

| ISSUE RECORD | | DESCRIPTION | |
|--------------|----------|------------------------|---|
| NO. | DATE | Updated C&MS Reference | Updated Notes for Work Zone Pavement Markings |
| 1 | 01/30/26 | | |
| 2 | 02/10/26 | | |

TRAFFIC INCIDENT MANAGEMENT (TIM) DURING MOT

OHIO TIM IS OHIO'S TRAFFIC INCIDENT MANAGEMENT PROGRAM WHICH IS COMMITTED TO MAINTAINING THE SAFE AND EFFECTIVE FLOW OF TRAFFIC DURING EMERGENCIES AS TO PREVENT FURTHER DAMAGE, INJURY OR UNDUE DELAY OF THE MOTORING PUBLIC. IN ADDITION TO COMPLYING WITH THE PROVISION OF OMUTCD CHAPTER 6I, CONTROL OF TRAFFIC THROUGH TRAFFIC INCIDENT MANAGEMENT AREAS, THE CONTRACTOR SHALL ACTIVELY PARTICIPATE IN TIM PLANNING AND IMPLEMENTATION AS OUTLINED BELOW.

1.

SUPERINTENDENT SHALL IDENTIFY THE INDIVIDUAL PERSONS ON THE PROJECT WHO WILL, OR MAY NEED TO, PERFORM THE DUTIES HEREIN. AT A MINIMUM, INCLUDE THE SUPERINTENDENT, FOREMEN AND SUPERVISORS (OR EQUIVALENT) AS WELL AS THE WORKSITE TRAFFIC SUPERVISOR (WTS; IF APPLICABLE TO THE PROJECT). THESE INDIVIDUALLY IDENTIFIED PERSONS SHALL COLLECTIVELY BE KNOWN AS CONTRACTOR TRAFFIC INCIDENT MANAGEMENT (TIM) CONTACTS. NOTIFY THE PROJECT ENGINEER OF THE CONTRACTOR TIM CONTACTS (ALONG WITH CONTACT INFORMATION FOR EACH) AT OR BEFORE THE PRECONSTRUCTION MEETING.
2.

SUPERINTENDENT SHALL NOTIFY THE ENGINEER IMMEDIATELY IF ANY CONTRACTOR TIM CONTACT IS ADDED, REMOVED OR THE CONTACT INFORMATION CHANGES OVER THE COURSE OF THE PROJECT.
3.

PRIOR THE FIRST DAY OF WORK IN THE FIELD, EACH CONTRACTOR TIM CONTACT ON THE PROJECT SHALL HAVE ATTENDED AND SUCCESSFULLY COMPLETED OHIO TIM TRAINING PROVIDED BY THE DEPARTMENT OR DESIGNEE. TRAINING INFORMATION CAN BE FOUND AT WWW.OHIOTIM.COM.
4.

SUPERINTENDENT, AT A MINIMUM, SHALL ATTEND AND ACTIVELY PARTICIPATE IN A DEPARTMENT SCHEDULED TIM MEETING BEFORE CONSTRUCTION WORK BEGINS AND BEFORE EACH PHASE CHANGE. THESE MEETINGS WILL RESULT IN A DEPARTMENT ISSUED PROJECT SPECIFIC TRAFFIC INCIDENT MANAGEMENT PLAN (TIMP). AT THE TIM MEETINGS THE ATTENDING CONTRACTOR TIM CONTACTS SHALL:

A.

COLLABORATE WITH ODOT AND SAFETY FORCES;

B.

SHARE PROJECT SPECIFIC DETAILS THAT IMPACT TIM RESPONDERS; AND

C.

RECOMMEND WAYS TO INCORPORATE NECESSARY EMERGENCY ACCESS AND OTHER TIM ELEMENTS FOR TIM RESPONDERS GIVEN PROJECT SPECIFIC WORK BEING COMPLETED AND PROJECT SPECIFIC PHASING.
5.

CONTRACTOR TIM CONTACTS SHALL IMPLEMENT COMPONENTS OF THE RESULTING TIMP (SUCH AS APPROVED EMERGENCY INGRESS/EGRESS POINTS, ETC), AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH 109.05.

TRAFFIC INCIDENT MANAGEMENT (TIM) DURING MOT (CONT'D)

6.

CONTRACTOR TIM CONTACTS SHALL PERFORM, AT A MINIMUM, THE FOLLOWING FUNCTIONS WHEN AN INCIDENT/CRASH OCCURS:

A.

IF OBSERVED OR PRESENT WHEN OCCURS, CALL 911 AND THEN NOTIFY THE TRAFFIC MANAGEMENT CENTER (TMC) TO PROVIDE THE FOLLOWING:

I.

LOCATION, INCLUDING MILEPOST NUMBER AND DIRECTION OF TRAVEL

II.

NUMBER AND TYPE OF VEHICLES INVOLVED, IF KNOWN

III.

ESTIMATED EXTENT OF DAMAGE OR INJURY, IF KNOWN

IV.

ESTIMATED NUMBER OF PATIENTS INVOLVED, IF KNOWN

V.

ANY POTENTIAL HAZARDOUS CONDITIONS, IF KNOWN

VI.

THE PLACARD NUMBER ON ANY HAZARDOUS MATERIALS PLACARD FROM A SAFE DISTANCE, IF APPLICABLE AND VISIBLE

B.

FOLLOWING AN INCIDENT/CRASH:

I.

INITIATE TRAFFIC MANAGEMENT/PROVIDE TEMPORARY TRAFFIC CONTROL AS INDICATED IN THE TIMP, AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH 109.05.

II.

RECOMMEND ROADWAY REPAIR NEEDS.

III.

PROVIDE REPAIR RESOURCES AND INITIATE REPAIRS, AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH 109.05.

IV.

ATTEND AND PARTICIPATE IN AN AFTER ACTION REVIEW (AAR).

ALL COSTS, UNLESS OTHERWISE SPECIFIED, RESULTING FROM THE ABOVE REQUIREMENTS SHALL BE CONSIDERED TO BE INCLUDED IN THE LUMP SUM PRICE FOR ITEM 614, MAINTAINING TRAFFIC. FAILURE TO PERFORM THE REQUIREMENTS OF THIS PLAN NOTE WILL RESULT IN A DAILY FINE OF 2% OF ITEM 614, MAINTAINING TRAFFIC AND MAY RESULT IN ONE OR MORE CONTRACTOR TIM CONTACTS BEING REMOVED FROM THE LIST OF OHIO TIM TRAINED INDIVIDUALS (AT THE SOLE DISCRETION OF THE OHIO TIM EXECUTIVE COMMITTEE). IN THE EVENT AN INDIVIDUAL IS REMOVED FROM THE OHIO TIM TRAINED LIST, THE INDIVIDUAL WILL BE REMOVED FROM CONTRACTOR TIM CONTACT RESPONSIBILITIES ON ALL PROJECTS.

DELINEATION OF PORTABLE AND PERMANENT BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL; AND, ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AT APPROXIMATELY 10-FOOT INTERVALS. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70 WITH THE MODIFICATION THAT OBJECT MARKER SPACING SHALL BE AT APPROXIMATELY 25-FOOT INTERVALS. WHEN THE PB OR PERMANENT BARRIER (INCLUDING BRIDGE PARAPETS) CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

THE FOLLOWING ITEMS HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

ITEM 614, BARRIER REFLECTOR, TYPE 1 (ONE-WAY)

ITEM 614, OBJECT MARKER, TWO-WAY

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE ABOVE ITEMS.

DELINEATION OF TEMPORARY AND PERMANENT GUARDRAIL

BARRIER REFLECTORS SHALL BE INSTALLED ON ALL TEMPORARY GUARDRAIL USED FOR TRAFFIC CONTROL; AND, ON ALL PERMANENT GUARDRAIL LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE. BARRIER REFLECTORS SHALL CONFORM TO C&MS 626 AND THE SPACING SHALL BE AT APPROXIMATELY 10-FOOT INTERVALS.

OBJECT MARKERS SHALL BE INSTALLED ON ALL TEMPORARY AND PERMANENT GUARDRAIL LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE. GUARDRAIL-MOUNTING OF OBJECT MARKERS SHALL BE MADE BY INSTALLING THE OBJECT MARKERS ON THE EXTENSION BLOCKS RATHER THAN DIRECTLY ONTO THE GUARDRAIL ITSELF. OBJECT MARKERS SHALL CONFORM TO C&MS 614.03 AND THE SPACING SHALL BE AT APPROXIMATELY 25-FOOT INTERVALS.]

THE FOLLOWING ITEMS HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

ITEM 614, BARRIER REFLECTOR, TYPE 2 (ONE-WAY)

ITEM 614, OBJECT MARKER, ONE-WAY

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE ABOVE ITEM(S).

WORK ZONE DELINEATION FOR CROSSOVERS AND LANE SHIFTS

THE SPACING OF RAISED PAVEMENT MARKERS PROVIDED FOR WORK ZONE DELINEATION AS INDICATED PER SCD MT-99.30 SHALL BE MODIFIED FOR THIS PROJECT. WITHIN THE LIMITS OF ALL TRANSITION AREA DELINEATION, RAISED PAVEMENT MARKER SPACING SHALL BE AT 10-FOOT INTERVALS. INCREASED QUANTITIES FOR ITEM 614, WORK ZONE RAISED PAVEMENT MARKER HAVE BEEN PROVIDED.

- ITEM 614, WORK ZONE PAVEMENT MARKING, MISC: LANE LINE, 6"

ITEM 614, WORK ZONE PAVEMENT MARKING, MISC: EDGE LINE, 6"

ITEM 614, WORK ZONE PAVEMENT MARKING, MISC: CHANNELIZING LINE, 12"

ITEM 614, WORK ZONE PAVEMENT MARKING, MISC: DOTTED LINE, 6"

2

FOR LONG TERM WORK ZONE PAVEMENT MARKINGS, THESE ITEMS SHALL MEET THE REQUIREMENTS OF ITEMS 614.11 AND 641 AND SHALL BE SPRAY THERMOPLASTIC AND MEET THE REQUIREMENTS OF 648 AND 740.

2

FOR SHORT TERM WORK ZONE PAVEMENT MARKINGS APPLIED DURING THE RESURFACING PROCESS, THESE ITEMS SHALL MEET THE REQUIREMENTS OF ITEMS 614.11 AND 641 AND SHALL BE CLASS III PAVEMENT MARKINGS AND MEET THE REQUIREMENTS OF 642 AND 740.

THE WORK ZONE PAVEMENT MARKINGS THAT WILL BE PLACED DURING RESURFACING ARE LISTED AS THE FOLLOWING ITEMS AND QUANTITIES AND HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614, WORK ZONE PAVEMENT MARKING, MISC: LANE LINE, 6"

1.47 MILE

ITEM 614, WORK ZONE PAVEMENT MARKING, MISC: EDGE LINE, 6"

4.56 MILE

ITEM 614, WORK ZONE PAVEMENT MARKING, MISC: CHANNELIZING LINE, 12"

6,788 FT

ITEM 614, WORK ZONE PAVEMENT MARKING, MISC: DOTTED LINE, 6"

1,069 FT

MAINTENANCE OF TRAFFIC GENERAL NOTES

| | |
|---------------|-------------|
| DESIGN AGENCY | |
| | |
| DESIGNER | MRC |
| REVIEWER | FR 08/27/25 |
| PROJECT ID | 121479 |
| SHEET | TOTAL |
| P.010 | 122 |

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW SHALL NOT BE PERMITTED AT PROJECT COST NOR TIME COMPENSATION. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING PERIODS WHERE TRAFFIC NEEDS TO BE DIRECTED CONTRARY TO A TRAFFIC CONTROL DEVICE (FLAGGER, SIGN [E.G. STOP SIGN, STREET OR HIGHWAY SIGNS, ETC], SIGNAL OR OTHER DEVICE USED TO REGULATE, WARN OR GUIDE TRAFFIC). TRAFFIC IN THIS INSTANCE INCLUDES VEHICULAR, PEDESTRIAN AND/OR SHARED USE PATH USERS.

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES THAT MEET ALL OF THE CRITERIA LISTED BELOW: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

CRITERIA

ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND

AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND,

AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS AND/OR IN CONTRARY TO OTHER TRAFFIC CONTROL DEVICE IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONT'D)

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE SHIFT DURATION SHALL NOT BE LESS THAN THE LEO'S MINIMUM SHOW-UP TIME REQUIRED BY THEIR LAW ENFORCEMENT AGENCY.THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 300 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

| NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE | | |
|---|------------------------|--|
| ITEM | DURATION OF CLOSURE | NOTICE DUE TO PERMITS & PIO |
| RAMP & ROAD CLOSURES | >=2 WEEKS | 21 CALENDAR DAYS PRIOR TO CLOSURE |
| | > 12 HOURS & < 2 WEEKS | 14 CALENDAR DAYS PRIOR TO CLOSURE |
| | <= 12 HOURS | 4 BUSINESS DAYS PRIOR TO CLOSURE |
| LANE CLOSURES & RESTRICTIONS | >=2 WEEKS | 14 CALENDAR DAYS PRIOR TO CLOSURE |
| | <2 WEEKS | 5 BUSINESS DAYS PRIOR TO CLOSURE |
| START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES | N/A | 14 CALENDAR DAYS PRIOR TO IMPLEMENTATION |

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

RESURFACING OF AREA IMPACTED BY MOT (LANES, PAVED SHOULDERS, AND RUMBLE STRIPS)

RESURFACING OF THE AREAS THAT ARE IMPACTED BY THE TEMPORARY PAVEMENT MARKINGS DUE TO MOT SHALL BE PERFORMED AT THE TIME THAT THE SURFACE COURSE IS BEING APPLIED TO THE ENTIRE PROJECT. PRIOR TO APPLICATION OF THE SURFACE COURSE ON THE PROJECT, THE EXISTING PAVEMENT WITHIN THE TRANSITION AREA SHALL BE REMOVED TO A DEPTH OF 1.5 INCHES. RESURFACING LIMITS FOR BOTH EASTBOUND AND WESTBOUND I.R. 277/U.S. 224 ARE AS FOLLOWS:

EASTBOUND: STA. 227+05 TO STA. 249+20
STA. 258+75 TO STA. 278+20
WESTBOUND: STA. 231+20 TO STA. 249+20
STA. 258+75 TO STA. 303+00

THE FOLLOWING ITEMS AND QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 254, PAVEMENT PLANING, ASPHALT CONCRETE (T=1.5") 65,556 SY

ITEM 442, ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (449) (T=1.5") 2,732 CY

ITEM 407, NON-TRACKING TACK COAT 5,245 GAL

ITEM 401, RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE) 24,055 FT

TEMPORARY DRAINAGE ITEMS

TEMPORARY DRAINAGE ITEMS LABELED ON THE MAINTENANCE OF TRAFFIC PLAN ARE ITEMIZED ON THE MOT PLANS AND CARRIED TO THE GENERAL SUMMARY.

