

LOCATION MAP
SEE SHEET 2

LOCATION MAP

LATITUDE: 39°25'52" N LONGITUDE: 84°17'03" W



PORTION TO BE IMPROVED
INTERSTATE HIGHWAY
FEDERAL ROUTES
STATE ROUTES
COUNTY & TOWNSHIP ROADS
OTHER ROADS

DESIGN DESIGNATION
SEE NOTE 1 ON SEE SHEET 2

DESIGN EXCEPTIONS

NONE

ADA DESIGN WAIVERS

NONE



PLAN PREPARED BY:

FISHBECK, THOMPSON, CARR & HUBER, INC.
10856 REED HARTMAN HIGHWAY, SUITE 175
CINCINNATI, OH 45242

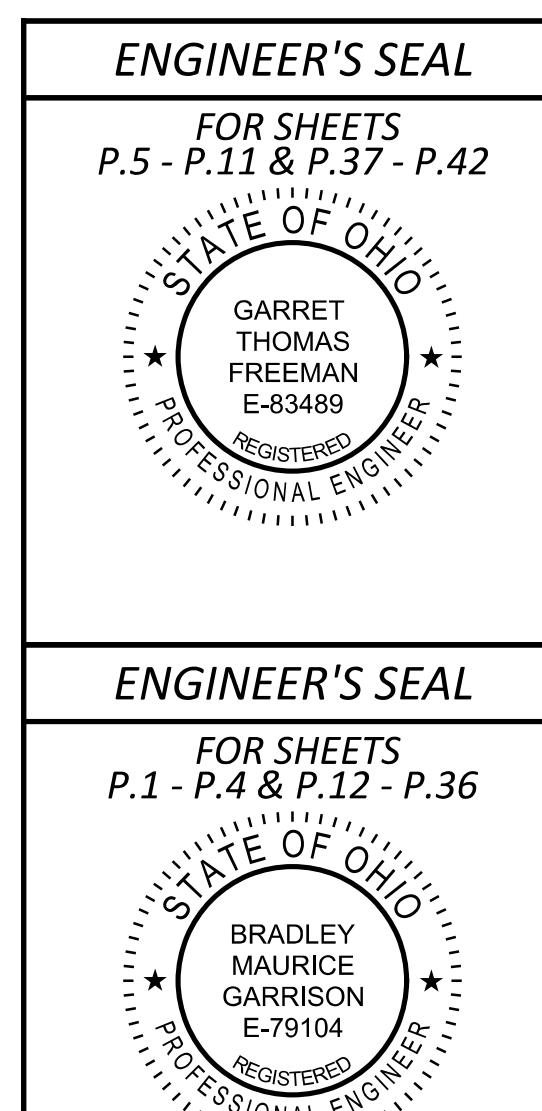


STATE OF OHIO
DEPARTMENT OF TRANSPORTATION
D08-BM-FY2026

CITY OF CINCINNATI
CITY OF MOUNT HEALTHY
CITY OF NORWOOD
COLERAIN TOWNSHIP
SYCAMORE TOWNSHIP
HAMILTON COUNTY
COLUMBIA TOWNSHIP
CITY OF NORTH COLLEGE HILL
CITY OF SHARONVILLE

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STANDARD CONSTRUCTION DRAWINGS

								SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
BP-2.4	7-19-13	RM-4.2	7-19-24	PCB-91	7-17-20			SS800 1-17-25	
BP-5.1	1-17-25			VPF-1-90	7-21-23			SS809 1-17-25	
								SS832 7-19-24	
								SS843 1-19-24	
MT-95.31	7-19-19							SS844 1-17-25	
MT-95.32	4-19-19							SS847 7-19-24	
MT-95.45	7-21-23							SS849 1-18-13	
MT-95.50	7-21-17								
MT-97.10	4-19-19								
MT-101.60	1-17-25								
MT-101.70	7-19-24								
MT-101.75	7-21-23								
MT-110.10	7-19-13								

FEDERAL PROJECT NUMBER

E250 (025)

RAILROAD INVOLVEMENT

IORY TRACKS WITHIN CSX RIGHT-OF-WAY

PROJECT DESCRIPTION

BRIDGE MAINTENANCE PROJECT INCLUDING VANDAL PROTECTION, FENCE REPLACEMENT AND REPAIR, BRIDGE RAILING REPAIR, PEDESTRIAN RAILING PAINTING, CONCRETE OVERLAY WITH SCARIFICATION, CONCRETE PATCHING AND REPAIR, SEALING OF CONCRETE BRIDGE DECKS, SEALING OF CONCRETE SURFACES, AND BEAM COLLISION IMPACT REPAIR.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: 0.0 ACRES

ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 0.0 ACRES

NOTICE OF INTENT EARTH DISTURBED AREA: N/A (NOI NOT REQUIRED)

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

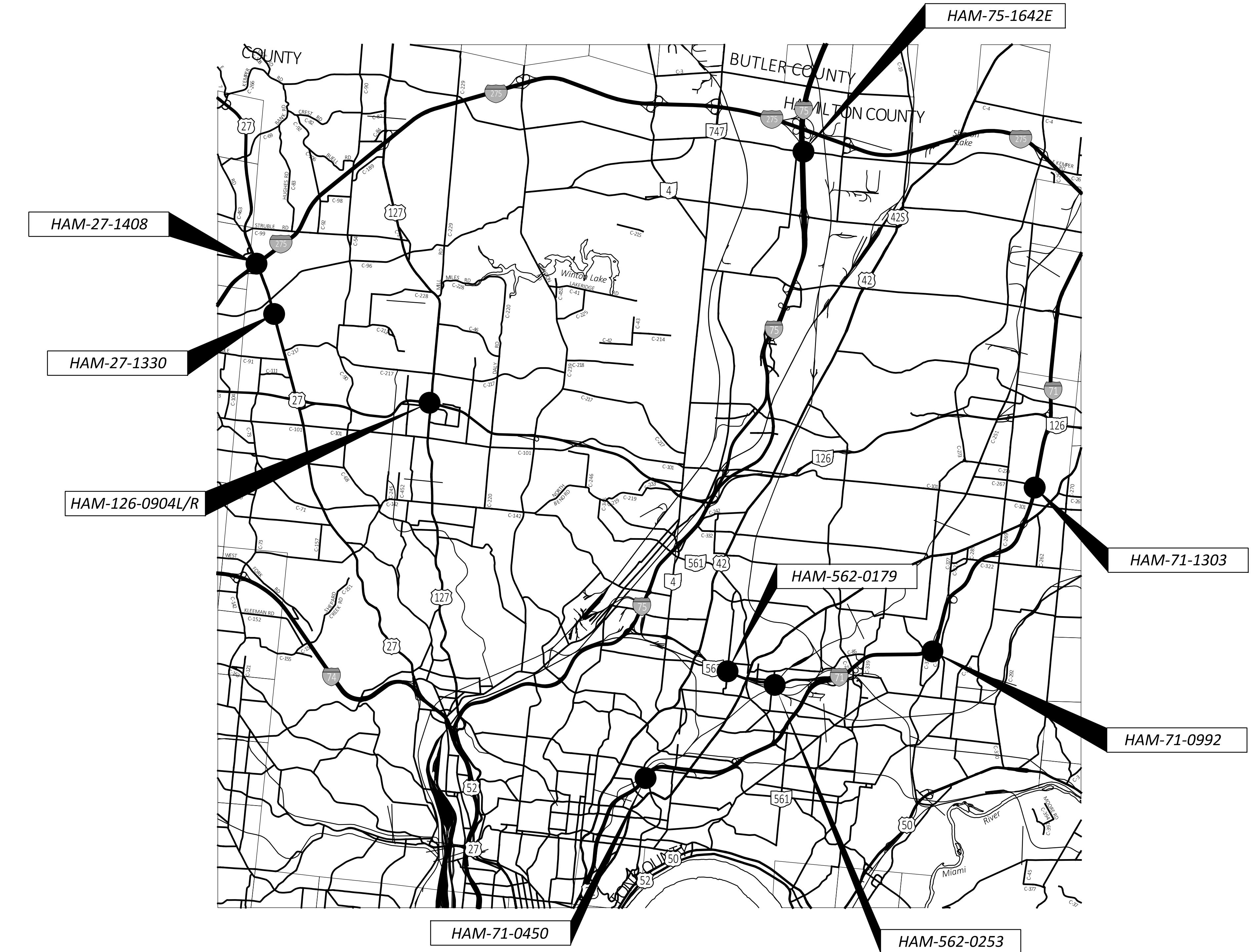
I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL REQUIRE THE CLOSING TO TRAFFIC OF THE ROADWAY AND THAT DETOURS WILL BE PROVIDED AS INDICATED ON SHEETS P.8 THRU P.11.

Douglas A. Gruber, P.E.
District 08 Deputy Director

Pamela Boratyn
Director, Department of Transportation

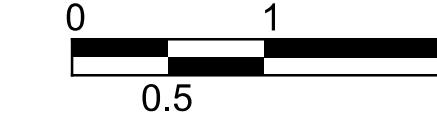
TITLE SHEET

DESIGN AGENCY	
DESIGNER	
JPC	
REVIEWER	
BMV 02/04/25	
PROJECT ID	
113006	
SHEET TOTAL	
P.01	42



LOCATION MAP

HORIZONTAL SCALE IN MILES



NOTES:

1. SEE SITE PLANS FOR DESIGN DESIGNATIONS AND TRAFFIC DATA.

LOCATION MAP

fishbeck

DESIGNER
JPCREVIEWER
BMV 02/04/25PROJECT ID
113006SHEET TOTAL
P.02 42

UTILITIES

LISTED BELOW ARE UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS. THE LOCATION OF THE UTILITIES SHOWN ON THESE PLANS ARE AS OBTAINED FROM THE INFORMATION PROVIDED FROM EACH OWNER THROUGH THE REQUEST OF PLAN DRAWINGS. FIELD MARKINGS ARE NOT INCORPORATED INTO THESE PLANS.

HAM-27-1330

ALTAFIBER
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GREATER CINCINNATI WATER WORKS
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ZAYO BANDWIDTH
WAYLON HIGGINS (765) 341-1199
9209 CASTLEGATE DR.
INDIANAPOLIS, IN 46256
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CITY OF MT HEALTHY STORM SEWER
JUSTIN WESTRICH (513) 930-0194
JWESTRICH@MTHEALTHY.ORG
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7700 PERRY STREET
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HAM-71-0992
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UTILITIES (CONTINUED)**HAM-71-0992 (CONTINUED)**

MCI/VERIZON
STEPHEN HOWELL (513) 839-3486
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CINCINNATI, OH 45249
STEPHEN.HOWELL@VERIZON.COM

ODOT D8 TRAFFIC
JIM JUDD (513) 933-6692
505 SOUTH SR741
LEBANON, OH 45036
JIM.JUDD@DOT.OHIO.GOV

ITS (FORMERLY ARTIMIS)
ODOT CENTRAL OFFICE OF TRAFFIC ENGINEERING
JASON YERAY (614) 466-2168
ITS LOCATE LINE: (614) 387-4113
1980 WEST BROAD STREET
COLUMBUS, OH 43223

HAM-75-1642E

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10920 KENWOOD ROAD
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CROWN CASTLE FIBER
CRAIG SNELL (513) 898-1595
10188 INTERNATIONAL BOULEVARD
CINCINNATI, OH 45246
CRAIG.SNELL@CROWNCastle.COM

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

CITY OF CINCINNATI NOTES

CONSTRUCTION NOTIFICATION:
TEN (10) BUSINESS DAYS PRIOR TO THE PRE-CONSTRUCTION MEETING THE PROJECT ENGINEER WILL NOTIFY THE CITY OF CINCINNATI LPA COORDINATOR OF THE PRECONSTRUCTION MEETING'S DATE, TIME, AND LOCATION. CONTACT CITY OF CINCINNATI, DEPARTMENT OF TRANSPORTATION AND ENGINEERING, DIRECTOR'S OFFICE, AT (513) 352-2366, ATTENTION: CHRIS KELLY AT (513) 352-3721 OR BY EMAIL AT CHRIS.KELLY@CINCINNATI-OH.GOV.

PERMITS:

A CITY OF CINCINNATI DEPARTMENT OF TRANSPORTATION AND ENGINEERING (DOTE) PERMIT IS REQUIRED PRIOR TO THE ODOT CONTRACTOR COMMENCING WORK IN THE CITY OF CINCINNATI'S PUBLIC RIGHT-OF-WAY. PERMIT APPLICATIONS FOR STREET USE, STREET BARRICADE, STREET OPENING, ETC. MAY BE MADE AT ROOM 425, CITY HALL, 801 PLUM STREET, CINCINNATI, OHIO 45202. CITY ISSUED PERMITS MAY REQUIRE MAJOR EVENT WORK RESTRICTIONS ON THE CONTRACTOR'S ACTIVITIES. THE CITY MAINTAINS A LIST OF KNOWN MAJOR EVENTS AT THE FOLLOWING WEBSITE:
[HTTP://CINCINNATI-OH.GOV/POLICE/SPECIAL-EVENTS-REGULATIONS-AUCTIONS/EVENTPERMITS/](http://CINCINNATI-OH.GOV/POLICE/SPECIAL-EVENTS-REGULATIONS-AUCTIONS/EVENTPERMITS/).

THE CITY OF CINCINNATI RESTRICTS NIGHTTIME CONSTRUCTION WORK BETWEEN THE HOURS OF 11:00 P.M. AND 7:00 A.M. CITY ISSUED PERMITS WILL REQUIRE THE CONTRACTOR TO SECURE THE CITY ENGINEER'S APPROVAL FOR NIGHTTIME WORK.

DEMOLITION DEBRIS

THE CONTRACTOR SHALL TAKE PRECAUTIONS TO AVOID AND/OR LIMIT DEMOLITION DEBRIS FROM ENTERING WATERWAYS OR FALLING ONTO TRAFFIC LANES. ANY MATERIAL THAT DOES FALL INTO A WATERWAY OR ONTO TRAFFIC LANES SHALL BE IMMEDIATELY REMOVED AT THE CONTRACTOR'S EXPENSE. DAMAGE TO PROPERTY AS A RESULT OF FALLING DEMOLITION DEBRIS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

WHILE SEALING ANY PORTION OF THE BRIDGE STRUCTURES, AN APPROPRIATE APRON WILL BE UTILIZED TO PREVENT DEBRIS, OVER SPRAY, AND SEALANTS FROM ENTERING THE WATERWAYS OR AFFECTING VEHICULAR/PEDESTRIAN TRAFFIC AND/OR PROTECTED AREAS.

CONSTRUCTION NOISE

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. THIS PROJECT WILL COMPLY WITH ALL LOCAL NOISE ORDINANCES.

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

FOR OPERATIONS WITHOUT POSITIVE PROTECTION OCCURRING WITHIN 10 FEET OF AN OPEN TRAVELED LANE THAT MEET ALL OF THE FOLLOWING CRITERIA:
ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND
AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION;
AND,
AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

"WITHOUT POSITIVE PROTECTION" MEANS USE OF DRUMS, CONES, SHADOW VEHICLE, ETC, WITHOUT PROTECTION FROM PORTABLE BARRIER OR OTHER RIGID BARRIER ALONG THE WORK AREA. THIS PHRASE DOES NOT APPLY TO CASES WHERE POSITIVE PROTECTION IS REQUIRED. MOBILE OPERATIONS ARE REGARDED AS "WITHOUT POSITIVE PROTECTION". FOR WORK ZONES USING A COMBINATION OF BARRIER AND TEMPORARY TRAFFIC CONTROL DEVICES (CONES, DRUMS, ETC), THE DESIGNATION SHALL BE BASED UPON THE TYPE OF DEVICES USED IN THE AREA THAT WORKERS ARE LOCATED.

IF MULTIPLE ACTIVE LOCALIZED QUALIFYING WORK AREAS OCCUR WITHOUT POSITIVE PROTECTION, PER MAINLINE TRAFFIC DIRECTION, PROVIDE A UNIFORMED LEO AND OFFICIAL PATROL CAR IN ADVANCE OF:
THE FIRST ACTIVE WORK AREA THAT DRIVERS WILL ENCOUNTER; OR THE ACTIVE WORK AREA LATERALLY CLOSEST TO THE OPEN TRAVELED LANE; OR OTHER LOCATION AS APPROVED BY THE ENGINEER.

THE UNIFORMED LEO AND OFFICIAL PATROL CAR MAY RELOCATE AMONG THE LISTED LOCATIONS AS APPROPRIATE AS THE OPERATIONS PROCEED IN THE LOCALIZED QUALIFYING WORK AREAS.

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONTINUED)

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 800 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN (CONTINUED)

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.) THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN (CONTINUED)

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 2 SIGN MONTHS
ASSUMING 2 PCMS SIGN(S) FOR 1 MONTH(S) AT HAM-27-1408

ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS (BIDIRECTIONAL)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

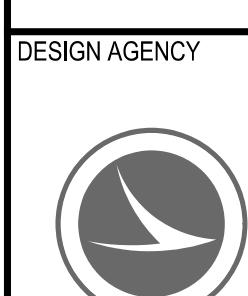
PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

DELINEATION OF PORTABLE AND PERMANENT BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL; AND, ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70. WHEN THE PB CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE ABOVE ITEMS.



DESIGNER

GTF

REVIEWER

SRK

6-05-25

PROJECT ID

113006

SHEET

TOTAL

P.6

42

SEQUENCE OF CONSTRUCTION**HAM-71-13.03:**

CLOSE KUGLER MILL RD. AT THE BRIDGE LOCATION. MAINTAIN ACCESS TO THE ADJACENT DRIVES AT ALL TIMES. DETOUR TRAFFIC AS SHOWN ON SHEET 10. COMPLETE CRITICAL WORK ACCORDING TO THE WINDOW CONTRACT TABLE.

HAM-27-14.08:

CLOSE THE RIGHT 2 LANES OF NORTHBOUND US-27 AS SHOWN ON SHEETS 8-9 AND PROVIDE EXTRA ADVANCED WARNING SIGNS PER MT-95.50. COMPLETE CRITICAL WORK ACCORDING TO THE WINDOW CONTRACT TABLE. MAINTAIN A 10' MIN. THRU LANE BY

CLOSE THE RIGHT 2 LANES OF SOUTHBOUND US-27 AS SHOWN ON SHEETS 8-9 AND PROVIDE EXTRA ADVANCED WARNING SIGNS PER MT-95.50. COMPLETE CRITICAL WORK ACCORDING TO THE WINDOW CONTRACT TABLE. MAINTAIN A 10' MIN. THRU LANE.

HAM-27-13.30:

MAINTAIN ALL LANES ON US-27, EXCEPT SHORT-TERM LANE CLOSURES ARE PERMITTED IN ACCORDANCE WITH THE LVCT. CLOSE BRIDGE TO PEDESTRIAN TRAFFIC AS NECESSARY TO COMPLETE WORK AND DURING CURING PERIODS. DETOUR PEDESTRIAN TRAFFIC TO THE CROSS WALK 0.05 MILES NORTH OF BRIDGE No.: HAM-27-13.30 PER MT-110.10.

HAM-126-09.04 L/R:

MAINTAIN ALL LANES ON SR-126, EXCEPT SHORT-TERM LANE CLOSURES ARE PERMITTED IN ACCORDANCE WITH THE LVCT. CLOSE THE SHOULDER PER MT-95.45 DURING THE REMOVAL AND REPAIR OF THE CONCRETE BRIDGE RAILING. LANE CLOSURES ON US-127 ARE PERMITTED IN ACCORDANCE WITH THE LVCT.

HAM-562-01.79:

MAINTAIN ALL LANES ON SR-562, EXCEPT SHORT-TERM LANE CLOSURES ARE PERMITTED IN ACCORDANCE WITH THE LVCT.

HAM-562-02.53:

MAINTAIN A MINIMUM OF ONE LANE OF TWO-WAY TRAFFIC USING FLAGGERS DURING WORKING HOURS. CLOSE THE SIDEWALK ON THE BRIDGE DURING WORKING HOURS AND DURING CURING PERIODS. DETOUR PEDESTRIAN TRAFFIC FROM HARRIS AVE. TO BEECH ST. TO NORWOOD AVE. PER MT-110.10.

HAM-71-04.50:

MAINTAIN ALL LANES ON IR-71, EXCEPT SHORT-TERM LANE CLOSURES ARE PERMITTED IN ACCORDANCE WITH THE LVCT.

HAM-71-09.92:

MAINTAIN ALL LANES ON IR-71, EXCEPT SHORT-TERM LANE CLOSURES ARE PERMITTED IN ACCORDANCE WITH THE LVCT.

HAM-75-16.52:

MAINTAIN ALL LANES ON IR-75. MAINTAIN ALL LANES OF TRAFFIC ON KEMPER ROAD EXCEPT SHORT-TERM LANE CLOSURES ARE PERMITTED IN ACCORDANCE WITH THE LVCT.

APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS

PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE APPROVED MOT EXCEPTIONS PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

APPROVED MOT EXCEPTIONS INCLUDE:

- THE CONTRACTOR IS PERMITTED TO CLOSE TWO LANES IN EACH DIRECTION AT BRIDGE No.: HAM-27-1408 THAT CARRIES US-27 OVER I-275 TO PERFORM CONCRETE OVERLAY ACCORDING TO THE WINDOW CONTRACT TABLE.

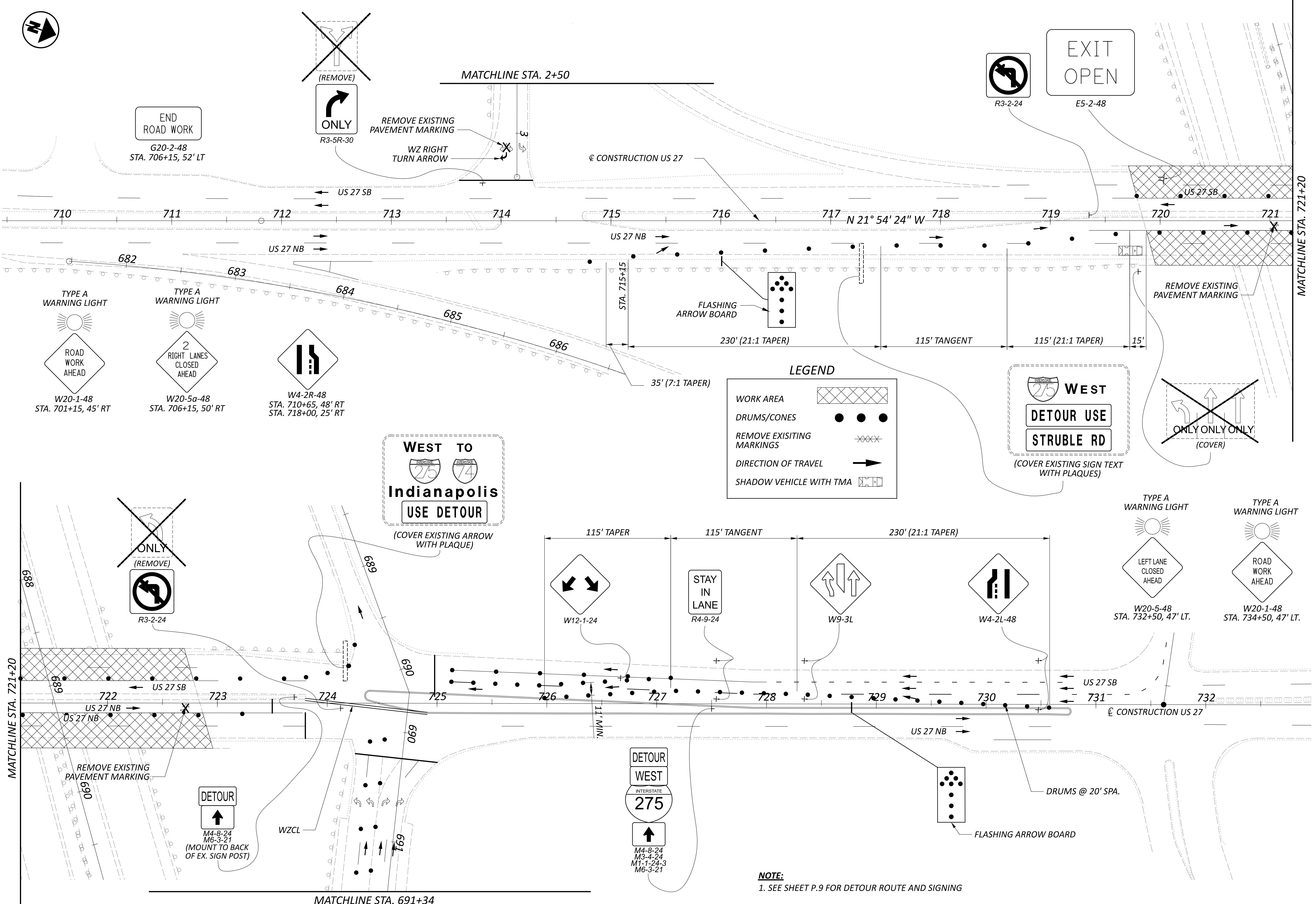
2 PCMS SHALL BE USED TO PROVIDE NOTICE OF CLOSURE AND DETOUR INFORMATION BEFORE AND DURING THE CLOSURE.

A MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF 30 CALENDAR DAYS PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER & HAMILTON COUNTY ADMINISTRATOR, AS WELL AS THE CONTRACTOR, AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.

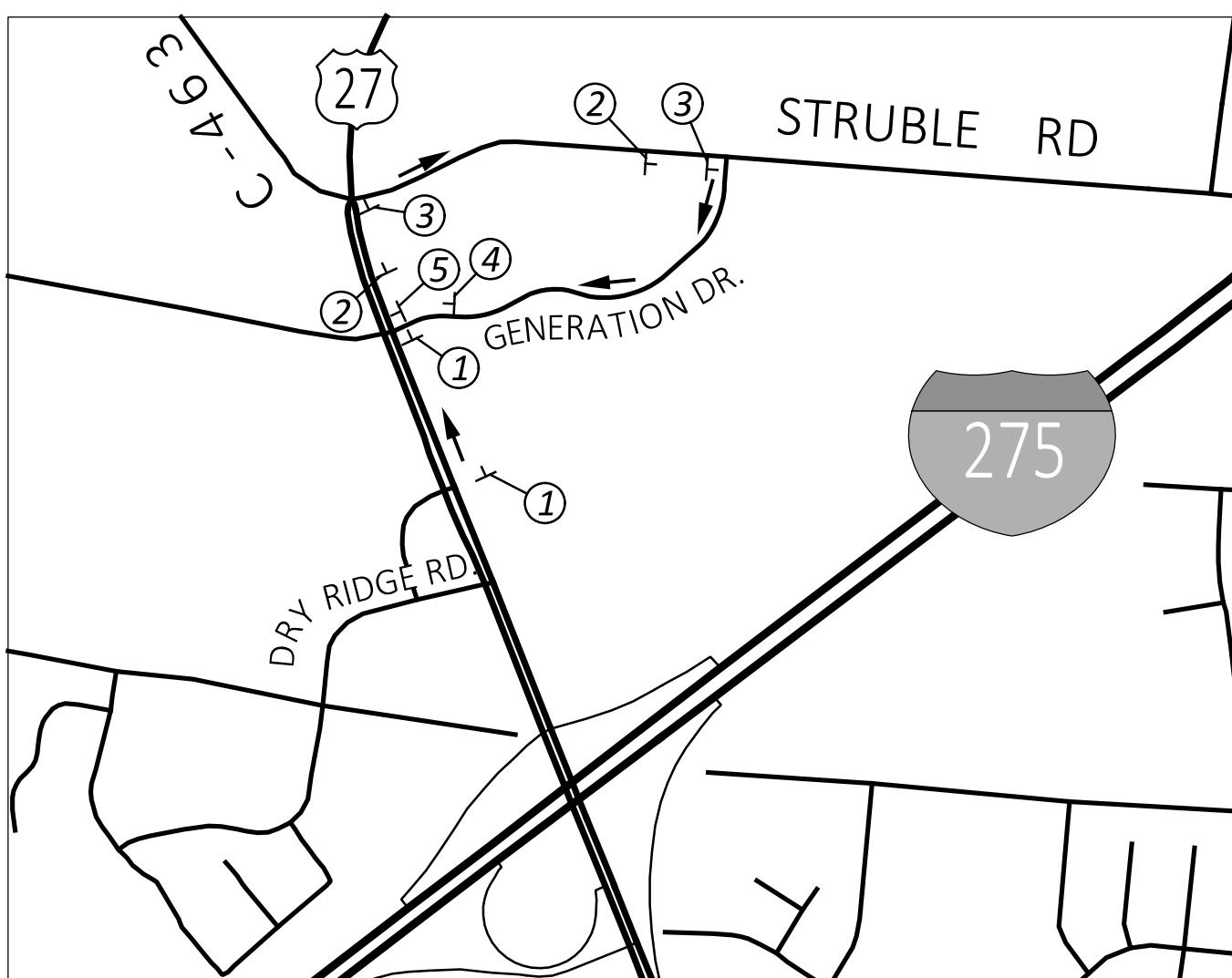
IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTIONS REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO THE OFFICE OF ROADWAY ENGINEERING, STATEWIDE TMC, DWZTM AND SPECIAL HAULING PERMITS AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE. REFERENCE "EXCEPTION REQUEST APPROVAL DATED 05/11/2023 FOR PID 113006" IN THE NOTIFICATION AND OTHER CORRESPONDENCE.

ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTIONS LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE MOT EXCEPTION COMMITTEE (MOTEC). IN THE EVENT THAT SUCH CHANGES ARE PROPOSED, THE REQUEST SHALL BE COORDINATED THROUGH THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM) A MINIMUM OF 30 CALENDAR DAYS PRIOR TO THE DESIRED IMPLEMENTATION DATE. IF THE DISTRICT AGREES WITH THE PROPOSED CHANGES THE DWZTM SHALL SEEK APPROVAL FROM THE MOTEC. IN THE EVENT THE PROPOSED CHANGES ARE APPROVED IN WRITING, THE CLOSURES ARE STILL SUBJECT TO NOTIFICATION REQUIREMENTS WITHIN THIS NOTE PRIOR TO IMPLEMENTATION.

