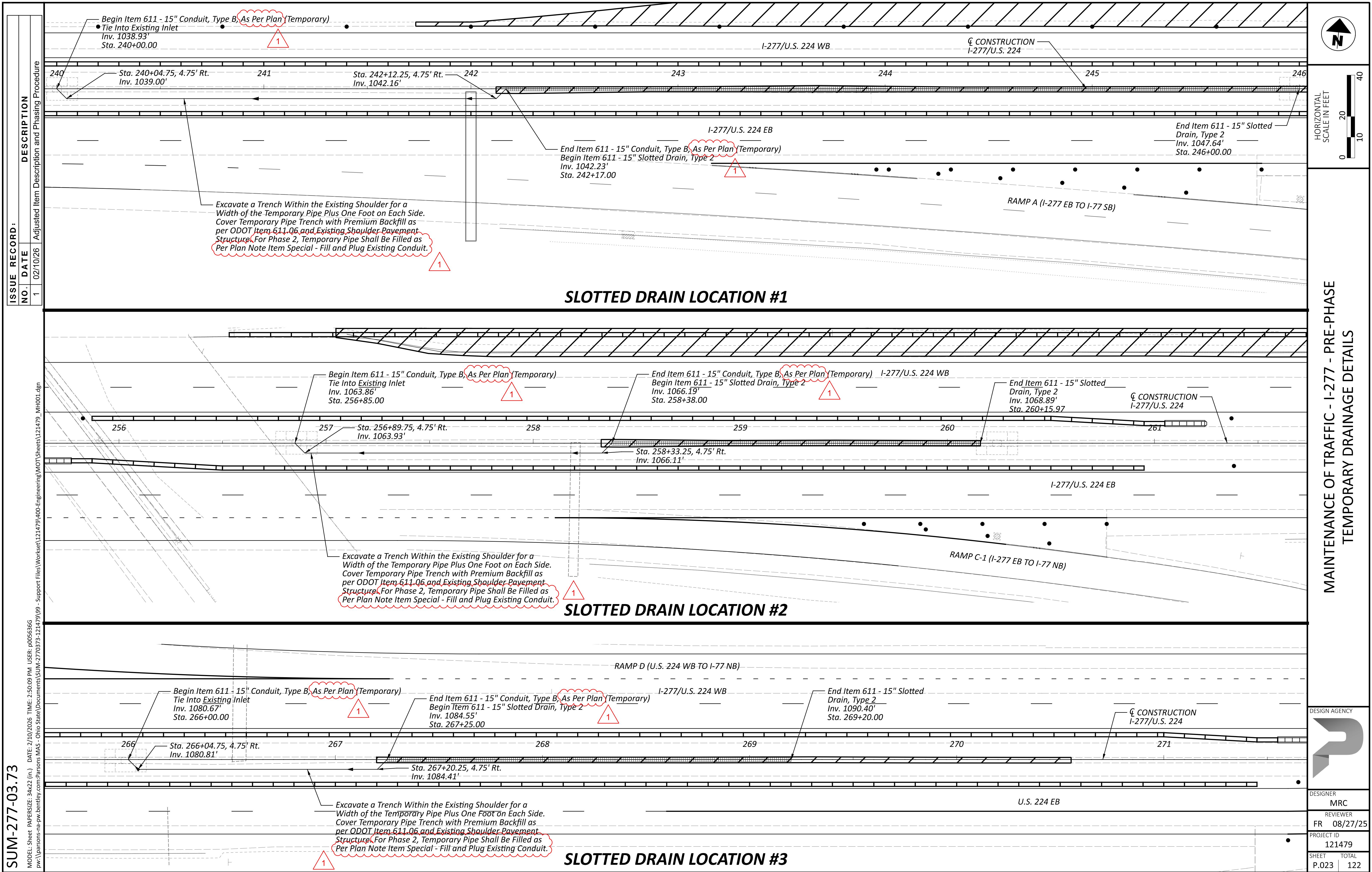


TRAFFIC INCIDENT MANAGEMENT (TIM) DURING MOT				TRAFFIC INCIDENT MANAGEMENT (TIM) DURING MOT (CONT'D)				DELINeATION OF PORTABLE AND PERMANENT BARRIER				DELINeATION OF TEMPORARY AND PERMANENT GUARDRAIL			
ISSUE RECORD:		DESCRIPTION		ISSUE RECORD:		DESCRIPTION		ISSUE RECORD:		DESCRIPTION		ISSUE RECORD:		DESCRIPTION	
NO.	DATE	C&MS Reference		NO.	DATE	C&MS Reference		NO.	DATE	C&MS Reference		NO.	DATE	C&MS Reference	
1	01/30/26	Updated C&MS Reference		2	02/10/26	Updated Notes for Work Zone Pavement Markings									

