

GENERAL

UTILITIES (G102A)

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

GAS ASPIRE ENERGY 300 TRACY BRIDGE ROAD ORRVILLE, OH 44667 330.682.7726	GAS COLUMBIA GAS OF OHIO 7080 FRY ROAD MIDDLEBURG HEIGHTS, OH 44130 440.891.2428
COMMUNICATION FRONTIER COM 83 TOWNSEND AVENUE NORWALK, OH 44857 419.744.3613	ELECTRIC LORAIN-MEDINA RURAL ELECTRIC P.O. BOX 158 WELLINGTON, OH 44090 800.222.8673
COMMUNICATION VERIZON BUSINESS 120 RAVINE STREET AKRON, OH 44303 330.253.8267	FIBER OPTIC LIT FIBER 255 CENTER ST SEVILLE, OH 44273 330.366.2008
TRAFFIC ODOT DISTRICT THREE 906 CLARK AVENUE ASHLAND, OH 44805 419.207.2868	COMMUNICATION EVERSTREAM SOLUTIONS 800 W ST CLAIR, 2ND FLOOR CLEVELAND, OH 44113 216.581.7972
CABLE CHARTER COMMUNICATIONS 5520 WHIPPLE AVENUE NW NORTH CANTON, OH 44720 330.494.9200	VILLAGE VILLAGE OF LODI 108 AINSWORTH STREET, P.O. BOX 95 LODI, OH 44254 330.948.2040

THE FOREMENTIONED UTILITY COMPANIES AND AGENCIES HAVE VARIOUS FACILITIES IN THE AREA THAT WILL REMAIN IN PLACE DURING CONSTRUCTION.

EXTREME CAUTION SHOULD BE EXERCISED IN AREAS WITH UTILITIES. SECTIONS 105.07 AND 107.16 OF THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS REQUIRE, AMONG OTHER THINGS, THAT THE CONTRACTOR COOPERATE WITH ALL UTILITIES LOCATED WITHIN THE LIMITS OF THIS CONSTRUCTION PROJECT AND TAKE RESPONSIBILITY FOR THE PROTECTION OF THE UTILITY PROPERTY AND SERVICES.

EXISTING PLANS (G103)

EXISTING PLANS ENTITLED MAY BE INSPECTED IN THE ODOT DISTRICT THREE OFFICE IN ASHLAND:

ROUTE	PLAN NAME	YEAR
MED-421	MED-421-0.00	1993
MED-421	MED-421-0.06	2012

WORK LIMITS (G106)

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

ROUTINE MAINTENANCE

BETWEEN THE TIME THAT BIDS ARE TAKEN AND THE START OF CONSTRUCTION, THE MAINTAINING AGENCY MAY ENTER UPON THE PROJECT AND PERFORM ROUTINE MAINTENANCE SUCH AS CRACK SEALING, PATCHING, AND BERM AND SHOULDER REPAIR. THE EFFECTS, IF ANY, OF THE PERFORMANCE OF ROUTINE MAINTENANCE SHALL BE CONSIDERED AS INHERENT IN WORK OF THE CHARACTER PROVIDED FOR IN THE PLAN AND THE RESULTING CONDITIONS SHALL NOT BE CONSIDERED AS DIFFERING MATERIALLY FROM THOSE EXISTING AT THE TIME BIDS WERE TAKEN.

COORDINATION OF WORK BETWEEN CONTRACTORS

THE CONTRACTOR SHOULD BE AWARE THAT THERE MAY BE OTHER WORK BEING PERFORMED BY A SEPARATE CONTRACT. THE VILLAGE OF LODI HAS A WATER LINE REPLACEMENT CONTRACT SCHEDULED TO BEGIN WORK IN THE 2026 CONSTRUCTION SEASON. COORDINATION OF WORK IS THE RESPONSIBILITY OF THE CONTRACTOR.

CONSTRUCTION NOISE (G104)

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, DO NOT OPERATE POWER-OPERATED CONSTRUCTION-TYPE DEVICES BETWEEN THE HOURS OF 8:00PM AND 7:00AM. IN ADDITION, DO NOT OPERATE AT ANY TIME ANY DEVICE IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT.

ADA WAIVER (R128)

AN APPROVED ADA DESIGN WAIVER IS REQUIRED ON THIS PROJECT. THE FOLLOWING FEATURES LISTED BELOW CANNOT FEASIBLY BE CONSTRUCTED TO MEET ADA GUIDELINES.

ADA DESIGN WAIVERS			
LOCATION DESCRIPTION	ADA FEATURE (ID NUMBER)	APPROVAL DATE	SEE SHEET NUMBER
SR 421 / WHEELING AND LAKE ERIE RR - SW	CURB RAMP (RMP0028721)	11/21/2025	P.26
SR 421 / WHEELING AND LAKE ERIE RR - SE	CURB RAMP (RMP0028719)	11/21/2025	P.27
SR 421 / DOLLAR GENERAL - SE	CURB RAMP (RMP0030715)	11/21/2025	P.27
SR 421 / AINSWORTH ST - SW	CURB RAMP (RMP0008955)	11/21/2025	P.30
SR 421 / AINSWORTH ST - SW	CURB RAMP (RMP0008956)	11/21/2025	P.30
SR 421 / AINSWORTH ST - SE	CURB RAMP (RMP0008959)	11/21/2025	P.30
SR 421 / WOOSTER ST - SW	CURB RAMP (RMP0008958)	11/21/2025	P.31
SR 421 / ELYRIA ST - NE	CURB RAMP (RMP0008960)	11/21/2025	P.32
SR 421 / MILL ST - NW	CURB RAMP (RMP0016309)	11/21/2025	P.33
SR 421 / GILBERT ST - NE	CURB RAMP (RMP0016315)	11/21/2025	P.34
SR 421 / PARK ST - NW	CURB RAMP (RMP0016317)	11/21/2025	P.35

PRECONSTRUCTION PEDESTRIAN FACILITY LAYOUT INSPECTION

THE PROPOSED LAYOUT OF THE PEDESTRIAN FACILITIES INCLUDED IN THESE PLANS IS TO BE FIELD REVIEWED AND VERIFIED FOR COMPLIANCE WITH THE PLANS AND APPROPRIATE STANDARDS PRIOR TO PERFORMING ANY ASSOCIATED REMOVAL OR CONSTRUCTION. THIS MEETING IS INTENDED TO REVIEW PROPOSED WORK AS LAID OUT BY THE CONTRACTOR PRIOR TO THE MEETING; THIS MEETING IS NOT INTENDED TO LAYOUT ALL LOCATIONS IN CONJUNCTION WITH THE CONTRACTOR. THE CONTRACTOR SHOULD ADHERE TO THE PROJECT PLANS ON INITIAL LAYOUT PRIOR TO THIS MEETING, DETERMINE IF THERE ARE QUESTIONS, CONCERNS, OR CONTRACTOR-PROPOSED MODIFICATIONS TO THE DESIGN AT EACH LOCATION, AND BE PREPARED TO DISCUSS ANY SUCH LOCATIONS.

THE MEETING PARTICIPANTS WILL REVIEW EACH LOCATION AS REQUESTED BY THE CONTRACTOR, ADHERING TO THE ABOVE DETAILS. ADDITIONAL LOCATIONS WILL BE VERIFIED BY DISTRICT PERSONNEL FOR ADHERENCE TO THE PLANS AND SPECIFICATIONS.

COORDINATE WITH THE PROJECT ENGINEER TO SCHEDULE THE MEETING WITH ALL APPROPRIATE STAKEHOLDERS IN ORDER TO PROVIDE A MINIMUM OF 14 CALENDAR DAY NOTICE TO ALL MEETING ATTENDEES. THE REQUIRED STAKEHOLDERS ARE THE DISTRICT ADA ENGINEER, DISTRICT ADA ENGINEER, MUNICIPAL REPRESENTATIVE (IF APPLICABLE), PROJECT ENGINEER, AND CONTRACTOR REPRESENTATIVE. THE ENGINEER OF RECORD, ODOT PROJECT MANAGER, ODOT DESIGNERS, AND CONSTRUCTION AREA ENGINEER SHOULD BE INVITED AS OPTIONAL ATTENDEES.

