TO REFLECT THE PROPOSED CENTER LINE AS DETERMINED FROM THE PROPOSED MARKINGS WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND TYPE OF WORK ZONE MARKINGS NEEDED MEETING THE REQUIREMENTS OF ITEM 614 BEFORE THE REMOVAL OR RESURFACING OBLITERATES THE EXISTING. TO BE USED FOR FINAL WZ MARKINGS.

LOCATION	BEGIN SLM	END SLM	SIDE	APP	TOTAL	UNIT
2	0.00	6.19		1	6.19	MI
					TOTAL	6.19 MI

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 614 - WORK ZONE CENTER LINE, CLASS II, 642 PAINT = 6.19 MI

ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS III, 642 PAINT:

WORK ZONE CHANNELIZING LINE SHALL BE PLACED TO REFLECT THE PROPOSED CHANNELIZING LINE AS DETERMINED FROM THE PROPOSED MARKINGS WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND TYPE OF WORK ZONE MARKINGS NEEDED MEETING THE REQUIREMENTS OF ITEM 614 BEFORE THE REMOVAL OR RESURFACING OBLITERATES THE EXISTING. TO BE USED ON INTERMEDIATE WZ MARKINGS.

LOCATION	BEGIN SLM	END SLM	SIDE	APP	TOTAL	UNIT
1	0.00	8.96	WB	3	14,442	FT
1	0.00	8.96	EB	3	10,485	FT
2	0.00	6.19	BOTH	2	400	FT
					TOTAL	25,327 FT

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS III, 642 PAINT = 25,327 FT

ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS II, 642 PAINT:

WORK ZONE CHANNELIZING LINE SHALL BE PLACED TO REFLECT THE PROPOSED CHANNELIZING LINE AS DETERMINED FROM THE PROPOSED MARKINGS WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND TYPE OF WORK ZONE MARKINGS NEEDED MEETING THE REQUIREMENTS OF ITEM 614 BEFORE THE REMOVAL OR RESURFACING OBLITERATES THE EXISTING. TO BE USED FOR FINAL WZ MARKINGS.

LOCATION	BEGIN SLM	END SLM	SIDE	APP	TOTAL	UNIT
1	0.00	8.96	WB	2	9,628	FT
1	0.00	8.96	EB	2	9,628	FT
2	0.00	6.19	BOTH	1	200	FT
					TOTAL	19,456 FT

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS II, 642 PAINT = 19,456 FT

**ITEM 614 - WORK ZONE EDGE LINE, CLASS III, 642 PAINT:**  
WORK ZONE EDGE LINE SHALL BE PLACED TO REFLECT THE PROPOSED EDGE LINE AS DETERMINED FROM THE PROPOSED MARKINGS WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND TYPE OF WORK ZONE MARKINGS NEEDED MEETING THE REQUIREMENTS OF ITEM 614 BEFORE THE REMOVAL OR RESURFACING OBLITERATES THE EXISTING. TO BE USED FOR FINAL WZ MARKINGS.

LOCATION	BEGIN SLM	END SLM	SIDE	APP	TOTAL	UNIT
1	0.00	8.96	WB	4	71.68	MI
1	0.00	8.96	EB	4	71.68	MI
2	0.00	6.19	BOTH	2	26.96	MI
TOTAL					170.32	MI

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 614 - WORK ZONE EDGE LINE, CLASS II, 642 PAINT  
= 170.32 MI

**ITEM 614 - WORK ZONE STOP LINE, CLASS II, 642 PAINT:**  
WORK ZONE STOP LINE SHALL BE PLACED TO REFLECT THE PROPOSED STOP LINE AS DETERMINED FROM THE PROPOSED MARKINGS WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND TYPE OF WORK ZONE MARKINGS NEEDED MEETING THE REQUIREMENTS OF ITEM 614 BEFORE THE REMOVAL OR RESURFACING OBLITERATES THE EXISTING. TO BE USED FOR FINAL WZ MARKINGS.

LOCATION	BEGIN SLM	END SLM	SIDE	APP	TOTAL	UNIT
2	0.00	6.19	BOTH	1	24	FT
TOTAL					24	FT

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 614 - WORK ZONE STOP LINE, CLASS II, 642 PAINT  
= 24 FT

**ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN:**  
THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT RECONSTRUCTION

**ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN (CONT. ):**  
CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

THE FOLLOWING QUANTITY HAS BEEN CALCULATED AT 2 FOR 60 DAYS:

LOCATION	# OF PCMS	DAYS	TOTAL	UNIT
1	2	60	4	SGN MNTH
TOTAL			4	SGN MNTH

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 4  
SIGN MONTH ASSUMING 2 PCMS SIGN(S) FOR 2 MONTH(S).

TOTAL OF 4 SGN MNTH CARRIED TO GENERAL SUMMARY

**WRITTEN NOTICE OF CLOSURE:**  
AT LEAST SEVEN DAYS PRIOR TO A CLOSURE, ADVANCED WRITTEN NOTIFICATION SHALL BE MADE TO LOCAL AGENCIES OR OFFICIALS HAVING JURISDICTION OVER EMERGENCY SERVICES IN THE AREA (POLICE, FIRE, MEDICAL), SCHOOL BUS ROUTES, UTILITY SERVICES, ALL RESIDENCES ADJACENT TO THE PROJECT SITE, AND MAINTENANCE OF LOCAL ROADS. SUCH NOTIFICATION SHALL ALSO BE MADE TO ODOT DISTRICT SIX PUBLIC INFORMATION OFFICER (740-833-8063) AND THE MAINTENANCE OF TRAFFIC ENGINEER (740-833-8323). THE NOTIFICATION SHALL STATE WHEN THE CLOSURE WILL START AND THE ANTICIPATED PERIOD OF CLOSURE.

IF, SUBSEQUENT TO THAT NOTIFICATION, THE START DATE IS CHANGED, THEN A NEW SEVEN-DAY NOTIFICATION WILL BE REQUIRED. THE ROAD CANNOT BE CLOSED UNLESS THIS PRIOR NOTIFICATION PROCESS HAS BEEN ACCOMPLISHED. THE SAME PARTIES SHALL BE NOTIFIED WHEN A CLOSURE HAS CONCLUDED.

**PUBLIC OUTREACH AND NOTIFICATION (RESURFACING PROJECTS):**  
THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE DISTRICT 6 PUBLIC INFORMATION OFFICE VIA EMAIL AT d06.pio@dot.ohio.gov TO COORDINATE EFFORTS TO NOTIFY ADJACENT RESIDENTS AND BUSINESSES OF THE UPCOMING RESURFACING PROJECT. ADVANCE NOTIFICATION SHALL OCCUR NO LATER THAN FOURTEEN (14) DAYS PRIOR TO THE FIRST DAY OF WORK. ALL NOTIFICATIONS SHALL BE MADE UTILIZING THE TEMPLATE PROVIDED BY THE DISTRICT 6 PUBLIC INFORMATION OFFICE.

