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LEGEND - PROPOSED ITEMS

- ① FLEXIBLE PAVEMENT OPTION USING:
ITEM 442 - 1 1/2" ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (447), AS PER PLAN (SHT. 43)
ITEM 407 - NON-TRACKING TACK COAT (RATE OF 0.06 GAL/SY USED FOR ESTIMATING)
ITEM 442 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE A (446), AS PER PLAN (SHT. 43)
ITEM 407 - NON-TRACKING TACK COAT (RATE OF 0.09 GAL/SY USED FOR ESTIMATING)
ITEM 302 - 4" ASPHALT CONCRETE BASE, PG64-44 (1st LIFT)
ITEM 407 - NON-TRACKING TACK COAT (RATE OF 0.09 GAL/SY USED FOR ESTIMATING)
ITEM 302 - 4" ASPHALT CONCRETE BASE, PG64-44 (2nd LIFT)

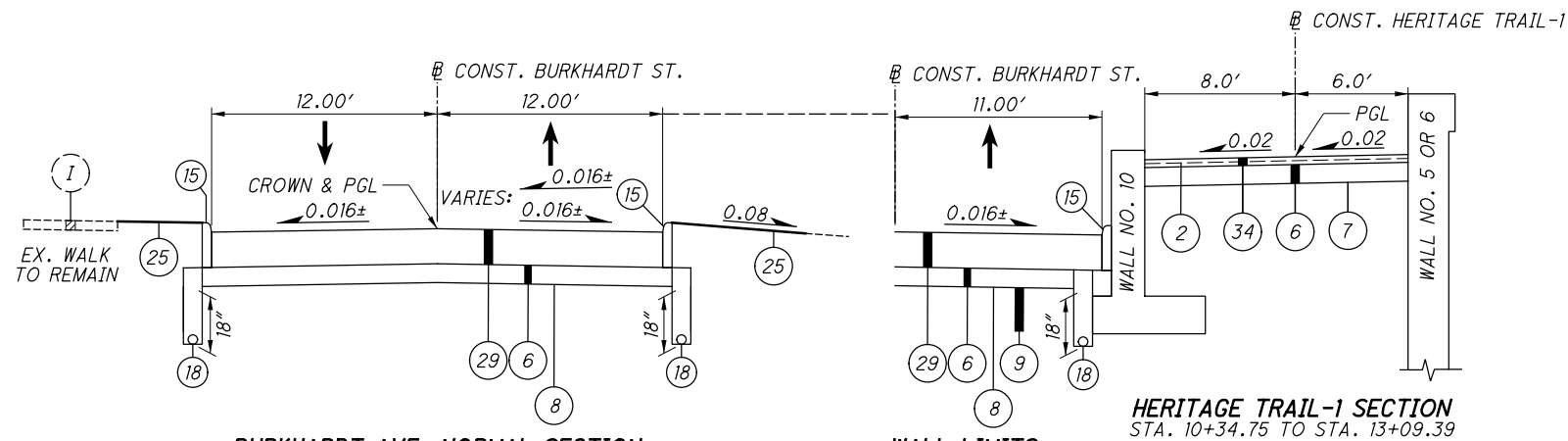
-OR-
RIGID PAVEMENT OPTION USING:
ITEM 452 - 13" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC1 WITH QC/QA
- ② ITEM 407 - NON-TRACKING TACK COAT (RATE OF 0.06 GAL/SY USED FOR ESTIMATING)
- ③ ITEM 452 - 6" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC 1P
- ⑥ ITEM 304 - 6" AGGREGATE BASE, AS PER PLAN (SHT. 43)
- ⑦ ITEM 204 - SUBGRADE COMPACTION
- ⑧ ITEM 204 - PROOF ROLLING
- ⑨ ITEM 206 - CEMENT STABILIZED SUBGRADE, 14 INCHES DEEP
ITEM 206 - CEMENT
ITEM 206 - CURING COAT
ITEM 206 - MIXTURE DESIGN FOR CHEMICALLY STABILIZED SOIL
- ⑩ ITEM 302 - 9" ASPHALT CONCRETE BASE (2 LIFTS)
- ⑪ ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2"
- ⑫ ITEM 442 - 1 1/2" ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), AS PER PLAN(SHT. 43)
- ⑬ ITEM 442 - 1 1/2" ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE B
- ⑭ ITEM 452 - 11" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC 1P
- ⑮ ITEM 609 - CURB, TYPE 6, AS PER PLAN (SHT. 42)
- ⑯ ITEM 608 - 6" CONCRETE WALK

- ⑰ ITEM 605 - 6" SHALLOW PIPE UNDERDRAIN W/ GEOTEXTILE FABRIC
- ⑱ ITEM 605 - 6" BASE PIPE UNDERDRAIN W/ GEOTEXTILE FABRIC
- ⑲ ITEM 606 - GUARDRAIL, TYPE MGS
- ⑳ ITEM 622 - CONCRETE BARRIER, SINGLE SLOPE, TYPE B1
- ㉑ ITEM 622 - CONCRETE BARRIER, SINGLE SLOPE, TYPE D
- ㉒ ITEM 609 - CURB, TYPE 4-C, AS PER PLAN (SHT. 42)
- ㉓ ITEM 526 - REINFORCED CONCRETE APPROACH SLABS (T=15")
- ㉔ ITEM 526 - REINFORCED CONCRETE APPROACH SLABS (T=17")
- ㉕ ITEM 659 - SEEDING AND MULCHING
- ㉖ ITEM 622 - CONCRETE BARRIER, SINGLE SLOPE, TYPE C, AS PER PLAN (SHT. 18)
- ㉗ ITEM 622 - CONCRETE BARRIER, SINGLE SLOPE, TYPE C
- ㉘ ITEM 622 - CONCRETE BARRIER, SINGLE SLOPE, TYPE C1
- ㉙ ITEM 452 - 9" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC 1P
- ⑳ ITEM 302 - 6" ASPHALT CONCRETE BASE
- ㉑ ITEM 442 - 2 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE B
- ㉒ ITEM 609 - COMBINATION CURB AND GUTTER, TYPE 2, AS PER PLAN (SHT. 42)
- ㉓ ITEM 304 - 8" AGGREGATE BASE, AS PER PLAN (SHT. 43)
- ㉔ ITEM 442 - 2 1/2" ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), AS PER PLAN(SHT. 43)
- ㉕ ITEM 452 - 13" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC 1P
- ㉖ LONGITUDINAL JOINT AS PER BP-2.1
- ㉗ ITEM 204 - EXCAVATION OF SUBGRADE
ITEM 204 - EMBANKMENT
- ㉘ ITEM 618 - RUMBLE STRIPS (SHT. 23)