EXISTING DRAINAGE SCUPPERS ON THE BRIDGE SHALL BE CLEANED AND UNCLOGGED PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITIES. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO PERFORM THE WORK AND SHALL BE PAID FOR UNDER ITEM 614, MAINTAINING TRAFFIC.

1

ITEM 611 - 15" CONDUIT, TYPE B, AS PER PLAN

THIS CONDUIT SHALL BE UTILIZED FOR TEMPORARY MAINTENANCE OF TRAFFIC OPERATIONS DURING CONSTRUCTION.

PAYMENT FOR THIS ITEM SHALL BE PAID FOR UNDER ITEM 611 - 15" CONDUIT, TYPE B, AS PER PLAN AND SHALL INCLUDE THE REMOVAL AND REPLACEMENT OF EXISTING PAVEMENT INCLUDING REQUIRED BACKFILL, THE PIPE FILL MATERIAL AS PER THE PLAN NOTE ITEM SPECIAL - FILL AND PLUG EXISTING CONDUIT, AND ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO PERFORM THE WORK.

2

PLANING EXISTING BRIDGE DECK

PLANING OF THE EXISTING BRIDGE DECK SHALL BE COMPLETED FOR MOT DRAINAGE PURPOSES AND FOLLOW THE REQUIREMENTS OF ITEM 254. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO PERFORM THE WORK AND SHALL BE PAID FOR UNDER ITEM 615, ROADS FOR MAINTAINING TRAFFIC.

1

DESIGN AGENCY



DESIGNER

MRC

REVIEWER

FR 08/27/25

PROJECT ID

121479

SHEET

P.011

TOTAL

122

| ISSUE RECORD | | DESCRIPTION | |
|--------------|----------|--|--|
| NO. | DATE | Updated Pre-Phase Sequence | |
| 1 | 01/30/26 | Added Notes for Filling Pipe and Ramp Closures | |
| 2 | 02/10/26 | | |

ITEM SPECIAL - FILL AND PLUG EXISTING CONDUIT

THIS ITEM CONSISTS OF THE CONSTRUCTION OF BULKHEADS IN THE EXISTING 15 INCH DIAMETER CONDUIT AND FILLING THE AREA SEALED OFF WITH ITEM 613, SAND OR OTHER MATERIAL APPROVED BY THE ENGINEER.

PUMP THE FILL MATERIAL INTO PLACE OR BY OTHER MEANS APPROVED BY THE ENGINEER, SO THAT AFTER SETTLEMENT, AT LEAST 90 PERCENT OF THE CROSS-SECTIONAL AREA OF THE CONDUIT, FOR ITS ENTIRE LENGTH IS FILLED. THE LENGTH OF FILLED AND PLUGGED CONDUIT TO BE PAID FOR IS THE ACTUAL NUMBER OF FEET (MEASURED ALONG THE CENTERLINE OF EACH CONDUIT FROM OUTER FACE TO OUTER FACE OF BULKHEADS) FILLED AND PLUGGED AS DESCRIBED ABOVE.

APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION

PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE APPROVED MOT EXCEPTION(S) PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

APPROVED MOT EXCEPTION(S) INCLUDE:
THE CONTRACTOR IS PERMITTED TWO SEPARATE CLOSURES OF THE RAMP FROM I-277EB TO I-77NB TO PERFORM MAINTENANCE OF TRAFFIC WORK. THE PERMITTED CLOSURES SHALL BE LIMITED FROM 8:00PM FRIDAY THROUGH 5:00AM THE FOLLOWING MONDAY.

A MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF 30 CALENDAR DAYS PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER AND AS WELL AS THE CONTRACTOR, WORKSITE TRAFFIC SUPERVISOR (WTS) AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.

IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO THE OFFICE OF ROADWAY ENGINEERING, STATEWIDE TMC, DWZTM AND SPECIAL HAULING PERMITS AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE. REFERENCE “EXCEPTION REQUEST APPROVAL DATED 02/10/2026 FOR PID 121479 IN THE NOTIFICATION AND OTHER CORRESPONDENCE.

ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTION(S) LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE MOT EXCEPTION COMMITTEE (MOTEC). IN THE EVENT THAT SUCH CHANGES ARE PROPOSED, THE REQUEST SHALL BE COORDINATED THROUGH THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM) A MINIMUM OF 30 CALENDAR DAYS PRIOR TO THE DESIRED IMPLEMENTATION DATE. IF THE DISTRICT AGREES WITH THE PROPOSED CHANGES THE DWZTM SHALL SEEK APPROVAL FROM THE MOTEC. IN THE EVENT THE PROPOSED CHANGES ARE APPROVED IN WRITING, THE CLOSURES ARE STILL SUBJECT TO NOTIFICATION REQUIREMENTS WITHIN THIS NOTE PRIOR TO IMPLEMENTATION.

SEQUENCE OF CONSTRUCTION

IT IS THE INTENT OF THE FOLLOWING SEQUENCE OF CONSTRUCTION TO PROVIDE A WORK AREA FOR THE CONTRACTOR WHILE ALSO MAINTAINING TRAFFIC IN A MANNER THAT IS SAFE FOR THE TRAVELING PUBLIC. NOT ALL WORK ITEMS ARE IDENTIFIED BELOW, AND THE SEQUENCE OF CONSTRUCTION DOES NOT SUPERSEDE ANY OTHER ELEMENT OF WORK WITHIN THESE PLANS. SOME WORK ELEMENTS CAN BE PERFORMED SIMULTANEOUSLY. ALL TEMPORARY OR PERMANENT PAVEMENT MARKINGS SHALL BE IN PLACE BEFORE ANY PAVEMENT IS OPENED TO TRAFFIC.

ALL EXISTING LANES, INCLUDING RAMPS, SHALL BE OPEN AND AVAILABLE TO TRAFFIC IN THE ORIGINAL OR PROPOSED FINAL ALIGNMENT BETWEEN OCTOBER 15 AND APRIL 1 IN THE EASTBOUND DIRECTION. PART WIDTH BRIDGE WORK IS ALLOWED YEAR-ROUND IN THE WESTBOUND DIRECTION. NO CONTRAFLOW TRAFFIC SHALL BE ALLOWED BETWEEN OCTOBER 15 AND APRIL 1.

PRE-PHASE

SET PRE-PHASE ON I.R. 277/U.S. 224 PER PLANS (SHEETS P.017 THROUGH P.022).

TRAFFIC:

SHIFT THE THREE (3) EB LANES TO THE OUTSIDE ALONG I.R. 277/U.S. 224 AS PER SCD MT-102.20. CLOSE INSIDE SHOULDER AND REMAINDER OF EXISTING INSIDE LANE. SHIFT THE THREE (3) WB LANES TO THE OUTSIDE ALONG I.R. 277/U.S. 224 AS PER SCD MT-102.20 UTILIZING TEMPORARY PAVEMENT. ALL RAMPS SHALL REMAIN OPEN.

CONSTRUCTION:

PRIOR TO THE CONSTRUCTION PRE-PHASE, ALL TEMPORARY PAVEMENT NORTH OF THE BRIDGE FOR WB I.R. 277/U.S. 224 SHALL BE COMPLETED AND EXISTING SCUPPERS SHALL BE CLEANED AND UNCLOGGED. DURING THE PRE-PHASE, THE REMAINING TEMPORARY PAVEMENT AND TEMPORARY DRAINAGE FEATURES SHOWN ON PLAN SHEETS P.018 THROUGH P.022 SHALL BE CONSTRUCTED. THIS INCLUDES PLANING THE NORTHERN BRIDGE DECK (SEE NOTE ON THE MOT TYPICAL SECTION ON P.025).

SEQUENCE OF CONSTRUCTION (CONT'D)

PHASE 1

SET PHASE 1 ON I.R. 277/U.S. 224 PER PLANS (SHEETS P.024 THROUGH P.030).

TRAFFIC:

SHIFT TRAFFIC INTO PHASE 1 STRIPING BEGINNING AT 8 PM ON A FRIDAY. AT THE SAME TIME, CLOSE THE I.R. 277/U.S. 224 EB RAMP TO I.R. 77 NB TO COMPLETE THE TEMPORARY RAMP CONNECTION (SHEET P.031). DETOUR RAMP TRAFFIC EAST TO THE KELLY AVE INTERCHANGE TO TURN AROUND TO I.R. 277/U.S. 224 WB TO ACCESS I.R. 77 NB. PROVIDE AT LEAST 4 PORTABLE CHANGEABLE MESSAGE SIGNS TO GUIDE TRAFFIC THROUGH THE DETOUR. THE TEMPORARY RAMP CONNECTION SHALL BE COMPLETED AND THE RAMP REOPENED TO TRAFFIC BY 5 AM THE FOLLOWING MONDAY.

SHIFT THE TWO (2) EB LANES ALONG I.R. 277/U.S. 224 TO THE WB SIDE (CONTRAFLOW) UTILIZING A CROSSOVER. ONE LANE WILL EXIT TO THE EB I.R. 277/U.S. 224 TO NB I.R. 77 RAMP EAST OF THE BRIDGE WITH THE USE OF A TEMPORARY RAMP RE-ALIGNMENT AND TEMPORARY PAVEMENT. CLOSE INSIDE LANE ALONG WB I.R. 277/U.S. 224 AND SHIFT THE ONE WB THROUGH LANE TO THE OUTSIDE WITH TEMPORARY PAVEMENT. WB I.R. 277/U.S. 224 TO SB I.R. 77 RAMP WILL REMAIN OPEN AS THE MAINLINE WB LANE WILL BE TEMPORARILY USED AS A CHOICE LANE.

CONSTRUCTION:

DEMO EXISTING SOUTHERN HALF OF I.R. 277/U.S. 224 SUPERSTRUCTURE, APPROACH SLABS, ROADWAY, ETC. AND CONSTRUCT NEW SOUTHERN HALF OF I.R. 277/U.S. 224 SUPERSTRUCTURE, APPROACH SLABS, ROADWAY, ETC.

PHASE 2

SET PHASE 2 ON I.R. 277/U.S. 224 PER PLANS (SHEETS P.032 THROUGH P.038).

TRAFFIC:

BEFORE SHIFTING TRAFFIC INTO PHASE 2 STRIPING, CLOSE THE I.R. 277/U.S. 224 EB RAMP TO I.R. 77 NB RAMP, AGAIN BEGINNING AT 8 PM ON A FRIDAY. UTILIZE THE SAME DETOUR WITH THE SAME MATERIALS AND SIGNAGE DETAILED IN THE PHASE 1 NOTES. REMOVE THE TEMPORARY PAVEMENT FOR THE TEMPORARY RAMP CONNECTION DURING THE CLOSURE AND RETURN THE MAINLINE AND RAMP PAVEMENT TO THE LAYOUT PRIOR TO CONSTRUCTION. PAVEMENT REMOVAL SHALL BE COMPLETED WITH THE RAMP REOPENED AND TRAFFIC SHIFTED INTO PHASE 2 STRIPING BY 5 AM THE FOLLOWING MONDAY.

RESTRIPE THE EB LANES ALONG I.R. 277/U.S. 224 TO MATCH THE STRIPING CONFIGURATION PRIOR TO PROJECT CONSTRUCTION AND CLOSE THE INSIDE SHOULDER WITH PORTABLE BARRIER AND DRUMS. MAINTAIN INSIDE LANE CLOSURE ALONG WB I.R. 277/U.S. 224 AND SHIFT THE ONE WB THROUGH LANE TO THE OUTSIDE UTILIZING TEMPORARY PAVEMENT CONSTRUCTION IN PHASE 1. WB I.R. 277/U.S. 224 TO SB I.R. 77 RAMP WILL REMAIN OPEN AS THE MAINLINE WB LANE WILL BE TEMPORARILY USED AS A CHOICE LANE.

CONSTRUCTION:

DEMO EXISTING NORTHERN PORTION OF I.R. 277/U.S. 224 SUPERSTRUCTURE, APPROACH SLABS, ROADWAY, ETC. TO THE LIMITS SHOWN ON THE PLAN SHEET. CONSTRUCT NEW NORTHERN PORTION OF I.R. 277/U.S. 224 SUPERSTRUCTURE, APPROACH SLABS, ROADWAY, ETC. TO THE LIMITS SHOWN ON THE PLAN SHEET.

SEQUENCE OF CONSTRUCTION (CONT'D)

PHASE 3

SET PHASE 3 ON I.R. 277/U.S. 224 PER PLANS (SHEETS P.039 THROUGH P.045).

TRAFFIC:

MAINTAIN EB LANES ALONG I.R. 277/U.S. 224 IN SAME EXISTING STRIPING CONFIGURATION AS IN PHASE 2 WITH INSIDE SHOULDER FULLY OPEN. MAINTAIN INSIDE LANE CLOSURE ALONG WB I.R. 277/U.S. 224 AND SHIFT THE ONE WB THROUGH LANE TO THE INSIDE. WB I.R. 277/U.S. 224 TO SB I.R. 77 RAMP WILL REMAIN OPEN AS THE MAINLINE WB LANE WILL BE TEMPORARILY USED AS A CHOICE LANE.

