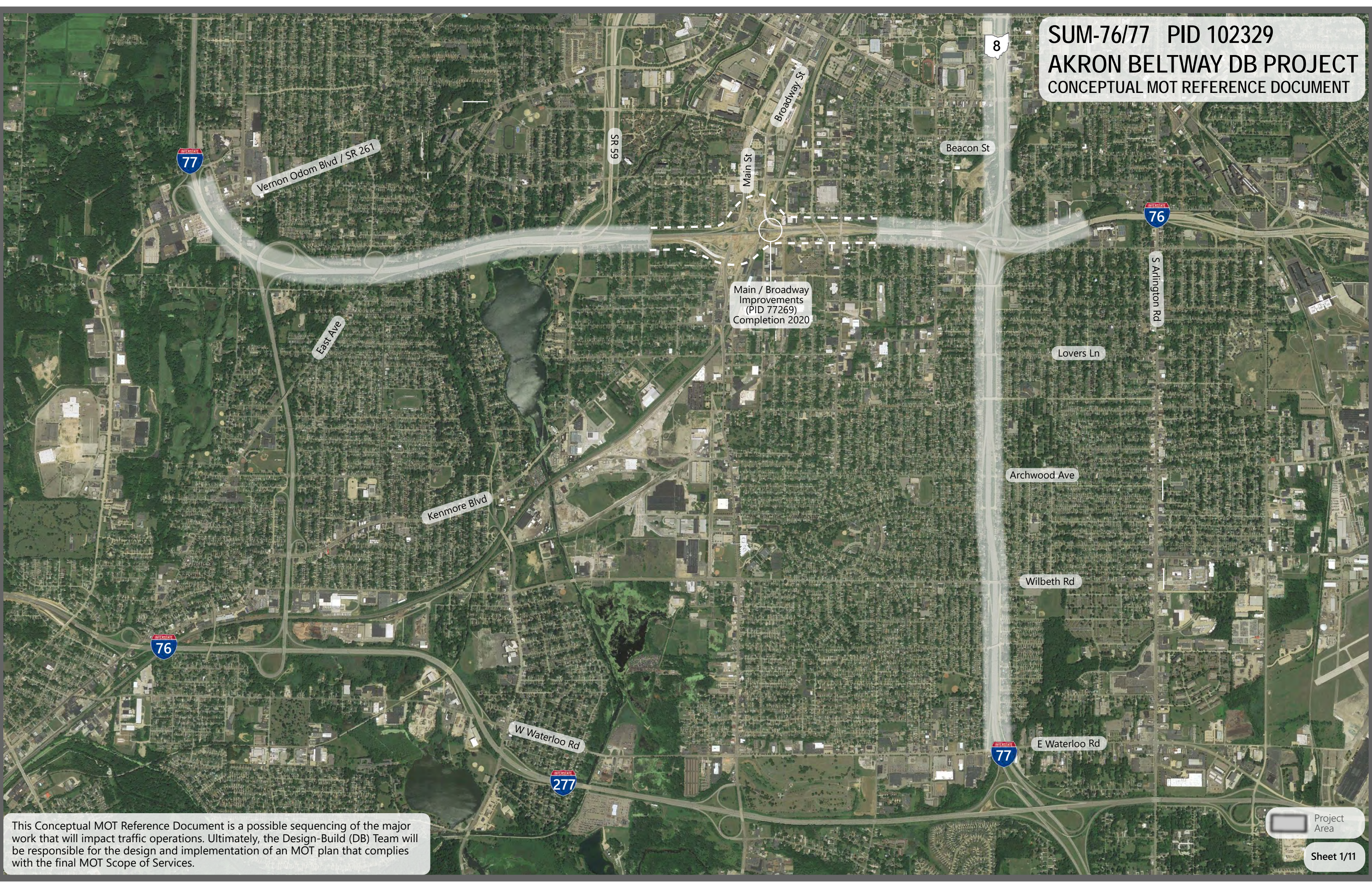


SUM-76/77 PID 102329
AKRON BELTWAY DB PROJECT
CONCEPTUAL MOT REFERENCE DOCUMENT



Main / Broadway
Improvements
(PID 77269)
Completion 2020

Project Area

This Conceptual MOT Reference Document is a possible sequencing of the major work that will impact traffic operations. Ultimately, the Design-Build (DB) Team will be responsible for the design and implementation of an MOT plan that complies with the final MOT Scope of Services.

