

# 7028

FED. RD. DIV. NO.	STATE	PROJECT	FISCAL YEAR
2	OHIO		1946

WILLIAMS COUNTY  
 S.H. 304 SEC. BRYAN (PT.)  
 S.H. 305 SEC. BRYAN (PT.)  
 S.H. 306 SEC. BRYAN (PT.)  
 S.H. 456 SEC. BRYAN (PT.)

# STATE OF OHIO DEPARTMENT OF HIGHWAYS

## BRYAN-FT. WAYNE ROAD S.H. 304 SEC. BRYAN (PT.) BRYAN-DEFIANCE ROAD S.H. 305 SEC. BRYAN (PT.) BRYAN-PIONEER ROAD S.H. 306 SEC. BRYAN (PT.) BRYAN-EVANSPORT ROAD S.H. 456 SEC. BRYAN (PT.)

### WILLIAMS COUNTY CITY OF BRYAN

#### CONVENTIONAL SIGNS.

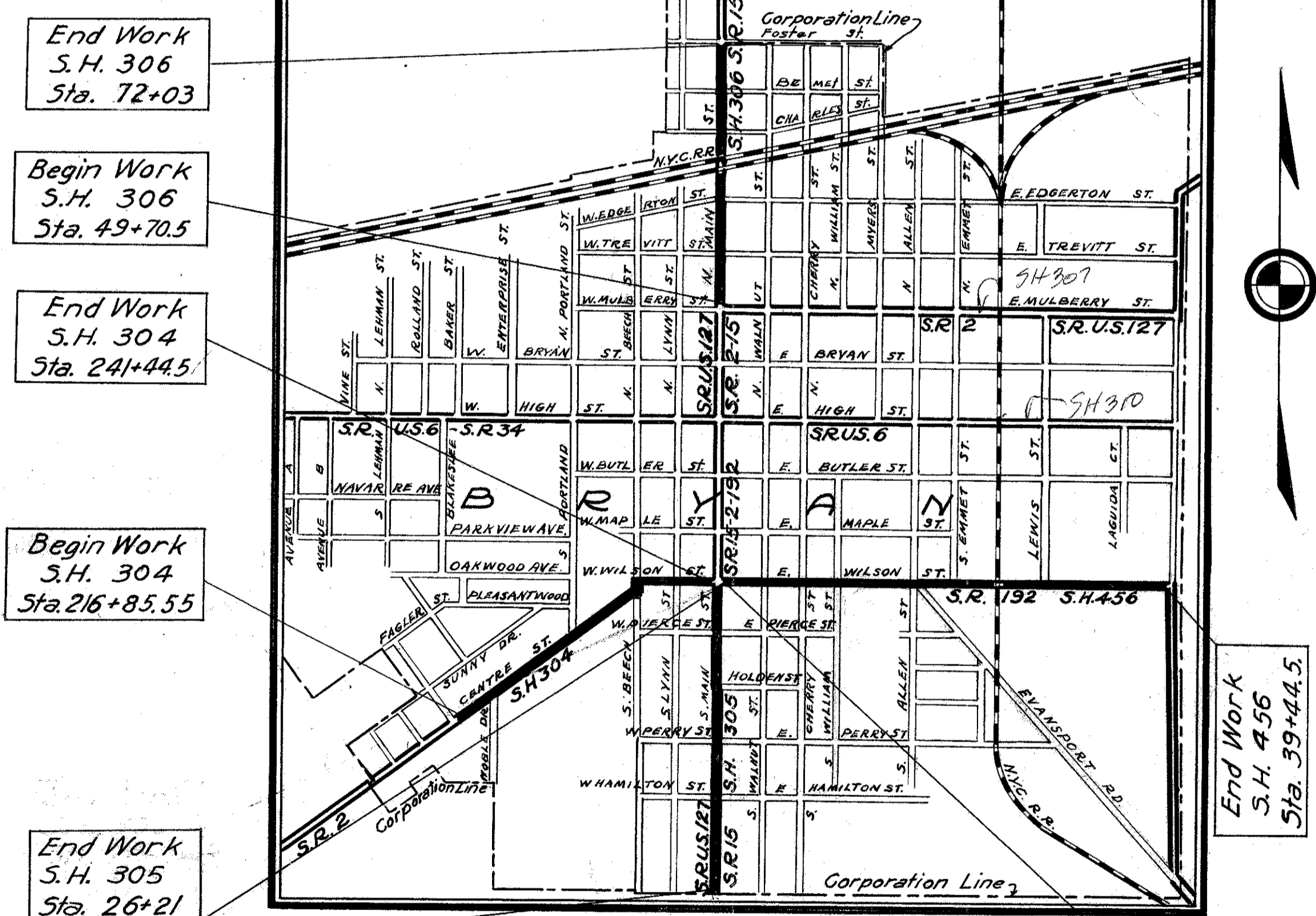
STATE LINE	-----
COUNTY LINE	-----
TOWNSHIP LINE	-----
SECTION LINE	-----
CENTER LINE	-----
PROPERTY LINE	-----
CITY OR VILLAGE LINE	-----
FENCE LINE	-x-x-x-x-
STEAM RAILROAD	====+====
ELECTRIC RAILROAD	---+---+---+---
DOLE LINE	o-o-o-o-o
GUARD RAIL	-----
DRAIN PIPE, NEW	-----
DRAIN PIPE, OLD	-----

#### INDEX OF SHEETS.

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#### LINE DATA

Begin S.H. 304 Sta 216+85.55 (Center St.)  
 End S.H. 304 Sta 241+44.50 (W. Wilson St.)  
 Gross Length = Net Length = 2458.95 Lin. Ft.  
 Begin S.H. 305 Sta 0+00 (S. Main St.)  
 End S.H. 305 Sta 26+21 (S. Main St.)  
 Gross Length = Net Length = 2621.00 Lin. Ft.  
 Begin S.H. 306 Sta 49+70.5 (N. Main St.)  
 End S.H. 306 Sta 72+03 (N. Main St.)  
 Gross Length = 2232.50 Lin. Ft.  
 Deduction for Railroad Sta 60+74 to 61+39 = 65.00 Lin. Ft.  
 Net Length = 2167.50 Lin. Ft.  
 Begin S.H. 456 Sta 0+20 (E. Wilson St.)  
 End S.H. 456 Sta 39+44.5 (E. Wilson St.)  
 Gross Length = 3924.50 Lin. Ft.  
 Deduction for Railroad Sta 23+16 to 23+98 = 82.00 Lin. Ft.  
 Net Length = 3842.50 Lin. Ft.  
 Total Length of Work (Municipal)  
 2458.95 + 2621.00 + 2167.50 + 3842.50 =  
 11,089.95 Lin. Ft. or 2.100 Mile



#### LOCATION PLAN.

SCALE: 1" = 1000'  
 DELIVERY POINT - BRYAN AVERAGE HAUL - 1 MILE

PORTION TO BE IMPROVED STATE HIGHWAYS  
 OTHER HIGHWAYS

#### SCALES

PLAN ~ 1" = 50'  
 PROFILE - HORIZONTAL ~ 1" = 50'  
 PROFILE - VERTICAL ~ 1" = 5'

The Standard Specifications of The State of Ohio, Department of Highways, including changes and Supplemental Specifications listed in the Proposal shall govern this improvement.

I hereby approve these plans and declare that the making of this improvement will not require the closing to traffic of the highway and that traffic will be maintained as shown on the plans and estimates.

The Right-of-Way necessary for this improvement will be provided by the State of Ohio.

Approved Paul W. Finch  
 Date 4/22/46 Assistant to the Chief Engineer

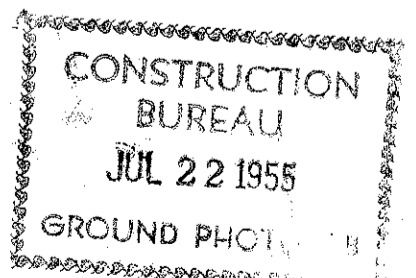
Approved \_\_\_\_\_  
 Date \_\_\_\_\_ Chief Engineer, Bureau of Maintenance.

Approved \_\_\_\_\_  
 Date \_\_\_\_\_ Chief Engineer, Bureau of Bridges & R.R. Crossings.

Approved Charles E. ...  
 Date 5/23/46 Chief Engineer, Bureau of Location & Design

Approved Edwin W. Collins  
 Date 8/23/46 First Asst. Director & Chief Engineer.

Approved Perry I. Ford  
 Date 8/23/46 Director of Highways.



#### STANDARD DRAWINGS.

G 707,	6-1-42
I-12	7-1-42
B-T-50-70-71 E No 1	12-1-43

#### SUPPLEMENTAL SPECIFICATIONS.

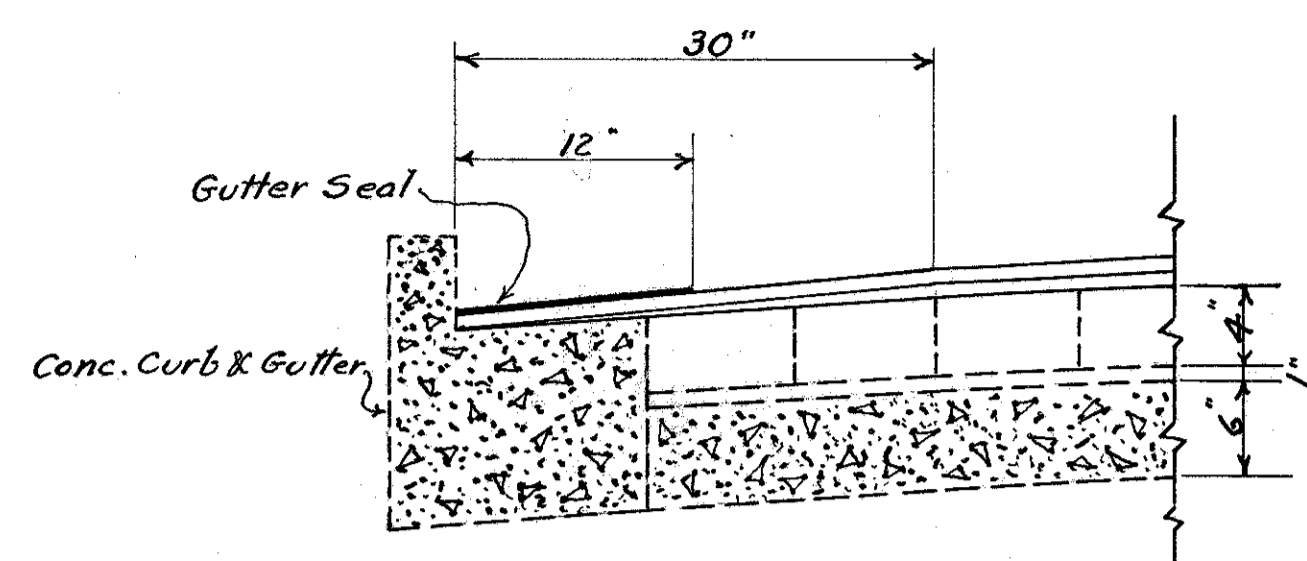
177	Rev. 4-21-44

FILE NO. WILLIAMS CO. S.H. 304, 305, 306, 456 SEC. BRYAN (PT.'S)  
 DATE OF LETTING 1946  
 CONTRACT NO.

# TYPICAL SECTIONS.

## TYPE T-35.

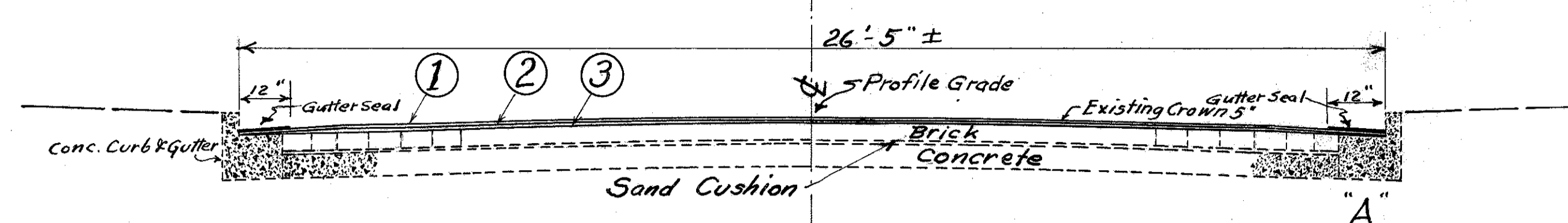
WILLIAMS COUNTY.  
 S.H.- 304, SEC. BRYAN (Pt.)  
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 S.H.- 306, SEC. BRYAN (Pt.)  
 S.H.- 456, SEC. BRYAN (Pt.)



Detail showing Gutter Resurfacing

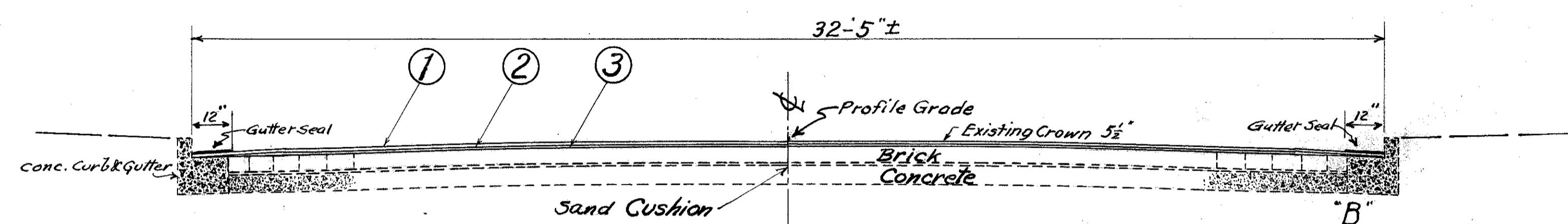
NOTE:- Special Care shall be exercised during construction to obtain maximum compaction of Bituminous Concrete in all gutters.

Average Height of Curbs - 6"  
 All existing pavement is grout filled Brick on Sand Cushion with 6" Concrete Base.