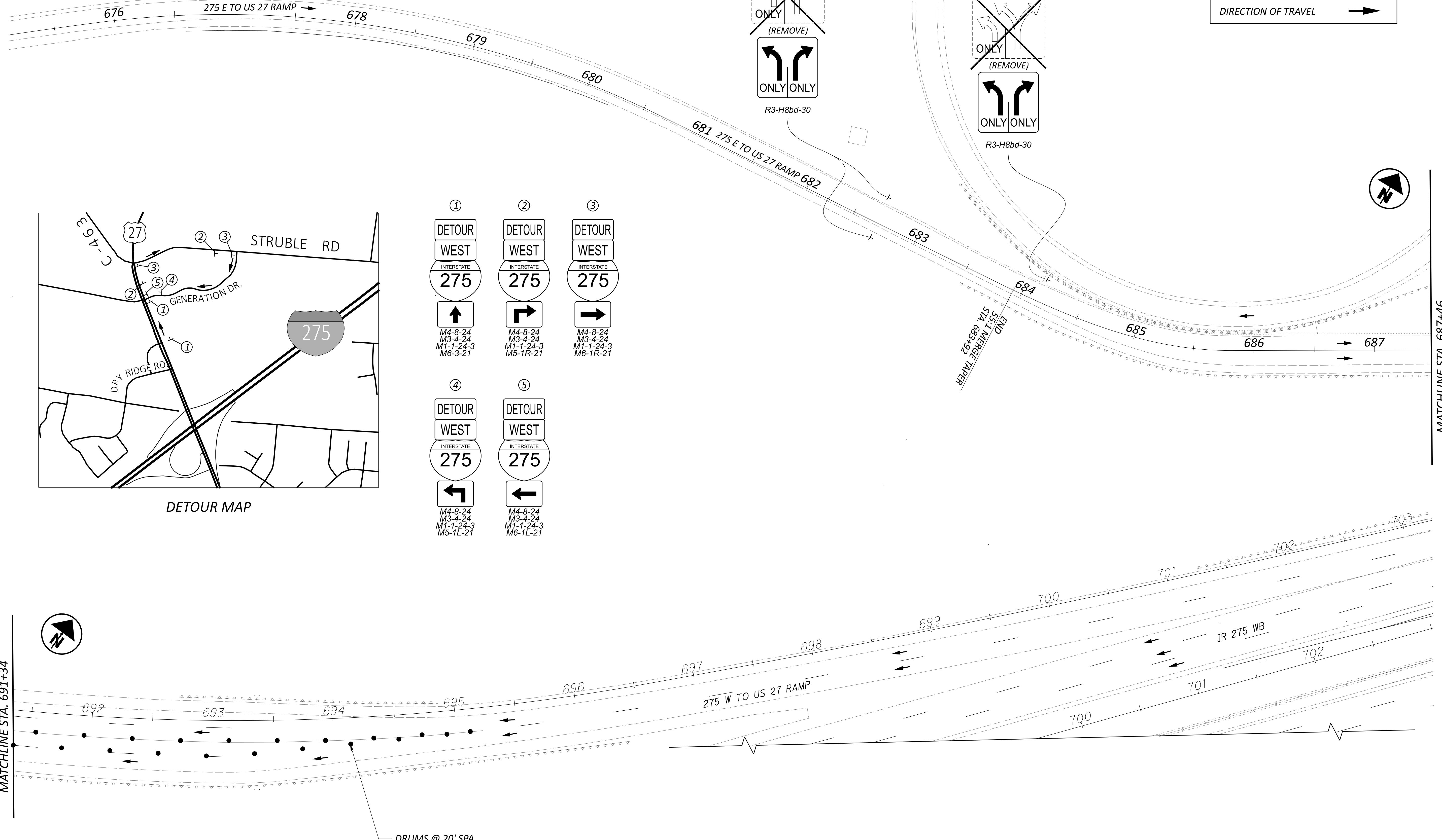
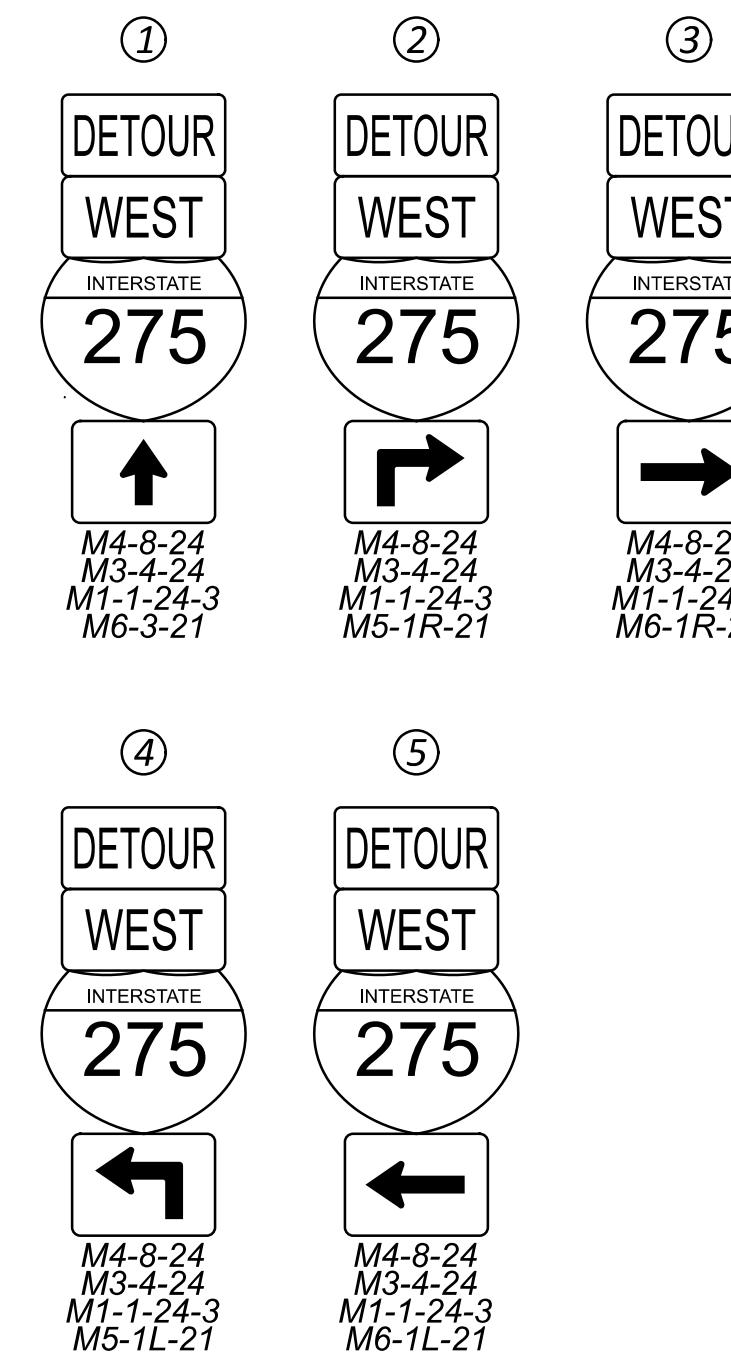
DESIGNER
GTFREVIEWER
SRK 6-05-25PROJECT ID
113006SHEET TOTAL
P.7 42



MATCHLINE STA. 691+34



DETOUR MAP

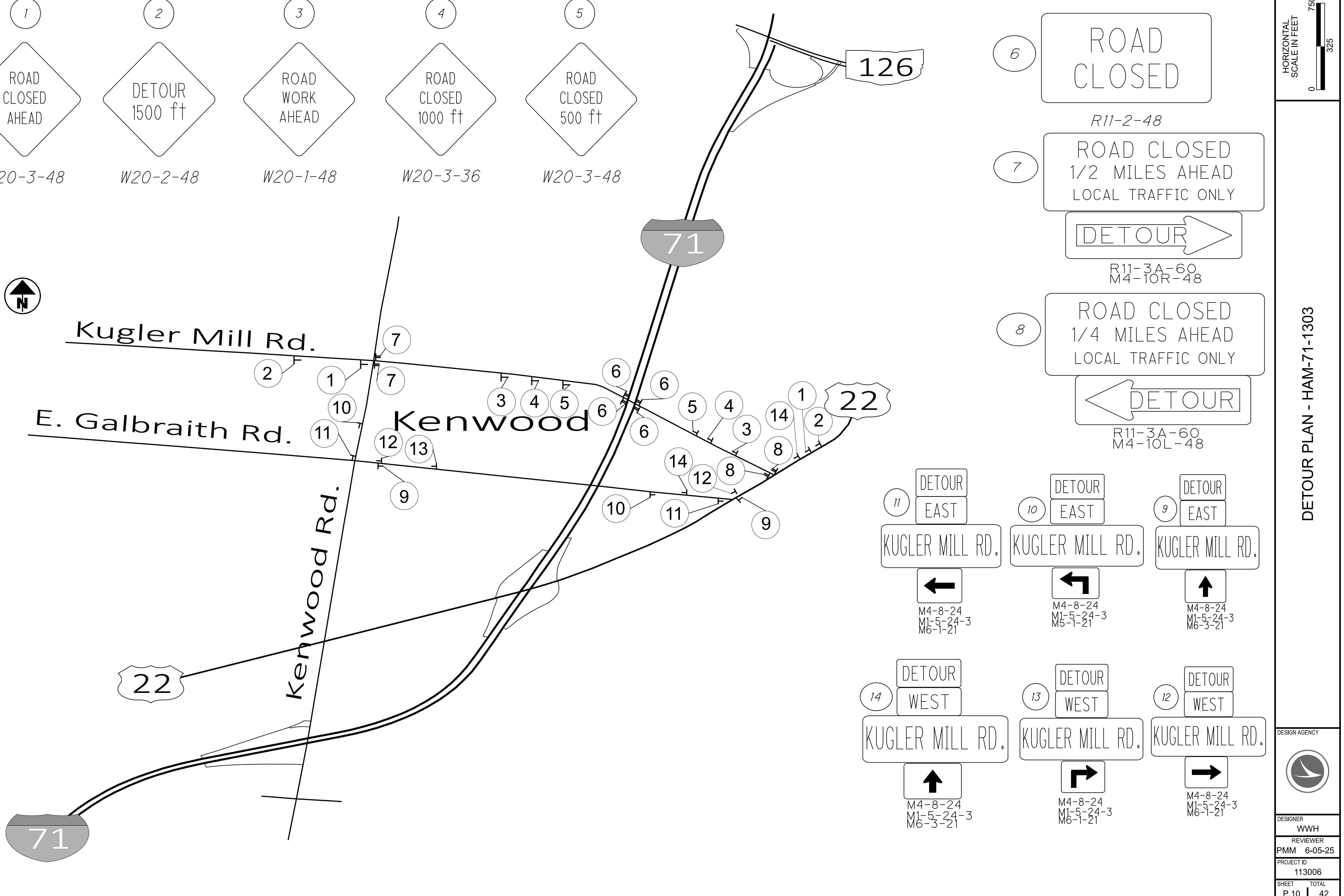


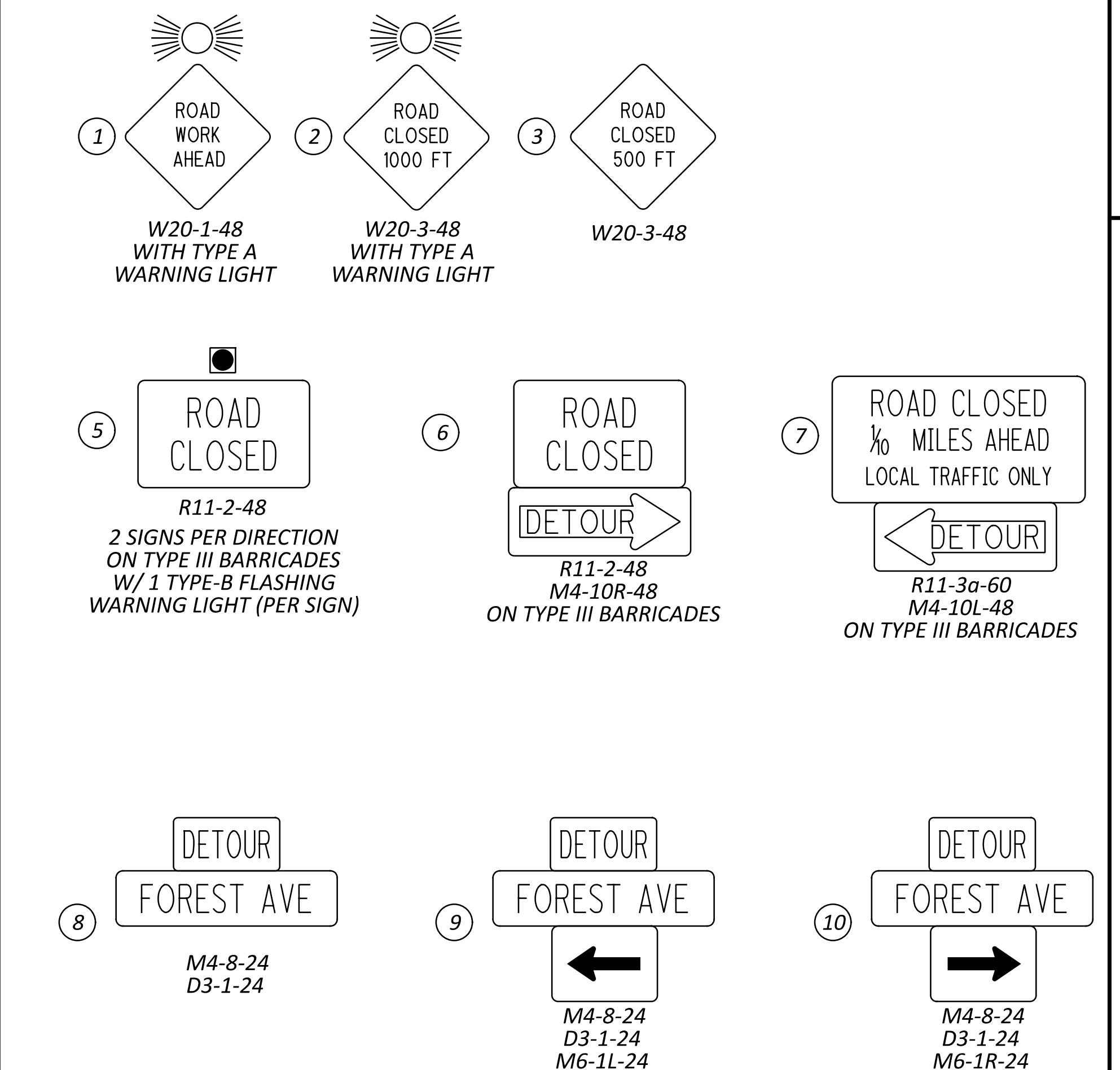
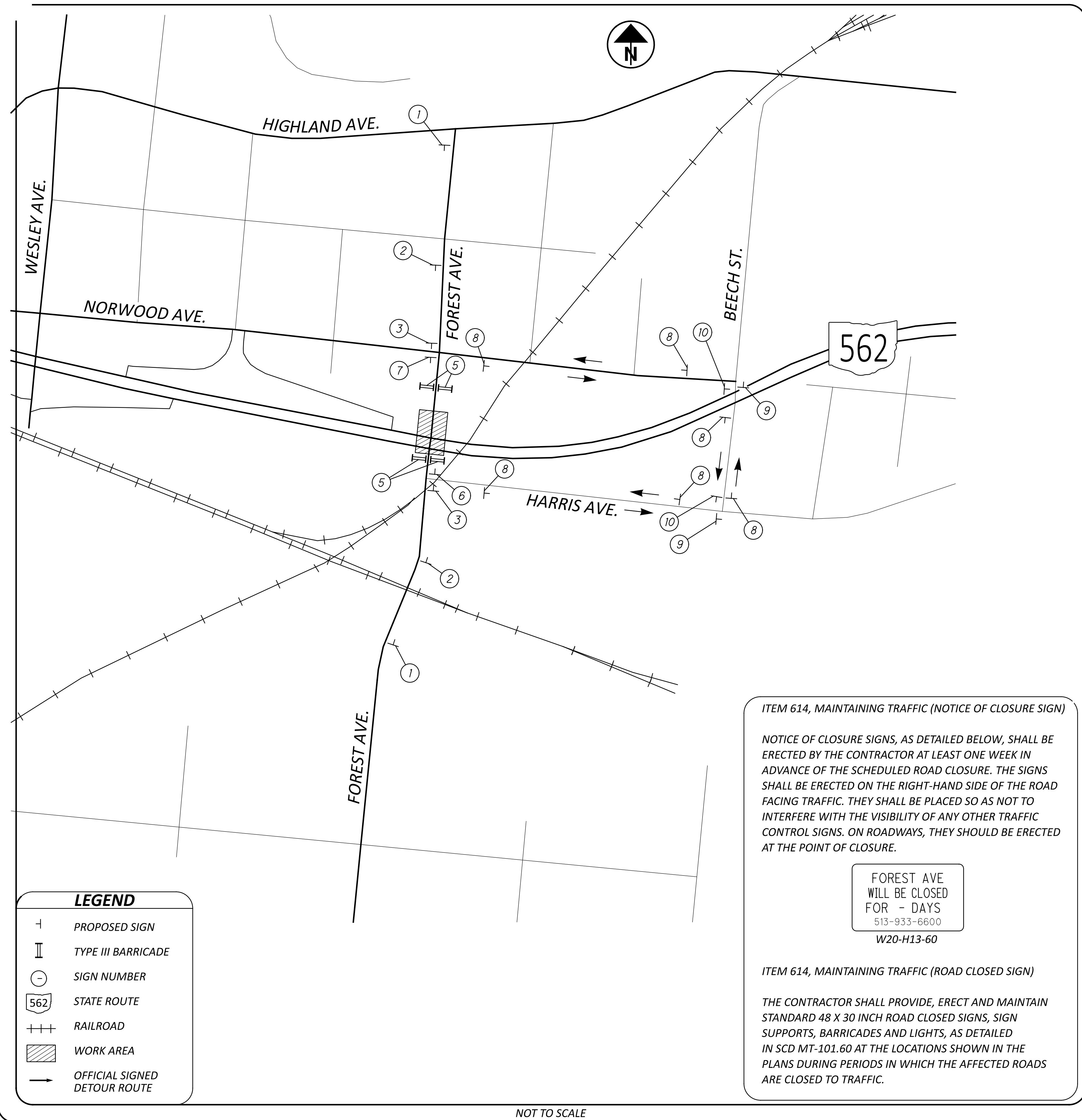
MAINTENANCE OF TRAFFIC PLAN
BRIDGE No.: HAM-27-1408



DESIGNER: GTF
REVIEWER: SRK 6-05-25
PROJECT ID: 113006
SHEET TOTAL: P.9 42

HORIZONTAL SCALE IN FEET
0 20 40 60 80





SHEET NUM.	PART.		ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
	01/IMS	02/NHS						
P.13							STRUCTURES OVER 20 FOOT SPAN	
							FOR HAM-00071-13.030 ESTIMATED QUANTITIES	P.16
							FOR HAM-00027-14.080 ESTIMATED QUANTITIES	P.16
							FOR HAM-00027-13.300 ESTIMATED QUANTITIES	P.16
							FOR HAM-00126-09.040L/R ESTIMATED QUANTITIES	P.16
							FOR HAM-00562-01.790 ESTIMATED QUANTITIES	P.17
							FOR HAM-00562-02.530 ESTIMATED QUANTITIES	P.17
							FOR HAM-00071-04.500 ESTIMATED QUANTITIES	P.17
							FOR HAM-00071-09.920 ESTIMATED QUANTITIES	P.17
							FOR HAM-00075-16.420E ESTIMATED QUANTITIES	P.17
							MAINTENANCE OF TRAFFIC	
4	400	400	614	11110	800	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	P.6
26		4	614	12384	4	EACH	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (BIDIRECTIONAL)	
26		26	614	13310	26	EACH	BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL)	
2		26	614	13360	26	EACH	OBJECT MARKER, TWO WAY	
LS		2	614	18601	2	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	P.6
1,120		LS	614	12420	LS		DETOUR SIGNING	
		1,120	622	41100	1,120	FT	PORTABLE BARRIER, UNANCHORED	
							INCIDENTALS	
		LS	LS	108	10000	LS	CPM PROGRESS SCHEDULE	
		LS	LS	614	11000	LS	MAINTAINING TRAFFIC	
		LS	LS	623	10000	LS	CONSTRUCTION LAYOUT STAKES AND SURVEYING	
		LS	LS	624	10000	LS	MOBILIZATION	

BRIDGE NO.	PHASE NO.	SHEET NO.	LENGTH (FEET)	614		614		614		614		622			
				WORK ZONE IMPACT ATTENUATOR, 24' WIDE HAZARDS (BI-DIR.)	EACH	BARRIER REFLECTOR, TYPE 1 (BI-DIR.)	EACH	EACH	SNMT	LS	FT	PORTABLE BARRIER, UNANCHORED			
HAM-71-1303	-	P.05										LUMP			
HAM-27-1330	-	P.05										LUMP			
HAM-27-1408	-	P.06-P.07								2		LUMP			
HAM-126-0904L	1	P.06	300		1	7	7					300			
HAM-126-0904L	2	P.06	260		1	6	6					260			
HAM-126-0904R	1	P.06	300		1	7	7					300			
HAM-126-0904R	2	P.06	260		1	6	6					260			
HAM-562-0253	-	P.05										LUMP			
TOTALS CARRIED TO GENERAL SUMMARY					4	26	26	2	LUMP		1120				

MAINTENANCE OF TRAFFIC SUBSUMMARY

DESIGN AGENCY
fishbeck

DESIGNER	BMG
REVIEWER	BMV 02/04/25
PROJECT ID	113006
SHEET	TOTAL
P.13	42

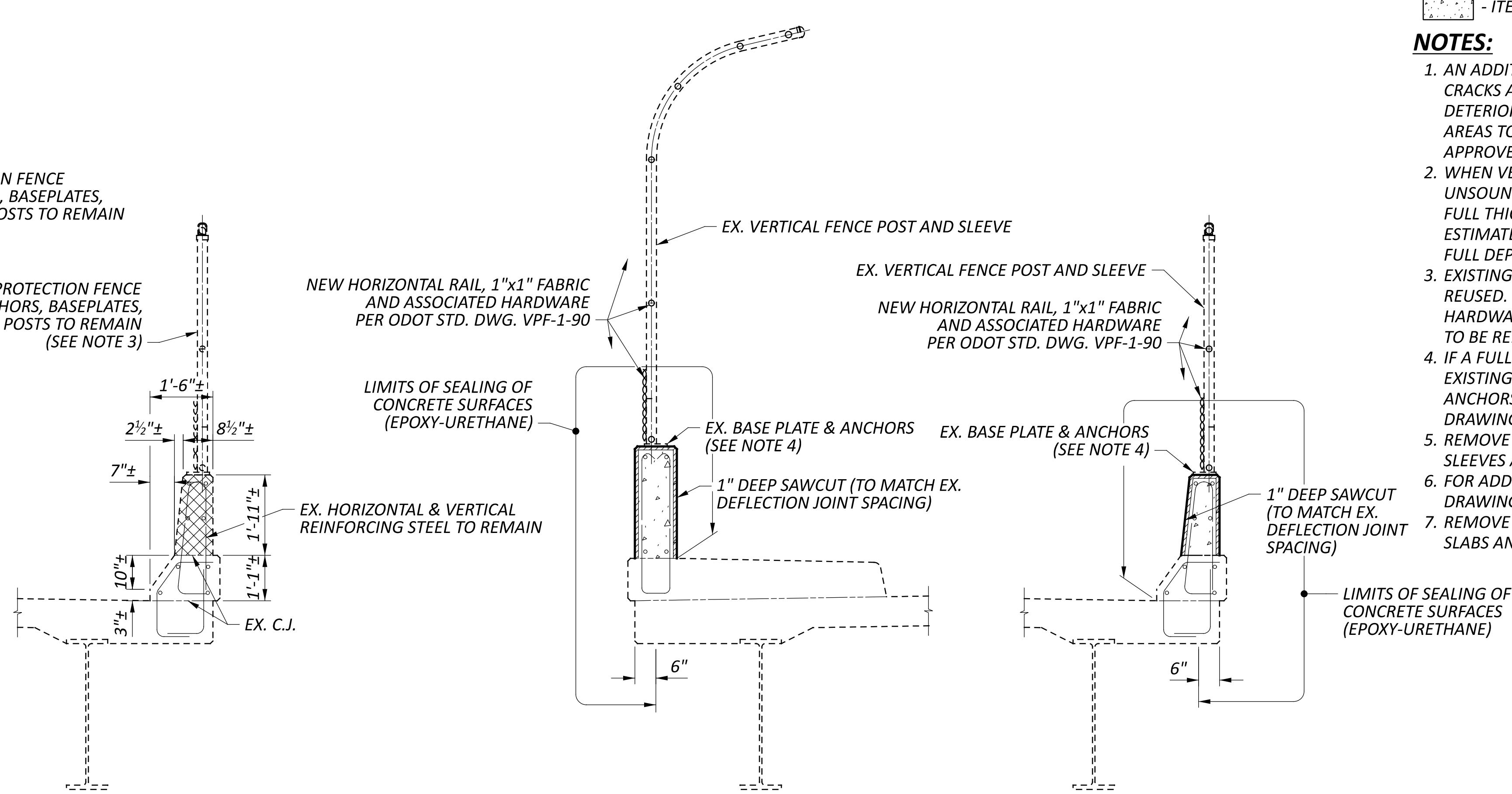
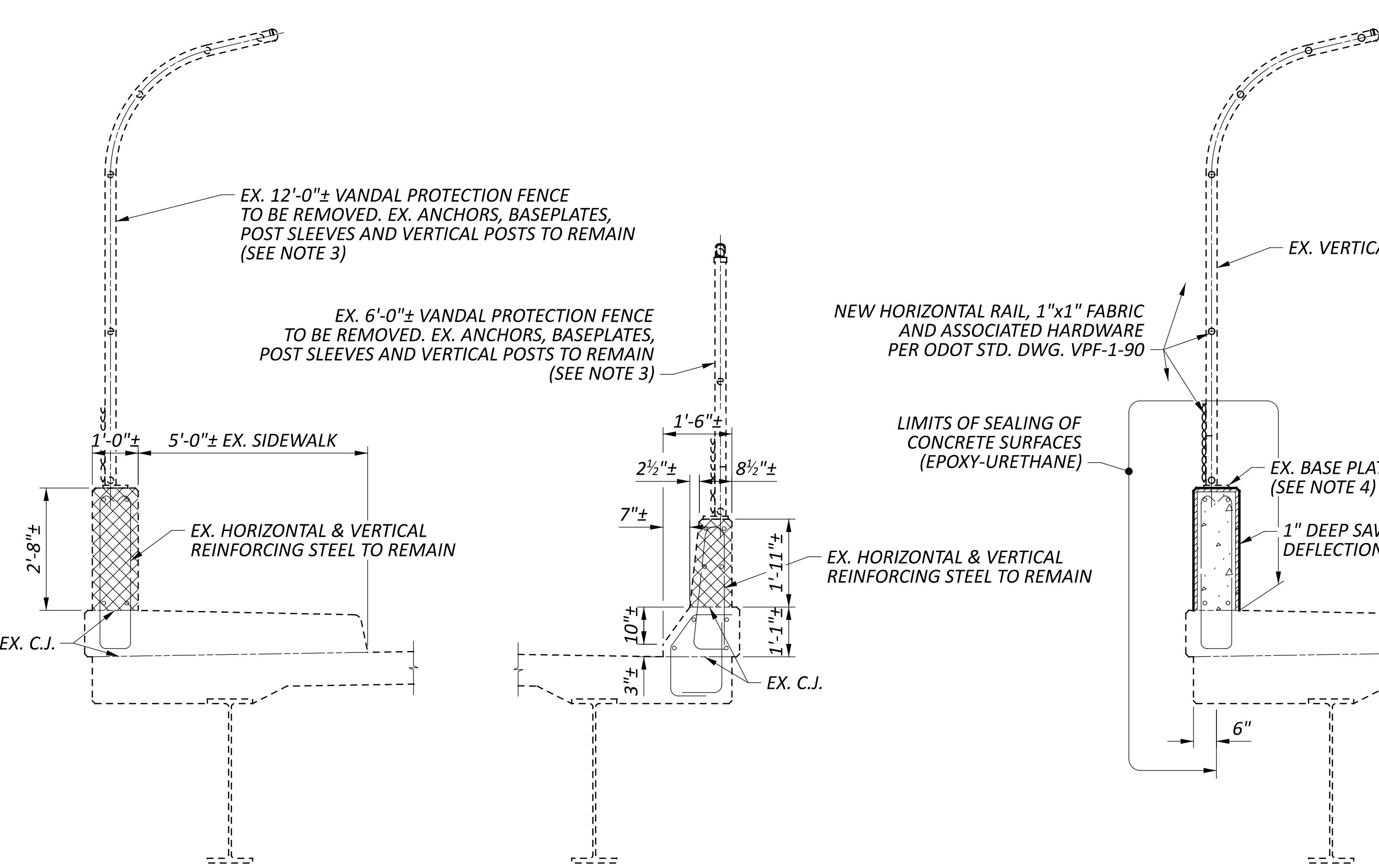
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ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION					ABUT.	PIER	SUPER.	GEN.	REFERENCE SHEET NO.	
202	11201	LUMP		PORTIONS OF STRUCTURE REMOVED, AS PER PLAN								LUMP	P.14/42	
509	20001	68	LB	CONCRETE REINFORCEMENT, REPLACEMENT OF EXISTING CONCRETE REINFORCEMENT, AS PER PLAN							68		P.14/42	
510	10001	38	EACH	DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT, AS PER PLAN							38		P.14/42	
511	34410	4	CY	CLASS QC2 CONCRETE, SUPERSTRUCTURE							4			
512	10100	689	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)							689			
512	10600	21	FT	CONCRETE REPAIR BY EPOXY INJECTION							21			
512	74000	668	SY	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES							668			
514	21001	LUMP		FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN							LUMP		P.14/42	
519	11101	275	SF	PATCHING CONCRETE STRUCTURE, AS PER PLAN							275		P.15/42	
607	98200	LUMP		FENCE, MISC.: VANDAL PROTECTION FENCE REBUILT							LUMP		P.15/42	
849	10000	LUMP		DAMAGE ASSESSMENT							LUMP			
849	10500	LUMP		SURFACE PREPARATION							LUMP			
849	10600	20	HOUR	REPAIRING DAMAGED MEMBERS BY GRINDING							20			
MADE BY: GTF CHECKED BY: MRD		DATE: 9/4/2025 DATE: 9/10/2025		HAM-00027-14.080 ESTIMATED QUANTITIES (01/IMS)								STRUCTURAL FILE NUMBER: 3101738		
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION					ABUT.	PIER	SUPER.	GEN.	REFERENCE SHEET NO.	
202	30000	96	SF	WALK REMOVED								96		
608	10000	96	SF	4" CONCRETE WALK								96		
644	01300	2	EACH	LANE ARROW								2		
644	30020	2	EACH	REMOVAL OF PAVEMENT MARKING								2		
646	20300	2	EACH	LANE ARROW								2		
646	10010	0.11	MILE	EDGE LINE, 6"								0.11		
646	10110	0.11	MILE	LANE LINE, 6"								0.11		
646	50000	2	EACH	REMOVAL OF PAVEMENT MARKING								2		
530	00200	LUMP		SPECIAL- STRUCTURES, TEMPORARY PROTECTIVE FALSE DECKING								LUMP		
848	10200	1,430	SY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION (T = 3")							1,430			
848	20000	1,430	SY	SURFACE PREPARATION USING HYDRODEMOLITION							1,430			
848	30200	7	CY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY							7			
848	50000	110	SY	HAND CHIPPING										
848	50100	LUMP		TEST SLAB								LUMP		
848	50200	6	CY	FULL DEPTH REPAIR							6			
848	50320	1,430	SY	EXISTING CONCRETE OVERLAY REMOVED (T = 2.5")							1,430			
MADE BY: BMG CHECKED BY: BMV		DATE: 7/2/2024 DATE: 7/3/2024		HAM-00027-13.300 ESTIMATED QUANTITIES (02/NHS)								STRUCTURAL FILE NUMBER: 3101703		
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION					ABUT.	PIER	SUPER.	GEN.	REFERENCE SHEET NO.	
202	11201	LUMP		PORTIONS OF STRUCTURE REMOVED, AS PER PLAN								LUMP	P.14/42	
512	10100	826	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)							799	27		
512	10301	446	SY	SEALING CONCRETE BRIDGE DECKS WITH HMWM RESIN, AS PER PLAN							371	75	P.14/42	
512	10601	249	FT	CONCRETE REPAIR BY EPOXY INJECTION, AS PER PLAN							249		P.14/42	
512	74000	826	SY	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES							799	27		
514	27710	929	FT	FIELD PAINTING, MISC.: PAINTING OF EXISTING RAILING, AS PER PLAN							929		P.15/42	
517	75501	90	FT	BRIDGE RAILING REBUILT, AS PER PLAN							90		P.15/42	
519	11101	24	SF	PATCHING CONCRETE STRUCTURE, AS PER PLAN							24		P.15/42	
843	50001	557	SF	PATCHING CONCRETE STRUCTURES WITH TROWELABLE MORTAR, AS PER PLAN							557		P.15/42	
MADE BY: BMG CHECKED BY: BMV		DATE: 7/2/2024 DATE: 7/3/2024		HAM-00126-09.040L ESTIMATED QUANTITIES (02/NHS)								STRUCTURAL FILE NUMBER: 3104664		
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION					ABUT.	PIER	SUPER.	GEN.	REFERENCE SHEET NO.	
202	11201	LUMP		PORTIONS OF STRUCTURE REMOVED, AS PER PLAN								LUMP	P.14/42	
509	20001	169	LB	CONCRETE REINFORCEMENT, REPLACEMENT OF EXISTING CONCRETE REINFORCEMENT, AS PER PLAN							169		P.14/42	
510	10001	55	EACH	DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT, AS PER PLAN							55		P.14/42	
511	34410	10	CY	CLASS QC2 CONCRETE, SUPERSTRUCTURE							10			
511	34411	1	CY	CLASS QC2 CONCRETE, SUPERSTRUCTURE, AS PER PLAN							1		P.30/42	

