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SEE SHEET 2 FOR LOCATION MAP

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION
MOT-MICRO-FY2017
CITIES OF DAYTON, GERMANTOWN
AND MORaine
CLAY, GERMAN, JEFFERSON,
AND MIAMI TOWNSHIPS
MONTGOMERY COUNTY

INDEX OF SHEETS:

TITLE SHEET	1
LOCATION MAP	2
GENERAL NOTES	3,4
GENERAL SUMMARY	5
PAVEMENT SUBSUMMARY	6
TRAFFIC CONTROL	7,8
BIKEWAY PAVEMENT MARKING DETAILS	9

PROJECT DESCRIPTION

MICROSURFACE VARIOUS ROUTES IN MONTGOMERY COUNTY.

MOT-4-4.30 TO 11.88
MOT-40-0.00 TO 6.22
MOT-725-8.31 TO 10.13

PROJECT EARTH DISTURBED AREA: N/A*
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A*
NOTICE OF INTENT EARTH DISTURBED AREA: N/A*
*MAINTENANCE PROJECT

2016 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

UNDERGROUND UTILITIES
CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.



Call Before You Dig
1-800-362-2764

OHIO
Utilities Protection
SERVICE


(Non-members must be called directly)

OIL & GAS PRODUCERS
UNDERGROUND PROTECTION SERVICE

1-800-925-0988

PLAN PREPARED BY:
OHIO DEPT. OF TRANSPORTATION
DISTRICT 7 PLANNING & ENGINEERING
SIDNEY, OH

ENGINEERS SEAL:



SIGNED: *Ryan P. Hanke*
DATE: 2/9/17

STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
BP-3.1	7/18/14	TC-41.20	10/18/13				
		TC-42.20	10/18/13				
DM-4.3	1/15/16	TC-52.10	10/18/13			800 1/20/17	
DM-4.4	1/15/16	TC-64.10	1/20/17			821 4/20/12	
		TC-65.10	1/17/14			832 1/17/14	
						921 4/20/12	
MT-95.61	7/19/13						
MT-95.31	1/20/17	TC-65.11	7/15/16				
MT-95.32	1/20/17	TC-71.10	1/20/17				
MT-97.10	7/18/14						
MT-97.12	1/20/17						
MT-99.20	7/19/13						
MT-99.30	1/16/15						
MT-105.10	7/19/13						