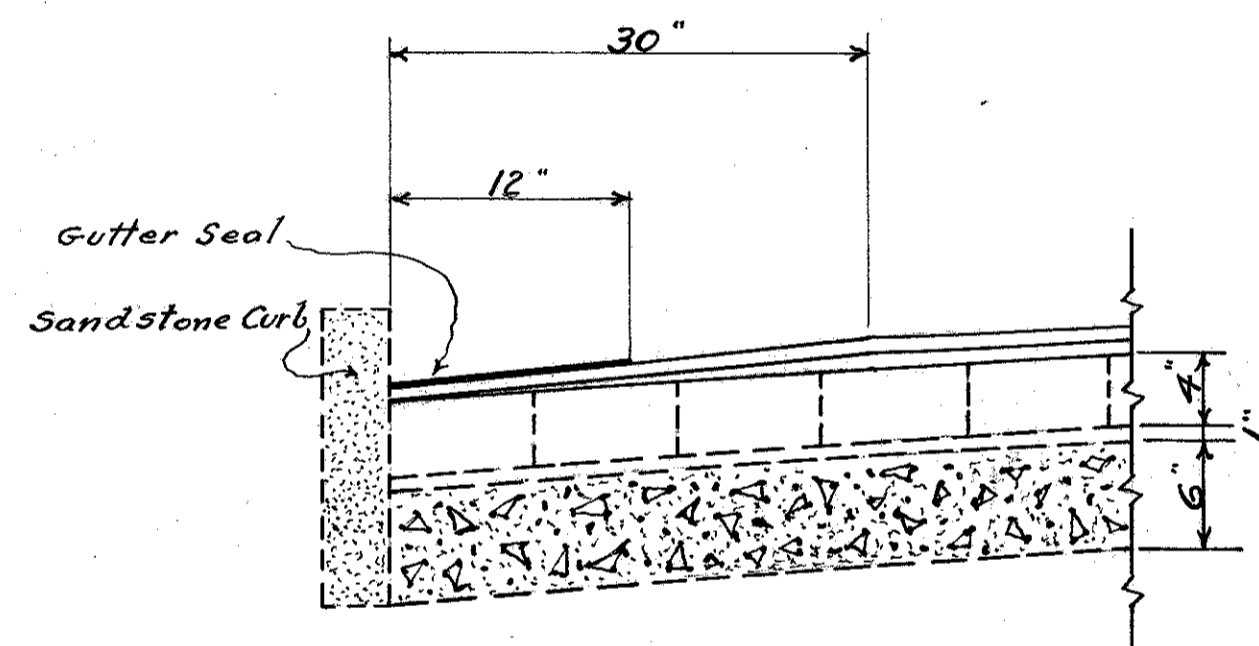
The above Section is to be used on S.H. 305

From Sta. 0+00 to Sta. 22+08 = 2208 Lin. Ft.

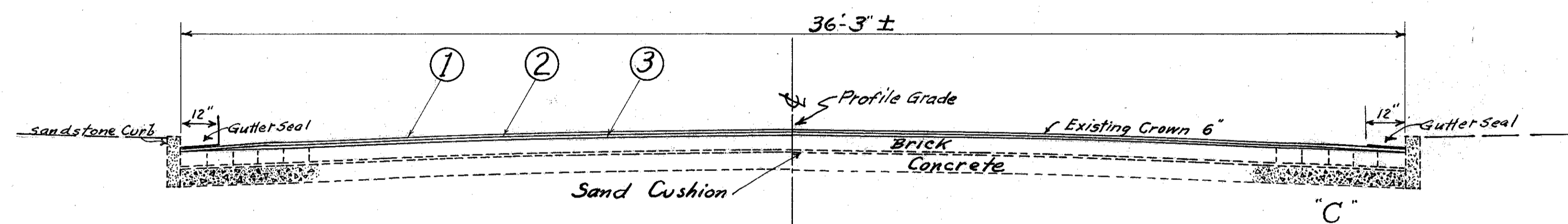


The above Section is to be used on S.H. 305

From Sta. 22+08 to Sta. 26+21 = 413 Lin. Ft.

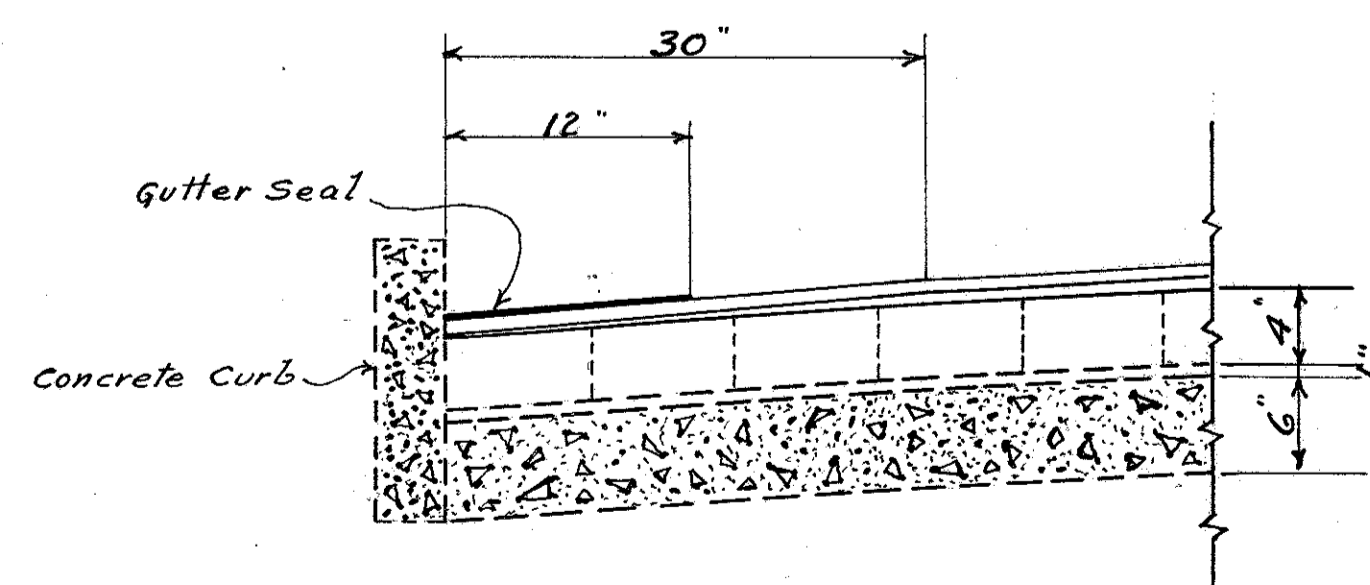


Detail showing Gutter Resurfacing

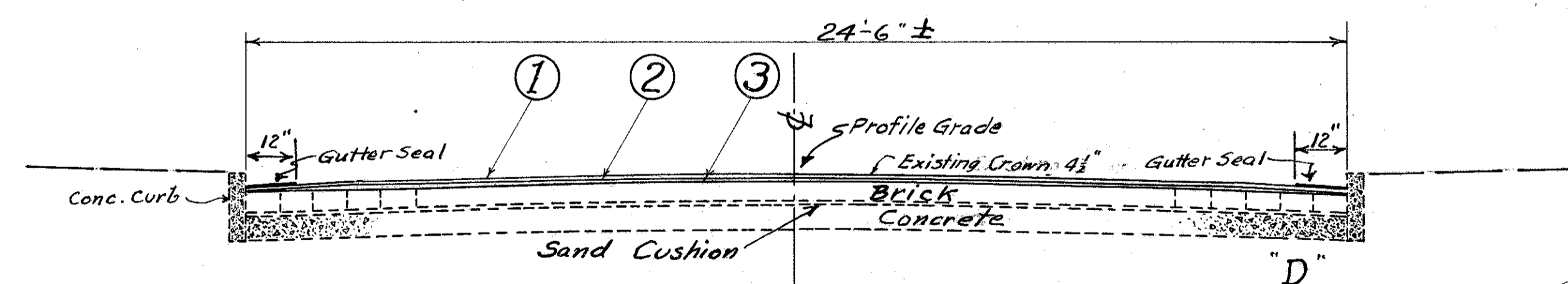


The above Section is to be used on S.H. 306

From Sta. 49+82.5 to Sta. 60+74 = 1091.5 Lin. Ft.



Detail showing Gutter Resurfacing



The above Section is to be used on S.H. 306

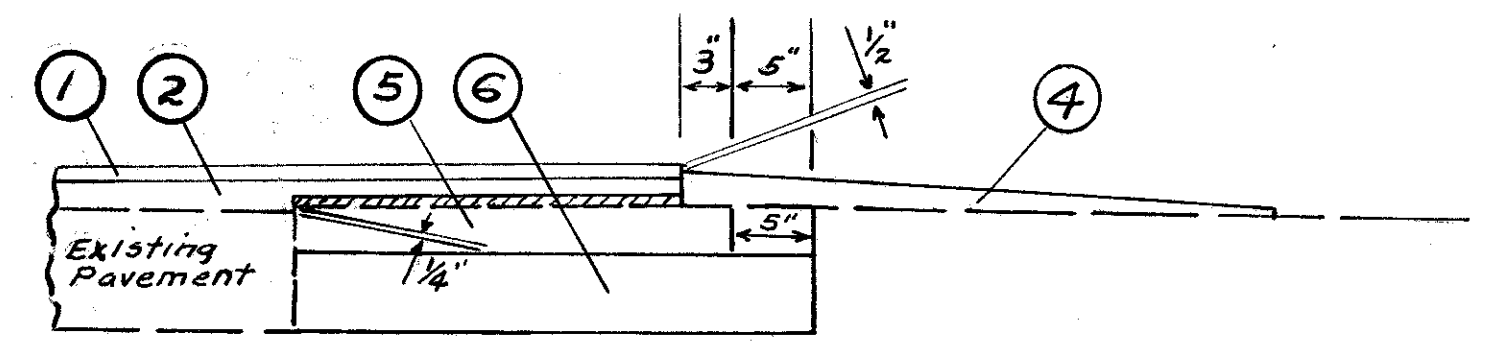
From Sta. 61+39 to Sta. 71+53 = 1014 Lin. Ft.

- ① Item T-35 - 1" Asphaltic Concrete Surface Course - Type "C".
- ② Item B-35 - 1" Minimum Thickness, Asphaltic Concrete Leveling Course.
- ③ Item T-30 - Bituminous Tack Coat, using Bituminous Material, Sec. M-5.5, MS-1, applied at the rate of 0.10 Gal. per Sq Yd., including Sand Cover.

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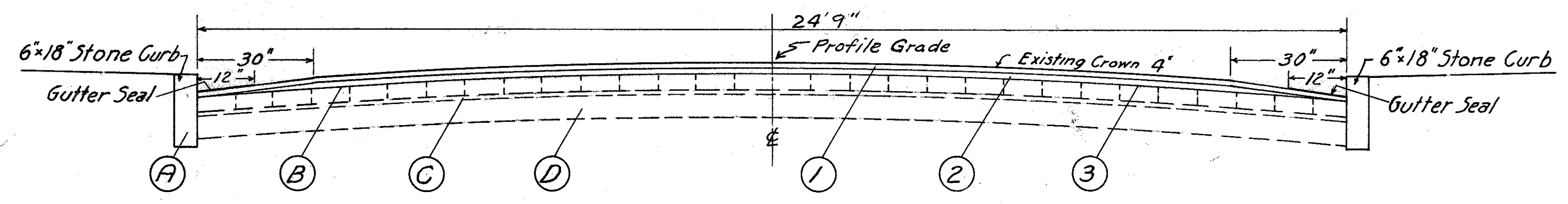
# TYPICAL SECTIONS.

## TYPE-T-35.



DETAIL OF EDGE CONSTRUCTION.

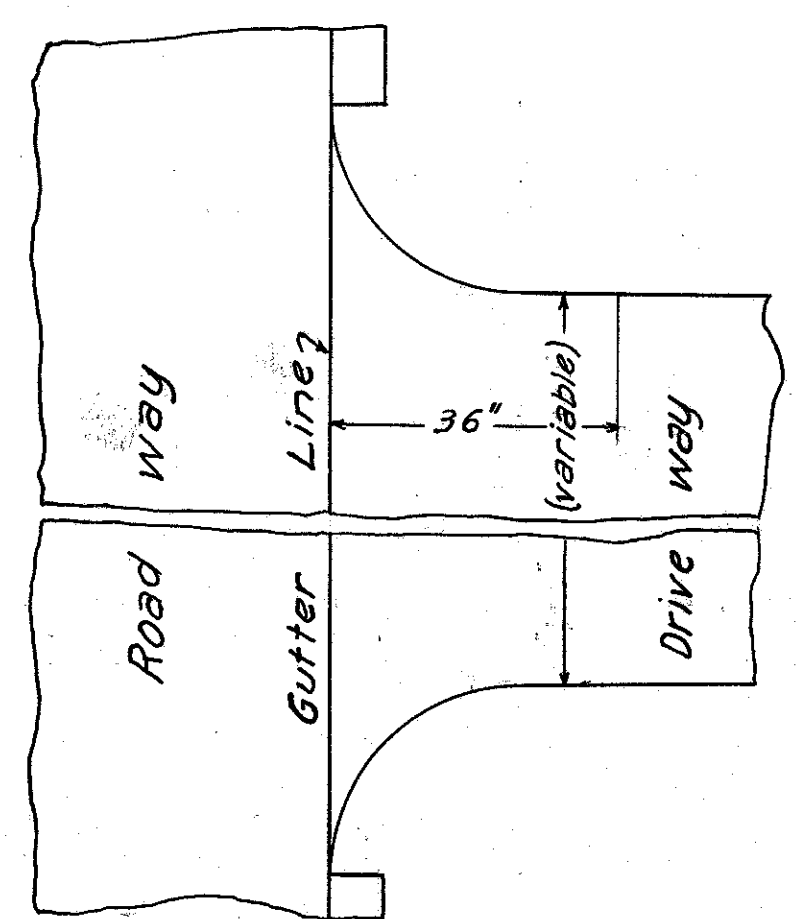
NOTE: Where the 3 inch leveling course in the widened area would finish more than 1/4 inch above the edge of the existing pavement that portion below the dashed line shall be placed and compacted in a separate operation. The hatched portion above the dashed line shall be placed simultaneously with the full width leveling course.



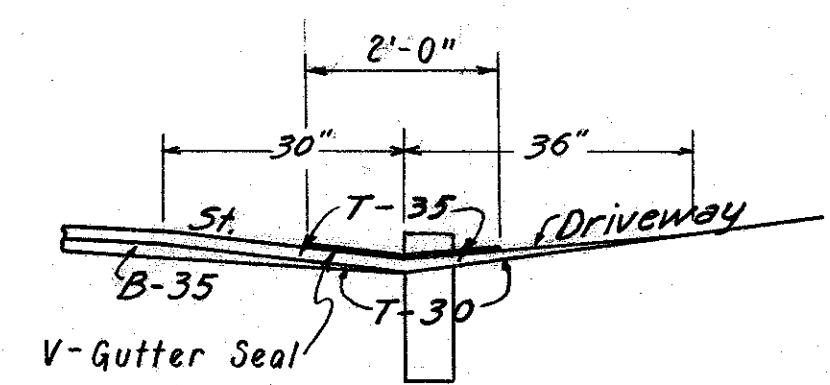
The above Typical Section applies from Sta. 216+85.55 to Sta. 233+96.14 = 1710.59 Lin. Ft. (S.H. 304) TYPICAL SECTION "E"

EXISTING PAVEMENT

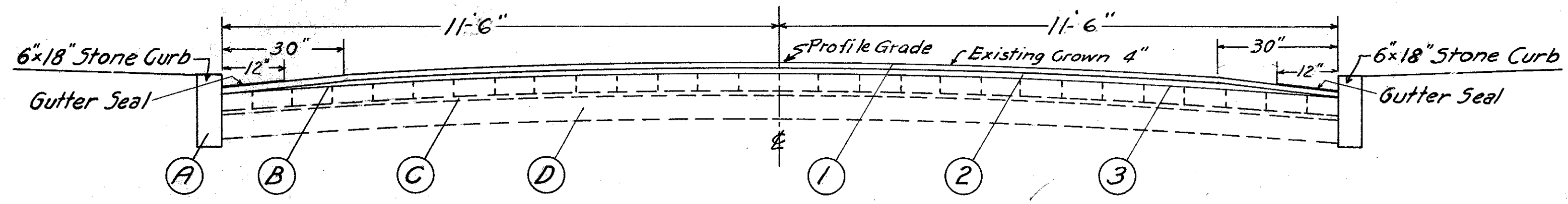
- (A) 6"x18" Stone Curb.
- (B) Light bituminous treatment on 4" brick pavement.
- (C) 1" Sand Cushion.
- (D) 6" Concrete Base.