ITEM SPECIAL - FILL AND PLUG EXISTING CONDUIT			
ISSUE RECORD:	DESCRIPTION	NO.	DATE
	<p>THIS ITEM CONSISTS OF THE CONSTRUCTION OF BULKHEADS IN THE EXISTING 15 INCH DIAMETER CONDUIT AND FILLING THE AREA SEALED OFF WITH ITEM 613, SAND OR OTHER MATERIAL APPROVED BY THE ENGINEER.</p> <p>PUMP THE FILL MATERIAL INTO PLACE OR BY OTHER MEANS APPROVED BY THE ENGINEER, SO THAT AFTER SETTLEMENT, AT LEAST 90 PERCENT OF THE CROSS-SECTIONAL AREA OF THE CONDUIT, FOR ITS ENTIRE LENGTH IS FILLED. THE LENGTH OF FILLED AND PLUGGED CONDUIT TO BE PAID FOR IS THE ACTUAL NUMBER OF FEET (MEASURED ALONG THE CENTERLINE OF EACH CONDUIT FROM OUTER FACE TO OUTER FACE OF BULKHEADS) FILLED AND PLUGGED AS DESCRIBED ABOVE.</p> <p>APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION</p> <p>PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE APPROVED MOT EXCEPTION(S) PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).</p> <p>APPROVED MOT EXCEPTION(S) INCLUDE: THE CONTRACTOR IS PERMITTED TWO SEPARATE CLOSURES OF THE RAMP FROM I-277EB TO I-77NB TO PERFORM MAINTENANCE OF TRAFFIC WORK. THE PERMITTED CLOSURES SHALL BE LIMITED FROM 8:00PM FRIDAY THROUGH 5:00AM THE FOLLOWING MONDAY.</p> <p>A MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF 30 CALENDAR DAYS PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER AND AS WELL AS THE CONTRACTOR, WORKSITE TRAFFIC SUPERVISOR (WTS) AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.</p> <p>IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO THE OFFICE OF ROADWAY ENGINEERING, STATEWIDE TMC, DWZTM AND SPECIAL HAULING PERMITS AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE. REFERENCE "EXCEPTION REQUEST APPROVAL DATED 02/10/2026 FOR PID 121479 IN THE NOTIFICATION AND OTHER CORRESPONDENCE.</p> <p>ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTION(S) LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE MOT EXCEPTION COMMITTEE (MOTEC). IN THE EVENT THAT SUCH CHANGES ARE PROPOSED, THE REQUEST SHALL BE COORDINATED THROUGH THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM) A MINIMUM OF 30 CALENDAR DAYS PRIOR TO THE DESIRED IMPLEMENTATION DATE. IF THE DISTRICT AGREES WITH THE PROPOSED CHANGES THE DWZTM SHALL SEEK APPROVAL FROM THE MOTEC. IN THE EVENT THE PROPOSED CHANGES ARE APPROVED IN WRITING, THE CLOSURES ARE STILL SUBJECT TO NOTIFICATION REQUIREMENTS WITHIN THIS NOTE PRIOR TO IMPLEMENTATION.</p>	1	01/30/26
	Updated Pre-Phase Sequence Addendum Notes for Filling Pipe and Ramp Closures	2	02/10/26

