

LOCATION MAP

LATITUDE: N41°03'25" LONGITUDE: W80°37'45"



PORTION TO BE IMPROVED	—————
INTERSTATE HIGHWAY	=====
FEDERAL ROUTES	=====
STATE ROUTES	=====
COUNTY & TOWNSHIP ROADS	=====
OTHER ROADS	—————

DESIGN DESIGNATION (DATA FROM TDMS, FOR INFORMATION ONLY)

CURRENT ADT (2022)	43,910
TRUCKS (24 HOUR B&C)	2,809
DESIGN SPEED	60 MPH
LEGAL SPEED	60 MPH
DESIGN FUNCTIONAL CLASSIFICATION:	
URBAN FREEWAYS AND EXPRESSWAYS	
NHS PROJECT	YES

DESIGN EXCEPTIONS

NONE

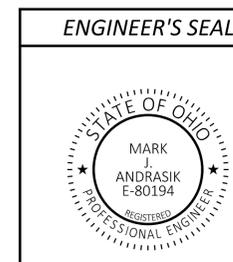
ADA DESIGN WAIVERS

UNDERGROUND UTILITIES
Contact Two Working Days Before You Dig

OHIO811.org
Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
ODOT DISTRICT 4 - CAPITAL PROGRAMS
2088 S. ARLINGTON ROAD
AKRON, OHIO 44306



fixed signature

BEGIN PROJECT
S.L.M. 7.37

END PROJECT
S.L.M. 12.12

STATE OF OHIO

DEPARTMENT OF TRANSPORTATION

MAH-680-7.37

CITY OF YOUNGSTOWN, BOARDMAN TOWNSHIP
MAHONING COUNTY

INDEX OF SHEETS:

TITLE SHEET	P. 1
TYPICAL SECTIONS	P. 2-3
GENERAL NOTES	P. 4-5D
MAINTENANCE OF TRAFFIC	P. 6-17
GENERAL SUMMARY	P. 18-19
SUBSUMMARIES & CALCULATIONS	P. 20-25
STRUCTURES	P. 26-35

FEDERAL PROJECT NUMBER

E200(173)

RAILROAD INVOLVEMENT

NONE

PROJECT DESCRIPTION

RESURFACING IR 680, SLM 7.31 TO SLM 12.00, IN MAHONING COUNTY, INCLUDES MINOR REHABILITATION BRIDGE WORK TO 9 STRUCTURES.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	4.68 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.25 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	NOI NOT REQUIRED

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2019 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS, CHANGES LISTED IN THE PROPOSAL, AND THE SUPPLEMENTAL SPECIFICATION 800 VERSION INDICATED ON THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY EXCEPT AS NOTED ON SHEET 9 , AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

Arthur G. Noiro Jr.
Arthur G. Noiro Jr., P.E.
District 04 Deputy Director

Jack Marchbanks
Jack Marchbanks, PhD
Director, Department of Transportation

STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
BP-2.1	1/21/22	MT-98.28	1/17/20	TC-52.10	10/18/13	800-2019 SEE PROPOSAL	ASBESTOS REPORT
BP-2.2	4/15/21	MT-98.29	1/17/20	TC-52.20	1/15/21	807 1/21/22	MAH-6880-0791E
BP-2.3	7/18/14	MT-99.20	4/19/19			808 1/18/19	
BP-3.1	1/21/22	MT-101.70	1/17/20	TC-65.10	1/17/14	821 4/20/12	ASBESTOS REPORT
BP-9.1	1/18/19	MT-101.75	1/17/20	TC-65.11	7/15/22	832 7/15/22	MAH-680-0794
DM-4.1	7/17/20	MT-101.90	7/17/20	TC-71.10	7/15/22	850 4/15/22	
DM-4.3	1/15/16	MT-102.10	1/17/20	TC-73.20	1/17/20	875 1/18/19	ASBESTOS REPORT
DM-4.4	1/15/16	MT-104.10	10/16/15			908 10/20/17	MAH-680-1073R
		MT-105.10	1/17/20	GSD-1-19	1/15/21	921 4/20/12	
MT-95.40	1/17/20						ASBESTOS REPORT
MT-95.41	1/17/20	TC-41.10	7/19/13				MAH-680-1073L
MT-95.50	7/21/17	TC-41.20	10/18/13				
MT-98.10	1/17/20	TC-41.30	10/18/13				
MT-98.11	1/17/20	TC-41.40	10/18/13				
MT-98.20	4/19/19	TC-42.10	10/18/13				
MT-98.22	1/17/20	TC-42.20	10/18/13				

DESIGN AGENCY



DESIGNER
MRS

REVIEWER
MJA 04-24-23

PROJECT ID
103883

SHEET TOTAL
P.1 P.35

UTILITIES

THE CONTRACTOR SHALL USE THE FOLLOWING PROCEDURE AT EACH LOCATION WHERE WORK IS PERFORMED, IN ACCORDANCE WITH SECTIONS 105.07 AND 107.16 IN THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER, OHIO811, THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 4 HEADQUARTERS (MICHELLE CHANEY AT 330-786-2267) AND ALL NON REGISTERED UTILITY OWNERS AT LEAST TWO (2) WORKING DAYS PRIOR TO COMMENCING CONSTRUCTION OPERATIONS IN ALL AREAS.

THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE NOT SHOWN ON THE PLANS, BUT CAN BE OBTAINED FROM THE OWNERS OF THE UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE TO UTILITIES.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

PAVEMENT MARKING LANE WIDTHS

THE NORMAL LANE WIDTH FOR THE PAVEMENT MARKINGS ON THIS PROJECT SHALL BE AS FOLLOWS [AT LEAST 3 DAYS PRIOR TO PERFORMING THE WORK CONTACT THE TRAFFIC OFFICE AT 330-786-3147 TO CONFIRM THE WIDTHS]:

ROUTE	S.L.M. TO S.L.M.	LANE WIDTH
IR 680	7.37 TO 12.12	12'

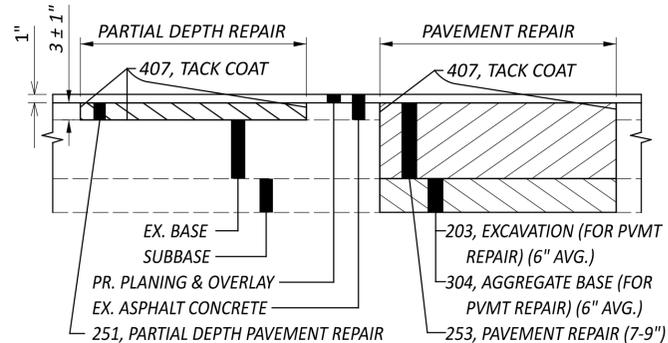
PAVEMENT MARKING DETAILS

THE PAVEMENT MARKING DETAIL SHEETS HAVE BEEN SUPPLIED AS REFERENCE DOCUMENTS FOR THIS PROJECT AND ARE AVAILBLE ON THE ODOT FTP SITE AT [**ITEM 253 - PAVEMENT REPAIR \(OUTSIDE SHOULDER REPAIR\)**](https://ftp.dot.state.oh.us/pub/contracts/Attach/ FOR THIS PROJECT. FOR ANY LOCATIONS THAT PAVEMENT MARKING DETAILS HAVE NOT BEEN MADE AVAILABLE TO THE CONTRACTOR, IT WILL BE THE CONTRACTORS RESPONSIBILITY TO PUT BACK NEW PAVEMENT MARKINGS IN THE ORIGINAL LOCATIONS.</p>
</div>
<div data-bbox=)

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL CONSIST OF CUTTING AND REMOVING DETERIORATED PAVEMENT FULL DEPTH AND PLACING 8" +/-1" 301 ASPHALT CONCRETE BASE, PG64-22. THE MAXIMUM COMPACTED DEPTH OF ANY ONE LAYER SHALL BE 6 INCHES. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING AND PRIOR TO THE PLACEMENT OF ASPHALT ON THE MILLED SURFACE.

IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REMOVED AND REPLACED TO THE LIMITS DESIGNATED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:
 253, PAVEMENT REPAIR (OUTSIDE SHOULDER REPAIR), 7000 SQ YD



ITEM 252 - RIGID REMOVAL FLEXIBLE REPLACEMENT (FLANGE BEAM JOINTS)

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL CONSIST OF CUTTING AND REMOVING DETERIORATED PAVEMENT FULL DEPTH AND PLACING 8-10" OF 301 ASPHALT CONCRETE BASE, PG64-22. THE MAXIMUM COMPACTED DEPTH OF ANY ONE LAYER SHALL BE 6 INCHES. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING AND PRIOR TO THE PLACEMENT OF ASPHALT ON THE MILLED SURFACE.

IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REMOVED AND REPLACED TO THE LIMITS DESIGNATED BY THE ENGINEER.

FLANGE BEAM JOINTS ARE PRESENT WITHIN 50' OF MAH-680-0794, MAH-680-0921, MAH-680-1073L, AND MAH-680-1073R.

THIS REPLACEMENT IS LIMITED TO THE LANES OF TRAVEL AND SCD BP-1.2 SHALL ONLY BE USED AS REFERENCE (SEE PAGES 5a-5d FOR SCD BP-1.2). SCD BP-1.2 SHOWS DETAILS FOR FLANGE BEAM SIZING, CONCRETE DIMENSIONING, AND CONCRETE REINFORCEMENT.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:
 252, RIGID REMOVAL FLEXIBLE REPLACEMENT (FLANGE BEAM JOINTS), 2000 SQ YD

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441) (LONGITUDINAL)

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THE ITEM SHALL CONSIST OF REPAIRING EXISTING LOCATIONS EXHIBITING SURFACE DETERIORATION AND PLACING ITEM 441 ASPHALT CONCRETE, TYPE 2. THE ASPHALT CONCRETE SHALL BE COMPACTED WITH A TYPE I PNEUMATIC TIRE ROLLER AND A STEEL WHEEL ROLLER AS PER 401.13. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. PAVEMENT REPAIRS WILL BE MARKED IN THE FIELD BY THE PROJECT ENGINEER ACCORDING TO CMS 251.02. MINIMUM WIDTH IS 2'. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING AND PRIOR TO THE PLACEMENT OF ASPHALT ON THE MILLED SURFACE. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REPAIR.

THE CONTRACTOR MAY PERFORM UP TO 75% OF THE PARTIAL DEPTH PAVEMENT REPAIRS PRIOR TO OCTOBER 30, 2023 BEFORE MAINLINE PAVEMENT PLANING OPERATIONS COMMENCE AS DIRECTED BY THE PROJECT ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:
 251, PARTIAL DEPTH PAVEMENT REPAIR (441) (LONGITUDINAL), 7200 SQ. YD.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441) (TRANSVERSE)

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THE ITEM SHALL CONSIST OF REPAIRING EXISTING LOCATIONS EXHIBITING SURFACE DETERIORATION AND PLACING ITEM 441 ASPHALT CONCRETE, TYPE 2. THE ASPHALT CONCRETE SHALL BE COMPACTED WITH A TYPE I PNEUMATIC TIRE ROLLER AND A STEEL WHEEL ROLLER AS PER 401.13. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. PAVEMENT REPAIRS WILL BE MARKED IN THE FIELD BY THE PROJECT ENGINEER ACCORDING TO CMS 251.02. MINIMUM WIDTH IS 2'. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING AND PRIOR TO THE PLACEMENT OF ASPHALT ON THE MILLED SURFACE. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REPAIR.

THE CONTRACTOR MAY PERFORM UP TO 75% OF THE PARTIAL DEPTH PAVEMENT REPAIRS PRIOR TO OCTOBER 30, 2023 BEFORE MAINLINE PAVEMENT PLANING OPERATIONS COMMENCE AS DIRECTED BY THE PROJECT ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:
 251, PARTIAL DEPTH PAVEMENT REPAIR (441) (TRANSVERSE), 800 SQ. YD.

ITEM 203 - EXCAVATION (FOR PAVEMENT REPAIR)

THIS ITEM OF WORK SHALL CONSIST OF REMOVING AND DISPOSING OF ALL UNSUITABLE MATERIAL BY EXCAVATING THE EXISTING SUBGRADE AND SUBBASE TO AN AVERAGE DEPTH OF 6 INCHES OR AS DIRECTED BY THE ENGINEER. EXACT LIMITS OF REMOVAL SHALL BE DETERMINED BY THE ENGINEER. ALL EQUIPMENT, LABOR, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203 EXCAVATION (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:
 203, EXCAVATION (FOR PAVEMENT REPAIR), 1500 CU YD

LINEAR GRADING

AREAS WHERE THE SHOULDER IS HIGHER THAN THE EDGE OF PAVEMENT WILL BE GRADED TO PROVIDE POSITIVE DRAINAGE. THIS WORK WILL ONLY BE PERFORMED IN AREAS NECESSARY AND WILL NOT BE PERFORMED ON THE ENTIRE PROJECT. AREAS FOR THE WORK WILL BE MARKED BY THE PROJECT ENGINEER. UNDER NO CIRCUMSTANCES WILL THIS WORK BE PERFORMED CONCURRENTLY WITH ANY OTHER OPERATION.

GRADING WILL BE ACCOMPLISHED BY THE REMOVAL OF MATERIAL TO PROVIDE A 0.08 POSITIVE SLOPE. THE GRADED AREAS WILL BE COMPACTED TO A SUFFICIENT DENSITY TO PREVENT EROSION UNTIL SEEDING AND MULCHING IS PERFORMED. ALL EXCESS MATERIAL WILL BE REMOVED FROM THE BERMS AND WILL BE DISPOSED OF OFF THE PROJECT BY THE CONTRACTOR.

SEEDING AND MUCHING, FERTILIZER AND LIME WILL BE PERFORMED WITHIN A PERIOD NOT TO EXCEED 10 DAYS AFTER THE LINEAR GRADING.

THE QUANTITY OF ITEM 209 IS NOT PERMITTED TO BE INCREASED. REDUCTIONS IN QUANTITIES ARE PERMITTED AS DETERMINED BY THE PROJECT ENGINEER.

ALL MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THIS WORK WILL BE INCLUDED IN THE UNIT PRICE FOR THE PERTINENT BID ITEM. THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

- 209, LINEAR GRADING, 810 STA.
- 659, SEEDING AND MULCHING, 22500 SQ YD
- 659, COMMERCIAL FERTILIZER, 3.04 TON
- 659, LIME, 4.68 ACRES
- 659, WATER, 121.5 M. GAL.

ITEM 304 - AGGREGATE BASE (FOR PAVEMENT REPAIR)

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN PROVIDED AND SHALL BE USED AS DIRECTED BY THE ENGINEER TO BACKFILL AREAS WHICH WERE EXCAVATED UNDER ITEM 203 EXCAVATION (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:
 304, AGGREGATE BASE (FOR PAVEMENT REPAIR), 1500 CU YD

DESIGN AGENCY



DESIGNER
MRS

REVIEWER
MJA 04-24-23

PROJECT ID
103883

SHEET TOTAL
P.4 P.35

MAINTENANCE OF TRAFFIC

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON EXISTING ROADWAYS AND RAMPS IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