CONSTRUCTION:

DEMO REMAINING EXISTING NORTHERN PORTION OF I.R. 277/U.S. 224 SUPERSTRUCTURE, APPROACH SLABS, ROADWAY, ETC. AND CONSTRUCT NEW NORTHERN PORTION OF I.R. 277/U.S. 224 SUPERSTRUCTURE, APPROACH SLABS, ROADWAY, ETC.

PHASE 4

SET PHASE 4 ON I.R. 277/U.S. 224 AS PER THE PERMANENT PAVEMENT MARKING PLANS (SHEETS P.059 THROUGH P.064). NOTE THAT SPRAY THERMOPLASTIC PAVEMENT MARKINGS SHALL BE USED ON ALL ASPHALT PAVEMENT TO SHIFT BOTH DIRECTIONS OF TRAFFIC INTO THIS CONFIGURATION PRIOR TO APPLYING THE SURFACE COURSE.

TRAFFIC:

MAINTAIN THE EB AND WB TRAFFIC LANES ALONG I.R. 277/U.S. 224 WITH ALL LANES AND SHOULDERS OPEN. DURING PAVING AND STRIPING OPERATIONS, OVERNIGHT LANE CLOSURES SHALL BE IMPLEMENTED AS PER SCD'S MT-95.30 (CLOSING RIGHT OR LEFT LANE OF A MULTI-LANE DIVIDED HIGHWAY WITH DRUMS) AND MT-99.20 (TRAFFIC CONTROL FOR LONG LINE PAVEMENT MARKING OPERATIONS).

CONSTRUCTION:

APPLY SURFACE COURSE OF ASPHALT PAVEMENT ALONG BOTH SIDES OF I.R. 277/U.S. 224 WITHIN THE LIMITS SHOWN IN THE GENERAL NOTES AND PLAN SHEETS. APPLY PERMANENT STRIPING THROUGHOUT THE CORRIDOR.

DESIGN AGENCY



DESIGNER

MRC

REVIEWER

FR 08/27/25

PROJECT ID

121479

SHEET

P.012

TOTAL


122

| ISSUE RECORD: | | | DESCRIPTION | | | | | | | | | | | | | | | | | | | | | | |
|-----------------------------------|-------------|-------------------------------|-------------|----|-----------|------|-----------------------------|----------------------------------|----------------------------|--|-------------------------------|-------------------------------------|----------------------------------|------------------------------------|------------------------------------|------------------------|------------------------|--|--|--|---|--|---|------------------------------------|------------------------------|
| NO. | DATE | Adjusted Item Description | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | 02/10/26 | | | | | | | | | | | | | | | | | | | | | | | | |
| REF NO. | SHEET NO. | STATION TO STATION | | | | | 606 | 611 | 611 | 614 | SPECIAL | 614 | 614 | 614 | 614 | 614 | 614 | 614 | 615 | 622 | 622 | 622 | 622 | | |
| | | | | | | | ANCHOR ASSEMBLY, MGS TYPE E | 15" CONDUIT, TYPE B, AS PER PLAN | SLOTTED DRAIN, TYPE 2, 15" | WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (BIDIRECTIONAL) | TYPE B FLASHING WARNING LIGHT | WORK ZONE CROSSOVER LIGHTING SYSTEM | WORK ZONE RAISED PAVEMENT MARKER | BARRIER REFLECTOR, TYPE 1, ONE-WAY | BARRIER REFLECTOR, TYPE 2, ONE-WAY | OBJECT MARKER, ONE WAY | OBJECT MARKER, TWO WAY | WORK ZONE PAVEMENT MARKING, MISC.: LANE LINE, 6" | WORK ZONE EDGE LINE, CLASS I, 6", 740.06, TYPE I | WORK ZONE PAVEMENT MARKING, MISC.: EDGE LINE, 6" | WORK ZONE PAVEMENT MARKING, MISC.: CHANNELIZING LINE, 12" | WORK ZONE PAVEMENT MARKING, MISC.: DOTTED LINE, 6" | PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A | PORTABLE BARRIER, 50", AS PER PLAN | PORTABLE BARRIER, UNANCHORED |
| | | | | | | EACH | FT | FT | EACH | EACH | EACH | EACH | EACH | EACH | EACH | MILE | MILE | MILE | FT | FT | SY | FT | FT | FT | EACH |
| | | I.R. 277/U.S. 224 - PRE-PHASE | | | | | | | | | | | | | | | | | | | | | | | |
| | P.020 | 263+02.27 | LT | TO | 263+54.79 | 1 | | | | | | | | | | | | | | | | | | | |
| LL-1 | P.018-P.021 | 237+30.00 | RT | TO | 268+60.00 | | | | | | | | | | | 0.60 | | | | | | | | | |
| RPM-LL-1 | P.018-P.021 | 237+30.00 | RT | TO | 268+60.00 | | | | | | | 27 | | | | | | | | | | | | | |
| LL-2 | P.018 | 237+30.00 | RT | TO | 240+87.59 | | | | | | | | | | | 0.07 | | | | | | | | | |
| RPM-LL-2 | P.018 | 237+30.00 | RT | TO | 240+87.59 | | | | | | | 3 | | | | | | | | | | | | | |
| LL-3 | P.019 | 250+35.97 | LT | TO | 274+30.00 | | | | | | | | | | | 0.46 | | | | | | | | | |
| RPM-LL-3 | P.019 | 250+35.97 | LT | TO | 274+30.00 | | | | | | | 20 | | | | | | | | | | | | | |
| RPM-EL-1A | P.017-P.018 | 231+20.00 | LT | TO | 242+00.00 | | | | | | | 108 | | | | | | | | | | | | | |
| RPM-EL-1B | P.021-P.022 | 274+30.00 | LT | TO | 284+71.69 | | | | | | | 105 | | | | | | | | | | | | | |
| EL-1 | P.017-P.022 | 231+20.00 | LT | TO | 284+71.69 | | | | | | | | | | | | | 1.02 | | | | | | | |
| RPM-EL-2 | P.017-P.018 | 231+20.00 | LT | TO | 242+00.00 | | | | | | | 108 | | | | | | | | | | | | | |
| EL-2 | P.017-P.019 | 234+20.00 | LT | TO | 247+78.44 | | | | | | | | | | | | | 0.26 | | | | | | | |
| RPM-EL-3A | P.017-P.018 | 227+05.20 | RT | TO | 237+30.00 | | | | | | | 103 | | | | | | | | | | | | | |
| RPM-EL-3B | P.021-P.022 | 268+60.00 | RT | TO | 278+20.07 | | | | | | | 97 | | | | | | | | | | | | | |
| EL-3 | P.017-P.022 | 227+05.20 | RT | TO | 278+20.07 | | | | | | | | | | | | | 0.97 | | | | | | | |
| RPM-EL-4 | P.017-P.018 | 227+05.20 | RT | TO | 237+30.00 | | | | | | | 103 | | | | | | | | | | | | | |
| EL-4 | P.017-P.018 | 227+05.20 | RT | TO | 240+40.27 | | | | | | | | | | | | | 0.26 | | | | | | | |
| EL-5 | P.018-P.020 | 245+81.77 | RT | TO | 259+76.24 | | | | | | | | | | | | | 0.27 | | | | | | | |
| EL-6 | P.019-P.020 | 248+74.60 | LT | TO | 264+70.74 | | | | | | | | | | | | | 0.31 | | | | | | | |
| RPM-EL-7 | P.021-P.022 | 274+30.00 | LT | TO | 284+71.69 | | | | | | | 105 | | | | | | | | | | | | | |
| EL-7 | P.020-P.022 | 265+64.60 | LT | TO | 284+71.69 | | | | | | | | | | | | | 0.37 | | | | | | | |
| EL-8 | P.020-P.021 | 260+76.94 | RT | TO | 271+60.00 | | | | | | | | | | | | | 0.21 | | | | | | | |
| CH-1 | P.017 | 231+20.00 | LT | TO | 234+20.00 | | | | | | | | | | | | | | 300 | | | | | | |
| RPM-CH-2 | P.017-P.018 | 227+05.20 | RT | TO | 237+30.00 | | | | | | | 103 | | | | | | | | | | | | | |
| CH-2 | P.017-P.018 | 227+05.20 | RT | TO | 237+30.00 | | | | | | | | | | | | | | 1025 | | | | | | |
| RPM-CH-3 | P.017-P.018 | 227+05.20 | RT | TO | 237+30.00 | | | | | | | 103 | | | | | | | | | | | | | |
| CH-3 | P.017-P.018 | 227+05.20 | RT | TO | 237+30.00 | | | | | | | | | | | | | | 1025 | | | | | | |
| CH-4 | P.018 | 243+16.19 | RT | TO | 245+81.77 | | | | | | | | | | | | | | 266 | | | | | | |
| CH-5 | P.019 | 247+78.44 | LT | TO | 250+35.97 | | | | | | | | | | | | | | 258 | | | | | | |
| CH-6 | P.019 | 247+78.44 | LT | TO | 250+35.97 | | | | | | | | | | | | | | 258 | | | | | | |
| CH-7 | P.020-P.021 | 264+68.44 | LT | TO | 267+25.97 | | | | | | | | | | | | | | 258 | | | | | | |
| CH-8 | P.020-P.021 | 264+70.74 | LT | TO | 267+25.97 | | | | | | | | | | | | | | 256 | | | | | | |
| CH-9 | P.020 | 258+10.72 | RT | TO | 260+76.94 | | | | | | | | | | | | | | 267 | | | | | | |
| CH-10 | P.020 | 258+10.72 | RT | TO | 260+28.06 | | | | | | | | | | | | | | 218 | | | | | | |
| RPM-CH-11 | P.021-P.022 | 274+30.00 | LT | TO | 284+71.69 | | | | | | | 105 | | | | | | | | | | | | | |
| CH-11 | P.021-P.022 | 274+30.00 | LT | TO | 286+02.68 | | | | | | | | | | | | | | | | | | | | |
| RPM-CH-12 | P.021-P.022 | 274+30.00 | LT | TO | 284+71.69 | | | | | | | 105 | | | | | | | | 1173 | | | | | |
| CH-12 | P.021-P.022 | 274+30.00 | LT | TO | 284+71.69 | | | | | | | | | | | | | | | 1042 | | | | | |
| RPM-CH-13 | P.021-P.022 | 268+60.00 | RT | TO | 278+20.07 | | | | | | | 97 | | | | | | | | | | | | | |
| CH-13 | P.021-P.022 | 268+60.00 | RT | TO | 278+20.07 | | | | | | | | | | | | | | | 961 | | | | | |
| RPM-CH-14 | P.021-P.022 | 268+60.00 | RT | TO | 278+20.07 | | | | | | | 97 | | | | | | | | | | | | | |
| CH-14 | P.021-P.022 | 271+60.00 | RT | TO | 278+20.07 | | | | | | | | | | | | | | 661 | | | | | | |
| DL-1 | P.019-P.020 | 250+69.40 | RT | TO | 258+10.72 | | | | | | | | | | | | | | | | 742 | | | | |
| DL-2 | P.021 | 267+25.97 | LT | TO | 274+30.00 | | | | | | | | | | | | | | | 705 | | | | | |
| | P.018 | 242+12.14 | CL | TO | 246+18.59 | | | | | | | | | | | | | | | | 125 | | | | |
| | P.018-P.019 | 241+73.35 | LT | TO | 247+75.94 | | | | | | | | | | | | | | | | 1180 | | | | |
| | P.020 | 257+04.77 | LT | | 264+69.32 | | | | | | | | | | | | | | | | 1101 | | | | |
| | P.020 | 258+32.85 | CL | | 260+15.97 | | | | | | | | | | | | | | | | 58 | | | | |
| TOTALS CARRIED TO GENERAL SUMMARY | | | | | | 1 | | | | | | 1389 | | | | | 1.13 | | 3.67 | 7968 | 1447 | 2464 | | | |

MODEL: Sheet PAPER:SIZE: 34x22 (in.) DATE: 2/10/2026 TIME: 12:36:35 PM USER: p005636G pw:\parsons-ne-pw-bentley.com\Parsons MAS - Ohio State\Documents\SUM-2770373-12147999 - Support Files\Worksheet\121479400-Engineering\MOT\Sheets\121479_MC001.dgn