ALL MATERIAL, EQUIPMENT, LABOR, AND INCIDENTALS NEEDED TO COMPLETE THIS MEETING ARE TO BE INCLUDED IN THE CONTRACT BID PRICE FOR THE APPROPRIATE PEDESTRIAN FACILITY ASSOCIATED WITH THIS WORK.

GUARDRAIL

CONNECTION BETWEEN EXISTING AND PROPOSED GUARDRAIL (R111)

WHEN IT IS NECESSARY TO SPLICE PROPOSED GUARDRAIL TO EXISTING GUARDRAIL, ONLY THE EXISTING GUARDRAIL SHALL BE CUT, DRILLED, OR PUNCHED. THE CONNECTION SHALL BE MADE USING A W-BEAM, BEAM SPLICE AS SHOWN IN AASHTO M 180-12, EXCEPT THE BEAM WASHERS ARE NOT TO BE USED. PAYMENT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE RESPECTIVE GUARDRAIL ITEMS.

LOCATIONS OF GUARDRAIL

THE GUARDRAIL PROTECTION PROVIDED IN THIS PLAN SHALL BE LOCATED IN THE FIELD TO ASSURE THAT THE INSTALLATION WILL AFFORD THE MAXIMUM PROTECTION FOR TRAFFIC. THIS LOCATION SHALL BE POSITIONED AS FAR AS POSSIBLE FROM THE EDGE OF PAVEMENT WHILE MAINTAINING PROPER GRADE IN FRONT OF GUARDRAIL AS PER STANDARD DRAWINGS AND PLAN DETAILS.

SUGGESTED SEQUENCE OF GUARDRAIL WORK

1. GUARDRAIL WORK IS TO BEGIN AFTER THE LINEAR GRADING IS COMPLETED AND THE 617 MATERIAL IS PLACED.
2. REMOVE THE GUARDRAIL.
3. PERFORM THE RESHAPING UNDER GUARDRAIL INCLUDING COMPLETING THE EMBANKMENT, AS PER PLAN.
4. REBUILD/CONSTRUCT THE GUARDRAIL RUN.
5. INSTALL BARRIER REFLECTORS.

ITEM 202 – BRIDGE TERMINAL ASSEMBLY REMOVED, AS PER PLAN

THIS ITEM SHALL BE USED TO REMOVE THE EXISTING TYPE 4 BRIDGE TERMINAL ASSEMBLY AND TYPE T CABLE ANCHOR ON THE SOUTHWEST CORNER OF THE MED-421-0053 STRUCTURE.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID PER EACH FOR ITEM 202 – BRIDGE TERMINAL ASSEMBLY REMOVED, AS PER PLAN, AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 606 – RAISING TYPE 5 GUARDRAIL

WHERE DESIGNATED ON THE PLAN, THE EXISTING TYPE 5 GUARDRAIL SHALL BE RAISED ON THE EXISTING WOOD POSTS AS PER STANDARD DRAWING GR-2.1 SO AS TO OBTAIN THE STANDARD 29 IN. HEIGHT. THE RAIL SHALL BE RE-ATTACHED TO THE POSTS USING NEW POST BOLTS.

THE RAIL SHALL BE DISMANTLED ONLY TO THE EXTENT NECESSARY TO FIELD BORE NEW BOLT HOLES IN THE WOOD POSTS, AND TO RECONNECT THE RAIL AND BLOCK TO THE EXISTING POSTS.

THE EXISTING TYPE “A” ANCHOR ASSEMBLIES THAT ARE TO REMAIN SHALL NOT BE ADJUSTED. THE LAST RAIL ELEMENT SHALL BE TRANSITIONED TO MEET THESE ASSEMBLIES.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER FOOT OF ITEM 606 – RAISING TYPE 5 GUARDRAIL, WHICH SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 203 – EMBANKMENT, AS PER PLAN

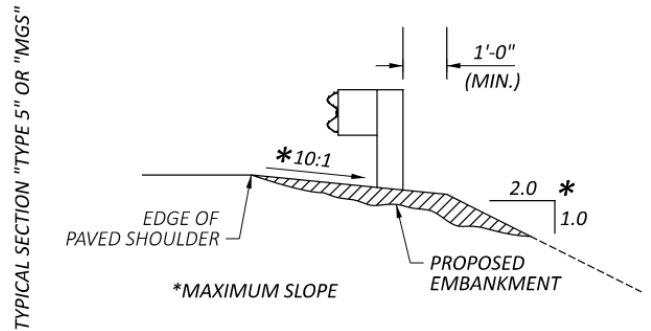
AT SPECIFIED LOCATIONS AND LOCATIONS AS DIRECTED BY THE ENGINEER, EMBANKMENT SHALL BE PLACED AS TO PROVIDE A SUITABLE AREA TO CONSTRUCT GUARDRAIL AND TO PROVIDE STRUCTURAL INTEGRITY OF THE ROADWAY SHOULDER.

AREAS WHERE EMBANKMENT MATERIAL IS TO BE PLACED SHALL BE SCALPED. THE REQUIREMENTS FOR BENCHING SHALL BE WAIVED. THE DEPTH OF LAYERS IN WHICH THE EMBANKMENT IS PLACED SHALL BE LIMITED TO EIGHT (8) INCHES IN THICKNESS. THE METHOD OF COMPACTION AND EQUIPMENT USED SHALL BE PER C&MS 203.07 OR 98% MAXIMUM DRY DENSITY.

AFTER THE EMBANKMENT HAS BEEN PLACED, THE AREAS SHALL BE FERTILIZED, SEEDED, MULCHED, AND WATERED AS PER ITEM 659. THE COST SHALL BE INCLUDED IN THIS ITEM FOR PAYMENT.

THE METHOD OF MEASUREMENT FOR EMBANKMENT MATERIAL SHALL BE BY THE NUMBER OF CUBIC YARDS MEASURED BY LOOSE VOLUME IN THE CARRIER AT THE WORK SITE, IN LIEU OF THE REQUIREMENTS OF 203.09. PAYMENT FOR ACCEPTED QUANTITIES WILL BE MADE AT THE CONTRACT UNIT BID PRICE PER CUBIC YARD FOR ITEM 203 - EMBANKMENT, AS PER PLAN AND SHALL INCLUDE ALL WORK DESCRIBED ABOVE.

CONTINGENCY QUANTITIES OF ITEM 203 – EMBANKMENT, AS PER PLAN HAVE BEEN PROVIDED IN LOCATIONS WHERE GUARDRAIL IMPROVEMENTS ARE TO BE MADE. THESE AREAS ARE NOT SHOWN ON THE PLANS FOR CLARITY. A QUANTITY OF 6 CY PER 100 FT OF GUARDRAIL IMPROVEMENT HAS BEEN USED FOR ESTIMATING PURPOSES. EXACT DIMENSIONS AND LOCATIONS ARE TO BE DETERMINED BY THE ENGINEER.



ITEM 209 – RESHAPING UNDER GUARDRAIL, AS PER PLAN

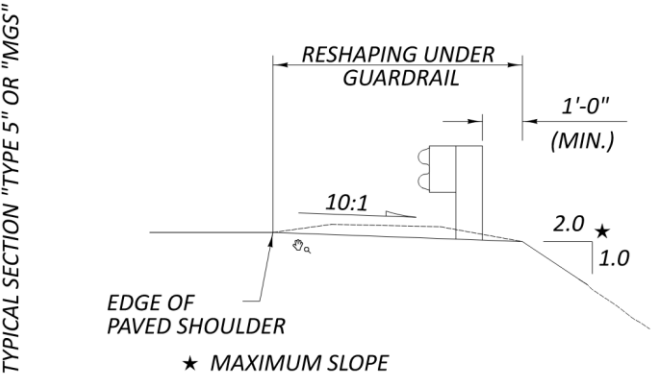
THIS ITEM SHALL BE USED AT LOCATIONS INDICATED IN THE PLANS.