SUM-76/77 PID 102329
AKRON BELTWAY DB PROJECT
 POTENTIAL PHASE: 1

SR 8 Resurfacing
 as per PLCC*


Work to be Completed:

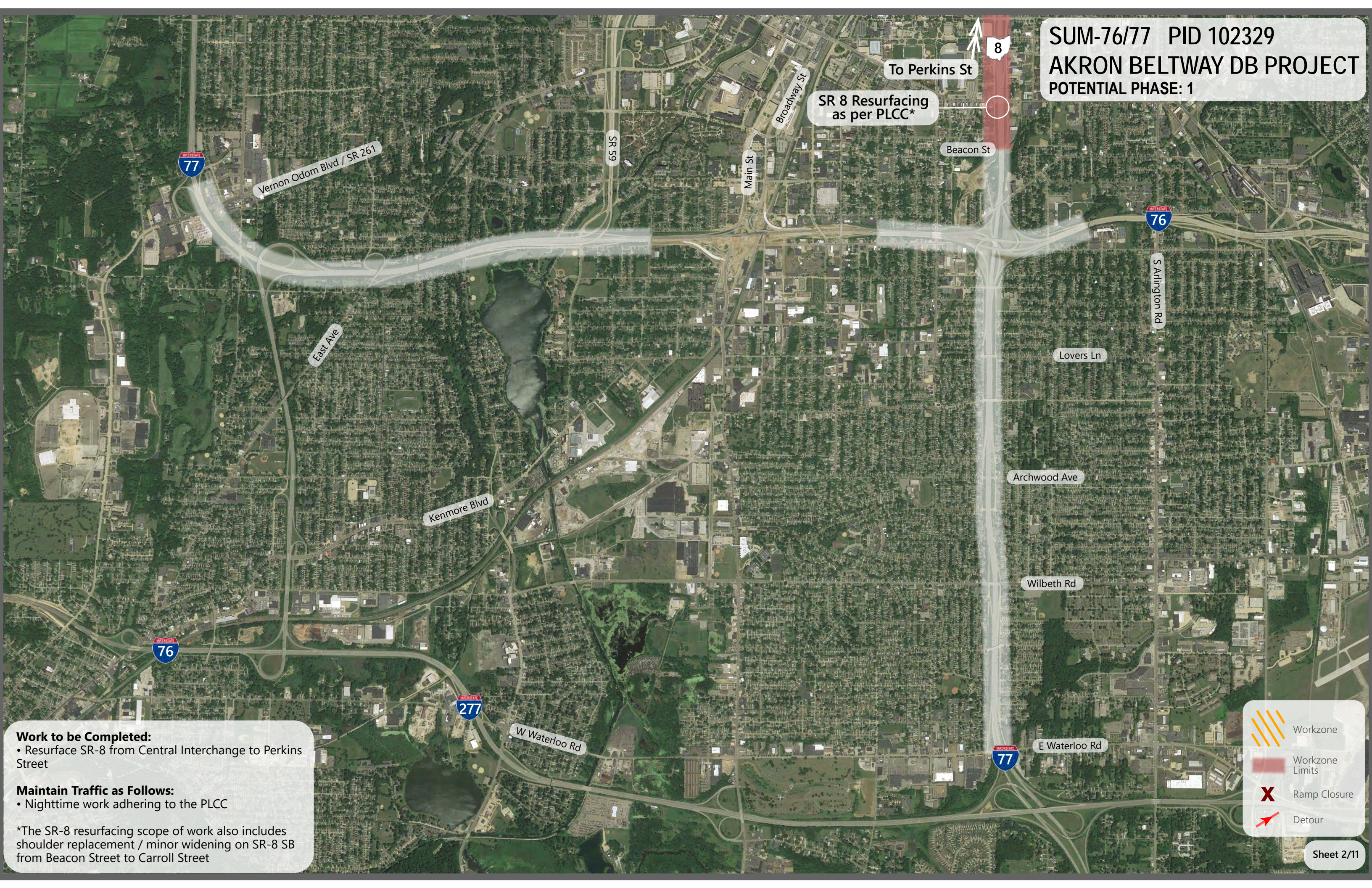
- Resurface SR-8 from Central Interchange to Perkins Street