SUM-277-03.73

DESIGN AGENCY



DESIGNER

AB

REVIEWER

MRC 08/27/25

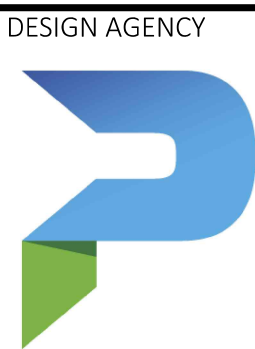
PROJECT ID

121479

SHEET TOTAL

P.013 122

MAINTENANCE OF TRAFFIC SUBSUMMARY



DESIGNER
AB

REVIEWER
MRC 08/27/25

PROJECT ID
121479

SHEET TOTAL
P.013 122

| ISSUE RECORD: | | | DESCRIPTION | | |
|---------------|----------|---------------------------|-------------|--|--|
| NO. | DATE | Adjusted Item Description | | | |
| 1 | 02/10/26 | | | | |

| REF NO. | SHEET NO. | STATION TO STATION | | | | | 606 | 611 | 611 | 614 | SPECIAL | 614 | 614 | 614 | 614 | 614 | 614 | 614 | 615 | 622 | 622 | 622 | 622 | | | | | |
|-----------------------------------|----------------------|-------------------------------|----------|----------|------------------------|----------|-----------------------------|----------------------------------|----------------------------|--|-------------------------------|-------------------------------------|----------------------------------|------------------------------------|------------------------------------|------------------------|------------------------|--|--|--|---|--|---|------------------------------------|------------------------------|----------------------------|---|------|
| | | | | | | | ANCHOR ASSEMBLY, MGS TYPE E | 15" CONDUIT, TYPE B, AS PER PLAN | SLOTTED DRAIN, TYPE 2, 15" | WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (BIDIRECTIONAL) | TYPE B FLASHING WARNING LIGHT | WORK ZONE Crossover LIGHTING SYSTEM | WORK ZONE RAISED PAVEMENT MARKER | BARRIER REFLECTOR, TYPE 1, ONE-WAY | BARRIER REFLECTOR, TYPE 2, ONE-WAY | OBJECT MARKER, ONE WAY | OBJECT MARKER, TWO WAY | WORK ZONE PAVEMENT MARKING, MISC.: LANE LINE, 6" | WORK ZONE EDGE LINE, CLASS I, 6", 740.06, TYPE I | WORK ZONE PAVEMENT MARKING, MISC.: EDGE LINE, 6" | WORK ZONE PAVEMENT MARKING, MISC.: CHANNELIZING LINE, 12" | WORK ZONE PAVEMENT MARKING, MISC.: DOTTED LINE, 6" | PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A | PORTABLE BARRIER, 50", AS PER PLAN | PORTABLE BARRIER, UNANCHORED | PORTABLE BARRIER, ANCHORED | BARRIER, MISC.: PCB TO PERMANENT BARRIER TRANSITION | |
| | | | | | | | EACH | FT | FT | EACH | EACH | EACH | EACH | EACH | EACH | EACH | EACH | MILE | MILE | MILE | FT | FT | SY | FT | FT | FT | FT | EACH |
| | | I.R. 277/U.S. 224 - PRE-PHASE | | | | | | | 1 | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| PB-1 | P.021 P.018-P.019 | 267+19.88 239+00.00 | CL LT | TO TO | 270+55.27 247+05.00 | CL LT | | | | 1 | | | | 81 | | | 33 | | | | | | 101 | | | 805 | | |
| PB-2 | P.018 | 238+77.00 | RT | TO | 246+95.00 | RT | | | | 1 | | | | 82 | | | 33 | | | | | | | | | 818 | | |
| PB-3 | P.019-P.020 | 256+53.25 | LT | TO | 263+73.98 | LT | | | | 1 | | | | 73 | | | 29 | | | | | | | | | 721 | | 1 |
| PB-4 | P.019-P.020 | 255+87.00 | LT | TO | 261+05.00 | LT | | | | 1 | | | | 52 | | | 21 | | | | | | | | | 518 | | |
| PB-5 | P.019-P.020 | 255+77.00 | RT | TO | 260+95.00 | RT | | | | 1 | | | | 52 | | | 21 | | | | | | | | | 518 | | |
| PB-6 | P.020-P.021 | 264+87.00 | LT | TO | 271+55.00 | LT | | | | 1 | | | | 67 | | | 27 | | | | | | | | | 668 | | |
| PB-7 | P.020-P.021 | 264+77.00 | RT | TO | 271+45.00 | RT | | | | 1 | | | | 67 | | | 27 | | | | | | | | | 668 | | |
| TD-1 | P.023 | 240+00.00 | CL | TO | 246+00.00 | CL | | 217 | 383 | | | | | | | | | | | | | | | | | | | |
| TD-2 | P.023 | 256+85.00 | CL | TO | 260+15.97 | CL | | 153 | 178 | | | | | | | | | | | | | | | | | | | |
| TD-3 | P.023 | 266+00.00 | CL | TO | 269+20.00 | CL | | 125 | 195 | | | | | | | | | | | | | | | | | | | |
| | | I.R. 277/U.S. 224 - PHASE 1 | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | P.024 | 245+98.34 | RT | | 246+18.59 | RT | | | | 1 | | | | | | | | | | | | | | | | | | |
| | P.026 | 259+95.72 | CL | | 260+15.97 | CL | | | | 1 | | | | | | | | | | | | | | | | | | |
| | P.024 | 243+89.41 | RT | | 243+89.41 | RT | | | | | 2 | | | | | | | | | | | | | | | | | |
| | P.024 | 245+06.64 | RT | | 245+06.64 | RT | | | | | 2 | | | | | | | | | | | | | | | | | |
| | P.024 | 245+69.96 | LT | | 245+69.96 | LT | | | | | 2 | | | | | | | | | | | | | | | | | |
| | P.027 | 268+39.66 | LT | | 268.39.66 | LT | | | | | 2 | | | | | | | | | | | | | | | | | |
| | P.024-P.025 | 240+97.90 | RT | | 247+28.67 | LT | | | | | | 1 | | | | | | | | | | | | | | | | |
| | P.026-P.027 | 264+75.91 | LT | | 270+82.75 | RT | | | | | | 1 | | | | | | | | | | | | | | | | |
| EL-9 | P.024 | 237+03.53 | RT | | 244+41.37 | RT | | | | | | | | | | | | | 0.14 | | | | | | | | | |
| EL-10 | P.024-P.027 | 240+97.90 | RT | | 270+82.75 | RT | | | | | | | | | | | | | 0.57 | | | | | | | | | |
| RPM-EL-10A | P.024-P.025 | 237+97.90 | RT | | 250+28.67 | RT | | | | | | | 124 | | | | | | | | | | | | | | | |
| RPM-EL-10B | P.026-P.027 | 261+75.91 | RT | | 273+82.75 | RT | | | | | | | 121 | | | | | | | | | | | | | | | |
| EL-11 | P.024-P.030 | 241+55.97 | LT | | 303+00.00 | LT | | | | | | | | | | | | | 1.17 | | | | | | | | | |
| RPM-EL-11A | P.024-P.025 | 238+55.97 | LT | | 249+88.78 | LT | | | | | | | 114 | | | | | | | | | | | | | | | |
| RPM-EL-11B | P.026-P.027 | 261+80.03 | LT | | 271+78.42 | LT | | | | | | | 100 | | | | | | | | | | | | | | | |
| EL-12 | P.024 | 241+55.97 | LT | | 246+11.90 | LT | | | | | | | | | | | | | 0.09 | | | | | | | | | |
| RPM-EL-12 | P.024 | 238+55.97 | LT | | 246+11.90 | LT | | | | | | | 76 | | | | | | | | | | | | | | | |
| EL-13 | P.024-P.026 | 244+14.85 | RT | | 262+86.63 | RT | | | | | | | | | | | | | 0.36 | | | | | | | | | |
| RPM-EL-13 | P.024-P.025 | 244+14.85 | RT | | 250+28.67 | RT | | | | | | | 62 | | | | | | | | | | | | | | | |
| EL-14 | P.024-P.026 | 246+33.51 | LT | | 264+20.18 | LT | | | | | | | | | | | | | 0.34 | | | | | | | | | |
| EL-15 | P.026-P.027 | 259+28.10 | LT | | 271+82.75 | LT | | | | | | | | | | | | | 0.24 | | | | | | | | | |
| RPM-EL-15 | P.026-P.027 | 261+75.91 | LT | | 270+82.75 | LT | | | | | | | 91 | | | | | | | | | | | | | | | |
| EL-16 | P.026 | 260+15.83 | RT | | 262+95.59 | RT | | | | | | | | | | | | | 0.06 | | | | | | | | | |
| EL-17 | P.026-P.027 | 264+58.06 | LT | | 269+40.00 | LT | | | | | | | | | | | | | 0.10 | | | | | | | | | |
| RPM-EL-17 | P.026-P.027 | 263+05.65 | LT | | 271+78.42 | LT | | | | | | | 88 | | | | | | | | | | | | | | | |
| CH-15 | P.024 | 237+03.53 | RT | | 244+42.13 | RT | | | | | | | | | | | | | | 739 | | | | | | | | |
| RPM-CH-15 | P.024 | 237+03.53 | RT | | 244+42.13 | RT | | | | | | | 74 | | | | | | | | | | | | | | | |
| CH-16 | P.024-P.025 | 237+30.00 | RT | | 253+33.29 | LT | | | | | | | | | | | | | | 1604 | | | | | | | | |
| RPM-CH-16 | P.024-P.025 | 237+30.00 | RT | | 250+28.67 | LT | | | | | | | 130 | | | | | | | | | | | | | | | |
| CH-17 | P.024 | 242+17.13 | RT | | 244+14.85 | RT | | | | | | | | | | | | | | 198 | | | | | | | | |
| RPM-CH-17 | P.024 | 242+17.13 | RT | | 244+14.85 | RT | | | | | | | 20 | | | | | | | | | | | | | | | |
| CH-18 | P.024 | 242+17.13 | RT | | 244+42.99 | RT | | | | | | | | | | | | | | 226 | | | | | | | | |
| CH-19 | P.024 | 246+11.90 | LT | | 246+88.78 | LT | | | | | | | | | | | | | | 77 | | | | | | | | |
| RPM-CH-19 | P.024 | 246+11.90 | LT | | 246+88.78 | LT | | | | | | | 8 | | | | | | | | | | | | | | | |
| CH-20 | P.024 | 246+11.41 | LT | | 246+88.78 | LT | | | | | | | | | | | | | | 78 | | | | | | | | |
| TOTALS CARRIED TO GENERAL SUMMARY | | | | | | | | 495 | 756 | 9 | 8 | 2 | 1008 | 474 | | | 191 | | 3.07 | 2922 | | | 101 | | | 4716 | | 1 |

SUM-277-03.73

MODEL: Sheet PAPER:SIZE: 34x22 (in.) DATE: 2/10/2026 TIME: 12:46:28 PM USER: p00636g
pw:\parsons-na-pw-bentley.com\Parsons MAS - Ohio State\Documents\SUM-2770373-12147999 - Support Files\Worksheet\121479\400-Engineering\MOT\Sheets\121479_MC002.dgn

DESIGN AGENCY

DESIGNER
AB

REVIEWER
MRC 08/27/25

PROJECT ID
121479

SHEET TOTAL
P.014 122

MAINTENANCE OF TRAFFIC SUBSUMMARY

MAINTENANCE OF TRAFFIC SUBSUMMARY

DESIGN AGENCY



DESIGNER

AB

REVIEWER

MRC 08/27/25

PROJECT ID

121479

SHEET

P.014

TOTAL

122

| ISSUE RECORD: | | | DESCRIPTION | | | | |
|---------------|----------|---------------------------|-------------|--|--|--|--|
| NO. | DATE | Adjusted Item Description | | | | | |
| 1 | 02/10/26 | | | | | | |