DESIGN AGENCY: fishbeck
PROJECT ID: 113006
SHEET TOTAL: P.16 42

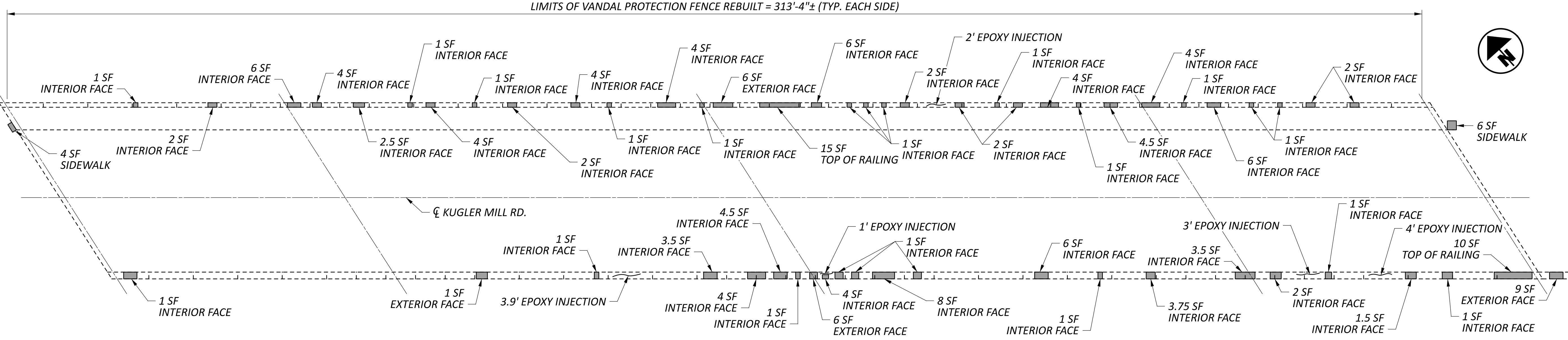
MADE BY: BMG		DATE: 7/2/2024		CHECKED BY: BMV		DATE: 7/3/2024		HAM-00562-01.790 ESTIMATED QUANTITIES (02/NHS)						STRUCTURAL FILE NUMBER: 3113949		
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION						ABUT.	PIER	SUPER.	GEN.	REFERENCE SHEET NO.		
512	10100	54	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)						5	49					
516	47001	LUMP		JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN										LUMP		P.15/42
519	11101	42	SF	PATCHING CONCRETE STRUCTURE, AS PER PLAN						26	16					P.15/42
SPECIAL	51911600	435	SF	SPECIAL - PATCHING CONCRETE STRUCTURE (GALVANIC ANODE PROTECTION)						131	304					P.15/42
844	20000	443	EACH	GALVANIC ANODE PROTECTION						69	374					
MADE BY: BMG		DATE: 7/2/2024		CHECKED BY: BMV		DATE: 7/3/2024		HAM-00562-02.530 ESTIMATED QUANTITIES (02/NHS)						STRUCTURAL FILE NUMBER: 3114023		
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION						ABUT.	PIER	SUPER.	GEN.	REFERENCE SHEET NO.		
202	11201	LUMP		PORTIONS OF STRUCTURE REMOVED, AS PER PLAN										LUMP		P.14/42
512	73500	694	SY	TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN								694				
516	12310	1040	LB	SIDEWALK COVER PLATE								1040				
519	11101	416	SF	PATCHING CONCRETE STRUCTURE, AS PER PLAN								416				P.15/42
607	98200	LUMP		FENCE, MISC.: VANDAL PROTECTION FENCE REBUILT									LUMP			P.15/42
609	24510	20	FT	CURB, TYPE 4-C										20		
642	00300	0.04	MILE	CENTER LINE, TYPE 1								0.04				
MADE BY: BMG		DATE: 5/20/2024		CHECKED BY: NCS		DATE: 5/24/2024		HAM-00071-04.500 ESTIMATED QUANTITIES (01/IMS)						STRUCTURAL FILE NUMBER: 3114562		
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION						ABUT.	PIER	SUPER.	GEN.	REFERENCE SHEET NO.		
519	12300	68	SY	PATCHING CONCRETE BRIDGE DECK - TYPE B (SEE PROPOSAL NOTE)								68				
MADE BY: D8		DATE: 1/30/2025		CHECKED BY: D8		DATE: 1/30/2025		HAM-00071-09.920 ESTIMATED QUANTITIES (01/IMS)						STRUCTURAL FILE NUMBER: 3115372		
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION						ABUT.	PIER	SUPER.	GEN.	REFERENCE SHEET NO.		
202	11501	4	EACH	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN (SECONDARY MEMBERS)								4				P.38/42
513	10201	217	LB	STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN								217				P.38/42
513	95000	5	FT	STRUCTURAL STEEL, MISC.: REPAIR OF DAMAGED SECONDARY MEMBER, FILLET WELDING								5				
514	20001	60	SF	FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN (TWO COAT)								60				P.38/42
849	10000	LUMP		DAMAGE ASSESSMENT									LUMP			
849	10500	LUMP		SURFACE PREPARATION									LUMP			
849	10600	9	.HOUR	REPAIRING DAMAGED MEMBERS BY GRINDING								9				
849	10700	LUMP		STRAIGHTENING DAMAGED MEMBERS									LUMP			
MADE BY: D8		DATE: 1/30/2025		CHECKED BY: D8		DATE: 1/30/2025		HAM-00075-16.420E ESTIMATED QUANTITIES (01/IMS) (03/IMS)						STRUCTURAL FILE NUMBER: 3111083		
ITEM	EXTENSION	TOTAL	UNIT	DESCRIPTION						ABUT.	SUPER. (01/IMS)	SUPER. (03/IMS)	GEN.	REFERENCE SHEET NO.		
202	11501	9	EACH	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN (SECONDARY MEMBERS)						3	6					P.41/42
513	10201	444	LB	STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN							148	296				P.41/42
513	95000	18	FT	STRUCTURAL STEEL, MISC.: REPAIR OF DAMAGED SECONDARY MEMBER, FILLET WELDING							6	12				
514	21001	LUMP		FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN							LUMP	LUMP				P.41/42
849	10000	LUMP		DAMAGE ASSESSMENT							LUMP	LUMP				
849	10500	LUMP		SURFACE PREPARATION							LUMP	LUMP				
849	10600	37	.HOUR	REPAIRING DAMAGED MEMBERS BY GRINDING								18.5	18.5			
849	10700	LUMP		STRAIGHTENING DAMAGED MEMBERS								LUMP	LUMP			

DESIGN AGENCY: fishbeck
 DESIGNER: NCS
 REVIEWER: BMV 02/04/25
 PROJECT ID: 113006
 SHEET TOTAL: P.17 42

REMOVAL DETAILS



PROPOSED DETAILS



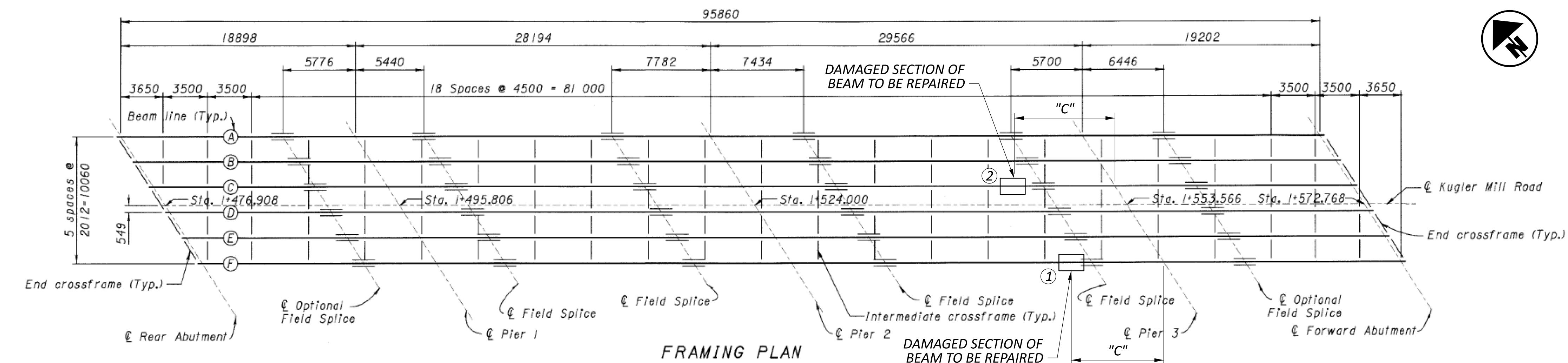
SUMMARY OF REPAIRS			
TYPE	MEASURED QUANTITIES	CONTINGENCY	TOTAL
ITEM 519 PATCHING	182.75 SF	1.5	275 SF
ITEM 512 EPOXY INJECTION	13.9 FT	1.5	21 FT
FULL THICKNESS REPAIR	2.5 CY	1.5	4 CY

LEGEND:

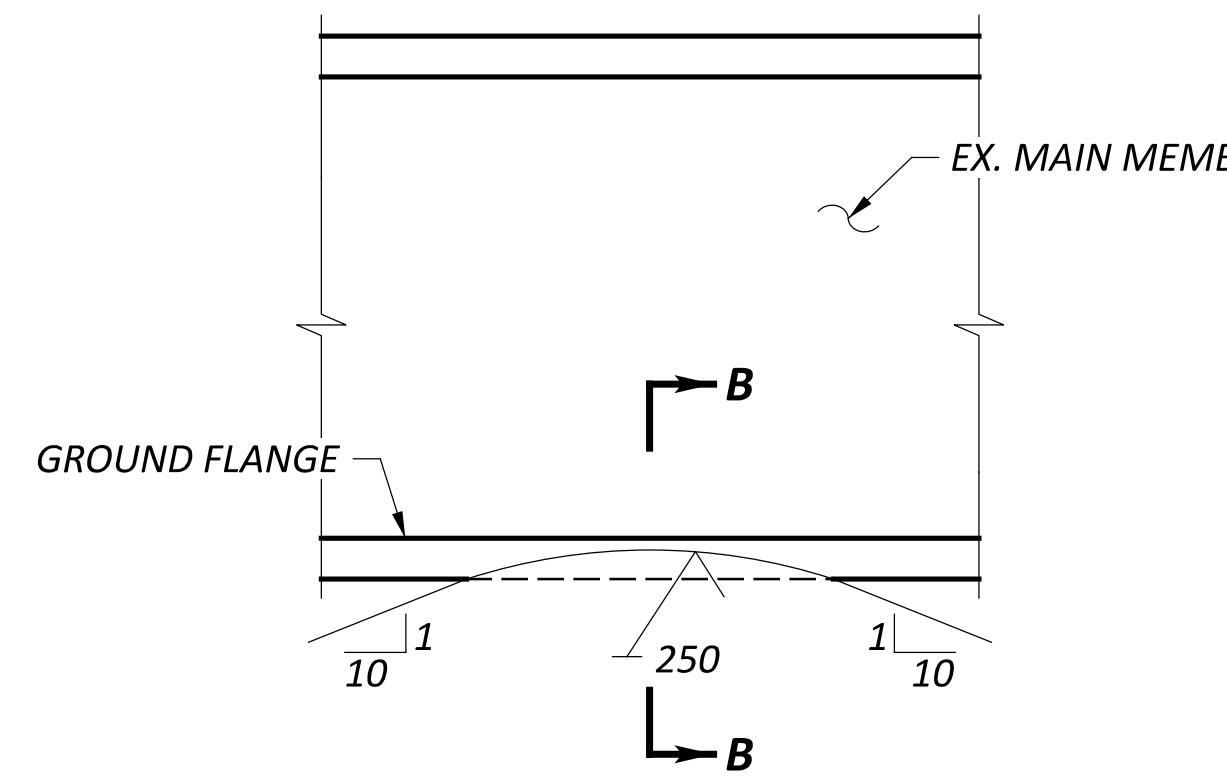
- INDICATES AREA TO BE REPAIRED WITH ITEM 519 - PATCHING CONCRETE STRUCTURE, AS PER PLAN
- ITEM 202, PORTIONS OF STRUCTURE REMOVED, AS PER PLAN (APPLIES TO FULL THICKNESS REPAIR LOCATIONS ONLY)
- ITEM 511, CLASS QC2 CONCRETE, SUPERSTRUCTURE

NOTES:

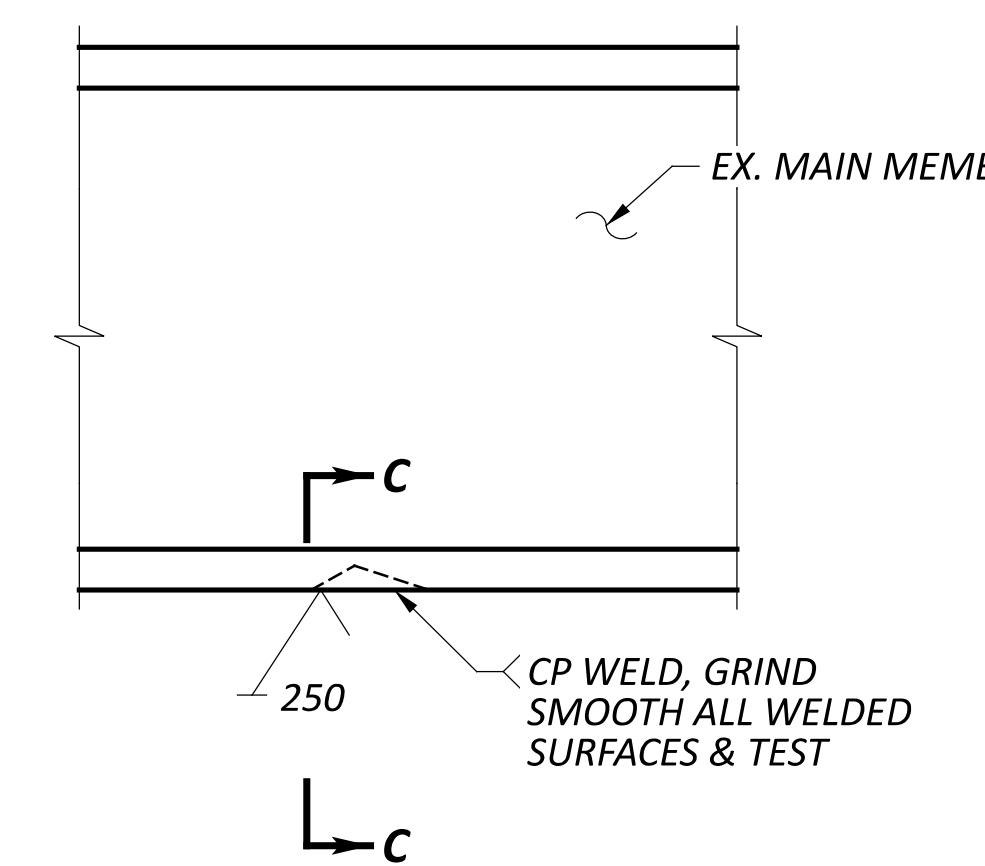
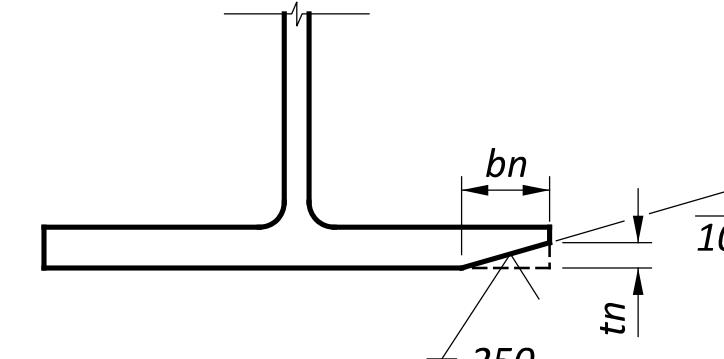
- AN ADDITIONAL 50% CONTINGENCY HAS BEEN ADDED TO THE FIELD MEASURED CRACKS AND PATCHING AREAS TO ALLOW FOR ADDITIONAL AREAS OF DETERIORATION. THE FINAL DIMENSIONS AND LOCATION OF THE DETERIORATED AREAS TO BE PATCHED SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER IN THE FIELD FOR FINAL PAYMENT.
- WHEN VERIFYING AREAS OF DETERIORATION IN THE FIELD, IF ANY LOCATIONS ARE UNSOUND ON THE INTERIOR AND EXTERIOR FACE THEY SHALL BE REPAIRED FULL THICKNESS PER THE DETAILS SHOWN ON THIS SHEET. A CONTINGENCY ESTIMATED QUANTITY HAS BEEN ADDED TO THIS SHEET FOR ANY POTENTIAL FULL DEPTH REPAIRS.
- EXISTING ANCHORS, BASE PLATES AND VERTICAL POSTS SHALL REMAIN AND BE REUSED. EXISTING FENCE MESH, HORIZONTAL RAILING AND ALL ASSOCIATED HARDWARE SHALL BE REMOVED AND REPLACED. TWO VERTICAL POSTS ARE TO BE REPLACED AS DIRECTED BY THE ENGINEER.
- IF A FULL THICKNESS REPAIR OCCURS AT A BASE PLATE LOCATION THE EXISTING BASE PLATE AND SLEEVE SHALL BE REUSED AND NEW 1/2" DIA. ANCHORS SHALL BE INSTALLED PER NOTE 5 ON SHEET 1 OF ODOT STANDARD DRAWING VPF-1-90.
- REMOVE RUST AND DETERIORATION FROM EXISTING BASE PLATES AND POST SLEEVES AND REPAIR GALVANIZATION ACCORDING TO C&MIS 711.02.
- FOR ADDITIONAL VANDAL PROTECTION FENCE DETAILS, SEE ODOT STANDARD DRAWING VPF-1-90
- REMOVE ALL EXISTING SEALER FROM BRIDGE RAILING, INCLUDING APPROACH SLABS AND APPLY NEW EPOXY URETHANE SEALER, FEDERAL COLOR NO.17778.



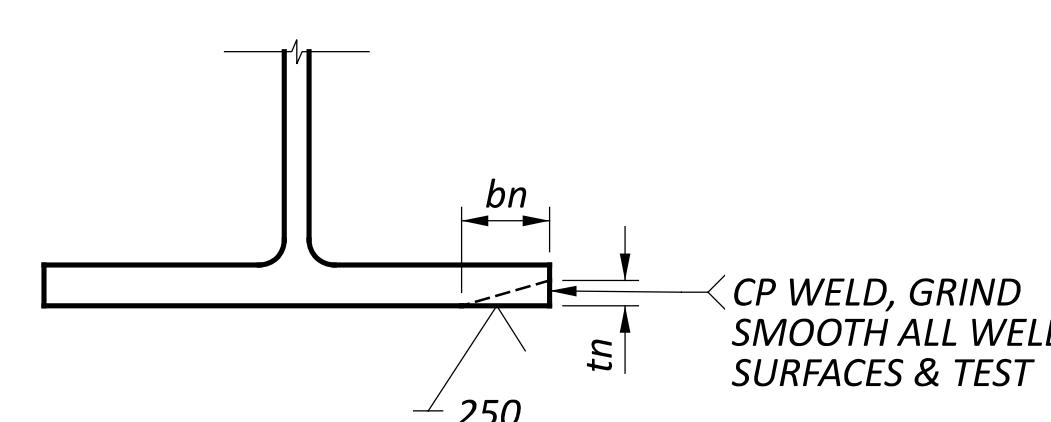
ITEM 849 REPAIRS									
DAMAGED AREA NO.	MEMBER LINE NO.	PIER	DIM. "C"	REPAIR DETAIL TYPE	DRILLING HOLES (EACH)	COPE HOLES (EACH)	STEEL MEMBER LEVEL UF (POUNDS)	CP WELD (FEET)	FILLET WELD (FEET)
1	BEAM F	3	45'±	FC2	0	0	0	0	0
2	BEAM C	3	45'±	FC2	0	0	0	0	0



COLLISION REPAIR FC2-2
IF AREA (tn, bn) AFTER GRINDING < 98% OF AREA (tf, bf) NOTE 1 APPLIES

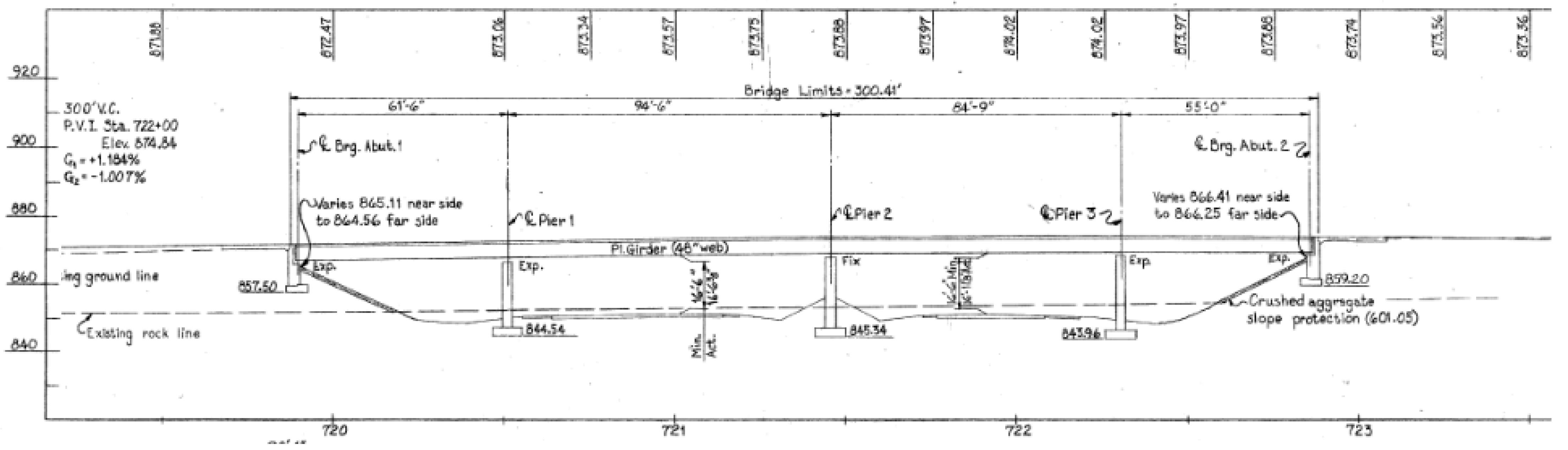
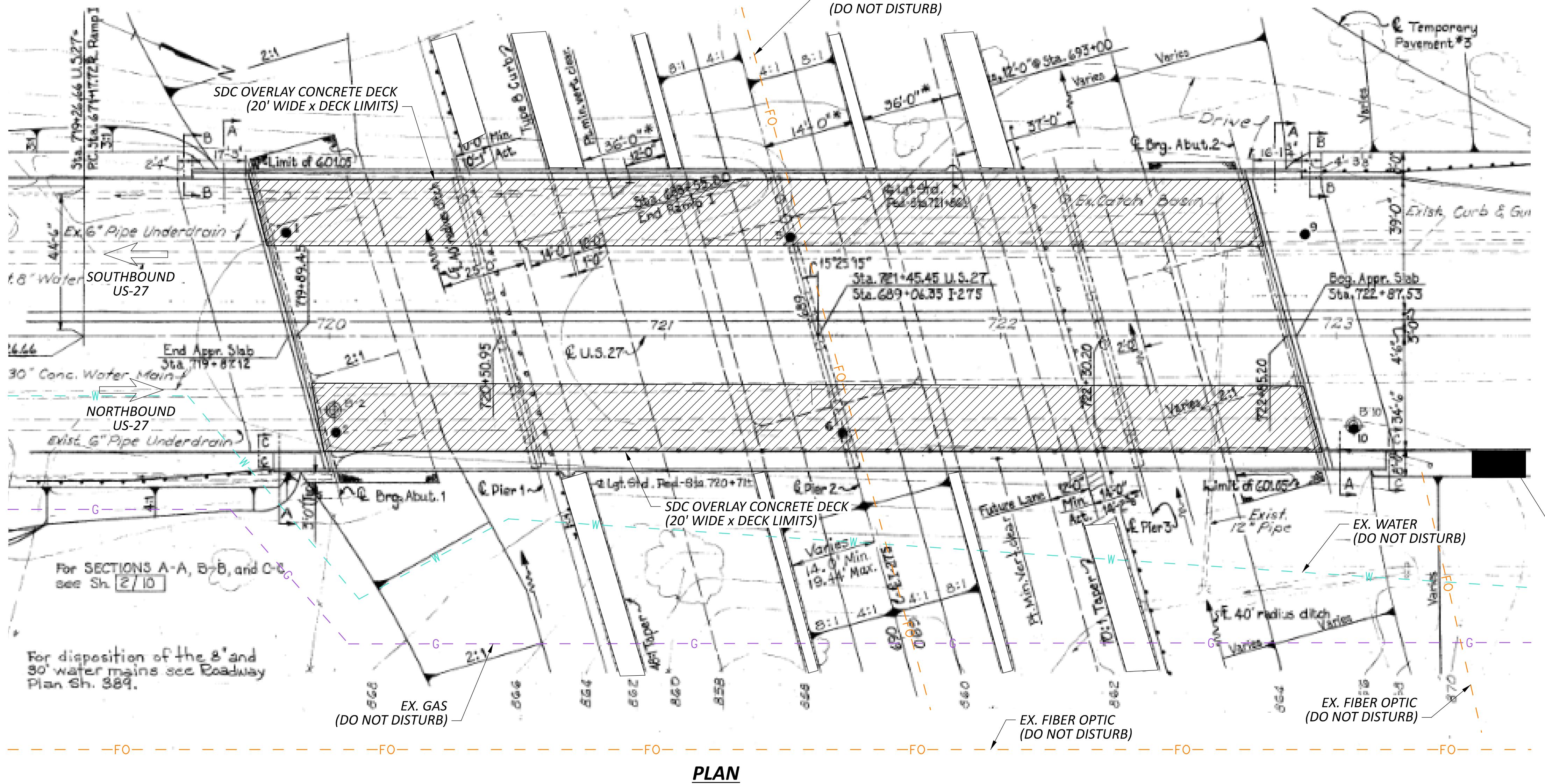


COLLISION REPAIR FC2-3
IF AREA (tn, bn) AFTER GRINDING > 98% OF AREA (tf, bf) NOTES 2 & 3 APPLY



NOTES:

1. IF NOTCH OR PARTIAL DEPTH CRACK CAN BE REMOVED BY GRINDING ACCORDING TO SUPPLEMENTAL SPECIFICATION 849, REPAIR DAMAGED MEMBERS. PERFORM GRINDING ACCORDING TO SUPPLEMENTAL SPECIFICATION 849 AND AS ILLUSTRATED IN DETAIL FC2-2.
2. IF NOTCH OR PARTIAL DEPTH CRACK MUST BE REPAIRED BY WELDING ACCORDING TO SUPPLEMENTAL SPECIFICATION 849 REPAIRING DAMAGED MEMBERS, AS ILLUSTRATED IN DETAIL FC2-3. PERFORM COMPLETE PENETRATION WELDING ACCORDING TO C&MS 513.21 BY ATTACHING RUN OFF TABS AND GRIND ALL WELDED SURFACES SMOOTH ACCORDING TO ANSI B46.1 OF 250 MIL.
3. PERFORM NDT TESTING ACCORDING TO C&MS 513.25A.
4. REPAIR DAMAGED PAINT WITH A TWO COAT SYSTEM PER ITEM 514. COLOR TO MATCH EXISTING (IZEU APRIL 2000).



NOTES:

DETAILS ON THIS SITE PLAN SHEET ARE FROM ARCHIVED PLANS AND SHOULD BE USED FOR REFERENCE ONLY.

LEGEND:

- LIMITS OF CONCRETE OVERLAY REPAIR PER ITEM 848, SDC OVERLAY USING HYDRODEMOLITION

DESIGN TRAFFIC:

HAM-US27-14.08

2027 ADT = 40,000 2027 ADTT = 4,000

2039 ADT = 40,500 2039 ADTT = 4,050

DIRECTIONAL DISTRIBUTION = 0.56

DESIGN SPEED: 35 MPH LEGAL SPEED: 35 MPH

DESIGN FUNCTIONAL CLASSIFICATION: 03-PRINCIPAL ARTERIAL OTHER (URBAN)
NHS ROUTE: YES

IR-275-16.02

2024 ADT = 84,915 2024 ADTT = 11,039

DESIGN SPEED: 65 MPH LEGAL SPEED: 65 MPH

DESIGN FUNCTIONAL CLASSIFICATION: 01-PRINCIPAL ARTERIAL INTERSTATE (URBAN)
NHS ROUTE: YES

ITEM 202 - WALK REMOVED, 96 SF
ITEM 607 - 4" CONCRETE WALK, 96 SF

EXISTING STRUCTURE

TYPE: CONTINUOUS PLATE GIRDER REINFORCED CONCRETE DECK AND SUBSTRUCTURE

SPANS: 61'-6"±, 94'-6"±, 84'-9"±, 55'-0"± C/C BEARINGS

ROADWAY: 39'-0"± T/T CURB (BOTH DIRECTIONS) WITH 3'-0"± MEDIAN CURB, 2'-0"± SAFETY CURB AND 5'-0"± SIDEWALK

LOADING: HS20-44

SKew: 15°25'15"± RF

WEARING SURFACE: 2½"± SUPERPLASTICIZED DENSE CONCRETE OVERLAY OR 2¼"± POLYESTER POLYMER CONCRETE OVERLAY

APPROACH SLABS: AS-1-72 (20'-0"± LONG)

ALIGNMENT: TANGENT

CROWN: 0.016± FT/FT

STRUCTURE FILE NUMBER: 3101738

DATE BUILT: 1977, REHABILITATED 1999 AND 2019

DISPOSITION: SEE PROPOSED WORK

PROPOSED WORK

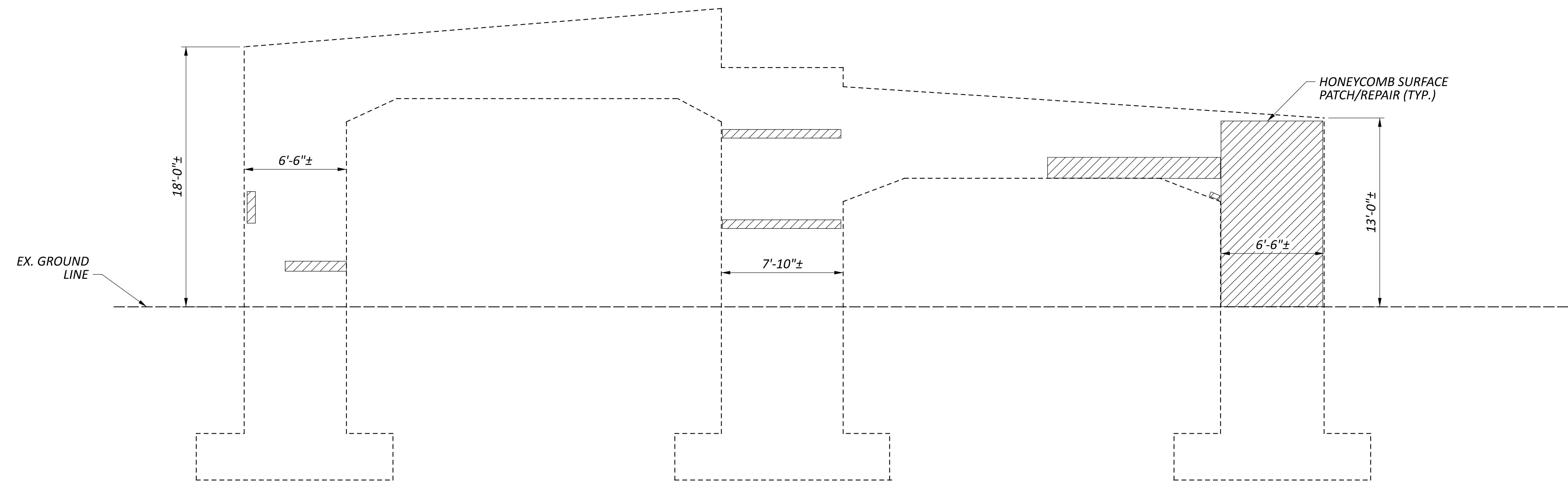
1. REMOVE THE EXISTING 2.5" SDC OVERLAY AND 0.5" OF THE EXISTING DECK USING HYDRODEMOLITION IN THE LOCATIONS SPECIFIED.