1. A MINIMUM OF ONE TEN FOOT LANE IN EACH DIRECTION SHALL BE MAINTAINED ON THE EXISTING PAVEMENT OR COMPLETED PAVEMENT DURING CONSTRUCTION OF THE WORK.
2. THE CONTRACTOR SHALL INFORM THE DISTRICT OFFICE (330) 786-2208, EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.
3. LANE RESTRICTIONS OR LANE REDUCTIONS SHALL NOT BE PERMITTED AFTER NORMAL WORKING HOURS. NORMAL WORKING HOURS SHALL BE THOSE HOURS DURING WHICH THE CONTRACTOR HAS A FULL COMPLEMENT OF EMPLOYEES AND EQUIPMENT ACTIVELY REMOVING AND/OR PLACING PAVEMENT MATERIALS.
4. ALL FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT OPERATIONS SHALL BE COMPLETED THE SAME DAY THE EXCAVATION IS MADE. IF THE CONTRACTOR CANNOT COMPLETE THE WORK, THE EXCAVATION SHALL BE BACKFILLED OR PROTECTED AS PER STANDARD CONSTRUCTION DRAWING MT-101.90.
5. TRUCK MOUNTED ATTENUATORS [TMA'S] SHALL BE USED AS SHOWN IN THE STANDARD CONSTRUCTION DRAWINGS.
6. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE SUCCESSIVE WORK ZONES UNLESS THE DISTANCE BETWEEN THE DRUMS, BARRICADES OR CONES EXCEEDS TWO (2) MILES RURAL OR ONE [1] MILE URBAN.
7. FOR ROUTES NOT ON THE PERMITTED LANE CLOSURE CHART, ONLY DURING OFF-PEAK PERIODS (ie ANY PERIOD OTHER THAN 6-8AM AND 3-6PM) SHALL THE CONTRACTOR INSTALL AND SUBSEQUENTLY RESET ALL TRAFFIC CONTROL NECESSARY FOR THE WORK ZONE FOR EACH CONSTRUCTION PHASE.
8. IN ADDITION TO THE REQUIREMENTS OF 614.11 WORK ZONE PAVEMENT MARKINGS, AT THE END OF EACH DAY OF WORK, THE CONTRACTOR SHALL REPLACE (WITH WORK ZONE MARKINGS) ALL LANE, CENTER, STOP OR CHANNELIZING LINES THAT WERE REMOVED OR COVERED DURING THE PAVEMENT REMOVAL OR PLACEMENT OPERATIONS. QUANTITIES FOR SUCH PLACEMENT ARE CARRIED AS PART OF THE ITEMS LISTED UNDER 614 WORK ZONE PAVEMENT MARKINGS.
9. A QUANTITY OF 20 CU. YDS. OF ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC SHALL BE PROVIDED FOR USE IN MAINTAINING PAVEMENT, SHOULDERS AND OTHER LOCATIONS AS DIRECTED BY THE ENGINEER.

THE FOLLOWING QUANTITIES SHALL BE USED FOR THE MAINTENANCE OF TRAFFIC ON THIS PROJECT:

614, WORK ZONE LANE LINE, CLASS I, 20.76 MILE
614, WORK ZONE STOP LINE, CLASS I, 183 FT
614, WORK ZONE CHANNELIZING LINE, CLASS I, 23900 FT
614, WORK ZONE MARKING SIGN,(ALL PHASES) 16 EACH

614, WORK ZONE LANE LINE, CLASS III, 642 PAINT 20.76 MILE
614, WORK ZONE STOP LINE, CLASS III, 642 PAINT 183 FT
614, WORK ZONE CHANNELIZING LINE, CLASS III, 642 PAINT 14460 FT

TO BE USED AS DIRECTED BY THE ENGINEER
614, WORK ZONE EDGE LINE, CLASS I, 31.28 MILE
614, WORK ZONE EDGE LINE, CLASS III, 26.00 MILE

614, WORK ZONE IMPACT ATTENUATOR, 4 EACH
622, PORTABLE BARRIER, UNANCHORED, 2240 FT

ITEM 614, MAINTAINING TRAFFIC (CLOSING PARAGRAPH FOR NOTE)

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ADVANCED NOTICE TO PAVE

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE DISTRICT CONSTRUCTION ENGINEER A DETAILED SCHEDULE 15 DAYS PRIOR TO THE PLACEMENT OF THE OVERLAY COURSES, ON HOW THEY PROPOSE TO PROSECUTE THE PAVING OPERATIONS. THE DETAILS SHALL SHOW THE ORDER OF PERFORMANCE OF EACH STAGE (START TO FINISH) OF THE WORK INCLUDING THE MAINTENANCE OF TRAFFIC THAT WILL BE USED.

TIME LIMITATION, TRAFFIC ON A MILLED SURFACE

THE MAXIMUM ALLOWABLE TIME FOR TRAFFIC TO BE PLACED ON A MILLED SURFACE SHALL BE 7 CONSECUTIVE CALENDAR DAYS. SHOULD THE CONTRACTOR FAIL TO MEET THIS REQUIREMENT, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$1000 PER DAY THAT THE TRAFFIC IS PLACED ON A MILLED SURFACE BEYOND THE SPECIFIED LIMIT.

PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN

PRIOR TO RESURFACING OPERATIONS, THE CONTRACTOR SHALL REINFORCE THE ROADWAY SHOULDERS USING FLEXIBLE PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN AS INDICATED IN THE MAINTENANCE OF TRAFFIC SCHEMATIC DETAIL. THE INTENTIONS ARE THAT THE REINFORCED ROADWAY SHOULDER WILL BE USED TO MAINTAIN SHIFTED TRAFFIC DURING BRIDGE JOINT REPAIR. THE FLEXIBLE PAVEMENT FOR MAINTAINING TRAFFIC SHALL REMAIN IN PLACE AFTER COMPLETION OF THE BRIDGE JOINT REPAIRS. THE SUBSEQUENT RESURFACING OPERATIONS SHALL MILL THE REINFORCED SHOULDER AS PART OF THE FINAL PAVING OPERATION. THE COST TO SAWCUT, EXCAVATE EXISTING PAVEMENT, AND ALL OTHER COSTS NECESSARY FOR THE PLACEMENT OF PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN SHALL BE CONSIDERED INCIDENTAL TO AND INCLUDED IN THE UNIT PRICE BID.

added THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

615, PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN 5500 SY

LANE CLOSURES (MAH-680)

DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AS PER THE PERMITTED LANE CLOSURE CHART. THE PERMITTED LANE CLOSURE CHART USED FOR THIS PROJECT SHALL BE THE MOST CURRENT CHART AVAILABLE ON THE DATE THIS PROJECT SELLS.

THE CHART CAN BE FOUND AT:
<http://plcm.dot.state.oh.us>

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THE REQUIREMENTS IN THE CHART, THE CONTRACTOR SHALL BE ASSESSED DISINCENTIVES IN THE AMOUNT OF \$1000 PER HOUR OR PORTION THEREOF THAT THE LANE REDUCTION REMAINS BEYOND THE SPECIFIED LIMIT.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
ROAD & RAMP CLOSURE	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS & > 12 HOURS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERNS CHANGE	N/A	14 CALENDAR DAYS PRIOR TO CLOSURE

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS:

NEW YEAR'S (OBSERVED), GENERAL/REGULAR ELECTION DAY ((NOV) TOTAL SOLAR ECLIPSE (4/8/24), THANKSGIVING, MEMORIAL DAY CHRISTMAS (OBSERVED), FOURTH OF JULY (OBSERVED), LABOR DAY (OTHER HOLIDAY OR SPECIAL EVENT)

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR SPECIAL EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MODAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
	(TOTAL SOLAR ECLIPSE) 12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
	(GEN./REG. ELECTION) 5:00 AM TUESDAY THROUGH 12:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
FRIDAY	(THANKSGIVING ONLY) 6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

DURING THE SAME PERIODS, MAINTAIN PEDESTRAIN ACCESS IF PEDESTRIAN ACCESS WAS PRESENT PRIOR TO CONSTRUCTION.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

LANE VALUE CONTRACT			
DESCRIPTION OF CRITICAL LANE/RAMP TO BE MAINTAINED	RESTRICTED TIME PERIOD	TIME UNIT	DISINCENTIVE PER TIME UNIT
IR 680	AS PER PLCS	PER LANE/PER MINUTE	\$115.00

TRAFFIC CONTROL INSPECTOR

THE CONTRACTOR SHALL DESIGNATE AN INDIVIDUAL OTHER THAN THE SUPERINTENDENT AND SUBJECT TO THE APPROVAL OF THE ENGINEER, TO CONTINUOUSLY INSPECT ALL TRAFFIC CONTROL DEVICES WHENEVER CONSTRUCTION WORK IS BEING PERFORMED WITHIN THE WORK LIMITS OF THE PROJECT. THE DESIGNATED INDIVIDUAL SHALL ALSO INSPECT ALL TRAFFIC DEVICES AT THE BEGINNING AND AT THE END OF EACH WORK DAY. THE DESIGNATED INDIVIDUAL OR A QUALIFIED REPRESENTATIVE SHALL ALSO BE AVAILABLE ON AN AROUND THE CLOCK BASIS TO REPAIR AND/OR REPLACE DAMAGED OR MISSING TRAFFIC CONTROL DEVICES. THESE INDIVIDUALS SHALL BE EQUIPPED WITH CELLULAR PHONES AND THEIR NAMES AND PHONE NUMBERS SHALL BE GIVEN TO THE PROJECT ENGINEER AT THE PRE-CONSTRUCTION MEETING. THE DESIGNATED INDIVIDUAL MAY HAVE OTHER CONSTRUCTION RELATED DUTIES AS LONG AS IMMEDIATE ATTENTION IS GIVEN TO TRAFFIC CONTROL. PAYMENT FOR THE SERVICES OF THE TRAFFIC CONTROL INSPECTOR SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.