Maintain Traffic as Follows:

- Nighttime work adhering to the PLCC

*The SR-8 resurfacing scope of work also includes shoulder replacement / minor widening on SR-8 SB from Beacon Street to Carroll Street

-  Workzone
-  Workzone Limits
-  Ramp Closure
-  Detour



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AKRON BELTWAY DB PROJECT
POTENTIAL PHASE: 2A

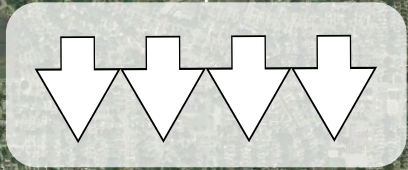
Conceptual 77 Part-Width Sequencing*



- Work to be Completed:**
- Replace outside halves of northbound and southbound pavement (approximately 4500 ft) and applicable ramp work along I-76/I-77 in northwest quadrant
- Maintain Traffic as Follows:**
- Restripe and add fourth lane along I-77SB south of Cole Ave as noted
 - Close I-77 SB to I-76 WB ramp
 - Close I-76 ramps to and from Kenmore leg at I-77 interchange
 - Close I-76 ramps to Kenmore leg at I-277 interchange

- 76EB to 77NB ramp closed
- 76WB/77NB to 76WB permissible ramp closure
- 77SB to 76WB ramp closed
- 76EB to 76EB ramp closed
- On-ramp
- On-ramp
- 277WB to 76EB ramp closed
- 76EB to 76EB ramp closed

Restripe 77SB to maintain four lanes south of Cole Ave. Auxiliary lane between Wilbeth and Waterloo will remain. Lane four will drop at 277WB. Lane three will merge with lane two within 277 interchange before US-224 WB on-ramp.

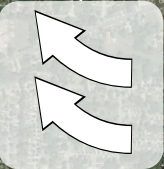


- Workzone
- Workzone Limits
- Ramp Closure
- Detour
- Ramp Open

*Part-width sequencing to be determined by Design-Build team

SUM-76/77 PID 102329
AKRON BELTWAY DB PROJECT
POTENTIAL PHASE: 2B

Temporarily restripe. Create a two-lane exit with a downstream inside merge over Morse Street.



Vernon Odom Blvd / SR 261

SR 59

Main St

Broadway St

Beacon St

8SB to 76WB ramp closed

77NB to 76WB ramp closed

S Arlington Rd

Lovers Ln

Cole Ave

Archwood Ave

Wilbeth Rd

E Waterloo Rd

Kenmore Blvd

W Waterloo Rd

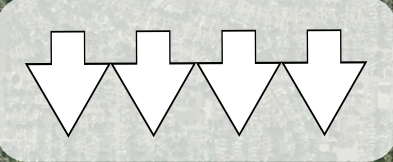
Detour

Conceptual 77 Part-Width Sequencing*



77 Alternate to SR 21

Restripe 77SB to maintain four lanes south of Cole Ave. Auxiliary lane between Wilbeth and Waterloo will remain. Lane four will drop at 277WB. Lane three will merge with lane two within 277 interchange before US-224 WB on-ramp.





- Workzone
- Workzone Limits
- Ramp Closure
- Detour
- Ramp Open
- * Part-width sequencing to be determined by Design-Build team
- ** Lanes represent upstream and/or downstream ramp lanes