THIS WORK SHALL BE COMPLETED AT LOCATIONS SPECIFIED FOR WORK AS WELL AS PER CMS 209.05 AND AS DESCRIBED HEREIN AND SHALL AT ALL TIMES BE AS DIRECTED BY THE ENGINEER.

THE AREA IN FRONT OF, UNDER, AND BEHIND THE PRESENTLY ERCTED OR TO BE ERCTED GUARDRAIL SHALL BE GRADED AND RESHAPED TO PROVIDE AN AREA THAT HAS A SLOPE OF 10:1 MAXIMUM (SEE DETAIL BELOW AS WELL AS THE GUARDRAIL DETAIL SHEETS FOR FURTHER DETAILS AND INFORMATION OF THE LIMITS OF THIS WORK).

EXCESS MATERIAL RESULTING SHALL BE USED ELSEWHERE FOR THIS ITEM IF SO DIRECTED OR DISPOSED OF PROPERLY. IF EXTRA MATERIAL IS REQUIRED IT SHALL BE PAID FOR WITH ITEM 203 - EMBANKMENT, AS PER PLAN. THIS WORK SHALL NOT BE STARTED UNTIL AFTER THE RESURFACING AND BERM WORK HAS BEEN COMPLETED.

THE ABOVE WORK SHALL BE PAID FOR PER STATION WITH ITEM 209 - RESHAPING UNDER GUARDRAIL, AS PER PLAN WITH THE EXCEPTION OF ANY EXTRA MATERIAL REQUIRED TO MEET THE SLOPE REQUIREMENTS WHICH SHALL BE PAID BY ITEM 203 - EMBANKMENT, AS PER PLAN.



1.) 6 FT FOR ONE MAILBOX SUPPORT, ADD 3 FT. FOR EACH ADDITIONAL MAILBOX SUPPORT.

ITEM 659 – TOPSOIL, AS PER PLAN

THIS ITEM CONSISTS OF PLACING TOPSOIL AT THE SPECIFIED LOCATIONS IN PLACE OF CURB RAMPS, WALKS, OR OTHER PEDESTRAIN FACILITIES OR PORTIONS OF PEDESTRAIN FACILITEIS TO BE REMOVED.

PLACE CLEAN TOPSOIL IN THE VOIDS LEFT BY ANY REMOVED SECTIONS OF PEDESTRAIN FACILITIES TO FINISH FLUSH WITH THE SURROUNDING GROUND AND/OR PROPOSED OR REMAINING PEDESTRAIN FACILITY AND/OR ROADWAY. AFTER THIS TOPSOIL HAS BEEN PLACED, SEED AND MULCH THE AREAS ACCORDING TO ITEM 659. THE COST FOR THIS SEEDING AND MULCHING IS TO BE CONSIDERED INCIDENTAL TO THIS TOPSOIL, AS PER PLAN ITEM.

THE METHOD OF MEASUREMENT FOR TOPSOIL MATERIAL IS TO BE THE NUMBER OF LOOSE CUBIC YARDS DELIVERED, PLACED, AND ACCEPTED IN LIEU OF THE REQUIREMENTS OF 659.24. PAYMENT FOR ACCEPTED QUANTITIES WILL BE MADE AT THE CONTRACT BID PRICE PER CUBIC YARD FOR ITEM 659 – TOPSOIL, AS PER PLAN (CURB RAMPS), WHICH IS TO INCLUDE ALL LABOR, MATERIAL, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THE WORK.

ITEMS OF WORK

ITEM 900 - SPECIAL - RAILROAD COORDINATION

THIS PROJECT INCLUDES WORK WITHIN THE RIGHT OF WAY OF ONE OR MORE RAILWAYS. THE CONTRACTOR SHALL COMPLETE AND SECURE ALL INSURANCE AND DOCUMENTATION REQUIREMENTS WITH EACH AFFECTED RAILROAD ON THE PROJECT. SEE THE PROJECT PROPOSAL FOR ADDITIONAL INFORMATION AND REQUIREMENTS AS PART OF THE RAILROAD AGREEMENT AND/OR RAILROAD CLAUSES INCLUDED WITH THE PROJECT.

RAILROAD FLAGGING WILL BE REQUIRED BY THE RAILWAY(S) WHEN ANY PROJECT OPERATIONS ARE BEING COMPLETED WITHIN RAILROAD RIGHTS OF WAY. IN ORDER TO LIMIT THE RISK OF DELAYS IN THE PROJECT SCHEDULE, THE CONTRACTOR SHALL MAKE EFFORTS TO SCHEDULE A RAILROAD FLAGGER WITH THE AFFECTED RAILROAD(S) AS EARLY AS POSSIBLE, PREFERABLY WITHIN TWO WEEKS OF THE AWARD OF THE CONTRACT. THE PROJECT PROPOSAL AND/OR RAILROAD CLAUSES LISTS THE RAILWAY-REQUESTED MINIMUM NOTIFICATION REQUIREMENTS FOR EACH AFFECTED RAILROAD. IF IT IS DETERMINED BY THE ENGINEER THAT THE CONTRACTOR DID NOT NOTIFY THE RAILROAD WITHIN THE REQUIRED TIMEFRAME TO SECURE A RAILROAD FLAGGER, AND THUS INCURS A DELAY IN PROJECT OPERATIONS, ANY DELAY CLAIMS MADE BY THE CONTRACTOR AS A RESULT OF DELAYED COORDINATION WITH SUCH RAILROAD(S) WILL BE DENIED.

IT MAY BE NECESSARY TO COMPLETE WORK WITHIN RAILROAD RIGHTS OF WAY UNDER A SEPARATE PHASE FROM THE REMAINDER OF THE PROJECT PENDING THE AVAILABILITY OF RAILROAD FLAGGING.

ALL WORK, INCLUDING LABOR, MATERIALS, EQUIPMENT, MOBILIZATION, AND INCIDENTALS, NEEDED TO COMPLETE THIS COORDINATION WITHIN THE RAILROAD RIGHT OF WAY SHALL BE INCLUDED IN THE CONTRACT LUMP SUM BID PRICE FOR ITEM 900 - SPECIAL - RAILROAD COORDINATION, UNLESS SEPARATELY ITEMIZED OR DETAILED IN THIS PLAN AND PROPOSAL.

CSX TRANSPORTATION GENERAL PROJECT NOTES

1. IF ANY ISSUE OR INCIDENT OCCURS WITHIN CSXT ROW, PLEASE CONTACT THE CSXT PUBLIC SAFETY COORDINATION CENTER AT 800-232-0144.
2. ROADWAY FLAGGERS MUST BE PRESENT ON EACH SIDE OF THE RAILROAD CROSSING, ANYTIME TRAFFIC IS DIRECTED INTO OPPOSING TRAFFIC LANES AT THE RAILROAD CROSSING.
3. CHANNELIZATION/MOT DEVICES SHALL NOT BE PLACED WITHIN 15 FT FROM CENTER LINE OF RAILROAD TRACKS.
4. NO REDUCTION IN VERTICAL CLEARANCE IS PERMITTED UNDER THE CSX RAILROAD BRIDGES. CONTRACTOR SHALL TAKE PRE-CONSTRUCTION AND POST-CONSTRUCTION MEASUREMENTS OF THE MINIMUM VERTICAL CLEARANCES IN THE PRESENCE OF A CSXT REPRESENTATIVE AND REPORT THE MEASUREMENTS TO BRAD_ARMSTRONG@CSX.COM, CFRANK@BENESCH.COM, AND GTORRES@BENESCH.COM. CLEARANCE SIGNS SHALL BE REPLACED IF/AS NECESSARY TO REFLECT ACTUAL CONDITIONS.