**TRUCK MOUNTED ATTENUATOR (TMA) - TWO LANE ROADS:**  
WHEN WORKING IN A CLOSED LANE OR SHOULDER ON A TWO LANE HIGHWAY WITHOUT TEMPORARY OR PERMANENT TRAFFIC BARRIERS SEPARATING THE WORK AREA FROM THE TRAVELED LANE, A TRUCK MOUNTED ATTENUATOR (TMA) SHALL BE PROVIDED TO PROTECT EACH WORK AREA IN ACCORDANCE WITH OMUTCD TYPICAL APPLICATION (TA) 4, TA-6 AND TA-17, ALONG WITH STANDARD CONSTRUCTION DRAWING (SCD) MT-97.10. THE TMA SHALL BE PLACED IN SUCH A WAY TO ADEQUATELY PROTECT THE WORKERS INSIDE THE WORK ZONE. THE TMA IS NOT INTENDED TO BE USED AS OR SUBSTITUTED FOR THE FLAGGERS AND/OR WARNING SIGNS AND DEVICES. THE TMA SHALL MEET NCHRP 350 TEST LEVEL 3 CRITERIA FOR STANDARD AND OPTIONAL TESTS AT 100 KM/H (62 MPH) FOR DESIGN IMPACTS. THE COST FOR PROVIDING THE TMA SHALL INCLUDE ALL MATERIAL, LABOR, EQUIPMENT, AND HARDWARE REPLACEMENT AND IS TO BE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

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0



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<div><div>16</div><div>36</div></div>	MAD-70 / 56-0.00 / 0.00	GUARDRAIL PLAN SUBSUMMARY	CALCULATED
			MAK
			CHECKED
			DKR



 EMBANKMENT, AS PER PLAN  
AND LINEAR GRADING

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36

MAD-70/56-0.00/0.00

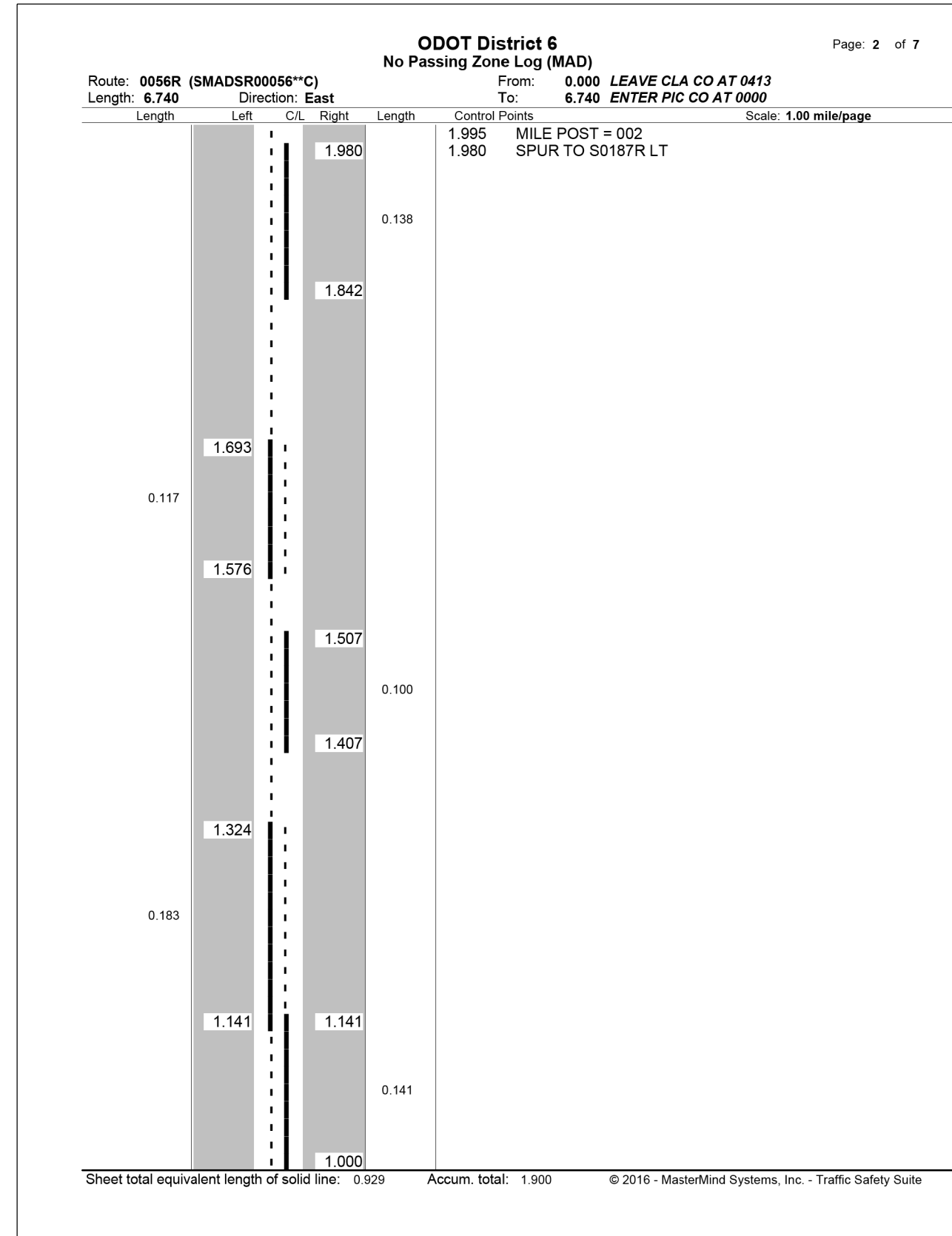


DETAIL	STANDARD DRAWING TC-65.11
10	APPROACH W/ LEFT TURN LANE
11	STOP APPROACH
12	TWO WAY LEFT TURN LANE
13	ONE LANE BRIDGE
14	HORIZONTAL CURVE

