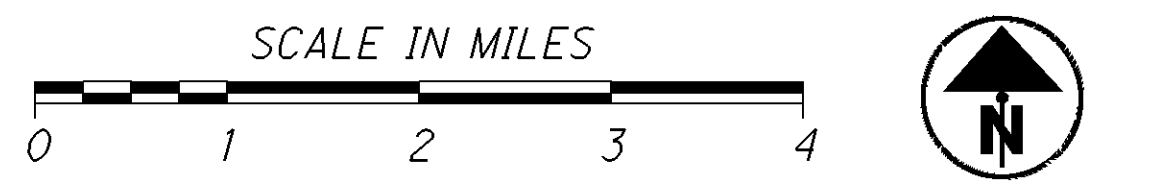
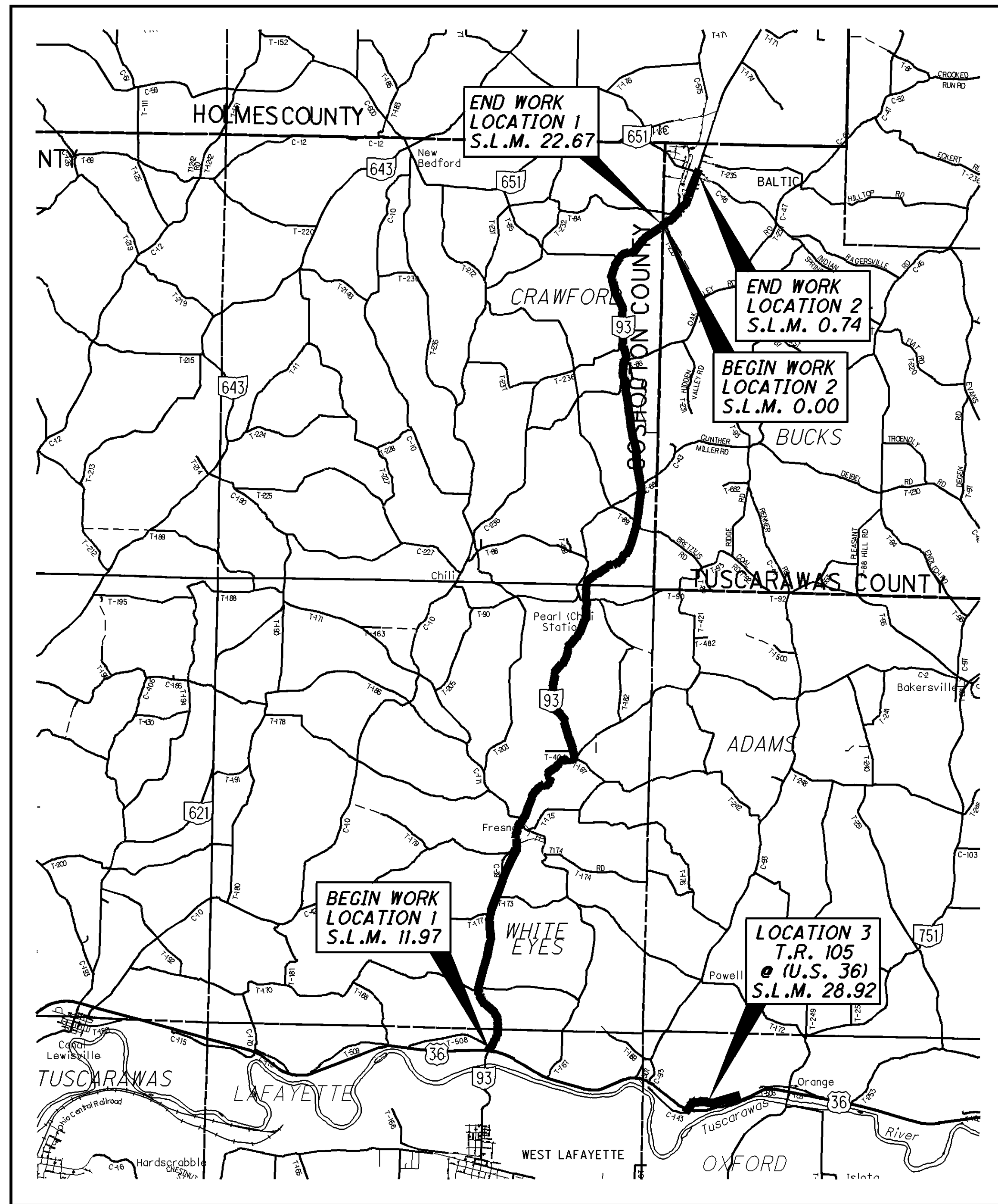