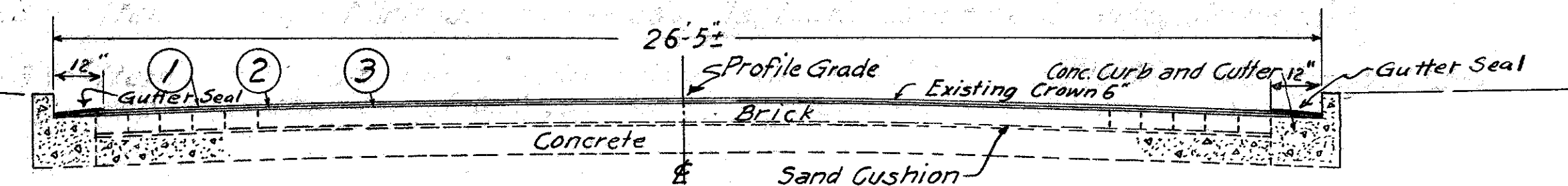
TYPICAL TREATMENT OF DRIVEWAYS.



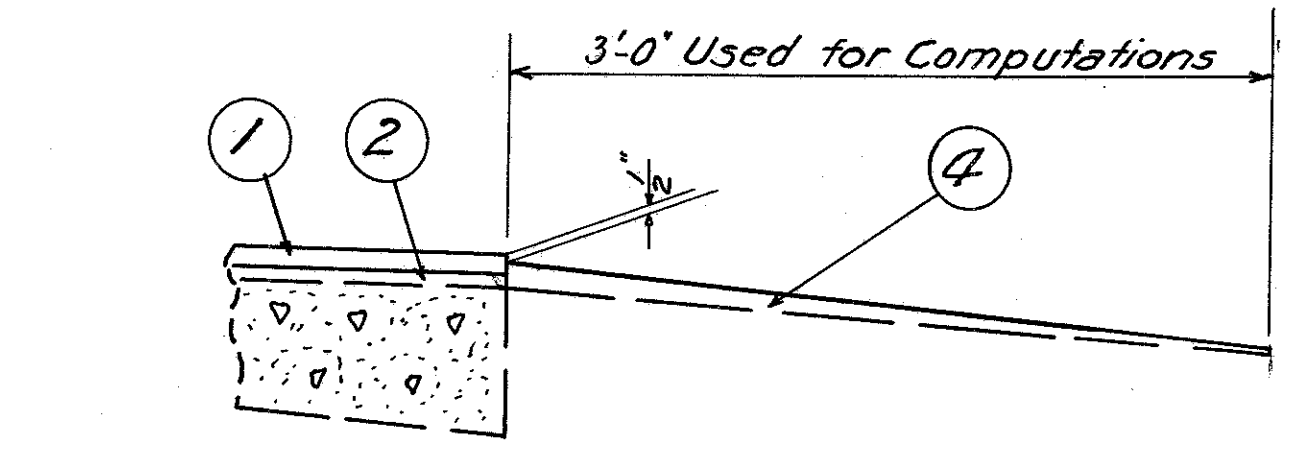
TYPICAL SEC. THRU & DRIVEWAYS.



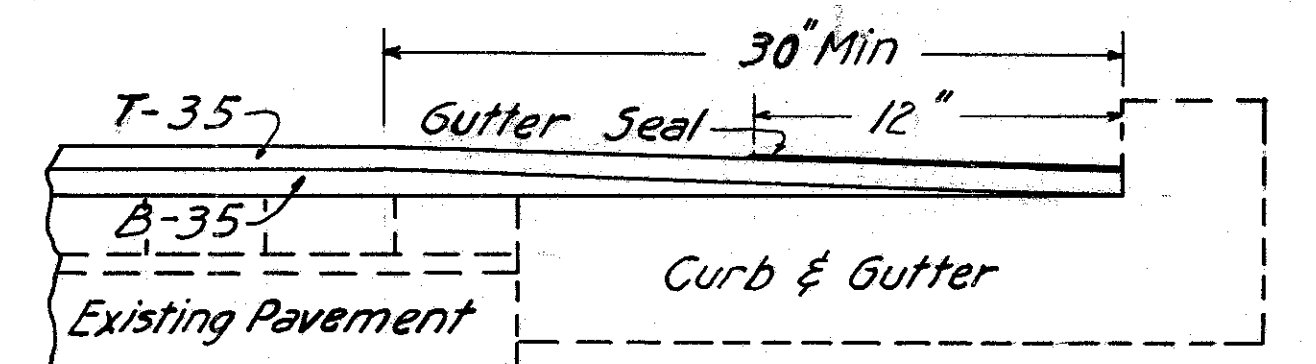
Above Typical Section applies from Sta. 234+50.45 to Sta. 241+44.5 = 694.05 Lin. Ft. (S.H. 304) TYPICAL SECTION "F"



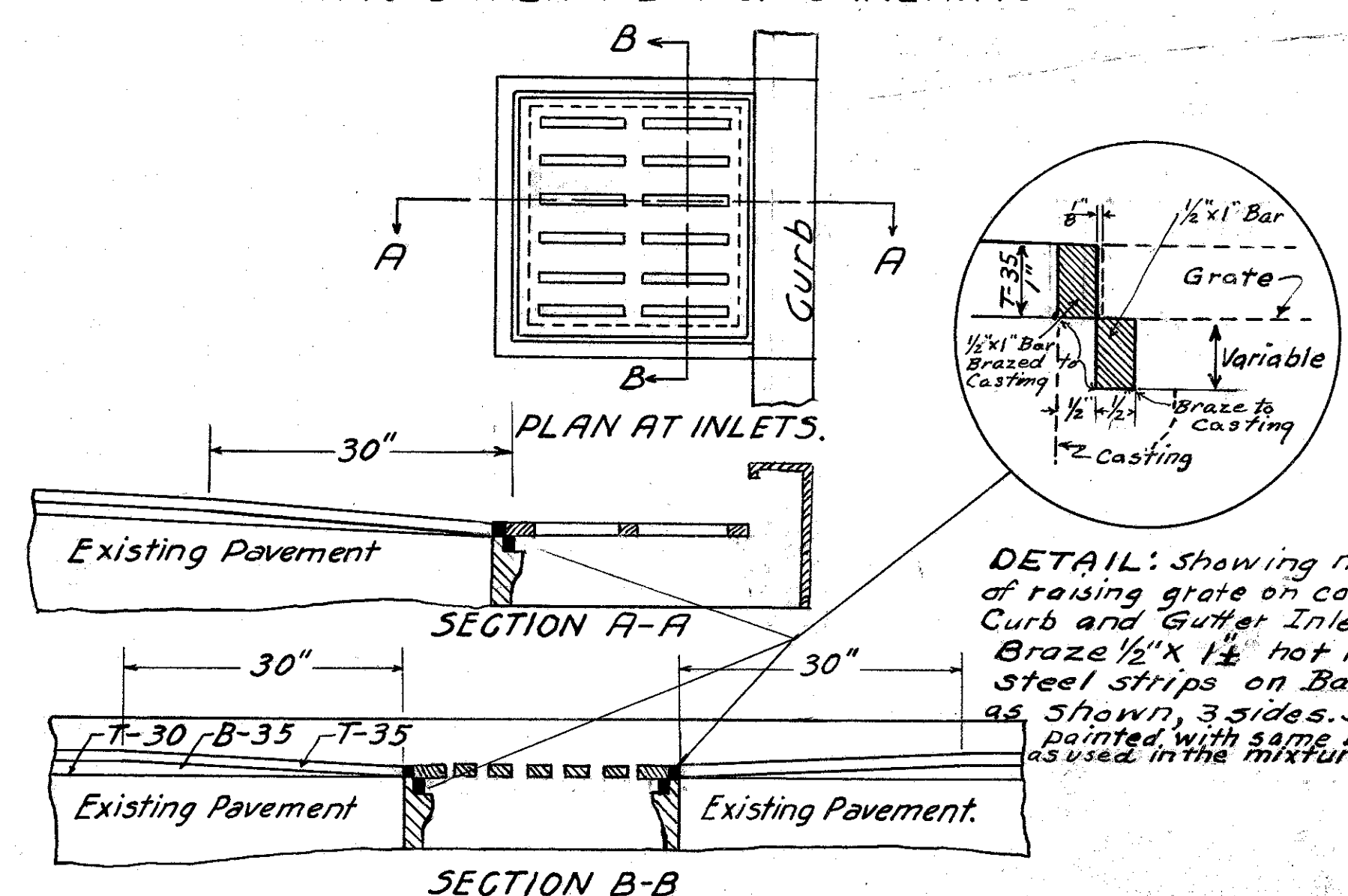
The above Typical Section applies from Sta. 0+20.0 to Sta. 26+44.5 = 2624.5 Lin. Ft. (S.H. 456) TYPICAL SECTION "G"  
 Deduction for R.R. Sta. 23+16.0 to Sta. 23+98.0 = 82.0 Lin. Ft.  
 Net = 2542.5 Lin. Ft.



DETAIL SHOWING BERM TREATMENT AT EDGE OF PAVEMENT.

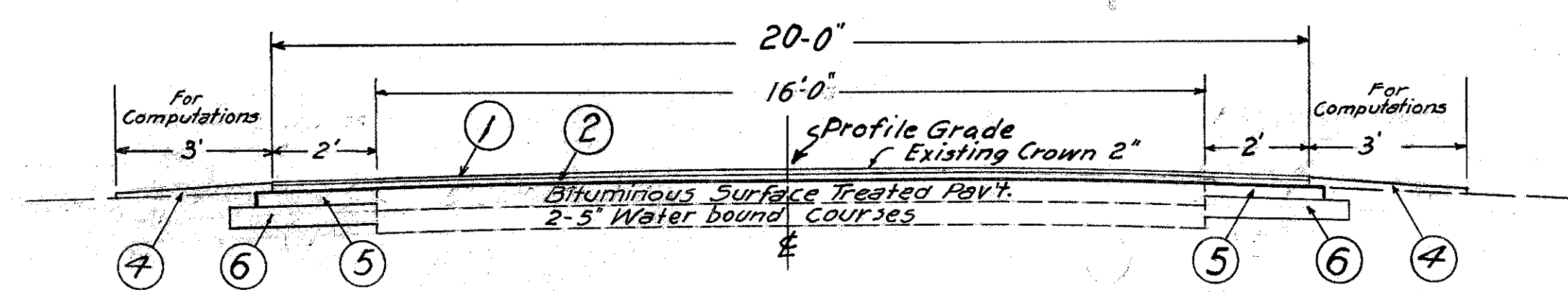


DETAIL SHOWING GUTTER RESURFACING  
 Special care shall be exercised during construction to obtain maximum compaction of bituminous concrete in all gutters.

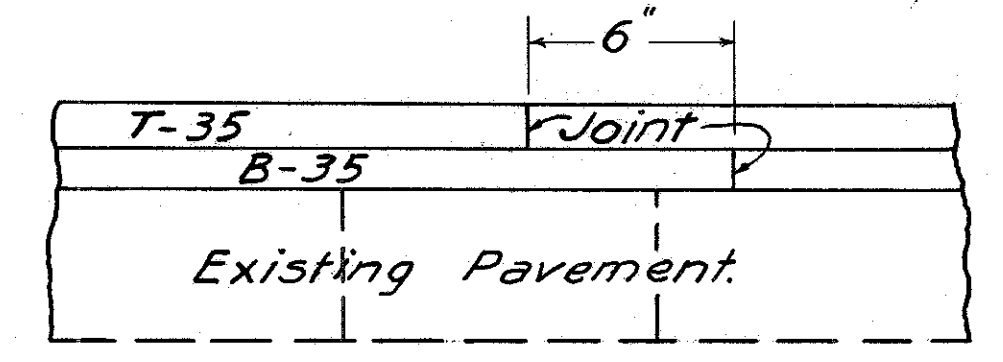


DETAIL OF SURFACING AT INLETS.

DETAIL: Showing method of raising grate on combined Curb and Gutter Inlets.  
 Braze 1/2" x 1 1/2" hot rolled mild steel strips on Base Castings as shown, 3 sides. Steel strips to be painted with same bituminous material as used in the mixture.



The above Typical Section applies from Sta. 26+44.5 to Sta. 39+44.5 = 1300 Lin. Ft. (S.H. 456) TYPICAL SECTION "H"



Detail showing method of lapping longitudinal joints in bituminous concrete construction.

- ① Item T-35, 1" Asphaltic Concrete Surface Course, Type "C".
- ② Item B-35, 1" Minimum Thickness, Asphaltic Concrete Leveling Course.
- ③ Item T-30, Bituminous Tack Coat, using Bituminous Material Sec. M-5.5, M-5-1 applied at the rate of 0.10 gal. per sq. yd. including sand cover.
- ④ Item I-17, Side Approaches, Mail Box Turnouts, and Berm Material.
- ⑤ Item B-35, 3" Asphaltic Concrete Leveling Course.
- ⑥ Item I-20, 5" Insulation Course, Coarse Graded Type.

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# PAVEMENT CALCULATIONS.

Net Length Typical Section "A" (Width 26.42')	=	2208	Lin. Ft.
Area (2208 × 26.42) ÷ 9	=	6482	Sq. Yds.
Net Length Typical Section "B" (Width 32.42)	=	413	Lin. Ft.
Area (413 × 32.42) ÷ 9	=	1488	Sq. Yds.
Net Length Typical Section "C" (Width 36.25)	=	1091.50	Lin. Ft.
Area (1091.5 × 36.25) ÷ 9	=	4396	Sq. Yds.
Net Length Typical Section "D" (Width 24.5)	=	1014	Lin. Ft.
Area (1014 × 24.5) ÷ 9	=	2760	Sq. Yds.
Net Length Typical Section "E" (Width 24.75)	=	1710.59	Lin. Ft.
Area (1710.59 × 24.75) ÷ 9	=	4704	Sq. Yds.
Net Length Typical Section "F" (Width 23.0)	=	694.05	Lin. Ft.
Area (694.05 × 23.0) ÷ 9	=	1774	Sq. Yds.
Net Length Typical Section "G" (Width 26.42)	=	2542.50	Lin. Ft.
Area (2542.5 × 26.42) ÷ 9	=	7464	Sq. Yds.
Net Length Typical Section "H" (Width 20.0)	=	1300	Lin. Ft.
Area (1300 × 20.0) ÷ 9	=	2889	Sq. Yds.
Total Area 6482 + 1488 + 4396 + 2760 + 4704 + 1774 + 7464 + 2889	=	31957	Sq. Yds.