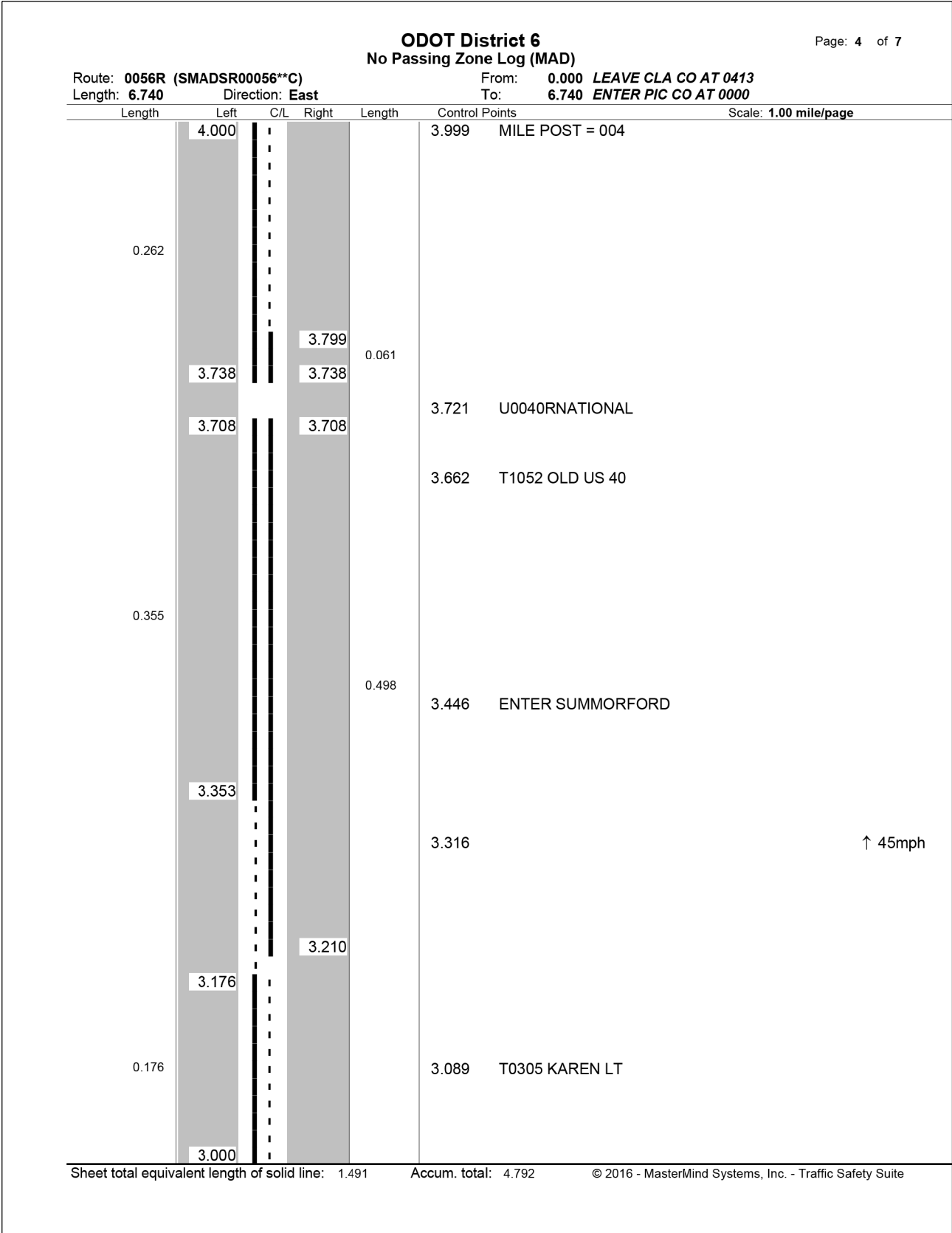
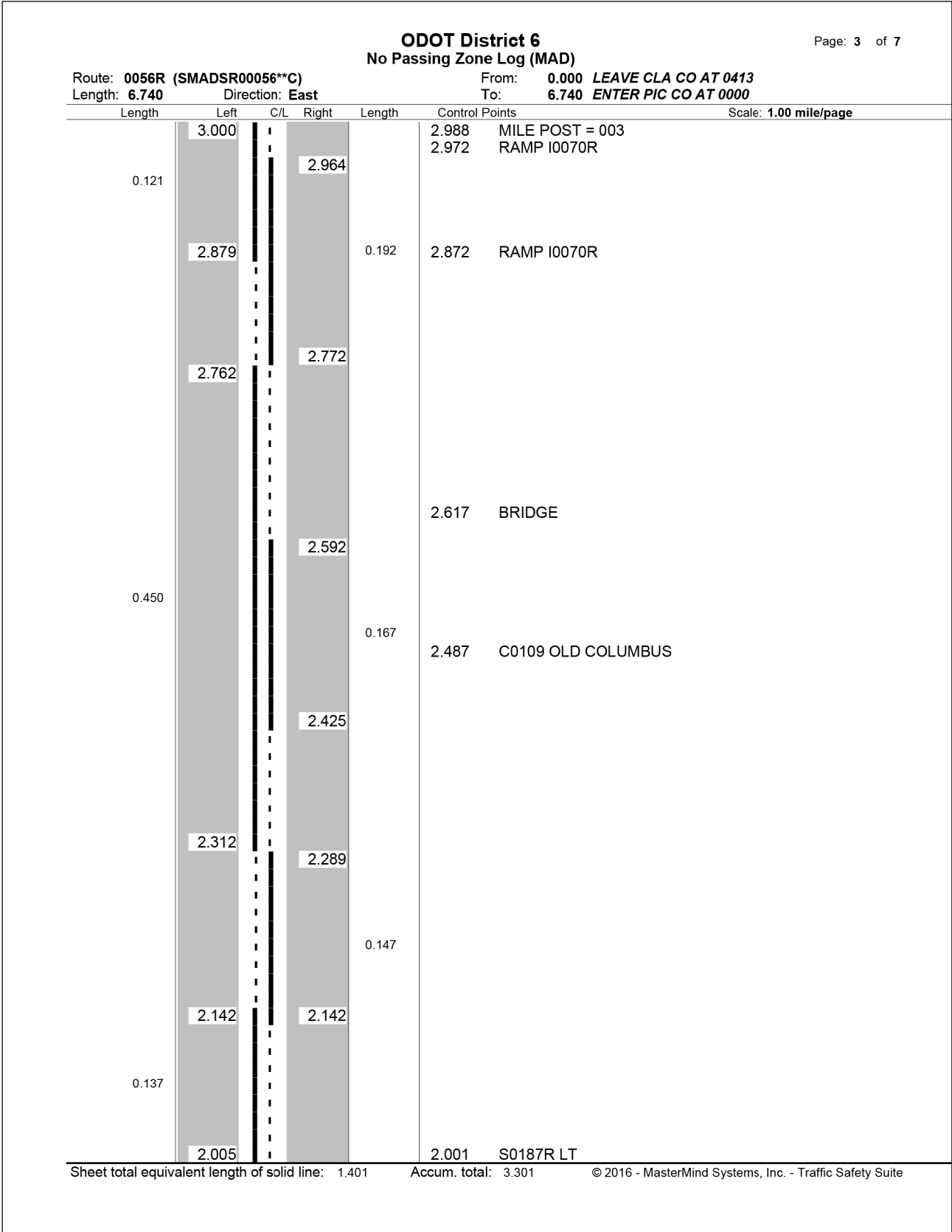
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**MAD-70 / 56 - 0.00 / 0.00**

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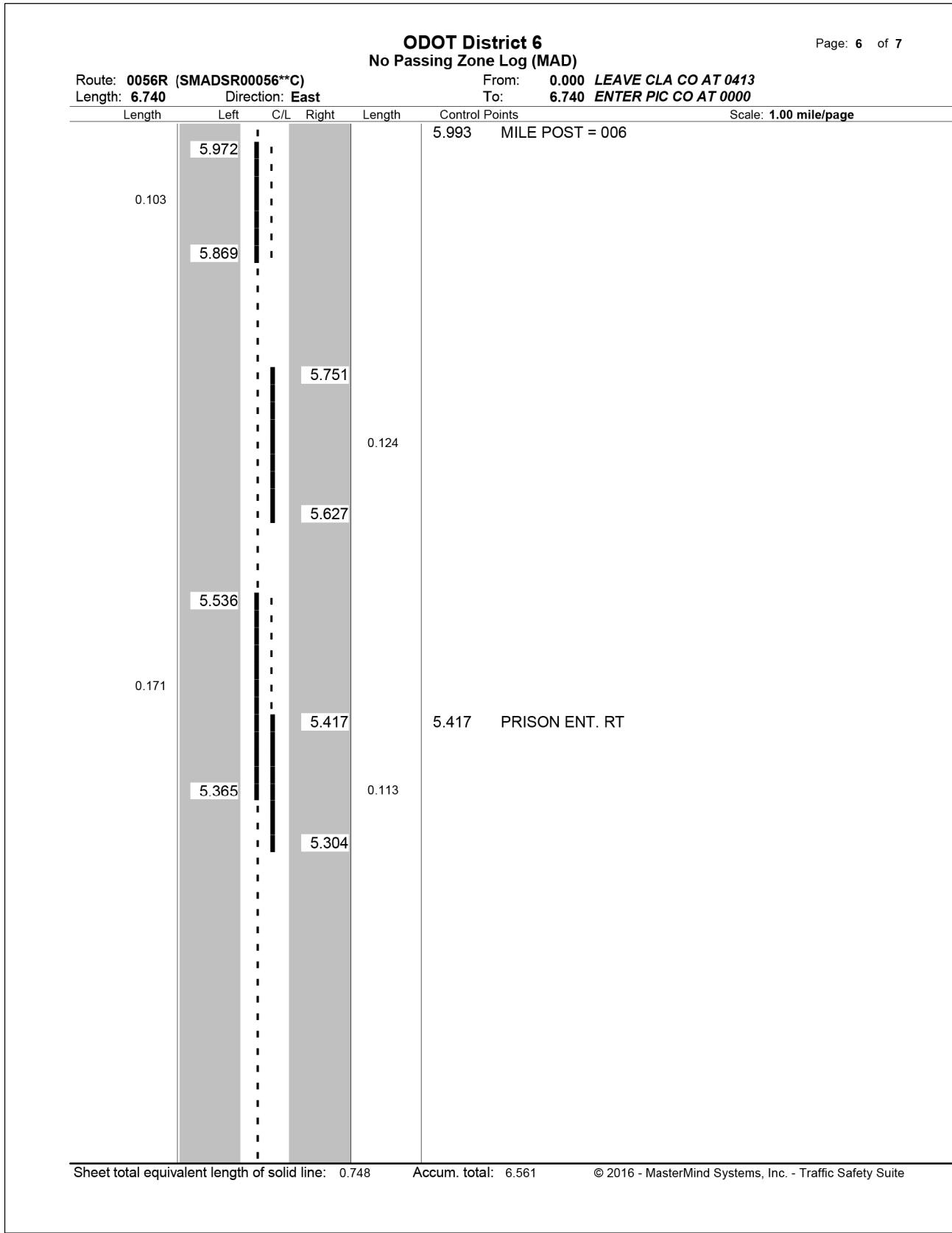
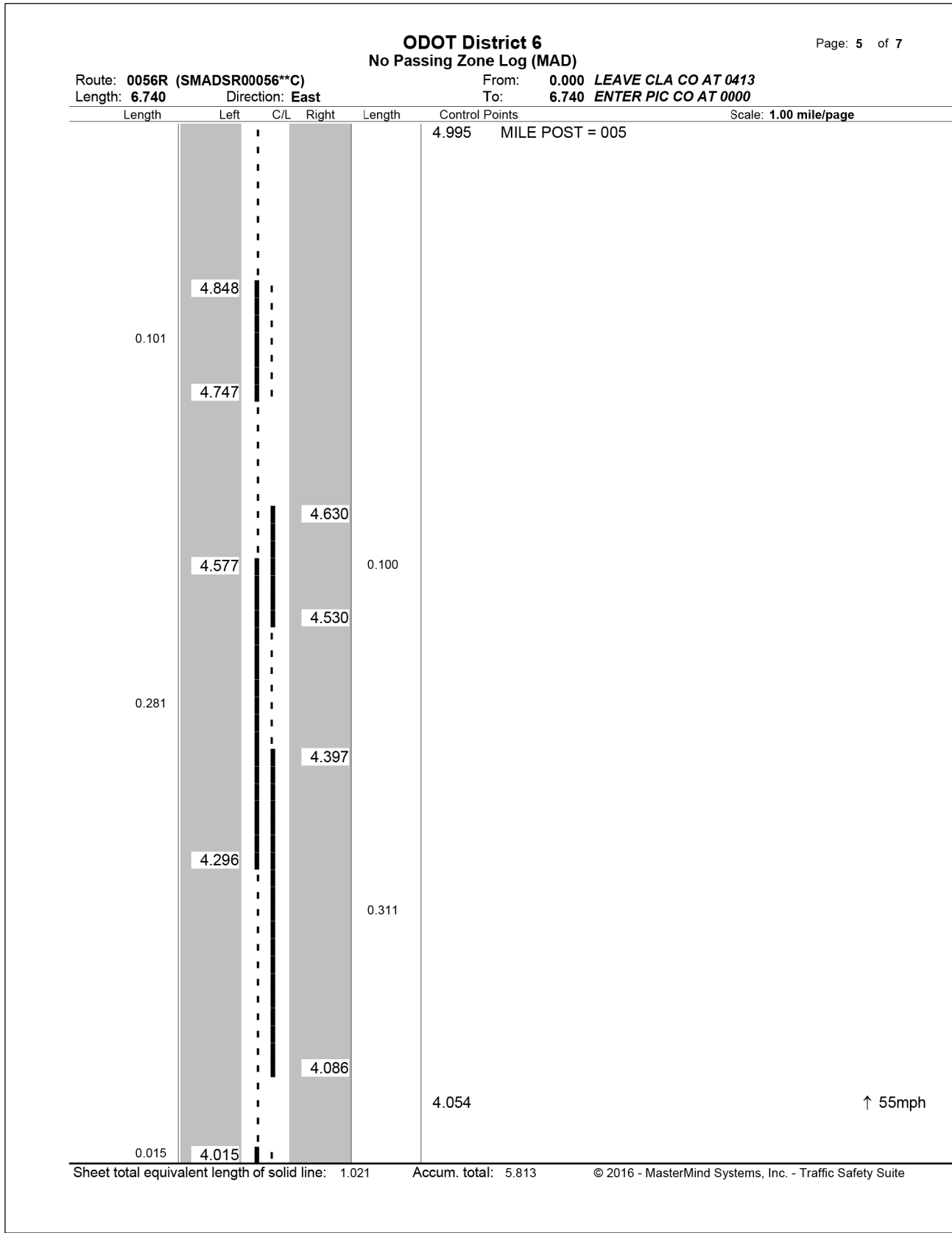