APPROVED *Randy Chevalley PE PS/sum*
DATE 2/9/17 DISTRICT DEPUTY DIRECTOR

APPROVED _____
DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION

FEDERAL PROJECT NO.
NON-FEDERAL

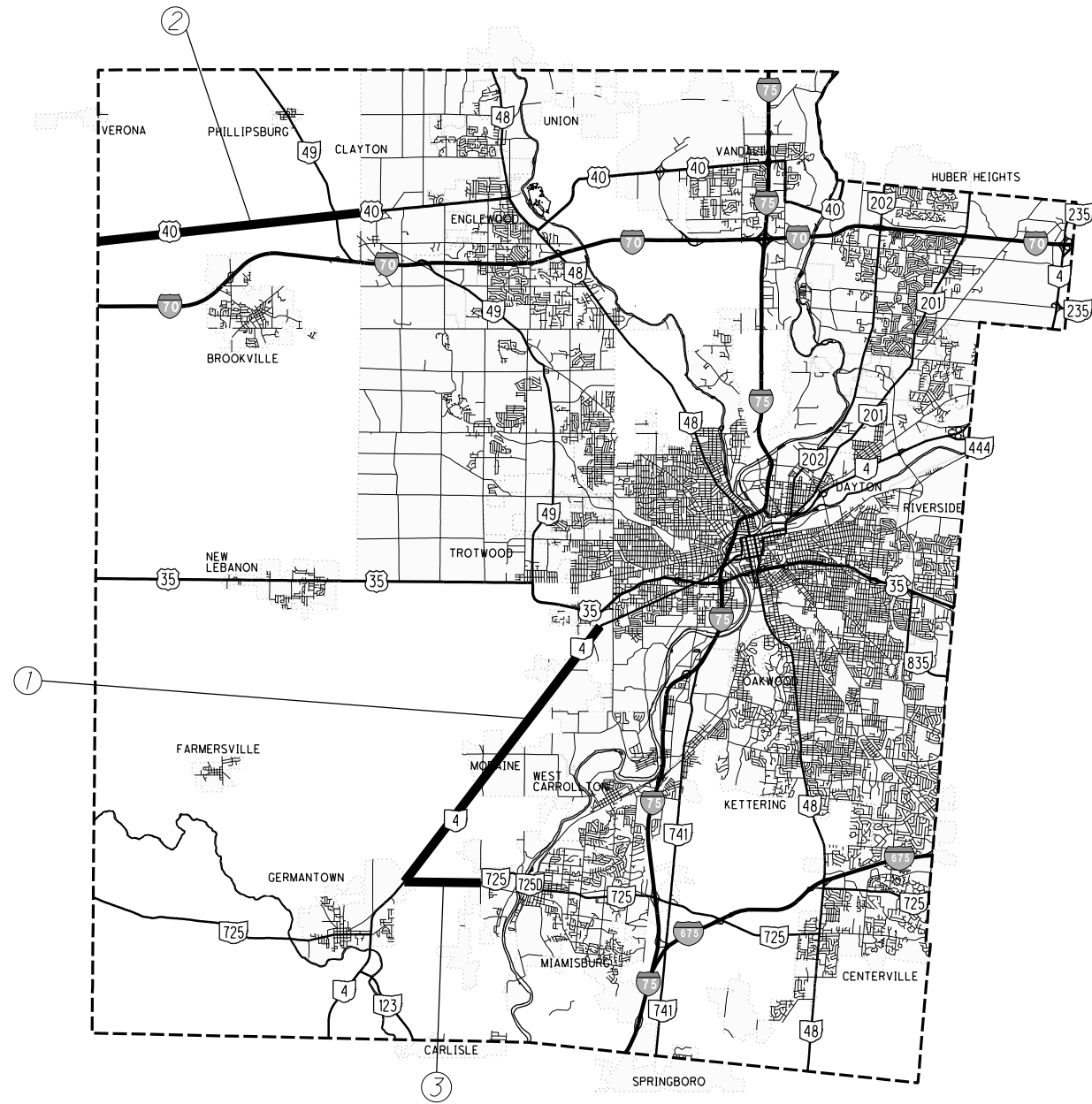
PID NO.
101062

CONSTRUCTION PROJECT NO.

RAILROAD INVOLVEMENT
NONE

**MOT-MICRO -
FY2017**

1
9



MONTGOMERY COUNTY

PROJECT LOCATIONS

- 1 MOT-4-4.30 TO 11.88
- 2 MOT-40-0.00 TO 6.22
- 3 MOT-725-8.31 TO 10.13

LATITUDE: 39° 45' 39" LONGITUDE: 84° 22' 17"



PORTION TO BE IMPROVED	—————	—————
INTERSTATE HIGHWAY	—————	—————
FEDERAL ROUTES	—————	—————
STATE ROUTES	—————	—————
OTHER ROADS	—————	—————

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WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

ALIGNMENT AND PROFILE

THE WORK PROPOSED BY THIS PROJECT CONSISTS OF MICROSURFACING OF THE EXISTING PAVEMENT. ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED AND THE PROFILE OF THE PROPOSED SURFACE WILL BE SIMILAR TO THAT OF THE EXISTING PAVEMENT.

ITEM 253, PAVEMENT REPAIR, AS PER PLAN

PAVEMENT REPAIR SHALL BE IN ACCORDANCE WITH ITEM 253 - PAVEMENT REPAIR, WITH THE FOLLOWING ADDITIONS:

THE PAVEMENT REPAIR AREAS SHALL BE ROUGHLY RECTANGULAR IN SHAPE AND SAWED OR MILLED TO A NEAT LINE. THE DEPTH OF REMOVAL, AS DIRECTED BY THE ENGINEER, SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT. THE REPLACEMENT MATERIAL SHALL BE ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448).