Item E-8, Removal and Disposal of Existing Brick Wearing Course (Estimated) = 500 Sq. Yds. To be used for correction of crown of intersecting pavements as directed by the Engineer.

Item T-35, 1" Asphaltic Concrete Surface Course, Type "C"	=		
Area from Total Typical Sections	=	31957	Sq. Yds.
Add from Table "P"	=	3080	Sq. Yds.
Add from Table "A"	=	302	Sq. Yds.
Total	=	35339	Sq. Yds.
Volume (35339 × 1) ÷ 36	=	982	Cu. Yds.

Item B-35, 1" Asphaltic Concrete Leveling Course	=		
Area from Typical Sections	=	31957	Sq. Yds.
Add from Table "P"	=	2274	Sq. Yds.
Gross Area	=	34231	Sq. Yds.
Deduction for Edge Feathering on Typical Sections A, B, C, D, E, F, & G (9,673.64 × 2.5) ÷ 9	=	2687	Sq. Yds.
Total Net Area	=	31544	Sq. Yds.
Volume (31544 × 1) ÷ 36	=	876	Cu. Yds.
Extra Leveling to Strengthen Weak Areas Estimated	=	250	Cu. Yds.
Extra Leveling to remove surface irregularities Estimated	=	250	Cu. Yds.
Total to use	=	1376	Cu. Yds.

Item T-30, Bituminous Tack Coat	=		
Area from Typical Sections A, B, C, D, E, F, & G	=	29068	Sq. Yds.
Add from Table "P"	=	3080	Sq. Yds.
Add from Table "A"	=	302	Sq. Yds.
Total Area	=	32450	Sq. Yds.
Volume 32450 × 0.10 Gal. per Sq. Yd.	=	3245	Gals.

Item E-8, Removal and Disposal of Bituminous Wearing Surface	=		
From Table "P"	=	518	Sq. Yds.

Item I-17, Side Approaches, Mail Boxes, Turnouts and Berm Material	=		
Net Length Typical Section "H"	=	1300	Lin. Ft.
Estimated average depth 2" width 3"	=		
Volume (1300 × 3' × 2" × 2) ÷ 27 × 12	=	48	Cu. Yds.
Add from Table "P"	=	4	Cu. Yds.
Add from Table "A"	=	40	Cu. Yds.
Total	=	92	Cu. Yds.
Add 15% for Compaction 92 × 115%	=	106	Cu. Yds.

Item B-35, 3" Asphaltic Concrete Leveling Course	=		
Net length Typical Section "H" (Width 2'-3")	=	1300	Lin. Ft.
Area (1300 × 2.25 × 2) ÷ 9	=	650	Sq. Yds.
Volume (650 × 3) ÷ 36	=	54	Cu. Yds.

Item I-20, 5" Insulation Course, Course - Graded Type	=		
Net Length Typical Section "H" (Width 2'-8")	=	1300	Lin. Ft.
Area (1300 × 2.67 × 2) ÷ 9	=	772	Sq. Yds.

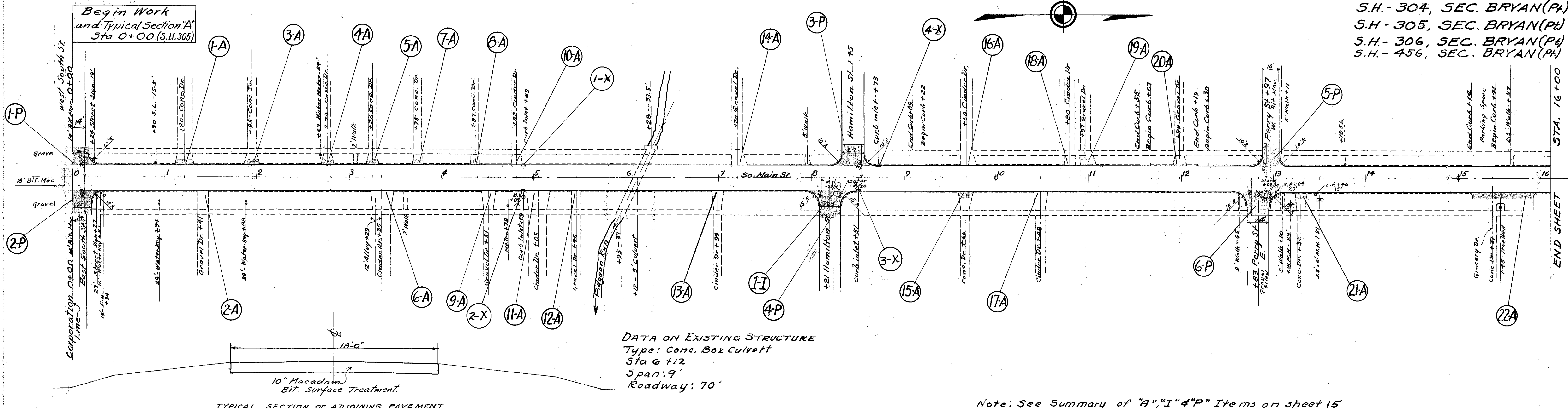
Item E-1, Roadway Excavation (Unclassified)	=		
Net length Typical Section "H" Width 2'-8", Depth 8"	=		
Volume (1300 × 2.67 × .67 × 2) ÷ 27	=	172	Cu. Yds.
Add from B-P 75 Cu Yd + 172	=	247	Cu. Yds.

## GENERAL SUMMARY.

ITEM	Quantity	UNIT	DESCRIPTION.
E-1	247	Cu. Yd.	Roadway Excavation (Unclassified)
E-8	518	Sq. Yd.	Removal & Disposal of Existing Bituminous Wearing Surface.
E-8	132	Lin. Ft.	Removal & Storage of Existing Stone Curb
E-8	550	Sq. Ft.	Removal and Disposal of Concrete Sidewalk
I-8	10	Each	Manhole Castings adjusted to Grade.
I-8	30	Each	Inlet Castings adjusted to Grade.
I-8	5	Each	Monument Boxes and Iron Pin adjusted to Grade.
I-17	106	Cu. Yd.	Side Approaches, Mail Box Turnouts and Berm Material
I-13	405	Sq. Ft.	4" Concrete Sidewalk
E-8	500	Sq. Yd.	Removal and Disposal of Existing Brick Wearing Course
T-30	3245	Gal.	Bituminous Tack Coat, Sec. M-55, M5-1, including sand cover.
T-35	982	Cu. Yd.	Asphaltic Concrete Surface Course, Type "C."
B-35	1430	Cu. Yd.	Asphaltic Concrete Leveling Course.
B-70	139	Sq. Yd.	8" Portland Cement Concrete Base Course.
I-12	118	Lin. Ft.	Type 2B Concrete Curb and Gutter.
I-20	772	Sq. Yd.	5" Insulation Course

Note: T-35 Surface Course feathered to grade of existing surface Sta 0+00.

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 S.H. - 456, SEC. BRYAN (PA)



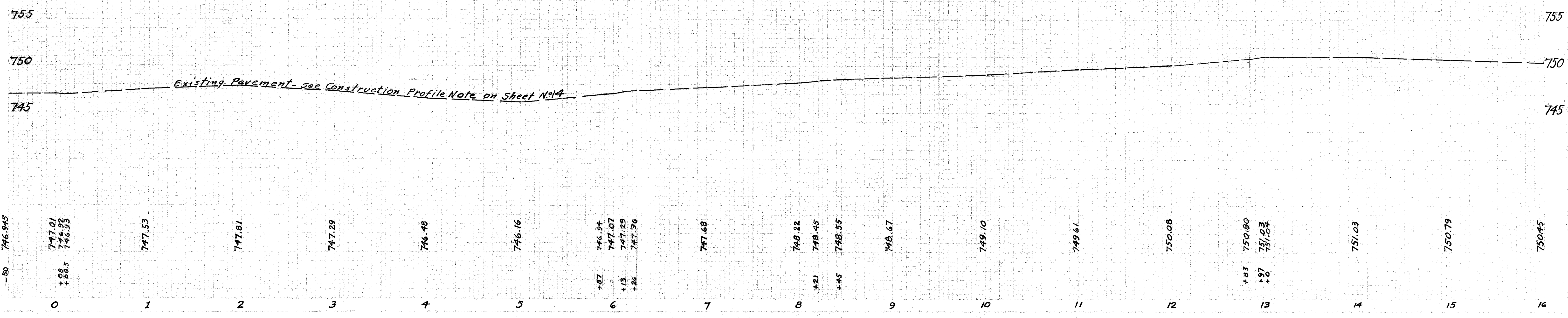
DATA ON EXISTING STRUCTURE  
 Type: Conc. Box Culvert  
 Sta 6+12  
 Span: 9'  
 Roadway: 70'

Note: See Summary of "A", "I" & "P" Items on sheet 15  
 See Summary of "X" Items on sheet No. 14

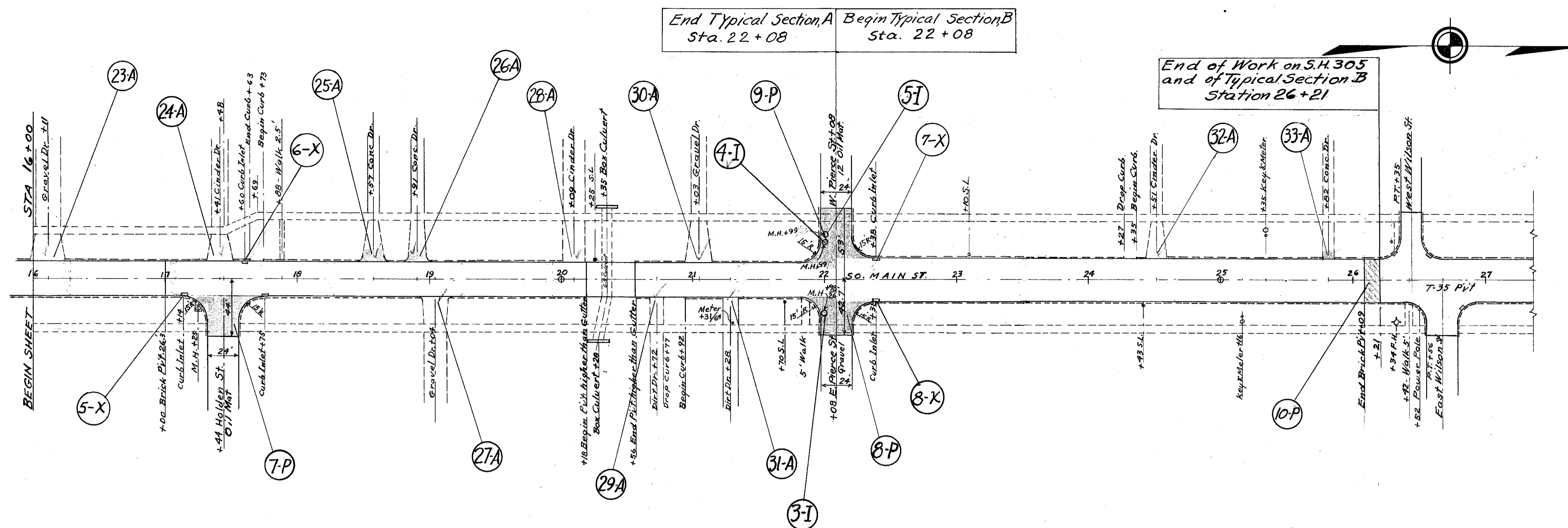
B.M. No. 1 - W. Bolt Hydrant Cap  
 19.0' Rt. - Sta. 0+34  
 Elev. 750.19

B.M. No. 2 - N.E. Cor. Top Headwall  
 33.5' Lt. - Sta. 6+33  
 Elev. 749.34

B.M. No. 3 - S.W. Bolt Hydrant Cap  
 29.0' Rt. - Sta. 13+18  
 Elev. 752.76



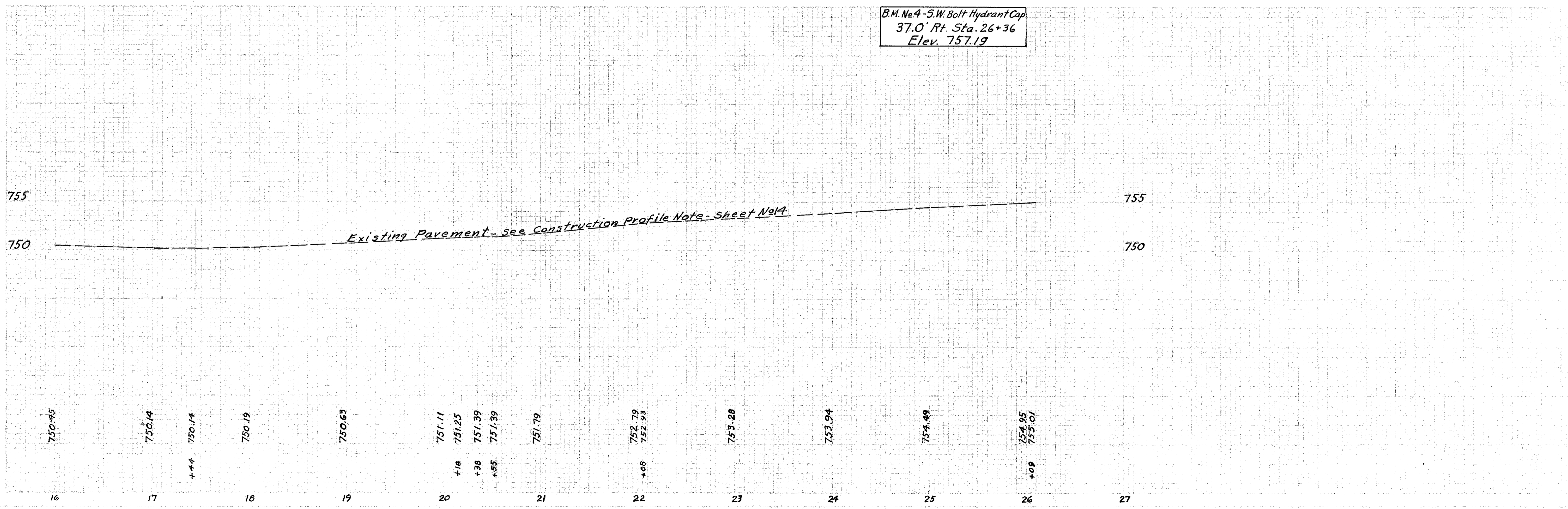
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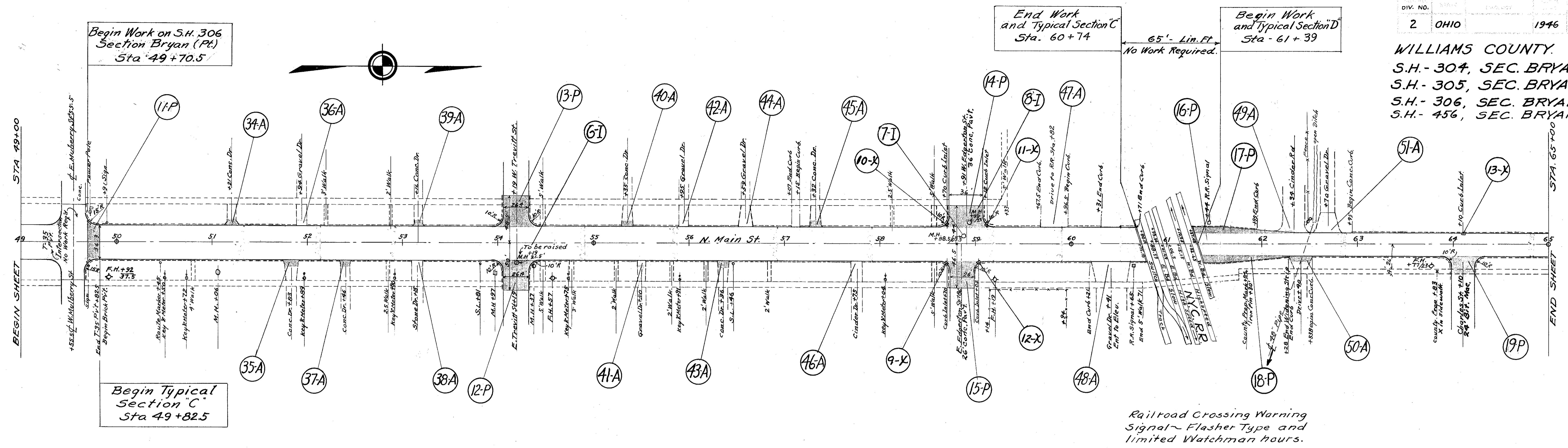
DATA ON EXISTING STRUCTURE  
 Sta 20+35  
 Type: Conc. Box Culvert  
 Span: 6.9'  
 Rdwy: 98'