LEGEND - EXISTING ITEMS

- Ⓐ 3" ± ASPHALT CONCRETE
- ⒶⒶ 7" ± ASPHALT CONCRETE
- ⒶⒷ 1" ± ASPHALT CONCRETE
- ⒶⒸ 4 1/2" ± ASPHALT CONCRETE
- ⒶⒹ 5 1/4" ± ASPHALT CONCRETE
- Ⓑ 9" ± PLAIN CONCRETE PAVEMENT
- ⒷⒶ 9" ± REINFORCED CONCRETE PAVEMENT
- ⒷⒷ VARIABLE DEPTH PLAIN CONCRETE PAVEMENT
- ⒷⒸ 11" ± REINFORCED CONCRETE PAVEMENT
- Ⓒ 6" ± SUBBASE
- ⒸⒶ 3" ± WATERPROOFED AGGREGATE BASE
- ⒸⒷ 4" ± AGGREGATE BASE
- Ⓓ 3" TO 6" ± STABILIZED CRUSHED AGGREGATE
- ⒹⒶ 7" ± STABILIZED CRUSHED AGGREGATE
- Ⓔ PIPE UNDERDRAIN
- Ⓕ CONCRETE BARRIER, TYPE B-50
- ⒻⒶ CONCRETE BARRIER, TYPE D
- Ⓖ GUARDRAIL
- Ⓕ CURB
- ⒻⒶ CURB AND GUTTER
- Ⓖ WALK
- Ⓖ 6" ± ASPHALT CONCRETE BASE
- ⒼⒶ 3" ± ASPHALT CONCRETE BASE
- ⒼⒷ 10" ± ASPHALT CONCRETE BASE
- Ⓖ 15" ± REINFORCED CONCRETE APPROACH SLAB

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BURKHARDT AVE. NORMAL SECTION

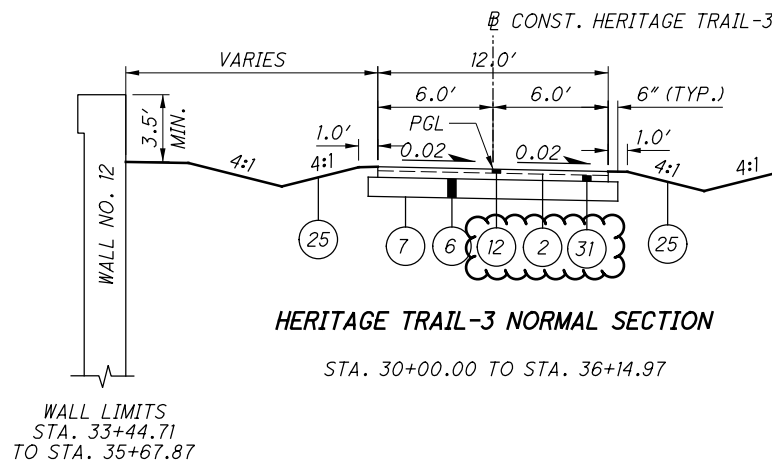
STA. 312+70.00 TO STA. 313+44.29
 (NOTE: LAFOLLETTE INTERSECTION
 STA. 313+44.29 TO STA. 316+19.77)
 STA. 316+19.77 TO STA. 318+20.00

WALL LIMITS

TRAIL STA. 10+45.00 TO
 STA. 12+90.26

HERITAGE TRAIL-1 SECTION

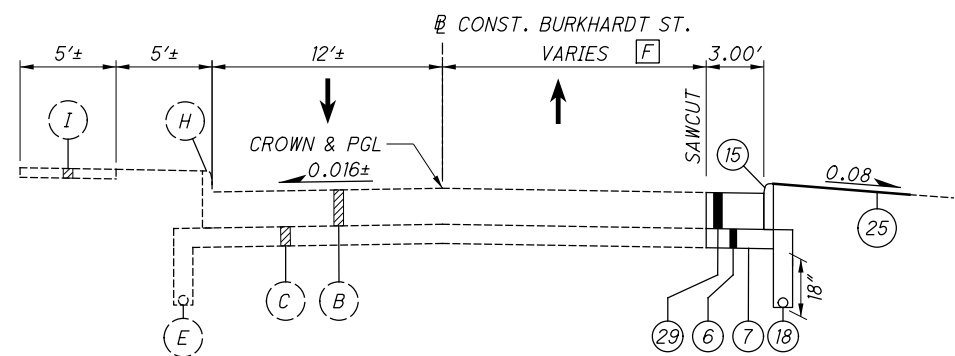
STA. 10+34.75 TO STA. 13+09.39



HERITAGE TRAIL-3 NORMAL SECTION

STA. 30+00.00 TO STA. 36+14.97

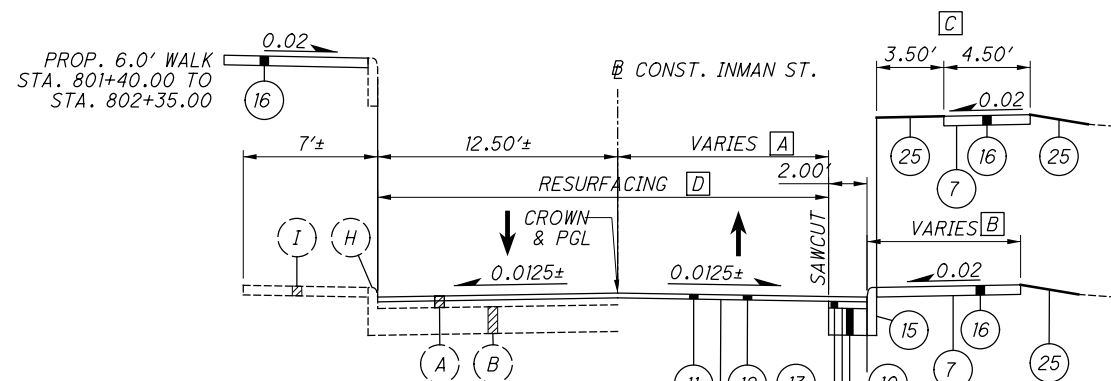
WALL LIMITS
 STA. 33+44.71
 TO STA. 35+67.87



BURKHARDT AVE. NORMAL SECTION

STA. 300+80.00 TO STA. 303+64.92

F VARIES FROM 12.50' AT STA. 300+80.00 TO 9.00' AT STA. 303+64.92

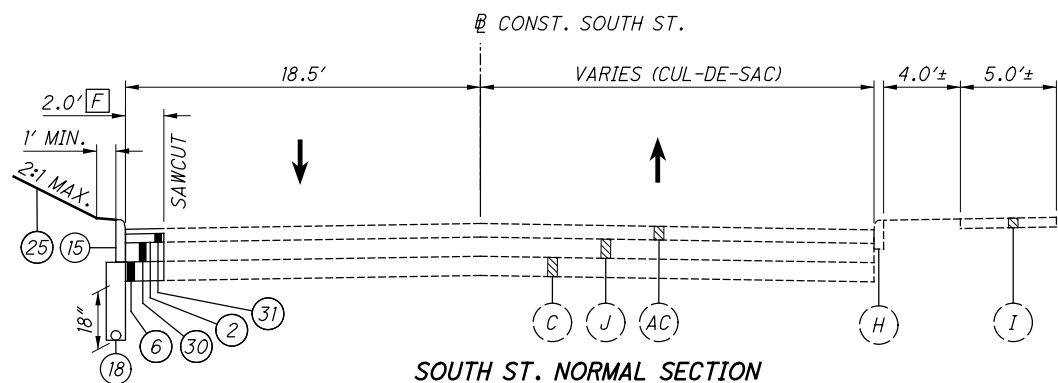


INMAN ST. NORMAL SECTION

STA. 802+42.72 TO STA. 803+84.00

- A** VARIES FROM 16.5' AT STA. 802+42.72 TO 11.00' AT STA. 803+52.82
11.0' FROM STA. 803+52.82 TO STA. 803+84.00
- B** 7.00' SIDEWALK FROM STA. 802+42.82 TO STA. 803+31.92
VARIES FROM 7.00' AT STA 803+31.92 TO 8.00' AT STA. 803+52.85
- C** FROM STA. 803+52.85 TO STA. 803+84.00
- D** SEE PLAN VIEW FOR LIMITS OF RESURFACING