THE ESTIMATED PAVEMENT REPAIR AREAS SHALL BE A MINIMUM OF FOUR FEET (4.0') IN WIDTH & FOUR INCHES (4") IN DEPTH UNLESS OTHERWISE DIRECTED BY THE ENGINEER AND SHALL BE FINISHED TO ACCOMMODATE THE PROPOSED OVERLAY.

THE ESTIMATED PAVEMENT REPAIR AREAS LOCATED ON THE CENTERLINE SHALL BE THREE FEET (3.0') IN WIDTH AND FOUR INCHES (4") IN DEPTH UNLESS OTHERWISE DIRECTED BY THE ENGINEER AND SHALL BE FINISHED BEFORE THE PROPOSED OVERLAY IS PLACED.

SR - 4 3500 SQ. YD.
SR - 725 300 SQ. YD.
US - 40 200 SQ. YD.

A QUANTITY OF 4000 SQ. YD. OF ITEM 253, PAVEMENT REPAIR, AS PER PLAN HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

ITEM 253 - PAVEMENT REPAIR, AS PER PLAN = 4000 SQ YD

PAVEMENT MARKINGS

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DOCUMENT THE LAYOUT OF THE EXISTING PAVEMENT MARKINGS.

THE CONTRACTOR SHALL COORDINATE AND CORRABORATE THE LAYOUT OF ALL PAVEMENT MARKINGS WITH THE CITIES OF GERMANTOWN, DAYTON, MORaine AND ODOT.

ITEM 618 - CENTERLINE RUMBLE STRIPE (ASPHALT CONCRETE)

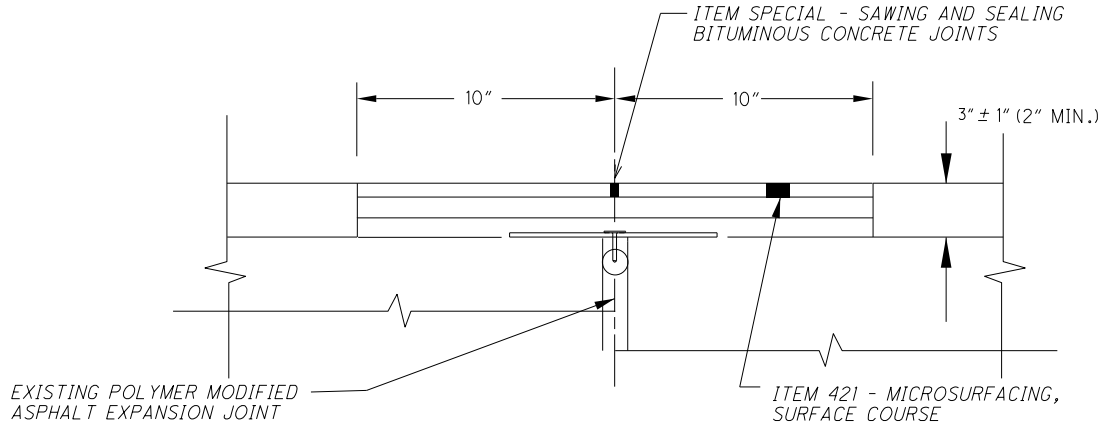
AN ESTIMATED QUANTITY OF 0.75 MILE HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED TO REPLACE ANY EXISTING CENTERLINE RUMBLE STRIPE THAT WILL BE REMOVED BY THE REQUIRED PAVEMENT REPAIRS. THE 3 FOOT (3') WIDE PAVEMENT REPAIR AREA AND THE INSTALLATION OF THE CENTERLINE RUMBLE STRIPE SHALL BE INSTALLED PRIOR TO THE MICROSURFACING.

ITEM SPECIAL - SAWING AND SEALING BITUMINOUS CONCRETE JOINTS

THE ENTIRE OVERLAY OF THE STRUCTURES SHALL BE SAWED AND SEALED IN ACCORDANCE WITH 2016 C&MS, ITEM 516.