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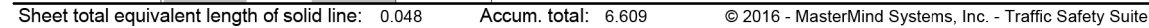




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Length	Left	C/L	Right	Length	Control Points	Scale: 1.00 mile/page
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24  
36



## STRUCTURE ESTIMATED QUANTITIES



NOTE 1:  
DO NOT PAVE BRIDGE DECK. PROPOSED SEALING OF CONCRETE  
BRIDGE DECK WITH HMWM.

BRIDGE PLAN

MAD-38-1744 (4900391)

CONTINUOUS STEEL BEAM UNDER S.R. 38



### PROPOSED BRIDGE TREATMENT

# STRUCTURE SUBSUMMARY AND DETAILS

## MAD -38-1744

**MAD-70/ 56-0.00/ 0.00**

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CALCULATED	
MAK	
CHECKED	
XXX	

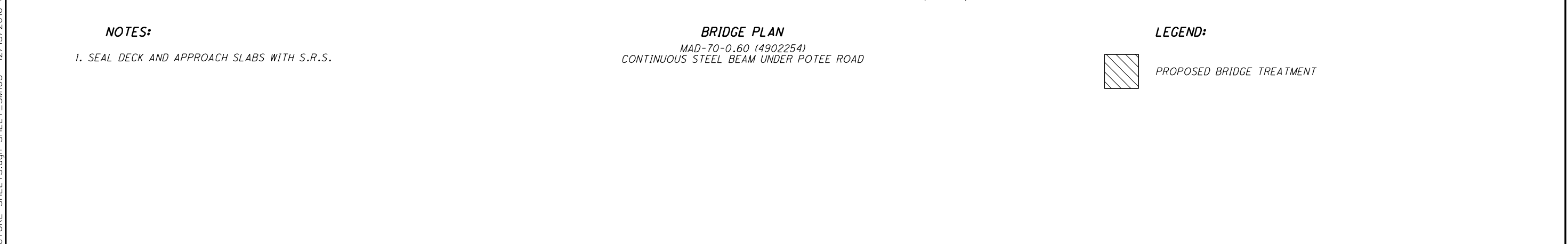
DESIGN AGENCY  
DIST. #6  
IN-HOUSE DESIGN