DRAINAGE

ITEM 611 – CASTINGS ADJUSTED TO GRADE

THE CASTING TO BE ADJUSTED MAY OR MAY NOT HAVE AN EXISTING FRAME. THE WORK SHALL CONSIST OF ADJUSTING THE EXISTING CASTING TO THE SATISFACTION OF THE ENGINEER. IT IS NOT INTENDED TO PLACE NEW FRAMES WHERE NONE CURRENTLY EXIST. THE CONTRACTOR IS REMINDED TO FIELD CHECK ALL ADJUSTMENT TO GRADE ITEMS PRIOR TO BIDDING, AS NO ADDITIONAL COMPENSATION WILL BE GRANTED FOR LABOR AND MATERIALS REQUIRED TO SATISFACTORILY ADJUST CASTINGS WITHOUT FRAMES.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY AND LISTED UNDER THE APPROPRIATE ADJUSTING ITEM:

COUNTY	ROUTE	SLM	SIDE	CASTING TYPE	QUANTITY
MEDINA	421	1.08	RT	CATCH BASIN	1
MEDINA	421	1.50	CL	MANHOLE	1
MEDINA	421	1.55	CL	MANHOLE	1
MEDINA	421	1.61	CL	MANHOLE	1
MEDINA	421	1.63	CL	MANHOLE	1
MEDINA	421	1.72	LT	WATER VALVE	1
MEDINA	421	1.72	CL	MANHOLE	1
MEDINA	421	1.80	CL	MANHOLE	1
MEDINA	421	1.80	RT	CATCH BASIN	1
MEDINA	421	1.84	RT	MANHOLE	2
MEDINA	421	1.89	RT	MANHOLE	1
MEDINA	421	1.90	LT	WATER VALVE	1
MEDINA	421	1.94	RT	MANHOLE	1
MEDINA	421	1.97	LT	CATCH BASIN	1
MEDINA	421	1.97	RT	WATER VALVE	1
MEDINA	421	1.97	RT	MANHOLE	1
MEDINA	421	1.98	RT	MANHOLE	1
MEDINA	421	1.99	LT	CATCH BASIN	1

MEDINA	421	1.99	RT	CATCH BASIN	1
MEDINA	421	2.06	RT	WATER VALVE	3
MEDINA	421	2.06	RT	CATCH BASIN	3
MEDINA	421	2.06	CL	MANHOLE	3
MEDINA	421	2.06	LT	CATCH BASIN	1
MEDINA	421	2.13	RT	MANHOLE	1
MEDINA	421	2.13	CL	MANHOLE	1
MEDINA	421	2.13	LT	CATCH BASIN	1
MEDINA	421	2.13	RT	CATCH BASIN	1
MEDINA	421	2.22	CL	MANHOLE	1
MEDINA	421	2.22	RT	WATER VALVE	1
MEDINA	421	2.22	RT	MANHOLE	4
MEDINA	421	2.23	CL	MANHOLE	1
MEDINA	421	2.28	CL	MANHOLE	2
MEDINA	421	2.28	LT	CATCH BASIN	1
MEDINA	421	2.28	RT	CATCH BASIN	1
MEDINA	421	2.34	LT	CATCH BASIN	1
MEDINA	421	2.34	RT	CATCH BASIN	1
MEDINA	421	2.34	RT	MANHOLE	1
MEDINA	421	2.46	RT	WATER VALVE	1
MEDINA	421	2.46	RT	CATCH BASIN	1
MEDINA	421	2.48	RT	WATER VAVLE	2
MEDINA	421	2.48	RT	CATCH BASIN	1
MEDINA	421	2.54	LT	WATER VALVE	1
MEDINA	421	2.54	RT	WATER VALVE	1
MEDINA	421	2.55	CL	MANHOLE	1
MEDINA	421	2.75	RT	MANHOLE	1
MEDINA	421	2.83	RT	MANHOLE	1

ITEM 611 – CATCH BASIN ADJUSTED TO GRADE 17 EACH (01/STR)
ITEM 611 – MANHOLE ADJUSTED TO GRADE 29 EACH (01/STR)
ITEM 638 – VALVE BOX ADJUSTED TO GRADE 12 EACH (01/STR)

ITEM 611 – INLET RECONSTRUCTED TO GRADE, AS PER PLAN

THIS ITEM SHALL INCLUDE THE REMOVAL AND REPLACEMENT OF THE UPPER BOX SECTION OF THE PAVEMENT INLET WITH A PRECAST CONCRETE TOP AS INDENTIFIED IN THE DETAIL BELOW AND/OR AS DIRECTED BY THE ENGINEER. THE ITEM SHALL ASLO INCLUDE THE REMOVAL AND REPLACEMENT OF THE EXISTING INLET WALLS BELOW THE UPPER BOX SECTION TO THE FLOW LINE IF FOUND TO BE UNSOUND AFTER REMOVAL OF THE UPPER BOX SECTION AS DIRECTED BY THE ENGINEER. CLASS QC1 CONCRETE, 8” THICK, CAST IN PLACE SHALL BE USED FOR THE WALL REPLACMENT AS DIRECTED BY THE ENGINEER.

A NEW CONCRETE APRON SHALL BE PROVIDED AS DETAILED IN STANDARD CONSTRUCTION DRAWING I –2A.

SEE STANDARD CONSTRUCTION DRAWING I-2A FOR ADDITIONAL DETAILS NOT SHOWN ON THIS SHEET.

ALL DRAINAGE CONDUITS OUT-LETTING INTO THE INLET SHALL BE MAINTAINED WITH A CONDUIT OF THE SAME SIZE WITH A CONCRETE COLLAR POURED COMPLETELY AROUND THE JOINT. ANY VOIDS FOUND AROUND THE INLET DURING THE RECONSTRUCTION WILL BE FILLED WITH LOW STRENGTH MORTAR BACKFILL AND SHALL BE INCLUDED IN THIS ITEM.

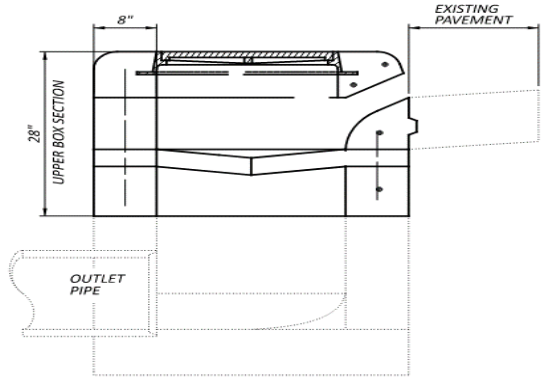
THIS ITEM SHALL ALSO INCLUDE THE NECESSARY TOPSOIL, SEEDING, AND MULCHING FOR THE ASSOCIATED DISTURBED AREAS. THE CONTRACTOR SHALL ENSURE A DENSITY OF AT LEAST 70% GRASS COVER. REPAIR SEEDING AND MULCHING MAY BE NECESSARY. NO SEPARATE PAYMENT WILL BE MADE FOR REPAIR SEEDING AND MULCHING.

PAYMENT FOR THE CURB AT THE INLET SHALL BE INCLUDED IN THIS ITEM.

PAYMENT FOR ITEM 611 – INLET RECONSTRUCTED TO GRADE, AS PER PLAN SHALL BE MADE AT THE UNIT PRICE BID PER EACH AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS REQUIRED TO PERFORM THE ITEM OF WORK AS DESCRIBED IN THESE SECTIONS. APPROXIMATE LOCATIONS OF INLETS ARE AS FOLLOWS:

COUNTY	ROUTE	SLM	SIDE	QUANTITY
MEDINA	421	1.49	LT	1
MEDINA	421	1.66	LT	1
MEDINA	421	1.72	LT	1
MEDINA	421	1.75	LT	1
MEDINA	421	1.97	RT	1

ITEM 611 – INLET RECONSTRUCTED TO GRADE, AS PER PLAN 5 EACH (01/STR)



PAVEMENT

PROFILE AND ALIGNMENT

PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. (PREVIOUS CONSTRUCTION PLANS SHOWING THE ORIGINAL ALIGNMENT AND PROFILE, ARE AVAILABLE FOR INSPECTION AT THE ODOT DISTRICT 3 OFFICE). PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

ITEM 254 - PATCHING PLANED SURFACE

AN ESTIMATED QUANTITY OF ITEM 254 - PATCHING PLANED SURFACE HAS BEEN SET UP TO BE USED AS DIRECTED BY THE ENGINEER AS DESCRIBED IN CMS 254.04. THE LIMIT OF THE PATCHING DEPTH IS 0 TO 2 IN.