DESIGN SPECIFICATIONS

THIS STRUCTURE CONFORMS TO THE "LRFD BRIDGE DESIGN SPECIFICATIONS" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 17TH EDITION, INCLUDING THE 2012 INTERIM SPECIFICATIONS AND THE ODOT BRIDGE DESIGN MANUAL, 2019.

EXISTING STRUCTURE VERIFICATION

EXISTING STRUCTURE VERIFICATION: DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05, 105.02 AND 513.04.

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS WHICH HAVE BEEN VERIFIED IN THE FIELD.

PROPOSED WORK:

MAH-680-0791E (RAMP E OVER DEWEY AVENUE)

- PATCH ALL UNSOUND AREAS OF THE EXISTING CONCRETE WEARING SURFACE, INCLUDING THE APPROACH SLABS
- REMOVAL OF FORWARD AND REAR SLIDING PLATE EXPANSION JOINTS AND REPLACE WITH ADHESIVE JOINT SEAL SYSTEM
- PATCH ALL UNSOUND AREAS OF CONCRETE ABUTMENTS BACKWALLS, AND DECK UNDERSIDE. SEAL WITH EPOXY-URETHANE. USE FIBER WRAP IN REPAIRS OVER TRAFFIC
- REMOVE EXISTING SEALANT FROM ABUTMENT AND BACKWALL SURFACES AND RESEAL WITH EPOXY-URETHANE SEALER
- RESET AND REFURBISH THE EXISTING ABUTMENT BEARINGS
- CLEAN OUT EXISTING SCUPPERS
- CLEARING AND GRUBBING 15' AROUND STRUCTURE TO REMOVE ALL VEGETATION
- PROVIDE NEW CORRECT STRUCTURE IDENTIFICATION SIGNS

MAH-680-0794 (OVER DEWEY AVENUE CR-533P)

- PATCH ALL UNSOUND AREAS OF THE EXISTING CONCRETE WEARING SURFACE, INCLUDING THE APPROACH SLABS
- REMOVAL OF FORWARD AND REAR SLIDING PLATE EXPANSION JOINTS AND REPLACE WITH ADHESIVE JOINT SEAL SYSTEM
- PATCH ALL UNSOUND AREAS OF CONCRETE ABUTMENTS BACKWALLS, AND DECK UNDERSIDE. SEAL WITH EPOXY-URETHANE. USE FIBER WRAP IN REPAIRS OVER TRAFFIC
- REMOVE EXISTING SEALANT FROM ABUTMENT AND BACKWALL SURFACES AND RESEAL WITH EPOXY-URETHANE SEALER
- RESET AND REFURBISH THE EXISTING ABUTMENT BEARINGS
- CLEAN OUT EXISTING SCUPPERS
- CLEARING AND GRUBBING 15' AROUND STRUCTURE TO REMOVE ALL VEGETATION
- PROVIDE NEW CORRECT STRUCTURE IDENTIFICATION SIGNS

MAH-680-0817 (UNDER INDIANOLA AVENUE CR-514)

- PATCH ALL UNSOUND AREAS OF CONCRETE PIERS AND ABUTMENTS. SEAL WITH EPOXY-URETHANE
- ASPHALT PAVING TO TRANSITION THE APPROACH PAVEMENT AND APPROACH SLAB ON THE SOUTH END OF THE BRIDGE
- REPLACEMENT OF PRESSURE RELIEF JOINT ASPHALT
- CLEARING AND GRUBBING 15' AROUND STRUCTURE TO REMOVE ALL VEGETATION
- PROVIDE NEW CORRECT STRUCTURE IDENTIFICATION SIGNS

MAH-680-0837 (UNDER SHIRLEY ROAD CR-529)

- PATCH ALL UNSOUND AREAS OF CONCRETE PIERS AND ABUTMENTS. SEAL WITH EPOXY-URETHANE
- REPAIR WASHOUT AND EROSION AROUND PIER 1
- CLEARING AND GRUBBING 15' AROUND STRUCTURE TO REMOVE ALL VEGETATION
- PROVIDE NEW CORRECT STRUCTURE IDENTIFICATION SIGNS

MAH-680-0921 (OVER SR-170 MIDLOTHIAN BLVD)

- PATCH ALL UNSOUND AREAS OF CONCRETE PIERS AND ABUTMENTS. SEAL WITH EPOXY-URETHANE
- REMOVE EXISTING SEALANT FROM PARAPET FACE SURFACES AND RESEAL WITH EPOXY-URETHANE SEALER
- RESET AND REFURBISH THE EXISTING BEARINGS
- CLEAN OUT EXISTING SCUPPERS
- CLEARING AND GRUBBING 15' AROUND STRUCTURE TO REMOVE ALL VEGETATION
- PROVIDE NEW CORRECT STRUCTURE IDENTIFICATION SIGNS

MAH-680-0990 (UNDER THALIA AVENUE TR-1682)

- PATCH ALL UNSOUND AREAS OF CONCRETE PIERS. SEAL WITH EPOXY-URETHANE
- CLEARING AND GRUBBING 15' AROUND STRUCTURE TO REMOVE ALL VEGETATION
- PROVIDE NEW CORRECT STRUCTURE IDENTIFICATION SIGNS

MAH-680-1073L (OVER MATHEWS ROAD CR-102)

- PATCH ALL UNSOUND AREAS OF THE EXISTING CONCRETE WEARING SURFACE, INCLUDING THE APPROACH SLABS
- REMOVAL OF FORWARD AND REAR SLIDING PLATE EXPANSION JOINTS AND REPLACE WITH ADHESIVE JOINT SEAL SYSTEM
- PATCH ALL UNSOUND AREAS OF CONCRETE ABUTMENTS BACKWALLS, DECK EDGES, AND DECK UNDERSIDE. SEAL WITH EPOXY-URETHANE. USE FIBER WRAP IN REPAIRS OVER TRAFFIC
- REMOVE EXISTING SEALANT FROM ABUTMENT AND BACKWALL SURFACES AND RESEAL WITH EPOXY-URETHANE SEALER
- RESET AND REFURBISH THE EXISTING ABUTMENT BEARINGS
- CLEAN OUT EXISTING SCUPPERS
- CLEARING AND GRUBBING 15' AROUND STRUCTURE TO REMOVE ALL VEGETATION
- PROVIDE NEW CORRECT STRUCTURE IDENTIFICATION SIGNS

MAH-680-1073R (OVER MATHEWS ROAD CR-102)