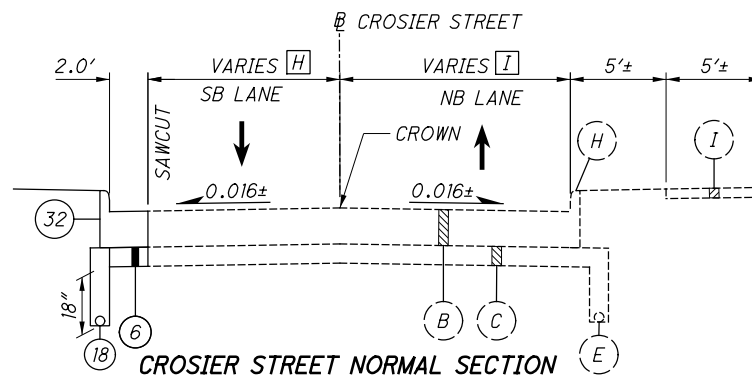
PROP. 6.0' WALK
 STA. 801+50.00 TO
 STA. 802+42.82



SOUTH ST. NORMAL SECTION

STA. 23+06.28 TO STA. 25+38.86

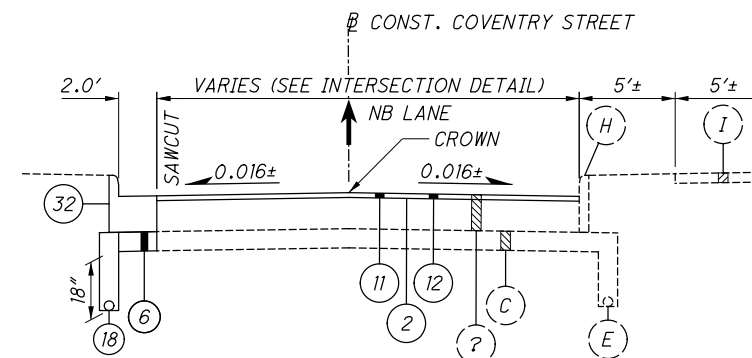
F FULL DEPTH PVMT. AND CURB FROM STA. 23+06.28 TO STA. 25+35.40



CROSIER STREET NORMAL SECTION

STA. 200+00.00 TO STA. 200+78.05

- H** FROM 10.0' TO 14.8' (SEE INTERSECTION DETAIL)
- I** FROM 16.80' TO 21.7' (SEE INTERSECTION DETAIL)



COVENTRY ST & KIPLING ST INTERSECTION

FROM COVENTRY ST. STA. 53+21.11, 12.67' LT
 TO KIPLING ST. STA. 600+50, 12.00' LT

NOTES
 FOR LEGEND SEE SHEET 17
 FOR PLAN ABBREVIATIONS, SEE GENERAL NOTES
 FOR DITCH AND GRADING INFORMATION, SEE CROSS SECTIONS AND SHEETS 37 TO 39

NATIONAL GEODETIC SURVEY (NGS) BENCHMARKS

ACCORDING TO THE NATIONAL GEODETIC SURVEY (NGS) EXPLORER WEBSITE (<https://www.ngs.noaa.gov/NGSDataExplorer/>), ONE EXISTING NGS BENCHMARK IS PRESENT ON STRUCTURES SUM-77-1184 (LAFOLLETTE STREET OVER SR-8). NOTIFY THE DISTRICT SURVEYOR, BY PHONE AT (330)-786-3100 AT LEAST THREE (3) WEEKS PRIOR TO REMOVAL OF THE STRUCTURE THAT THE NATIONAL GEODETIC SURVEY (NGS) DISK WILL BE REMOVED. REMOVE THE NGS DISK WITHOUT DAMAGING THE FACE AND SUBMIT IT TO THE DISTRICT SURVEYOR. ALL COSTS ASSOCIATED WITH THE REMOVAL AND SALVAGE OF THE NGS DISK ARE INCLUDED IN THE PAYMENT FOR ITEM 202 STRUCTURE REMOVED, OR ITEM 202, PORTIONS OF STRUCTURE REMOVED.

ENDANGERED BAT HABITAT REMOVAL

THIS PROJECT IS LOCATED WITHIN THE KNOWN HABITAT RANGES OF THE FEDERALLY LISTED AND PROTECTED INDIANA BAT AND NORTHERN LONG-EARED BAT. NO TREES SHALL BE REMOVED UNDER THIS PROJECT FROM APRIL 1 THROUGH SEPTEMBER 30. ALL NECESSARY TREE REMOVAL SHALL OCCUR FROM OCTOBER 1 THROUGH MARCH 31. THIS REQUIREMENT IS NECESSARY TO AVOID AND MINIMIZE IMPACTS TO THESE SPECIES AS REQUIRED BY THE ENDANGERED SPECIES ACT. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS: A LIVE, DYING, OR DEAD WOODY PLANT, WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE THE GROUND SURFACE, AND WITH A MINIMUM HEIGHT OF 13 FEET.

MONUMENT ASSEMBLIES

CONSTRUCT MONUMENT ASSEMBLIES IN ACCORDANCE WITH THE DETAILS SHOWN ON THE STANDARD CONSTRUCTION DRAWINGS AND AT THE LOCATIONS SHOWN IN THE R/W PLANS.

AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED A HEIGHT OF X FT. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA), AND THE ODOT OFFICE OF AVIATION, WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. THE CONTRACTOR WILL BE REQUIRED TO FILE A NEW FAA FORM 7460-1, ADVISING THE FAA THAT AERONAUTICAL STUDY NO. X IS BEING RESUBMITTED AND THAT AN ALTERATION TO THE ORIGINAL SUBMISSION IS REQUESTED.

NOTIFY THE ODOT OFFICE OF AVIATION WHEN RESUBMITTING FAA FORM 7460-1. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL A COPY OF THE FAA APPROVAL AND THE ODOT OFFICE OF AVIATION PERMIT HAS BEEN FURNISHED TO THE PROJECT ENGINEER.

FAA APPROVAL MAY TAKE UP TO 45 DAYS. ALL SUBMISSIONS SHALL BE DIRECTED TO THESE OFFICES:

EXPRESS PROCESSING CENTER THE FEDERAL AVIATION ADMINISTRATION SOUTHWEST REGIONAL OFFICE AIR TRAFFIC AIRSPACE BRANCH ASW-520 2601 MEACHAM BLVD. FORT WORTH, TX 76137-4298	OHIO DEPARTMENT OF TRANSPORTATION OFFICE OF AVIATION 2829 WEST DUBLIN- GRANVILLE ROAD COLUMBUS, OHIO 43235 614-387-2346
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ABANDONED (BURIED) STRUCTURES

PER RECORD PLAN SUM-76/77-11.27/12.12, THE PIERS, ABUTMENTS, FOUNDATIONS AND OTHER SUBSTRUCTURAL ELEMENTS OF STRUCTURES SUM-76-1137R, SUM-76-1137L AND SUM-77-1212R WERE ABANDONED AND BURIED UNDER EMABANKMENT. THE ABANDONED STRUCTURAL ELEMENTS ARE DEPICTED IN THE PLANS USING THE AVAILABLE RECORD PLAN INFORMATION, AND ARE INTENDED TO BE LEFT IN PLACE AND UNDISTURBED/UMIMPACTED DURING AND AFTER THE CONSTRUCTION OF THE PROJECT. WHILE WORKING IN OR AROUND THE LOCATION(S) WHERE KNOWN SUBSURFACE ABANDONED STRUCTURAL ELEMENTS MAY BE PRESENT, THE CONTRACTOR SHALL TAKE CAUTION NOT TO IMPACT OR DISTURB THESE EXISTING STRUCTURES. SHOULD THE CONTRACTOR EXPOSE, IMPACT, DAMAGE OR OTHERWISE MAKE CONTACT WITH THESE ABANDONED STRUCTURES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IMMEDIATELY AND SUSPEND ALL WORK IN THE AREA UNTIL THE PROJECT ENGINEER GIVES NOTICE TO RESUME WORK.