| REF NO. | SHEET NO. | STATION TO STATION | | | | 606 | 611 | 611 | 614 | SPECIAL | 614 | 614 | 614 | 614 | 614 | 614 | 614 | 614 | 615 | 622 | 622 | 622 | 622 | | | | |
|-----------------------------------|-------------|-----------------------------|----|--|-----------|-----------------------------|----------------------------------|----------------------------|--|-------------------------------|-------------------------------------|----------------------------------|------------------------------------|------------------------------------|------------------------|------------------------|--|--|--|---|--|---|-------------------------------------|------------------------------|----------------------------|---|------|
| | | | | | | ANCHOR ASSEMBLY, MGS TYPE E | 15" CONDUIT, TYPE B, AS PER PLAN | SLOTTED DRAIN, TYPE 2, 15" | WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (BIDIRECTIONAL) | TYPE B FLASHING WARNING LIGHT | WORK ZONE Crossover LIGHTING SYSTEM | WORK ZONE RAISED PAVEMENT MARKER | BARRIER REFLECTOR, TYPE 1, ONE-WAY | BARRIER REFLECTOR, TYPE 2, ONE-WAY | OBJECT MARKER, ONE WAY | OBJECT MARKER, TWO WAY | WORK ZONE PAVEMENT MARKING, MISC.: LANE LINE, 6" | WORK ZONE EDGE LINE, CLASS I, 6", 740.06, TYPE I | WORK ZONE PAVEMENT MARKING, MISC.: EDGE LINE, 6" | WORK ZONE PAVEMENT MARKING, MISC.: CHANNELIZING LINE, 12" | WORK ZONE PAVEMENT MARKING, MISC.: DOTTED LINE, 6" | PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A | PORTABLE BARRIER, 50" , AS PER PLAN | PORTABLE BARRIER, UNANCHORED | PORTABLE BARRIER, ANCHORED | BARRIER, MISC.: PCB TO PERMANENT BARRIER TRANSITION | |
| | | | | | | EACH | FT | FT | EACH | EACH | EACH | EACH | EACH | EACH | EACH | EACH | MILE | MILE | MILE | FT | FT | SY | FT | FT | FT | FT | EACH |
| | | I.R. 277/U.S. 224 - PHASE 1 | | | | | | 1 | | | | | | | | | | | | | | | | | | | |
| CH-21 | P.026 | 257+29.77 | LT | | 259+28.10 | LT | | | | | | | | | | | | | | 199 | | | | | | | |
| CH-22 | P.026 | 257+29.77 | LT | | 260+15.83 | RT | | | | | | | | | | | | | | 287 | | | | | | | |
| CH-23 | P.026 | 264+20.18 | LT | | 265+65.22 | LT | | | | | | | | | | | | | | 146 | | | | | | | |
| CH-24 | P.026-P.027 | 264+20.18 | LT | | 272+37.91 | LT | | | | | | | | | | | | | | 818 | | | | | | | |
| RPM-CH-24 | P.026-P.027 | 261+80.03 | LT | | 272+37.91 | LT | | | | | 106 | | | | | | | | | | | | | | | | |
| DL-3 | P.024-P.025 | 246+88.78 | LT | | 250+18.51 | LT | | | | | | | | | | | | | | | 330 | | | | | | |
| DL-4 | P.025-P.026 | 253+33.29 | LT | | 257+29.77 | LT | | | | | | | | | | | | | | | 397 | | | | | | |
| DL-5 | P.029-P.030 | 295+80.00 | LT | | 303+00.00 | LT | | | | | | | | | | | | | | | 720 | | | | | | |
| | P.026 | 259+92.97 | RT | | 262+66.62 | RT | | | | | | | | | | | | | | | | 865 | | | | | |
| BT-PB-8A | P.024 | 242+12.14 | CL | | 242+24.64 | CL | | | | | | | | | | | | | | | | | | | | 1 | |
| PB-8 | P.024-P.027 | 242+24.64 | CL | | 270+42.77 | CL | | | | | | | 2705 | | | 113 | | | | | | | | 2819 | | | |
| BT-PB-8B | P.027 | 270+42.77 | CL | | 270+55.27 | CL | | | | | | | | | | | | | | | | | | | | 1 | |
| PB-9 | P.024 | 244+70.77 | RT | | 246+18.59 | CL | | | 1 | | | | 15 | | | 6 | | | | | | | | 148 | | 1 | |
| PB-10 | P.025-P.026 | 249+48.66 | LT | | 257+42.79 | LT | | | 1 | | | | 80 | | | 32 | | | | | | | | 125 | 670 | | |
| | P.025 | 248+70.00 | LT | | 250+79.00 | LT | | | | | | | | 21 | 9 | | | | | | | | | | | | |
| | P.025 | 250+79.00 | LT | | 256+53.00 | LT | | | | | | | 58 | | | 23 | | | | | | | | | | | |
| | | I.R. 277/U.S. 224 - PHASE 2 | | | | | | | | | | | | | | | | | | | | | | | | | |
| | P.032 | 242+92.83 | LT | | 242+92.83 | LT | | | | 2 | | | | | | | | | | | | | | | | | |
| | P.032 | 245+46.20 | LT | | 245+46.20 | LT | | | | 2 | | | | | | | | | | | | | | | | | |
| EL-18 | P.032-P.035 | 240+15.14 | LT | | 273+18.10 | LT | | | | | | | | | | | | 0.63 | | | | | | | | | |
| RPM-EL-18A | P.032-P.033 | 237+15.14 | LT | | 248+37.33 | LT | | | | | | 113 | | | | | | | | | | | | | | | |
| RPM-EL-18B | P.034-P.035 | 260+33.07 | LT | | 276+18.10 | LT | | | | | | 159 | | | | | | | | | | | | | | | |
| EL-19 | P.032 | 240+15.14 | LT | | 245+37.33 | LT | | | | | | | | | | | | 0.10 | | | | | | | | | |
| RPM-EL-19 | P.032 | 237+15.14 | LT | | 245+37.33 | LT | | | | | | 83 | | | | | | | | | | | | | | | |
| EL-20 | P.032 | 244+35.87 | LT | | 245+46.19 | LT | | | | | | | | | | | | 0.03 | | | | | | | | | |
| EL-21 | P.032-P.034 | 244+43.22 | LT | | 264+12.26 | LT | | | | | | | | | | | | 0.38 | | | | | | | | | |
| RPM-EL-21 | P.034 | 260+33.07 | LT | | 264+12.26 | LT | | | | | | 38 | | | | | | | | | | | | | | | |
| EL-22 | P.034-P.035 | 265+80.09 | LT | | 273+18.10 | LT | | | | | | | | | | | | 0.14 | | | | | | | | | |
| RPM-EL-22 | P.034-P.035 | 263+95.74 | LT | | 276+18.10 | LT | | | | | | 123 | | | | | | | | | | | | | | | |
| CH-25 | P.032 | 245+37.33 | LT | | 246+78.15 | LT | | | | | | | | | | | | | 141 | | | | | | | | |
| RPM-CH-25 | P.032 | 245+37.33 | LT | | 246+78.15 | LT | | | | | | 15 | | | | | | | | | | | | | | | |
| CH-26 | P.034 | 245+46.19 | LT | | 246+78.15 | LT | | | | | | | | | | | | | | 132 | | | | | | | |
| CH-27 | P.034 | 264+11.35 | LT | | 266+53.55 | LT | | | | | | | | | | | | | | | 243 | | | | | | |
| CH-28 | P.034-P.035 | 264+12.26 | LT | | 273+18.10 | LT | | | | | | | | | | | | | | | 906 | | | | | | |
| RPM-CH-28 | P.034-P.035 | 264+12.26 | LT | | 276+18.10 | LT | | | | | | 121 | | | | | | | | | | | | | | | |
| DL-6 | P.032-P.033 | 246+78.15 | LT | | 250+08.15 | LT | | | | | | | | | | | | | | | | 330 | | | | | |
| PB-11 | P.032 | 241+45.00 | RT | | 246+39.91 | RT | | | 1 | | | | 50 | | | 20 | | | | | | | | 495 | | | |
| PB-12 | P.032 | 241+99.38 | LT | | 246+55.00 | LT | | | 1 | | | | 46 | | | 19 | | | | | | | | 456 | | | |
| PB-13 | P.033-P.034 | 250+00.00 | LT | | 260+98.68 | LT | | | 1 | | | | 110 | | | 44 | | | | | | | | 462 | 637 | | |
| PB-14 | P.034 | 257+45.00 | RT | | 260+40.00 | RT | | | 1 | | | | 30 | | | 12 | | | | | | | | 295 | | | |
| PB-15 | P.034-P.035 | 266+15.00 | RT | | 270+70.00 | RT | | | 1 | | | | 46 | | | 19 | | | | | | | | 455 | | | |
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| TOTALS CARRIED TO GENERAL SUMMARY | | | | | | | | | 7 | 4 | | 758 | 3140 | 21 | 9 | 288 | | | 1.28 | 2872 | 1777 | 865 | 2819 | 2436 | 1307 | 3 | |

MODEL: Sheet PAPER:SIZE: 34x22 (in.) DATE: 2/10/2026 TIME: 1:27:09 PM USER: p005636G
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SUM-277-03.73

MAINTENANCE OF TRAFFIC SUBSUMMARY

DESIGN AGENCY

DESIGNER

AB

REVIEWER

MRC 08/27/25

PROJECT ID

121479

SHEET

P.015

TOTAL

122


SUM-277-03.73

MODEL: Sheet PAPER SIZE: 34x22 (in.) DATE: 2/10/2026 TIME: 1:34:03 PM USER: p005636G
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| ISSUE RECORD: | |
|---------------|------------------------------------|
| NO. | DATE DESCRIPTION |
| 1 | 02/10/26 Adjusted Item Description |

| REF NO. | SHEET NO. | STATION TO STATION | | | | 606 | 611 | 611 | 614 | SPECIAL | 614 | 614 | 614 | 614 | 614 | 614 | 614 | 614 | 615 | 622 | 622 | 622 | 622 | |
|-----------|-------------|-----------------------------|----|----|-----------|-----------------------------|----------------------------------|----------------------------|--|-------------------------------|-------------------------------------|----------------------------------|------------------------------------|------------------------------------|------------------------|------------------------|--|--|--|---|--|---|------------------------------------|------------------------------|
| | | | | | | ANCHOR ASSEMBLY, MGS TYPE E | 15" CONDUIT, TYPE B, AS PER PLAN | SLOTTED DRAIN, TYPE 2, 15" | WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (BIDIRECTIONAL) | TYPE B FLASHING WARNING LIGHT | WORK ZONE CROSSOVER LIGHTING SYSTEM | WORK ZONE RAISED PAVEMENT MARKER | BARRIER REFLECTOR, TYPE 1, ONE-WAY | BARRIER REFLECTOR, TYPE 2, ONE-WAY | OBJECT MARKER, ONE WAY | OBJECT MARKER, TWO WAY | WORK ZONE PAVEMENT MARKING, MISC.: LANE LINE, 6" | WORK ZONE EDGE LINE, CLASS I, 6", 740.06, TYPE I | WORK ZONE PAVEMENT MARKING, MISC.: EDGE LINE, 6" | WORK ZONE PAVEMENT MARKING, MISC.: CHANNELIZING LINE, 12" | WORK ZONE PAVEMENT MARKING, MISC.: DOTTED LINE, 6" | PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A | PORTABLE BARRIER, 50", AS PER PLAN | PORTABLE BARRIER, UNANCHORED |
| | | | | | | EACH | FT | FT | EACH | EACH | EACH | EACH | EACH | EACH | EACH | MILE | MILE | MILE | FT | FT | SY | FT | FT | EACH |
| | | I.R. 277/U.S. 224 - PHASE 3 | | | | | | 1 | | | | | | | | | | | | | | | | |
| EL-23 | P.039-P.040 | 240+15.14 | LT | TO | 250+84.79 | LT | | | | | | | | | | | | 0.21 | | | | | | |
| EL-24 | P.039 | 240+15.14 | LT | TO | 245+86.70 | LT | | | | | | | | | | | | 0.11 | | | | | | |
| EL-25 | P.039 | 244+32.50 | LT | TO | 245+88.90 | LT | | | | | | | | | | | | 0.03 | | | | | | |
| EL-26 | P.039-P.042 | 245+06.79 | LT | TO | 250+75.68 | LT | | | | | | | | | | | | 0.11 | | | | | | |
| EL-27 | P.041-P.042 | 257+01.26 | LT | TO | 273+18.10 | LT | | | | | | | | | | | | 0.31 | | | | | | |
| RPM-EL-27 | P.041-P.042 | 261+24.29 | LT | TO | 273+18.10 | LT | | | | | 120 | | | | | | | | | | | | | |
| EL-28 | P.040-P.042 | 256+91.94 | LT | TO | 268+83.96 | LT | | | | | | | | | | | 0.23 | | | | | | | |
| REL-1 | P.040-P.041 | 250+84.79 | LT | TO | 257+01.26 | LT | | | | | | | | | | 0.12 | | | | | | | | |
| REL-2 | P.040 | 250+75.68 | LT | TO | 256+91.94 | LT | | | | | | | | | | 0.12 | | | | | | | | |
| CH-29 | P.039-P.040 | 245+86.70 | LT | TO | 247+42.12 | LT | | | | | | | | | | | | | 156 | | | | | |
| CH-30 | P.039-P.040 | 245+88.90 | LT | TO | 247+42.12 | LT | | | | | | | | | | | | | | 154 | | | | |
| CH-31 | P.042 | 268+83.96 | LT | TO | 273+18.10 | LT | | | | | | | | | | | | | | 435 | | | | |
| RPM-CH-31 | P.042 | 268+83.96 | LT | TO | 273+18.10 | LT | | | | | 44 | | | | | | | | | | | | | |
| DL-7 | P.040 | 247+42.12 | LT | TO | 250+72.12 | LT | | | | | | | | | | | | | 330 | | | | | |
| PB-16 | P.040-P.041 | 249+55.00 | LT | TO | 265+45.22 | LT | | | | 1 | | | 160 | | | 64 | | | | | | 1591 | | |
| PB-17 | P.041 | 262+81.27 | LT | TO | 265+54.97 | LT | | | | 1 | | | 28 | | | 11 | | | | | | 274 | | |
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DESIGN AGENCY



DESIGNER

AB

REVIEWER

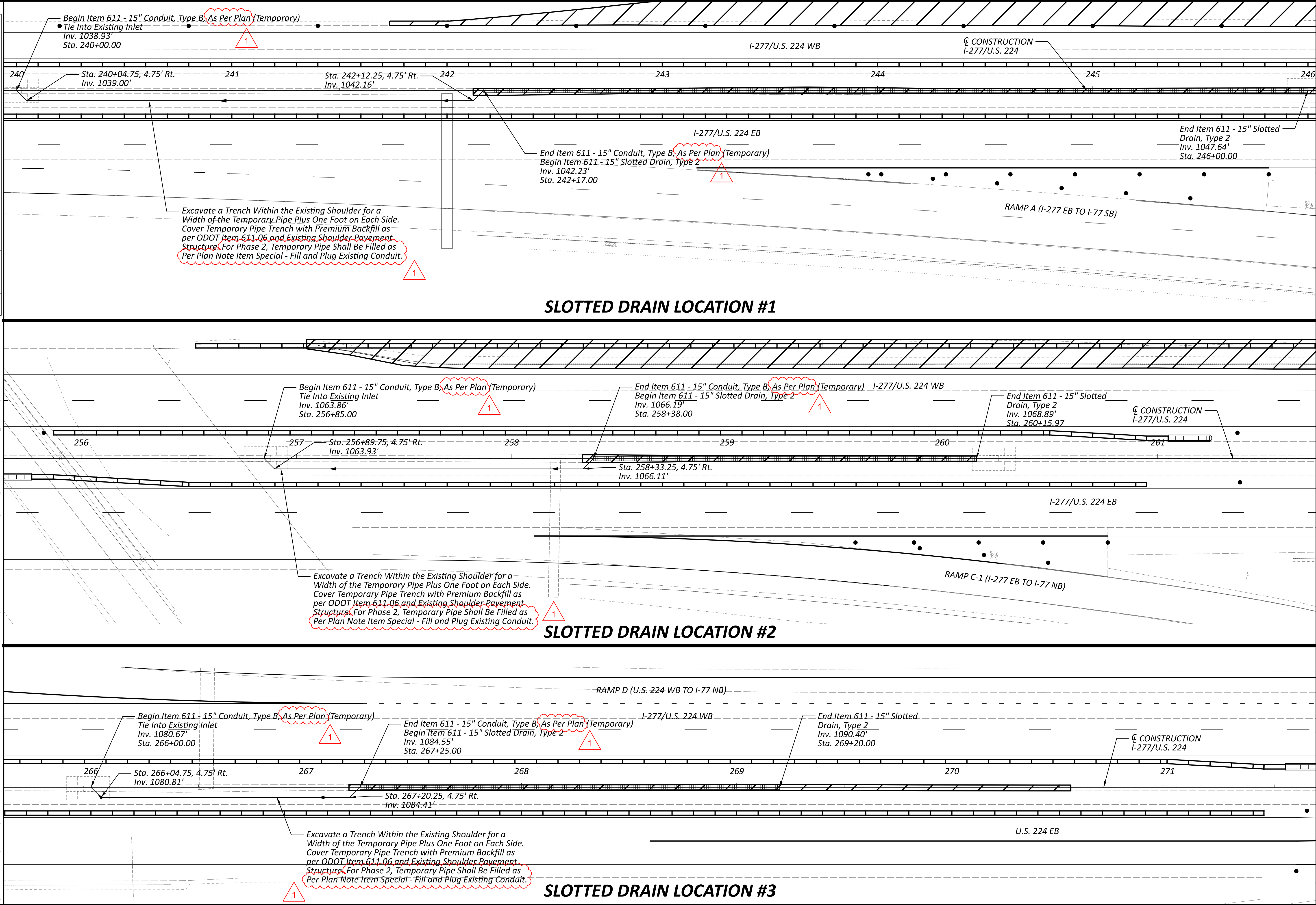
MRC 08/27/25

PROJECT ID

121479

| SHEET | TOTAL |
|-------|-------|
| P.016 | 122 |

| ISSUE RECORD: | | DESCRIPTION |
|---------------|----------|---|
| NO. | DATE | |
| 1 | 02/10/26 | Adjusted Item Description and Phasing Procedure |



