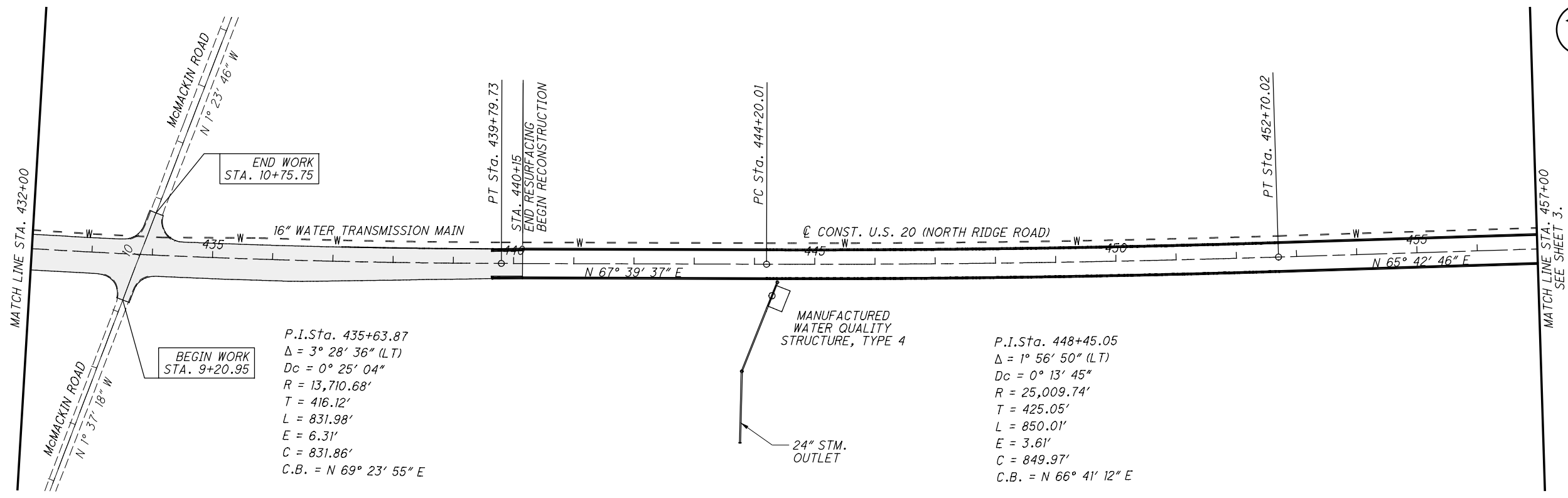


CALCULATED	0
JMP	100
CHECKED	50
WDB	200

HORIZONTAL SCALE IN FEET

SCHEMATIC PLAN
STA. 407+00 TO STA. 457+00



BEGIN WORK
STA. 9+20.95

P.I. Sta. 435+63.87
 $\Delta = 3^\circ 28' 36''$ (LT)
 $Dc = 0^\circ 25' 04''$
 $R = 13,710.68'$
 $T = 416.12'$
 $L = 831.98'$
 $E = 6.31'$
 $C = 831.86'$
 $C.B. = N 69^\circ 23' 55'' E$

MANUFACTURED
WATER QUALITY
STRUCTURE, TYPE 4

24" STM.
OUTLET

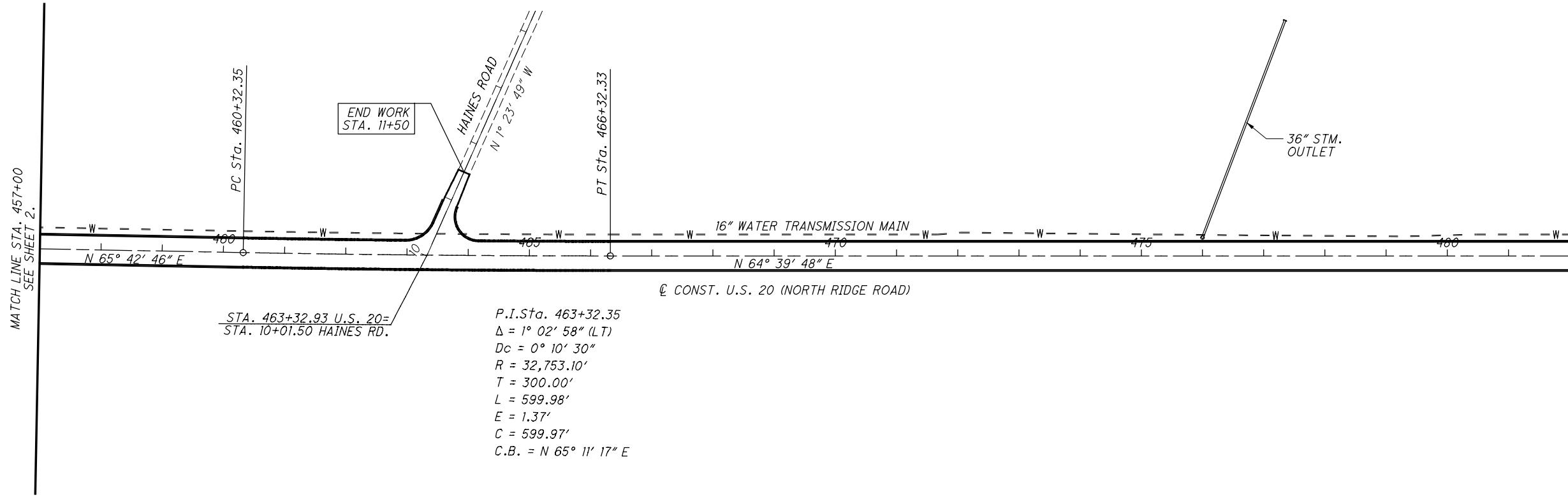
P.I. Sta. 448+45.05
 $\Delta = 1^\circ 56' 50''$ (LT)
 $Dc = 0^\circ 13' 45''$
 $R = 25,009.74'$
 $T = 425.05'$
 $L = 850.01'$
 $E = 3.61'$
 $C = 849.97'$
 $C.B. = N 66^\circ 41' 12'' E$

LEGEND

RESURFACING AREA

LAK-US-20-24.99
PART 2

H:\2018\180743\0001\Project\Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CB008.dgn Sheet 7/15/2022 2:49:56 PM troyer



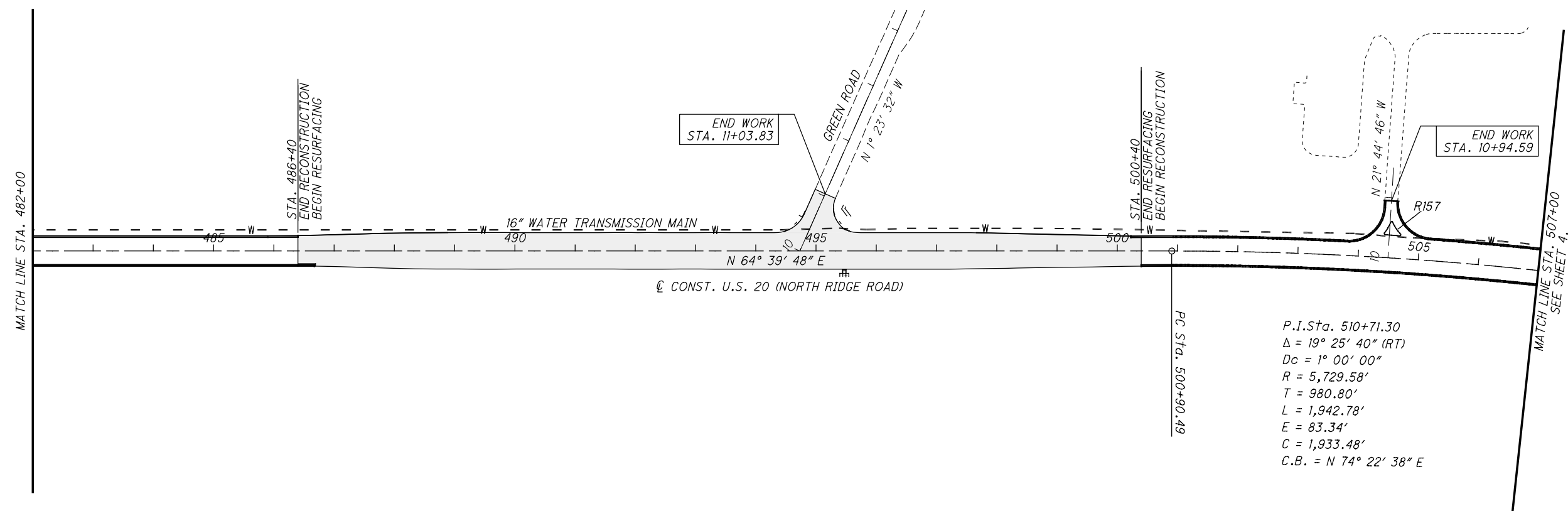
$\frac{STA. 463+32.93 \text{ U.S. 20} =}{STA. 10+01.50 \text{ HAINES RD.}}$
 P.I. Sta. 463+32.35
 $\Delta = 1^\circ 02' 58'' \text{ (LT)}$
 $Dc = 0^\circ 10' 30''$
 $R = 32,753.10'$
 $T = 300.00'$
 $L = 599.98'$
 $E = 1.37'$
 $C = 599.97'$
 $C.B. = N 65^\circ 11' 17'' E$

LEGEND
 RESURFACING AREA



CALCULATED	JMP	CHECKED	WDB

0 100 200
 HORIZONTAL SCALE IN FEET

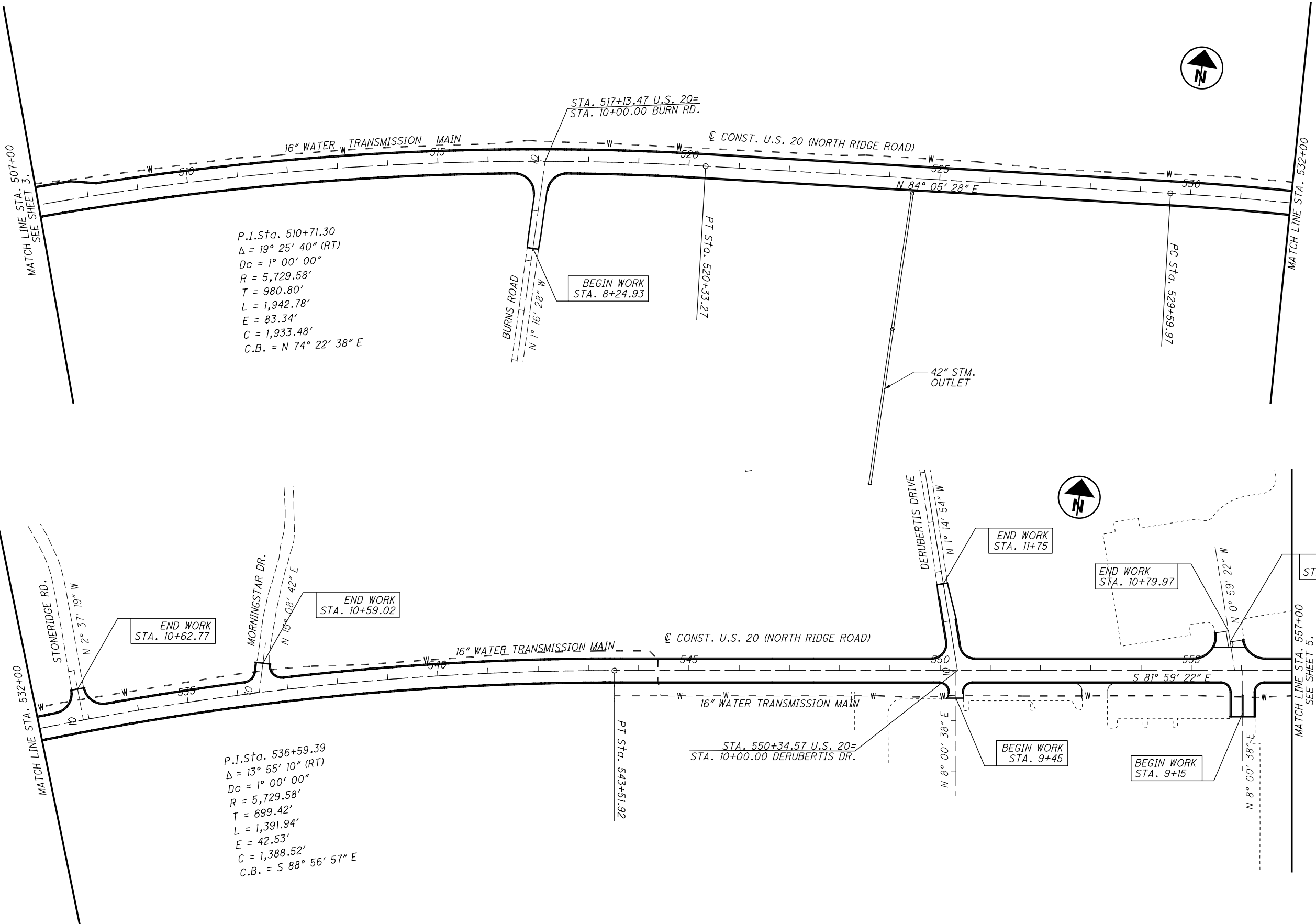


P.I. Sta. 510+71.30
 $\Delta = 19^\circ 25' 40'' \text{ (RT)}$
 $Dc = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 980.80'$
 $L = 1,942.78'$
 $E = 83.34'$
 $C = 1,933.48'$
 $C.B. = N 74^\circ 22' 38'' E$



LAK-US-20-24.99
PART 2
SCHEMATIC PLAN
STA. 457+00 TO STA. 507+00

H:\2018\180743\0001\Project\Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CB009.dgn Sheet 7/15/2022 2:49:57 PM troyer



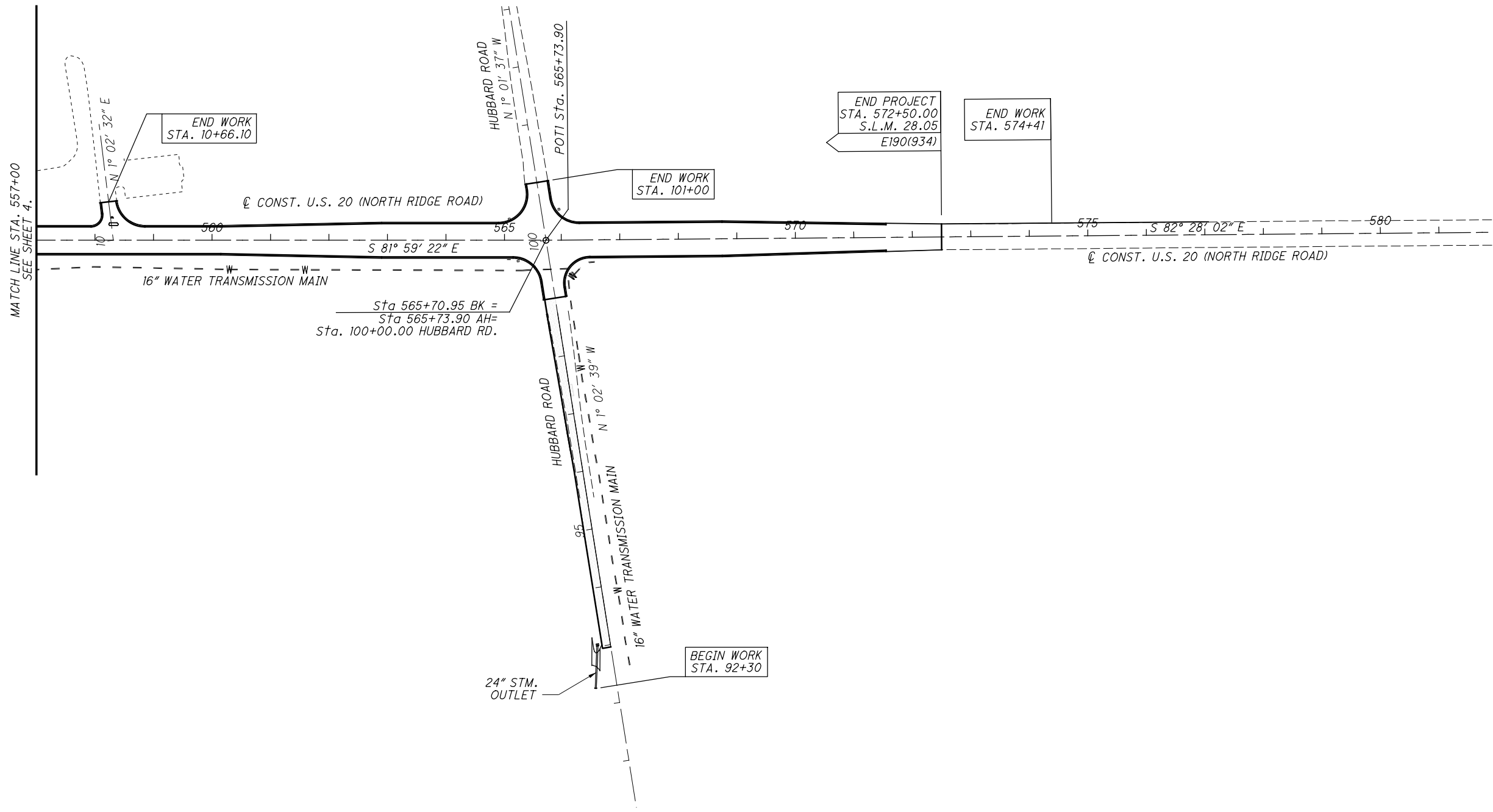
P.I. Sta. 510+71.30
 $\Delta = 19^\circ 25' 40''$ (RT)
 $D_c = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 980.80'$
 $L = 1,942.78'$
 $E = 83.34'$
 $C = 1,933.48'$
 $C.B. = N 74^\circ 22' 38'' E$

P.I. Sta. 536+59.39
 $\Delta = 13^\circ 55' 10''$ (RT)
 $D_c = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 699.42'$
 $L = 1,391.94'$
 $E = 42.53'$
 $C = 1,388.52'$
 $C.B. = S 88^\circ 56' 57'' E$



CALCULATED	JMP	CHECKED	WDB

SCHEMATIC PLAN
STA. 507+00 TO STA. 557+00

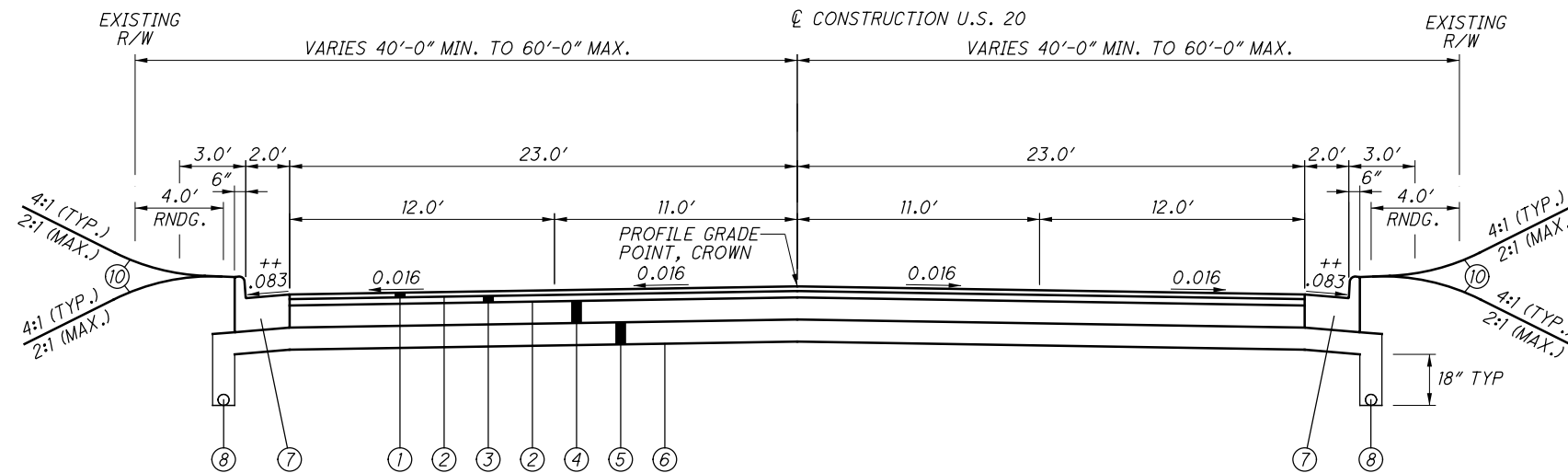


CALCULATED
JMP
CHECKED
WDB

0 100 200
HORIZONTAL
SCALE IN FEET

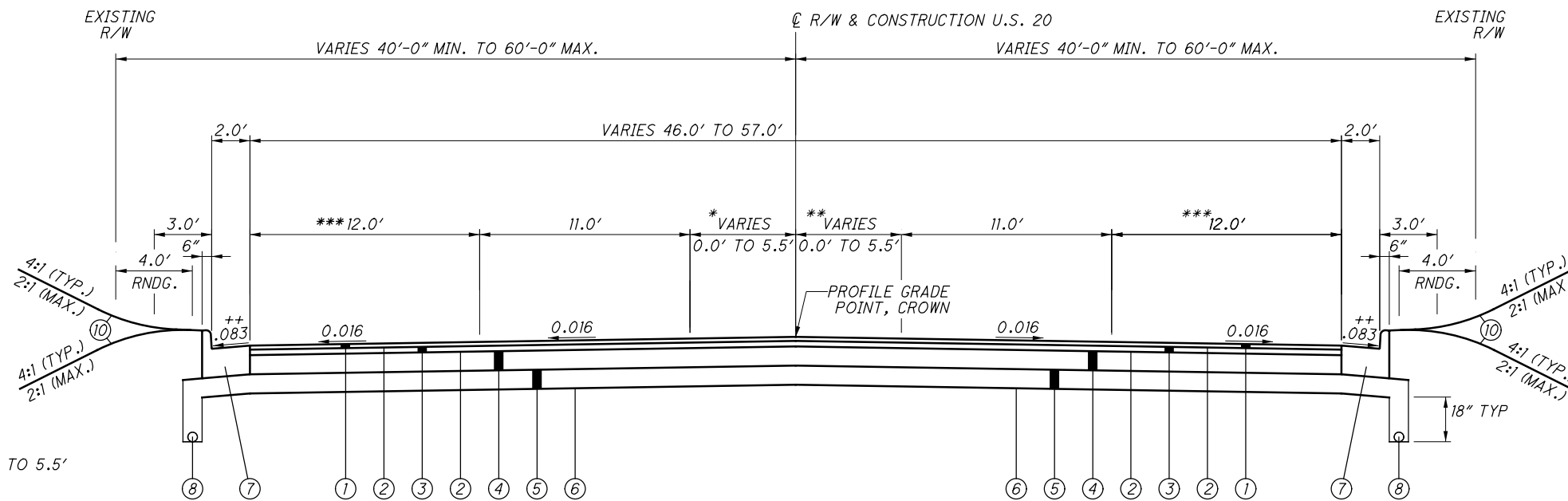
SCHEMATIC PLAN
STA. 557+00 TO STA. 582+00

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway_Sheets\09270_C\001.dgn MAINLINE TYP | 7/15/2022 2:49:58 PM Troyer



U.S. 20 - NORMAL SECTION

STA. 411+70.00 TO STA. 426+00.00
 STA. 440+15.00 TO STA. 485+90.00
 STA. 521+38.88 TO STA. 528+54.37
 STA. 544+57.52 TO STA. 560+15.00



U.S. 20 - 4 TO 5-LANE NORMAL SECTION

STA. 411+20.00 TO STA. 411+70.00
 STA. 485+90.00 TO STA. 486+40.00
 STA. 560+15.00 TO STA. 562+90.00
 STA. 568+75.00 TO STA. 571+55.00

STA. 411+20.00 TO STA. 411+70.00
 *VARIES 1.3' TO 0.0'
 **0.0'

STA. 485+90.00 TO STA. 486+40.00
 *VARIES 0.0' TO 0.3'
 **0.0'

STA. 571+11.92 TO STA. 571+55.00
 ***VARIES 12.0' TO 11.0'

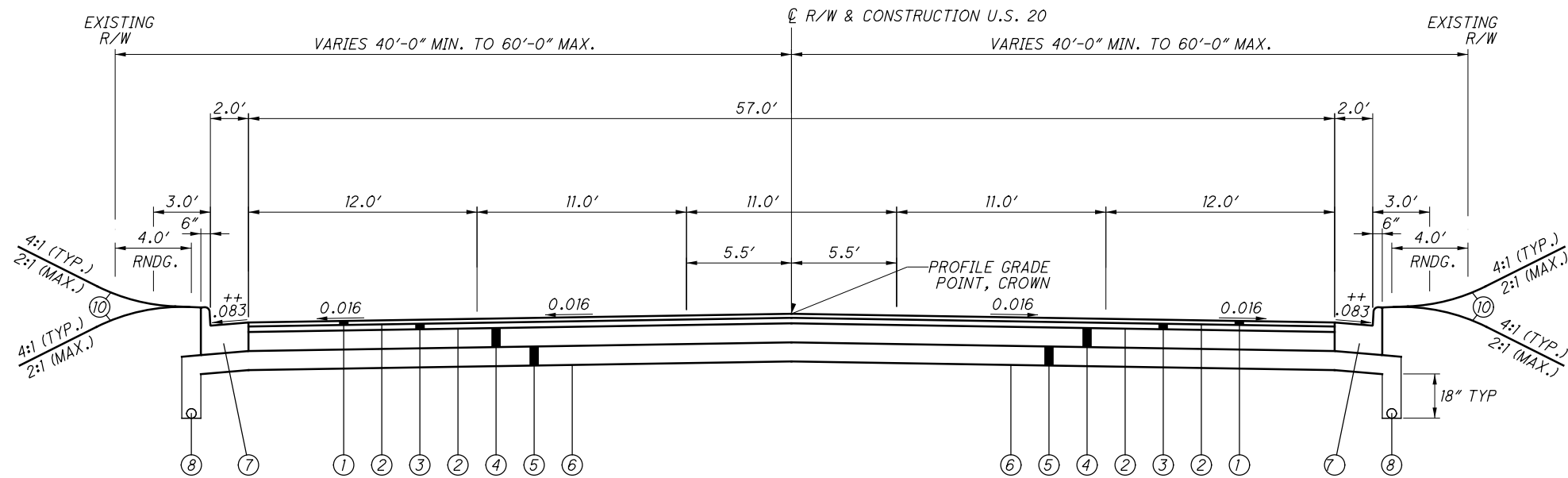
LEGEND

- | | |
|-------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------|
| ① ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG70-22M, AS PER PLAN | ⑨ ITEM 606 - GUARDRAIL, TYPE MGS |
| ② ITEM 407 - NON-TRACKING TACK COAT | ⑩ ITEM 659 - SEEDING AND MULCHING, CLASS 1 |
| ③ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446) | ⑪ ITEM 254 - 3" PAVEMENT PLANING, ASPHALT CONCRETE |
| ④ ITEM 301 - 6" ASPHALT CONCRETE BASE, (449), AS PER PLAN, PG64-22 | ⑫ ITEM 617 - 3" COMPACTED AGGREGATE |
| ⑤ ITEM 304 - 6" AGGREGATE BASE | ⑬ ITEM 608 - 4" CONCRETE WALK |
| ⑥ ITEM 204 - SUBGRADE COMPACTION | ⑭ ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), PG64-22 |
| ⑦ ITEM 609 - COMBINATION CURB AND GUTTER, TYPE 2 | ⑮ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (449) |
| ⑧ ITEM 605 - 6" BASE PIPE UNDERDRAINS WITH GEOTEXTILE FABRIC | ⑯ ITEM 609 - CURB, TYPE 6 |
| | ⑰ ITEM 301 - 6" ASPHALT CONCRETE BASE, PG64-22, (449) |

NOTE: ROADWAY TYPICAL SECTIONS INDICATE THE FINAL CONDITION OF 1.25 INCHES ASPHALT SURFACE COURSE PLACED UPON AN INTERMEDIATE COURSE. DURING MOT PHASES 1, 2, 3, AND 4, 3.0 INCHES OF ASPHALT INTERMEDIATE COURSE WILL TEMPORARILY BE PLACED. DURING MOT PHASE 5, 1.25 INCHES OF THE INTERMEDIATE COURSE WILL BE MILLED AND 1.25 INCHES OF ASPHALT SURFACE COURSE WILL BE PLACED. SEE MAINTENANCE OF TRAFFIC GENERAL NOTES AND TYPICAL SECTIONS.

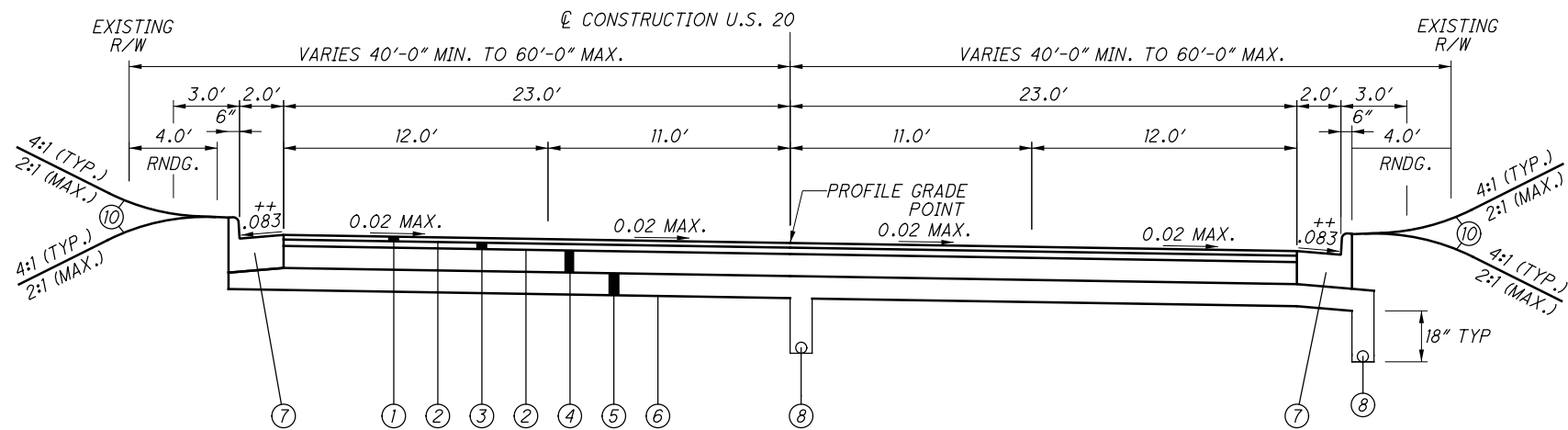
++ FLATTEN GUTTER CROSS SLOPE TO 0.016 WHERE INDICATED ON THE PLANS TO MATCH EXISTING PAVEMENT

H:\2018\180743\000\Proj\Drawings\Roadway\Sheets\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_C001.dgn MAINLINE TYP 2 7/15/2022 2:49:59 PM troyer



U.S. 20 - 5-LANE NORMAL SECTION

STA. 562+90.00 TO STA. 565+70.95
 STATION EQUATION: STA. 563+73.90 AHEAD = STA. 565+70.95 BACK
 STA. 565+73.90 TO STA. 568+75.00



U.S. 20 - 4-LANE SUPERELEVATED SECTION

STA. 500+40.00 TO STA. 503+81.03
 STA. 508+20.00 TO STA. 521+38.88
 STA. 528+54.37 TO STA. 544+57.52

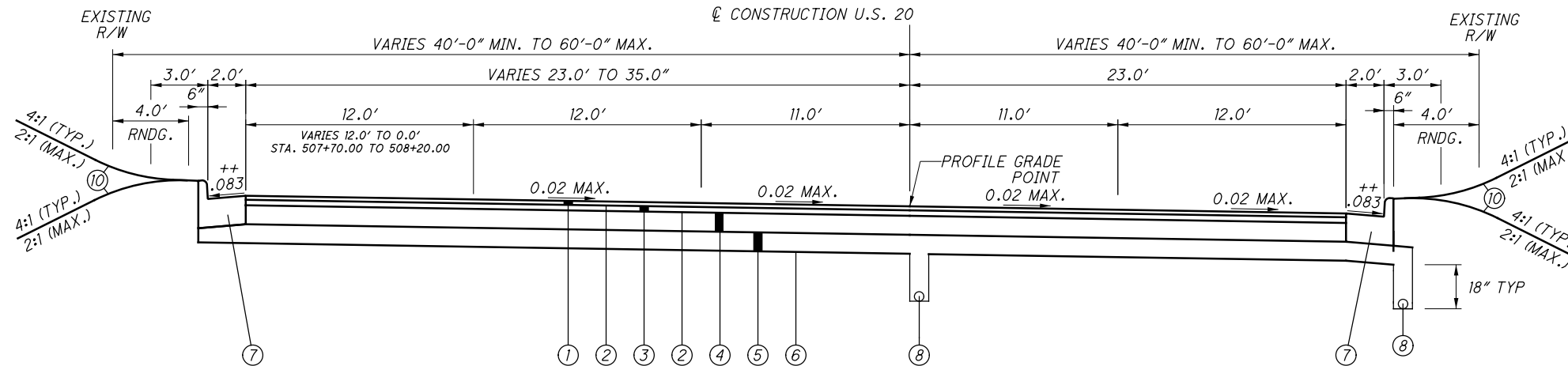
LEGEND

- | | |
|-------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------|
| ① ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG70-22M, AS PER PLAN | ⑨ ITEM 606 - GUARDRAIL, TYPE MGS |
| ② ITEM 407 - NON-TRACKING TACK COAT | ⑩ ITEM 659 - SEEDING AND MULCHING, CLASS 1 |
| ③ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446) | ⑪ ITEM 254 - 3" PAVEMENT PLANING, ASPHALT CONCRETE |
| ④ ITEM 301 - 6" ASPHALT CONCRETE BASE, (449), AS PER PLAN, PG64-22 | ⑫ ITEM 617 - 3" COMPACTED AGGREGATE |
| ⑤ ITEM 304 - 6" AGGREGATE BASE | ⑬ ITEM 608 - 4" CONCRETE WALK |
| ⑥ ITEM 204 - SUBGRADE COMPACTION | ⑭ ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), PG64-22 |
| ⑦ ITEM 609 - COMBINATION CURB AND GUTTER, TYPE 2 | ⑮ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (449) |
| ⑧ ITEM 605 - 6" BASE PIPE UNDERDRAINS WITH GEOTEXTILE FABRIC | ⑯ ITEM 609 - CURB, TYPE 6 |
| | ⑰ ITEM 301 - 6" ASPHALT CONCRETE BASE, PG64-22, (449) |

NOTE: ROADWAY TYPICAL SECTIONS INDICATE THE FINAL CONDITION OF 1.25 INCHES ASPHALT SURFACE COURSE PLACED UPON AN INTERMEDIATE COURSE. DURING MOT PHASES 1, 2, 3, AND 4, 3.0 INCHES OF ASPHALT INTERMEDIATE COURSE WILL TEMPORARILY BE PLACED. DURING MOT PHASE 5, 1.25 INCHES OF THE INTERMEDIATE COURSE WILL BE MILLED AND 1.25 INCHES OF ASPHALT SURFACE COURSE WILL BE PLACED. SEE MAINTENANCE OF TRAFFIC GENERAL NOTES AND TYPICAL SECTIONS.

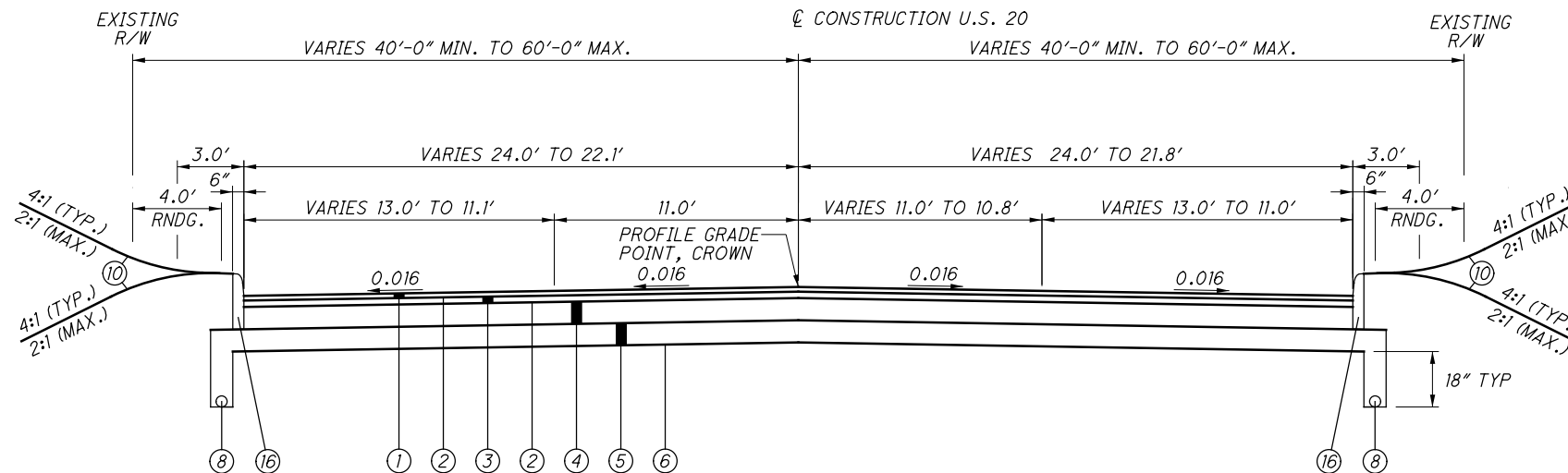
++ FLATTEN GUTTER CROSS SLOPE TO 0.016 WHERE INDICATED ON THE PLANS TO MATCH EXISTING PAVEMENT

H:\2018\180743\000\Proj\Drawings\Roadway\Sheets\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_C\001.dgn MAINLINE TYP 3 7/15/2022 2:50:00 PM troyer



U.S. 20 - 5-LANE SUPERELEVATED SECTION

STA. 503+81.03 TO STA. 508+20.00



U.S. 20 - NORMAL SECTION

STA. 571+55.00 TO STA. 572+50.00

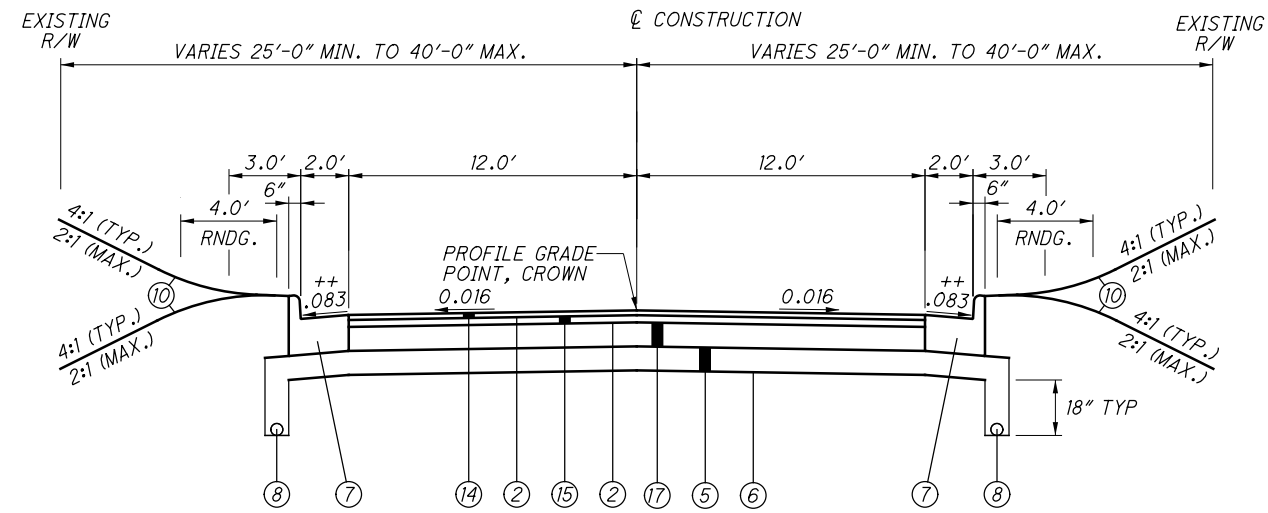
LEGEND

- | | |
|-------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------|
| ① ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG70-22M, AS PER PLAN | ⑨ ITEM 606 - GUARDRAIL, TYPE MGS |
| ② ITEM 407 - NON-TRACKING TACK COAT | ⑩ ITEM 659 - SEEDING AND MULCHING, CLASS 1 |
| ③ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446) | ⑪ ITEM 254 - 3" PAVEMENT PLANING, ASPHALT CONCRETE |
| ④ ITEM 301 - 6" ASPHALT CONCRETE BASE, (449), AS PER PLAN, PG64-22 | ⑫ ITEM 617 - 3" COMPACTED AGGREGATE |
| ⑤ ITEM 304 - 6" AGGREGATE BASE | ⑬ ITEM 608 - 4" CONCRETE WALK |
| ⑥ ITEM 204 - SUBGRADE COMPACTION | ⑭ ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), PG64-22 |
| ⑦ ITEM 609 - COMBINATION CURB AND GUTTER, TYPE 2 | ⑮ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (449) |
| ⑧ ITEM 605 - 6" BASE PIPE UNDERDRAINS WITH GEOTEXTILE FABRIC | ⑯ ITEM 609 - CURB, TYPE 6 |
| | ⑰ ITEM 301 - 6" ASPHALT CONCRETE BASE, PG64-22, (449) |

NOTE: ROADWAY TYPICAL SECTIONS INDICATE THE FINAL CONDITION OF 1.25 INCHES ASPHALT SURFACE COURSE PLACED UPON AN INTERMEDIATE COURSE. DURING MOT PHASES 1, 2, 3, AND 4, 3.0 INCHES OF ASPHALT INTERMEDIATE COURSE WILL TEMPORARILY BE PLACED. DURING MOT PHASE 5, 1.25 INCHES OF THE INTERMEDIATE COURSE WILL BE MILLED AND 1.25 INCHES OF ASPHALT SURFACE COURSE WILL BE PLACED. SEE MAINTENANCE OF TRAFFIC GENERAL NOTES AND TYPICAL SECTIONS.

++ FLATTEN GUTTER CROSS SLOPE TO 0.016 WHERE INDICATED ON THE PLANS TO MATCH EXISTING PAVEMENT

H:\2018\180743\000\Proj\Drawings\Roadway\Roadway_Sheets\09270_C\001.dgn SIDESTREET TYP I 7/15/2022 2:50:00 PM Troyer



HAINES RD.

STA. 9+98.52 TO STA. 10+92.79

BURNS RD.

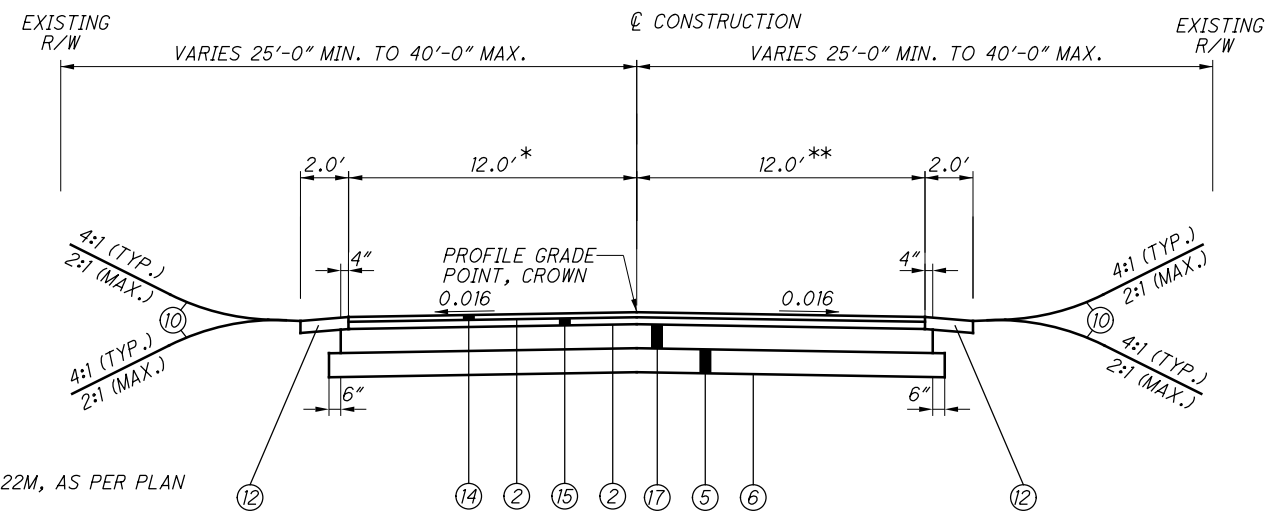
STA. 9+32.30 TO STA. 10+00.00 LT

STA. 9+43.32 TO STA. 10+00.00 RT

DERUBERTIS RD.

STA. 10+00.00 TO STA. 11+50.00 LT

STA. 10+00.00 TO STA. 11+00.00 RT



HAINES RD.

STA. 10+92.79 TO STA. 11+50.50

BURNS RD.

STA. 8+25.00 TO STA. 9+32.30 LT

STA. 8+25.00 TO STA. 9+43.32 RT

DERUBERTIS RD.

STA. 11+50.00 TO STA. 11+75.00 LT

STA. 11+00.00 TO STA. 11+75.00 RT

LEGEND

- ① ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG70-22M, AS PER PLAN
- ② ITEM 407 - NON-TRACKING TACK COAT
- ③ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446)
- ④ ITEM 301 - 6" ASPHALT CONCRETE BASE, (449), AS PER PLAN, PG64-22
- ⑤ ITEM 304 - 6" AGGREGATE BASE
- ⑥ ITEM 204 - SUBGRADE COMPACTION
- ⑦ ITEM 609 - COMBINATION CURB AND GUTTER, TYPE 2
- ⑧ ITEM 605 - 6" BASE PIPE UNDERDRAINS WITH GEOTEXTILE FABRIC
- ⑨ ITEM 606 - GUARDRAIL, TYPE MGS
- ⑩ ITEM 659 - SEEDING AND MULCHING, CLASS 1
- ⑪ ITEM 254 - 3" PAVEMENT PLANING, ASPHALT CONCRETE
- ⑫ ITEM 617 - 3" COMPACTED AGGREGATE
- ⑬ ITEM 608 - 4" CONCRETE WALK
- ⑭ ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
- ⑮ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)
- ⑯ ITEM 609 - CURB, TYPE 6
- ⑰ ITEM 301 - 6" ASPHALT CONCRETE BASE, PG64-22, (449)

HAINES RD.

* VARIES 12' TO 9.98'

** VARIES 12' TO 9.91'

BURNS RD.

* VARIES 12' TO 11.40'

** VARIES 12' TO 11.30'

DERUBERTIS RD.

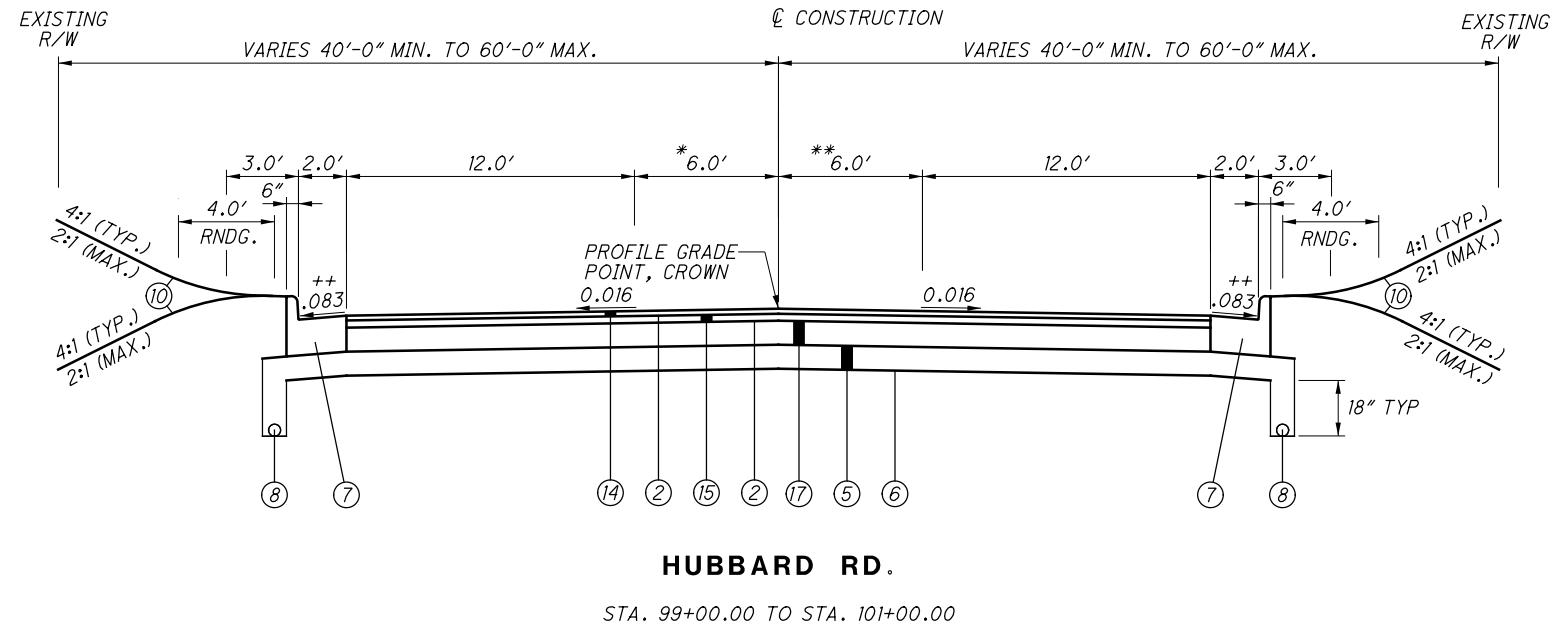
* VARIES 12' TO 12.55'

** VARIES 12' TO 8.15'

NOTE: ROADWAY TYPICAL SECTIONS INDICATE THE FINAL CONDITION OF 1.25 INCHES ASPHALT SURFACE COURSE PLACED UPON AN INTERMEDIATE COURSE. DURING MOT PHASES 1, 2, 3, AND 4, 3.0 INCHES OF ASPHALT INTERMEDIATE COURSE WILL TEMPORARILY BE PLACED. DURING MOT PHASE 5, 1.25 INCHES OF THE INTERMEDIATE COURSE WILL BE MILLED AND 1.25 INCHES OF ASPHALT SURFACE COURSE WILL BE PLACED. SEE MAINTENANCE OF TRAFFIC GENERAL NOTES AND TYPICAL SECTIONS.

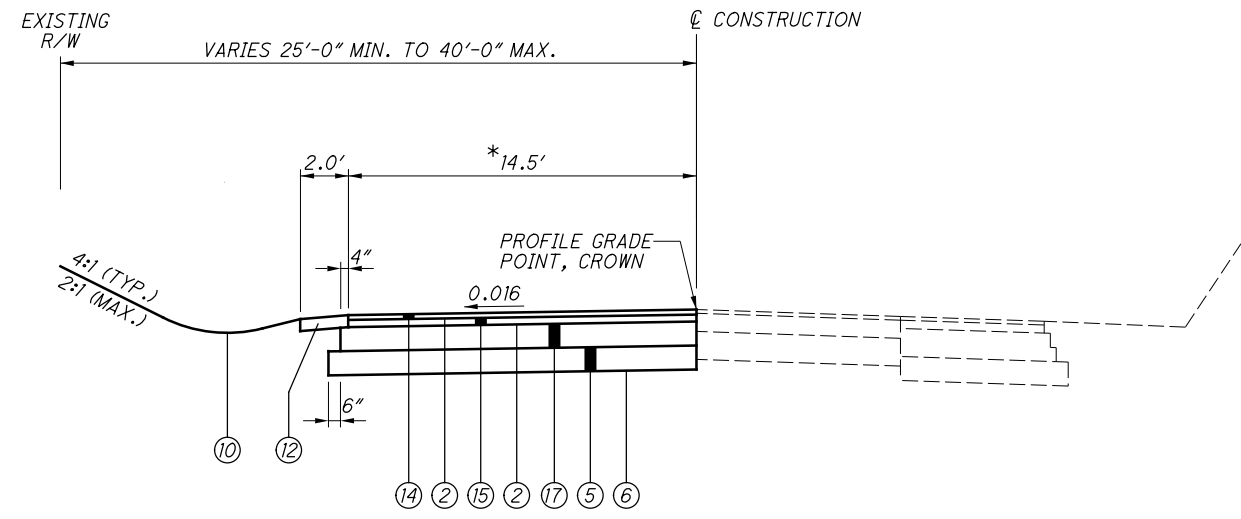
++ FLATTEN GUTTER CROSS SLOPE TO 0.016 WHERE INDICATED ON THE PLANS TO MATCH EXISTING PAVEMENT

H:\2018\180743\000T\Proj\ec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_C001.dgn SIDESTREET TYP 2 7/15/2022 2:50:01PM troyer



HUBBARD RD.
STA. 99+00.00 TO STA. 101+00.00

STA. 100+00.00 TO STA. 101+00.00
*7.0'
**5.0'



HUBBARD RD.
STA. 92+96.00 TO STA. 99+00.00

* VARIES 14.5' TO 18.0'
STA. 96+00.00 TO STA. 99+00.00

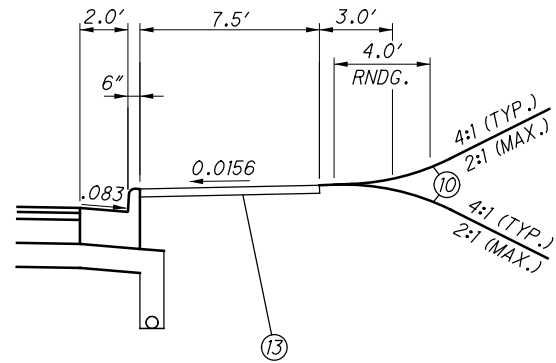
LEGEND

- ① ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG70-22M, AS PER PLAN
- ② ITEM 407 - NON-TRACKING TACK COAT
- ③ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446)
- ④ ITEM 301 - 6" ASPHALT CONCRETE BASE, (449), AS PER PLAN, PG64-22
- ⑤ ITEM 304 - 6" AGGREGATE BASE
- ⑥ ITEM 204 - SUBGRADE COMPACTION
- ⑦ ITEM 609 - COMBINATION CURB AND GUTTER, TYPE 2
- ⑧ ITEM 605 - 6" BASE PIPE UNDERDRAINS WITH GEOTEXTILE FABRIC
- ⑨ ITEM 606 - GUARDRAIL, TYPE MGS
- ⑩ ITEM 659 - SEEDING AND MULCHING, CLASS 1
- ⑪ ITEM 254 - 3" PAVEMENT PLANING, ASPHALT CONCRETE
- ⑫ ITEM 617 - 3" COMPACTED AGGREGATE
- ⑬ ITEM 608 - 4" CONCRETE WALK
- ⑭ ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
- ⑮ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)
- ⑯ ITEM 609 - CURB, TYPE 6
- ⑰ ITEM 301 - 6" ASPHALT CONCRETE BASE, PG64-22, (449)

NOTE: ROADWAY TYPICAL SECTIONS INDICATE THE FINAL CONDITION OF 1.25 INCHES ASPHALT SURFACE COURSE PLACED UPON AN INTERMEDIATE COURSE. DURING MOT PHASES 1, 2, 3, AND 4, 3.0 INCHES OF ASPHALT INTERMEDIATE COURSE WILL TEMPORARILY BE PLACED. DURING MOT PHASE 5, 1.25 INCHES OF THE INTERMEDIATE COURSE WILL BE MILLED AND 1.25 INCHES OF ASPHALT SURFACE COURSE WILL BE PLACED. SEE MAINTENANCE OF TRAFFIC GENERAL NOTES AND TYPICAL SECTIONS.

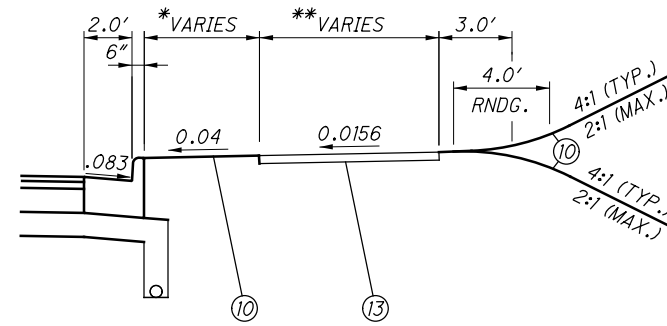
++ FLATTEN GUTTER CROSS SLOPE TO 0.016 WHERE INDICATED ON THE PLANS TO MATCH EXISTING PAVEMENT

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_C\001.dgn SIDEWALK TYP 7/15/2022 2:50:01PM troyer



SIDEWALK SECTION

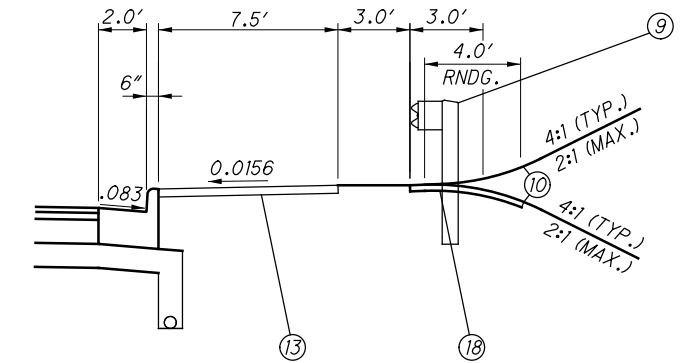
STA. 411+20.00 TO STA. 414+92.00 RT
 STA. 417+05.75 TO STA. 426+00.00 RT
 STA. 440+15.00 TO STA. 486+39.93 RT
 STA. 501+02.64 TO STA. 548+96.33 RT
 STA. 545+70.87 TO STA. 565+20.16 LT
 STA. 560+80.25 TO STA. 564+79.05 RT
 STA. 568+51.49 TO STA. 572+50.00 LT



SIDEWALK SECTION

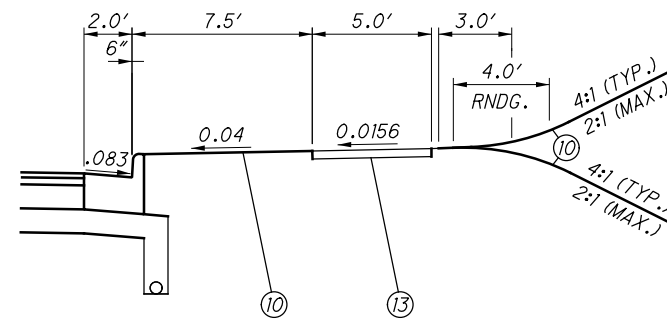
STA. 548+96.33 TO STA. 550+08.41 RT

*VARIES 0.0' TO 7.5'
 **VARIES 5.0' TO 7.5'



SIDEWALK / GUARDRAIL SECTION

STA. 414+92.00 TO STA. 417+05.75



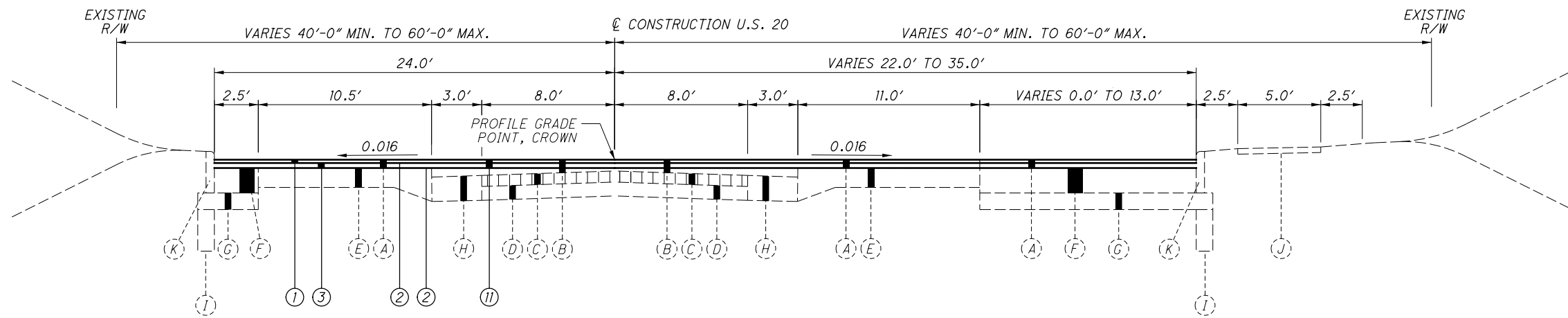
SIDEWALK SECTION

STA. 550+83.59 TO STA. 558+96.92 RT

LEGEND

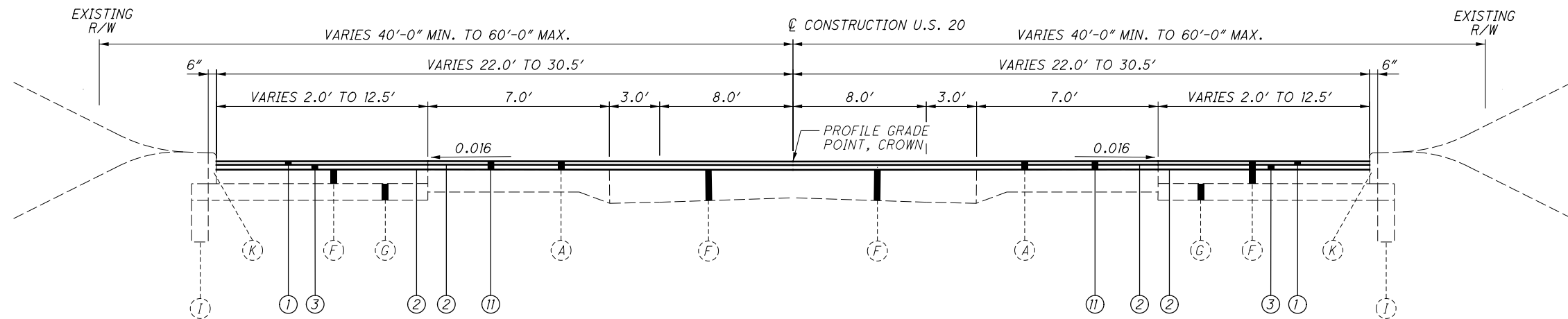
- ① ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG70-22M, AS PER PLAN
- ② ITEM 407 - NON-TRACKING TACK COAT
- ③ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446)
- ④ ITEM 301 - 6" ASPHALT CONCRETE BASE, (449), AS PER PLAN, PG64-22
- ⑤ ITEM 304 - 6" AGGREGATE BASE
- ⑥ ITEM 204 - SUBGRADE COMPACTION
- ⑦ ITEM 609 - COMBINATION CURB AND GUTTER, TYPE 2
- ⑧ ITEM 605 - 6" BASE PIPE UNDERDRAINS WITH GEOTEXTILE FABRIC
- ⑨ ITEM 606 - GUARDRAIL, TYPE MGS
- ⑩ ITEM 659 - SEEDING AND MULCHING, CLASS 1
- ⑪ ITEM 254 - 3" PAVEMENT PLANING, ASPHALT CONCRETE
- ⑫ ITEM 617 - 3" COMPACTED AGGREGATE
- ⑬ ITEM 608 - 4" CONCRETE WALK
- ⑭ ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
- ⑮ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)
- ⑯ ITEM 609 - CURB, TYPE 6
- ⑰ ITEM 301 - 6" ASPHALT CONCRETE BASE, PG64-22, (449)
- ⑱ ITEM 441 - 3" ASPHALT INTERMEDIATE COURSE, TYPE 1, (449), (UNDER GUARDRAIL), AS PER PLAN

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_C\001.dgn RESURFACING TYP 7/15/2022 2:50:02 PM troyer



U.S. 20 - RESURFACING 5 LANE SECTION

STA. 426+00.00 TO STA. 440+15.00



U.S. 20 - RESURFACING 4 TO 5 LANE SECTION

STA. 486+40.00 TO STA. 500+40.00

LEGEND

- | | |
|-------------------------|-----------------------------|
| (A) 3"-5" ASPHALT | (G) 6" AGGREGATE BASE |
| (B) 6"-8" ASPHALT | (H) 9" CONCRETE |
| (C) 4" BRICK | (I) 4" BASE PIPE UNDERDRAIN |
| (D) 4"-6" CONCRETE | (J) 4" CONCRETE WALK |
| (E) 7"-8" CONCRETE | (K) CONCRETE CURB |
| (F) 9" ASPHALT CONCRETE | |

LEGEND

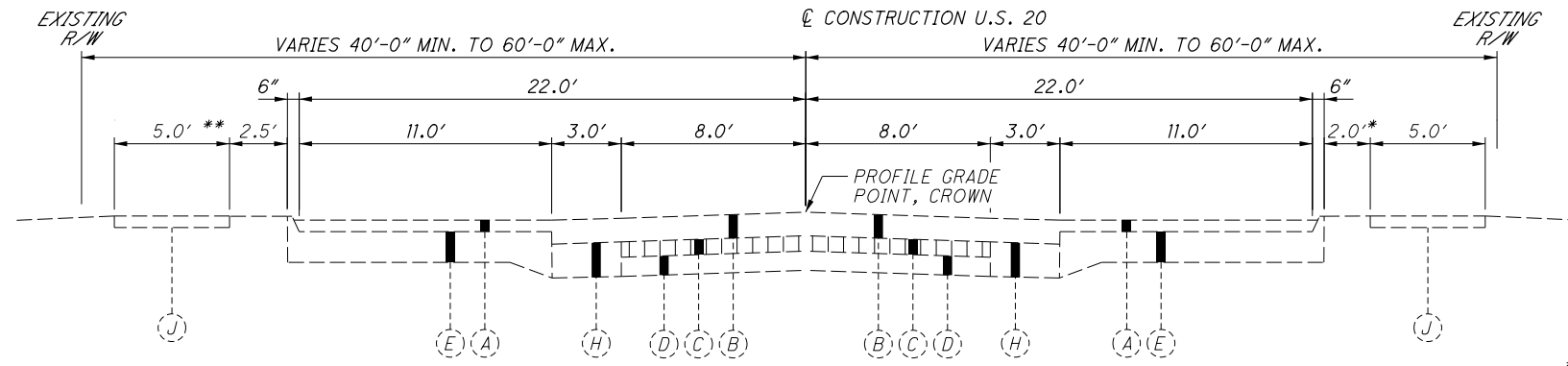
- ① ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG70-22M, AS PER PLAN
- ② ITEM 407 - NON-TRACKING TACK COAT
- ③ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446)
- ④ ITEM 301 - 6" ASPHALT CONCRETE BASE, (449), AS PER PLAN, PG64-22
- ⑤ ITEM 304 - 6" AGGREGATE BASE
- ⑥ ITEM 204 - SUBGRADE COMPACTION
- ⑦ ITEM 609 - COMBINATION CURB AND GUTTER, TYPE 2
- ⑧ ITEM 605 - 6" BASE PIPE UNDERDRAINS WITH GEOTEXTILE FABRIC
- ⑨ ITEM 606 - GUARDRAIL, TYPE MGS
- ⑩ ITEM 659 - SEEDING AND MULCHING, CLASS 1
- ⑪ ITEM 254 - 3" PAVEMENT PLANING, ASPHALT CONCRETE
- ⑫ ITEM 617 - 3" COMPACTED AGGREGATE
- ⑬ ITEM 608 - 4" CONCRETE WALK
- ⑭ ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22
- ⑮ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)
- ⑯ ITEM 609 - CURB, TYPE 6
- ⑰ ITEM 301 - 6" ASPHALT CONCRETE BASE, PG64-22, (449)

TYPICAL SECTIONS

LAK-US-20-24.99

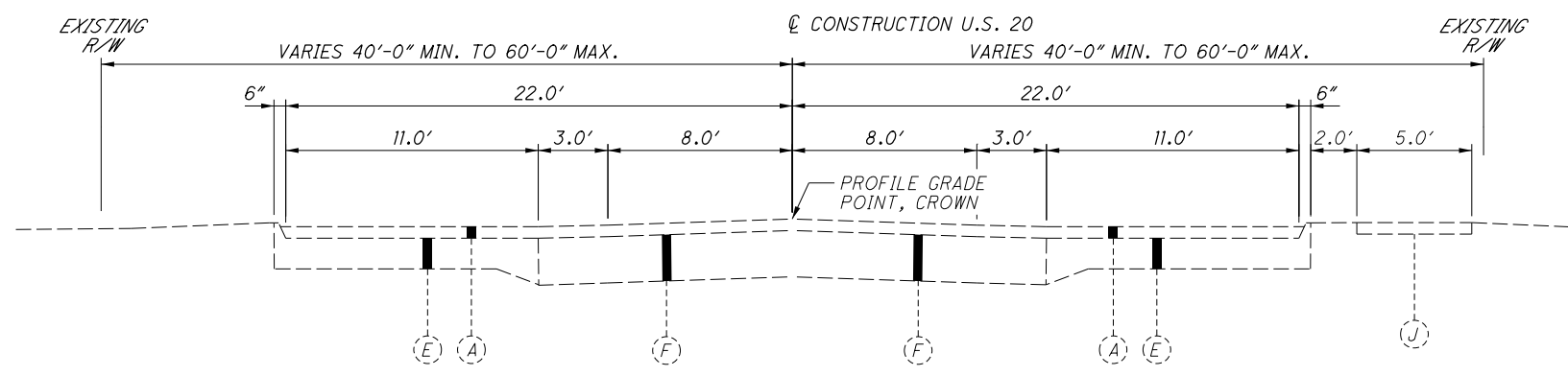
PART 2

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_C0001.dgn EXISTING TYP | 7/15/2022 2:50:03 PM Troyer



* VARIES STA 548+93.34 TO STA 560+15.00
 ** STA 545+80.22 TO STA 560+15.00

U.S. 20 - EXISTING 4 LANE SECTION
 STA. 440+15.00 TO STA. 486+40.00
 STA. 500+40.00 TO STA. 560+15.00

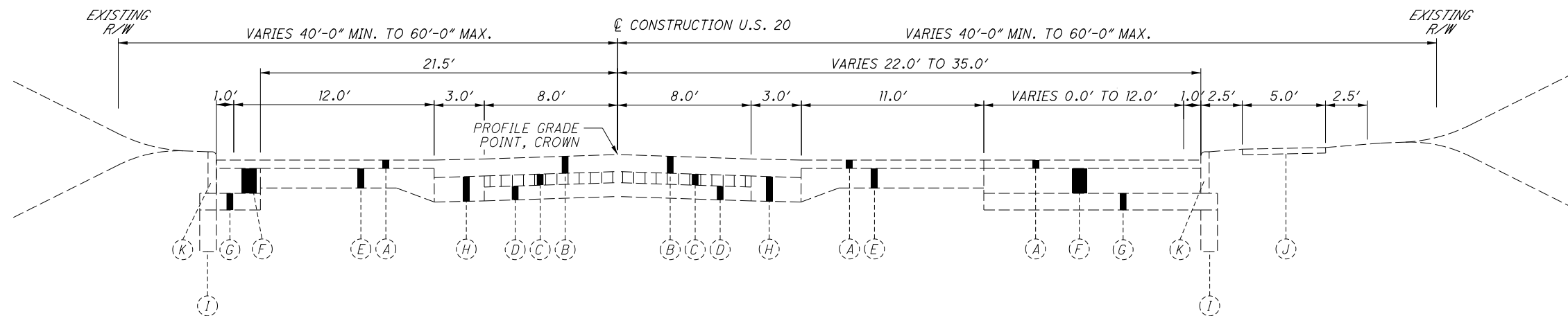


U.S. 20 - EXISTING 4 LANE SECTION
 STA. 411+20.00 TO STA. 426+00.00

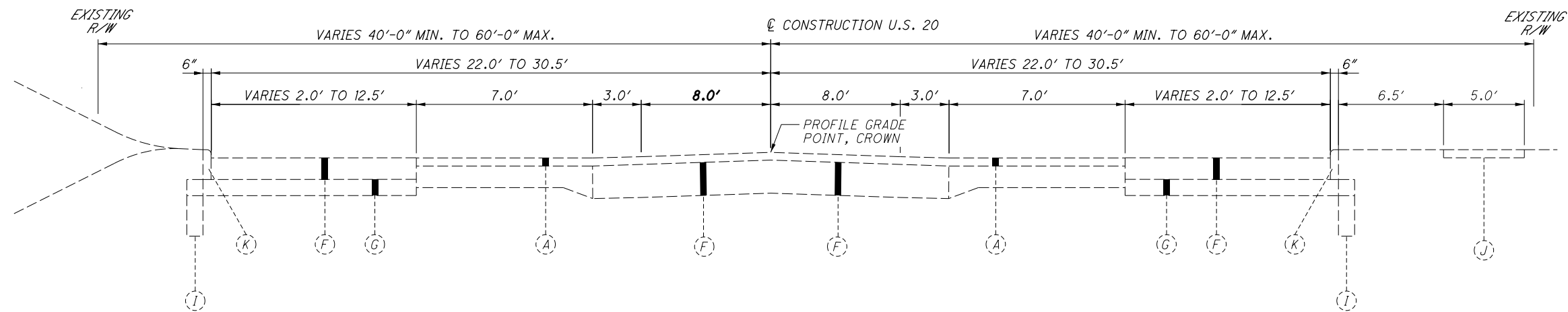
LEGEND

- | | |
|-------------------------|-----------------------------|
| (A) 3"-5" ASPHALT | (G) 6" AGGREGATE BASE |
| (B) 6"-8" ASPHALT | (H) 9" CONCRETE |
| (C) 4" BRICK | (I) 4" BASE PIPE UNDERDRAIN |
| (D) 4"-6" CONCRETE | (J) 4" CONCRETE WALK |
| (E) 7"-8" CONCRETE | (K) CONCRETE CURB |
| (F) 9" ASPHALT CONCRETE | |

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_C\001.dgn EXISTING TYP 2 7/15/2022 2:50:03 PM troyer



U.S. 20 - EXISTING 4 TO 5 LANE SECTION
STA. 426+00.00 TO STA. 440+15.00

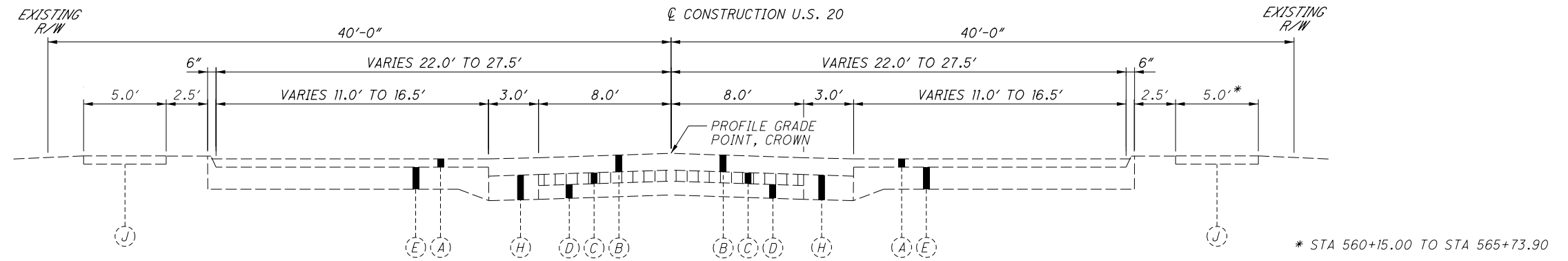


U.S. 20 - EXISTING 4 TO 5 LANE SECTION
STA. 486+40.00 TO STA. 500+40.00

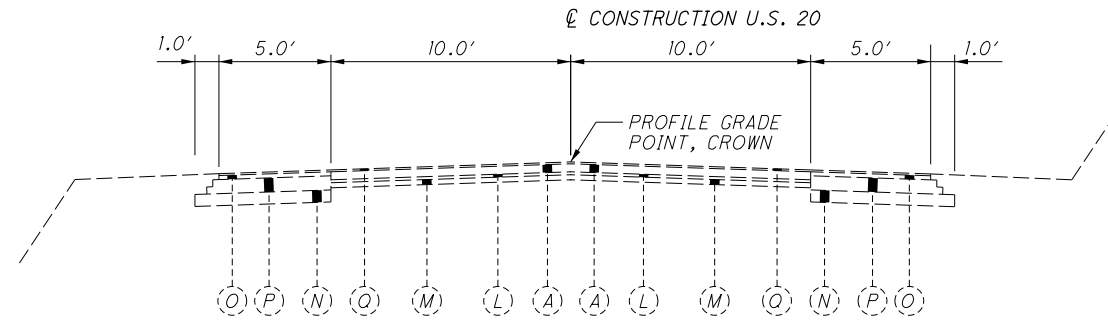
LEGEND

- | | |
|-------------------------|-----------------------------|
| (A) 3"-5" ASPHALT | (G) 6" AGGREGATE BASE |
| (B) 6"-8" ASPHALT | (H) 9" CONCRETE |
| (C) 4" BRICK | (I) 4" BASE PIPE UNDERDRAIN |
| (D) 4"-6" CONCRETE | (J) 4" CONCRETE WALK |
| (E) 7"-8" CONCRETE | (K) CONCRETE CURB |
| (F) 9" ASPHALT CONCRETE | |

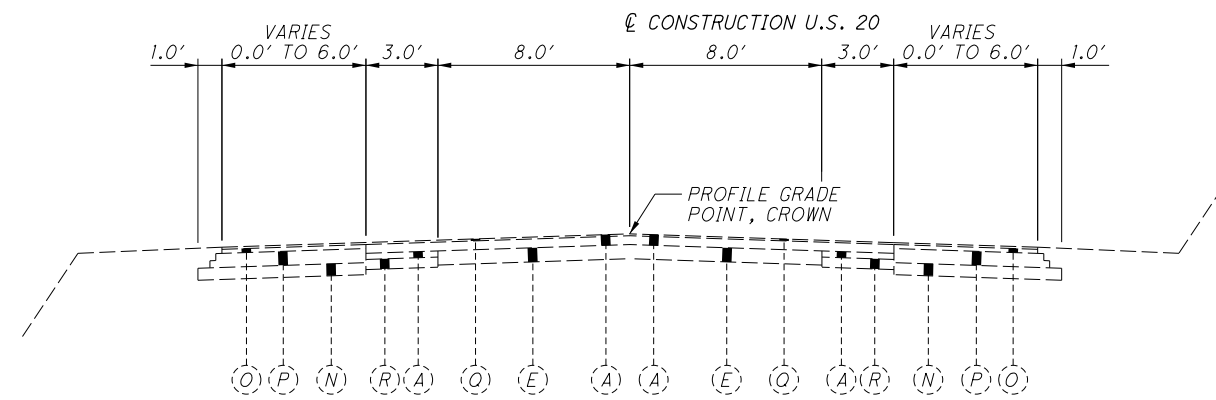
H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_C\001.dgn EXISTING TYP 3 7/15/2022 2:50:04 PM troyer



U.S. 20 - EXISTING 4 TO 5 LANE SECTION
STA. 560+15.00 TO STA. 572+50.00



HUBBARD ROAD - EXISTING 2 LANE SECTION
STA. 100+00.00 TO STA. 101+00.00



HUBBARD ROAD S.R. 528 - EXISTING 2 LANE SECTION
STA. 92+96.00 TO STA. 100+00.00

LEGEND

- | | |
|-----------------------------|------------------------------|
| (A) 3"-5" ASPHALT | (L) 1 1/2" SEAL COAT |
| (B) 6"-8" ASPHALT | (M) 2 1/2" AGGREGATE |
| (C) 4" BRICK | (N) 6" SANDY GRANULARE BASE |
| (D) 4"-6" CONCRETE | (O) 2" ASPHALT CONCRETE |
| (E) 7"-8" CONCRETE | (P) 7" ASPHALT CONCRETE BASE |
| (F) 9" ASPHALT CONCRETE | (Q) 1" ASPHALT CONCRETE |
| (G) 6" AGGREGATE BASE | (R) 5" GRANULAR SLAG |
| (H) 9" CONCRETE | |
| (I) 4" BASE PIPE UNDERDRAIN | |
| (J) 4" CONCRETE WALK | |
| (K) CONCRETE CURB | |

GENERAL

ROUNDING

THE ROUNDING AT SLOPE BREAKPOINTS SHOWN ON THE TYPICAL SECTIONS APPLIES TO ALL CROSS-SECTIONS EVEN THOUGH OTHERWISE SHOWN.

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

GAS
DOMINION ENERGY OHIO
320 SPRINGSIDE DR. SUITE 320
AKRON, OHIO 44333
330-664-2409

NORTHEAST OHIO NATURAL GAS CORP.
8470 STATION STREET
MENTOR, OHIO 44060
TIM REILLY: 440-701-5100

ELECTRIC
THE ILLUMINATING COMPANY
7755 AUBURN ROAD
CONCORD, OHIO 44077
FRED RANDOLPH: 440-358-4991

WATER & SANITARY
LAKE COUNTY DEPARTMENT OF UTILITIES
105 MAIN STREET
PAINESVILLE, OHIO 44077
SARAH CEROVSKI: 440-350-2652

STORM
OHIO DEPARTMENT OF TRANSPORTATION
5500 TRANSPORTATION BLVD.
GARFIELD HEIGHTS, OHIO 94125
216-581-2100

COMMUNICATIONS
AT&T
13630 LORAIN AVE. 2ND FLOOR
CLEVELAND, OHIO 99111
SCOTT KLEBE: 216-476-6142

CHARTER COMMUNICATIONS (SPECTRUM)
7820 DIVISION DRIVE
MENTOR, OHIO 44060
EMIL SYMISTER: 216-575-8016
EXT. 2165551105

WINDSTREAM
245 N. MAIN STREET
HUDSON, OHIO 44236
JEFF GULYAS: 216-385-1669

ZAYO
4199 KINROSS LAKES PARKWAY, SUITE 10
RICHFIELD, OH 44286
DAVE GALUSKA: 234-281-0025

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

EXISTING PLANS

EXISTING PLANS ENTITLED LAK-20-18.72, LAK-20-22.75, LAK-20-25.52 AND LAK-20-27.92 MAY BE INSPECTED IN THE ODOT DISTRICT 12 OFFICE IN GARFIELD HEIGHTS, OHIO.

CONSTRUCTION NOISE

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, DO NOT OPERATE POWER-OPERATED CONSTRUCTION-TYPE DEVICES BETWEEN THE HOURS OF 9 PM AND 7 AM. IN ADDITION, DO NOT OPERATE AT ANY TIME ANY DEVICE IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT.

SURVEYING PARAMETERS

PRIMARY PROJECT CONTROL MONUMENTS GOVERN ALL POSITIONING ON ODOT PROJECTS. SEE SHEETS 601-607 OF THE PLANS FOR TABLES CONTAINING PROJECT CONTROL INFORMATION.

USE THE FOLLOWING PROJECT CONTROL, VERTICAL POSITIONING, AND HORIZONTAL POSITIONING PARAMETERS FOR ALL SURVEYING:

PROJECT CONTROL

POSITIONING METHOD: ODOT VRS
MONUMENT TYPE: TYPE B

VERTICAL POSITIONING

ORTHOMETRIC HEIGHT DATUM: NAVD88
GEOID: 2012B

HORIZONTAL POSITIONING

REFERENCE FRAME: NAD83_(COORS2011)
ELLIPSOID: GRS80
MAP PROJECTION: LAMBERT CONFORMAL CONIC
COORDINATE SYSTEM: OHIO STATE PLANE, NORTH ZONE
COMBINED SCALE FACTOR: 1.000000885
ORIGIN OF COORDINATE SYSTEM: 0,0

USE THE POSITIONING METHODS AND MONUMENT TYPE USED IN THE ORIGINAL SURVEY TO RESTORE ALL MONUMENTS RELATED TO PRIMARY PROJECT CONTROL THAT ARE DAMAGED OR DESTROYED BY CONSTRUCTION ACTIVITIES. RESTORE THE DAMAGED OR DESTROYED MONUMENTS IN ACCORDANCE WITH CMS 623.

UNITS ARE IN U.S. SURVEY FEET.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

CLEARING AND GRUBBING

REMOVE ALL TREES AND STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE CONSTRUCTION LIMITS UNDER THE LUMP SUM BID FOR ITEM 201, CLEARING AND GRUBBING. THE FOLLOWING IS AN APPROXIMATE ESTIMATE OF THE NUMBER OF TREES AND STUMPS TO BE REMOVED.

SIZES	NO. TREES	NO. STUMPS	TOTAL
18"	6	6	6
30"	4	4	4
48"	3	3	3
60"	1	1	1

BENCHING OF FOUNDATION SLOPES

ALTHOUGH CROSS-SECTIONS INDICATE SPECIFIC DIMENSIONS FOR PROPOSED BENCHING OF THE EMBANKMENT FOUNDATIONS IN CERTAIN AREAS, NO WAIVER OF THE SPECIFICATIONS IS INTENDED. BENCH ALL OTHER SLOPED EMBANKMENT AREAS AS SET FORTH IN 203.05. NO ADDITIONAL PAYMENT WILL BE MADE FOR BENCHING REQUIRED UNDER THE PROVISIONS OF 203.05.

MONUMENT ASSEMBLIES

CONSTRUCT MONUMENT ASSEMBLIES IN ACCORDANCE WITH THE DETAILS SHOWN ON THE STANDARD CONSTRUCTION DRAWINGS AND AT THE LOCATIONS SHOWN ON SHEET NO. 578-584.

ITEM 204 - PROOF ROLLING

THE FOLLOWING QUANTITY IS PROVIDED IN THE GENERAL SUMMARY TO ADDRESS LOCATIONS REQUIRING PROOF ROLLING.

ITEM 204 - PROOF ROLLING 30 HOUR

AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPOINTS

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PRIVATE USE AIRPORT OR HELIPOINT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT, AT MAXIMUM OPERATING HEIGHT, SHALL EXCEED A HEIGHT OF ZERO (0) FT. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, COORDINATION WITH THE AIRPORT OWNER AND THE ODOT OFFICE OF AVIATION WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. FOR PRIVATE USE AIRPORTS OR HELIPOINTS, COORDINATE WITH THE AIRPORT OWNER AND THE ODOT OFFICE OF AVIATION. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL COORDINATION IS MET AND DOCUMENTATION HAS BEEN FURNISHED TO THE PROJECT ENGINEER. IF COORDINATION IS NOT OBTAINED, THEN THE PROJECT ENGINEER WILL HAVE THE AUTHORITY TO PROVIDE RESTRICTIONS AS REQUIRED.

LAKE HEALTH MADISON CAMPUS HELIPAD
LAKE HEALTH
6270 NORTH RIDGE ROAD
MADISON, OH 44057
(440) 428-0280

ITEM 204 - SUBGRADE COMPACTION AND PROOF ROLLING

CONSTRUCT THE SUBGRADE AS FOLLOWS AND IN THE FOLLOWING SEQUENCE:

1. SHAPE THE SUBGRADE TO WITHIN 0.2 FEET OF THE PLAN SUBGRADE ELEVATION.

2. EXCAVATE AND REPLACE UNSUITABLE SUBGRADE BEFORE PROOF ROLLING. THE EXCAVATION LIMITS ARE SHOWN AND LABELED ON THE CROSS SECTIONS AS UNSUITABLE SUBGRADE.

UNSUITABLE SUBGRADE INCLUDES UNSUITABLE SOIL (A-4B, A-2-5, A-5, A-7-5, AND SOIL WITH A LIQUID LIMIT GREATER THAN 65) AND ANY COAL, SHALE, OR ROCK WHICH NEEDS TO BE REMOVED ACCORDING TO 204.05.

IF THERE IS UNSUITABLE SUBGRADE IN A SHALLOW FILL LOCATION, EXCAVATE AND REPLACE THE UNSUITABLE SUBGRADE BEFORE CONSTRUCTING THE SHALLOW FILL AND SHAPING THE SUBGRADE.

3. COMPACT THE SUBGRADE ACCORDING TO 204.03.

4. APPROXIMATE LIMITS FOR EXCAVATION OF UNSTABLE SUBGRADE ARE SHOWN AND LABELED ON THE CROSS SECTIONS AS UNSTABLE SUBGRADE AND APPROXIMATE STATION LIMITS ARE AS FOLLOWS:

STA 465+00 TO STA 469+25 = 425 FT.

THE ENGINEER WILL IDENTIFY THE ACTUAL LIMITS OF EXCAVATION FOR UNSTABLE SUBGRADE BASED ON THE PROOF ROLLING RESULTS AND VISUAL OBSERVATIONS.

PROOF ROLL THE COMPACTED SUBGRADE ACCORDING TO 204.06.

5. EXCAVATE UNSTABLE SUBGRADE AS DIRECTED BY THE ENGINEER AND STABILIZE BY REPLACING WITH THE SPECIFIED MATERIALS ACCORDING TO 204.07. EXCAVATIONS WILL EXTEND 18 INCHES BEYOND THE EDGE OF THE SURFACE OF THE PAVEMENT, PAVED SHOULDERS, OR PAVED MEDIANS.

6. PROOF ROLL THE STABILIZED AREAS ACCORDING TO 204.06 TO VERIFY STABILITY.

7. FINE GRADE THE SUBGRADE TO THE SPECIFIED GRADE.

THE QUANTITIES FOR EXCAVATING THE UNSUITABLE SUBGRADE AND UNSTABLE SUBGRADE ARE BOTH PAID UNDER ITEM 204 EXCAVATION OF SUBGRADE.

THE FOLLOWING QUANTITIES FOR THE REMEDIATION OF UNSTABLE SUBGRADE HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 204 - EXCAVATION OF SUBGRADE	1,600 CY
ITEM 204 - GRANULAR MATERIAL, TYPE B	1,600 CY
ITEM 204 - GEOTEXTILE FABRIC	3,200 SY
ITEM 204 - GEOGRID	3,200 SY

ITEM SPECIAL - PRECONSTRUCTION VIDEO DOCUMENTATION

THIS WORK SHALL CONSIST OF FURNISHING THE DEPARTMENT A COMPLETE DIGITAL COLOR AUDIO-VIDEO RECORD OF THE SURFACE FEATURES WITHIN AND IMMEDIATELY ADJACENT (WITHIN 50 FEET) TO THE PROPOSED PROJECT AREA AS SHOWN IN THE CONTRACT DRAWINGS. THIS RECORD SHALL INCLUDE, BUT NOT BE LIMITED TO, ALL AUDIO-VIDEO USB THUMB DRIVES, VIDEO LOGS, AND INDEXES. THE PURPOSE OF THIS COVERAGE SHALL BE TO ACCURATELY DOCUMENT THE PRECONSTRUCTION CONDITION OF THESE SURFACE FEATURES.

THE AUDIO-VIDEO DOCUMENTATION SHALL BE PERFORMED BY A RESPONSIBLE COMMERCIAL FIRM KNOWN TO BE SKILLED AND REGULARLY ENGAGED IN THE BUSINESS OF PRECONSTRUCTION DIGITAL COLOR AUDIO-VIDEO DOCUMENTATION. THE FIRM SHALL FURNISH SUCH INFORMATION AS THE DEPARTMENT DEEMS NECESSARY TO DETERMINE THE ABILITY OF THAT FIRM TO PERFORM THE WORK IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS.

AUDIO-VIDEO DOCUMENTATION SHALL NOT BE MADE MORE THAN 60 DAYS PRIOR TO CONSTRUCTION IN EACH PHASE OF WORK.

THE AUDIO-VIDEO SYSTEM AND THE PROCEDURES EMPLOYED IN ITS USE SHALL BE SUCH AS TO PRODUCE A FINISHED PRODUCT THAT WILL MEET PROFESSIONAL STANDARDS. THE VIDEO PORTION OF THE RECORDING SHALL BE BRIGHT, SHARP, CLEAR PICTURES WITH ACCURATE COLORS AND SHALL BE FREE FROM DISTORTION OR ANY OTHER FORM OF PICTURE IMPERFECTION. ALL VIDEO RECORDINGS SHALL BY ELECTRONIC MEANS DISPLAY ON THE SCREEN THE TIME OF DAY, THE MONTH, DAY AND YEAR OF THE RECORDING. THE TIME AND DATE INFORMATION MUST BE CONTINUOUSLY AND SIMULTANEOUSLY GENERATED WITH THE ACTUAL RECORDING. THE AUDIO PORTION OF THE RECORDING SHALL BE PERFORMED BY THE CAMERA OPERATOR DURING THE RECORDING PROCESS AND SHALL BE OF HIGH CLARITY AND FREE FROM DISTORTION.

THE RECORDINGS SHALL CONTAIN COVERAGE OF ALL VISIBLE FEATURES WITHIN THE CONSTRUCTION ZONE BEING AFFECTED BY THE WORK. THESE FEATURES SHALL INCLUDE, BUT NOT BE LIMITED TO, ALL ROADWAYS, PAVEMENT, RETENTION PONDS, RAILROAD TRACKS, CURBS, DRIVEWAYS, SIDEWALKS, CULVERTS, HEAD-WALLS, RETAINING WALLS, LANDSCAPING, TREES, FENCES, DRAINAGE DITCHES, CREEKS, STREAMS, VISIBLE UTILITIES, STRUCTURES, AND BUILDINGS. OF PARTICULAR CONCERN SHALL BE THE CONDITION OF EXISTING VEGETATION, TERRAIN, AND STRUCTURES AND THE EXISTENCE OR NON-EXISTENCE OF ANY FAULTS, FRACTURES OR DEFECTS. PANNING, ZOOM-IN AND ZOOM OUT RATES SHALL BE SUFFICIENTLY CONTROLLED.