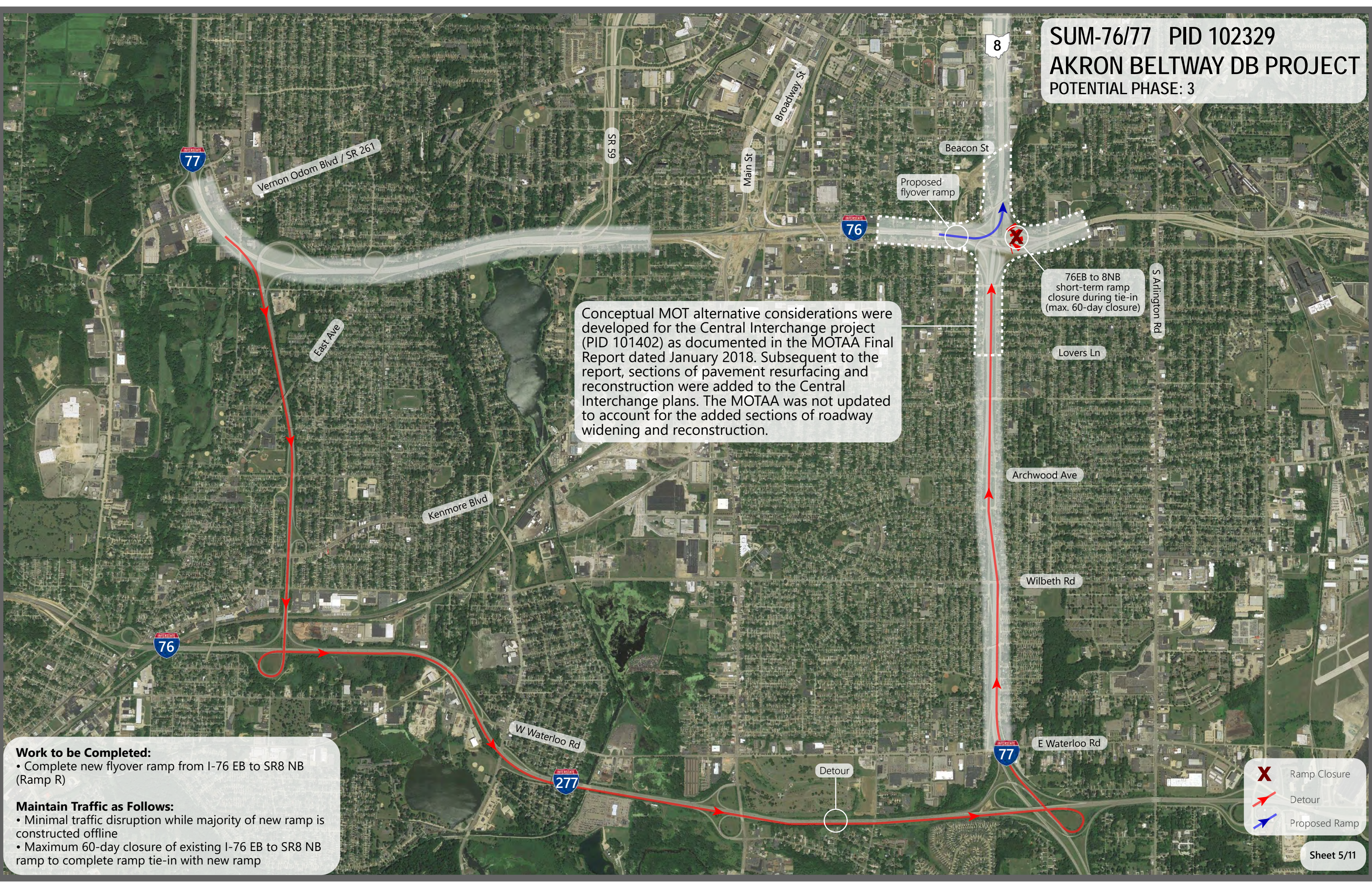
- Work to be Completed:**
- Replace median half of northbound and southbound pavement (approximately 4500 ft) along I-76/I-77 in northwest quadrant
- Maintain Traffic as Follows:**
- Restripe and add fourth lane along I-77SB south of Cole Ave as noted
 - Close I-77 through movement in both directions
 - Close the SR8 SB to I-76 WB ramp to reduce I-77 NB traffic
 - Close the I-77 NB to I-76 WB ramp to reduce I-77 NB traffic
 - Divert I-77 NB traffic to I-277/Kenmore leg/SR21
 - Divert I-77 SB traffic down Kenmore leg to I-277
 - Divert SR8 SB traffic south to marked detour

SUM-76/77 PID 102329
AKRON BELTWAY DB PROJECT
POTENTIAL PHASE: 3

Conceptual MOT alternative considerations were developed for the Central Interchange project (PID 101402) as documented in the MOTAA Final Report dated January 2018. Subsequent to the report, sections of pavement resurfacing and reconstruction were added to the Central Interchange plans. The MOTAA was not updated to account for the added sections of roadway widening and reconstruction.