REVIEWED	DATE
SFN:	4900391



### PROPOSED BRIDGE TREATMENT





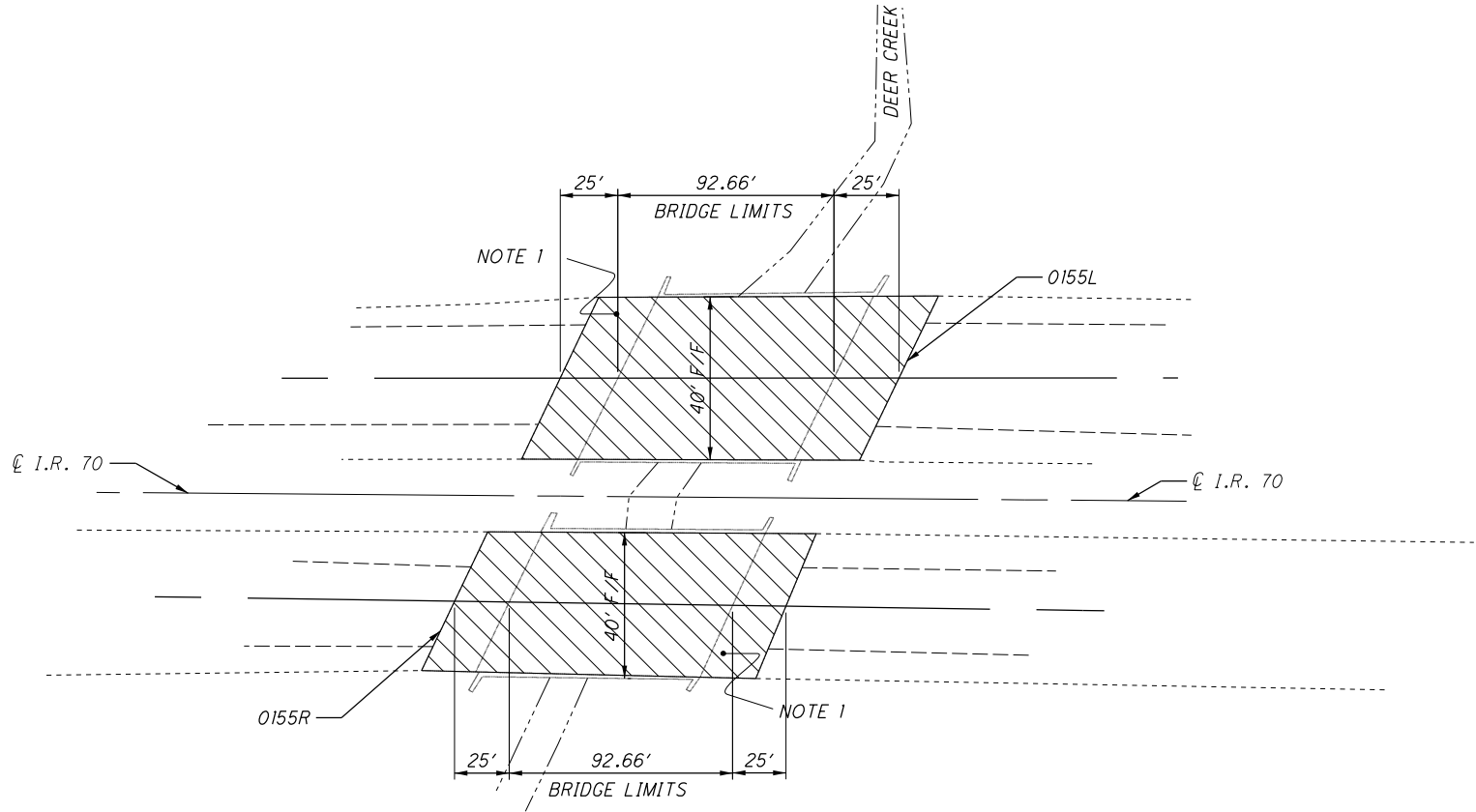
LOCATION				QUANTITIES						REMARKS	STRUCTURE NOTES	
C O U N T Y	R O U T E	S L M	D E C K A R E A		512					SCOPE OF WORK	MAD-70-0060 SFN: 4902254	
					TREATING OF CONCRETE BRIDGE DECKS WITH SRS, AS PER PLAN							SKEW: 34°  REINFORCED CONCRETE DECK
					SY							
	70	0.60	9,784		1,088					DECK AREA		
			1,400		156					APPROACH SLABS		
TOTALS CARRIED TO STRUCTURE QUANTITIES					1,244							



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NOTES:

1. SEAL DECK AND APPROACH SLABS WITH SRS



BRIDGE PLAN

MAD-70-0155L (4902289)  
MAD-70-0155R (4902319)  
CONTINUOUS CONCRETE SLAB OVER DEER CREEK

LEGEND:



PROPOSED BRIDGE TREATMENT

LOCATION				QUANTITIES					REMARKS	STRUCTURE NOTES
C O U N T Y	R O U T E	S L M	D E C K A R E	512					SCOPE OF WORK	MAD-70-0155L SFN: 4902289 MAD-70-0155R SFN: 4902319  SKEW: 25°  REINFORCED CONCRETE DECK
				TREATING OF CONCRETE BRIDGE DECKS WITH SRS, AS PER PLAN						
				SY						
MAD	70	155L	6,609	735					DECK AREA	
			3,450	384					APPROACH SLABS	
MAD	70	155R	5,838	649					DECK AREA	
			3,050	339					APPROACH SLABS	
TOTALS CARRIED TO STRUCTURE QUANTITIES				2,107						

STRUCTURE SUBSUMMARY AND DETAILS  
MAD-70-0155 L / R

MAD-70 / 56-0.00 / 0.00  
PID NO.: 101616

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36

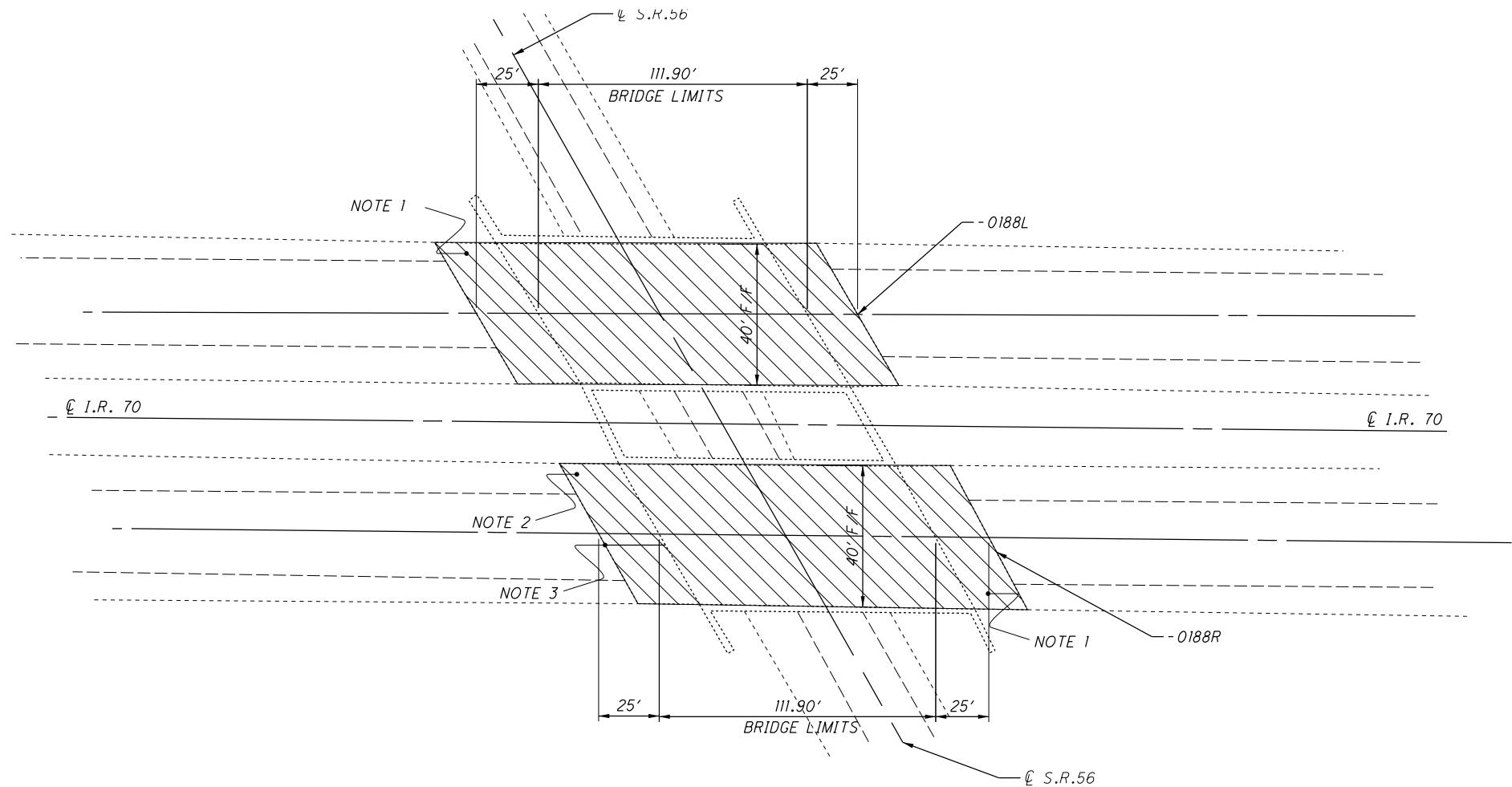
DESIGN AGENCY  
DIST. #6  
IN-HOUSE DESIGN

REVIEWED  
SFN: 4902289  
4902319

DATE  
4902289  
4902319

CALCULATED  
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CHECKED  
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NOTES:

1. SEAL DECK AND APPROACH SLABS WITH SRS
2. RAISE APPROACH SLAB WITH URETEK
3. SEAL CRACK USING EPOXY INJECTION

BRIDGE PLAN

MAD-70-0188L (4902343)  
MAD-70-0188R (4902351)  
SIMPLE STEEL BEAM OVER SR 56

LEGEND:



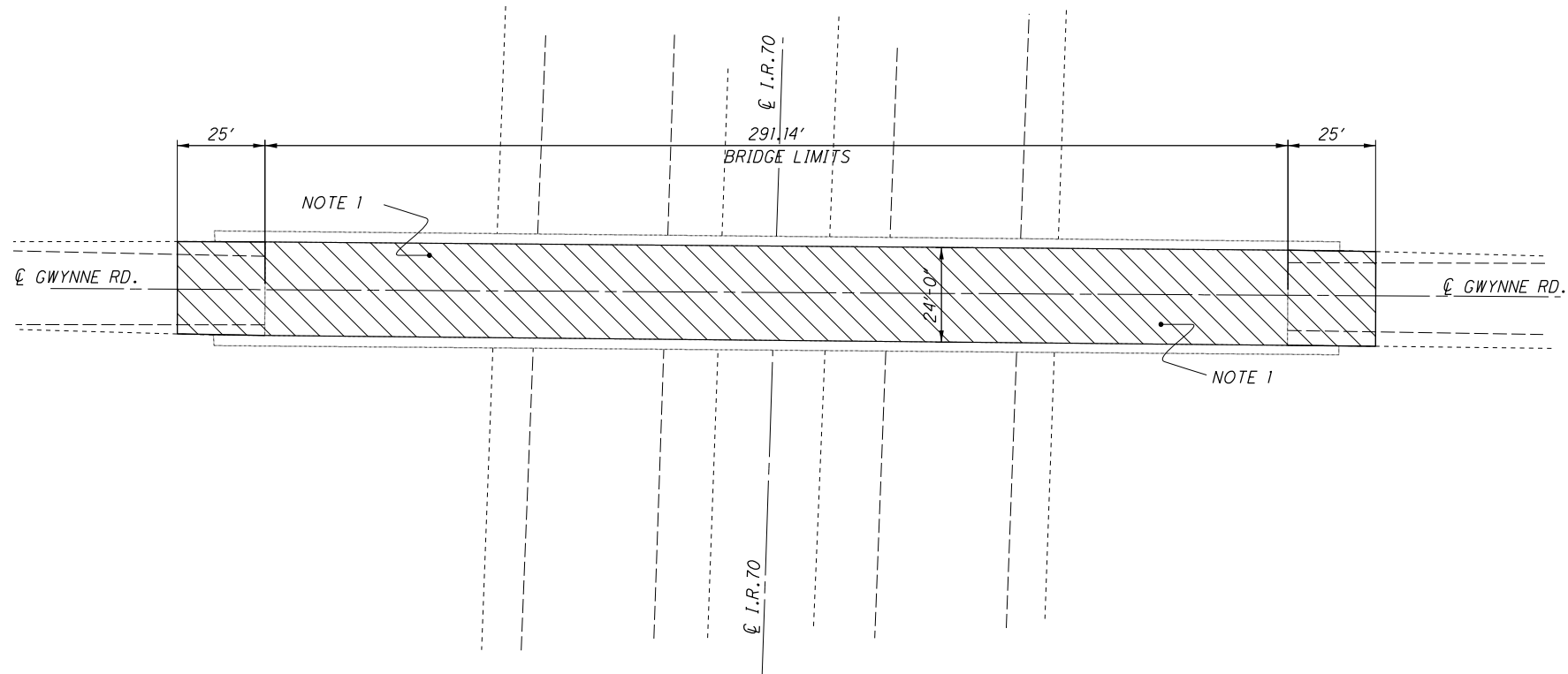
PROPOSED BRIDGE TREATMENT

LOCATION				QUANTITIES				REMARKS	STRUCTURE NOTES
C O U N T Y	R O U T E	S L O T N O	D E C K A R E A		512		842	SCOPE OF WORK	MAD-70-0188L SFN: 4902343 MAD-70-0188R SFN: 4902351  SKEW: 30°  REINFORCED CONCRETE DECK
					TREATING OF CONCRETE BRIDGE DECKS WITH SRS, AS PER PLAN	CONCRETE REPAIR BY EPOXY INJECTION, AS PER PLAN	CORRECTING ELEVATION OF CONCRETE APPROACH SLABS WITH HIGH DENSITY POLYURETHANE		
					SY	FT	LB		
MAD	70	188L	6,254		695			DECK AREA	
			3,536		393			APPROACH SLABS	
MAD	70	188R	7,050		784			DECK AREA	
			3,536		393	25	1,190	APPROACH SLABS	
TOTALS CARRIED TO STRUCTURE QUANTITIES					2,265	25	1,190		

STRUCTURE SUBSUMMARY AND DETAILS  
MAD-70-0188 L / R

MAD-70 / 56-0.00 / 0.00  
PID NO.: 101616

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**NOTES:**  
1. SEAL DECK AND APPROACH SLABS WITH SRS.

**BRIDGE PLAN**  
MAD-70-0332 (4902467)  
CONTINUOUS STEEL BEAM UNDER GWYNNE ROAD

**LEGEND:**  
 PROPOSED BRIDGE TREATMENT

LOCATION				QUANTITIES					REMARKS	STRUCTURE NOTES	
C O U N T Y	R O U T E	S L M	D E C K A R E A		512				SCOPE OF WORK	MAD-70-0332 SFN: 4902467	
					TREATING OF CONCRETE BRIDGE DECKS WITH SRS, AS PER PLAN					SKEW: 0°	
					SY						
MAD	70	3.32	8,471 1,285		942 143				DECK AREA APPROACH SLABS		
TOTALS CARRIED TO STRUCTURE QUANTITIES					1,085						

STRUCTURE SUBSUMMARY AND DETAILS

MAD-70-0332  
MAD-70-0332

MAD-70 / 56-0.00 / 0.00  
PID NO.: 101616

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DESIGN AGENCY  
DIST. #6  
IN-HOUSE DESIGN

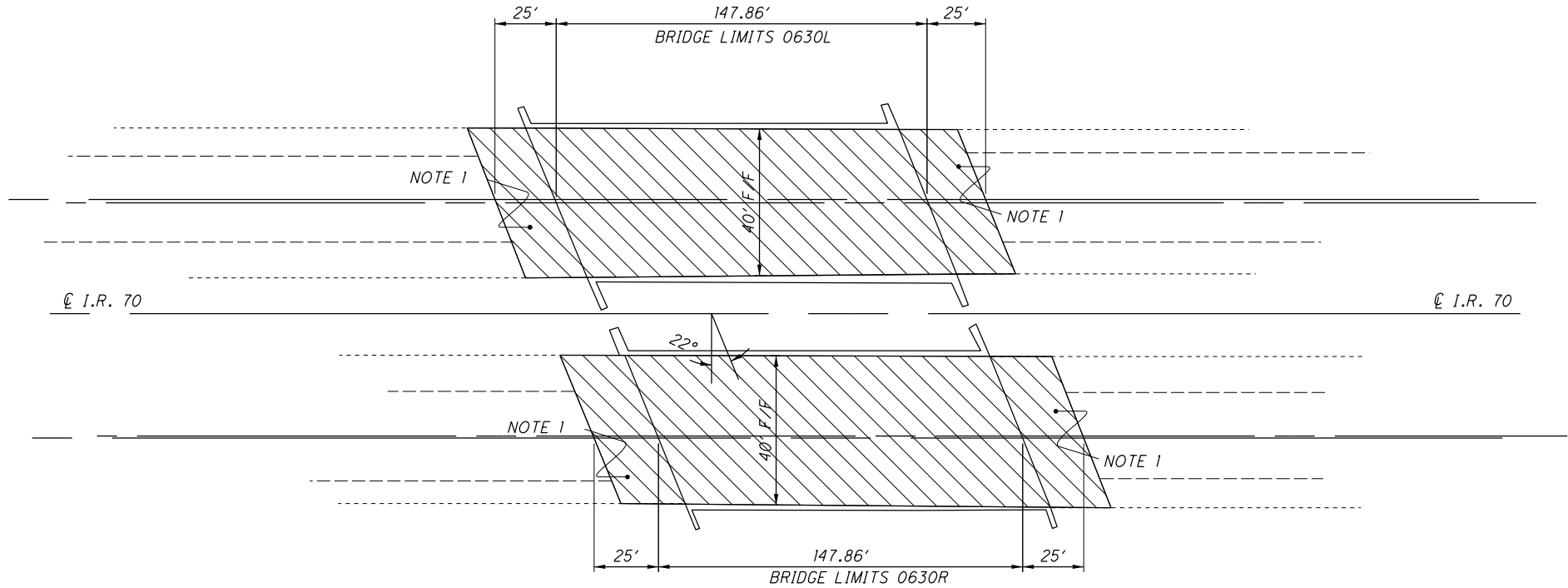
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NOTES:  
1. SEAL DECK AND APPROACH SLABS WITH SRS.