HORIZONTAL SCALE IN FEET

0 10 20 40

MAINTENANCE OF TRAFFIC - I-277 - PRE-PHASE

TEMPORARY DRAINAGE DETAILS

| | |
|---------------|----------|
| DESIGN AGENCY | |
| DESIGNER | |
| MRC | REVIEWER |
| FR | 08/27/25 |
| PROJECT ID | 121479 |
| SHEET | TOTAL |
| P.023 | 122 |

| ISSUE RECORD: | | | | | | | | | | DESCRIPTION | | | | | | | | | | | | | | | | | | |
|---------------|-----------|--------------------|----|--------------|-----------|----|------|-------------------|--------------------------|-------------|-----------------------------|------|---------------------|--------------------------------------|--------------------------------------|---|---|--------------------|--------------------------------------|--|---|---|--|--|--|--|--|--|
| NO. | | DATE | | REVISED ITEM | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | | 2/10/26 | | | | | | | | | | | | | | | | | | | | | | | | | | |
| REF NO. | SHEET NO. | STATION TO STATION | | | | | | 202 | 202 | | 209 | | 606 | 606 | 606 | | 622 | 622 | 622 | 622 | | 809 | | | | | | |
| | | | | | | | | GUARDRAIL REMOVED | CONCRETE BARRIER REMOVED | | LINEAR GRADING, AS PER PLAN | | GUARDRAIL, TYPE MGS | MGS BRIDGE TERMINAL ASSEMBLY, TYPE 1 | MGS BRIDGE TERMINAL ASSEMBLY, TYPE 2 | | CONCRETE BARRIER, SINGLE SLOPE, TYPE B1 | BARRIER TRANSITION | CONCRETE BARRIER END SECTION, TYPE D | CONCRETE BARRIER, END ANCHORAGE, REINFORCED, TYPE B1 | | ITS PULL BOX WITH PAD AND HINGED LID ASSEMBLY, 32" WIDE, TYPE 1 | | | | | | |
| | | | | | | | | FT | FT | | STA | | FT | EACH | EACH | | FT | EACH | EACH | EACH | | EACH | | | | | | |
| G1 | P.53 | 249+92.71 | LT | TO | 250+17.67 | LT | | | | 0.25 | | 25 | | 1 | | | | | | | | | | | | | | |
| R1 | P.53 | 249+92.71 | LT | TO | 250+79.32 | LT | 62.5 | | | | | | | | | | | | | | | | | | | | | |
| B1 | P.53 | 250+15.67 | LT | TO | 250+29.67 | LT | | | | | | | | | | | | 1 | | | | | | | | | | |
| G2 | P.53 | 250+17.72 | RT | TO | 250+96.10 | RT | | | | 0.5 | | 50 | 1 | | | | | | | | | | | | | | | |
| R2 | P.53 | 250+17.72 | RT | TO | 251+61.82 | RT | 150 | | | | | | | | | | | | | | | | | | | | | |
| B2 | P.53 | 250+95.40 | RT | TO | 251+09.40 | RT | | | | | | | | | | | | 1 | | | | | | | | | | |
| R5 | P.53 | 250+15.00 | CL | TO | 250+92.06 | CL | | 77 | | | | | | | | | | | | | | | | | | | | |
| B3 | P.53 | 250+15.00 | CL | TO | 250+35.00 | CL | | | | | | | | | | | 1 | | | | | | | | | | | |
| B3 | P.53 | 250+35.00 | CL | TO | 250+54.00 | CL | | | | | | | | | | 7 | | | | 1 | | | | | | | | |
| B3 | P.53 | 250+74.00 | CL | TO | 250+89.00 | CL | | | | | | | | | | | | | | 1 | | | | | | | | |
| R6 | P.53 | 257+03.25 | CL | TO | 257+75.00 | CL | | 72 | | | | | | | | | | | | | | | | | | | | |
| B4 | P.53 | 257+05.79 | CL | TO | 257+20.79 | CL | | | | | | | | | | | | | | | | | | | | | | |
| B4 | P.53 | 257+40.79 | CL | TO | 257+55.00 | CL | | | | | | | | | | | 2 | | | | 1 | | | | | | | |
| B4 | P.53 | 257+55.00 | CL | TO | 257+75.00 | CL | | | | | | | | | | | | 1 | | | | | | | | | | |
| R3 | P.53 | 256+53.25 | LT | TO | 257+75.00 | LT | 72.5 | | | | | | | | | | | | | | | | | | | | | |
| G3 | P.53 | 256+53.02 | LT | TO | 257+75.00 | LT | | | | 0.8 | | 87.5 | 1 | | | | | | | | | | | | | | | |
| R4 | P.53 | 257+15.97 | RT | TO | 257+74.86 | RT | 50 | | | | | | | | | | | | | | | | | | | | | |
| G4 | P.53 | 257+15.97 | RT | TO | 257+74.86 | RT | | | | | | 25 | | 1 | | | | | | | | | | | | | | |
| T1 | P.53 | 250+29.46 | LT | | | | | | | | | | | | | | | | | | | 1 | | | | | | |
| T2 | P.53 | 256+61.86 | LT | | | | | | | | | | | | | | | | | | | 1 | | | | | | |
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ROADWAY SUBSUMMARY

DESIGN AGENCY



ms consultants, inc.

DESIGNER

TSB

REVIEWER

JDK 9-30-25

PROJECT ID

121479

SHEET

P.049

TOTAL

122

ISSUE RECORD:

DESCRIPTION

NO. DATE

1 1/28/26 ADDED ITEMS TO QUANTITY LIST

2 2/9/26 ADDED ITEMS AND REVISED QUANTITY

SUM-277-03.73


MODEL: Sheet PAPER: SIZE: 34x22 (in.) DATE: 2/11/2026 TIME: 11:25:16 AM USER: p008959H

pw:\parsons-na-pw-bentley.com\Parsons MAS - Ohio State\Documents\SUM-2770373-121479\99 - Support Files\Worksheets\SFN 7709811\Sheets\121479\400-Engineering\Structures\SFN 7709811_S0001.dgn

| ESTIMATED QUANTITIES - SUM-277-3.856 (01/IMS) | | | | | MADE BY: ZES | | DATE: 9/29/2025 | CHECKED BY: JRE | DATE: 9/30/2025 |
|---|----------------|---------|------|---|--------------|--------|-----------------|-----------------|-----------------|
| ITEM | ITEM EXTENSION | TOTAL | UNIT | DESCRIPTION | ABUTMENTS | PIERS | SUPERSTRUCTURE | GENERAL | SHEET REFERENCE |
| 202 | 11203 | LUMP | | PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN | | | | | 5/58 |
| 202 | 22900 | 400 | SY | APPROACH SLAB REMOVED | | | | 400 | |
| 202 | 23500 | 6,626 | SY | WEARING COURSE REMOVED | | | | 6,626 | |
| 204 | 30011 | 350 | CY | GRANULAR MATERIAL, TYPE B, AS PER PLAN | 350 | | | | 20/58 |
| 204 | 50001 | 1670 | SY | GEOTEXTILE FABRIC, AS PER PLAN | 1670 | | | | 20/58 |
| 503 | 11101 | LUMP | | COFFERDAMS AND EXCAVATION BRACING, AS PER PLAN | | | | | 5/58 |
| 503 | 21300 | LUMP | | UNCLASSIFIED EXCAVATION | | | | | |
| 509 | 10000 | 516,605 | LB | EPOXY COATED STEEL REINFORCEMENT | 15,746 | | 500,859 | | |
| 509 | 30020 | 17,611 | FT | NO. 4 DEFORMED GFRP REINFORCEMENT | | | 17,611 | | |
| 510 | 10000 | 1075 | EACH | DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT | 789 | | 286 | | |
| 511 | 33500 | 4 | EACH | SEMI-INTEGRAL DIAPHRAGM GUIDE | | | | 4 | |
| 511 | 34446 | 1,929 | CY | CLASS QC2 CONCRETE WITH QC/QA, BRIDGE DECK | | | 1,929 | | |
| 511 | 34450 | 385 | CY | CLASS QC2 CONCRETE WITH QC/QA, BRIDGE DECK (PARAPET) | | | 385 | | |
| 511 | 44110 | 50 | CY | CLASS QC1 CONCRETE, ABUTMENT NOT INCLUDING FOOTING | 50 | | | | |
| 511 | 46010 | 46 | CY | CLASS QC1 CONCRETE, RETAINING/WINGWALL NOT INCLUDING FOOTING | 46 | | | | |
| 511 | 46510 | 15 | CY | CLASS QC1 CONCRETE, FOOTING | 15 | | | | |
| 512 | 10100 | 4,137 | SY | SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) | 280 | 1,879 | 1,978 | | |
| 512 | 33000 | 112 | SY | TYPE 2 WATERPROOFING | 112 | | | | |
| 512 | 74000 | 1,898 | SY | REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES | 19 | 1,879 | | | |
| 513 | 10201 | 20,500 | LB | STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN | | | 20,500 | | |
| 513 | 20000 | 18,480 | EACH | WELDED STUD SHEAR CONNECTORS | | | 18,480 | | 5/58 |
| 513 | 90000 | 24,500 | LB | STRUCTURAL STEEL, MISC.: FLANGE PLATE RETROFITS | | | 24,500 | | |
| 514 | 00100 | LUMP | | SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL | | | | | |
| 514 | 00200 | LUMP | | FIELD PAINTING OF EXISTING STRUCTURAL STEEL, PRIME COAT | | | | | |
| 514 | 00300 | LUMP | | FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT | | | | | |
| 514 | 00400 | LUMP | | FIELD PAINTING STRUCTURAL STEEL, FINISH COAT | | | | | |
| 514 | 00504 | 130 | MNHR | GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL | | | 130 | | |
| 514 | 10000 | 38 | EACH | FINAL INSPECTION REPAIR | | | 38 | | |
| 514 | 27700 | 1,629 | SF | FIELD PAINTING, MISC.: COATING OF EXISTING GIRDER ENDS | | | 1,629 | | 5/58 |
| 516 | 10010 | 269 | FT | ARMORLESS PREFORMED JOINT SEAL | 269 | | | | |
| 516 | 13600 | 35 | SF | 1" PREFORMED EXPANSION JOINT FILLER | | | 35 | | |
| 516 | 13900 | 233 | SF | 2" PREFORMED EXPANSION JOINT FILLER | | | 233 | | |
| 516 | 14020 | 272 | FT | SEMI-INTEGRAL ABUTMENT EXPANSION JOINT SEAL | 272 | | | | |
| 516 | 44201 | 56 | EACH | ELASTOMERIC BEARING WITH INTERNAL LAMINATES AND LOAD PLATE (NEOPRENE), AS PER PLAN (19" x 20" x 3.878" WITH 20" x 21" x 1.5" PLATE) | | 56 | | | 26/58 |
| 516 | 44201 | 14 | EACH | ELASTOMERIC BEARING WITH INTERNAL LAMINATES AND LOAD PLATE (NEOPRENE), AS PER PLAN (14" x16" x 3.878" WITH 16" x 23" x 1.5" PLATE) | | 14 | | | 26/58 |
| 516 | 44301 | 28 | EACH | ELASTOMERIC BEARING WITH INTERNAL LAMINATES AND LOAD PLATE (NEOPRENE), AS PER PLAN (14" x14" x 4.628" WITH 15" x 15" x 1.5" PLATE) | 28 | | | | 26/58 |
| 516 | 47001 | LUMP | | JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN | | | | | 5/58 |
| 518 | 12201 | 6 | EACH | SCUPPERS, INCLUDING SUPPORTS, AS PER PLAN | | | 6 | | 5/58 |
| 518 | 21200 | 31 | CY | POROUS BACKFILL WITH GEOTEXTILE FABRIC | 31 | | | | |
| 518 | 40000 | 275 | FT | 6" PERFORATED CORRUGATED PLASTIC PIPE | 275 | | | | |
| 518 | 40010 | 75 | FT | 6" NON-PERFORATED CORRUGATED PLASTIC PIPE, INCLUDING SPECIALS | 75 | | | | |
| SPECIAL | 51900100 | 11,303 | SF | SPECIAL - COMPOSITE FIBER WRAP SYSTEM | | 11,303 | | | 6/58 |
| 526 | 25001 | 589 | SY | REINFORCED CONCRETE APPROACH SLABS (T=15"), AS PER PLAN | | | | 589 | 53/58 & 54/58 |
| 526 | 90030 | 269 | FT | TYPE C INSTALLATION | | | | 269 | |
| 601 | 20011 | 50 | CY | CRUSHED AGGREGATE SLOPE PROTECTION, AS PER PLAN | | | | 50 | 5/58 |
| 601 | 34100 | 50 | CY | ROCK CHANNEL PROTECTION, TYPE B WITHOUT FILTER | | | | 50 | |
| 625 | 98200 | LUMP | | LIGHTING, MISC.: REMOVE AND REERECT EXISTING UNDERPASS LIGHTING | | | | | 6/58 |
| 809 | 00530 | 4 | EACH | ITS JUNCTION BOX, 17" x 24" x 6" | | | | 4 | |
| 809 | 23900 | 675 | FT | CONDUIT, 2" DIAMETER, HDPE | | | 675 | | |
| 840 | 23001 | 1221 | CY | SELECT GRANULAR BACKFILL, AS PER PLAN | 1221 | | | | 20/58 |
| 863 | 00100 | 5568 | SY | GEOGRID, TYPE PI | 5568 | | | | 20/58 |