WRITTEN DOCUMENTATION MUST COINCIDE WITH THE INFORMATION ON THE AUDIO-VIDEO SO AS TO MAKE EASY RETRIEVAL OF LOCATIONS SOUGHT FOR AT A LATER DATE.

TWO COPIES OF THE DIGITAL COLOR AUDIO-VIDEO RECORDING SHALL BE DELIVERED TO THE DEPARTMENT ON USB THUMB DRIVES.

THE DEPARTMENT WILL MAKE PARTIAL PAYMENTS ACCORDING TO 109.09, PRORATED BY THE LENGTH OF THE WORK PHASE. VIDEO DOCUMENTATION WILL BE PAID FOR AT THE CONTRACT PRICE FOR:

ITEM SPECIAL - PRECONSTRUCTION VIDEO DOCUMENTATION, LUMP SUM

CALCULATED
JMP
CHECKED
WDB

GENERAL NOTES

LAK-US-20-24.99
PART 2

H:\2018\80743\ODOT\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CN001.dgn Sheet 11/15/2022 2:50:04 PM troyer

H:\2018\180743\0D0T\Project\Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CN001.dgn Sheet 2 7/15/2022 2:50:05 PM troyer

ROADWAY

CONNECTION BETWEEN EXISTING AND PROPOSED GUARDRAIL

WHEN IT IS NECESSARY TO SPLICE PROPOSED GUARDRAIL TO EXISTING GUARDRAIL, ONLY THE EXISTING GUARDRAIL SHALL BE CUT, DRILLED, OR PUNCHED. THE CONNECTION SHALL BE MADE USING A W-BEAM, BEAM SPLICE AS SHOWN IN AASHTO M 180-12, EXCEPT THE BEAM WASHERS ARE NOT TO BE USED. PAYMENT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE RESPECTIVE GUARDRAIL ITEMS.

PAVING UNDER GUARDRAIL

THIS OPERATION SHALL INCLUDE PREPARATION OF THE GRADED SHOULDER USING ITEM 209, LINEAR GRADING, AS PER PLAN AND PAVING UNDER THE GUARDRAIL USING 441 ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (449), UNDER GUARDRAIL, AS PER PLAN.

ITEM 209, LINEAR GRADING, AS PER PLAN SHALL CONSIST OF EXCAVATING TOPSOIL, AND PLACING GRANULAR MATERIAL.

ALL COLLECTED DEBRIS AND TOPSOIL, INCLUDING RHIZOMES, ROOTS AND OTHER VEGETATIVE PLANT MATERIAL SHALL BE REMOVED AND DISPOSED OF AS SPECIFIED IN 105.17.

THE REMOVED MATERIAL SHALL BE REPLACED WITH COMPACTABLE GRANULAR MATERIAL CONFORMING TO 703.16 PLACED TO GRADE AS DETAILED ON THE TYPICAL SECTION OR AS APPROVED BY THE ENGINEER.

ALL EQUIPMENT, MATERIALS AND LABOR REQUIRED TO PERFORM THE WORK OUTLINED ABOVE SHALL BE INCLUDED FOR PAYMENT UNDER ITEM 209, LINEAR GRADING, AS PER PLAN.

PAVING UNDER GUARDRAIL SHALL CONSIST OF PLACING ITEM 441 TO THE DEPTH SPECIFIED USING ONE OF THE FOLLOWING METHODS:

METHOD A:

- 1. SET GUARDRAIL POSTS
- 2. PLACE ITEM 441

METHOD B:

- 1. PLACE ITEM 441
- 2. BORE ASPHALT AT POST LOCATIONS (MAY BE OMITTED IF STEEL POSTS ARE USED)
- 3. SET GUARDRAIL POSTS
- 4. PATCH AROUND POSTS. THE MATERIALS USED FOR

PATCHING SHALL BE AN ASPHALT CONCRETE APPROVED BY THE ENGINEER. PATCHED AREAS SHALL BE COMPACTED USING EITHER HAND OR MECHANICAL METHODS. FINISHED SURFACES SHALL BE SMOOTH AND SLOPED TO DRAIN AWAY FROM THE POSTS.

ALL EQUIPMENT, MATERIALS AND LABOR REQUIRED TO PERFORM THE WORK OUTLINED ABOVE, WITH THE EXCEPTION OF SETTING GUARDRAIL POSTS, SHALL BE INCLUDED FOR PAYMENT UNDER ITEM 441, ASPHALT CONCRETE, INTERMEDIATE COURSE, TYPE 1, (449), UNDER GUARDRAIL, AS PER PLAN.

ITEM SPECIAL - MAILBOX SUPPORT

THIS WORK SHALL CONSIST OF FURNISHING AND ERECTING MAILBOX SUPPORTS AND ANY ASSOCIATED MOUNTING HARDWARE IN ACCORDANCE WITH PLAN DETAILS, AND ATTACHING AN OWNER-SUPPLIED MAILBOX AT LOCATIONS SPECIFIED IN THE PLAN, OR OTHERWISE ESTABLISHED BY THE ENGINEER.

WOOD POSTS SHALL BE NOMINAL 4 INCHES BY 4 INCHES SQUARE OR 4.5 INCHES DIAMETER ROUND, AND CONFORM TO 710.14.

STEEL POSTS SHALL BE NOMINAL PIPE SIZE 2 INCHES I.D., AND CONFORM TO AASHTO M 181.

ALL HARDWARE INCLUDING BUT NOT LIMITED TO PLATES, SCREWS, BOLTS, AND ETC. SHALL BE COMMERCIAL-GRADE GALVANIZED STEEL.

POSTS SHALL BE SET PER THE FIRST PARAGRAPH OF 606.03, AND SHALL IN NO INSTANCE BE ENCASED IN CONCRETE.

SUPPORT HARDWARE SHALL ACCOMMODATE EITHER A SINGLE OR A DOUBLE MAILBOX INSTALLATION, AND NO MORE THAN TWO BOXES MAY BE MOUNTED ON A SINGLE POST.

THE MAILBOX SHALL BE SECURELY AND NEATLY ATTACHED BY THE CONTRACTOR TO THE NEW SUPPORT. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ATTACHMENT HARDWARE (NUTS, BOLTS, PLATES, SPACERS, AND WASHERS) AS NECESSARY TO ACCOMMODATE THE COMPLETE INSTALLATION.

IN THE ABSENCE OF A NEW BOX SUPPLIED BY THE OWNER, THE CONTRACTOR SHALL SALVAGE THE EXISTING BOX AND PLACE IT ON THE NEW SUPPORT. DUE CARE SHALL BE EXERCISED IN SUCH AN OPERATION, AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY BOX DAMAGED BY IMPROPER HANDLING ON HIS PART, AS JUDGED AND DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE LOCAL POST MASTER REGARDING THE TIMING OF THE MOVEMENT OF ANY MAILBOX TO A NEW LOCATION.

PAYMENT UNDER THIS ITEM SHALL BE LIMITED TO FINAL PERMANENT INSTALLATIONS. TEMPORARY INSTALLATIONS SHALL BE IN ACCORDANCE WITH 107.10. HOWEVER, THE SAME MATERIAL AND SIZE LIMITATIONS AS FOR PERMANENT INSTALLATIONS SHALL APPLY.

MAILBOX SUPPORTS, COMPLETE IN PLACE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH, FOR ITEM SPECIAL MAILBOX SUPPORT SYSTEM, (SINGLE) (DOUBLE).

ITEM 202 REMOVAL MISC.: BUSINESS SIGN

REMOVE THE SIGN, SUPPORT, FOUNDATION AND ANY ASSOCIATED WIRING AS INDICATED FOR REMOVAL WITHIN THE LIMITS OF THE RIGHT OF WAY OR TEMPORARY RIGHT-OF WAY. CUT AND CAP THE EXISTING WIRING AT THE RIGHT OF WAY LINE. REMOVE FOUNDATIONS TO A MINIMUM OF 1 FOOT BELOW THE PROPOSED GROUND SURFACE OR REMOVE ENTIRE FOUNDATION IF NECESSARY FOR THE COMPLETION OF OTHER WORK IN THE CONTRACT. BACKFILL THE CAVITY CREATED BY THE REMOVAL ITEM ACCORDING TO 202.02, EXCEPT WHEN THE CAVITY LIES WITHIN THE LIMITS OF SUBSEQUENT EXCAVATION OR OTHER WORK.

DELIVER THE EXISTING SIGN TO THE PROPERTY OWNER OR DISPOSE OF IF THE OWNER DOES NOT WANT TO SALVAGE THE REMOVED MATERIALS. THE CONTRACTOR SHALL COORDINATE WITH THE PROPERTY OWNER REGARDING THIS PROPOSED WORK FOR THE DISCONNECTION OF THE CIRCUIT AND TO DETERMINE THE PROPERTY OWNERS INTENT TO SALVAGE THE EXISTING BUSINESS SIGN.

THE ABOVE NOTED WORK SHALL BE COMPLETED AT THE DIRECTION OF THE ENGINEER. PAYMENT FOR ALL OF THE ABOVE NOTED WORK SHALL BE INCLUDED IN THE CONTRACT PRICE FOR:

ITEM 202 REMOVAL MISC.: BUSINESS SIGN, EACH

ITEM 202 REMOVAL MISC.: BOULDER

REMOVE THE BOULDERS AS INDICATED FOR REMOVAL IN THEIR ENTIRETY. BACKFILL ANY CAVITY CREATED BY THE REMOVAL ITEM ACCORDING TO 202.02, EXCEPT WHEN THE CAVITY LIES WITHIN THE LIMITS OF SUBSEQUENT EXCAVATION OR OTHER WORK.

BOULDERS REMOVED AND ALL OF THE ABOVE WORK WILL BE PAID FOR AT THE CONTRACT PRICE FOR:

ITEM 202 REMOVAL MISC.: BOULDER, EACH

ITEM 202 REMOVAL MISC.: LIGHT POLE

REMOVE THE LUMINAIRE, SUPPORT, FOUNDATION AND ANY ASSOCIATED WIRING AS INDICATED FOR REMOVAL WITHIN THE LIMITS OF THE RIGHT OF WAY OR TEMPORARY RIGHT OF WAY. CUT AND CAP THE EXISTING WIRING AT THE RIGHT OF WAY LINE. REMOVE FOUNDATIONS TO A MINIMUM OF 1 FOOT BELOW THE PROPOSED GROUND SURFACE OR REMOVE ENTIRE FOUNDATION IF NECESSARY FOR THE COMPLETION OF OTHER WORK IN THE CONTRACT. BACKFILL THE CAVITY CREATED BY THE REMOVAL ITEM ACCORDING TO 202.02, EXCEPT WHEN THE CAVITY LIES WITHIN THE LIMITS OF SUBSEQUENT EXCAVATION OR OTHER WORK.

DELIVER THE EXISTING LUMINAIRE AND SUPPORT TO THE PROPERTY OWNER OR DISPOSE OF IF THE OWNER DOES NOT WANT TO SALVAGE REMOVED MATERIALS. THE CONTRACTOR SHALL COORDINATE WITH THE PROPERTY OWNER REGARDING THIS PROPOSED WORK FOR THE DISCONNECTION OF THE CIRCUIT AND TO DETERMINE THE PROPERTY OWNERS INTENT TO SALVAGE THE EXISTING LUMINAIRE AND SUPPORT.

THE ABOVE NOTED WORK SHALL BE COMPLETED AT THE DIRECTION OF THE ENGINEER. PAYMENT FOR ALL OF THE ABOVE NOTED WORK SHALL BE INCLUDED IN THE CONTRACT PRICE FOR:

ITEM 202 REMOVAL MISC.: LIGHT POLE, EACH

ITEM 202 REMOVAL MISC.: SHED

REMOVE AND DISPOSE OF THE SHED IN ITS ENTIRETY AS INDICATED.

THE ABOVE NOTED WORK SHALL BE COMPLETED AT THE DIRECTION OF THE ENGINEER. PAYMENT FOR ALL OF THE ABOVE NOTED WORK SHALL BE INCLUDED IN THE CONTRACT PRICE FOR:

ITEM 202 REMOVAL MISC.: SHED, EACH

EROSION CONTROL

SEEDING AND MULCHING

THE FOLLOWING QUANTITIES ARE PROVIDED TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDED AREAS:

659, SOIL ANALYSIS TEST	2 EACH
659, TOPSOIL	6717 CU. YD.
659, SEEDING AND MULCHING, CLASS 1	60506 SQ. YD.
659, REPAIR SEEDING AND MULCHING	3026 SQ. YD.
659, INTER-SEEDING	3026 SQ. YD.
659, COMMERCIAL FERTILIZER	8.44 TON
659, LIME	12.50 ACRES
659, WATER	344 M. GAL.

SEEDING AND MULCHING SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE RIGHT-OF-WAY LINES, AND WITHIN THE CONSTRUCTION LIMITS FOR AREAS OUTSIDE THE RIGHT-OF-WAY LINES COVERED BY WORK AGREEMENT OR SLOPE EASEMENT. QUANTITY CALCULATIONS FOR SEEDING AND MULCHING ARE BASED ON LIMITS IDENTIFIED AS NECESSARY IN THE CROSS-SECTIONS. ANY ADDITIONAL AREAS OUTSIDE OF THE AREAS IDENTIFIED IN THE CROSS-SECTIONS THAT ARE DISTURBED BY THE CONTRACTOR TO FACILITATE CONSTRUCTION MUST BE RESTORED IN ACCORDANCE WITH C&MS 107.10 AND CONSIDERED INCIDENTAL TO THE WORK. NO ADDITIONAL COMPENSATION WILL BE MADE FOR THESE AREAS.

CALCULATED
JMP
CHECKED
WDB

GENERAL NOTES

LAK-US-20-24.99
PART 2

ENVIRONMENTAL

NOTIFICATION OF COMMENCEMENT

THE CONTRACTOR SHALL NOTIFY THE FOLLOWING A MINIMUM OF TWO WEEKS PRIOR TO COMMENCEMENT OF WORK:

PAINESVILLE TOWNSHIP	440-352-1443
PAINESVILLE TOWNSHIP FIRE	440-358-6996
RIVERSIDE LOCAL SCHOOLS	440-352-0668
PERRY TOWNSHIP	440-259-5140
PERRY JOINT FIRE DISTRICT	440-259-3251
PERRY LOCAL SCHOOLS	440-259-9200
NORTH PERRY VILLAGE	440-259-4994
NORTH PERRY VILLAGE POLICE	440-259-4994
MADISON TOWNSHIP	440-428-5128
MADISON FIRE DISTRICT	440-428-1522
MADISON TOWNSHIP POLICE	440-428-2116
MADISON LOCAL SCHOOLS	440-428-2166
LAKE COUNTY SHERIFF	
(PAINESVILLE TWP., PERRY TWP.)	440-350-5601

ENDANGERED BAT HABITAT REMOVAL

THIS PROJECT IS LOCATED WITHIN THE KNOWN HABITAT RANGES OF THE FEDERALLY LISTED AND PROTECTED INDIANA BAT AND NORTHERN LONG-EARED BAT. NO TREES SHALL BE REMOVED UNDER THIS PROJECT FROM APRIL 1 THROUGH SEPTEMBER 30. ALL NECESSARY TREE REMOVAL SHALL OCCUR FROM OCTOBER 1 THROUGH MARCH 31. THIS REQUIREMENT IS NECESSARY TO AVOID AND MINIMIZE IMPACTS TO THESE SPECIES AS REQUIRED BY THE ENDANGERED SPECIES ACT. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS: A LIVE, DYING, OR DEAD WOODY PLANT, WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE THE GROUND SURFACE, AND WITH A MINIMUM HEIGHT OF 13 FEET.

ITEM SPECIAL - PETROLEUM CONTAMINATED SOILS

STUDIES INDICATE THAT PETROLEUM CONTAMINATED SOILS WILL BE ENCOUNTERED DURING EXCAVATIONS FOR CONSTRUCTION ACTIVITIES AT:

STA 431+00 TO STA 434+50
5477 NORTH RIDGE ROAD
STA 462+90 TO STA 470+00
5745 NORTH RIDGE ROAD
STA 565+00 TO STA 568+50
STA 97+00 TO STA 100+00 (HUBBARD)
6686 NORTH RIDGE ROAD

THE CONTRACTOR SHALL MANAGE THE EXCAVATED MATERIAL AS FOLLOWS. THE ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THIS WORK. ALL EXCAVATIONS AT THE AFOREMENTIONED LOCATION SHALL BE PAID FOR UNDER THE ORIGINAL PLAN BID ITEMS.

THE ENGINEER MAY PERMIT THE CONTRACTOR TO DIRECT LOAD THE EXCAVATED MATERIAL INTO TRUCKS FOR TRANSPORT AND DISPOSAL. AS AN ALTERNATE, THE ENGINEER MAY PERMIT THE CONTRACTOR TO TEMPORARILY STOCKPILE THE EXCAVATED MATERIAL ON AN IMPERMEABLE MEMBRANE, IN AN AREA PROVIDED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER. THE MEMBRANE SHALL BE SURROUNDED BY BALES OF STRAW TO PREVENT THE EXCAVATED MATERIAL FROM COMING IN CONTACT WITH THE UNDERLYING SOIL. AN IMPERMEABLE MEMBRANE SHALL BE PLACED OVER THE STOCKPILE TO PREVENT CONTACT WITH PRECIPITATION AND/OR SURFACE RUN-OFF.

THE EXCAVATED MATERIAL SHALL BE PROPERLY TRANSPORTED AND DISPOSED OF AS A SOLID WASTE IN A LICENSED (BY THE LOCAL HEALTH DEPARTMENT) AND PERMITTED (BY THE OHIO ENVIRONMENTAL PROTECTION AGENCY) SOLID WASTE FACILITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY TESTING, PERMITS, AND APPROVALS TO TRANSPORT AND DISPOSE OF THE MATERIAL. IF EXCAVATIONS WITHIN THE AFOREMENTIONED LIMITS REQUIRE DEWATERING FOR CONSTRUCTION PURPOSES, THE CONTRACTOR SHALL DEWATER, CONTAINERIZE AND DISPOSE OF BY METHODS APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL OBTAIN ALL THE NECESSARY PERMITS AND/OR AUTHORIZATIONS NEEDED TO STORE, TEST, TRANSPORT AND DISPOSE OF THE WATER IN ACCORDANCE WITH APPLICABLE LOCAL, STATE OR FEDERAL REGULATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DISPOSAL OF REGULATED WATER WITH A METHOD APPROVED BY THE ENGINEER. WORK INVOLVED WITH THIS ITEM SPECIAL INCLUDES COMPLYING WITH THE HANDLING, STORAGE, AND DISPOSAL OF REGULATED AND NON-REGULATED WATER.

PAYMENT FOR THIS WORK SHALL BE MADE AT THE CONTRACT PRICE BID PER TON AND/OR GALLON. THE BASIS FOR CONVERSION FROM TONS TO CUBIC YARDS IS 1.5 TON/CUBIC YARD. ALL EXCAVATIONS WITHIN THE AFOREMENTIONED LIMITS SHALL BE PAID FOR UNDER THE ORIGINAL PLAN BID ITEMS. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE WORK NOTED ABOVE:

690E65016 ITEM SPECIAL - WORK INVOLVING PETROLEUM CONTAMINATED SOIL	3800 TON
690E65024 ITEM SPECIAL - WORK INVOLVING REGULATED WATER	1900 GALLON
690E65022 ITEM SPECIAL - WORK INVOLVING NON-REGULATED WATER	1900 GALLON

DRAINAGE

ITEM SPECIAL - FILL AND PLUG EXISTING CONDUIT

THIS ITEM SHALL CONSIST OF THE CONSTRUCTION OF BULKHEADS IN AN EXISTING CONDUIT AND FILLING THE AREA THUS SEALED OFF WITH ITEM 613, SAND OR OTHER MATERIAL APPROVED BY THE ENGINEER.

BULKHEADS SHALL BE LOCATED AT THE LIMITS OF THE AREA TO BE FILLED AS INDICATED ON THE PLANS. THE BULKHEADS SHALL CONSIST OF BRICK OR CONCRETE MASONRY WITH A MINIMUM THICKNESS OF 12 INCHES.

THE FILL MATERIAL SHALL BE PUMPED INTO PLACE, OR PLACED BY OTHER MEANS APPROVED BY THE ENGINEER, SO THAT, AFTER SETTLEMENT, AT LEAST 90 PERCENT OF THE GROSS SECTIONAL AREA OF THE CONDUIT, FOR ITS ENTIRE LENGTH, SHALL BE FILLED. THE LENGTH OF FILLED AND PLUGGED CONDUIT TO BE PAID FOR SHALL BE THE ACTUAL NUMBER OF FEET (MEASURED ALONG THE CENTERLINE OF EACH CONDUIT FROM OUTER FACE TO OUTER FACE OF BULKHEADS) FILLED AND PLUGGED AS DESCRIBED ABOVE.

THE LENGTH, MEASURED AS PROVIDED ABOVE, SHALL BE PAID FOR AT THE CONTRACT PRICE PER FOOT FOR, ITEM SPECIAL, FILL AND PLUG EXISTING CONDUIT.

CROSSINGS AND CONNECTIONS TO EXISTING PIPES AND UTILITIES

WHERE PLANS PROVIDE FOR A PROPOSED CONDUIT TO BE CONNECTED TO, OR CROSS OVER OR UNDER AN EXISTING SEWER OR UNDERGROUND UTILITY, THE CONTRACTOR SHALL LOCATE THE EXISTING PIPES OR UTILITIES BOTH AS TO LINE AND GRADE BEFORE STARTING TO LAY THE PROPOSED CONDUIT.

IF IT IS DETERMINED THAT THE ELEVATION OF THE EXISTING CONDUIT, OR EXISTING APPURTENANCE TO BE CONNECTED, DIFFERS FROM THE PLAN ELEVATION OR RESULTS IN A CHANGE IN THE PLAN CONDUIT SLOPE, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WILL BE AFFECTED BY THE VARIANCE IN THE EXISTING ELEVATIONS.

IF IT IS DETERMINED THAT THE PROPOSED CONDUIT WILL INTERSECT AN EXISTING SEWER OR UNDERGROUND UTILITY IF CONSTRUCTED AS SHOWN ON THE PLAN, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WOULD BE AFFECTED BY THE INTERFERENCE WITH AN EXISTING FACILITY.

PAYMENT FOR ALL THE OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEM.

PIPE CONNECTIONS TO CORRUGATED METAL STRUCTURES

CONNECTIONS OF PROPOSED LONGITUDINAL DRAINAGE TO CORRUGATED METAL STRUCTURES SHALL BE MADE BY MEANS OF A SHOP FABRICATED OR FIELD WELDED STUB ON THE STRUCTURE. THE STUB SHALL MEET THE REQUIREMENTS OF 707 AND HAVE A MINIMUM LENGTH OF 2 FEET AND A MINIMUM WALL THICKNESS OF 0.064 INCHES.

THE LOCATION AND ELEVATION OF THE STUB ARE TO BE CONSIDERED APPROXIMATE AND MAY BE ADJUSTED BY THE ENGINEER TO AVOID CUTTING THROUGH JOINTS IN THE STRUCTURE.

THE FIELD WELDED JOINT, IF USED, SHALL BE THOROUGHLY CLEANED AND REGALVANIZED OR OTHERWISE SUITABLY REPAIRED. WELDING SHALL MEET THE REQUIREMENTS OF 513.21.

A MASONRY COLLAR, AS PER STANDARD DRAWING DM-1.1, WILL BE REQUIRED TO CONNECT THE LONGITUDINAL DRAINAGE TO THE STUB, WHEN PIPE OTHER THAN CORRUGATED METAL IS PROVIDED FOR THE LONGITUDINAL DRAINAGE.

PAYMENT FOR CUTTING INTO THE STRUCTURE AND PROVIDING THE CONNECTION DESCRIBED, SHALL BE INCLUDED IN THE CONTRACT PRICE FOR ITEM 611 OR 522.

ITEM 605 - AGGREGATE DRAINS

AGGREGATE DRAINS SHALL BE PLACED AT 50 FOOT INTERVALS ON EACH SIDE OF NORMAL CROWNED SECTIONS, STAGGERED SO THAT EACH DRAIN IS 25 FEET FROM THE ADJACENT DRAIN ON THE OPPOSITE SIDE, AND AT 25 FOOT INTERVALS ON THE LOW SIDE ONLY OF SUPERELEVATED SECTIONS. AN AGGREGATE DRAIN SHALL BE PLACED AT THE LOW POINT OF EACH SAG VERTICAL CURVE. LINE THE AGGREGATE DRAIN TRENCH WITH GEOTEXTILE FABRIC. PLACE THE GEOTEXTILE FABRIC TO COMPLETELY SURROUND THE GRANULAR MATERIAL. OVERLAP THE GEOTEXTILE FABRIC AT THE TOP OF THE TRENCH. MATCH THE OVERLAP TO THE TRENCH WIDTH. AT OTHER SEAMS, OVERLAP GEOTEXTILE FABRIC A MINIMUM OF 12 INCHES.

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS WORK AS DIRECTED BY THE DEPARTMENT:

ITEM 605 AGGREGATE DRAINS	250 FT
---------------------------	--------

ITEM 611 CATCH BASIN ADJUSTED TO GRADE. AS PER PLAN

ALL CASTINGS SHALL BE ADJUSTED TO THE FINISHED ROADWAY ELEVATION BY THE CONTRACTOR. THE TIME BETWEEN ADJUSTING THE THE CASTINGS AND RESURFACING SHALL BE KEPT TO AN ABSOLUTE MINIMUM. NO ADJUSTING RINGS SHALL BE PERMITTED.

THE FOLLOWING QUANTITY IS PROVIDED TO ADJUST EXISTING CATCH BASINS TO GRADE IN RESURFACING AREAS OF THE PROJECT, AS DIRECTED BY THE DEPARTMENT.

ITEM 611 CATCH BASIN ADJUSTED TO GRADE, AS PER PLAN, 2Z EACH	
--------------------------------------------------------------	--

EXISTING SUBSURFACE DRAINAGE

PROVIDE UNOBSTRUCTED OUTLETS FOR ALL EXISTING UNDERDRAINS OR AGGREGATE DRAINS ENCOUNTERED DURING CONSTRUCTION.

PROVIDE AN OUTLET PER STANDARD CONSTRUCTION DRAWING DM-1.1 FOR ALL UNDERDRAINS THAT OUTLET TO A SLOPE.

UNDERDRAINS THAT CAN BE CONNECTED TO THE NEW OR EXISTING UNDERDRAINS AT THE END OF THE PROJECT LIMITS AS WELL AS ALL NECESSARY BENDS OR BRANCHES REQUIRED FOR CONNECTION ARE INCLUDED IN THE BASIS OF PAYMENT FOR UNCLASSIFIED PIPE UNDERDRAINS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE WORK NOTED ABOVE:

ITEM 605 - AGGREGATE DRAINS	200 FT.
ITEM 605 - 6" UNCLASSIFIED PIPE UNDERDRAINS WITH GEOTEXTILE FABRIC	500 FT.
ITEM 611 - 6" CONDUIT, TYPE F	50 FT.

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CN001.dgn Sheet 3 7/15/2022 2:50:06 PM troyer

CALCULATED
JMP
CHECKED
WDB

GENERAL NOTES

LAK-US-20-24.99
PART 2

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CN001.dgn Sheet 4 7/15/2022 2:50:07 PM troyer

DRAINAGE DISCHARGE CONTINUANCE

FURNISH A DRAINAGE DISCHARGE CONTINUANCE FOR ANY DRAINAGE DISCHARGE DISTURBED BY THE WORK AND NOT SHOWN IN THE PLANS. THE LOCATION, TYPE (CONDUIT OR SWALE), SIZE AND GRADE OF THE DRAINAGE DISCHARGE CONTINUANCE WILL BE AGREED TO BY THE ENGINEER

FURNISH AN INSPECTION WELL AT THE RIGHT OF WAY LINE IN ACCORDANCE WITH SCD DM-3.1 FOR EACH DRAINAGE DISCHARGE THAT OUTLETS THROUGH A CURB OPENING, OR INTO A STORM SEWER OR DRAINAGE STRUCTURE. THE COST IS INCLUDED IN ITEM 611, INSPECTION WELL.

FURNISH A DRILLED HOLE OR A CURB SECTION WITH A HOLE WHEN OUTLETING A CONDUIT THROUGH A CURB OPENING. THE COST OF DRILLING, OR FURNISHING THE CURB SECTION WITH HOLE IS INCLUDED IN ITEM 611, CONDUIT, MISC TYPE - FOR DRAINAGE DISCHARGE CONTINUANCE.

FURNISH A DRILLED CORE HOLE WHEN OUTLETING INTO A STORM SEWER OR DRAINAGE STRUCTURE. THE COST OF THE DRILLED CORE HOLE IS INCLUDED IN ITEM 611, CONDUIT, MISC TYPE - FOR DRAINAGE DISCHARGE CONTINUANCE.

DOCUMENTATION: THE CONTRACTOR SHALL FURNISH WRITTEN DOCUMENTATION TO THE ENGINEER AND TO THE DISTRICT R/W PERMIT OFFICE. THE DOCUMENTATION INCLUDES THE CONSTRUCTION PROJECT NUMBER, PID, COUNTY, ROUTE, SECTION, LATITUDE AND LONGITUDE OF THE DRAINAGE DISCHARGE AT THE R/W, THE NAME OF PROPERTY OWNER WITH ADDRESS, THE DATE THE DRAINAGE DISCHARGE WAS LOCATED, THE DATE THE DRAINAGE DISCHARGE CONTINUANCE WAS FURNISHED, A DETAILED DESCRIPTION OF THE WORK AND PICTURES OF THE DRAINAGE DISCHARGE CONTINUANCE (IN PDF OR JPEG FORMAT). THE DOCUMENTATION IS INCLUDED IN ITEM 611, CONDUIT, MISC TYPE - FOR DRAINAGE DISCHARGE CONTINUANCE.

CONDUIT MATERIAL TYPES THE FOLLOWING CONDUIT MATERIAL TYPES MAY BE USED: 707.33, 707.41 NON-PERFORATED, 707.42, 707.43, 707.45, 707.46, 707.47, 707.51, AND 707.52 SDR35.

PAY ITEMS: EACH OF THE PAY ITEMS LISTED BELOW FOR CONDUIT MISCELLANEOUS TYPES B, C, E AND F FOR DRAINAGE DISCHARGE CONTINUANCE INCLUDE CONDUIT SIZES 2 INCH TO 10 INCH. THERE IS NO COST DIFFERENTIATION FOR SIZE IN THESE PAY ITEMS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER IN MAKING THE ABOVE DRAINAGE DISCHARGE CONTINUANCE:

ITEM 611, INSPECTION WELL 10 EACH
ITEM 611, CONDUIT, MISC TYPE B FOR DRAINAGE DISCHARGE CONTINUANCE 500 FEET
ITEM 611, CONDUIT, MISC TYPE C FOR DRAINAGE DISCHARGE CONTINUANCE 500 FEET

REVIEW OF DRAINAGE FACILITIES

BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE BY THE STATE, REPRESENTATIVES OF THE STATE AND THE CONTRACTOR, ALONG WITH LOCAL REPRESENTATIVES, SHALL MAKE AN INSPECTION OF ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCE SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING BY THE STATE.

ALL NEW CONDUITS, INLETS, CATCH BASINS, AND MANHOLES CONSTRUCTED AS A PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEAN CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE STATE.

ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE MENTIONED PARTIES SHALL BE MAINTAINED AND LEFT IN A CONDITION REASONABLY COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. ANY CHANGE IN THE CONDITION RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEMS.

ITEM SPECIAL - MISCELLANEOUS METAL

EXISTING CASTINGS MAY PROVE TO BE UNSUITABLE FOR REUSE, AS DETERMINED BY THE ENGINEER. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE THE CASTINGS OF THE REQUIRED TYPE, SIZE AND STRENGTH (HEAVY OR LIGHT DUTY) FOR THE PARTICULAR STRUCTURE IN QUESTION. ALL MATERIAL SHALL MEET ITEM 611 OF THE SPECIFICATIONS AND SHALL HAVE THE PRIOR APPROVAL OF THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

SPECIAL, MISCELLANEOUS METAL 10000 POUNDS

THE CONTRACTOR IS CAUTIONED TO USE EXTREME CARE IN THE REMOVAL, STORAGE AND REPLACEMENT OF ALL EXISTING CASTINGS. CASTINGS DAMAGED BY THE NEGLIGENCE OF THE CONTRACTOR, AS DETERMINED BY THE ENGINEER, SHALL BE REPLACED WITH THE PROPER NEW CASTINGS AT THE EXPENSE OF THE CONTRACTOR.

ITEM 611 CATCH BASIN, NO. 6. AS PER PLAN

THE CATCH BASIN SHALL BE AS PER ITEM 611, EXCEPT THAT THE GRATE SHALL BE WELDED TO THE FRAME WHEN CALLED FOR IN THE PLANS. THE CATCH BASIN SHALL BE TEMPORARILY USED DURING PHASES 1 AND 3 AND REMOVED IN PHASES 2 AND 4. THE CATCH BASINS CAN BE USED ELSEWHERE IN THE PROJECT IF APPLICABLE. ANY CATCH BASINS LEFT OVER AFTER COMPLETION OF THE PROJECT SHALL BECOME THE PROPERTY OF THE CONTRACTOR.

ITEM 611 CATCH BASIN RECONSTRUCTED TO GRADE

THE FOLLOWING QUANTITY IS PROVIDED TO RECONSTRUCT EXISTING CATCH BASINS TO GRADE IN RESURFACING AREAS OF THE PROJECT, AS DIRECTED BY THE DEPARTMENT.

ITEM 611 CATCH BASIN RECONSTRUCTED TO GRADE, 5 EACH

ITEM 611 INLET, SIDE DITCH

THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED TO ADDRESS LOW AREAS INADVERTENTLY CREATED WITHIN THE PROJECT LIMITS, AS DIRECTED BY THE DEPARTMENT.

ITEM 611 INLET, SIDE DITCH 5 EACH
ITEM 611 12" CONDUIT, TYPE C 500 FEET

ITEM SPECIAL - PIPE CLEANOUT

THIS WORK SHALL CONSIST OF REMOVING SEDIMENT AND DEBRIS FROM THE EXISTING DRAINAGE CONDUITS SPECIFIED IN THE PLANS. ALL MATERIAL REMOVED SHALL BE DISPOSED OF AS PER 105.16 AND 105.17. ALL SEWERS SHALL BE CLEANED OUT TO THE SATISFACTION OF THE ENGINEER.

CLEANOUT OF THE PIPE SHALL BE PAID FOR AT THE UNIT PRICE BID FOR ITEM SPECIAL - PIPE CLEANOUT. THIS PRICE SHALL INCLUDE THE COST FOR MATERIAL, EQUIPMENT, LABOR, AND ALL INCIDENTALS REQUIRED TO COMPLETE THE CLEANOUT.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE ABOVE NOTED WORK:

SPECIAL, PIPE CLEANOUT, 24" AND UNDER 1300 FEET
SPECIAL, PIPE CLEANOUT, 27" TO 48" 750 FEET

TEMPORARY DRAINAGE ITEMS

TEMPORARY DRAINAGE ITEMS LABELED ON THE MAINTENANCE OF TRAFFIC PLAN ARE ITEMIZED ON THE MOT PLANS. PAYMENT FOR THE TEMPORARY DRAINAGE ITEMS ARE ITEMIZED AND CARRIED TO THE GENERAL SUMMARY.

WATER QUALITY

POST CONSTRUCTION STORM WATER TREATMENT

THIS PLAN UTILIZES STRUCTURAL BEST MANAGEMENT PRACTICES (BMP'S) FOR POST CONSTRUCTION STORM WATER TREATMENT.

MANUFACTURED WATER QUALITY STRUCTURE

THIS PLAN UTILIZES MANUFACTURED WATER QUALITY STRUCTURES FOR WATER QUALITY TREATMENT. AREAS HAVE BEEN SHOWN IN THE PLANS FOR PLACEMENT OF AN OFF-LINE SYSTEM. PAYMENT FOR THESE DEVICES SHALL BE MADE AT THE CONTRACT UNIT PRICE FOR ITEM 895, MANUFACTURED WATER QUALITY STRUCTURE, TYPE 4.

PAVEMENT

PART-WIDTH CONSTRUCTION

BECAUSE OF THE NECESSITY TO BUILD THIS PROJECT UNDER TRAFFIC AND TO CONSTRUCT THE FULL PAVEMENT WIDTH IN STAGES, EXERCISE CARE TO PREVENT THE CONSTRUCTION OF A BUTT JOINT IN THE BASE COURSES. LAP LONGITUDINAL JOINTS AS SHOWN ON STANDARD CONSTRUCTION DRAWING BP-3.1.

PAVEMENT RESTORATION FOR PIPE INSTALLATIONS AND/OR REMOVALS

THE FOLLOWING QUANTITY HAS BEEN PROVIDED FOR PAVEMENT RESTORATION FOLLOWING INSTALLATION AND/OR REMOVAL OF PIPES.

ITEM 301 - ASPHALT CONCRETE BASE, PG64-22, (449) 100 CU. YDS.
ITEM 304 - AGGREGATE BASE 100 CU. YDS.

THE ABOVE QUANTITY IS BASED ON A 301 THICKNESS OF 6 INCHES AND A PAVEMENT RESTORATION WIDTH THAT INCLUDES THE TRENCH WIDTH PLUS TWO FEET ON EACH SIDE OF THE TRENCH.

PROVIDE ANY MATERIALS USED OUTSIDE THE LIMITS STATED ABOVE AT NO ADDITIONAL COST.

PAVEMENT RESTORATION FOR DRAINAGE STRUCTURE INSTALLATIONS

THE FOLLOWING QUANTITY IS PROVIDED FOR PAVEMENT RESTORATION FOLLOWING INSTALLATION OF ITEM 611, DRAINAGE STRUCTURES.

ITEM 301, ASPHALT CONCRETE BASE, PG64-22, (449) 10 CU. YDS.
ITEM 304 - AGGREGATE BASE 10 CU. YDS.

THE ABOVE QUANTITY IS BASED ON A 301 AND 304 THICKNESS OF 6 INCHES AND A WIDTH OF TWO FEET AROUND THE PERIMETER OF THE DRAINAGE STRUCTURE.

PROVIDE ANY MATERIALS USED OUTSIDE THE LIMITS STATED ABOVE AT NO ADDITIONAL COST.

ITEM 202 - PAVEMENT REMOVED, AS PER PLAN

THIS ITEM SHALL INCLUDE THE REMOVAL OF ALL INTEGRAL CURB AND ALL TYPES OF EXISTING PAVEMENT, INCLUDING ASPHALT, CONCRETE AND BRICK.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441)

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR PARTIAL DEPTH PAVEMENT REPAIR:

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441) 900 SY.

REMOVAL DEPTH SHALL BE 3 INCHES OR AS DIRECTED BY THE ENGINEER.

ITEM 441 ASPHALT CONCRETE SURFACE COURSE, TYPE 1. (446). AS PER PLAN, PG 70-22M

THE COARSE VIRGIN AGGREGATE FOR THIS ITEM SHALL CONSIST OF A BLEND OF 60% MIN. AIR COOLED BLAST FURNACE SLAG (ACBFS) OR TRAP ROCK FROM ONTARIO WITH LIMESTONE COMPRISING THE REMAINING PERCENTAGE.

ITEM 301 - ASPHALT CONCRETE BASE, (449), AS PER PLAN, PG64-22

FOR THE PLACEMENT OF 301 ASPHALT BASE, USE ANTI-SEGREGATION EQUIPMENT CONFORMING TO THE REQUIREMENTS OF 401.03.C EXCLUDING THE USE OF REMIXING PAVERS. ALL COSTS ASSOCIATED WITH THIS PROVISION SHALL BE INCIDENTAL TO ITEM 301 - ASPHALT CONCRETE BASE, (449), AS PER PLAN, PG64-22.

CALCULATED
JMP
CHECKED
WDB

GENERAL NOTES

LAK-US-20-24.99
PART 2

WATER WORK

WATER WORK GENERAL

1. ALL WATER WORK SHALL BE PERFORMED IN ACCORDANCE WITH ITEM 638, THE LAKE COUNTY DEPARTMENT OF UTILITIES (LCDU) RULES AND REGULATIONS, LATEST EDITION AND PLAN DETAILS. WHERE A CONFLICT BETWEEN THE SPECIFICATIONS ARISES, THE LAKE COUNTY DEPARTMENT OF UTILITIES RULES AND REGULATIONS, LATEST EDITION WILL PREVAIL. THE LAKE COUNTY DEPARTMENT OF UTILITIES RULES AND REGULATIONS CAN BE FOUND AT THE FOLLOWING WEB ADDRESS: www.lakecountyohio.gov/utilities/rules-and-regulations/
2. ONLY WATER/SEWER CONTRACTORS LICENSED BY THE LAKE COUNTY BOARD OF COMMISSIONERS MAY INSTALL WATER MAINS.
3. THE CONTRACTOR SHALL NOTIFY THE LCDU AT LEAST 48 HOURS IN ADVANCE OF ANY WORK IN THEIR SYSTEMS.
4. THE LCDU SHALL PERFORM INSPECTION SERVICES. THE COST OF INSPECTION SHALL BE INCLUDED AS PART OF THIS CONSTRUCTION PROJECT AT THE CURRENT BASE RATE AS ESTABLISHED BY THE LAKE COUNTY BOARD OF COMMISSIONERS (SEE LAKE COUNTY UTILITIES DEPARTMENT RULES AND REGULATIONS SECTION 7 FEE SCHEDULE). COSTS FOR LAKE COUNTY INSPECTION FEES AND ALL OTHER WATERWORK RELATED FEES SHALL BE INCLUDED IN THE UNIT BID PRICES FOR THE VARIOUS WATERWORK ITEMS. NO SEPARATE PAYMENT WILL BE MADE FOR LCDU FEES.
5. WATERLINE WORK SHALL NOT BEGIN UNTIL THE AREAS OF CONSTRUCTION ARE ROUGH GRADED.
6. ALL HYDRANTS SHALL BE POSITIONED SO THAT THE STEAMER NOZZLES POINT TOWARD THE ROADWAY PAVEMENT.
7. ALL BOLTS SHALL BE STAINLESS STEEL TYPE 304 OR 316. WITH ANTI-GALLING AGENT.
8. ALL SHOP DRAWINGS SHALL BE SUBMITTED TO ODOT FOR PRELIMINARY CHECKING. ODOT SHALL FORWARD CHECKED SHOP DRAWINGS TO THE LCDU FOR FINAL CHECKING AND APPROVAL.
9. THE LCDU SHALL PROVIDE WATER FOR THE NEW WATER MAIN WITHOUT COST FOR THE INITIAL OPERATION. ALL WATER FOR FLUSHING OPERATIONS SHALL BE PAID FOR BY THE CONTRACTOR AT CURRENT RATES AS ESTABLISHED BY THE LAKE COUNTY BOARD OF COMMISSIONERS PER 100 CUBIC FEET OF WATER USED. (SEE SECTION 7 FEE SCHEDULE)
10. ALL WATER MAIN PIPE SHALL BE DUCTILE IRON PIPE, ANSI A21.51, THICKNESS CLASS 53, UNLESS OTHERWISE SHOWN ON THE PLANS, WITH MECHANICAL JOINTS, CEMENT LINED ANSI A 21.4. FITTINGS TO BE FULL BODY-ONLY.
11. RESTRAINED MECHANICAL JOINTS SHALL BE OF THE PIPE MANUFACTURER'S STANDARD DESIGN OR SHALL BE AS LISTED IN THE LCDU APPROVED MATERIALS LIST.
12. LCDU WILL MAKE THE NECESSARY NEW SERVICE CONNECTION TAPS ON EXISTING LCDU MAINS FOR THE CONTRACTOR AT CURRENT RATES AS ESTABLISHED BY THE LAKE COUNTY BOARD OF COMMISSIONERS PER 8" AND GREATER TAPS WITHIN RIGHT OF WAY. (SEE SECTION 7 FEE SCHEDULE) SERVICE CONNECTIONS TO EXISTING BUILDINGS SHALL BE MADE BY THE CONTRACTOR.
13. LCDU WILL SHUT DOWN LIMITED PORTIONS OF WATER MAINS (BETWEEN 2 ADJACENT VALVES, GENERALLY SPACED AT 1200 FT TO 1500 FT) FOR PERIODS NOT TO EXCEED 8 HOURS WITHIN A 24 HOUR PERIOD. ALL WORK WITHIN ONE SHUT DOWN AREA SHALL BE COMPLETED WITHIN THAT SHUT DOWN PERIOD. COORDINATE WITH LCDU A MINIMUM OF 48 HOURS IN ADVANCE OF SUCH PLANNED WORK. LOCAL BUSINESSES AND RESIDENTS WITHIN THE SHUT DOWN LIMITS SHALL BE NOTIFIED 48 HOURS IN ADVANCE.
14. NOTHING IN THESE NOTES SHALL BE CONSTRUED TO SUPERCEDE THE FEDERAL BUY AMERICAN REQUIREMENTS AS SET FORTH IN PN 133.

ITEM SPECIAL 1" COPPER WATER SERVICE LINE. LCDU STANDARD

FURNISH AND INSTALL COPPER SERVICE BRANCHES, TUBING AND FITTINGS, AS NECESSARY, OR AS SHOWN IN THE CONTRACT DOCUMENTS INCLUDING THE REMOVAL OF THE EXISTING SERVICE BRANCHES OR SERVICE BOXES, AS REQUIRED ACCORDING TO THE FOLLOWING SITUATIONS.

TAPPING SADDLES, CORPORATION STOPS, SERVICE STOPS, AND SERVICE BOXES, IF REQUIRED, ARE SEPARATE FROM THIS ITEM:

WHERE A SERVICE BRANCH IS DISTURBED FOR LOWERING, RAISING, OR RELOCATION BETWEEN THE WATER MAIN AT THE CORPORATION STOP AND THE SERVICE STOP, REPLACE IT WITH NEW MATERIALS WITHIN THESE LIMITS UNLESS THE CONNECTIONS CAN BE MADE OUTSIDE THE LIMITS OF THE PROPOSED PAVEMENT, PAVED SHOULDER, OR CURB.

WHERE A SERVICE BRANCH IS DISTURBED FOR LOWERING, RAISING, EXTENDING, OR SHORTENING ON THE PROPERTY SIDE OF THE SERVICE STOP, REPLACE IT WITH NEW MATERIALS FROM THE EXISTING SERVICE STOP TO THE PROPOSED SERVICE STOP.

IF THE EXISTING SERVICE BRANCH ENCOUNTERED IS FOUND TO BE LEAD OR GALVANIZED PIPE, REPLACE IT FROM THE CORPORATION STOP TO THE SERVICE STOP WITH NEW MATERIAL.

THE DEPARTMENT WILL MEASURE COPPER SERVICE BRANCH BY THE NUMBER OF FEET OF EACH CONSTRUCTED.

THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR THE RELOCATION OF SERVICE BRANCHES WHICH MAY BE IN CONFLICT WITH OTHER WORK IN THE CONTRACT:

ITEM SPECIAL, 1" COPPER WATER SERVICE LINE, LCDU STANDARD 2400 FT

ITEM 638 METER AND CHAMBER REMOVED AND RESET. AS PER PLAN

REMOVE EXISTING CHAMBERS. SET THE CHAMBER ON BRICK BLOCKS. DISCONNECT EXISTING METERS, AND REPLACE THEM WITH SUITABLE CONNECTIONS IF NECESSARY. RECONNECT THE METERS AT NEW LOCATIONS.

THE DEPARTMENT WILL MEASURE METER AND CHAMBER REMOVED AND RESET BY THE NUMBER OF EACH.

THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 638, METER AND CHAMBER REMOVED AND RESET, AS PER PLAN, 10 EACH

**ITEM SPECIAL VALVE BOX ADJUSTED TO GRADE. LCDU STANDARD
ITEM SPECIAL SERVICE BOX ADJUSTED TO GRADE. LCDU STANDARD**

RAISE OR LOWER EXISTING VALVE BOXES AND SERVICE BOXES TO GRADE. THE CONTRACTOR MAY REUSE EXISTING VALVE BOXES AND SERVICE BOXES IF THEY ARE IN GOOD CONDITION, AS DETERMINED BY THE ENGINEER.

EXCAVATE AROUND THE VALVE BOX OR SERVICE BOX TO PERMIT THE RAISING OR LOWERING OF THE VALVE BOX OR SERVICE BOX. THE CONTRACTOR MAY ADJUST ANY SCREW-TYPE BOXES WITHOUT THE USE OF EXTENSIONS PROVIDED THAT AMPLE THREAD REMAINS ON THE BOX TO PROVIDE ADEQUATE RIGIDITY TO THE BOX. PROVIDE EXTENSION SECTIONS FOR BOXES THAT ARE NOT OF THE SCREW-TYPE AND BOXES NOT HAVING AMPLE THREAD FOR RIGIDITY TO ADJUST THE TOP TO GRADE. THE ENGINEER WILL ALLOW INSERTS OR ADAPTERS. ADEQUATELY SECURE NEW SECTIONS OF BOX STEM TO THE EXISTING STEMS. BACKFILL THE HOLE AFTER THE BOX HAS BEEN ADJUSTED TO GRADE.

THE DEPARTMENT WILL MEASURE VALVE BOXES AND SERVICE BOXES ADJUSTED TO GRADE SEPARATELY BY THE NUMBER OF EACH, INCLUDING ANY EXTENSION SECTIONS OF VALVE BOXES OR SERVICE BOXES REQUIRED.

THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

ITEM SPECIAL, VALVE BOX ADJUSTED TO GRADE, LCDU STANDARD, 10 EACH
ITEM SPECIAL, SERVICE BOX ADJUSTED TO GRADE, LCDU STANDARD 20 EACH

ITEM SPECIAL FIRE HYDRANT REMOVED FOR STORAGE. LCDU STANDARD

WHERE EXISTING HYDRANTS ARE INDICATED FOR REMOVAL, PROVIDE ADEQUATE SUPPORT FOR THE HYDRANT BEFORE DISCONNECTING IT. STORE HYDRANTS AN SINGLE LOCATION ON SITE FOR PICK-UP BY LCDU PERSONNEL. NEW HYDRANT INSTALLATION SHALL IMMEDIATELY FOLLOW HYDRANT REMOVAL.

THE DEPARTMENT WILL MEASURE FIRE HYDRANTS REMOVED FOR STORAGE BY THE NUMBER OF EACH, INCLUDING ANY EXTENSION SECTIONS OF VALVE OR SERVICE BOXES REQUIRED.

THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES AT THE CONTRACT UNIT PRICES AS FOLLOWS:

ITEM SPECIAL FIRE HYDRANT REMOVED FOR STORAGE, LCDU STANDARD, EACH

ITEM SPECIAL 6" FIRE HYDRANT. LCDU STANDARD

THIS ITEM INCLUDES EXCAVATION AND FURNISHING AND INSTALLING A NEW FIRE HYDRANT COMPLETE WITH PROPER JOINTING, BLOCKING, AND BACKFILLING AS OUTLINED BELOW, AS SHOWN IN THE PLAN DETAILS, AND ALL OTHER INCIDENTAL WORK NECESSARY TO COMPLETE THIS ITEM OF WORK. THE WORK WILL INCLUDE ALL HYDRANT BRANCHES, GATE VALVES, AND VALVE BOXES REQUIRED TO PERFORM THE WORK.

EXCAVATE ACCORDING TO 638.04. EXCAVATE A DRAINAGE PIT 2 FEET IN DIAMETER AND 3 FEET DEEP BELOW THE HYDRANT AND FILL IT WITH GRANULAR MATERIAL. PROVIDE A 3 FOOT MINIMUM RADIUS UNOBSTRUCTED AREA AROUND ALL HYDRANTS. SET THE SIDEWALK FLANGE 2 INCHES ABOVE FINISHED GRADE. SET HYDRANTS ON SOLID CONCRETE BLOCKS ACCORDING TO 638.06.K. PROVIDE THRUST BLOCKING ACCORDING TO 638.06.L. COVER ANY HYDRANT NOT IN SERVICE WITH A BURLAP OR STURDY OPAQUE PLASTIC BAG.

CONSTRUCT HYDRANT BRANCHES USING A SECTION OF DUCTILE IRON PIPE FROM THE MAIN TO THE HYDRANT, AND INCLUDE A GATE VALVE AND VALVE BOX SET VERTICALLY AND PLACED IN THE LINE AS INDICATED. LOCATE THE VALVE ADJACENT TO THE ANCHORING TEE ON THE MAIN. PROVIDE GATE VALVE AND VALVE BOXES FOR HYDRANT BRANCHES CONFORMING TO 638.13.

AFTER HYDRANTS HAVE BEEN SET, OPEN BRANCH AND HYDRANT VALVES UNTIL WATER FLOW EXPELS ALL AIR AND DIRT.

THE DEPARTMENT WILL MEASURE 6" FIRE HYDRANT ASSEMBLIES COMPLETE BY THE NUMBER OF EACH.

THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES AT THE CONTRACT UNIT PRICES AS FOLLOWS:

ITEM SPECIAL 6" FIRE HYDRANT, LCDU STANDARD, EACH

ITEM SPECIAL 12" WATER MAIN DIP CLASS 53 MECHANICAL JOINTS AND FITTINGS. LCDU STANDARD

THIS ITEM SHALL INCLUDE INSTALLATION OF WATERLINES IN ACCORDANCE WITH ITEM 638 AND THE LCDU RULES AND REGULATIONS.

THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES AT THE CONTRACT UNIT PRICES AS FOLLOWS: ITEM SPECIAL 12" WATER MAIN DIP CLASS 53 MECHANICAL JOINTS AND FITTINGS, LCDU STANDARD, FEET

SANITARY

SANITARY GENERAL

1. ALL SANITARY SEWER WORK SHALL BE PERFORMED IN ACCORDANCE WITH ITEM 611, THE LAKE COUNTY DEPARTMENT OF UTILITIES (LCDU) RULES AND REGULATIONS, LATEST EDITION AND PLAN DETAILS. WHERE A CONFLICT BETWEEN THE SPECIFICATIONS ARISES, THE LAKE COUNTY DEPARTMENT OF UTILITIES RULES AND REGULATIONS, LATEST EDITION WILL PREVAIL. THE LAKE COUNTY DEPARTMENT OF UTILITIES RULES AND REGULATIONS CAN BE FOUND AT THE FOLLOWING WEB ADDRESS: www.lakecountyohio.gov/utilities/rules-and-regulations/
2. ONLY WATER/SEWER CONTRACTORS LICENSED BY THE LAKE COUNTY BOARD OF COMMISSIONERS MAY INSTALL SANITARY SEWERS.
3. THE CONTRACTOR SHALL NOTIFY THE LCDU AT LEAST 48 HOURS IN ADVANCE OF ANY WORK IN THEIR SYSTEMS.
4. THE LCDU SHALL PERFORM INSPECTION SERVICES. THE COST OF INSPECTION SHALL BE INCLUDED AS PART OF THIS CONSTRUCTION PROJECT AT THE CURRENT BASE RATE AS ESTABLISHED BY THE LAKE COUNTY BOARD OF COMMISSIONERS (SEE LAKE COUNTY UTILITIES DEPARTMENT RULES AND REGULATIONS SECTION 7 FEE SCHEDULE) COST FOR LAKE COUNTY INSPECTION FEE. AND OTHER FEES SHALL BE INCLUDED IN THE UNIT PRICES BID FOR OTHER WATERLINE ITEMS.
5. ROOF DRAINS, FOUNDATION DRAINS AND OTHER CLEAN WATER CONNECTIONS TO THE SANITARY SEWER ARE PROHIBITED

ITEM 611 MANHOLE ADJUSTED TO GRADE. AS PER PLAN

CAREFULLY REMOVE THE EXISTING COVER AND INSTALL PRECAST CONCRETE GRADE RING TO THE NEW GRADE.

ALL MANHOLES ADJUSTED TO GRADE SHALL HAVE EXTERNAL MANHOLE CHIMNEY SEALS AS LISTED IN THE LCDU SANITARY APPROVED MATERIALS LIST OF HE LAKE COUNTY UTILITIES DEPARTMENT RULES AND REGULATIONS, LATEST EDITION

ALL BRICK WORK ABOVE THE MANHOLE CONE SECTION SHALL BE REPLACED AND A CHIMNEY SEAL SHALL BE INSTALLED PER THE MANUFACTURER'S RECOMMENDATIONS.

THE DEPARTMENT WILL MEASURE MANHOLES RECONSTRUCTED OR ADJUSTED TO GRADE, BY THE NUMBER OF EACH TYPE OF STRUCTURE COMPLETE AND ACCEPTED.

THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES AT THE CONTRACT PRICES AS FOLLOWS:

ITEM 611 MANHOLE ADJUSTED TO GRADE, AS PER PLAN, EACH

ITEM 611 6" CONDUIT. TYPE B. 748.01. AS PER PLAN

THIS ITEM SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM 638 WATER MAINS. MATERIALS SHALL BE DUCTILE IRON PIPE, ANSI CLASS 52, MECHANICAL JOINTS AND FITTINGS.

THE PIPE AND FITTINGS SHALL HAVE MECHANICAL RESTRAINED JOINTS. ALL PIPES SHALL BE COATED WITH A BITUMINOUS MATERIAL ON THE OUTSIDE AND SHALL BE CEMENT MORTAR LINED DURING FABRICATION IN ACCORDANCE WITH AWWA C104. FITTINGS SHALL BE OF DUCTILE IRON, SHALL CONFORM TO AWWA C153, AND SHALL BE FACTORY COATED AND LINED AND HAVE JOINTS AS SPECIFIED FOR THE PIPE. MECHANICAL JOINTS SHALL BE IN ACCORDANCE WITH AWWA C111, INCORPORATING RUBBER GASKETS. ALL MATERIALS USED SHALL BE ON THE APPROVED MATERIALS LIST OF THE LCDU RULES AND REGULATIONS.

THE DEPARTMENT WILL MEASURE CONDUIT, TYPE B, 748.01, AS PER PLAN BY THE NUMBER OF FEET CONSTRUCTED.

THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES AT THE CONTRACT PRICES AS FOLLOWS:

ITEM 611 6" CONDUIT, TYPE B, 748.01, AS PER PLAN, FEET

TRAFFIC CONTROL

ITEM 621 - RAISED PAVEMENT MARKER REMOVED

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS WORK:

ITEM 621 - RAISED PAVEMENT MARKER REMOVED 700 EACH

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST_Design\Roadway\Sheets\09270_CN001.dgn Sheet 5 7/15/2022 2:50:07 PM troyer

CALCULATED
JMP
CHECKED
WDB

GENERAL NOTES

LAK-US-20-24.99
PART 2

20
697

ITEM 614. MAINTAINING TRAFFIC

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES ON NORTH RIDGE ROAD (U.S. 20) BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, ITEM 615 ROADS FOR MAINTAINING TRAFFIC, AND TEMPORARY SURFACES USING ITEMS 410 AND 614.

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES ON HAINES ROAD AND BURNS ROAD, EXCEPT FOR A PERIOD NOT TO EXCEED 14 CONSECUTIVE CALENDAR DAYS, WHEN THROUGH TRAFFIC MAY BE DETOURED AS SHOWN ON SHEETS 25-26. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$800 PER DAY FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC.

ITEM 608, TEMPORARY ASPHALT CONCRETE WALK 86000 SQ. FT.

ITEM 615, ROADS FOR MAINTAINING TRAFFIC, TYPE A LUMP

ITEM 616, WATER 50 M. GAL.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NOTICE OF CLOSURE SIGN (SIDE ROADS)

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED SIDE ROAD CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. (AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.)

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE		
ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
RAMP & ROAD CLOSURES	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

PLACEMENT OF ASPHALT CONCRETE

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

TRENCH FOR WIDENING

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

OVERNIGHT TRENCH CLOSING

THE BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN 5 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT EXCEPT FOR A SHORT LENGTH (25 FEET OR LESS) OF A WORK SECTION AT THE END OF THE TRENCH. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING SHALL BE BACKFILLED AT THE DIRECTION OF THE ENGINEER. THESE REQUIREMENTS APPLY TO ALL PERMANENT AND TEMPORARY PAVEMENT NOT PROTECTED BY BARRIER.

EARTHWORK FOR MAINTAINING TRAFFIC

THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED IN THE PLAN FOR INFORMATION ONLY:

EXCAVATION FOR MAINTAINING TRAFFIC 7100 CU. YD.
EMBANKMENT FOR MAINTAINING TRAFFIC 240 CU. YD.

WHEN UNDERCUTS ARE NECESSARY FOR MAINLINE PAVEMENT OR EMBANKMENT CONSTRUCTION, EVALUATE THE NEED FOR TEMPORARY ROAD UNDERCUTS IF WITHIN A CLOSE PROXIMITY TO THE MAINLINE UNDERCUTS. A GEOTECHNICAL EVALUATION SHOULD BE CONSIDERED TO DETERMINE IF THE EXISTING SOIL CONDITIONS ARE ADEQUATE TO SUPPORT THE TEMPORARY ROAD. ADDITIONAL SOIL BORINGS ALONG THE TEMPORARY ROAD ARE NOT NORMALLY REQUIRED. ANY UNDERCUTS SHALL BE FILLED THE SAME DAY AS EXCAVATION.

WORK ZONE MARKINGS (WINTER APPLICATION)

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS PER THE REQUIREMENTS OF C&MS 614.11. MARKINGS SHALL BE APPLIED ON ROADWAY AFFECTED BY PHASE 1 AND PHASE 2 CONSTRUCTION:

WORK ZONE LANE LINE, CLASS I, 4", 642 PAINT 3.61 MILE
WORK ZONE CENTER LINE, CLASS I, 642 PAINT 2.71 MILE
WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT 1.33 MILE
WORK ZONE CHANNELIZING LINE, CLASS I, 8", 642 PAINT 1506 FT
WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT 648 FT
WORK ZONE STOP LINE, CLASS I, 642 PAINT 566 FT
WORK ZONE CROSSWALK LINE, CLASS I, 12", 642 PAINT 1582 FT
WORK ZONE ARROW, CLASS I, 642 PAINT 23 EACH
WORK ZONE ISLAND MARKING, CLASS I 51 SF

WORK ZONE MARKINGS (PRE-FINAL APPLICATION)

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS PER THE REQUIREMENTS OF C&MS 614.11. MARKINGS SHALL BE APPLIED AFTER RESURFACING AT THE END OF THE PROJECT AND PRIOR TO FINAL STRIPING APPLICATION:

WORK ZONE LANE LINE, CLASS III, 642 PAINT 6.19 MILE
WORK ZONE CENTER LINE, CLASS III, 642 PAINT 4.33 MILE
WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT 2.13 MILE
WORK ZONE CHANNELIZING LINE, CLASS III, 8", 642 PAINT 2016 FT
WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS III, 642 PAINT 838 FT
WORK ZONE STOP LINE, CLASS III, 642 PAINT 669 FT
WORK ZONE CROSSWALK LINE, CLASS III, 12", 642 PAINT 1695 FT
WORK ZONE ARROW, CLASS III, 642 PAINT 31 EACH
WORK ZONE ISLAND MARKING, CLASS III, 642 PAINT 51 SF

ITEM 614. WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS (UNIDIRECTIONAL OR BIDIRECTIONAL)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NONGATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

ITEM 614. WORK ZONE TRAFFIC SIGNAL

THE ADDITION OF TEMPORARY TRAFFIC SIGNALS AND EQUIPMENT AND MODIFICATIONS TO EXISTING TRAFFIC SIGNALS AND EQUIPMENT SHALL BE PAID FOR UNDER THE UNIT PRICE BID FOR ITEM 614 WORK ZONE TRAFFIC SIGNAL AT THE FOLLOWING INTERSECTIONS:

PHASE 1: GREEN ROAD, BURNS ROAD, DERUBERTIS DRIVE, SERVICE ROADS, HUBBARD ROAD

PHASE 1A: HUBBARD ROAD
PHASE 1B: HUBBARD ROAD

PHASE 2: GREEN ROAD, BURNS ROAD, DERUBERTIS DRIVE, SERVICE ROADS, HUBBARD ROAD

PHASE 2A: HUBBARD ROAD
PHASE 2B: HUBBARD ROAD

PHASE 3: TOWNLINE ROAD, McMACKIN ROAD, GREEN ROAD

PHASE 4: TOWNLINE ROAD, McMACKIN ROAD, GREEN ROAD

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS WORK:

ITEM 614, WORK ZONE TRAFFIC SIGNAL 20 EACH

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616, WATER 500 M. GAL.

ITEM 614. PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET(S) 23 OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

(THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN 6 HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.)

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.) THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE, AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 120 SIGN MONTH
ASSUMING 5 PCMS SIGNS FOR 24 MONTHS

H:\2018\180743\ODOT\ProjectData\09270_LAK_20_East\Design\MOT\Sheets\09270_MN001.dgn Sheet 7/15/2022 2:50:08 PM troyer

MAINTENANCE OF TRAFFIC SIGNAL/FLASHER INSTALLATION

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRAFFIC SIGNAL/FLASHER INSTALLATIONS WITHIN THE PROJECT UNDER THE FOLLOWING CONDITIONS:

- EXISTING SIGNAL/FLASHER INSTALLATIONS WHICH THE PLANS REQUIRE THE CONTRACTOR TO ADJUST, MODIFY, ADD ONTO OR REMOVE, OR WHICH THE CONTRACTOR ACTUALLY ADJUSTS, MODIFIES OR OTHERWISE DISTURBS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ENTIRE INSTALLATION (AT AN INTERSECTION) FROM THE TIME HIS OPERATIONS FIRST DISTURB THE INSTALLATION UNTIL THE INSTALLATION HAS BEEN SUBSEQUENTLY REMOVED OR MODIFIED AND THE WORK IS ACCEPTED.
- NEW OR REUSED SIGNAL/FLASHER INSTALLATIONS OR DEVICES, INSTALLED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF THESE FROM THE TIME OF INSTALLATION UNTIL THE WORK IS ACCEPTED.

THE CONTRACTOR SHALL CORRECT AS QUICKLY AS POSSIBLE ALL OUTAGES OR MALFUNCTIONS. HE SHALL PROVIDE THE MAINTAINING AGENCY AND THE ENGINEER SUCH ADDRESSES AND PHONE NUMBERS WHERE HIS MAINTENANCE FORCES CAN BE CONTACTED. THE CONTRACTOR SHALL PROVIDE ONE OR MORE PERSONS TO RECEIVE ALL CALLS AND DISPATCH THE NECESSARY MAINTENANCE FORCES TO CORRECT OUTAGES. SUCH A PERSON OR PERSONS MAY BE USED TO PERFORM OTHER DUTIES AS LONG AS PROMPT ATTENTION IS GIVEN TO THESE CALLS AND A PERSON IS READILY AVAILABLE CONTINUOUSLY 24 HOURS A DAY, 7 DAYS A WEEK. ALL LAMP OUTAGES, CABLE OUTAGES, ELECTRICAL FAILURES, EQUIPMENT MALFUNCTIONS AND MISALIGNED SIGNAL HEADS SHALL BE CORRECTED TO THE SATISFACTION OF THE ENGINEER WITH THE SIGNAL BACK TO SERVICE WITHIN FOUR HOURS AFTER THE CONTRACTOR HAS BEEN NOTIFIED OF THE OUTAGE.

IN THE EVENT NEW SIGNALS ARE DAMAGED PRIOR TO ACCEPTANCE, ALL DAMAGED EQUIPMENT EXCEPT POLES AND CONTROL EQUIPMENT SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER WITH THE SIGNAL BACK IN SERVICE WITHIN 8 HOURS AFTER THE CONTRACTOR'S NOTIFICATION OF THE OUTAGE. THE CONTRACTOR SHALL ARRANGE FOR FULL TRAFFIC CONTROL UNTIL THE SIGNAL IS BACK IN OPERATION.

IF POLES AND/OR CONTROL EQUIPMENT ARE DAMAGED AND MUST BE REPLACED, THE CONTRACTOR SHALL MAKE TEMPORARY REPAIRS AS NECESSARY TO BRING THE SIGNAL BACK INTO FULL OPERATION WITHIN THE ALLOWED 8-HOUR PERIOD, AND SHALL MAKE PERMANENT REPAIRS OR REPLACEMENT AS SOON THEREAFTER AS POSSIBLE.

NONE OF THE ABOVE SHALL BE CONSTRUED AS COLLECTIVE OR CONSECUTIVE OUTAGE TIME PERIODS AT ANY ONE LOCATION. THAT IS, WHERE MORE THAN ONE OUTAGE OCCURS AT ANY ONE LOCATION THEN THE ALLOTTED TIME LIMIT SHALL BE FOR THE WORST SINGLE OUTAGE.

WHERE OUTAGES ARE THE DIRECT RESULT OF A VEHICLE ACCIDENT THE RESPONSE OF THE CONTRACTOR SHALL BE AS OUTLINED ABOVE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COLLECTION OF ANY COMPENSATION FOR THIS WORK FROM THOSE PARTIES RESPONSIBLE FOR THE DAMAGE.

WHERE THE CONTRACTOR HAS FAILED TO, OR CANNOT RESPOND TO, AN OUTAGE OR SIGNAL EQUIPMENT MALFUNCTION, AT THESE LOCATIONS WITHIN HIS RESPONSIBILITY, WITHIN PERIODS AS SPECIFIED ABOVE, THE ENGINEER MAY INVOKE THE PROVISIONS OF SECTION 105.15 AND ANY SUBSEQUENT BILLINGS TO THE STATE OR THE VILLAGE OF NORTY PERRY FOR POLICE SERVICES AND MAINTENANCE SERVICES BY CITY FORCES SHALL BE DEDUCTED FROM MONIES DUE OR TO BECOME DUE THE CONTRACTOR IN ACCORDANCE WITH PROVISIONS OF SECTION 105.15.

THE CONTRACTOR SHALL PROVIDE THE MAINTENANCE SERVICE ENTIRELY WITH HIS FORCES OR HE MAY CHOOSE TO ENTER INTO A COOPERATIVE UNDERSTANDING WITH THE LOCAL MAINTAINING AGENCY TO PROVIDE THE MAINTENANCE. THE CONTRACTOR SHALL INFORM THE ENGINEER, IN WRITING, OF THE MAINTENANCE METHOD SELECTED.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ANY TRAFFIC SIGNAL COMPONENTS REQUIRED TO BE HANDLED DURING THE RELOCATION OF POLES AND REVISIONS TO THE SIGNAL SYSTEM. WHEN A TRAFFIC SIGNAL MUST BE TAKEN OUT OF SERVICE BY THE CONTRACTOR, DUE TO CONSTRUCTION PROCEDURES, THIS OUTAGE SHALL NOT EXCEED 4 HOURS AND SHALL NOT INCLUDE THE HOURS OF 7 AM TO 9 AM OR 3 PM TO 6 PM. ANY SIGNALIZED INTERSECTION, WHERE THE SIGNAL IS OUT OF SERVICE DUE TO CONSTRUCTION PROCEDURES, OR DUE TO AN OUTAGE OR MALFUNCTION OF EQUIPMENT AS DESCRIBED ABOVE, SHALL BE PROTECTED, BY THE CONTRACTOR, BY THE INSTALLATION OF TEMPORARY "STOP" SIGNS, EXCEPT FOR THE FOLLOWING INTERSECTIONS WHICH SHALL BE PROTECTED BY OFF-DUTY VILLAGE OF NORTH PERRY POLICE, HIRED BY THE CONTRACTOR:

- MCMACKIN ROAD
- HUBBARD ROAD

ANY VEHICULAR TRAFFIC SIGNAL HEAD, EITHER NEW OR EXISTING WHICH WILL BE OUT OF OPERATION SHALL BE COVERED IN THE MANNER DESCRIBED IN 632.25.

THE CONTRACTOR SHALL MAINTAIN COMPLETE RECORDS OF MALFUNCTIONS INCLUDING:

- TIME OF NOTIFICATION OF MALFUNCTION;
- TIME OF WORK CREWS ARRIVAL TO CORRECT THE MALFUNCTION;
- ACTIONS TAKEN TO CORRECT THE MALFUNCTION, INCLUDING A LIST OF PARTS REPAIRED OR REPLACED;
- A DIAGNOSIS OF REASON FOR THE MALFUNCTION AND PROBABILITY OF REOCCURRENCE;
- TIME OF COMPLETION OF THE REPAIR AND SYSTEM RESTORED TO FULL SERVICE.

A COPY OF THESE RECORDS SHALL BE PROVIDED TO THE ENGINEER WITHIN THREE (3) WORKING DAYS FOLLOWING COMPLETION OF EACH REPAIR.

ALL COSTS RESULTING FROM THE ABOVE REQUIREMENTS SHALL BE CONSIDERED TO BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC.

ITEM 614 - BUSINESS ENTRANCE (M4-H15) SIGN, AS PER PLAN

THE BUSINESS ENTRANCE (M4-H15) SIGN SHOULD BE PROVIDED AT EACH TEMPORARILY RELOCATED COMMERCIAL DRIVEWAY FOR WHICH THE RELOCATION IS NOT OBVIOUS TO THE MOTORIST. THE PROJECT ENGINEER SHALL DETERMINE WHETHER OR NOT THE DRIVEWAY RELOCATION IS, OR IS NOT, OBVIOUS AND WHETHER OR NOT A SIGN SHOULD BE PROVIDED. ONLY ONE SIGN PER BUSINESS SHALL BE PERMITTED. THE SIGN SHALL BE 36 INCH X 48 INCH IN SIZE WITH TYPE G OR TYPE H ORANGE RETROREFLECTIVE SHEETING. THE SIGN LEGEND SHALL BE PLACED ON BOTH SIDES OF THE SIGN (BACK TO BACK). THE SIGN SHALL HAVE THE STANDARD M4-H15 LEGEND WITH THE WORD "BUSINESS" ON THE TOP LINE, EXCEPT UNDER UNUSUAL CIRCUMSTANCES WHERE IT MAY NOT BE INTUITIVE THAT A DRIVEWAY SERVES A SPECIFIC BUSINESS. IN SUCH UNUSUAL CASES, THE ACTUAL BUSINESS NAME MAY BE SUBSTITUTED FOR THE WORD "BUSINESS".

THE SIGN SHALL BE MOUNTED ON TWO #3 POSTS OR ON TEMPORARY POSTS IN ACCORDANCE WITH SCD MT-105.10 AND IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION. THE SIGN SHALL BE CLEARLY VISIBLE AND SHALL CLEARLY IDENTIFY THE LOCATION OF THE DRIVEWAY. THE SIGN SHOULD BE POSITIONED AT 90 DEGREES TO THE DIRECTION(S) OF TRAFFIC. THE SIGN MAY NEED TO BE MOVED FOR EACH PHASE OF THE MAINTENANCE OF TRAFFIC OPERATIONS.

PAYMENT FOR ALL COSTS ASSOCIATED WITH MANUFACTURING, MOUNTING, RELOCATING, AND REMOVING THE SIGN, INCLUDING ALL LABOR, MATERIALS AND EQUIPMENT SHALL BE INCLUDED IN THE CONTRACT PRICE PER EACH FOR ITEM 614-BUSINESS ENTRANCE SIGN.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS ITEM:

ITEM 614, BUSINESS ENTRANCE SIGN, AS PER PLAN
65 EACH

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) MAY BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN A NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING THE SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 1000 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AND THE CONTACTS LISTED BELOW IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER AND THE CONTACTS LISTED BELOW TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER AND THE CONTACTS LISTED BELOW PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER AND THE CONTACTS LISTED BELOW.

NOTIFICATION TIME FRAME TABLE		
ITEM	DURATION OF CLOSURE	NOTIFICATION DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 weeks	21 BUSINESS DAYS PRIOR TO CLOSURE
	> 12 hours & < 2 weeks	14 BUSINESS DAYS PRIOR TO CLOSURE
	< 12 hours	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 weeks	14 BUSINESS DAYS PRIOR TO CLOSURE
	< 2 weeks	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

THE CONTRACTOR SHALL ALSO NOTIFY THE FOLLOWING CONTACTS PER THE CONDITIONS IN THE NOTE ABOVE:

MADISON TOWNSHIP POLICE:
2065 HUBBARD ROAD
MADISON, OHIO 44057
440-428-2116
ATTN: CHIEF MATTHEW BYERS

MADISON FIRE DISTRICT:
840 RIVER STREET
MADISON, OHIO 44057
440-428-1522

MADISON TOWNSHIP SERVICE DEPT.:
1747 HUBBARD ROAD
MADISON, OHIO 44057
440-428-4981
PAUL COOK, SUPERVISOR

LAKE COUNTY SHERIFF:
104 EAST ERIE STREET
PAINESVILLE, OHIO 44077
440-350-5590
ATTN: SHERIFF FRANK LEONBRUNO

MADISON LOCAL SCHOOL DISTRICT:
1956 RED BIRD ROAD
MADISON, OHIO 44057
440-428-2166
ATTN: ANGELA SMITH, SUPERINTENDENT

MAINTAINING DRIVEWAY ACCESS

COMMERCIAL AND HANDICAP PROPERTIES: ACCESS TO ALL COMMERCIAL AND HANDICAP PROPERTY SHALL BE MAINTAINED AT ALL TIMES, AS FOLLOWS:

FOR PROPERTIES WITH MULTIPLE DRIVEWAYS: DRIVEWAY CONSTRUCTION WILL BE SCHEDULED SO THAT AT LEAST ONE ACCESS DRIVEWAY IS OPEN AND CLEAR WHILE THE OTHER DRIVEWAY(S) ARE BEING CONSTRUCTED.

FOR PROPERTIES WITH ONLY ONE ACCESS: IF THE PROPERTY CANNOT BE WITHOUT ACCESS TEMPORARILY (SUCH AS OUTSIDE OF NORMAL BUSINESS HOURS OR OVER THE WEEKEND), ACCESS TO THE PROPERTY SHALL BE MAINTAINED AS FOLLOWS:

THE PAVEMENT IN FRONT OF THE DRIVEWAY SHALL BE REMOVED AND TRAFFIC COMPACTED SURFACE FURNISHED, INSTALLED, AND COMPACTED ON TOP OF THE EXPOSED SUBGRADE. THIS WORK SHALL BE PERFORMED IN ONE OPERATION SO THAT THE DISRUPTION TO THE PROPERTY IS KEPT AT AN ABSOLUTE MINIMUM. THE CONTRACTOR WILL MAINTAIN THE RAMP AND REPLACE MATERIAL AS NECESSARY.

THE INSTALLATION OF THE PROPOSED DRIVEWAY AND THE CONCRETE PAVEMENT IN FRONT OF THE DRIVEWAY SHALL BE PERFORMED AS ONE OPERATION. IF THE DRIVEWAY IS MORE THAN 20 FEET WIDE, CONSTRUCTION ON THE DRIVEWAY AND PAVEMENT MAY BE IN TWO (2) PHASES, WITH ONE HALF OF THE DRIVE REMAINING OPEN WHILE THE OTHER HALF IS CLOSED AND BEING CONSTRUCTED. FOR DRIVEWAYS LESS THAN 20 FEET WIDE, THE DRIVEWAY AND CONCRETE PAVEMENT SHALL BE CONSTRUCTED USING CLASS MS CONCRETE.

IN LOCATIONS WHERE A DRIVEWAY TO A PARKING LOT IS CLOSED TO TRAFFIC, THE DRIVE SHALL HAVE PROPER PROTECTION, SUCH AS SIGNS, FENCING, BARRICADES, AND DRUMS, PLACED AND MAINTAINED AROUND IT.

WHERE DIRECTED BY THE ENGINEER, ONE (1) BUSINESS ENTRANCE SIGN (M4-H15) SIGN SHALL BE PROVIDED, INSTALLED, MAINTAINED, AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR AT AFFECTED COMMERCIAL DRIVEWAYS. THE SIGN SHALL BE 36 INCH X 48 INCH IN SIZE WITH TYPE G OR TYPE H ORANGE RETROREFLECTIVE SHEETING AND BE MOUNTED ON TWO #3 POSTS OR ON TEMPORARY POSTS IN ACCORDANCE WITH ODOT STANDARD CONSTRUCTION DRAWING MT-105.10. THE SIGN SHALL HAVE THE STANDARD M4-H15 LEGEND, EXCEPT THAT THE ACTUAL BUSINESS NAME SHALL BE SUBSTITUTED FOR THE WORD "BUSINESS."

RESIDENTIAL PROPERTIES: THE CONTRACTOR SHALL MAKE REASONABLE PROVISIONS TO ALLOW RESIDENTIAL ACCESS AT ALL TIME OR OTHERWISE ALLOW RESIDENTS TO PARK SAFELY WITHIN THE PROJECT LIMITS WHEN ACCESS IS NOT POSSIBLE. THE MAXIMUM OUT OF SERVICE TIME FOR ANY RESIDENTIAL DRIVE SHALL BE ONE (1) DAY. RESIDENTIAL ACCESS SHALL BE MAINTAINED USING THE FOLLOWING PROCEDURES, AS DIRECTED BY THE ENGINEER:

AFTER THE EXISTING PAVEMENT AND CURB HAVE BEEN REMOVED, THE CONTRACTOR SHALL IMMEDIATELY MAINTAIN VEHICULAR ACCESS TO THE DRIVEWAY USING ITEM 410, TRAFFIC COMPACTED SURFACE, BETWEEN THE PAVEMENT AND THE DRIVEWAY. STEEL PLATES MAY ALSO BE USED.

WHILE THE CONCRETE PAVEMENT AND CURB IS BEING INSTALLED AND IS CURING, THE CONTRACTOR SHALL PROVIDE PARKING AREAS SAFELY WITHIN THE PROJECT WORK LIMITS OR ALONG ADJACENT SIDE STREETS. DURING THIS TIME, THE EXISTING SIDEWALKS BETWEEN THE SIDE STREETS SHALL REMAIN IN SERVICE FOR ACCESS FROM THE PARKING AREA TO THE PROPERTY.

PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT A PLAN, FOR THE APPROVAL OF THE ENGINEER, WHICH OUTLINES HIS/HER STRATEGY FOR THE MAINTENANCE OF SAFE ACCESS TO COMMERCIAL, HANDICAP AND RESIDENTIAL PROPERTY AND/OR FOR ALLOWING RESIDENTS TO PARK SAFELY WITHIN THE PROJECT LIMITS WHEN ACCESS IS NOT POSSIBLE. CLOSING A DRIVEWAY AND ALLOWING PARKING WITHIN THE PROJECT LIMITS SHALL BE A LAST RESORT.

THE PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS REQUIRED TO MAINTAIN COMMERCIAL, HANDICAP AND RESIDENTIAL ACCESS AND/OR SAFE PARKING AREAS FOR RESIDENTS, INCLUDING COMPACTED AGGREGATE SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC; EXCEPT FOR THE SEPARATELY ESTIMATED ITEMS/QUANTITIES ADDITIONALLY PROVIDED IN THE PLANS FOR MAINTAINING TRAFFIC (ACCESS).

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC OF DRIVEWAYS:

ITEM 410, TRAFFIC COMPACTED SURFACE, TYPE A OR B 1560 CU. YD.

ITEM 614, ASPHALT CONCRETE FOR MAINTAINING TRAFFIC 300 CU. YD.

EXISTING SIGNS

IN ANY PHASE, WHEN A MAINTENANCE OF TRAFFIC SIGN CONTRADICTS AN EXISTING SIGN, THE EXISTING SIGN SHALL BE COVERED. ALL OTHER SIGNS SHALL BE MAINTAINED DURING CONSTRUCTION AND RELOCATED NEARBY IF NECESSARY.

LAKETRAN BUS STOPS

LAKETRAN BUS STOPS SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION. EXISTING BUS STOP SIGNS SHALL BE MOVED TO A NEARBY LOCATION IF NECESSARY. ALL BUS STOP SIGNS SHALL BE MAINTAINED AND RELOCATED AS NECESSARY.

MAIL BOXES

MAIL BOXES SHALL BE CAREFULLY RELOCATED AS NECESSARY BY THE CONTRACTOR DURING CONSTRUCTION SO THAT THEY ARE ACCESSIBLE TO MAIL DELIVERY TRUCKS. ANY DAMAGE TO THE MAILBOX SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. DAMAGED MAILBOXES SHALL BE REPLACED BY THE CONTRACTOR IN KIND.

ITEM 611 CATCH BASIN, NO. 6. AS PER PLAN

THIS ITEM INCLUDES THE REMOVAL AND DISPOSAL OF THE CATCH BASIN AFTER THE MAINTENANCE OF TRAFFIC PHASE IN WHICH IT IS USED IS COMPLETE.

ITEM 611 12" CONDUIT, TYPE B, AS PER PLAN

THIS ITEM INCLUDES THE REMOVAL OF THE CONDUIT, OR ABANDONEMENT OF THE CONDUIT IF THE CONDUIT HAS BEEN PLACED UNDER PERMANENT PAVEMENT, AFTER THE MAINTENANCE OF TRAFFIC PHASE IN WHICH IT IS USED HAS BEEN COMPLETED. THE CONDUIT SHALL BE FILLED AND PLUGGED AS PER THE ITEM SPECIAL-FILL AND PLUG EXISTING CONDUIT NOTE SHOWN ON SHEET 17.

ITEM 611 CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN

WHEN RECONSTRUCTING CATCH BASINS TO GRADE FOR THE PURPOSE OF MAINTAINING TRAFFIC, FOLLOW THE PROCEDURE BELOW.

- CAREFULLY REMOVE AND CLEAN THE EXISTING CASTINGS.
- REMOVE EXISTING WALLS OF CATCH BASINS AND INLETS BELOW THE GRATES TO THE DEPTH NECESSARY FOR PLACEMENT OF TEMPORARY PAVEMENT, OR ANY POINTS OF WALL FAILURE.
- INSTALL A 1-1/4 INCH MINIMUM STEEL ROAD PLATE AS SHOWN IN THE TEMPORARY DRAINAGE DETAIL.
- WHERE EXISTING BASINS WILL NOT BE REMOVED IN A LATER PHASE OF THE CONTRACT, REMOVE THE ROAD PLATE AND USING THE SALVAGED CASTING, RECONSTRUCT THE STRUCTURE TO THE ORIGINAL PLAN GRADE, CONFORMING AS NEARLY AS PRACTICABLE TO THE EXISTING DIMENSION AND TYPE OF CONSTRUCTION.

ITEM 606 - GUARDRAIL, TYPE MGS, AS PER PLAN
ITEM 606 - ANCHOR ASSEMBLY, MGS TYPE E, AS PER PLAN (MASH 2016)
ITEM 606 - ANCHOR ASSEMBLY, MGS TYPE T, AS PER PLAN

GUARDRAIL AND ANCHOR ASSEMBLIES INSTALLED FOR MAINTAINING TRAFFIC SHALL BE REMOVED AND DISPOSED OF AFTER COMPLETION OF THE PHASE IN WHICH THEY WERE CONSTRUCTED AND USED.

ITEM 614 - DETOUR SIGNING

SIZE AND PLACEMENT OF DETOUR SIGNS (M4-9) SHOULD FOLLOW THE REQUIREMENTS OF THE ODOT SECTION 6F.03, SECTION 2A.11 AND TABLE 6F.01.

DETOUR SIGNING SHALL PROVIDE DRIVERS ADEQUATE TIME TO CLEARLY READ THE SIGNS AND MAKE THE PROPER DECISIONS AT EACH REQUIRED TURNING MOVEMENT. THE DESIGNATED DETOUR ROUTE SHALL BE SIGNED IN ACCORDANCE WITH THE REQUIREMENTS BELOW:

- APPROXIMATELY 1500 FEET PRIOR TO TIP OF THE PAINTED GORE AT AN INTERCHANGE WHEN EXITING A HIGH SPEED (45 MPH OR HIGHER) FACILITY.
- AT OR NEAR THE EXISTING SIGN IN THE GORE OF AN INTERCHANGE RAMP.
- AT OR NEAR THE FIRST EXISTING LANE ASSIGNMENT SIGN ON AN INTERCHANGE EXIT RAMP.
- AT OR NEAR THE EXISTING LANE ASSIGNMENT SIGN OR EXISTING ROUTE MARKER AT THE END OF AN EXIT RAMP.
- APPROXIMATELY 500 FEET PRIOR TO A REQUIRED TURN AT AN INTERSECTION NOT CONTROLLED BY A STOP SIGN (FOR 45 MPH OR HIGHER ONLY).
- AT OR NEAR THE EXISTING LANE ASSIGNMENT SIGN OR EXISTING ROUTE MARKER AT AN INTERSECTION.
- EVERY TWO MILES ALONG A TANGENT SECTION BETWEEN TURNING MOVEMENTS OUTSIDE A CITY.
- EVERY TWO BLOCKS ALONG A TANGENT SECTION BETWEEN TURNING MOVEMENTS WITHIN A CITY.
- AT ANY OTHER INTERSECTION OR DECISION POINT WHERE THE DETOUR ROUTE IS CONTRARY TO THE NORMAL, EXPECTED TURNING MANEUVER OR OTHERWISE UNCLEAR.

DETOUR SIGNS SHALL BE PLACED, WHEN POSSIBLE, NEXT TO BUT NOT BLOCKING EXISTING ROUTE MARKERS OR LANE ASSIGNMENT SIGNS. DETOUR SIGNS SHALL NOT OBSCURE OR BE OBSCURED BY OTHER EXISTING OR TEMPORARY SIGNS.

DETOUR SIGNS SHALL BE ERECTED AND/OR UNCOVERED PRIOR TO THE ROAD OR RAMP BEING CLOSED TO TRAFFIC BUT NO EARLIER THAN FOUR HOURS PRIOR TO THE CLOSURE. DETOUR SIGNS SHALL BE COVERED AND/OR REMOVED NO LATER THAN FOUR HOURS FOLLOWING THE ROAD OR RAMP RE-OPENING TO TRAFFIC.

PAYMENT FOR ACCEPTED QUANTITIES WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, PROPER SIGN PLACEMENT AND SIZING, TIMELY ERECTING AND/OR UNCOVERING OF SIGNS, MAINTAINING SIGNS, AND TIMELY COVERING AND/OR REMOVING SIGNS AND SUPPORTS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.
ITEM 614 - DETOUR SIGNING LUMP SUM

ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN

ANY ASPHALT CURBS THAT ARE CONSTRUCTED ON TOP OF THE TEMPORARY PAVEMENT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN. THE FLEXIBLE PAVEMENT OPTION SHALL BE USED.

BEGIN CONSTRUCTION DATE

CONSTRUCTION SHALL NOT BEGIN UNTIL FEBRUARY 1, 2024 UNLESS APPROVED BY THE DISTRICT.

CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC GENERAL NOTES

LAK-US-20-24.99
PART 2

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MN003.dgn_Sheet 7/15/2022 2:50:11PM troyer

MAINTENANCE OF TRAFFIC NARRATIVE

THIS PROJECT HAS BEEN DIVIDED INTO TWO PARTS: PART 1 AND PART 2. PART 2 (LAK-US-20-24.99) IS A TWO YEAR PROJECT AND SHALL BE BUILT FIRST. PART 1 (LAK-US-20-19.59) IS A THREE YEAR PROJECT AND SHALL BE BUILT SECOND. WITHIN EACH OF THESE PARTS, PHASES SHOULD BE CONSTRUCTED FROM EAST TO WEST PER THE CONSTRUCTION SEQUENCES. CONSTRUCTION OF TEMPORARY PAVEMENT SHALL ONLY OCCUR IN THE PHASE WHERE IT IS IMMEDIATELY NEEDED. NO CONSTRUCTION SHALL OCCUR DURING THE WINTER MONTHS.

SEQUENCE OF CONSTRUCTION

PRE-PHASE 1:

THE TEMPORARY PAVEMENT AND THE TEMPORARY DRAINAGE ARE TO BE CONSTRUCTED UNDER THIS PHASE. WORK SHALL BE COMPLETED BY APRIL 1, 2024.

PHASE 1:

THIS PHASE WILL INVOLVE FIRSTLY THE RECONSTRUCTION OF THE WESTERN SIDE OF HUBBARD ROAD AND ITS DRAINAGE OUTLET; AND SECONDLY THE SOUTH SIDE OF U.S. 20 FROM STA. 500+40 TO STA. 572+50. ONE (1) LANE OF WESTBOUND TRAFFIC AND ONE (1) LANE OF EASTBOUND TRAFFIC WILL BE MAINTAINED AT ALL TIMES ALONG THIS SECTION OF U.S. 20. ONE (1) LANE OF NORTHBOUND TRAFFIC AND ONE (1) LANE OF SOUTHBOUND TRAFFIC WILL BE MAINTAINED AT ALL TIMES ALONG THIS SECTION OF HUBBARD ROAD. HUBBARD ROAD AND ITS DRAINAGE OUTLET SHALL BE CONSTRUCTED PRIOR TO THE WORK ON U.S. 20. CONSTRUCTION OF THE INTERSECTION OF HUBBARD ROAD AND U.S. 20 SHALL BE PERFORMED ON WEEKENDS.