SEQUENCE OF CONSTRUCTION	SEQUENCE OF CONSTRUCTION (CONT'D)	SEQUENCE OF CONSTRUCTION (CONT'D)
<p>IT IS THE INTENT OF THE FOLLOWING SEQUENCE OF CONSTRUCTION TO PROVIDE A WORK AREA FOR THE CONTRACTOR WHILE ALSO MAINTAINING TRAFFIC IN A MANNER THAT IS SAFE FOR THE TRAVELING PUBLIC. NOT ALL WORK ITEMS ARE IDENTIFIED BELOW, AND THE SEQUENCE OF CONSTRUCTION DOES NOT SUPERSEDE ANY OTHER ELEMENT OF WORK WITHIN THESE PLANS. SOME WORK ELEMENTS CAN BE PERFORMED SIMULTANEOUSLY. ALL TEMPORARY OR PERMANENT PAVEMENT MARKINGS SHALL BE IN PLACE BEFORE ANY PAVEMENT IS OPENED TO TRAFFIC.</p> <p>ALL EXISTING LANES, INCLUDING RAMPS, SHALL BE OPEN AND AVAILABLE TO TRAFFIC IN THE ORIGINAL OR PROPOSED FINAL ALIGNMENT BETWEEN OCTOBER 15 AND APRIL 1 IN THE EASTBOUND DIRECTION. PART WIDTH BRIDGE WORK IS ALLOWED YEAR-ROUND IN THE WESTBOUND DIRECTION. NO CONTRAFLOW TRAFFIC SHALL BE ALLOWED BETWEEN OCTOBER 15 AND APRIL 1.</p> <p>PRE-PHASE</p> <p>SET PRE-PHASE ON I.R. 277/U.S. 224 PER PLANS (SHEETS P.017 THROUGH P.022).</p> <p>TRAFFIC:</p> <p>SHIFT THE THREE (3) EB LANES TO THE OUTSIDE ALONG I.R. 277/U.S. 224 AS PER SCD MT-102.20. CLOSE INSIDE SHOULDER AND REMAINDER OF EXISTING INSIDE LANE. SHIFT THE THREE (3) WB LANES TO THE OUTSIDE ALONG I.R. 277/U.S. 224 AS PER SCD MT-102.20 UTILIZING TEMPORARY PAVEMENT. ALL RAMPS SHALL REMAIN OPEN.</p> <p>CONSTRUCTION:</p> <p>PRIOR TO THE CONSTRUCTION PRE-PHASE, ALL TEMPORARY PAVEMENT NORTH OF THE BRIDGE FOR WB I.R. 277/U.S. 224 SHALL BE COMPLETED AND EXISTING SCUPPERS SHALL BE CLEANED AND UNCLOSED. DURING THE PRE-PHASE, THE REMAINING TEMPORARY PAVEMENT AND TEMPORARY DRAINAGE FEATURES SHOWN ON PLAN SHEETS P.018 THROUGH P.022 SHALL BE CONSTRUCTED. THIS INCLUDES PLACING THE NORTHERN BRIDGE DECK (SEE NOTE ON THE MOT TYPICAL SECTION ON P.025).</p>	<p>PHASE 1</p> <p>SET PHASE 1 ON I.R. 277/U.S. 224 PER PLANS (SHEETS P.024 THROUGH P.030).</p> <p>TRAFFIC:</p> <p>SHIFT TRAFFIC INTO PHASE 1 STRIPING BEGINNING AT 8 PM ON A FRIDAY. AT THE SAME TIME, CLOSE THE I.R. 277/U.S. 224 EB RAMP TO I.R. 77 NB TO COMPLETE THE TEMPORARY RAMP CONNECTION (SHEET P.031). DETOUR RAMPS TRAFFIC EAST TO THE KELLY AVE INTERCHANGE TO TURN AROUND TO I.R. 277/U.S. 224 WB TO ACCESS I.R. 77 NB. PROVIDE AT LEAST 4 PORTABLE CHANGEABLE MESSAGE SIGNS TO GUIDE TRAFFIC THROUGH THE DETOUR. THE TEMPORARY RAMP CONNECTION SHALL BE COMPLETED AND THE RAMP REOPENED TO TRAFFIC BY 5 AM THE FOLLOWING MONDAY.</p>	<p>PHASE 3</p> <p>SET PHASE 3 ON I.R. 277/U.S. 224 PER PLANS (SHEETS P.039 THROUGH P.045).</p> <p>TRAFFIC:</p> <p>MAINTAIN EB LANES ALONG I.R. 277/U.S. 224 IN SAME EXISTING STRIPING CONFIGURATION AS IN PHASE 2 WITH INSIDE SHOULDER FULLY OPEN. MAINTAIN INSIDE LANE CLOSURE ALONG WB I.R. 277/U.S. 224 AND SHIFT THE ONE WB THROUGH LANE TO THE INSIDE. WB I.R. 277/U.S. 224 TO SB I.R. 77 RAMP WILL REMAIN OPEN AS THE MAINLINE WB LANE WILL BE TEMPORARILY USED AS A CHOICE LANE.</p>