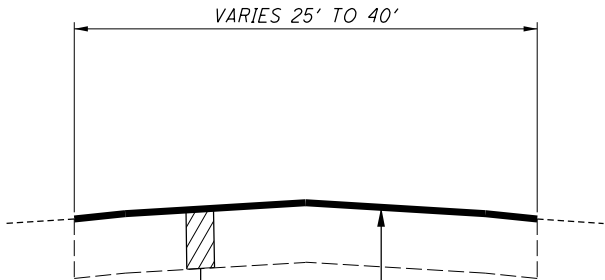
STRUCTURES MOT-4-0677, MOT-4-1006 AND MOT-4-1046 SHALL HAVE THE APPROACH SLAB/BRIDGE JOINT SAWED AND SEALED.

THE CONTRACTOR SHALL MARK THE LOCATIONS PRIOR TO ANY PAVING.



TYPICAL PRESTRESSED BOX BEAM OR CONCRETE SLAB JOINT

JOINT DETAIL FOR
BR. MOT-4-6.77
BR. MOT-4-10.06
BR. MOT-4-10.46



MOT 4 FROM SLM 4.30 TO 11.88 = 7.58 MILES
SUSPEND WORK SLM 7.12 TO 7.17
(STRUCTURE MOT-4-0712)
MOT 40 FROM SLM 0.00 TO 6.22 = 6.22 MILES
SUSPEND WORK SLM 5.64 TO 5.67
(SR 49)
SUSPEND WORK SLM 5.75 TO 5.76
(STRUCTURE MOT-40-0587)
MOT 725 FROM SLM 8.31 TO 10.13 = 1.82 MILES

LEGEND

A - EXISTING ASPHALT PAVEMENT

1 - ITEM 421, MICROSURFACING, SURFACE COURSE

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ITEM 614 - MAINTAINING TRAFFIC

TRAFFIC SHALL BE MAINTAINED AT ALL TIMES. THE LENGTH OF RESTRICTED TRAFFIC WORK ZONES SHALL BE KEPT TO A MAXIMUM TWO (2.0) MILE WORK ZONE CONSISTENT WITH THE SPECIFICATION REQUIREMENTS FOR A PROTECTION OF COMPLETED COURSES. IN ADDITION TO THE REQUIREMENTS AS INDICATED IN THE "OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", AND PERTINENT ITEMS OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS, THE FOLLOWING REQUIREMENTS SHALL APPLY.

IT IS THE INTENTION TO PERFORM THE REQUIRED WORK WITH THE LEAST INCONVENIENCE TO AND THE MAXIMUM SAFETY OF THE CONTRACTOR AND THE TRAVELING PUBLIC. ANY VARIANCES FROM THESE MAINTENANCE OF TRAFFIC NOTES MUST BE APPROVED IN ADVANCE IN WRITING BY THE DIRECTOR. TRAFFIC IS TO BE MAINTAINED IN A UNIFORM PATTERN THROUGHOUT THE ENTIRE LENGTH OF THE PROJECT AND NOT BE SUBJECTED TO CONSTANT LANE SHIFTS.

THE CONTRACTOR'S OPERATIONS SHALL BE ARRANGED TO PREVENT ANY INTERFERENCE TO THE CONTINUOUS FLOW OF TRAFFIC. ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE APPROVED BY THE ENGINEER.

ON NORTH END OF THE SR-4 SECTION THERE ARE 2 NORTH-BOUND AND 1 SOUTHBOUND LANE. UTILIZE MT-95.31 AND MT-95.32 TO CLOSE EITHER THE CENTER ON OUTSIDE NORTH-BOUND LANES. UTILIZE MT-95.61 TO CLOSE THE SOUTH-BOUND LANE AND PLACE THE SOUTHBOUND TRAFFIC INTO THE CENTER NORTHBOUND LANE.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE, ERECT, MAINTAIN (IN PROPER POSITION, CLEAN, LEGIBLE AND GOOD WORKING CONDITION) AND REMOVE ALL LIGHTS, SIGNS BARRICADES, CONES AND ALL OTHER TRAFFIC CONTROL DEVICES NECESSARY FOR THE MAINTENANCE OF TRAFFIC, INCLUDING PAVEMENT MARKINGS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONTINUED...)