2. REPAIR WEARING SURFACE WITH SDC OVERLAY PER SUPPLEMENTAL SPECIFICATION 848.

SFN
3101738
DESIGN AGENCY

fishbeck

DESIGNER	CHECKER
BMG	NCS
REVIEWER	
JPC	02/04/25
PROJECT ID	113006
SUBSET	TOTAL
S.1	1
SHEET	TOTAL
P.21	42

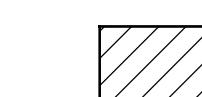


WEST PIER ELEVATION
(LOOKING WEST)

SUMMARY OF PATCHING AREAS ITEM 843			
LOCATION	MEASURED (SF)*	CONTINGENCY	TOTAL (SF)
WEST PIER	140	1.5	210

* - SEE NOTE 1 FOR MEASURED AREA

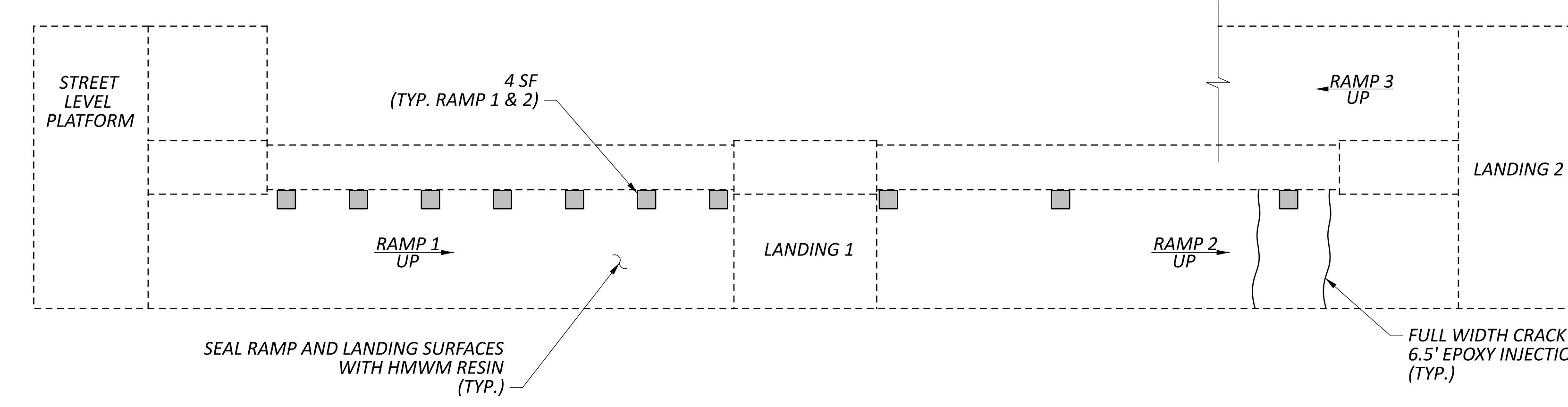
LEGEND:



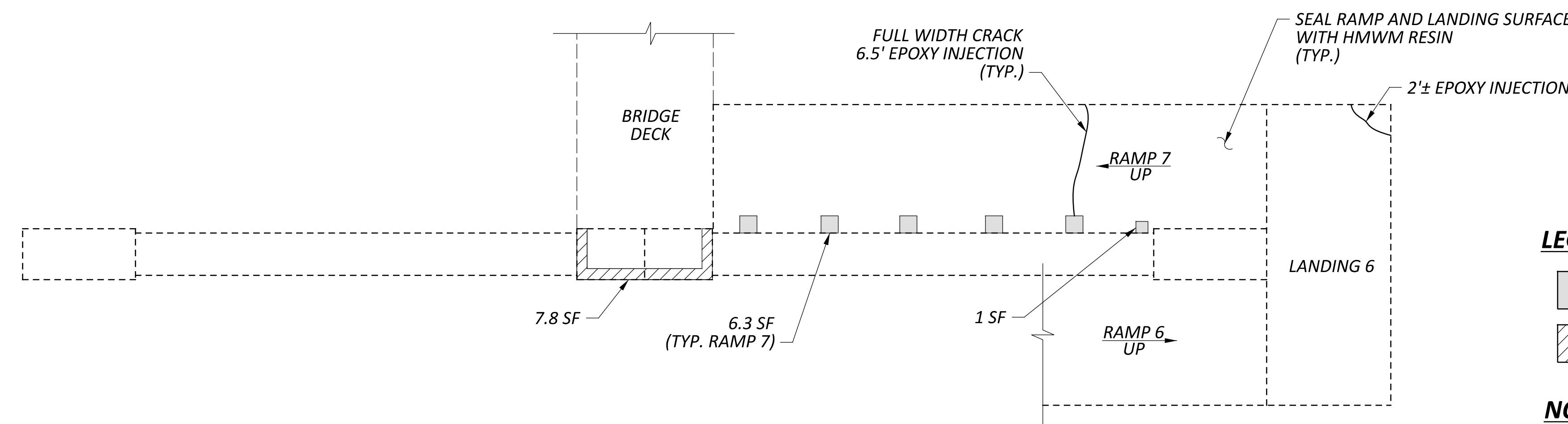
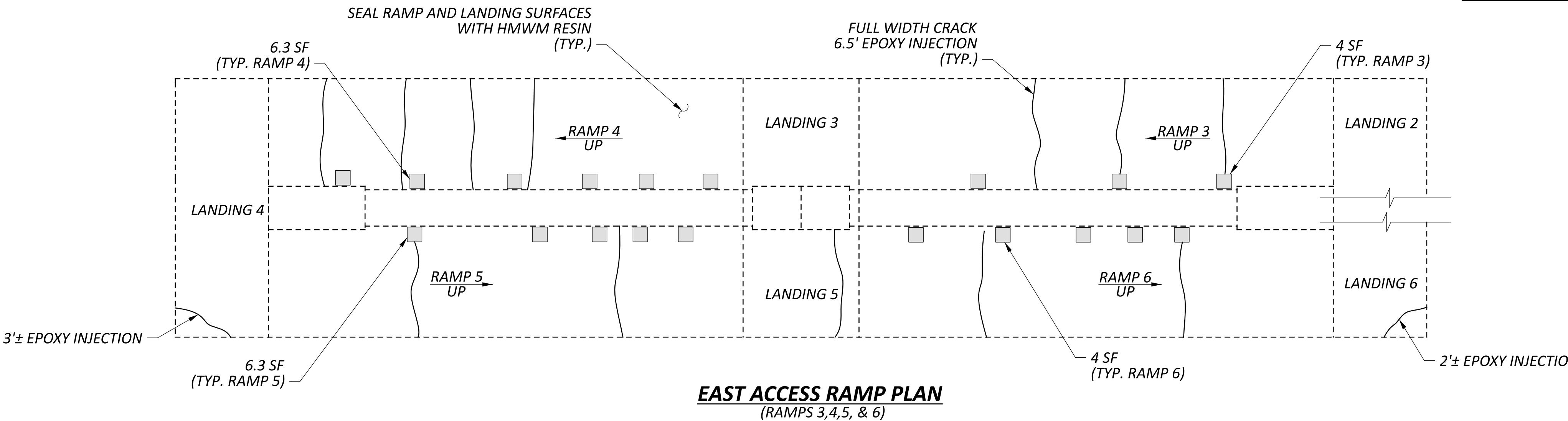
- INDICATES AREAS TO BE REPAIRED PER SUPPLEMENTAL SPECIFICATION ITEM 843

NOTES:

1. FOR ESTIMATING PURPOSES, 10% OF THE WEST PIER SURFACE AREA HAS BEEN INCLUDED WITH ITEM 843 FOR PAYMENT. FOR ADDITIONAL DETAILS, SEE GENERAL NOTES SHEET [P.15 / 42].
2. APPLY EPOXY-URETHANE SEALER TO ALL EXPOSED SURFACES OF THE WEST AND EAST PIERS AFTER PATCHING HAS BEEN COMPLETED. USE FEDERAL COLOR NO. 17778.



SUMMARY OF REPAIRS			
TYPE	MEASURED QUANTITIES	CONTINGENCY	TOTAL
ITEM 519 PATCHING	7.8 SF	1.5	12 SF
ITEM 512 EPOXY INJECTION	104.5 FT	1.5	157 FT
ITEM 843 MORTAR PATCH	173.8 SF	1.5	261 SF

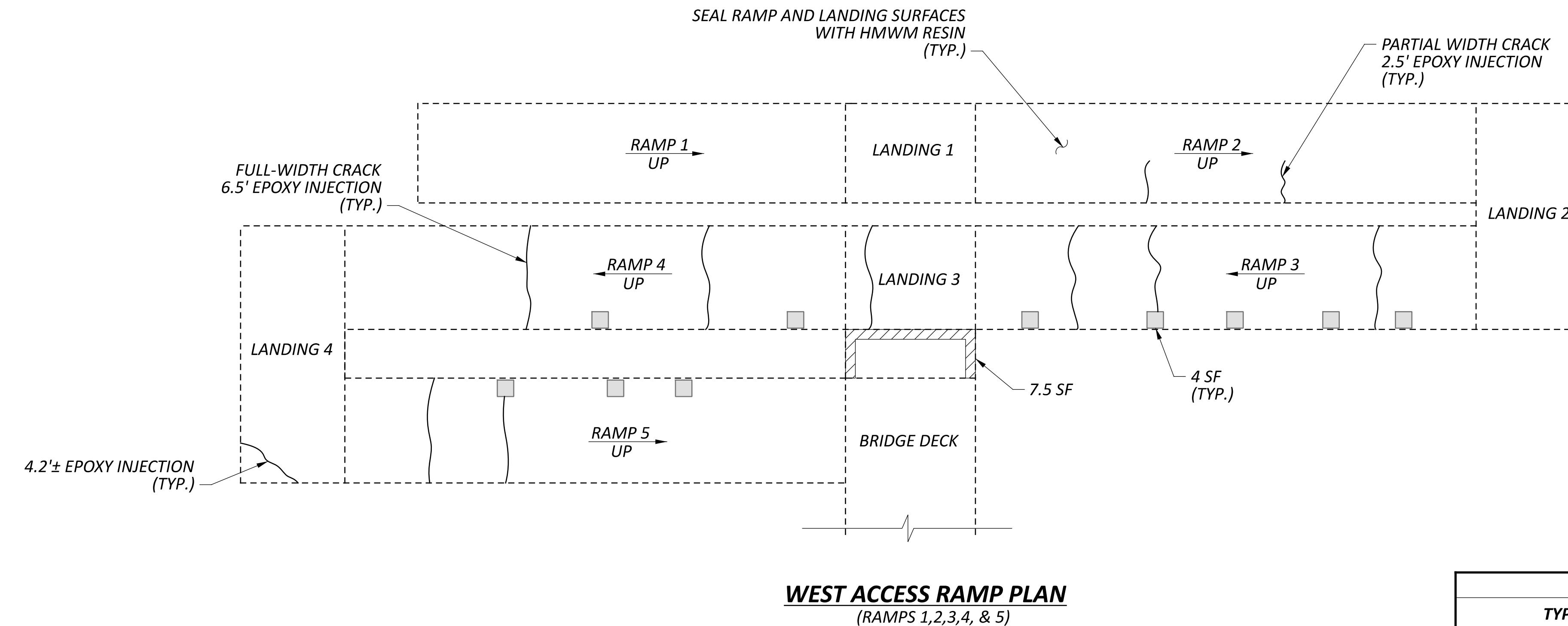


LEGEND:

- INDICATES AREA ON UNDERSIDE OF RAMP TO BE REPAIRED PER SUPPLEMENTAL SPECIFICATION ITEM 843
- INDICATES AREA TO BE REPAIRED PER ITEM 519, CONCRETE PATCHING, AS PER PLAN

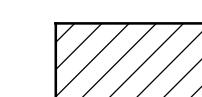
NOTES:

- SEE GENERAL NOTES SHEET P.14 / 42 FOR ADDITIONAL HMWM AND EPOXY INJECTION NOTES.
- AN ADDITIONAL 50% CONTINGENCY HAS BEEN ADDED TO THE FIELD MEASURED CRACKS AND PATCHING AREAS TO ALLOW FOR ADDITIONAL AREAS OF DETERIORATION. THE FINAL DIMENSIONS AND LOCATION OF THE DETERIORATED AREAS TO BE PATCHED SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER IN THE FIELD FOR FINAL PAYMENT.



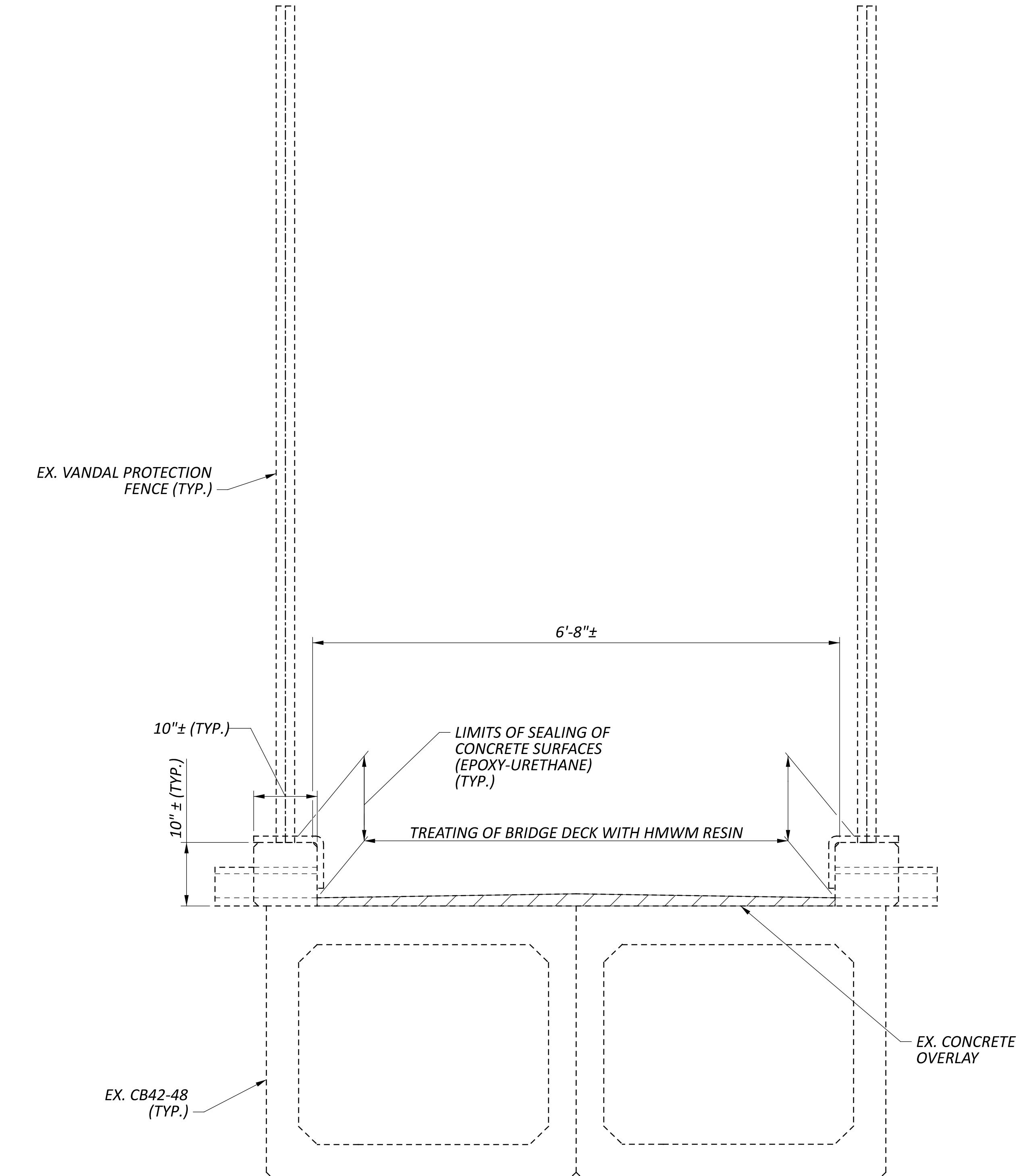
SUMMARY OF REPAIRS			
TYPE	MEASURED QUANTITIES	CONTINGENCY	TOTAL
ITEM 519 PATCHING	7.5 SF	1.5	12 SF
ITEM 512 EPOXY INJECTION	61.2 FT	1.5	92 FT
ITEM 843 MORTAR PATCH	40 SF	1.5	60 SF

LEGEND:

-  - INDICATES AREA ON UNDERSIDE OF RAMP TO BE REPAIRED PER SUPPLEMENTAL SPECIFICATION ITEM 843
-  - INDICATES AREA TO BE REPAIRED PER ITEM 519, CONCRETE PATCHING, AS PER PLAN

NOTES:

- SEE GENERAL NOTES SHEET P.14 / 42 FOR ADDITIONAL HMWM AND EPOXY INJECTION NOTES.
- AN ADDITIONAL 50% CONTINGENCY HAS BEEN ADDED TO THE FIELD MEASURED CRACKS AND PATCHING AREAS TO ALLOW FOR ADDITIONAL AREAS OF DETERIORATION. THE FINAL DIMENSIONS AND LOCATION OF THE DETERIORATED AREAS TO BE PATCHED SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER IN THE FIELD FOR FINAL PAYMENT.



PEDESTRIAN BRIDGE TYPICAL SECTION

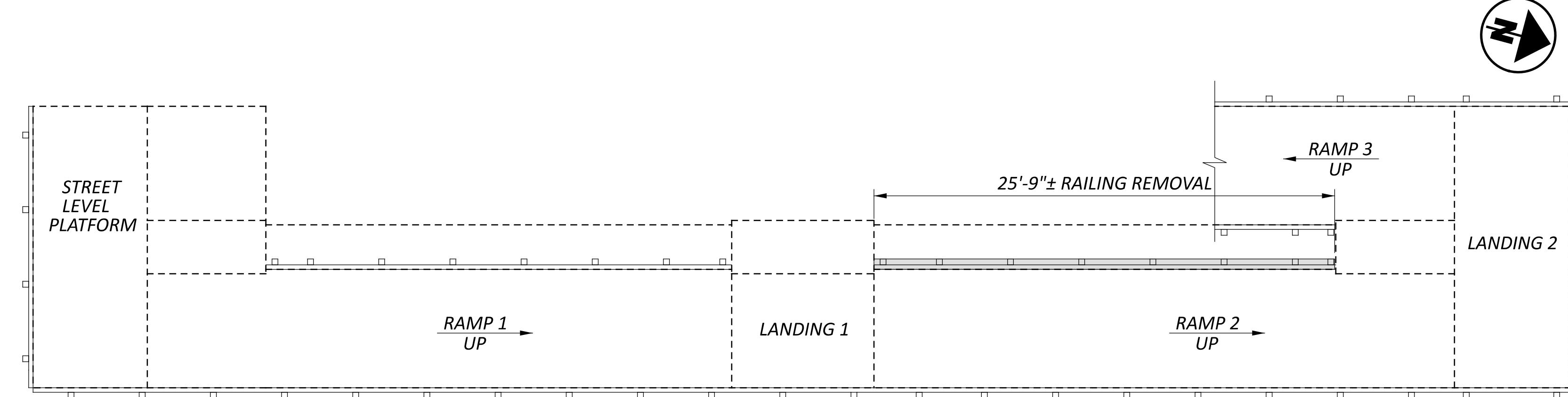
NOTES:

1. SEE GENERAL NOTES SHEET P.14 / 42 FOR ADDITIONAL HMWM NOTES.
2. EPOXY URETHANE SEALER SHALL BE FEDERAL COLOR NO. 17778.

PEDESTRIAN BRIDGE TYPICAL SECTION REPAIRS
BRIDGE NO. HAM-00027-13.300
PEDESTRIAN BRIDGE OVER US-27

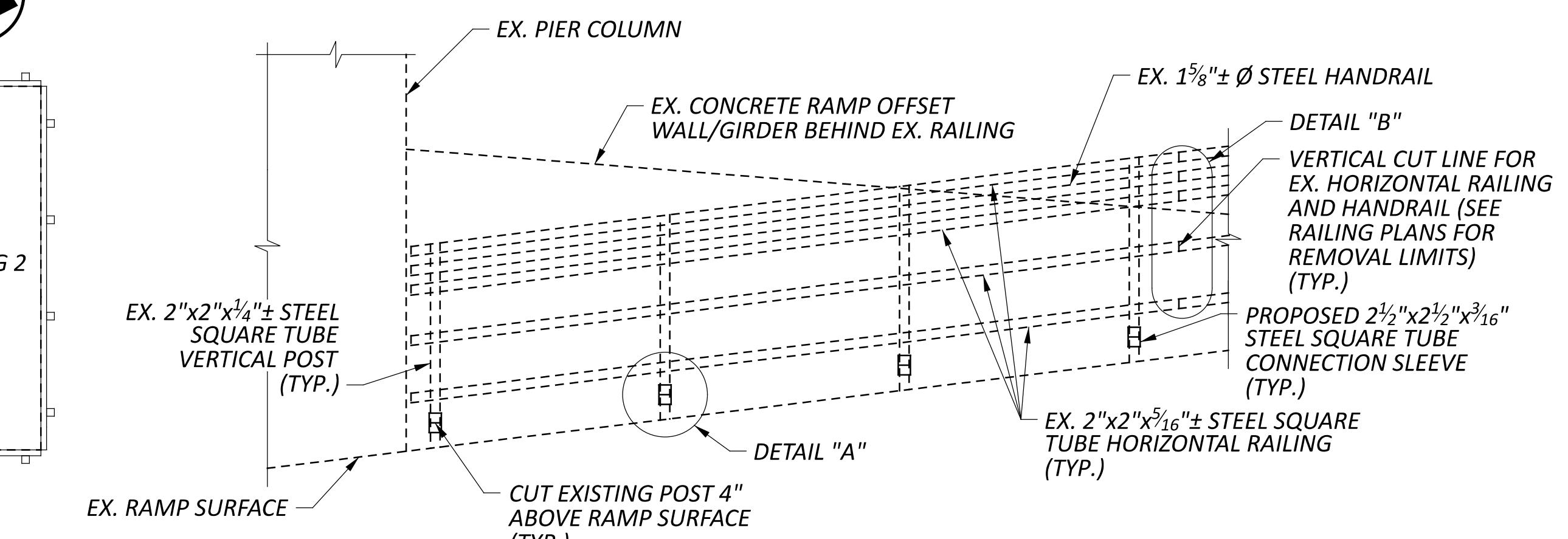
SFN
3101703
DESIGN AGENCY
fishbeck

DESIGNER NCS	CHECKER BMG
REVIEWER JPC 02/04/25	
PROJECT ID 113006	
SUBSET S.5	TOTAL 6
SHEET P.26	
TOTAL 42	



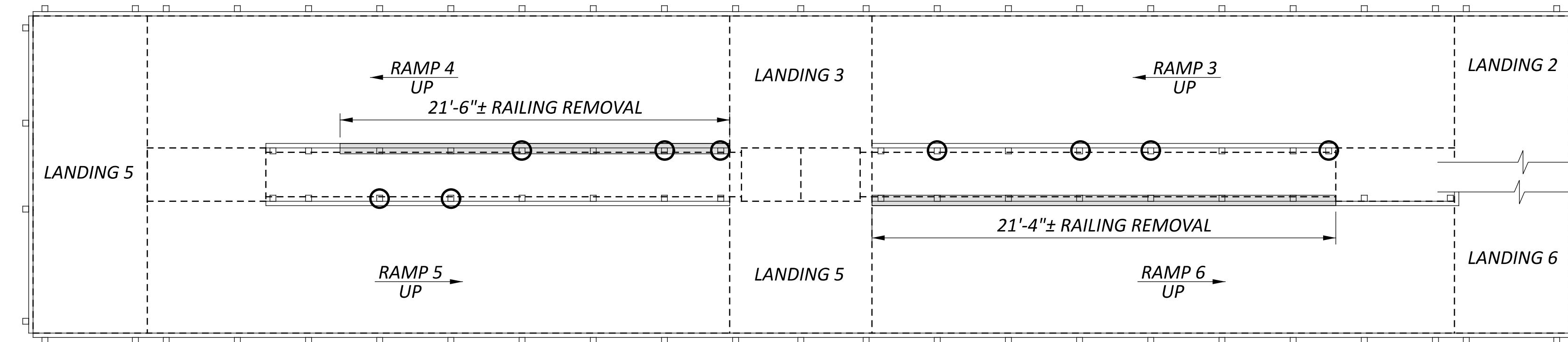
EAST ACCESS RAMP RAILING PLAN

(RAMPS 1 & 2)



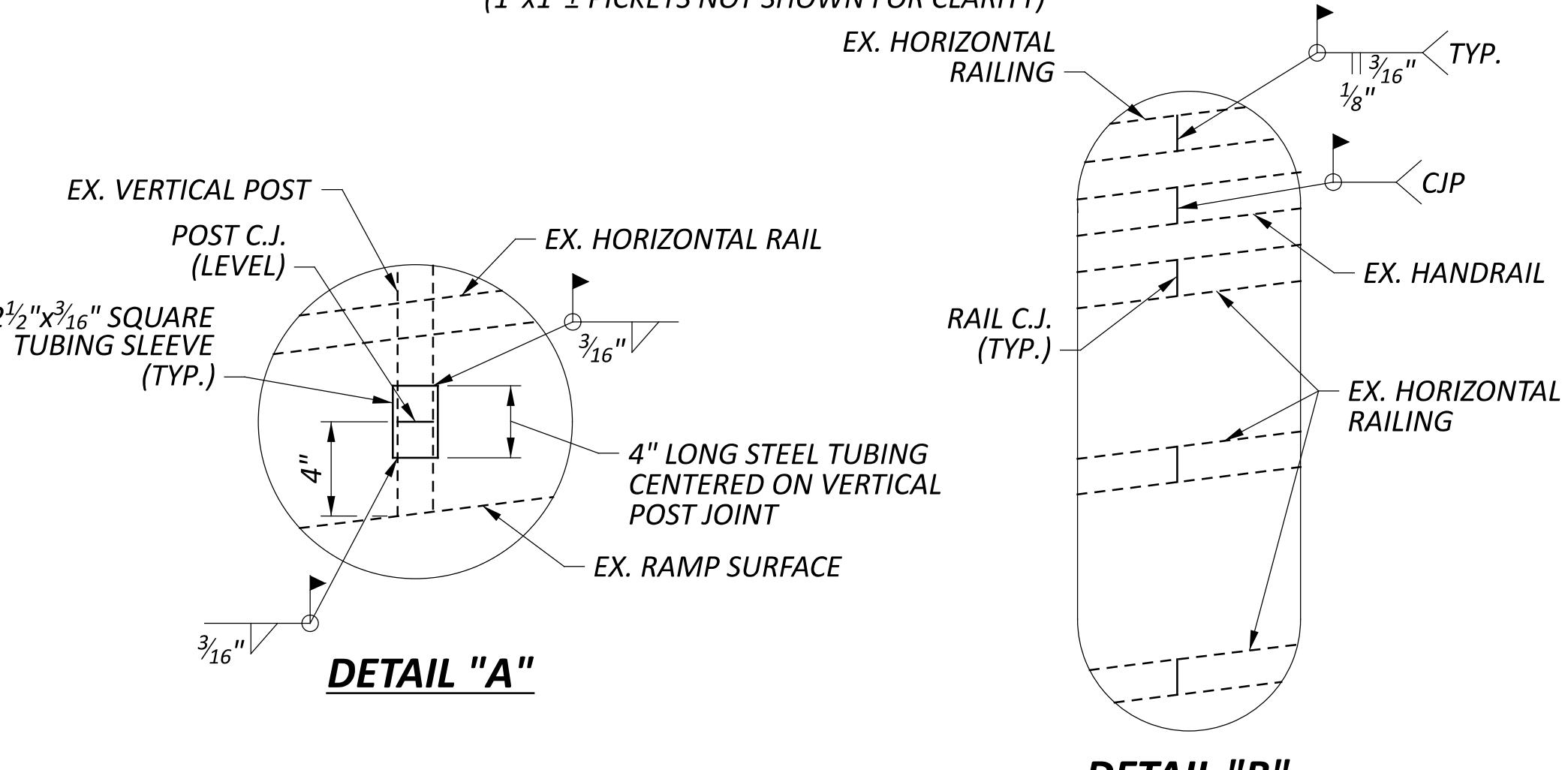
TYPICAL RAILING REMOVAL ELEVATION

(1" x 1"± PICKETS NOT SHOWN FOR CLARITY)



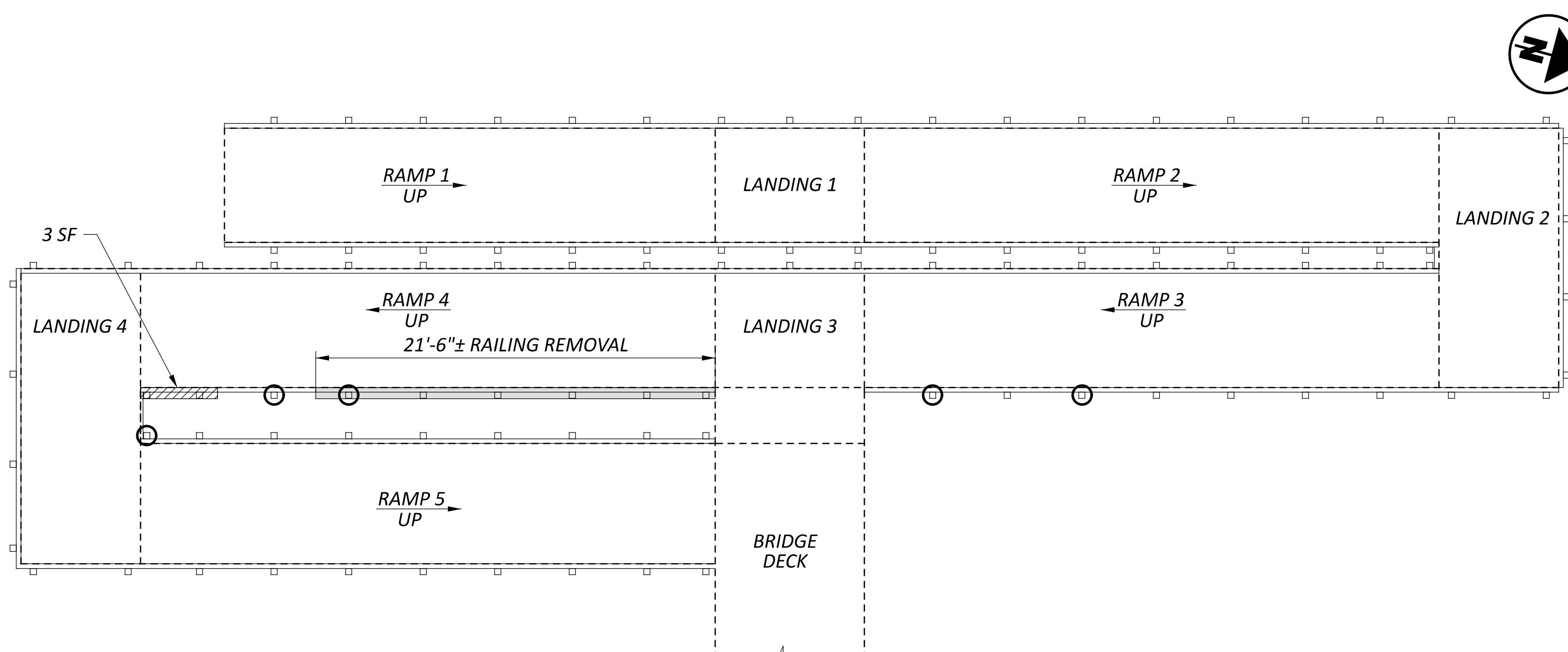
EAST ACCESS RAMP RAILING PLAN

(RAMPS 3,4,5, & 6)



DETAIL "A"

DETAIL "B"



WEST ACCESS RAMP RAILING PLAN

(RAMPS 1,2,3,4, & 5)