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE

THE INTENT OF THE PLANING IS TO MILL 3.0 INCHES AT THE CENTER OF PAVEMENT AT NON-CURBED AREAS. THE PAVEMENT SLOPE SHALL BE 0.010 MINIMUM AND 0.016 PREFERRED, CONTINUOUS BETWEEN THE CROWN AND THE PROPOSED EDGELINE/SHOULDER. THE MILLING DEPTH SHALL BE CONTROLLED FROM THE CENTER OF PAVEMENT IN CONFORMANCE WITH THE ABOVE GUIDELINES.

SPECIAL ATTENTION SHALL BE GIVEN TO SUPERELEVATED CURVES. THE SUPERELEVATION SHALL BE MAINTAINED AND/OR RESTORED, IF NECESSARY, AS DIRECTED BY THE ENGINEER. IF THERE IS NO INFORMATION IN THE PLANS TO CHANGE THE SUPERELEVATION, THE INTENT IS TO MAINTAIN THE EXISTING SUPERELEVATION.

THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE TO ALL CATCH BASINS AND INLETS.

THE PROGRESSION OF THE PLANING SHALL PROCEED IN SUCH A MANNER THAT NORMAL TRAFFIC WILL NOT BE REQUIRED TO RUN OVER THE PLANED ROADWAY SURFACE MORE THAN FOURTEEN (14) CALENDAR DAYS. FOR EACH CALENDAR DAY BEYOND THE 14 DAYS THAT THE ROADWAY REMAINS EXPOSED TO THE PLANED SURFACE, THE CONTRACTOR WILL BE ASSESSED A DISINCENTIVE FEE OF \$2,500 PER DAY.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT PLANING, ASPHALT CONCRETE. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE.

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (CURBED SECTION)

THE INTENT OF THE PLANING IS TO MILL THE SPECIFIED DEPTH ALONG THE CURB CONTINGENT ON THE FOLLOWING: THE MAXIMUM CROSS SLOPE SHALL BE 0.02 WHILE THE MINIMUM CROSS SLOPE SHALL BE 0.01. THE PREFERRED CROSS SLOPE IS 0.016. THE CROWN OF THE PAVEMENT SHALL BE LOCATED BETWEEN THE TRAVELED LANES, OR AS DIRECTED BY THE ENGINEER. THE MILLING DEPTH SHALL BE CONTROLLED FROM THE CURB, TO PRODUCE A CROSS SLOPE IN CONFORMANCE WITH THE ABOVE GUIDELINES.

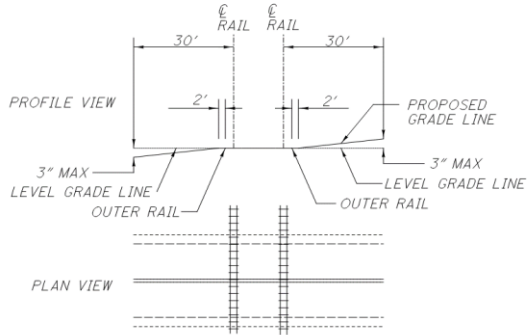
SPECIAL ATTENTION SHALL BE GIVEN TO SUPERELEVATED CURVES. THE SUPERELEVATION SHALL BE MAINTAINED AND/OR RESTORED, IF NECESSARY, AS DIRECTED BY THE ENGINEER. IF THERE IS NO INFORMATION IN THE PLANS TO CHANGE THE SUPERELEVATION, THE INTENT IS TO MAINTAIN THE EXISTING SUPERELEVATION.

THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE TO ALL CATCH BASINS AND INLETS.

THE PROGRESSION OF THE PLANING SHALL PROCEED IN SUCH A MANNER THAT NORMAL TRAFFIC WILL NOT BE REQUIRED TO RUN OVER THE PLANED ROADWAY SURFACE MORE THAN FOURTEEN (14) CALENDAR DAYS. FOR EACH CALENDAR DAY BEYOND THE 14 DAYS THAT THE ROADWAY REMAINS EXPOSED TO THE PLANED SURFACE, THE CONTRACTOR WILL BE ASSESSED A DISINCENTIVE FEE OF \$2,500 PER DAY.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT PLANING, ASPHALT CONCRETE. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE.

WORK THE CROWN OUT OF THE PROPOSED PAVEMENT ON EACH SIDE OF THE RAILROAD CROSSING, BEGINNING 50 FEET FROM THE NEAREST RAIL, BY RAISING THE EDGES OF THE NEW PAVEMENT TO MEET THE PLATFORM ELEVATION.



The diagram illustrates the components of a railroad right-of-way. The top portion, labeled "PLAN VIEW", shows a top-down perspective of a track with a central track bed and two side tracks, all enclosed within a larger boundary. The bottom portion shows a cross-section of the track bed, with arrows pointing to the "RAILROAD RIGHT OF WAY" boundary lines on either side.

THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE. PAVEMENT REPAIR SHALL BE PERFORMED AFTER PAVEMENT PLANING AND BEFORE PLACEMENT OF THE OVERLAY COURSES. THE DEPTH OF REMOVAL SHALL BE AS DIRECTED BY THE ENGINEER WITH A MAXIMUM DEPTH OF 6". THE MINIMUM WIDTH OF REPAIR SHALL BE 4FT. REPLACEMENT MATERIAL SHALL BE ITEM 301 ASPHALT CONCRETE BASE (449) AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES, ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN IS TO BE A MAXIMUM OF 6" DEEP. THE FOLLOWING ITEMS AND QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

THE CONTRACTOR SHALL APPLY ONE COAT OF MC-70 (AS PER SECTION 702) AT A RATE OF 0.40 GAL/SY TO THE COMPLETED AGGREGATE SHOULDER (ITEM 617) AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID BITUMINOUS MATERIAL ONTO THE EDGE OF PAVEMENT OR EDGE LINE. THE CONTRACTOR SHALL APPLY THE PRIME COAT TO THE AGGREGATE SHOULDER WITHIN 7 (SEVEN) CALENDAR DAYS OF THE BERM MATERIAL BEING LAYED ON THE SHOULDER. PRIOR TO APPLYING THE PRIME COAT LAYER, IT IS ALSO THE CONTRACTOR'S RESPONSIBILITY TO CORRECT ANY DAMAGES TO AGGREGATE SHOULDER THAT HAS OCCURRED DURING THE PERIOD OF TIME BETWEEN THE LAYING OF THE BERM MATERIAL AND THE SPRAYING OF PRIME COAT. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO 107.10 OF THE SPECIFICATIONS.

THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE. PAVEMENT REPAIR SHALL BE PERFORMED AFTER PAVEMENT PLANING AND BEFORE PLACEMENT OF THE OVERLAY COURSES. THE DEPTH OF REMOVAL SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT WITH A MINIMUM DEPTH OF 6" AND A MAXIMUM DEPTH OF 12" AND A MINIMUM WIDTH OF 4FT. FOR FULL DEPTH REPAIRS WHERE CONCRETE IS UNDERLYING ASPHALT BUT CONCRETE IS NOT BEING REPLACED AS PART OF THE REPAIR, REMOVE ALL ASPHALT TO THE TOP OF CONCRETE THEN COMPLETE FLEXIBLE REPAIR ON TOP OF EXISTING CONCRETE. REPLACEMENT MATERIAL SHALL BE ITEM 301 ASPHALT CONCRETE BASE (449) AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE. THE MAXIMUM PAVEMENT LIFT THICKNESS IS 6".