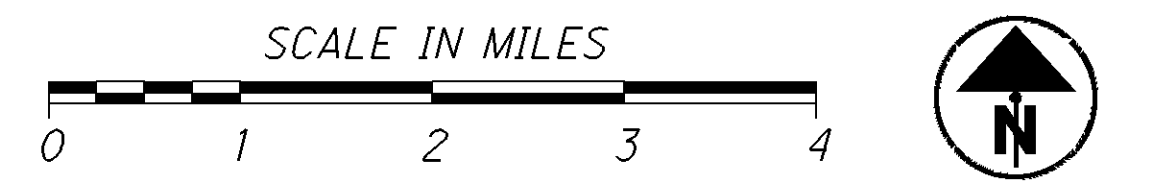
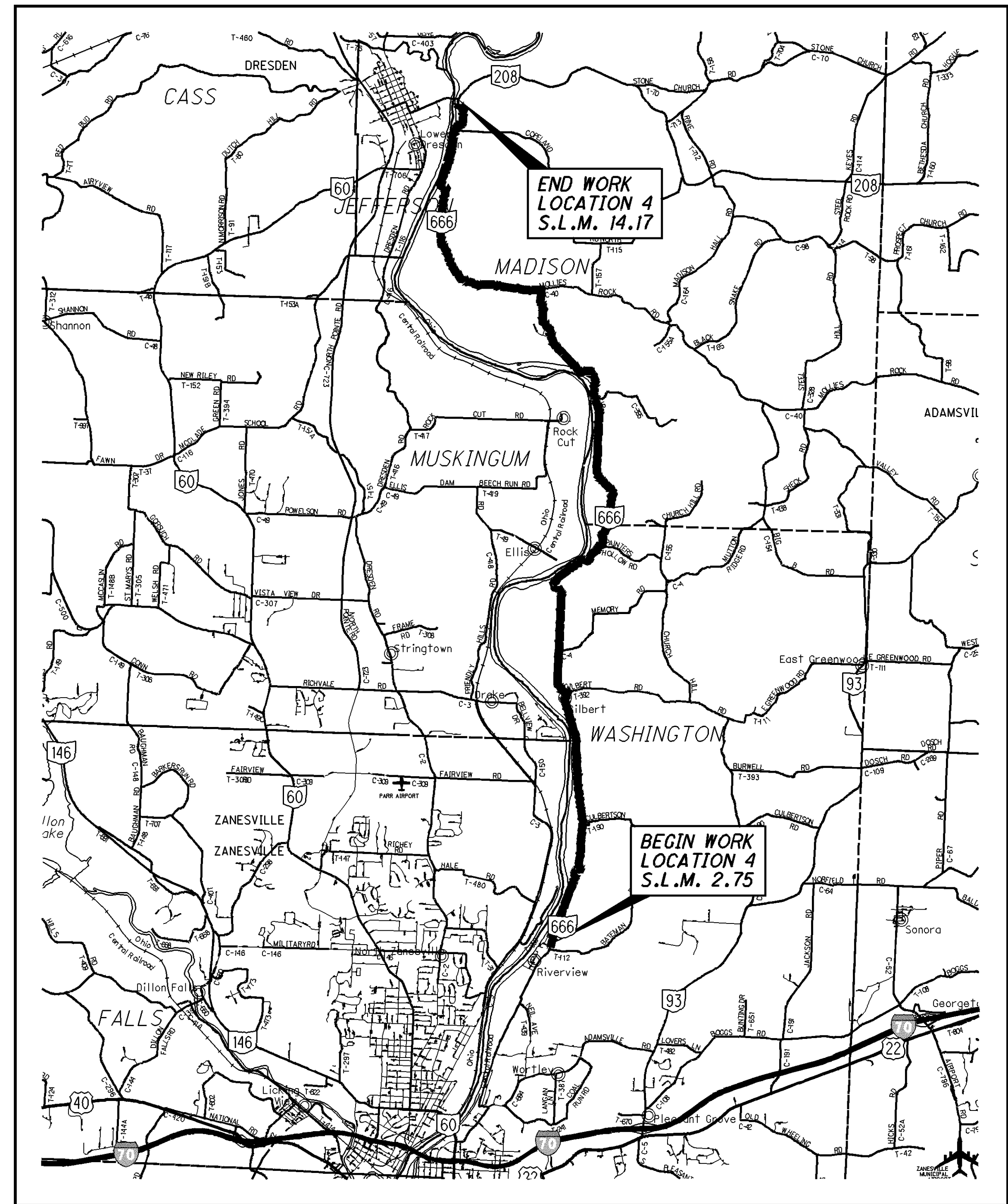