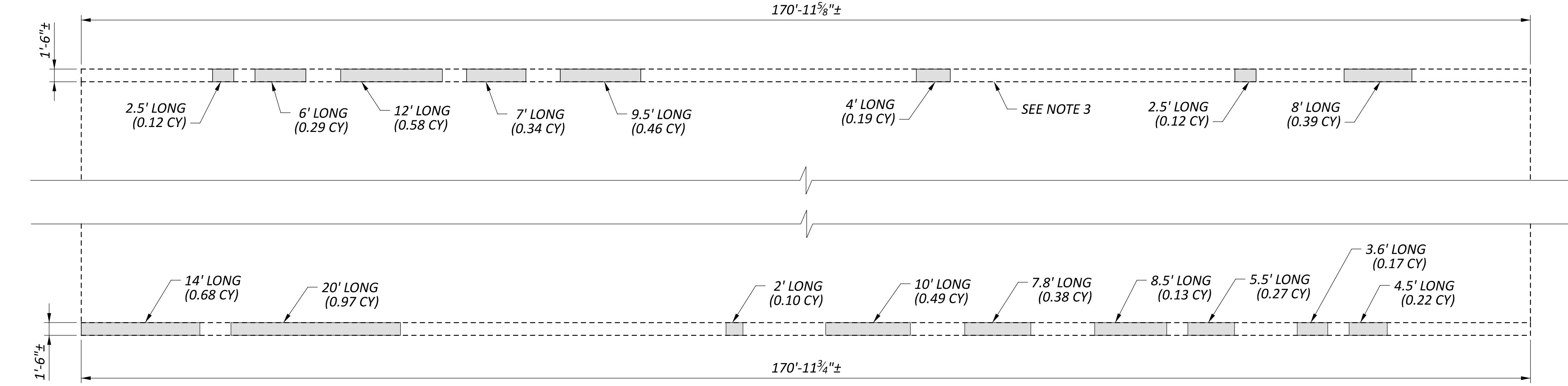
SUMMARY OF PATCHING AREAS ITEM 843			
LOCATION	MEASURED (SF)	CONTINGENCY	TOTAL (SF)
WEST ACCESS RAMP	8	1.5	12
EAST ACCESS RAMP	9	1.5	14
TOTAL			26

LEGEND:

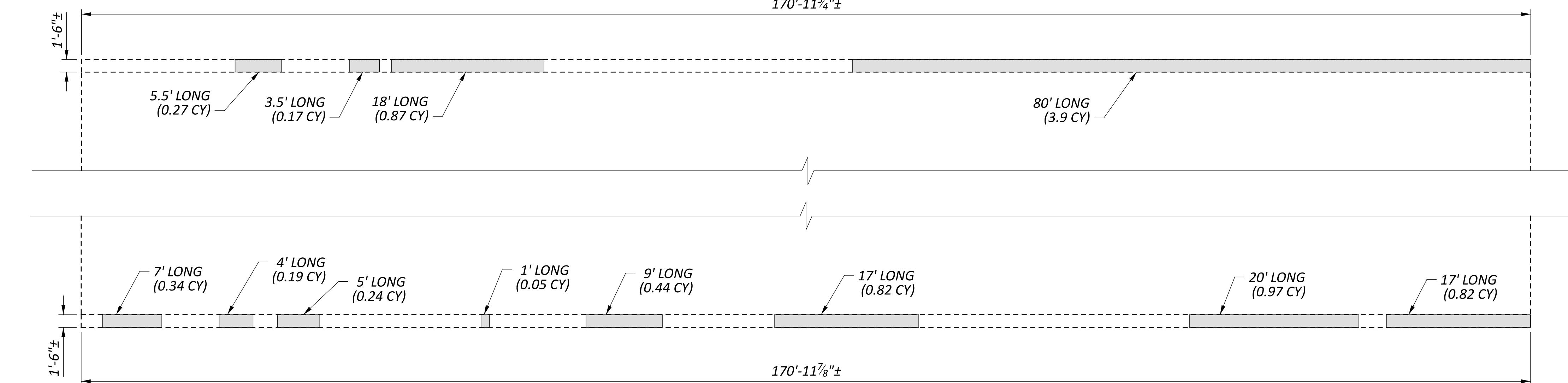
- INDICATES EXISTING BRIDGE RAILING TO BE REMOVED AND REBUILT PER ITEM 517 BRIDGE RAILING REBUILT, AS PER PLAN
- - INDICATES AREA ON RAMP SURFACE AROUND EX. POST (1"x1"±) TO BE REPAIRED PER SUPPLEMENTAL SPECIFICATION ITEM 843
- - INDICATES AREA TO BE REPAIRED PER SUPPLEMENTAL SPECIFICATION ITEM 843

NOTES:

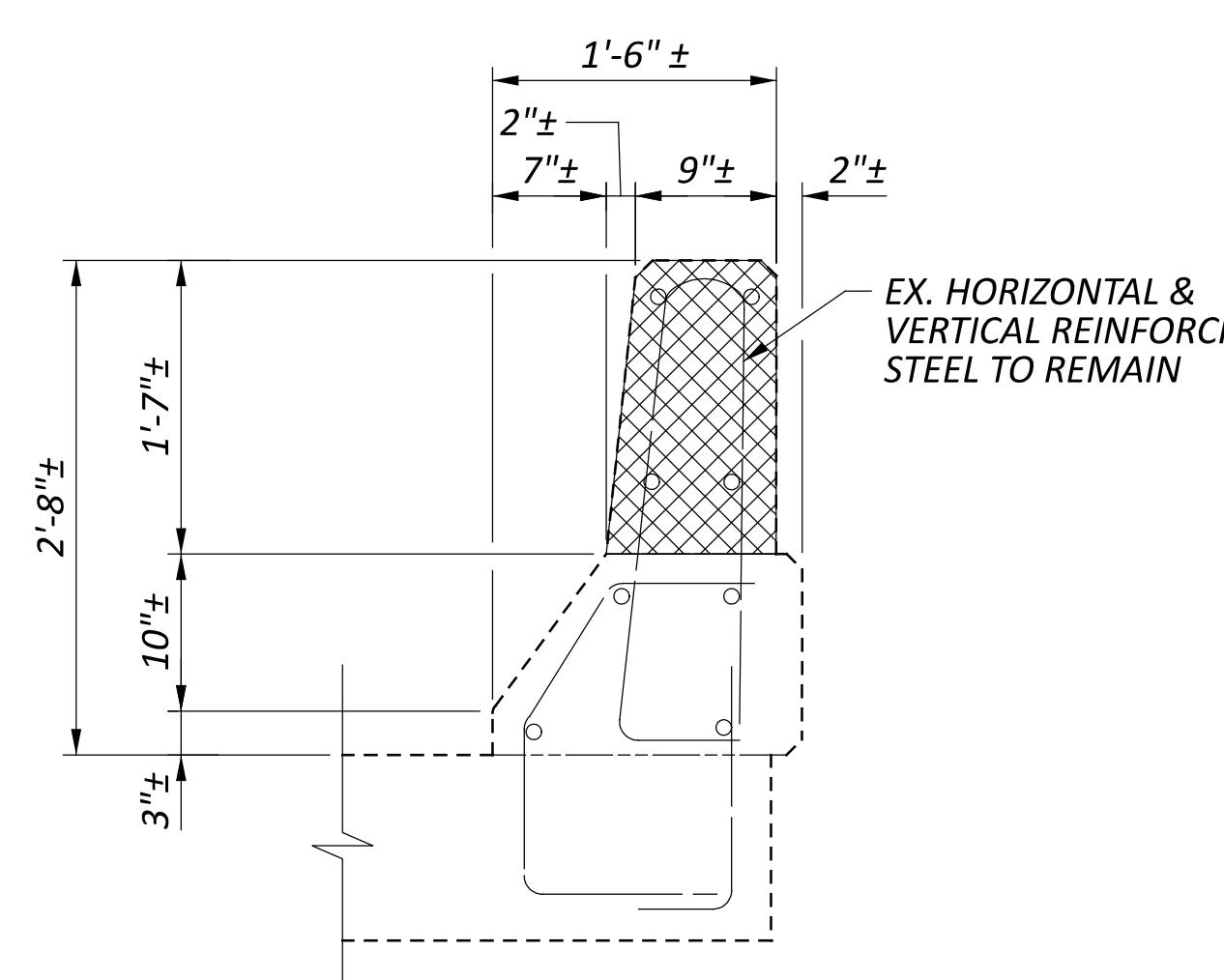
- PAINT THE ENTIRE SURFACE AREA OF ALL EXISTING STEEL RAILING PER ITEM 514, FIELD PAINTING, MISC.: PAINTING OF EXISTING RAILINGS, AS PER PLAN.
- APPLY EPOXY-URETHANE SEALER TO ALL EXPOSED SURFACES OF THE WEST AND EAST ACCESS RAMPS, EXCLUDING THE RAMP WALKWAY SURFACE. USE FEDERAL COLOR NO. 17778.
- FOR ADDITIONAL BRIDGE RAILING PAINTING AND REMOVAL NOTES, SEE GENERAL NOTES SHEET P.15 / 42 .
- STEEL TUBING SLEEVES SHALL BE GALVANIZED ASTM A709 GRADE 36 - YIELD STRENGTH 36 KSI.
- AN ADDITIONAL 50% CONTINGENCY HAS BEEN ADDED TO THE FIELD MEASURED PATCHING AREAS TO ALLOW FOR ADDITIONAL AREAS OF DETERIORATION. THE FINAL DIMENSIONS AND LOCATION OF THE DETERIORATED AREAS TO BE PATCHED SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER IN THE FIELD FOR FINAL PAYMENT.
- ADDITIONAL BRIDGE RAILING SHALL BE REMOVED, IN ADDITION TO WHAT IS SHOWN IN THE PLANS, AS NEEDED TO COMPLETE THE SURFACE PREPARATION AND SEALING OF THE EXISTING CONCRETE SURFACES. REMOVAL OF EXISTING BRIDGE RAILING IS INCLUDED WITH ITEM 202, PORTIONS OF STRUCTURE REMOVED, AS PER PLAN FOR PAYMENT.



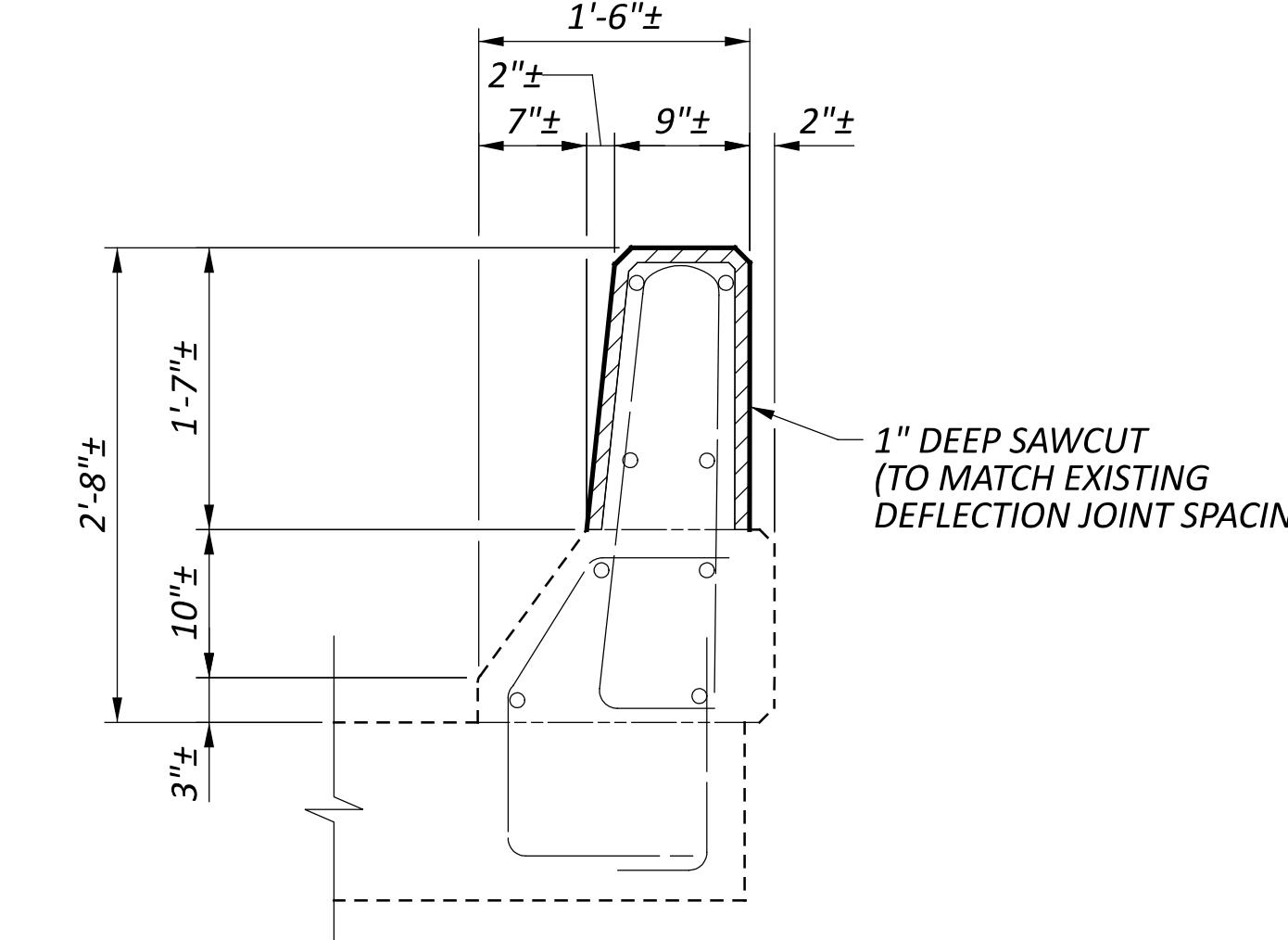
BRIDGE RAILING PLAN - LEFT BRIDGE



BRIDGE RAILING PLAN - RIGHT BRIDGE



PARTIAL HEIGHT BRIDGE RAILING REMOVAL DETAIL



PARTIAL HEIGHT BRIDGE RAILING REPAIR DETAIL

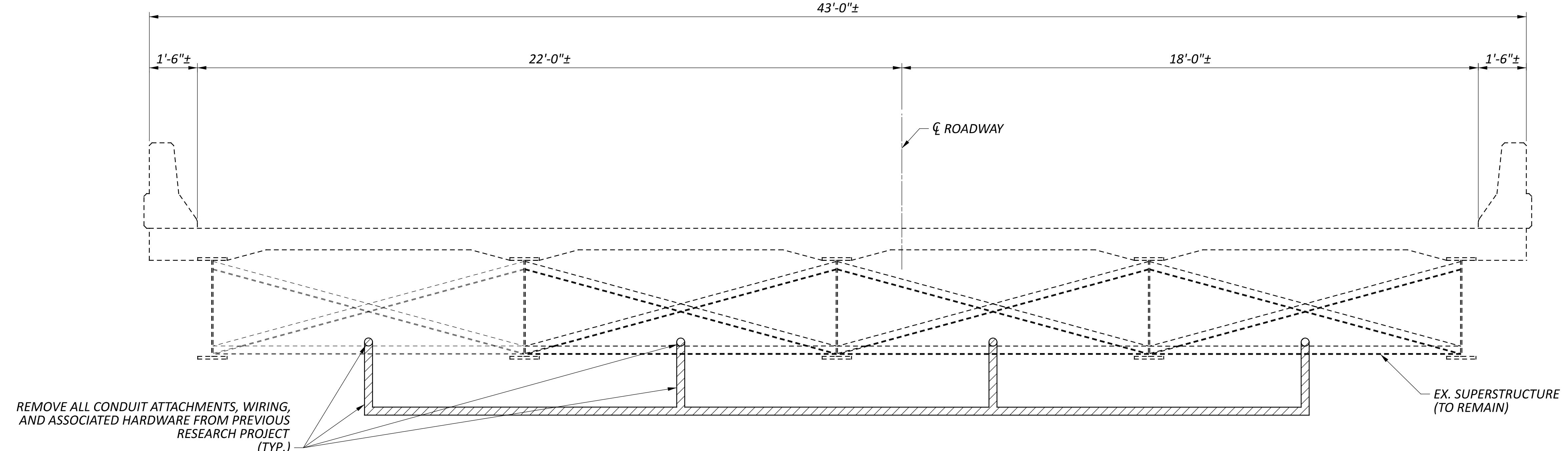
SUMMARY OF PARTIAL HEIGHT REPAIR			
LOCATION	MEASURED (CY)	CONTINGENCY	TOTAL (CY)
LEFT BRIDGE, NORTH BRIDGE RAILING	2.5	1.5	4
LEFT BRIDGE, SOUTH BRIDGE RAILING	3.4	1.5	6
RIGHT BRIDGE, NORTH BRIDGE RAILING	5.2	1.5	8
RIGHT BRIDGE, SOUTH BRIDGE RAILING	3.9	1.5	6
TOTAL			24

LEGEND:

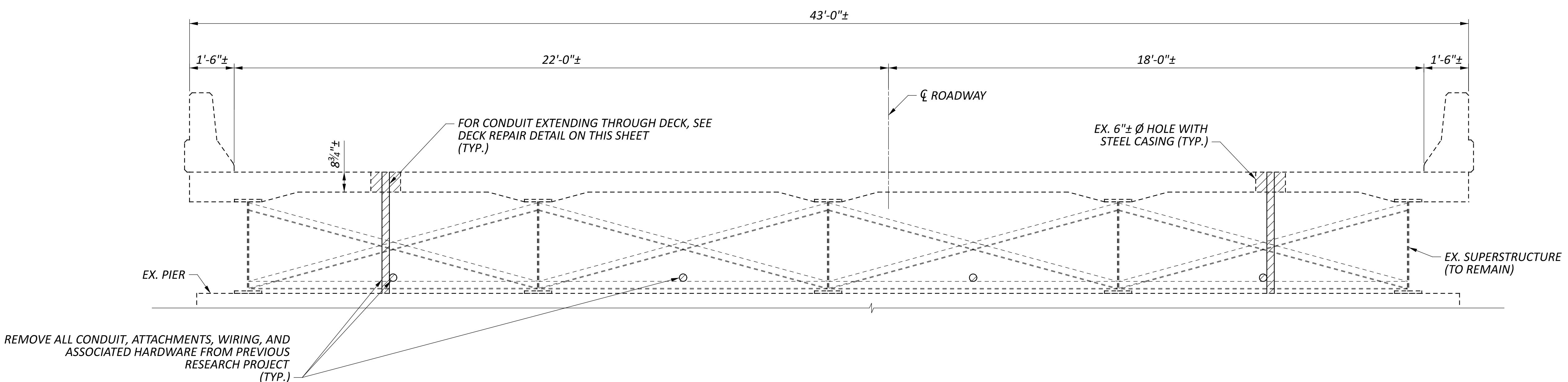
- AREA TO BE REPAIRED PARTIAL HEIGHT OF BRIDGE RAILING
- ITEM 202, PORTIONS OF STRUCTURE REMOVED, AS PER PLAN

NOTES:

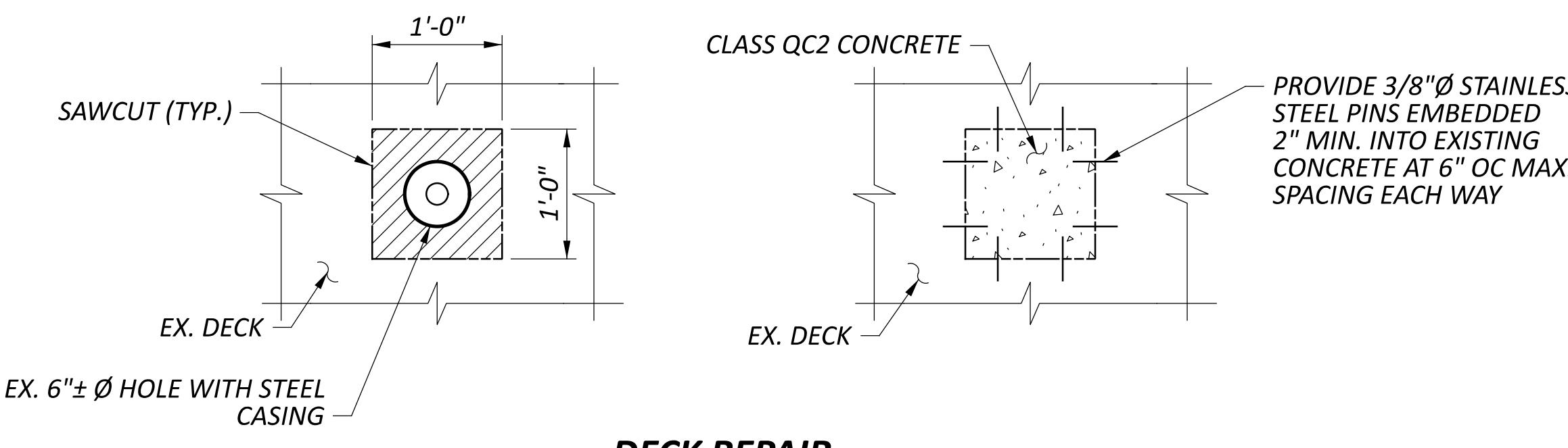
- AN ADDITIONAL 50% CONTINGENCY HAS BEEN ADDED TO THE FIELD MEASURED AREAS TO ALLOW FOR ADDITIONAL AREAS OF DETERIORATION. THE FINAL DIMENSIONS AND LOCATION OF THE DETERIORATED AREAS TO BE REPAIRED SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER IN THE FIELD FOR FINAL PAYMENT.
- EXISTING REINFORCING STEEL SHALL BE CLEANED AND PRESERVED WITHOUT DAMAGE TO THE SATISFACTION OF THE ENGINEER. DAMAGED REINFORCEMENT SHALL BE REPLACED IN KIND AS DIRECTED BY THE ENGINEER.
- THE EXISTING NORTH BRIDGE RAILING ON THE LEFT BRIDGE HAS JUNCTION BOXES FROM A PREVIOUS RESEARCH PROJECT. IF AN EXISTING JUNCTION BOX EXISTS IN AN AREA TO BE REPAIRED, THE CONTRACTOR SHALL COMPLETELY REMOVE THE JUNCTION BOX AND REPAIR SECTION WITH SOLID CONCRETE. DO NOT DISTURB JUNCTION BOXES LOCATED OUTSIDE OF REPAIR AREAS. PAYMENT FOR JUNCTION BOX REMOVAL SHALL BE INCLUDED IN ITEM 202, PORTIONS OF STRUCTURE REMOVED, AS PER PLAN.



LEFT BRIDGE TYPICAL SECTION AT ABUTMENTS
(CONDUIT AND WIRING LOCATED ON LEFT BRIDGE ONLY)



LEFT BRIDGE TYPICAL SECTION OVER PIER
(CONDUIT AND WIRING LOCATED ON LEFT BRIDGE ONLY)



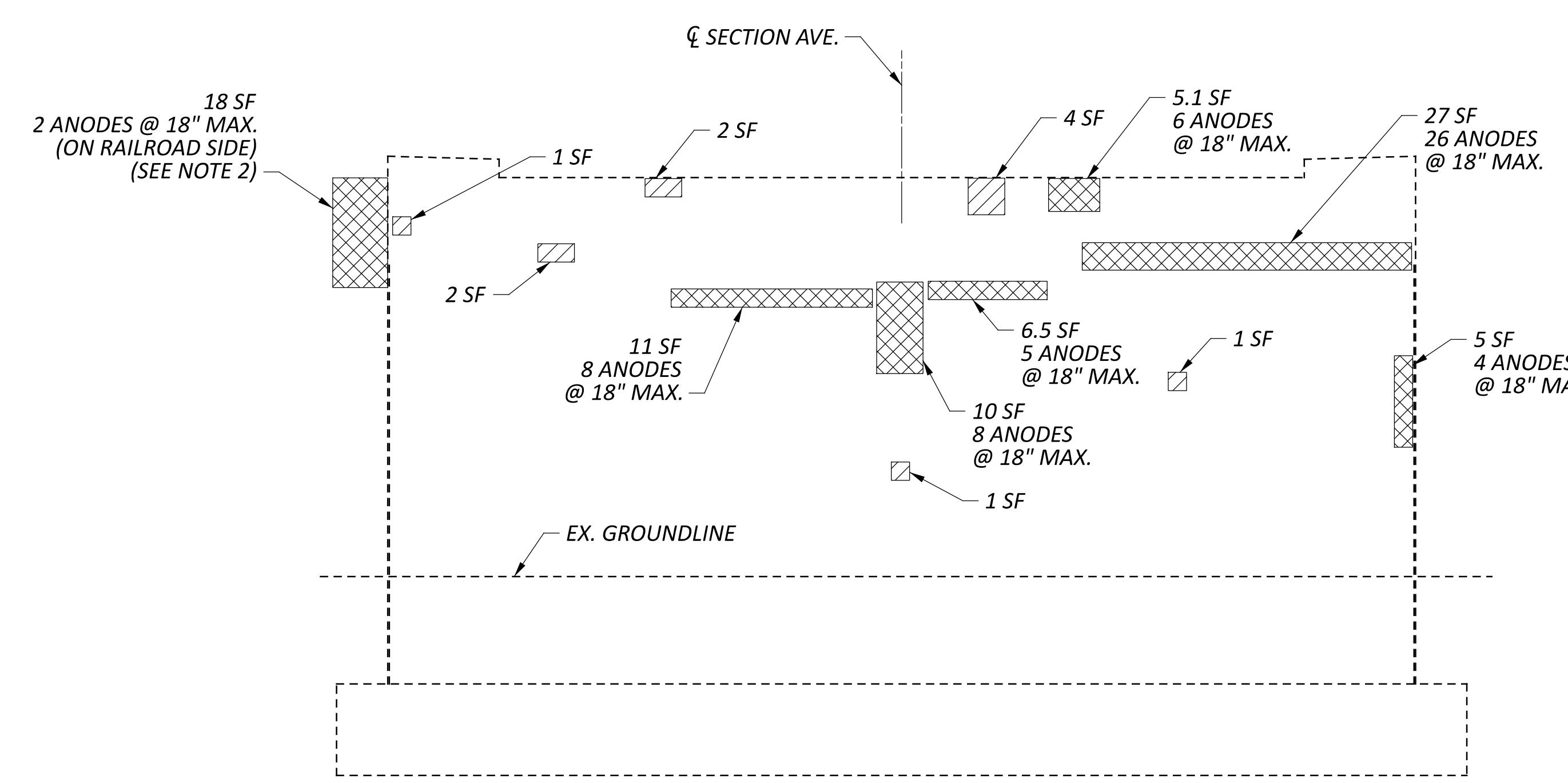
DECK REPAIR

LEGEND:

- ITEM 202, PORTIONS OF STRUCTURE REMOVED, AS PER PLAN

NOTES:

1. REMOVAL OF ALL CONDUIT, ATTACHMENTS, WIRES AND ASSOCIATED HARDWARE FROM PREVIOUS RESEARCH PROJECT SHALL BE INCLUDED WITH ITEM 202, PORTIONS OF STRUCTURE REMOVED, AS PER PLAN.
2. STAINLESS STEEL PINS SHALL BE INCLUDED WITH ITEM 511, CLASS QC2 CONCRETE, SUPERSTRUCTURE, AS PER PLAN.



ABUTMENT 1 ELEVATION
(LOOKING DOWNSTATION)

SUMMARY OF PATCHING AREAS ITEM 519			
LOCATION	MEASURED (SF)	CONTINGENCY	TOTAL (SF)
ABUTMENT 1	11	1.5	17
ABUTMENT 2	6	1.5	9
TOTAL			26

SUMMARY OF PATCHING AREAS ITEM 519 SPECIAL			
LOCATION	MEASURED (SF)	CONTINGENCY	TOTAL (SF)
ABUTMENT 1	87	1.5	131
ABUTMENT 2	0	1.5	0
TOTAL			131

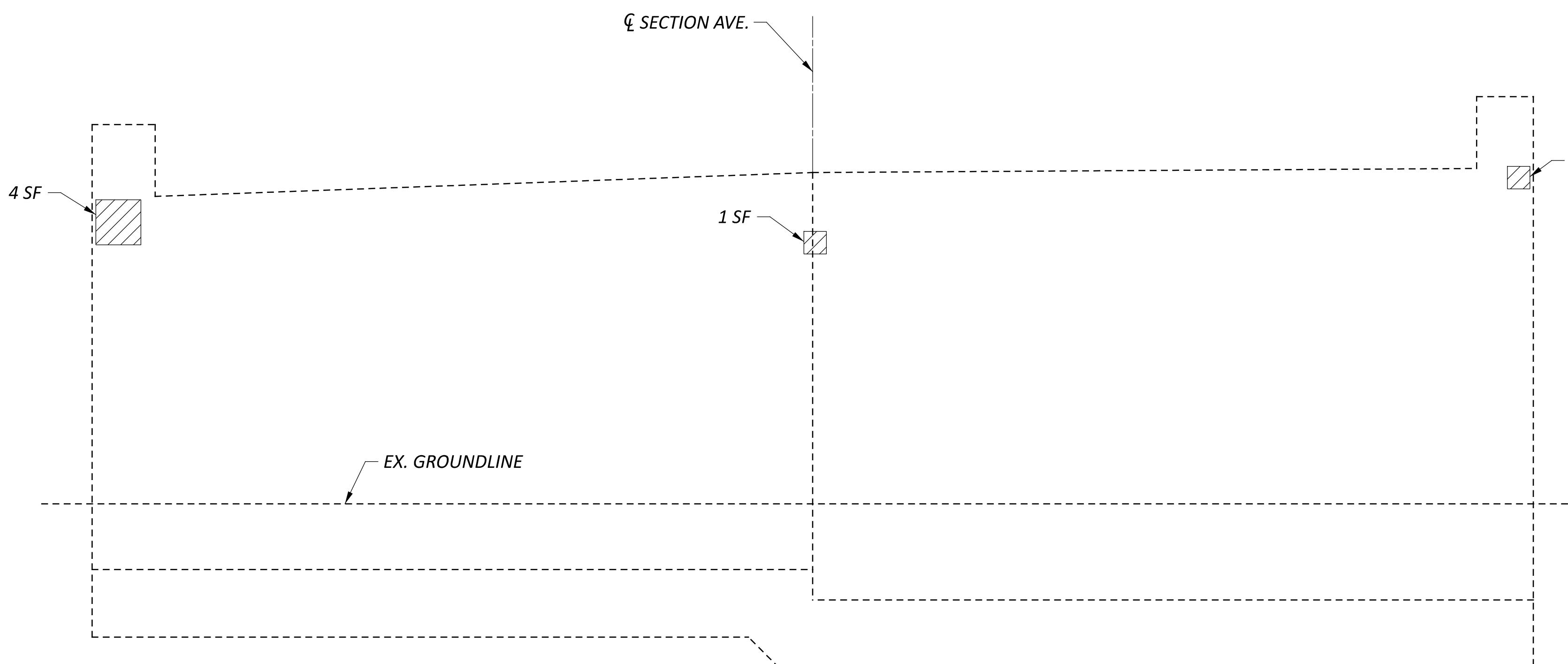
LEGEND:

- INDICATES AREA TO BE REPAIRED PER ITEM 519 - PATCHING CONCRETE STRUCTURE, AS PER PLAN

- INDICATES AREA TO BE REPAIRED PER ITEM 519 - SPECIAL - PATCHING CONCRETE STRUCTURE (GALVANIC ANODE PROTECTION)

NOTES:

1. AN ADDITIONAL 50% CONTINGENCY HAS BEEN ADDED TO THE FIELD MEASURED PATCHING AREAS TO ALLOW FOR ADDITIONAL AREAS OF DETERIORATION. THE FINAL DIMENSIONS AND LOCATION OF THE DETERIORATED AREAS TO BE PATCHED SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER IN THE FIELD FOR FINAL PAYMENT
2. IF THE AREA TO BE REPAIRED EXTENDS UNDER THE EXISTING BEAM SEAT THEN THE EXISTING SUPERSTRUCTURE OVER THE RAILROAD SHALL BE TEMPORARILY JACKED AND SUPPORTED. THE CONCRETE REPAIRS IN THIS AREA SHALL BE PERFORMED DURING THE JACKING OPERATION.
3. APPLY EPOXY URETHANE SEALER TO ALL AREAS THAT HAVE BEEN PATCHED ONLY. USE FEDERAL COLOR NO. 17778.