CLEARING AND GRUBBING

REMOVE ALL TREES AND STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE CONSTRUCTION LIMITS UNDER THE LUMP SUM BID FOR ITEM 201, CLEARING AND GRUBBING.

INDIVIDUAL TREE/STUMP SIZES WITHIN HEAVILY VEGITATED AREAS MAY NOT BE KNOWN AND WERE NOT SURVEYED, WHICH COVERS MOST OF THE VEGETATED AREAS WITHIN THE CONSTRUCTION LIMITS. UNLESS SPECIFICALLY MARKED AS "DO NOT DISTURB" IN THE PLANS, CLEARING OF ALL HEAVILY VEGETATED AREAS SHOWN WITHIN THE CONSTRUCTION LIMITS SHALL BE PAID FOR UNDER ITEM 201, CLEARING AND GRUBBING.

CONSTRUCTION NOISE

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, DO NOT OPERATE POWER-OPERATED CONSTRUCTION TYPE DEVICES BETWEEN THE HOURS OF 8AM AND 10PM, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. IN ADDITION, DO NOT OPERATE AT ANY TIME ANY DEVICE IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

ROADWAY NOTES

ROUNDING

THE ROUNDING AT SLOPE BREAKPOINTS SHOWN ON THE TYPICAL SECTIONS APPLIES TO ALL CROSS-SECTIONS EVEN THOUGH OTHERWISE SHOWN.

EXISTING UTILITIES AND SUBGRADE TREATMENT

THE CONTRACTOR SHALL VERIFY THE DEPTH OF ALL EXISTING UNDERGROUND UTILITIES AND SEWERS WITHIN THE PROPOSED PAVEMENT LIMITS TO ENSURE NO UTILITIES OR SEWERS ARE IMPACTED OR DAMAGED DURING CEMENT STABILIZATION AND/OR UNDERCUT ACTIVITIES. THE CONTRACTOR SHALL LOCATE AND TAKE CARE TO FLAG ALL EXISTING UTILITIES WITHIN THE PROPOSED PAVEMENT LIMITS PRIOR TO PERFORMING CEMENT STABILIZATION OR UNDERCUT, AS DESIGNATED IN THE PLANS. SHOULD THE CONTRACTOR ENCOUNTER A POTENTIAL UTILITY CONFLICT, THE CONTRACTOR SHALL NOTIFY PROJECT ENGINEER AND STOP CEMENT STABILIZATION/UNDERCUT ACTIVITIES AT THE CONFLICT LOCATION IMMEDIATELY.

MEDIAN AND/OR CURBING ON APPROACH SLABS

WITHIN THE LIMITS OF THE APPROACH SLAB, TRANSITION THE SHAPE OF THE MEDIAN AND/OR CURBING ON APPROACH SLABS FROM THE STANDARD SECTION ON THE APPROACHES TO THE SECTION USED ON THE BRIDGE.

SHEETING & BRACING

ANY SHEETING AND BRACING USED BY THE CONTRACTOR AND NOT OTHERWISE CALLED FOR IN THE PLANS SHALL BE FURNISHED, INSTALLED, AND MAINTAINED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE. NO SEPARATE PAYMENT SHALL BE MADE FOR SHEETING AND BRACING. AT ALL TIMES THE CONTRACTOR SHALL BE REQUIRED TO EXCAVATE IN A MANNER THAT IS SAFE TO ALL WORKERS AND THE GENERAL TRAVELING PUBLIC. ALL OSHA REQUIREMENTS SHALL BE UPHELD AND SOUND SAFETY PRACTICES SHALL BE EXERCISED AT ALL TIMES. REMOVAL OF SHEETING AND BRACING ITEMS UPON COMPLETION OF WORK WILL BE REQUIRED AS DIRECTED BY ODOT REPRESENTATIVES.

ITEM 203 EMBANKMENT, AS PER PLAN

THE FOLLOWING REQUIREMENTS ARE IN ADDITION TO THOSE OF CMS 203

AT RETAINING WALLS: EMBANKMENT PLACED FOR SECTIONS OF APPROACH RAMPS N & Q UPON WHICH SUPPORTS AND IS CONTAINED BY RETAINING WALLS SHALL BE CONSTRUCTED IN THE FOLLOWING MANNER:

1. THE INITIAL EMBANKMENT LIFTS FOR THE ENTIRE WIDTH OF APPROACH EMBANKMENT SHALL BE CONSTRUCTED TO AN ELEVATION AT OR ABOVE THE TOP OF FOOTING HEEL FOR EACH INDIVIDUAL SECTION OF RETAINING WALL. WHERE BACK-TO-BACK WALLS OCCUR (SUCH AS RETAINING WALLS 2 AND 3) THE ELEVATION OF THE INITIAL FILL PLACEMENT SHALL BE BUILT TO LEVELS THAT ARE HIGH ENOUGH TO COVER ALL INDIVIDUAL FOOTING SECTIONS OF THE RETAINING WALLS.
2. AFTER INITIAL EMBANKMENT PLACEMENT IS COMPLETE FOR EACH APPROACH RAMP THE SPECIFIED EMBANKMENT SETTLEMENT WAITING PERIOD SHALL COMMENCE. SEE RAMP BRIDGE PLANS FOR WAITING PERIOD DURATION.
3. FOLLOWING EXPIRATION OF THE SPECIFIED SETTLEMENT WAITING PERIOD FOR THE INITIAL EMBANKMENT PLACEMENT, EXCAVATIONS FOR THE RETAINING WALL FOOTINGS MAY COMMENCE.
4. ALL CONCRETE FOR RETAINING WALL FOOTINGS SHALL BE CAST NEAT AGAINST THE VERTICAL SIDES OF THE INDIVIDUAL FOOTING EXCAVATION. IF FORMS ARE USED TO CONSTRUCT THE VERTICAL SIDES OF THE FOOTINGS ANY VOIDS BETWEEN THE VERTICAL SIDES OF THE FOOTINGS AND THE ADJACENT UNDISTURBED GROUND SHALL BE BACKFILLED WITH CLASS OCI CONCRETE.