Note: See Summary of "A" "I" "P" Items on sheet No 15  
 See Summary of "X" Items on sheet No. 14

B.M. No 4 - S.W. Bolt Hydrant Cap  
 37.0' Rt. Sta. 26+36  
 Elev. 757.19



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Begin Work on S.H. 306  
 Section Bryan (Pt.)  
 Sta. 49+70.5

End Work  
 and Typical Section C  
 Sta. 60+74

Begin Work  
 and Typical Section D  
 Sta. 61+39

Begin Typical  
 Section C  
 Sta. 49+82.5

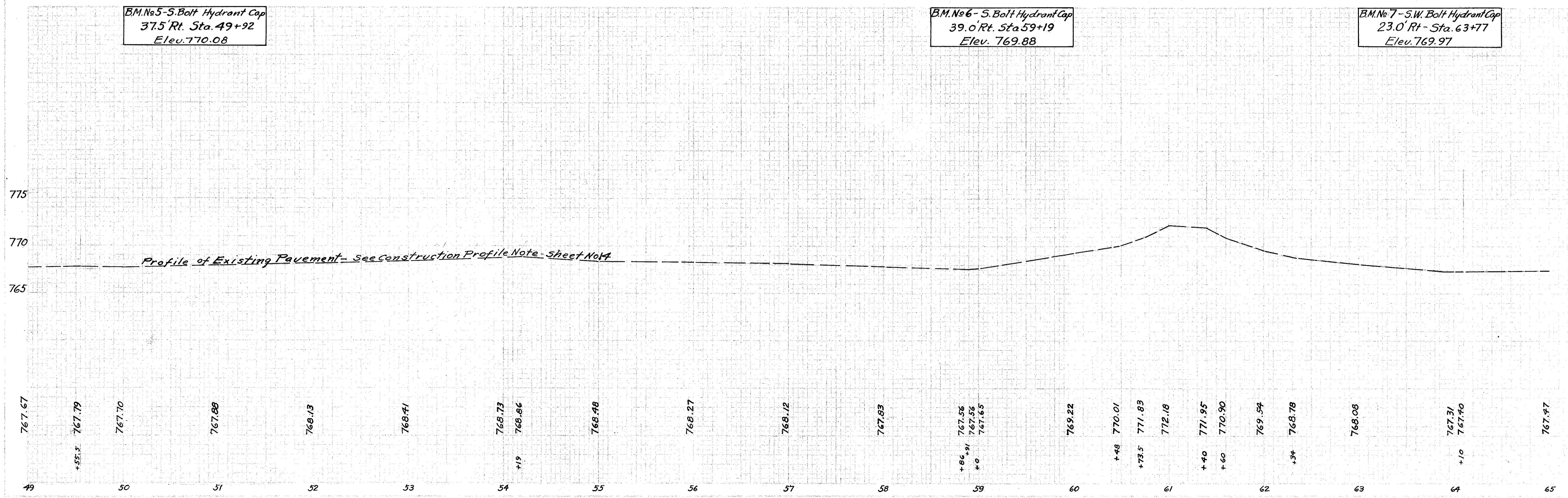
Note: See Summary of "A", "I" & "P" Items on sheet No. 15  
 See Summary of "X" Items on sheet No. 14

Railroad Crossing Warning  
 Signal - Flasher Type and  
 limited Watchman hours.

B.M. No. 5 - S. Bolt Hydrant Cap  
 37.5 Rt. Sta. 49+92  
 Elev. 770.08

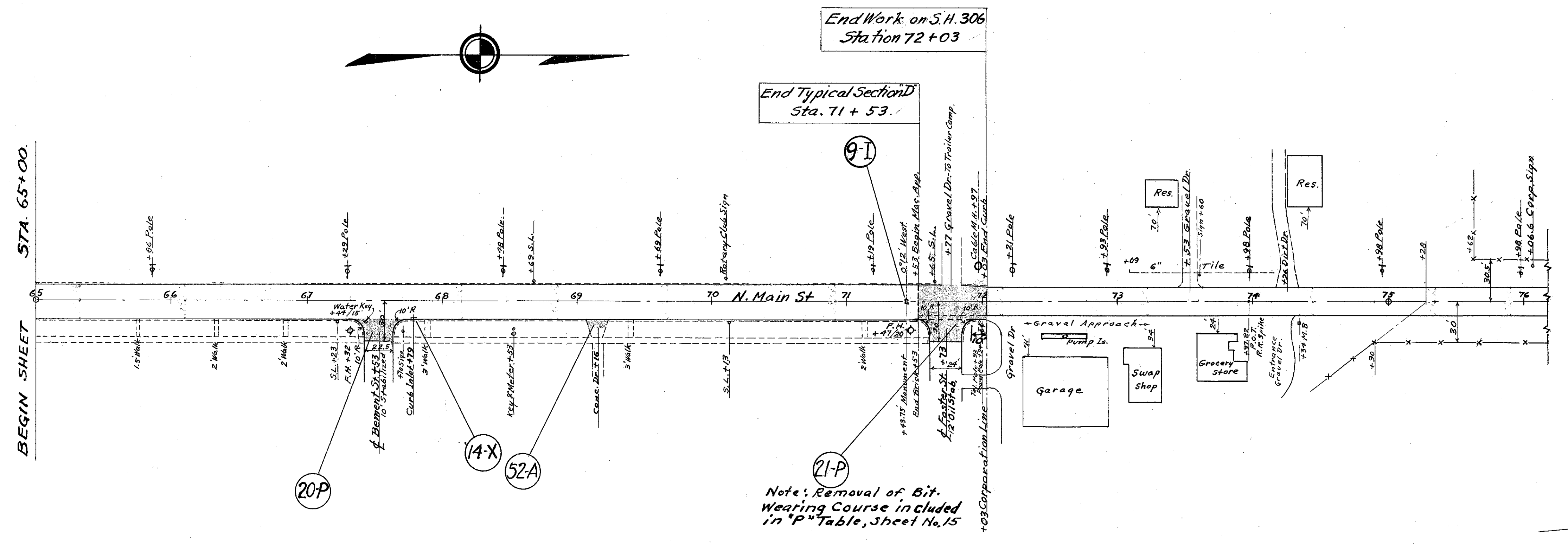
B.M. No. 6 - S. Bolt Hydrant Cap  
 39.0 Rt. Sta. 59+19  
 Elev. 769.88

B.M. No. 7 - S.W. Bolt Hydrant Cap  
 23.0 Rt. Sta. 63+77  
 Elev. 769.97



Profile of Existing Pavement - See Construction Profile Note - Sheet No. 4

WILLIAMS COUNTY.  
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End Work on S.H. 306  
 Station 72+03

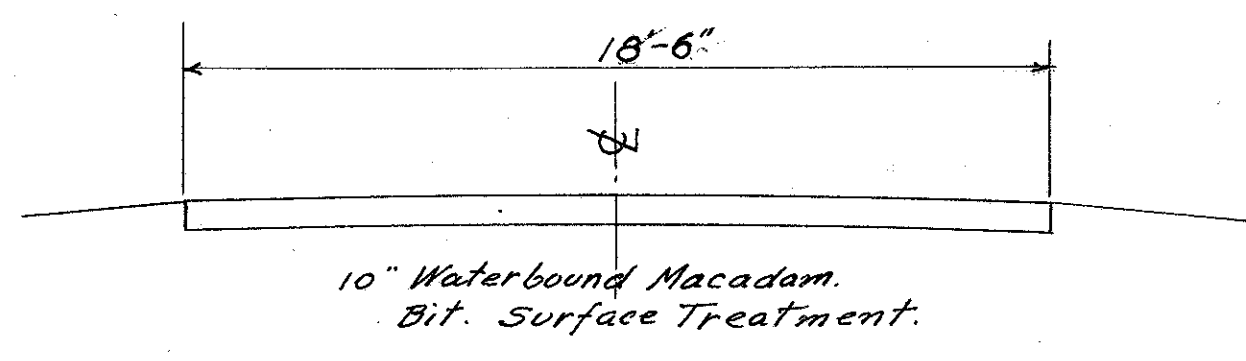
End Typical Section D  
 Sta. 71+53

20-P

14-X  
 52A

21-P

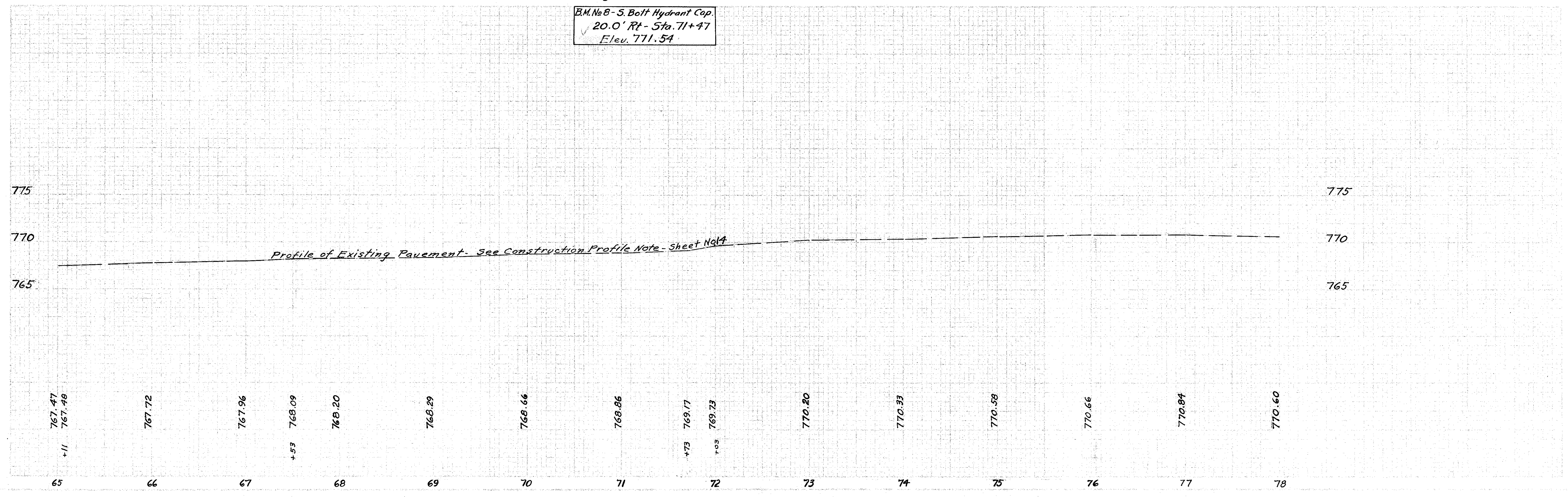
Note: Removal of Bit.  
 Wearing Course included  
 in "P" Table, Sheet No. 15



TYPICAL SECTION OF ADJOINING PAVEMENT.

Note: See summary of "A", "J" & "P" Items on sheet No. 15  
 See summary of "X" Items on sheet No. 14

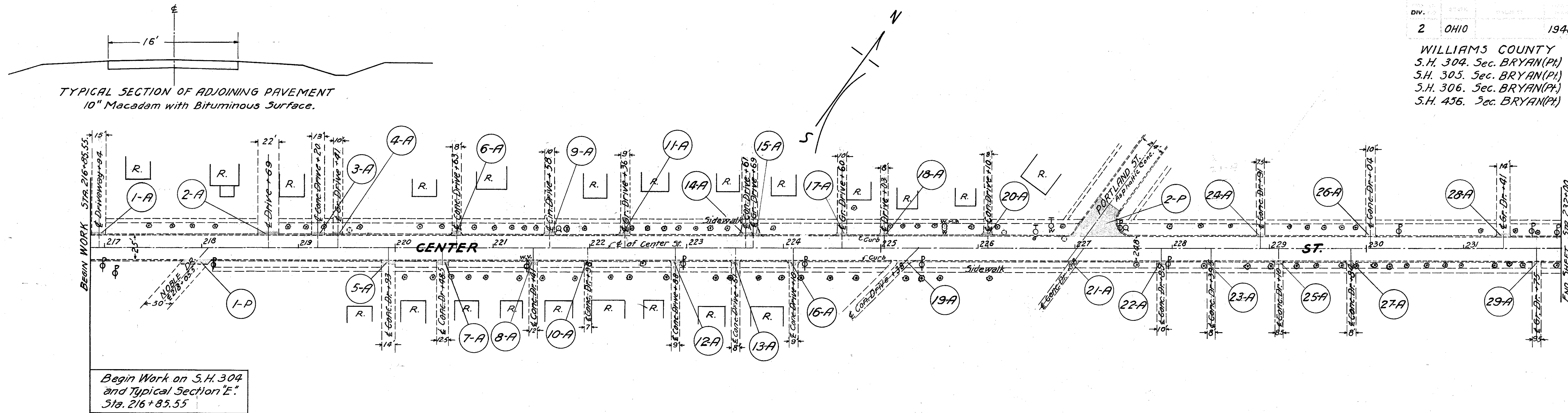
B.M. No 8 - 5 Bolt Hydrant Cap.  
 20.0' Rt - Sta. 71+47  
 Elev. 771.54