THE SURFACE COURSE SHALL BE NON-PERFORMED AT THIS TIME AND THE INTERMEDIATE COURSE SHALL BE BUILT UP TO THE TOP OF PAVEMENT WITH A THICKNESS OF 3 INCHES.

THERE WILL BE NO RESURFACING DURING THIS PHASE.

THE CONTRACTOR SHALL BE ALLOWED TO CLOSE EACH SIDE STREET FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT THE SIDE STREET REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE FOLLOWING IS A LIST OF SIDE STREETS TO BE CLOSED DURING THIS PHASE:

BURNS ROAD STA. 517+13 RT SIDE STREET

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT SIDE STREETS AT THE SAME TIME.

THE CONTRACTOR SHALL BE ALLOWED TO CLOSE EACH ACCESS DRIVEWAY FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT THE DRIVEWAY REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE FOLLOWING IS A LIST OF ACCESS DRIVEWAYS TO BE CLOSED DURING THIS PHASE:

ACCESS DRIVE STA. 550+31 RT MAJOR DRIVE
ACCESS DRIVE STA. 556+02 RT MAJOR DRIVE

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT MAJOR DRIVEWAYS AT THE SAME TIME.

THE FOLLOWING IS A LIST OF SIDE STREETS TO BE CONSTRUCTED UTILIZING PART-WIDTH CONSTRUCTION:

HUBBARD ROAD STA. 565+71 RT
(INCLUDING THE DRAINAGE OUTLET)

PEDESTRIAN TRAFFIC SHALL BE MAINTAINED.

PHASE 1 SHALL BE COMPLETED BY JULY 1, 2024.

PHASE 2:

THIS PHASE WILL INVOLVE THE RECONSTRUCTION OF THE NORTH SIDE OF U.S. 20 FROM STA. 500+40 TO STA. 572+50 AND THE EASTERN SIDE OF HUBBARD ROAD. ONE (1) LANE OF WESTBOUND TRAFFIC AND ONE (1) LANE OF EASTBOUND TRAFFIC WILL BE MAINTAINED AT ALL TIMES ALONG THIS SECTION OF U.S. 20. ONE (1) LANE OF NORTHBOUND TRAFFIC AND ONE (1) LANE OF SOUTHBOUND TRAFFIC WILL BE MAINTAINED AT ALL TIMES ALONG THIS SECTION OF HUBBARD ROAD. CONSTRUCTION OF THE INTERSECTION OF HUBBARD ROAD AND U.S. 20 SHALL BE PERFORMED ON WEEKENDS.

THE SURFACE COURSE SHALL BE NON-PERFORMED AT THIS TIME AND THE INTERMEDIATE COURSE SHALL BE BUILT UP TO THE TOP OF PAVEMENT WITH A THICKNESS OF 3 INCHES.

THERE WILL BE NO RESURFACING DURING THIS PHASE.

THE FOLLOWING ITEMS SHALL BE PERFORMED AT THE END OF PHASE 2 IN THE PHASE 1 / 2 WORK AREA:
A. TRAFFIC SIGNAL AND FINAL CURB AND GUTTER WORK AT THE HUBBARD RD INTERSECTION.
B. ALL OTHER TRAFFIC SIGNAL WORK.
C. PERMANENT SIGNING.
D. TEMPORARY STRIPING.

THE CONTRACTOR SHALL BE ALLOWED TO CLOSE EACH SIDE STREET FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT THE SIDE STREET REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE FOLLOWING IS A LIST OF SIDE STREETS TO BE CLOSED DURING THIS PHASE:

WALMART DR. STA. 504+50 LT SIDE STREET
STONEYRIDGE DR. STA. 532+95 LT SIDE STREET
MORNINGSTAR DR. STA. 536+43 LT SIDE STREET

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT SIDE STREETS AT THE SAME TIME.

THE CONTRACTOR SHALL BE ALLOWED TO CLOSE EACH ACCESS DRIVEWAY FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT THE DRIVEWAY REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE FOLLOWING IS A LIST OF ACCESS DRIVEWAYS TO BE CLOSED DURING THIS PHASE:

ACCESS DRIVE STA. 558+30 LT MAJOR DRIVE

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT MAJOR DRIVEWAYS AT THE SAME TIME.

THE FOLLOWING IS A LIST OF SIDE STREETS TO BE CONSTRUCTED UTILIZING PART-WIDTH CONSTRUCTION:

DERUBERTIS ROAD STA. 550+35 LT
HUBBARD ROAD STA. 565+71 LT

PEDESTRIAN TRAFFIC SHALL BE MAINTAINED.

APPLY TEMPORARY STRIPING ON NEWLY CONSTRUCTED PAVEMENT AS PER THE STRIPING PLANS.

PHASE 2 SHALL BE COMPLETED BY OCTOBER 15, 2024.

PRE-PHASE 3:

THE TEMPORARY PAVEMENT AND TEMPORARY DRAINAGE TO BE CONSTRUCTED UNDER THIS PHASE. WORK SHALL BE COMPLETED BY APRIL 1, 2025.

PHASE 3:

THIS PHASE WILL INVOLVE THE RECONSTRUCTION OF THE SOUTH SIDE OF U.S. 20 FROM STA. 411+20 TO STA. 426+00 AND FROM STA. 440+15 TO STA. 486+40. ONE (1) LANE OF WESTBOUND TRAFFIC AND ONE (1) LANE OF EASTBOUND TRAFFIC WILL BE MAINTAINED AT ALL TIMES ALONG THIS SECTION OF U.S. 20.

THE SURFACE COURSE SHALL BE NON-PERFORMED AT THIS TIME AND THE INTERMEDIATE COURSE SHALL BE BUILT UP TO THE TOP OF PAVEMENT WITH A THICKNESS OF 3 INCHES. THERE WILL BE NO RESURFACING DURING THIS PHASE.

PEDESTRIAN TRAFFIC SHALL BE MAINTAINED.

PHASE 3 SHALL BE COMPLETED BY JULY 1, 2025.

PHASE 4:

THIS PHASE WILL INVOLVE THE RECONSTRUCTION OF THE NORTH SIDE OF U.S. 20 FROM STA. 411+20 TO STA. 426+00 AND FROM STA. 440+15 TO STA. 486+40. ONE (1) LANE OF WESTBOUND TRAFFIC AND ONE (1) LANE OF EASTBOUND TRAFFIC WILL BE MAINTAINED AT ALL TIMES ALONG THIS SECTION OF U.S. 20. THE SURFACE COURSE SHALL BE NON-PERFORMED AT THIS TIME AND THE INTERMEDIATE COURSE SHALL BE BUILT UP TO THE TOP OF PAVEMENT WITH A THICKNESS OF 3 INCHES. THERE WILL BE NO RESURFACING DURING THIS PHASE.

THE FOLLOWING ITEMS SHALL BE PERFORMED AT THE END OF PHASE 4:

- A. TRAFFIC SIGNAL WORK.
- B. PERMANENT SIGNING.
- C. TEMPORARY STRIPING.

THE CONTRACTOR SHALL BE ALLOWED TO CLOSE EACH SIDE STREET FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT THE SIDE STREET REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE FOLLOWING IS A LIST OF SIDE STREETS TO BE CLOSED DURING THIS PHASE:

HAINES ROAD STA. 463+33 LT SIDE STREET

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT SIDE STREETS AT THE SAME TIME.

PEDESTRIAN TRAFFIC SHALL BE MAINTAINED.

PHASE 4 SHALL BE COMPLETED BY SEPTEMBER 15, 2025.

PHASE 5:

MILL OFF 1.25 INCHES OFF THE TOP OF THE PAVEMENT AND THEN FILL WITH ITEM 441 SURFACE COURSE PER THE FINAL PAVEMENT BUILDUP SHOWN IN THE TYPICAL SECTIONS WHILE MAINTAINING TRAFFIC USING SCD MT-97.12 OVER THE ENTIRE PROJECT (NEWLY CONSTRUCTED PAVEMENT AND RESURFACING AREA ON U.S. 20 (BETWEEN STA. 411+20 TO STA. 552+50), HUBBARD ROAD, AND ALL OF THE SIDE ROADS. AFTER RESURFACING, THE ENTIRE PROJECT SHALL BE RESTRIPIED PER THE STRIPING PLAN. FIRST WITH CLASS 3 PRE-STRIPING AND THEN WITH THE FINAL STRIPING.

PEDESTRIAN TRAFFIC SHALL BE MAINTAINED.

PHASE 5 SHALL BE COMPLETE BY OCTOBER 15, 2025.

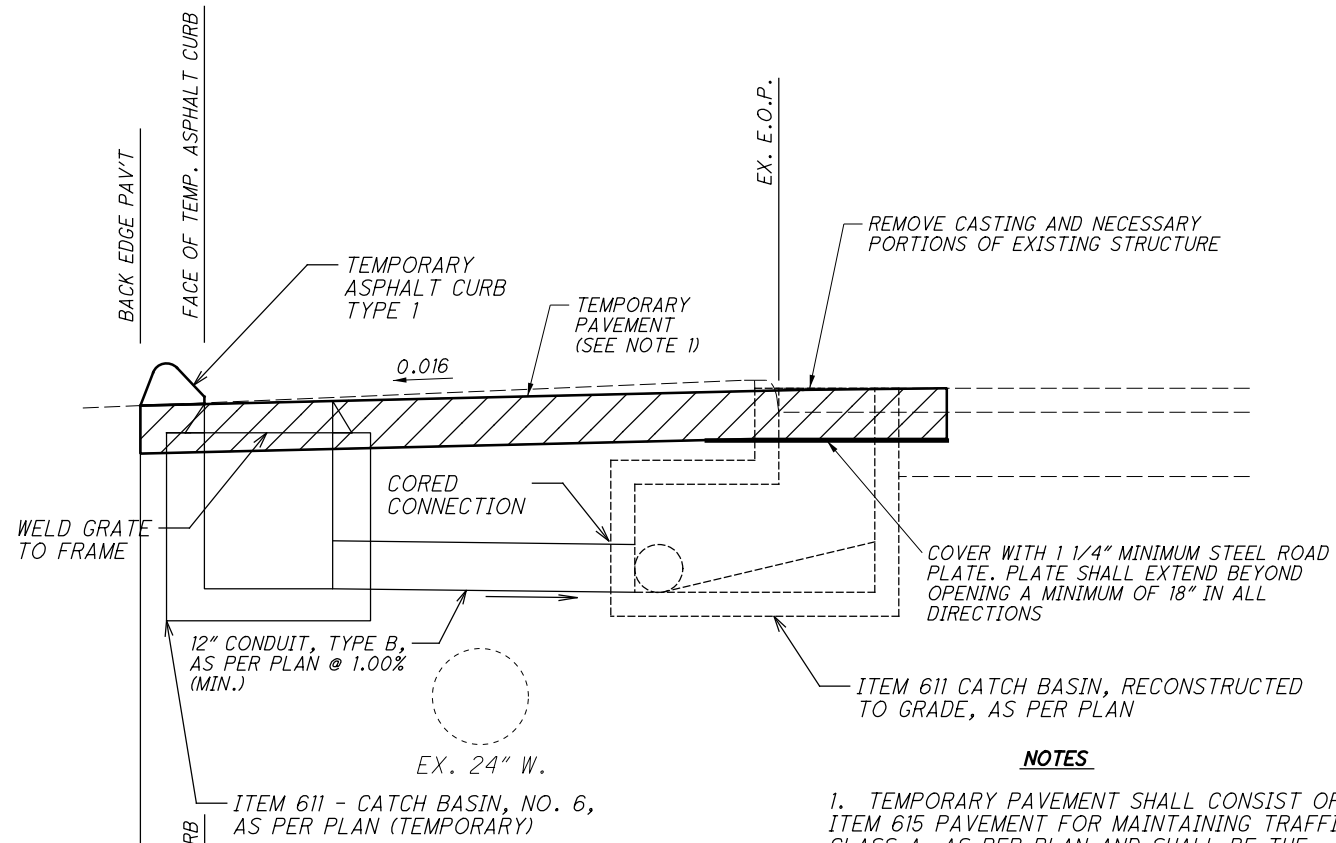
H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MN004.dgn Sheet 7/15/2022 2:50:42 PM troyer

CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC GENERAL NOTES

LAK-US-20-24.99
PART 2

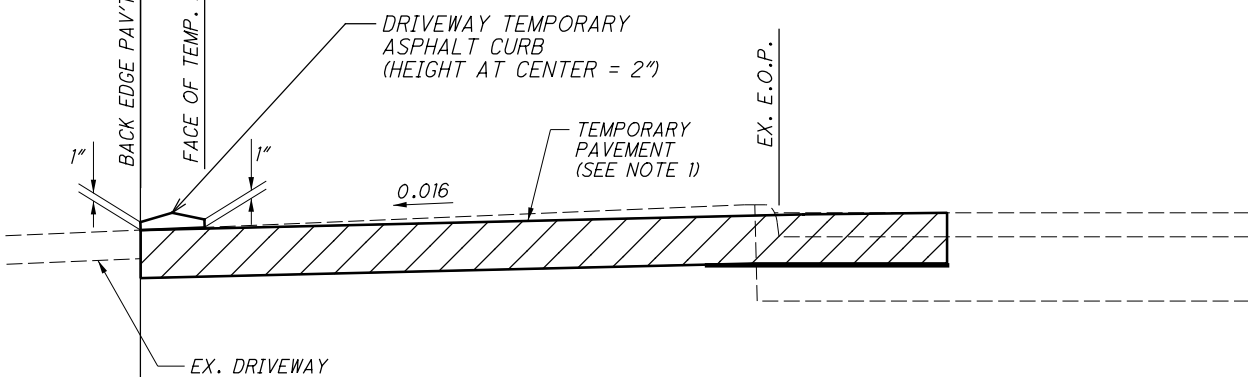
H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\DOT\Sheets\09270_MN005.dgn Sheet 7/15/2022 2:50:13 PM troyer



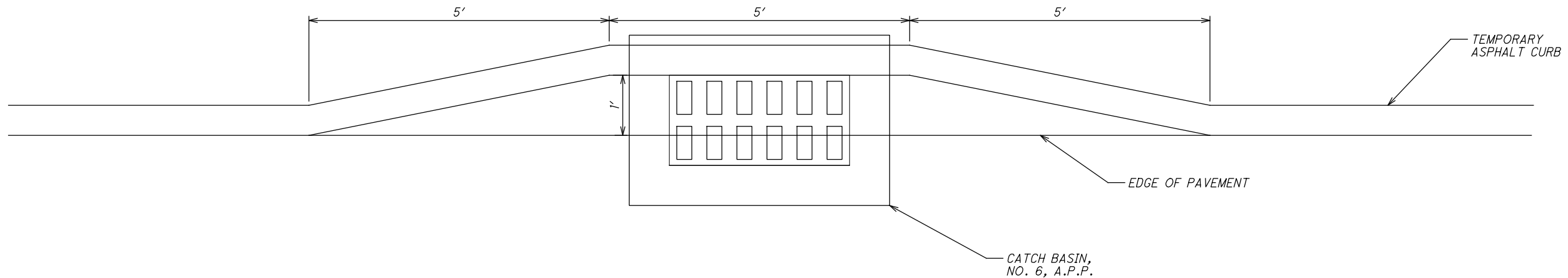
**TEMPORARY DRAINAGE DETAIL
PHASE 1 AND PHASE 3**

NOTES

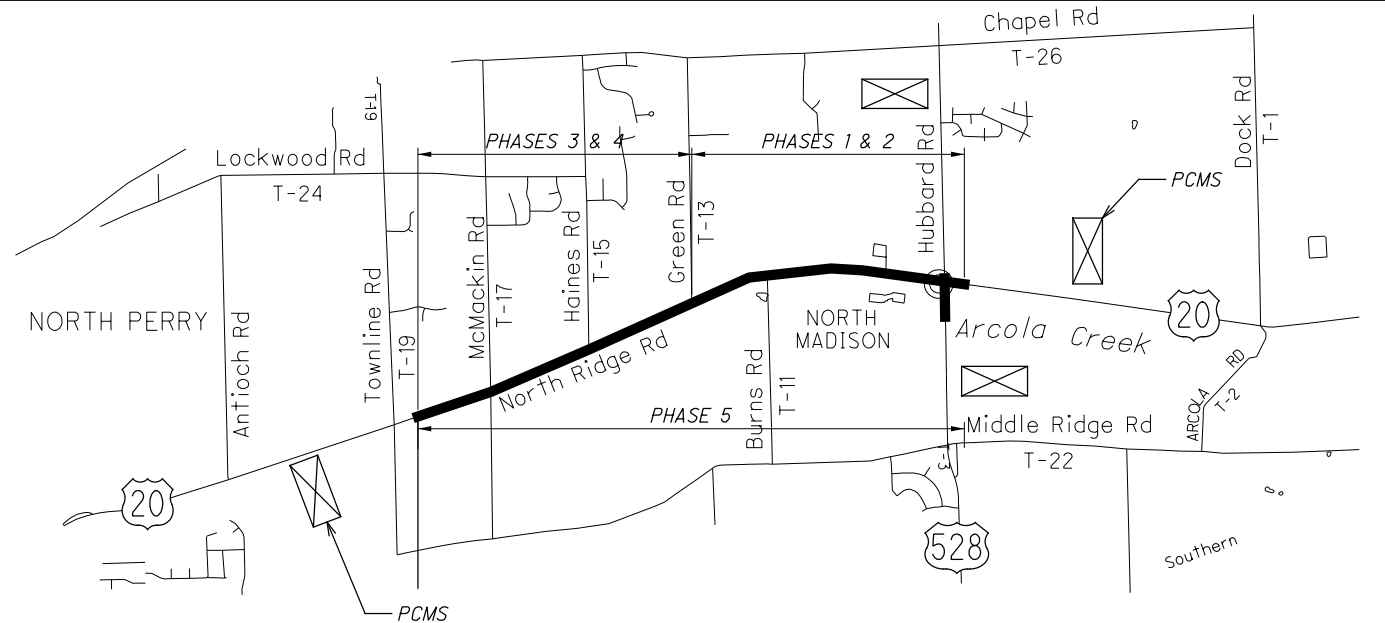
1. TEMPORARY PAVEMENT SHALL CONSIST OF ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN AND SHALL BE THE FLEXIBLE BUILDUP.
2. CONTRACTOR TO DETERMINE FLOWLINE OF TEMPORARY CATCH BASIN AND PIPE.



**TEMPORARY ASPHALT CURB AT DRIVEWAYS
PHASE 1, PHASE 3, AND PHASE 5**



CATCH BASIN BUMPOUT DETAIL



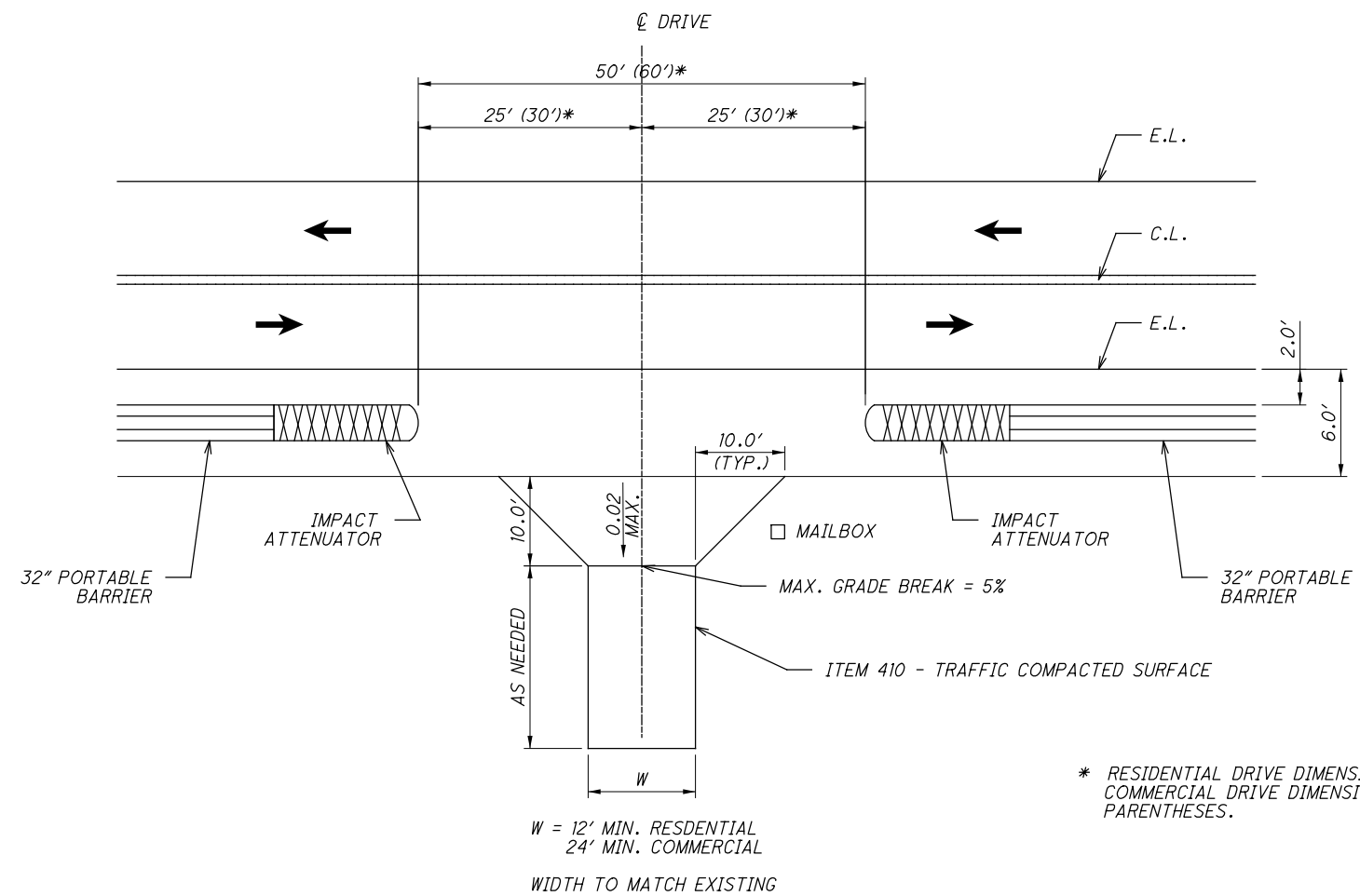
PCMS LOCATION AND PHASING DETAIL

CALCULATED
EJT
CHECKED
WDB

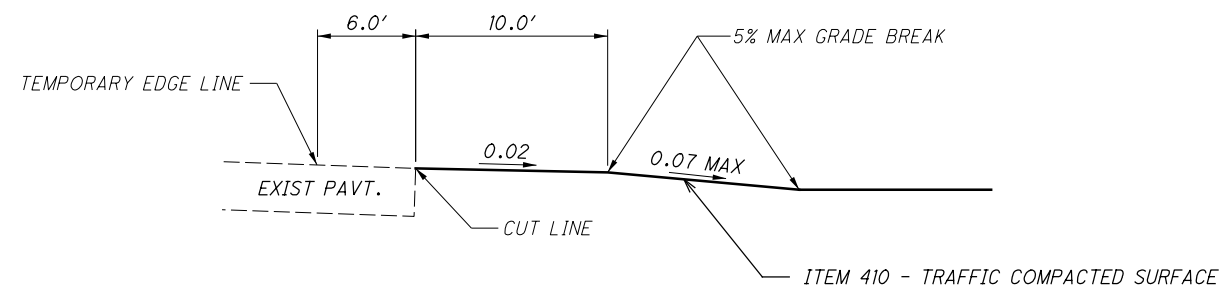
MAINTENANCE OF TRAFFIC GENERAL NOTES

**LAK-US-20-24.99
PART 2**

H:\2018\180743\000T\Project\Data\09270_LAK_20_EAST\Design\M0T\Sheets\09270_MN006.dgn_Sheet 7/15/2022 2:50:43 PM troyer



TEMPORARY DRIVE DETAIL



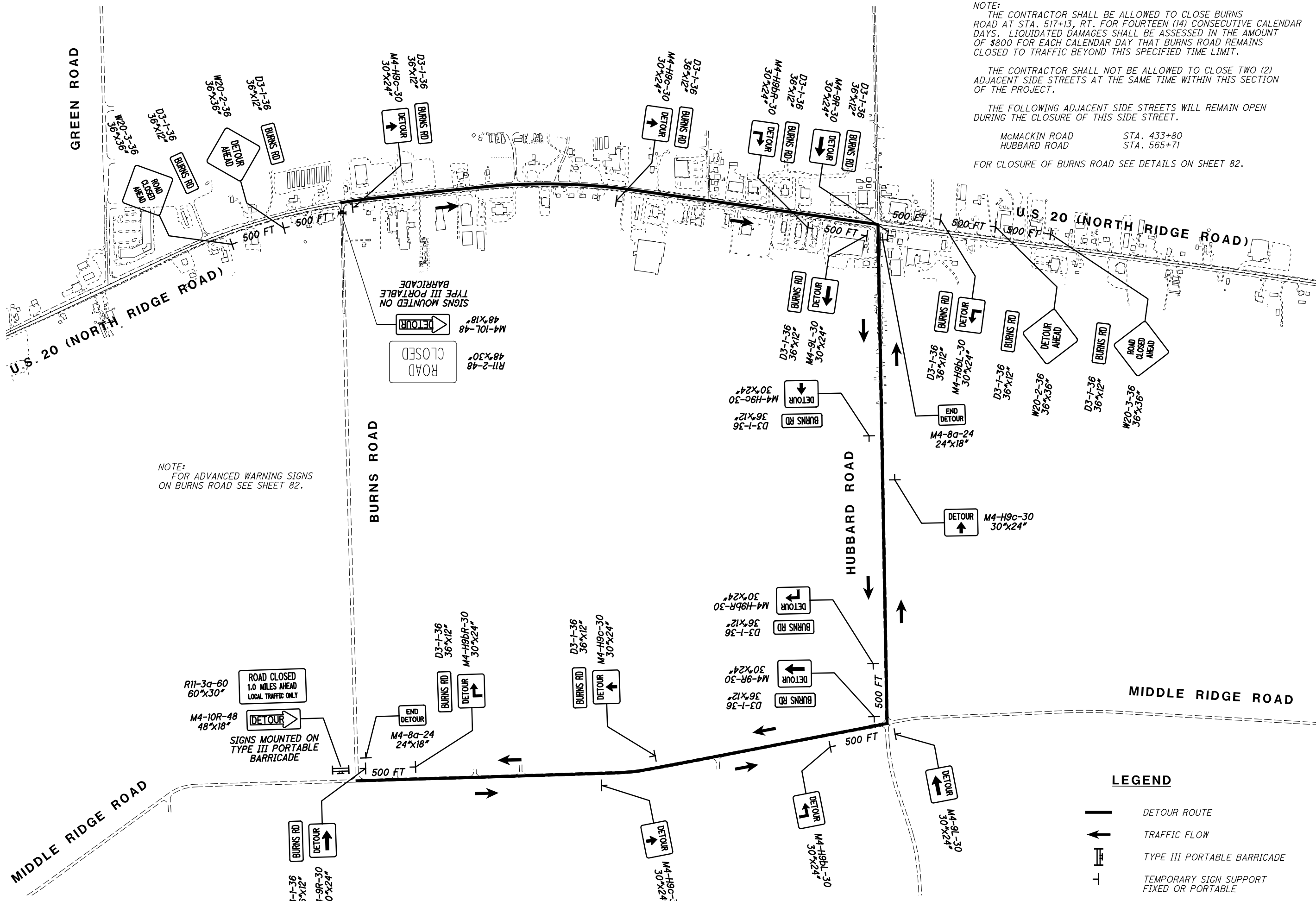
TEMPORARY DRIVE PROFILE

CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC MISCELLANEOUS DETAILS

LAK-US-20-24.99
PART 2

H:\2018\180743\000\Project\Data\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MD009.dgn Sheet 7/15/2022 2:50:17 PM troyer



NOTE:
FOR ADVANCED WARNING SIGNS
ON BURNS ROAD SEE SHEET 82.

NOTE:
THE CONTRACTOR SHALL BE ALLOWED TO CLOSE BURNS ROAD AT STA. 517+13, RT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$800 FOR EACH CALENDAR DAY THAT BURNS ROAD REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT SIDE STREETS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

THE FOLLOWING ADJACENT SIDE STREETS WILL REMAIN OPEN DURING THE CLOSURE OF THIS SIDE STREET.

McMACKIN ROAD STA. 433+80
HUBBARD ROAD STA. 565+71

FOR CLOSURE OF BURNS ROAD SEE DETAILS ON SHEET 82.

CALCULATED JUMP CHECKED WDB

0 200 400 800

HORIZONTAL SCALE IN FEET

↑

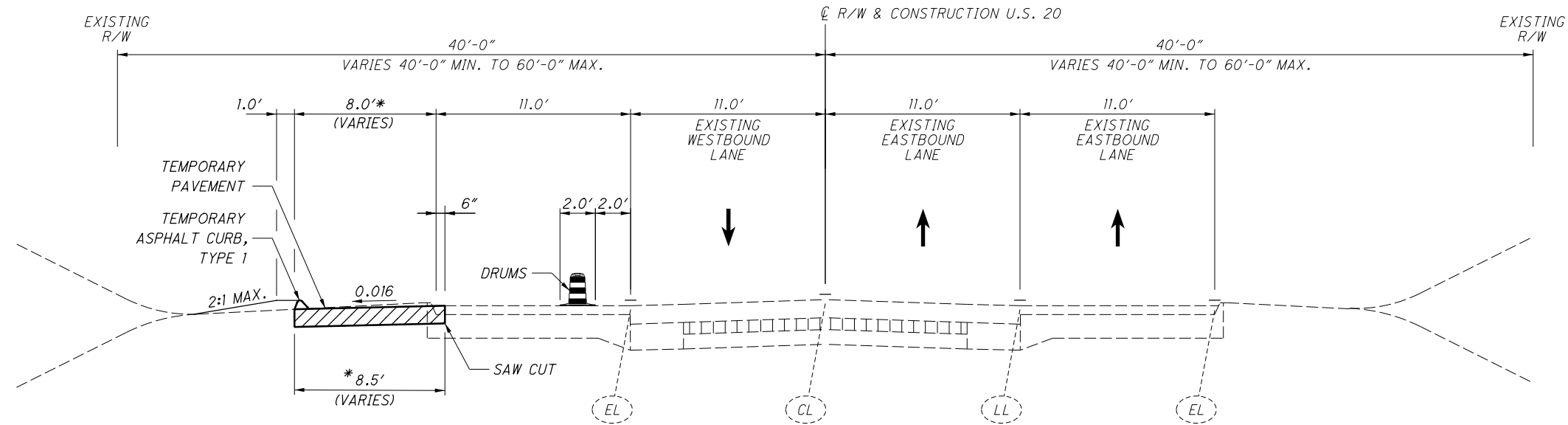
MAINTENANCE OF TRAFFIC DETOUR PLAN
BURNS ROAD

LAK-US-20-24.99
PART 2

LEGEND

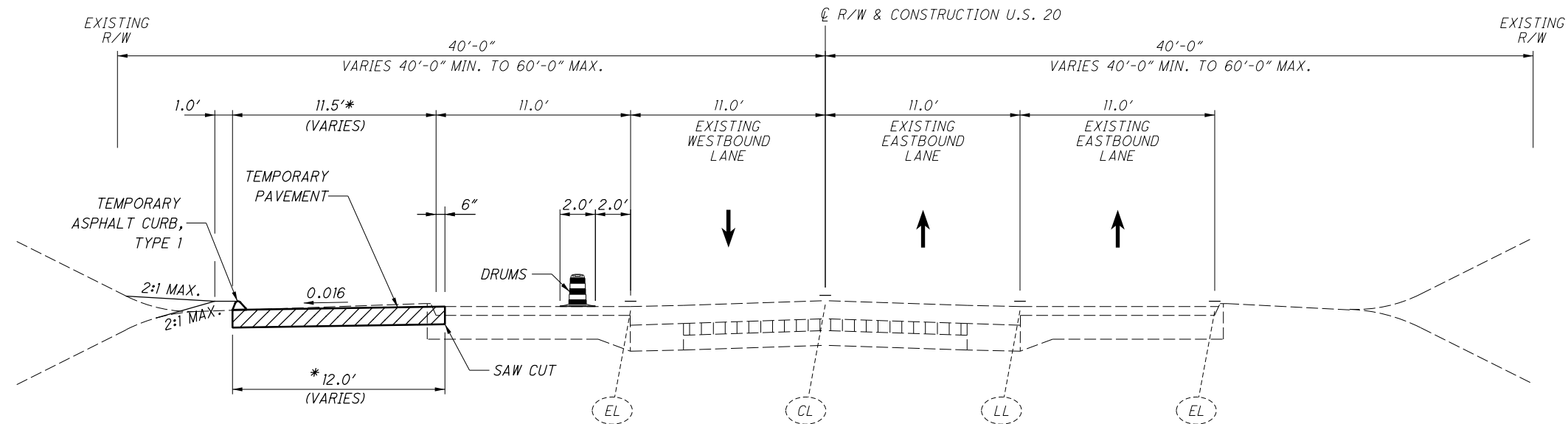
- DETOUR ROUTE
- ↑ TRAFFIC FLOW
- TYPE III PORTABLE BARRICADE
- TEMPORARY SIGN SUPPORT FIXED OR PORTABLE

H:\2018\80743\000T\Projec+Data\092TO_LAK_20_EAST\Design\MOT\Sheets\09270_My001.dgn MOT-01.ALT I 7/15/2022 2:50:17 PM troyer



PRE-PHASE 1
STA. 512+00 TO STA. 577+06.4

*TEMPORARY PAVEMENT
FROM STA. 512+00 TO STA. 514+50
VARIES FROM 33.5' LT TO 30.0' LT



PRE-PHASE 1
STA. 497+66.1 TO STA. 512+00

PRE-PHASE 3
STA. 409+04.9 TO STA. 430+24.8
STA. 436+55 TO STA. 488+88.6

NOTES

1. CONSTRUCT TEMPORARY PAVEMENT BY TAPERING EASTBOUND TRAFFIC FROM TWO LANES TO ONE LANE USING SCD MT-95.31.

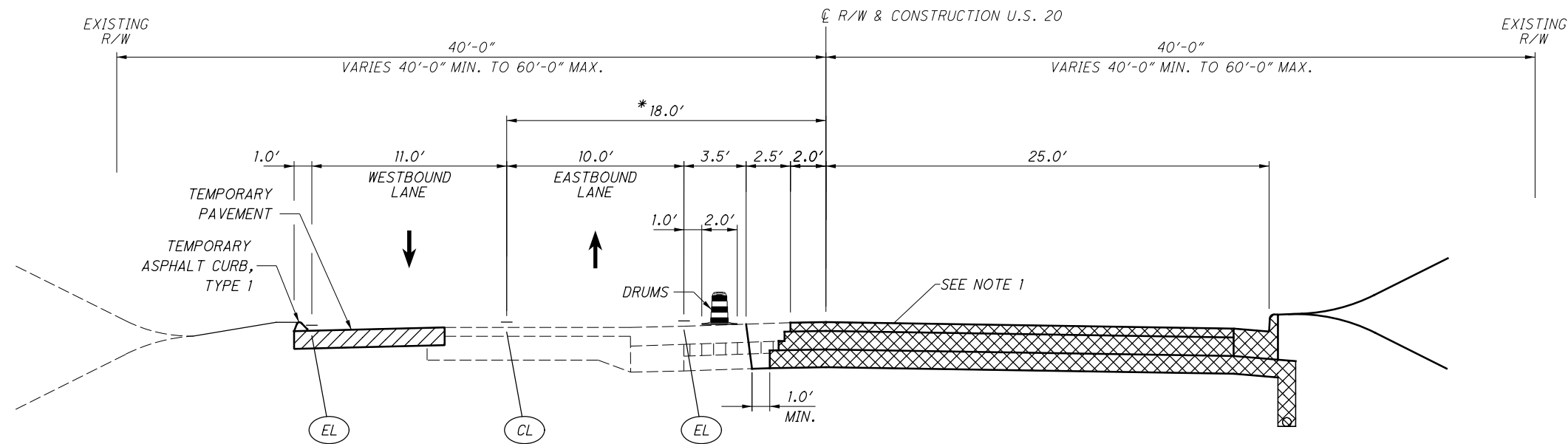
LEGEND

- (CL) EXISTING CENTER LINE
- (EL) EXISTING EDGE LINE
- (LL) EXISTING CHANNELIZING LINE
- [Hatched Box] PROPOSED TEMPORARY PAVEMENT CONSTRUCTED THIS PHASE
- ← TRAFFIC FLOW

LEGEND

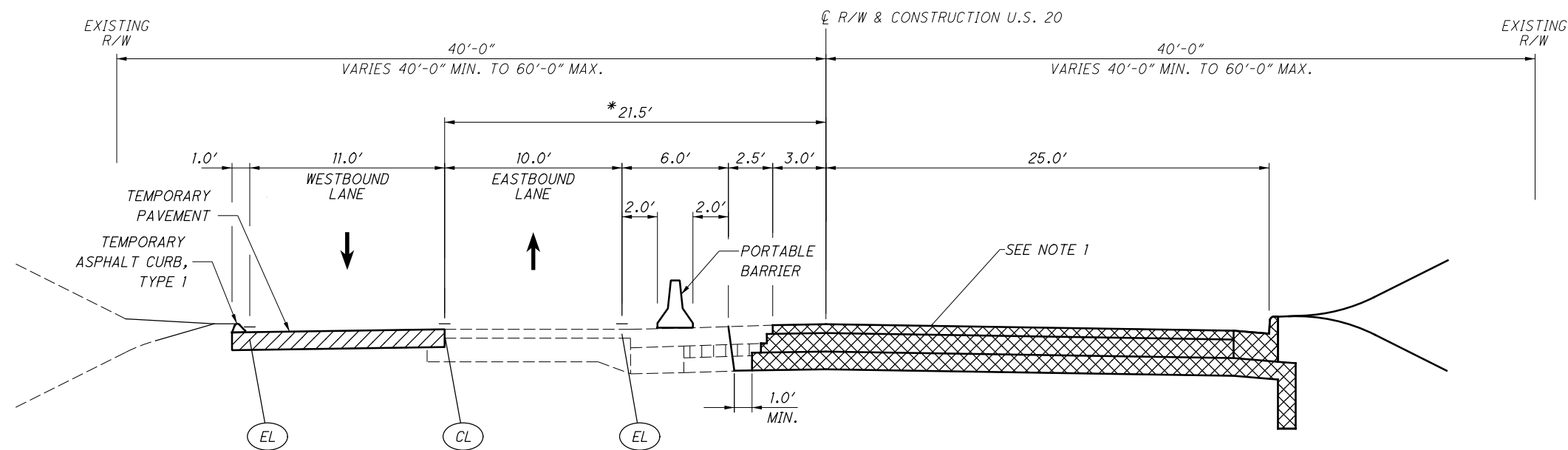
- (CL) EXISTING CENTER LINE
- (EL) EXISTING EDGE LINE
- (LL) EXISTING CHANNELIZING LINE
- [Hatched Box] PROPOSED TEMPORARY PAVEMENT CONSTRUCTED THIS PHASE
- ← TRAFFIC FLOW

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MY001.dgn MOT-02 ALT 1 7/15/2022 2:50:18 PM troyer



PHASE 1
STA. 516+30 TO STA. 572+50

*LANE SHIFT
FROM STA. 512+00 TO STA. 514+50
VARIES FROM 21.5' LT TO 18.0' LT



PHASE 1
STA. 500+40 TO STA. 516+30

PHASE 3
STA. 411+20 TO STA. 426+00
STA. 440+15 TO STA. 486+40

NOTES

1. THE SURFACE COURSE SHALL BE TEMPORARILY NON-PERFORMED IN PHASES 1 AND 3, AND THE INTERMEDIATE COURSE SHALL BE PLACED WITH A THICKNESS OF 3 INCHES.

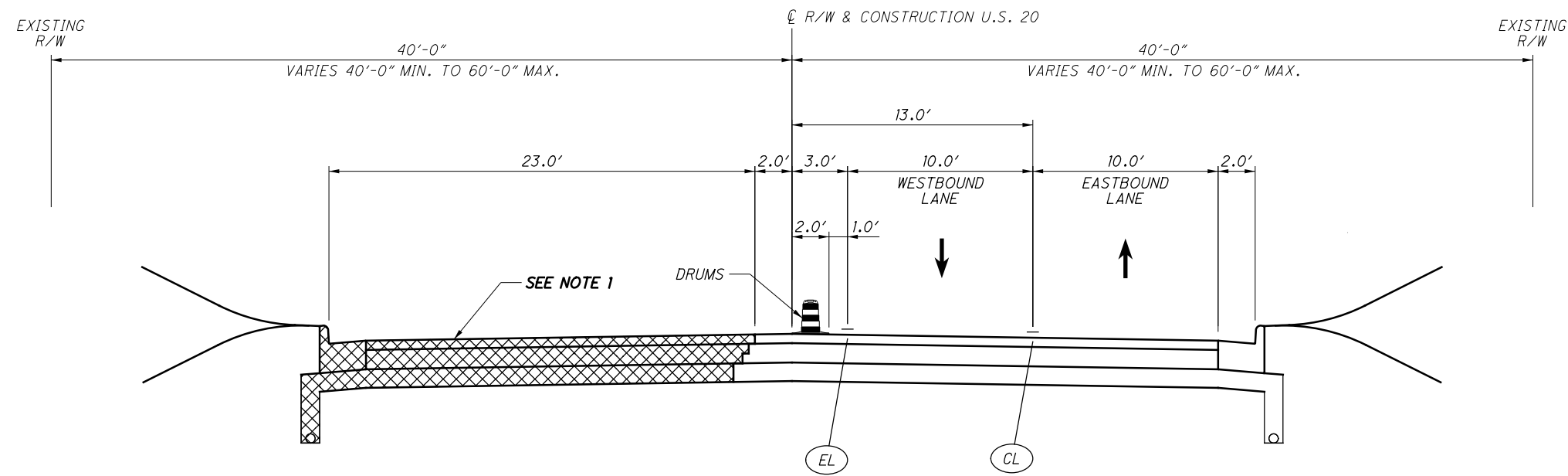
LEGEND

- (CL) WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (EL) WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- [Cross-hatched] PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- [Diagonal lines] PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- ← TRAFFIC FLOW

LEGEND

- (CL) WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (EL) WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- [Cross-hatched] PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- [Diagonal lines] PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1 & 3
- ← TRAFFIC FLOW

H:\2018\180743\ODOT\Projec+Data\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MY001.dgn MOT-03 ALT 1 7/15/2022 2:50:19 PM troyer







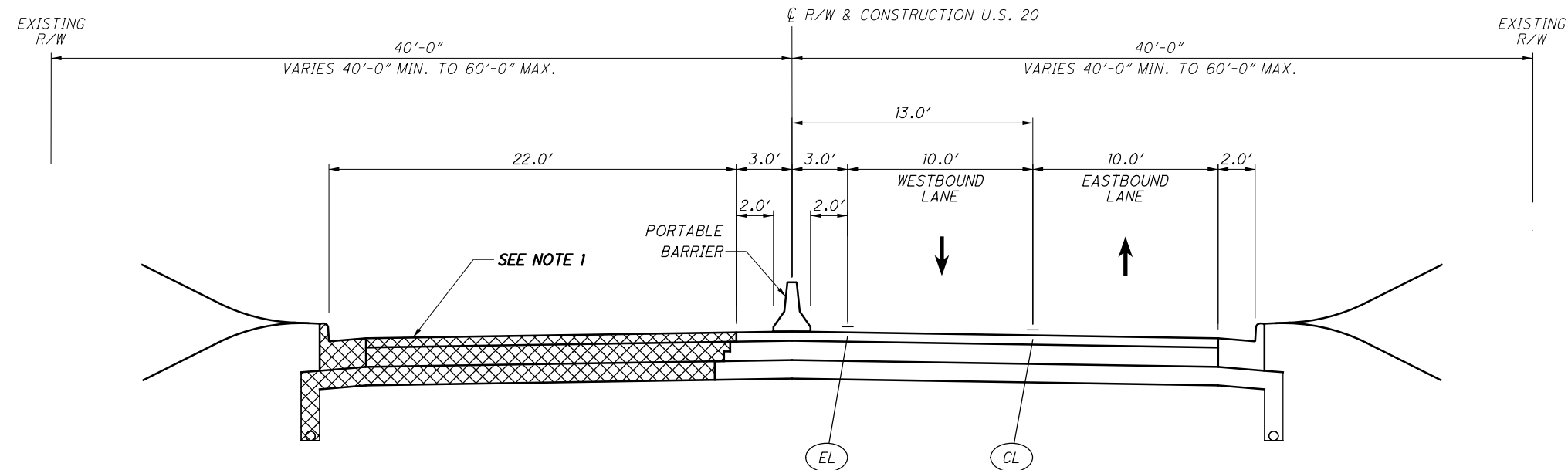
PHASE 2
STA. 516+90 TO STA. 572+50

NOTES

1. THE SURFACE COURSE SHALL BE TEMPORARILY NON-PERFORMED IN PHASES 2 AND 4, AND THE INTERMEDIATE COURSE SHALL BE PLACED WITH A THICKNESS OF 3 INCHES.

LEGEND





-  WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  TRAFFIC FLOW



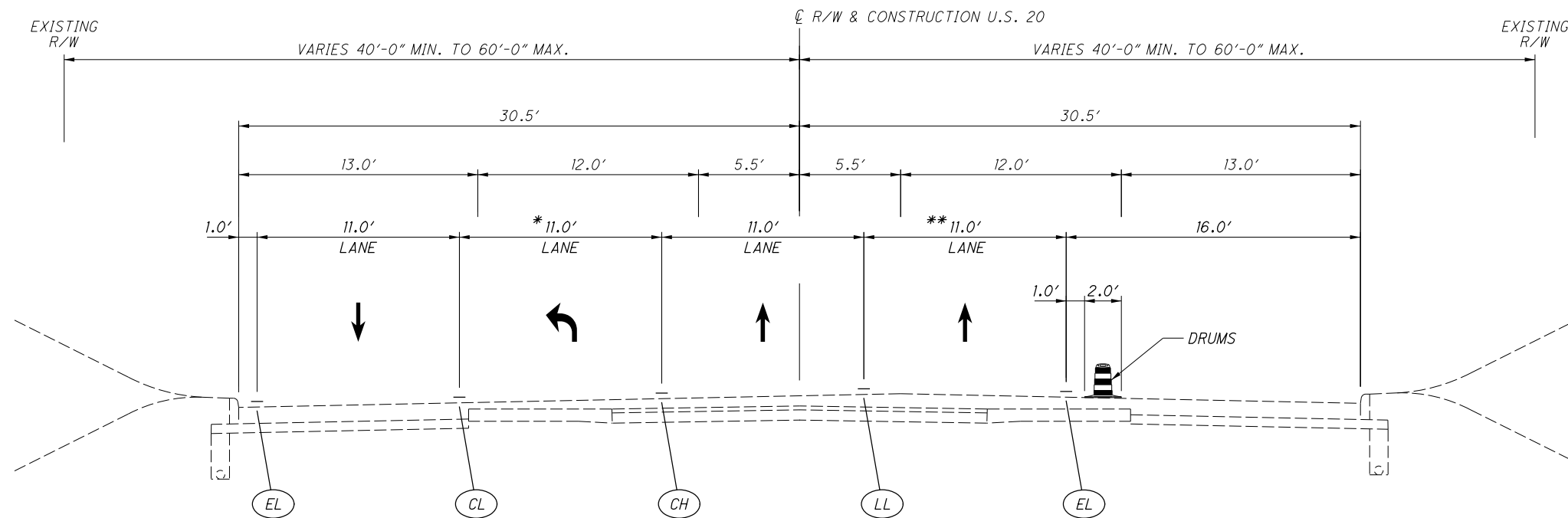
PHASE 2
STA. 500+40 TO STA. 516+90

PHASE 4
STA. 411+20 TO STA. 426+00
STA. 440+15 TO STA. 486+40

LEGEND

-  WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  TRAFFIC FLOW

H:\2018\180743\OD0T\Projec+Data\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MY001.dgn MOT-04 ALT I 7/15/2022 2:50:19 PM troyer



PHASE 2
* GREEN ROAD W.B. - STA. 494+73.13

PHASE 4
TOWNLINE ROAD W.B. - STA. 405+82.64 BK =
STA. 405+83.06 AH

PHASE 3
GREEN ROAD E.B. - STA. 494+73.13

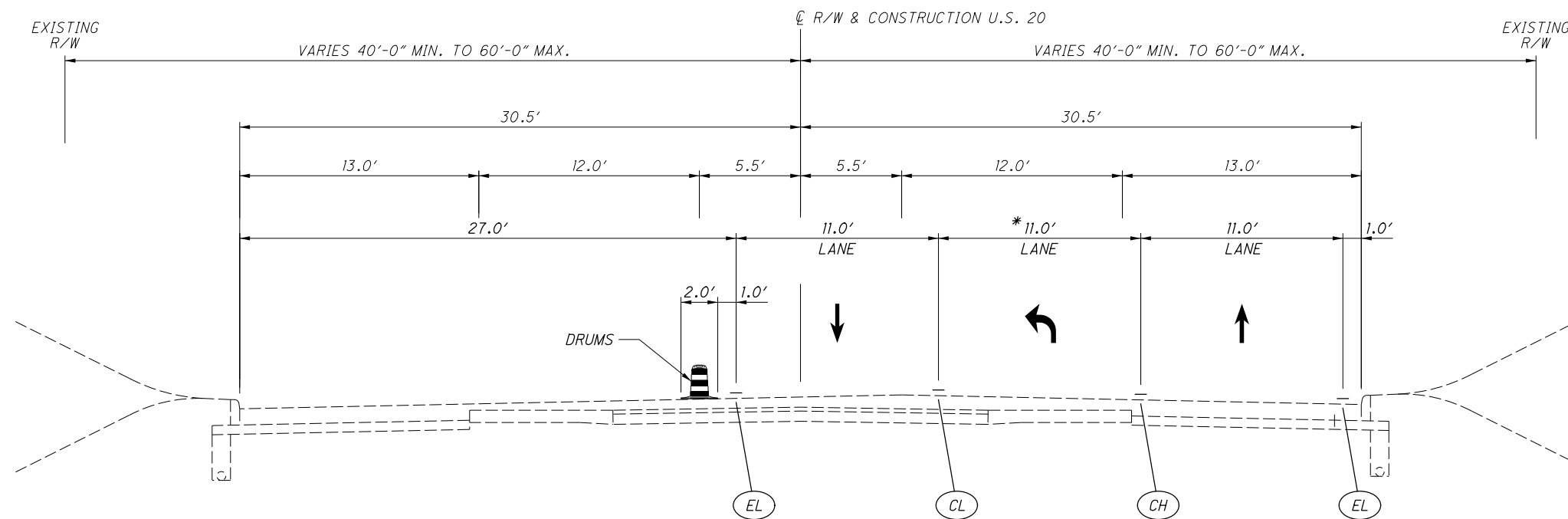
PHASE 4
** McMACKIN ROAD W.B. - STA. 433+79.82

PHASE 3
** McMACKIN ROAD E.B. - STA. 433+79.82

* GREEN ROAD W.B. - NO LEFT TURN LANE
** McMACKIN ROAD W.B. - RIGHT TURN LANE

LEGEND

- (CL) WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (EL) WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (LL) WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (CH) WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ← TRAFFIC FLOW



PHASE 1
* GREEN ROAD W.B. - STA. 494+73.13

PHASE 3
TOWNLINE ROAD W.B. - STA. 405+82.64 BK =
STA. 405+83.06 AH

PHASE 4
GREEN ROAD E.B. - STA. 494+73.13

PHASE 3
McMACKIN ROAD W.B. - STA. 433+79.82

PHASE 4
McMACKIN ROAD E.B. - STA. 433+79.82

* GREEN ROAD W.B. - NO LEFT TURN

LEGEND

- (CL) WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (EL) WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ← TRAFFIC FLOW

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MS001.dgn Sheet 7/15/2022 2:50:20 PM troyer

REF NO.	SHEET NO.	STATION TO STATION		SIDE	611	611	611	614	614	614	614	614	614	614	614	614	614	614	615	622		
					12" CONDUIT, TYPE B, AS PER PLAN FT	CATCH BASIN, NO. 6, AS PER PLAN EACH	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN EACH	WORK ZONE LANE LINE, CLASS I, 642 PAINT MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT MILE	WORK ZONE EDGE LINE, CLASS I, 6" 642 PAINT MILE	WORK ZONE CHANNELIZING LINE, CLASS I, 8" 642 PAINT FT	WORK ZONE DOTTED LINE, CLASS I, 4" 642 PAINT FT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT FT	WORK ZONE STOP LINE, CLASS I, 642 PAINT FT	WORK ZONE CROSSWALK LINE, CLASS I, 24" 642 PAINT FT	WORK ZONE ARROW, CLASS I, 642 PAINT EACH	WORK ZONE ISLAND MARKING, CLASS I SF	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS (BIDIRECTIONAL) EACH	BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL) EACH	OBJECT MARKER, TWO WAY EACH	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN SY	PORTABLE BARRIER, UNANCHORED FT
		PHASE 1	TO																			
LL-1	54	477+60		479+00				0.027														
LL-2	54	477+60		479+00				0.027														
CL-1	54	477+60		479+00					0.027													
EL-1	55	479+60		484+00						0.083												
LL-1	55	479+00		484+00			0.095															
LL-2	55	479+00		479+60			0.011															
CL-1	55	479+00		484+00				0.095														
DL-1	55	479+60		484+00							440											
LL-1	56	484+00		487+05			0.058															
CL-1	56	484+00		489+00	C/L & LT			0.095														
CL-2	56	487+05		489+00	LT			0.037														
EL-1	56	485+55		489+00	LT				0.067													
EL-2	56	484+00		489+00	RT				0.095													
DL-1	56	484+00		484+55	RT					55												
TL-1	56	487+05		489+00	LT						25											
CL-1	57	489+00		494+00	LT			0.095														
CL-2	57	489+00		490+95	LT			0.037														
EL-1	57	489+00		494+00	RT				0.095													
EL-2	57	489+00		494+00	LT				0.095													
CH-1	57	491+20		493+74	LT					254												
TL-1	57	489+00		490+95	LT							77										
SL-1	57	493+74			LT						22											
LA-1	57	491+32			LT								1									
LA-2	57	491+90			LT								1									
LA-3	57	492+48			LT								1									
LA-4	57	493+06			LT								1									
LA-5	57	493+64			LT								1									
CL-1	58	494+00		499+00	LT			0.095														
CL-2	58	495+70		499+00	LT			0.0625														
EL-1	58	494+00		499+00	LT				0.095													
EL-2	58	494+00		499+00	RT				0.095													
CW-1	58	495+50			RT & LT								130									
CW-2	58	495+00			LT								144									
SL-1	58	495+70			LT						11											
SL-2	58	10+77			LT						13											
SL-3	58	11+13			LT						11											
TL-1	58	495+70		499+00	RT & LT							135										
IM-1	58	495+70			RT & LT										51							
D-1	58	498+49			LT	10	1	1														
P-1	58	497+66.1		499+00	LT													45				
PB-1	58	497+94		499+00	RT														86			
CL-1	59	499+00		504+00	LT			0.095														
CL-2	59	499+00		500+40	LT			0.0265														
EL-1	59	499+00		504+00	LT				0.095													
EL-2	59	499+00		504+00	LT				0.095													
TL-1	59	499+00		500+40	RT & LT						14											
P-2	59	499+00		504+00	RT														549			
PB-2	59	499+00		504+00	RT & LT											2	5	5		400		
CL-1	60	504+00		509+00	LT			0.095														
EL-1	60	504+00		509+00	LT				0.095													
EL-2	60	504+00		509+00	LT				0.095													
SUBTOTAL SHEET 33								0.218	0.760	1.005	254	495	251	57	274	5	51	3	8	8	594	486

MAINTENANCE OF TRAFFIC SUBSUMMARY

LAK-US-20-24.99 PART 2

CALCULATED
ARK
CHECKED
EJT

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MS002.dgn Sheet 7/15/2022 2:50:21PM Troyer

REF NO.	SHEET NO.	STATION TO STATION		SIDE	611	611	611	614	614	614	614	614	614	614	614	614	614	614	615	622		
					12" CONDUIT, TYPE B, AS PER PLAN FT	CATCH BASIN, NO. 6, AS PER PLAN EACH	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN EACH	WORK ZONE LANE LINE, CLASS I, 642 PAINT MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT MILE	WORK ZONE EDGE LINE, CLASS I, 6" 642 PAINT MILE	WORK ZONE CHANNELIZING LINE, CLASS I, 8" 642 PAINT FT	WORK ZONE DOTTED LINE, CLASS I, 4" 642 PAINT FT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT FT	WORK ZONE STOP LINE, CLASS I, 642 PAINT FT	WORK ZONE CROSSWALK LINE, CLASS I, 24" 642 PAINT FT	WORK ZONE ARROW, CLASS I, 642 PAINT EACH	WORK ZONE ISLAND MARKING, CLASS I SF	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS (BIDIRECTIONAL) EACH	BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL) EACH	OBJECT MARKER, TWO WAY EACH	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN SY	PORTABLE BARRIER, UNANCHORED FT
			TO																			
SL-1	60	504+32		LT											14							
P-3	60	504+00	509+00	LT															148			
PB-3	60	504+00	509+00	LT														4	4	4	300	
CL-1	61	509+00	514+00	LT				0.095														
EL-1	61	509+00	514+00	LT					0.095													
EL-2	61	509+00	514+00	LT					0.095													
P-4	61	509+00	514+00	LT															667			
PB-4	61	509+00	514+00	LT														4	4	4	310	
CL-1	62	514+00	519+00	LT				0.095														
CL-2	62	8+25	9+58	RT				0.025														
EL-1	62	514+00	519+00	LT					0.095													
EL-2	62	514+00	519+00	LT					0.095													
EL-3	62	8+25	9+34	LT					0.021													
EL-4	62	8+25	9+44	RT					0.023													
SL-1	62	516+23		LT										10								
SL-2	62	517+72		LT										11								
SL-3	62	9+58		RT										15								
P-5	62	514+00	519+00	LT															500			
PB-5	62	514+00	515+09.5	LT														1	2	2	90	
CL-1	63	519+00	524+00	LT				0.095														
EL-1	63	519+00	524+00	LT					0.095													
EL-2	63	519+00	524+00	LT					0.095													
D-2	63	523+32	523+75	LT&RT	58	1	1															
P-6	63	519+00	524+00	LT															500			
CL-1	64	524+00	529+00	LT				0.095														
EL-1	64	524+00	529+00	LT					0.095													
EL-2	64	524+00	529+00	LT					0.095													
D-3	64	525+48	526+00	LT&RT	63	1	1															
P-7	64	524+00	529+00	LT															500			
CL-1	65	529+00	534+00	LT				0.095														
EL-1	65	529+00	534+00	LT					0.095													
EL-2	65	529+00	534+00	LT					0.095													
SL-1	65	10+42		LT										15								
P-8	65	529+00	534+00	LT															440			
CL-1	66	534+00	539+00	LT				0.095														
EL-1	66	534+00	539+00	LT					0.095													
EL-2	66	534+00	539+00	LT					0.095													
SL-1	66	10+42		LT										13								
P-9	66	534+00	539+00	LT															450			
CL-1	67	539+00	544+00	LT				0.095														
EL-1	67	539+00	544+00	LT					0.095													
EL-2	67	539+00	544+00	LT					0.095													
P-10	67	539+00	544+00	LT															500			
SUBTOTAL SHEET 34					121	2	2		0.690	1.374				78				9	10	10	3705	700

MAINTENANCE OF TRAFFIC SUBSUMMARY

CALCULATED
ARK
CHECKED
EJT

**LAK-US-20-24.99
PART 2**

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\DOT\Sheets\09270_MS003.dgn Sheet 7/15/2022 2:50:22 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION		SIDE	611	611	611	614	614	614	614	614	614	614	614	614	614	614	614	615	622	
					12" CONDUIT, TYPE B, AS PER PLAN FT	CATCH BASIN, NO. 6, AS PER PLAN EACH	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN EACH	WORK ZONE LANE LINE, CLASS I, 642 PAINT MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT MILE	WORK ZONE EDGE LINE, CLASS I, 6" 642 PAINT MILE	WORK ZONE CHANNELIZING LINE, CLASS I, 8" 642 PAINT FT	WORK ZONE DOTTED LINE, CLASS I, 4" 642 PAINT FT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT FT	WORK ZONE STOP LINE, CLASS I, 642 PAINT FT	WORK ZONE CROSSWALK LINE, CLASS I, 24" 642 PAINT FT	WORK ZONE ARROW, CLASS I, 642 PAINT EACH	WORK ZONE ISLAND MARKING, CLASS I SF	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS (BIDIRECTIONAL) EACH	BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL) EACH	OBJECT MARKER, TWO WAY EACH	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN SY	PORTABLE BARRIER, UNANCHORED FT
CL-1	68	544+00	TO	549+00																		
EL-1	68	544+00		549+00					0.095													
EL-2	68	544+00		549+00						0.095												
SL-1	68	0+52														32						
D-4	68	546+50		546+97																		
P-11	68	544+00		549+00	60	1	1														500	
CL-1	69	549+00		554+00					0.095													
EL-1	69	549+00		554+00						0.095												
EL-2	69	549+00		554+00						0.095												
SL-1	69	549+61														10						
SL-2	69	550+88														11						
SL-3	69	10+68														14						
SL-4	69	9+52														13						
SL-5	69	0+52														24						
D-5	69	549+47		550+05	68	1	1															
D-6	69	551+63		551+88	35	1	1															
P12	69	549+00		554+00																	440	
CL-1	70	554+00		559+00					0.095													
EL-1	70	554+00		559+00						0.095												
EL-2	70	554+00		559+00						0.095												
SL-1	70	555+22														10						
SL-2	70	556+69														11						
SL-3	70	10+54														30						
SL-4	70	9+52														22						
D-7	70	554+50		554+51	13	1	1															
D-8	70	556+98		557+17	48	1	1															
P-13	70	554+00		559+00																	500	
CL-1	71	559+00		564+00					0.095													
EL-1	71	559+00		564+00						0.095												
EL-2	71	559+00		564+00						0.095												
D-9	71	562+37		562+50	28	1	1															
P-14	71	559+00		564+00																	333	
CL-1	72	564+00		569+00					0.095													
CL-2	72	98+40		99+23					0.016													
EL-1	72	564+00		564+99						0.095												
EL-2	72	564+00		564+99						0.095												
SL-1	72	564+74														10						
SL-2	72	566+67														11						
SL-3	72	100+50														32						
SL-4	72	100+70														11						
SL-5	72	99+43														21						
SL-6	72	99+23														11						
CH-1	72	98+60		99+43																		
LA-1	72	98+72																				
LA-2	72	99+13																				
SUBTOTAL SHEET 35					252	6	6		0.491	0.950	83					273						1773

MAINTENANCE OF TRAFFIC SUBSUMMARY

CALCULATED
ARK
CHECKED
EJT

**LAK-US-20-24.99
PART 2**

REF NO.	SHEET NO.	STATION TO STATION		SIDE	611	611	611	614	614	614	614	614	614	614	614	614	614	614	615	622
					12" CONDUIT, TYPE B, AS PER PLAN FT	CATCH BASIN, NO. 6, AS PER PLAN EACH	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN EACH	WORK ZONE LANE LINE, CLASS I, 642 PAINT MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT MILE	WORK ZONE EDGE LINE, CLASS I, 6" 642 PAINT MILE	WORK ZONE CHANNELIZING LINE, CLASS I, 8" 642 PAINT FT	WORK ZONE DOTTED LINE, CLASS I, 4" 642 PAINT FT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT FT	WORK ZONE STOP LINE, CLASS I, 642 PAINT FT	WORK ZONE CROSSWALK LINE, CLASS I, 24" 642 PAINT FT	WORK ZONE ARROW, CLASS I, 642 PAINT EACH	WORK ZONE ISLAND MARKING, CLASS I SF	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS (BIDIRECTIONAL) EACH	BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL) EACH	OBJECT MARKER, TWO WAY EACH
D-10	72	564+88	TO	565+05	RT															
P-15	72	564+00		569+00	LT	28														130
CL-1	73	569+00		574+00	LT				0.095											
CL-2	73	573+00		574+00	LT				0.019											
EL-1	73	569+00		574+00	LT					0.095										
EL-2	73	569+00		574+00	LT					0.095										
D-11	73	569+00		569+29	LT&RT	45	1	1												
D-12	73	571+50		571+50	LT&RT	5	1	1												
P-16	73	569+00		574+00	LT														380	
CL-1	74	574+00		579+00	C/L				0.095											
CL-2	74	574+00		579+00	LT				0.095											
EL-1	74	574+00		577+78	RT					0.072										
EL-2	74	574+00		577+06	LT					0.058										
LL-1	74	577+78		579+00	RT			0.023												
P-17	74	574+00		577+06.4	LT														138	
CL-1	75	579+00		583+49	C/L				0.085											
CL-2	75	579+00		581+59	LT				0.049											
LL-1	75	579+00		583+49	RT			0.085												
LL-2	75	581+59		583+49	LT			0.036												
DL-1	75	579+28		581+59	LT							231								
BURNS ROAD																				
CL-1	82	516+23		517+72	RT				0.028											
EL-1	82	516+38		517+87	RT				0.028											
HUBBARD ROAD, PHASE 1A																				
CL-1	86	89+00		92+50	C/L				0.066											
EL-1	86	89+00		92+50	LT					0.066										
EL-2	86	89+00		92+50	RT					0.066										
CL-1	86	92+50		97+50	RT				0.095											
EL-1	86	92+50		97+50	RT					0.095										
EL-2	86	92+50		97+50	RT					0.095										
SUBTOTAL SHEET 36						78	2	2	0.144	0.655	0.642		231						648	

MAINTENANCE OF TRAFFIC SUBSUMMARY

CALCULATED
ARK
CHECKED
EJT

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\DOT\Sheets\09270_MS005.dgn Sheet 7/15/2022 2:50:23 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION		SIDE	611	611	611	614	614	614	614	614	614	614	614	614	614	614	615	622		
					12" CONDUIT, TYPE B, AS PER PLAN FT	CATCH BASIN, NO. 6, AS PER PLAN EACH	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN EACH	WORK ZONE LANE LINE, CLASS I, 642 PAINT MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT MILE	WORK ZONE EDGE LINE, CLASS I, 6" 642 PAINT MILE	WORK ZONE CHANNELIZING LINE, CLASS I, 8" 642 PAINT FT	WORK ZONE DOTTED LINE, CLASS I, 4" 642 PAINT FT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT FT	WORK ZONE STOP LINE, CLASS I, 642 PAINT FT	WORK ZONE CROSSWALK LINE, CLASS I, 24" 642 PAINT FT	WORK ZONE ARROW, CLASS I, 642 PAINT EACH	WORK ZONE ISLAND MARKING, CLASS I SF	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS (BIDIRECTIONAL) EACH	BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL) EACH	OBJECT MARKER, TWO WAY EACH	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN SY	PORTABLE BARRIER, UNANCHORED FT
CL-1	87	97+50	TO	102+50						0.095												
EL-1	87	97+50		102+50							0.095											
EL-2	87	97+50		102+50							0.095											
SL-1	87	99+00													10							
SL-2	87	101+00													10							
DL-1	87	99+00		101+00										200								
DL-2	87	100+08		101+00										92								
CL-1	88	102+50		107+50						0.095												
EL-1	88	102+50		107+50							0.095											
EL-2	88	102+50		107+50							0.095											
HUBBARD ROAD, PHASE 1B																						
CL-1	89	96+19		97+50						0.025												
EL-1	89	96+19		97+50							0.025											
EL-2	89	96+19		97+50							0.025											
CL-1	90	97+50		102+50						0.095												
EL-1	90	97+50		102+50							0.095											
EL-2	90	97+50		102+50							0.095											
SL-1	90	99+50													10							
SL-2	90	101+00													10							
CL-1	91	102+50		104+81						0.044												
EL-1	91	102+50		104+81							0.044											
EL-2	91	102+50		104+81							0.044											
MAJOR ACCESS DRIVES																						
EL-1	92	549+76		551+03							0.024											
EL-2	92	555+37		556+84							0.028											
SUBTOTAL SHEET 37												0.354	0.760		292		40					
SUBTOTALS FROM SHEET 33					10	1	1	0.218	0.760	1.005	254	495	251	57	274	5	51	3	8	8	594	486
SUBTOTALS FROM SHEET 34					121	2	2		0.690	1.374				78			9	10	10		3705	700
SUBTOTALS FROM SHEET 35					252	6	6		0.491	0.950	83			273		2					1773	
SUBTOTALS FROM SHEET 36					78	2	2	0.144	0.655	0.642		231									648	
TOTAL					461	11	11	0.36	2.95	4.73	337	1018	251	448	274	7	51	12	18	18	6720	1186
TOTALS CARRIED TO GENERAL SUMMARY					461	11	11	0.36	2.95	4.73	337	1018	251	448	274	7	51	12	18	18	6720	1186

MAINTENANCE OF TRAFFIC SUBSUMMARY

LAK-US-20-24.99 PART 2

CALCULATED
ARK
CHECKED
EJT

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\DOT\Sheets\09270_MS010.dgn Sheet 7/15/2022 2:50:28 PM troyer

REF NO.	SHEET NO.	STATION TO STATION		SIDE	606	606	606	611	611	611	611	614	614	614	614	614	614	614	614	614	614	614	615	622	
					GUARDRAIL, TYPE MGS FT	ANCHOR ASSEMBLY, TYPE E EACH	ANCHOR ASSEMBLY, TYPE T EACH	12" CONDUIT, TYPE B, AS PER PLAN FT	CATCH BASIN, NO. 6, AS PER PLAN EACH	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN EACH	MANHOLE ADJUSTED TO GRADE EACH	WORK ZONE LANE LINE, CLASS I, 642 PAINT MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT MILE	WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT MILE	WORK ZONE CHANNELIZING LINE, CLASS I, 8", 642 PAINT FT	WORK ZONE DOTTED LINE, CLASS I, 4", 642 PAINT FT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT FT	WORK ZONE STOP LINE, CLASS I, 642 PAINT FT	WORK ZONE CROSSWALK LINE, CLASS I, 24", 642 PAINT FT	WORK ZONE ARROW, CLASS I, 642 PAINT EACH	WORK ZONE ISLAND MARKING, CLASS I SF	WORK ZONE IMPACT ATTENUATOR 24" WIDE HAZARDS (BIDIRECTIONAL) EACH	BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL) EACH	BARRIER REFLECTOR, TYPE 2 (BIDIRECTIONAL) EACH	OBJECT MARKER, TWO WAY EACH
		PHASE 3	TO																						
CL-1	142	390+53		394+00									0.066												
LL-1	142	390+53		394+00								0.066													
LL-2	142	390+53		394+00								0.066													
EL-1	142	392+53		394+00										0.028											
DL-1	142	392+53		394+00												147									
CL-1	143	394+00		399+00									0.095												
LL-1	143	394+00		399+00								0.095													
EL-1	143	394+00		399+00										0.095											
EL-2	143	398+23		399+00										0.015											
DL-1	143	394+00		397+03												303									
CL-1	144	399+00		404+00									0.095												
CL-2	144	399+00		403+18									0.079												
EL-1	144	399+00		404+00										0.095											
EL-2	144	399+00		404+00										0.095											
CH-1	144	403+43		404+00											57										
CH-2	144	403+43		404+00											57										
LA-1	144	403+54																				1			
LA-2	144	403+54																				1			
TL-1	144	399+00		403+18													106								
CL-1	145	404+00		409+00									0.095												
CL-2	145	407+55		409+00									0.027												
EL-1	145	404+00		409+00										0.095											
EL-2	145	404+00		409+00										0.095											
CH-1	145	404+00		404+88											88										
CH-2	145	404+00		404+88											88										
CH-3	145	406+59		407+30											71										
SL-1	145	404+88																							
SL-2	145	406+59																							
SL-3	145	406+00																							
SL-4	145	406+00																							
CW-1	145	405+26																							
CW-2	145	406+50																							
CW-3	145	406+00																							
CW-4	145	406+00																							
LA-1	145	404+16																							
LA-2	145	404+78																							
LA-3	145	404+16																							
LA-4	145	404+78																							
LA-5	145	406+69																							
LA-6	145	407+17																							
TL-1	145	407+55		409+00																					
PB-1	145	406+84		409+00																					
CL-1	146	409+00		414+00									0.095												
CL-2	146	409+00		411+20									0.042												
EL-1	146	409+00		414+00										0.095											
EL-2	146	409+00		414+00										0.095											
TL-1	146	409+00		411+20																					
D-1	146	410+71					7	1	1																
P-1	146	409+04.9		414+00																			444		
PB-2	146	409+00		414+00																			5	310	
SUBTOTAL SHEET 42								7	1	1		0.227	0.594	0.708	361	450	200	112	621	8	5	8	8	444	510

MAINTENANCE OF TRAFFIC SUBSUMMARY	LAK-US-20-24.99
PART 2	PART 2
CALCULATED ARK CHECKED EJT	42 697

H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_M501.dgn Sheet 7/15/2022 2:50:29 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION	SIDE	606	606	606	611	611	611	611	614	614	614	614	614	614	614	614	614	614	614	614	615	622		
				GUARDRAIL, TYPE MGS	ANCHOR ASSEMBLY, TYPE E	ANCHOR ASSEMBLY, TYPE T	12" CONDUIT, TYPE B, AS PER PLAN	CATCH BASIN, NO. 6, AS PER PLAN	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN	MANHOLE ADJUSTED TO GRADE	WORK ZONE LANE LINE, CLASS I, 642 PAINT	WORK ZONE CENTER LINE, CLASS I, 642 PAINT	WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT	WORK ZONE CHANNELIZING LINE, CLASS I, 8", 642 PAINT	WORK ZONE DOTTED LINE, CLASS I, 4", 642 PAINT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT	WORK ZONE STOP LINE, CLASS I, 642 PAINT	WORK ZONE CROSSWALK LINE, CLASS I, 24", 642 PAINT	WORK ZONE ARROW, CLASS I, 642 PAINT	WORK ZONE ISLAND MARKING, CLASS I	WORK ZONE IMPACT ATTENUATOR 24" WIDE HAZARDS (BIDIRECTIONAL)	BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL)	BARRIER REFLECTOR, TYPE 2 (BIDIRECTIONAL)	OBJECT MARKER, TWO WAY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN	PORTABLE BARRIER, UNANCHORED
TO				FT	EACH	EACH	FT	EACH	EACH	EACH	MILE	MILE	MILE	FT	FT	FT	FT	FT	EACH	SF	EACH	EACH	EACH	EACH	SY	FT
CL-1	147	414+00	419+00	LT							0.095															
EL-1	147	414+00	419+00	LT										0.095												
EL-2	147	414+00	419+00	LT										0.095												
D-2	147	415+50	415+50	LT			10	1	1																	
D-3	147	418+00	418+04	LT			40	1	1																	
P-2	147	414+00	419+00	LT																				667		
PB-3	147	414+00	419+00	LT															4	5	4	5		667	310	
GR-1	147	415+60	419+00	LT	290	1																4	4			
CL-1	148	419+00	424+00	LT							0.095															
EL-1	148	419+00	424+00	LT										0.095												
EL-2	148	419+00	424+00	LT										0.095												
D-4	148	420+40	420+50	LT&RT			37	1	1																	
D-5	148	423+00	423+25	LT&RT			40	1	1																	
P-3	148	419+00	424+00	LT																				667		
PB-4	148	419+00	424+00	LT															4	4	1	4		667	255	
GR-2	148	419+00	419+40	LT	35	1																1	1			
CL-1	149	424+00	429+00	LT							0.095															
CL-2	149	426+00	429+00	LT							0.057															
EL-1	149	424+00	429+00	RT & LT										0.095												
EL-2	149	424+00	429+00	LT										0.095												
TL-1	149	426+00	429+00	LT												36										
D-6	149	425+25	425+28	LT			12	1	1																	
D-7	149	429+00	429+00	LT			5	1	1																	
P-4	149	424+00	429+00	LT																				444		
PB-5	149	424+00	429+00	LT&RT															2	5		5		444	400	
CL-1	150	429+00	434+00	LT							0.095															
CL-2	150	429+00	431+12	LT							0.04															
EL-1	150	429+00	434+00	RT										0.095												
EL-2	150	429+00	434+00	LT										0.095												
TL-1	150	429+00	431+12	LT																						
CH-1	150	431+37	433+10	C/L											173											
CH-2	150	431+37	433+10	RT											173											
LA-1	150	431+68		LT																		1				
LA-2	150	432+34		LT																		1				
LA-3	150	433+00		LT																		1				
LA-4	150	431+68		RT																		1				
LA-5	150	432+34		RT																		1				
LA-6	150	433+00		RT																		1				
SL-1	150	433+10		LT																						
SL-2	150	10+50		LT											33											
SL-3	150	9+50		RT											10											
CW-1	150	433+23		RT											10											
P-5	150	429+00	430+24.8	LT																				42		
PB-6	150	429+00	430+50	RT																			2		134	
SUBTOTAL SHEET 43					325	1	1	144	6	6		0.477	0.760	346		116	53	119	6	11	16	10	21	1820	1099	

MAINTENANCE OF TRAFFIC SUBSUMMARY

LAK-US-20-24.99 PART 2

CALCULATED
ARK
CHECKED
EJT

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MS012.dgn Sheet 7/15/2022 2:50:29 PM troyer

REF NO.	SHEET NO.	STATION TO STATION		SIDE	STATION TO STATION																							
					606	606	606	611	611	611	611	614	614	614	614	614	614	614	614	614	614	614	614	615	622			
			TO		GUARDRAIL, TYPE MGS	ANCHOR ASSEMBLY, TYPE E	ANCHOR ASSEMBLY, TYPE T	12" CONDUIT, TYPE B, AS PER PLAN	CATCH BASIN, NO. 6, AS PER PLAN	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN	MANHOLE ADJUSTED TO GRADE	WORK ZONE LANE LINE, CLASS I, 642 PAINT	WORK ZONE CENTER LINE, CLASS I, 642 PAINT	WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT	WORK ZONE CHANNELIZING LINE, CLASS I, 8", 642 PAINT	WORK ZONE DOTTED LINE, CLASS I, 4", 642 PAINT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT	WORK ZONE STOP LINE, CLASS I, 642 PAINT	WORK ZONE CROSSWALK LINE, CLASS I, 24", 642 PAINT	WORK ZONE ARROW, CLASS I, 642 PAINT	WORK ZONE ISLAND MARKING, CLASS I	WORK ZONE IMPACT ATTENUATOR 24" WIDE HAZARDS (BIDIRECTIONAL)	BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL)	BARRIER REFLECTOR, TYPE 2 (BIDIRECTIONAL)	OBJECT MARKER, TWO WAY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN	PORTABLE BARRIER, UNANCHORED	
					FT	EACH	EACH	FT	EACH	EACH	EACH	MILE	MILE	MILE	FT	FT	FT	FT	FT	EACH	SF	EACH	EACH	EACH	EACH	SY	FT	
CL-1	151	434+00	439+00	C/L & LT								0.095																
CL-2	151	435+53	439+00	LT								0.066																
EL-1	151	434+00	439+00	RT & LT										0.095														
EL-2	151	434+00	439+00	LT										0.095														
CH-1	151	434+50	435+28	LT											78													
TL-1	151	435+53	439+00	LT													83											
LA-1	151	434+60		LT																	1							
LA-2	151	435+08		LT																	1							
SL-1	151	434+50		LT															22									
D-8	151	438+00	438+00	LT					1																			
P-6	151	436+55	439+00	LT																						109		
PB-7	151	434+74	439+00	RT																		1	4		4		310	
CL-1	152	439+00	444+00	LT								0.095																
CL-2	152	439+00	440+15	LT								0.022																
EL-1	152	439+00	444+00	LT										0.095														
EL-2	152	439+00	444+00	LT										0.095														
TL-1	152	439+00	440+15	LT													8											
D-9	152	439+59	439+59	LT				10	1	1																		
D-10	152	442+00	442+10	LT&RT				40	1	1																		
P-7	152	439+00	444+00	LT																							596	
PB-8	152	439+00	444+00	LT																		2	5		5		410	
CL-1	153	444+00	449+00	LT								0.095																
EL-1	153	444+00	449+00	LT										0.095														
EL-2	153	444+00	449+00	LT										0.095														
D-11	153	444+38	444+39	LT				15	1	1																		
D-12	153	446+16	446+49	LT/RT				55	1	1																		
P-8	153	444+00	449+00	LT																							667	
PB-9	153	444+00	449+00	LT																			6		6		500	
CL-1	154	449+00	454+00	LT								0.095																
EL-1	154	449+00	454+00	LT										0.095														
EL-2	154	449+00	454+00	LT										0.095														
D-13	154	449+00	449+00	LT				10	1	1																		
D-14	154	451+40	451+50	LT&RT				43	1	1																		
P-9	154	454+00	459+00	LT																							667	
PB-10	154	454+00	459+00	LT																							410	
CL-1	155	454+00	459+00	LT								0.095																
EL-1	155	454+00	459+00	LT										0.095														
EL-2	155	454+00	459+00	LT										0.095														
D-15	155	456+88	457+00	LT&RT				35	1	1	1																	
P-10	155	454+00	459+00	LT																							667	
PB-11	155	454+00	459+00	LT																			2	5		5		400
CL-1	156	459+00	464+00	LT								0.095																
EL-1	156	459+00	464+00	LT										0.095														
EL-2	156	459+00	464+00	LT										0.095														
SL-1	156	463+46		LT																								
D-16	156	459+41	459+50	LT&RT				10	1	1	1																	
D-17	156	461+93	462+90	LT&RT				105	1	1	1																	
P-11	156	459+00	464+00	LT																							573	
PB-12	156	459+00	464+00	LT																							400	
SUBTOTAL SHEET 44								323	9	10	3		0.658	1.140	78			91	22		2		9	30		30	3279	2430

LAK-US-20-24.99 PART 2	MAINTENANCE OF TRAFFIC SUBSUMMARY	CALCULATED ARK CHECKED EJT
44 697		

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MS013.dgn Sheet 7/15/2022 2:50:30 PM troyer

REF NO.	SHEET NO.	STATION TO STATION	SIDE	606	606	606	611	611	611	611	614	614	614	614	614	614	614	614	614	614	614	614	615	622		
				GUARDRAIL, TYPE MGS	ANCHOR ASSEMBLY, TYPE E	ANCHOR ASSEMBLY, TYPE T	12" CONDUIT, TYPE B, AS PER PLAN	CATCH BASIN, NO. 6, AS PER PLAN	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN	MANHOLE ADJUSTED TO GRADE	WORK ZONE LANE LINE, CLASS I, 642 PAINT	WORK ZONE CENTER LINE, CLASS I, 642 PAINT	WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT	WORK ZONE CHANNELIZING LINE, CLASS I, 8", 642 PAINT	WORK ZONE DOTTED LINE, CLASS I, 4", 642 PAINT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT	WORK ZONE STOP LINE, CLASS I, 642 PAINT	WORK ZONE CROSSWALK LINE, CLASS I, 24", 642 PAINT	WORK ZONE ARROW, CLASS I, 642 PAINT	WORK ZONE ISLAND MARKING, CLASS I	WORK ZONE IMPACT ATTENUATOR 24" WIDE HAZARDS (BIDIRECTIONAL)	BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL)	BARRIER REFLECTOR, TYPE 2 (BIDIRECTIONAL)	OBJECT MARKER, TWO WAY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN	PORTABLE BARRIER, UNANCHORED
				FT	EACH	EACH	FT	EACH	EACH	EACH	MILE	MILE	MILE	FT	FT	FT	FT	FT	FT	EACH	SF	EACH	EACH	EACH	SY	FT
			TO																							
CL-1	157	464+00	469+00	LT							0.095															
EL-1	157	464+00	469+00	LT									0.095													
EL-2	157	464+00	469+00	LT									0.095													
D-18	157	464+70	464+75	LT&RT			44	1	1	1																
D-19	157	467+44	467+59	L&RT			34	1	1	1																
P-12	157	464+00	469+00	LT																			667			
PB-13	157	464+00	469+00	LT																		6		6	410	
CL-1	158	469+00	474+00	LT							0.095															
EL-1	158	469+00	474+00	LT									0.095													
EL-2	158	469+00	474+00	LT									0.095													
D-20	158	469+95	470+00	LT&RT			10	1	1	1																
D-21	158	472+45	472+50	LT&RT			10	1	1	1																
P-13	158	469+00	474+00	LT																			667			
PB-14	158	469+00	474+00	LT																		2	5	5	410	
CL-1	159	474+00	479+00	LT							0.095															
EL-1	159	474+00	479+00	LT									0.095													
EL-2	159	474+00	479+00	LT									0.095													
D-22	159	474+95	475+13	LT&RT			37	1	1	1																
D-23	159	477+50	477+80	LT&RT			45	1	1	1																
P-14	159	474+00	479+00	LT																			667			
PB-15	159	474+00	479+00	LT																		2	5	5	400	
CL-1	160	479+00	484+00	LT							0.095															
EL-1	160	479+00	484+00	LT									0.095													
EL-2	160	479+00	484+00	LT									0.095													
D-24	160	480+53	481+00	LT&RT			70	1	1	1																
P-15	160	479+00	484+00	LT																			667			
PB-16	160	479+00	484+00	LT																		2	5	5	400	
CL-1	161	484+00	489+00	LT							0.095															
CL-2	161	486+40	489+00	LT							0.049															
EL-1	161	484+00	489+00	LT									0.095													
EL-2	161	484+00	489+00	LT									0.095													
TL-1	161	486+40	489+00	LT											36											
D-25	161	485+18		LT			10	1	1																	
P-16	161	484+00	488+88.6	LT&RT																			436			
PB-17	161	484+00	489+00	LT&RT																		6		6	500	
CL-1	162	489+00	494+00	LT							0.095															
CL-2	162	489+00	490+05	LT							0.02															
EL-1	162	489+00	494+00	LT									0.095													
EL-2	162	489+00	494+00	RT									0.095													
LL-1	162	490+30	494+00	RT									0.070													
CH-1	162	490+30	493+74	LT											344											
TL-1	162	489+00	490+05	LT																						
SL-1	162	493+74		RT																						
LA-1	162	490+44		LT																						
LA-2	162	491+08		LT																						
LA-3	162	491+72		LT																						
LA-4	162	492+36		LT																						
LA-5	162	493+00		LT																						
LA-6	162	493+64		LT																						
PB-18	162	489+00	489+35	RT																		1	2	2	20	
SUBTOTAL SHEET 45							260	8	8	7		0.639	1.210	344		94	33		6	7	29		29	3104	2140	

MAINTENANCE OF TRAFFIC SUBSUMMARY	LAK-US-20-24.99 PART 2	CALCULATED ARK CHECKED EJT
------------------------------------------	-----------------------------------------	-------------------------------------

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MS04.dgn Sheet 7/15/2022 2:50:31PM troyer

REF NO.	SHEET NO.	STATION TO STATION		SIDE	606	606	606	611	611	611	611	614	614	614	614	614	614	614	614	614	614	614	615	622			
					GUARDRAIL, TYPE MGS FT	ANCHOR ASSEMBLY, TYPE E EACH	ANCHOR ASSEMBLY, TYPE T EACH	12" CONDUIT, TYPE B, AS PER PLAN FT	CATCH BASIN, NO. 6, AS PER PLAN EACH	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN EACH	MANHOLE ADJUSTED TO GRADE EACH	WORK ZONE LANE LINE, CLASS I, 642 PAINT MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT MILE	WORK ZONE EDGE LINE, CLASS I, 6" 642 PAINT MILE	WORK ZONE CHANNELIZING LINE, CLASS I, 8" 642 PAINT FT	WORK ZONE DOTTED LINE, CLASS I, 4" 642 PAINT FT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT FT	WORK ZONE STOP LINE, CLASS I, 642 PAINT FT	WORK ZONE CROSSWALK LINE, CLASS I, 24" 642 PAINT FT	WORK ZONE ARROW, CLASS I, 642 PAINT EACH	WORK ZONE ISLAND MARKING, CLASS I SF	WORK ZONE IMPACT ATTENUATOR 24" WIDE HAZARDS (BIDIRECTIONAL) EACH	BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL) EACH	BARRIER REFLECTOR, TYPE 2 (BIDIRECTIONAL) EACH	OBJECT MARKER, TWO WAY EACH	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN SY	PORTABLE BARRIER, UNANCHORED FT
			TO																								
CL-1	163	495+70		499+00									0.063														
CL-2	163	494+00		499+00									0.095														
EL-1	163	494+00		499+00										0.095													
EL-2	163	494+00		499+00										0.095													
LL-1	163	494+00		499+00								0.095															
IM-1	163	495+70																			51						
SL-1	163	495+70																									
SL-2	163	10+77																									
SL-3	163	11+13																									
CW-1	163	495+50																									
CW-2	163	10+50																									
CL-1	164	499+00		504+00									0.095														
CL-2	164	499+00		504+00									0.095														
EL-1	164	499+00		500+35										0.045													
EL-2	164	499+00		500+20										0.042													
LL-1	164	499+00		504+00								0.095															
CL-1	165	504+00		509+00									0.095														
CL-2	165	504+00		509+00									0.095														
EL-1	165	504+20.5		504+39										0.004													
EL-2	165	504+70		505+18										0.009													
EL-3	165	504+20.5		505+18										0.018													
LL-1	165	504+00		509+00								0.095															
CH-1	165	505+18		507+45											227												
SL-1	165	504+20																									
DL-1	165	508+04		509+00												96											
LA-1	165	505+68																									
LA-2	165	506+34																									
LA-3	165	507+00																									
CL-1	166	509+00		514+00									0.095														
CL-2	166	509+00		513+44									0.084														
LL-1	166	509+00		514+00								0.095															
LL-2	166	513+44		514+00								0.011															
DL-1	166	509+00		513+44												444											
CL-1	167	514+00		515+44									0.027														
LL-1	167	514+00		515+44								0.027															
LL-2	167	514+00		515+44								0.027															
SUBTOTAL SHEET 46												0.445	0.744	0.308	227	540		58	276	3	51						
TOTALS FROM SHEET 42								7	1	1		0.227	0.594	0.708	361	450	200	112	621	8	5	8	8	444	510		
TOTALS FROM SHEET 43					325	1	1	144	6	6			0.477	0.760	346		116	53	119	6	11	16	10	21	1099		
TOTALS FROM SHEET 44								323	9	10	3		0.658	1.140	78		91	22		2	9	30	30	3279	2430		
TOTALS FROM SHEET 45								260	8	8	7		0.639	1.210	344		94	33		6	7	29	29	3104	2140		
TOTAL					325	1	1	734	24	25	10	0.672	3.112	4.126	1356	990	501	278	1016	25	51	32	83	10	88	8647	6179
TOTALS CARRIED TO GENERAL SUMMARY					325	1	1	734	24	25	10	0.67	3.11	4.13	1356	990	501	278	1016	25	51	32	83	10	88	8647	6179

MAINTENANCE OF TRAFFIC SUBSUMMARY

CALCULATED
ARK
CHECKED
EJT

**LAK-US-20-24.99
PART 2**

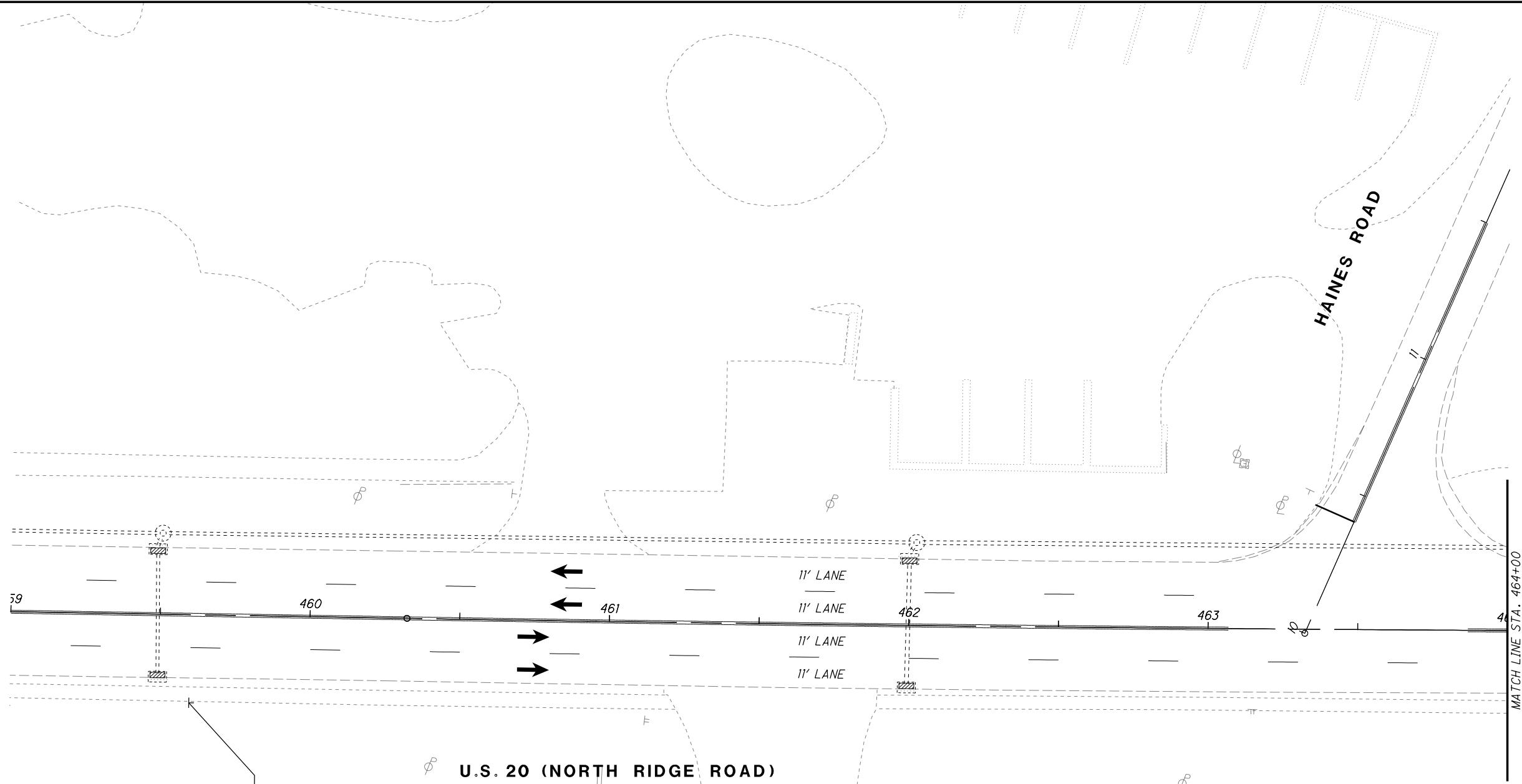
H:\2018\80743\DOT\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP083E.dgn Sheet 7/15/2022 2:50:37 PM troyer

CALCULATED
 EJT
 CHECKED
 WDB

0 20 40
 HORIZONTAL
 SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 459+00 TO STA. 464+00

LAK-US-20-24.99
PART 2



LEGEND

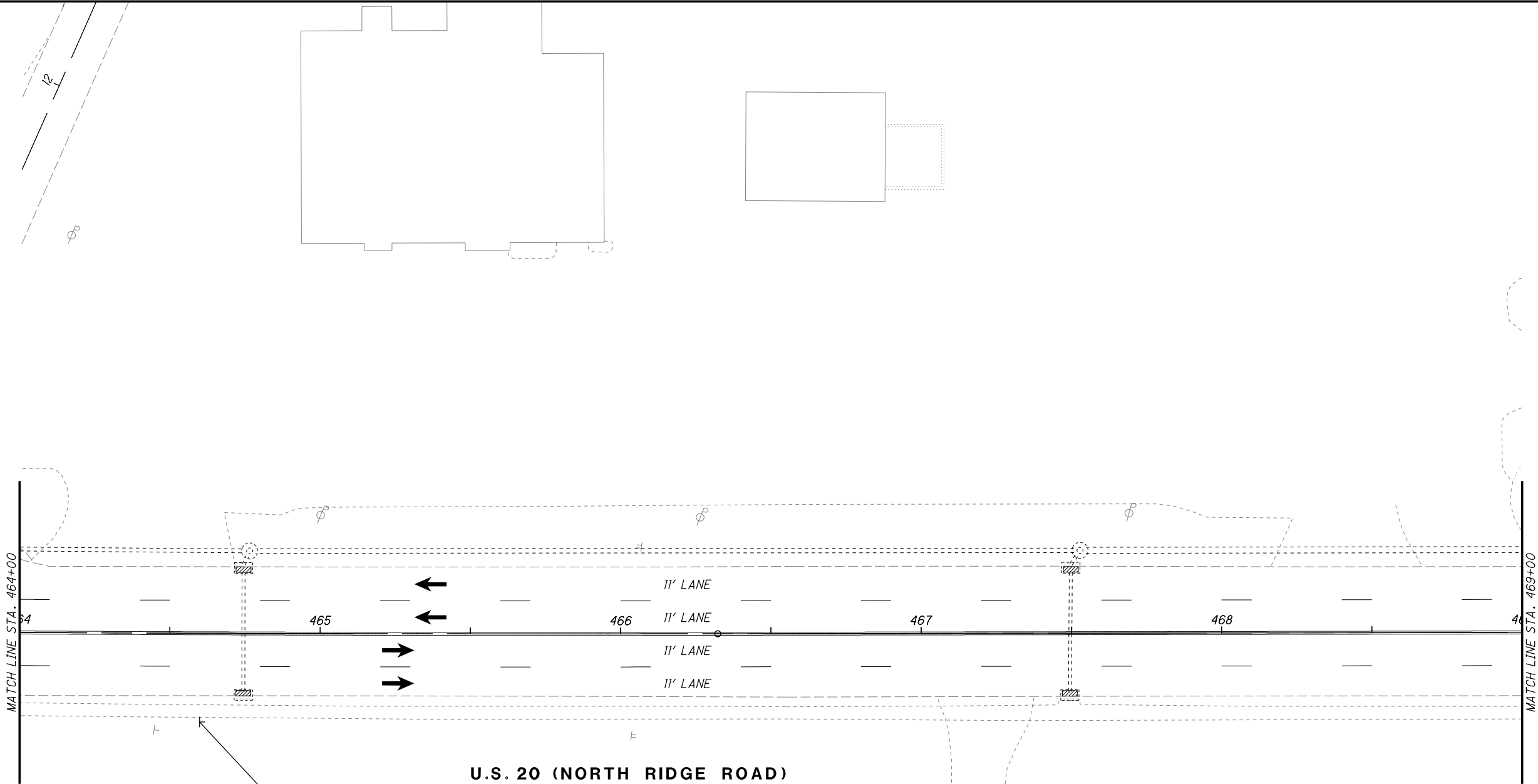
- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW




H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP084.dgn Sheet 7/15/2022 2:50:38 PM Troyer















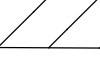

U.S. 20 (NORTH RIDGE ROAD)

W9-3R-36
36"x36"
STA. 464+60, RT.