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES, ITEM 253 PAVEMENT REPAIR, AS PER PLAN IS TO BE GREATER THAN 6" DEEP. THE FOLLOWING ITEMS AND QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

ALL OPEN TRANSVERSE JOINTS SHALL BE TAPERED TO MEET EXISTING PAVEMENT BEFORE INTRODUCING TRAFFIC. A "BUMP" SIGN (W8-1-36) SHALL BE ERECTED ON EACH SIDE OF TRANSVERSE JOINTS LEFT OPEN OVER NIGHT, INCLUDING A SPEED ADVISORY SIGN. THESE SIGNS SHALL BE REMOVED IMMEDIATELY AFTER THE JOINT HAS BEEN CLOSED. PLACEMENT OF SIGNS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC. CARE SHALL BE TAKEN TO MATCH EXISTING PAVEMENT ELEVATIONS AT EXISTING PAVED BERMS, DRIVES, INTERSECTIONS, ETC. THE USE OF VIBRATORY ROLLERS WITHIN THE SR 83 / SR 421 INTERSECTION AREA (SLM 2.16 TO 2.20) IS PROHIBITED.

C-R-S	ASPHALT (IN)	CONCRETE (IN)	BRICK (IN)	LOCATION	DIRECTION	YEAR CORED
MED-421-0.165	11			LWP	EB	2024
MED-421-0.167	11	7.5		CENTER	EB	2024
MED-421-0.170	10			EDGE LINE	EB	2024
MED-421-0.814	10			LWP	EB	2024
MED-421-0.814	10.5			RWP	EB	2024
MED-421-0.817	8			EDGE LINE	EB	2024
MED-421-1.351	14			LWP	EB	2024
MED-421-1.354	17			RWP	EB	2024
MED-421-1.356	7			SHOULDER	EB	2024
MED-421-1.871	9.5	5		LWP	EB	2024
MED-421-1.873	9	9.5		RWP	EB	2024
MED-421-1.874	8.5			EDGE LINE	EB	2024
MED-421-2.504	11			EDGE LINE	NB	2024
MED-421-2.505	10		4	LWP	NB	2024
MED-421-2.508	10		4	CENTER	NB	2024
MED-421-2.839	5	9		RWP	NB	2015
MED-421-2.840	6	9		LWP	NB	2015
MED-421-2.840	12			SHOULDER	NB	2015
MED-421-3.271	9			EDGE LINE	NB	2024
MED-421-3.271	6			RWP	NB	2024
MED-421-3.275	6			LWP	NB	2024
MED-421-3.326	9.5			SHOULDER	NB	2015
MED-421-3.326	5.5	9		LWP	NB	2015
MED-421-3.327	5	9		RWP	NB	2015

STRUCTURE REPAIR

STANDARD DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS (BDM 601.2-1)

REFER TO THE FOLLWING STANDARD BRIDGE DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS:

<u>STANDARD DRAWING</u>	<u>REVISED</u>
DBR-2-73	7/19/2002
DBR-3-11	7/15/2011

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLAN PERTAINING TO THE EXISTING STRUCUTRES HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURES AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURES AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CM&S SECTIONS 102.05 AND 105.02.

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURES. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

EXISTING PLANS

EXISTING PLANS MAY BE INSPECTED IN THE ODOT DISTRICT THREE OFFICE IN ASHLAND:

<u>PLAN NAME</u>	<u>DATE</u>
MED-421-0049	1990

ITEM 517 - DEEP BEAM BRIDGE RETROFIT RAILING

THIS ITEM SHALL INCLUDE THE RETROFIT OF ALL EXISTING DEEP BEAM BRIDGE RAILING AT MED-421-0.530. THE REMOVAL AND REPLACEMENT OF ALL BOLTS AND HARDWARE NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THIS ITEM. THE RAIL ELEMENTS SHALL BE PLACED IN ACCORDANCE WITH STANDARD CONSTRUCTION DRAWING DBR-3-11, DATED 7/15/2011. VERIFY THE LOCATION OF THE REPAIR IN THE FIELD PRIOR TO BEGINNING WORK. THE REPAIR SHOULD BE CENTERED ON THE BRIDGE ON BOTH ENDS.

PAYMENT FOR ALL THE ABOVE SHALL BE AT THE UNIT PRICE BID PER FOOT FOR ITEM 517 – DEEP BEAM BRIDGE RETROFIT RAILING, WHICH WILL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

TRAFFIC CONTROL

STOP BAR PLACEMENT

IN ORDER TO COMPLY WITH THE REQUIREMENTS OF THE TRAFFIC ENGINEERING MANUAL AND THE OMUTCD, AT NORMAL STOP CONTROLLED INTERSECTIONS WITHOUT CROSSWALK, PLACE THE LEADING EDGE OF THE STOP BAR (CLOSEST TO THE CENTER OF THE INTERSECTION) IN ACCORDANCE WITH THE BELOW TABLE UNLESS SPECIFIED OTHERWISE IN THESE PLANS. IN LOCATIONS WHERE THE INTERSECTION IS SKEWED, PLACE THE STOP BAR PARALLEL TO THE INTERSECTED (MAJOR) ROADWAY RATHER THAN PERPENDICULAR TO THE STOP APPROACH.

<u>SHOULDER WIDTH OF INTERSECTED ROADWAY</u>	<u>PLACE THE LEADING EDGE OF STOP BAR ON INTERSECTING/APPROACH ROADWAY</u>
0 FEET < SHOULDER WIDTH ≤ 2 FEET	4 FEET BACK FROM EDGE OF PAVED SHOULDER OF INTERSECTED ROADWAY
2 FEET < SHOULDER WIDTH ≤ 4 FEET	2 FEET BACK FROM EDGE OF PAVED SHOULDER OF INTERSECTED ROADWAY
SHOULDER WIDTH > 4 FEET	IN LINE WITH EDGE OF PAVED SHOULDER OF INTERSECTED ROADWAY

PAVEMENT MARKING LOG

PRIOR TO REMOVING, GRINDING, OR OTHERWISE DESTROYING ANY EXISTING PAVEMENT MARKINGS, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CREATE AN EXISTING PAVEMENT MARKING LOG IN ORDER TO PLACE THE PROPOSED PAVEMENT MARKINGS IN THE SAME LOCATION AS THEIR EXISTING CONFIGURATION. SUBMIT THE EXISTING PAVEMENT MARKING LOG TO THE ENGINEER AND OBTAIN HIS OR HER APPROVAL PRIOR TO REMOVING, GRINDING, OR OTHERWISE DESTROYING THE EXISTING PAVEMENT MARKINGS.

ALL LABOR, MATERIAL, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THIS WORK SHOULD BE INCLUDED IN THE CONTRACT LUMP SUM BID PRICE FOR ITEM 614 – MAINTAINING TRAFFIC.

LANE WIDTHS

LANES SHALL BE STRIPED AT 11’ WIDTHS ON THE PROJECT.

LAYOUT OF WORK

PRIOR TO REMOVING OR ERECTING SIGNS OR SIGN SUPPORTS, THE CONTRACTOR SHALL FIELD LAYOUT AND IDENTIFY, BY TYPE OF WORK, SIGNS AND SIGN SUPPORTS TO BE ERECTED OR REMOVED. THIS LAYOUT MAY BE ACCOMPLISHED BY STAKING (USING WHITE STAKES OR WHITE FLAGS) OR BY PLACING CLEARLY DISCERNABLE WHITE PAINTED MARKINGS ON THE EDGE OF PAVEMENT. IN NO CASE SHALL THE CONTRACTOR PLACE ANY PERMANENT MARKINGS ON ANY EXISTING SIGN OR SIGN SUPPORT.