LOCATION MAP COSHOCTON COUNTY  
 LATITUDE: 40°22'56" N LONGITUDE: 81°42'57" W



PORTION TO BE IMPROVED .....  
 INTERSTATE & DIVIDED HIGHWAY .....  
 UNDIVIDED STATE & FEDERAL ROUTES .....  
 OTHER ROADS .....

LOCATION MAP MUSKINGUM COUNTY  
 LATITUDE: 40°03'18" N LONGITUDE: 81°57'59" W



PORTION TO BE IMPROVED .....  
 INTERSTATE & DIVIDED HIGHWAY .....  
 UNDIVIDED STATE & FEDERAL ROUTES .....  
 OTHER ROADS .....

CALCULATED  
 JLS  
 CHECKED  
 DNM

LOCATION MAPS

COS-93-11.97 (TUS)  
 COS-T.R. 105-0.00  
 MUS-666-2.75

**UTILITIES**

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

**ITEM 253, PAVEMENT REPAIR, AS PER PLAN**

AN ESTIMATED QUANTITY FOR PAVEMENT REPAIR HAS BEEN INCLUDED IN THE PLAN TO BE USED AS DIRECTED BY THE ENGINEER. THE INTENT OF THIS OPERATION IS TO REPAIR THOSE AREAS OF PAVEMENT WHICH HAVE COMPLETELY FAILED (PUMPING OF SUBBASE MATERIAL) AND NOT TO CORRECT SURFACE IRREGULARITIES.

THE ESTIMATED PAVEMENT REPAIR AREAS SHALL BE 4' (MINIMUM) IN WIDTH AND 7" IN DEPTH.

AFTER EXCAVATION HAS BEEN COMPLETED, THE FACE OF THE REPAIR SHALL BE COATED WITH 407 TACK COAT. REPLACEMENT MATERIAL WILL BE 6" OF ITEM 301, ASPHALT CONCRETE BASE, PG64-22 (PLACED AND COMPACTED AS DIRECTED) AND 1" OF ITEM 448, ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22.

ALL EXCAVATION, MATERIALS, LABOR, EQUIPMENT, TOOLS, MAINTENANCE OF TRAFFIC AND INCIDENTALS NEEDED TO COMPLETE THE WORK DESCRIBED ABOVE, SHALL BE PAID FOR UNDER ITEM 253, PAVEMENT REPAIR, AS PER PLAN.

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR THE ABOVE DESCRIBED PURPOSE.

LOCATION 1 - 7,500 SQ. YD.  
LOCATION 2 - 500 SQ. YD.  
LOCATION 4 - 10,000 SQ. YD.

**LOCATION 3: T.R. 105 RESURFACING**

THE FOLLOWING ESTIMATED QUANTITIES ARE TO BE USED AS DIRECTED BY THE ENGINEER TO RESTORE T.R. 105.