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

LEGEND

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
-  TRAFFIC FLOW

CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET



**MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 464+00 TO STA. 469+00**

**LAK-US-20-24.99
PART 2**

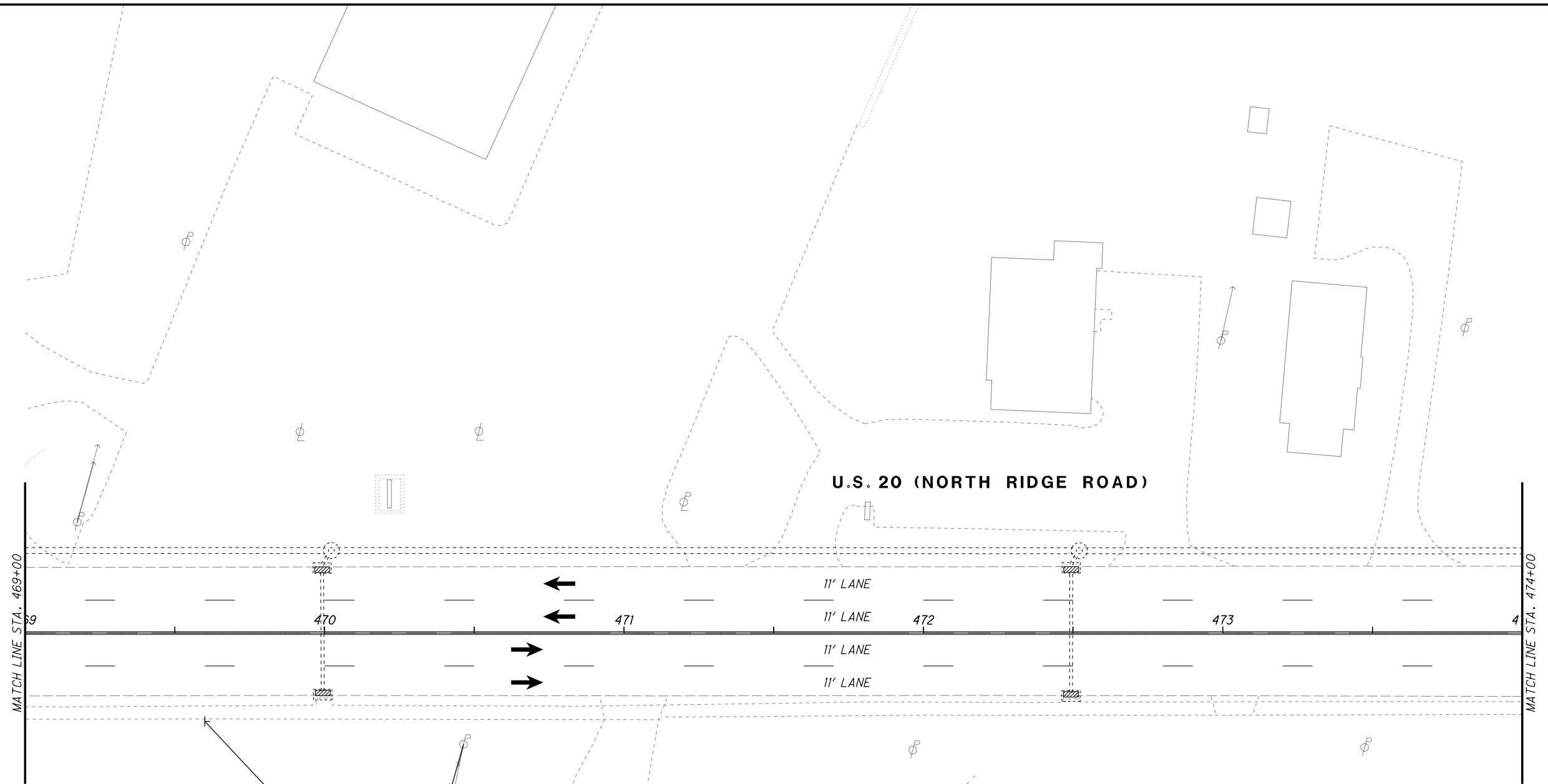
H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP084A.dgn Sheet 7/15/2022 2:50:38 PM troyer

CALCULATED
EJT
CHECKED
WDB

0 20 40
10
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 469+00 TO STA. 474+00

LAK-US-20-24.99
PART 2



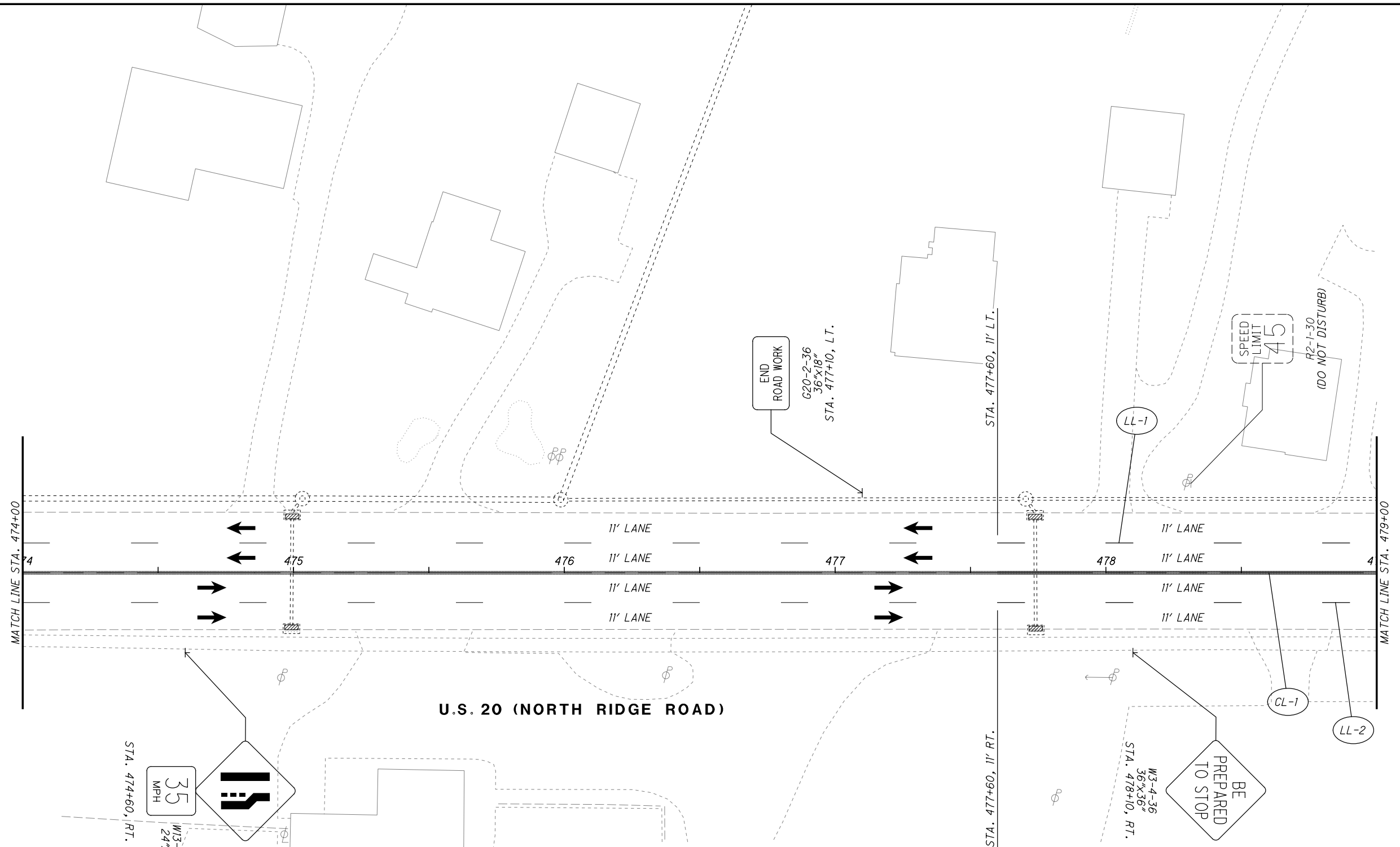
LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN




- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT








LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW






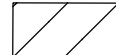



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

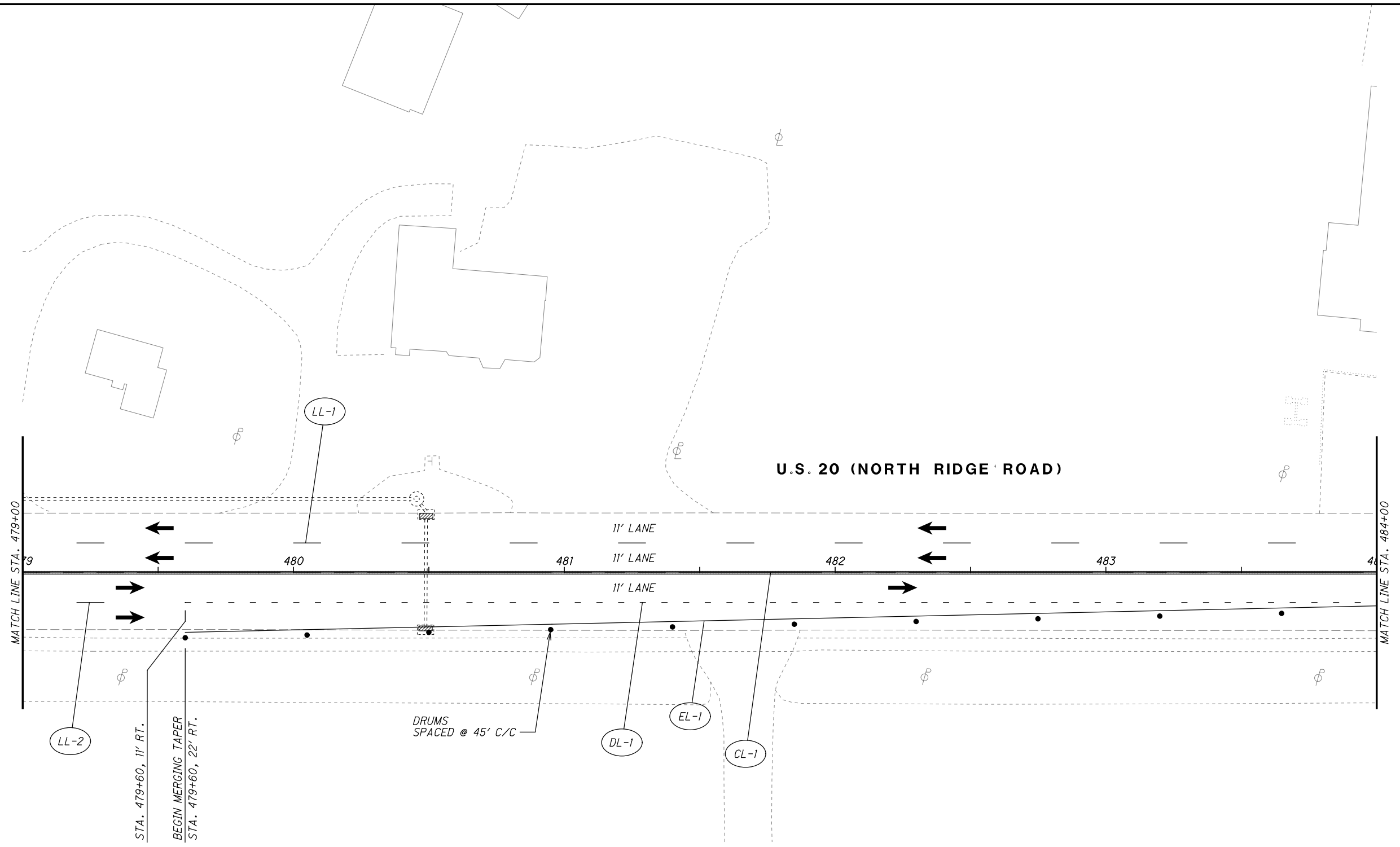
-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
-  TRAFFIC FLOW



CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 479+00 TO STA. 484+00

LAK-US-20-24.99
PART 2



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW

LEGEND

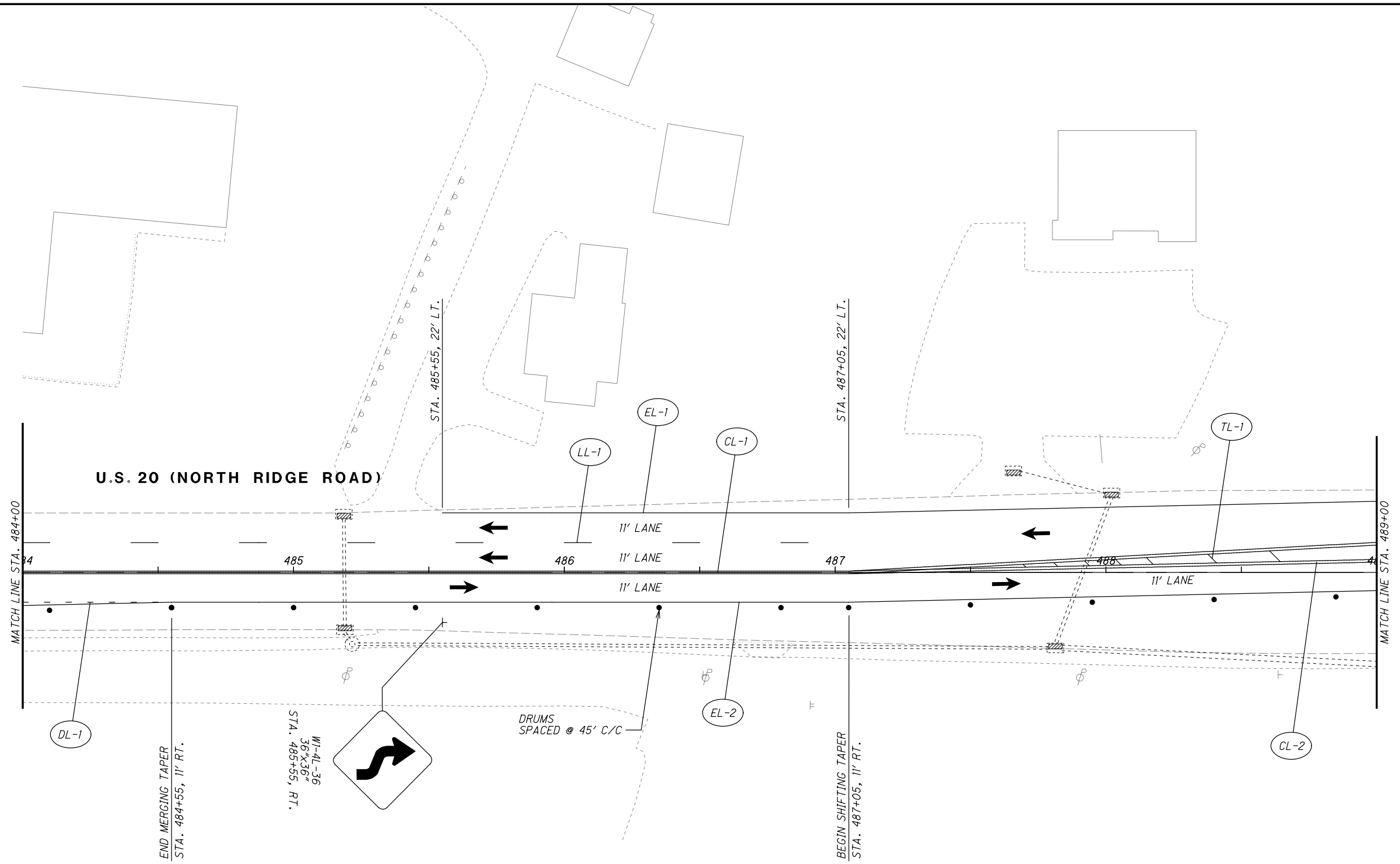
U.S. 20 (NORTH RIDGE ROAD)

DRUMS
SPACED @ 45' C/C




STA. 479+60, 11' RT.
BEGIN MERGING TAPER
STA. 479+60, 22' RT.








MATCH LINE STA. 479+00

MATCH LINE STA. 484+00






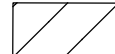



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
-  TRAFFIC FLOW

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP084E.dgn Sheet 7/15/2022 2:50:40 PM troyer

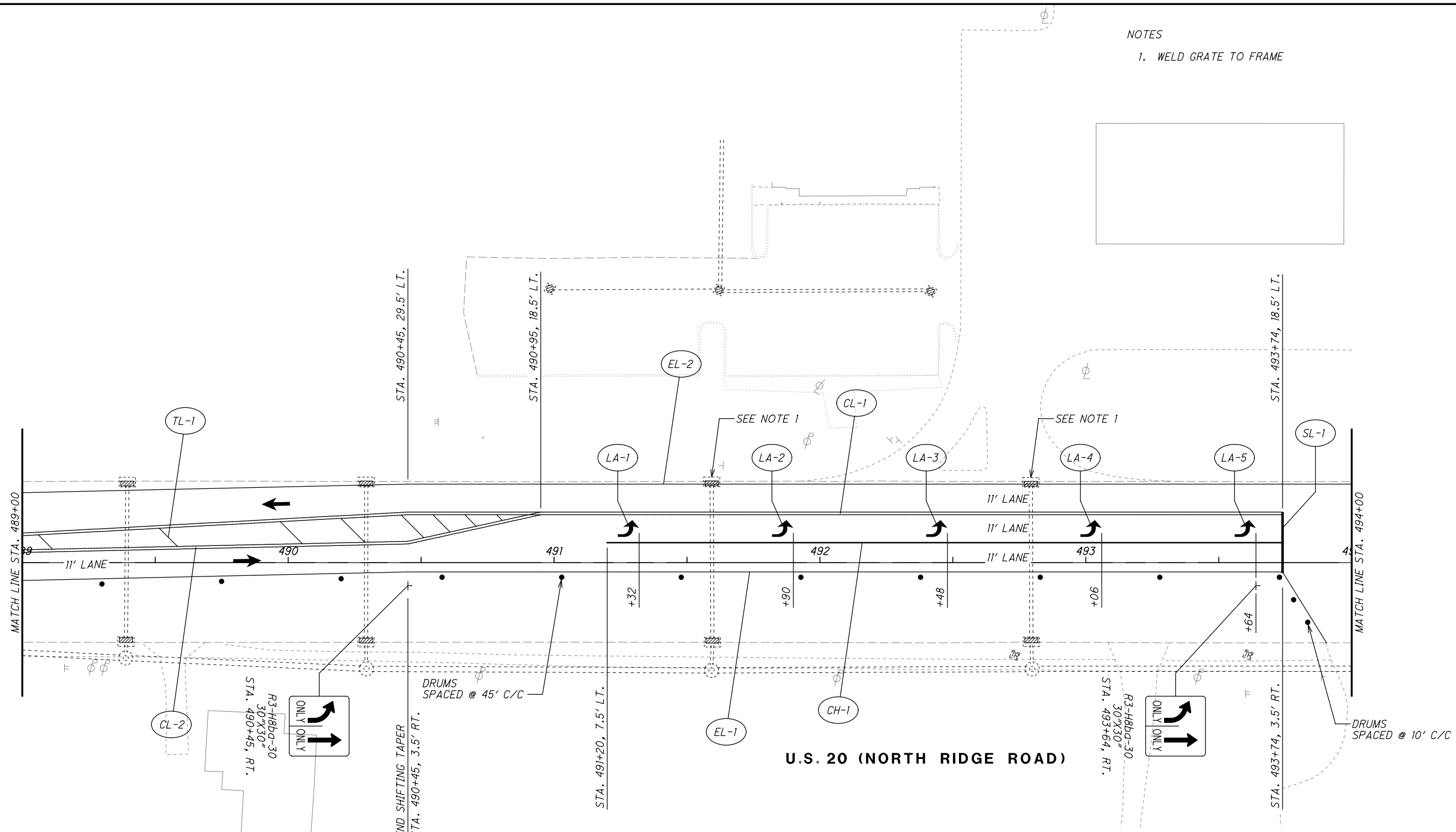
CALCULATED
 EJT
 CHECKED
 WDB

0 10 20 40
 HORIZONTAL
 SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 489+00 TO STA. 494+00

LAK-US-20-24.99
PART 2

NOTES
 1. WELD GRATE TO FRAME



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP084F.dgn Sheet 7/15/2022 2:50:46 PM troyer

TRAFFIC SIGNAL NOTES

- MOVE EXISTING TRAFFIC SIGNAL HEADS, "A" AND "B" TO THE FOLLOWING NEW LOCATIONS:
 SIGNAL HEAD "A": 34.2 FEET FROM POLE P1
 SIGNAL HEAD "B": 44.9 FEET FROM POLE P1
- MOVE ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.

NOTES

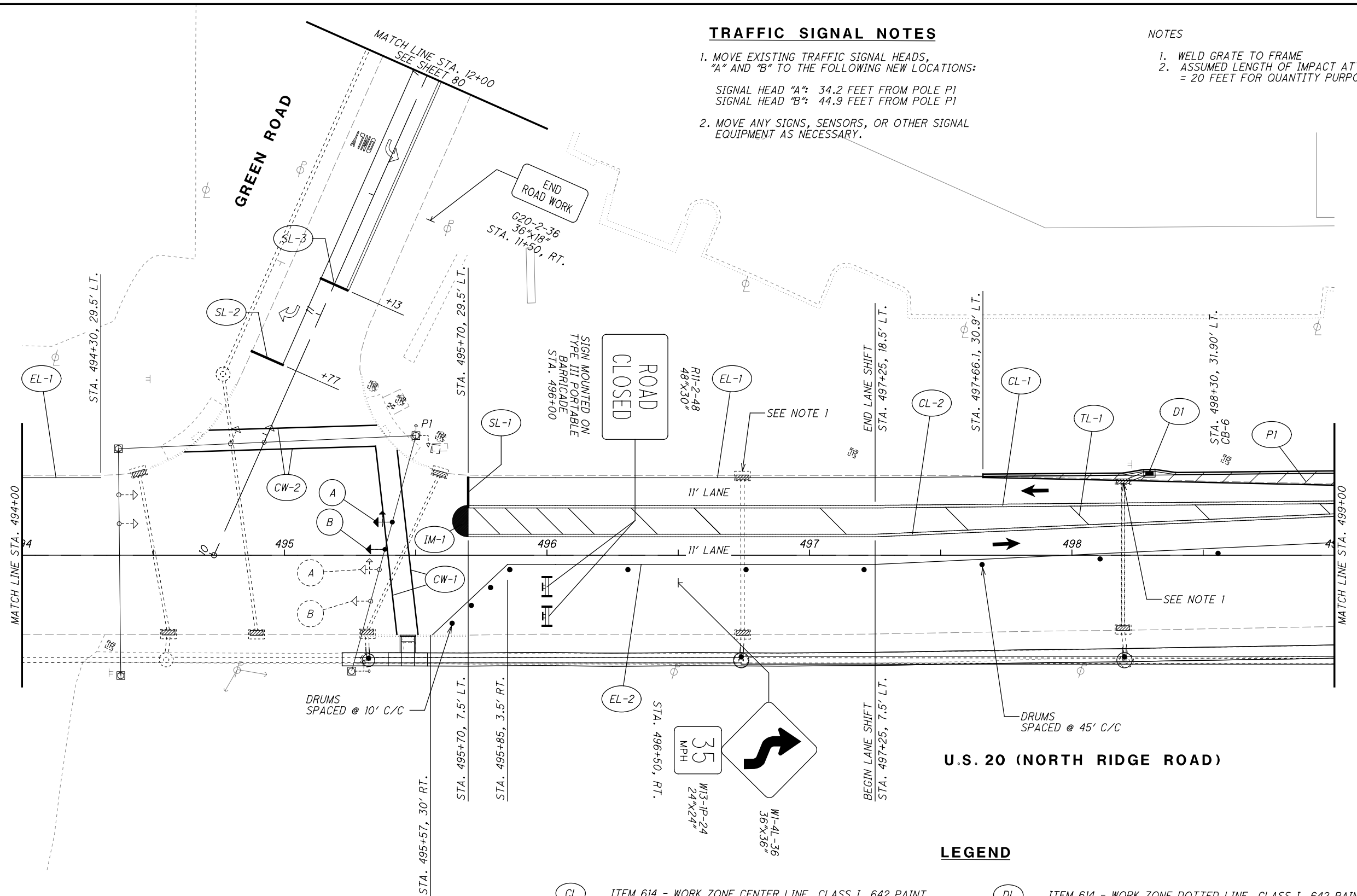
- WELD GRATE TO FRAME
- ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



CALCULATED EJT
 CHECKED WDB

**MAINTENANCE OF TRAFFIC PLAN - PHASE 1
 STA. 494+00 TO STA. 499+00**

**LAK-US-20-24.99
 PART 2**



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW

H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP084G.dgn Sheet 7/15/2022 2:50:47 PM troyer

NOTES

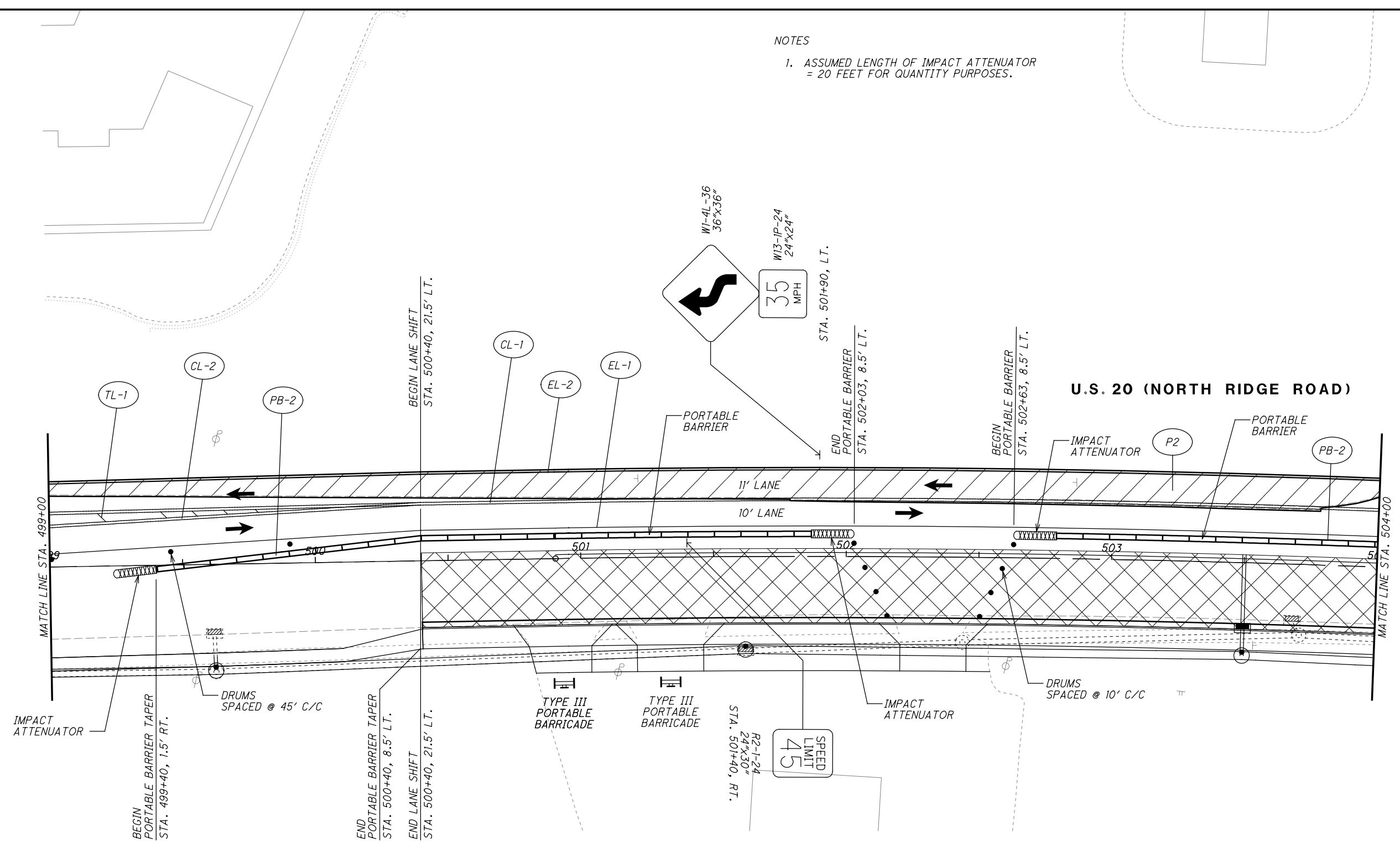
1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

CALCULATED
EJT
CHECKED
WDB

0 20 40
10
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 499+00 TO STA. 504+00

LAK-US-20-24.99
PART 2



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW

LEGEND

U.S. 20 (NORTH RIDGE ROAD)

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP084H.dgn Sheet 7/15/2022 2:50:48 PM troyer



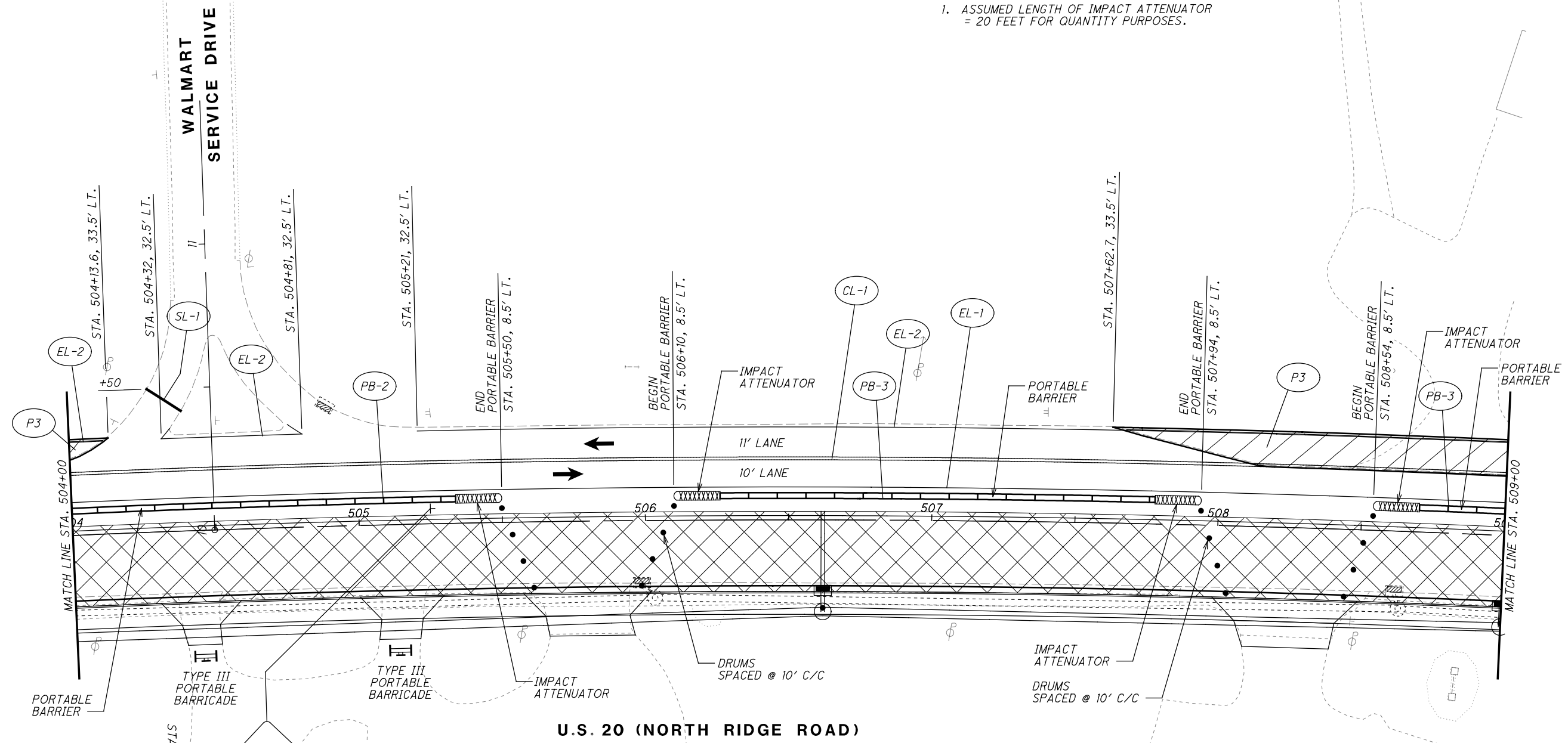
CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 504+00 TO STA. 509+00

LAK-US-20-24.99
PART 2

60
697

NOTES
1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



U.S. 20 (NORTH RIDGE ROAD)

LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

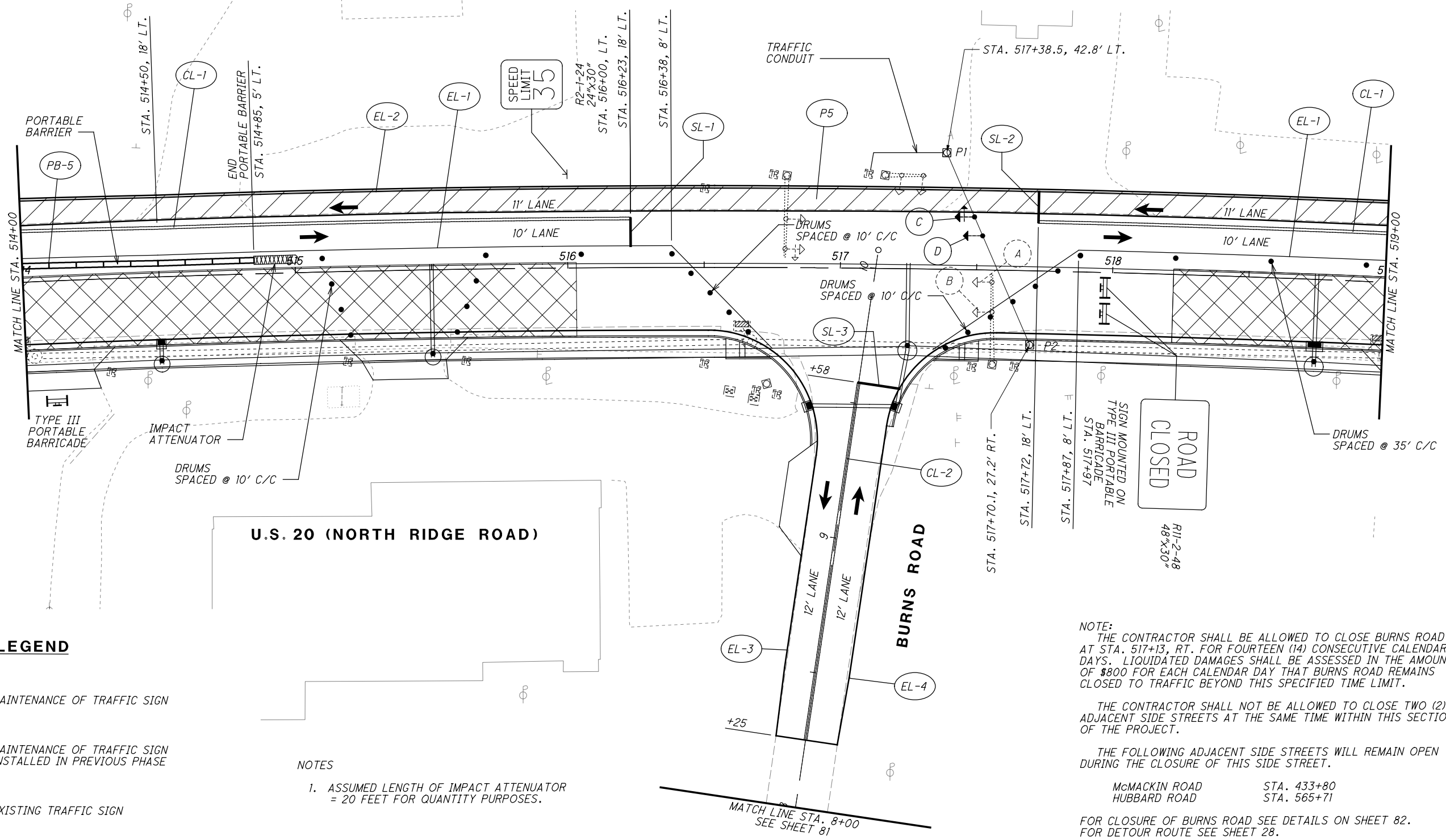
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW

TRAFFIC SIGNAL NOTES

1. CONSTRUCT TEMPORARY POLES, P1 AND P2, AS PER SCD MT-96.20. PLACE SIGNAL HEADS AT THE FOLLOWING LOCATIONS:
SIGNAL HEAD "C": 25.7 FEET FROM POLE P1
SIGNAL HEAD "D": 33.2 FEET FROM POLE P1
2. CONSTRUCT TRAFFIC CONDUIT AND CABLES. CONNECT TO POWER SOURCE.
3. BAG EXISTING TRAFFIC SIGNAL HEADS, "A" AND "B".
4. MOVE OR ADD ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.

LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | () | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |



LEGEND

- () MAINTENANCE OF TRAFFIC SIGN
- () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- () EXISTING TRAFFIC SIGN

NOTES

1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

NOTE:
THE CONTRACTOR SHALL BE ALLOWED TO CLOSE BURNS ROAD AT STA. 517+13, RT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$800 FOR EACH CALENDAR DAY THAT BURNS ROAD REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT SIDE STREETS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

THE FOLLOWING ADJACENT SIDE STREETS WILL REMAIN OPEN DURING THE CLOSURE OF THIS SIDE STREET.

McMACKIN ROAD STA. 433+80
HUBBARD ROAD STA. 565+71

FOR CLOSURE OF BURNS ROAD SEE DETAILS ON SHEET 82.
FOR DETOUR ROUTE SEE SHEET 28.

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP084J.dgn Sheet 7/15/2022 2:50:49 PM troyer

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 514+00 TO STA. 519+00

LAK-US-20-24.99
PART 2

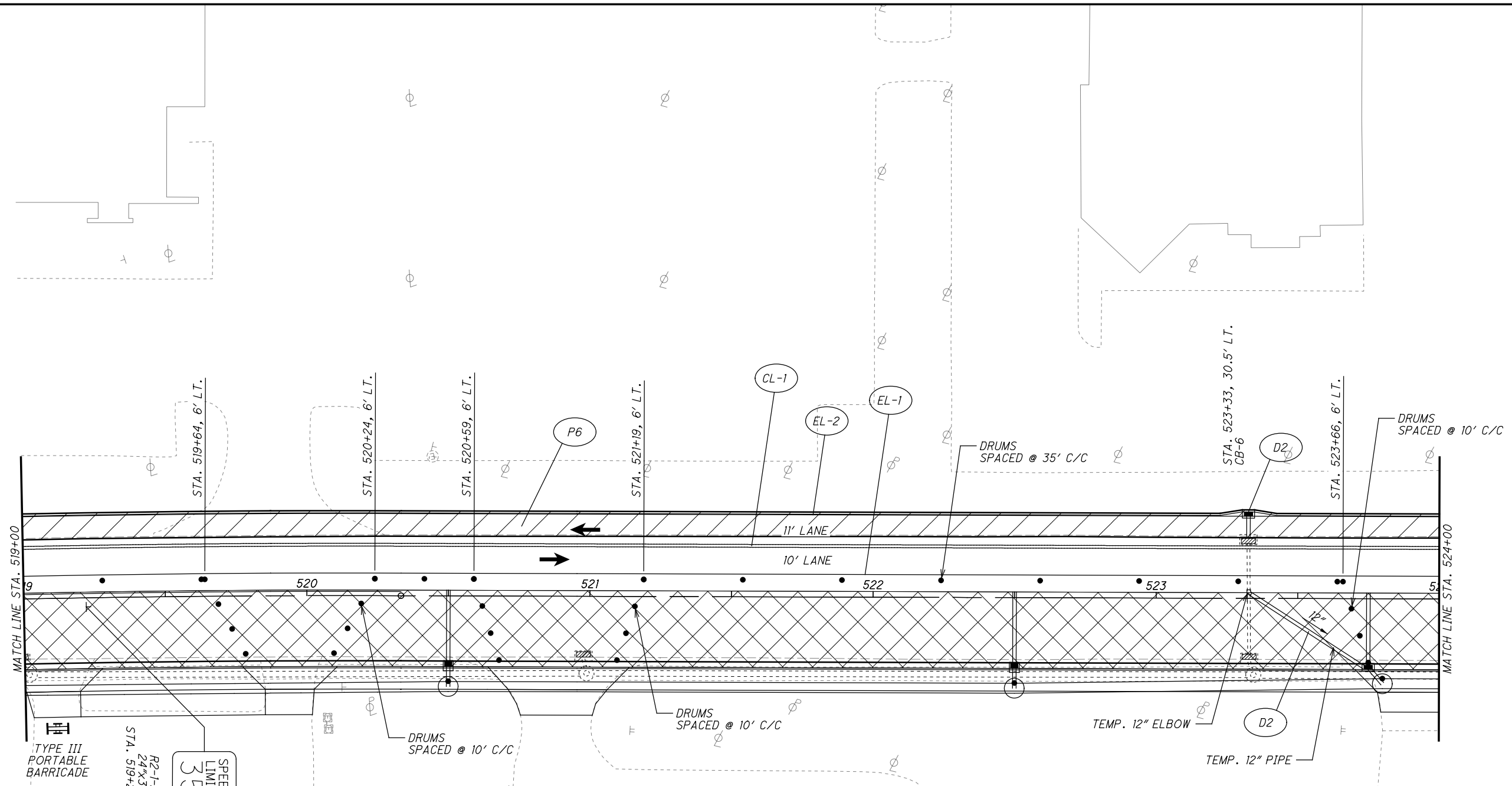
H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP084k.dgn Sheet 7/15/2022 2:50:50 PM troyer

CALCULATED
 EJT
 CHECKED
 WDB

0 20 40
 HORIZONTAL
 SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 519+00 TO STA. 524+00

LAK-US-20-24.99
PART 2



LEGEND

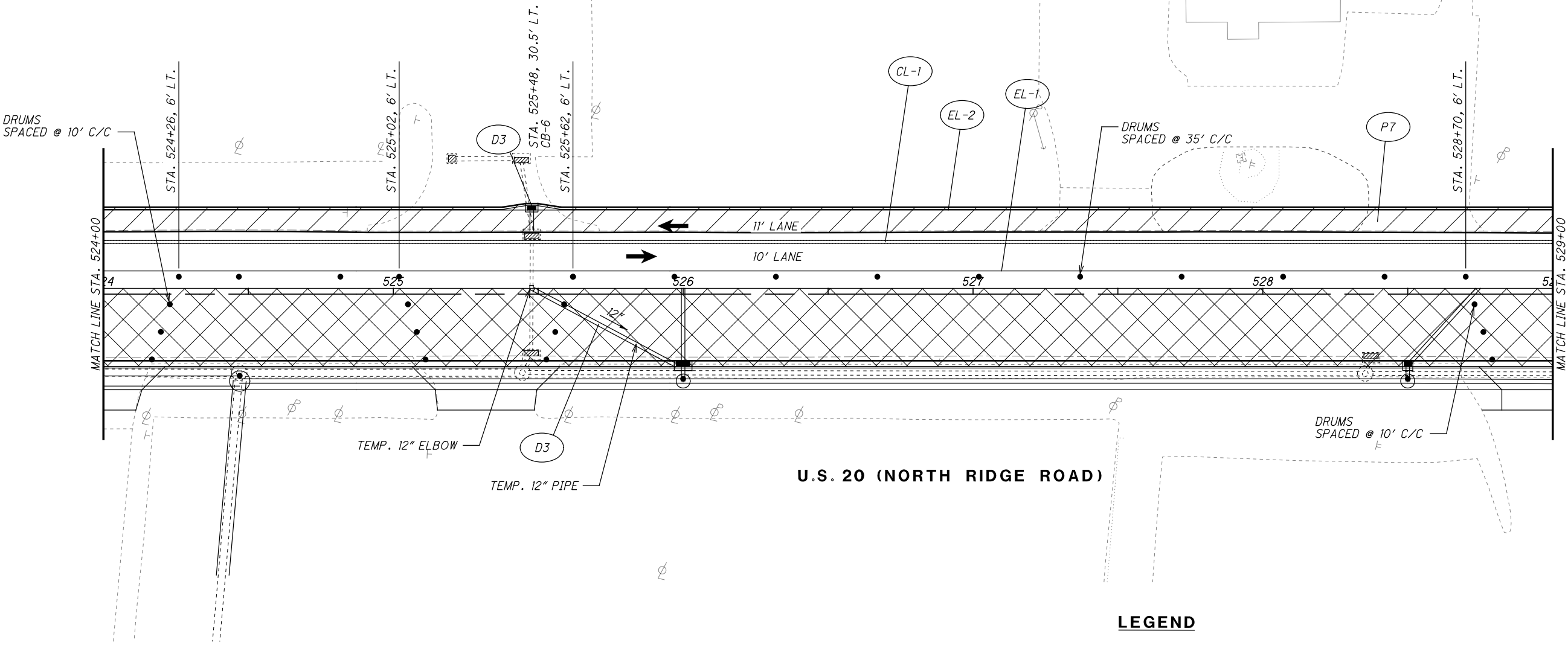
- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP084L.dgn Sheet 7/15/2022 2:50:51PM troyer



LEGEND

	MAINTENANCE OF TRAFFIC SIGN
	MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
	EXISTING TRAFFIC SIGN

	ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

	ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
	WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
	PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
	PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE I
	TRAFFIC FLOW




CALCULATED
EJT
CHECKED
WDB

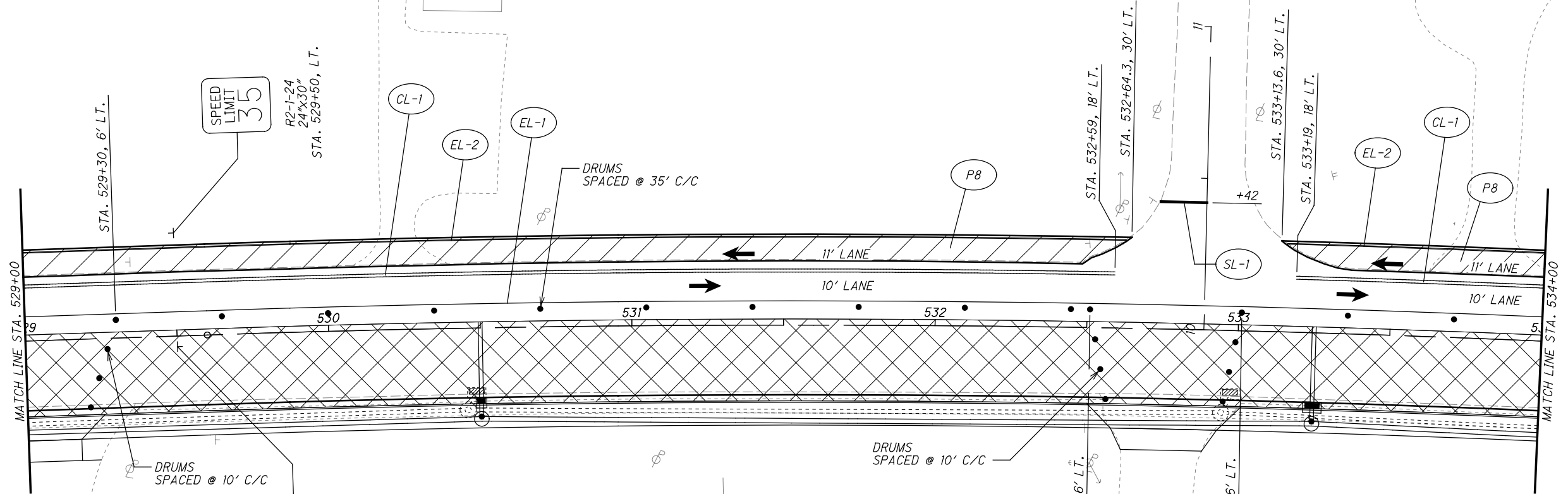
0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 524+00 TO STA. 529+00

LAK-US-20-24.99
PART 2










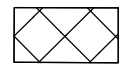

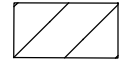


H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP085.dgn Sheet 7/15/2022 2:50:52 PM Troyer

- LEGEND**
-  MAINTENANCE OF TRAFFIC SIGN
 -  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
 -  EXISTING TRAFFIC SIGN



U.S. 20 (NORTH RIDGE ROAD)

LEGEND

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE I |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET



MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 529+00 TO STA. 534+00

LAK-US-20-24.99
PART 2

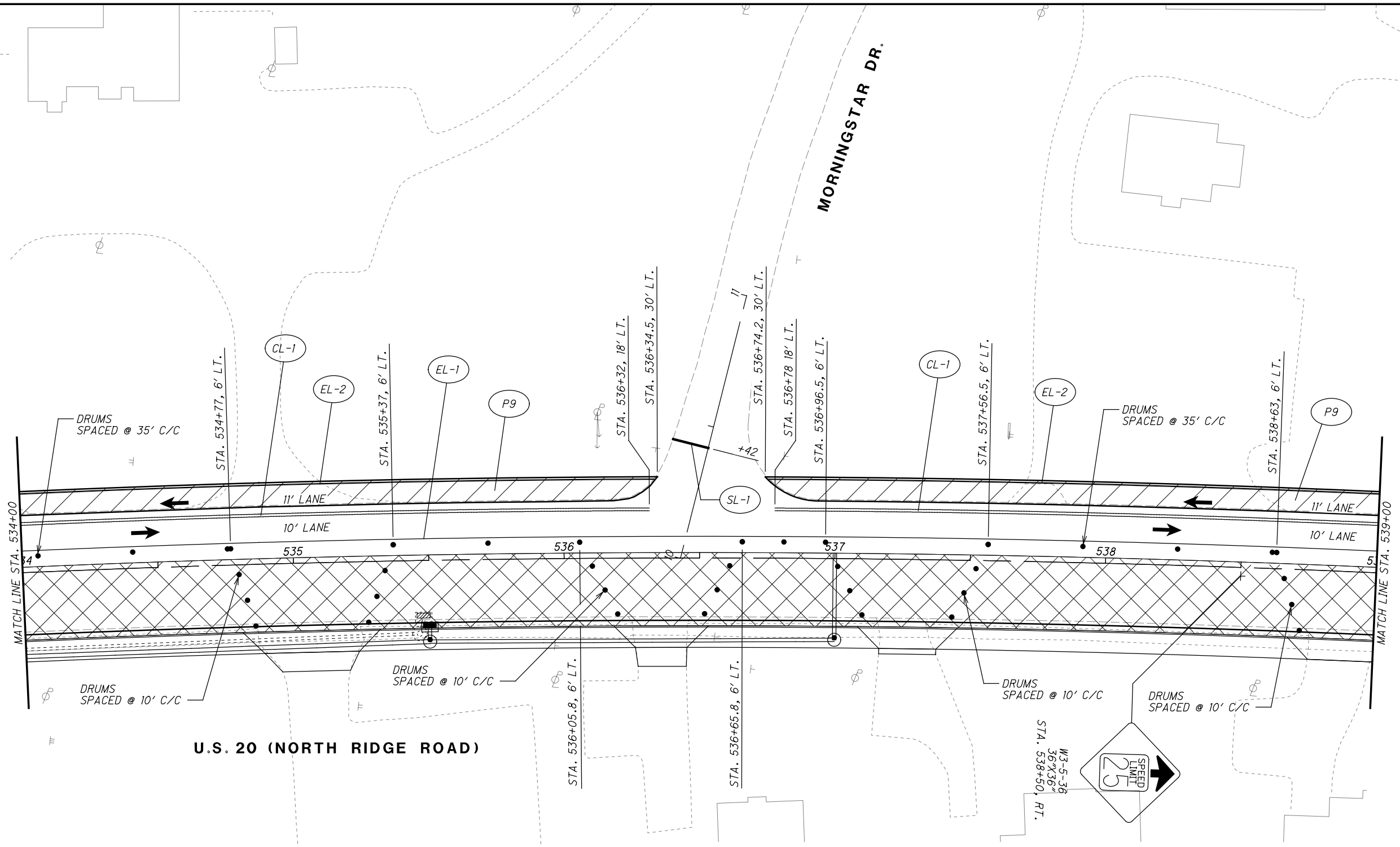
H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP086.dgn Sheet 7/15/2022 2:50:52 PM Troyer

CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 534+00 TO STA. 539+00

LAK-US-20-24.99
PART 2



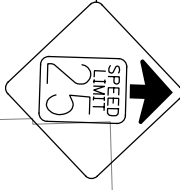
LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

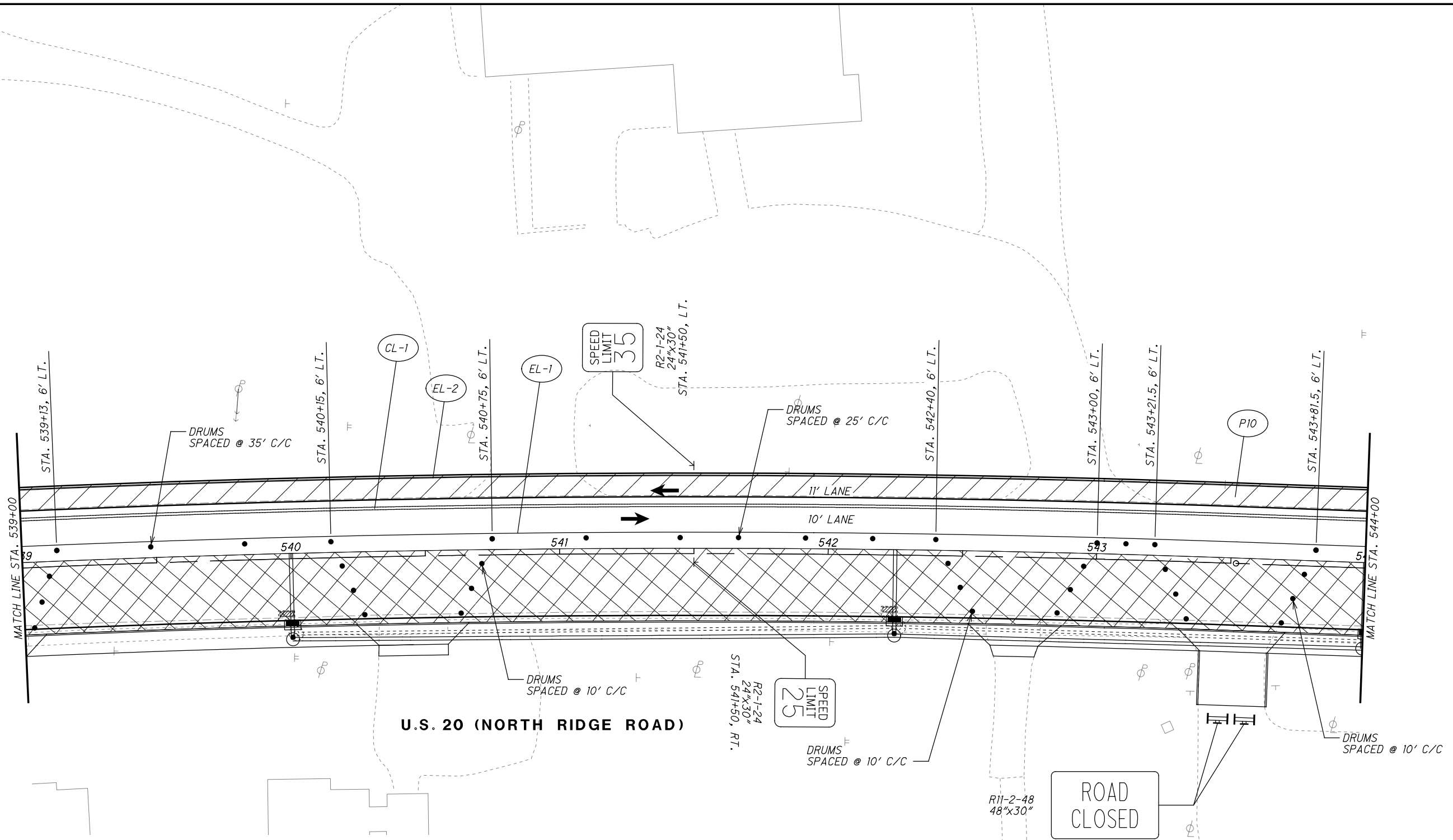
- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW



H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP087.dgn Sheet 7/15/2022 2:50:53 PM Troyer



U.S. 20 (NORTH RIDGE ROAD)

LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | (○) | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

LEGEND

- (□) MAINTENANCE OF TRAFFIC SIGN
- (□) MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- (□) EXISTING TRAFFIC SIGN

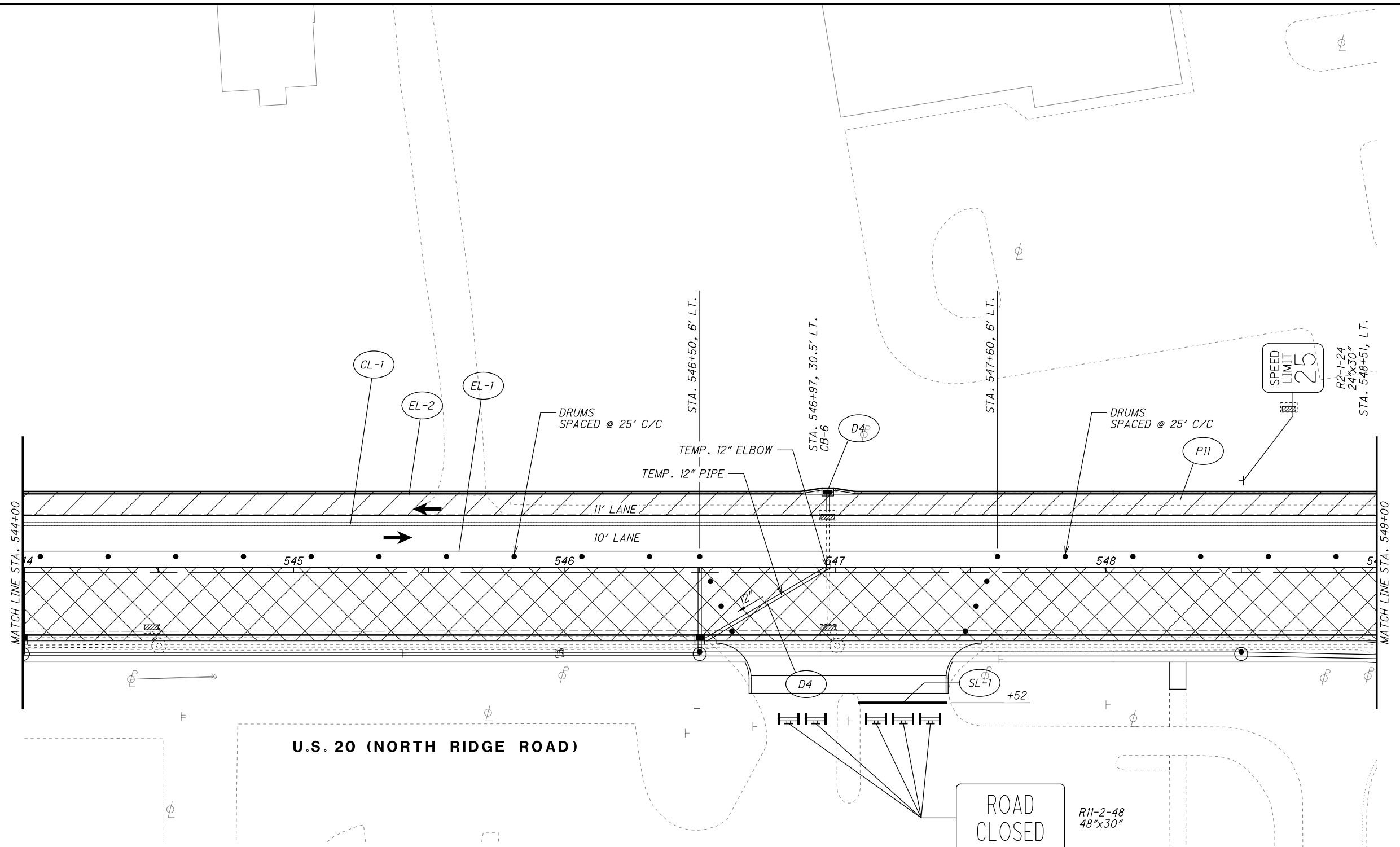
CALCULATED
EJT
CHECKED
WDB

0 10 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 539+00 TO STA. 544+00

LAK-US-20-24.99
PART 2

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP088.dgn_Sheet 7/15/2022 2:50:54 PM Troyer



LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | () | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

LEGEND

- () MAINTENANCE OF TRAFFIC SIGN
- () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- () EXISTING TRAFFIC SIGN

CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 544+00 TO STA. 549+00

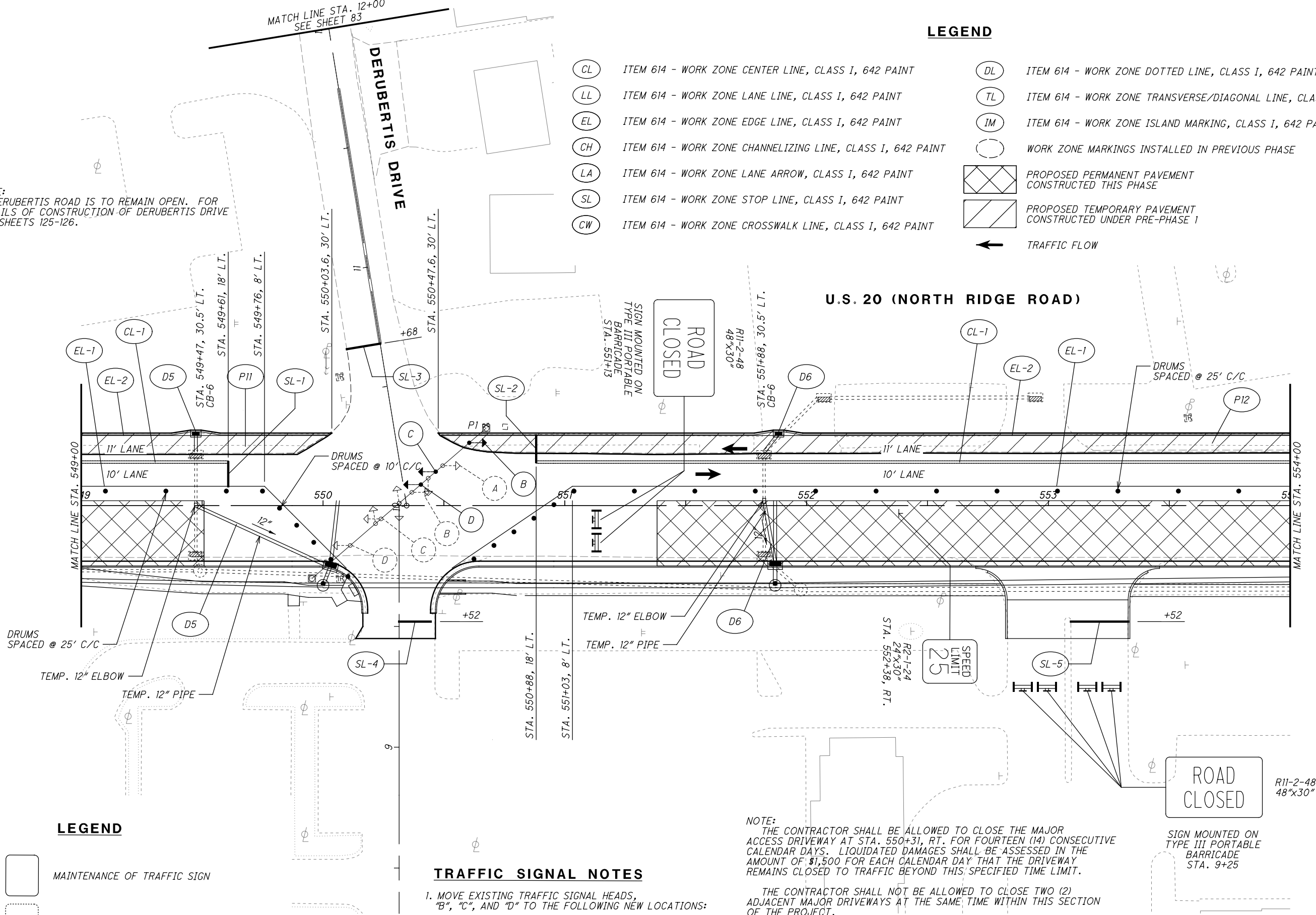
LAK-US-20-24.99
PART 2

H:\2018\80743\00T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP089.dgn Sheet 7/15/2022 2:50:54 PM Troyer

NOTE:
 DERUBERTIS ROAD IS TO REMAIN OPEN. FOR DETAILS OF CONSTRUCTION OF DERUBERTIS DRIVE SEE SHEETS 125-126.

LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | () | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |



LEGEND

- () MAINTENANCE OF TRAFFIC SIGN
- () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- () EXISTING TRAFFIC SIGN

TRAFFIC SIGNAL NOTES

- MOVE EXISTING TRAFFIC SIGNAL HEADS, "B", "C", AND "D" TO THE FOLLOWING NEW LOCATIONS:
 SIGNAL HEAD "B": 9.3 FEET FROM POLE P1
 SIGNAL HEAD "C": 27.6 FEET FROM POLE P1
 SIGNAL HEAD "D": 35.7 FEET FROM POLE P1
- MOVE ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.

NOTE:
 THE CONTRACTOR SHALL BE ALLOWED TO CLOSE THE MAJOR ACCESS DRIVEWAY AT STA. 550+31, RT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT THE DRIVEWAY REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT MAJOR DRIVEWAYS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.
 THE FOLLOWING ADJACENT MAJOR DRIVEWAYS WILL REMAIN OPEN DURING THE CLOSURE OF THIS DRIVE.
 MAJOR ACCESS DRIVEWAY STA. 556+02
 FOR CLOSURE OF MAJOR ACCESS DRIVEWAY SEE DETAILS ON SHEET 92.



CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 554+00 TO STA. 559+00

LAK-US-20-24.99
PART 2

NOTE:
THE CONTRACTOR SHALL BE ALLOWED TO CLOSE THE MAJOR ACCESS DRIVEWAY AT STA. 556+02, RT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT THE DRIVEWAY REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT MAJOR DRIVEWAYS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

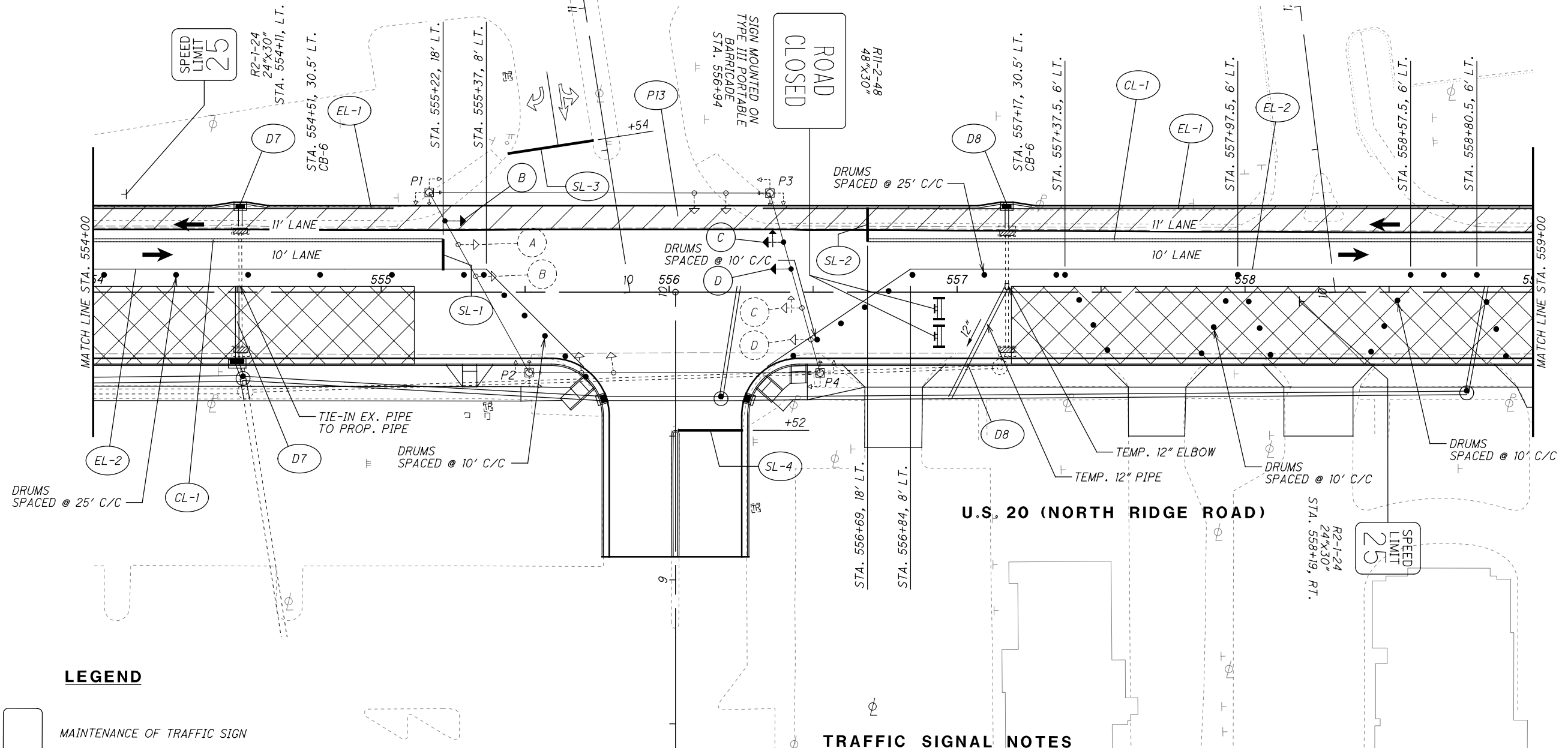
THE FOLLOWING ADJACENT MAJOR DRIVEWAYS WILL REMAIN OPEN DURING THE CLOSURE OF THIS DRIVE.

MAJOR ACCESS DRIVEWAY STA. 550+31

FOR CLOSURE OF MAJOR ACCESS DRIVE SEE DETAILS ON SHEET 92.

LEGEND

- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
 - (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
 - (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
 - (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
 - (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
 - (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
 - (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
 - (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
 - (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
 - (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
 - () WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
 - (X) PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
 - (/) PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- ← TRAFFIC FLOW



LEGEND

- () MAINTENANCE OF TRAFFIC SIGN
- () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- () EXISTING TRAFFIC SIGN

TRAFFIC SIGNAL NOTES

1. MOVE EXISTING TRAFFIC SIGNAL HEADS, "A", "B", AND "C" TO THE FOLLOWING NEW LOCATIONS:
 - SIGNAL HEAD "B": 11.3 FEET FROM POLE P1
 - SIGNAL HEAD "C": 17.7 FEET FROM POLE P3
 - SIGNAL HEAD "D": 27.4 FEET FROM POLE P3
2. MOVE ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP090.dgn Sheet 7/15/2022 2:50:55 PM Troyer

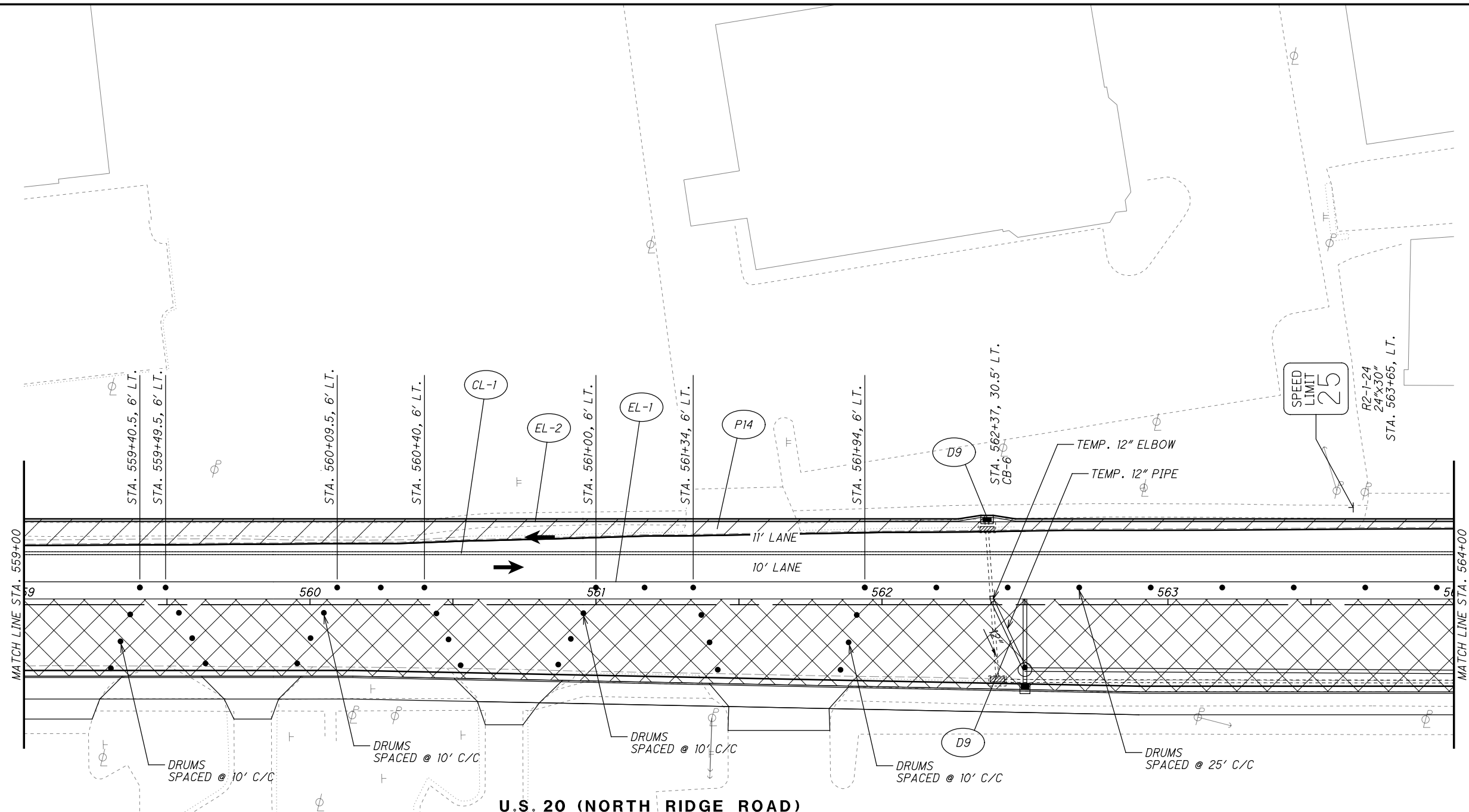
H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP091.dgn Sheet 7/15/2022 2:50:56 PM troyer

CALCULATED
 EJT
 CHECKED
 WDB

0 20 40
 HORIZONTAL
 SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 559+00 TO STA. 564+00

LAK-US-20-24.99
PART 2



U.S. 20 (NORTH RIDGE ROAD)

LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW

NOTES

- 1. WELD GRATE TO FRAME

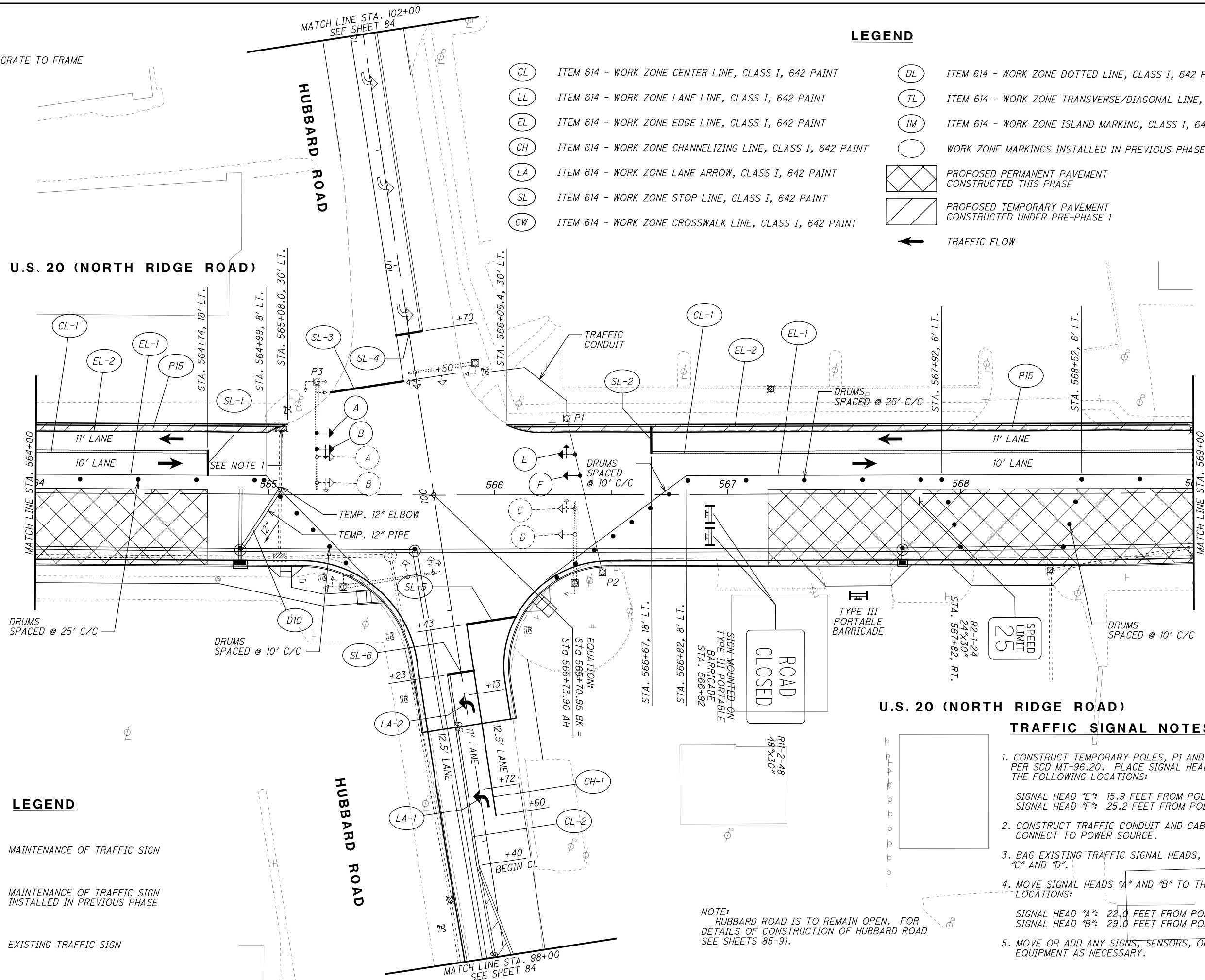
LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | (○) | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |



CALCULATED EJT
 CHECKED WDB
MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 564+00 TO STA. 569+00

U.S. 20 (NORTH RIDGE ROAD)



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

TRAFFIC SIGNAL NOTES

- CONSTRUCT TEMPORARY POLES, P1 AND P2, AS PER SCD MT-96.20. PLACE SIGNAL HEADS AT THE FOLLOWING LOCATIONS:
 SIGNAL HEAD "E": 15.9 FEET FROM POLE P1
 SIGNAL HEAD "F": 25.2 FEET FROM POLE P1
- CONSTRUCT TRAFFIC CONDUIT AND CABLES. CONNECT TO POWER SOURCE.
- BAG EXISTING TRAFFIC SIGNAL HEADS, "C" AND "D".
- MOVE SIGNAL HEADS "A" AND "B" TO THE FOLLOWING LOCATIONS:
 SIGNAL HEAD "A": 22.0 FEET FROM POLE P3
 SIGNAL HEAD "B": 29.0 FEET FROM POLE P3
- MOVE OR ADD ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.

NOTE:
 HUBBARD ROAD IS TO REMAIN OPEN. FOR DETAILS OF CONSTRUCTION OF HUBBARD ROAD SEE SHEETS 85-91.