Profile of Existing Pavement - See Construction Profile Note - Sheet No. 14



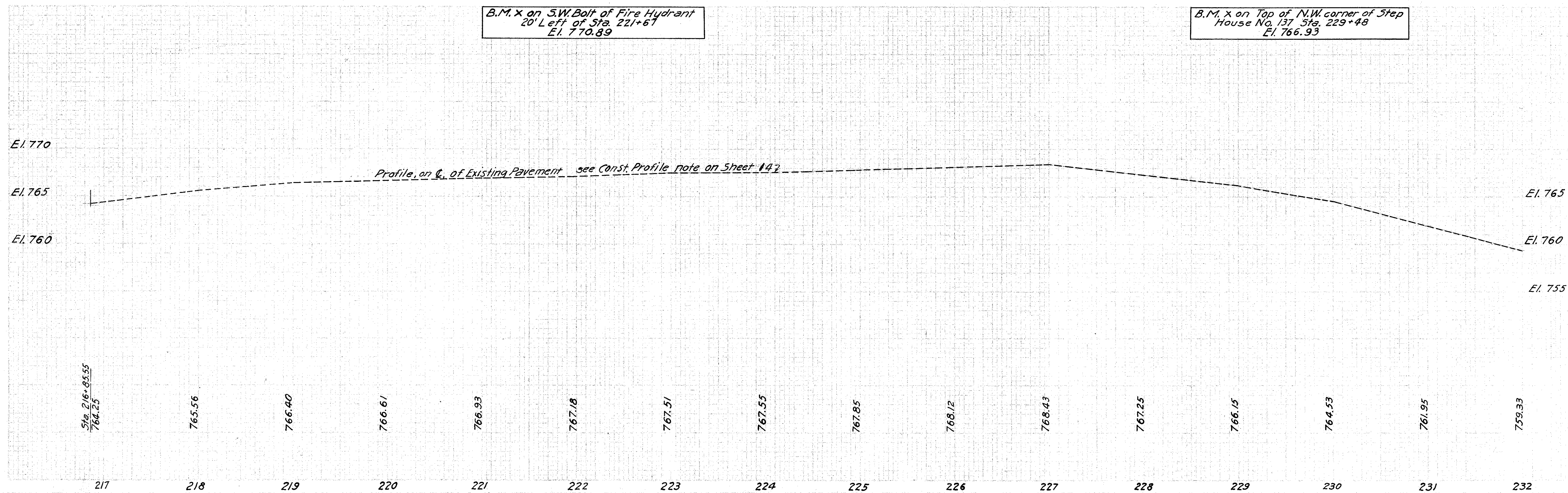
WILLIAMS COUNTY  
 S.H. 304. Sec. BRYAN (Pt.)  
 S.H. 305. Sec. BRYAN (Pt.)  
 S.H. 306. Sec. BRYAN (Pt.)  
 S.H. 456. Sec. BRYAN (Pt.)



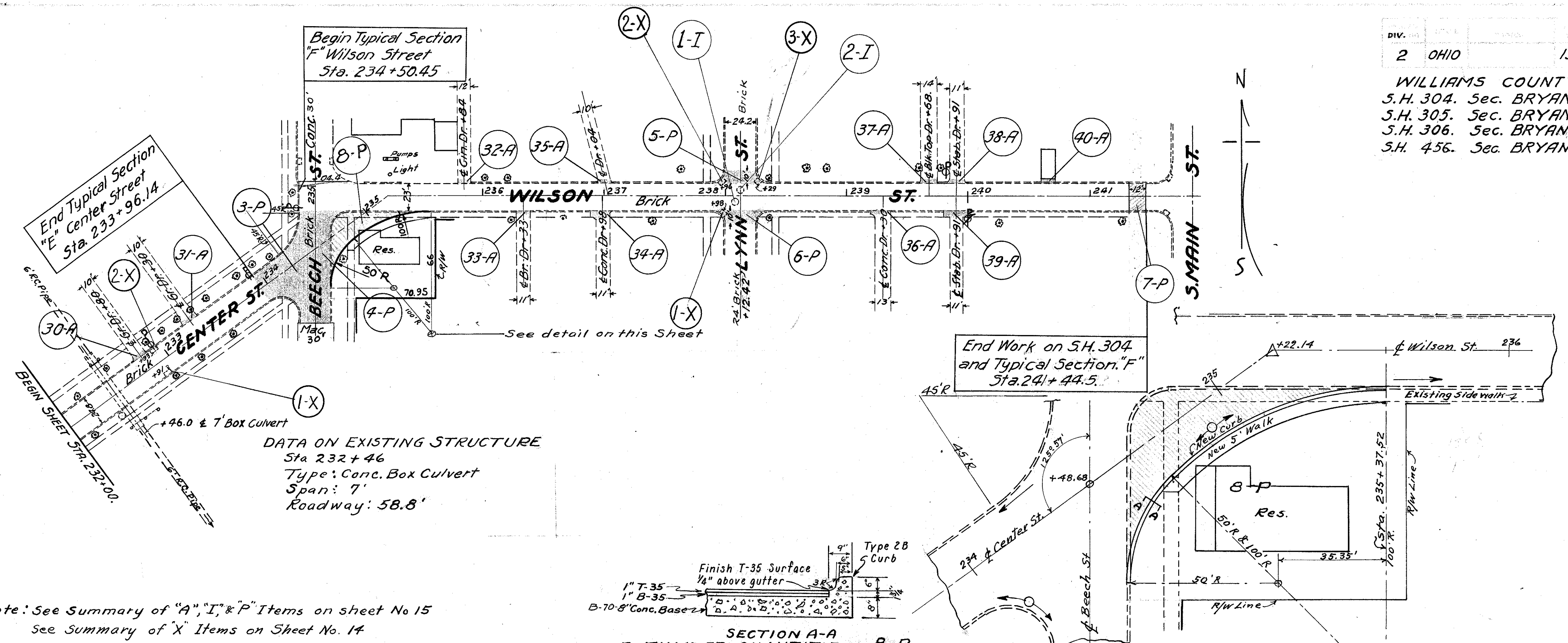
Note: See summary of "A" & "P" Items on Sheet No 15

B.M. X on S.W. Bolt of Fire Hydrant  
 20' Left of Sta. 221+67  
 El. 770.89

B.M. X on Top of N.W. corner of Step  
 House No. 137 Sta. 229+48  
 El. 766.93

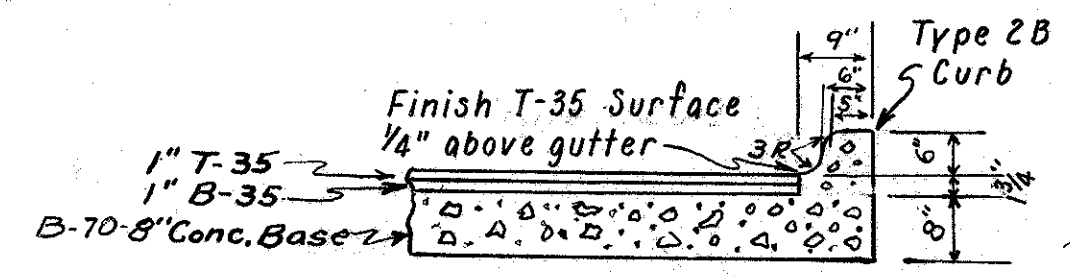


WILLIAMS COUNTY  
 S.H. 304. Sec. BRYAN (PH)  
 S.H. 305. Sec. BRYAN (PH)  
 S.H. 306. Sec. BRYAN (PH)  
 S.H. 456. Sec. BRYAN (PH)



DATA ON EXISTING STRUCTURE  
 Sta 232+46  
 Type: Conc. Box Culvert  
 Span: 7'  
 Roadway: 58.8'

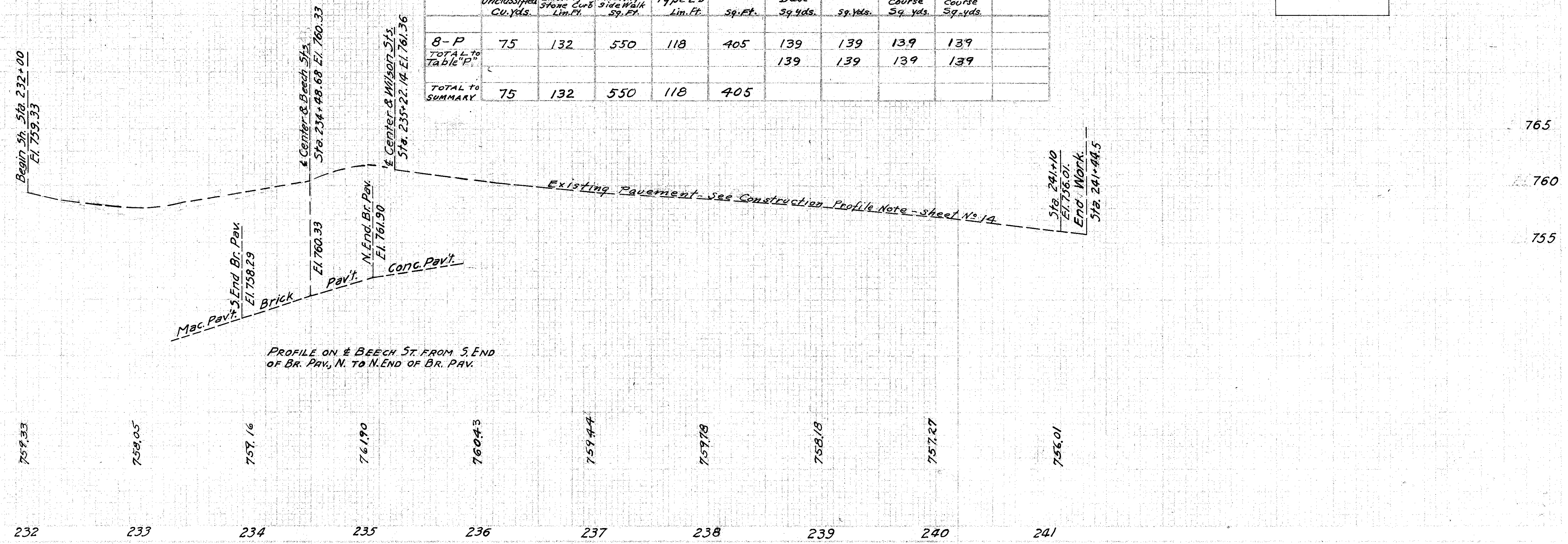
Note: See Summary of "A", "I", & "P" Items on sheet No 15  
 See Summary of "X" Items on Sheet No. 14



SECTION A-A  
 ESTIMATED QUANTITIES - B-P

Ref. No.	E-1 Roadway Excavation Unclassified Cu. Yds.	E-8 Removal & Disposal of Existing Stone Curb Side Walk Lin. Ft.	E-8 Removal & Disposal of Existing Curb Sq. Ft.	I-12 New Curb Type 2-B Lin. Ft.	I-13 Conc. Walk 5'-0" x 4" Sq. Ft.	B-70 8" Concrete Base Sq. Yds.	T-30 Prime Coat Sq. Yds.	B-35 Asphaltic Conc. Level Course Sq. Yds.	T-35 Asphaltic Conc. Surf. Course Sq. Yds.
8-P	75	132	550	118	405	139	139	139	139
TOTAL to Table "P"						139	139	139	139
TOTAL to SUMMARY	75	132	550	118	405				

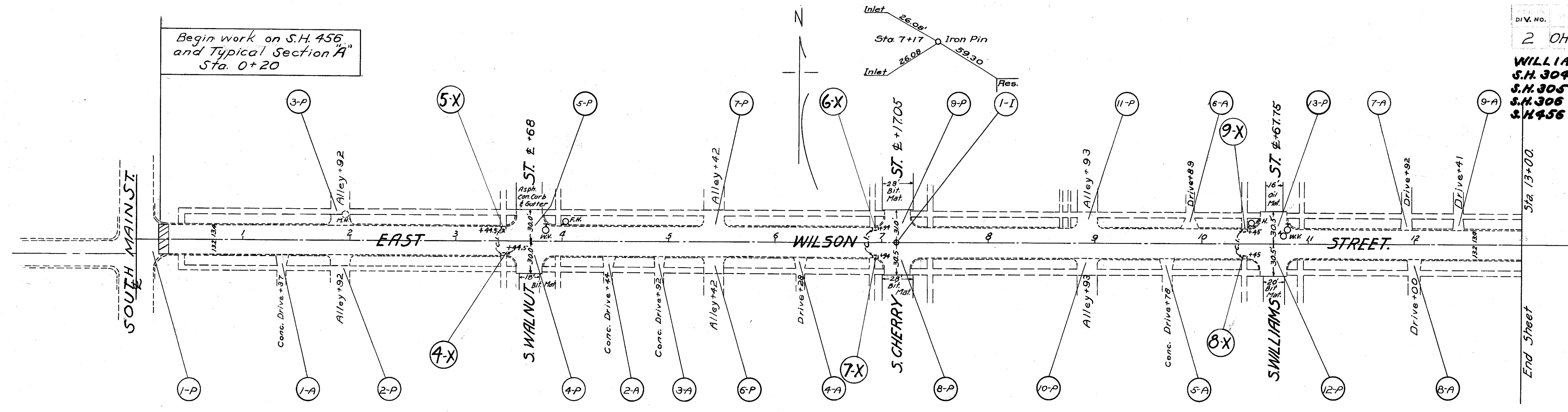
B.M. X on S.W. cor. N. Head Wall  
 30' Lt. Sta 232+41.  
 El. 760.72



PROFILE ON & BEECH ST FROM S. END  
 OF BR. PAV. N. TO N. END OF BR. PAV.