RESTORATION OF DISTURBED AREAS

THE CONTRACTOR SHALL RESTORE ALL SEEDED AND SODDED AREAS, PAVED BERMS, AND OTHER DISTURBED AREAS TO A CONDITION EQUAL TO OR BETTER TO THAT EXISTING BEFORE THIS WORK WAS STARTED. ALL RESTORATION WORK SHALL BE DONE IN ACCORDANCE WITH THE PERTINENT SPECIFICATION ITEM AND AS DIRECTED BY THE ENGINEER. PAYMENT FOR ALL RESTORATION WORK, INCLUDING MATERIALS AND LABOR SHALL BE INCLUDED WITH THE UNIT PRICE BID FOR THE VARIOUS 630 AND 631 ITEMS IN THIS PLAN.

WORKING DRAWINGS

THE REQUIREMENTS OF CMS 625.06, 630.03, 632.04, AND 633.04 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS ARE HEREBY MODIFIED TO REQUIRE TWO (2) SETS OF SHOP DRAWINGS, CATALOG CUTS, SPECIFICATIONS, PHOTOMETRIC DATA, BROCHURES, DATA SHEETS AND WIRING DIAGRAMS FOR REVIEW AND APPROVAL, AS REQUIRED BY THE DIRECTOR, OF APPARATUS AND EQUIPMENT TO BE FURNISHED. THESE DOCUMENTS ARE TO BE PROVIDED TO THE ODOT DISTRICT THREE CONSTRUCTION ADMINISTRATOR, 906 CLARK AVENUE, ASHLAND, OHIO 44805 FOR APPROVAL BEFORE THE ITEMS ARE FURNISHED.

ITEM 630 – GROUND MOUNTED SUPPORT, NO. 3 POST, AS PER PLAN

GROUND MOUNTED POST SUPPORTS USED ON THIS PROJECT SHALL BE U-CHANNEL DESIGN AND BE PER CMS 630.06 AND SCD TC-41.20 WITH THE FOLLOWING EXCEPTIONS.

POST SUPPORTS EXPOSED TO TRAFFIC SHALL BE STUBBED AND SPLICED PER THE FOLLOWING PROCEDURE:

- 1) DRIVE 60” LONG STUB TO WITHIN 12” OF GROUND SURFACE.
- 2) BOLT UPPER SIGN POST TO STUB USING FOUR EVENLY SPACED 5/16” ALUMINUM BOLTS & FOUR ZINC COATED SERRATED FLANGE HEX LOCK NUTS.
- 3) THE UPPER POST SHALL BE SPLICED BEHIND THE STUB POST TO MINIMIZE THE POSSIBILITY OF VEHICLE SNAGGING DURING A COLLISION.
- 4) CARE SHALL BE EXERCISED TO DRIVE AND ERECT POSTS VERTICALLY PLUMB AND ERECT SIGNS SO THEY ARE LEVEL WHEN VIEWED FROM THE ROADWAY.
- 5) PLACE NO MORE THAN TWO POSTS WITHIN 7’ VEHICLE PATH UNLESS LOCATED BEHIND GUARDRAIL. SEE SCD TC-41.20.
- 6) DESIGNATE SUPPORTS INSTALLED WITH THIS METHOD AS “BREAKAWAY TYPE = U-CHANNEL SPLICE (NON-BREAKWAY)” ON DISTRICT 3 SIGN DATA COLLECTION FORM.
- 7) COST OF SPLICE CONNECTION AND OVERLAP OF POSTS SHALL BE INCIDENTAL TO THE COST OF EACH SIGN SUPPORT.
- 8) COST OF REMOVING AND REINSTALLING SIGN POST REFLECTORS SHALL BE INCIDENTAL TO THE COST OF THE SIGN SUPPORT.

BASIS OF PAYMENT SHALL BE AT THE UNIT PRICE BID PER FOOT. IN ADDITION TO 630.06, ALL EXISTING GROUND MOUNTED POST SUPPORTS SHALL BE REMOVED AND NEW POSTS INSTALLED. LOCATE GROUND MOUNTED POST SUPPORTS AS FAR FROM THE EDGE OF PAVEMENT (AND ON FAR SIDE OF DITCH) AS POSSIBLE UP TO WITHIN FOUR (4) FEET OF THE APPARENT RIGHT OF WAY TO MINIMIZE THE CHANCE OF IMPACT FROM RUN-OFF THE ROAD INCIDENTS.

ALL GROUND MOUNTED POST SUPPORTS SHALL BE U-CHANNEL DESIGN. SQUARE TUBE POST SUPPORTS SHALL BE USED FOR ONE-WAY SUPPORTS (SCD TC-41.50).

ALL GROUND MOUNTED POST SUPPORTS SHALL BE DRIVEN VERTICALLY PLUMB TO A MINIMUM EMBEDMENT DEPTH OF 48 INCHES. IF GROUND CONDITIONS PREVENT DRIVING TO THIS DEPTH, THE SIGN INSTALLATION SHALL BE INSTALLED USING TWO POSTS SIDE BY SIDE TO FORM A DUAL POST INSTALLATION.

TRAFFIC SIGNALS

ITEM 202 – REMOVAL MISC.: ABANDONED SUPPORT FOUNDATION

THIS ITEM SHALL BE USED TO REMOVE AN ABANDONED SIGNAL SUPPORT FOUNDATION AT THE SOUTHEAST CORNER OF THE DOLLAR GENERAL DRIVE INTERSECTION TO FACILITATE PROPOSED CURB RAMP WORK.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID PER EACH FOR ITEM 202 – REMOVAL MISC.: ABANDONED SIGNAL FOUNDATION, AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 202 – REMOVAL MISC.: RETAINING WALL

THIS ITEM SHALL BE USED TO REMOVE PORTIONS OF AN EXISTING RETAINING WALL AT THE SOUTHEAST CORNER OF THE DOLLAR GENERAL DRIVE INTERSECTION TO FACILITATE PROPOSED CURB RAMP WORK.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE LUMP SUM PRICE BID FOR ITEM 202 – REMOVAL MISC.: RETAINING WALL, AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 632 – PEDESTAL, MISC.: REUSE OF PEDESTRIAN PEDESTAL

THIS ITEM SHALL BE USED TO RELOCATE THE EXISTING PEDESTALS ONTO NEW FOUNDATIONS AT LOCATIONS SHOWN IN THE PLANS. THE CONTRACTOR SHALL REFER TO C&MS 632.27.

THE CONTRACTOR SHALL FURNISH AND INSTALL NEW ANCHOR BOLTS UNDER THIS ITEM OF WORK.

THE CONTRACTOR AND THE ENGINEER SHALL INSPECT THE PEDESTAL SUPPORT PRIOR TO RELOCATION FOR THE PURPOSE OF DOCUMENTING ANY EXISTING DAMAGE. ANY DAMAGE IDENTIFIED AFTER RELOCATION AND NOT DOCUMENTED IN THE PRE-RELOCATION INSPECTION REPORT WILL BE PRESUMED TO HAVE BEEN CAUSED BY THE CONTRACTOR. THE CONTRACTOR WILL BE REQUIRED TO REPAIR OR REPLACE THE DAMAGED EQUIPMENT AT THE OPTION OF THE ENGINEER.

THE COST OF ANY TRENCHING, CONDUIT, CABLE, AND CONNECTIONS NECESSARY FOR RELOCATION SHALL BE CONSIDERED INCIDENTAL TO THIS ITEM. THE REMOVAL OF THE PEDESTAL FROM THE EXISTING FOUNDATION SHALL BE CONSIDERED INCIDENTAL TO THIS ITEM.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE CONTRACT PRICE BID FOR ITEM 632 – PEDESTAL, MISC.: REUSE OF EXISTING PEDESTAL, AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 632 – PEDESTAL FOUNDATION, AS PER PLAN

THIS ITEM SHALL BE AS PER CMS 632, 732 AND SCD TC-83.20 EXCEPT THE CONTRACTOR SHALL FURNISH AND INSTALL NEW ANCHOR BOLTS EQUAL IN NUMBER AND SIZE ASSOCIATED WITH THE EXISTING PEDESTAL WHICH WILL BE REMOVED AND REUSED. THE CONTRACTOR IS ALSO RESPONSIBLE FOR PROVIDING AN ANCHOR BOLT TEMPLATE COMPATIBLE WITH THE EXISTING PEDESTAL.