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP092.dgn Sheet 7/15/2022 2:50:57 PM Troyer

LAK-US-20-24.99
PART 2

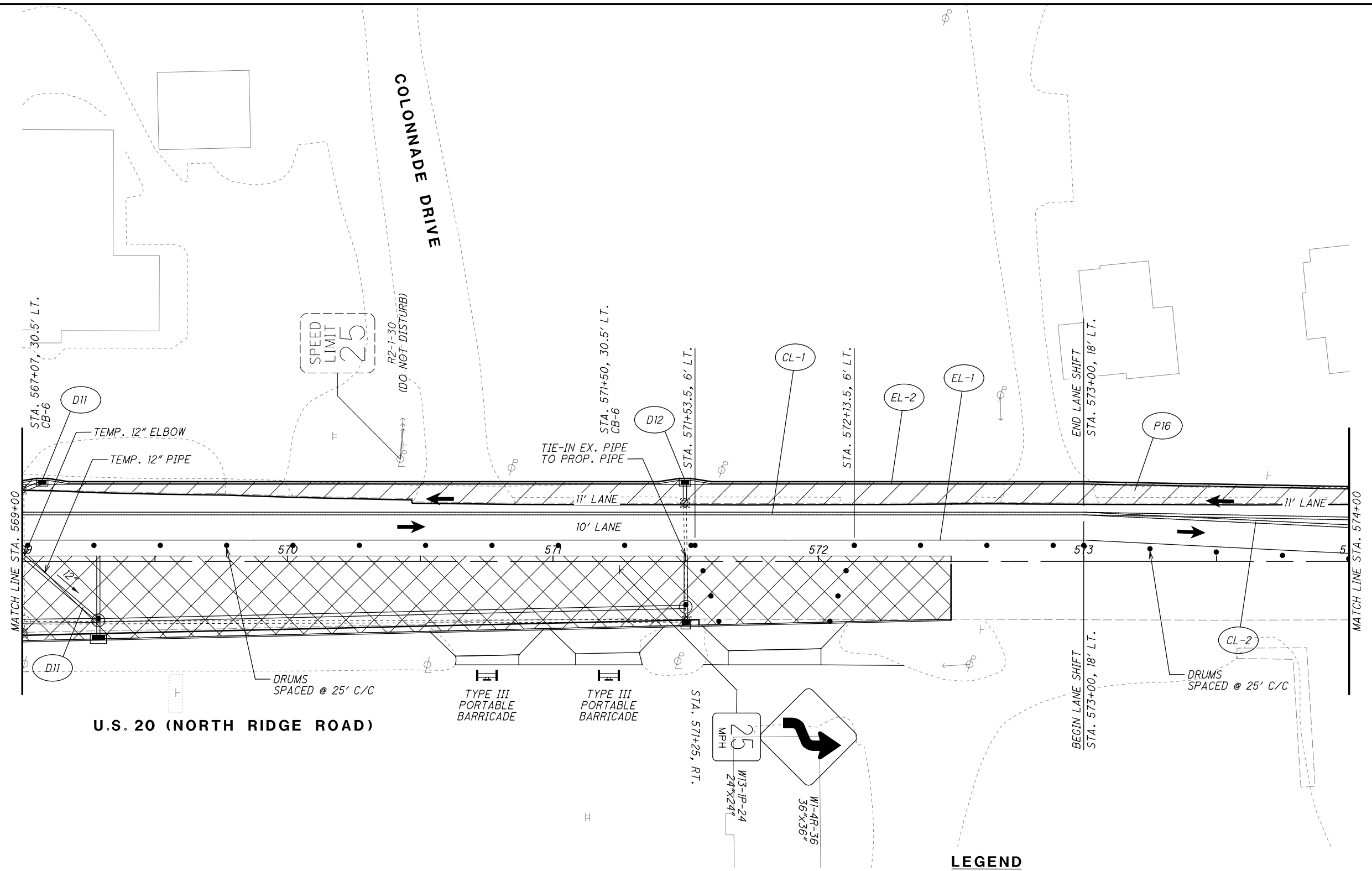
H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP093.dgn Sheet 7/15/2022 2:50:57 PM Troyer

CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 569+00 TO STA. 574+00

LAK-US-20-24.99
PART 2



U.S. 20 (NORTH RIDGE ROAD)

LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW

LEGEND

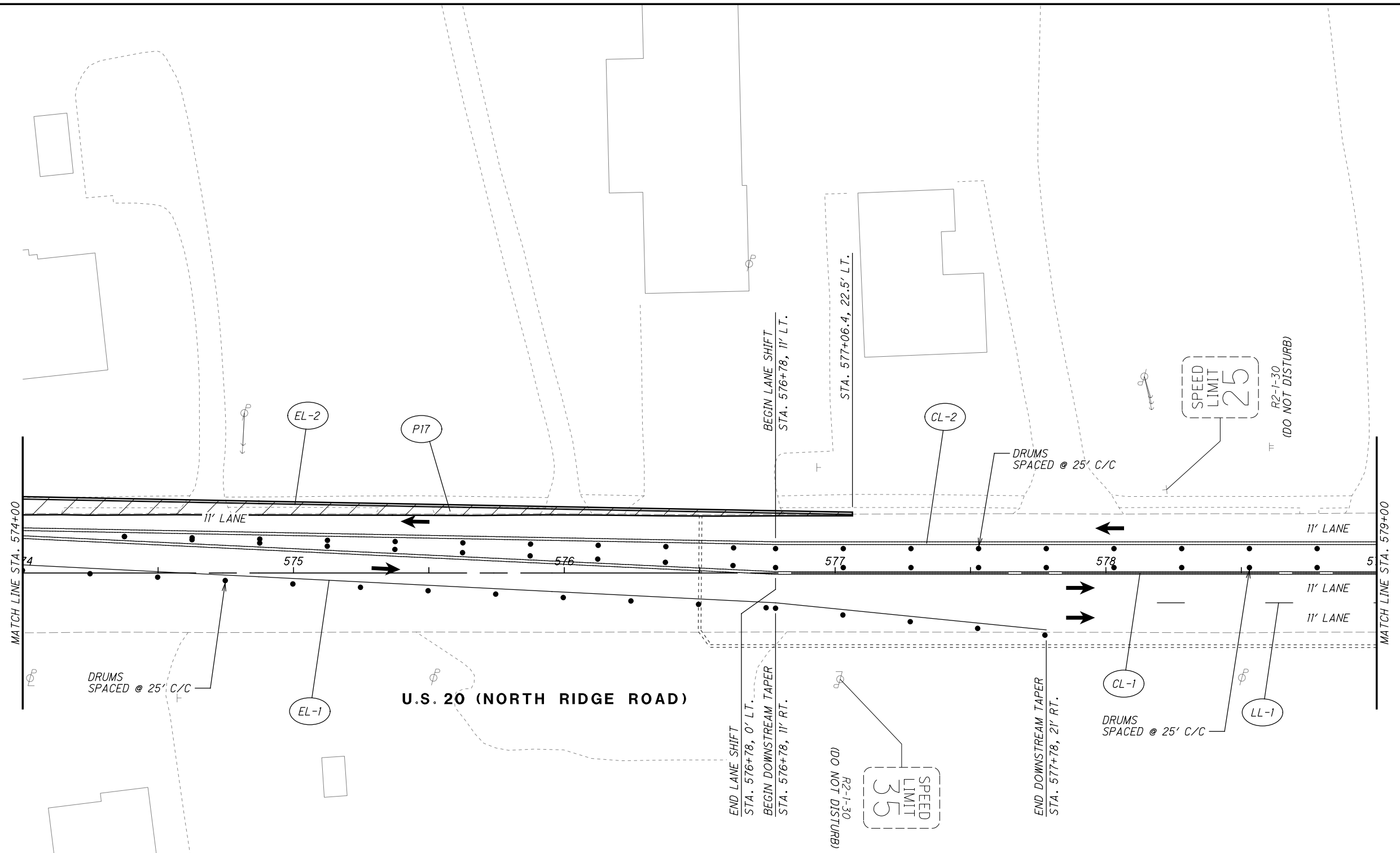
H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP094.dgn_Sheet 7/15/2022 2:50:58 PM Troyer

CALCULATED
 EJT
 CHECKED
 WDB

0 20 40
 HORIZONTAL
 SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 574+00 TO STA. 579+00

LAK-US-20-24.99
PART 2



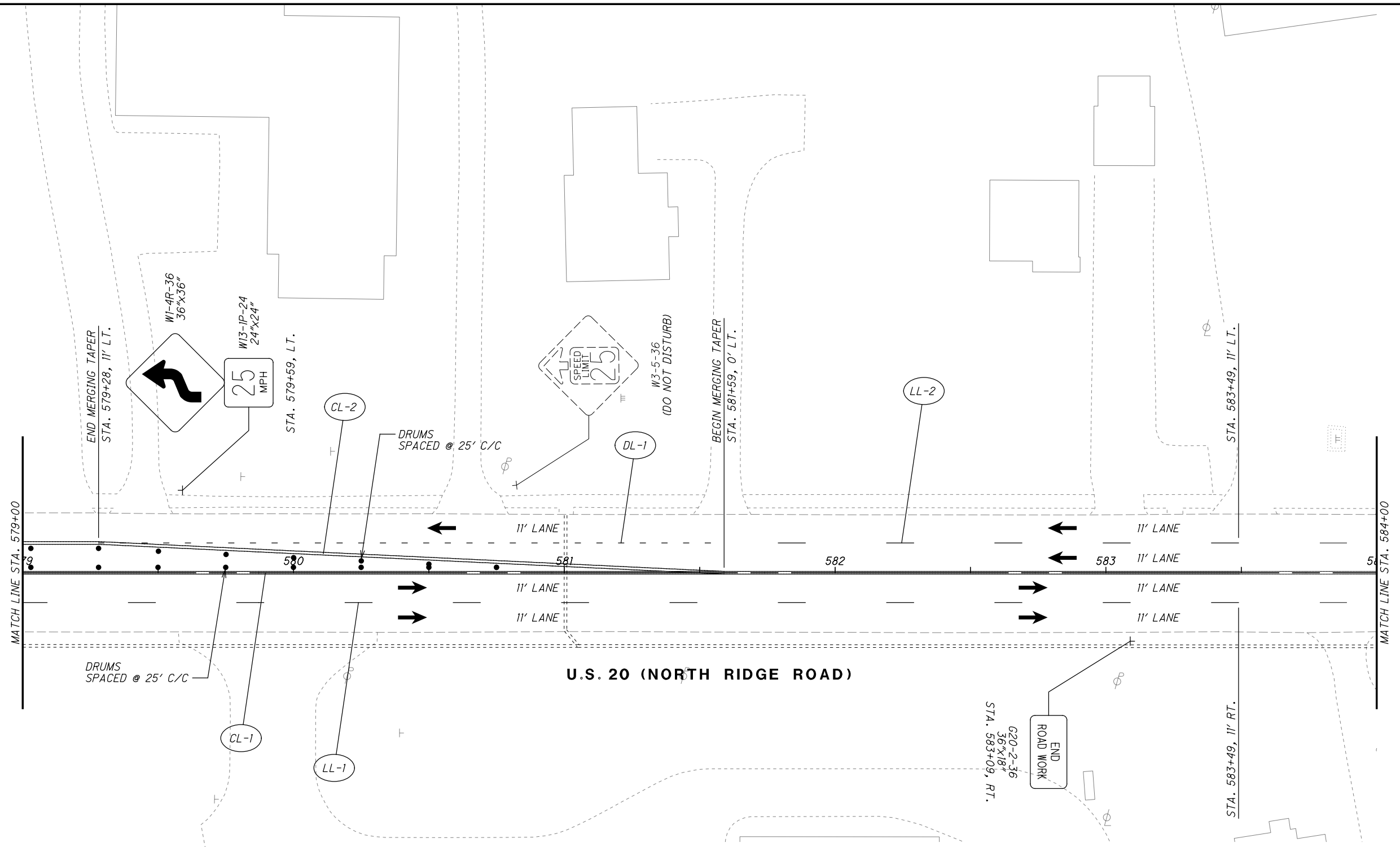
LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN




- CL ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- LL ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- EL ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- CH ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- LA ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- SL ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- CW ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT








LEGEND

- DL ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- TL ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- IM ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW





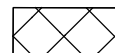
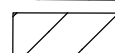
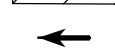


LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
-  TRAFFIC FLOW

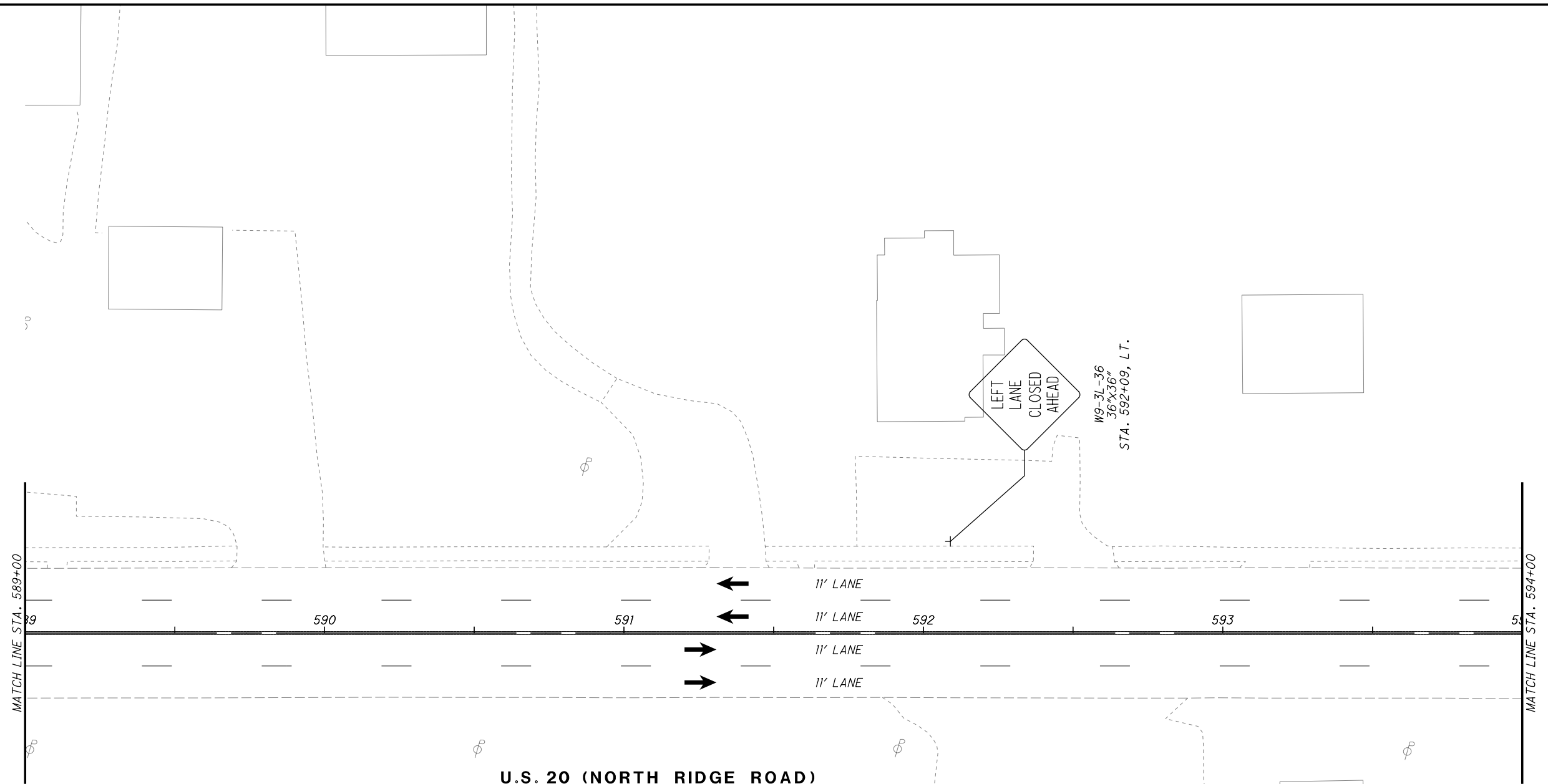
H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP094C.dgn Sheet 7/15/2022 2:51:00 PM Troyer

CALCULATED
EJT
CHECKED
WDB

0 10 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 589+00 TO STA. 594+00

LAK-US-20-24.99
PART 2



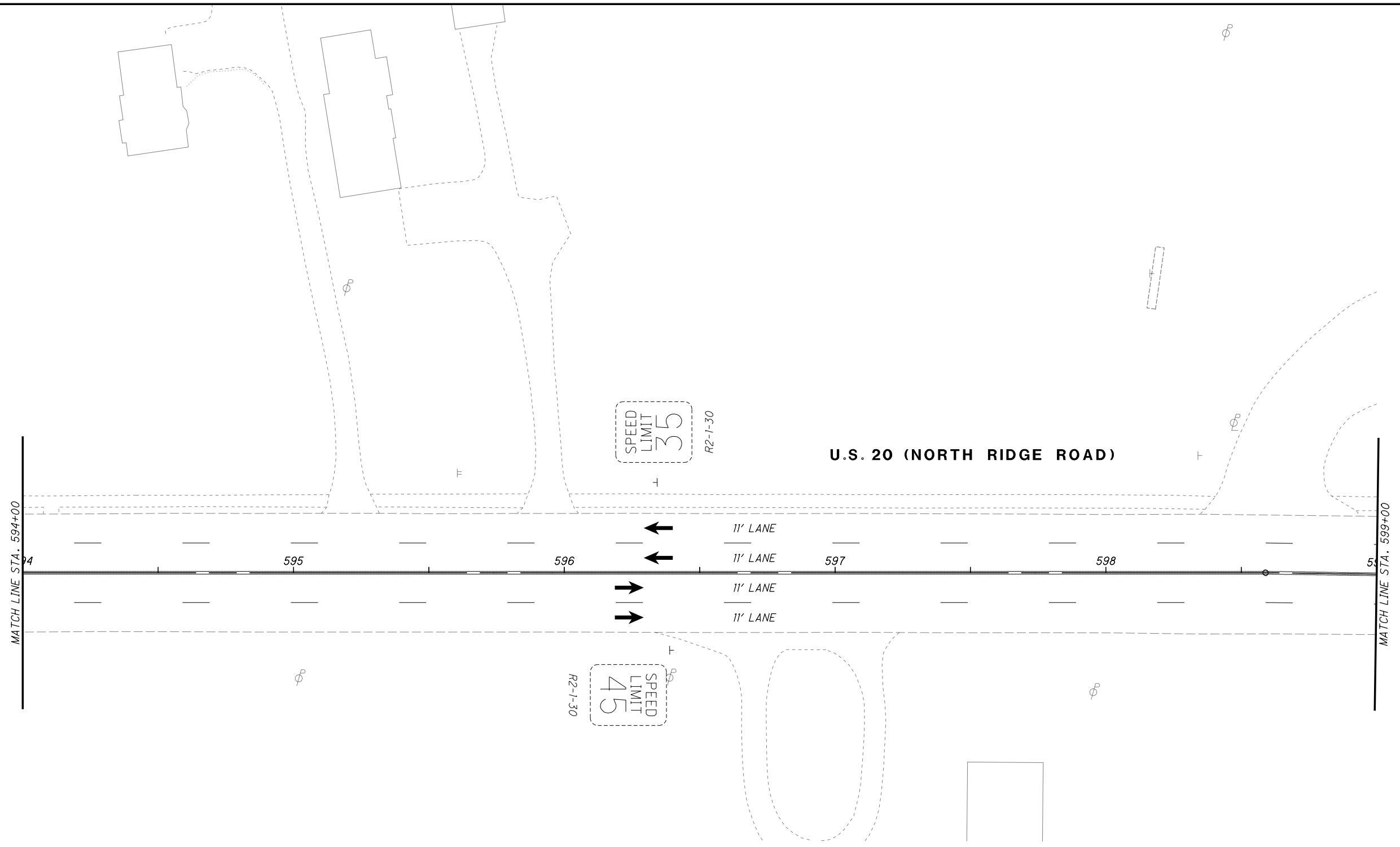
U.S. 20 (NORTH RIDGE ROAD)

LEGEND




- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

LEGEND












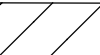


- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

LEGEND

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

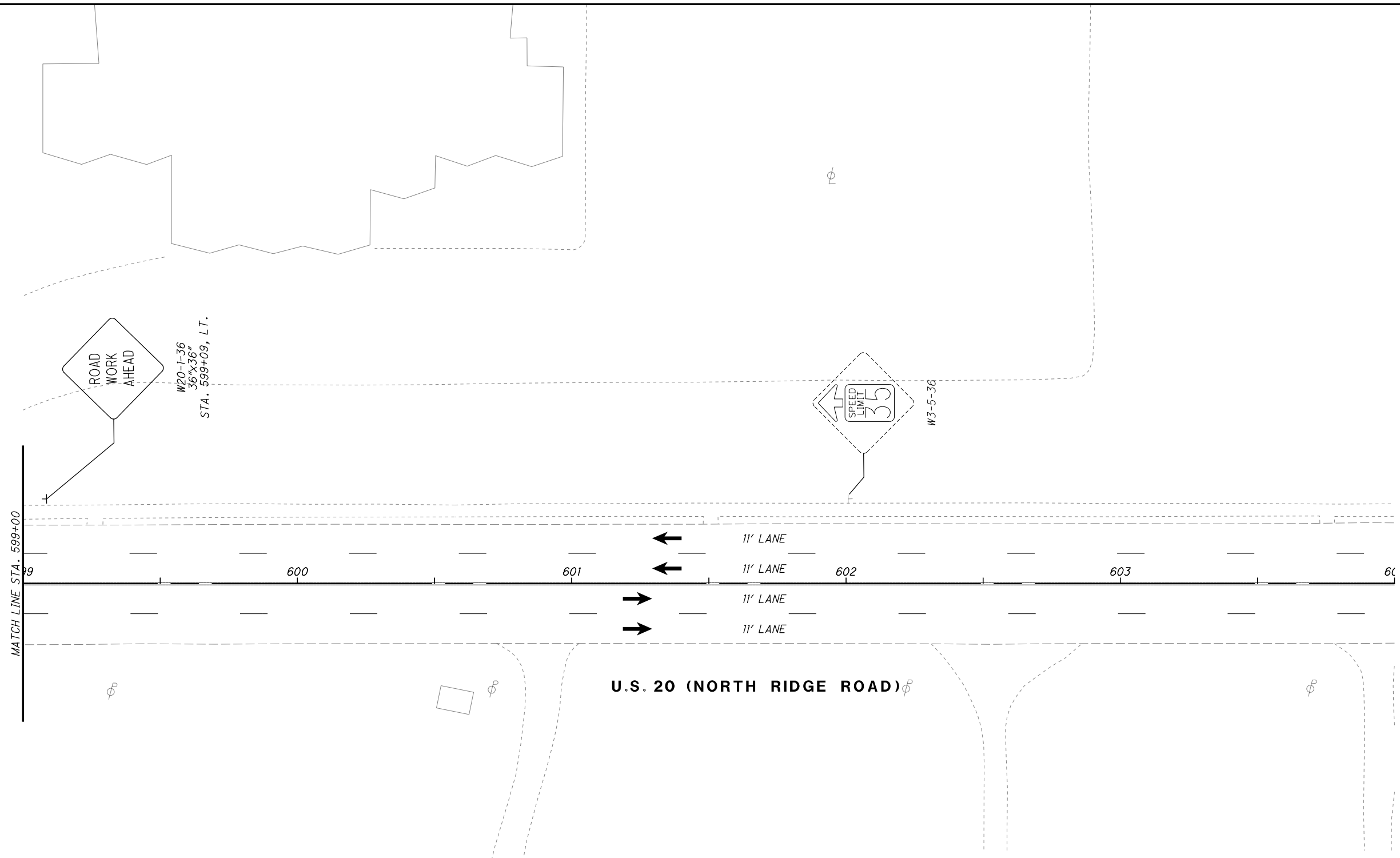
H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP095.dgn_Sheet 7/15/2022 2:51:01PM troyer

CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
STA. 599+00 TO STA. 604+00

LAK-US-20-24.99
PART 2

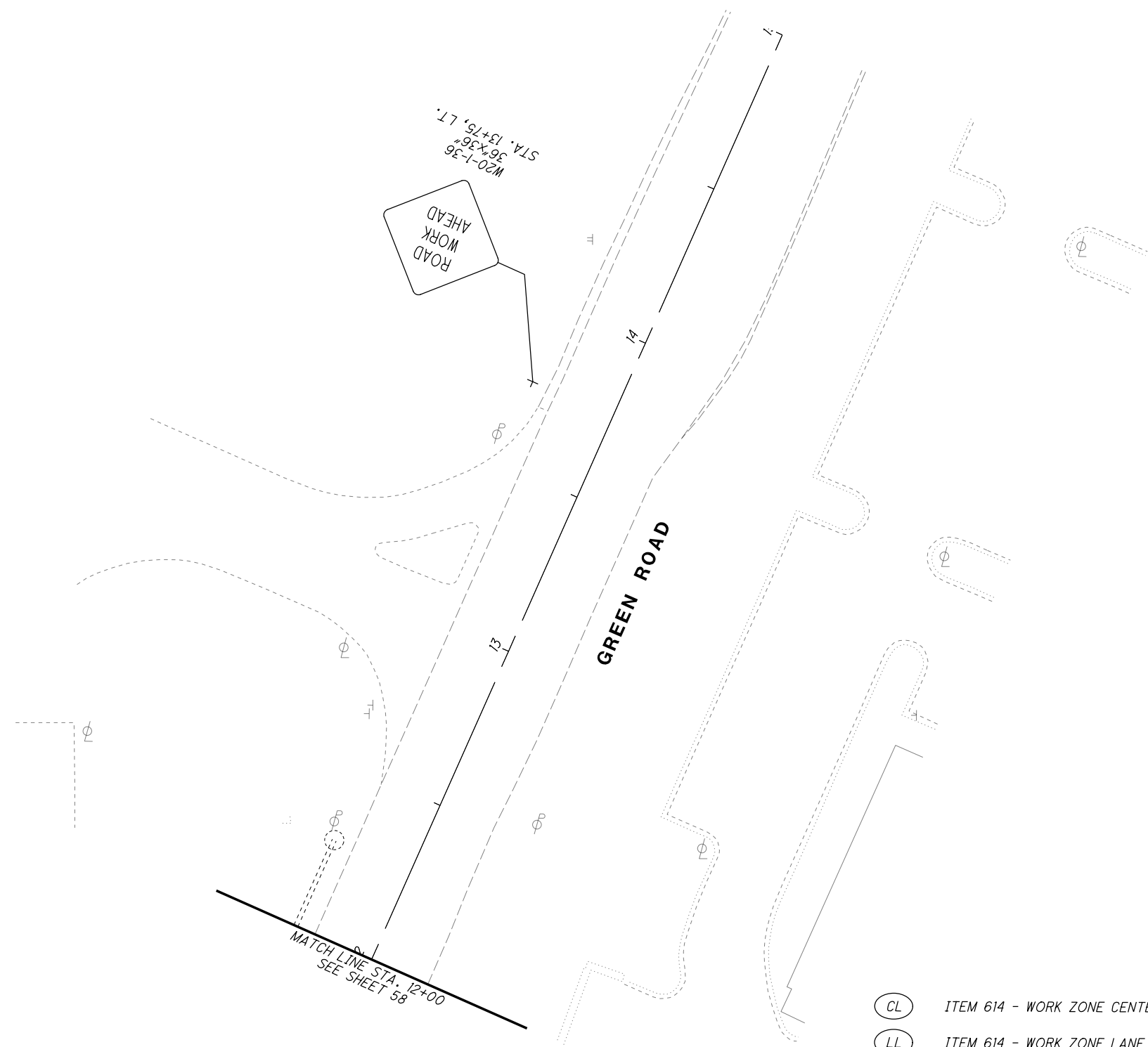


LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

LEGEND

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW



MATCH LINE STA. 12+00
SEE SHEET 58

W20-1-36
36'x36'
STA. 13+15, LT.
ROAD
WORK
AHEAD

GREEN ROAD

- LEGEND**
- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
 - (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
 - (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
 - (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
 - (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
 - (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
 - (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

- LEGEND**
- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
 - (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
 - (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
 - () WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
 - PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
 - PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE I
 - TRAFFIC FLOW

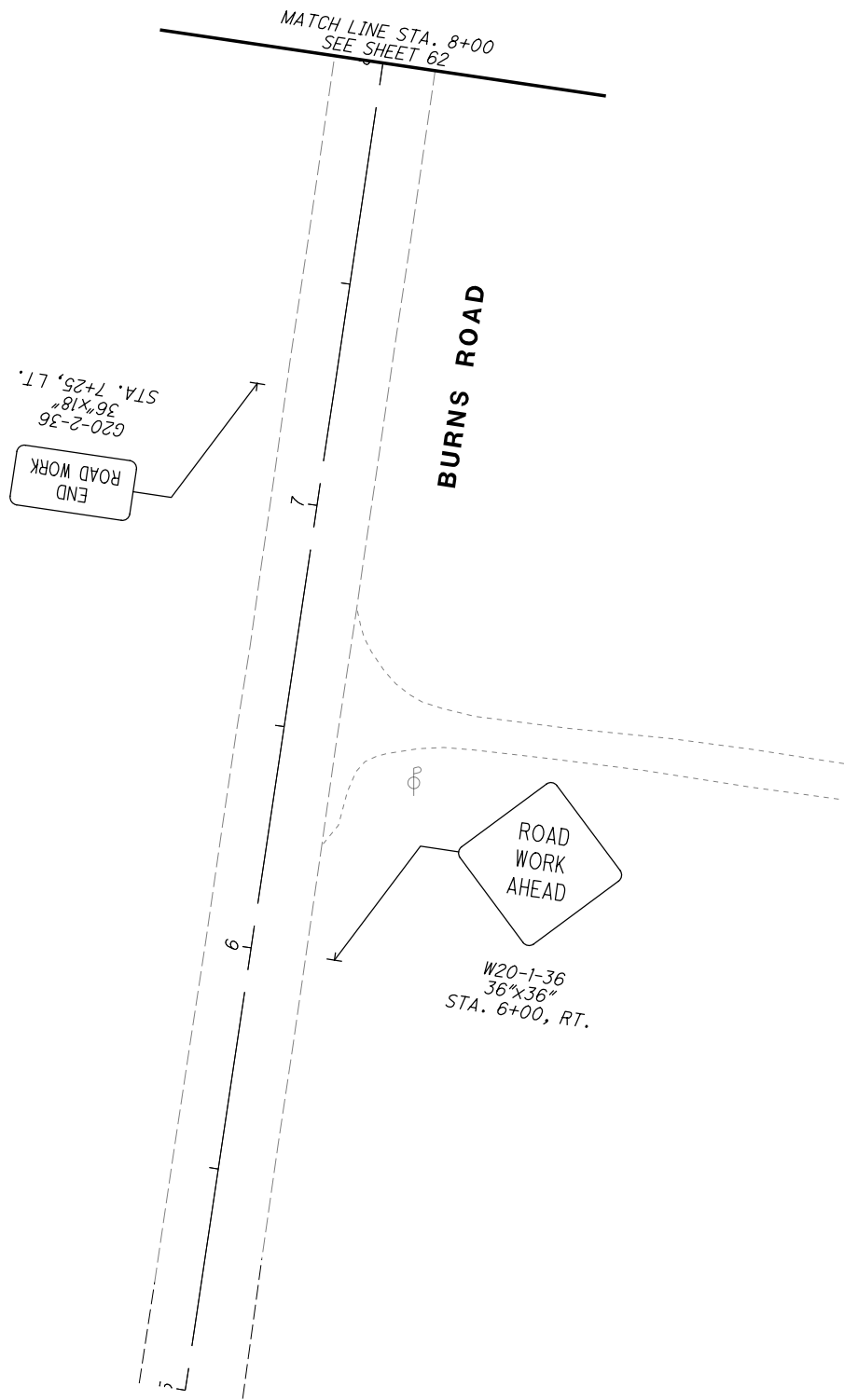
- LEGEND**
- MAINTENANCE OF TRAFFIC SIGN
 - MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
 - EXISTING TRAFFIC SIGN

CALCULATED EJT
CHECKED WDB




0 20 40
HORIZONTAL SCALE IN FEET

**MAINTENANCE OF TRAFFIC PLAN - PHASE 1
GREEN ROAD**

**LAK-US-20-24.99
PART 2**



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN















CALCULATED EJT
CHECKED WDB

0 20 40
HORIZONTAL SCALE IN FEET

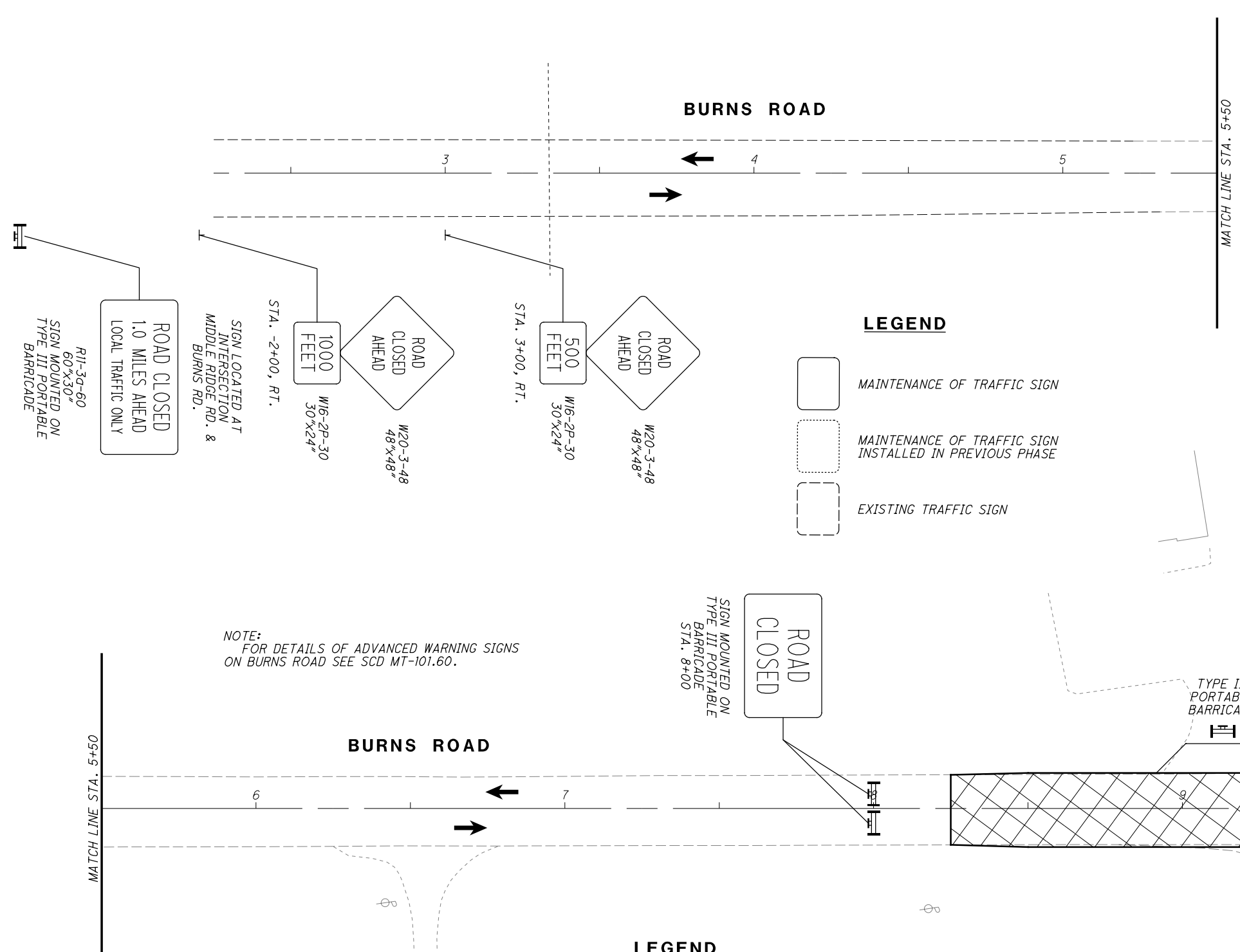
**MAINTENANCE OF TRAFFIC PLAN - PHASE 1
BURNS ROAD**

**LAK-US-20-24.99
PART 2**

LEGEND

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP3084.dgn Sheet 7/15/2022 2:51:03 PM troyer



NOTE:
 THE CONTRACTOR SHALL BE ALLOWED TO CLOSE BURNS ROAD AT STA. 517+13, RT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT BURNS ROAD REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT SIDE STREETS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

THE FOLLOWING ADJACENT SIDE STREETS WILL REMAIN OPEN DURING THE CLOSURE OF THIS SIDE STREET.

McMACKIN ROAD	STA. 433+80
HUBBARD ROAD	STA. 565+71

FOR DETOUR ROUTE SEE SHEET 28.

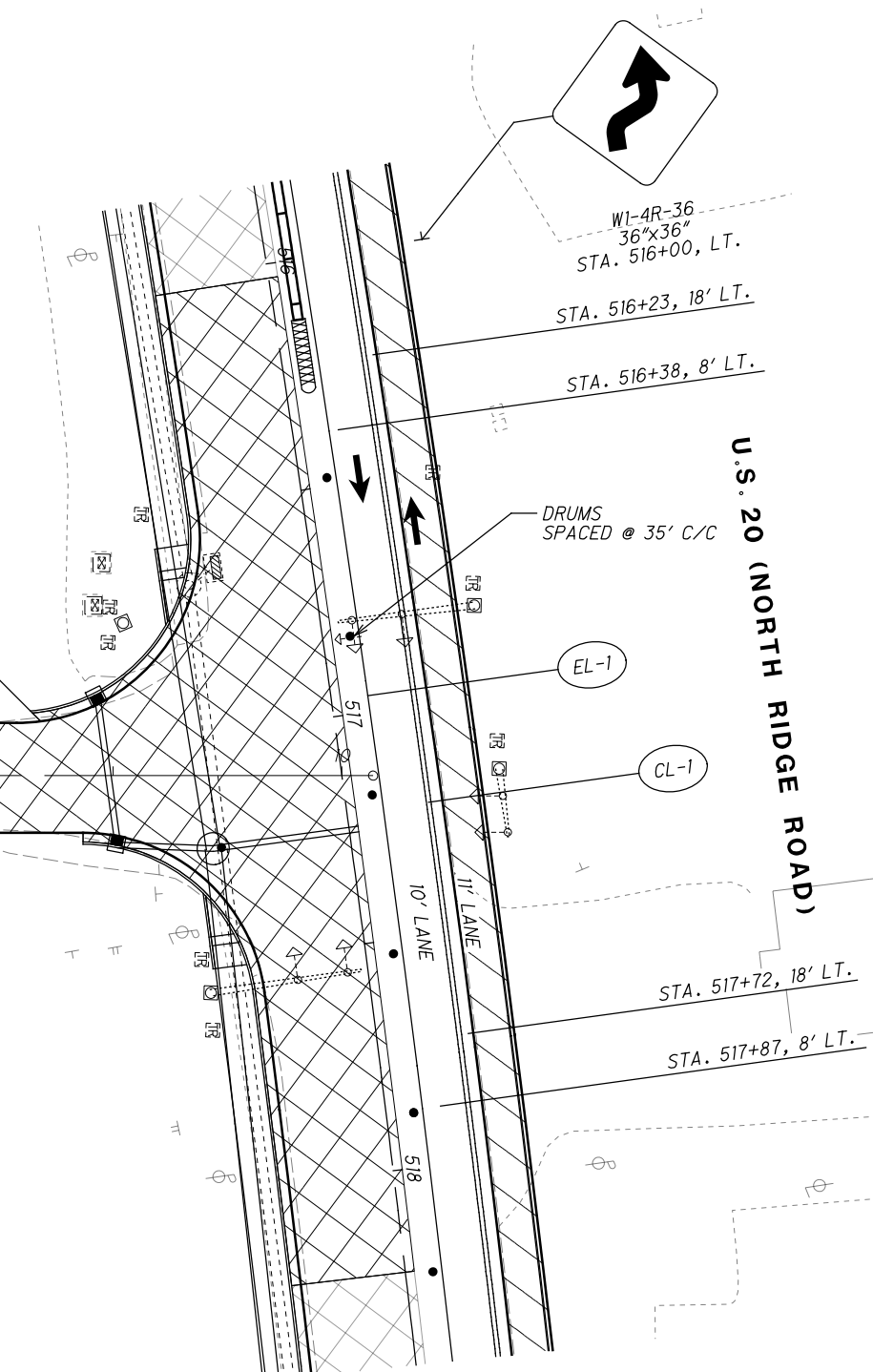


LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

NOTE:
 FOR DETAILS OF ADVANCED WARNING SIGNS ON BURNS ROAD SEE SCD MT-101.60.

- | | |
|------------------------------------------------------------|-------------------------------------------------------------------|
| ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1 |
| ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | TRAFFIC FLOW |






MAINTENANCE OF TRAFFIC PLAN - PHASE 1
BURNS ROAD












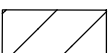


LAK-US-20-24.99
PART 2

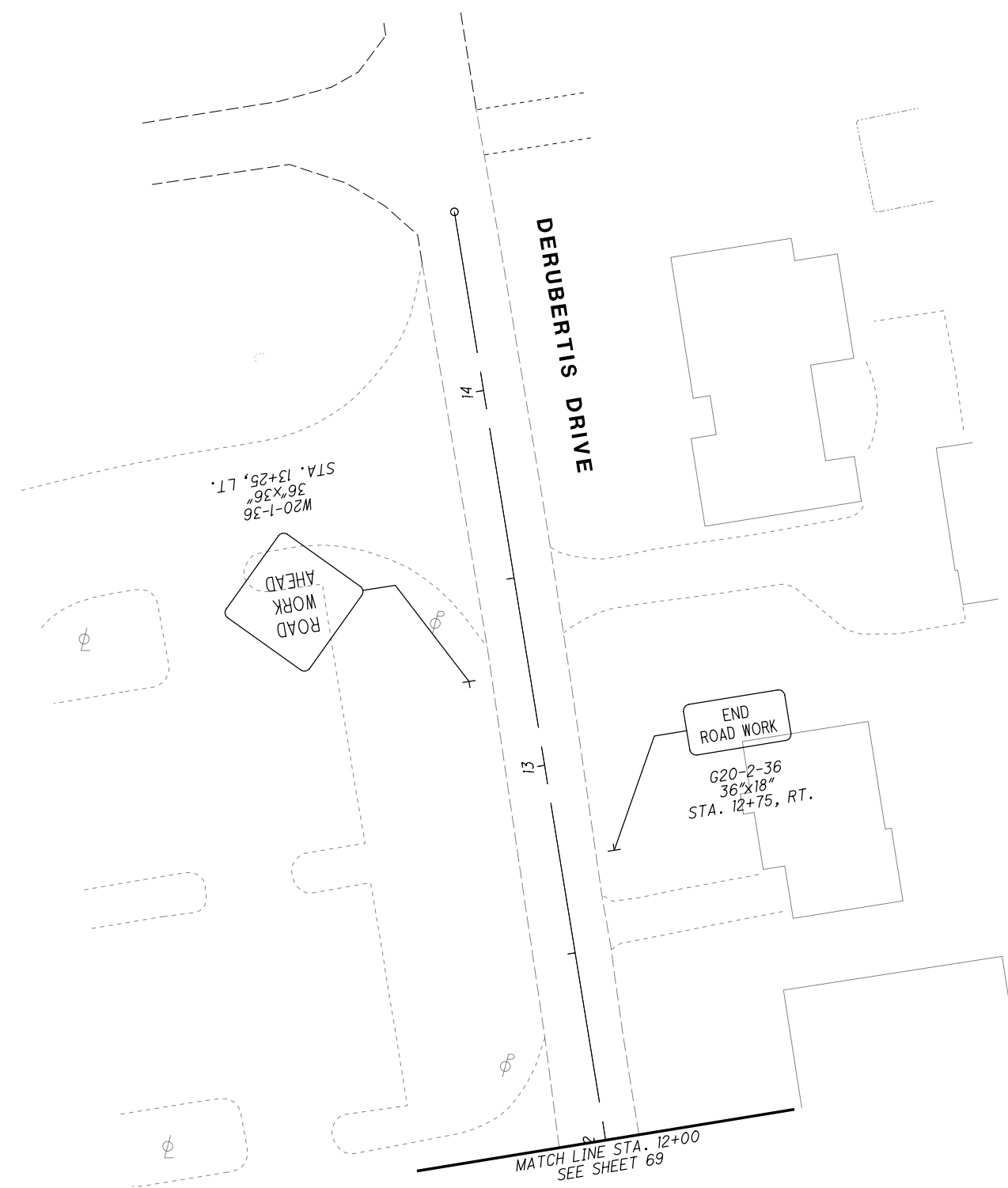
H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP094G.dgn Sheet 7/15/2022 2:51:04 PM Troyer

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

LEGEND

- | | |
|------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |



CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
DERUBERTIS DRIVE

LAK-US-20-24.99
PART 2

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP094H.dgn Sheet 7/15/2022 2:51:04 PM Troyer

CALCULATED
EJT
CHECKED
WDB

0 10 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1
HUBBARD ROAD

LAK-US-20-24.99
PART 2



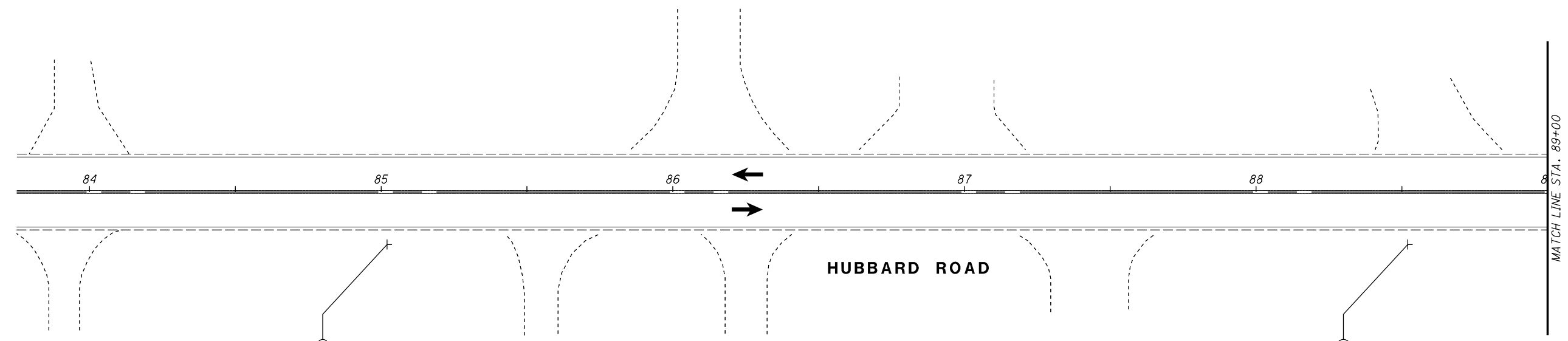
LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW



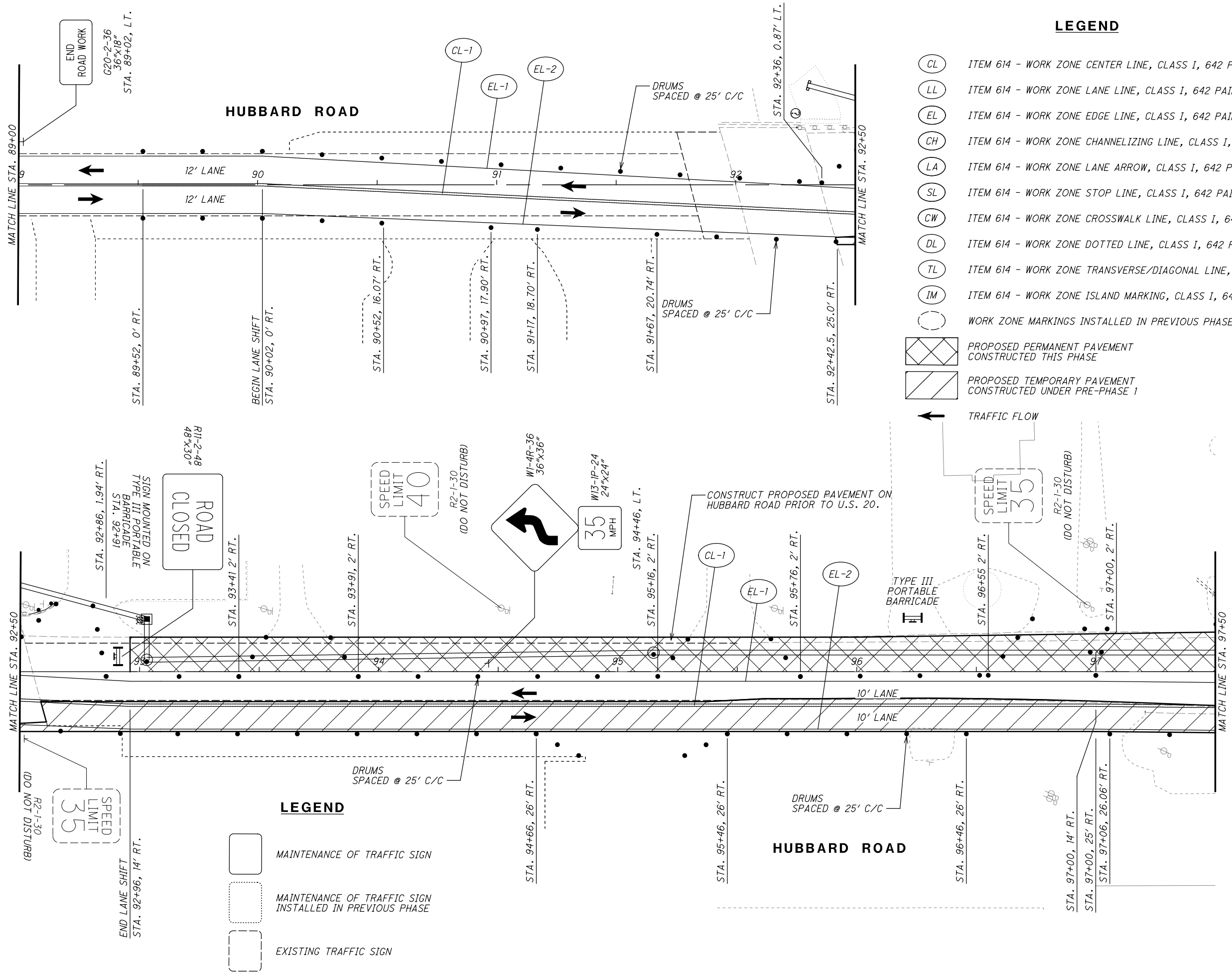
LEGEND

	MAINTENANCE OF TRAFFIC SIGN
	MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
	EXISTING TRAFFIC SIGN

LEGEND

	ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
	WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
	PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
	PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
	TRAFFIC FLOW

<p>HORIZONTAL SCALE IN FEET</p>				
<table border="1" style="border-collapse: collapse; width: 100%;"> <tr> <td style="font-size: 8px;">CALCULATED</td> <td style="font-size: 8px;">EJT</td> </tr> <tr> <td style="font-size: 8px;">CHECKED</td> <td style="font-size: 8px;">WDB</td> </tr> </table>	CALCULATED	EJT	CHECKED	WDB
CALCULATED	EJT			
CHECKED	WDB			
MAINTENANCE OF TRAFFIC PLAN - PHASE 1A HUBBARD RD - STA. 84+00 TO STA. 89+00				
LAK-US-20-24.99 PART 2				
<table border="1" style="border-collapse: collapse; width: 40px; height: 40px; margin: auto;"> <tr> <td style="text-align: center; font-weight: bold;">85</td> </tr> <tr> <td style="text-align: center; font-weight: bold;">697</td> </tr> </table>	85	697		
85				
697				



LEGEND

- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- () WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- (X) PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- (/) PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1

LEGEND

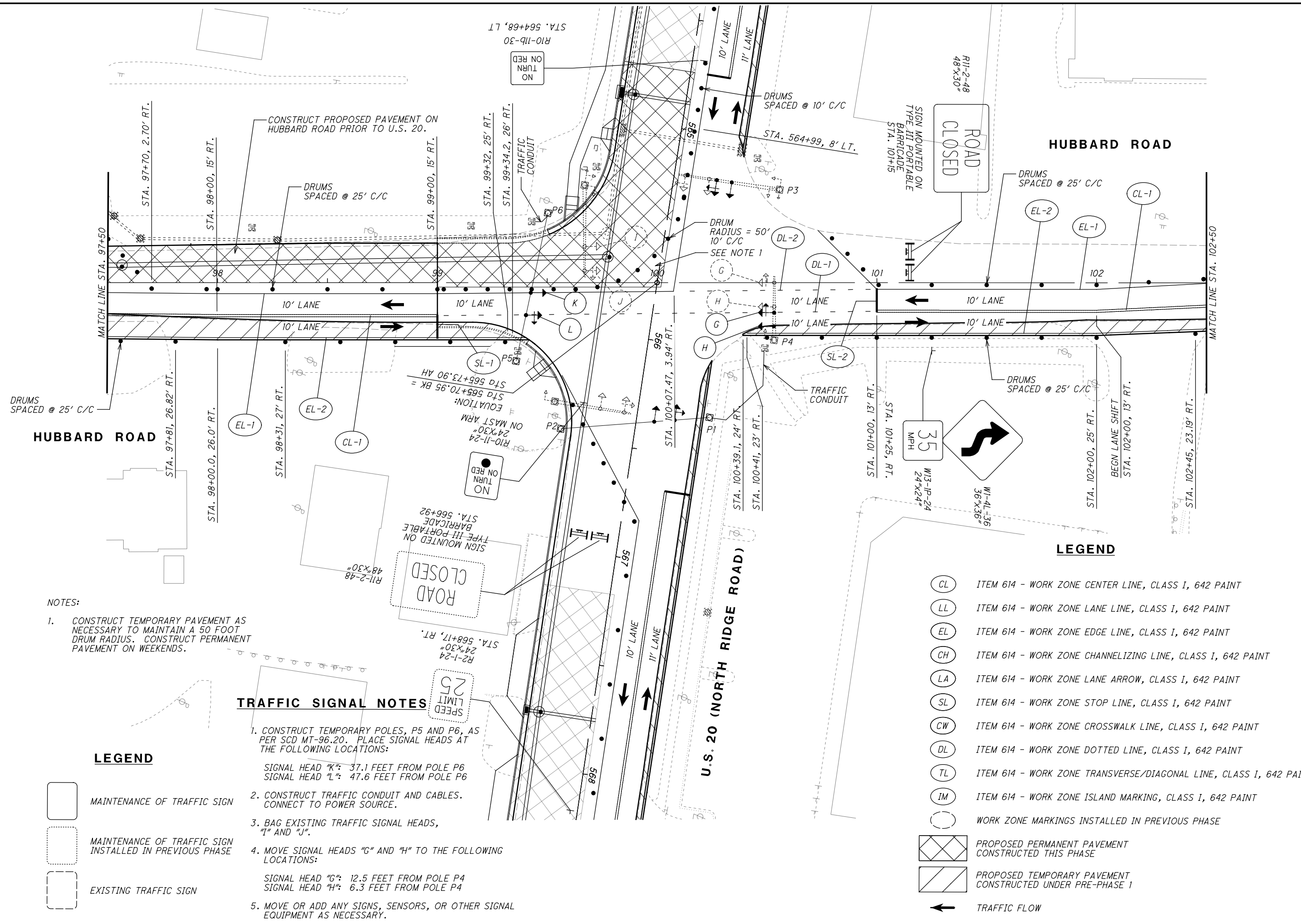
- () MAINTENANCE OF TRAFFIC SIGN
- () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- () EXISTING TRAFFIC SIGN



MAINTENANCE OF TRAFFIC PLAN - PHASE 1A
HUBBARD RD - STA. 89+00 TO STA. 97+50

LAK-US-20-24.99
PART 2

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP096.dgn Sheet 7/15/2022 2:51:07 PM troyer



NOTES:

- CONSTRUCT TEMPORARY PAVEMENT AS NECESSARY TO MAINTAIN A 50 FOOT DRUM RADIUS. CONSTRUCT PERMANENT PAVEMENT ON WEEKENDS.

TRAFFIC SIGNAL NOTES

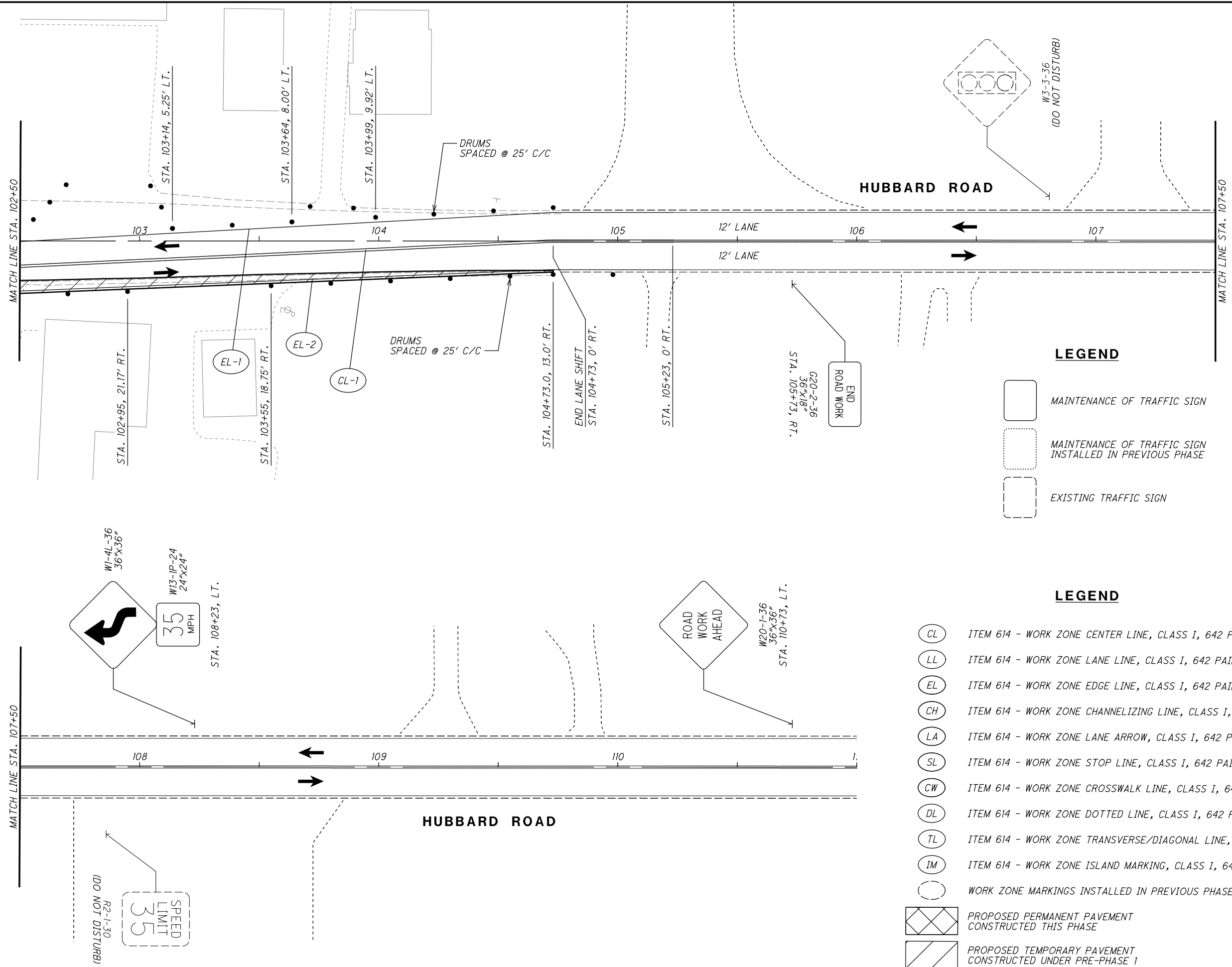
- CONSTRUCT TEMPORARY POLES, P5 AND P6, AS PER SCD MT-96.20. PLACE SIGNAL HEADS AT THE FOLLOWING LOCATIONS:
 SIGNAL HEAD "K": 37.1 FEET FROM POLE P6
 SIGNAL HEAD "L": 47.6 FEET FROM POLE P6
- CONSTRUCT TRAFFIC CONDUIT AND CABLES. CONNECT TO POWER SOURCE.
- BAG EXISTING TRAFFIC SIGNAL HEADS, "I" AND "J".
- MOVE SIGNAL HEADS "G" AND "H" TO THE FOLLOWING LOCATIONS:
 SIGNAL HEAD "G": 12.5 FEET FROM POLE P4
 SIGNAL HEAD "H": 6.3 FEET FROM POLE P4
- MOVE OR ADD ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.

LEGEND




- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

LEGEND













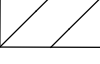

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW

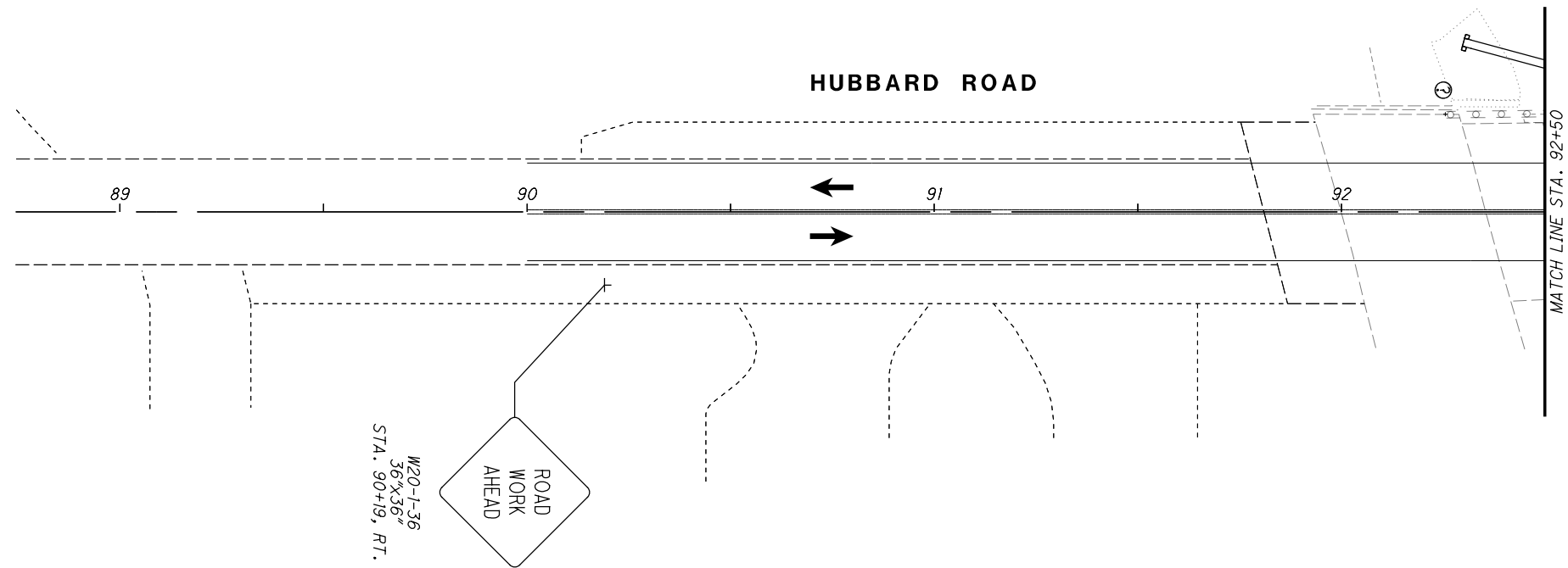


LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

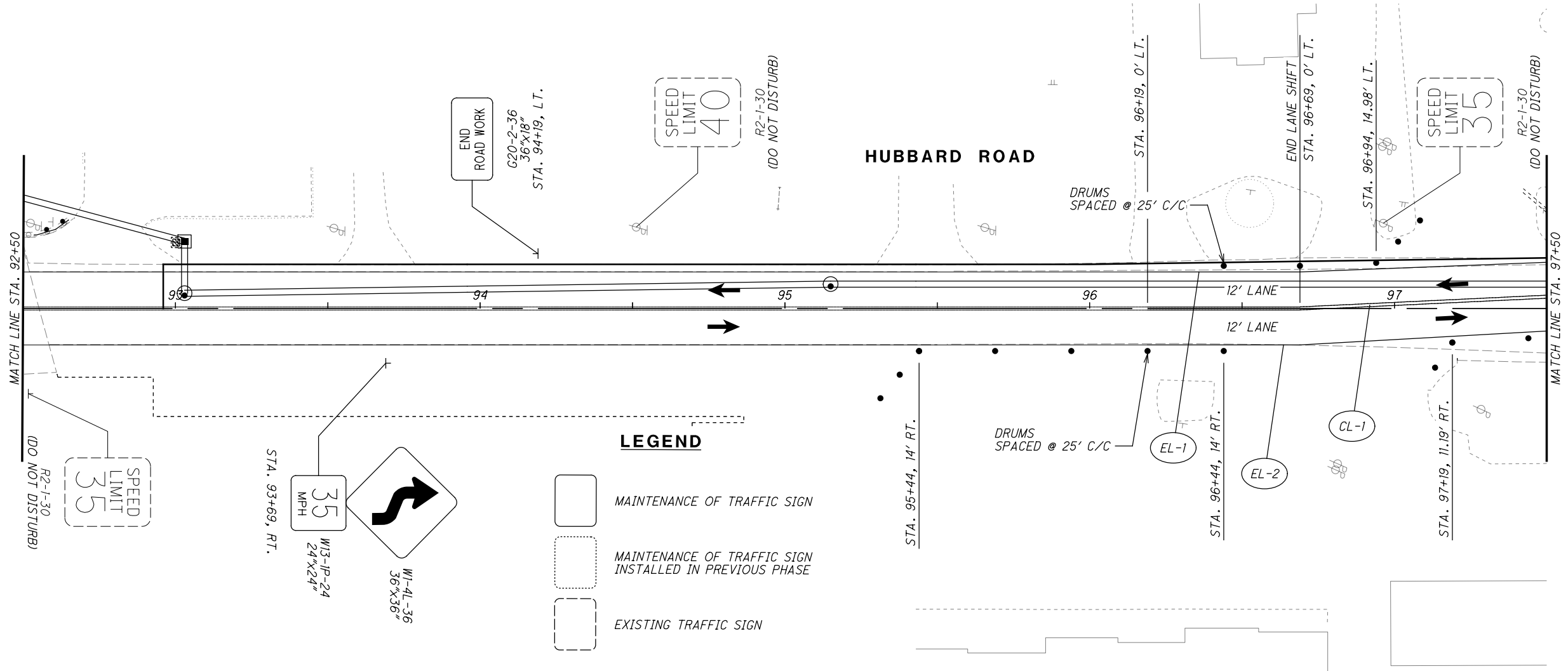
LEGEND

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
-  TRAFFIC FLOW



LEGEND

- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- () WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- (X) PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- (/) PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- (←) TRAFFIC FLOW



LEGEND

- () MAINTENANCE OF TRAFFIC SIGN
- () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- () EXISTING TRAFFIC SIGN

MAINTENANCE OF TRAFFIC PLAN - PHASE 1B
HUBBARD RD STA. 89+50 TO STA. 97+50

LAK-US-20-24.99
PART 2

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP097.dgn Sheet 7/15/2022 2:51:09 PM troyer

TRAFFIC SIGNAL NOTES

1. CONSTRUCT TEMPORARY POLES, P7 AND P8, AS PER SCD MT-96.20. PLACE SIGNAL HEADS AT THE FOLLOWING LOCATIONS:
 SIGNAL HEAD "M": 24.6 FEET FROM POLE P7
 SIGNAL HEAD "N": 30.9 FEET FROM POLE P7
2. CONSTRUCT TRAFFIC CONDUIT AND CABLES. CONNECT TO POWER SOURCE.
3. BAG EXISTING SIGNAL HEADS, "G" AND "H"
4. REMOVE BAGS FROM EXISTING TRAFFIC SIGNAL HEADS, "I" AND "J".
5. REMOVE TEMPORARY SIGNAL POLES P5 AND P6 AND ASSOCIATED SIGNAL HEADS AND EQUIPMENT FROM PREVIOUS PHASE.
6. MOVE OR ADD ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.

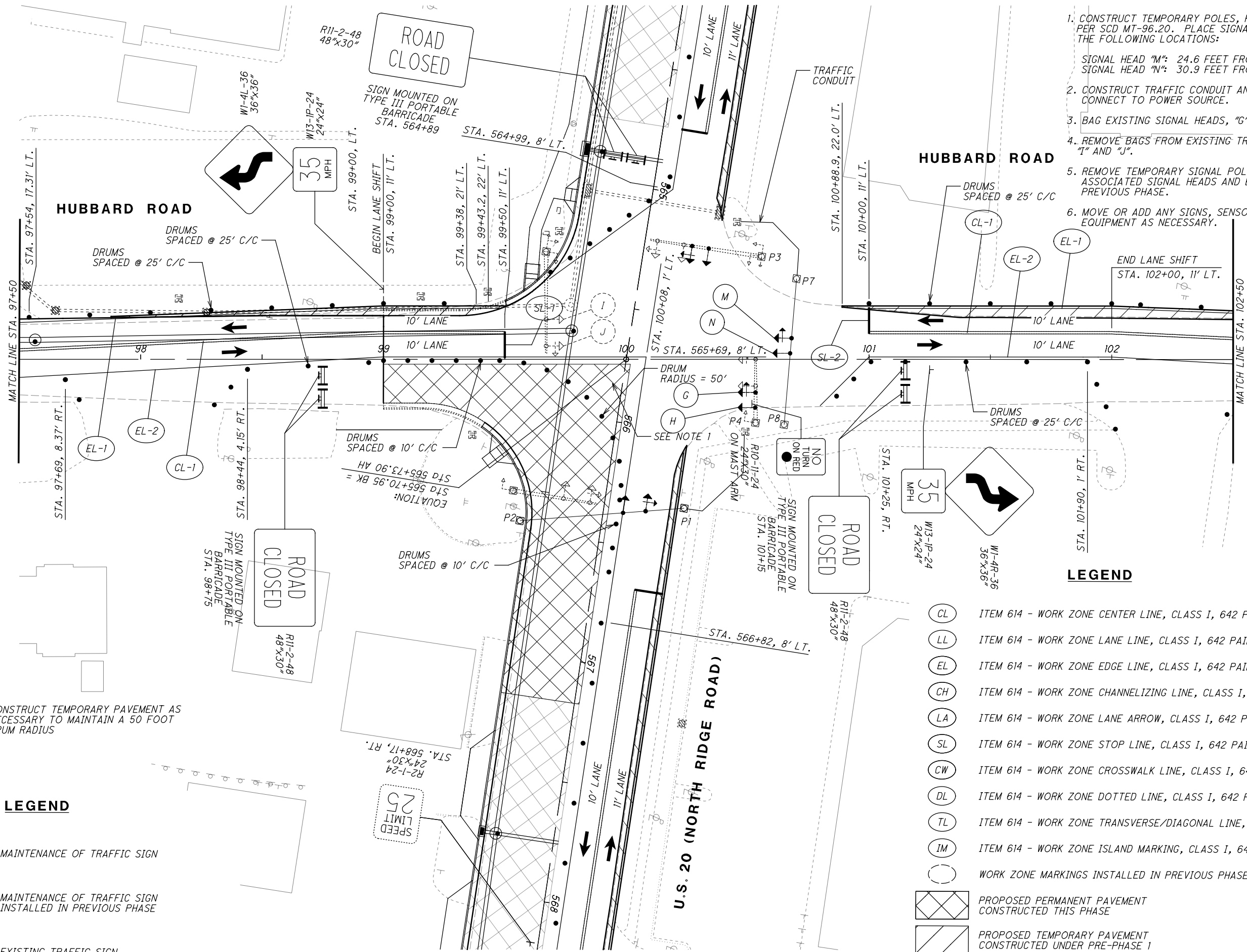
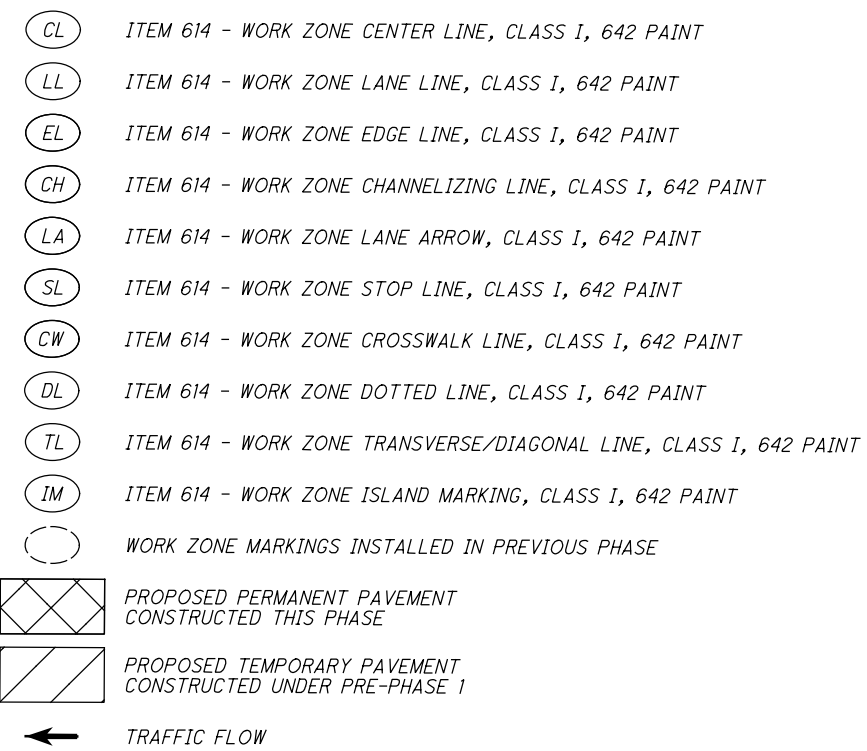
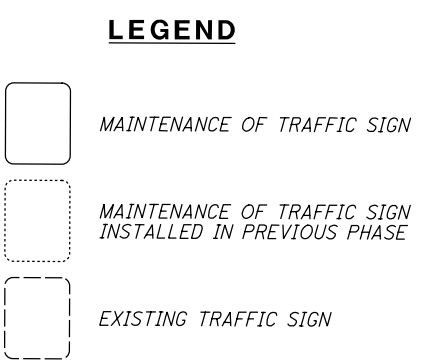


MAINTENANCE OF TRAFFIC PLAN - PHASE 1B
HUBBARD RD - STA. 97+50 TO STA. 102+50

LAK-US-20-24.99
PART 2

90
697

- NOTES:**
1. CONSTRUCT TEMPORARY PAVEMENT AS NECESSARY TO MAINTAIN A 50 FOOT DRUM RADIUS



25
SPEED LIMIT

ROAD CLOSED

ROAD CLOSED

ROAD CLOSED

35
MPH

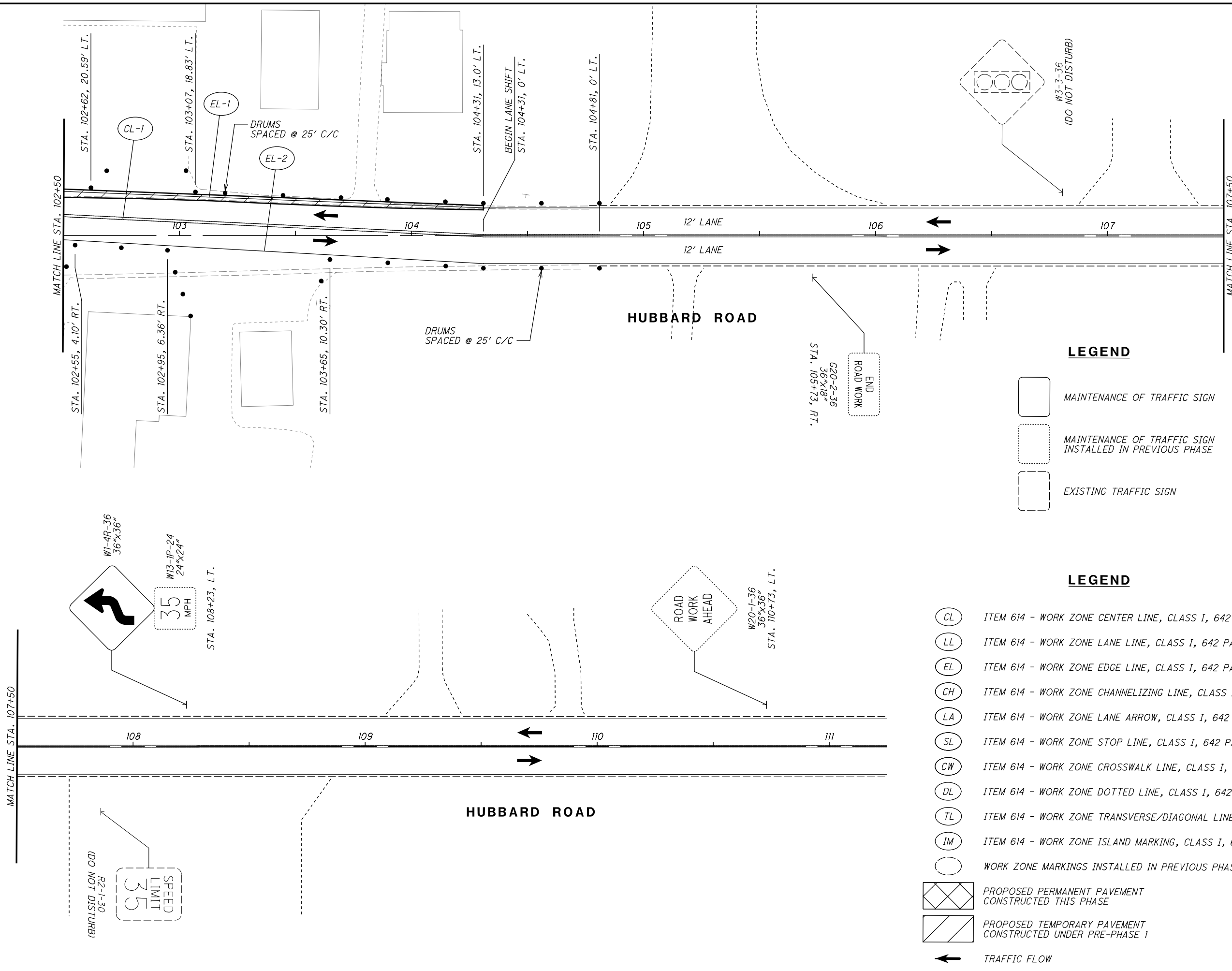
35
MPH

35
MPH




HUBBARD ROAD

HUBBARD ROAD













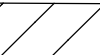

U.S. 20 (NORTH RIDGE ROAD)



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

LEGEND

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
-  TRAFFIC FLOW

CALCULATED EJT
CHECKED WDB




HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 1B
HUBBARD RD - STA. 102+50 TO STA. 110+50

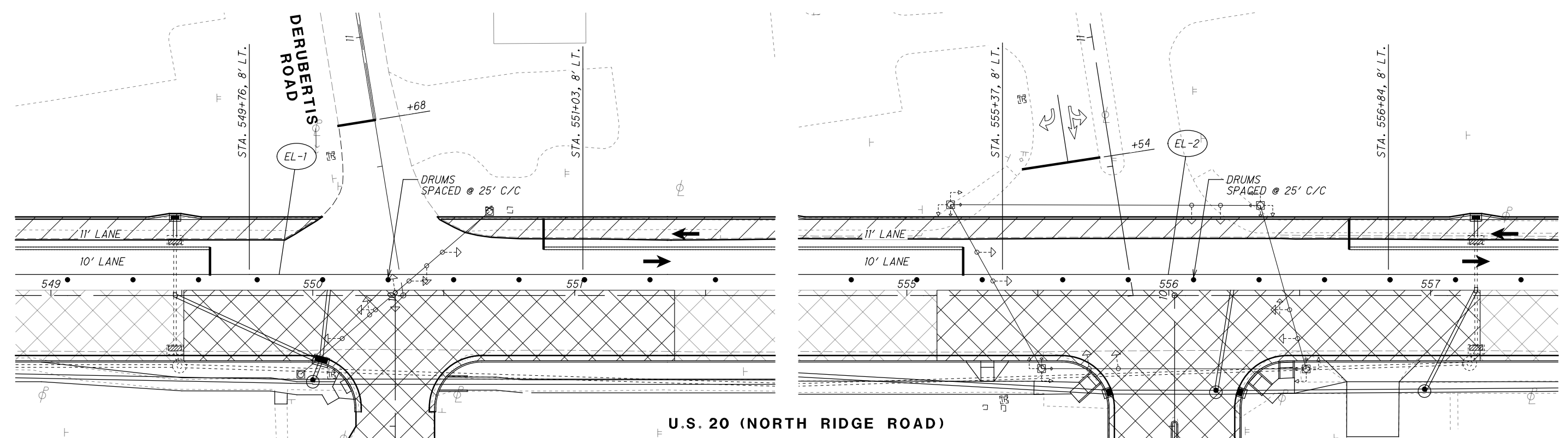
LAK-US-20-24.99
PART 2

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP3089.dgn Sheet 7/15/2022 2:51:13 PM troyer



MAINTENANCE OF TRAFFIC PLAN - PHASE 1
MAJOR ACCESS DRIVEWAYS

LAK-US-20-24.99
PART 2



U.S. 20 (NORTH RIDGE ROAD)

ROAD CLOSED
R11-2-48
48"x30"

SIGN MOUNTED ON
TYPE III PORTABLE
BARRICADE
STA. 9+40

ROAD CLOSED
R11-2-48
48"x30"

SIGN MOUNTED ON
TYPE III PORTABLE
BARRICADE
STA. 9+05

NOTE:
THE CONTRACTOR SHALL BE ALLOWED TO CLOSE THE MAJOR ACCESS DRIVEWAY AT STA. 550+31, RT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT THE DRIVEWAY REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT MAJOR DRIVEWAYS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

THE FOLLOWING ADJACENT MAJOR DRIVEWAYS WILL REMAIN OPEN DURING THE CLOSURE OF THIS DRIVE.

MAJOR ACCESS DRIVEWAY STA. 556+02

NOTE:
THE CONTRACTOR SHALL BE ALLOWED TO CLOSE THE MAJOR ACCESS DRIVEWAY AT STA. 556+02, RT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT THE DRIVEWAY REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT MAJOR DRIVEWAYS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

THE FOLLOWING ADJACENT MAJOR DRIVEWAYS WILL REMAIN OPEN DURING THE CLOSURE OF THIS DRIVE.

MAJOR ACCESS DRIVEWAY STA. 550+31

LEGEND




- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN








LEGEND

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 1
- TRAFFIC FLOW





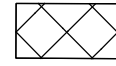
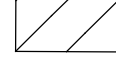

H:\2018\80743\DOT\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP283E.dgn Sheet 7/15/2022 2:51:44 PM troyer

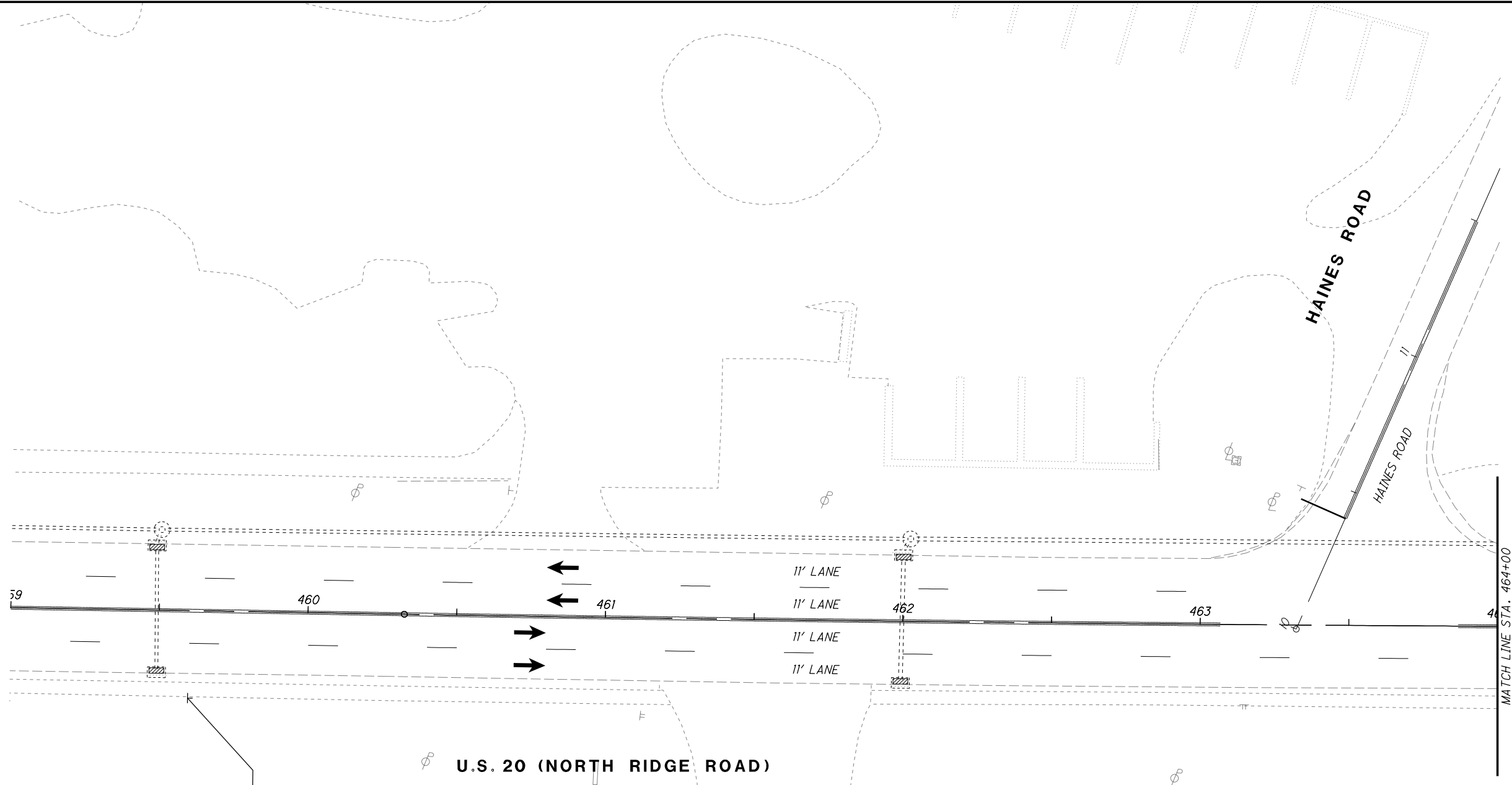
LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
-  TRAFFIC FLOW



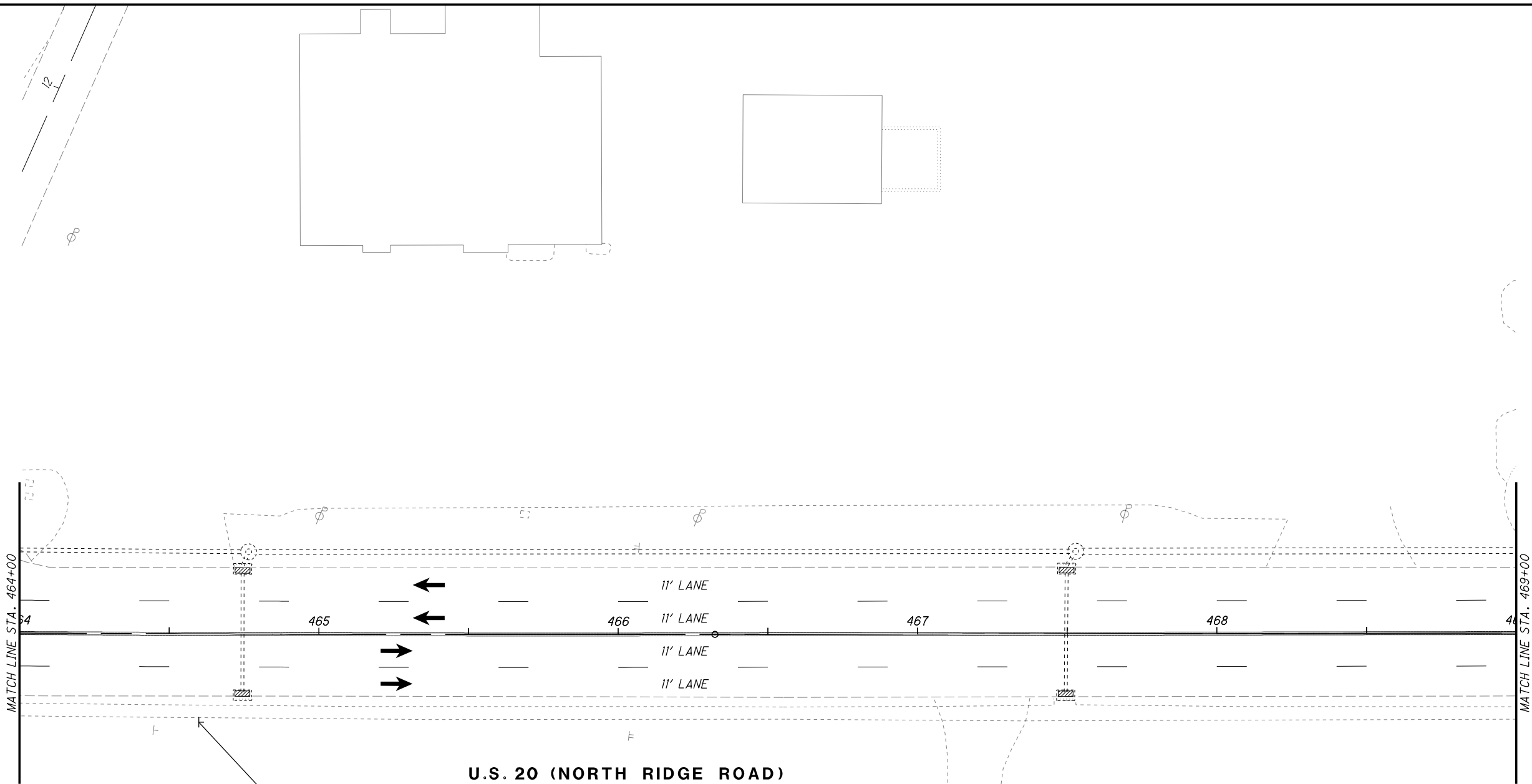
CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 459+00 TO STA. 464+00

LAK-US-20-24.99
PART 2

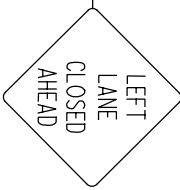


H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP284.dgn Sheet 7/15/2022 2:51:15 PM Troyer

















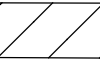


U.S. 20 (NORTH RIDGE ROAD)

W9-3L-36
36"x36"
STA. 464+60, RT.



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

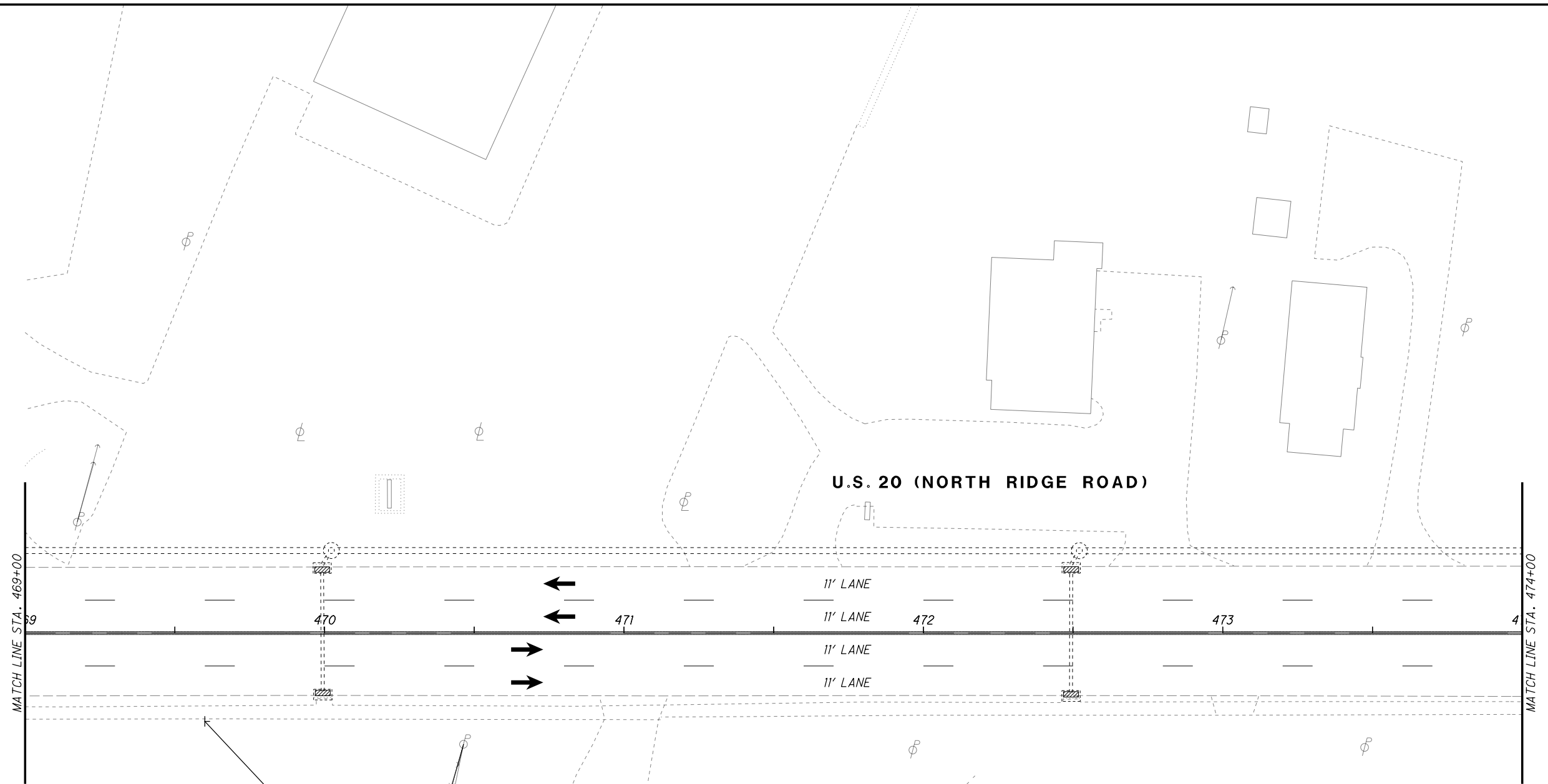
- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

CALCULATED
EJT
CHECKED
WDB

HORIZONTAL SCALE IN FEET

**MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 464+00 TO STA. 469+00**

**LAK-US-20-24.99
PART 2**



R2-1-24
24"x30"
STA. 469+60, RT.