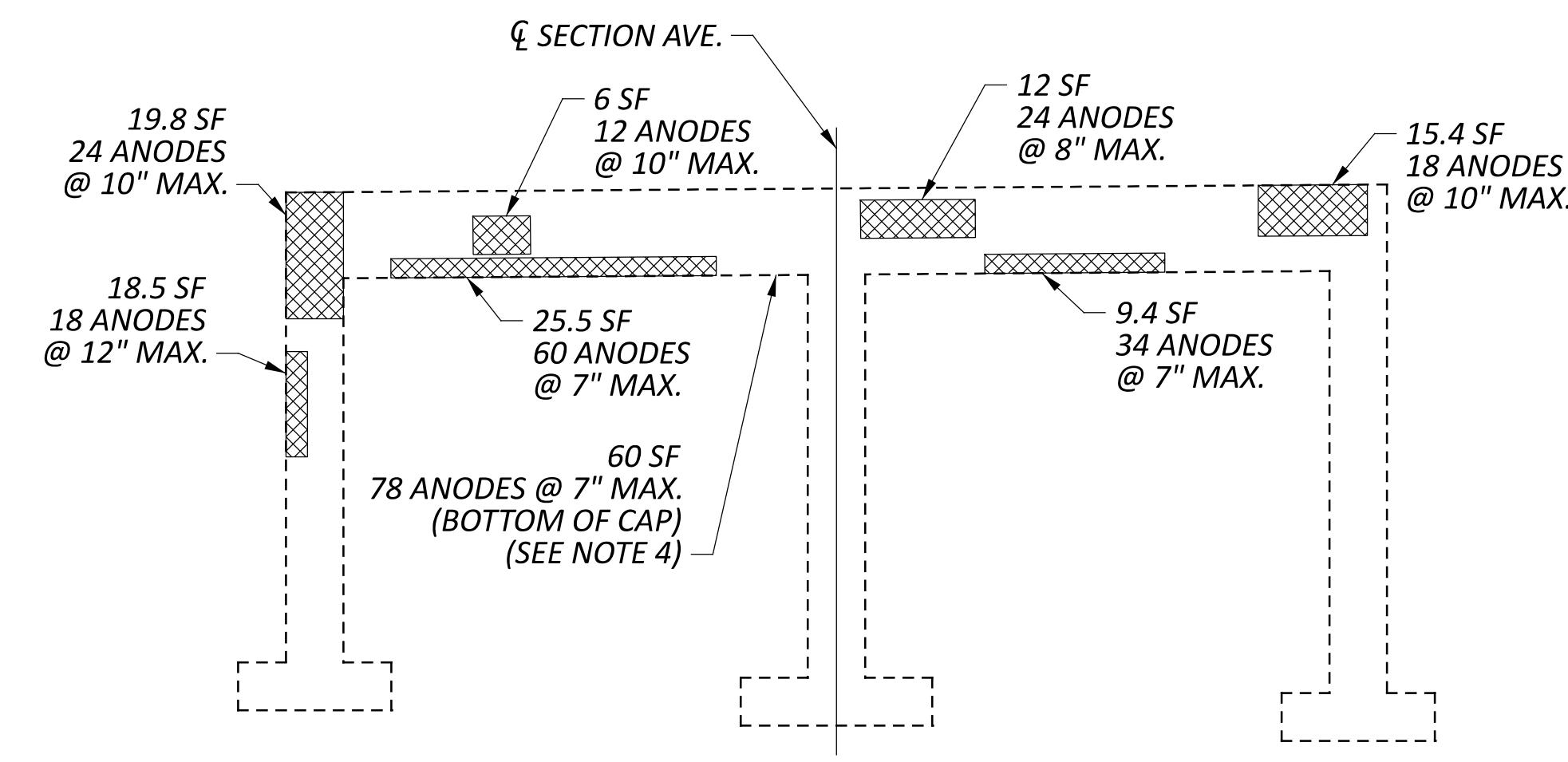


ABUTMENT 2 ELEVATION
(LOOKING UPSTATION)

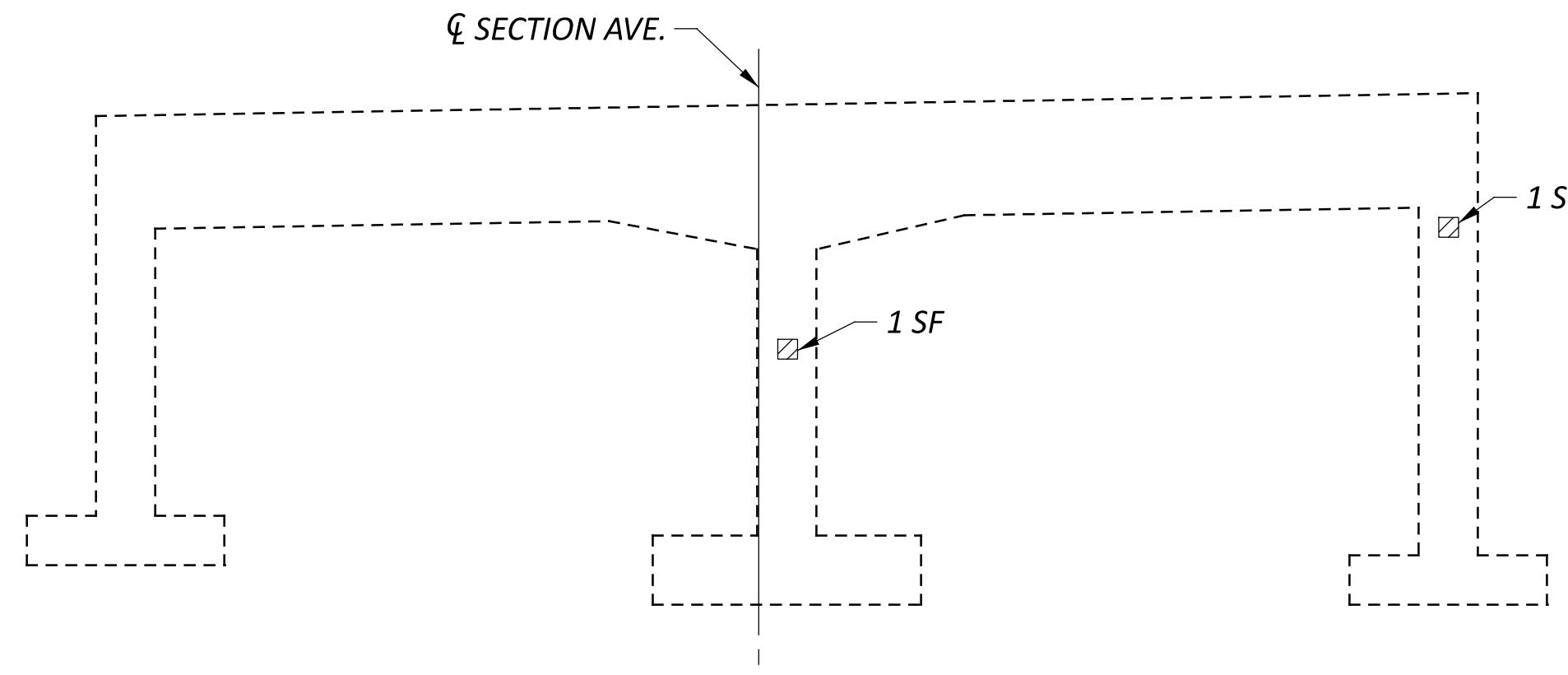
ABUTMENT REPAIR DETAILS
BRIDGE NO. HAM-00562-01.792
SECTION AVENUE OVER SR-562

SFN
3113949
DESIGN AGENCY
fishbeck

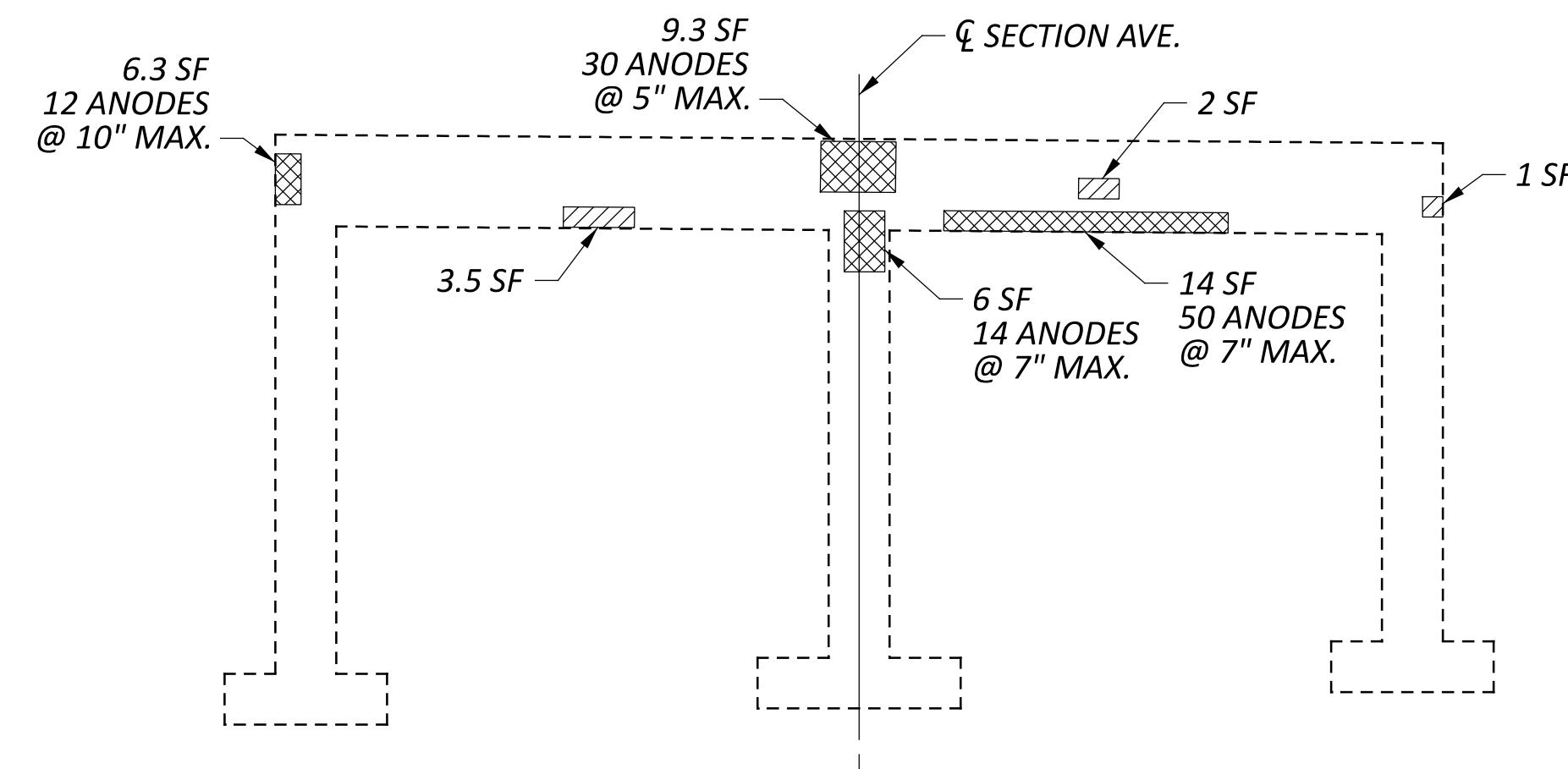
DESIGNER NCS	CHECKER BMG
REVIEWER JPC	02/04/25
PROJECT ID 113006	
SUBSET S.2	TOTAL 3
SHEET P.32	TOTAL 42



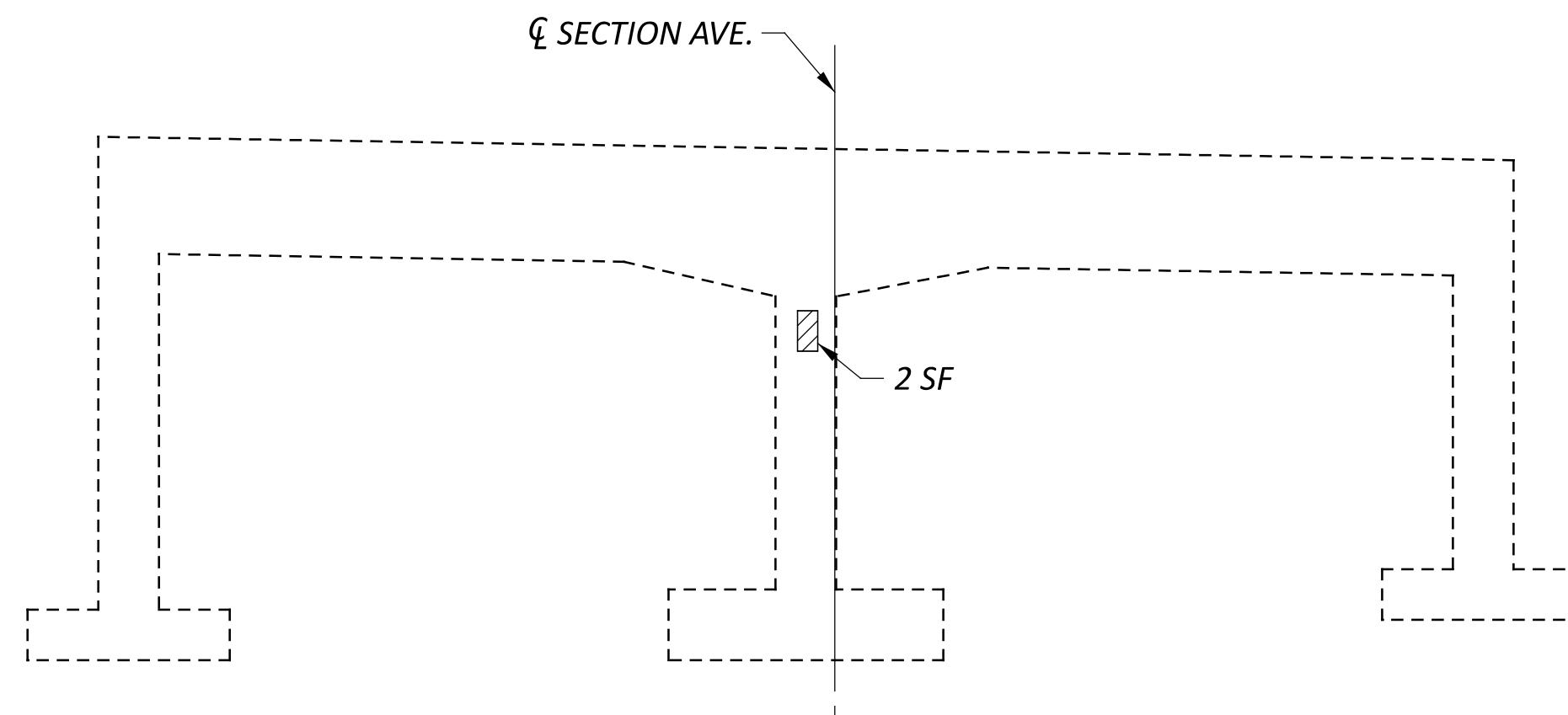
PIER 1 ELEVATION
(LOOKING UPSTATION)
(SEE NOTE 1)



PIER 2 ELEVATION
(LOOKING UPSTATION)



PIER 1 ELEVATION
(LOOKING DOWNSTATION)
(SEE NOTE 1)



PIER 2 ELEVATION
(LOOKING DOWNSTATION)

LEGEND:

- INDICATES AREA TO BE REPAIRED PER ITEM 519 - PATCHING CONCRETE STRUCTURE, AS PER PLAN

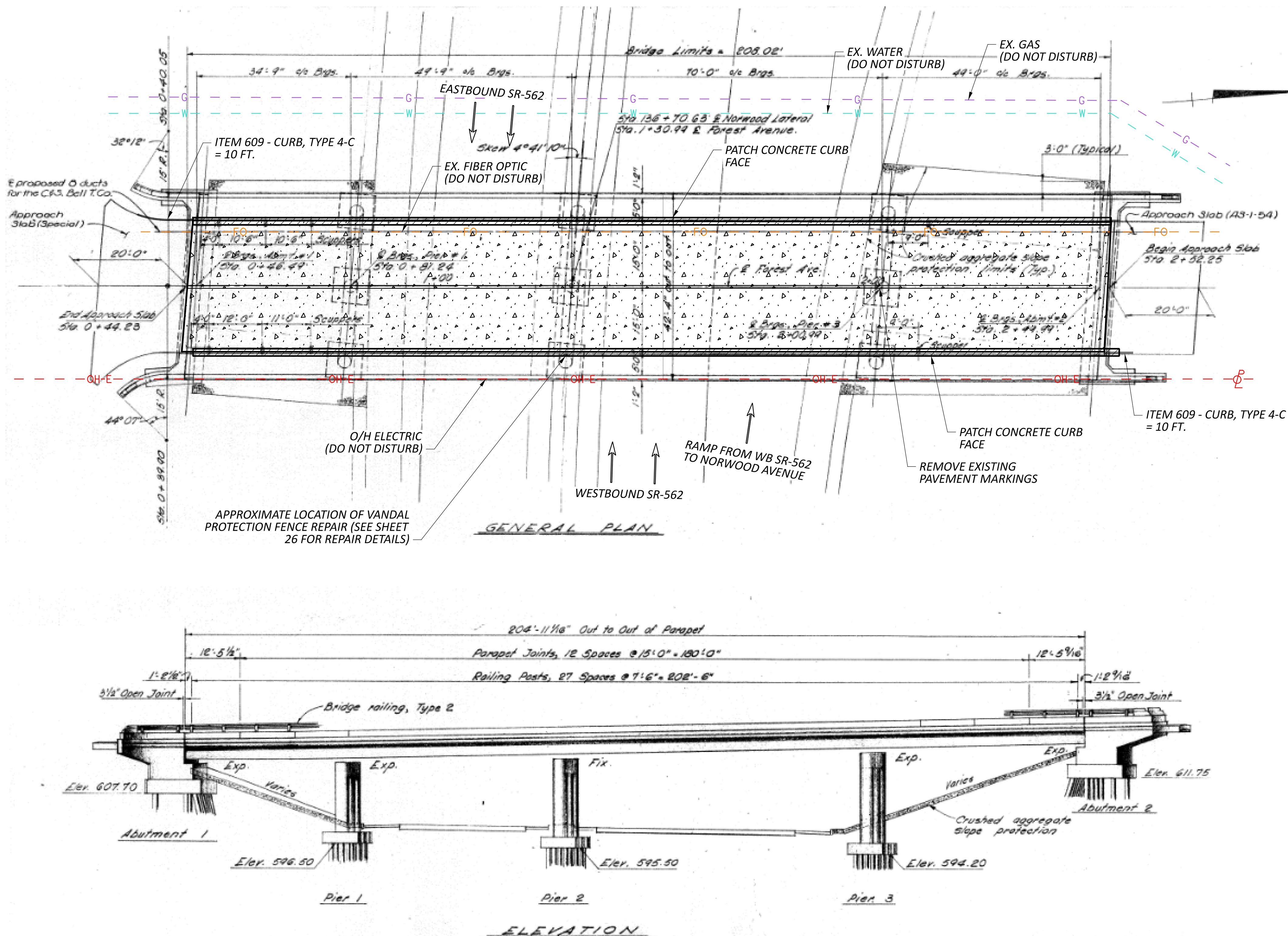
- INDICATES AREA TO BE REPAIRED PER ITEM 519 - SPECIAL - PATCHING CONCRETE STRUCTURE (GALVANIC ANODE PROTECTION)

NOTES:

1. THE REMOVAL LIMITS ON PIER 1 AT ANY GIVEN TIME SHALL BE LIMITED TO A SINGLE FACE AND ADJACENT CORNER OF ONE COLUMN. THE PATCHING MATERIAL SHALL REACH 3000 PSI STRENGTH BEFORE ANOTHER SIDE OR ADJACENT CORNER REMOVAL CAN COMMENCE.
2. AN ADDITIONAL 50% CONTINGENCY HAS BEEN ADDED TO THE FIELD MEASURED PATCHING AREAS TO ALLOW FOR ADDITIONAL AREAS OF DETERIORATION. THE FINAL DIMENSIONS AND LOCATION OF THE DETERIORATED AREAS TO BE PATCHED SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER IN THE FIELD FOR FINAL PAYMENT.
3. APPLY EPOXY URETHANE SEALER TO ALL AREAS THAT HAVE BEEN PATCHED ONLY. USE FEDERAL COLOR NO. 17778.
4. THE PIER CAP IN THIS SPAN SHALL BE TEMPORARILY SUPPORTED DURING THE PATCHING WORK OF THE BOTTOM OF THE PIER CAP. COST SHALL BE INCLUDED WITH ITEM 516, JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN.

SUMMARY OF PATCHING AREAS ITEM 519			
LOCATION	MEASURED (SF)	CONTINGENCY	TOTAL (SF)
PIER 1	6.5	1.5	10
PIER 2	4	1.5	6
TOTAL			16

SUMMARY OF PATCHING AREAS ITEM 519 SPECIAL			
LOCATION	MEASURED (SF)	CONTINGENCY	TOTAL (SF)
PIER 1	202.2	1.5	304
PIER 2	0	1.5	0
TOTAL			304



NOTES:

**DETAILS ON THIS SITE PLAN SHEET ARE FROM ARCHIVED PLANS AND
SHOULD BE USED FOR REFERENCE ONLY**

DESIGN TRAFFIC:

FOREST AVENUE

2023 ADT = 2,144 2023 ADTT = 107
DESIGN SPEED = 25 MPH *LEGAL SPEED = 25 MPH*
DESIGN FUNCTIONAL CLASSIFICATION: 05-MAJOR COLLECTOR (URBAN)
NHS ROUTE: NO

HAM-SR562-2.53

2027 ADT = 72,500 2027 ADTT = 2,900
2039 ADT = 80,500 2039 ADTT = 3,220
DIRECTIONAL DISTRIBUTION = 0.55
DESIGN SPEED = 55 MPH LEGAL SPEED = 55 MPH
DESIGN FUNCTIONAL CLASSIFICATION: 02-PRINCIPAL ARTERIAL FREEWAY
NHS ROUTE: YES

LEGEND:

- ITEM 519, PATCHING CONCRETE STRUCTURE, AS PER PLAN
- ITEM 512, TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN
- ITEM 609, CURB, TYPE 4-C

EXISTING STRUCTURE

TYPE: CONTINUOUS ROLLED STEEL BEAM AND WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE

SPANS: 34'-9"±, 49'-9"±, 70'-0"±, 49'-0"±

ROADWAY: 30'-0"± F/F CURB WITH 5'-0"± SIDEWALKS

LOADING: C.F. = 2000 (57)

SKEW: 4°41'10"± LF

WEARING SURFACE: 1 1/4"± MICRO-SILICA MODIFIED CONCRETE OVERLAY

APPROACH SLABS: AS-1-67 (20-0"± LONG)

ALIGNMENT: TANGENT

CROWN: 0.016± FT/FT

STRUCTURE FILE NUMBER: 3114023

DATE BUILT: 1969, REHABILITATED 1990 & 1993

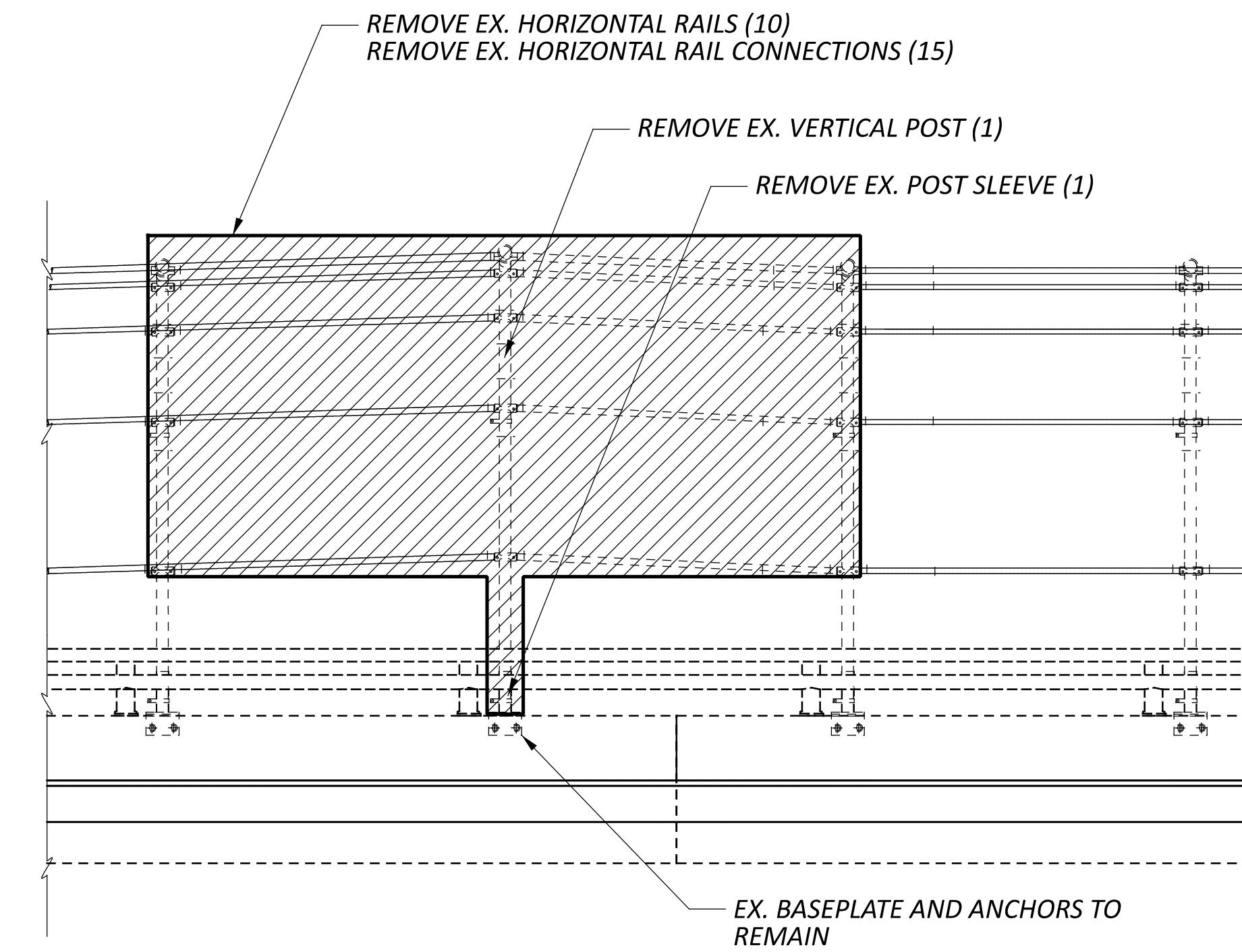
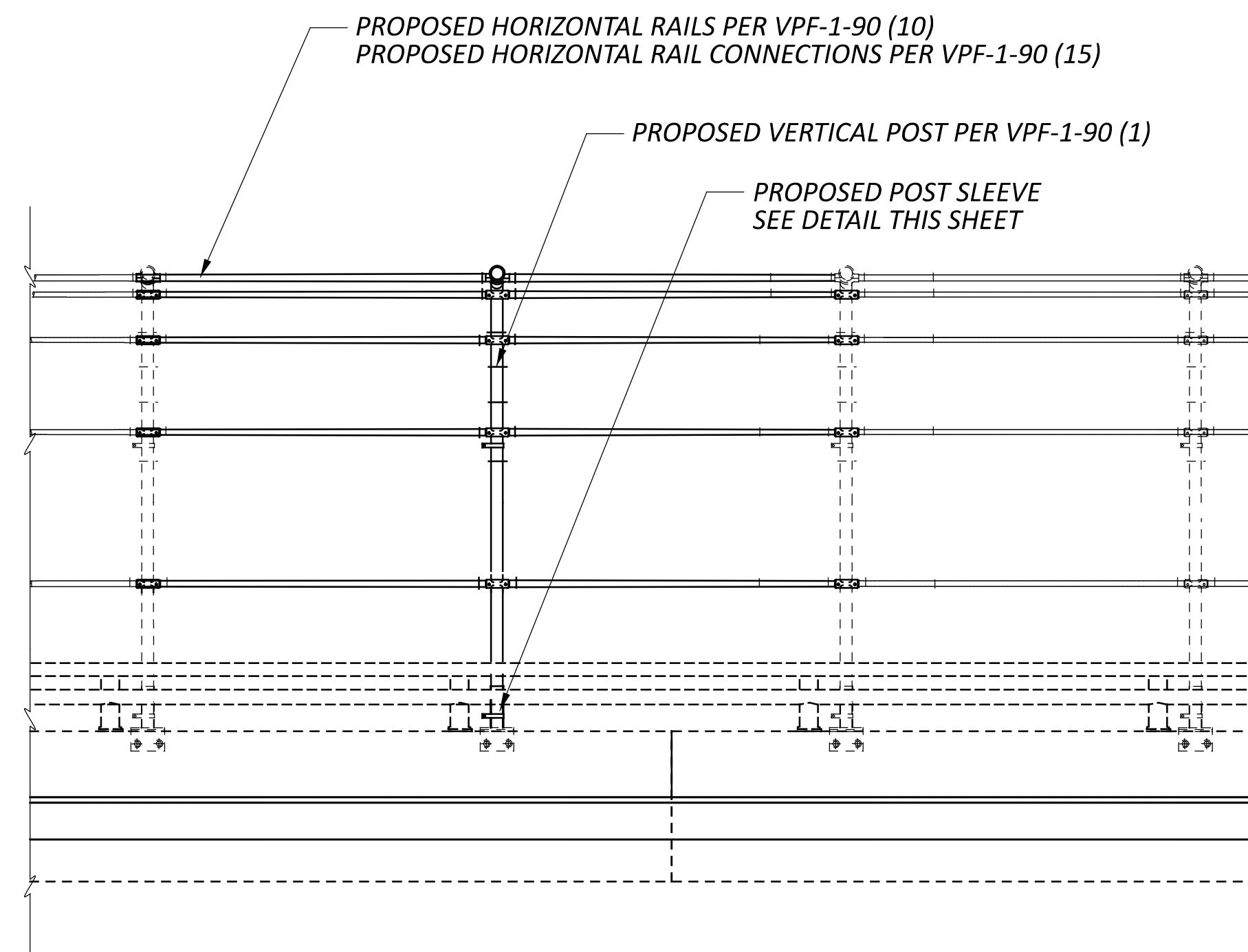
DISPOSITION: SEE PROPOSED WORK

PROPOSED WORK

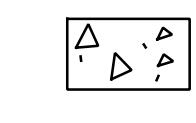
1. *REPAIR THE DETERIORATED CURB FACE OF THE SIDEWALK ON THE BRIDGE PER 519 PATCHING.*
2. *REMOVE AND REPLACE EXISTING SIDEWALK EXPANSION JOINT COVER PLATES.*
3. *REPLACE THE DETERIORATED PORTIONS OF THE CURB ON THE APPROACH SLABS.*
4. *SEAL THE WEARING SURFACE WITH GRAVITY FED RESIN.*
5. *REPLACE DAMAGED PORTIONS OF THE VANDAL PROTECTION FENCE.*

SFN
3114023
DESIGN AGENCY
fishbeck

DESIGNER	CHECKER
NCS	BMG
REVIEWER	
JPC	02/04/25
PROJECT ID	
113006	
SUBSET	TOTAL
S.1	2
SHEET	TOTAL
P.34	42

**VANDAL PROTECTION FENCE REMOVAL DETAIL****VANDAL PROTECTION FENCE CONSTRUCTION DETAIL****LEGEND**

△ - PROVIDE A $\frac{1}{8}$ " HORIZONTAL x $\frac{1}{4}$ " VERTICAL BEVEL AT THE EXPOSED EDGE OF THE $\frac{1}{2}$ " COVER PLATE AND $\frac{1}{2}$ " X 1" BAR