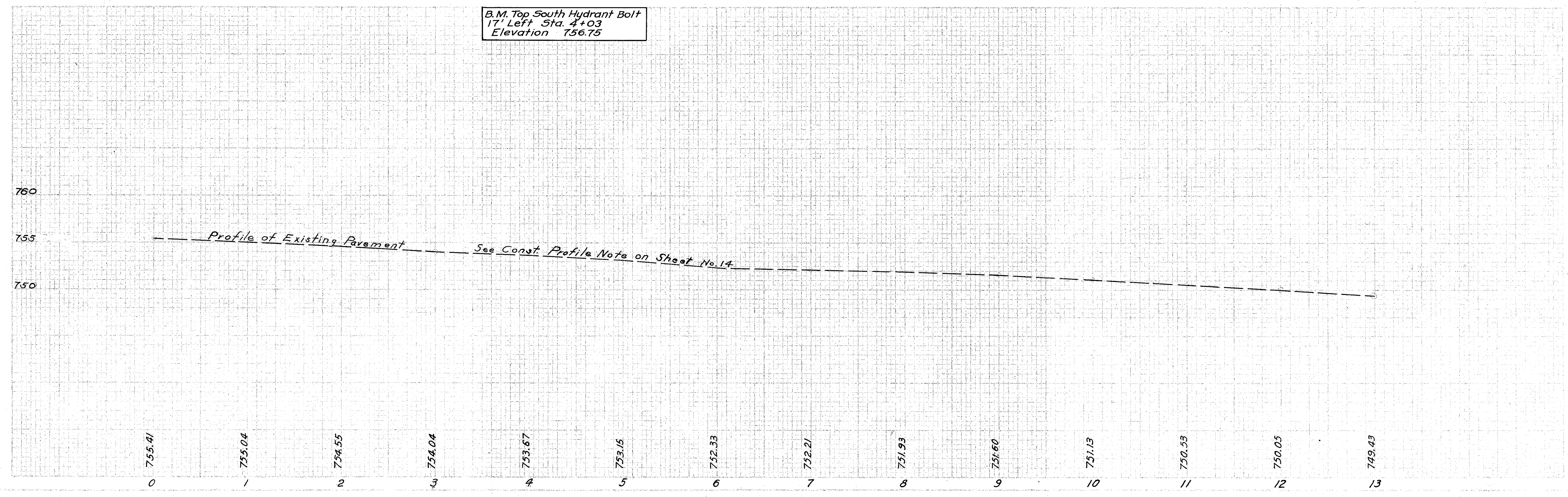
WILLIAMS COUNTY  
 S.H. 304 SEC. BRYAN (PT)  
 S.H. 305 SEC. BRYAN (PT)  
 S.H. 306 SEC. BRYAN (PT)  
 S.H. 456 SEC. BRYAN (PT)

Begin work on S.H. 456  
 and Typical Section A  
 Sta. 0+20

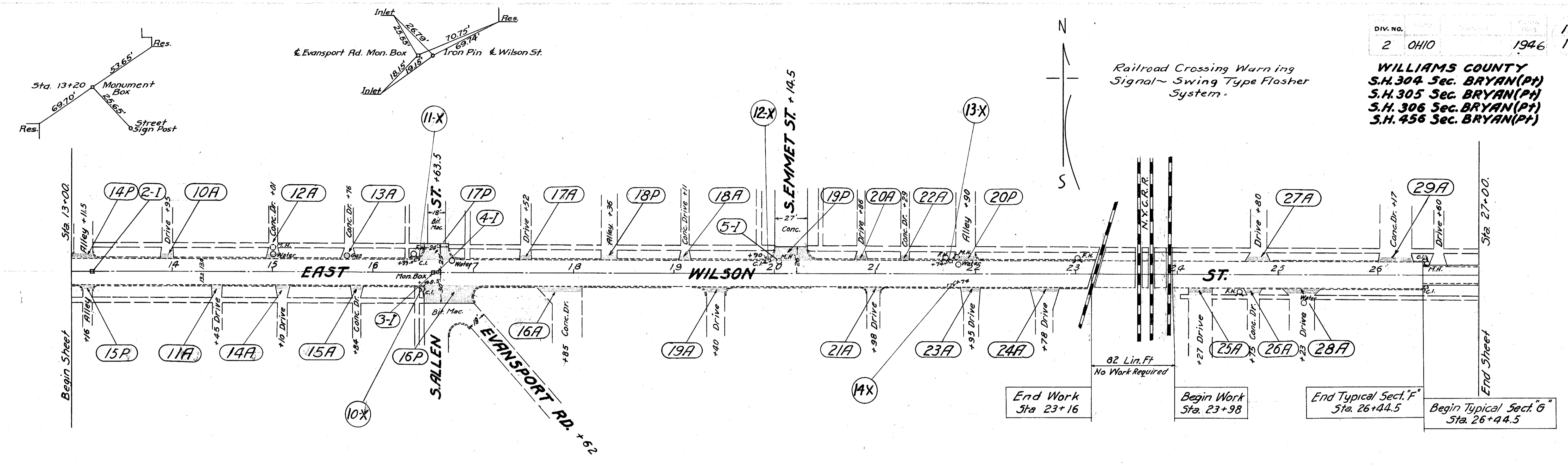


Note: See Summary of "A", "P" & "X" Items on Sheet No. 14  
 See Summary of "I" Items on sheet No. 15

B.M. Top South Hydrant Bolt  
 17' Left Sta. 4+03  
 Elevation 756.75



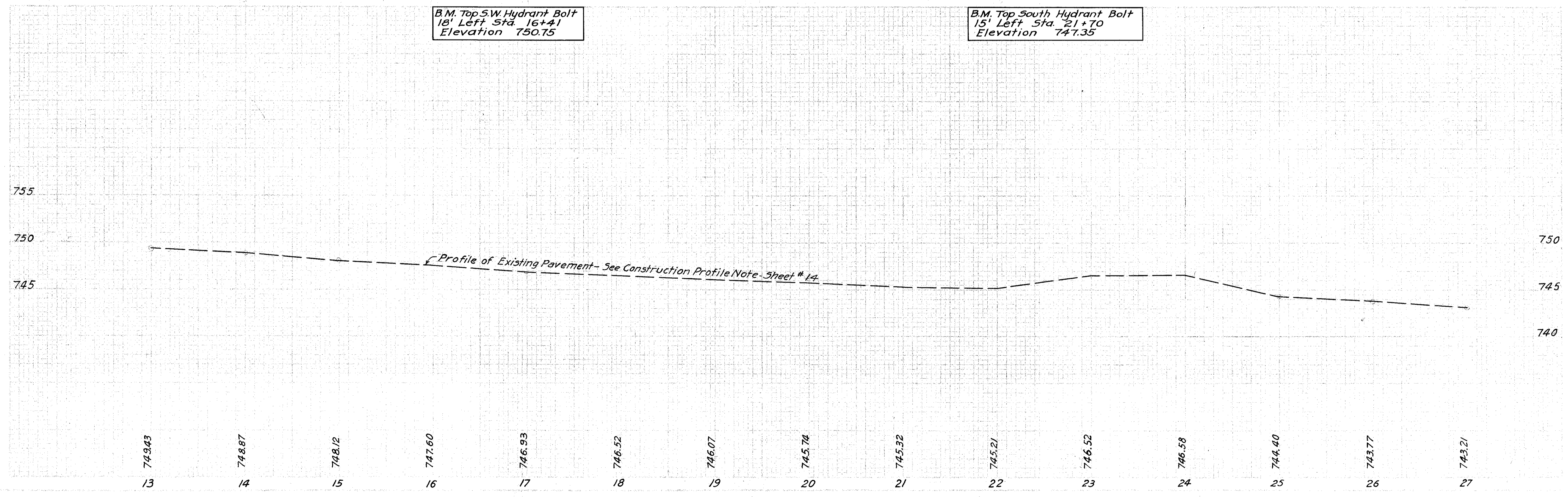
WILLIAMS COUNTY  
 S.H. 304 Sec. BRYAN (PT)  
 S.H. 305 Sec. BRYAN (PT)  
 S.H. 306 Sec. BRYAN (PT)  
 S.H. 456 Sec. BRYAN (PT)



Note: See Summary of "A", "P" & "X" Items on Sheet No. 14  
 See Summary of "I" Items on Sheet No. 15

B.M. Top S.W. Hydrant Bolt  
 18' Left Sta. 16+41  
 Elevation 750.75

B.M. Top South Hydrant Bolt  
 15' Left Sta. 21+70  
 Elevation 747.35





### GENERAL NOTES.

**PROFILE:**— The profile of the proposed surface course shall be approximately 2 inches above that of the existing pavement.

**TACK COAT INCLUDING SAND COVER:**— Bituminous tack Coat Sec.M-5.5 MS1 shall be applied by distributor or by brooms at the rate of 0.10 gal. per Sq.Yd. After the bituminous material has been applied, all material not required to give a uniform coating to the surface shall be swept into all cracks and open joints before the sand cover is placed. Sand cover shall be uniformly spread at a rate of from 2 to 5 pounds per square yard. The sand shall be spread at such time and shall be in such condition that it will adhere to the bituminous material. Payment for sand cover is included in the price bid per gallon for bituminous material.

**FILLING MAJOR DEPRESSIONS:**— Major depressions in existing pavement shall be filled and compacted with bituminous concrete leveling material in advance of placing the regular leveling course. These depressions shall be filled in layers not to exceed 3" in depth when compacted.

**SEALING EXISTING EDGES:**— Vertical faces of existing work, such as castings, curbs, etc. against which the new bituminous concrete is to be placed shall be painted or sealed with bituminous material in accordance with Sec. T-35.18. The cost of such operations and material shall be included in the price bid for bituminous concrete.

**TREATMENT OF FEATHERED AREA:**— Where directed the new surface course shall be feathered. The area upon which less than one (1) inch of surface is to be placed shall be considered as the area to be feathered. A paint coat of the same bituminous material used in the mix shall be applied to this area before placing the bituminous concrete surface course. The cost of such operation and material shall be included in the unit price bid for bituminous concrete.

**UTILITIES:**— All work required to relocate or adjust etc. all gas, oil telegraph, telephone, electric, water or other services to conform to the new grade and alignment shall be completed by the Utilities in question or the City of Bryan.

**GUTTER SEAL:**— After placing and compacting the bituminous concrete surface course, the gutter surface shall be sealed with the same bituminous material contained in the mixture. Only enough material shall be applied to coat the surface for a distance of 12 inches from the curb or 24 inches wide for a "V" gutter. The material shall be applied by an approved method at a temperature of from 300 deg. F. to 350 deg. F. The cost of such operation and material shall be included in the price bid for bituminous concrete.

**TRAFFIC:**— Traffic shall be maintained at all times. The length of one way traffic zones shall be kept to a minimum, consistent with the requirements of Sec T-35.23. In addition to the requirements of Sec. 6-8.07, Barricades, Danger and Warning Signs, the Contractor shall display one "PLEASE-MEN WORKING ON ROAD" sign furnished by the State at each end of each one way zone and in such a position as to be visible to traffic approaching the one way zone. The Contractor shall be responsible for the preservation of these signs, shall advance the signs as work progresses and shall return the signs to the State at the completion of the work. The item of "Maintaining Traffic" shall include furnishing lights, signs (other than those mentioned above), barricades and watchmen, plus the displaying and advancing of the "PLEASE-MEN WORKING ON ROAD" signs to secure the flow of traffic twenty-four (24) hours daily.

**ADJUSTMENT OF MANHOLE CASTINGS:**— Portions of the existing pavement removed to adjust manhole castings to the grade of the surface shall be replaced full depth with Class "C" concrete. Necessary adjustment of the structure walls shall be made with Class "C" concrete. The cost of the concrete shall be included in the unit price bid for Item I-8, Reset Manhole Castings. This item shall be performed after comple-

### GENERAL NOTES (CONTINUED)

tion of the leveling course and prior to placing the surface course. Compaction of the material around castings, inaccessible to rollers shall be accordance with the requirements of Sec. T-50.21

**CLEANING EXISTING PAVEMENT:**— As a part of the work required by Section T-35.16, the Contractor shall remove all accumulations of material built up at all joints or cracks before spreading any bituminous materials.

**RAILROAD CROSSINGS:**— The new surface course shall be feathered to meet rail grades if necessary.

**MONUMENTS:**— All existing monuments will be referenced by the State Forces before raising to the proposed surface grade.

**HAND FINISHING:**— B-70 Concrete Pavement may be hand finished as per Specifications, Item B-70.191.

**STORAGE OF STONE CURB:**— Existing stone curb removed and stored shall be placed within Rights-of-Way limits for City Forces.

**COMPACTION OF SUBGRADE:**— Loosening and watering of subgrade in cuts according to Section E-1.11 will not be required if density requirements can be met by additional rolling. However if at any time the subgrade contains an excess of moisture as indicated by distortion under the roller, the subgrade shall be aerated by discing or other suitable means until the moisture content has been reduced sufficiently to permit recompaction to the density required by the specifications.

**CROWN AT RAILROAD CROSSING:**— The crown shall be worked out of the pavement beginning 50' each side of the railroad crossing by raising the edge of the pavement to meet the rail elevation.

WILLIAMS COUNTY  
S.H. 304. Sec. BRYAN(Pt)  
S.H. 305. Sec. BRYAN(Pt)  
S.H. 306. Sec. BRYAN(Pt)  
S.H. 456. Sec. BRYAN(Pt)

### RESETTING CURB INLETS "X"

Sheet No.	Ref. No.	Station	Side	I-B Curb Inlets Adjusted to Grade Each	Street	Sheet No.	Ref. No.	Station	Side	I-B Curb Inlets Adjusted to Grade Each	Street
5	1-X	4 + 89	Lt.	1	Main St.	10	1-X	237 + 98	Rt.	1	W. Wilson St.
5	2-X	4 + 89	Rt.	1	"	10	2-X	237 + 96	Lt.	1	"
5	3-X	8 + 51	Rt.	1	"	10	3-X	238 + 29	Lt.	1	"
5	4-X	8 + 73	Lt.	1	"	11	4-X	3 + 44.5	Lt.	1	E. Wilson St.
6	5-X	17 + 14	Rt.	1	"	11	5-X	3 + 44.5	Rt.	1	"
6	6-X	17 + 60	Lt.	1	"	11	6-X	6 + 94	Lt.	1	"
6	7-X	22 + 38	Lt.	1	"	11	7-X	6 + 94	Rt.	1	"
6	8-X	22 + 38	Rt.	1	"	11	8-X	10 + 45	Rt.	1	"
7	9-X	58 + 70	Rt.	1	"	11	9-X	10 + 45	Lt.	1	"
7	10-X	58 + 70	Lt.	1	"	12	10-X	16 + 48.5	Rt.	1	"
7	11-X	59 + 12	Lt.	1	"	12	11-X	16 + 39	Lt.	1	"
7	12-X	59 + 02	Rt.	1	"	12	12-X	19 + 90	Lt.	1	"
7	13-X	64 + 10	Lt.	1	"	12	13-X	21 + 74	Lt.	1	"
8	14-X	67 + 79	Rt.	1	"	12	14-X	21 + 74	Rt.	1	"
10	1-X	232 + 91	Rt.	1	Center St.						
10	2-X	232 + 93	Lt.	1	"						
Total										30	

### EXTRA PAVEMENT "P"

EAST WILSON ST. S.H. 456

SHEET No.	Ref. No.	Station	Side	W	T-30 sq. yds.	B-35 sq. yds.	T-35 sq. yds.	I-17 cu. yds.	E-8 sq. yds.
Various Stations Rt. & Lt.									240
11	1-P	0 + 20	O + 30	Ctr.					30
11	2-P	1 + 92		R.	11	29	20	29	
11	3-P	1 + 92		Lt.	11	29	20	29	
11	4-P	3 + 68		Rt.	17	54	32	54	
11	5-P	3 + 68		Lt.	17	54	32	54	
11	6-P	5 + 42		Rt.	17	38	24	38	
11	7-P	5 + 42		Lt.	17	38	24	38	
11	8-P	6 + 17		Rt.	17	54	32	54	
11	9-P	6 + 17		Lt.	17	54	32	54	
11	10-P	8 + 93		Rt.	17	38	24	38	
11	11-P	8 + 93		Lt.	17	38	24	38	
11	12-P	10 + 67.75		Rt.	17	54	32	54	
11	13-P	10 + 67.75		Lt.	17	54	32	54	
12	14-P	13 + 11.5		Lt.	17	38	24	38	
12	15-P	13 + 16		Rt.	17	20	15	20	
12	16-P	16 + 62		Rt.	16	99	67	99	
12	17-P	16 + 63.5		Lt.	17	53	23	53	
12	18-P	18 + 36		Lt.					1.0
12	19-P	20 + 14.5		Lt.	17	49	26	49	
12	20-P	21 + 90		Lt.	17	38	24	38	
13	21-P	28 + 09		Lt.	No work required				
Totals to Sheet No. 15					831	507	831	1.0	270