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO

DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONTINUED...)

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 24 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE D7 PUBLIC INFORMATION OFFICER. THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTICE TO D7 PIO TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO D7 PIO
RAMP & ROAD CLOSURES	>= 2 WKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HRS & < 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HRS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES AND RESTRICTIONS	>= 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WKS	2 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTICE TO D7 PIO TIME TABLE.

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ROUTE	PARTICIPATION	SLM		LENGTH		AVG WIDTH	PVMT AREA	421			516	519	INFORMATION ONLY										CALCULATED SSWL CHECKED TML
								MICROSURFACING, SURFACE COURSE			SAWING & SEALING BITUMINOUS CONCRETE JOINTS	PATCHING CONCRETE BRIDGE DECK TYPE B	LENGTH (BRIDGE LIMITS)	WIDTH	BRIDGE DECK AREA								
		FROM	TO	MILE	FT	FT	SQ YD	SQ YD	FT	SQ. YD	FT	FT	SQ. YD.										
MOT-4	01/NHS/PV	4.30	4.55	0.25	1320	31	4579	4579															
		4.55	4.75	0.20	1056	32	3782	3782															
		4.75	6.88	2.13	11246	20	25031	25031															
		6.88	7.12	0.24	1267	30	4223	4223															
		7.12	7.17	Skip Structure MOT-4-7.12																			
		7.17	7.25	0.24	1267	30	4223	4223															
		7.25	7.95	0.70	3696	29	11909	11909															
		7.95	8.93	0.98	5174	33	18971	18971															
		8.93	9.36	0.43	2270	32	8071	8071															
		9.36	11.58	2.22	11722	27	35196	35196															
		11.58	11.88	0.30	1584	40	7040	7040															
		Structure MOT-4-5.87 (SFN 5700221)																					
		Structure MOT-4-6.77 (SFN 5700256)								83		73	36	293									
		Structure MOT-4-7.77 (SFN 5700310)																					
		Structure MOT-4-8.40 (SFN 5700345)																					
		Structure MOT-4-9.87 (SFN 5700361)																					
		Structure MOT-4-10.06 (SFN 5700396)								78	3	48	30	160									
		Structure MOT-4-10.46 (SFN 5700426)								64		39	32	139									
MOT-40	02/STR/PV	0.00	5.64	5.64	29779	30	99263	99263															
		Structure MOT-40-0.66 (SFN 5703131)																					
		Structure MOT-40-2.33 (SFN 5703190)																					
MOT-40	03/S<2/PV	5.67	6.22	0.55	2904	30	9680	9680															
		Skip SR49 (SLM 5.64-5.67)																					
		Extra Area for EB 40 to SB 49 Turn Lane										1157											
MOT-725	01/NHS/PV	8.31	10.13	1.82	9610	25	26694	26694															