AT BRIDGE APPROACHES: AT THE FOLLOWING LOCATIONS, PLACE AND COMPACT EMBANKMENT MATERIAL IN 6 INCH LIFTS FOR THE CONSTRUCTION OF THE APPROACH EMBANKMENT AT BRIDGES:

- | | |
|----------------------------|--|
| 1. RAMP Q (SUM-76-1148Q) | STA. 2530+67.83 TO 2531+67.83
STA. 2540+16.04 TO 2541+16.04 |
| 2. RAMP N (SUM-76-1152N) | STA. 3331+41.18 TO 3332+41.18
STA. 3338+79.43 TO 3339+79.43 |
| 3. IR-76 EB (SUM-76-1127) | STA. 520+75.68 TO 521+75.68
STA. 523+35.89 TO 524+35.89 |
| 4. IR-76 WB (SUM-76-1180L) | STA. 548+46.24 TO 549+46.24
STA. 550+06.00 TO 551+06.00 |

IN ADDITION TO THE REQUIREMENTS STATED ABOVE, PORTIONS OF THE EMBANKMENT BEHIND BOTH ABUTMENTS OF THE RAMP N BRIDGE AND THE FORWARD ABUTMENT OF THE RAMP Q BRIDGE SHALL BE COMPRISED OF TYPE B GRANULAR EMBANKMENT. SEE SHEETS 757/1022 AND 821/1022 FOR FURTHER CLARIFICATION OF THE LIMITS OF GRANULAR EMBANKMENT.

PAYMENT FOR PLACING THE EMBANKMENT DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR ITEM 203 - EMBANKMENT, AS PER PLAN CALCULATED IN THE ROADWAY CROSS SECTIONS.

BENCHING OF FOUNDATION SLOPES

ALTHOUGH CROSS-SECTIONS INDICATE SPECIFIC DIMENSIONS FOR PROPOSED BENCHING OF THE EMBANKMENT FOUNDATIONS IN CERTAIN AREAS, NO WAIVER OF THE SPECIFICATIONS IS INTENDED. BENCH ALL OTHER SLOPED EMBANKMENT AREAS AS SET FORTH IN 203.05. NO ADDITIONAL PAYMENT WILL BE MADE FOR BENCHING REQUIRED UNDER THE PROVISIONS OF 203.05

CALCULATED
TK
CHECKED
MRT

GENERAL NOTES

SUM-76/77/8
10.99/11.54/0.00

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1022

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SHEET NUM.										PART.		ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
43	44	45	61	65	426	530	641	OFFICE CALC	09/IMS/PV	10/NHS/PV							
				4					4		611	99575	4	EACH	MANHOLE, NO. 3, AS PER PLAN	44	
				2					2		611	99654	2	EACH	MANHOLE ADJUSTED TO GRADE		
				2					2		611	99655	2	EACH	MANHOLE ADJUSTED TO GRADE, AS PER PLAN	44	
				10					10		611	99660	10	EACH	MANHOLE RECONSTRUCTED TO GRADE		
				3					3		611	99661	3	EACH	MANHOLE RECONSTRUCTED TO GRADE, AS PER PLAN	45	
	10	10			7				17		611	99710	17	EACH	PRECAST REINFORCED CONCRETE OUTLET		
									10		611	99720	10	EACH	INSPECTION WELL		
				24					24		638	06712	24	FT	30" STEEL PIPE ENCASEMENT, OPEN CUT		
				27					27		899	10000	27	FT	CURED-IN-PLACE PIPE LINER, 48" DIAMETER	45	
															PAVEMENT		
250									250		251	01020	250	SY	PARTIAL DEPTH PAVEMENT REPAIR (442)		
250									250		253	01000	250	SY	PAVEMENT REPAIR		
								39,636	8,832	30,804	254	01000	39,636	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1.5"		
405								17	422		302	46000	422	CY	ASPHALT CONCRETE BASE, PG64-22		
								554	554		304	20001	554	CY	AGGREGATE BASE, AS PER PLAN	43	
110								27,752	26,014	1,848	407	20000	27,862	GAL	NON-TRACKING TACK COAT		
140									140		442	10100	140	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, 19 MM, TYPE A (446)		
									367	1,284	442	10301	1,651	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), AS PER PLAN	43	
									269		442	10351	269	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE B (447), AS PER PLAN	43	
									6		442	20250	6	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, 19 MM, TYPE B (448)		
								1,741	1,741		452	13010	1,741	SY	9" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC 1P		
				5,328					5,328		609	24511	5,328	FT	CURB, TYPE 4-C, AS PER PLAN	42	
				1,817					1,817		609	26001	1,817	FT	CURB, TYPE 6, AS PER PLAN	42	
															PAVEMENT DESIGN - OPTION A (FLEXIBLE)		
								29,350	29,350		302	46000	29,350	CY	ASPHALT CONCRETE BASE, PG64-22		
								18,526	18,526		304	20001	18,526	CY	AGGREGATE BASE, AS PER PLAN	43	
								25,204	25,204		407	20000	25,204	GAL	NON-TRACKING TACK COAT		
3,281									3,281		442	00100	3,281	CY	ANTI-SEGREGATION EQUIPMENT		
								4,336	4,336		442	10301	4,336	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), AS PER PLAN	43	
								5,218	5,218		442	10101	5,218	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, 19 MM, TYPE A (446), AS PER PLAN	43	
															PAVEMENT DESIGN - OPTION B (RIGID)		
											304	20001		CY	AGGREGATE BASE, AS PER PLAN	43	
											452	16020		SY	13" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC 1P WITH QC/QA		
															WATER WORK		
						35			35		638	01205	35	FT	8" WATER MAIN DUCTILE IRON PIPE ANSI CLASS 53, PUSH-ON JOINTS AND FITTINGS, AS PER PLAN	531	
						1			1		638	10481	1	EACH	FIRE HYDRANT REMOVED, AS PER PLAN	532	
						9			9		638	10801	9	EACH	VALVE BOX ADJUSTED TO GRADE, AS PER PLAN	531	
															LIGHTING		
															SEE SHEET 645 FOR LIGHTING GENERAL SUMMARY		
															TRAFFIC SURVEILLANCE OPTION A		
						995			995		625	23000	995	FT	NO. 4 AWG 600 VOLT DISTRIBUTION CABLE		
						1,228			1,228		625	25410	1,228	FT	CONDUIT, 2", 725.052		
						12			12		625	25504	12	FT	CONDUIT, 3", 725.051		
						1,234			1,234		625	29001	1,234	FT	TRENCH, AS PER PLAN	640	
						10			10		625	30700	10	EACH	PULL BOX, 725.08, 18"		
						2			2		625	31510	2	EACH	PULL BOX REMOVED		
						2			2		625	31600	2	EACH	PULL BOX, MISC.: ADJUSTED TO GRADE	640	
						1			1		625	32000	1	EACH	GROUND ROD		
						1,020			1,020		632	62810	1,020	FT	INTERCONNECT CABLE, MISC.: CABLE RELOCATED	640	
						1			1		633	67100	1	EACH	CABINET FOUNDATION		
						1			1		633	67200	1	EACH	CONTROLLER WORK PAD		
						1			1		809	65000	1	EACH	ITS CABINET - GROUND MOUNTED		
						1			1		809	65990	1	EACH	ITS DEVICE, MISC.: REMOVAL OF SMART JACK	640	
						1			1		809	65990	1	EACH	ITS DEVICE, MISC.:RELOCATION OF CCTV CONTROL PANEL	640	
															TRAFFIC SURVEILLANCE OPTION B (ATC)		
									LS		SPECIAL	80999000	LS		ITS		

QUANTITY IS BASED ON PAVEMENT BUILD-UP PRIOR TO ADDENDUM 8 THAT CHANGED THE SUP SURFACE COURSE DEPTH AND TYPE.

QUANTITY IS BASED ON PAVEMENT BUILD-UP PRIOR TO ADDENDUM 4 THAT CHANGED THE ASPHALT CONCRETE BASE DEPTH.