BRIDGE PLAN  
MAD-70-0630L (4902491)  
MAD-70-0630R (4902521)  
CONTINUOUS STEEL BEAM OVER DEER CREEK

LEGEND:  
 PROPOSED BRIDGE TREATMENT

LOCATION				QUANTITIES					REMARKS	STRUCTURE NOTES
C O U N T Y	R O U T E	S L M	D E C K A R E	512					SCOPE OF WORK	MAD-70-0630L SFN: 4902491
				TREATING OF CONCRETE BRIDGE DECKS WITH SRS, AS PER PLAN						MAD-70-0630R SFN: 4902521
				SY						SKEW: 22°
MAD	70	6.30L	9,311 3,004	1,035 334					DECK AREA APPROACH SLABS	
MAD	70	6.30R	9,311 3,004	1,035 334					DECK AREA APPROACH SLABS	
TOTALS CARRIED TO STRUCTURE QUANTITIES				2,738						

STRUCTURE SUBSUMMARY AND DETAILS  
MAD-70-0630 L / R

MAD-70 / 56-0.00 / 0.00  
PID NO.: 101616

9 / 12

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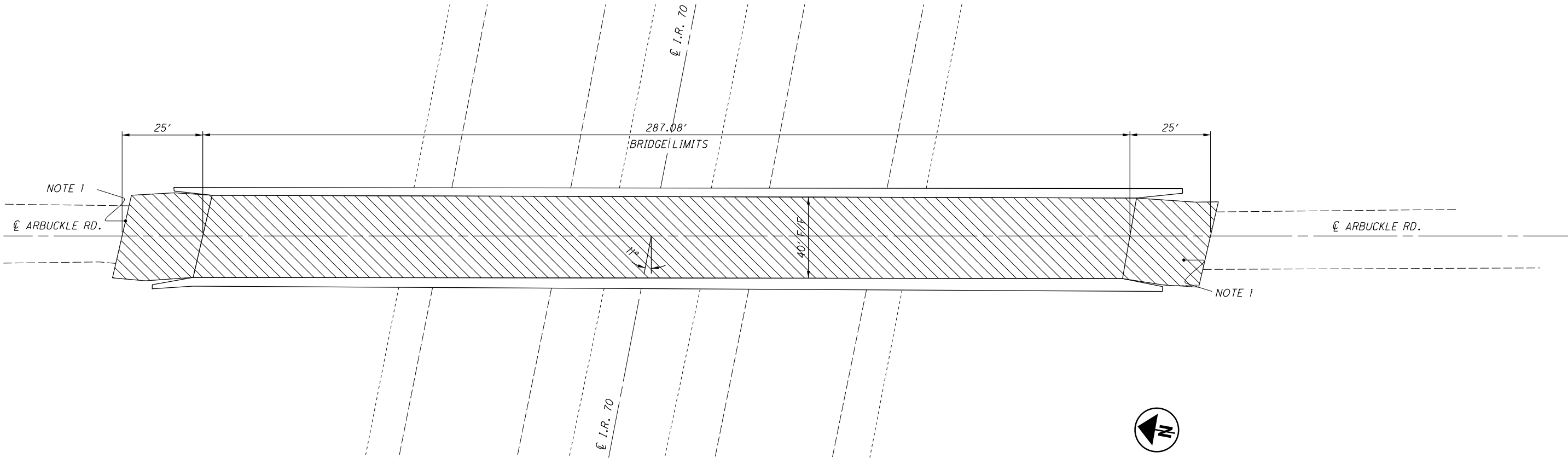
DESIGN AGENCY  
DIST. #6  
IN-HOUSE DESIGN

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SFN: 4902491  
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NOTES:

1. SEAL DECK AND APPROACH SLABS WITH SRS.

BRIDGE PLAN

MAD-70-0644 (4902556)  
CONTINUOUS STEEL BEAM UNDER ARBUCKLE ROAD

LEGEND:



PROPOSED BRIDGE TREATMENT

LOCATION				QUANTITIES					REMARKS	STRUCTURE NOTES	
C O U N T Y	R O U T E	S L O W	D E C K A R E		512				SCOPE OF WORK	MAD-70-0643 SFN: 4902556	
					TREATING OF CONCRETE BRIDGE DECKS WITH SRS, AS PER PLAN					SKEW: 11°	
					SY						
MAD	70	6.44	8,611 1,324		957 148				DECK AREA APPROACH SLABS		
TOTALS CARRIED TO STRUCTURE QUANTITIES					1,105						

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MAK

DESIGN AGENCY  
DIST. #6  
IN-HOUSE DESIGN

REVIEWED  
SFN: 4902556

DATE

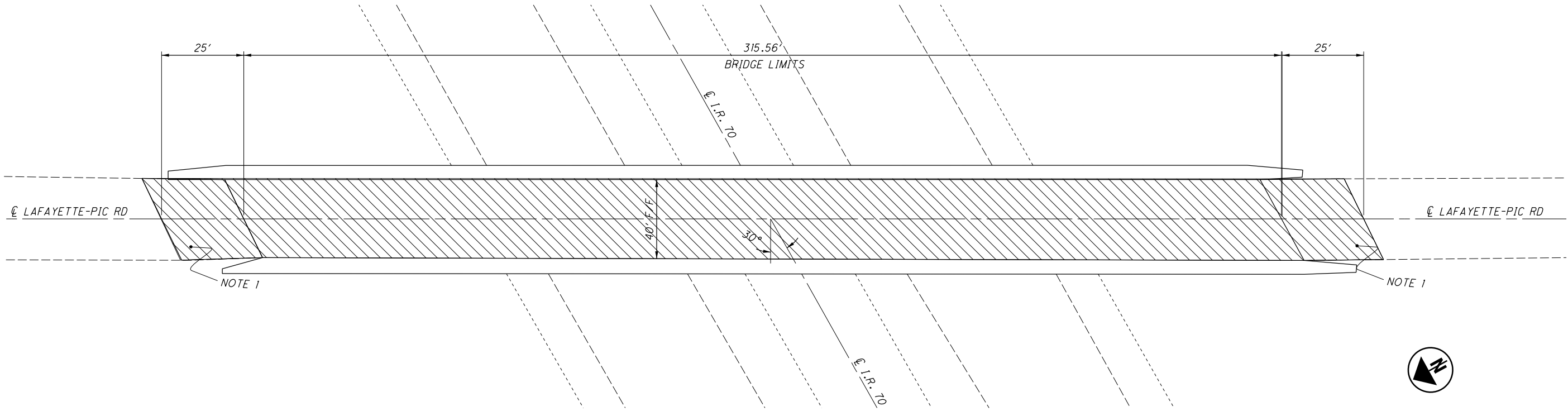
STRUCTURE SUBSUMMARY AND DETAILS  
MAD-70-0644

MAD-70 / 56-0.00 / 0.00  
PID NO.: 101616

10 / 12


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**NOTES:**  
1. SEAL DECK AND APPROACH SLABS WITH SRS.