- Work to be Completed:**
- Complete new flyover ramp from I-76 EB to SR8 NB (Ramp R)
- Maintain Traffic as Follows:**
- Minimal traffic disruption while majority of new ramp is constructed offline
 - Maximum 60-day closure of existing I-76 EB to SR8 NB ramp to complete ramp tie-in with new ramp



X Ramp Closure
 Detour
 Proposed Ramp

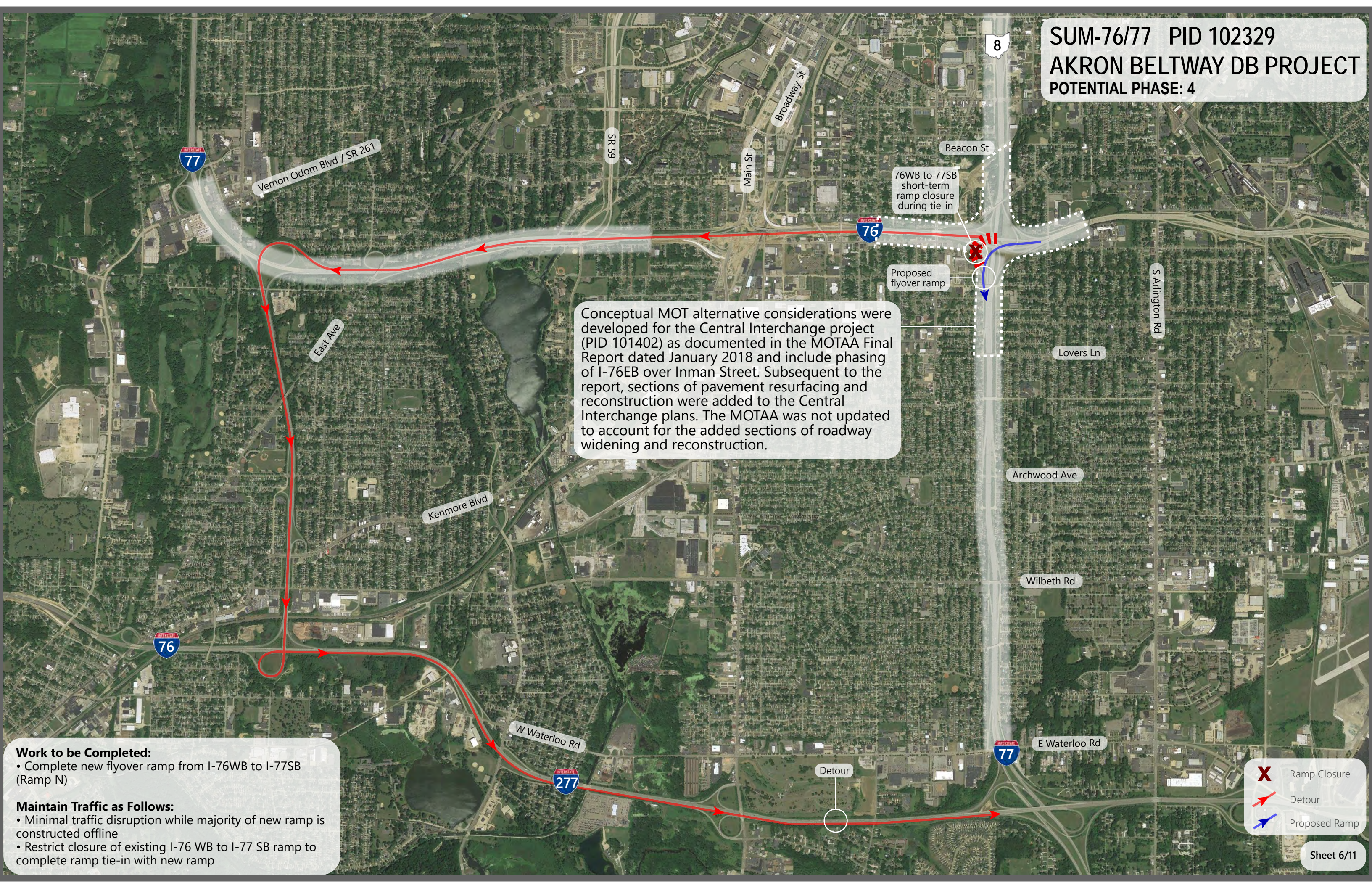


SUM-76/77 PID 102329
AKRON BELTWAY DB PROJECT
POTENTIAL PHASE: 4

Conceptual MOT alternative considerations were developed for the Central Interchange project (PID 101402) as documented in the MOTAA Final Report dated January 2018 and include phasing of I-76EB over Inman Street. Subsequent to the report, sections of pavement resurfacing and reconstruction were added to the Central Interchange plans. The MOTAA was not updated to account for the added sections of roadway widening and reconstruction.

- Work to be Completed:**
- Complete new flyover ramp from I-76WB to I-77SB (Ramp N)
- Maintain Traffic as Follows:**
- Minimal traffic disruption while majority of new ramp is constructed offline
 - Restrict closure of existing I-76 WB to I-77 SB ramp to complete ramp tie-in with new ramp

X Ramp Closure
 Detour
 Proposed Ramp



SUM-76/77 PID 102329
 AKRON BELTWAY DB PROJECT
 POTENTIAL PHASE: 5A



Work to be Completed:

- Construct interior half of I-76/I-77 EB mainline bridges in the eastbound direction over SR-93, Bowery Street/Ohio Canal, and Lakeshore Blvd
- Replace 1.5 miles of mainline pavement
- Replace I-76 EB pavement east of Central Interchange to Inman Street bridge

Maintain Traffic as Follows:

- Maintain two lanes along I-76/I-77 EB
- Close I-76 EB to I-76/I-77 EB ramp (at I-77) to reduce traffic volumes