- PATCH ALL UNSOUND AREAS OF THE EXISTING CONCRETE WEARING SURFACE, INCLUDING THE APPROACH SLABS
- REMOVAL OF FORWARD AND REAR SLIDING PLATE EXPANSION JOINTS AND REPLACE WITH ADHESIVE JOINT SEAL SYSTEM
- PATCH ALL UNSOUND AREAS OF CONCRETE ABUTMENTS BACKWALLS, AND DECK UNDERSIDE. SEAL WITH EPOXY-URETHANE. USE FIBER WRAP IN REPAIRS OVER TRAFFIC
- REMOVE EXISTING SEALANT FROM ABUTMENT, BACKWALL, AND INSIDE & TOP OF PARAPETS SURFACES. RESEAL WITH EPOXY-URETHANE SEALER
- RESET AND REFURBISH THE EXISTING ABUTMENT BEARINGS
- CLEAN OUT EXISTING SCUPPERS
- CLEARING AND GRUBBING 15' AROUND STRUCTURE TO REMOVE ALL VEGETATION
- PROVIDE NEW CORRECT STRUCTURE IDENTIFICATION SIGNS

MAH-224-1964 (OVER MAH-680-11.80)

- PATCH ALL UNSOUND AREAS OF CONCRETE PIERS AND ABUTMENTS. SEAL WITH EPOXY-URETHANE
- REMOVE EXISTING SEALANT FROM INSIDE & TOP OF PARAPETS SURFACES. RESEAL WITH EPOXY-URETHANE SEALER
- CLEARING AND GRUBBING 15' AROUND STRUCTURE TO REMOVE ALL VEGETATION
- PROVIDE NEW CORRECT STRUCTURE IDENTIFICATION SIGNS

ITEM 201 - CLEARING AND GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS

ALTHOUGH NO TREES OR STUMPS ARE SPECIFICALLY MARKED FOR REMOVAL WITHIN THE PLANS, A LUMP SUM QUANTITY IS INCLUDED IN THE STRUCTURE GENERAL SUMMARY FOR ITEM 201 – CLEARING AND GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS. SCALPING IS NOT REQUIRED FOR THIS ITEM OF WORK. ALL VEGETATION, TREE STUMPS AND TREE REMOVAL DEBRIS, AND STANDING/ DOWNED TREES SHALL BE REMOVED WITHIN 15 FEET (OR TO THE R/W LIMITS, WHICHEVER IS CLOSER) OF THE HEADWALLS, ABUTMENTS AND/OR PIERS.

ALL OTHER PROVISIONS AS SET FORTH IN THE CMS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 201 – CLEARING AND GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS.

ITEM 516 - JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN

THIS WORK CONSISTS OF RAISING OR RE-POSITIONING EXISTING STRUCTURES TO THE DIMENSIONS AND REQUIREMENTS DEFINED IN THE PROJECT PLANS. SUBMIT CONSTRUCTION PLANS IN ACCORDANCE WITH C&MS 501.05. IF, DURING THE JACKING OPERATIONS, CRACKING OF THE CONCRETE SUPERSTRUCTURE, SEPARATION OF THE CONCRETE DECK FROM THE STEEL STRINGERS, OR OTHER DAMAGE TO THE STRUCTURE IS VISUALLY OBSERVED, IMMEDIATELY CEASE THE JACKING OPERATION AND INSTALL SUPPORTS TO THE SATISFACTION OF THE ENGINEER. ANALYZE THE DAMAGE AND SUBMIT A METHOD OF CORRECTION TO THE ENGINEER FOR APPROVAL. EPOXY INJECT ALL BEAMS THAT SEPARATE FROM THE DECK FOR A DISTANCE OF THE SEPARATION IN ACCORDANCE WITH C&MS 512.07. THE DEPARTMENT WILL NOT PAY FOR THE COST OF THIS EPOXY INJECTION OR OTHER REQUIRED REPAIRS. THE BRIDGE BEARINGS SHALL BE FULLY SEATED ALL CONTACT AREAS. IF FULL SEATING IS NOT ATTAINED, SUBMIT A REPAIR PLAN TO THE ENGINEER. THE DEPARTMENT WILL NOT PAY FOR THE REPAIR COSTS TO ENSURE FULL SEATING ON BEARINGS. THE DEPARTMENT WILL MEASURE THIS WORK ON A LUMP SUM BASIS. THE DEPARTMENT WILL PAY FOR THE ACCEPTED QUANTITIES AT THE CONTRACT PRICE FOR ITEM 516, JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN.

ITEM 516 - REFURBISHING BEARING DEVICES, AS PER PLAN

THIS ITEM SHALL INCLUDE ALL WORK NECESSARY TO PROPERLY ALIGN BRIDGE BEARINGS, AS WELL AS THEIR CLEARING AND PAINTING. INCLUDED SHALL BE THE DISASSEMBLY OF THE BEARINGS, HAND TOOL CLEANING (GRINDING IF NECESSARY), PAINTING ACCORDING TO ITEM 514, REPLACEMENT OF ANY DAMAGED SHEET LEAD WITH PREFORMED BEARING PADS (C&MS 711.21), INSTALLATION OF ANY NECESSARY STEEL SHIMS OF THE SAME SIZE AS THE BEARINGS TO PROVIDE A SNUG FIT, REALIGNMENT OF THE UPPER BEARING PLATE BY REMOVING EXISTING WELDS AND REWELDING SO THAT THE BEARINGS ARE VERTICALLY ALIGNED AT 60 DEGREES FARENHEIT, LUBRICATING SLIDING SURFACES, AND REASSEMBLY OF THE BEARINGS. ASSURE ALL BEARINGS ARE SHIMMED ADEQUATELY AND THAT NO BEAMS AND/OR BEARING DEVICES ARE "FLOATING". AT NO ADDITIONAL COST TO THE STATE, THE CONTRACTOR MAY INSTALL NEW BEARINGS OF THE SAME TYPE AS THE EXISTING IN PLACE OF REFURBISHING THE BEARINGS. ALL WORK SHALL BE TO THE SATISFACTION OF THE ENGINEER. PAYMENT FOR ALL OF THE ABOVE DESCRIBED LABOR AND MATERIALS WILL BE MADE AT THE CONTRACT PRICE BID FOR ITEM 516 - REFURBISH BEARING DEVICES, AS PER PLAN.

ITEM 519 - PATCHING CONCRETE STRUCTURES, AS PER PLAN

PRIOR TO THE SURFACE CLEANING SPECIFIED IN 519.04 AND WITHIN 24 HOURS OF PLACING PATCHING MATERIAL, BLAST CLEAN ALL SURFACES TO BE PATCHED INCLUDING THE EXPOSED REINFORCING STEEL. ACCEPTABLE METHODS INCLUDE HIGH-PRESSURE WATER BLASTING WITH OR WITHOUT ABRASIVES IN THE WATER, ABRASIVE BLASTING WITH CONTAINMENT, OR VACUUM ABRASIVE BLASTING.

SPECIAL - STRUCTURES: CONCRETE SPALL REMOVAL

THIS WORK WILL CONSIST OF REMOVING ALL VISIBLY SPALLED AREAS OF THE BOTTOM DECK FLOOR OF STRUCTURE(S) MAH-680-0791E, MAH-680-0794, MAH-680-1073L, AND MAH-680-1073R WITHOUT SOUNDING. AFTER SPALLED CONCRETE AREAS HAVE BEEN REMOVED, REMOVAL AREAS WILL BE SEALED WITH ITEM SPECIAL - SEALING OF CONCRETE SURFACES

CONCRETE SPALL REMOVAL WILL BE PAID FOR AT THE UNIT BID PRICE FOR SPECIAL – STRUCTURE MISC.: CONCRETE SPALL REMOVAL. THIS PRICE WILL INCLUDE THE COST OF LABOR, EQUIPMENT, AND ALL INCIDENTALS REQUIRED TO COMPLETE THIS WORK.

ALL SPALLED AREAS OF THE BOTTOM DECK FLOOR OVER TRAFFIC SHALL ALSO BE PATCHED USING ITEM 843 - PATCHING CONCRETE STRUCTURES WITH TROWELABLE MORTAR AND ITEM 519 - SPECIAL - COMPOSITE FIBER WRAP SYSTEM AS DIRECTED BY THE PROJECT ENGINEER.