ESTIMATED QUANTITIES
BRIDGE NO. SUM-277-3.856
I.R. 277/U.S. 224 OVER I.R. 77

SFN
7709811

DESIGN AGENCY


DESIGNER
JRE

CHECKER
KDC

REVIEWER
TES 10/06/25

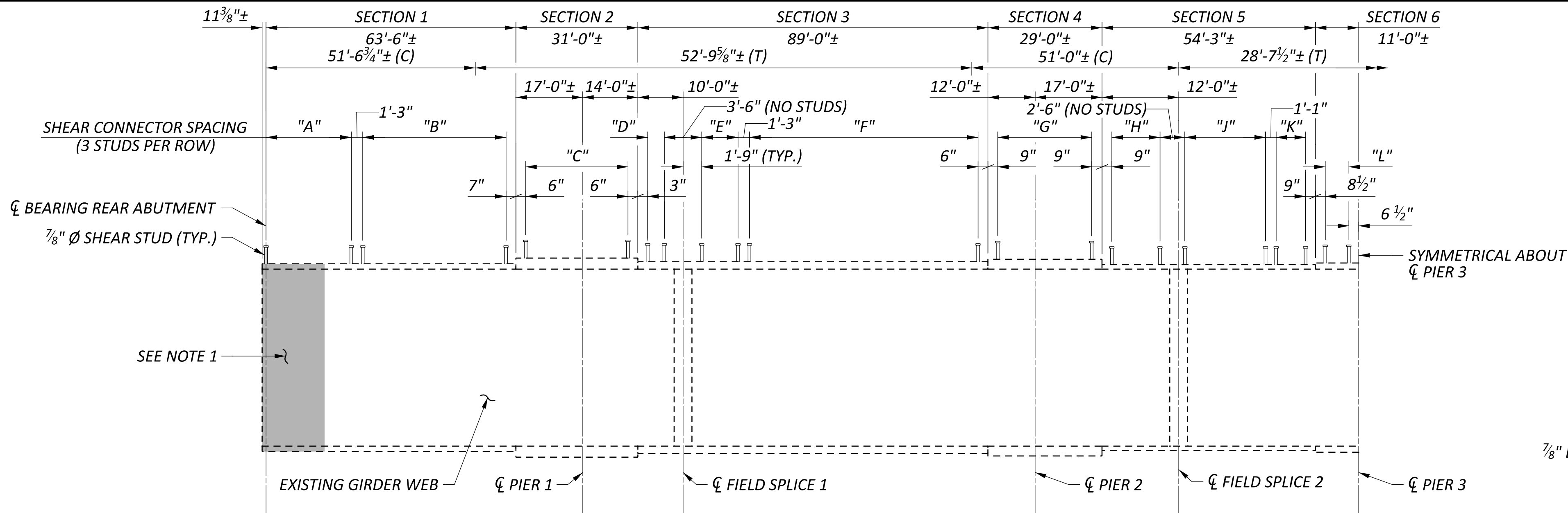
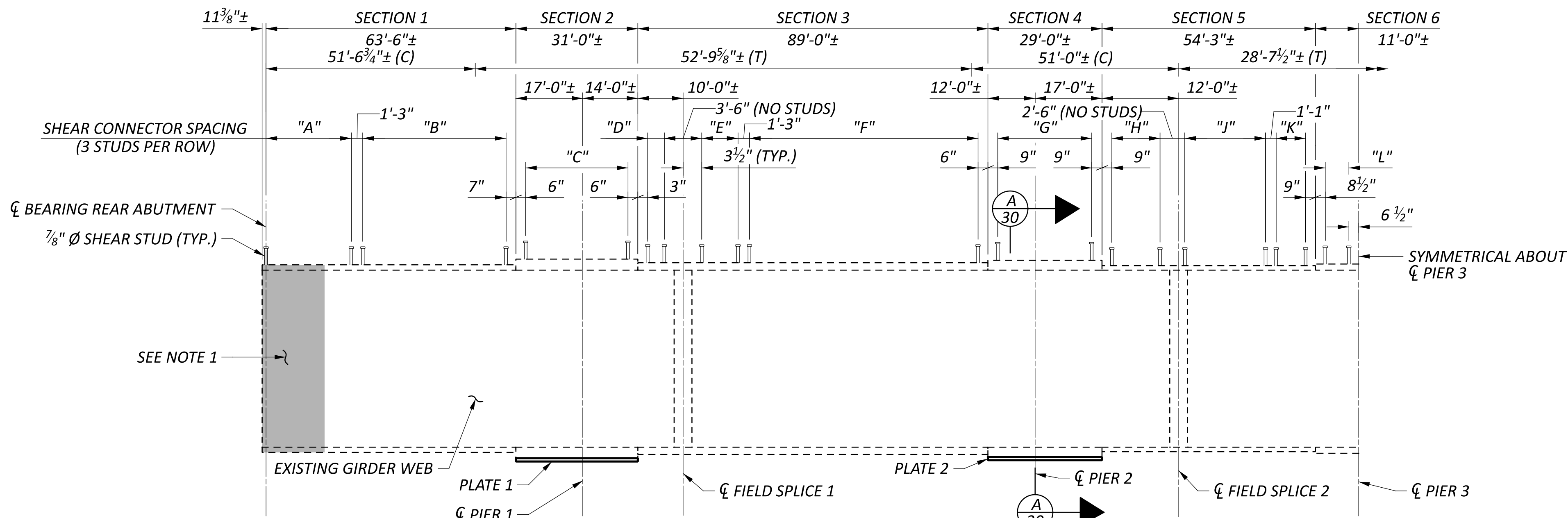
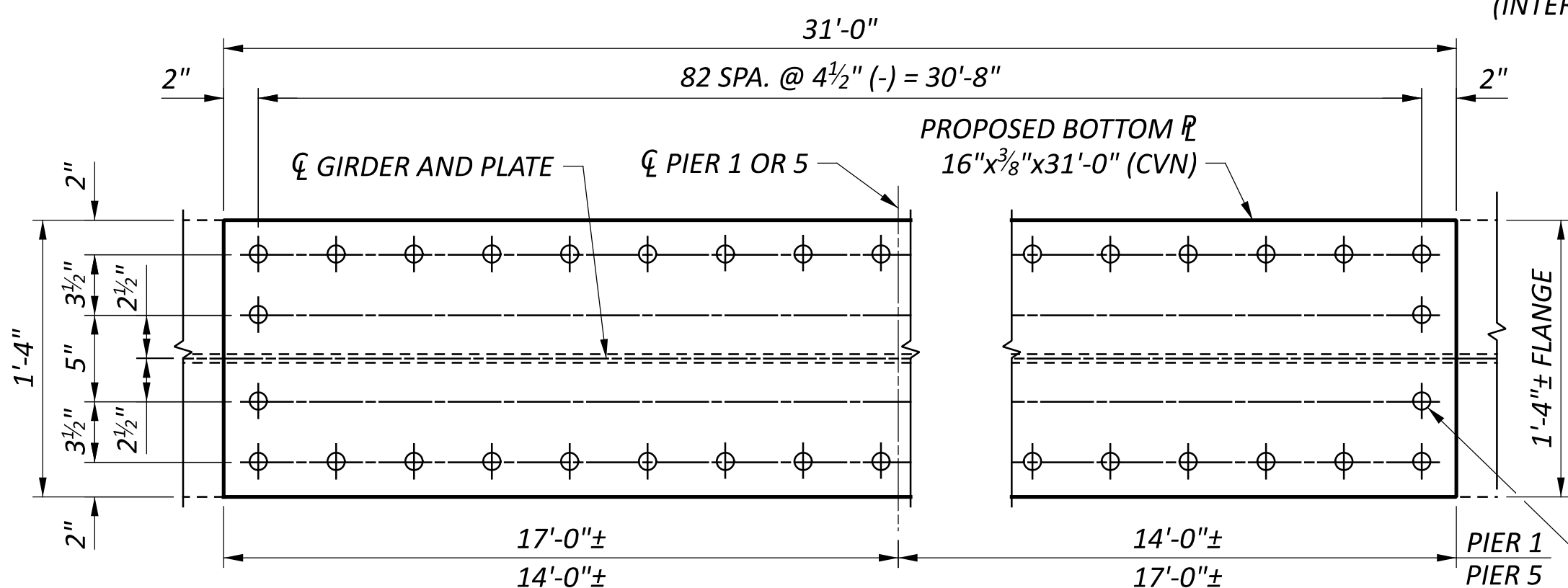
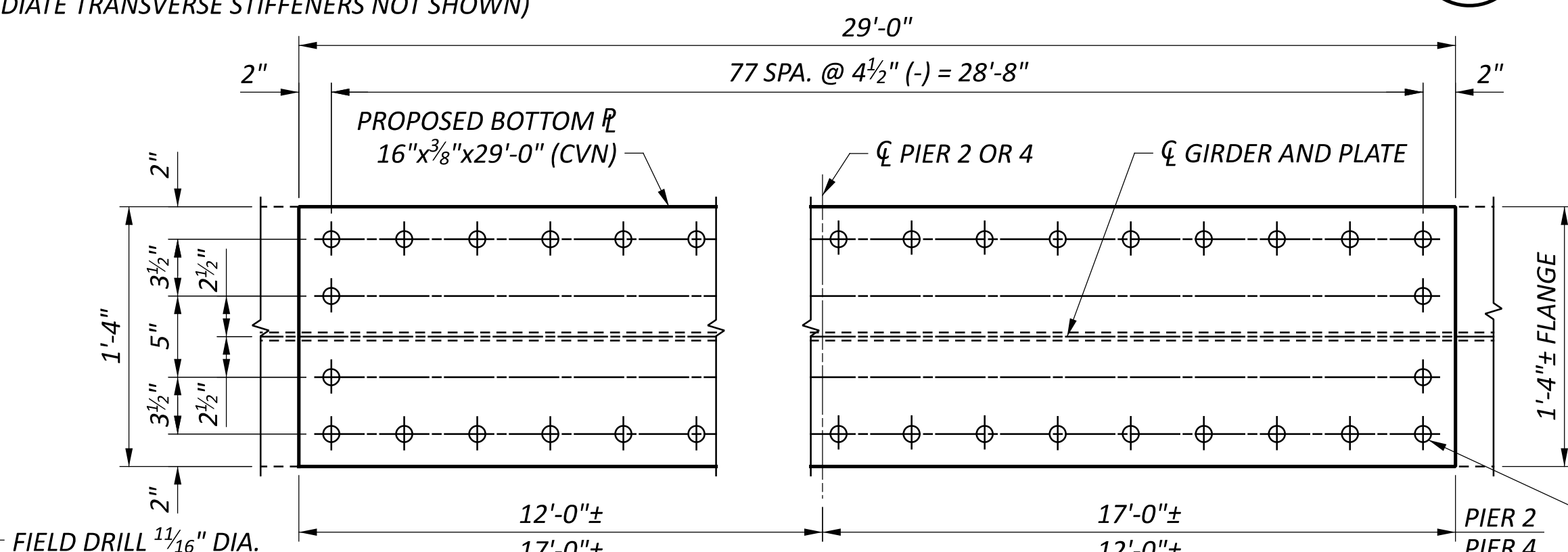
PROJECT ID
121479