SPEED LIMIT 45

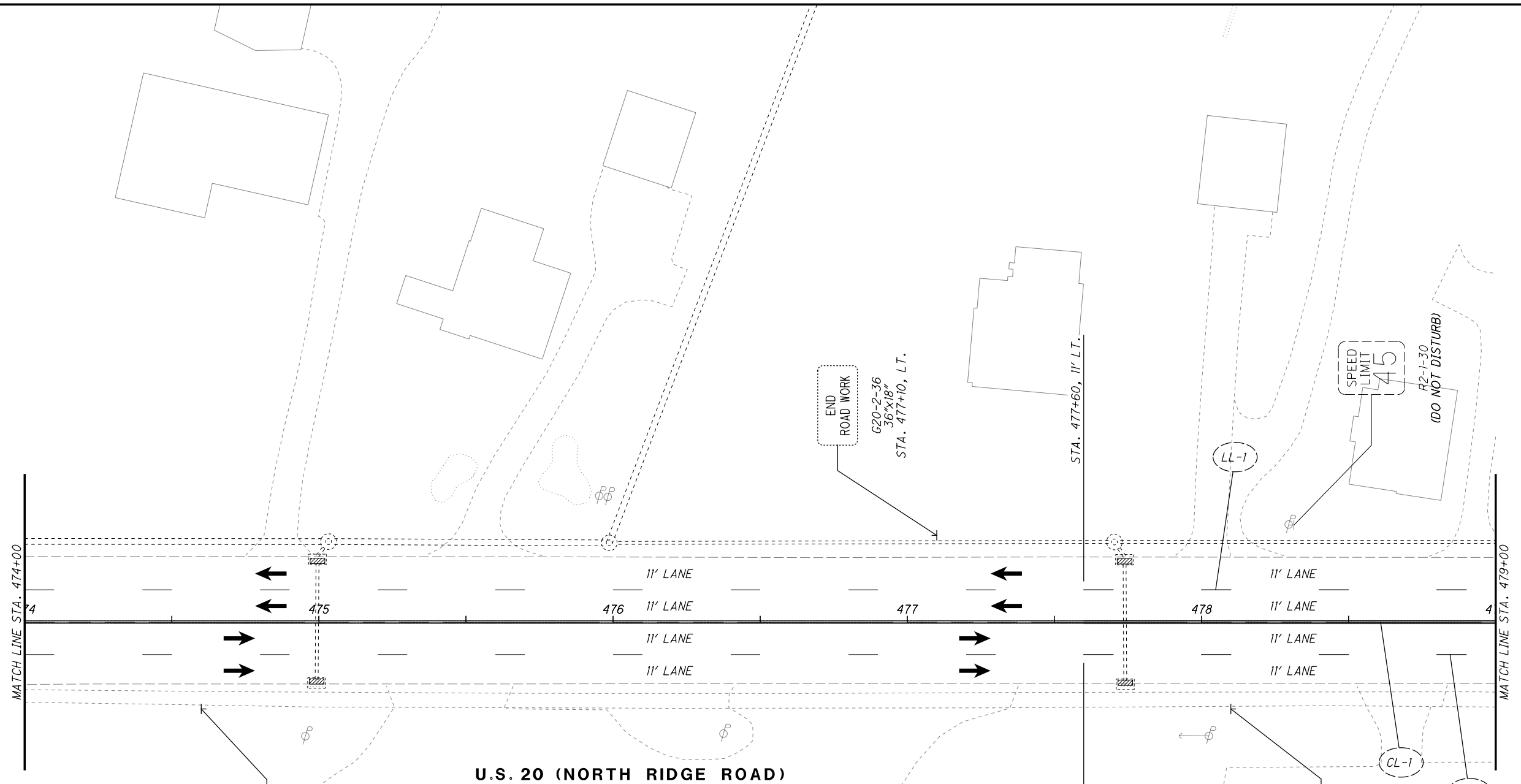
LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN




- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT








LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
- TRAFFIC FLOW






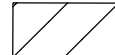



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
-  TRAFFIC FLOW

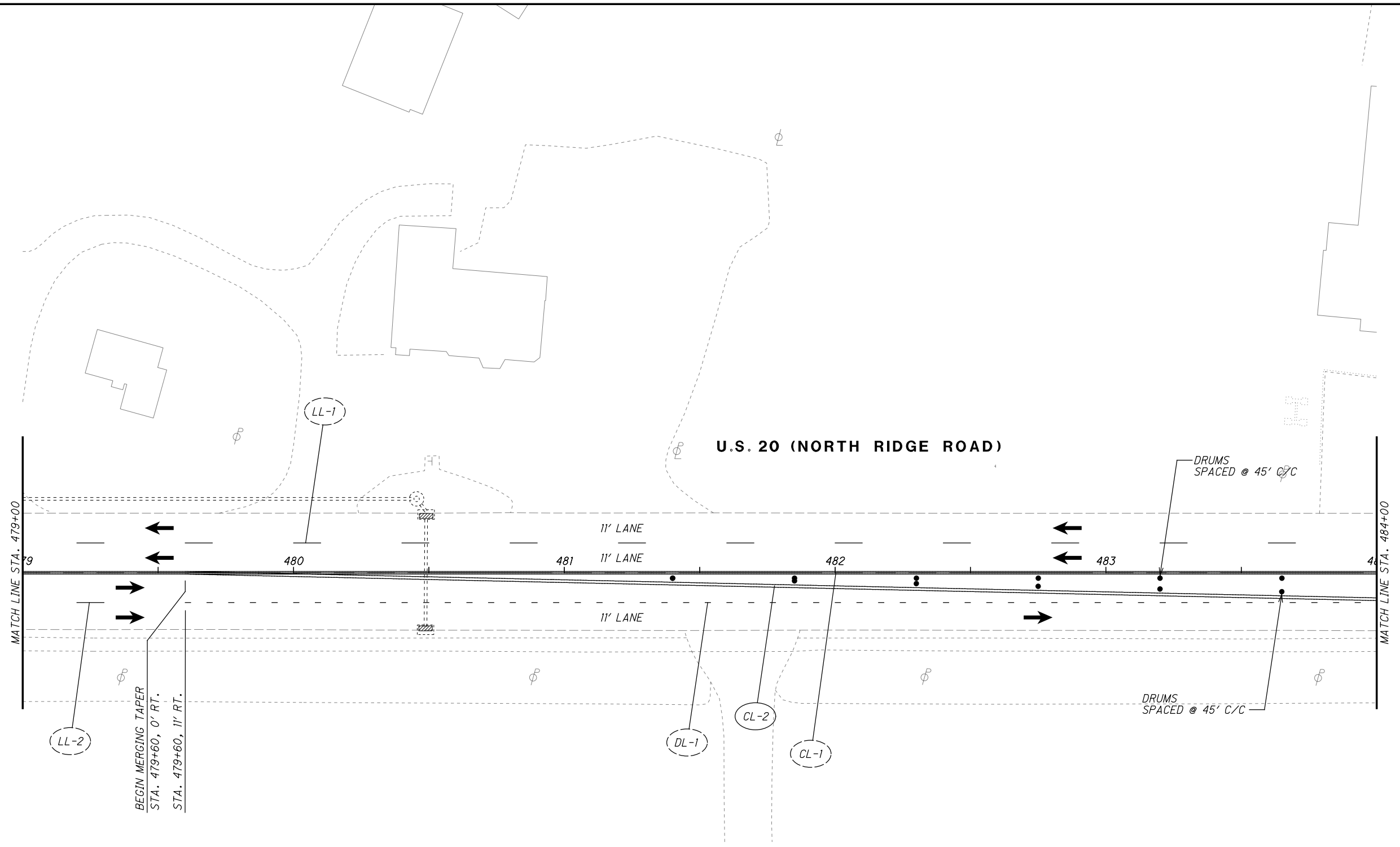
H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP284C.dgn Sheet 7/15/2022 2:51:17 PM troyer

CALCULATED
EJT
CHECKED
WDB




0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 479+00 TO STA. 484+00













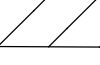

LAK-US-20-24.99
PART 2

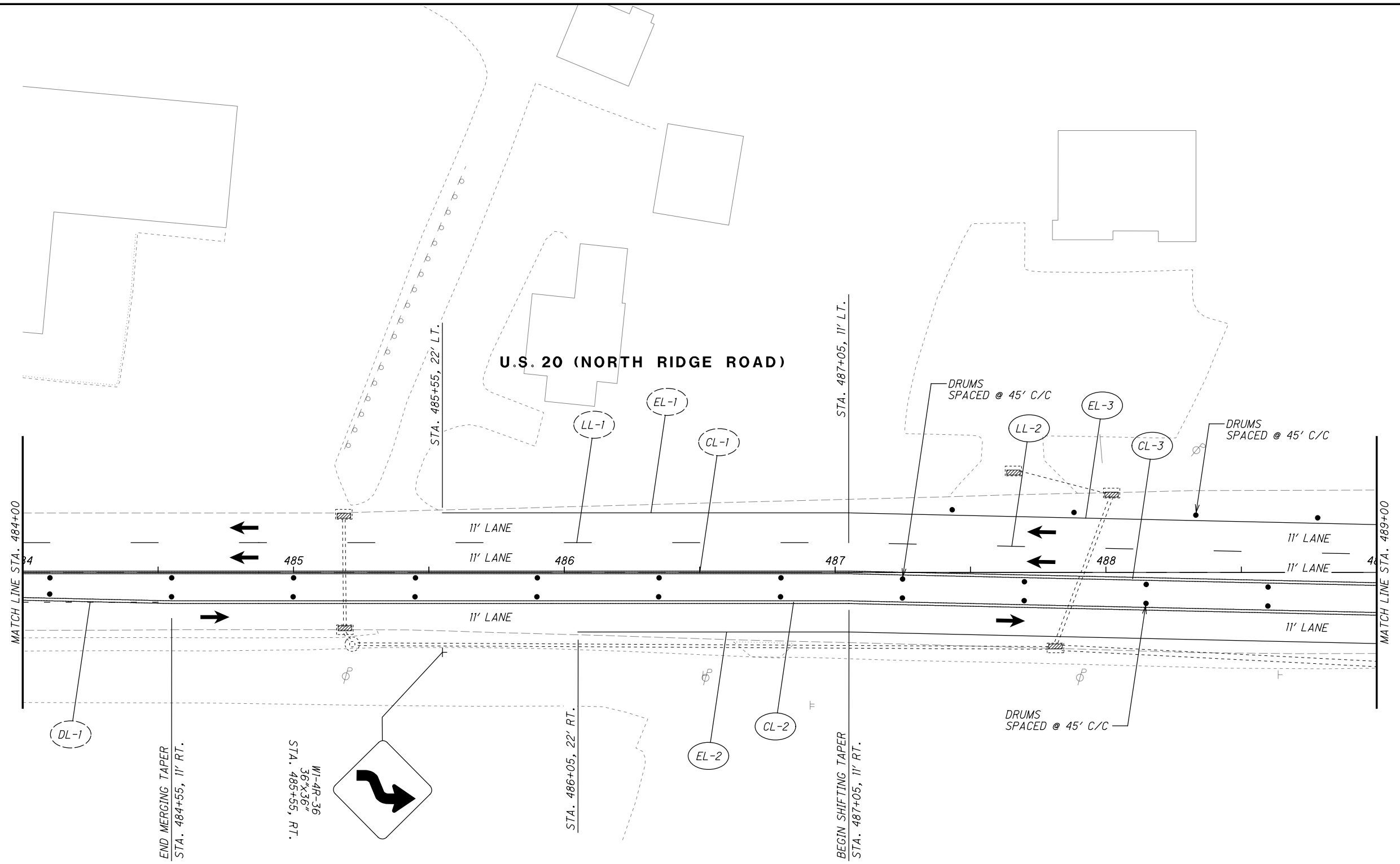


LEGEND




-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN








LEGEND






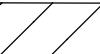

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
-  TRAFFIC FLOW



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
-  TRAFFIC FLOW

LEGEND

NOTES

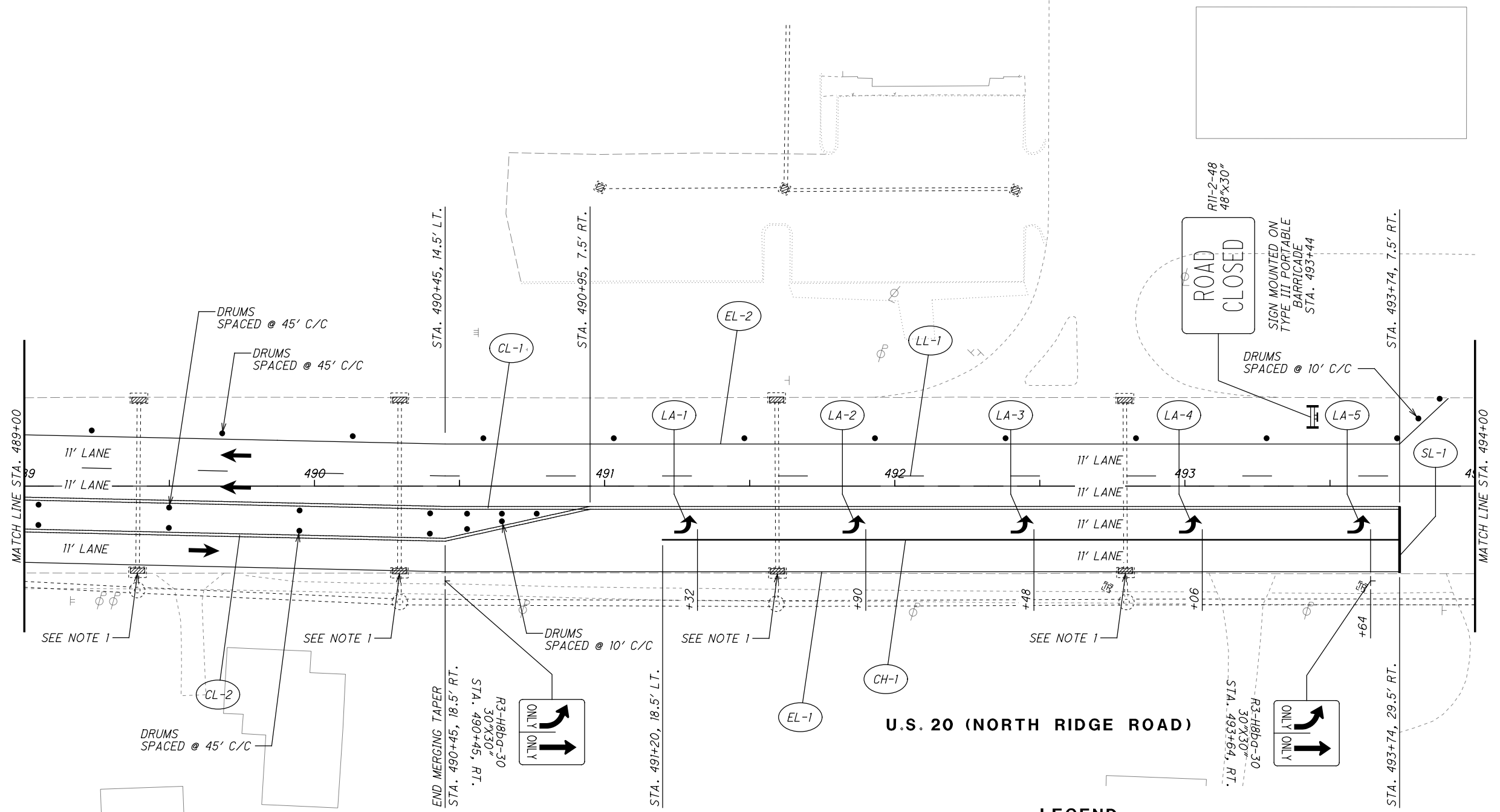
- 1. WELD GRATE TO FRAME



CALCULATED	EJT
CHECKED	WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 489+00 TO STA. 494+00

LAK-US-20-24.99
PART 2



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
- TRAFFIC FLOW

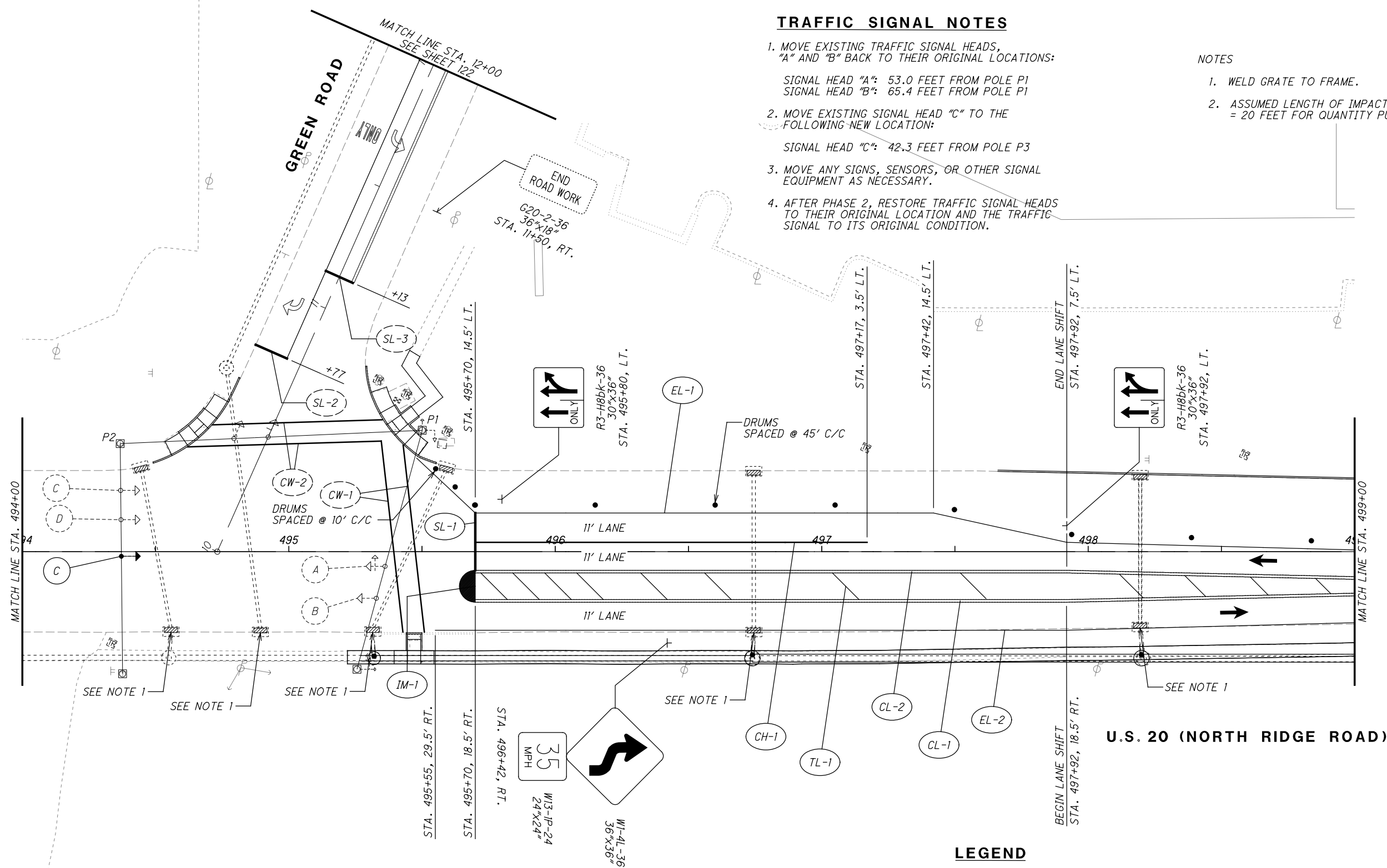
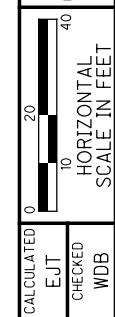
H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\01\Sheets\09270_MP284E.dgn Sheet 7/15/2022 2:51:48 PM troyer

TRAFFIC SIGNAL NOTES

1. MOVE EXISTING TRAFFIC SIGNAL HEADS, "A" AND "B" BACK TO THEIR ORIGINAL LOCATIONS:
 SIGNAL HEAD "A": 53.0 FEET FROM POLE P1
 SIGNAL HEAD "B": 65.4 FEET FROM POLE P1
2. MOVE EXISTING SIGNAL HEAD "C" TO THE FOLLOWING NEW LOCATION:
 SIGNAL HEAD "C": 42.3 FEET FROM POLE P3
3. MOVE ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.
4. AFTER PHASE 2, RESTORE TRAFFIC SIGNAL HEADS TO THEIR ORIGINAL LOCATION AND THE TRAFFIC SIGNAL TO ITS ORIGINAL CONDITION.

NOTES

1. WELD GRATE TO FRAME.
2. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- | | |
|------------------------------------------------------------|-------------------------------------------------------------------|
| ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
| ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | TRAFFIC FLOW |

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP284G.dgn Sheet 7/15/2022 2:51:19 PM troyer





 HORIZONTAL SCALE IN FEET

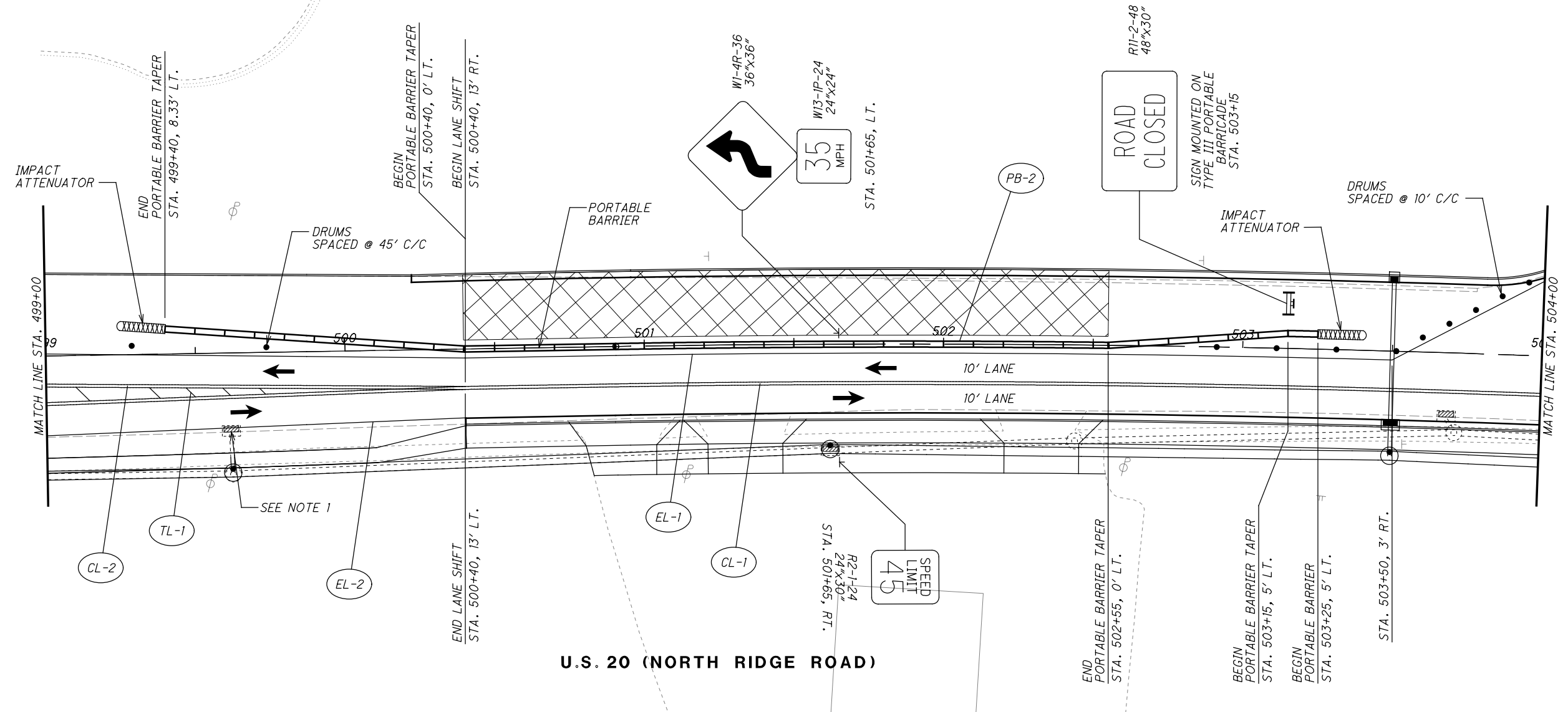
LAK-US-20-24.99
MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 499+00 TO STA. 504+00

PART 2




101
697








NOTES

1. WELD GRATE TO FRAME.
2. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.






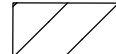



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

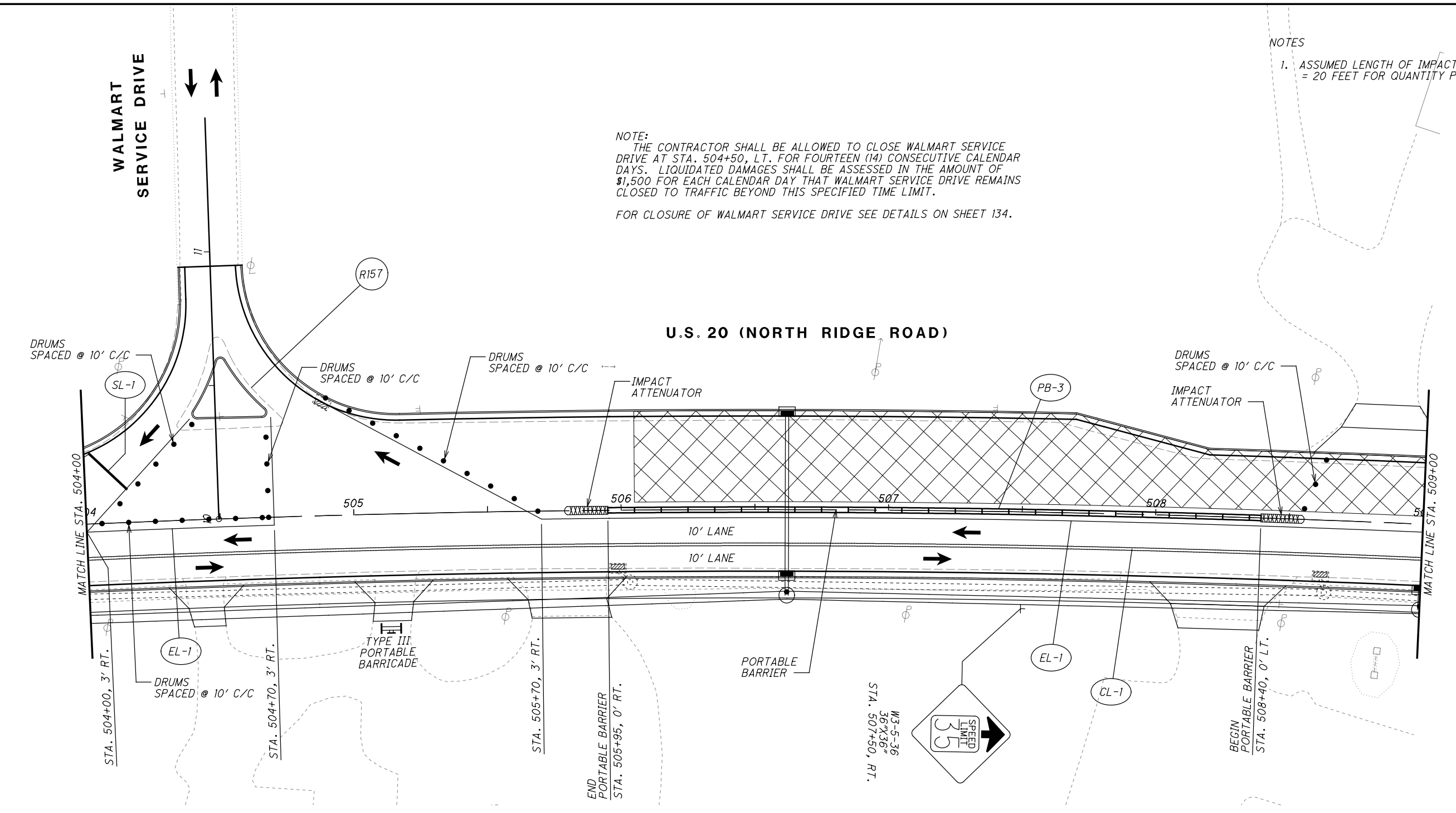
LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
-  TRAFFIC FLOW

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP284H.dgn Sheet 7/15/2022 2:51:20 PM Troyer

NOTES
1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

NOTE:
THE CONTRACTOR SHALL BE ALLOWED TO CLOSE WALMART SERVICE DRIVE AT STA. 504+50, LT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT WALMART SERVICE DRIVE REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.
FOR CLOSURE OF WALMART SERVICE DRIVE SEE DETAILS ON SHEET 134.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- | | | | |
|----|------------------------------------------------------------|----|-------------------------------------------------------------------|
| CL | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | DL | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| LL | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | TL | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| EL | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | IM | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| CH | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| LA | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| SL | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
| CW | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | | TRAFFIC FLOW |

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\01\Sheets\09270_MP2841.dgn Sheet 7/15/2022 2:51:21PM troyer

CALCULATED EJT CHECKED WDB

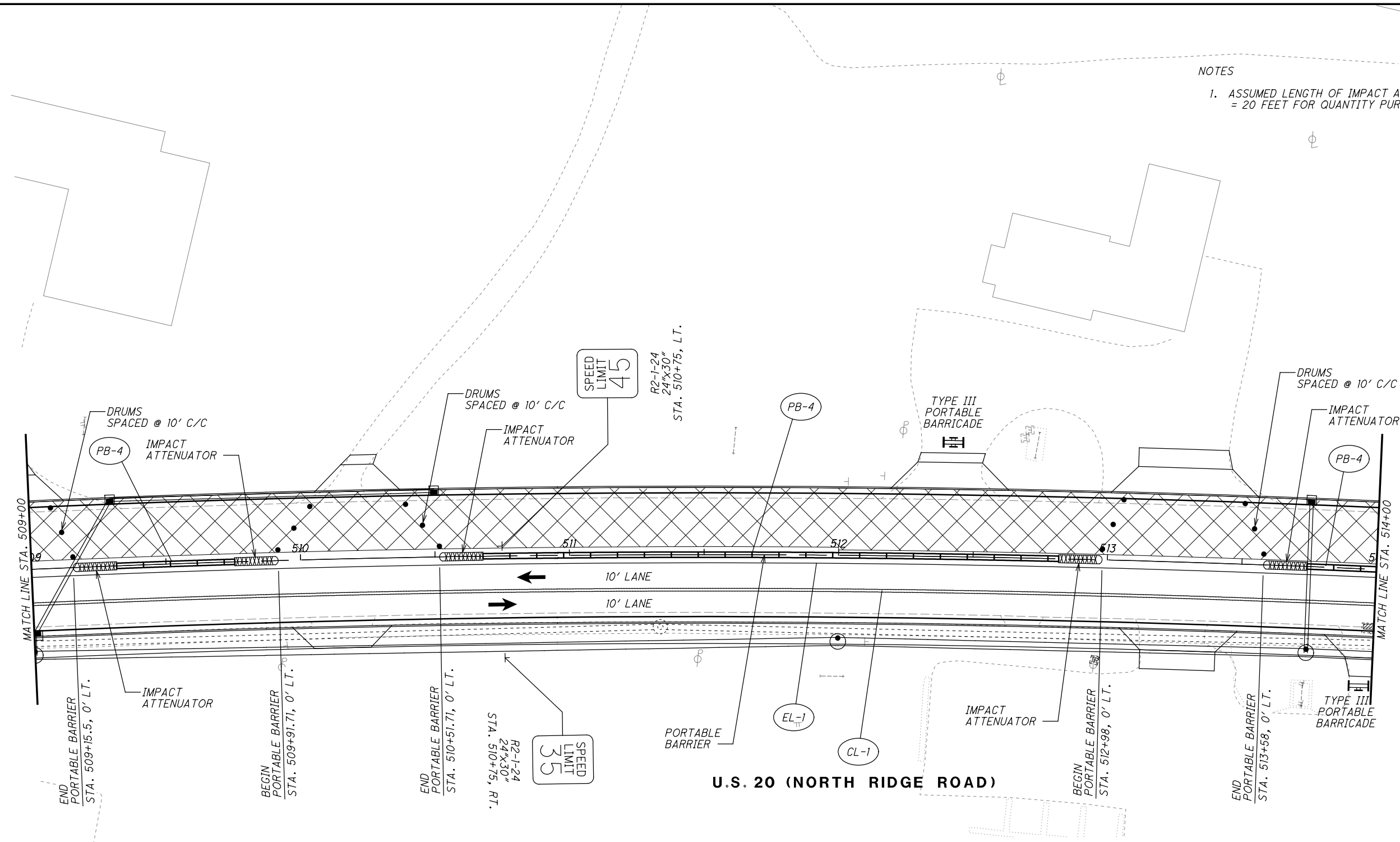
0 20 40
HORIZONTAL SCALE IN FEET

103
697

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 509+00 TO STA. 514+00

LAK-US-20-24.99
PART 2

NOTES
1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
- TRAFFIC FLOW

TRAFFIC SIGNAL NOTES

1. CONSTRUCT TEMPORARY POLES, P3 AND P4, AS PER SCD MT-96.20. PLACE SIGNAL HEADS AT THE FOLLOWING LOCATIONS:

SIGNAL HEAD "G": 37.6 FEET FROM POLE P3
SIGNAL HEAD "H": 48.0 FEET FROM POLE P3
2. CONSTRUCT TRAFFIC CONDUIT AND CABLES. CONNECT TO POWER SOURCE.
3. BAG EXISTING TRAFFIC SIGNAL HEADS, "E" AND "F".
4. REMOVE TEMPORARY SIGNAL SETUP FROM PREVIOUS PHASE (INCLUDES POLES P1 AND P2 AND SIGNAL HEADS "C" AND "D").
5. REMOVE BAGS FROM SIGNAL HEADS "A" AND "B".
6. MOVE OR ADD ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.
7. AFTER PHASE 2, RESTORE TRAFFIC SIGNAL HEADS TO THEIR ORIGINAL LOCATION AND THE TRAFFIC SIGNAL TO ITS ORIGINAL CONDITION.

NOTES

1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

LEGEND

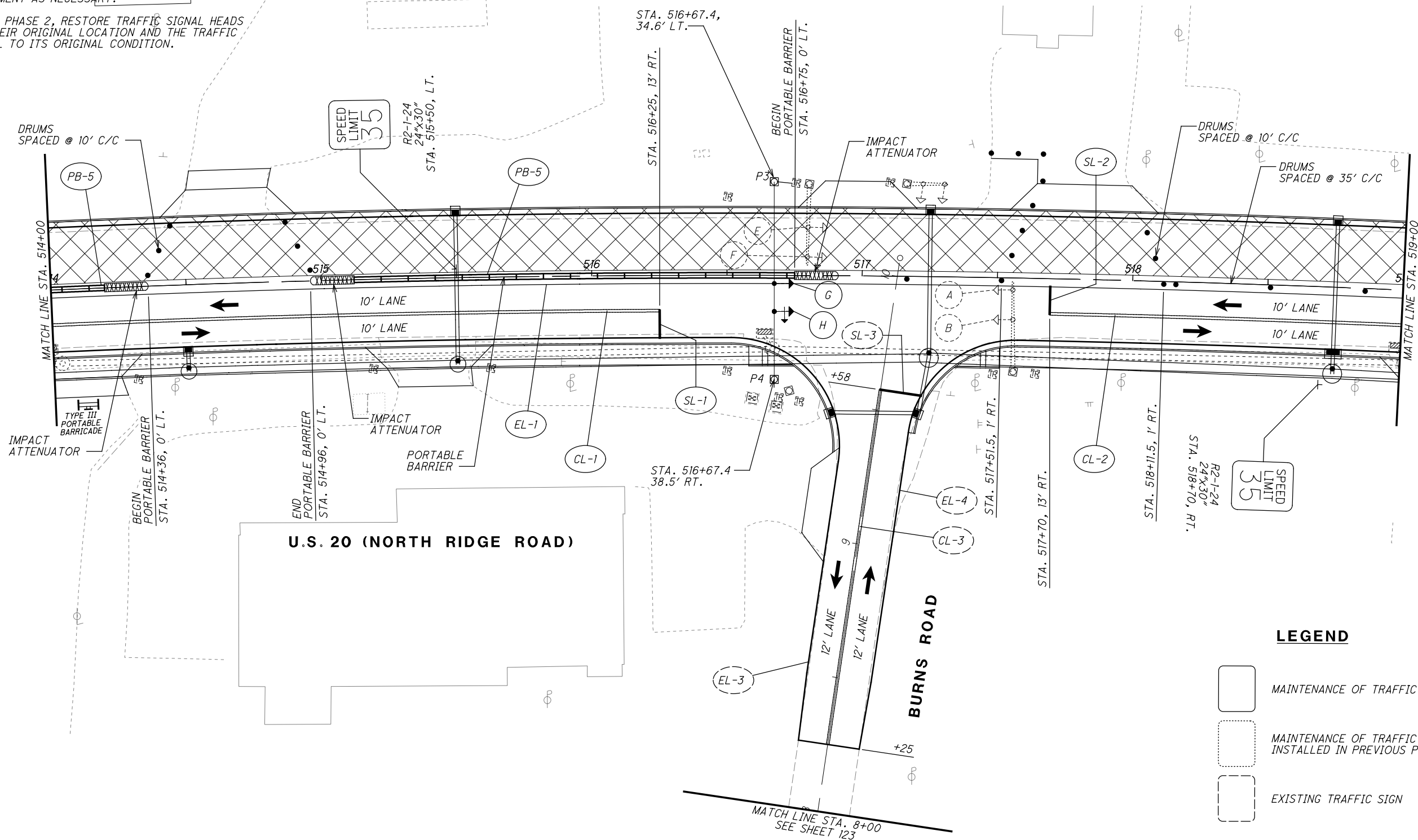
- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | (○) | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |



MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 514+00 TO STA. 519+00

LAK-US-20-24.99
PART 2

104
697



LEGEND

- (□) MAINTENANCE OF TRAFFIC SIGN
- (□) MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- (□) EXISTING TRAFFIC SIGN

H:\2018\180743\0D01\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP284J.dgn Sheet 7/15/2022 2:51:22 PM Troyer

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP284k.dgn Sheet 7/15/2022 2:51:23 PM Troyer

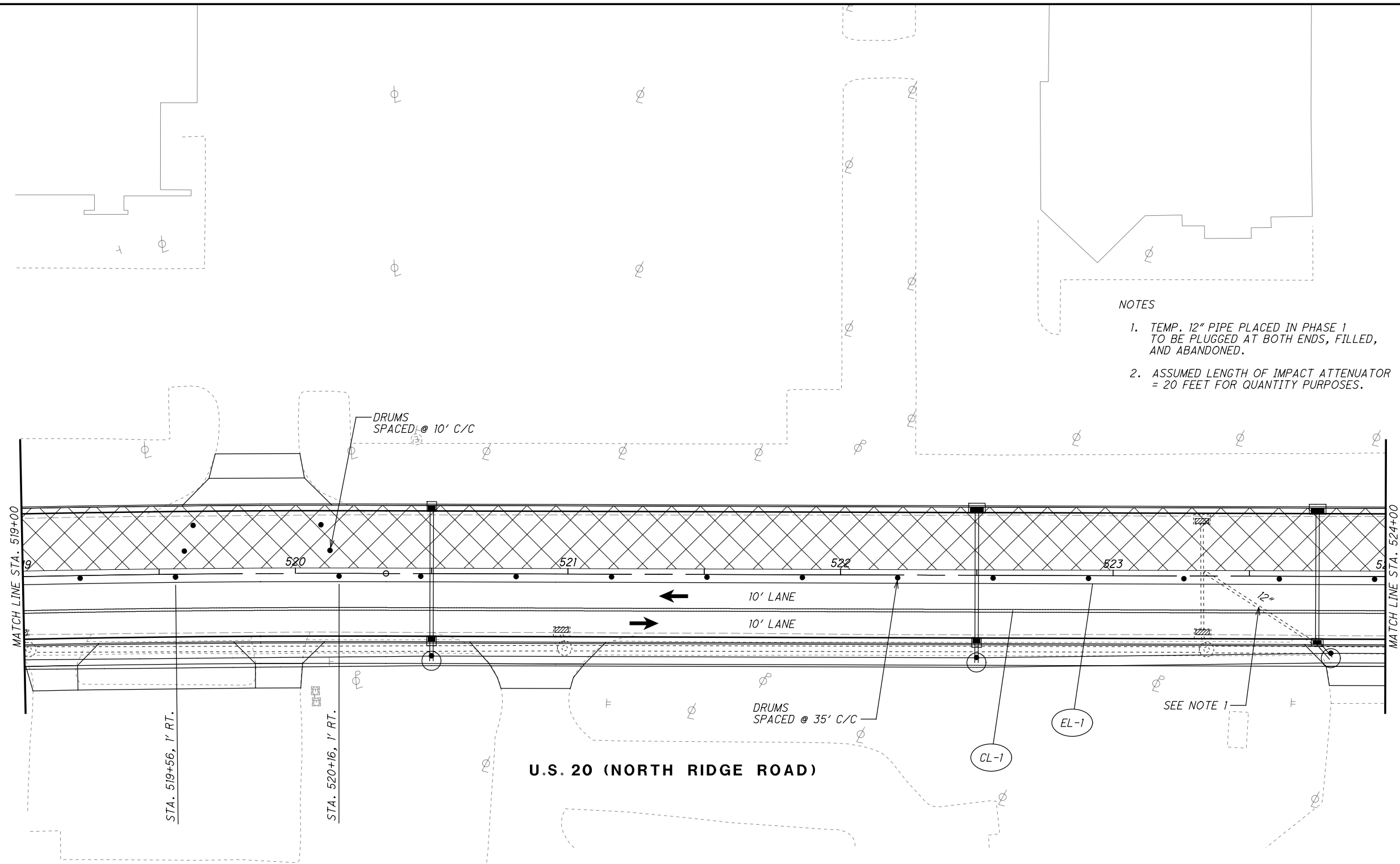
CALCULATED
EJT
CHECKED
WDB

0 20 40
10
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 519+00 TO STA. 524+00

LAK-US-20-24.99
PART 2

105
697



- NOTES**
1. TEMP. 12" PIPE PLACED IN PHASE 1 TO BE PLUGGED AT BOTH ENDS, FILLED, AND ABANDONED.
 2. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
- TRAFFIC FLOW



CALCULATED
EJT
CHECKED
WDB

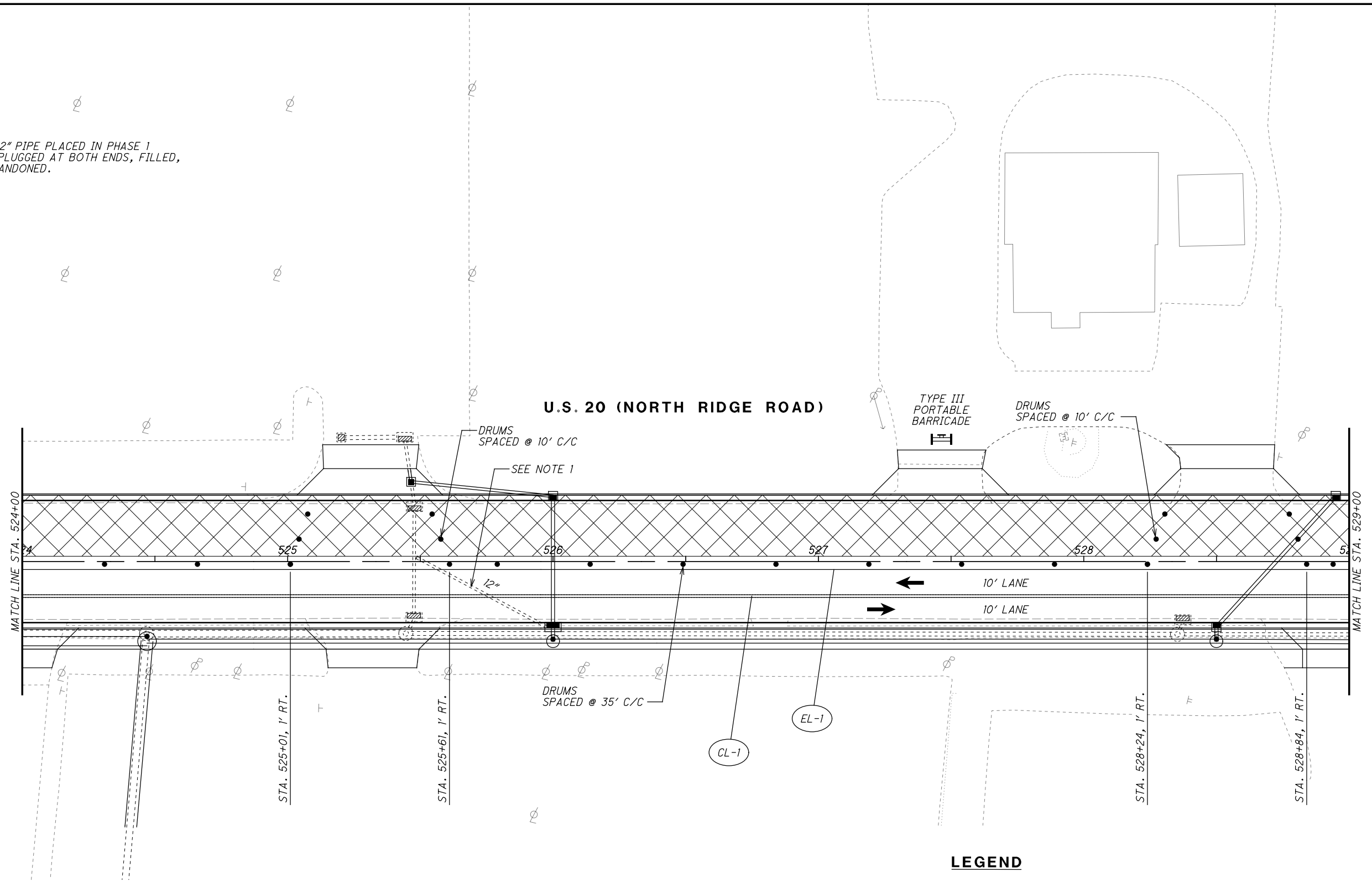
MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 524+00 TO STA. 529+00

LAK-US-20-24.99
PART 2

106
697

NOTES

- TEMP. 12" PIPE PLACED IN PHASE 1 TO BE PLUGGED AT BOTH ENDS, FILLED, AND ABANDONED.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
- TRAFFIC FLOW

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP284L.dgn Sheet 7/15/2022 2:51:23 PM Troyer

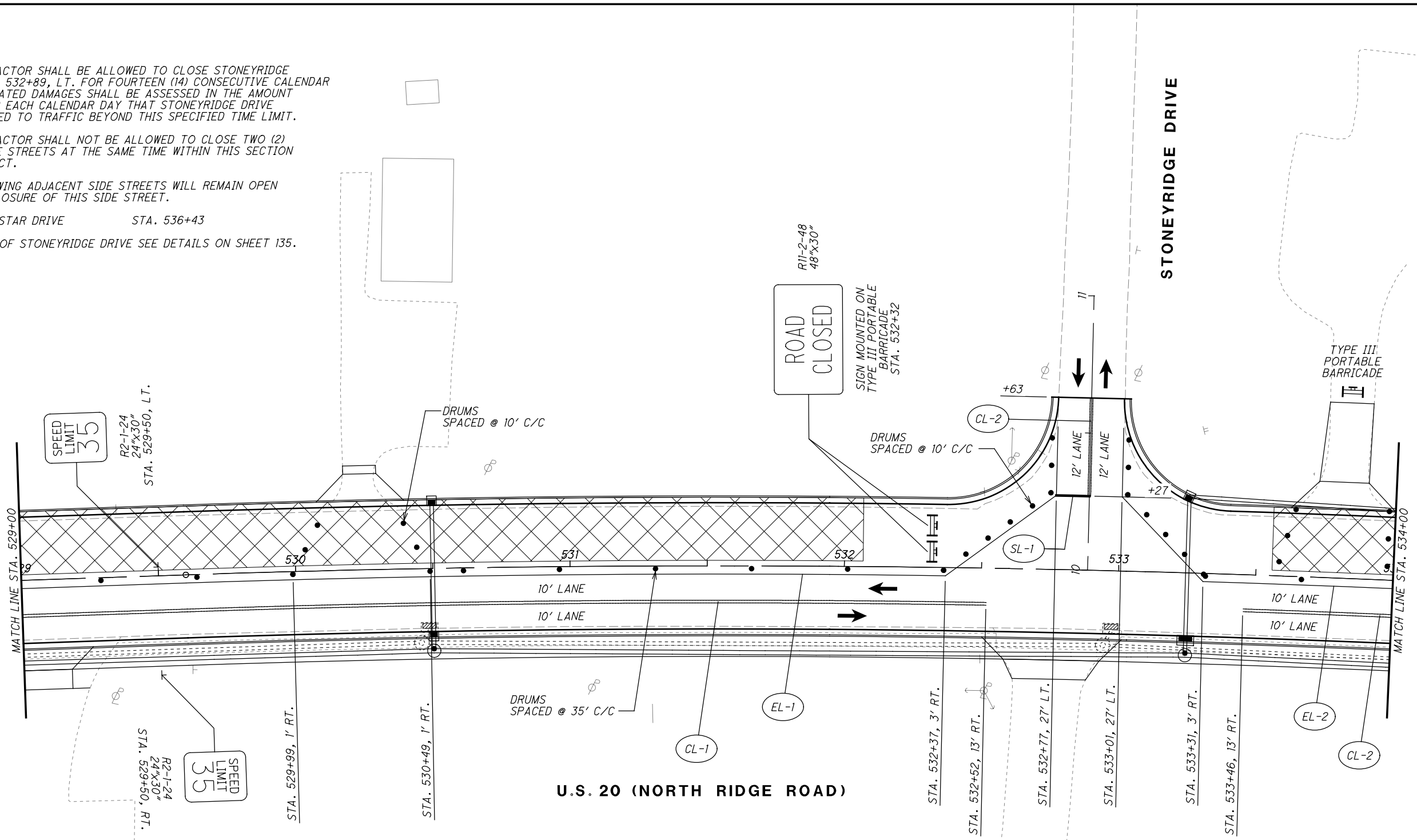
NOTE:
 THE CONTRACTOR SHALL BE ALLOWED TO CLOSE STONEYRIDGE DRIVE AT STA. 532+89, LT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT STONEYRIDGE DRIVE REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT SIDE STREETS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

THE FOLLOWING ADJACENT SIDE STREETS WILL REMAIN OPEN DURING THE CLOSURE OF THIS SIDE STREET.

MORNINGSTAR DRIVE STA. 536+43

FOR CLOSURE OF STONEYRIDGE DRIVE SEE DETAILS ON SHEET 135.



LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | () | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

LEGEND

- | | |
|----------------------|---------------------------------------------------------|
| (Sign Symbol) | MAINTENANCE OF TRAFFIC SIGN |
| (Dashed Sign Symbol) | MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE |
| (Solid Sign Symbol) | EXISTING TRAFFIC SIGN |



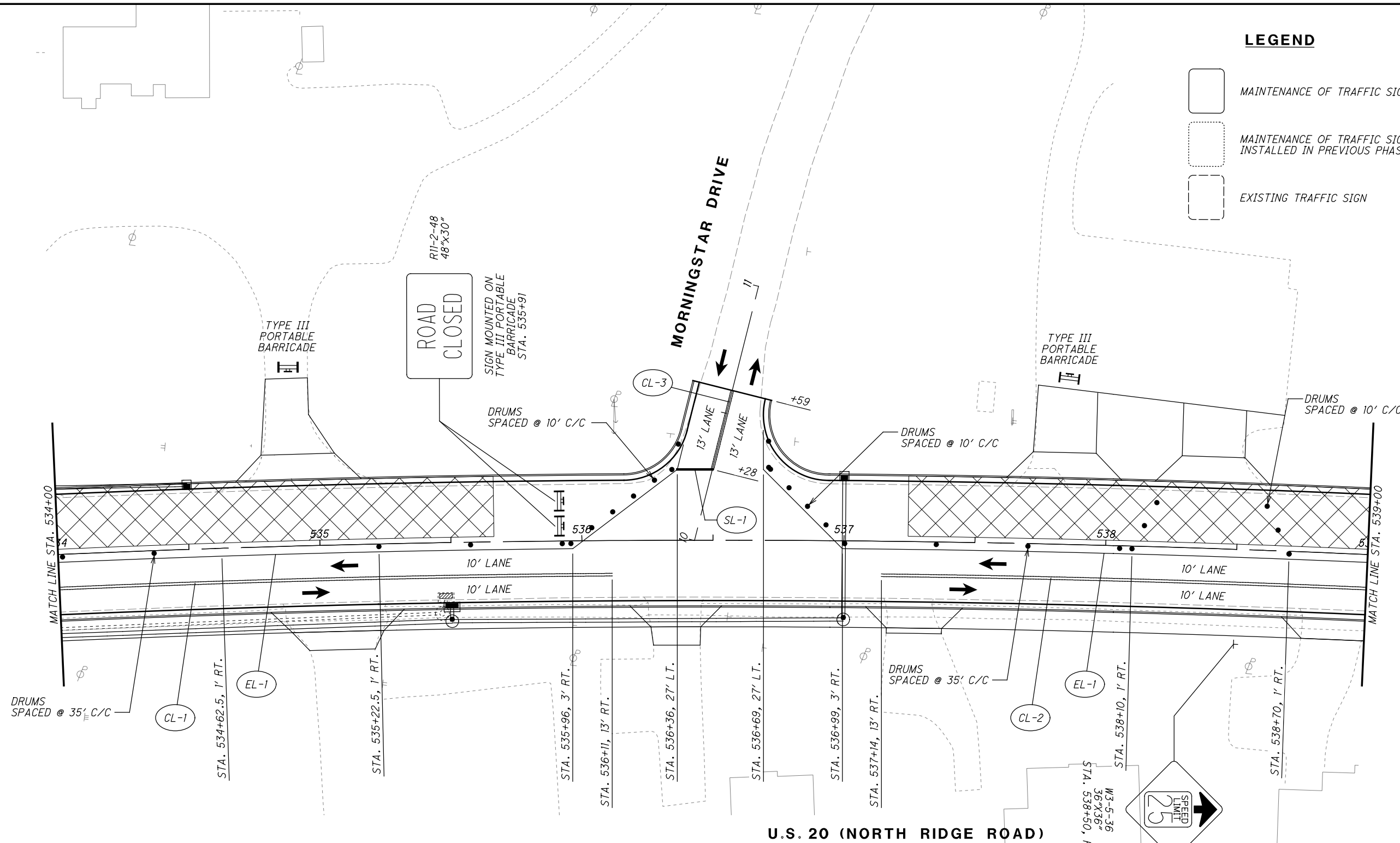
CALCULATED	EJT
CHECKED	WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
 STA. 529+00 TO STA. 534+00

LAK-US-20-24.99
 PART 2

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP285.dgn Sheet 7/15/2022 2:51:24 PM troyer

H:\2018\180743\0D01\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP286.dgn Sheet 7/15/2022 2:51:25 PM troyer



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

0 20 40
HORIZONTAL SCALE IN FEET

CALCULATED EJT
CHECKED WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 534+00 TO STA. 539+00

LEGEND

- | | |
|------------------------------------------------------------|-------------------------------------------------------------------|
| ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
| ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | TRAFFIC FLOW |

NOTE:
THE CONTRACTOR SHALL BE ALLOWED TO CLOSE MORNINGSTAR DRIVE AT STA. 536+43, LT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT MORNINGSTAR DRIVE REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT SIDE STREETS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

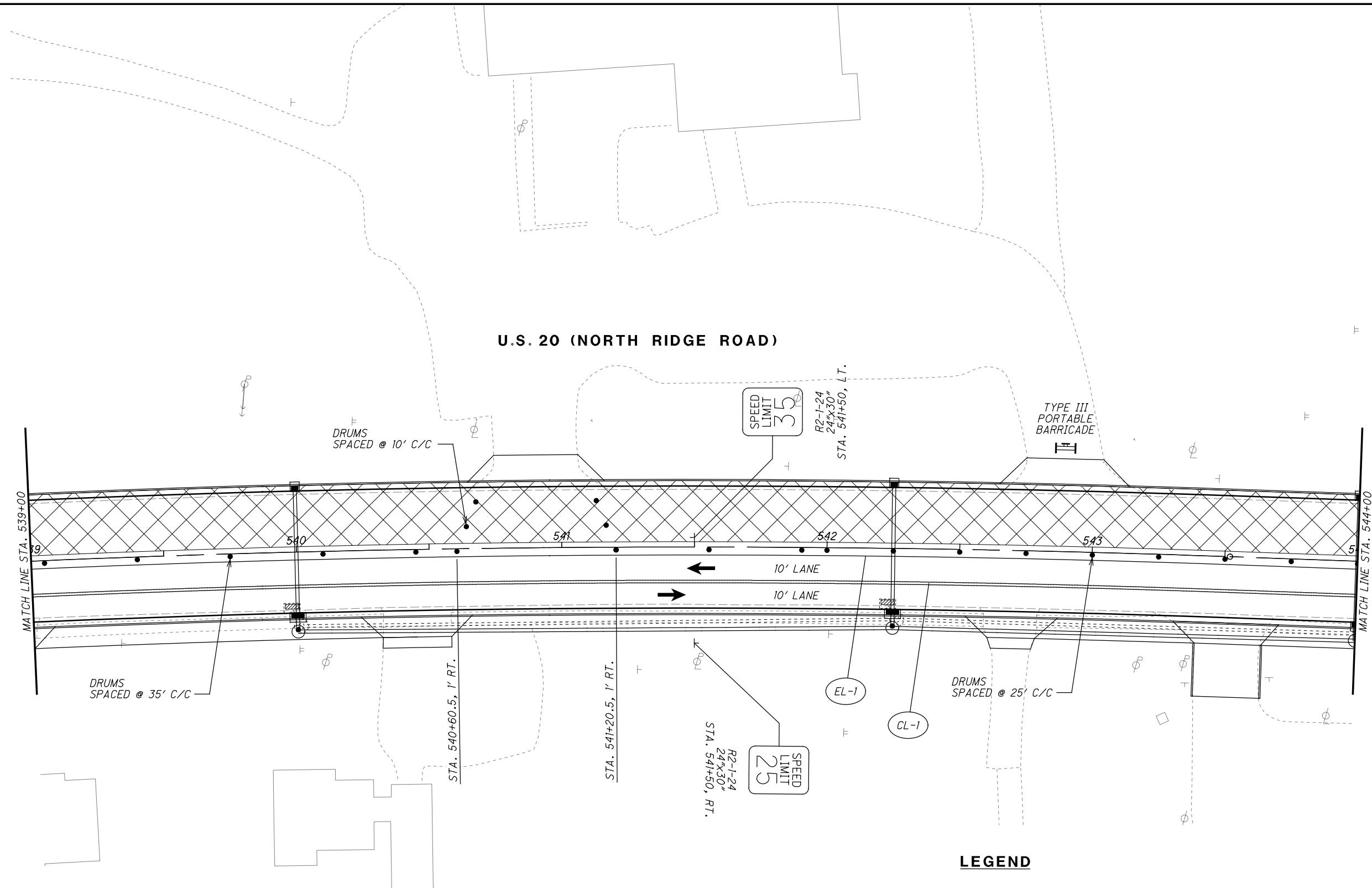
THE FOLLOWING ADJACENT SIDE STREETS WILL REMAIN OPEN DURING THE CLOSURE OF THIS SIDE STREET.

STONEYRIDGE DRIVE STA. 532+89

FOR CLOSURE OF MORNINGSTAR DRIVE SEE DETAILS ON SHEET 136.




LAK-US-20-24.99
PART 2








H:\2018\80743\0D0T\ProjectData\092TO_LAK_20_EAST\Design\MOT\Sheets\092TO_MP2B7.dgn Sheet 7/15/2022 2:51:26 PM troyer










U.S. 20 (NORTH RIDGE ROAD)

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
-  TRAFFIC FLOW

CALCULATED
EJT
CHECKED
WDB

0 10 20 40
HORIZONTAL
SCALE IN FEET



MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 539+00 TO STA. 544+00

LAK-US-20-24.99
PART 2

NOTES

1. TEMP. 12" PIPE PLACED IN PHASE 1 TO BE PLUGGED AT BOTH ENDS, FILLED, AND ABANDONED.

CALCULATED
EJT
CHECKED
WDB

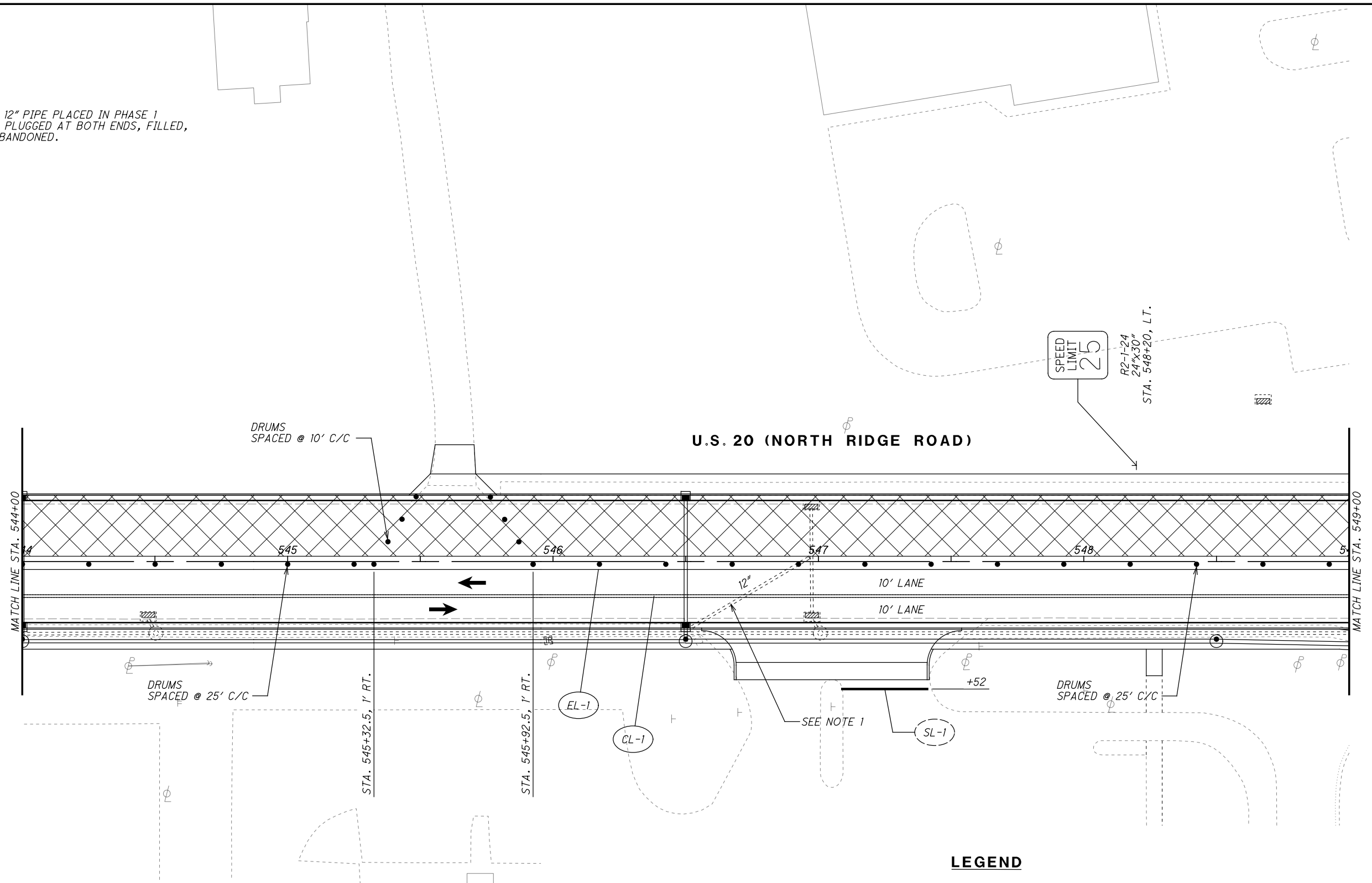
0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 544+00 TO STA. 549+00

LAK-US-20-24.99
PART 2

110
697

H:\2018\80743\00T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP288.dgn Sheet 7/15/2022 2:51:26 PM troyer



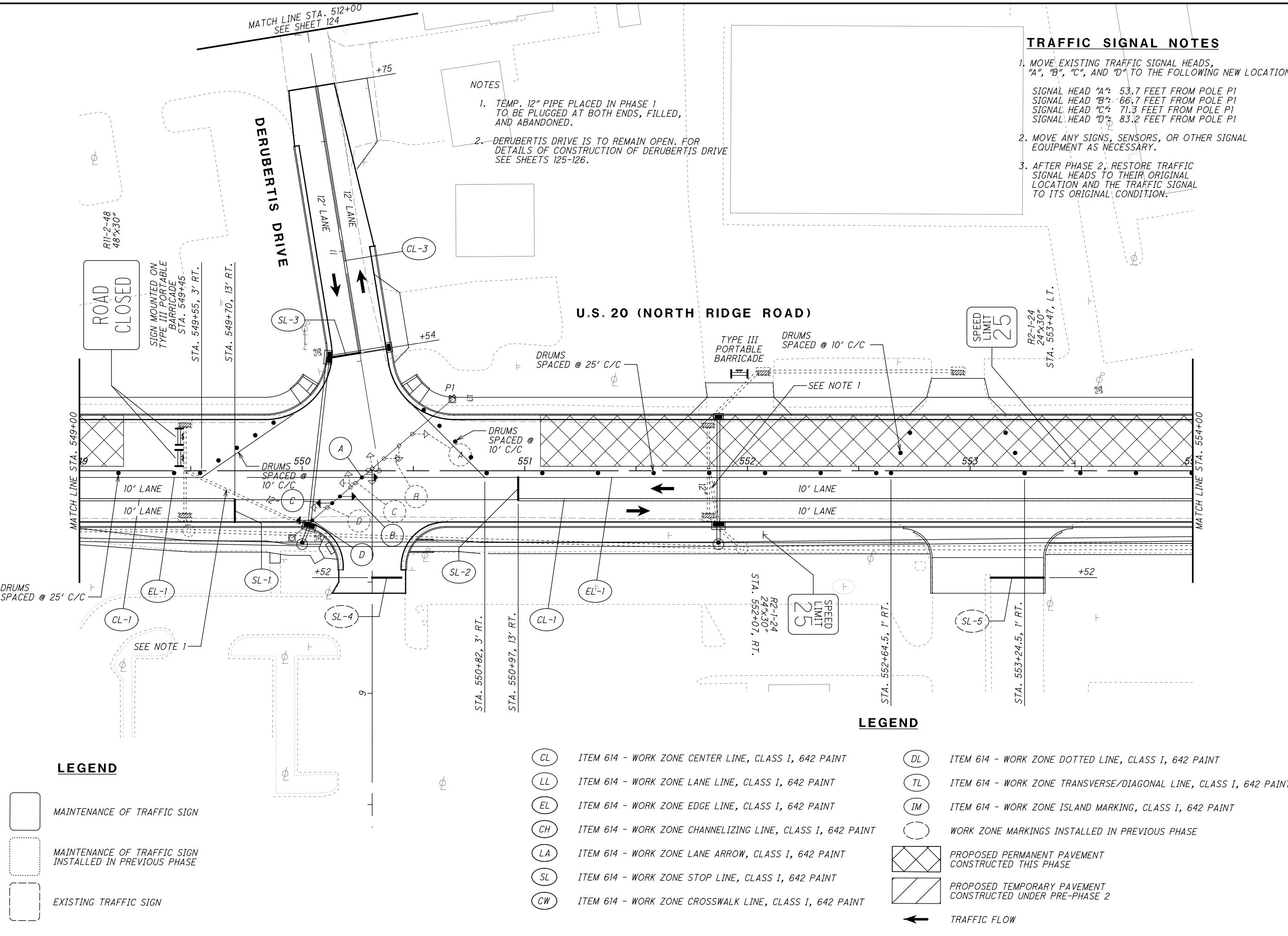
LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

LEGEND

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
- TRAFFIC FLOW

H:\2018\80743\0DOT\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP289.dgn Sheet 7/15/2022 2:51:27 PM troyer



MATCH LINE STA. 512+00
SEE SHEET 124

MATCH LINE STA. 549+00

MATCH LINE STA. 554+00

NOTES

1. TEMP. 12" PIPE PLACED IN PHASE 1 TO BE PLUGGED AT BOTH ENDS, FILLED, AND ABANDONED.
2. DERUBERTIS DRIVE IS TO REMAIN OPEN. FOR DETAILS OF CONSTRUCTION OF DERUBERTIS DRIVE SEE SHEETS 125-126.

TRAFFIC SIGNAL NOTES

1. MOVE EXISTING TRAFFIC SIGNAL HEADS, "A", "B", "C", AND "D" TO THE FOLLOWING NEW LOCATIONS:
 SIGNAL HEAD "A": 53.7 FEET FROM POLE P1
 SIGNAL HEAD "B": 66.7 FEET FROM POLE P1
 SIGNAL HEAD "C": 71.3 FEET FROM POLE P1
 SIGNAL HEAD "D": 83.2 FEET FROM POLE P1
2. MOVE ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.
3. AFTER PHASE 2, RESTORE TRAFFIC SIGNAL HEADS TO THEIR ORIGINAL LOCATION AND THE TRAFFIC SIGNAL TO ITS ORIGINAL CONDITION.



LAK-US-20-24.99
PART 2
MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 549+00 TO STA. 554+00

LAK-US-20-24.99
PART 2

111
697

LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- | | |
|------------------------------------------------------------|-------------------------------------------------------------------|
| ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
| ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | TRAFFIC FLOW |

LEGEND

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\01\Sheets\09270_MP290.dgn Sheet 7/15/2022 2:51:28 PM troyer

NOTES

1. TEMP. 12" PIPE PLACED IN PHASE 1 TO BE PLUGGED AT BOTH ENDS, FILLED, AND ABANDONED.

TRAFFIC SIGNAL NOTES

1. MOVE EXISTING TRAFFIC SIGNAL HEADS, "A", "B", AND "C" TO THE FOLLOWING NEW LOCATIONS:

SIGNAL HEAD "A": 42.3 FEET FROM POLE P1
 SIGNAL HEAD "B": 53.7 FEET FROM POLE P1
 SIGNAL HEAD "C": 49.6 FEET FROM POLE P3
 SIGNAL HEAD "D": 59.2 FEET FROM POLE P3

2. MOVE ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.

3. AFTER PHASE 2, RESTORE TRAFFIC SIGNAL HEADS TO THEIR ORIGINAL LOCATION AND THE TRAFFIC SIGNAL TO ITS ORIGINAL CONDITION.

- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

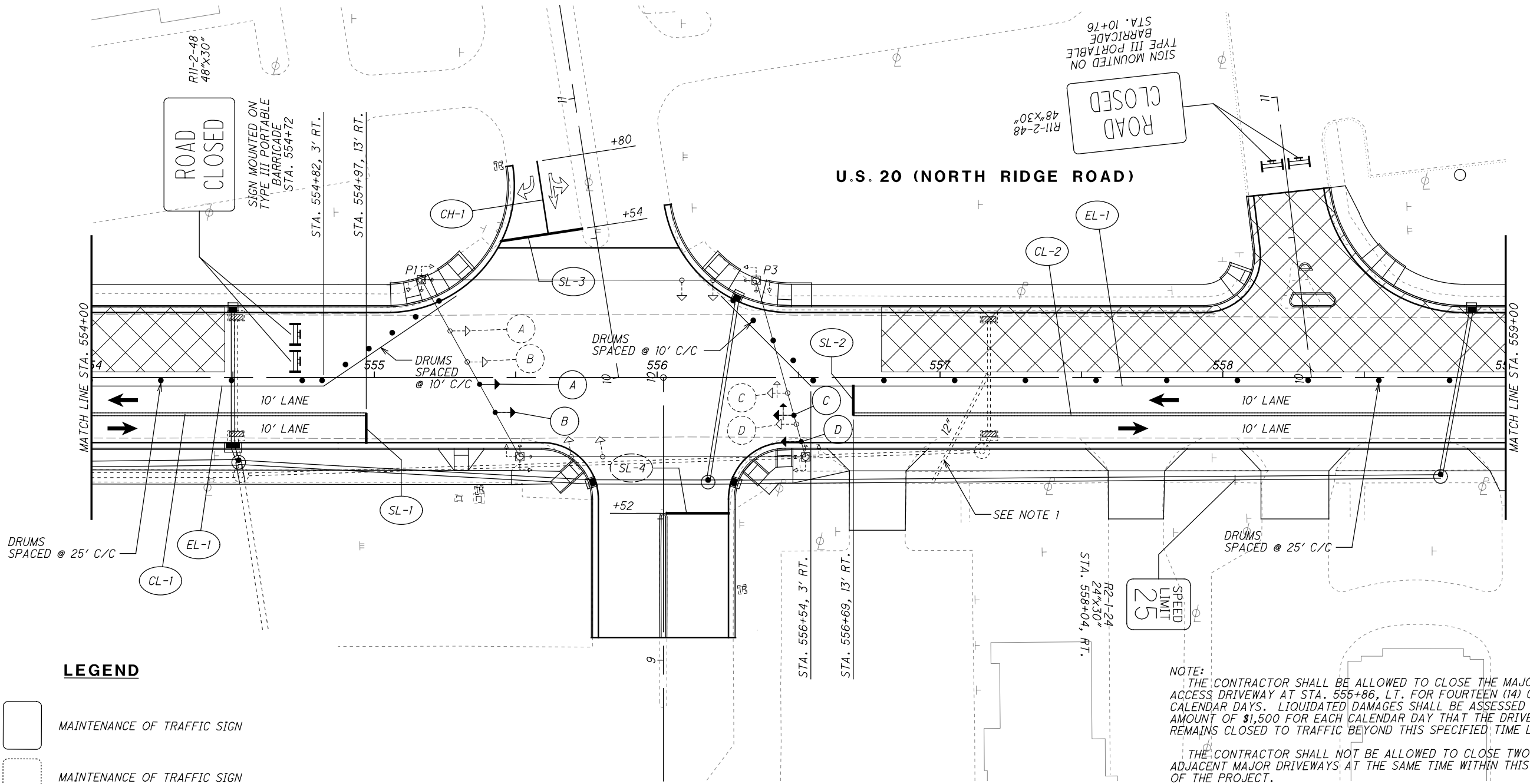
LEGEND

- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
 - (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
 - (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
 - () WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
 - (X) PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
 - (/) PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
- ← TRAFFIC FLOW



MAINTENANCE OF TRAFFIC PLAN - PHASE 2
 STA. 554+00 TO STA. 559+00

LAK-US-20-24.99
PART 2



- LEGEND**
- () MAINTENANCE OF TRAFFIC SIGN
 - () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
 - () EXISTING TRAFFIC SIGN

NOTE:
 THE CONTRACTOR SHALL BE ALLOWED TO CLOSE THE MAJOR ACCESS DRIVEWAY AT STA. 555+86, LT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT THE DRIVEWAY REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT MAJOR DRIVEWAYS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

THE FOLLOWING ADJACENT MAJOR DRIVEWAYS WILL REMAIN OPEN DURING THE CLOSURE OF THIS SIDE STREET.

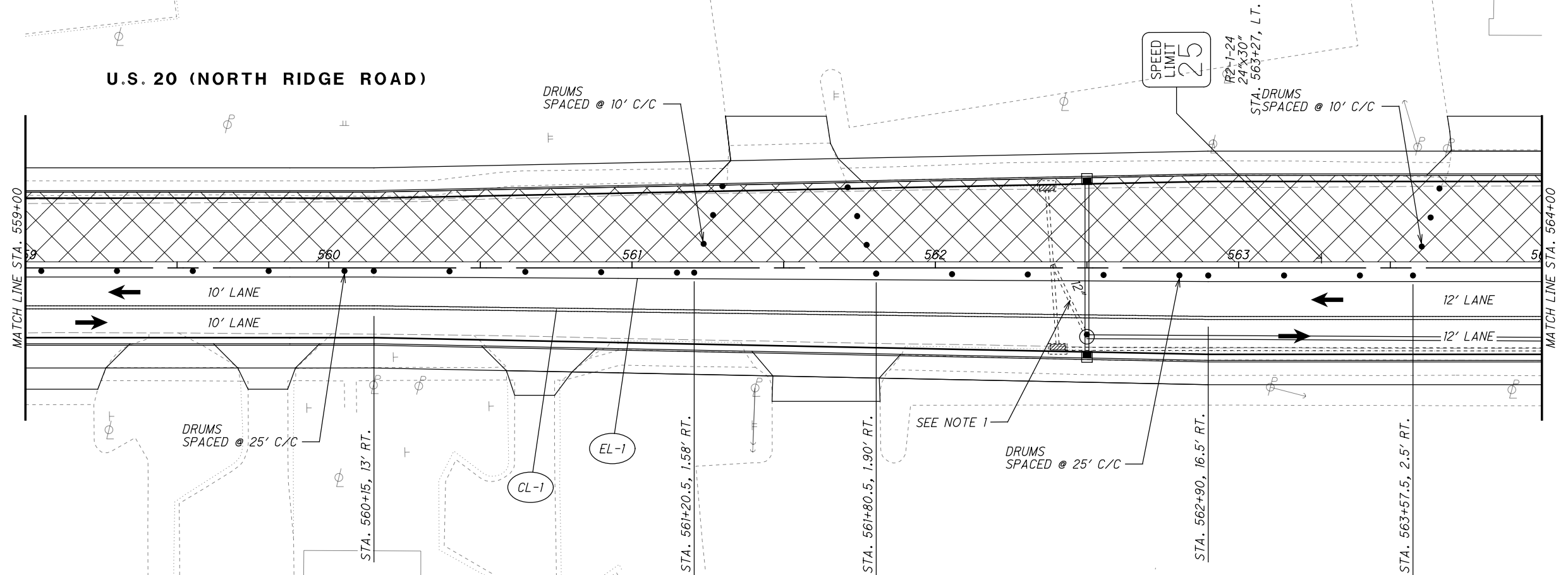
MAJOR ACCESS DRIVEWAY STA. 558+30
 FOR CLOSURE OF MAJOR ACCESS DRIVE SEE DETAILS ON SHEET 137.

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP291.dgn Sheet 7/15/2022 2:51:29 PM Troyer




U.S. 20 (NORTH RIDGE ROAD)








NOTES

1. TEMP. 12" PIPE PLACED IN PHASE 1 TO BE PLUGGED AT BOTH ENDS, FILLED, AND ABANDONED.






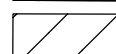
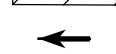


LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  CL ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  LL ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  EL ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  CH ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  LA ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  SL ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  CW ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  DL ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  TL ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  IM ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
-  TRAFFIC FLOW



CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 559+00 TO STA. 564+00

LAK-US-20-24.99
PART 2

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP292.dgn Sheet 7/15/2022 2:51:30 PM troyer



CALCULATED
 EJT
 CHECKED
 WDB

NOTES

- TEMP. 12" PIPE PLACED IN PHASE 1 TO BE PLUGGED AT BOTH ENDS, FILLED, AND ABANDONED.

ROAD CLOSED

SIGN MOUNTED ON TYPE III PORTABLE BARRICADE STA. 564+52

STA. 564+62, 4.5' RT.

STA. 564+77, 16.5' RT.

TYPE III PORTABLE BARRICADE

DRUMS SPACED @ 10' C/C

12' LANE

12' LANE

STA. 564+17.5, 2.5' RT.

DRUMS SPACED @ 25' C/C

NOTE:
HUBBARD ROAD IS TO REMAIN OPEN. FOR DETAILS OF CONSTRUCTION OF HUBBARD ROAD SEE SHEETS 128-133.

MATCH LINE STA. 102+00
SEE SHEET 127

HUBBARD ROAD

HUBBARD ROAD

MATCH LINE STA. 98+00
SEE SHEET 127

LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | (○) | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

U.S. 20 (NORTH RIDGE ROAD)

DRUMS SPACED @ 10' C/C

TYPE III PORTABLE BARRICADE

SPEED LIMIT 25

STA. 568+02, 2.5' RT.

STA. 568+62, 2.5' RT.

STA. 568+75, 16.5' RT.

DRUMS SPACED @ 25' C/C

TRAFFIC SIGNAL NOTES

- REMOVE TEMPORARY POLES, P1 AND P2, AND THE ASSOCIATED SIGNAL HEADS AND EQUIPMENT.
- MOVE SIGNAL HEADS "C" AND "D" TO THE FOLLOWING LOCATIONS:
SIGNAL HEAD "C": 21.6 FEET FROM POLE P5
SIGNAL HEAD "D": 9.7 FEET FROM POLE P5
- CONSTRUCT TEMPORARY POLES, P3 AND P4, AS PER SCD MT-96.20. PLACE SIGNAL HEADS AT THE FOLLOWING LOCATIONS:
SIGNAL HEAD "A": 50.0 FEET FROM POLE P3
SIGNAL HEAD "B": 62.2 FEET FROM POLE P3
- CONSTRUCT TRAFFIC CONDUIT AND CABLES. CONNECT TO POWER SOURCE.
- MOVE OR ADD ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.

LEGEND

- (□) MAINTENANCE OF TRAFFIC SIGN
- (□) MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- (□) EXISTING TRAFFIC SIGN

NOTES

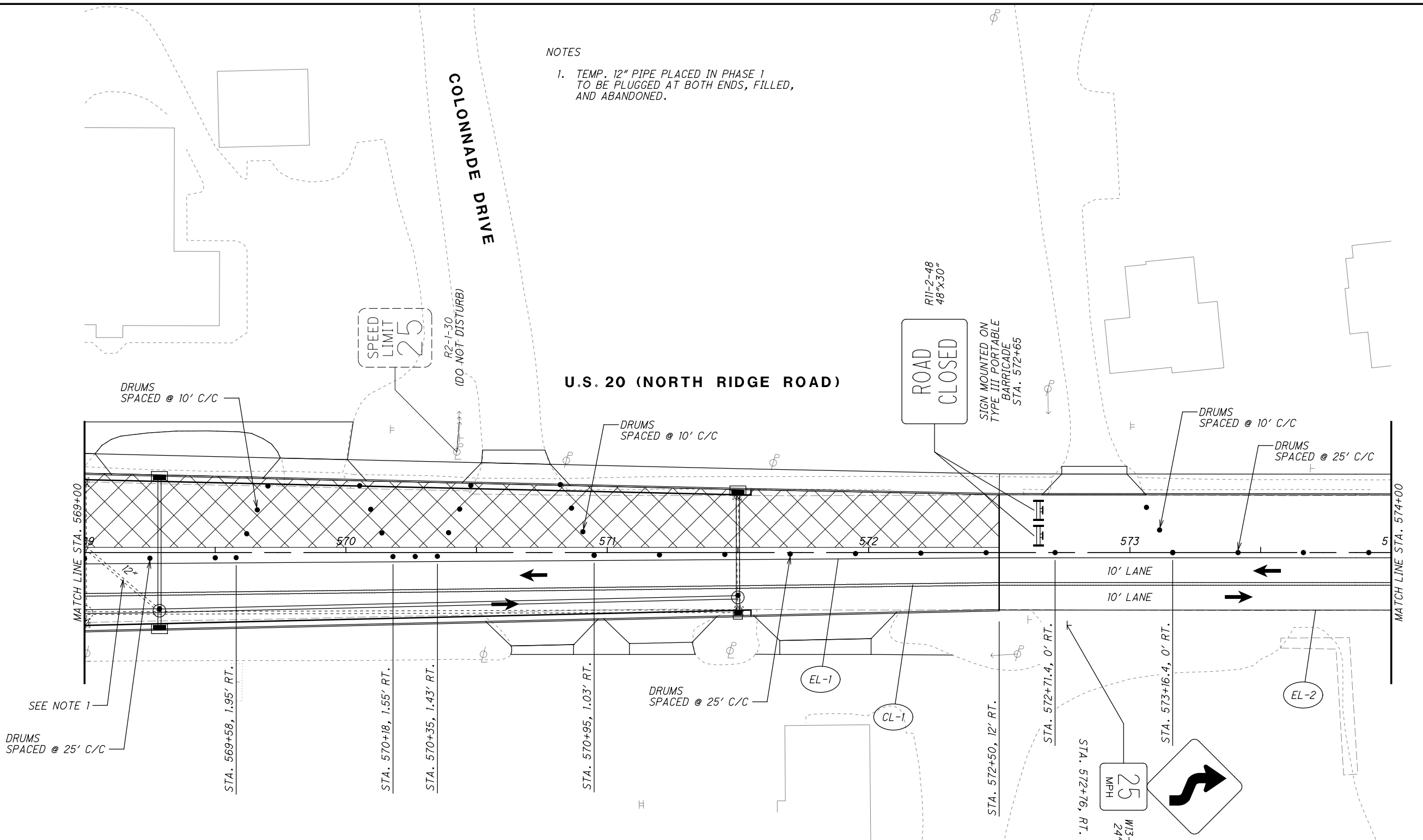
- TEMP. 12" PIPE PLACED IN PHASE 1 TO BE PLUGGED AT BOTH ENDS, FILLED, AND ABANDONED.



CALCULATED	EJT
CHECKED	WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 569+00 TO STA. 574+00

LAK-US-20-24.99
PART 2



LEGEND

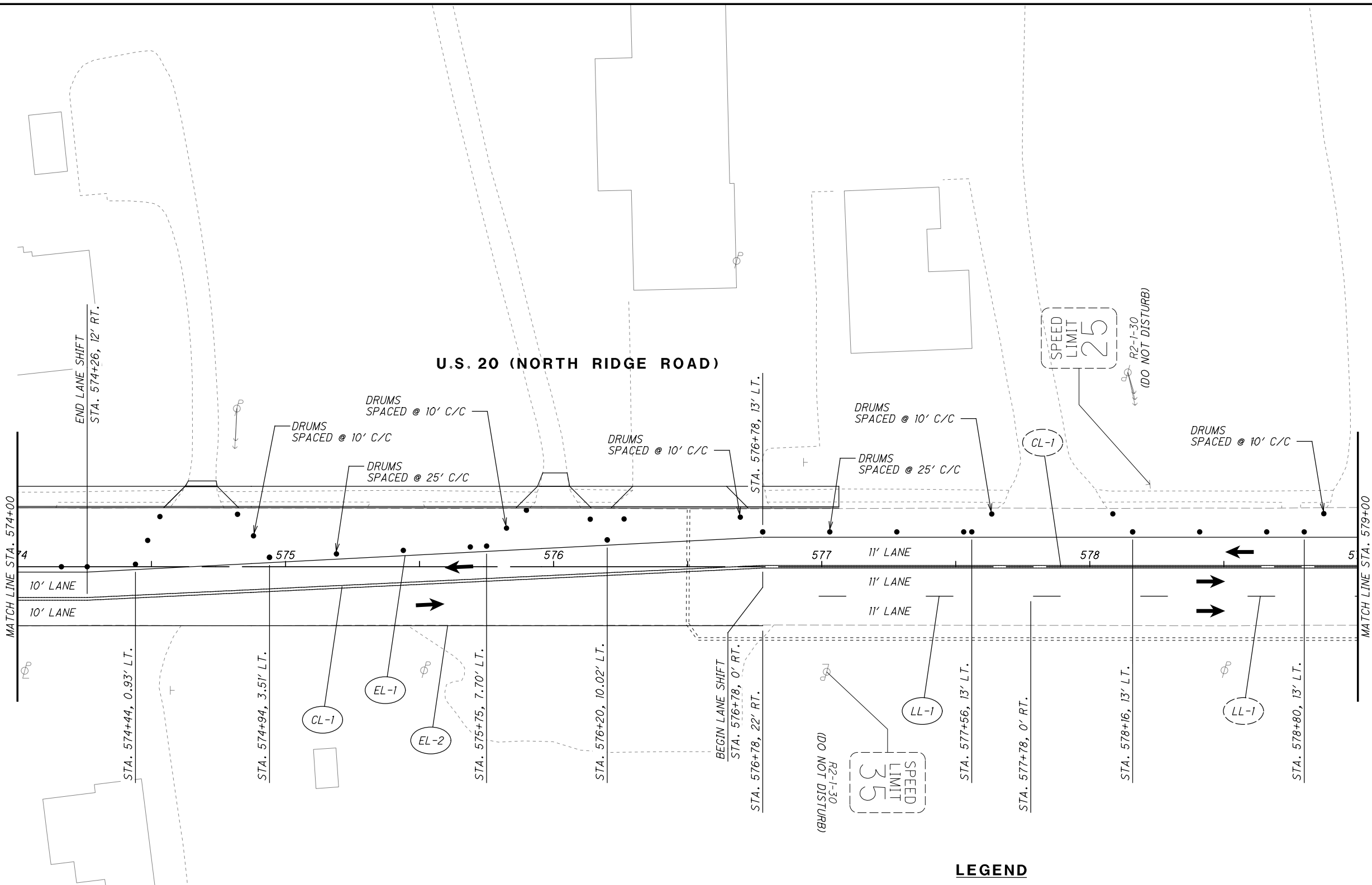
- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | () | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

LEGEND




- () MAINTENANCE OF TRAFFIC SIGN
- () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- () EXISTING TRAFFIC SIGN












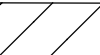


H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP293.dgn Sheet 7/15/2022 2:51:30 PM troyer

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP294.dgn Sheet 7/15/2022 2:51:31PM troyer



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN




- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |












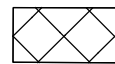
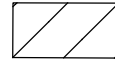

CALCULATED
EJT
CHECKED
WDB

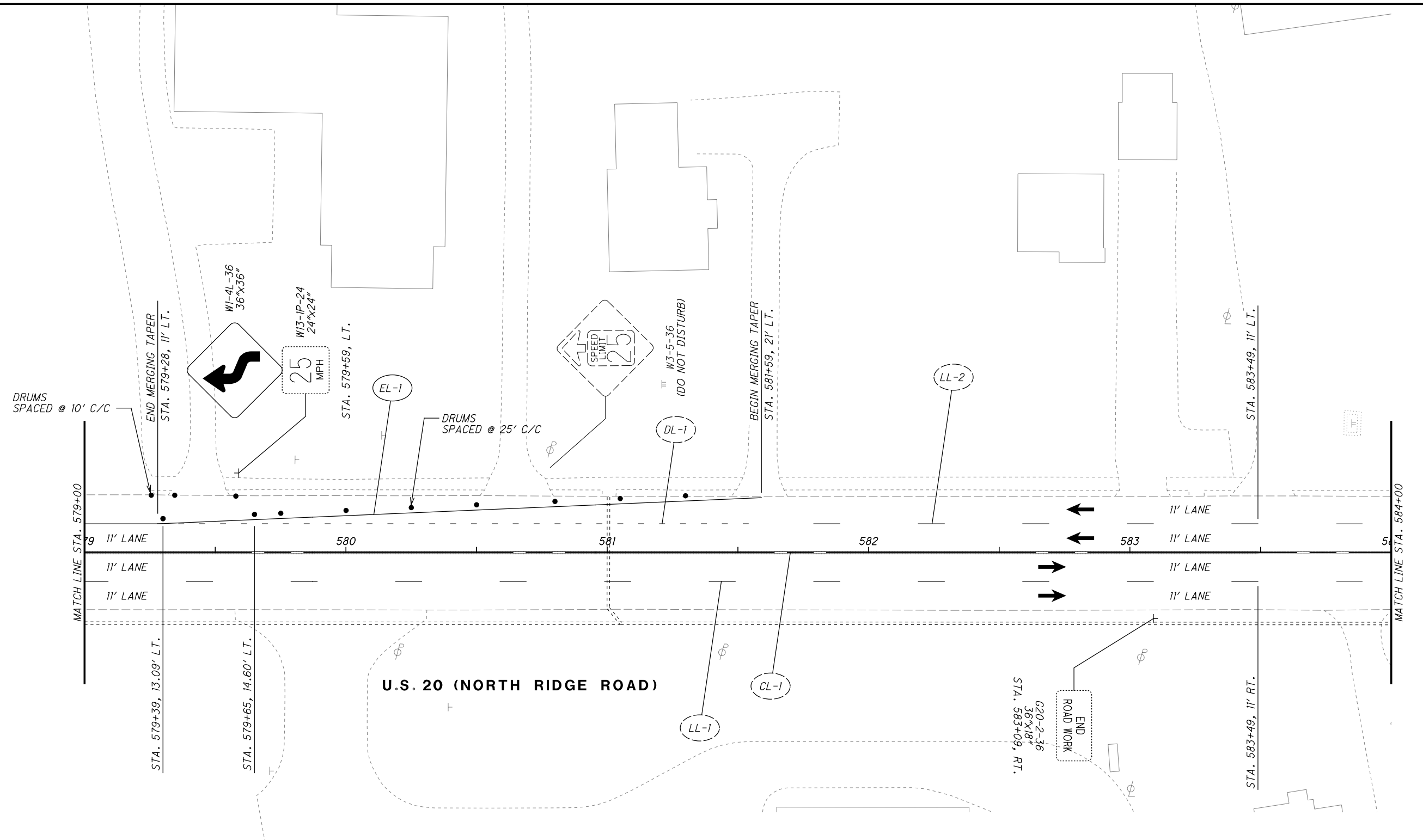
MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 574+00 TO STA. 579+00

LAK-US-20-24.99
PART 2

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP294A.dgn Sheet 7/15/2022 2:51:32 PM Troyer

- LEGEND**
-  MAINTENANCE OF TRAFFIC SIGN
 -  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
 -  EXISTING TRAFFIC SIGN

- LEGEND**
-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
 -  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
 -  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
 -  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
 -  TRAFFIC FLOW



CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 579+00 TO STA. 584+00

LAK-US-20-24.99
PART 2



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

LEGEND

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
- TRAFFIC FLOW

U.S. 20 (NORTH RIDGE ROAD)

EXIST. SIGN
(DO NOT DISTURB)
SPEED
LIMIT
35

SPEED
LIMIT
35

R2-1-30
(DO NOT DISTURB)

W4-2R-36
36"x36"
STA. 585+09, L.T.