14' (PAVEMENT WIDTH) x 1,000' (LENGTH)  
 $(14 \times 1,000) \div 9 = 1,555.6$  SQ. YD. (TOTAL PAVEMENT AREA)

ITEM 448, 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22.  
 $1,555.6 \times (1.25 \div 36) = 54.1$  CU. YD.

ITEM 448, 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-22.  
 $1,555.6 \times (0.75 \div 36) = 32.5$  CU. YD.

ITEM 407, TACK COAT  
 $1,555.6 \times 0.075 = 116.7$  GAL.

ITEM 407, TACK COAT FOR INTERMEDIATE COURSE  
 $1,555.6 \times 0.050 = 77.8$  GAL.

**ITEM 254 PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN**

DEPTH OF PLANING SHALL BE AS SHOWN ON SHEET 5 OR AS DIRECTED BY THE ENGINEER. THE ROADWAY SHALL BE PLANED FULL WIDTH SUCH THAT POSITIVE DRAINAGE IS CREATED FROM THE LANE LINE TO THE EDGE OF PAVEMENT IN TANGENT SECTIONS AND SHALL FOLLOW EXISTING SUPERELEVATIONS WHERE APPLICABLE. ALL REQUIREMENTS OF ITEM 254 SHALL APPLY.

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THE ABOVE DESCRIBED PURPOSE.

LOCATION 4 - 44,444 SQ. YD.

**SPOT LEVELING**

QUANTITIES AND DETAILS, AS SHOWN ON SHEET 5, HAVE BEEN PROVIDED, TO BE USED AS DIRECTED BY THE ENGINEER, TO RESTORE THE ROADWAY CROWN/PROFILE OF S.R. 666.

**ITEM 407, TACK COAT**

THE RATE OF APPLICATION OF THE 407 TACK COAT SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. PLAN QUANTITIES INDICATE AN AVERAGE APPLICATION RATE OF 0.075 GALLONS PER SQUARE YARD FOR ESTIMATING PURPOSES ONLY.

**ITEM 407, TACK COAT FOR INTERMEDIATE COURSE**

THE RATE OF APPLICATION OF THE 407 TACK COAT FOR INTERMEDIATE COURSE SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. PLAN QUANTITIES INDICATE AN AVERAGE APPLICATION RATE OF 0.050 GALLONS PER SQUARE YARD FOR ESTIMATING PURPOSES ONLY.

**ITEM 621, RPM AND ITEM 621, RAISED PAVEMENT MARKER REMOVED**

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED IN THE PLANS TO REMOVE RAISED PAVEMENT MARKERS FOR DISPOSAL BY THE CONTRACTOR. RPM REMOVAL SHALL NOT OCCUR SOONER THAN 10 DAYS PRIOR TO RESURFACING OF THE ROADWAY. ALL RPM'S REMOVED SHALL BECOME PROPERTY OF THE CONTRACTOR.

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR THE ABOVE DESCRIBED PURPOSE.

LOCATION 1:  
ITEM 621, RAISED PAVEMENT MARKER REMOVED - 10 EACH

LOCATION 4:  
ITEM 621, RAISED PAVEMENT MARKER REMOVED - 200 EACH

THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED BELOW TO REPLACE THE RAISED PAVEMENT MARKER REMOVED, AS NOTED ABOVE, AND HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

LOCATION 1:  
ITEM 621, RPM - 10 EACH

LOCATION 4:  
ITEM 621, RPM - 200 EACH

**ITEM 642, PAVEMENT MARKINGS**

THE FOLLOWING QUANTITIES SHALL BE USED TO REPLACE CENTER LINES AND EDGE LINES THROUGHOUT THE LIMITS OF THE PROJECT.