SUBSET
7

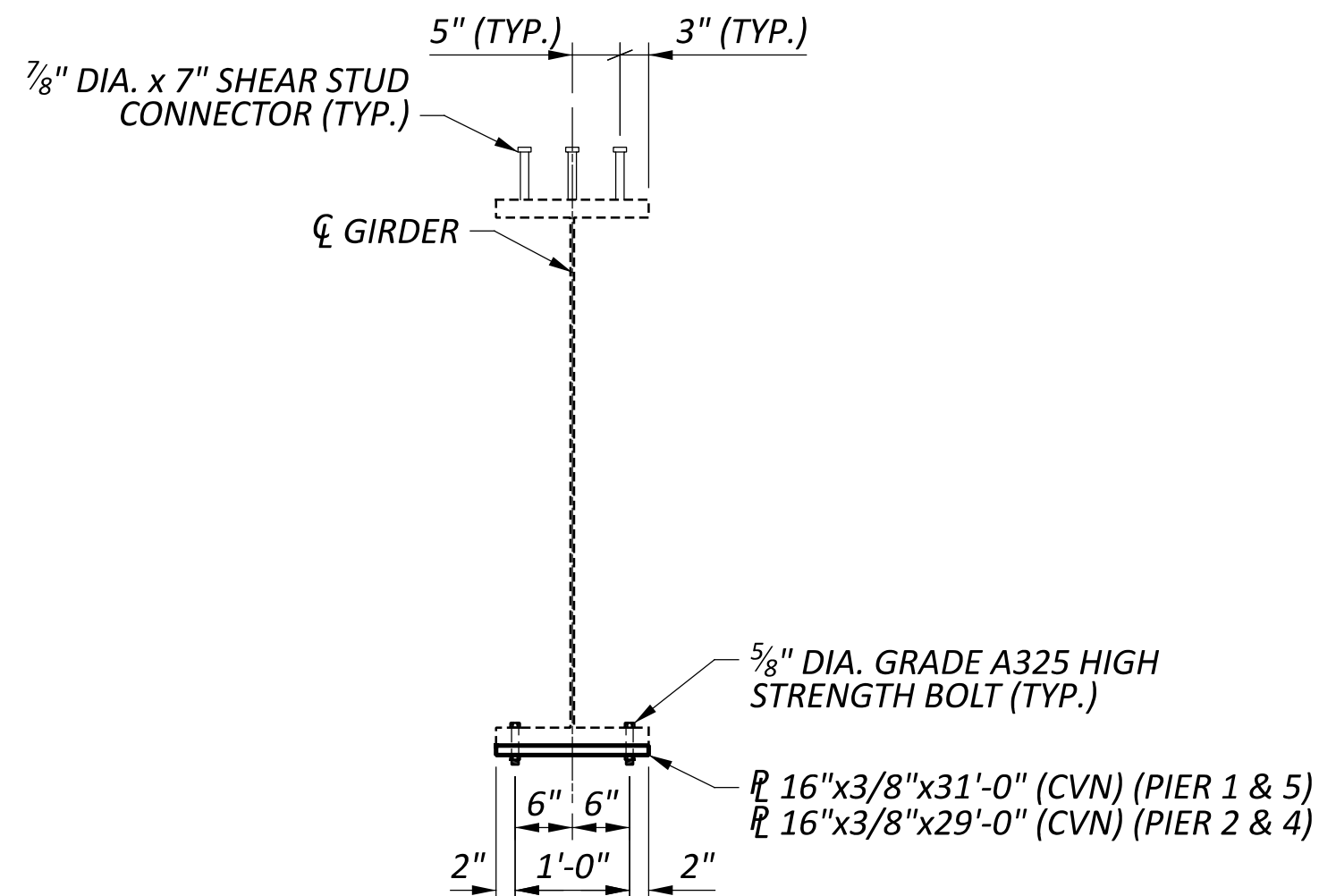
TOTAL
58

SHEET
P.071

TOTAL
122

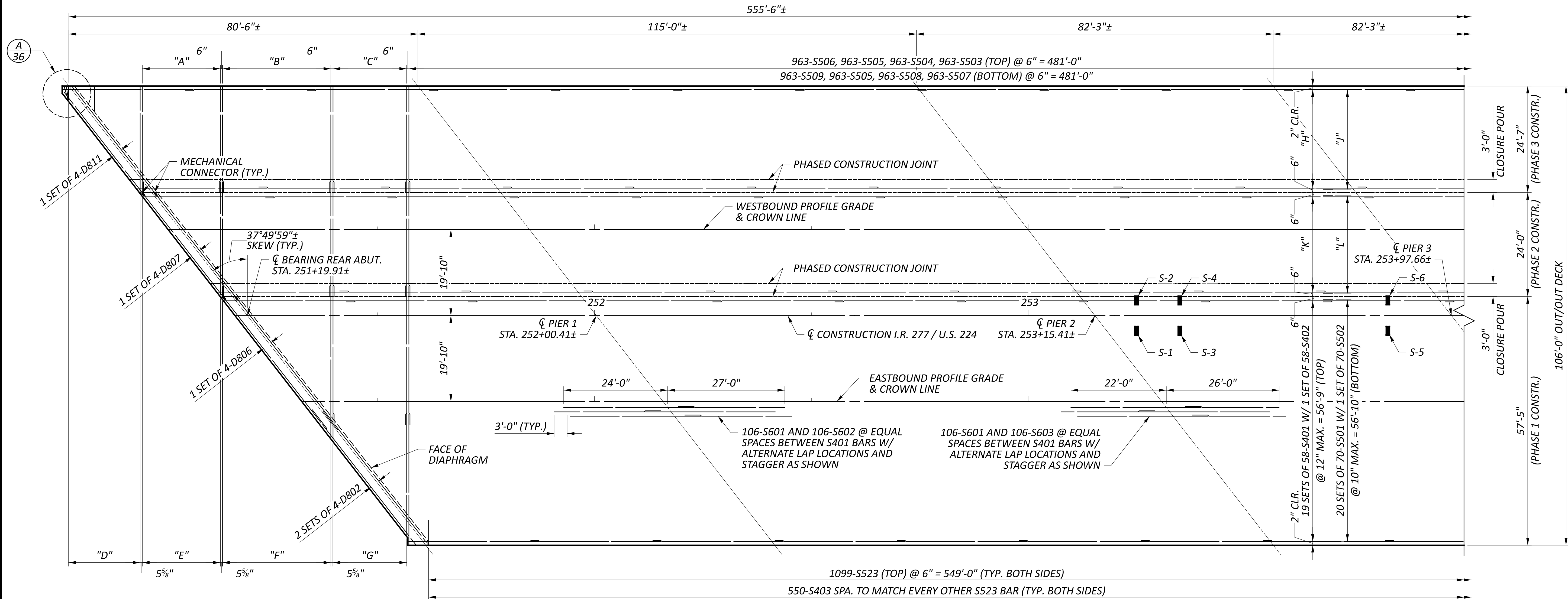
**EXISTING GIRDERS A, G, H, & P ELEVATION**(4 TOTAL, NOT TO SCALE)
(INTERMEDIATE TRANSVERSE STIFFENERS NOT SHOWN)**EXISTING GIRDERS B-F & J-N ELEVATION**(10 TOTAL, NOT TO SCALE)
(INTERMEDIATE TRANSVERSE STIFFENERS NOT SHOWN)**PLATE 1 PLAN**
PIERS 1 AND PIER 5**PLATE 2 PLAN**
PIERS 2 AND PIER 4

| SHEAR STUD SUMMARY TABLE | |
|--------------------------|------------------------|
| LOCATION | RANGE |
| "A" | 20 SPA. @ 13" = 21'-8" |
| "B" | 33 SPA. @ 15" = 41'-3" |
| "C" | 23 SPA. @ 15" = 28'-9" |
| "D" | 4 SPA. @ 24" = 8'-0" |
| "E" | 4 SPA. @ 24" = 8'-0" |
| "F" | 54 SPA. @ 15" = 67'-6" |
| "G" | 22 SPA. @ 15" = 27'-6" |
| "H" | 8 SPA. @ 15" = 10'-0" |
| "J" | 27 SPA. @ 15" = 33'-9" |
| "K" | 5 SPA. @ 13" = 5'-5" |
| "L" | 9 SPA. @ 13" = 9'-9" |

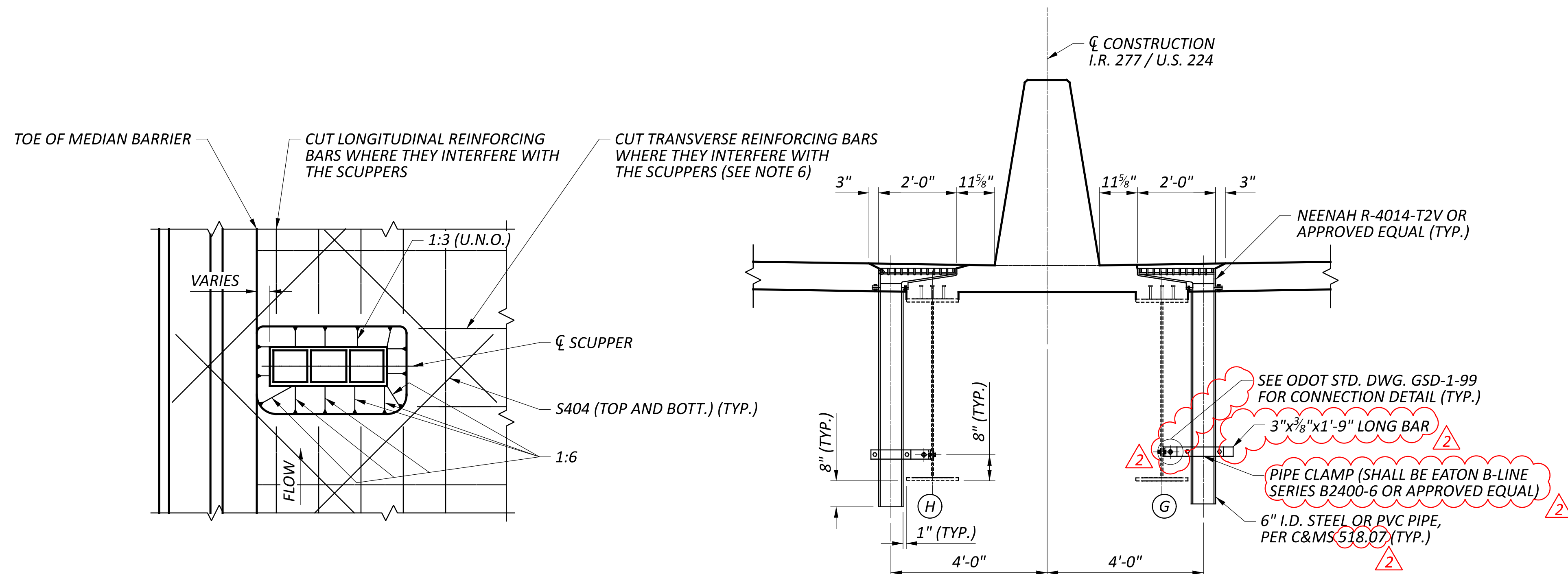
**A 30 TYPICAL GIRDER SECTION**
(BOTTOM COVER PLATE AND BOLTS ONLY APPLIES TO GIRDERS B-F & J-N)**LEGEND:**(C) - COMPRESSION
(T) - TENSION**NOTES:**

- FIELD PREPARE AND PAINT THE LAST 4'-0" AT EACH EXISTING GIRDER END, UNLESS NOTED OTHERWISE, PER ITEM 514 - FIELDPAINTING, MISC. COATING OF EXISTING GIRDER ENDS (SEE PLANNOTE ON SHEET 5/58).
- WELD ATTACHMENT OF SUPPORTS FOR CONCRETE DECK FINISHING MACHINE TO AREAS OF THE FASCIA GIRDER FLANGES DESIGNATED "COMPRESSION." DO NOT WELD ATTACHMENTS TO AREAS DESIGNATED "TENSION." FILLET WELDS TO COMPRESSION FLANGES SHALL BE AT LEAST 1" FROM EDGE OF FLANGE, BE NO MORE THAN 2" LONG, AND BE AT LEAST 1/4" FOR THICKNESSES UP TO 3/4" OR 3/16" FOR GREATER THAN 3/4" THICK.
- FOR FRAMING PLAN, SEE SHEET 28/58.
- ALL NEW STRUCTURAL STEEL SHALL BE ASTM A709 GRADE 50 (PAINTED).
- THE CONTRACTOR MAY PLACE THE SHEAR STUDS PARALLEL WITH THE BRIDGE SKEW TO FACILITATE THE PLACEMENT OF THE BRIDGE DECK REINFORCING BARS.
- SHEAR STUD CONNECTORS SHALL BE WELDED PER C&MS 513.22.
- WHEN A PLATE IS DESIGNATED (CVN), FURNISH MATERIAL THAT MEETS THE MINIMUM NOTCH TOUGHNESS REQUIREMENTS AS SPECIFIED IN 711.01.
- PRIOR TO INSTALLING PLATES 1 AND 2, THE CONTRACTOR SHALL BLAST CLEAN THE EXISTING FLANGES AND APPLY AN ORGANIC ZINC PRIME COAT IN ACCORDANCE WITH CMS 514.

FIELD DRILL 1 1/16" DIA.
HOLE IN BOTTOM FLANGE
FOR 5/8" DIA. BOLT (TYP.)



DECK PLAN



REINFORCING AT TYPE 2 SCUPPERS

TYPE 2 SCUPPER

SCUPPERS:

TYPE 2:

S-1: STA. 253+25, 4.00' RT
S-2: STA. 253+25, 4.00' LT
S-3: STA. 253+35, 4.00' RT
S-4: STA. 253+35, 4.00' LT
S-5: STA. 253+83, 4.00' RT
S-6: STA. 253+83, 4.00' LT

LEGEND:

"A" - 37-S506 (TOP) & 37-S509 (BOTTOM) @ 6" = 18'-0"
"B" - 51-S506 (TOP), 51-S505 (TOP) & 51-S509 (BOTTOM), 51-S505 (BOTTOM) @ 6" = 25'-0"
"C" - 35-S506 (TOP), 35-S505 (TOP), 35-S504 (TOP) & 35-S509 (BOTTOM), 35-S505 (BOTTOM), 35-S508 (BOTTOM) @ 6" = 17'-0"
"D" - 1 SER. OF 34-S516 (TOP) & 1 SER. OF 34-S521 (BOTTOM) @ 6" = 16'-6"
"E" - 1 SER. OF 37-S514 LAPPED WITH S506 (TOP) & 1 SER. OF 37-S514 LAPPED WITH S509 (BOTTOM)
"F" - 1 SER. OF 51-S512 LAPPED WITH S505 (TOP & BOTTOM)
"G" - 1 SER. OF 35-S510 LAPPED WITH S504 (TOP) & 1 SER. OF 35-S518 LAPPED WITH S508 (BOTTOM)
"H" - 19 SETS OF 25-S401 & 1 SET OF 25-S402 (TOP) @ 12" MAX. = 23'-11"
"J" - 20 SETS OF 30-S501 & 1 SET OF 30-S502 (BOTTOM) @ 10" MAX. = 24'-0"
"K" - 19 SETS OF 24-S401 & 1 SET OF 24-S402 (TOP) @ 12" = 23'-0"
"L" - 20 SETS OF 29-S501 & 1 SET OF 29-S502 (BOTTOM) @ 10" MAX. = 23'-2"

NOTES:

- ADJUST LONGITUDINAL REINFORCEMENT AS NECESSARY TO PROVIDE A MINIMUM CLEAR COVER OF 3" TO ANY CONSTRUCTION JOINT.
- FOR RAILING DETAILS, SEE SHEETS 51/58 AND 52/58.
- FOR TRANSVERSE SECTION, SEE SHEET 37/58.
- FOR PHASED CONSTRUCTION DETAILS, SEE SHEETS 8/58 AND 9/58.
- FOR SEMI-INTEGRAL DIAPHRAGM DETAILS, SEE SHEETS 31/58 THRU 34/58.
- FOR REINFORCING SCHEDULE, SEE SHEET 58/58.
- FOR ADDITIONAL SCUPPER DETAILS, SEE STD. DWG. GSD-1-99.

| MINIMUM LAP LENGTH | |
|--------------------|--------|
| NO. 4 BAR | 1'-11" |
| NO. 5 BAR | 2'-5" |
| NO. 6 BAR | 3'-7" |
| NO. 8 BAR | 5'-4" |

DECK PLAN (1 OF 2)
BRIDGE NO. SUM-277-3.856
I.R. 277/U.S. 224 OVER I.R. 77

SFN
7709811

DESIGN AGENCY



DESIGNER
AMK

CHECKER
KDC

REVIEWER
TES 10/06/25

PROJECT ID
121479

SUBSET TOTAL
35 58

SHEET TOTAL
P.099 122