### SIDE APPROACHES "A"

EAST WILSON ST. S.H. 456

SHEET No.	Ref. No.	Station	Side	W	T-30 Sq. Yds.	B-35 Sq. Yds.	T-35 Sq. Yds.	I-17 Cu. Yds.
11	1-A	1 + 37	Rt.		4		4	
11	2-A	4 + 44	Rt.		4		4	
11	3-A	4 + 92	Rt.		4		4	
11	4-A	6 + 28	Rt.					0.5
11	5-A	9 + 78	Rt.		6		6	
11	6-A	9 + 89	Lt.					0.5
11	7-A	11 + 92	Lt.					0.5
11	8-A	12 + 00	Rt.					0.5
11	9-A	12 + 41	Lt.					0.5
12	10-A	13 + 95	Lt.					0.5
12	11-A	14 + 45	Rt.					0.5
12	12-A	15 + 01	Lt.		5		5	
12	13-A	15 + 76	Lt.		6		6	
12	14-A	15 + 10	Rt.					0.5
12	15-A	15 + 84	Rt.		6		6	
12	16-A	17 + 85	Rt.		15		15	
12	17-A	17 + 52	Lt.					0.5
12	18-A	19 + 11	Lt.		4		4	
12	19-A	19 + 40	Rt.		8		8	
12	20-A	20 + 86	Lt.					0.5
12	21-A	20 + 98	Rt.					0.5
12	22-A	21 + 29	Lt.		4		4	
12	23-A	21 + 95	Rt.					0.5
12	24-A	22 + 78	Rt.					0.5
12	25-A	24 + 27	Rt.					1.0
12	26-A	24 + 75	Rt.		7		7	
12	27-A	24 + 80	Lt.					0.5
12	28-A	25 + 23	Rt.					0.5
12	29-A	26 + 17	Lt.		15		15	
Totals to Sheet No. 15					88		88	8.5

# EXTRA PAVEMENT "P"

N. & S. MAIN STREET - S.H. 305 & 306.

SHEET No.	Ref. No.	Station		Side	W	T-30	B-35	T-35	I-17	E-8 Bit Wearing Course
		From	To			sq. yds.	sq. yds.	sq. yds.		
5	1-P	0 + 00	0 + 30	Lt.	18.70	38	18	38		25
5	2-P	0 + 00	0 + 30	Rt.	26.70	63	35	63		25
5	3-P	8 + 45		Lt.	23.40	66	48	66		
5	4-P	8 + 21		Rt.	28.20	86	66	86		
5	5-P	12 + 97		Lt.	24.90	53	35	53		
5	6-P	12 + 83		Rt.	28.20	73	54	73		
6	7-P	17 + 44		Rt.	30.40	92	69	92		
6	8-P	22 + 08		Rt.	27.10	85	63	85		
6	9-P	22 + 08		Lt.	38.40	112	84	112		
6	10-P	26 + 09	26 + 21	Rt. & Lt.	12.00					43
7	11-P	49 + 70.5	49 + 82.5	R. & Lt.	12.00	48	48	48		48
7	12-P	54 + 19		Rt.	31.40	95	72	95		
7	13-P	54 + 19		Lt.	30.90	94	72	94		
7	14-P	58 + 91		Lt.	26.80	121	90	121		
7	15-P	58 + 86		Rt.	27.60	83	63	83		
7	16-P	61 + 39	61 + 57	Rt. & Lt.	36.20	72	54	72		
7	17-P	61 + 57	62 + 28	Lt.	6.00	24	18	24		
7	18-P	61 + 57	62 + 28	Rt.	6.25	28	21	28		
7	19-P	64 + 10		Rt.	17.10	48	36	48		
8	20-P	67 + 53		Rt.	17.30	46	35	46		
8	21-P	71 + 53	72 + 03	Rt. & Lt.	42.40	187	137	187	3.0	26
Various Stations										50
Sub Totals						1514	1118	1514	3.0	217.0
CENTER STREET - S.H. - 304										
9	1-P	218 + 05.5		Rt.	10	10	10			
9	2-P	Portland St.		Lt.	133	120	133			
10	3-P	233 + 96.14	234 + 48.68	Rt. & Lt.	257	228	257			
Sub Totals						400	358	400		
WEST WILSON STREET - S.H. 304										
10	4-P	Beech St.		Rt. or South	132	104	132			
10	5-P	238 + 12.42		Lt.	32	24	32			
10	6-P	238 + 12.42		Rt.	32	24	32			
10	7-P	241 + 32.5	241 + 44.5	Rt. & Lt.						31
10	8-P	231 + 42.0	235 + 37.52	Rt. & Lt.	139	139	139			
Sub Totals						335	291	335		31
E. Wilson St. Sub-totals from Sheet No. 14						831	507	831	1.0	270
Grand Totals						3080	2274	3080	4.0	518

# SIDE APPROACHES "A"

(CONTINUED)

SHEET	Ref. No.	Station		Side	W	T-30	T-35	I-17		
		From	To							
9	21-A	227 + 08		Rt.		3.0	3.0			
9	22-A	227 + 89		Rt.		4.0	4.0			
9	23-A	228 + 39.5		Rt.		3.0	3.0			
9	24-A	228 + 91		Lt.		3.0	3.0			
9	25-A	229 + 10		Rt.		4.0	4.0			
9	26-A	230 + 04		Lt.		4.0	4.0			
9	27-A	229 + 84		Rt.		3.0	3.0			
9	28-A	231 + 41		Lt.				0.5		
9	29-A	231 + 75		Rt.				0.5		
10	30-A	232 + 80		Lt.				0.5		
10	31-A	233 + 30		Lt.				0.5		
Sub Totals						69.0	69.0	6.0		
WEST WILSON ST. S.H. 304										
10	32-A	225 + 84		Lt.				0.5		
10	33-A	236 + 33		Rt.		4.0	4.0			
10	34-A	236 + 98		Rt.		4.0	4.0			
10	35-A	237 + 04		Lt.		4.0	4.0			
10	36-A	239 + 30		Rt.		5.0	5.0			
10	37-A	239 + 68		Lt.		5.0	5.0			
10	38-A	239 + 91		Lt.				0.5		
10	39-A	239 + 91		Rt.				0.5		
10	40-A	240 + 65		Lt.				0.5		
Sub Totals						22.0	22.0	2.0		
E. Wilson St. Sub-total from Sheet No. 14						88.0	88.0	8.5		
Grand Totals						302.0	302.0	40.0		

# SIDE APPROACHES "A"

N. & S. MAIN STREET - S.H. 305 & 306.

SHEET No.	Ref. No.	Station		Side	W	T-30	T-35	I-17		
		From	To			sq. yds.	sq. yds.		cu. yds.	
5	1-A	0 + 20		Lt.		7.0	7.0			
5	2-A	1 + 41		Rt.				0.5		
5	3-A	1 + 95		Lt.		6.0	6.0			
5	4-A	2 + 76		Lt.		5.0	5.0			
5	5-A	3 + 26		Lt.		5.0	5.0			
5	6-A	3 + 35		Rt.				2.0		
5	7-A	3 + 75		Lt.		4.0	4.0			
5	8-A	4 + 37		Lt.				0.5		
5	9-A	4 + 51		Rt.				0.5		
5	10-A	4 + 82		Lt.				0.5		
5	11-A	5 + 05		Rt.				0.5		
5	12-A	5 + 46		Rt.				0.5		
5	13-A	6 + 99		Rt.				0.5		
5	14-A	7 + 20		Lt.				0.5		
5	15-A	9 + 66		Rt.		5.0	5.0			
5	16-A	9 + 68		Lt.				0.5		
5	17-A	10 + 48		Rt.				0.5		
5	18-A	10 + 80		Lt.				0.5		
5	19-A	10 + 97		Lt.				0.5		
5	20-A	11 + 99		Lt.				0.5		
5	21-A	13 + 36		Rt.		4.0	4.0			
5	22-A	15 + 37		Rt.		25.0	25.0			
6	23-A	16 + 11		Lt.				0.5		
6	24-A	17 + 41		Lt.				0.5		
6	25-A	18 + 57		Lt.		7.0	7.0			
6	26-A	18 + 91		Lt.		5.0	5.0			
6	27-A	19 + 04		Rt.				0.5		
6	28-A	20 + 09		Rt.				0.5		
6	29-A	20 + 72		Lt.				0.5		
6	30-A	21 + 03		Lt.				0.5		
6	31-A	21 + 28		Rt.				0.5		
6	32-A	24 + 51		Lt.				0.5		
6	33-A	25 + 82		Lt.		4.0	4.0			
7	34-A	51 + 21		Lt.		5.0	5.0			
7	35-A	51 + 83		Rt.		5.0	5.0			
7	36-A	51 + 94		Lt.				0.5		
7	37-A	52 + 76		Rt.		5.0	5.0			
7	38-A	53 + 15		Rt.				0.5		
7	39-A	53 + 16		Lt.		4.0	4.0			
7	40-A	55 + 35		Lt.		4.0	4.0			
7	41-A	55 + 50		Rt.				0.5		
7	42-A	55 + 95		Lt.				0.5		
7	43-A	56 + 36		Rt.		3.0	3.0			
7	44-A	56 + 59		Lt.				0.5		
7	45-A	57 + 32		Lt.		5.0	5.0			
7	46-A	57 + 75		Rt.				0.5		
7	47-A	59 + 82		Lt.		10.0	10.0			
7	48-A	60 + 41		Rt.				3.0		
7	49-A	62 + 34		Lt.				4.0		
7	50-A	62 + 40		Rt.				0.5		
7	51-A	62 + 70		Lt.				0.5		
8	52-A	69 + 16		Rt.		5.0	5.0			
Sub Totals						123.0	123.0	23.5		
CENTER STREET - S.H. - 304										
9	1-A	216 + 94		Lt.				0.5		
9	2-A	218 + 69		Lt.				0.5		
9	3-A	219 + 20		Lt.		5.0	5.0			
9	4-A	219 + 41		Lt.				0.5		
9	5-A	219 + 93		Rt.		5.0	5.0			
9	6-A	220 + 63		Lt.		3.0	3.0			
9	7-A	220 + 48.5		Rt.		5.0	5.0			
9	8-A	221 + 41		Rt.		4.0	4.0			
9	9-A	221 + 58		Lt.				0.5		
9	10-A	221 + 98		Rt.		3.0	3.0			
9	11-A	222 + 36		Lt.				0.5		
9	12-A	222 + 88		Rt.		4.0	4.0			
9	13-A	223 + 51		Rt.		3.0	3.0			
9	14-A	223 + 61		Lt.		3.0	3.0			
9	15-A	223 + 69		Lt.				0.5		
9	16-A	224 + 10		Rt.		4.0	4.0			
9	17-A	224 + 60		Lt.				0.5		
9	18-A	225 + 03		Lt.				0.5		
9	19-A	225 + 38		Rt.		3.0	3.0			
9	20-A	226 + 10		Lt.		3.0	3.0			

Continued - Table at Left.

FED. RD. DIV. NO.	STATE	PROJECT	FISCAL YEAR
2	OHIO		1946

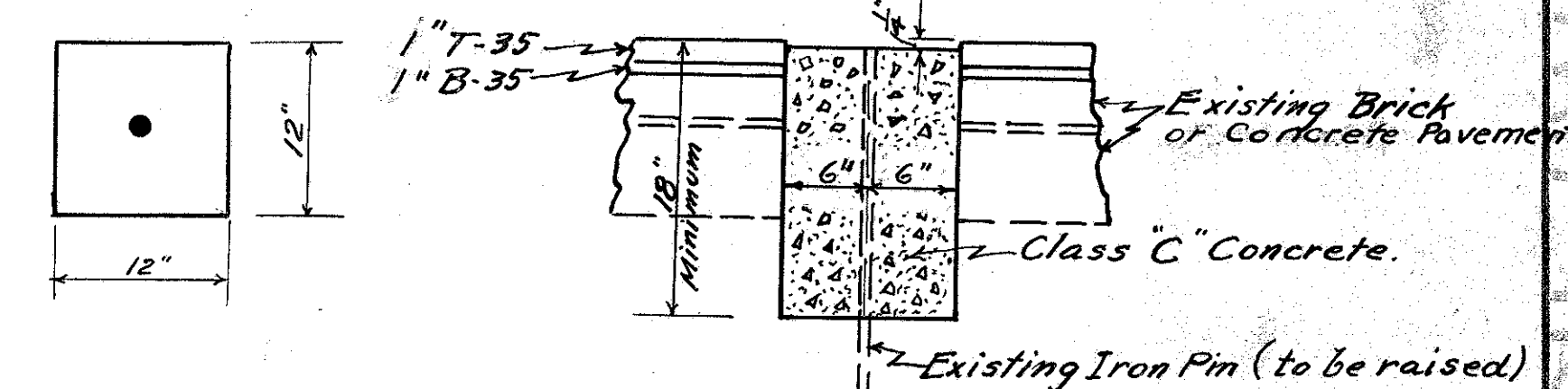
15  
15

WILLIAMS COUNTY  
S.H. - 304, SEC. BRYAN (P&)  
S.H. - 305, SEC. BRYAN (P&)  
S.H. - 306, SEC. BRYAN (P&)  
S.H. - 456, SEC. BRYAN (P&)

# RESETTING MANHOLES & MONUMENTS "I"

SHEET No.	Ref. No.	STATION	Side	I-8 Adjust Manhole to grade each	I-8 Adjust Monument Box & Iron Pin each	Street
5	1-I	8 + 28	Rt.	1		Main
6	3-I	21 + 98	Rt.	1		Main
6	4-I	21 + 99	Lt.	1		"
6	5-I	21 + 99	Lt.	1		"
7	6-I	54 + 19	Rt.	1		"
7	7-I	58 + 88.5	Lt.	1		"
7	8-I	58 + 93	Lt.	1		"
8	9-I	71 + 43.75	Lt.		1	"
10	1-I	238 + 08	Rt.	1		W. Wilson
10	2-I	238 + 13	Lt.	1		W. "
11	1-I	74 + 17.05	ctr.		1	E. "
12	2-I	13 + 20	ctr.		1	E. "
12	3-I	16 + 62	ctr.		1	E. "
12	4-I	16 + 63.5	ctr.		1	E. "
12	5-I	20 + 05	Lt.	1		E. "
Total to Summary				10	5	

Detail Showing Iron Pin Monument adjusted to Grade.



PLAN CROSS SECTION  
Note: Cost of necessary Pavement Removal & Raising Iron Pin and of Class "C" Concrete is included in the Unit Price for adjusting Monuments to Grade, Item I-8.

WILLIAMS COUNTY FILES 501-502