<p>CONSTRUCTION:</p> <p>DEMO REMAINING EXISTING NORTHERN PORTION OF I.R. 277/U.S. 224 SUPERSTRUCTURE, APPROACH SLABS, ROADWAY, ETC. AND CONSTRUCT NEW NORTHERN PORTION OF I.R. 277/U.S. 224 SUPERSTRUCTURE, APPROACH SLABS, ROADWAY, ETC.</p>	<p>PHASE 4</p> <p>SET PHASE 4 ON I.R. 277/U.S. 224 AS PER THE PERMANENT PAVEMENT MARKING PLANS (SHEETS P.059 THROUGH P.064). NOTE THAT SPRAY THERMOPLASTIC PAVEMENT MARKINGS SHALL BE USED ON ALL ASPHALT PAVEMENT TO SHIFT BOTH DIRECTIONS OF TRAFFIC INTO THIS CONFIGURATION PRIOR TO APPLYING THE SURFACE COURSE.</p> <p>TRAFFIC:</p> <p>MAINTAIN THE EB AND WB TRAFFIC LANES ALONG I.R. 277/U.S. 224 WITH ALL LANES AND SHOULDERS OPEN. DURING PAVING AND STRIPING OPERATIONS, OVERNIGHT LANE CLOSURES SHALL BE IMPLEMENTED AS PER SCD'S MT-95.30 (CLOSING RIGHT OR LEFT LANE OF A MULTI-LANE DIVIDED HIGHWAY WITH DRUMS) AND MT-99.20 (TRAFFIC CONTROL FOR LONG LINE PAVEMENT MARKING OPERATIONS).</p>	<p>CONSTRUCTION:</p> <p>APPLY SURFACE COURSE OF ASPHALT PAVEMENT ALONG BOTH SIDES OF I.R. 277/U.S. 224 WITHIN THE LIMITS SHOWN IN THE GENERAL NOTES AND PLAN SHEETS. APPLY PERMANENT STRIPING THROUGHOUT THE CORRIDOR.</p>
<p>PHASE 2</p> <p>SET PHASE 2 ON I.R. 277/U.S. 224 PER PLANS (SHEETS P.032 THROUGH P.038).</p> <p>TRAFFIC:</p> <p>BEFORE SHIFTING TRAFFIC INTO PHASE 2 STRIPING, CLOSE THE I.R. 277/U.S. 224 EB RAMP TO I.R. 77 NB RAMP, AGAIN BEGINNING AT 8 PM ON A FRIDAY. UTILIZE THE SAME DETOUR WITH THE SAME MATERIALS AND SIGNAGE DETAILED IN THE PHASE 1 NOTES. REMOVE THE TEMPORARY PAVEMENT FOR THE TEMPORARY RAMP CONNECTION DURING THE CLOSURE AND RETURN THE MAINLINE AND RAMP PAVEMENT TO THE LAYOUT PRIOR TO CONSTRUCTION. PAVEMENT REMOVAL SHALL BE COMPLETED WITH THE RAMP REOPENED AND TRAFFIC SHIFTED INTO PHASE 2 STRIPING BY 5 AM THE FOLLOWING MONDAY.</p>	<p>PHASE 2</p> <p>RESTRIPE THE EB LANES ALONG I.R. 277/U.S. 224 TO MATCH THE STRIPING CONFIGURATION PRIOR TO PROJECT CONSTRUCTION AND CLOSE THE INSIDE SHOULDER WITH PORTABLE BARRIER AND DRUMS. MAINTAIN INSIDE LANE CLOSURE ALONG WB I.R. 277/U.S. 224 AND SHIFT THE ONE WB THROUGH LANE TO THE OUTSIDE UTILIZING TEMPORARY PAVEMENT CONSTRUCTION IN PHASE 1. WB I.R. 277/U.S. 224 TO SB I.R. 77 RAMP WILL REMAIN OPEN AS THE MAINLINE WB LANE WILL BE TEMPORARILY USED AS A CHOICE LANE.</p>	<p>CONSTRUCTION:</p> <p>DEMO EXISTING NORTHERN PORTION OF I.R. 277/U.S. 224 SUPERSTRUCTURE, APPROACH SLABS, ROADWAY, ETC. TO THE LIMITS SHOWN ON THE PLAN SHEET. CONSTRUCT NEW NORTHERN PORTION OF I.R. 277/U.S. 224 SUPERSTRUCTURE, APPROACH SLABS, ROADWAY, ETC. TO THE LIMITS SHOWN ON THE PLAN SHEET.</p>
		<p>DESIGN AGENCY  DESIGNER MRC REVIEWER FR 08/27/25 PROJECT ID 121479 SHEET TOTAL P.012 122</p>