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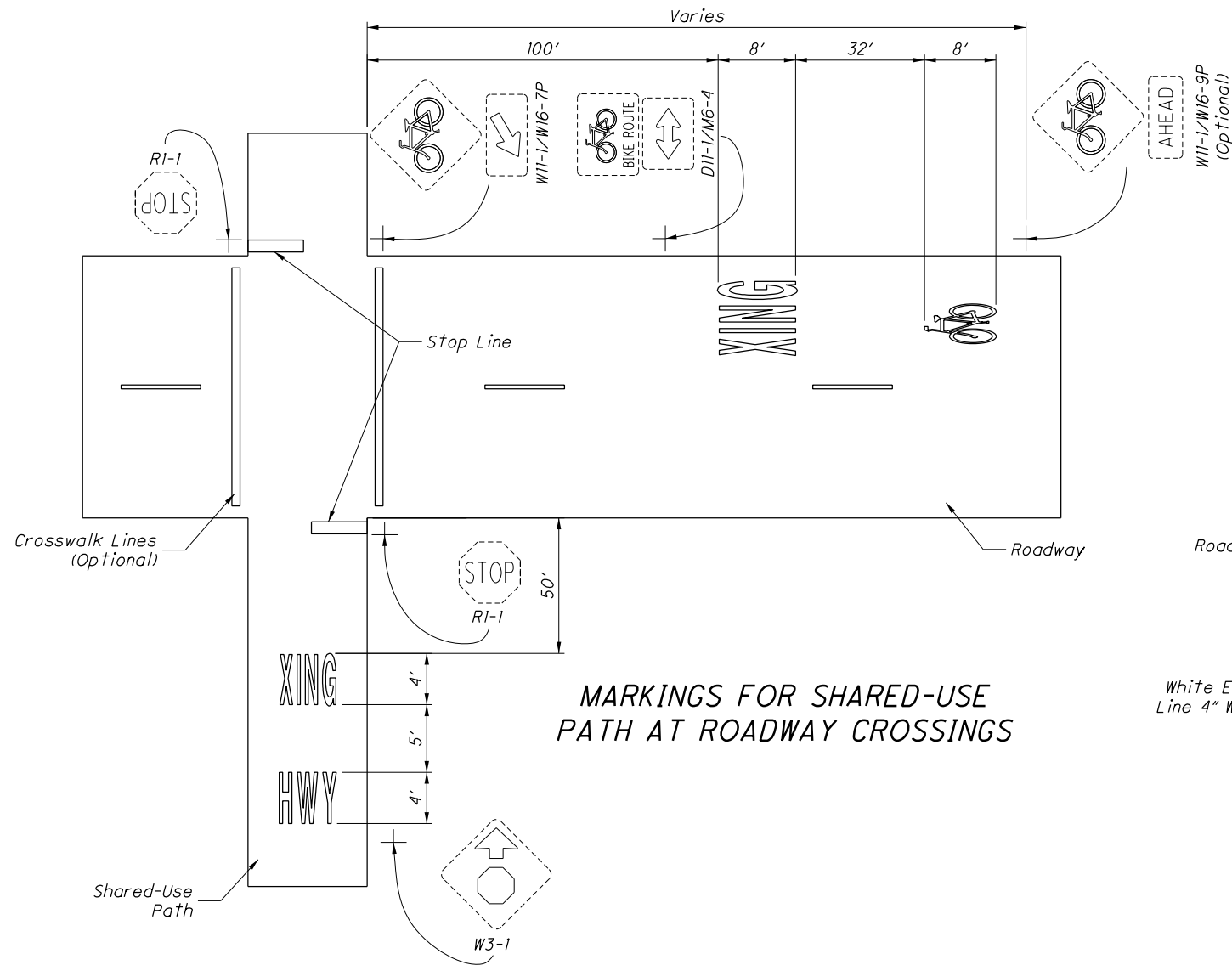
RAISE PAVEMENT MARKER DETAIL No.:

- 1 - TYPICAL SPACING/MULTILANE DIVIDED
- 2 - TAPERED ACCELERATION LANE
- 3 - DECELERATION LANE
- 4 - PARALLEL ACCELERATION LANE
- 5 - MULTILANE DIVIDED/EXPRESSWAY
- 6 - STOP APPROACH
- 7 - 1-LANE APPROACH W/ LEFT TURN LANE
- 8 - THRU APPROACH
- 9 - 2-LANE APPROACH W/ LEFT TURN LANE
- 10 - 4-LANE DIVIDED TO 2 LANE TRANSITION
- 11 - 4-LANE UNDIVIDED TO 2 LANE TRANSITION
- 12 - 2-LANE NARROW BRIDGE
- 13 - TWO-WAY LEFT TURN LANE
- 14 - 1-LANE BRIDGE
- 15 - HORIZONTAL CURVE
- 16 - HORIZONTAL CURVE, ALT.
- 17 - STOP APPROACH, ALT.