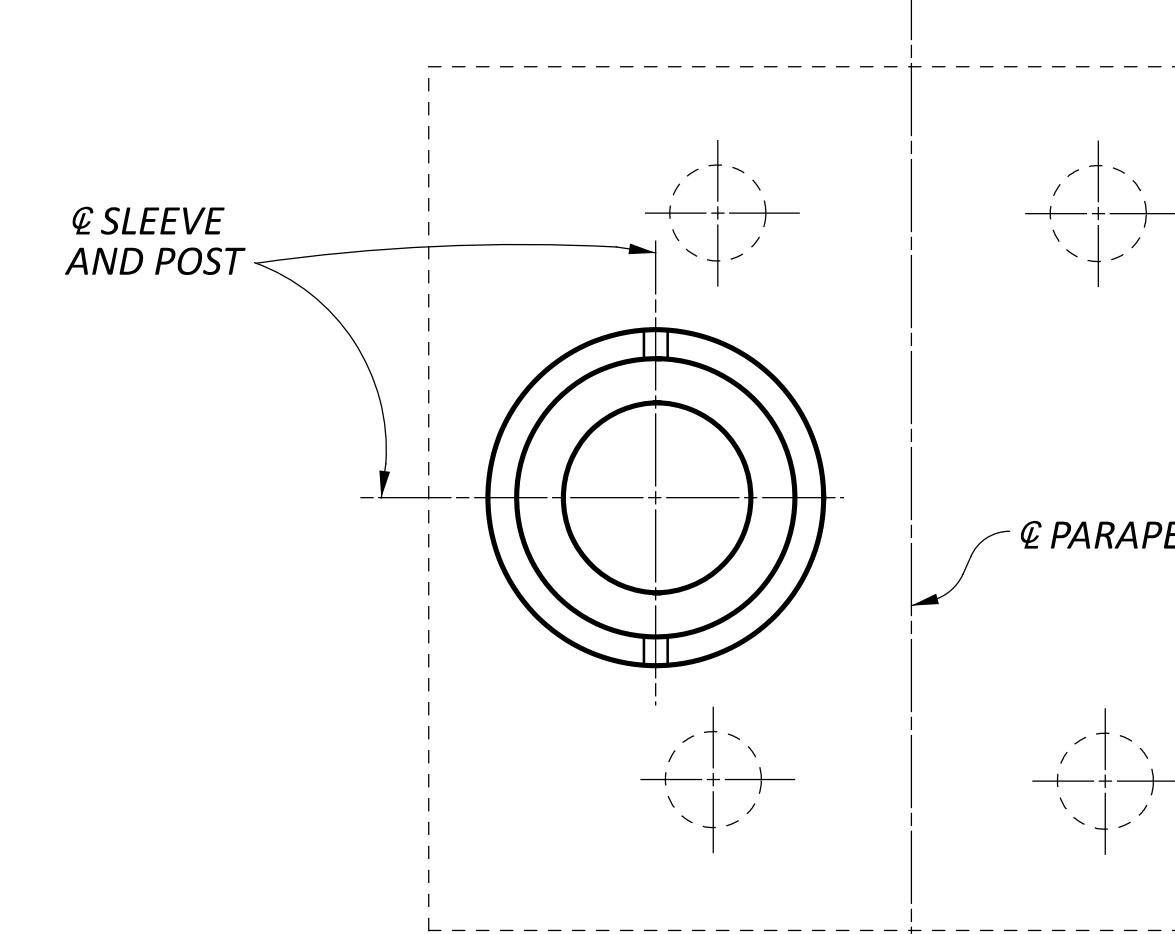
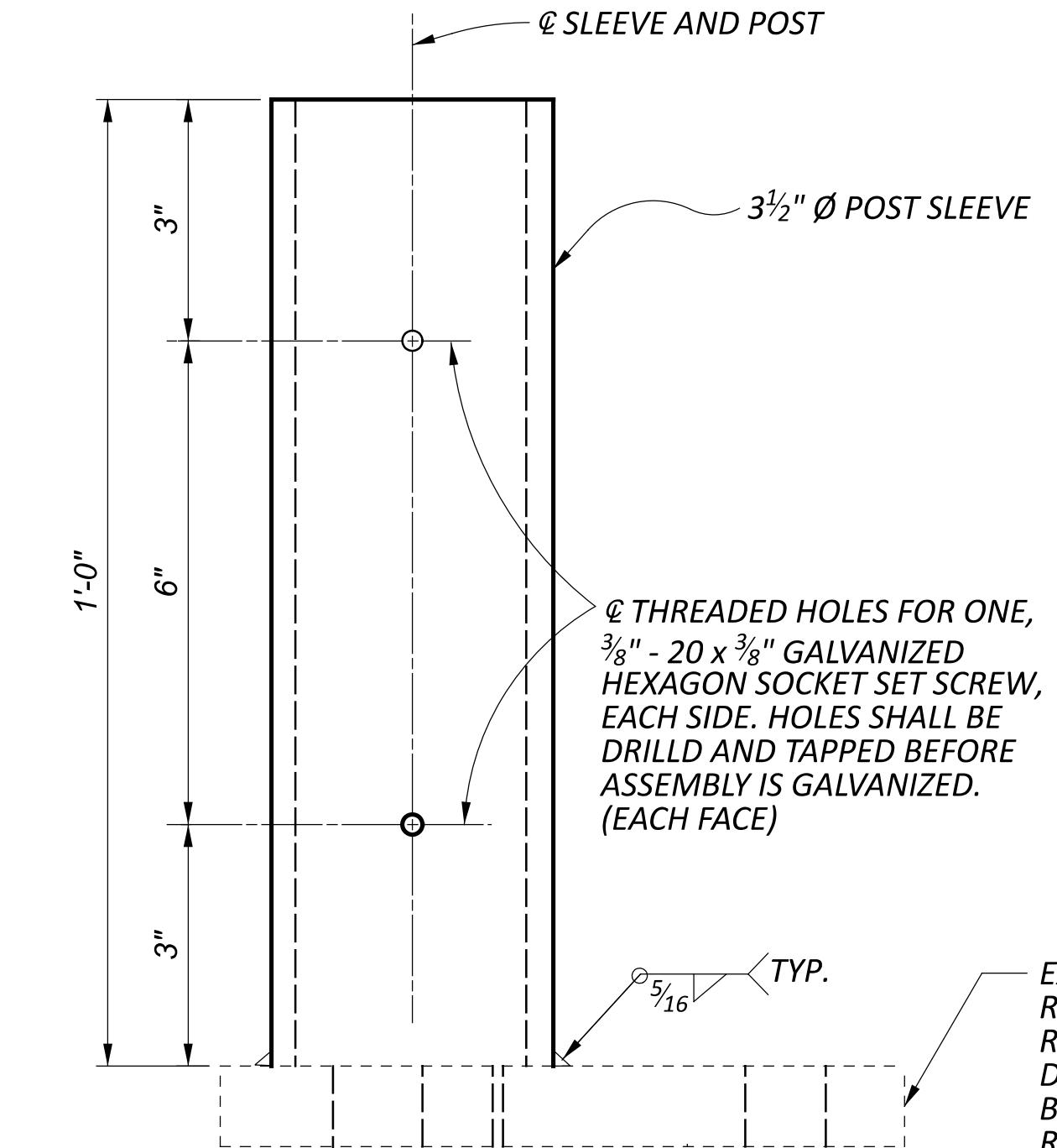
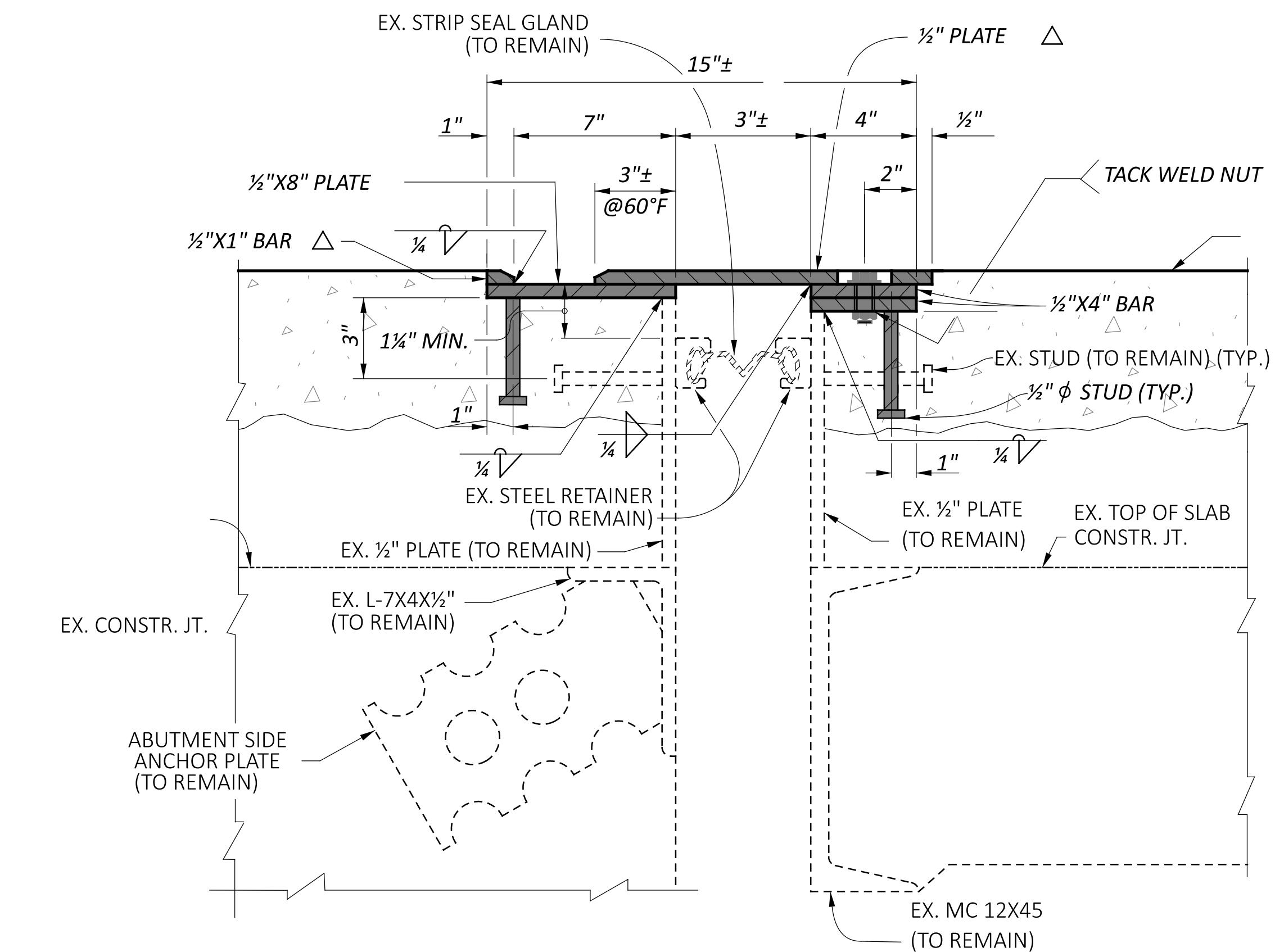


- ITEM 519 - PATCHING CONCRETE STRUCTURE

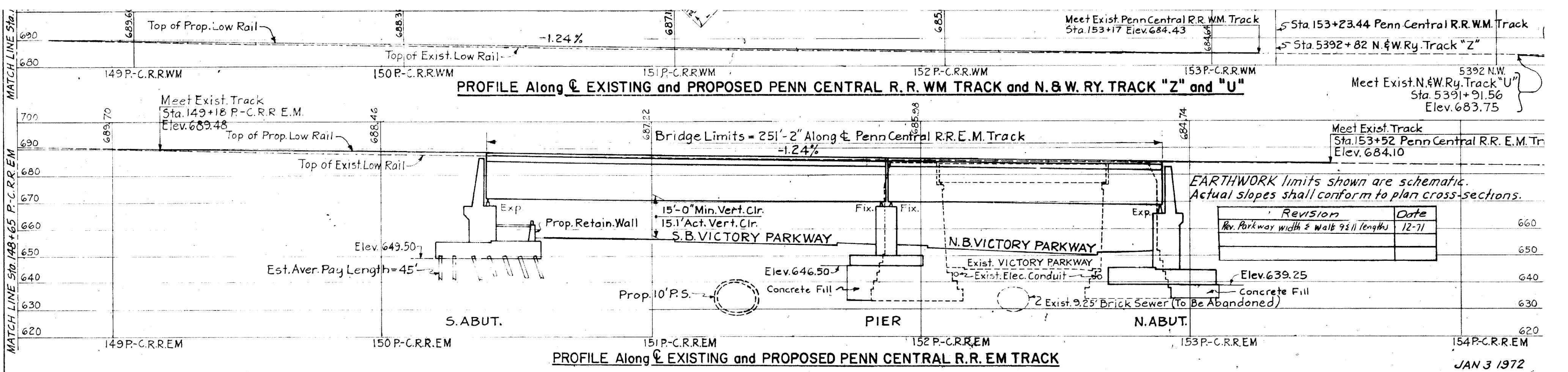
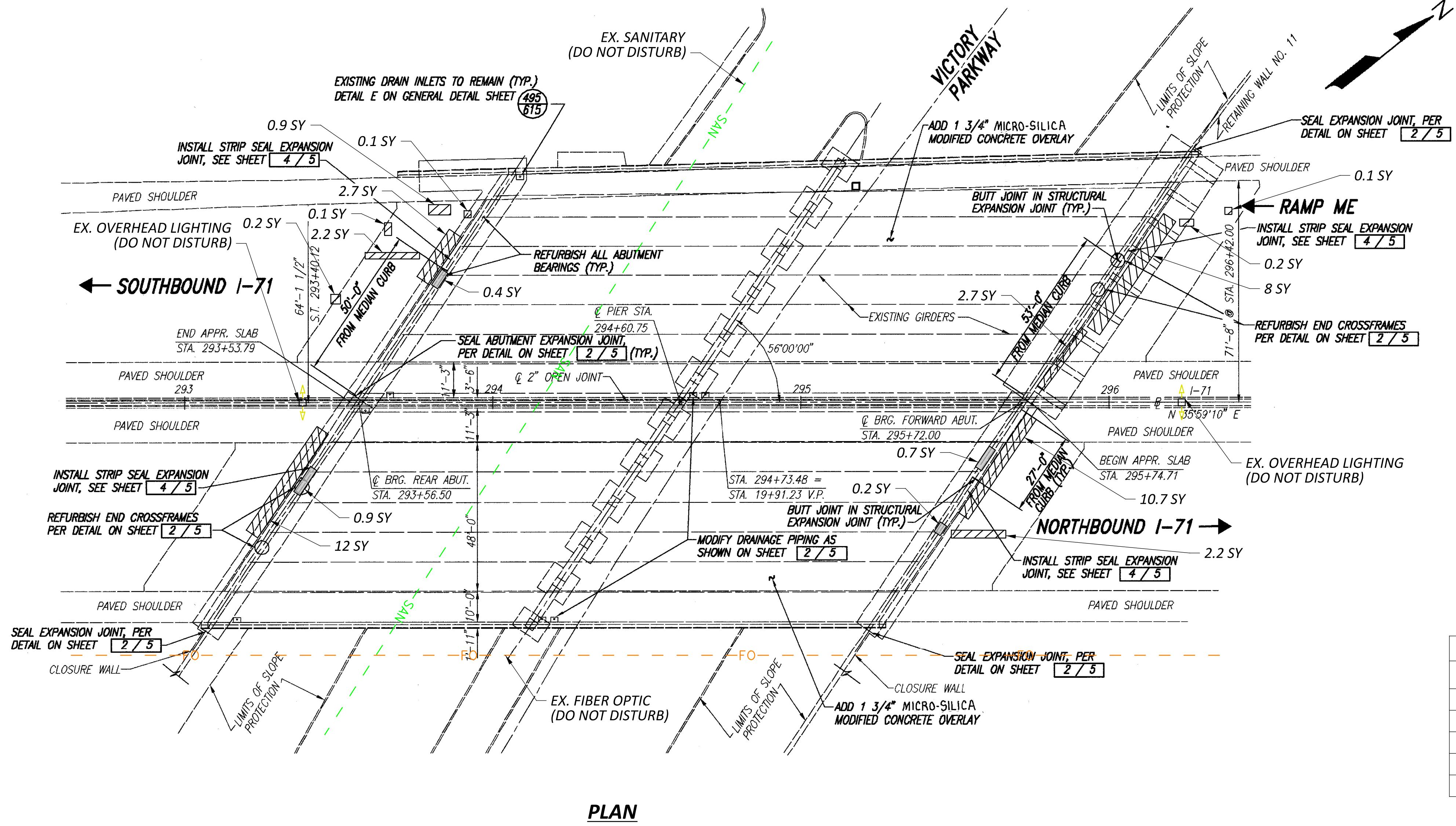
▨ - ITEM 516 - SIDEWALK COVER PLATE



- ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN

**POST SLEEVE PLAN**
POST AND SET SCREWS NOT SHOWN**POST SLEEVE ELEVATION**
POST AND SET SCREWS NOT SHOWN**SIDEWALK COVER PLATE REPLACEMENT DETAIL****NOTES:**

1. FOR ADDITIONAL VANDAL PROTECTION FENCE DETAILS, SEE ODOT STANDARD DRAWING VPF-1-90
2. FENCE REMOVAL AND REPLACEMENT WORK SHALL OCCUR DURING THE SAME WORKING SHIFT.
3. PERFORM SIDEWALK COVER PLATE REPLACEMENT AT BOTH FORWARD AND REAR ABUTMENTS ON THE RIGHT AND LEFT SIDE FOR FULL WIDTH OF EXISTING SIDEWALK EXPANSION JOINT ARMOR

**NOTES:**

1. AN ADDITIONAL 50% CONTINGENCY HAS BEEN ADDED TO THE FIELD MEASURED PATCHING AREAS TO ALLOW FOR ADDITIONAL AREAS OF DETERIORATION. THE FINAL DIMENSIONS AND LOCATION OF THE DETERIORATED AREAS TO BE PATCHED SHALL BE DETERMIENED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER IN THE FIELD FOR FINAL PAYMENT.

2. DETAILS ON THIS SITE PLAN SHEET ARE FROM ARCHIVED PLANS AND SHOULD BE USED FOR REFERENCE ONLY.

DESIGN TRAFFIC:**HAM-71-0450**

2027 ADT = 149,000 2027 ADTT = 16,390
2039 ADT = 163,000 2039 ADTT = 17,930

DIRECTIONAL DISTRIBUTION = 0.70

DESIGN SPEED = 55 MPH LEGAL SPEED = 55 MPH

DESIGN FUNCTIONAL CLASSIFICATION: 01-PRINCIPAL ARTERIAL INTERSTATE (URBAN)
NHS ROUTE: YES

VICTORY PARKWAY

2023 ADT = 12,156 2023 ADTT = 197

DESIGN SPEED = 35 MPH LEGAL SPEED = 35 MPH

DESIGN FUNCTIONAL CLASSIFICATION: 04-MINOR ARTERIAL (URBAN)
NHS ROUTE: NO

LEGEND:

- INDICATES APPROACH SLAB AREA TO BE REPAIRED PER ITEM 519 - PATCHING CONCRETE BRIDGE DECK - TYPE B (PER PN 512)

- INDICATES BACKWALL AREA TO BE REPAIRED PER ITEM 519 - PATCHING CONCRETE BRIDGE DECK - TYPE B (PER PN 512)

SUMMARY OF PATCHING AREAS ITEM 519			
LOCATION	MEASURED (SY)	CONTINGENCY	TOTAL (SY)
NORTHERN APPROACH SLAB	23.9	1.5	36
SOUTHERN APPROACH SLAB	18.2	1.5	28
NORTHERN BACKWALL	0.9	1.5	2
SOUTHERN BACKWALL	1.3	1.5	2
TOTAL			68

EXISTING STRUCTURE

TYPE: CONTINUOUS STEEL PLATE GIRDERS WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE

SPANS: 104'-3"± AND 111'-3"± C/C BEARINGS

ROADWAY: VARIES; 148'-3"± AVERAGE WIDTH FACE TO FACE OF PARAPET

LOADING: HS 20-44 AND THE INTERSTATE ALTERNATE LOADING

SKW: 34°00'0"± LF

WEARING SURFACE: 1 1/4"± MICRO-SILICA MODIFIED CONCRETE OVERLAY

APPROACH SLABS: AS-1-67 (30'-0"± LONG)

ALIGNMENT: 1°45'0"± CURVE LEFT & 4°00'0"± CURVE LEFT

SUPERELEVATION: VARIES

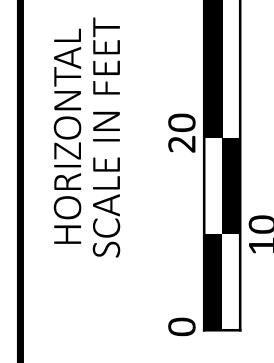
STRUCTURE FILE NUMBER: 3114562

DATE BUILT: 1972, REHABILITATED 1995 & 2008

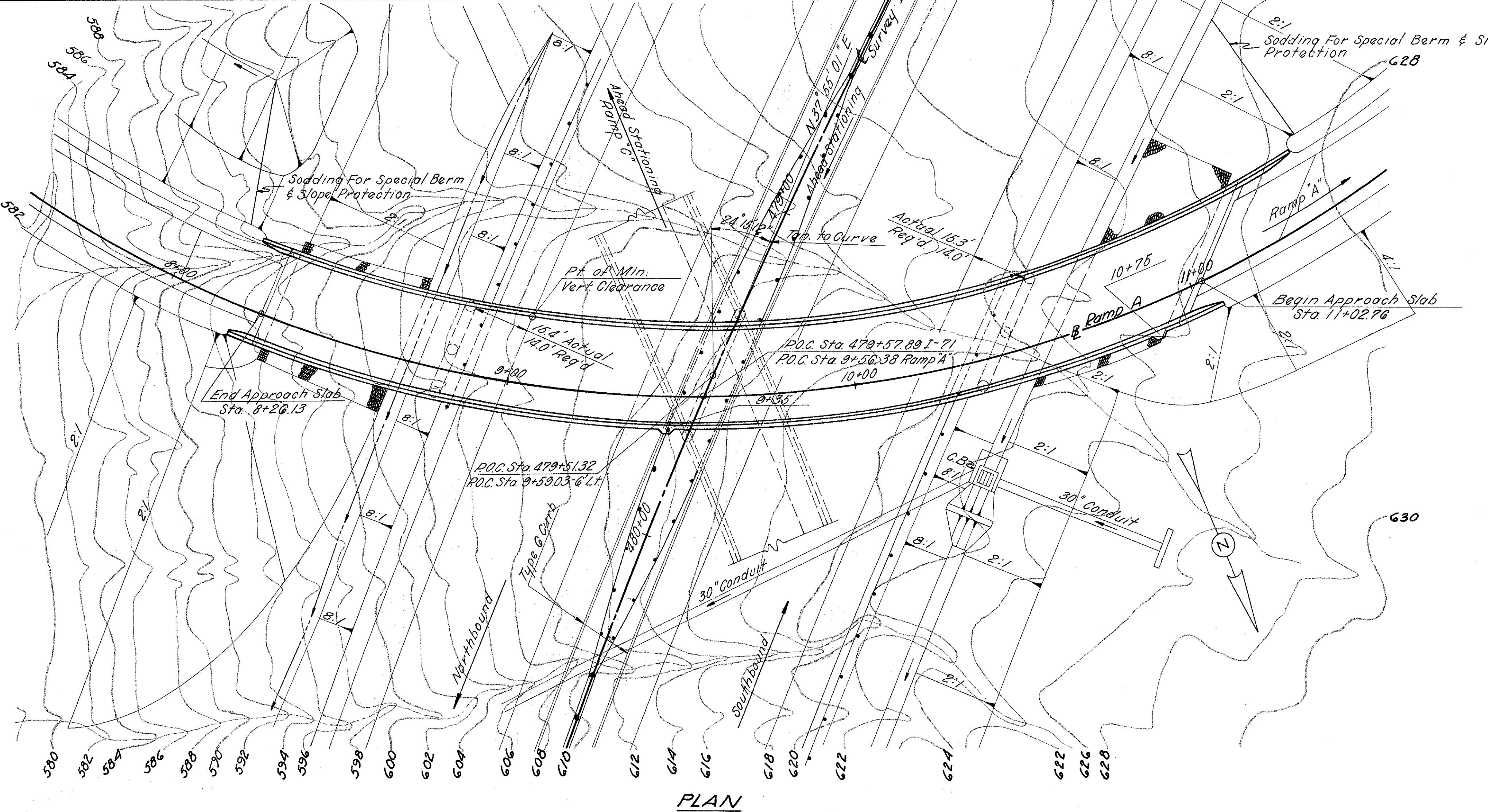
DISPOSITION: SEE PROPOSED WORK

PROPOSED WORK

- PATCH SPALLED AREAS ON TOP OF BACKWALLS ALONG EXPANSION JOINTS, AND APPROACH SLABS WITH CONCRETE PER PROPOSAL NOTE 512 (TYPE B)



REFERENCE M.P.C. STA. 478+0000
REFERENCE C.S. STA. 485+4300
B.M. top of 6" I-Beam Guard Rail Post @ End of S.W. Wingwall of Bridge on Erdling Road 175' Rt. of Sta. 483+80
Elev. 578.50



This diagram is a Partial Schematic Framing Plan for a bridge structure. It features a horizontal bridge deck supported by vertical piers. Key elements include:

- SB I-71** (Southbound Interstate 71) is shown as a downward-pointing arrow.
- NB I-71** (Northbound Interstate 71) is shown as an upward-pointing arrow.
- EDGE OF PAVEMENT** is indicated by a dashed line with arrows.
- Φ PIER 3** is labeled near the left side.
- Φ I-71 & C** is labeled at the top right.
- 22°15'12"** is the angle of the bridge deck relative to the horizontal.
- B** is a dimension line indicating a width of 10' 0" (120") between two vertical piers.
- C** is a dimension line indicating a width of 10' 0" (120") between two vertical piers.
- H** is a dimension line indicating a width of 10' 0" (120") between two vertical piers.
- J** is a dimension line indicating a width of 10' 0" (120") between two vertical piers.
- K** is a dimension line indicating a width of 10' 0" (120") between two vertical piers.
- L** is a dimension line indicating a width of 10' 0" (120") between two vertical piers.

PARTIAL SCHEMATIC FRAMING PLATE

The diagram illustrates a truss structure within a rectangular frame. The top horizontal member is labeled 'STRUT 4S'. The bottom horizontal member is labeled 'STRUT 3S'. Two diagonal members, labeled 'DIAGONAL 1D' and 'DIAGONAL 2D', intersect in the center of the truss. A circular callout in the bottom-left corner highlights a joint or connection point.

*REMOVE ACCORDING TO ITEM 202-PORTIONS OF SECONDARY
MEMBERS REMOVED, AS PER PLAN. REPLACE BY MATCHING
EXISTING DETAIL. SEE GSD-1-96 FOR ADDITIONAL CLARIFICATION*

*MAIN AND SECONDARY MEMBER DAMAGE IS
NOT SHOWN. FOR CLARITY SEE SECTION A-A
SEE SHEET XX FOR ADDITIONAL COLLISION REPAIR NOTE*

SECTION B-

SECONDARY MEMBER BAY No. M

N - NUMBER OF CROSSFRAME BRACES COUNTED FROM THE PIER OR ABUTMENT IDENTIFIED IN TABLE

TABLE #2 DAMAGED SECONDARY MEMBER TO BE REPLACED (LBS)						
CROSSFRAME BAY M	PIER/ABUT.	N	1D	2D	3S	4S
3	2	5	55	NA	NA	NA
3	2	6	55	55	53	53

The diagram illustrates a beam section with a flange and a web. The flange is shown in two states: a horizontal dashed line and a tilted solid line. The angle between these lines is labeled F_1 and F_2 , representing flange tilt. The web is shown in a curved, buckled shape, labeled G WEB BUCKEL. A vertical dashed line represents the center line of the beam. The distance between the center line and the flange is labeled E , representing the center line offset. Arrows indicate the magnitude of the flange tilt and the center line offset.

SECTION A-

*NEGATIVE E VALUES ARE BENT LEFT
NEGATIVE F VALUES ARE BENT DOWN
NEGATIVE G VALUES ARE BENT LEFT*

ORIENTATION NOTE
ABUTMENTS AND PIERS ARE NUMBERED IN THE CARDINAL DIRECTION (FROM SOUTH TO NORTH OR WEST TO EAST). BEAMS ARE NUMBERED FROM LEFT TO RIGHT WHEN FACING IN THE CARDINAL DIRECTION.
BAYS ARE NUMBERED TO MATCH THE MAIN MEMBERLINE NUMBER TO THE LEFT OF THE CROSSFRAME BAY WHEN FACING IN THE CARDINAL DIRECTION.

CROSSFRAME NOTE
*REMOVAL AND REPLACEMENT OF PORTIONS OF THE
CROSSFRAME IS PERMITTED IN KIND WITH APPROVAL OF
THE ENGINEER, BUT AT NO ADDITIONAL COST TO THE
PROJECT.*

The diagram illustrates a bridge pier (A) with a longitudinal web. The pier is labeled 'A' at the top and bottom. The longitudinal web is labeled 'E' at the top right. The diagram shows the longitudinal web offset from the pier centerline. Key points labeled are 'B', 'C', 'D', and 'E'. Point 'B' is at the bottom left, point 'C' is in the center, point 'D' is at the bottom right, and point 'E' is at the top right. A horizontal line labeled 'TANGENT' is shown at point 'C'. A vertical line labeled 'TANGENT' is shown at point 'E'. A dashed line labeled 'PIER/ABUTMENT IDENTIFIED IN THE TABLE' is shown at point 'B'. A label 'MAIN MEMBER N' is at the top left. A label 'Q MAXIMUM OFFSET' is at the top center. A label 'Q OF WEB' is at the top right.

EXISTING STRUCTURE: HAM-71-0992
ROUTE ON STRUCTURE: RAMP A, RED BANK RD. TO SB I-71
ROUTE BELOW STRUCTURE: I-71
TYPE: CONTINOUS STEEL BEAM WITH REINFORCED CONCRETE
DECK & SUBSTRUCTURE
SPANS: 53'-0", 75'-0", 84'-0", 59'-0"
ROADWAY WIDTH: 27'-0" TOE/TOE OF PARAPET
SKEW: 22°-15'-12" LT. FORWARD
ALIGNMENT: 17° HORIZONTAL CURVE TO THE LEFT
SUPERELEVATION: 0.083 FT/FT
YEAR BUILT: 1969
NUMBER OF BEAMS: 4
STEEL TYPE: ASTM-A36
PAINT TYPE: OZEU
PAINT DATE: 1996

COLLISION REPAIR AND HEAT STRAIGHTENING PLAN
BRIDGE NO. HAM-71-0992
RAMP A FROM REDBANK EXPRESSWAY TO I-71 SOUTHBBOUND

SFN
3115372
DESIGN AGENCY

DESIGNER	CHECKER
GTF	JAB
REVIEWER	
CAH	6-05-25
PROJECT ID	
113006	
SUBSET	TOTAL
2	3
SHEET	TOTAL
P.38	42

1. DETERMINE IF IMPACT NOTCH IS CRACKED USING MAGNETIC PARTICLE INSPECTION
2. IF CRACK DOES NOT EXTEND THROUGH THE FLANGE. DETERMINE DEPTH OF CRACK BY GRINDING
3. IF NOTCH OR PARTIAL DEPTH CRACK CAN BE REMOVED BY GRINDING ACCORDING TO SUPPLEMENTAL SPECIFICATION 849, REPAIR DAMAGED MEMBERS. PERFORM GRINDING ACCORDING TO SUPPLEMENTAL SPECIFICATION 849 AND AS ILLUSTRATED IN DETAIL FC2-2. IF THE CRACK SHALL GROW TO $\frac{5}{8}$ " OR MORE ON EITHER SURFACE DUE TO HEAT STRAIGHTENING, THE CRACK SHALL BE WELDED.
4. IF NOTCH OR PARTIAL DEPTH CRACK MUST BE REPAIRED BY WELDING ACCORDING TO SUPPLEMENTAL SPECIFICATION 849 REPAIRING DAMAGED MEMBERS, AS ILLUSTRATED IN DETAIL FC2-3. PERFORM COMPLETE PENETRATION WELDING ACCORDING TO C&MS 513.21 BY ATTACHING RUN OFF TABS AND GRIND ALL WELDED SURFACES SMOOTH ACCORDING TO ANSI B46.1 OF 250 mil
5. AN INDEPENDENT TESTING AGENCY SHALL PERFORM NDT TESTING ACCORDING TO C&MS 513.25A. THIS WORK SHALL BE INCLUDED WITH THE PAYMENT FOR COMPLETE PENETRATION WELDING.

TABLE #3 513 REPAIRS

SEE PARTIAL FRAMING PLAN FOR DIMENSION C

***ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN
(SECONDARY MEMBERS):***

(SECONDARY MEMBERS).
AN ESTIMATED QUANTITY FOR EACH LOCATION IS PROVIDED IN TABLE 2, FOR REMOVAL OF SECONDARY MEMBERS AS DETERMINED BY FIELD INSPECTION ACCORDING TO ITEM 849, DAMAGE ASSESSMENT OR AS DIRECTED BY THE ENGINEER. SUPPORT THE EXISTING SECONDARY MEMBERS ACCORDING TO ITEM 849, STRAIGHTENING WORK PLAN. FLAME OR SAW CUT THE EXISTING MEMBERS TO WITHIN $\frac{1}{8}$ INCH OF THE EXISTING MAIN MATERIAL USING A MECHANICAL GUIDE ACCORDING TO C&MS 513.12 PROVIDE SHIELDING AS NECESSARY TO PREVENT DAMAGE TO MAIN OR SECONDARY MATERIALS THAT REMAIN. GRIND THE EXISTING MAIN OR SECONDARY MEMBER SMOOTH IN PREPARATION FOR COMPLETE PENETRATION OR FILLET WELDING. PROVIDE A SURFACE FINISH ACCORDING TO ANSI B46.1 OF 250 MIL (TO ACCOMMODATE THE PROPOSED REPLACEMENT MATERIALS). DETERMINE FINAL QUANTITIES BY FIELD MEASUREMENTS. THE DEPARTMENT WILL INCLUDE ALL MATERIALS, TOOLS, LABOR, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK FOR PAYMENT WITH ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN (SECONDARY MEMBERS): POUND.