LOCATION 1:  
ITEM 642, EDGE LINE, TYPE 1 - 4.25 MILE  
ITEM 642, CENTER LINE, TYPE 1 - 0.25 MILE

LOCATION 2:  
ITEM 642, EDGE LINE, TYPE 1 - 0.30 MILE

LOCATION 4:  
ITEM 642, EDGE LINE, TYPE 1 - 5.68 MILE  
ITEM 642, CENTER LINE, TYPE 1 - 3.58 MILE

**AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS**

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED A HEIGHT OF 202 FT. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA), AND ODOT OFFICE OF AVIATION, WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. THE CONTRACTOR WILL BE REQUIRED TO SUBMIT FORM 7460-1 TO THE FAA. A COPY OF THE SUBMISSION AND TWO COPIES OF FORM 7460-1 SHALL BE FORWARDED TO THE ODOT OFFICE OF AVIATION.

NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL A COPY OF THE FAA APPROVAL AND ODOT OFFICE OF AVIATION PERMIT HAS BEEN FURNISHED TO THE PROJECT ENGINEER.

Express Processing Center  
The Federal Aviation Administration  
Southwest Regional Office  
Air Traffic Airspace Branch ASW-520  
2601 Meacham Blvd.  
Fort Worth, TX 76137-4298

Ohio Department of Transportation  
Office of Aviation  
2829 West Dublin-Granville Road  
Columbus, Ohio 43235  
614.387.2346

**ITEM 614, MAINTAINING TRAFFIC (AT ALL TIMES)**

A MINIMUM OF 1 LANE OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT AND FLAGGERS.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT, IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

AREAS THAT ARE PLANED SHALL NOT BE OPENED TO TRAFFIC. ALL PLANED AREAS MUST BE INLAID WITH PROPOSED COURSE OF ASPHALT CONCRETE PRIOR TO BEING OPENED TO TRAFFIC. OVERNIGHT CLOSURES MUST MEET SPECIFICATIONS AS OUTLINED IN THE CONSTRUCTION AND MAINTENANCE OPERATIONS SECTION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS. ROADWAY SHALL NOT BE OPENED TO TRAFFIC WITHOUT EITHER THE PERMANENT OR WORK ZONE MARKINGS IN PLACE.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

**DROPOFFS IN WORK ZONES**

DROPOFFS THAT DEVELOP DURING CONSTRUCTION OPERATIONS SHALL BE TREATED AS PER STD. CONST. DWG. MT-101.90. WHERE THE PLANS DO NOT PROVIDE SPECIFIC ITEMS FOR LABOR, EQUIPMENT, OR MATERIALS TO IMPLEMENT THE DROP-OFF TREATMENTS SPECIFIED, THEY SHALL BE INCLUDED FOR PAYMENT IN THE LUMP SUM BID FOR ITEM 614, MAINTAINING TRAFFIC.

CALCULATED  
JLS  
CHECKED  
DNM

GENERAL NOTES

COS-93-11-97 (TUS)  
COS-T.R. 105-0.00  
MUS-666-2.75

3  
6

81223\_MGN\_001.DGN 2/09/09

**ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR**

IN ADDITION TO THE REQUIREMENTS OF 614 AND THE LATEST EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED LAW ENFORCEMENT OFFICER AND OFFICIAL PATROL CAR WITH WORKING TOP MOUNTED EMERGENCY FLASHING LIGHTS WILL BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS:

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED IN THIS NOTE WILL NOT GENERALLY BE PERMITTED AT PROJECT COST UNLESS PRIOR APPROVAL HAS BEEN OBTAINED FROM THE ENGINEER. LEOS SHOULD NOT BE USED WHERE THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD) INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS:

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED. IN GENERAL, LEOS SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH A LIST OF THE APPROPRIATE LAW ENFORCEMENT AGENCY(S), INCLUDING ADDRESS AND TELEPHONE NUMBER.

THE LEO SHOULD REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING THE SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF THE SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHOULD NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF THE SHIFT.

LAW ENFORCEMENT OFFICERS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY SHEET 6.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR 150 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR.

**ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN**

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN, ON SITE, FOR THE DURATION OF THE PROJECT. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS MAINTAINED BY THE DIRECTOR (OFFICE OF MATERIALS MANAGEMENT). THE APPROVED LIST OF PORTABLE CHANGEABLE MESSAGE SIGNS CAN BE FOUND ON THE ODOT WEBSITE BY CLICKING ON THE SERVICES MENU, THEN CLICKING ON MATERIALS MANAGEMENT. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 650 FT. AND 475 FT., RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. PCMS TRAILERS SHOULD BE DELINEATED ON A PERMANENT BASIS BY AFFIXING RETROREFLECTIVE MATERIAL, IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER AS SEEN BY ONCOMING ROAD USERS.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET(S) OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED,

FACING AWAY FROM ALL TRAFFIC, AND SHALL DISPLAY ONE OR MORE TYPE G YELLOW RETROREFLECTIVE SHEETING SURFACES OF 9-INCH BY 15-INCH MINIMUM SIZE FACING TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

(THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN 2 HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.)

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.)

**ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN(cont'd)**

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF CMS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

A TOTAL OF 2 PCMS SHALL BE REQUIRED FOR THIS PROJECT.

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO GENERAL SUMMARY SHEET 6:

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 4 SIGN-MONTH

**NOTIFICATION OF ROAD CLOSURE OR RESTRICTION**

IN ORDER FOR ODOT TO PROPERLY PERMIT OVERSIZE LOADS, PREPAR PROPER SIGNING WHEN REQUIRED AND FURTHER TO NOTIFY THE GENERAL MOTORING PUBLIC, THE CONTRACTOR SHALL NOTIFY (IN WRITING) THE DISTRICT 5 HIGHWAY MANAGEMENT ADMINISTRATOR WITH COPIES FOR THE DISTRICT 5 ROADWAY SERVICES MANAGER AND PROJECT ENGINEER NOT LESS THAN 21 DAYS BEFORE SUCH CLOSURE OR LANE RESTRICTIONS.

SEND NOTIFICATION TO:

DISTRICT 5 HIGHWAY MANAGEMENT ADMINISTRATOR  
P.O. BOX 306  
JACKSONSTOWN, OH 43030  
PHONE: 740.323.4400 EXT. 5241

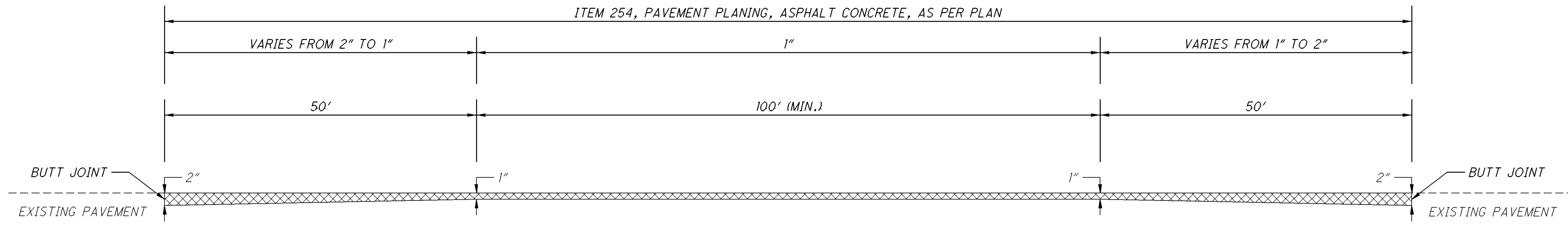
CALCULATED  
JLS  
CHECKED  
DNM

GENERAL NOTES

COS-93-11.97 (TUS)  
COS-T.R.105-0.00  
MUS-666-2.75

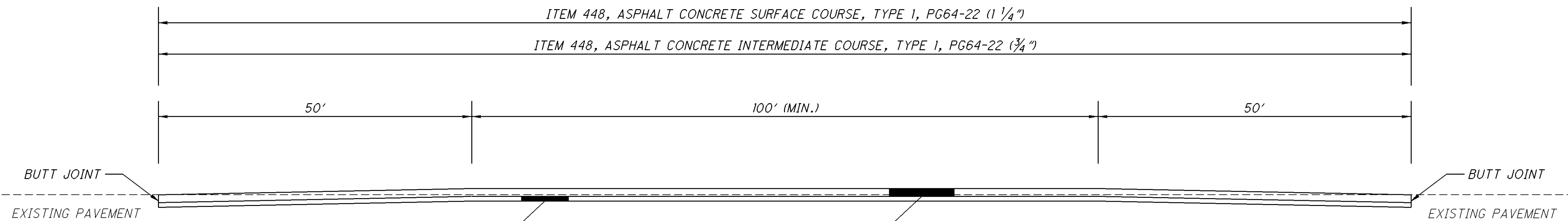
4  
6

**REMOVAL DETAIL**  
S.R. 666



@ LOCATIONS AS DIRECTED BY THE ENGINEER.  
20' (PAVEMENT WIDTH) x 200' (MINIMUM LENGTH)

**PROPOSED DETAIL**  
S.R. 666



ITEM 448, ASPHALT CONCRETE  
INTERMEDIATE COURSE

ITEM 448, ASPHALT CONCRETE  
SURFACE COURSE

ITEM 448, ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22 (1 1/4")  
 $44,444 \times (1.25 \div 36) = 1543.19$  CU. YD.

ITEM 448, ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-22 (3/4")  
 $44,444 \times (0.75 \div 36) = 925.92$  CU. YD.

ITEM 407, TACK COAT  
 $44,444 \times 0.075 = 3,333.3$  GAL.

ITEM 407, TACK COAT FOR INTERMEDIATE COURSE  
 $44,444 \times 0.050 = 2,222.20$  GAL.

100 LOCATIONS AS DIRECTED BY THE ENGINEER.  
20' (PAVEMENT WIDTH) x 200' (MINIMUM LENGTH) = 4000 SQ. FT.  
(TOTAL PAVEMENT AREA - (100 x 4,000.0 SQ. FT.)/9 = 44,444 SQ. YD.)

ALL QUANTITIES CARRIED TO THE GENERAL SUMMARY.

CALCULATED  
JLS  
CHECKED  
DNM

PLAN DETAIL SHEET (SPOT LEVELING)

COS-93-11.97 (TUS)  
COS-T.R.105-0.00  
MUS-666-2.75

5  
6

LOCATION 1		LOCATION 2		LOCATION 3		LOCATION 4			80% FED./ 20% STATE	100% STATE	ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET
SHT. 3	SHT. 4	SHT. 3	SHT. 4	SHT. 3	SHT. 4	SHT. 3	SHT. 4	SHT. 5								
7,500		500				10,000			8,000	10,000	253	01001	18,000	SQ YD	PAVEMENT REPAIR, AS PER PLAN	3
						44,444				44,444	254	01001	44,444	SQ YD	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN	3
				117				3,334		3,451	407	10000	3,451	GALLON	TACK COAT	
				78				2,223		2,301	407	14000	2,301	GALLON	TACK COAT FOR INTERMEDIATE COURSE	
				33				926		959	448	46020	959	CU YD	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-22	
				55				1,544		1,599	448	47020	1,599	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22	
	75						75		75	75	614	11100	150	hour	LAW ENFORCEMENT OFFICER WITH PATROL CAR	
	2						2		2	2	614	18601	4	SIGN MNTH	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	4
10						200			10	200	621	00100	210	EACH	RPM	
10						200			10	200	621	54000	210	EACH	RAISED PAVEMENT MARKER REMOVED	
4.55	0.30					5.68			4.85	5.68	642	00100	10.53	MILE	EDGE LINE, TYPE 1	
0.25						3.58			0.25	3.58	642	00300	3.83	MILE	CENTER LINE, TYPE 1	
											103	05000	LUMP		PREMIUM FOR CONTRACT PERFORMANCE BOND AND FOR PAYMENT BOND	
											614	11000	LUMP		MAINTAINING TRAFFIC	
											623	10000	LUMP		CONSTRUCTION LAYOUT STAKES	
											624	10000	LUMP		MOBILIZATION	

CALCULATED  
JLS  
CHECKED  
DNM

**GENERAL SUMMARY**

COS-93-11.97 (TUS)  
COS-T.R. 105-0.00  
MUS-666-2.75

6  
6