THE QUANTITIES PER STRUCTURE ARE AS FOLLOWS:
SPEC. STRUCTURES: CONCRETE SPALL REMOVAL, 25 SQ YD
ITEM SPECIAL - SEALING, SEALING OF CONCRETE SURFACES, 25 SQ YD
ITEM SPECIAL - COMPOSITE FIBER WRAP SYSTEM, 20 SF
843, PATCHING CONCRETE STRUCTURES WITH TROWELABLE MORTAR, 20 SF

ITEM 202, PORTIONS OF STRUCTURE REMOVED, AS PER PLAN:

THIS ITEM SHALL INCLUDE THE ELEMENTS INDICATED IN THE PLANS AND GENERAL NOTES AND THAT ARE NOT SEPARATELY LISTED FOR PAYMENT, EXCEPT FOR WEARING COURSE REMOVAL. ITEMS TO BE REMOVED INCLUDE ALL EXISTING MATERIALS BEING REPLACED BY NEW CONSTRUCTION AND MISCELLANEOUS ITEMS THAT ARE NOT SHOWN TO BE INCORPORATED INTO THE FINAL CONSTRUCTION AND ARE DIRECTED TO BE REMOVED BY THE ENGINEER. THE USE OF EXPLOSIVES, HEADACHE BALLS AND/OR HOE-RAMS WILL NOT BE PERMITTED. THE METHOD OF REMOVAL AND THE WEIGHT OF HAMMER SHALL BE APPROVED BY THE ENGINEER. PERFORM ALL WORK IN A MANNER THAT WILL NOT CUT, ELONGATE OR DAMAGE THE EXISTING REINFORCING STEEL TO BE PRESERVED. CHIPPING HAMMERS SHALL NOT BE HEAVIER THAN THE NOMINAL 90-POUND CLASS. PNEUMATIC HAMMERS SHALL NOT BE PLACED IN DIRECT CONTACT WITH REINFORCING STEEL THAT IS TO BE RETAINED IN THE REBUILT STRUCTURE. SUBMIT CONSTRUCTION PLANS ACCORDING TO CMS 501.05.

EROSION REPAIR

THIS WORK WILL CONSIST OF REPAIRING THE EROSION AT THE APPROXIMATE LOCATIONS DETAILED BELOW AND AT THE DIRECTION OF THE ENGINEER. REPAIR WORK WILL BE PAID FOR BY THE FOLLOWING ITEMS.

- MAH-680-0837: REPAIR EROSION AROUND PIERS
- ITEM 203, BORROW, 10 CY
- ITEM 601, DUMPED ROCK FILL, TYPE C, 5 CY
- ITEM 613, LOW STRENGTH MORTAR BACKFILL, 5 CY

WEARING COURSE REMOVED, AS PER PLAN (MAH-680-0817)

THE CONTRACTOR SHALL REMOVE THE TOP 3" OF ASPHALT IN THE PRESSURE RELIEF JOINT AT MAH-680-0817 AS DIRECTED BY THE PROJECT ENGINEER.

added

SFN	0
DESIGN AGENCY	
DESIGNER	CHECKER
CMR	MJA
REVIEWER	
MJA 04-24-23	
PROJECT ID	103883
SUBSET	TOTAL
P.1	P.10
SHEET	TOTAL
P.26	P.35

SCREEDING:

THE PATCHING MATERIAL WILL BE PLACED, CONSOLIDATED, AND FINISHED TO THE ADJACENT GRADE. PATCHES EXCEEDING 50 SQ FT (4.6 SQ M) WILL BE LEVELED AND CONSOLIDATED WITH A MECHANICAL VIBRATING SCREED. SMALLER PATCHES WILL BE HAND VIBRATED AND LEVELED WITH A STRAIGHTEDGE. THE SCREED WILL BE PLACED PARALLEL TO THE BRIDGE CENTERLINE SO THAT THE DECK PROFILE REMAINS CONSISTENT WITH THE WORN SURFACE.

DO NOT ADD WATER TO AID THE FINISHING AND AN EVAPORATION RETARDANT MAY NOT BE USED.

AFTER THE PATCHES HAVE BEEN CONSOLIDATED AND FINISHED THEY WILL BE TEXTURED IN ACCORDANCE WITH 451.09. THE CONTRACTOR WILL TEST THE SURFACE OF THE PLASTIC CONCRETE FOR TRUENESS AND FOR BEING FLUSH WITH THE EDGES OF THE ADJACENT SURFACES BY USE OF A STRAIGHTEDGE. THE STRAIGHTEDGE WILL BE DONE BY PLACING THE STRAIGHTEDGE PARALLEL TO THE BRIDGE CENTERLINE WITH THE ENDS RESTING ON THE EXISTING WEARING SURFACE ADJACENT TO THE PATCH AND DRAWING THE STRAIGHTEDGE ACROSS THE PATCH. ANY HIGH OR LOW AREAS EXCEEDING 1#8 INCH IN 10 FEET (3 MM IN 3 M) WILL BE CORRECTED. IF ANY CORRECTIONS ARE MADE, THE SURFACE WILL BE RECHECKED.

CURING:

COVER THE FINISHED PATCHED SURFACES WITH A SINGLE LAYER OF CLEAN WET BURLAP AND COVER THE BURLAP WITH A 4-MIL WHITE OPAQUE POLYETHYLENE FILM FOR A MINIMUM OF 4 HOURS FOLLOWED BY A MEMBRANE CURE PER 511.17 METHOD (B).

ADEQUATE PRECAUTIONS WILL BE TAKEN TO PROTECT THE FRESHLY PLACED VES-LMC FROM RAIN.

THE CONTRACTOR WILL SUPPLY A PROPERLY CALIBRATED IMPACT REBOUND HAMMER TO VERIFY THAT THE PATCHES HAVE REACHED 3000 PSI COMPRESSIVE STRENGTH PRIOR TO OPENING TO TRAFFIC.

INSPECTION AND SOUNDING OF CONCRETE PATCHES:

AFTER CURING AND BEFORE FINAL ACCEPTANCE, ALL PATCHED AREAS WILL BE SOUNDED. ALL DELAMINATED AREAS WILL BE REMOVED AND REPATCHED ACCORDING TO THIS NOTE. ALL PATCHES WHICH ARE SOUND BUT SHOW SIGNS OF CRACKING WILL BE SEALED AND THE PERIMETER OF ALL PATCHES WILL ALSO BE SEALED WITH GRAVITY FED RESIN.

ALL SOUNDING AND REPLACEMENT OF REJECTED AREAS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR AND INCLUDED IN THE UNIT BID PRICE FOR THIS ITEM.

METHOD OF MEASUREMENT:

PAYMENT WILL BE MADE AT THE CONTRACTOR PRICE PER CUBIC YARD FOR ITEM SPECIAL - PATCHING CONCRETE STRUCTURES, MISC.: VES-LMC (VERY EARLY STRENGTH LATEX MODIFIED CONCRETE) WHICH WILL INCLUDE ALL MATERIALS AND LABOR REQUIRED TO PERFORM THIS WORK INCLUDING REMOVAL AND DISPOSAL OF THE EXISTING MATERIAL.

ITEM SPECIAL - PATCHING CONCRETE STRUCTURES, MISC.: TRIAL BATCH FOR VES-LMC (VERY EARLY STRENGTH LATEX MODIFIED CONCRETE)

MAKE ONE OR MORE, ON CUBIC YARD, TRIAL BATCHES OF THE VES-LMC MATERIAL AT LEAST 14 DAYS PRIOR TO THE MATERIAL BEING PLACED. DEMONSTRATE THE ABILITY TO ACHIEVE THE REQUIREMENTS OF THE MATERIAL AS PER THE PLAN NOTE.

PAYMENT WILL BE MADE AT THE LUMP SUM CONTRACT PRICE FOR ITEM SPECIAL - PATCHING CONCRETE STRUCTURES, MISC.: TRIAL BATCH FOR VES-LMC (VERY EARLY STRENGTH LATEX MODIFIED CONCRETE) WHICH WILL INCLUDE ALL MATERIALS AND LABOR REQUIRED TO PERFORM THIS WORK.