- Close I-76 EB to I-77 SB ramp (at Central Interchange) to reduce traffic volumes
- Close I-76 EB to I-76 EB ramp (at I-277) to reduce traffic volumes
- Close East Avenue on-ramp to reduce traffic volumes

SUM-76/77 PID 102329
AKRON BELTWAY DB PROJECT
POTENTIAL PHASE: 5B



Temporarily restripe. Make lane three a decision lane, create a two-lane exit with a downstream inside merge over Morse Street, and reduce existing 76EB to 77NB two-lane ramp to a single-lane ramp.

Temporarily restripe 76WB to maintain one additional thru lane from Main St. to 76WB off-ramp.

Reduce to one lane

76EB to 76EB ramp closed

On-ramp

Conceptual 76 EB Part-Width Sequencing*

76EB to 77SB ramp closed

76EB to 76EB ramp closed

Conceptual 77 SB Part-Width Sequencing

PHASE X*

PHASE Y*

Wilbeth Rd On-ramp

77SB to 277WB ramp closed

77 Detour

- Work to be Completed:**
- Construct outside half of I-76/I-77 EB mainline bridges in the eastbound direction over SR-93, Bowery Street/Ohio Canal, and Lakeshore Boulevard
 - Replace 1.5 miles of mainline pavement
 - Replace pavement along I-76 EB from Allyn Street to I-77 SB
 - Replace 3 miles of pavement along I-77 SB from Beacon Street to I-277 and construct add lane
- Maintain Traffic as Follows:**
- Restripe/create four lanes along I-76 WB from Main Street to I-76/I-77 NW interchange
 - Maintain two lanes along I-76/I-77 EB
 - Close I-76 EB to I-76/I-77 EB ramp (at I-77) to reduce traffic volumes
 - Close I-76 EB to I-77 SB ramp (at Central Interchange)
 - Close I-76 EB to I-76 EB ramp (at I-277) to reduce traffic volumes
 - Close I-77 SB to I-277 WB ramp to reduce traffic volumes
 - Close East Avenue and Willbeth Road on-ramps to reduce traffic volumes