THE FORMED TOP OF THE ANCHOR BASE POLE FOUNDATION SHALL BE ORIENTED PARALLEL TO THE SIDEWALK AS SHOWN ON THE PEDESTRIAN FACILITY DETAILS. THE TOP OF THE FOUNDATION SHALL BE FLUSH WITH ANY ADJACENT SIDEWALK OR CONCRETE AREA.

PRIOR TO PLACING THE FOUNDATION, THE CONTRACTOR SHALL CONTACT OHIO811 TO HAVE ALL THE UTILITIES LOCATED IN THE FIELD, THEN MEET WITH THE PROJECT ENGINEER TO LOCATE THE PROPOSED FOUNDATIONS TO ENSURE THERE ARE NO CONFLICTS WITH UTILITIES. ADDITIONALLY, THE FOUNDATIONS SHALL BE EXCAVATED OR VACUUM EXCAVATED TO TEST FOR CONFLICTS. FOUNDATION VOIDS SHALL BE TEMPORARY BACKFILLED AND SECURELY COVERED. EXCAVATIONS SHALL NOT BE LEFT OPEN OVERNIGHT. EXCAVATION FOR THE FOUNDATION SHALL BE CONSIDERED INCIDENTAL TO THIS ITEM OF WORK.

IF UTILITY CONFLICTS ARE ENCOUNTERED DURING CONSTRUCTION, THE ENGINEER SHALL BE NOTIFIED. MODIFICATIONS TO THE FOUNDATION LOCATION MAY BE REQUIRED. VACUUM EXCAVATION AND FOUNDATION LOCATION ADJUSTMENTS SHALL BE CONSIDERED INCIDENTAL TO THIS ITEM OF WORK. THIS WORK SHALL INCLUDE BACKFILLING, COMPACTING, AND RESTORATION OF UNUSED EXCAVATION TO THE SITE’S ORIGINAL CONDITION.

PAYMENT WILL BE MADE AT THE CONTRACT UNIT PRICE FOR ITEM 632 – PEDESTAL FOUNDATION, AS PER PLAN AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY FOR EACH FOUNDATION, IN PLACE, COMPLETE AND ACCEPTED.

ITEM 632 – REUSE OF PEDESTRIAN SIGNAL HEAD, AS PER PLAN

ITEM 632 – REUSE OF PEDESTRIAN PUSHBUTTON, AS PER PLAN

THIS ITEM SHALL BE USED TO RELOCATE EXISTING PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS TO THE RELOCATED PEDESTALS AT LOCATIONS SHOWN IN THE PLANS.

ALL NEW MOUNTING HARDWARE SHALL BE FURNISHED AND INSTALLED.

PRIOR TO DEACTIVATION AND REMOVAL OR MODIFICATION OF EXISTING EQUIPMENT, THE CONTRACTOR AND THE ENGINEER SHALL INSPECT THE ITEM PRIOR TO RELOCATION FOR THE PURPOSE OF DOCUMENTING ANY EXISTING DAMAGE. ANY DAMAGE IDENTIFIED AFTER RELOCATION AND NOT DOCUMENTED IN THE PRE-RELOCATION INSPECTION REPORT WILL BE PRESUMED TO HAVE BEEN CAUSED BY THE CONTRACTOR. THE CONTRACTOR WILL BE REQUIRED TO REPAIR OR REPLACE THE DAMAGED EQUIPMENT AT THE OPTION OF THE ENGINEER. IF REPLACEMENT IS NECESSARY DUE TO DAMAGE CAUSED BY THE CONTRACTOR, THE REPLACEMENT ITEMS SHALL MATCH THE MAKE AND MODEL OF EXISTING COMPONENTS. REPLACEMENT OR REPAIR OF ANY NON-OPERATIONAL COMPONENTS IDENTIFIED DURING THE PRE-CONSTRUCTION INSPECTION SHALL BE THE RESPONSIBILITY OF THE CITY.

THE COST OF ANY TRENCHING, CONDUIT, CABLE, AND CONNECTIONS NECESSARY FOR RELOCATION OF THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL BE CONSIDERED INCIDENTAL TO THIS ITEM. THE REMOVAL OF THE PEDESTRIAN SIGNAL HEAD SHALL BE CONSIDERED INCIDENTAL TO THIS ITEM.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE CONTRACT PRICE BID FOR ITEM 632 –REUSE OF PEDESTRIAN SIGNAL HEAD, AS PER PLAN, AND ITEM 632 – REUSE OF PEDESTRIAN PUSHBUTTON, AS PER PLAN, AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 632 – REMOVAL OF MISCELLANEOUS TRAFFIC SIGNAL ITEM (PEDESTAL FOUNDATION)

THIS ITEM SHALL BE USED TO REMOVE THE EXISTING PEDESTAL FOUNDATIONS AT LOCATIONS SHOWN IN THE PLANS. THE REMOVAL SHALL INCLUDE THE CONCRETE FOUNDATION AND GROUND ROD TO A DEPTH OF 1 FOOT BELOW SUBGRADE OR FINISHED GROUND LINE. THE REMOVAL OF THE PEDESTAL FOUNDATION SHALL BE AS DESCRIBED IN C&MS 632.26 INCLUDING BACKFILLING, SURFACE RESTORATION, AND DISPOSAL OF SURPLUS MATERIAL. THIS WORK SHALL TAKE PLACE AFTER RELOCATION OF THE EXISTING PEDESTAL AND ASSOCIATED ITEMS TO THE NEW PEDESTAL FOUNDATION.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE CONTRACT PRICE BID FOR ITEM 632 – REMOVAL OF MISCELLANEOUS TRAFFIC SIGNAL ITEM (PEDESTAL FOUNDATION), AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

INCIDENTALS

ITEM 623 – CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN

AFTER COMPLETION OF ALL WORK, BUT PRIOR TO FINAL ACCEPTANCE OF THE PROJECT, AN OHIO REGISTERED PROFESSIONAL SURVEYOR SHALL DETERMINE THE MINIMUM VERTICAL CLEARANCES OF ALL EXISTING AND NEW BRIDGES WITHIN THE PROJECT LIMITS. AT A MINIMUM, MEASUREMENTS SHALL BE TAKEN ALONG EACH FASCIA BEAM AT THE EDGE OF SHOULDERS, EDGE LINES, LANE LINES, AND CROWN OF THE ROADWAY BELOW. THE ODOT DISTRICT 12 VERTICAL CLEARANCE SURVEY FORM SHALL BE USED, WHERE APPLICABLE, TO DOCUMENT THE MEASUREMENTS. WHERE THE ODOT DISTRICT 12 VERTICAL CLEARANCE SURVEY FORM IS NOT APPLICABLE, THE MEASUREMENTS SHALL BE DOCUMENTED ON A CONTRACTOR-DEVELOPED FORM THAT CLOSELY RESEMBLES THE ODOT DISTRICT 12 VERTICAL CLEARANCE SURVEY FORM AND ACCURATELY DEPICTS THE BRIDGE AND BELOW LANE AND SHOULDER CONFIGURATION. THE COMPLETED FORM SHALL BEAR THE STAMP OR SEAL OF THE OHIO REGISTERED PROFESSIONAL SURVEYOR WHO HAS TAKEN THE MEASUREMENTS AND SHALL BE SUBMITTED TO THE PROJECT ENGINEER PRIOR TO FINAL ACCEPTANCE OF THE PROJECT.

THE ODOT DISTRICT 12 VERTICAL CLEARANCE SURVEY FORM CAN BE DOWNLOADED FROM THE FOLLOWING HYPERLINK:

ODOT DISTRICT 12 VERTICAL CLEARANCE SURVEY FORM.PDF (STATE.OH.US)