ITEM 509 REINFORCING STEEL, REPLACEMENT OF EXISTING REINFORCING STEEL, AS PER PLAN

REPLACE ALL EXISTING REINFORCING BARS DEEMED BY THE ENGINEER TO BE UNUSABLE BECAUSE OF CORROSION. THE DEPARTMENT WILL MEASURE THE REPLACEMENT REINFORCING STEEL BY THE NUMBER OF POUNDS ACCEPTED IN PLACE.

REPLACE ALL EXISTING REINFORCING STEEL BARS WHICH ARE TO BE INCORPORATED INTO THE NEW WORK AND ARE DEEMED BY THE ENGINEER TO BE MADE UNUSABLE BY CONCRETE REMOVAL OPERATIONS WITH NEW EPOXY COATED REINFORCING STEEL OF THE SAME SIZE AT NO COST TO THE DEPARTMENT.

ITEM 509 - EPOXY COATED REINFORCING STEEL, AS PER PLAN

ITEM 509 - EPOXY COATED REINFORCING STEEL, AS PER PLAN: IN ADDITION TO THE PROVISIONS OF ITEM 509, FIELD BEND AND/OR FIELD CUT THE REINFORCING STEEL DESIGNATED IN THE PLANS, AS NECESSARY, IN ORDER TO MAINTAIN THE REQUIRED CLEARANCES AND BAR SPACINGS. REPAIR ALL DAMAGE TO THE EPOXY COATING, AS A RESULT OF THIS WORK, ACCORDING TO 709.00.

SPECIAL - COMPOSITE FIBER WRAP SYSTEM

FIBER WRAP SYSTEM SHALL BE USED ON PATCHING OF SPALLED AREAS OF THE BOTTOM DECK FLOOR AND DECK EDGES LOCATED OVER VEHICULAR, RAIL OR PEDESTRIAN TRAFFIC. USE OF FIBER WRAP SHALL BE AS DIRECTED BY THE PROJECT ENGINEER. FOR DETAILS SEE PROPOSAL NOTE 519 - COMPOSITE FIBER WRAP SYSTEM.

**ITEM 611 - MANHOLE ADJUSTED TO GRADE, AS PER PLAN
ITEM 638 - VALVE BOX ADJUSTED TO GRADE, AS PER PLAN**

IN ADDITION TO THE REQUIREMENTS OF CMS 611.10.D FOR MANHOLES, OR 638.18 FOR VALVE BOXES, THE CONTRACTOR WILL MAKE A CLEAN CIRCULAR CUT AROUND THE CASTING (48" DIAMETER FOR STORM AND SANITARY MANHOLE CASTINGS, 24"-28" FOR VALVE BOXES AND MONUMENT ASSEMBLIES, AND 2' IN DIAMETER LARGER THAN THE CASTING DIAMETER FOR ANY CASTINGS THAT ARE LARGER THAN STANDARD MANHOLES) AND REMOVE AND DISCARD THE EXISTING CASTING. INSTALL A NEW CASTING TO GRADE (ACCORDING TO TOLERANCES AS SHOWN ON STANDARD CONSTRUCTION DRAWING BP-3.1) AFTER THE PAVEMENT SURFACE COURSE HAS BEEN REPLACED.

CMS 499 CLASS QCMS CONCRETE (DYE THE CONCRETE SUCH THAT ITS COLOR CLOSELY MATCHES THE COLOR OF THE SURROUNDING PAVEMENT) WILL BE USED FOR BACKFILLING THE FULL PAVEMENT SECTION AND THE JOINT BETWEEN THE ASPHALT AND CONCRETE WILL BE SEALED WITH CMS 702.01 PG BINDER. EPOXY COATED REBAR SHALL BE PLACED IN THE CONCRETE AT 6" MAXIMUM ON CENTER AND A MINIMUM OF 3.5" CLEARANCE FROM THE TOP, BOTTOM AND SIDES. THE CONCRETE WILL BE VIBRATED SUFFICIENTLY TO ELIMINATE AIR POCKETS UNDER THE FRAME.

PAYMENT WILL INCLUDE REMOVAL OF THE EXISTING MATERIAL, INSTALLATION AND FURNISHING OF A NEW CASTING, AND ALL LABOR AND MATERIALS REQUIRED TO COMPLETE THIS ITEM OF WORK AS DESCRIBED.

PRESSURE RELIEF JOINT (MAH-680-0817)

THE FOLLOWING ITEMS SHALL BE USED AS DIRECTED BY THE PROJECT ENGINEER TO REMOVE AND REPLACE THE TOP 3" OF ASPHALT IN THE PRESSURE RELIEF ITEM 441, ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448), 2 CY ITEM 202, WEARING COURSE REMOVED, AS PER PLAN, 14 SY

ASBESTOS NOTIFICATION (MAH-680-0791E)

A CERTIFIED ASBESTOS HAZARD EVALUATION SPECIALIST INSPECTED THE BRIDGE STRUCTURE SCHEDULED FOR DEMOLITION AND/OR REHABILITATION;

THE INSPECTION DETERMINED THAT MAH-680-0791E CONTAINS ASBESTOS. THE ASBESTOS CONTAINING MATERIAL SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. THE CONTRACTOR SHALL ENSURE THAT THE ABATEMENT, TRANSPORT, AND DISPOSAL OF ASBESTOS CONTAINING MATERIAL IS CONDUCTED IN ACCORDANCE WITH ALL FEDERAL, STATE, AND LOCAL REGULATIONS. THE CONTRACTOR SHALL ENSURE THAT ALL DOCUMENTATION RELATED TO THE ABATEMENT, TRANSPORT, AND DISPOSAL OF ASBESTOS CONTAINING MATERIAL IS SUBMITTED TO THE PROJECT ENGINEER FOR RECORD KEEPING WITHIN 2 WEEKS OF COMPLETION.

THE DEPARTMENT HAS PROVIDED A COPY OF THE OHIO ENVIRONMENTAL PROTECTION AGENCY (OEPA) NOTIFICATION OF DEMOLITION AND RENOVATION FORM (PARTIALLY COMPLETED) AND THE ASBESTOS I INSPECTION REPORT IN THE REFERENCE FILES FOR THIS PROJECT. THE CONTRACTOR SHALL COMPLETE THE FORM AND SUBMIT IT TO THE OEPA AT LEAST TEN (10) WORKING DAYS PRIOR TO THE START OF ANY DEMOLITION AND/OR RENOVATION. ONLINE SUBMISSION IS AVAILABLE AT <http://www.epa.ohio.gov/asbestos> AND IS ENCOURAGED, OR THE CONTRACTOR SHALL SUBMIT IT TO ONE OF THE ADDRESSES BELOW:

ASBESTOS PROGRAM
OHIO EPA, DAPC
P.O. BOX 1049
COLUMBUS, OH 43216-1049

ASBESTOS PROGRAM
OHIO EPA, DAPC
50 W. TOWN ST., SUITE 700
COLUMBUS, OH 43215

THE FORM SHALL INCLUDE:

- 1. THE CONTRACTOR'S NAME AND ADDRESS
- 2. THE SCHEDULED DATES FOR THE START AND COMPLETION OF THE STRUCTURE DEMOLITION AND/OR RENOVATION
- 3. DESCRIPTION OF THE PLANNED DEMOLITION WORK AND METHODS BE USED
- 4. ALL NECESSARY FEES

THE CONTRACTOR SHALL PROVIDE A COPY OF THE COMPLETED NOTIFICATION OF DEMOLITION AND RENOVATION FORM TO THE PROJECT ENGINEER AT LEAST TEN (10) WORKING DAYS PRIOR TO THE START OF ANY DEMOLITION AND/OR RENOVATION

THE CONTRACTOR SHALL FURNISH ALL FEES, LABOR, AND MATERIALS NECESSARY TO COMPLETE AND SUBMIT THE OEPA NOTIFICATION FORM. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN.

THE CONTRACTOR SHALL FURNISH ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO PROPERLY ABATE, TRANSPORT, AND DISPOSE OF ASBESTOS CONTAINING MATERIAL IN A LANDFILL LICENSED BY THE LOCAL HEALTH DEPARTMENT AND PERMITTED BY THE OHIO ENVIRONMENTAL PROTECTION AGENCY - DIVISION OF AIR POLLUTION CONTROL TO ACCEPT ASBESTOS CONTAINING MATERIAL. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN ITEM SPECIAL - REMOVAL OF ASBESTOS CONTAINING MATERIAL.