SUM-277-03.73

MODEL: Sheet PAPER SIZE: 34x22 (in.) DATE: 2/10/2026 TIME: 4:29:35 PM USER: p005636G
Parsons no 22 now\Parsons MAS\Ohio State\Documents\SLIM 2770373 121479\00 Support Files\Works of\121479\00 Engineering\Broadway\Shoots\121479 GS001.dwg

ESTIMATED QUANTITIES - SUM-277-3.856 (01/IMS)

MADE BY: ZES

ABUTMENTS

PIERS

DATE: 9/29/2025

CHECKED BY: JRE

DATE: 9/30/2025

SUPERSTRUCTURE

GENERAL

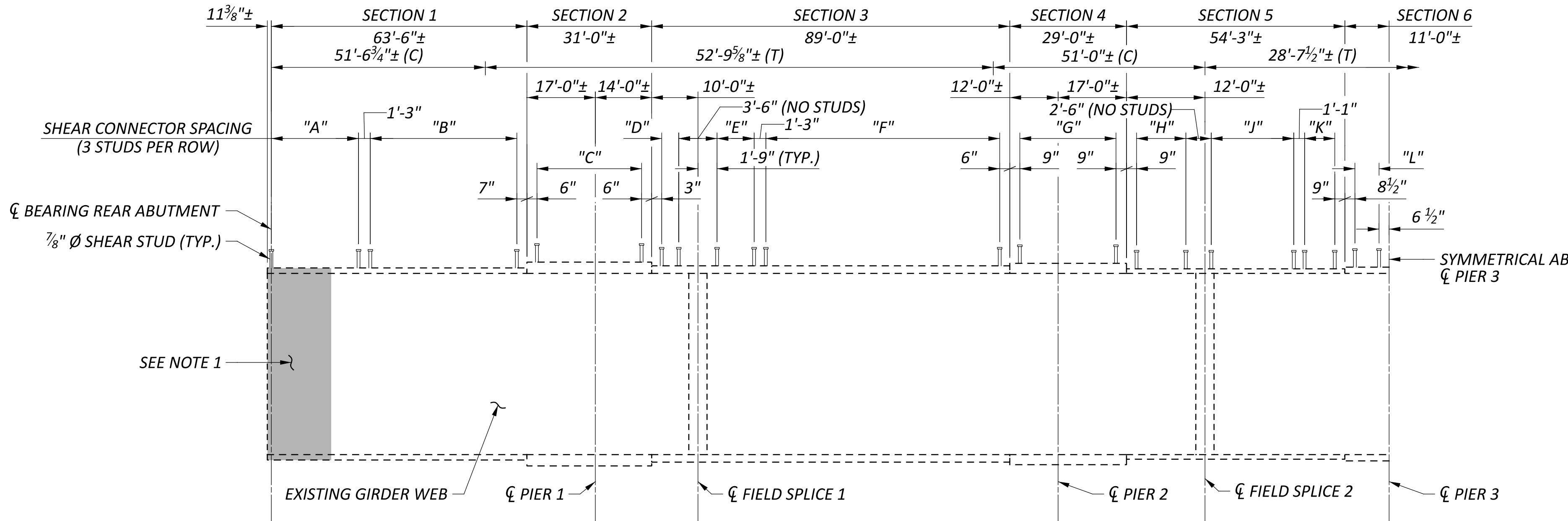
SHEET REFERENCE

5/58

ITEM	ITEM EXTENSION	TOTAL	UNIT	DESCRIPTION	MADE BY: ZES	ABUTMENTS	PIERS	DATE: 9/29/2025	CHECKED BY: JRE	DATE: 9/30/2025	SHEET REFERENCE
202	11203	LUMP		PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN							5/58
202	22900	400	SY	APPROACH SLAB REMOVED						400	
202	23500	6,626	SY	WEARING COURSE REMOVED						6,626	
204	30011	350	CY	GRANULAR MATERIAL, TYPE B, AS PER PLAN		350					20/58
204	50001	1670	SY	GEOTEXTILE FABRIC, AS PER PLAN		1670					20/58
503	11101	LUMP		COFFERDAMS AND EXCAVATION BRACING, AS PER PLAN							5/58
503	21300	LUMP		UNCLASSIFIED EXCAVATION							
509	10000	516,605	LB	EPOXY COATED STEEL REINFORCEMENT		15,746		500,859			
509	30020	17,611	FT	NO. 4 DEFORMED GFRP REINFORCEMENT				17,611			
510	10000	1075	EACH	DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT		789			286		
511	33500	4	EACH	SEMI-INTEGRAL DIAPHRAGM GUIDE						4	
511	34446	1,929	CY	CLASS QC2 CONCRETE WITH QC/QA, BRIDGE DECK				1,929			
511	34450	385	CY	CLASS QC2 CONCRETE WITH QC/QA, BRIDGE DECK (PARAPET)				385			
511	44110	50	CY	CLASS QC1 CONCRETE, ABUTMENT NOT INCLUDING FOOTING		50					
511	46010	46	CY	CLASS QC1 CONCRETE, RETAINING/WINGWALL NOT INCLUDING FOOTING		46					
511	46510	15	CY	CLASS QC1 CONCRETE, FOOTING		15					
512	10100	4,137	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)		280	1,879	1,978			
512	33000	112	SY	TYPE 2 WATERPROOFING		112					
512	74000	1,898	SY	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES		19	1,879				
513	10201	20,500	LB	STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN				20,500			
513	20000	18,480	EACH	WELDED STUD SHEAR CONNECTORS				18,480			5/58
513	90000	24,500	LB	STRUCTURAL STEEL, MISC.: FLANGE PLATE RETROFITS				24,500			
514	00100	LUMP		SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL							
514	00200	LUMP		FIELD PAINTING OF EXISTING STRUCTURAL STEEL, PRIME COAT							
514	00300	LUMP		FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT							
514	00400	LUMP		FIELD PAINTING STRUCTURAL STEEL, FINISH COAT							
514	00504	130	MNHR	GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL				130			
514	10000	38	EACH	FINAL INSPECTION REPAIR				38			
514	27700	1,629	SF	FIELD PAINTING, MISC.: COATING OF EXISTING GIRDER ENDS				1,629			5/58
516	10010	269	FT	ARMORLESS PREFORMED JOINT SEAL		269					
516	13600	35	SF	1" PREFORMED EXPANSION JOINT FILLER				35			
516	13900	233	SF	2" PREFORMED EXPANSION JOINT FILLER				233			
516	14020	272	FT	SEMI-INTEGRAL ABUTMENT EXPANSION JOINT SEAL		272					
516	44201	56	EACH	ELASTOMERIC BEARING WITH INTERNAL LAMINATES AND LOAD PLATE (NEOPRENE), AS PER PLAN (19" x 20" x 3.878" WITH 20" x 21" x 1.5" PLATE)		56					26/58
516	44201	14	EACH	ELASTOMERIC BEARING WITH INTERNAL LAMINATES AND LOAD PLATE (NEOPRENE), AS PER PLAN (14" x 16" x 3.878" WITH 16" x 23" x 1.5" PLATE)		14					26/58
516	44301	28	EACH	ELASTOMERIC BEARING WITH INTERNAL LAMINATES AND LOAD PLATE (NEOPRENE), AS PER PLAN (14" x 14" x 4.628" WITH 15" x 15" x 1.5" PLATE)		28					26/58
516	47001	LUMP		JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN							5/58
518	12201	6	EACH	SCUPPERS, INCLUDING SUPPORTS, AS PER PLAN				6			5/58
518	21200	31	CY	POROS BACKFILL WITH GEOTEXTILE FABRIC		31					
518	40000	275	FT	6" PERFORATED CORRUGATED PLASTIC PIPE		275					
518	40010	75	FT	6" NON-PERFORATED CORRUGATED PLASTIC PIPE, INCLUDING SPECIALS		75					
SPECIAL	51900100	11,303	SF	SPECIAL - COMPOSITE FIBER WRAP SYSTEM				11,303			6/58
526	25001	589	SY	REINFORCED CONCRETE APPROACH SLABS (T=15"), AS PER PLAN						589	53/58 & 54/58
526	90030	269	FT	TYPE C INSTALLATION						269	
601	20011	50	CY	CRUSHED AGGREGATE SLOPE PROTECTION, AS PER PLAN						50	5/58
601	34100	50	CY	ROCK CHANNEL PROTECTION, TYPE B WITHOUT FILTER						50	
625	98200	LUMP		LIGHTING, MISC.: REMOVE AND REERECT EXISTING UNDERPASS LIGHTING							6/58
809	00530	4	EACH	ITS JUNCTION BOX, 17" x 24" x 6"						4	
809	23900	675	FT	CONDUIT, 2" DIAMETER, HDPE				675			
840	23001	1221	CY	SELECT GRANULAR BACKFILL, AS PER PLAN		1221					20/58
863	00100	5568	SY	GEOGRID, TYPE P1		5568					20/58