GENERAL SUMMARY

**SUM-76/77/8
10.99/11.54/0.00**

WB-1 P.I. Sta. 522+21.92
 $\Delta = 2^\circ 07' 37''$ (RT)
 $D_c = 0^\circ 30' 00''$
 $R = 11,459.16'$
 $T = 212.72'$
 $L = 425.39'$
 $E = 1.97'$
 $\Theta_{max} = \text{MATCH EX.}$
 (APPROX. 0.030)
 P.C. Sta. 520+09.20
 P.T. Sta. 524+34.58

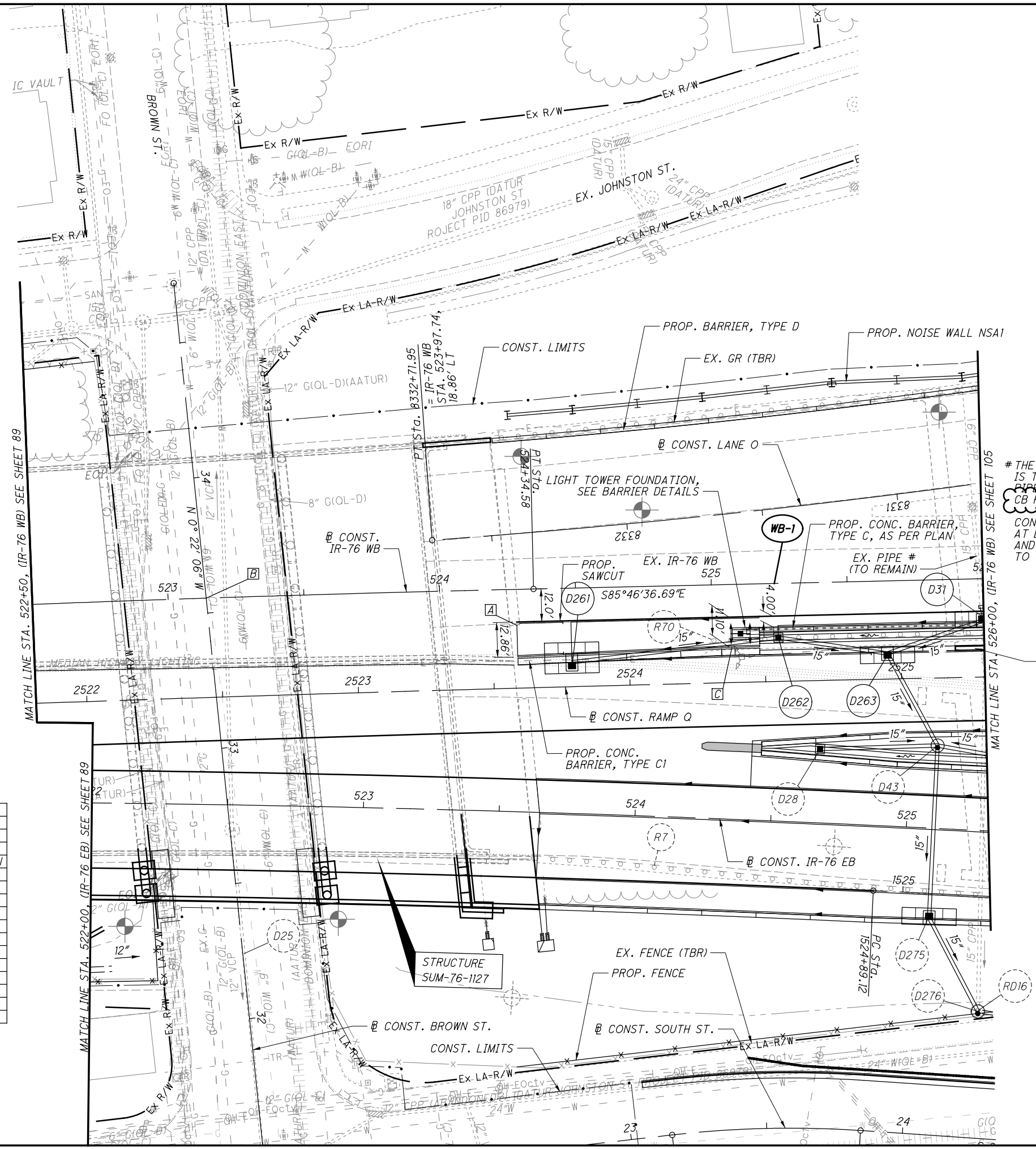
- LEGEND**
- SEE GENERAL NOTES FOR LIST OF PLAN ABBREVIATIONS
 - PAVEMENT PLANNING & RESURFACING
 - ITEM 670 DITCH EROSION PROTECTION
 - BEG. SAWCUT AND CONC. MEDIAN BARRIER AT EX. BRIDGE APPROACH SLAB APPROX. STA. 524+28.13
 - CONST. IR-76 WB STA. 523+14.02 = CONST. BROWN ST. STA. 33+55.53
 - END SHLDR TAPER STA. 525+07.34, 23.10' RT



PLAN - IR-76 WB
 STA. 522+50.00 TO STA. 526+00.00

SUM-76/77/8
 10.99/11.54/0.00

103
 1022



CROSS REFERENCES	
SHEET NO.	DESCRIPTION
7 - 9	GEOMETRIC LAYOUT
51 - 71	SUBSUMMARIES
77 - 83	PAVEMENT REMOVAL PLAN
237 - 284	CROSS SECTIONS
403 - 412	SUPERELEVATION TABLES
419 - 420	RAMP TERMINAL DETAILS
424 - 425	STORM SEWER PROFILES
435 - 441	UNDERDRAIN DETAILS
442 - 529	RETAINING WALLS
537 - 556	BARRIER DETAILS
571 - 620	TRAFFIC CONTROL
643 - 673	LIGHTING PLANS
682 - 715	NOISE WALLS
716 - 950	STRUCTURES DETAILS
952 - 958	FENCE PLANS