END CROSSFRAMES (MAH-680-1073L&R)

THE CONTRACTOR SHALL REMOVE AND INSTALL NEW END CROSSFRAMES AT THE ABUTMENTS OF MAH-680-1073L&R DURING THE CONCRETE PATCHING OF THE BACKWALLS AS DIRECTED BY THE PROJECT ENGINEER. THE CONTRACTOR SHALL NOT REMOVE ABUTTING END CROSSFRAMES AT THE SAME TIME. THE CONTRACTOR SHALL NOT REMOVE BOTH END CROSSFRAMES IN THE SAME BAY AT THE SAME TIME. ALL CROSSFRAME STEEL SHALL BE PAINTED TO MATCH THE EXISTING COLOR.

THIS WORK SHALL BE PAID FOR BY THE FOLLOWING ITEMS PER STRUCTURE:

ITEM 513 - STRUCTURAL STEEL MEMBERS, LEVEL UF, 2,543 LBS
ITEM 513 - STRUCTURAL STEEL MISC.: FILLET WELDING, 120 FT
ITEM 514 - FIELD PAINTING OF STRUCTURAL STEEL CROSSFRAMES, LS

ITEM 513 - STRUCTURAL STEEL, MISC.: REPLACEMENT OF DAMAGED CROSSFRAMES

THIS WORK CONSISTS OF REPLACING DAMAGED CROSSFRAMES THAT ARE BENT OF HAVE SECTION LOSS. THIS ITEM WILL INCLUDE SUPPLYING NEW CROSSFRAMES AND WELDING THEM BACK TO THE ORIGINAL POSITIONS OF THE CROSSFRAMES THAT ARE BEING REPLACED. AFTER REMOVAL, ALL WELDS WILL BE GROUND SMOOTH IN PREPARATION OF WELDING THE NEW CROSSFRAMES IN PLACE. ALL CROSSFRAMES TO BE REPLACED WILL BE FIELD MEASURED TO VERIFY SIZE AND LENGTHS PRIOR TO ORDERING MATERIAL. THE NEW CROSSFRAMES WILL BE WELDED TO THE GIRDDERS OR BEAMS ON BOTH SIDES OF THE VERTICAL LEG AND ON THE TOP SIDE OF THE HORIZONTAL LEG. THE ANGLE WILL BE WELDED USING A 1/4" CONTINUOUS FILLET WELD. STEEL MEMBERS TO BE FABRICATED UNDER THIS ITEM WILL NOT REQUIRE SHOP DRAWINGS PRIOR TO FABRICATION. AISC CERTIFICATION IS NOT REQUIRED. THE CONTRACTOR WILL TAKE THE NECESSARY FIELD MEASUREMENTS TO VERIFY MEASUREMENTS BEFORE ORDERING MATERIALS. THE ENGINEER WILL HAVE THE AUTHORITY AND THE RESPONSIBILITY FOR ENSURING THAT THE STEEL IS ACCEPTABLE. AFTER FABRICATION THE PAY WEIGHTS SHALL BE COMPUTED IN COMPLIANCE WITH ITEM 513 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS AND SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL.

ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM EXCEPT FOR PAINT WILL BE INCLUDED FOR PAYMENT UNDER ITEM 513 - STRUCTURAL STEEL MISC.: REPLACEMENT OF DAMAGED CROSSFRAMES.

ASBESTOS NOTIFICATION (MAH-680-0794, MAH-680-1073L&R)

A CERTIFIED ASBESTOS HAZARD EVALUATION SPECIALIST INSPECTED THE BRIDGE STRUCTURE SCHEDULED FOR DEMOLITION AND/OR REHABILITATION;

THE SURVEY DETERMINED THAT NO ASBESTOS IS PRESENT ON THE STRUCTURE.

THE DEPARTMENT HAS PROVIDED A COPY OF THE OHIO ENVIRONMENTAL PROTECTION AGENCY (OEPA) NOTIFICATION OF DEMOLITION AND RENOVATION FORM (PARTIALLY COMPLETED) AND THE ASBESTOS I INSPECTION REPORT IN THE REFERENCE FILES FOR THIS PROJECT. THE CONTRACTOR SHALL COMPLETE THE FORM AND SUBMIT IT TO THE OEPA AT LEAST TEN (10) WORKING DAYS PRIOR TO THE START OF ANY DEMOLITION AND/OR RENOVATION. ONLINE SUBMISSION IS AVAILABLE AT <http://www.epa.ohio.gov/asbestos> AND IS ENCOURAGED, OR THE CONTRACTOR SHALL SUBMIT IT TO ONE OF THE ADDRESSES BELOW:

ASBESTOS PROGRAM
OHIO EPA, DAPC
P.O. BOX 1049
COLUMBUS, OH 43216-1049

OR
ASBESTOS PROGRAM
OHIO EPA, DAPC
50 W. TOWN ST., SUITE 700
COLUMBUS, OH 43215

THE FORM SHALL INCLUDE:

- 1. THE CONTRACTOR'S NAME AND ADDRESS
- 2. THE SCHEDULED DATES FOR THE START AND COMPLETION OF THE STRUCTURE DEMOLITION AND/OR RENOVATION
- 3. DESCRIPTION OF THE PLANNED DEMOLITION WORK AND METHODS BE USED
- 4. ALL NECESSARY FEES

THE CONTRACTOR SHALL PROVIDE A COPY OF THE COMPLETED NOTIFICATION OF DEMOLITION AND RENOVATION FORM TO THE PROJECT ENGINEER AT LEAST TEN (10) WORKING DAYS PRIOR TO THE START OF ANY DEMOLITION AND/OR RENOVATION

THE CONTRACTOR SHALL FURNISH ALL FEES, LABOR, AND MATERIALS NECESSARY TO COMPLETE AND SUBMIT THE OEPA NOTIFICATION FORM. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN.

SFN	0
DESIGN AGENCY	
DESIGNER	CHECKER
CMR	MJA
REVIEWER	
MJA 04-24-23	
PROJECT ID	
103883	
SUBSET	TOTAL
P.3	P.10
SHEET	
P.28	P.35

CALC:	CR	DATE:	3/24/2023
CHECKED:	MJA	DATE:	4/12/2023

ESTIMATED QUANTITIES

BRIDGE NO. / STRUCTURE FILE NO.									ITEM	EXTENSION	UNIT	DESCRIPTION	SEE SHEET
MAH-680-0791E 5007348 02/IMS/47									201	11001	LS	CLEARING AND GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS	1/10
	MAH-680-0794 5007380 02/IMS/47								202	11201	LS	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	1/10
		MAH-680-0817 5007429 02/IMS/47							518	12500	EACH	SCUPPER, MISC.: SCUPPER CLEANOUT	
			MAH-680-0837 5007534 02/IMS/47						203	40000	CY	BORROW	
				MAH-680-0921 5007577 02/IMS/47									
					MAH-680-0990 5007615 02/IMS/47								
						MAH-680-1073L 5007712 02/IMS/47							
							MAH-680-1073R 5007720 02/IMS/47						
								MAH-224-1964 5004853 02/IMS/47					
									201				
									202				
									518				
									203				
									254				
									407				
									441				
									509				
									509				
									512				
									512				
									613				
									516				
									516				
									SPECIAL				
									519				
									519				
									SPECIAL				
									SPECIAL				
									601				
									611				
									638				
									843				
									844				
									513				
									513				
									SPECIAL				
									SPECIAL				
									441				
									510				
									202				

2 — adjusted

added

STRUCTURE ESTIMATED QUANTITIES

SFN	0
DESIGN AGENCY	
DESIGNER	CHECKER
CMR	MJA
REVIEWER	
MJA 04-24-23	
PROJECT ID	
103883	
SUBSET	TOTAL
P.4	P.10
SHEET	TOTAL
P.29	P.35