ESTIMATED QUANTITIES
BRIDGE NO. SUM-277-3.856
I.R. 277/U.S. 224 OVER I.R. 77

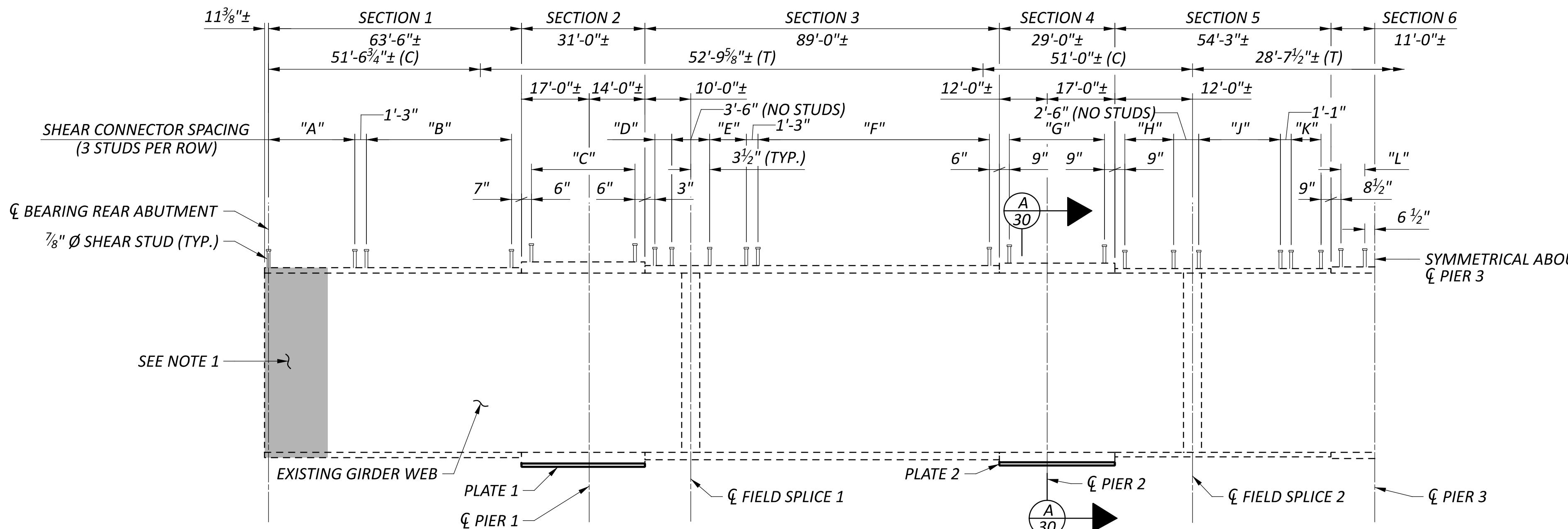
DESIGNER	CHECKER
JRE	KDC
REVIEWER	TES
PROJECT ID	121479
SUBSET	TOTAL
7	58
SHEET	TOTAL
P.071	122



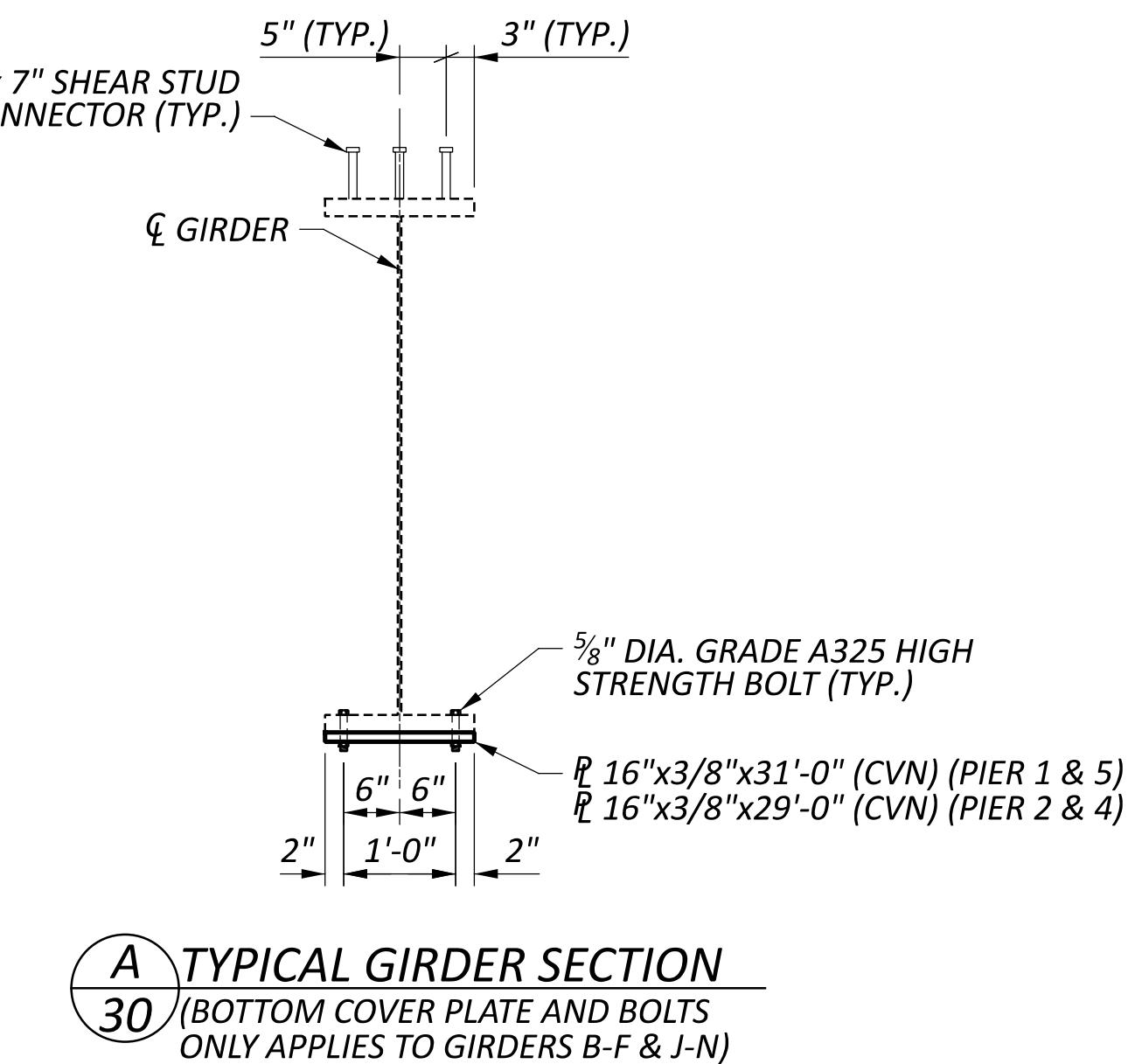
EXISTING GIRDERS A, G, H, & P ELEVATION

(4 TOTAL, NOT TO SCALE)
(INTERMEDIATE TRANSVERSE STIFFENERS NOT SHOWN)