**BRIDGE PLAN**  
MAD-70-0716 (4902580)  
CONTINUOUS STEEL BEAM UNDER LAFAYETTE - PLAIN CITY ROAD

**LEGEND:**  
 PROPOSED BRIDGE TREATMENT

LOCATION				QUANTITIES					REMARKS	STRUCTURE NOTES	
C O U N T Y	R O U T E	S L M	D E C K A R E		512				SCOPE OF WORK	MAD-70-0715 SFN: 4902580	
					TREATING OF CONCRETE BRIDGE DECKS WITH SRS, AS PER PLAN					SKEW: 30°	
					SY						
MAD	70	7.16	9,462 1,308		1,052 146				DECK AREA APPROACH SLABS		
TOTALS CARRIED TO STRUCTURE QUANTITIES					1,198						

CALCULATED  
MAK

DESIGN AGENCY  
DIST. #6  
IN-HOUSE DESIGN

REVIEWED  
SFN: 4902580

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STRUCTURE SUBSUMMARY AND DETAILS  
MAD-70-0716

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STANDARD DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS:  
REFER TO THE FOLLOWING STANDARD BRIDGE DRAWING(S):

AS-1-15 DATED 7/17/2015  
AS-2-15 DATED 7/17/2015

REFER TO THE FOLLOWING SUPPLEMENTAL SPECIFICATION(S):

SS 842 DATED 7/15/2011

DESIGN SPECIFICATIONS:  
THIS STRUCTURE CONFORMS TO "STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 2002, AND THE ODOT BRIDGE DESIGN MANUAL.

EXISTING STRUCTURE VERIFICATION:  
DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE, AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK, BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CONSTRUCTION MATERIAL AND SPECIFICATIONS (CMS) SECTION 102.05.

CONTRACT BID PRICES SHALL BE BASED UPON RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PRE-BID EXAMINATION OF THE EXISTING STRUCTURE BY THE CONTRACTOR. HOWEVER, ALL PROJECT WORK SHALL BE BASED UPON ACTUAL DETAILS AND DIMENTIONS, WHICH HAVE BEEN VERIFIED BY THE CONTRACTOR IN THE FIELD.

CONTINGENCY QUANTITIES:  
THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED AS DIRECTED BY THE ENGINEER, UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THE PROJECT.

ITEM 512 - TREATING OF CONCRETE BRIDGE DECKS WITH SRS, AS PER PLAN:  
THIS ITEM SHALL BE FOR TREATING THE FOLLOWING WITH SRS: DECKS AND APPROACH SLABS

PAYMENT: THE DEPARTMENT WILL CONSIDER THE UNIT BID PRICE PER SQUARE YARD TO INCLUDE ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK UNDER ITEM 512 - TREATING OF CONCRETE BRIDGE DECK WITH SRS, AS PER PLAN.

ITEM 512 - SEALING OF CONCRETE BRIDGE DECKS WITH HMWM RESIN, AS PER PLAN:  
THIS ITEM SHALL BE FOR SEALING THE FOLLOWING WITH HMWM RESIN: DECKS AND APPROACH SLABS

PAYMENT: THE DEPARTMENT WILL CONSIDER THE UNIT BID PRICE PER SQUARE YARD TO INCLUDE ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK UNDER ITEM 512 - SEALING OF CONCRETE BRIDGE DECKS WITH HMWM RESIN, AS PER PLAN.

ITEM 512 - CONCRETE REPAIR BY EPOXY INJECTION, AS PER PLAN:  
THIS ITEM SHALL BE FOR REPAIRING THE LONGITUDINAL CRACK IN THE REAR APPROACH SLAB FOR STRUCTURE MAD-70-0188R. EPOXY INJECTION SHALL BE COMPLETED ONLY AFTER SUCESSFUL COMPLETION OF ITEM 842 - CORRECTING ELEVATION OF CONCRETE APPROACH SLABS WITH HIGH DENSITY POLYURETHANE.

ITEM 512 - CONCRETE REPAIR BY EPOXY INJECTION, AS PER PLAN (CONT.):  
PAYMENT: THE DEPARTMENT WILL CONSIDER THE UNIT BID PRICE PER LINEAL FOOT TO INCLUDE ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK UNDER ITEM 512 - CONCRETE REPAIR BY EPOXY INJECTION, AS PER PLAN.

ITEM 530 - SPECIAL - STRUCTURES: CLEAN OUT STRIP SEAL GLAND:  
THE CONTRACTOR IS TO CLEAN OUT THE STRIP SEAL GLAND ON BOTH ENDS OF THE BRIDGE WITH COMPRESSED AIR OR POWER WASHING. ANY DAMAGE TO THE STRIP SEAL WILL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.

PAYMENT: THE DEPARTMENT WILL CONSIDER THE UNIT BID PRICE PER LINEAL FOOT TO INCLUDE ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK UNDER ITEM 530 - SPECIAL - STRUCTURES: CLEAN OUT STRIP SEAL GLAND.

MAD-38-1744 (4900391):  
THE BRIDGE IS TO BE REPAIRED AS INDICATED ON SHEET 28. THE DECK AND APPROACH SLABS ARE TO BE SEALED USING ITEM 512 - HMWM RESIN.

MAD-56-0262 (4901746):  
THE BRIDGE IS TO BE REPAIRED AS INDICATED ON SHEET 29. THE DECK AND APPROACH SLABS ARE TO BE SEALED USING ITEM 512 - SOLUABLE REACTIVE SILICATE (SRS), AND THE STRIP SEAL GLANDS ARE TO BE CLEANED AS PER ITEM 530 - SPECIAL - STRUCTURES: CLEAN OUT STRIP SEAL GLAND.

MAD-70-0060 (4902254):  
THE BRIDGE IS TO BE REPAIRED AS INDICATED ON SHEET 30. THE DECK AND APPROACH SLABS ARE TO BE SEALED USING ITEM 512 - SOLUABLE REACTIVE SILICATE (SRS).

MAD-70-0155L (4902289):  
MAD-70-0155R (4902319):  
THE BRIDGES ARE TO BE REPAIRED AS INDICATED ON SHEET 31. THE DECK AND APPROACH SLABS ARE TO BE SEALED USING ITEM 512 - SOLUABLE REACTIVE SILICATE (SRS).

MAD-70-0188L (4902343):  
THE BRIDGE IS TO BE REPAIRED AS INDICATED ON SHEET 32. THE DECK AND APPROACH SLABS ARE TO BE SEALED USING ITEM 512 - SOLUABLE REACTIVE SILICATE (SRS).

MAD-70-0188R (4902351):  
THE BRIDGE IS TO BE REPAIRED AS INDICATED ON SHEET 32. THE REAR APPROACH SLAB IS TO BE RAISED USING ITEM 842 - CORRECTING ELEVATION OF CONCRETE APPROACH SLABS WITH HIGH DENSITY POLYURETHANE AND THE LONGITUDIANL CRACK IS THEN TO BE REPAIRED USING UNDER ITEM 512 - CONCRETE REPAIR BY EPOXY INJECTION.

THE DECK AND APPROACH SLABS ARE TO THEN TO BE SEALED USING ITEM 512 - SOLUABLE REACTIVE SILICATE (SRS).

MAD-70-0332 (4902467):  
THE BRIDGE IS TO BE REPAIRED AS INDICATED ON SHEET 33. THE DECK AND APPROACH SLABS ARE TO BE SEALED USING ITEM 512 - SOLUABLE REACTIVE SILICATE (SRS).

MAD-70-0630L (4902491):  
MAD-70-0630R (4902521):  
THE BRIDGES ARE TO BE REPAIRED AS INDICATED ON SHEET 34. THE DECK AND APPROACH SLABS ARE TO BE SEALED USING ITEM 512 - SOLUABLE REACTIVE SILICATE (SRS).

MAD-70-0644 (4902556):  
THE BRIDGE IS TO BE REPAIRED AS INDICATED ON SHEET 35. THE DECK AND APPROACH SLABS ARE TO BE SEALED USING ITEM 512 - SOLUABLE REACTIVE SILICATE (SRS).

MAD-70-0716 (4902580):  
THE BRIDGE IS TO BE REPAIRED AS INDICATED ON SHEET 36. THE DECK AND APPROACH SLABS ARE TO BE SEALED USING ITEM 512 - SOLUABLE REACTIVE SILICATE (SRS).

<div><div>36</div><div>36</div></div>		12 / 12	MAD-70 / 56-0.00 / 0.00	STRUCTURE NOTES			DESIGN AGENCY		CALCULATED
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			SFN:	VARIOUS	IN-HOUSE DESIGN		CHECKED	XXX	