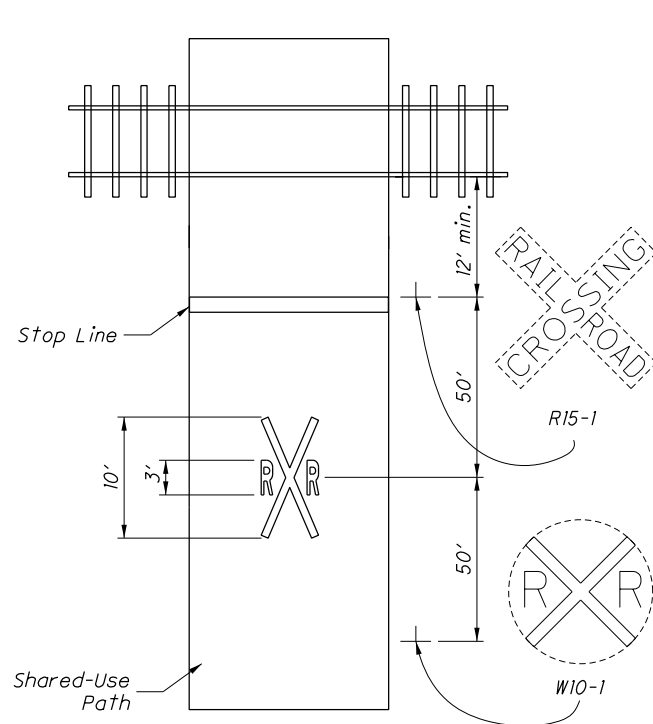
GAP - CENTERLINE AT 80 FT (TYPICAL)

TRAFFIC CONTROL SUBSUMMARY

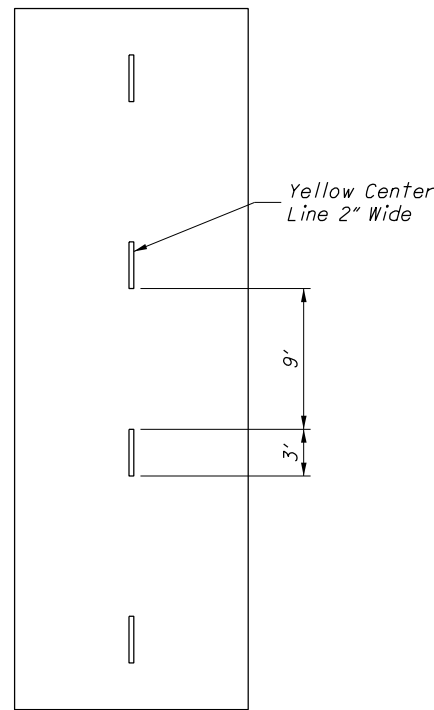
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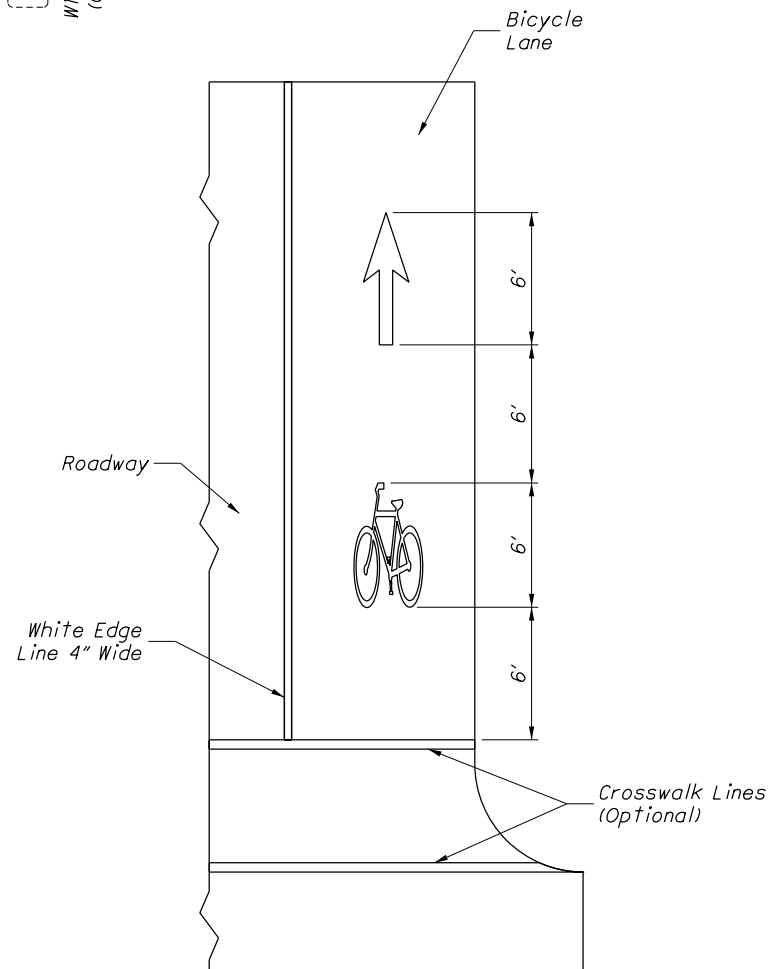
MARKINGS FOR SHARED-USE
PATH AT ROADWAY CROSSINGS