SHEAR STUD SUMMARY TABLE	
LOCATION	RANGE
"A"	20 SPA. @ 13" = 21'-8"
"B"	33 SPA. @ 15" = 41'-3"
"C"	23 SPA. @ 15" = 28'-9"
"D"	4 SPA. @ 24" = 8'-0"
"E"	4 SPA. @ 24" = 8'-0"
"F"	54 SPA. @ 15" = 67'-6"
"G"	22 SPA. @ 15" = 27'-6"
"H"	8 SPA. @ 15" = 10'-0"
"J"	27 SPA. @ 15" = 33'-9"
"K"	5 SPA. @ 13" = 5'-5"
"L"	9 SPA. @ 13" = 9'-9"



EXISTING GIRDERS B-F & J-N ELEVATION

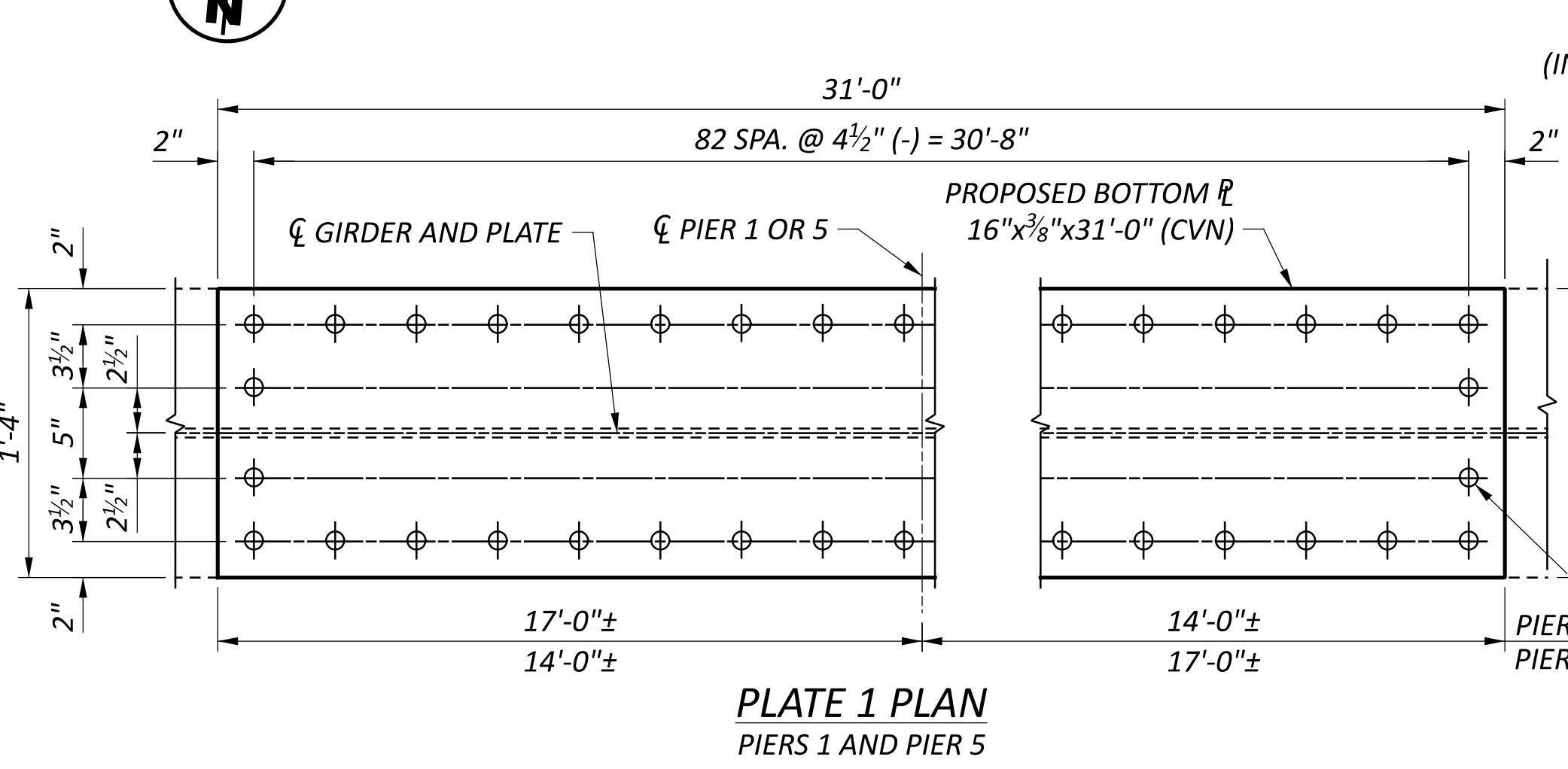
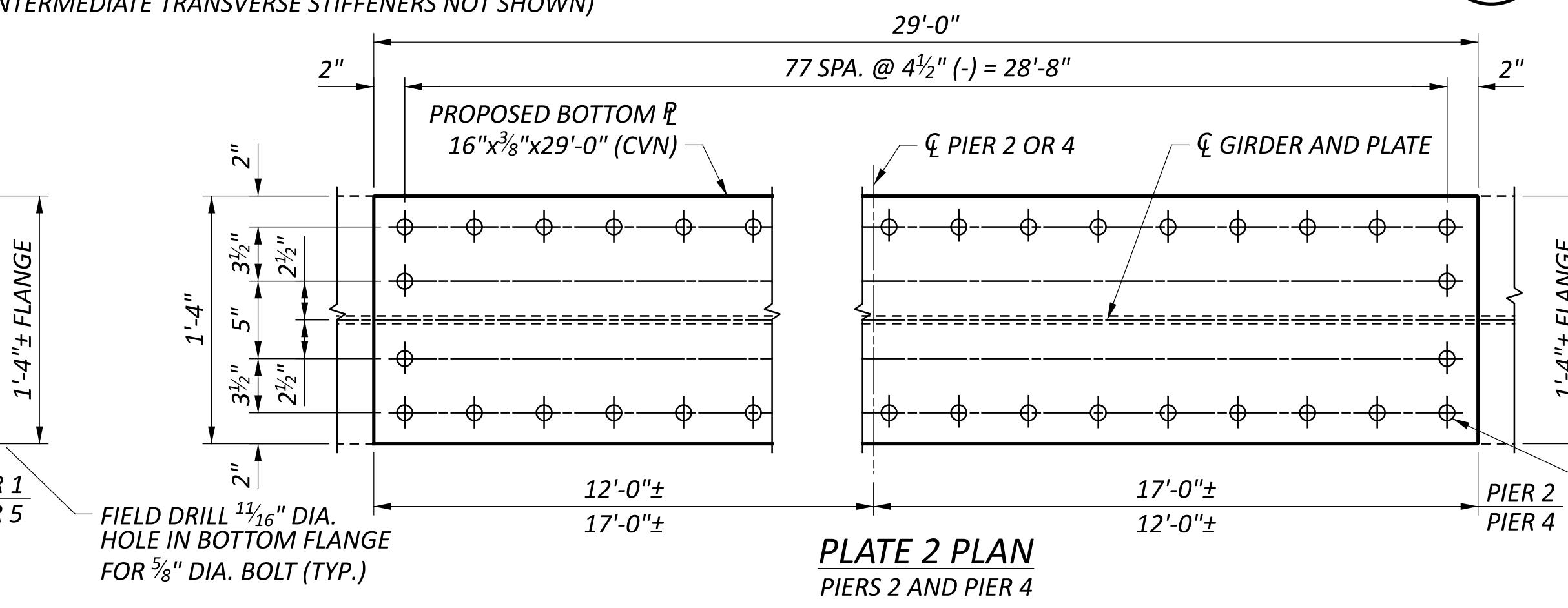
(10 TOTAL, NOT TO SCALE)
(INTERMEDIATE TRANSVERSE STIFFENERS NOT SHOWN)