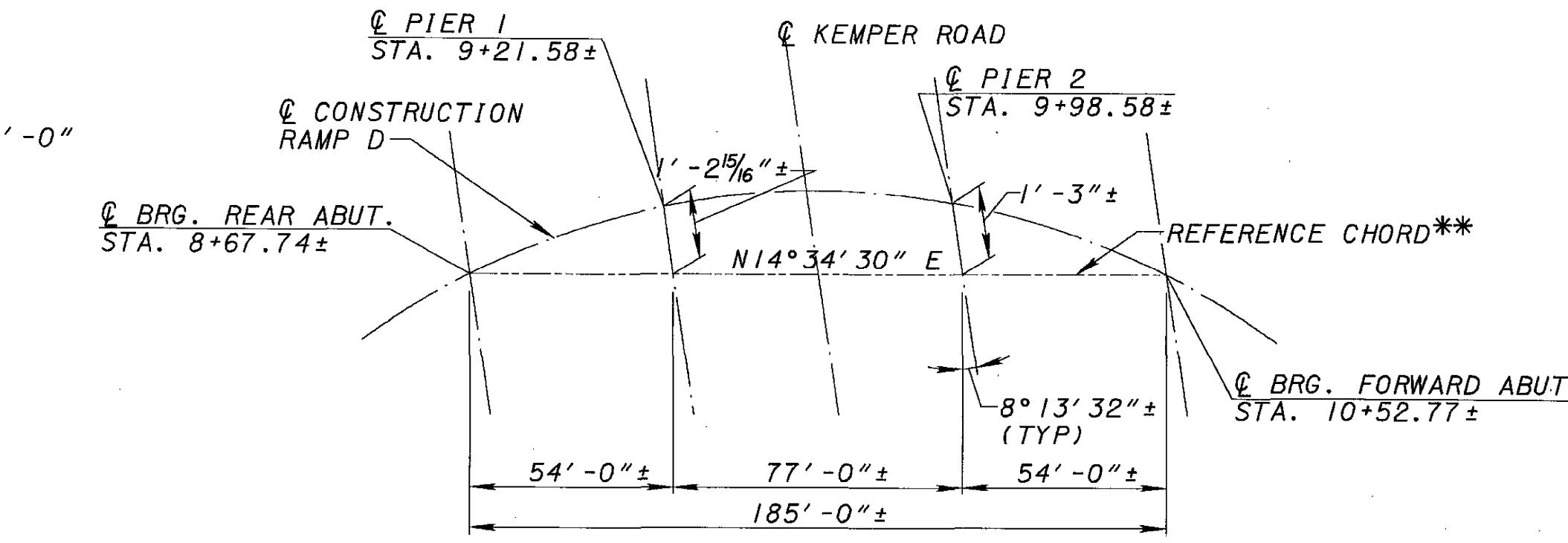
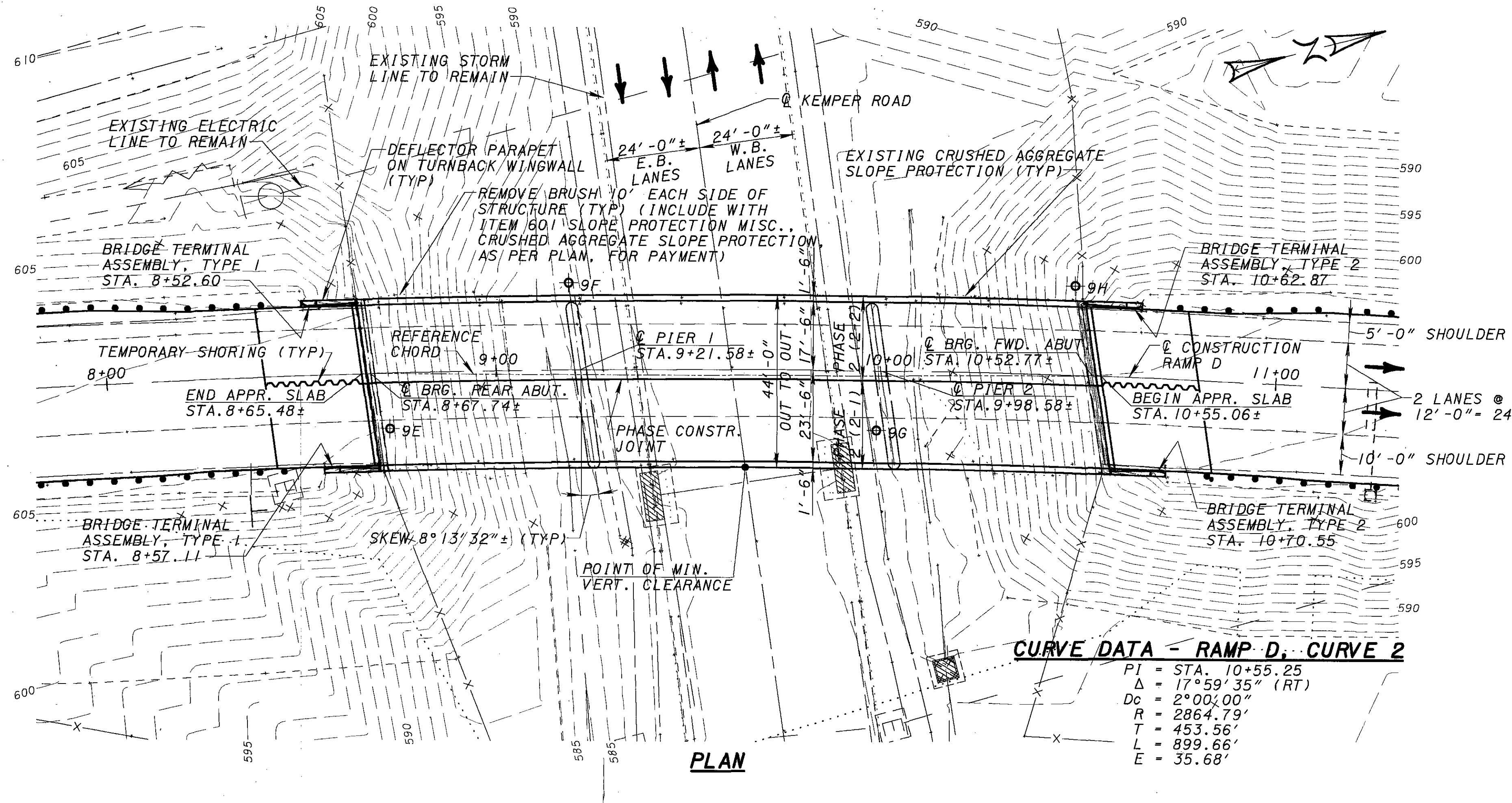
**ITEM 513 - STRUCTURAL STEEL MISC.: REPAIR OF DAMAGED
SECONDARY MEMBER. FILLET WELDING:**

SECONDARY MEMBER, FILLET WELDING:
AFTER DAMAGED AREAS HAVE BEEN INSPECTED ACCORDING TO ITEM 849 DAMAGE ASSESSMENT. PREPARE THE DAMAGED MATERIAL FOR WELDING, PERFORMING $5\frac{1}{16}$ INCH FILLET WELDS ACCORDING TO ITEM 513 USING APPROVED ELECTRODES, PROCEDURES AND WELDERS. WELD EACH SECONDARY MEMBER ACCORDING TO PLAN DETAILS. MAGNETIC PARTICLE INSPECT ALL FILLET WELDS ACCORDING TO C&MS 513.25B. THE ENGINEER MAY OBTAIN TECHNICAL ASSISTANCE FROM THE OFFICE OF MATERIALS MANAGEMENT. THE DEPARTMENT WILL INCLUDE ALL MATERIALS; TOOLS; LABOR; EQUIPMENT; AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK FOR PAYMENT WITH ITEM 513 - STRUCTURAL STEEL MISC.: REPAIR OF DAMAGED MAIN OR SECONDARY MEMBERS, FILLET WELDING: FOOT.

ITEM 513 - STRUCTURAL STEEL MEMBERS LEVEL UF, AS PER PLAN:
ALL REQUIREMENTS OF 513 APPLY TO SHOP FABRICATED MEMBERS.
PERFORM WORK FOR FIELD FABRICATED MEMBERS ACCORDING TO
ITEM 513, EXCEPT AS MODIFIED HEREIN. THE DEPARTMENT WILL NOT
REQUIRE THE CONTRACTOR PERFORMING FIELD FABRICATION TO BE
PRE-QUALIFIED AS SPECIFIED IN SUPPLEMENT 1078. SUBMIT A WRITTEN
LETTER OF MATERIAL ACCEPTANCE, 501.06, TO THE ENGINEER. PROVIDE
SHOP DRAWINGS ACCORDING TO 513.06 OR SUPPLY THE ENGINEER
WITH "AS BUILT" DRAWINGS MEETING 513.06 AFTER COMPLETION OF
FIELD FABRICATION. THE ENGINEER WILL REVIEW THE SUBMITTED
DRAWINGS FOR CONCURRENCE WITH THE FINAL AS-BUILT CONDITION.
THE ENGINEER MAY CONTACT THE OFFICE OF STRUCTURAL ENGINEERING
FOR TECHNICAL ASSISTANCE. IF THE ENGINEER IS SATISFIED WITH THE
"AS-BUILT" DRAWINGS AND THE DELIVERED MATERIALS. SUPPLY A
COPY OF THE DRAWINGS STAMPED, SEALED, AND DATED, ACCORDING
TO SUPPLEMENT 1002, TO THE STRUCTURAL WELDING AND METALS
SECTION OF THE OFFICE OF MATERIAL MANAGEMENT FOR RECORD
PURPOSES. THE MEMBERS INCLUDED IN THIS ITEM ARE PROVIDED IN
TABLE 2 AND 3. THE DEPARTMENT WILL INCLUDE ALL MATERIALS,
TOOLS, LABOR, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE
THE ABOVE WORK FOR PAYMENT WITH ITEM 513 - STRUCTURAL STEEL
MEMBERS LEVEL UF, AS PER PLAN: POUND.

COLLISION REPAIR OF FLANGE FC2
BRIDGE NO. HAM-71-0992
RAMP A FROM BENDBANK EXPRESSWAY TO I-71 SOUTHBOUND

SFN	
3115372	
DESIGN AGENCY	
	
DESIGNER	CHECK
GTF	JAE
REVIEWER	
CAH	6-05-2
PROJECT ID	
113006	
SUBSET	TOTAL
3	3
SHEET	TOTAL
P 39	42

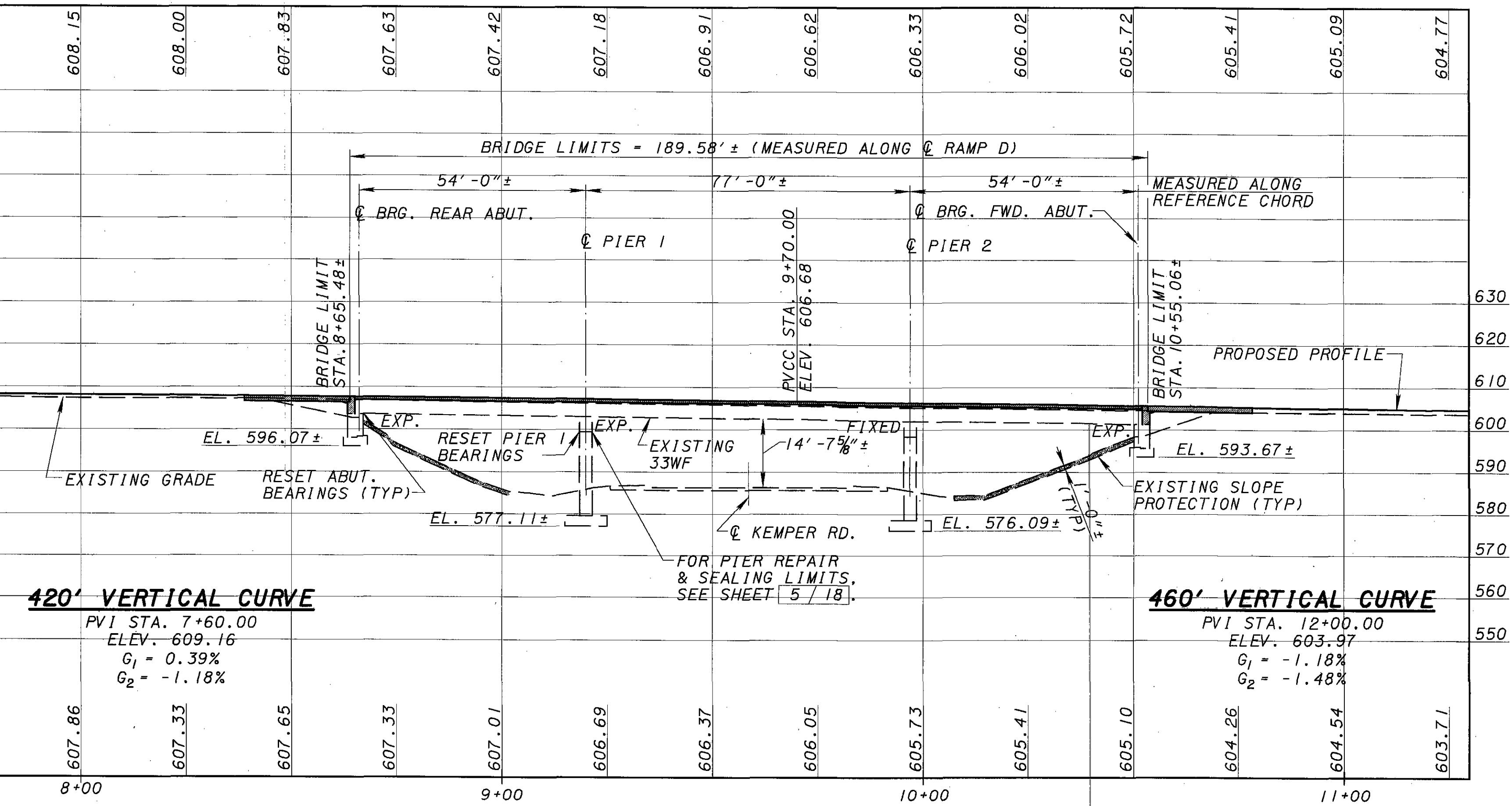


REFERENCE CHORD LAYOUT

** THE REFERENCE CHORD EXTENDS FROM CENTERLINE ABUTMENT BEARING TO CENTERLINE ABUTMENT BEARING.

EXISTING STRUCTURE

TYPE: THREE SPAN NON-COMPOSITE CONTINUOUS STEEL BEAM WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE
SPANs: 54'-0"±, 77'-0"±, AND 54'-0"± c/c BEARINGS
ROADWAY: 41'-0" FACE TO FACE OF PARAPETS
LOADING: HS20 CASE I, THE ALTERNATE MILITARY LOADING, 50 psf FUTURE WEARING SURFACE, AND 15 psf STAY-IN-PLACE FORMS
SKEW: 8°13'32"± RIGHT FORWARD (TO REFERENCE CHORD)
WEARING SURFACE: MONOLITHIC CONCRETE
APPROACH SLABS: 25'-0" LONG AS-1-81
ALIGNMENT: 2° CURVE RIGHT
SUPERELEVATION: 0.033 FT/FT
STRUCTURE FILE NUMBER: 3111083
DATE BUILT: 1960
DISPOSITION: MINOR MAINTENANCE REQUIRED



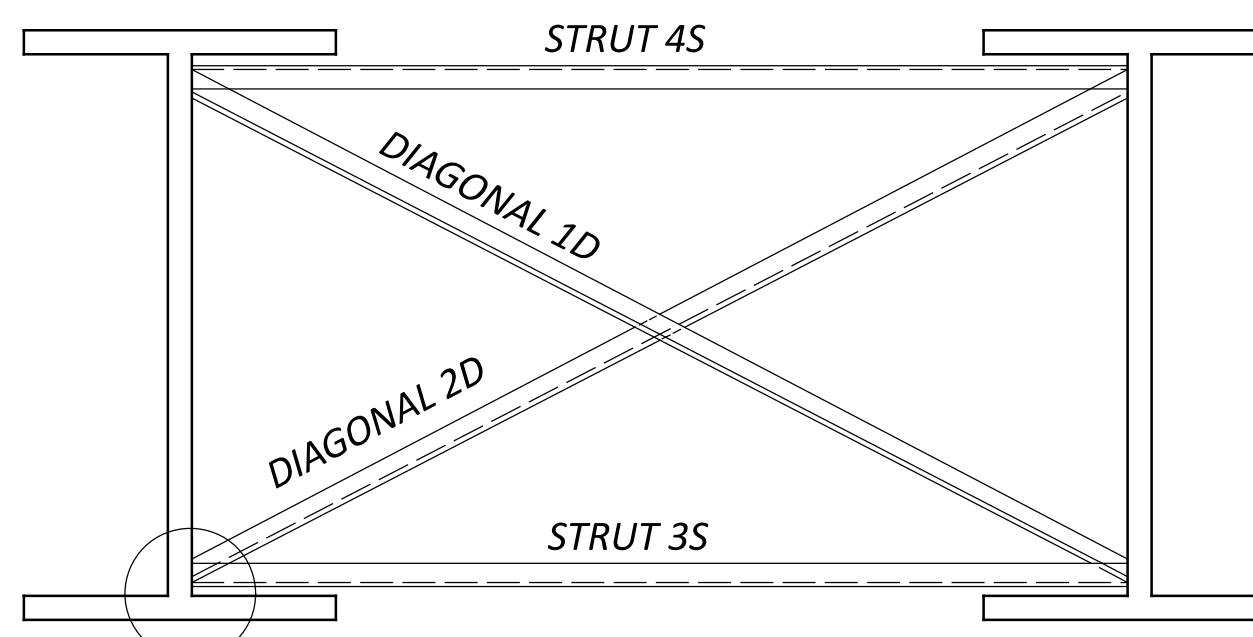
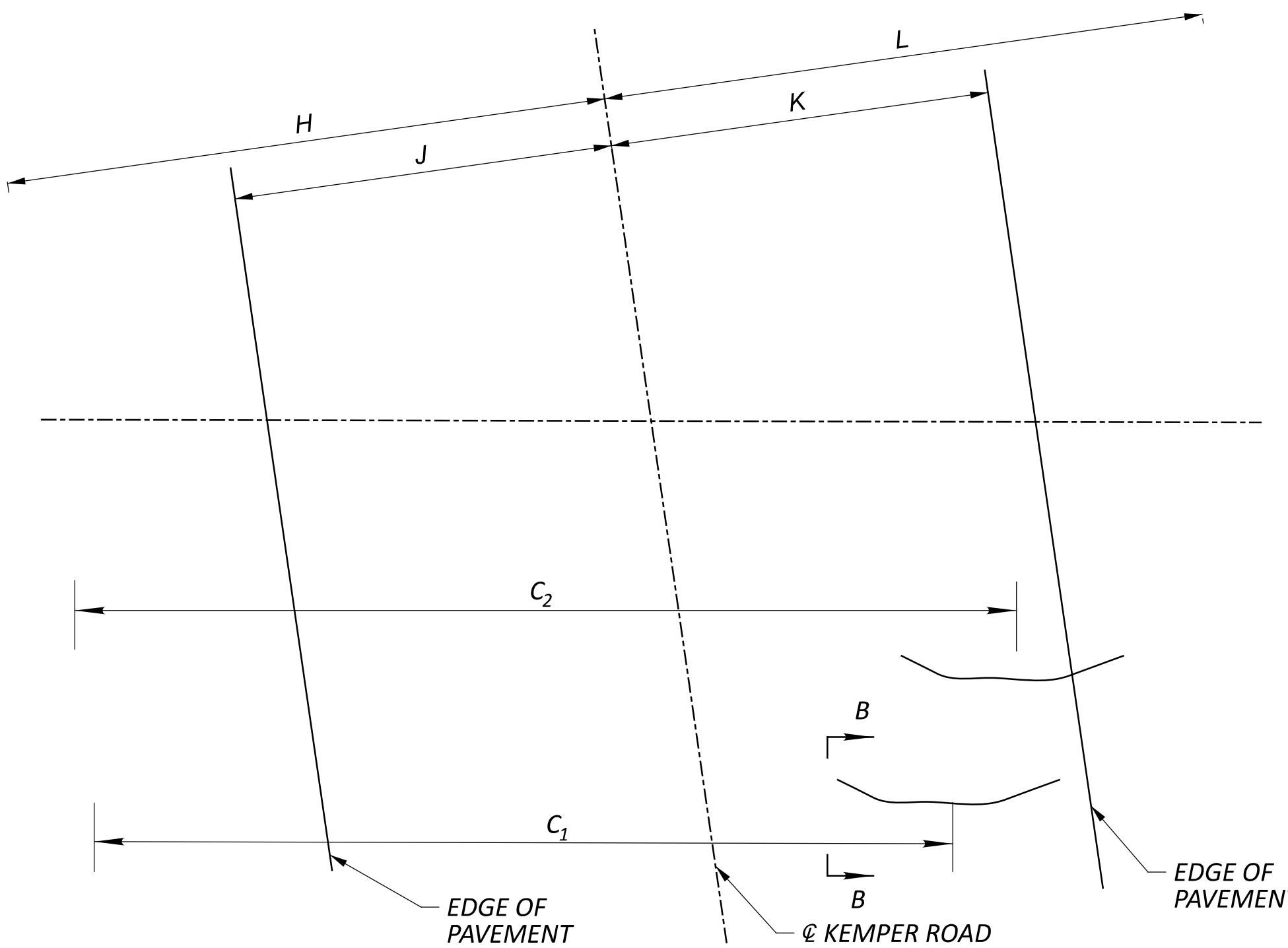
BRIDGE SITE PLAN
BRIDGE NO. HAM-75-1642E

I-75 NORTHBOUND RAMP OVER KEMPER ROAD

HORIZONTAL SCALE IN FEET
0 10 20 40



DESIGNER	CHECKER
GTF	JAB
REVIEWER	CAH 6-05-25
PROJECT ID	113006
SUBSET	TOTAL
1	3
SHEET	TOTAL
P.40	42



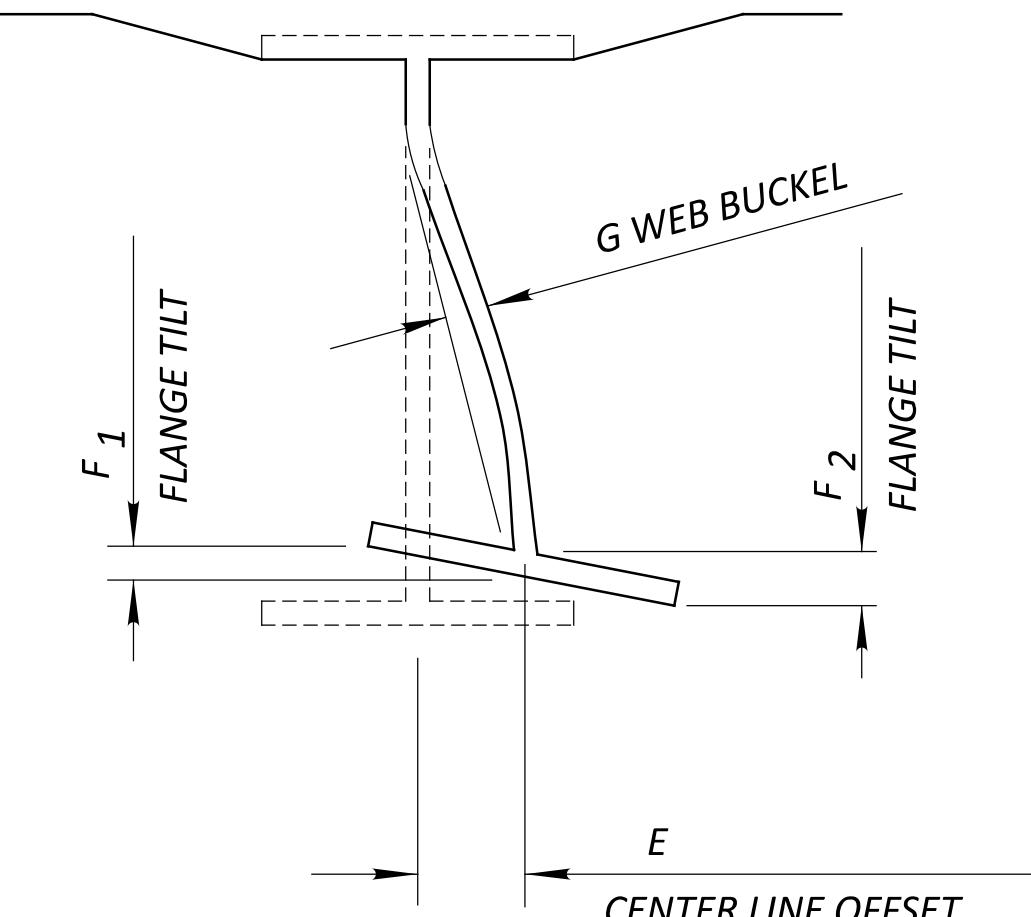
REMOVE ACCORDING TO ITEM 202-PORTIONS OF SECONDARY MEMBERS REMOVED, AS PER PLAN. REPLACE BY MATCHING EXISTING DETAIL. SEE GSD-1-96 FOR ADDITIONAL CLARIFICATION.

MAIN AND SECONDARY MEMBER DAMAGE IS NOT SHOWN. FOR CLARITY SEE SECTION A-A SEE SHEET XX FOR ADDITIONAL COLLISION REPAIR NOTES

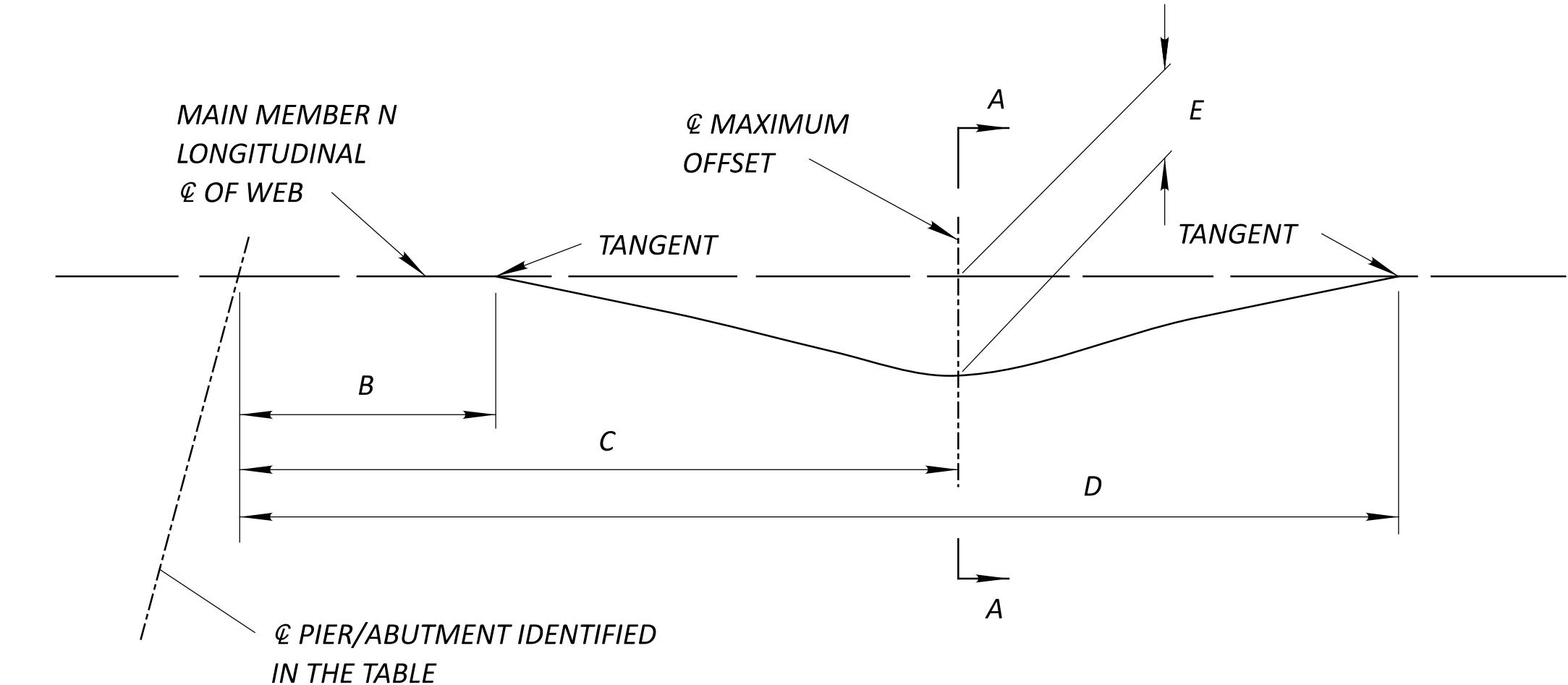
SECTION B-B

SECONDARY MEMBER BAY No. M

N- NUMBER OF CROSSFRAME BRACES COUNTED FROM THE PIER OR ABUTMENT IDENTIFIED IN TABLE



NEGATIVE E VALUES ARE BENT LEFT
NEGATIVE F VALUES ARE BENT DOWN
NEGATIVE G VALUES ARE BENT LEFT



ORIENTATION NOTE
ABUTMENTS AND PIERS ARE NUMBERED IN THE CARDINAL DIRECTION (FROM SOUTH TO NORTH OR WEST TO EAST). BEAMS ARE NUMBERED FROM LEFT TO RIGHT WHEN FACING IN THE CARDINAL DIRECTION. BAYS ARE NUMBERED TO MATCH THE MAIN MEMBERLINE NUMBER TO THE LEFT OF THE CROSSFRAME BAY WHEN FACING IN THE CARDINAL DIRECTION.

CROSSFRAME NOTE
REMOVAL AND REPLACEMENT OF PORTIONS OF THE CROSSFRAME IS PERMITTED IN KIND WITH APPROVAL OF THE ENGINEER, BUT AT NO ADDITIONAL COST TO THE PROJECT.

EXISTING STRUCTURE: HAM-75-1642E
ROUTE ON STRUCTURE: I-75 NORTHBOUND RAMP TO I-275
ROUTE BELOW STRUCTURE: KEMPER ROAD
TYPE: CONTINUOUS STEEL BEAM WITH REINFORCED CONCRETE DECK & SUBSTRUCTURE
SPANS: 54'-0", 77'-0", AND 54'-0" c/c BEARINGS
ROADWAY WIDTH: 41'-0" FACE TO FACE OF PARAPETS
SKEW: 8°-13'-32" RIGHT FORWARD (TO REFERENCE CHORD)
ALIGNMENT: 2° HORIZONTAL CURVE TO THE RIGHT
SUPERELEVATION: 0.033 FT/FT
YEAR BUILT: 1960
NUMBER OF BEAMS: 6
STEEL TYPE: ASTM-A36
PAINT TYPE: OZEU
PAINT DATE: 1993

TABLE #2 DAMAGED SECONDARY MEMBER TO BE REPLACED (LBS)							
CROSSFRAME BAY M	PIER/ABUT.	N	1D	2D	3S	4S	
4	1	4	50	50	48	N/A	
5	1	4	50	50	48	N/A	
5	1	5	50	50	48	N/A	

TABLE #1 DAMAGED MAIN MEMBERS TO BE HEAT STRAIGHTENED														
DAMAGE AREA No.	MEMBER LINE No. A	PIER OR ABUT.	B	C ₁	C ₂	D	E	F ₁	F ₂	G	H	J	K	L
1	SPAN 2 BEAM 5	PIER 1	49'-9"	N/A	60'	70'-3"	0"	0"	5/8"	0"	38'±	24'	24'	38'±
2	SPAN 2 BEAM 6	PIER 1	41'	53'-6"	N/A	66'	-7/16"	7/16"	7/16"	0"	38'±	24'	24'	38'±

COLLISION REPAIR AND HEAT STRAIGHTENING PLAN
BRIDGE NO. HAM-75-1642E
I-75 NORTHBOUND RAMP OVER KEMPER ROAD



DESIGNER	CHECKER
GTF	JAB
REVIEWER	6-05-25
PROJECT ID	113006
SUBSET	TOTAL
2	3
SHEET	TOTAL
P.41	42