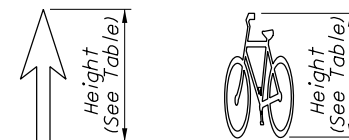
MARKINGS FOR SHARED-USE
PATH AT RAILROAD CROSSINGS



CENTER LINE MARKINGS
FOR SHARED-USE PATH



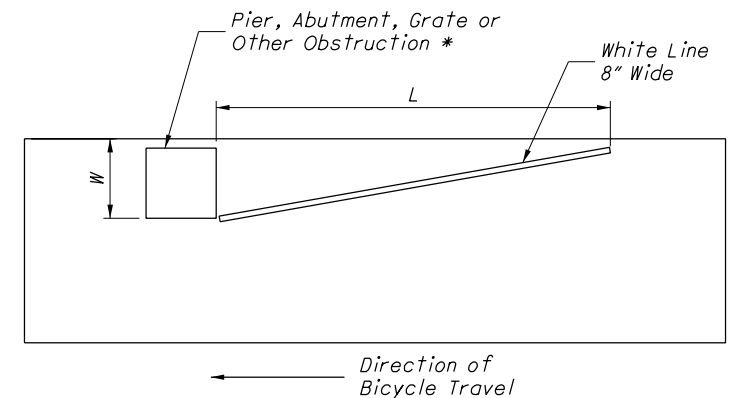
MARKINGS FOR BICYCLE LANE
(See Note 4)



FACILITY TYPE	HEIGHT	STRAIGHT ARROW	BIKE SYMBOL	HWY	XING	RXR SYMBOL
	Feet					
SHARED-USE PATH	4	4	3	5	6	----
BICYCLE LANE	6	6	5	----	16	----
ROADWAY- URBAN	6	5	5	----	16	----
ROADWAY- RURAL	8	7	7	----	21	----

NOTES:

1. Edge lines are optional on the shared-use path. If used, the edge line should be 2" wide.
2. Center line is optional on the shared-use path.
3. Arrows, words and symbol markings are optional on the shared-use path.
4. Green colored pavement may be used for bike lanes if called for in the plans.



TYPICAL MARKINGS AROUND
AN OBSTRUCTION

$L \text{ (ft)} = WS$

W - Width in feet
S - Bicycle Approach Speed
in Miles per Hour

* Provide an Additional Foot of
Offset for a Raised Obstruction
and Use the Formula $L=(W+1)S$ for
the Taper Length.

THIS DRAWING REPLACES PLS 207000 DATED 01-18-2013.

MOT - MICRO - FY2017

BIKEWAY PAVEMENT MARKING DETAILS

PLAN INSERT SHEET

OFFICE OF
ROADWAY
ENGINEERING

DESIGNED
XXX
REVIEWED
XXX
CHECKED
XXX

1/1