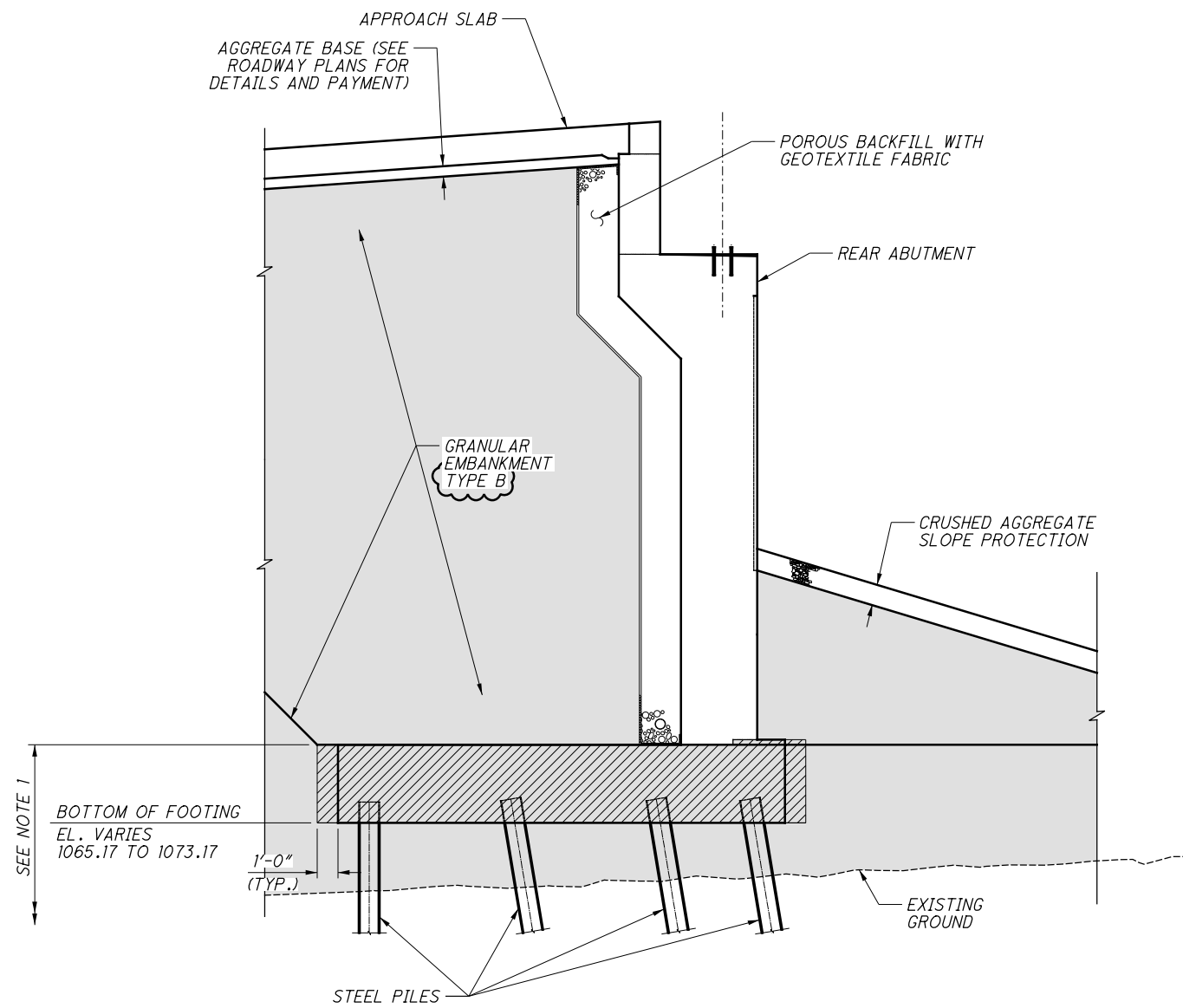
THE EXISTING PIPE TO THE NORTH FROM D31 IS TO REMAIN. THE PROPOSED INVERT OF THE PIPE AT D31 IS UNKNOWN DUE TO THE EXISTING CB RD15 NOT BEING FIELD LOCATED.

CONTRACTOR TO VERIFY INVERT OF EX. PIPE AT D31 AND ENSURE PIPE INVERTS BETWEEN D31 AND D263 PROMOTE POSITIVE DRAINAGE PRIOR TO ORDERING ANY PRECAST STRUCTURE FOR D31.

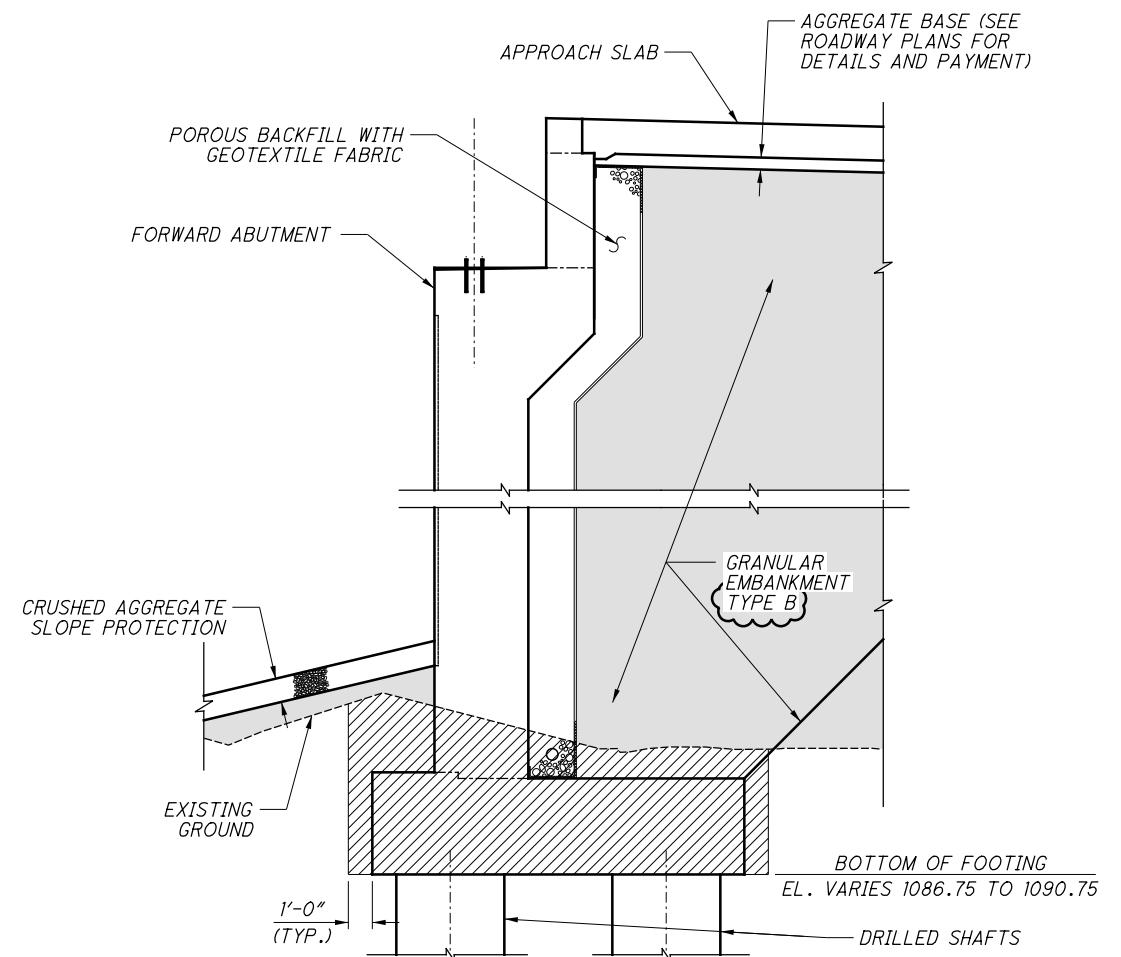
PROP. RETAINING WALL # 3
 SEE SHEET 458

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EXCAVATION & EMBANKMENT DIAGRAM - REAR ABUTMENT



EXCAVATION & EMBANKMENT DIAGRAM - FORWARD ABUTMENT

LEGEND:

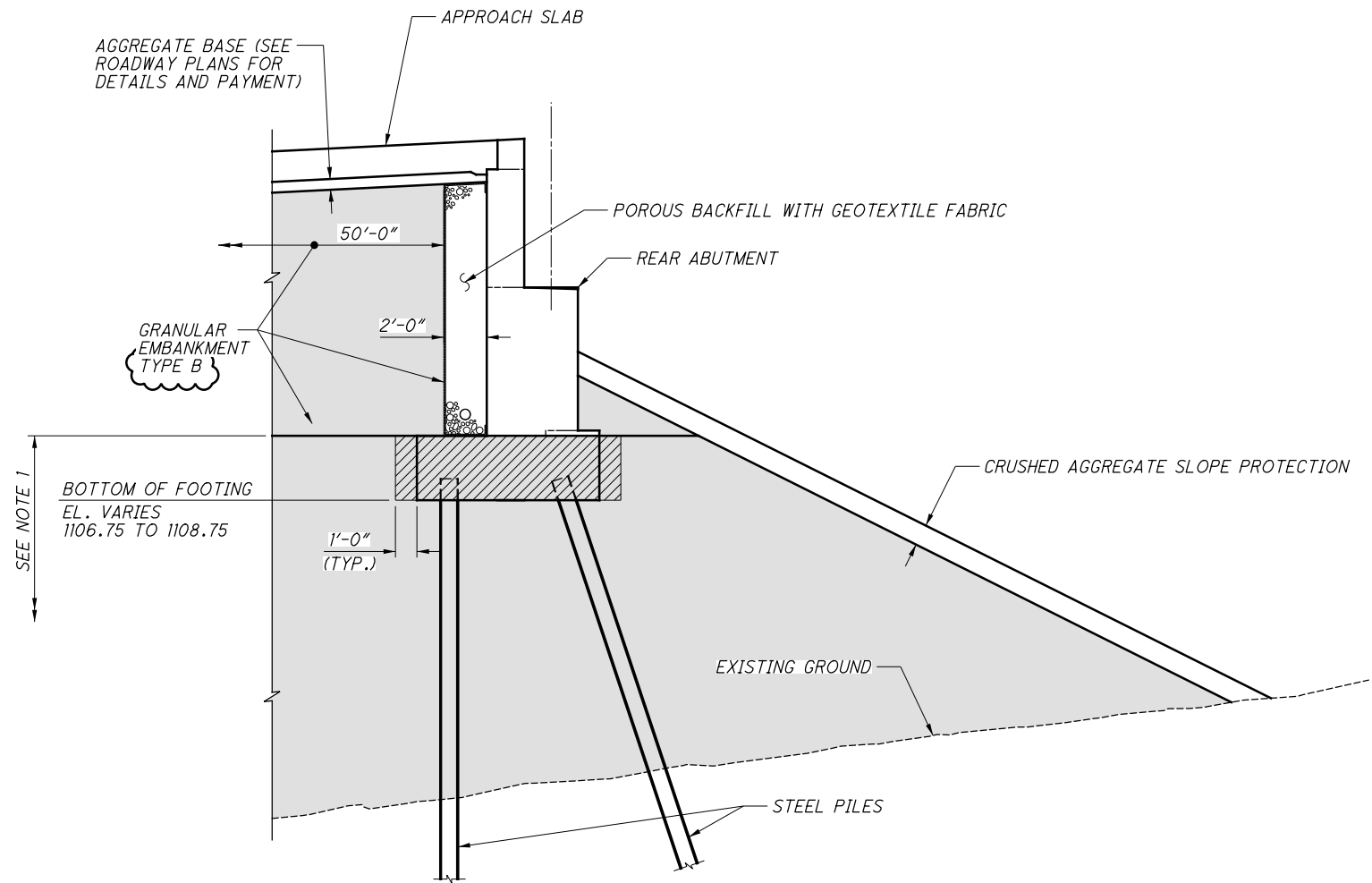
= LIMITS OF ITEM 203 - EMBANKMENT, AS PER PLAN, SEE ROADWAY PLANS FOR ADDITIONAL DETAILS & PAYMENT

= LIMITS OF ITEM 503 - UNCLASSIFIED EXCAVATION

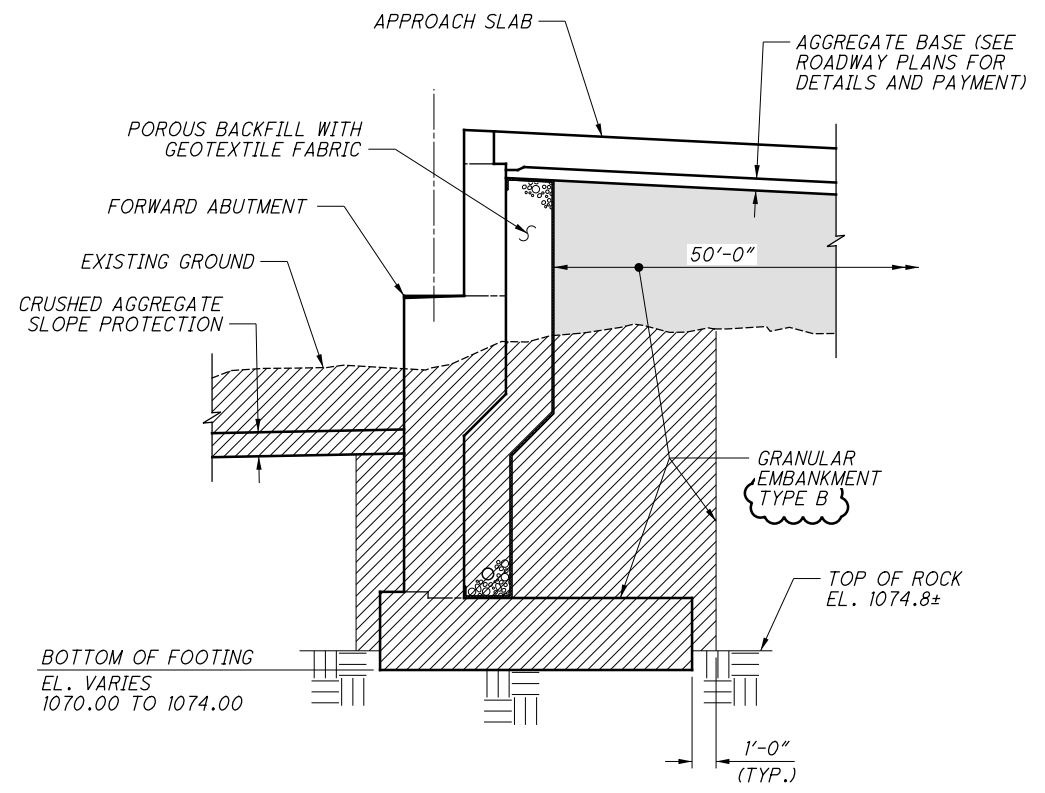
NOTES:

1. SEE GENERAL NOTES FOR REQUIREMENTS REGARDING CONSTRUCTION OF EMBANKMENT BELOW TOP OF REAR ABUTMENT FOOTING, AND REQUIRED WAITING PERIOD PRIOR TO DRIVING PILES.

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EXCAVATION & EMBANKMENT DIAGRAM - REAR ABUTMENT



EXCAVATION & EMBANKMENT DIAGRAM - FORWARD ABUTMENT

LEGEND:

- = LIMITS OF ITEM 203 - EMBANKMENT, AS PER PLAN, SEE ROADWAY PLANS FOR ADDITIONAL DETAILS & PAYMENT
- = LIMITS OF ITEM 503 - UNCLASSIFIED EXCAVATION

NOTES:

1. SEE GENERAL NOTES FOR REQUIREMENTS REGARDING CONSTRUCTION OF EMBANKMENT BELOW TOP OF REAR ABUTMENT FOOTING, AND REQUIRED WAITING PERIOD PRIOR TO DRIVING PILES.