- Workzone
- Workzone Limits
- Ramp Closure
- Detour
- Ramp Open

*Part-width sequencing to be determined by Design-Build team

SUM-76/77 PID 102329
 AKRON BELTWAY DB PROJECT
 POTENTIAL PHASE: 6A



Conceptual 76 WB
 Part-Width Sequencing*

- Work to be Completed:**
- Construct interior half of I-76/I-77 WB mainline bridges in the westbound direction over SR-93, Bowery Street/Ohio Canal, and Lakeshore Blvd
 - Replace 1.5 miles of mainline pavement
- Maintain Traffic as Follows:**
- Maintain two lanes along I-76/I-77 WB
 - Close I-77 NB to I-76 WB ramp (at Central Interchange) to reduce traffic volumes
 - Close SR-8 SB to I-76 WB ramp to reduce traffic volumes

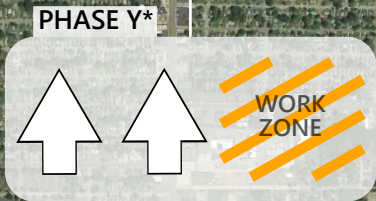
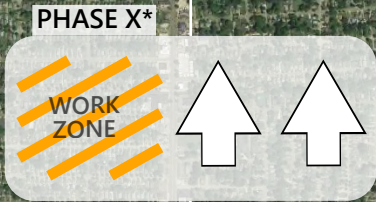
- Workzone
 - Workzone Limits
 - Ramp Closure
 - Detour
 - Ramp Open
- *Part-width sequencing to be determined by Design-Build team