A 30 TYPICAL GIRDER SECTION
(BOTTOM COVER PLATE AND BOLTS
ONLY APPLIES TO GIRDERS B-F & J-N)

LEGEND:
(C) - COMPRESSION
(T) - TENSION

NOTES:

- FIELD PREPARE AND PAINT THE LAST 4'-0" AT EACH EXISTING GIRDER END, UNLESS NOTED OTHERWISE, PER ITEM 514 - FIELDPAINTING, MISC. COATING OF EXISTING GIRDER ENDS (SEE PLANNOTE ON SHEET 5/58).
- WELD ATTACHMENT OF SUPPORTS FOR CONCRETE DECK FINISHING MACHINE TO AREAS OF THE FASCIA GIRDER FLANGES DESIGNATED "COMPRESSION." DO NOT WELD ATTACHMENTS TO AREAS DESIGNATED "TENSION." FILLET WELDS TO COMPRESSION FLANGES SHALL BE AT LEAST 1" FROM EDGE OF FLANGE, BE NO MORE THAN 2" LONG, AND BE AT LEAST $\frac{1}{4}$ " FOR THICKNESSES UP TO $\frac{3}{4}$ " OR $\frac{5}{16}$ " FOR GREATER THAN $\frac{3}{4}$ " THICK.
- FOR FRAMING PLAN, SEE SHEET 28/58.
- ALL NEW STRUCTURAL STEEL SHALL BE ASTM A709 GRADE 50 (PAINTED).
- THE CONTRACTOR MAY PLACE THE SHEAR STUDS PARALLEL WITH THE BRIDGE SKEW TO FACILITATE THE PLACEMENT OF THE BRIDGE DECK REINFORCING BARS.
- SHEAR STUD CONNECTORS SHALL BE WELDED PER C&MS 513.22.
- WHEN A PLATE IS DESIGNATED (CVN), FURNISH MATERIAL THAT MEETS THE MINIMUM NOTCH TOUGHNESS REQUIREMENTS AS SPECIFIED IN 711.01.
- PRIOR TO INSTALLING PLATES 1 AND 2, THE CONTRACTOR SHALL BLAST CLEAN THE EXISTING FLANGES AND APPLY AN ORGANIC ZINC PRIME COAT IN ACCORDANCE WITH CMS 514.

PLATE 1 PLAN
PIERS 1 AND PIER 5PLATE 2 PLAN
PIERS 2 AND PIER 4