SUM-76/77 PID 102329
AKRON BELTWAY DB PROJECT
POTENTIAL PHASE: 6B



Conceptual 76 WB Part-Width Sequencing*

Conceptual 77 NB Part-Width Sequencing

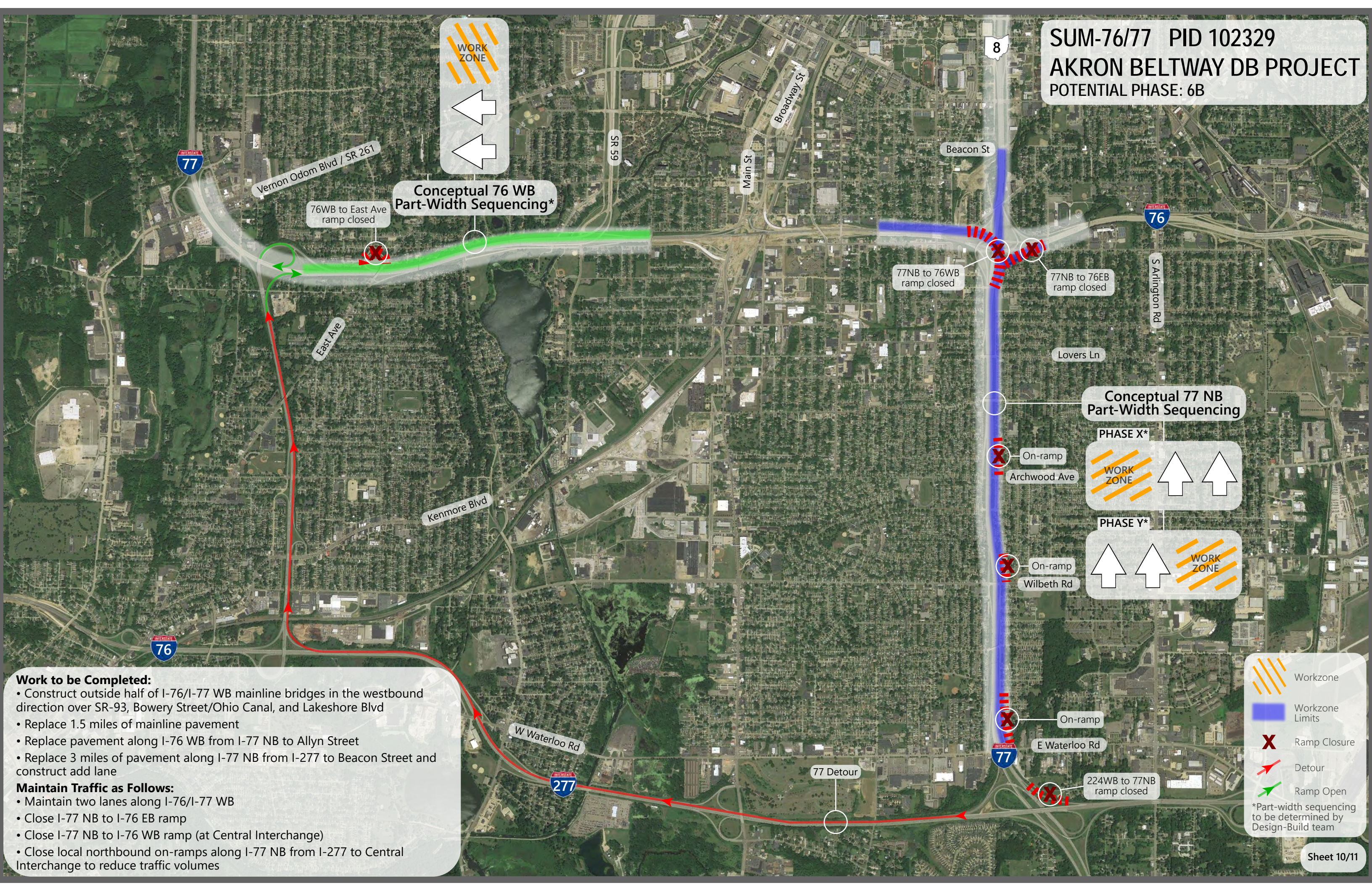


Legend:

- Workzone
- Workzone Limits
- Ramp Closure
- Detour
- Ramp Open

*Part-width sequencing to be determined by Design-Build team

- Work to be Completed:**
- Construct outside half of I-76/I-77 WB mainline bridges in the westbound direction over SR-93, Bowery Street/Ohio Canal, and Lakeshore Blvd
 - Replace 1.5 miles of mainline pavement
 - Replace pavement along I-76 WB from I-77 NB to Allyn Street
 - Replace 3 miles of pavement along I-77 NB from I-277 to Beacon Street and construct add lane
- Maintain Traffic as Follows:**
- Maintain two lanes along I-76/I-77 WB
 - Close I-77 NB to I-76 EB ramp
 - Close I-77 NB to I-76 WB ramp (at Central Interchange)
 - Close local northbound on-ramps along I-77 NB from I-277 to Central Interchange to reduce traffic volumes

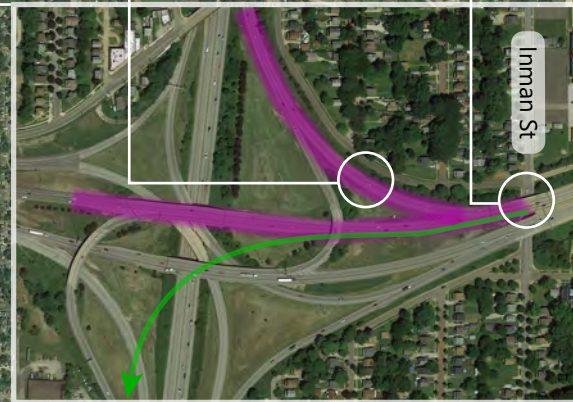


SUM-76/77 PID 102329
AKRON BELTWAY DB PROJECT
POTENTIAL PHASE: 7

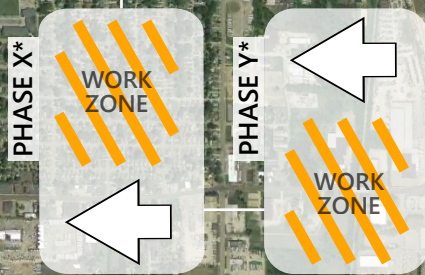
76WB to 8NB ramp traffic shall be maintained at all times via part-width construction.

For Inman Street phased construction details, see "SUM-76/77/8-10.99/11.54 /0.00 Plans"

Lane reduction permitted after new 76WB to 77SB flyover is open.



Conceptual 76 WB Part-Width Sequencing



- Workzone
- Workzone Limits
- Ramp Closure
- Detour
- Ramp Open

*Part-width sequencing to be determined by Design-Build team

- Work to be Completed:**
- Replace pavement along I-76 WB from Inman Street through Central Interchange
 - Inman Street bridge work
- Maintain Traffic as Follows:**
- I-76 WB lane reduction permissible after new I-76 WB to I-77 SB ramp (Ramp N) is opened to traffic