MATCH LINE STA. 584+00

MATCH LINE STA. 589+00

585

586

587

588

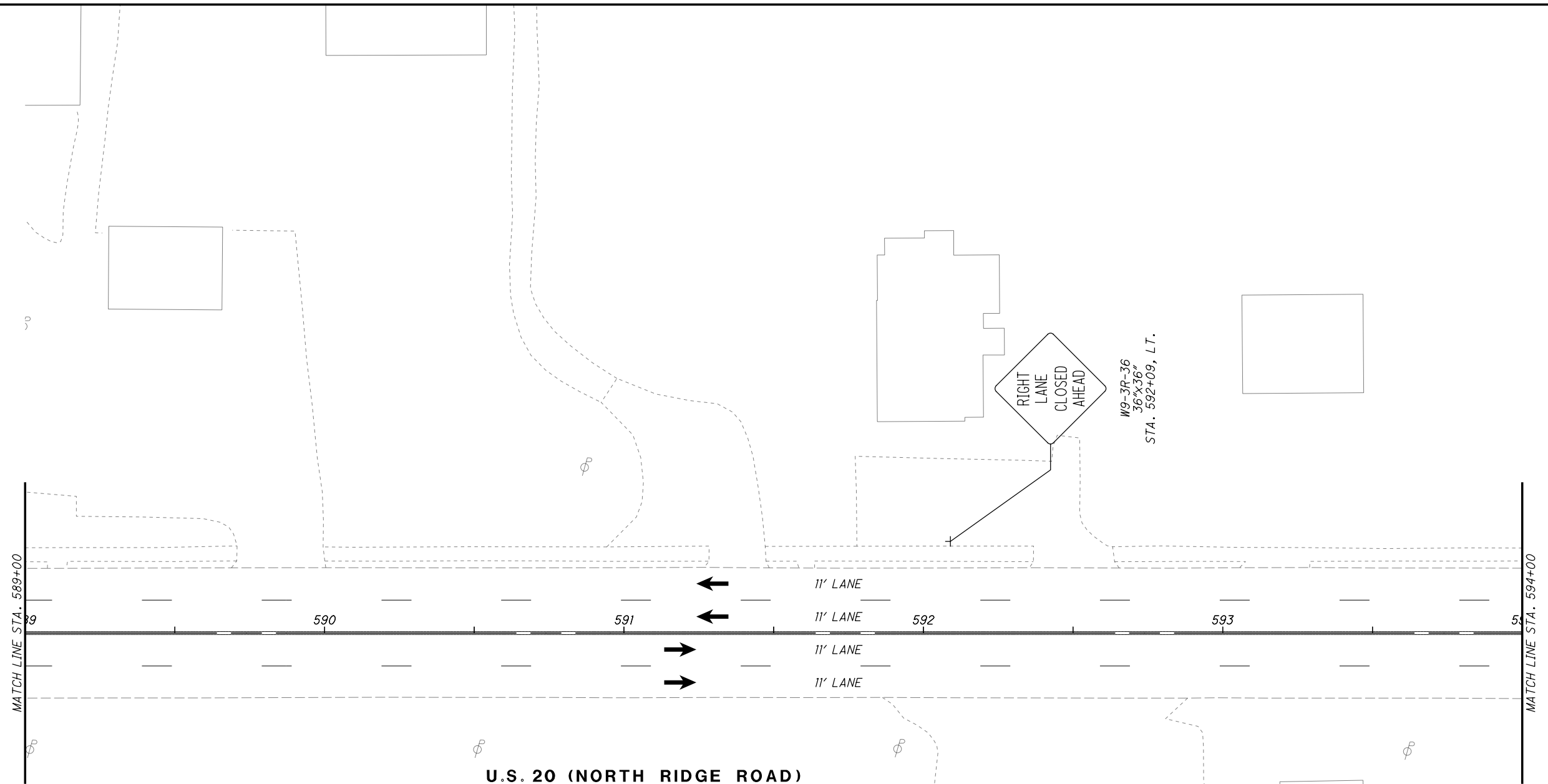
589

11' LANE

11' LANE

11' LANE

11' LANE






LEGEND










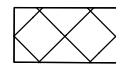

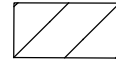


- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

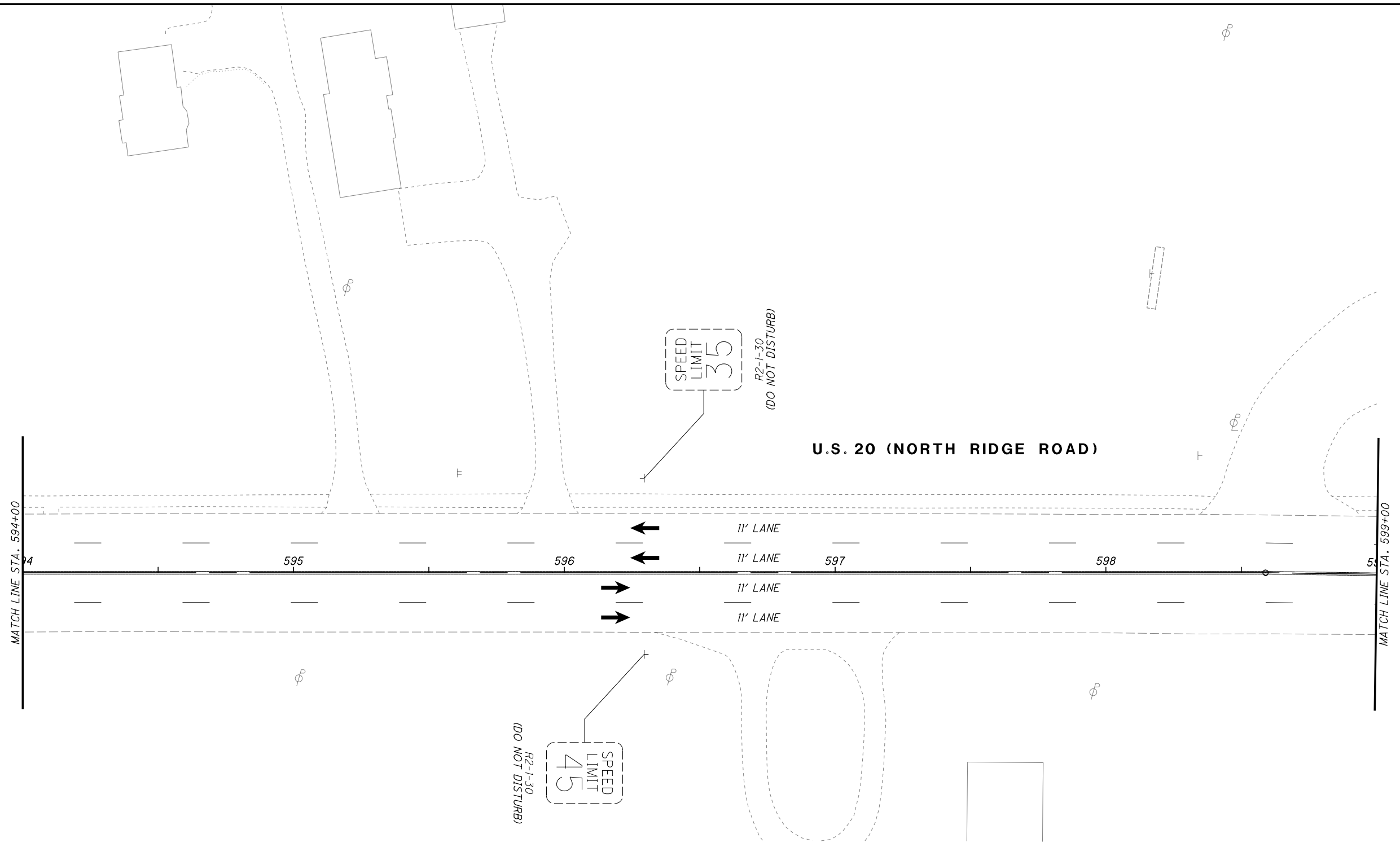
LEGEND

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
- TRAFFIC FLOW

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP294D.dgn Sheet 7/15/2022 2:51:34 PM Troyer

- LEGEND**
-  MAINTENANCE OF TRAFFIC SIGN
 -  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
 -  EXISTING TRAFFIC SIGN

- LEGEND**
- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |



CALCULATED
EJT

CHECKED
WDB

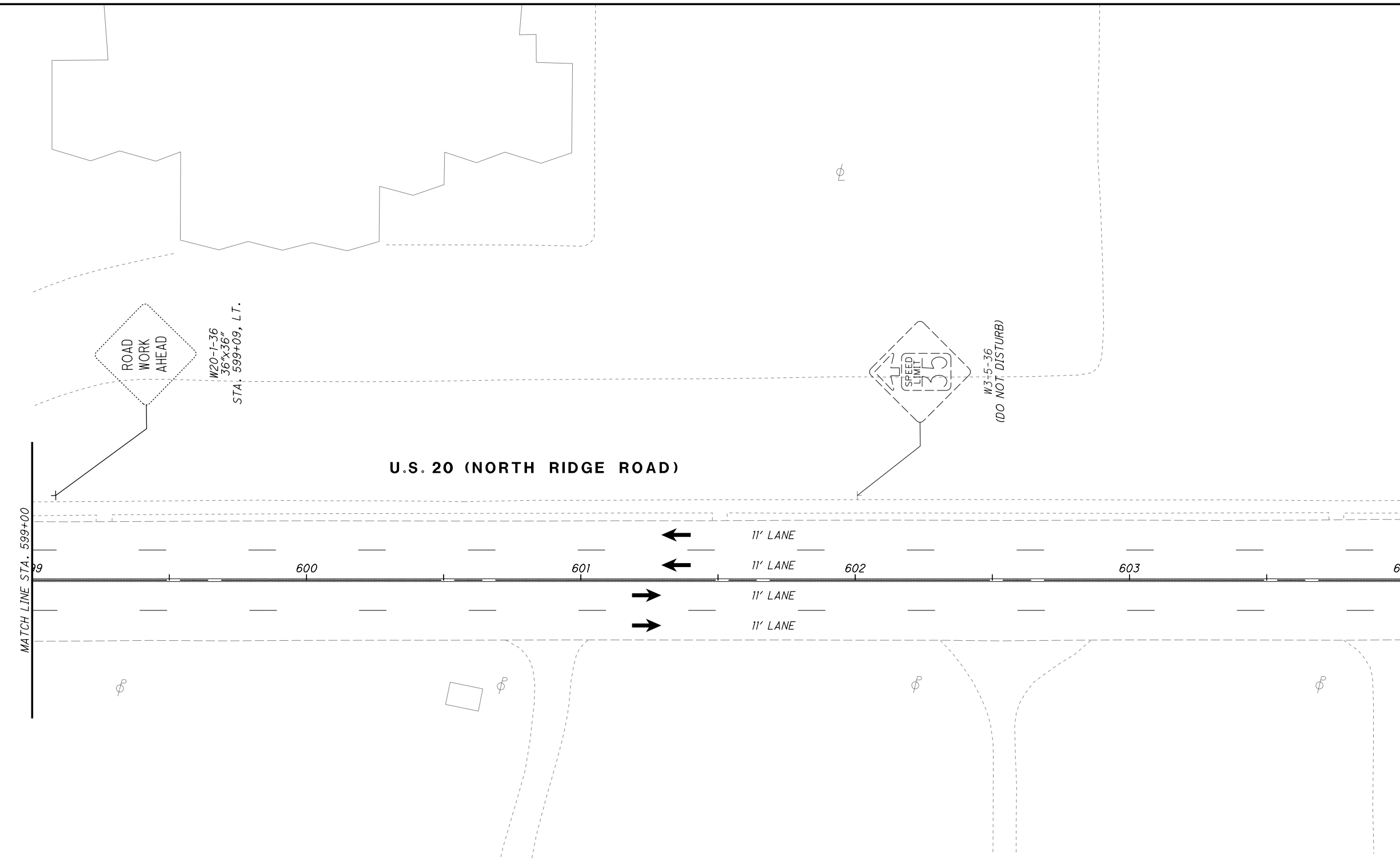
0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 594+00 TO STA. 599+00




LAK-US-20-24.99
PART 2








120
697

H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\M0T\Sheets\09270_MP295.dgn Sheet 7/15/2022 2:51:34 PM troyer






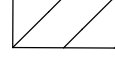



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
-  TRAFFIC FLOW

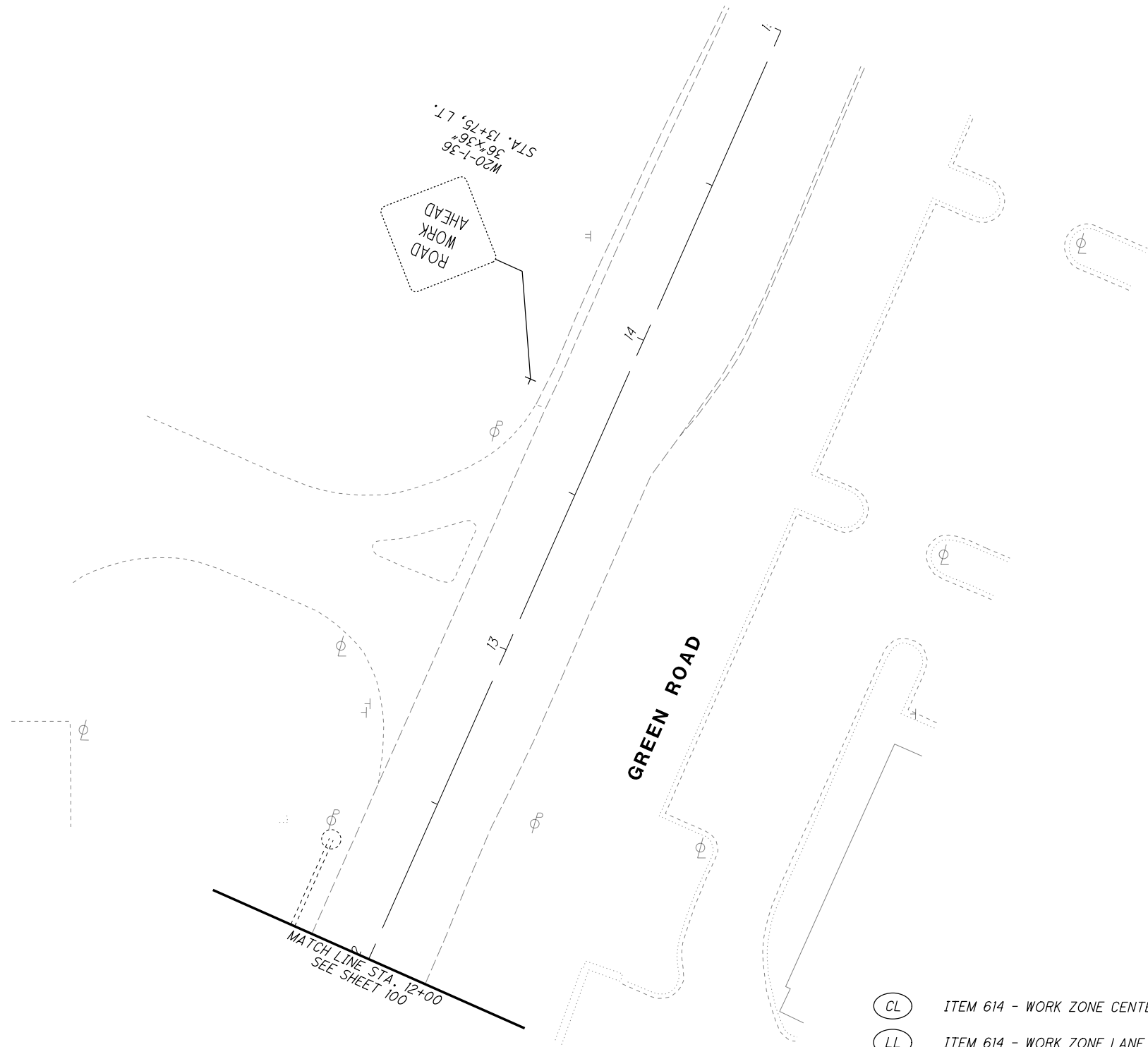
CALCULATED
EJT
CHECKED
WDB







HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 599+00 TO STA. 604+00

LAK-US-20-24.99
PART 2



LEGEND















-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

CALCULATED
EJT
CHECKED
WDB

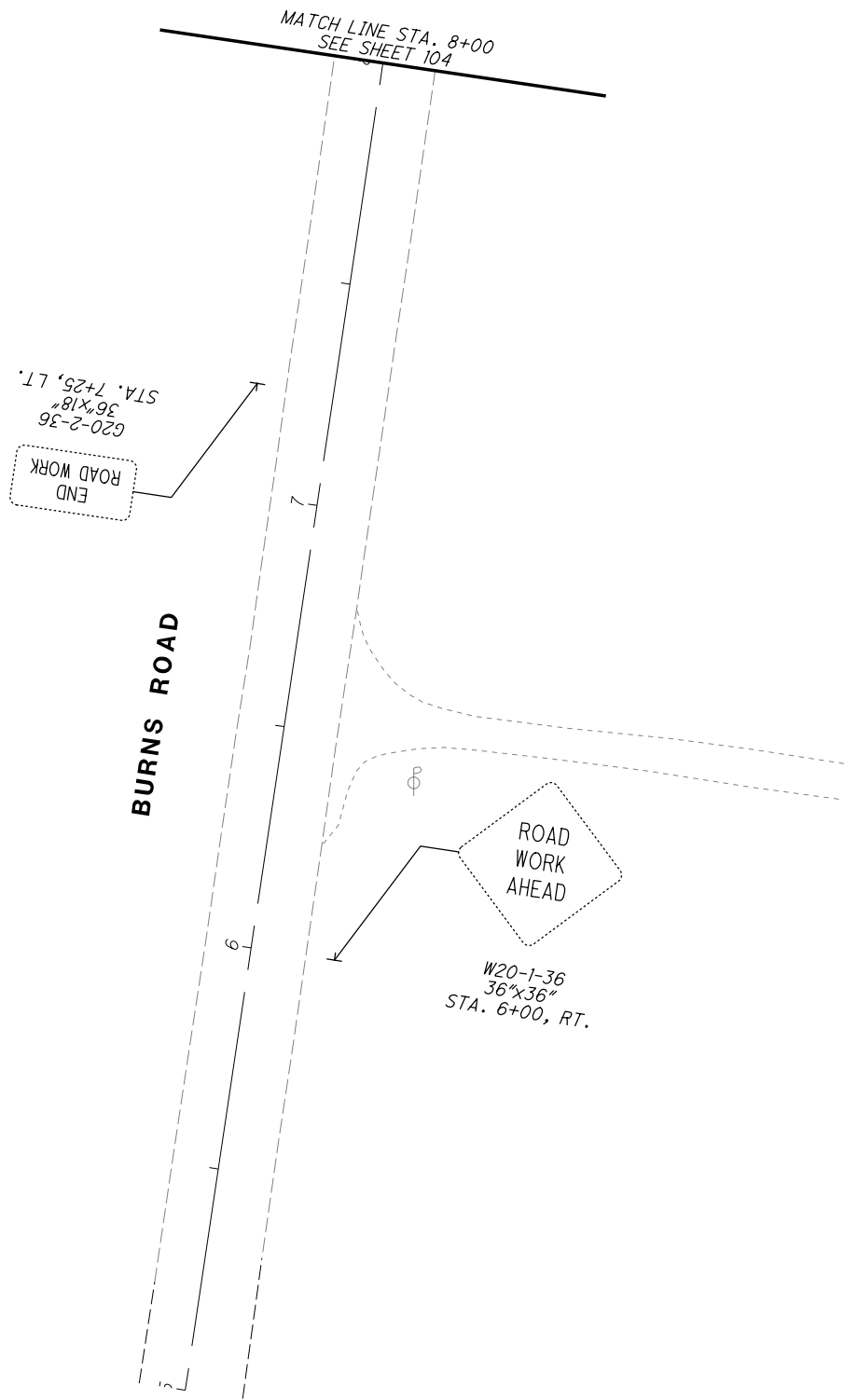
0 20 40
HORIZONTAL
SCALE IN FEET

**MAINTENANCE OF TRAFFIC PLAN - PHASE 2
GREEN ROAD**




LEGEND

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

**LAK-US-20-24.99
PART 2**



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN















CALCULATED
EJT

CHECKED
WDB

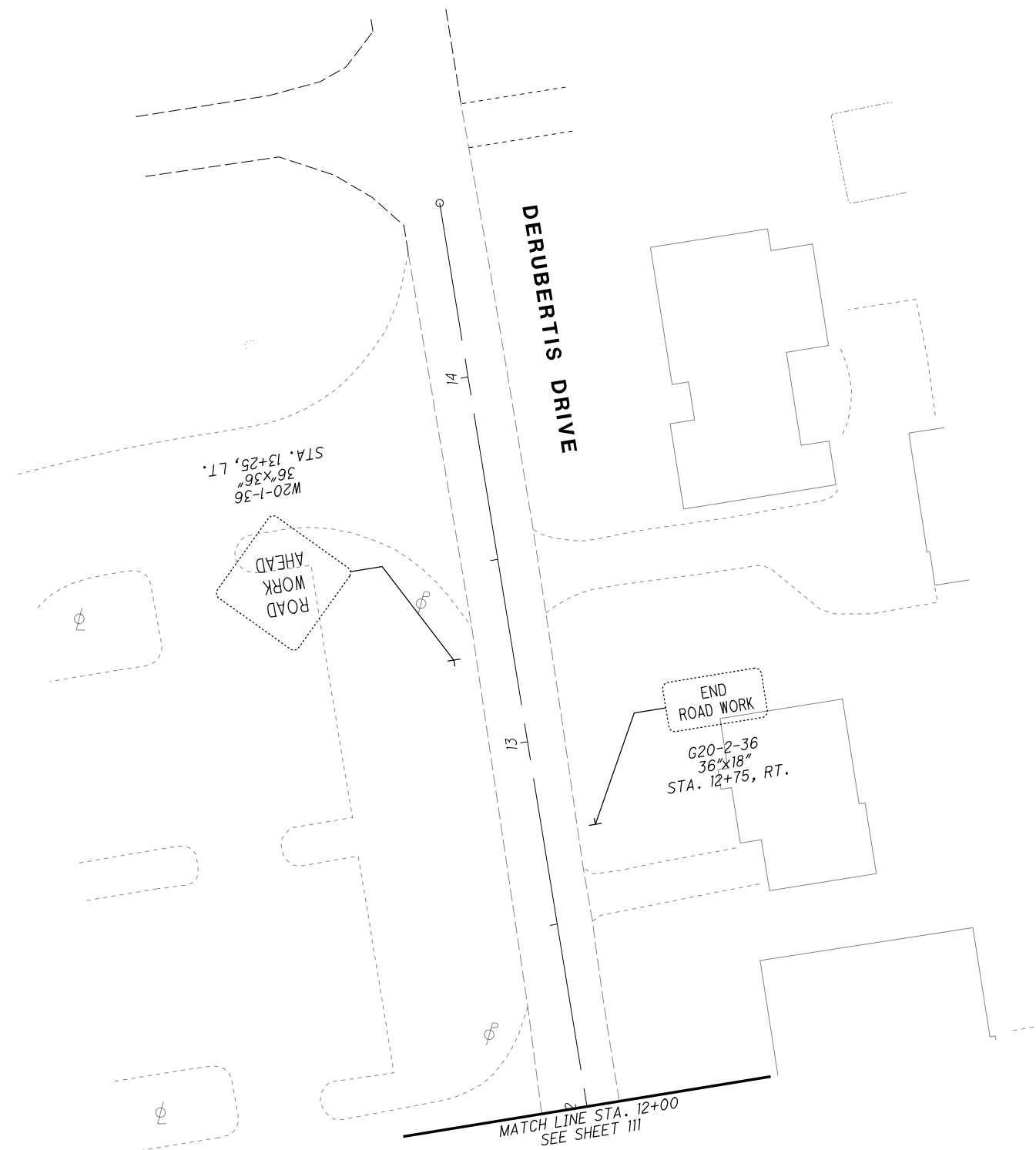
0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 514+00 TO STA. 519+00




LEGEND

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

LAK-US-20-24.99
PART 2



LEGEND












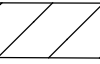


-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STA. 549+00 TO STA. 554+00

LEGEND

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

LAK-US-20-24.99
PART 2

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP2941.dgn Sheet 7/15/2022 2:51:37 PM troyer

LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | () | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

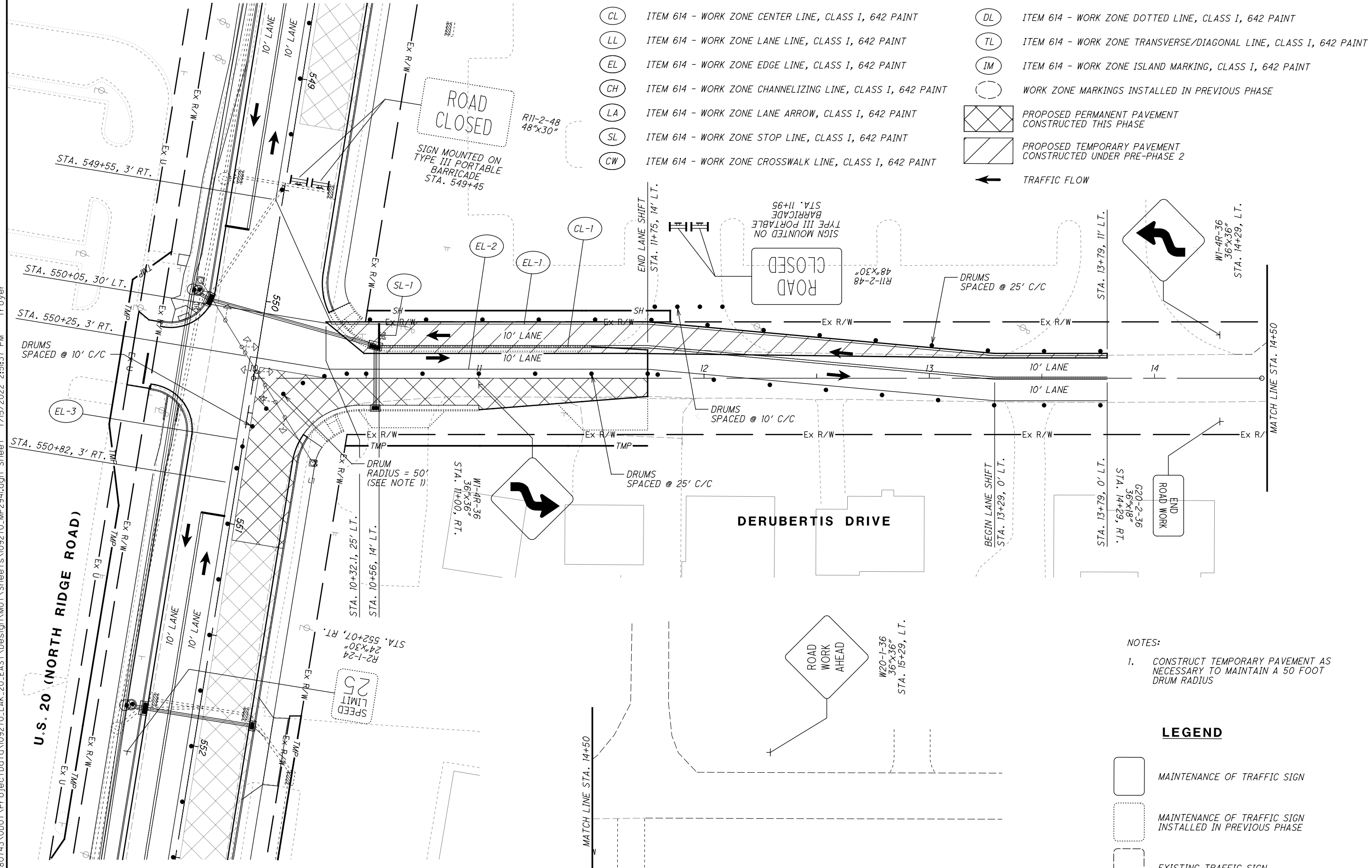
0 20 40

 HORIZONTAL

 SCALE IN FEET

LAK-US-20-24.99 PART 2

MAINTENANCE OF TRAFFIC PLAN - PHASE 2A

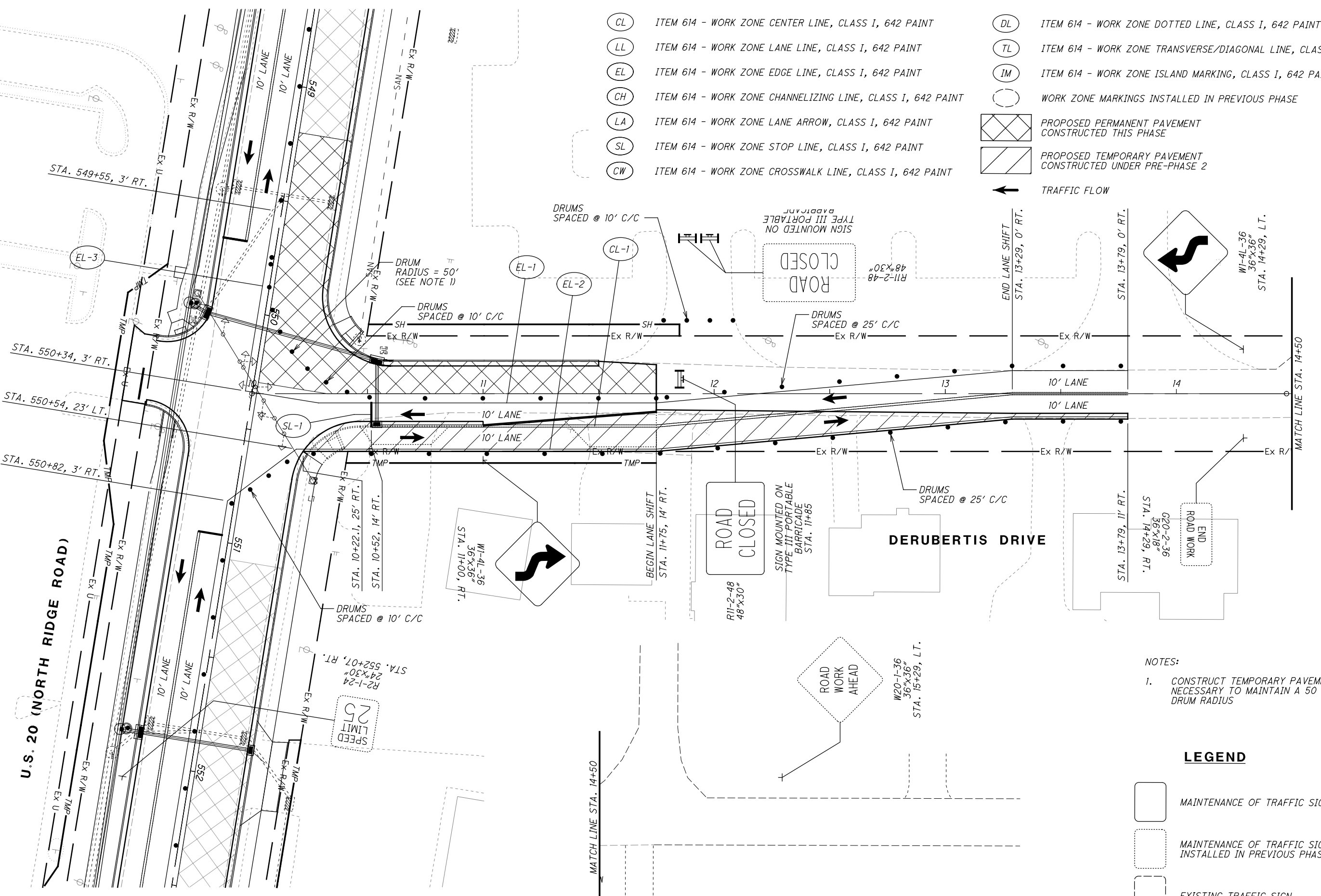


NOTES:
 1. CONSTRUCT TEMPORARY PAVEMENT AS NECESSARY TO MAINTAIN A 50 FOOT DRUM RADIUS

- LEGEND**
- MAINTENANCE OF TRAFFIC SIGN
 - MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
 - EXISTING TRAFFIC SIGN

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP294J.dgn Sheet 7/15/2022 2:51:38 PM Troyer

U.S. 20 (NORTH RIDGE ROAD)



LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | (○) | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

CALCULATED EJT
CHECKED WDB

0 20 40
HORIZONTAL SCALE IN FEET

LAK-US-20-24.99 PART 2
MAINTENANCE OF TRAFFIC PLAN - PHASE 2B
DERUBERTIS DR - STA. 549+00 TO STA. 554+00

NOTES:
1. CONSTRUCT TEMPORARY PAVEMENT AS NECESSARY TO MAINTAIN A 50 FOOT DRUM RADIUS

LEGEND

- (□) MAINTENANCE OF TRAFFIC SIGN
- (□) MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- (□) EXISTING TRAFFIC SIGN






CALCULATED
 EJT
 CHECKED
 WDB












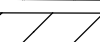


0 20 40
 HORIZONTAL
 SCALE IN FEET

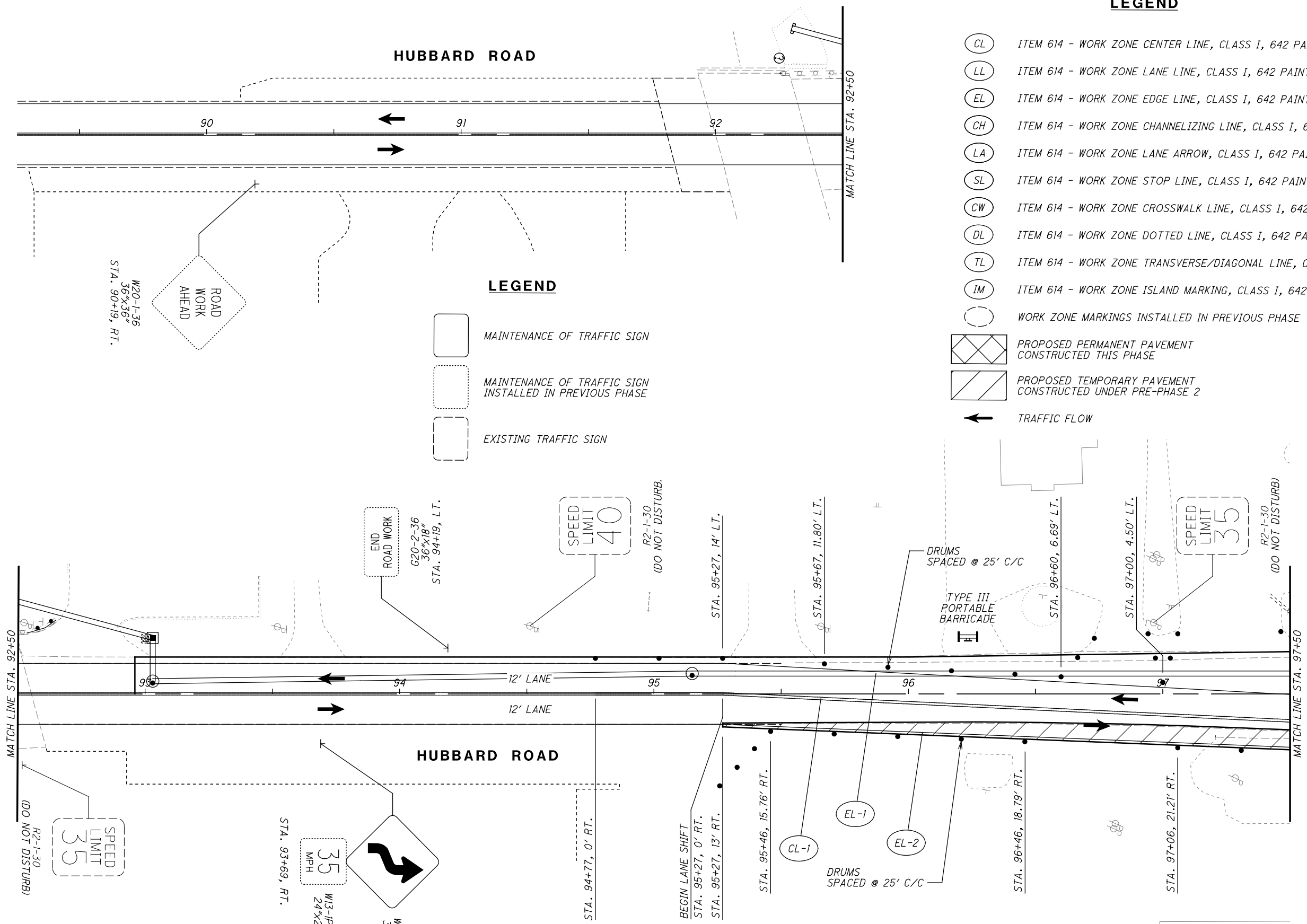
MAINTENANCE OF TRAFFIC PLAN - PHASE 2
HUBBARD ROAD

LAK-US-20-24.99
PART 2

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |



LEGEND

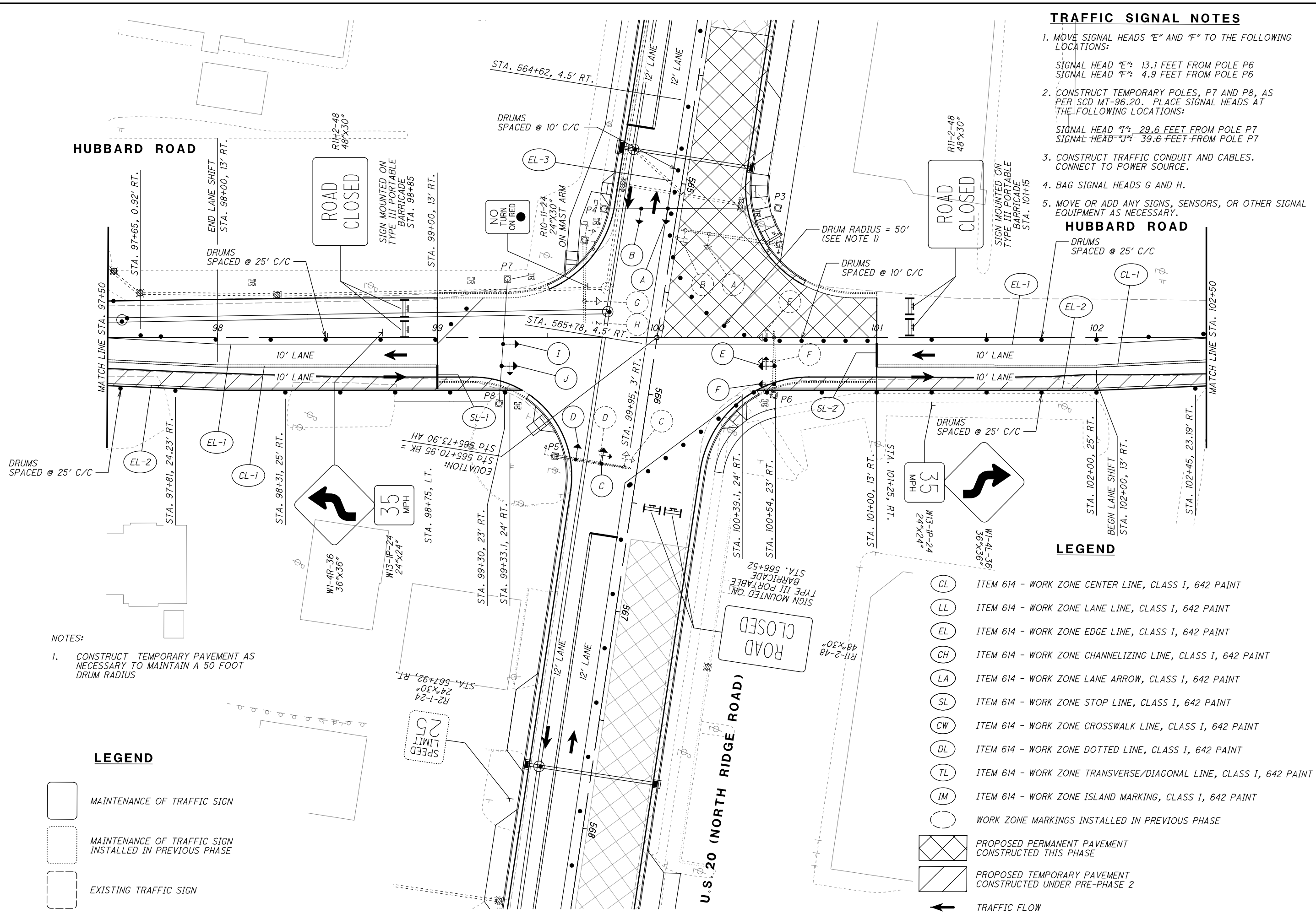
- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

LEGEND

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2

← TRAFFIC FLOW

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP296.dgn Sheet 7/15/2022 2:51:41PM troyer



TRAFFIC SIGNAL NOTES

1. MOVE SIGNAL HEADS "E" AND "F" TO THE FOLLOWING LOCATIONS:
 SIGNAL HEAD "E": 13.1 FEET FROM POLE P6
 SIGNAL HEAD "F": 4.9 FEET FROM POLE P6
2. CONSTRUCT TEMPORARY POLES, P7 AND P8, AS PER SCD MT-96.20. PLACE SIGNAL HEADS AT THE FOLLOWING LOCATIONS:
 SIGNAL HEAD "I": 29.6 FEET FROM POLE P7
 SIGNAL HEAD "J": 39.6 FEET FROM POLE P7
3. CONSTRUCT TRAFFIC CONDUIT AND CABLES. CONNECT TO POWER SOURCE.
4. BAG SIGNAL HEADS G AND H.
5. MOVE OR ADD ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.

HUBBARD ROAD

DRUMS SPACED @ 25' C/C

LEGEND

- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- () WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- (X) PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- (/) PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
- (→) TRAFFIC FLOW

- NOTES:**
1. CONSTRUCT TEMPORARY PAVEMENT AS NECESSARY TO MAINTAIN A 50 FOOT DRUM RADIUS

LEGEND

- () MAINTENANCE OF TRAFFIC SIGN
- () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- () EXISTING TRAFFIC SIGN

LAK-US-20-24.99 PART 2

MAINTENANCE OF TRAFFIC PLAN - PHASE 2A

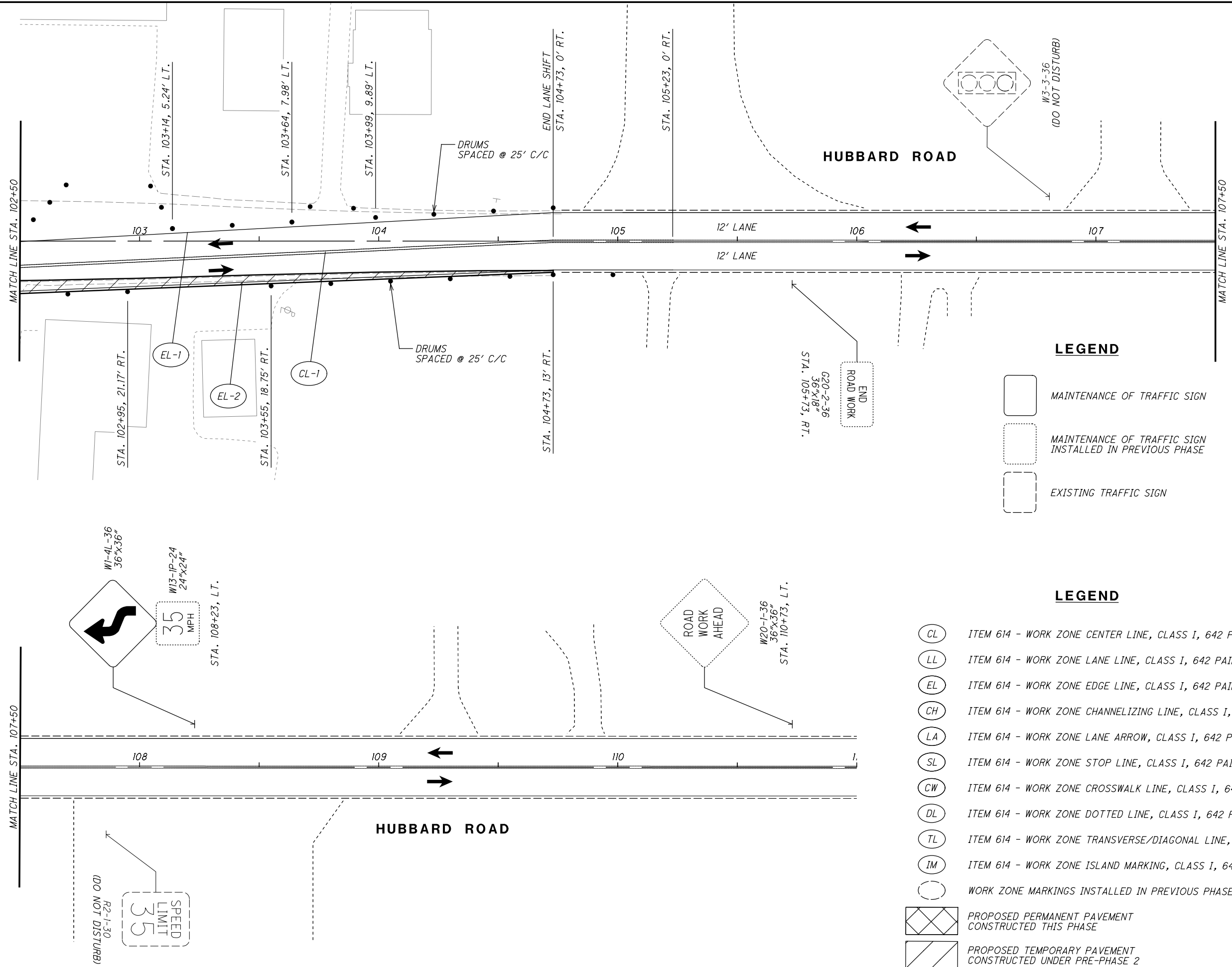
HUBBARD RD - STA. 97+50 TO STA. 102+50

CALCULATED: EJT




CHECKED: WDB

129













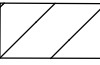

697

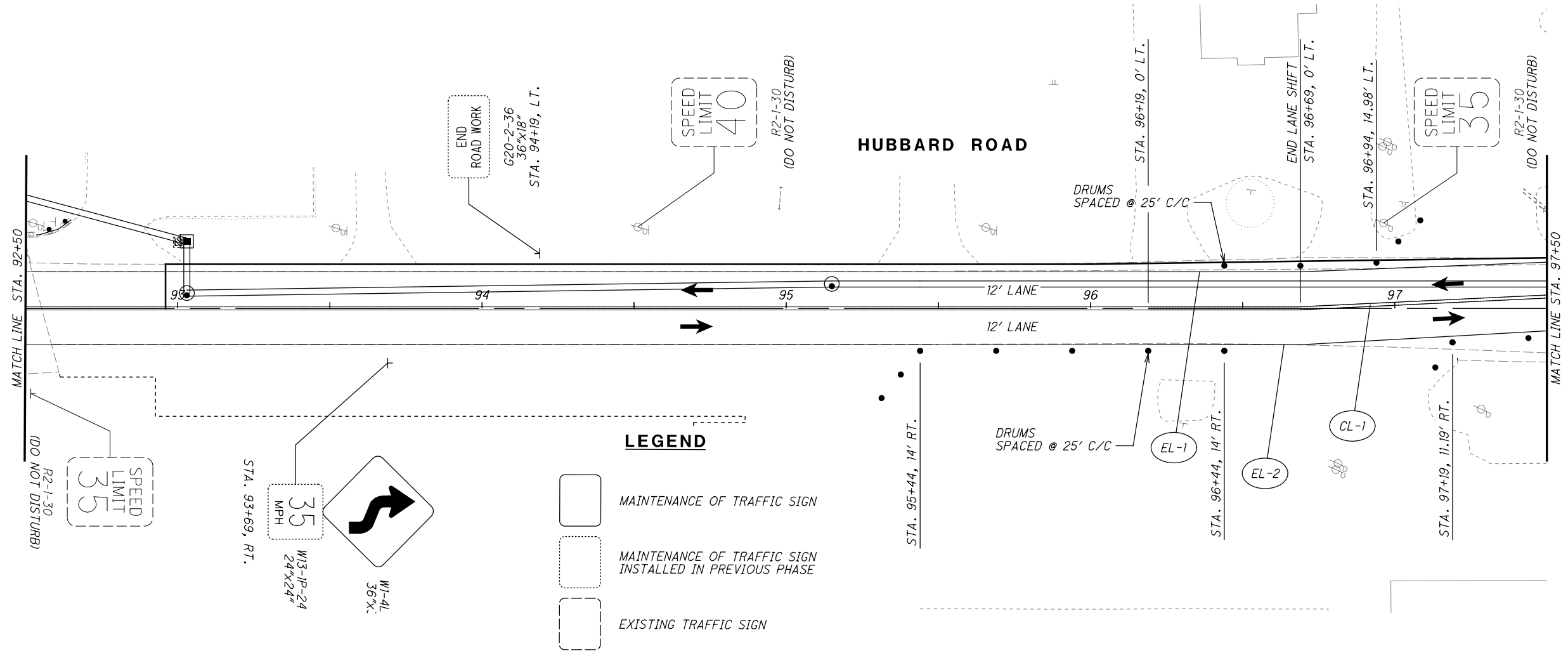
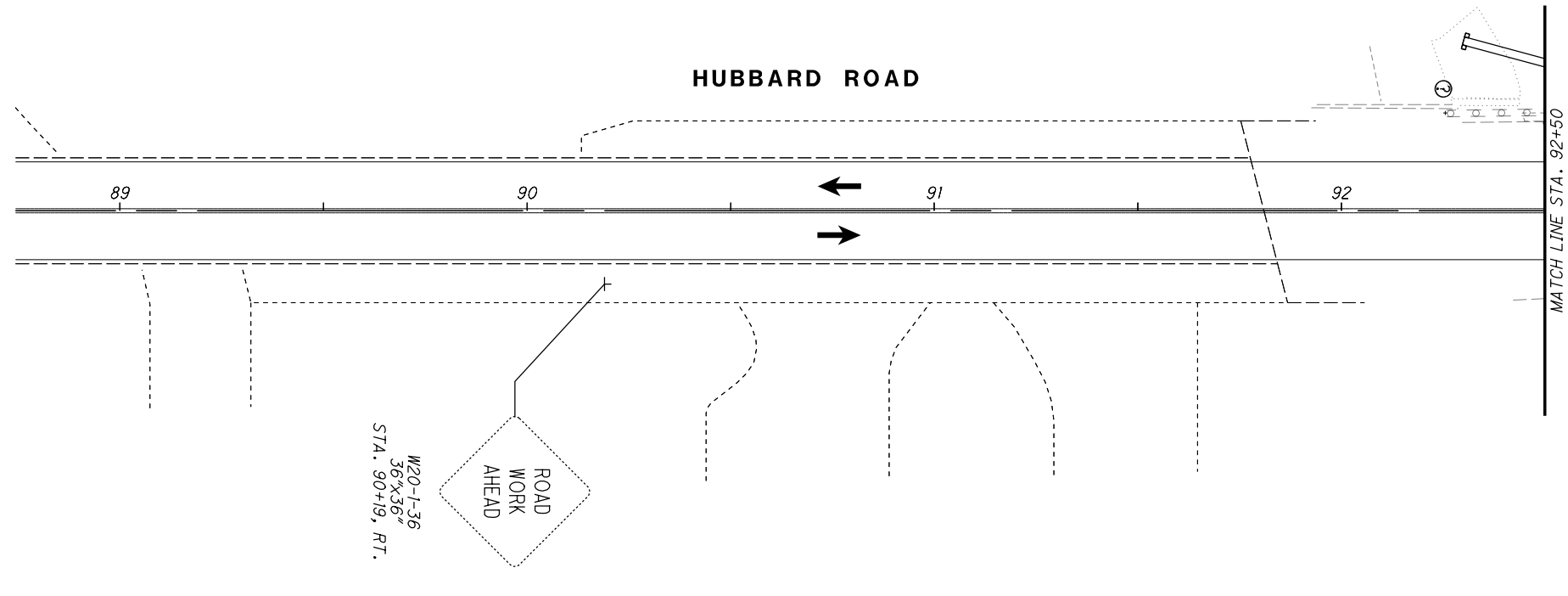


LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

LEGEND

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
-  TRAFFIC FLOW



LEGEND

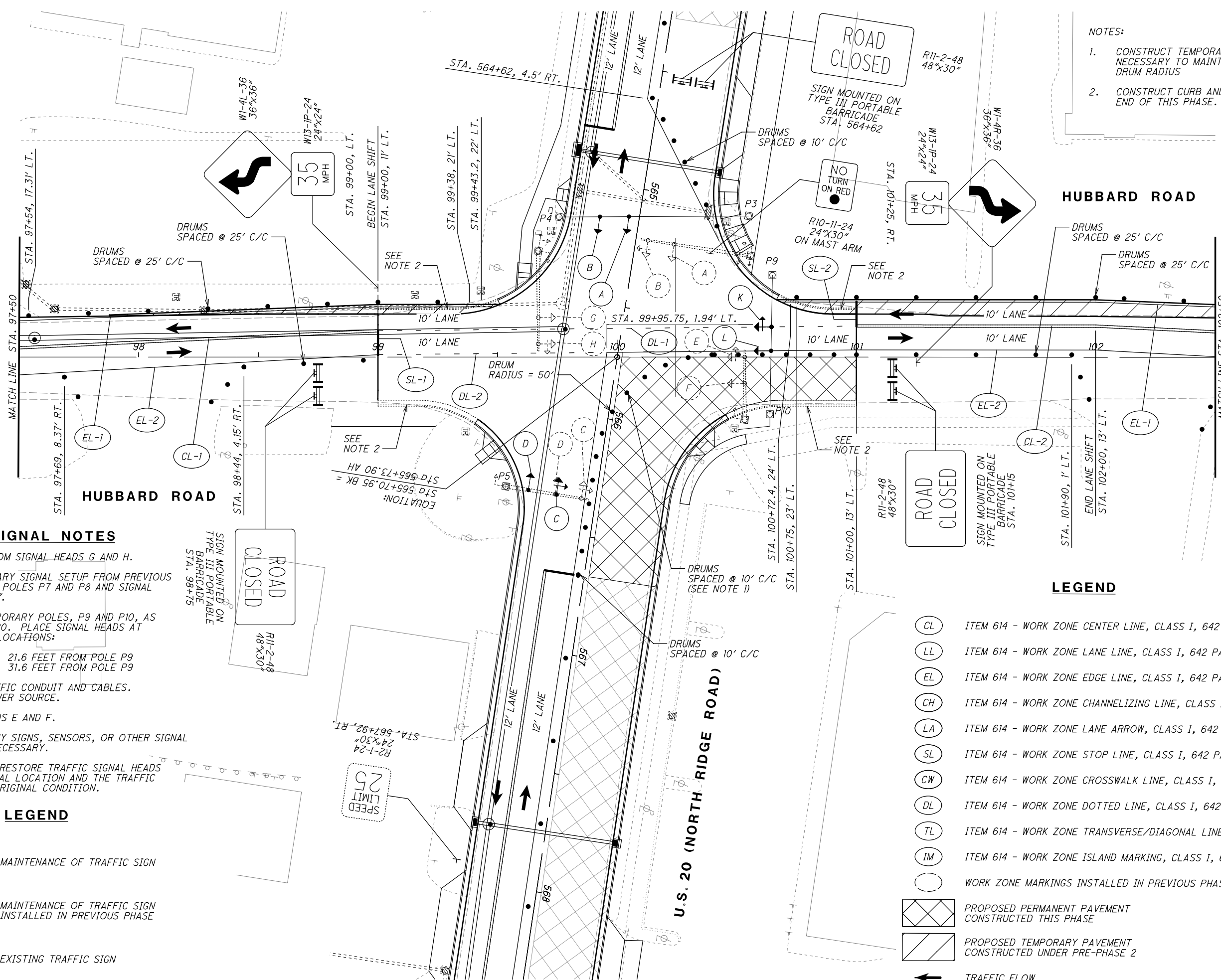
- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- (○) WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- (▨) PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- (▨) PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
- ← TRAFFIC FLOW

LEGEND

- (□) MAINTENANCE OF TRAFFIC SIGN
- (□) MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- (□) EXISTING TRAFFIC SIGN



H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP297.dgn Sheet 7/15/2022 2:51:44 PM troyer



- NOTES:
- CONSTRUCT TEMPORARY PAVEMENT AS NECESSARY TO MAINTAIN A 50 FOOT DRUM RADIUS
 - CONSTRUCT CURB AND GUTTER AT THE END OF THIS PHASE.

CALCULATED
EJT
CHECKED
WDB

TRAFFIC SIGNAL NOTES

- REMOVE BAGS FROM SIGNAL HEADS G AND H.
- REMOVE TEMPORARY SIGNAL SETUP FROM PREVIOUS PHASE (INCLUDES POLES P7 AND P8 AND SIGNAL HEADS "I" AND "J").
- CONSTRUCT TEMPORARY POLES, P9 AND P10, AS PER SCD MT-96.20. PLACE SIGNAL HEADS AT THE FOLLOWING LOCATIONS:
 SIGNAL HEAD "K": 21.6 FEET FROM POLE P9
 SIGNAL HEAD "L": 31.6 FEET FROM POLE P9
- CONSTRUCT TRAFFIC CONDUIT AND CABLES. CONNECT TO POWER SOURCE.
- BAG SIGNAL HEADS E AND F.
- MOVE OR ADD ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.
- AFTER PHASE 2, RESTORE TRAFFIC SIGNAL HEADS TO THEIR ORIGINAL LOCATION AND THE TRAFFIC SIGNAL TO ITS ORIGINAL CONDITION.

LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

LEGEND

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
- TRAFFIC FLOW

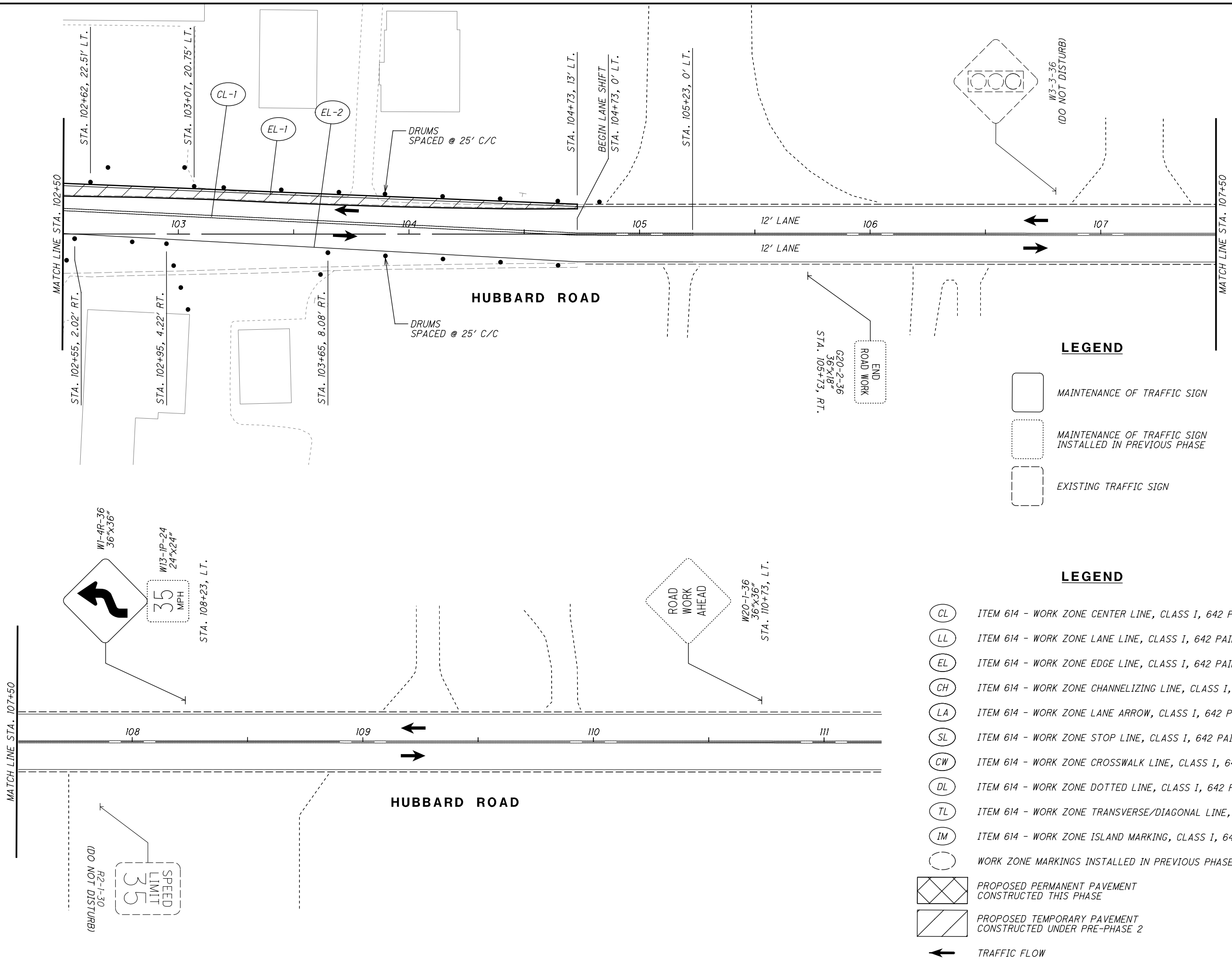
EQUATION:
 $STA. 565+70.95 BK =$
 $STA. 565+73.90 AH$






CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 2B
HUBBARD ROAD - STA. 102+50 TO STA. 110+50













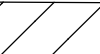

LAK-US-20-24.99
PART 2



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

LEGEND

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2
-  TRAFFIC FLOW

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP4284H.dgn Sheet 7/15/2022 2:51:46 PM troyer



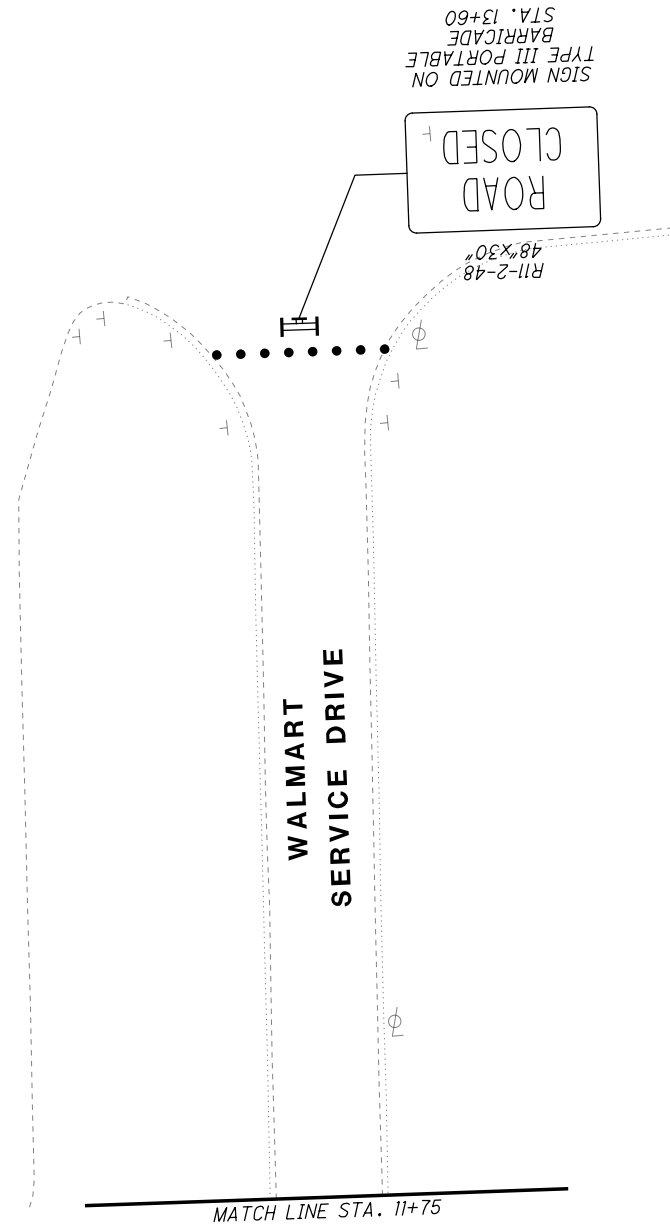
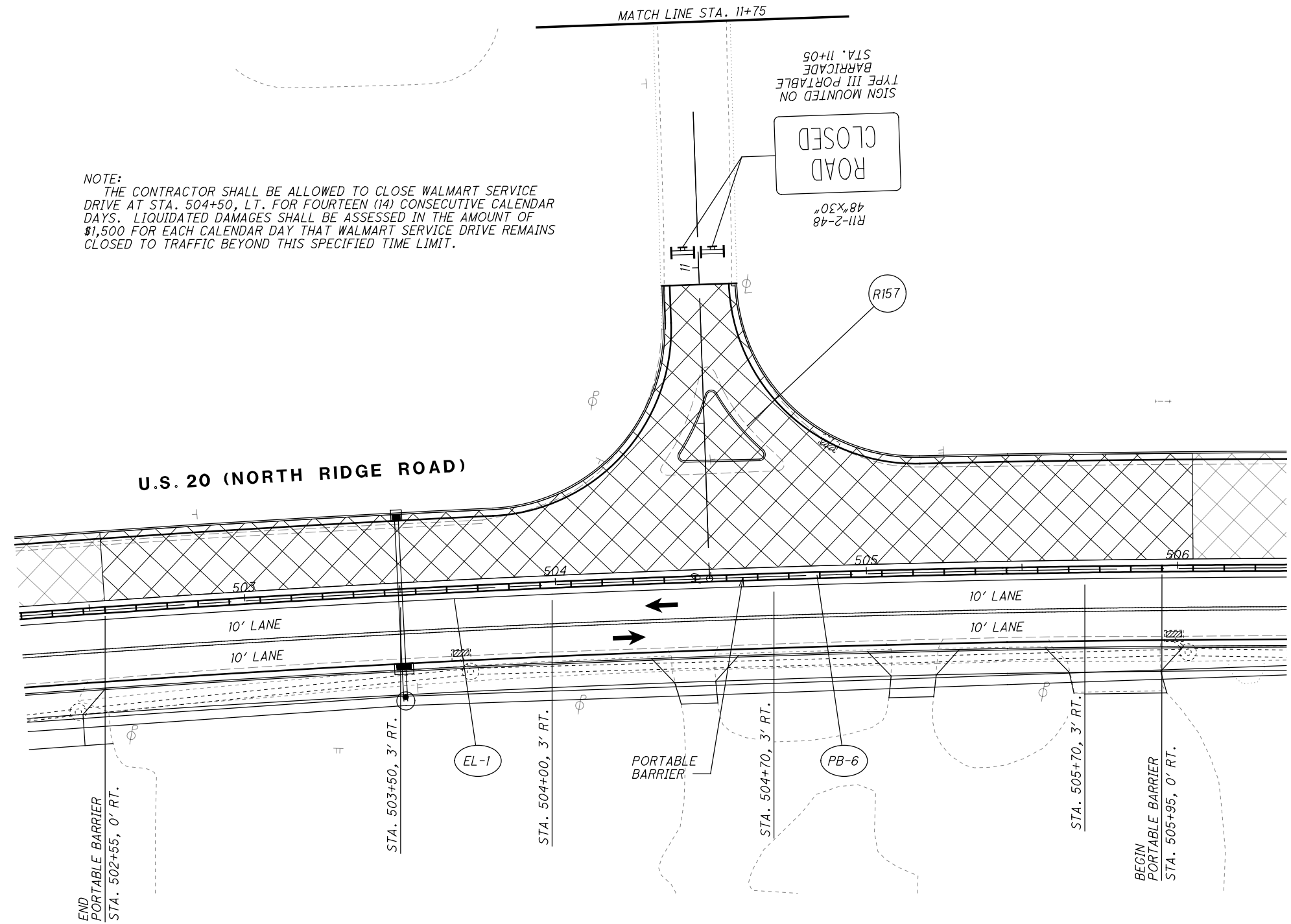


 HORIZONTAL SCALE IN FEET












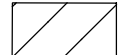


MAINTENANCE OF TRAFFIC PLAN - PHASE 2
WALMART SERVICE DRIVE

LAK-US-20-24.99
PART 2




NOTE:
 THE CONTRACTOR SHALL BE ALLOWED TO CLOSE WALMART SERVICE DRIVE AT STA. 504+50, LT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT WALMART SERVICE DRIVE REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.



LEGEND

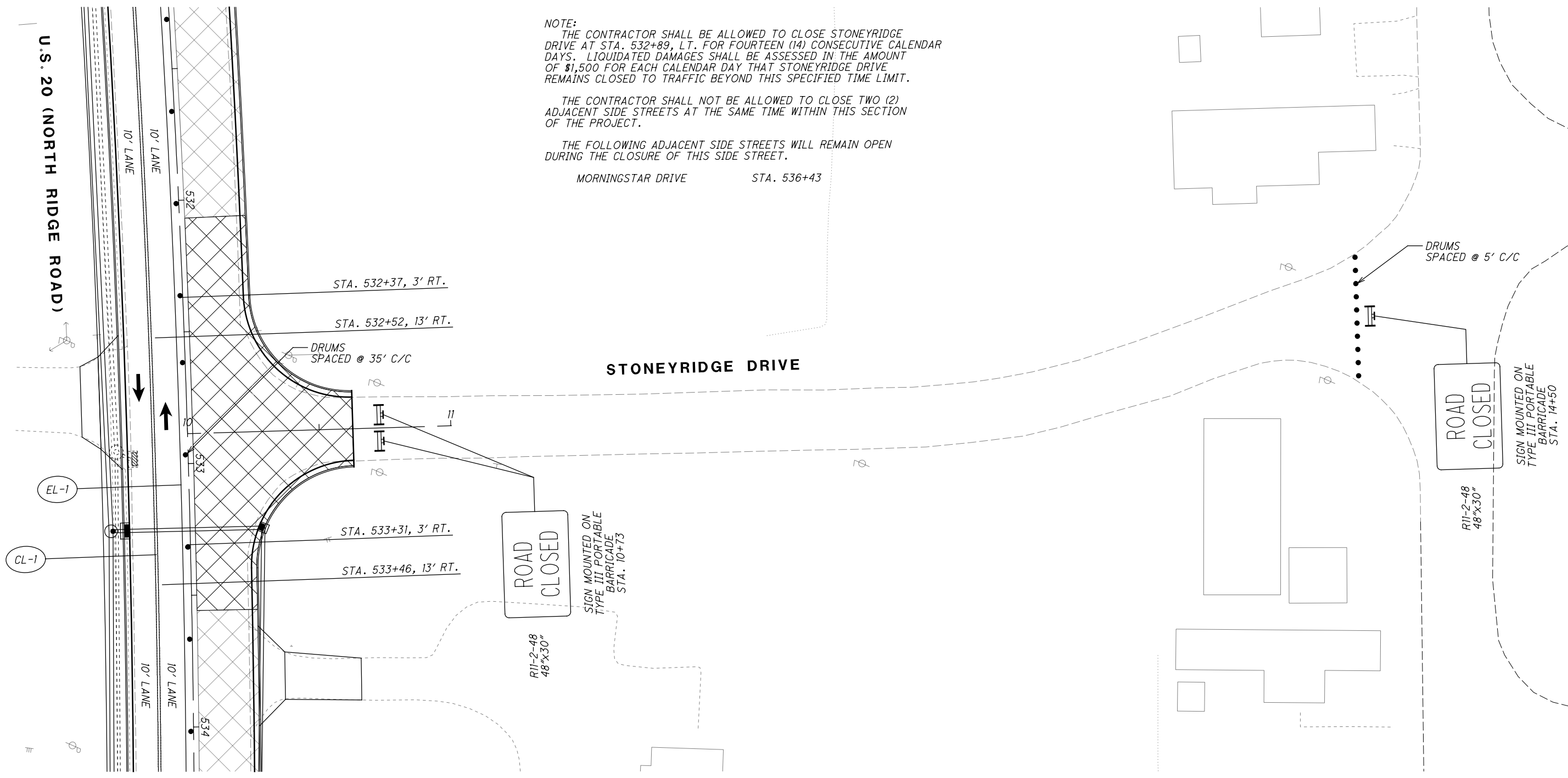
- | | | | |
|-------------------------------------------------------------------------------------|------------------------------------------------------------|--------------------------------------------------------------------------------------|-------------------------------------------------------------------|
|  | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
|  | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  | TRAFFIC FLOW |

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP4285.dgn Sheet 7/15/2022 2:51:47 PM Troyer

U.S. 20 (NORTH RIDGE ROAD)






NOTE:
 THE CONTRACTOR SHALL BE ALLOWED TO CLOSE STONEYRIDGE DRIVE AT STA. 532+89, LT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT STONEYRIDGE DRIVE REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.












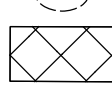
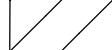
THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT SIDE STREETS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

THE FOLLOWING ADJACENT SIDE STREETS WILL REMAIN OPEN DURING THE CLOSURE OF THIS SIDE STREET.

MORNINGSTAR DRIVE STA. 536+43

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2

 TRAFFIC FLOW



CALCULATED	EJT
CHECKED	WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 2
STONEYRIDGE DRIVE

LAK-US-20-24.99
PART 2

135
697

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP4286.dgn Sheet 7/15/2022 2:51:47 PM Troyer

U.S. 20 (NORTH RIDGE ROAD)

EL-1
CL-1

10' LANE
10' LANE

STA. 535+96, 3' RT.
STA. 536+11, 13' RT.

DRUMS
SPACED @ 35' C/C

STA. 536+99, 3' RT.
STA. 537+14, 13' RT.

ROAD CLOSED
SIGN MOUNTED ON
TYPE III PORTABLE
BARRICADE
STA. 10+89

R11-2-48
48"x30"

DRUMS
SPACED @ 5' C/C

ROAD CLOSED
SIGN MOUNTED ON
TYPE III PORTABLE
BARRICADE
STA. 12+65

R11-2-48
48"x30"

MORNINGSTAR DRIVE

SIGN LOCATED AT
INTERSECTION
YELLOW SPRINGS DR. &
MORNINGSTAR DR.
ROAD CLOSED
0.2 MILES AHEAD
LOCAL TRAFFIC ONLY

R11-3a-60
60"x30"




SIGN MOUNTED ON
TYPE III PORTABLE










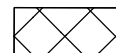

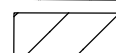


NOTE:
THE CONTRACTOR SHALL BE ALLOWED TO CLOSE MORNINGSTAR DRIVE AT STA. 536+43, LT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT MORNINGSTAR DRIVE REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT SIDE STREETS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

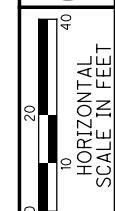
THE FOLLOWING ADJACENT SIDE STREETS WILL REMAIN OPEN DURING THE CLOSURE OF THIS SIDE STREET.
STONEYRIDGE DRIVE STA. 532+89

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

LEGEND



H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP4290.dgn Sheet 7/15/2022 2:51:48 PM Troyer



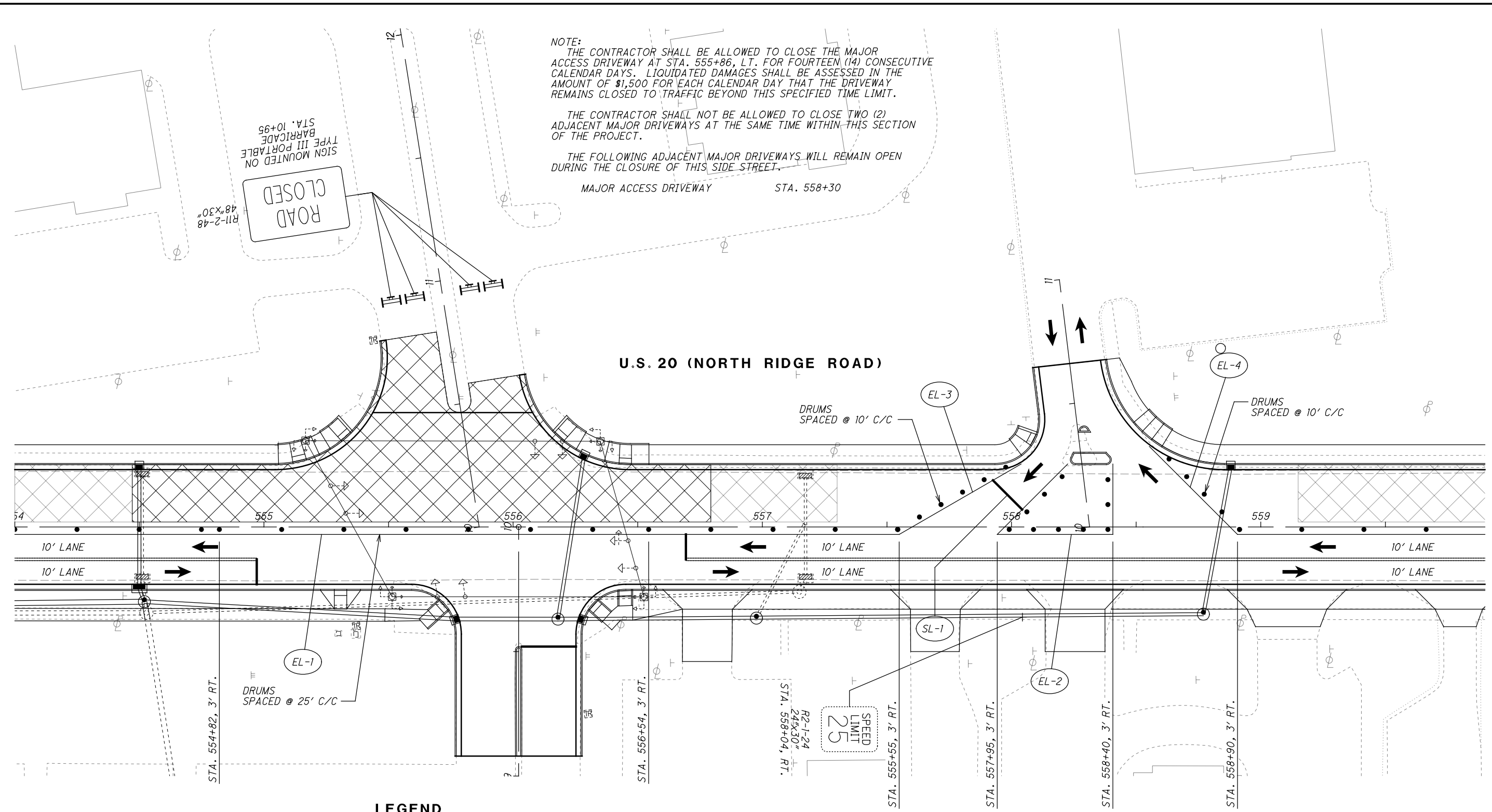
CALCULATED: EJT
 CHECKED: WDB

NOTE:
 THE CONTRACTOR SHALL BE ALLOWED TO CLOSE THE MAJOR ACCESS DRIVEWAY AT STA. 555+86, LT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT THE DRIVEWAY REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

 THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT MAJOR DRIVEWAYS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

 THE FOLLOWING ADJACENT MAJOR DRIVEWAYS WILL REMAIN OPEN DURING THE CLOSURE OF THIS SIDE STREET.

 MAJOR ACCESS DRIVEWAY STA. 558+30



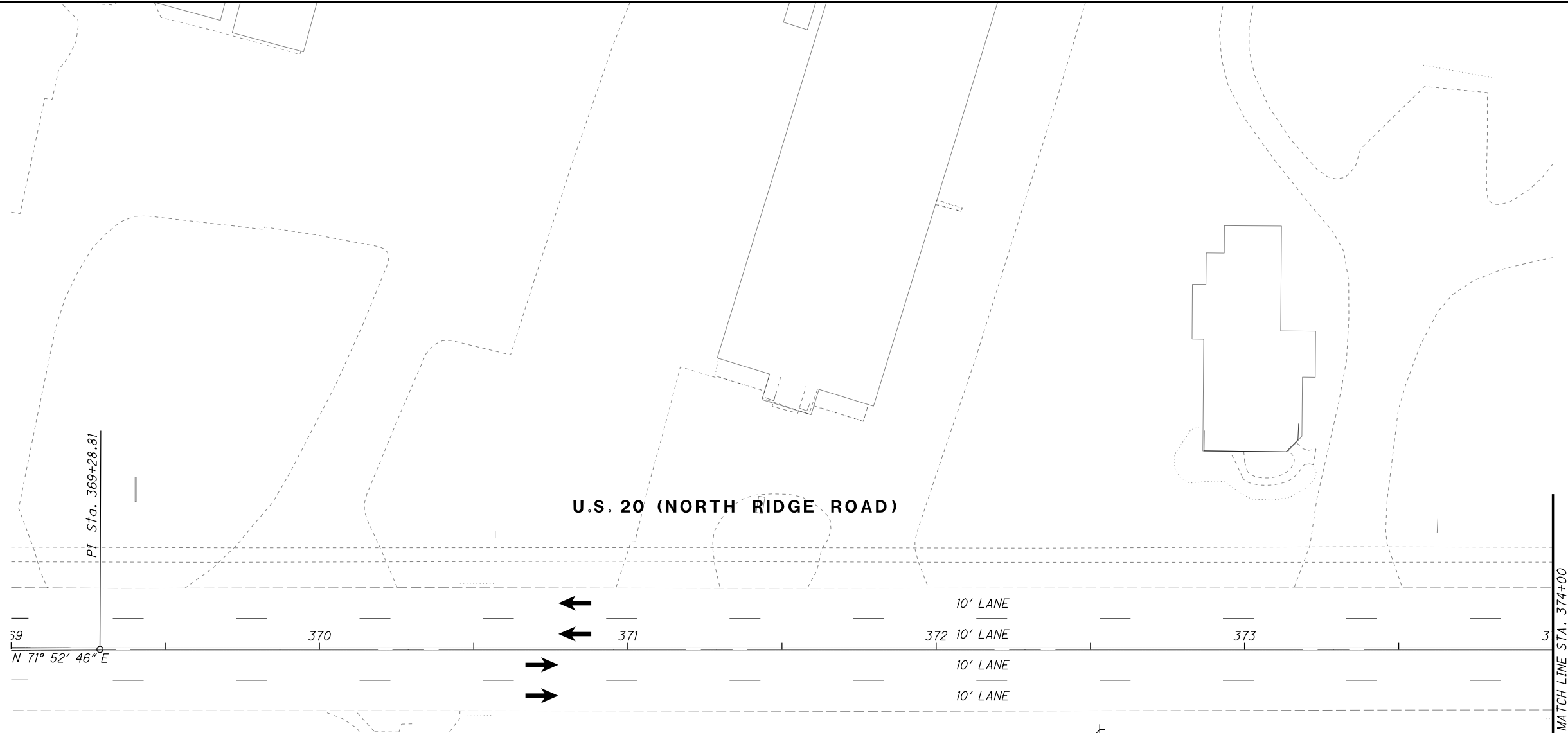
LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | () | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 2 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |




LEGEND








- () MAINTENANCE OF TRAFFIC SIGN
- () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- () EXISTING TRAFFIC SIGN








H:\2018\180743\ODOT\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP065F.dgn Sheet 7/15/2022 2:51:49 PM Troyer

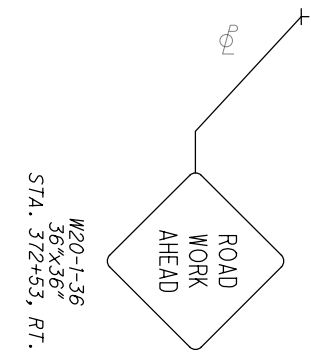


U.S. 20 (NORTH RIDGE ROAD)

- LEGEND**
-  MAINTENANCE OF TRAFFIC SIGN
 -  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
 -  EXISTING TRAFFIC SIGN

- LEGEND**
-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

- LEGEND**
-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
 -  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
 -  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
 -  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
 -  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
 -  TRAFFIC FLOW



CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 369+00 TO STA. 374+00

LAK-US-20-24.99
PART 2

138
697

H:\2018\80743\ODOT\ProjectData\09270_LAK_20_EAST\Design\DOT\Sheets\09270_MP065G.dgn Sheet 7/15/2022 2:51:50 PM Troyer

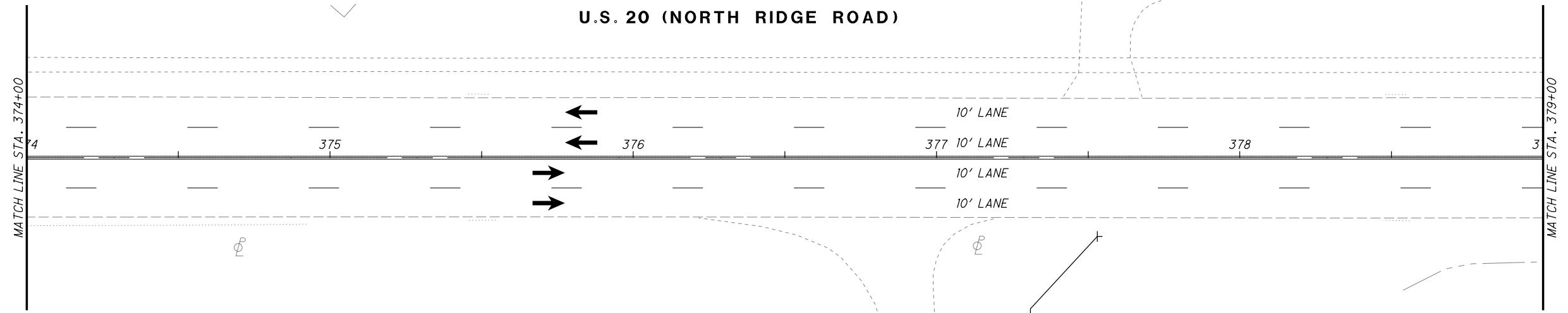
CALCULATED
EJT
CHECKED
WDB

HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 374+00 TO STA. 379+00

LAK-US-20-24.99
PART 2

139
697



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP066.dgn Sheet 7/15/2022 2:51:50 PM troyer

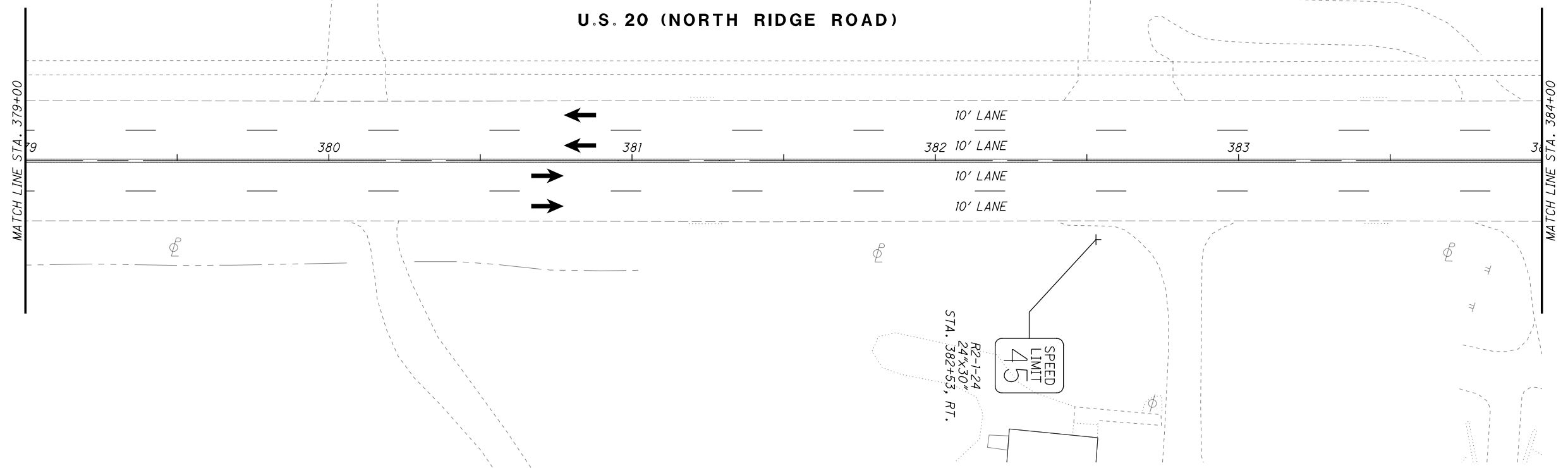
CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET




MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 379+00 TO STA. 384+00

LAK-US-20-24.99
PART 2













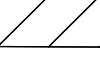

140
697



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

LEGEND

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW

R2-1-24
24"x30"
STA. 382+53, RT.
SPEED
LIMIT
45

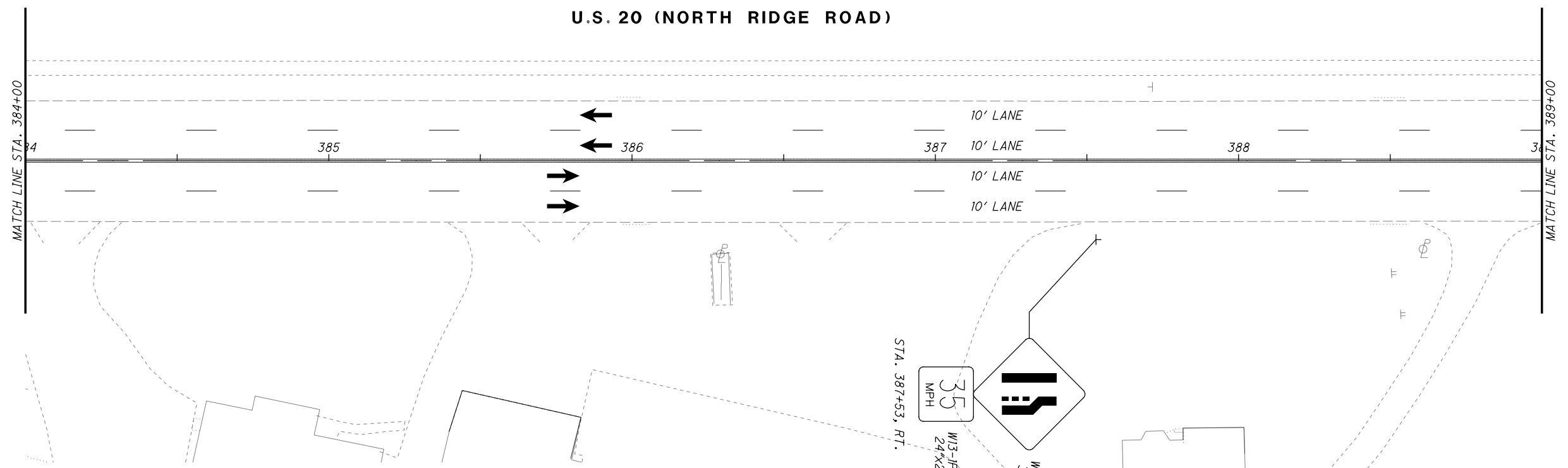
H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP066A.dgn Sheet 7/15/2022 2:51:51PM troyer

CALCULATED
EJT
CHECKED
WDB

0 10 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 384+00 TO STA. 389+00

LAK-US-20-24.99
PART 2



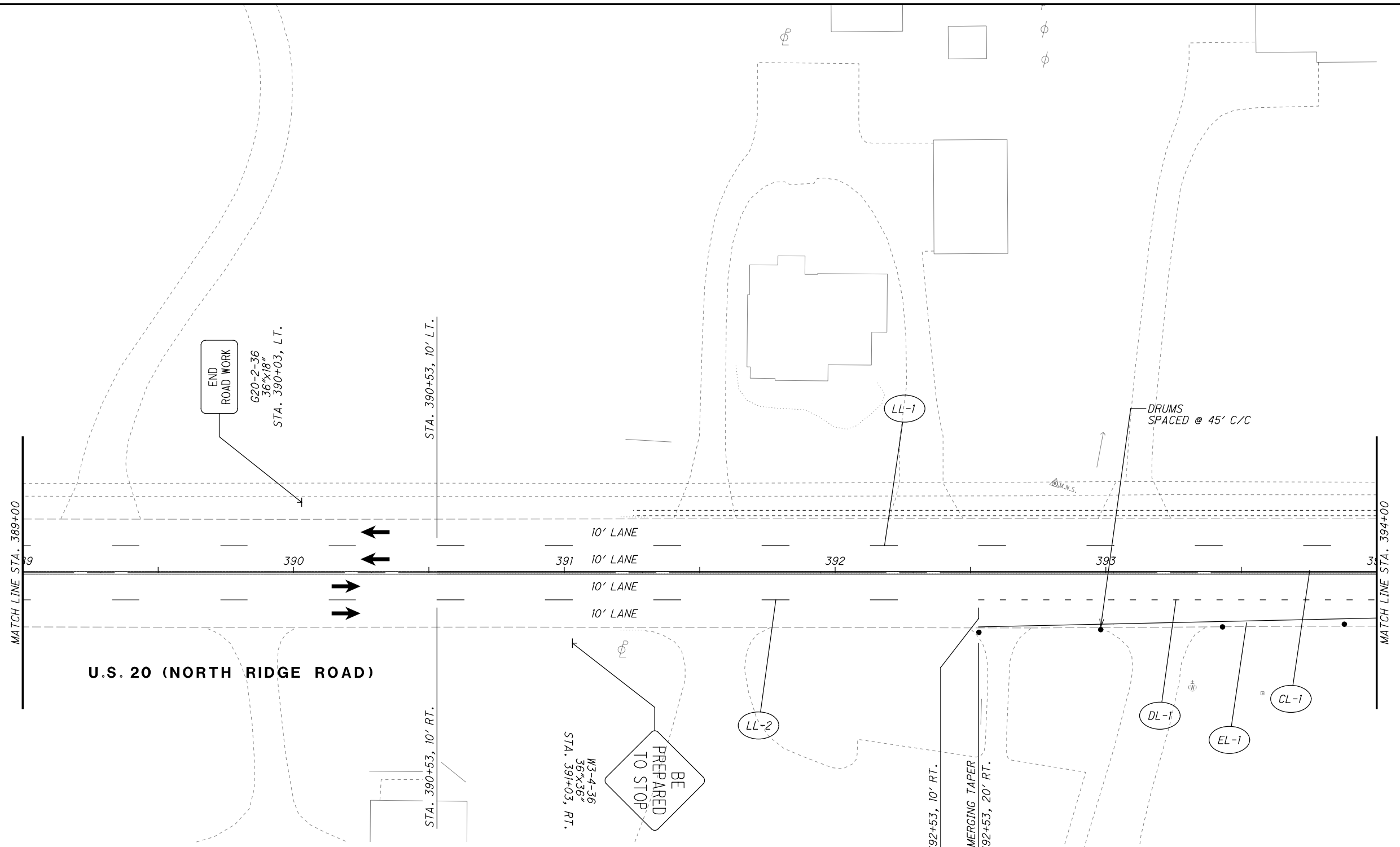
LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | () | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

LEGEND

- | | |
|-----|---------------------------------------------------------|
| () | MAINTENANCE OF TRAFFIC SIGN |
| () | MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE |
| () | EXISTING TRAFFIC SIGN |

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP066B.dgn Sheet 7/15/2022 2:51:52 PM Troyer



- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- () WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

- LEGEND**
- MAINTENANCE OF TRAFFIC SIGN
 - MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
 - EXISTING TRAFFIC SIGN

CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 389+00 TO STA. 394+00

LAK-US-20-24.99
PART 2

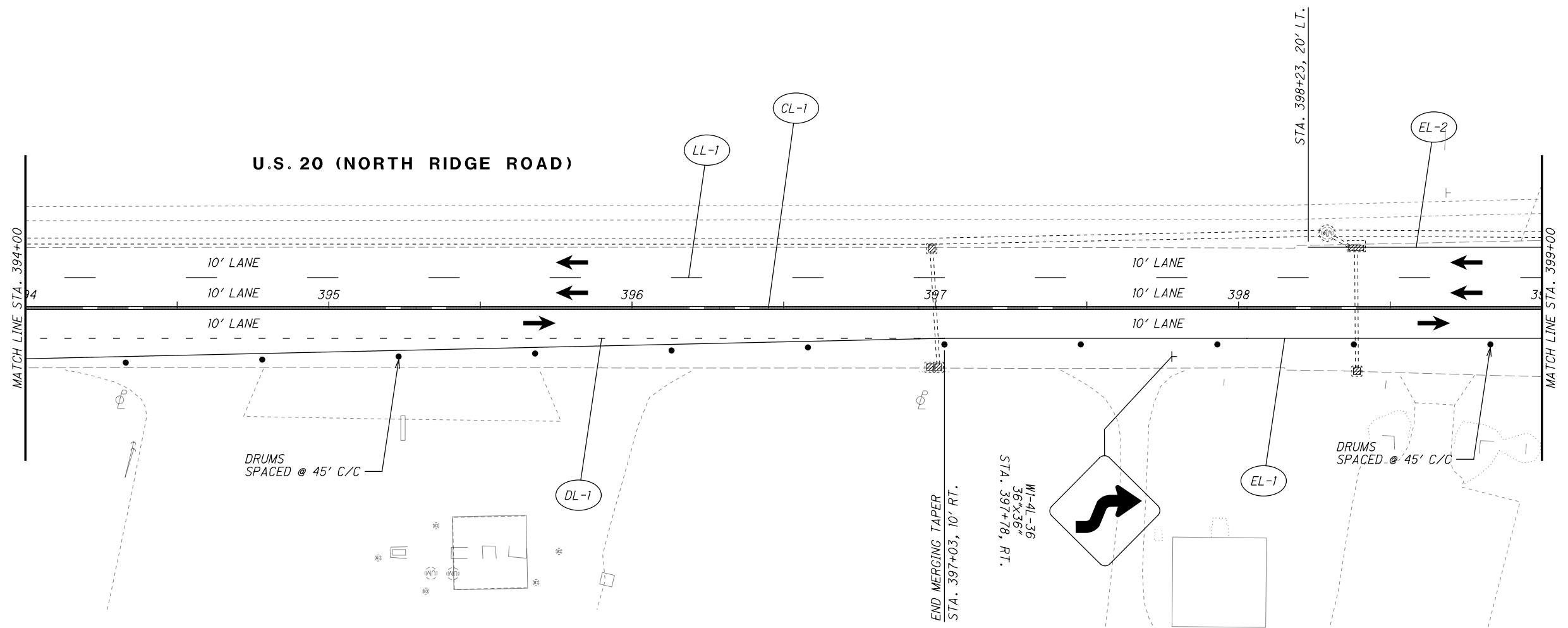
H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP066C.dgn Sheet 7/15/2022 2:51:52 PM Troyer

CALCULATED
EJT
CHECKED
WDB

0 10 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 394+00 TO STA. 399+00

LAK-US-20-24.99
PART 2



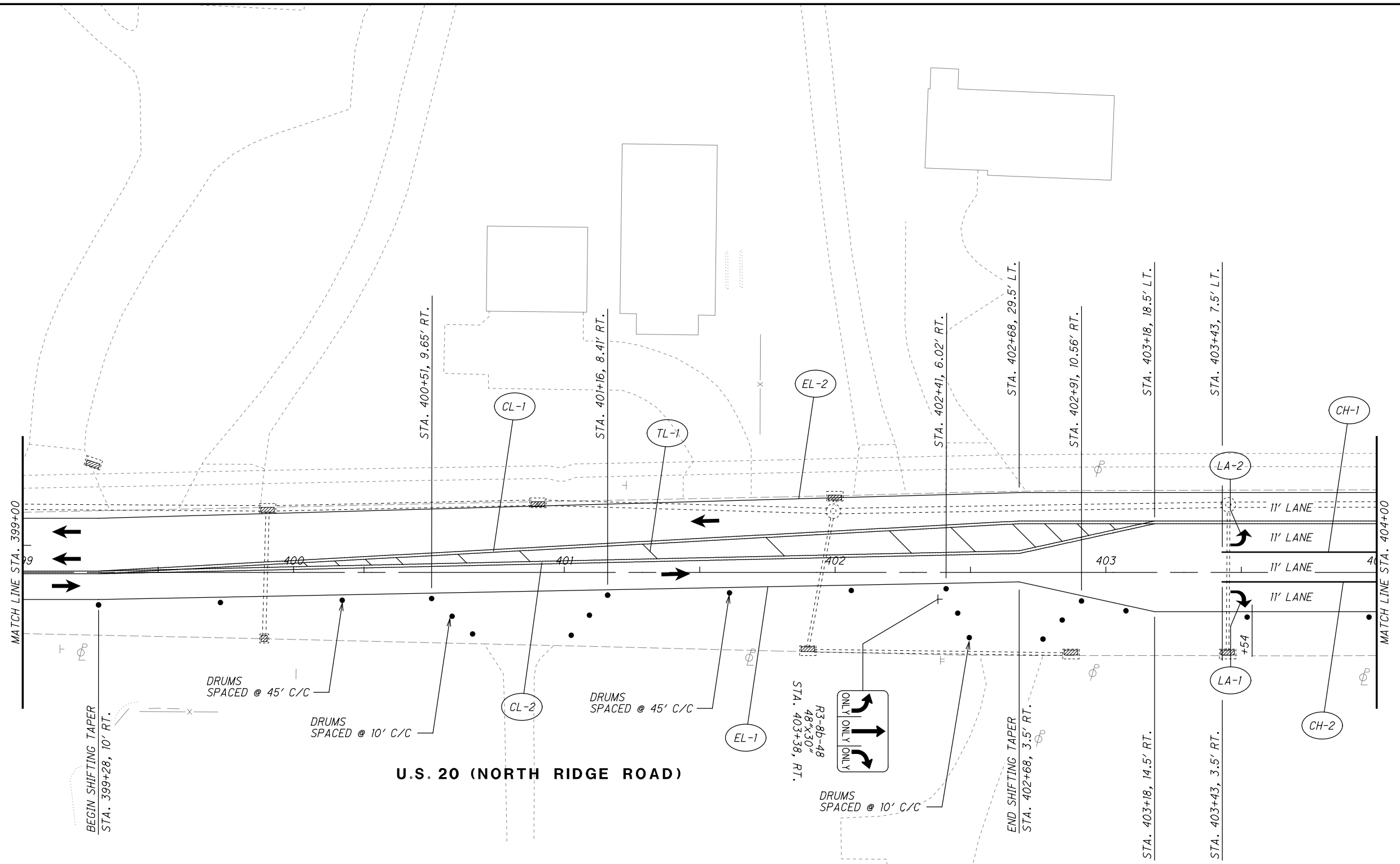
LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | () | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

LEGEND

- () MAINTENANCE OF TRAFFIC SIGN
- () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- () EXISTING TRAFFIC SIGN

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP066D.dgn Sheet 7/15/2022 2:51:53 PM Troyer



U.S. 20 (NORTH RIDGE ROAD)

LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | (○) | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

LEGEND

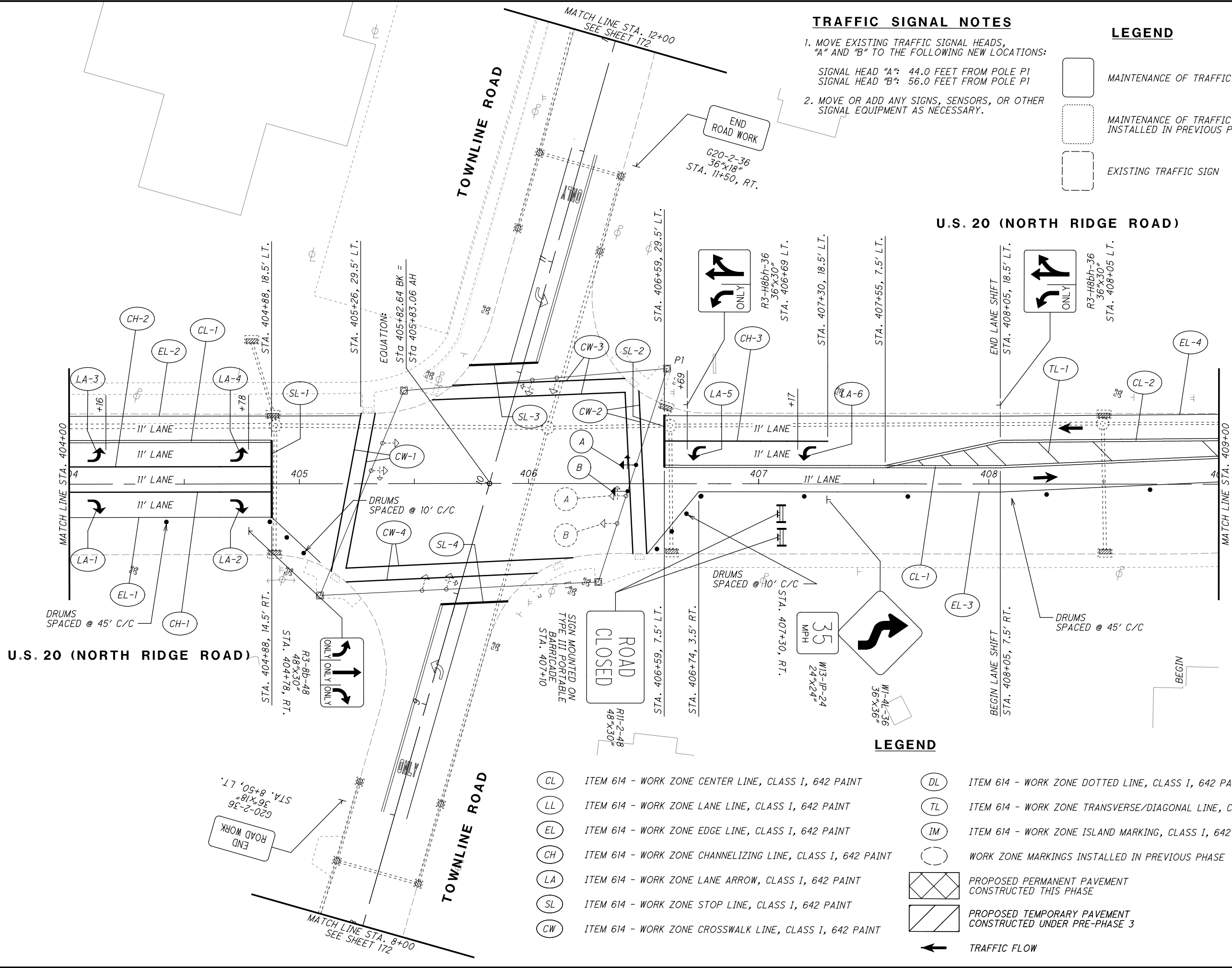
- | | |
|-----|---------------------------------------------------------|
| (□) | MAINTENANCE OF TRAFFIC SIGN |
| (□) | MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE |
| (□) | EXISTING TRAFFIC SIGN |

CALCULATED
EJT
CHECKED
WDB

0 10 20 40
HORIZONTAL
SCALE IN FEET

**MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 399+00 TO STA. 404+00**

**LAK-US-20-24.99
PART 2**



TRAFFIC SIGNAL NOTES

1. MOVE EXISTING TRAFFIC SIGNAL HEADS, "A" AND "B" TO THE FOLLOWING NEW LOCATIONS:
 SIGNAL HEAD "A": 44.0 FEET FROM POLE P1
 SIGNAL HEAD "B": 56.0 FEET FROM POLE P1
2. MOVE OR ADD ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.

LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

U.S. 20 (NORTH RIDGE ROAD)

LEGEND

- | | |
|------------------------------------------------------------|-------------------------------------------------------------------|
| ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
| ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | TRAFFIC FLOW |

CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL SCALE IN FEET

H:\2018\80743\00T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP066F.dgn Sheet 7/5/2022 2:51:54 PM Troyer

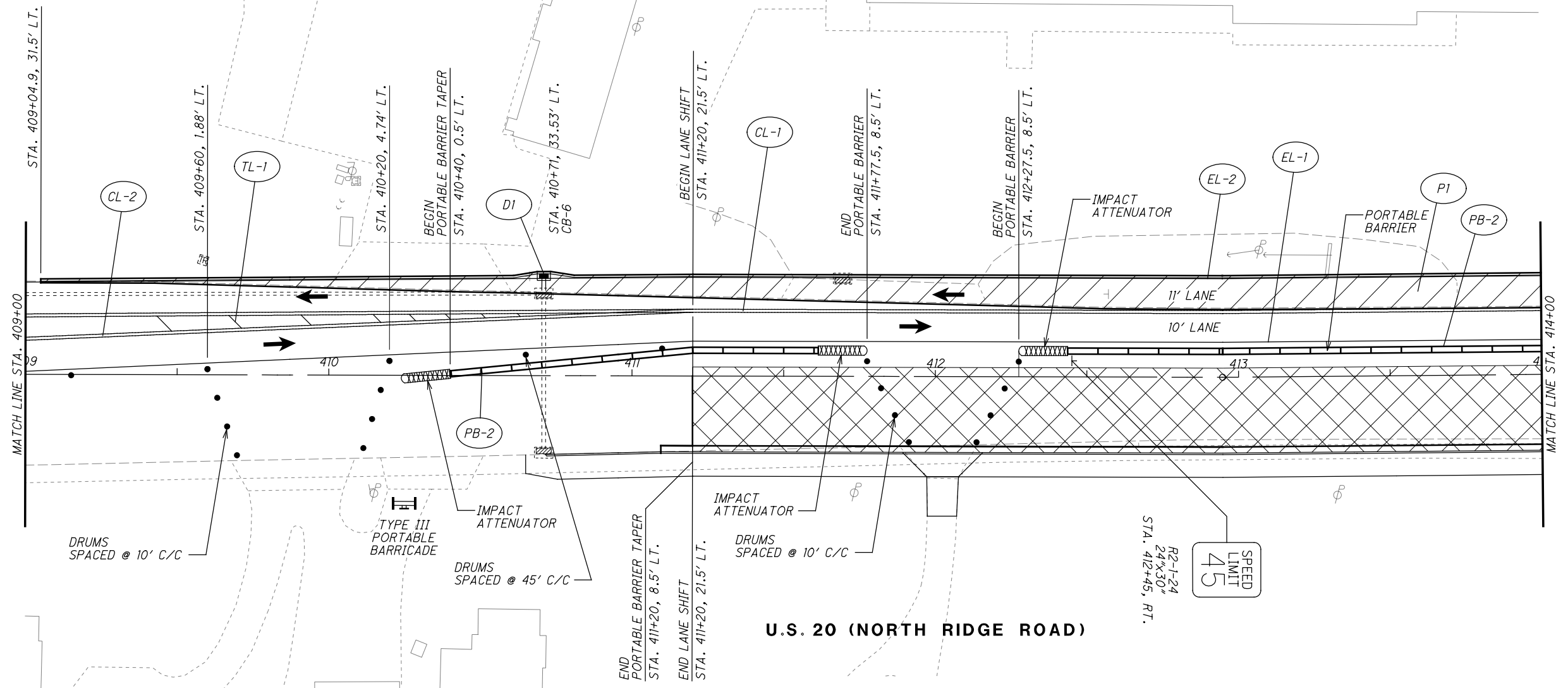
CALCULATED
 EJT
 CHECKED
 WDB

0 10 20 40
 HORIZONTAL
 SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 409+00 TO STA. 414+00

LAK-US-20-24.99
PART 2

NOTES
 1. ASSUMED LENGTH OF IMPACT ATTENUATOR
 = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP066G.dgn Sheet 7/5/2022 2:51:55 PM Troyer





 HORIZONTAL SCALE IN FEET

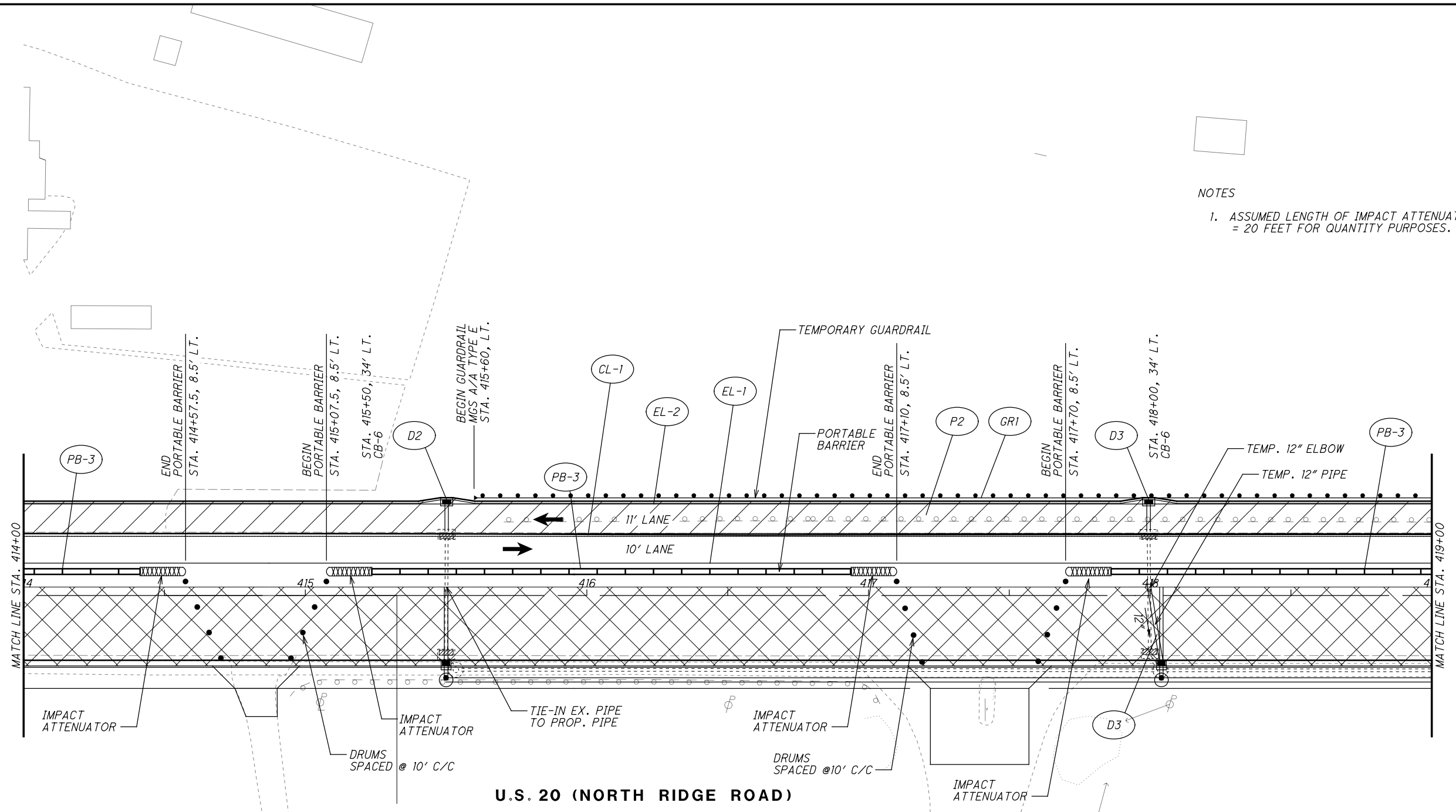
LAK-US-20-24.99
MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 414+00 TO STA. 419+00

PART 2

147
697




NOTES








1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.







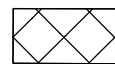
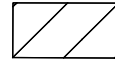

U.S. 20 (NORTH RIDGE ROAD)

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP066H.dgn Sheet 7/5/2022 2:51:56 PM Troyer



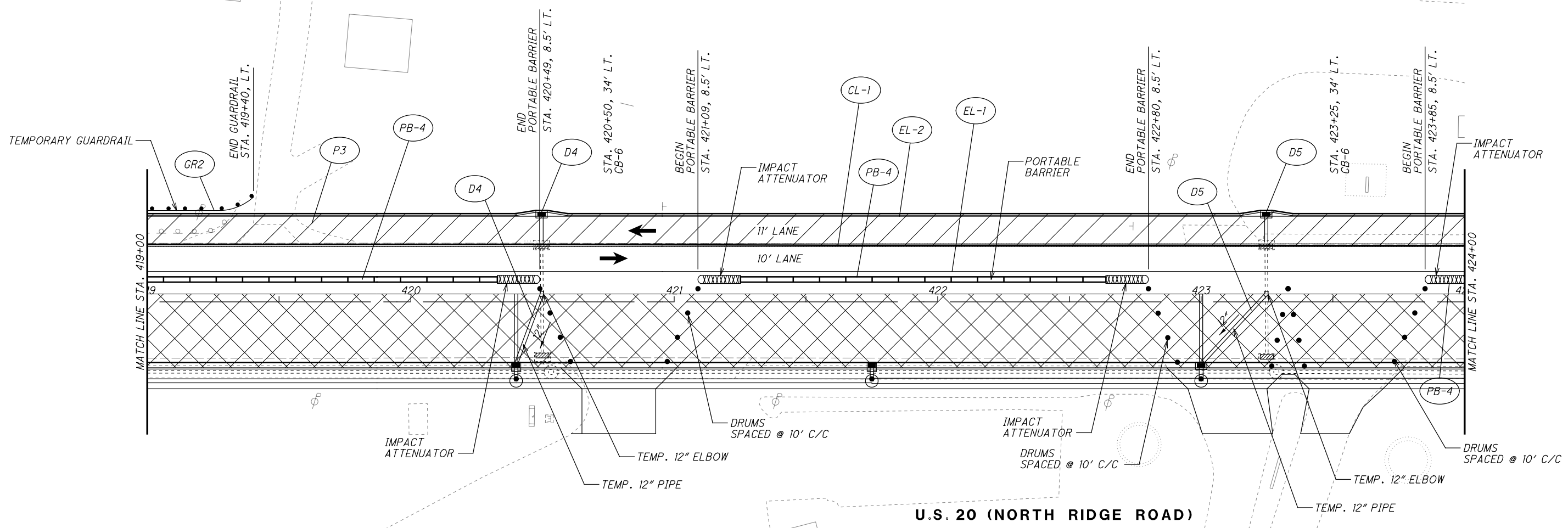
CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 419+00 TO STA. 424+00

LAK-US-20-24.99
PART 2

NOTES

1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\ODOT\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP0661.dgn Sheet 7/15/2022 2:51:57 PM troyer

CALCULATED
 EJT
 CHECKED
 WDB

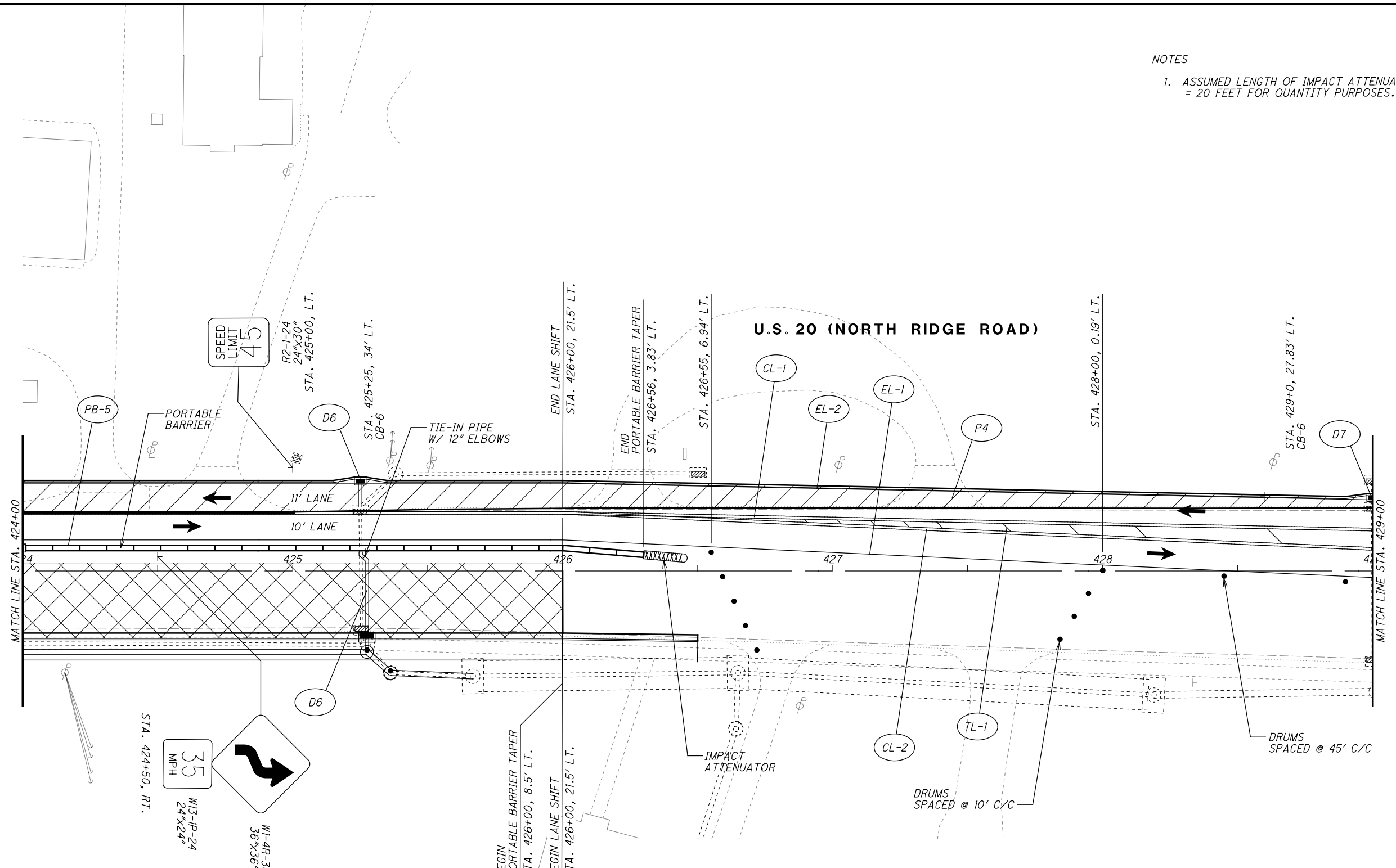
0 10 20 40
 HORIZONTAL
 SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 424+00 TO STA. 429+00

LAK-US-20-24.99
PART 2

NOTES

1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN




- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\180743\0D0T\ProjectData\092TO_LAK_20_EAST\Design\MOT\Sheets\09270_MP066J.dgn Sheet 7/15/2022 2:51:57 PM Troyer

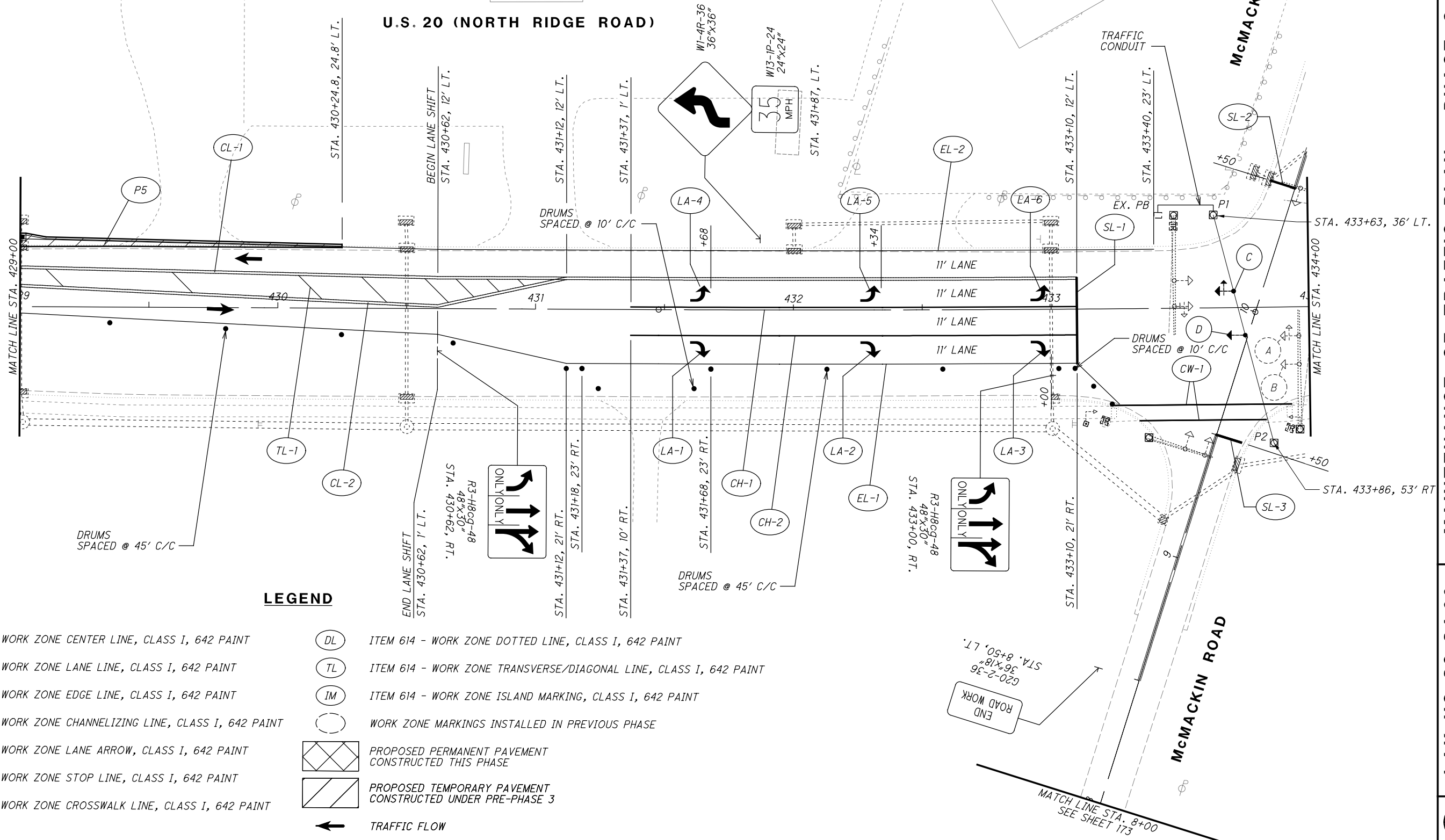
LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN












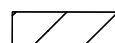


TRAFFIC SIGNAL NOTES

1. CONSTRUCT TEMPORARY POLES, P1 AND P2, AS PER SCD MT-96.20. PLACE SIGNAL HEADS AT THE FOLLOWING LOCATIONS:
 SIGNAL HEAD "C": 31 FEET FROM POLE P1
 SIGNAL HEAD "D": 48 FEET FROM POLE P1
2. CONSTRUCT TRAFFIC CONDUIT AND CABLES. CONNECT TO POWER SOURCE.
3. BAG EXISTING TRAFFIC SIGNAL HEADS, "A" AND "B".
4. MOVE OR ADD ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.

U.S. 20 (NORTH RIDGE ROAD)



LEGEND

- | | |
|------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

CALCULATED EJT
 CHECKED WDB



HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 429+00 TO STA. 434+00

LAK-US-20-24.99
PART 2

150
 697

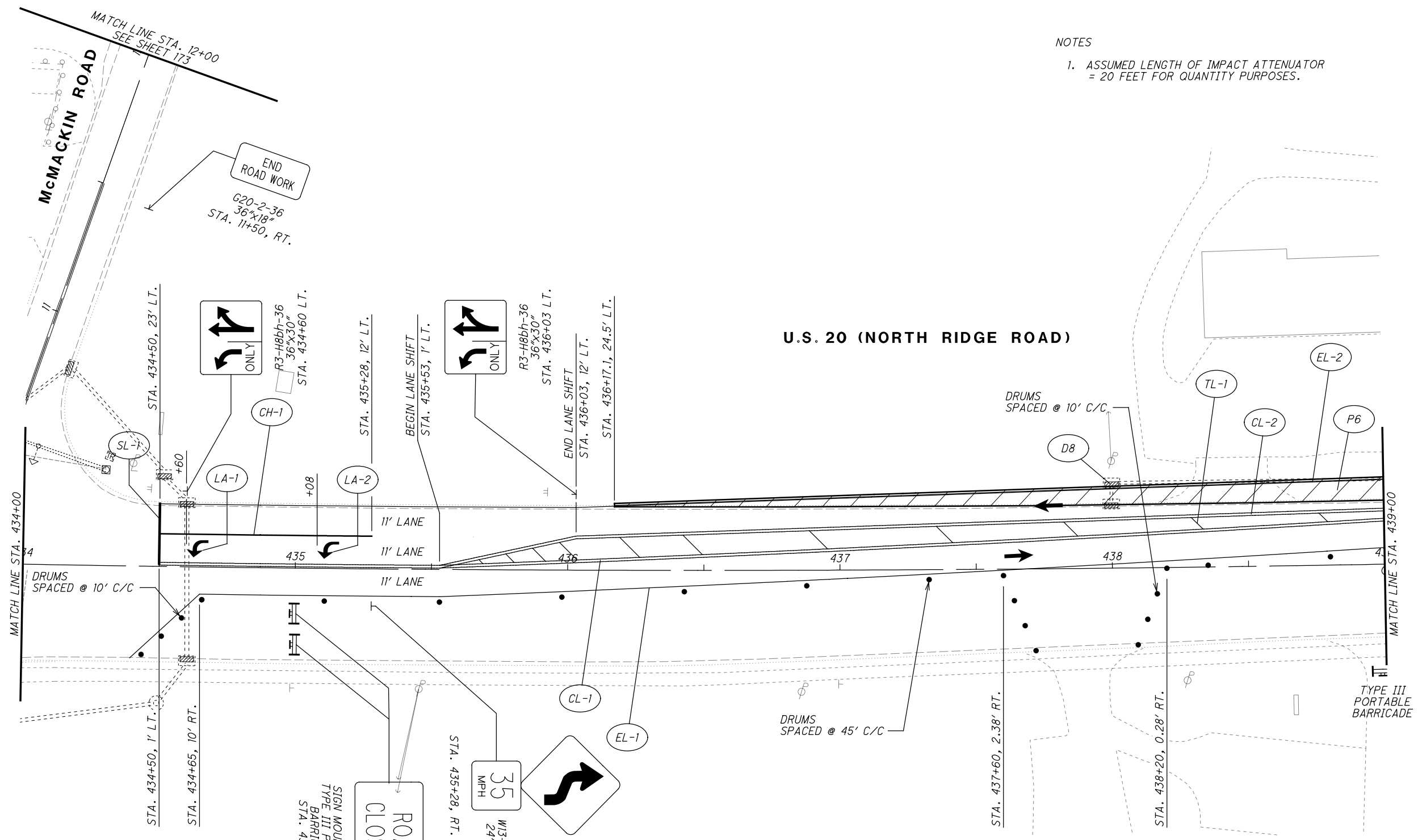


CALCULATED	EJT
CHECKED	WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 434+00 TO STA. 439+00

LAK-US-20-24.99
PART 2

NOTES
 1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

	MAINTENANCE OF TRAFFIC SIGN
	MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
	EXISTING TRAFFIC SIGN

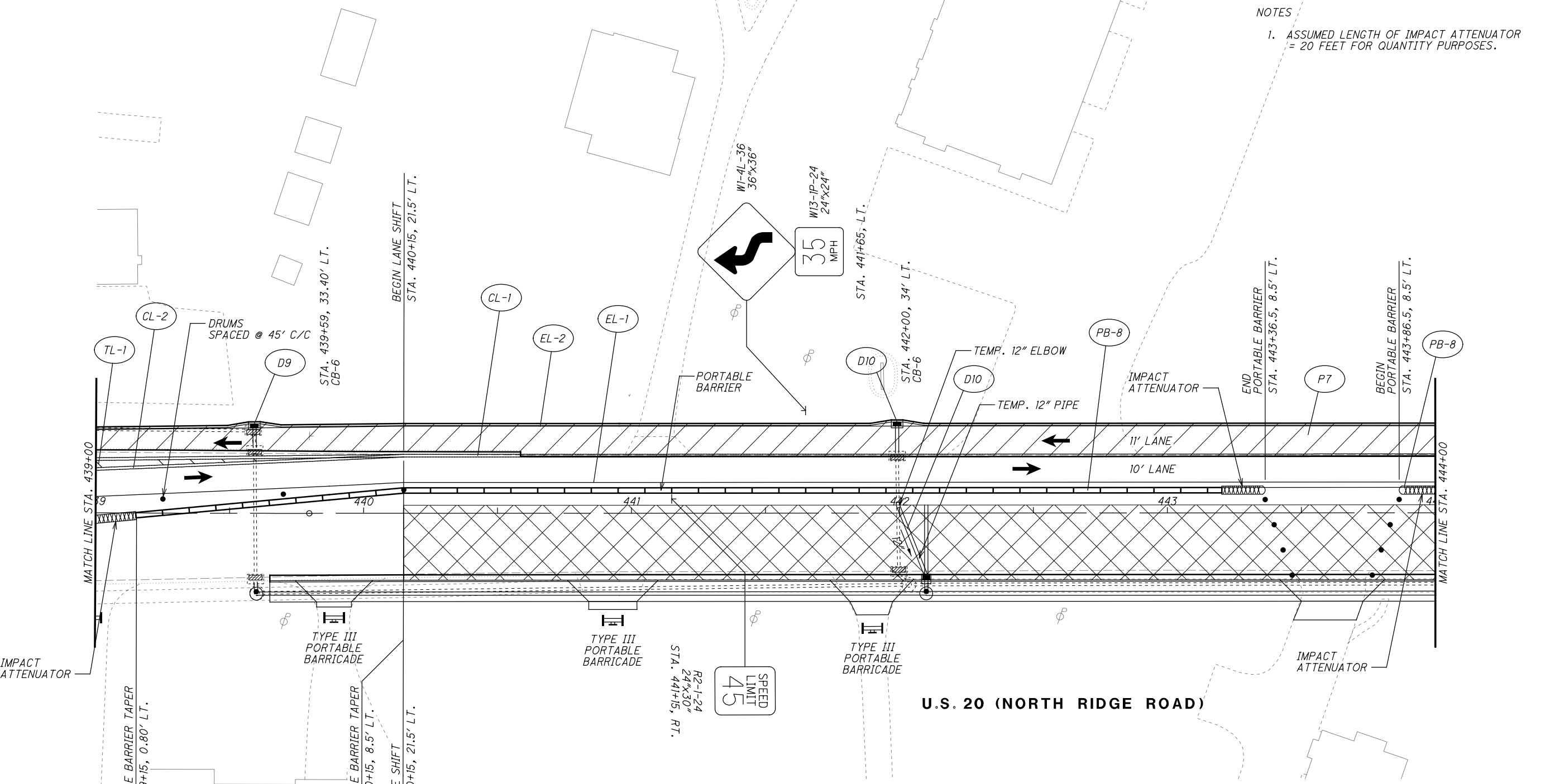
LEGEND

(CL)	ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
(LL)	ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
(EL)	ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
(CH)	ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
(LA)	ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
(SL)	ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
(CW)	ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

(DL)	ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
(TL)	ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
(IM)	ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
(○)	WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
	PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
	PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
	TRAFFIC FLOW

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP067.dgn Sheet 7/15/2022 2:51:59 PM troyer



NOTES
 1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

LEGEND

	MAINTENANCE OF TRAFFIC SIGN
	MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
	EXISTING TRAFFIC SIGN

	ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

	ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
	WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
	PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
	PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
	TRAFFIC FLOW

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 439+00 TO STA. 444+00

LAK-US-20-24.99
PART 2

152
697

CALCULATED
EJT

CHECKED
WDB

HORIZONTAL SCALE IN FEET

H:\2018\80743\0DOT\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP068.dgn_Sheet 7/15/2022 2:52:00 PM Troyer

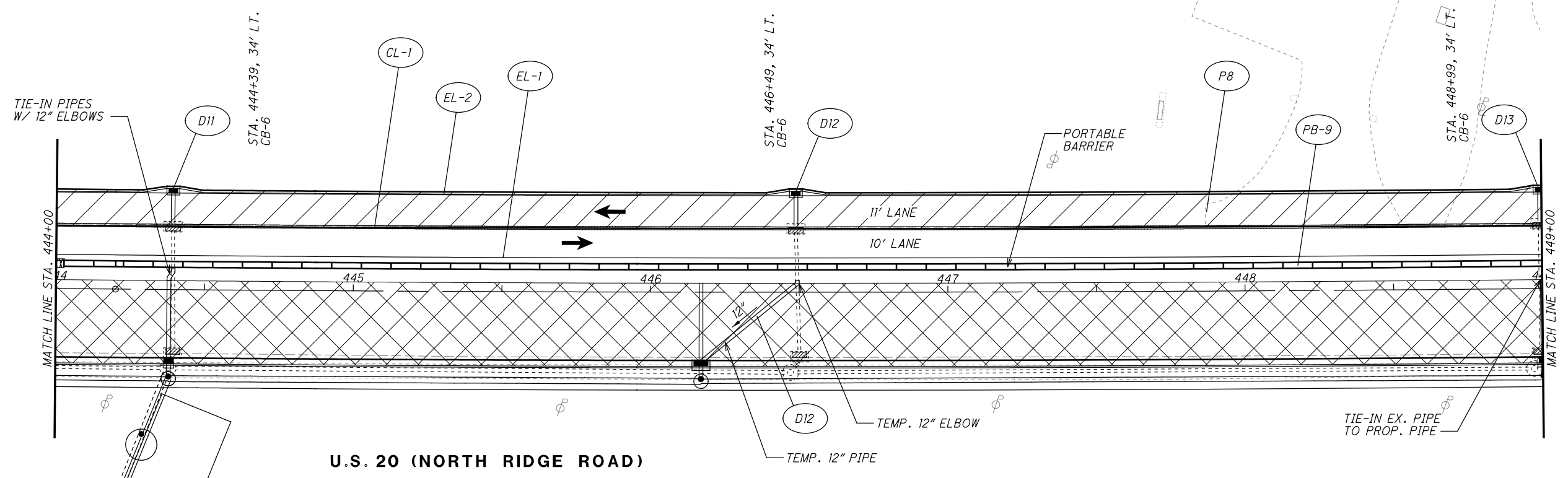
NOTES
 1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

CALCULATED EJT
 CHECKED WDB

0 20 40
 HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 444+00 TO STA. 449+00

LAK-US-20-24.99
PART 2



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP069.dgn_Sheet 7/15/2022 2:52:00 PM Troyer





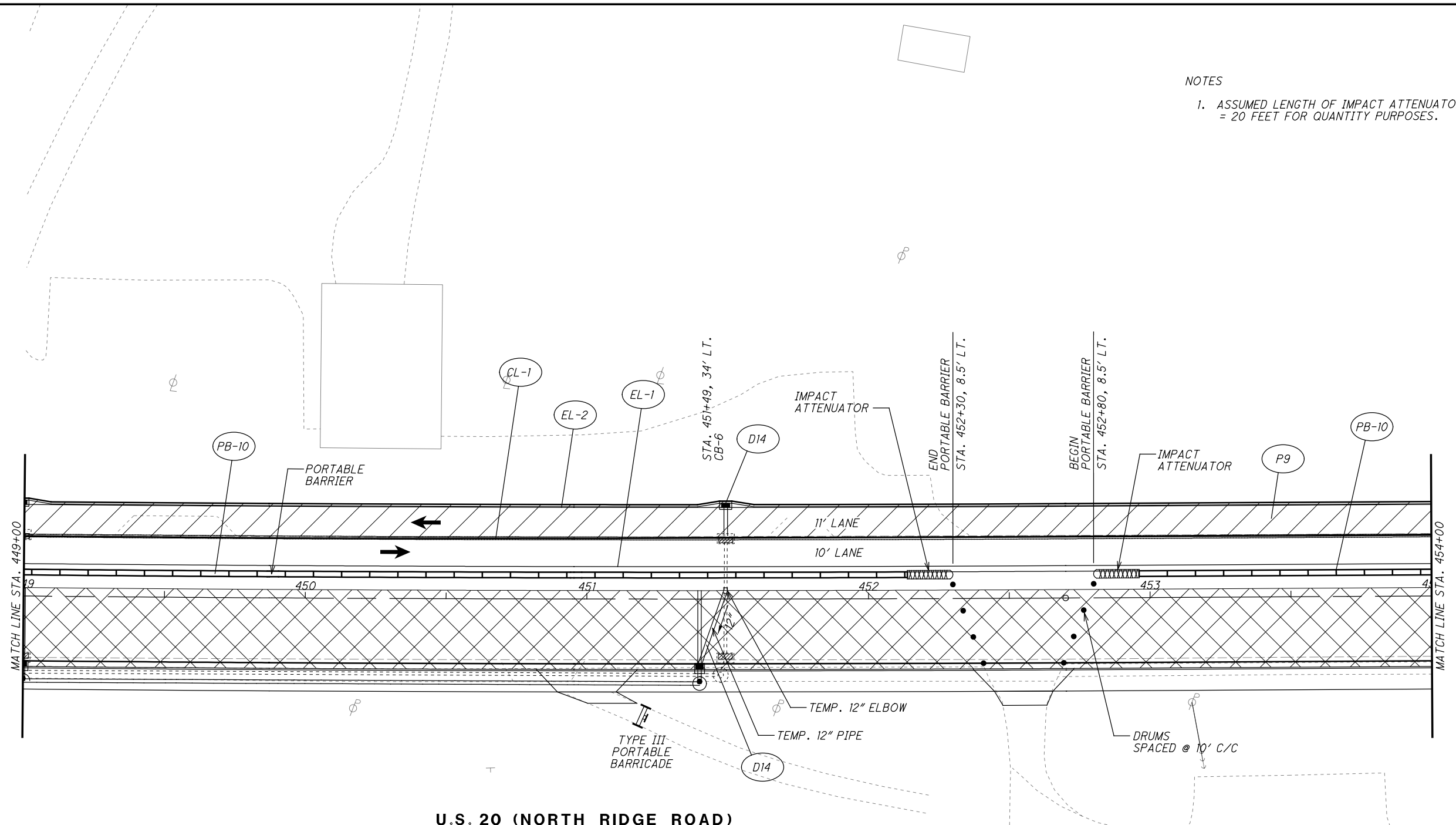
 HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 449+00 TO STA. 454+00

LAK-US-20-24.99
PART 2




NOTES








1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.








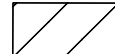

U.S. 20 (NORTH RIDGE ROAD)

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP070.dgn Sheet 7/15/2022 2:52:01PM Troyer



CALCULATED
EJT
CHECKED
WDB

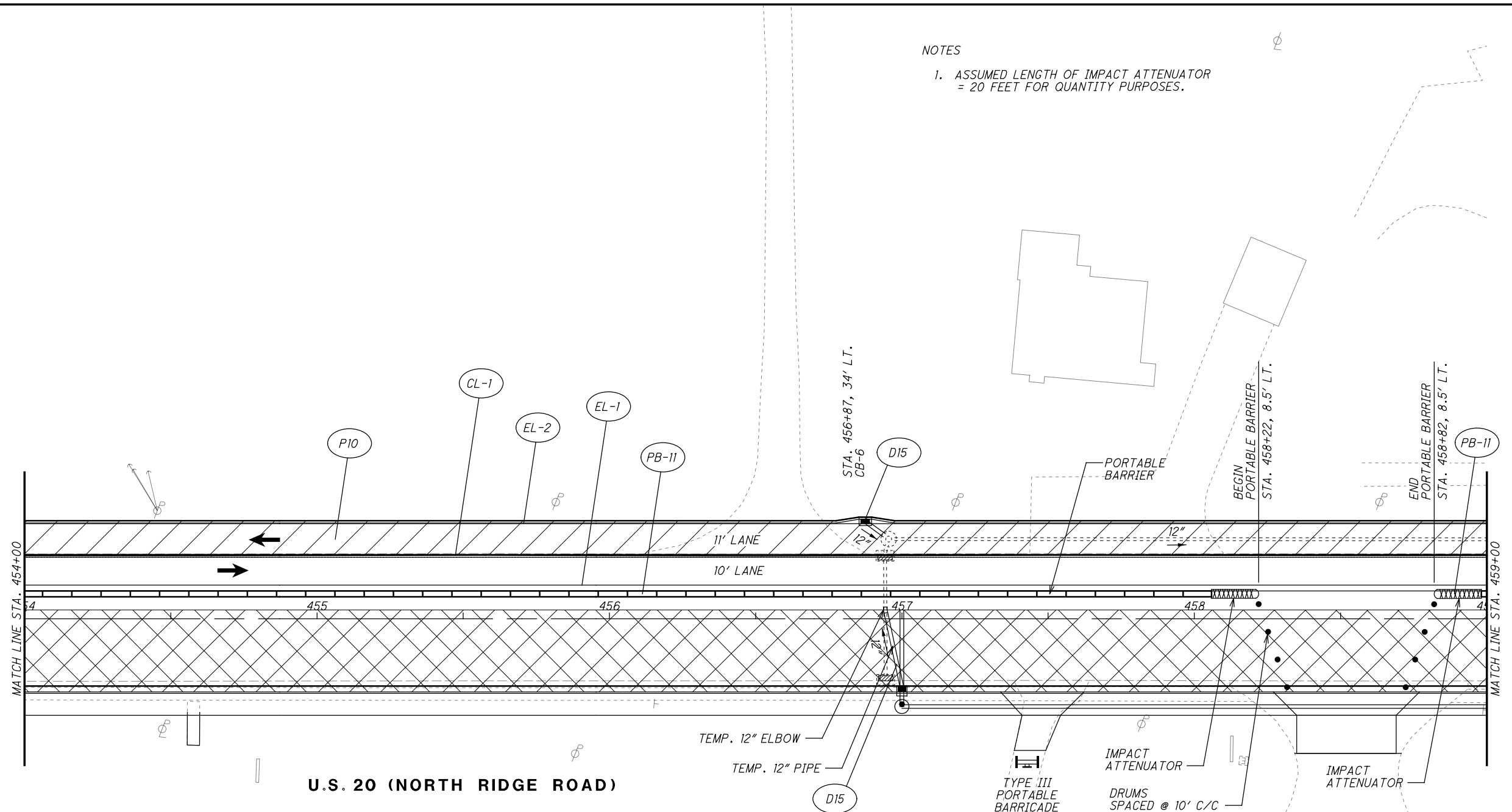
MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 454+00 TO STA. 459+00

LAK-US-20-24.99
PART 2

155
697

NOTES

1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



U.S. 20 (NORTH RIDGE ROAD)

LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

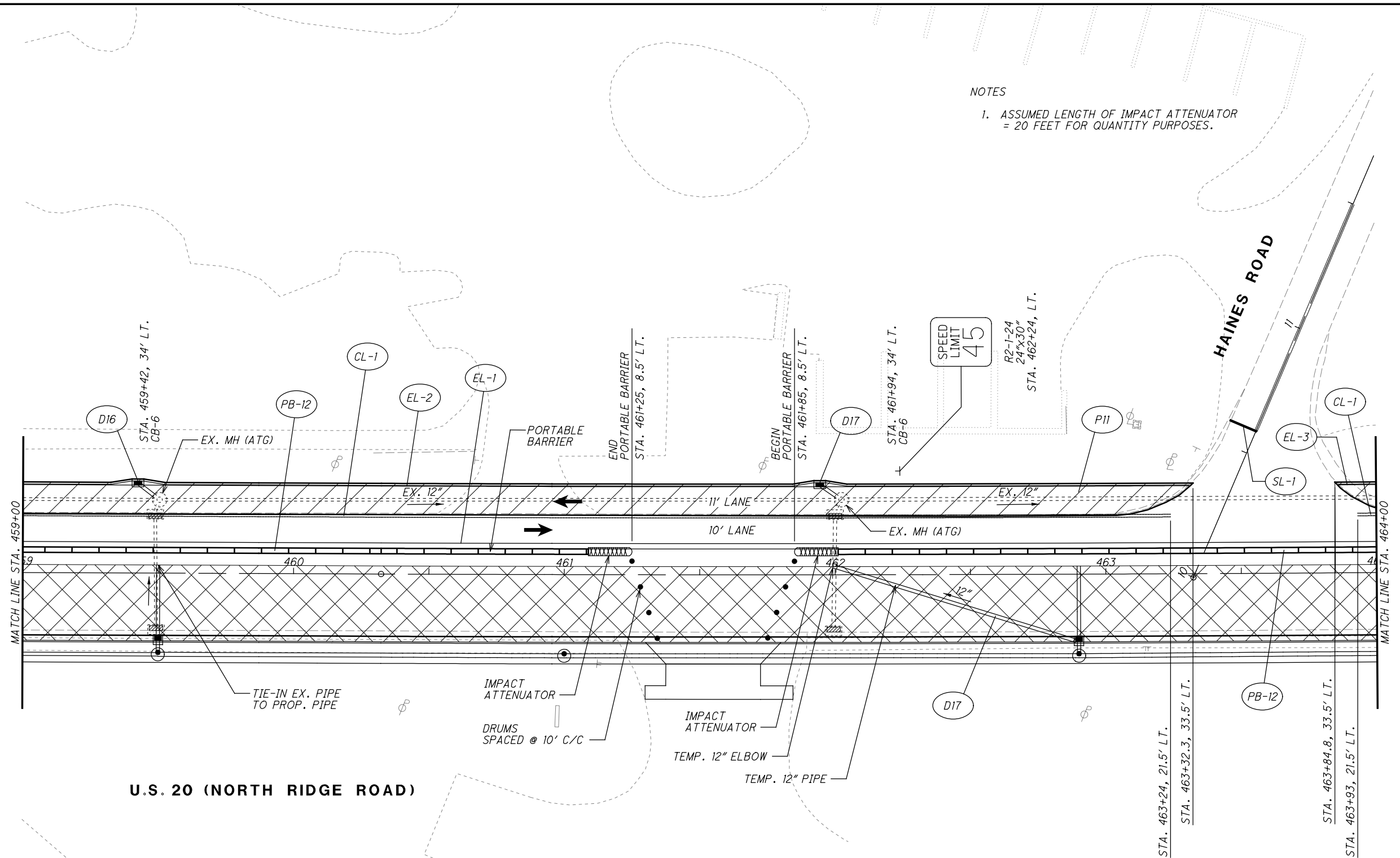
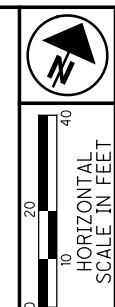
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP071.dgn Sheet 7/15/2022 2:52:02 PM troyer

NOTES

1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

CALCULATED
EJT
CHECKED
WDB



U.S. 20 (NORTH RIDGE ROAD)

LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP072.dgn_Sheet 7/15/2022 2:52:03 PM Troyer

MATCH LINE STA. 12+00
SEE SHEET 174

HAINES ROAD

NOTES
1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

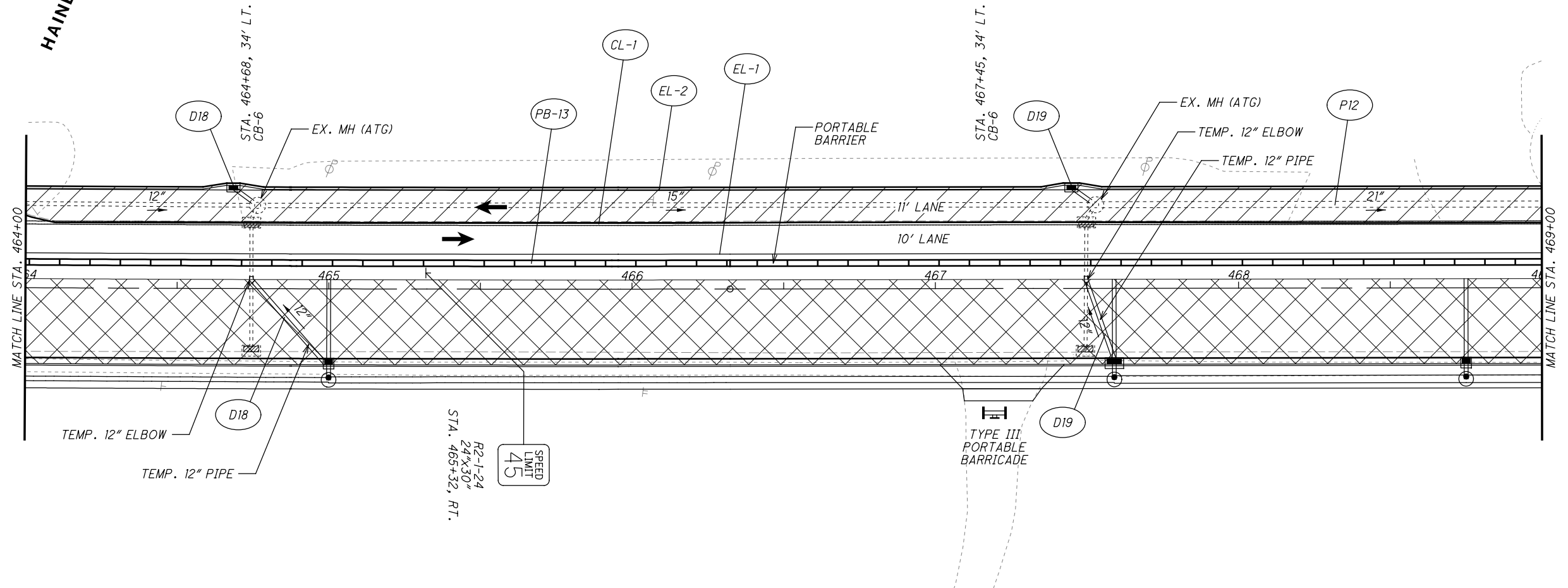
CALCULATED
EJT
CHECKED
WDB

0 10 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 464+00 TO STA. 469+00

LAK-US-20-24.99
PART 2

157
697



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN
INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- CL ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- LL ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- EL ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- CH ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- LA ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- SL ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- CW ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- DL ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- TL ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- IM ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT
CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT
CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

SPEED
LIMIT
45

R2-1-24
24"x30"
STA. 465+32, RT.

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP073.dgn Sheet 7/15/2022 2:52:09 PM Troyer



 0 10 20 40

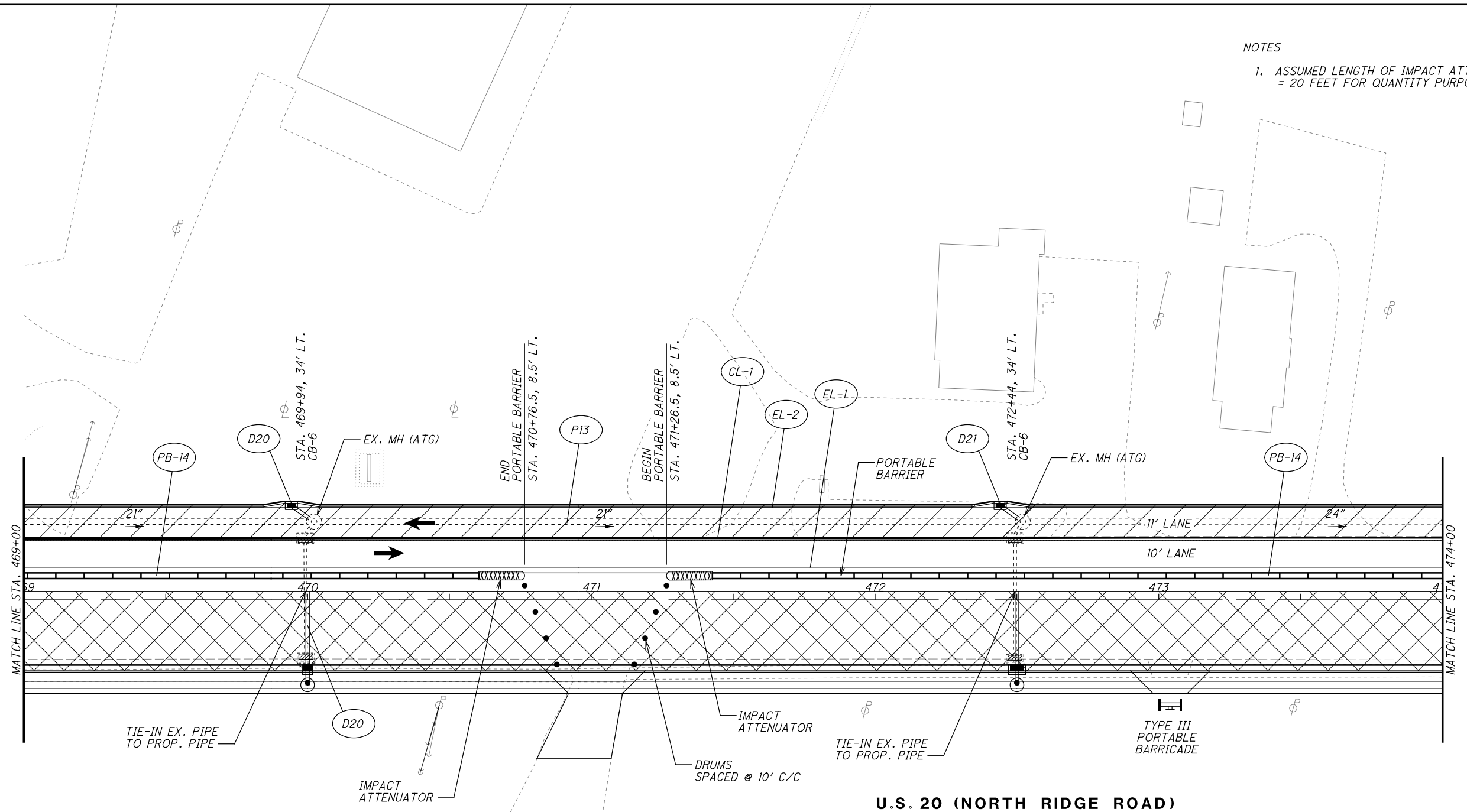
 HORIZONTAL

 SCALE IN FEET

 CALCULATED EJT




 CHECKED WDB








NOTES
 1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.







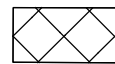
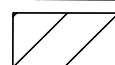
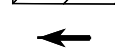
U.S. 20 (NORTH RIDGE ROAD)

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 469+00 TO STA. 474+00

LAK-US-20-24.99
PART 2

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP074.dgn Sheet 7/15/2022 2:52:10 PM troyer

NOTES
 1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

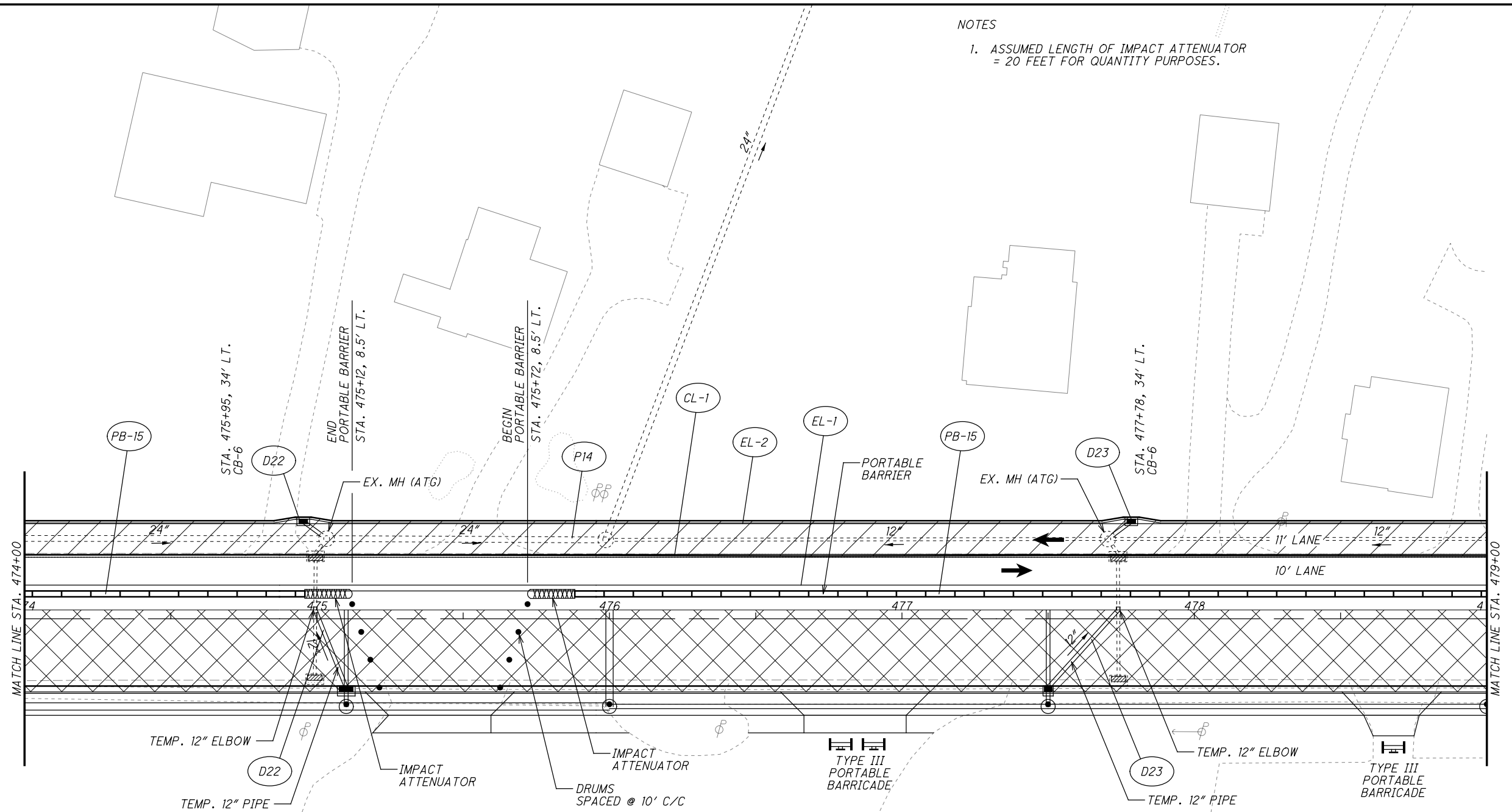
CALCULATED
 EJT
 CHECKED
 WDB

0 10 20 40
 HORIZONTAL
 SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 474+00 TO STA. 479+00

LAK-US-20-24.99
PART 2

159
 697



U.S. 20 (NORTH RIDGE ROAD)

LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

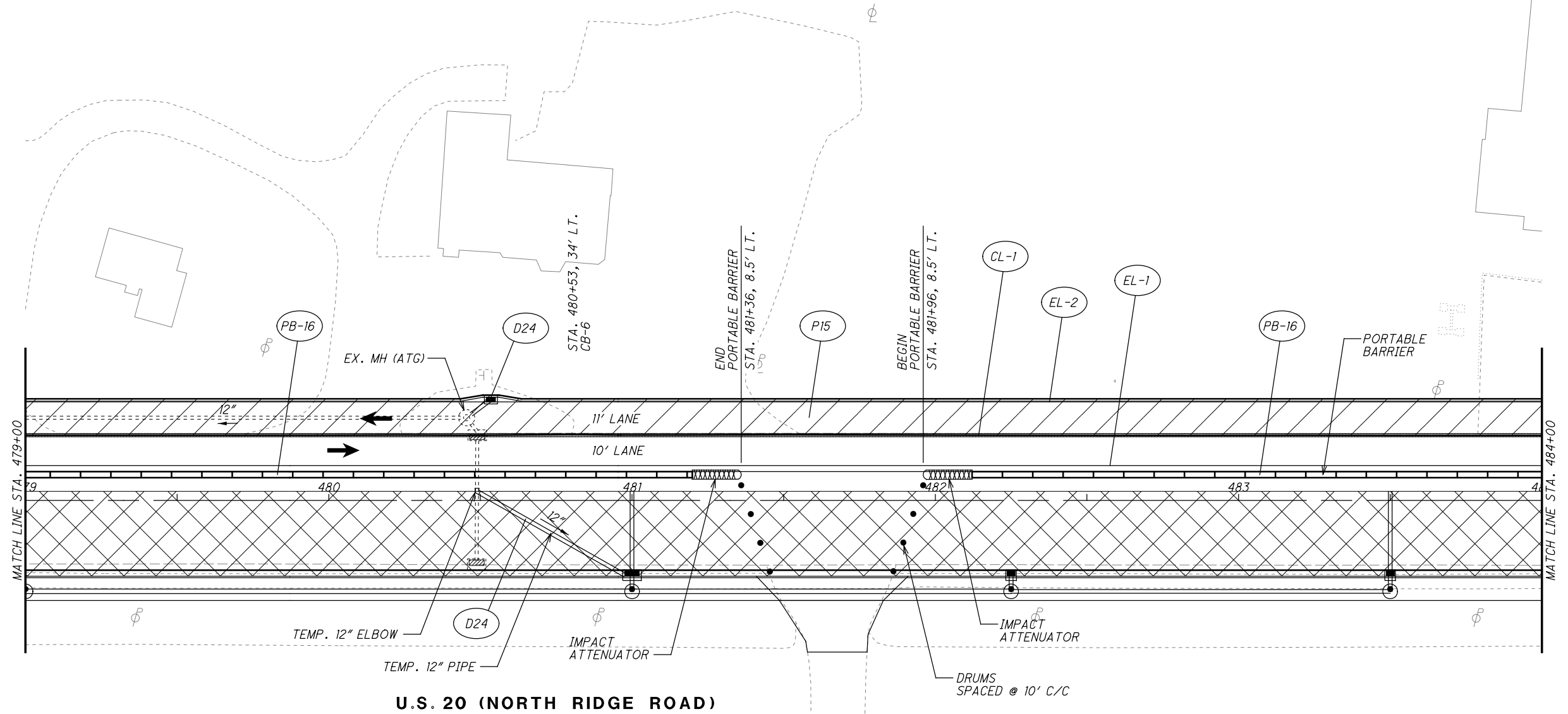
LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP075.dgn Sheet 7/15/2022 2:52:11 PM troyer

NOTES

- 1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



U.S. 20 (NORTH RIDGE ROAD)

LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

CALCULATED EJT
CHECKED WDB

0 10 20 40
HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 479+00 TO STA. 484+00

LAK-US-20-24.99
PART 2

160
697

H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP076.dgn Sheet 7/15/2022 2:52:42 PM troyer

CALCULATED
 EJT
 CHECKED
 WDB

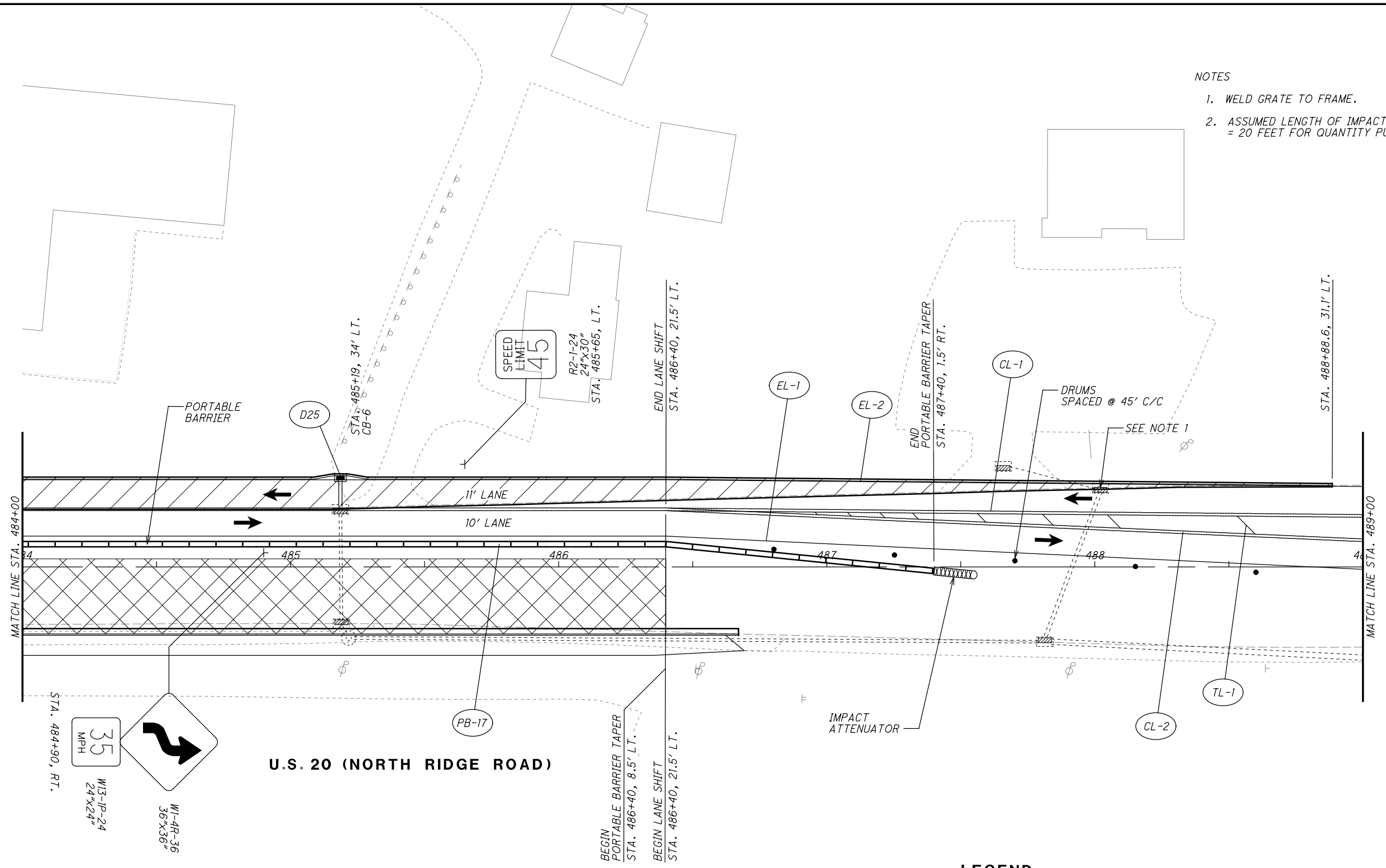
0 10 20 40
 HORIZONTAL
 SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 484+00 TO STA. 489+00

LAK-US-20-24.99
PART 2

161
697

- NOTES**
1. WELD GRATE TO FRAME.
 2. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

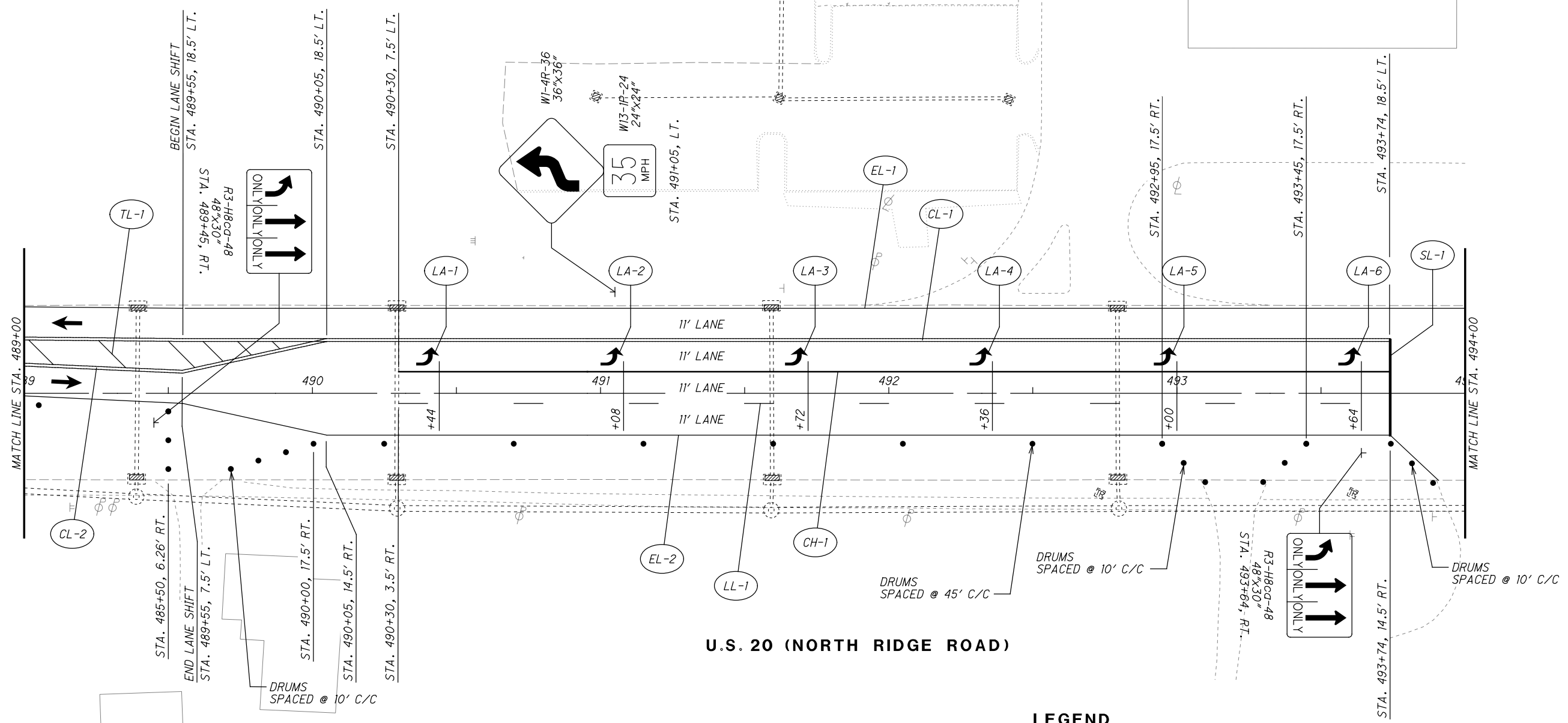
U.S. 20 (NORTH RIDGE ROAD)

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP071.dgn Sheet 7/15/2022 2:52:13 PM troyer

	MAINTENANCE OF TRAFFIC SIGN
	MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
	EXISTING TRAFFIC SIGN

LEGEND

	ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT		ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT		ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT		ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
	ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT		WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
	ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT		PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
	ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT		PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
	ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT		TRAFFIC FLOW



U.S. 20 (NORTH RIDGE ROAD)

CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

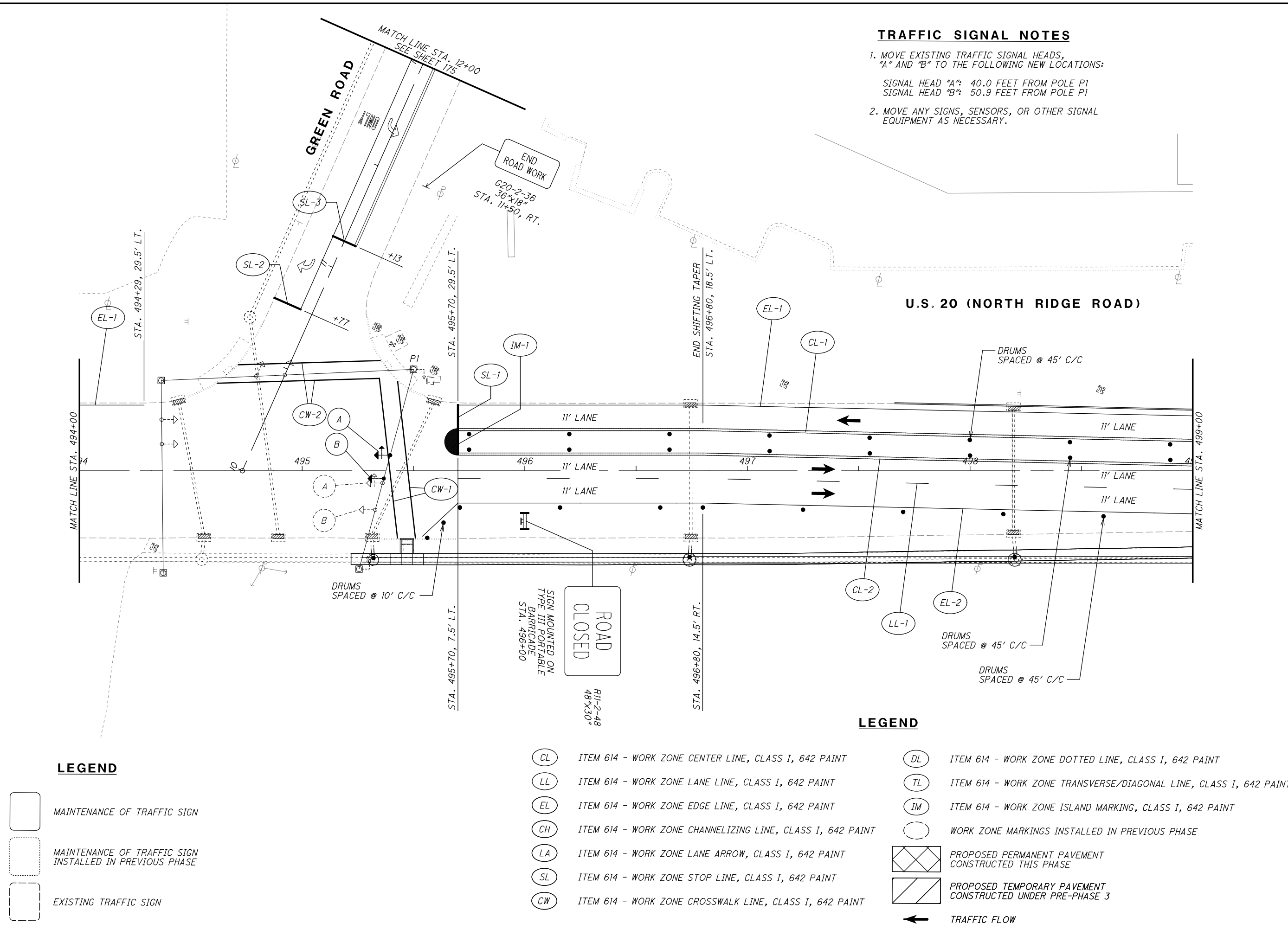
MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 489+00 TO STA. 494+00

LAK-US-20-24.99
PART 2

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP078.dgn Sheet 7/15/2022 2:52:13 PM troyer

TRAFFIC SIGNAL NOTES

- MOVE EXISTING TRAFFIC SIGNAL HEADS, "A" AND "B" TO THE FOLLOWING NEW LOCATIONS:
 SIGNAL HEAD "A": 40.0 FEET FROM POLE P1
 SIGNAL HEAD "B": 50.9 FEET FROM POLE P1
- MOVE ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- CL ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- LL ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- EL ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- CH ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- LA ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- SL ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- CW ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- DL ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- TL ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- IM ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP079.dgn Sheet 7/15/2022 2:52:14 PM troyer

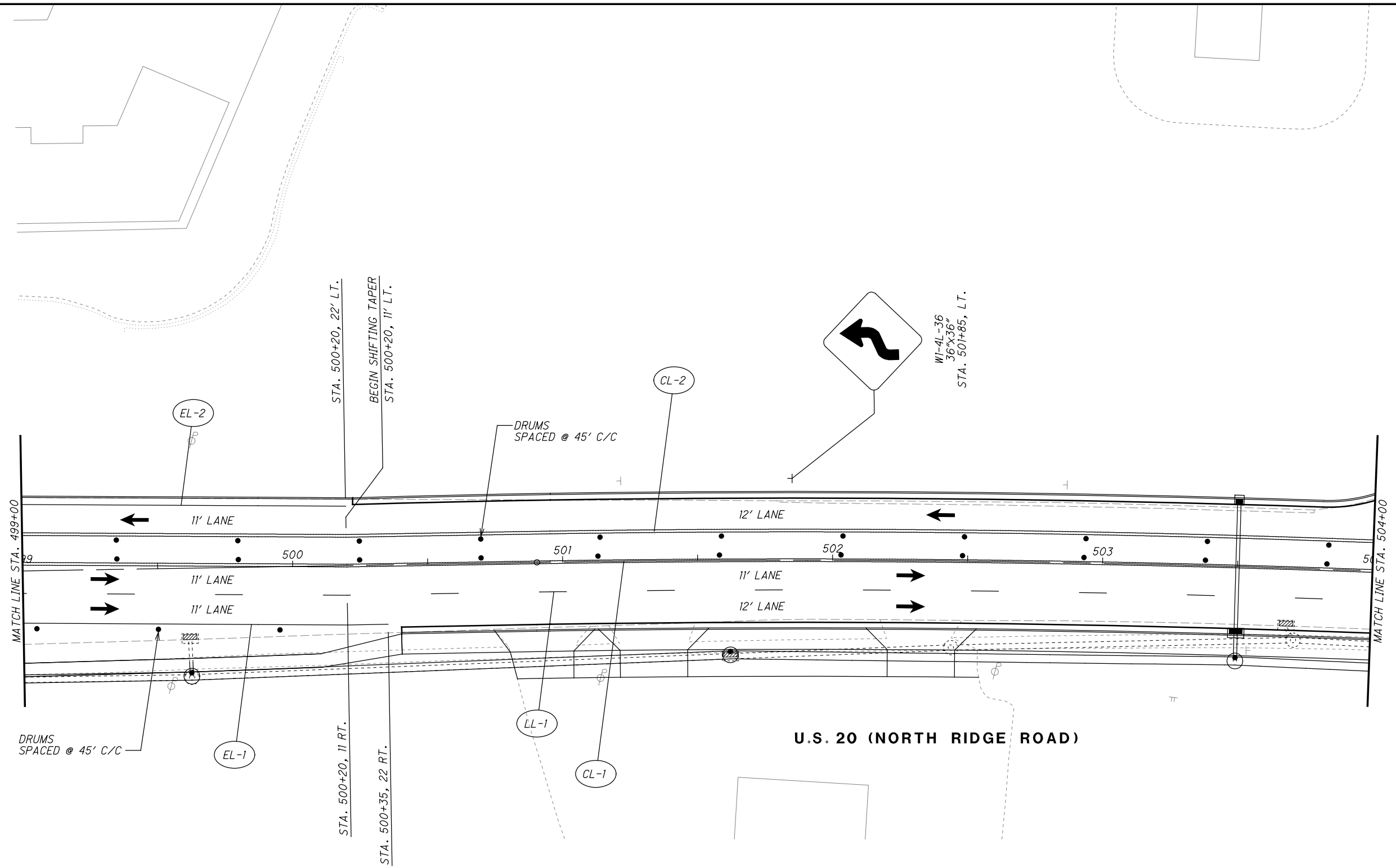







 HORIZONTAL SCALE IN FEET








MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 499+00 TO STA. 504+00






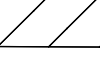

LAK-US-20-24.99
PART 2



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\01\Sheets\09270_MP080.dgn Sheet 7/15/2022 2:52:15 PM troyer

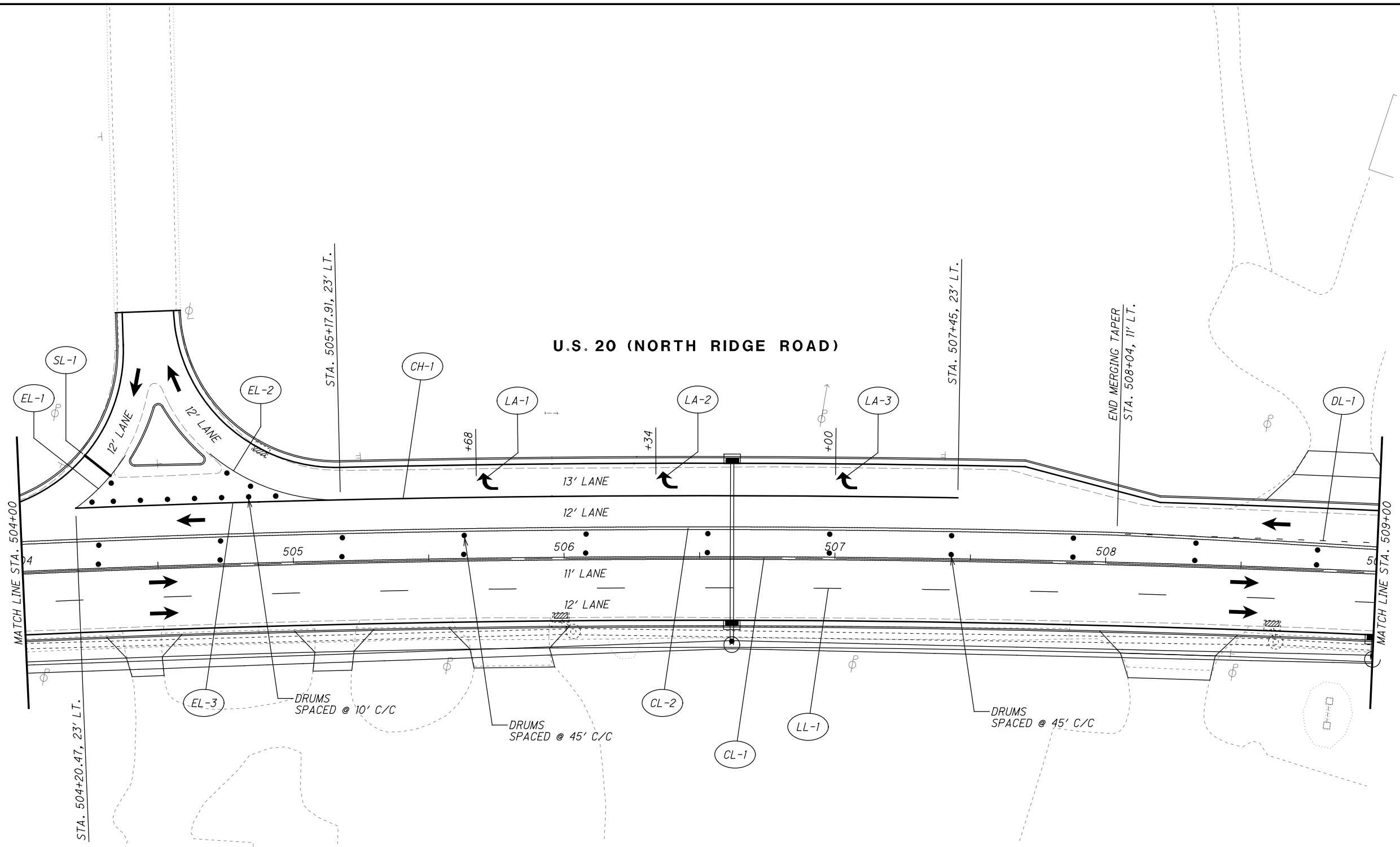
CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 504+00 TO STA. 509+00

LAK-US-20-24.99
PART 2

165
697



LEGEND

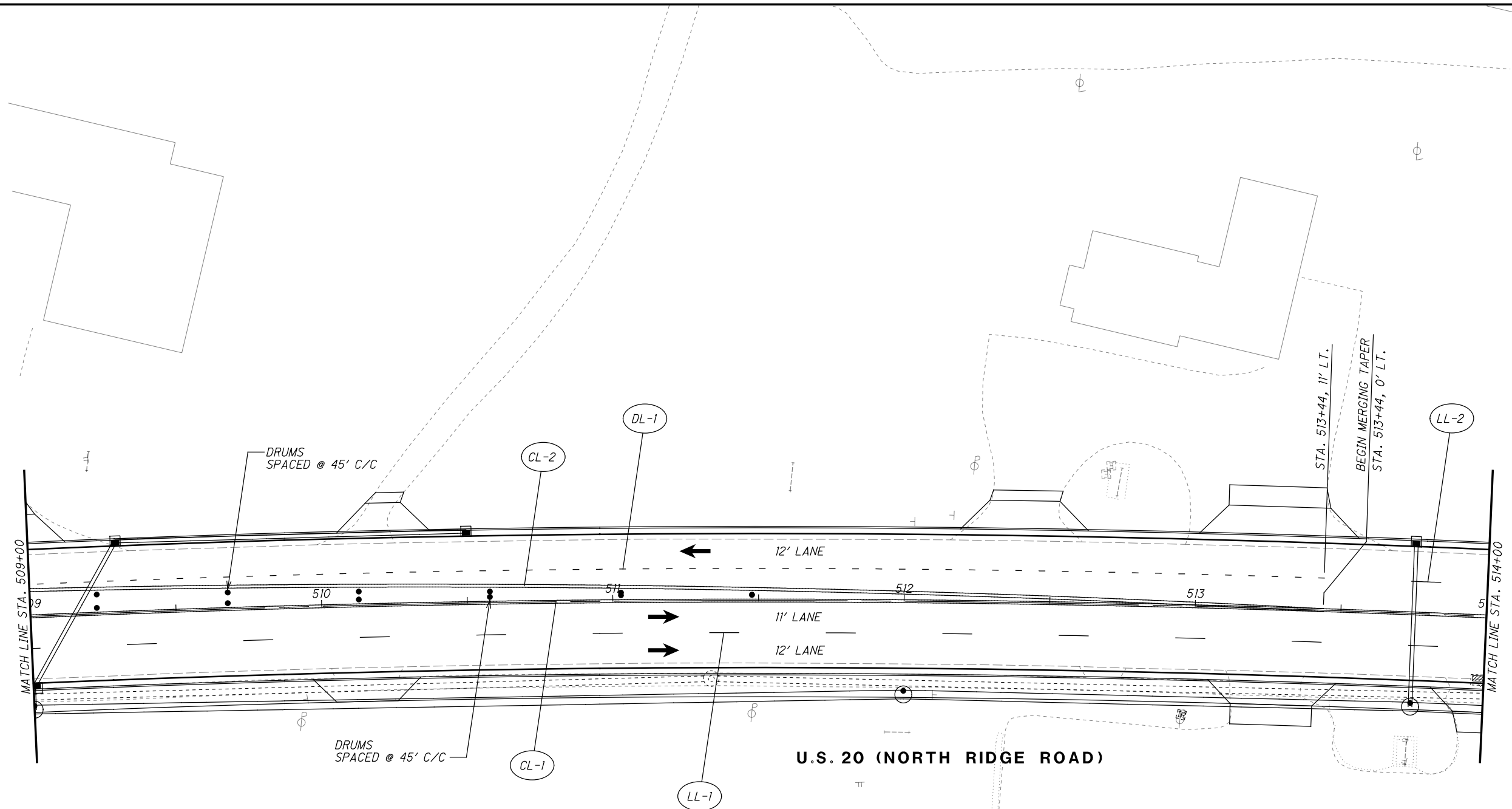
- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT




LEGEND












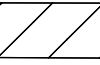


- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\00T\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP08.dgn Sheet 7/15/2022 2:52:16 PM Troyer

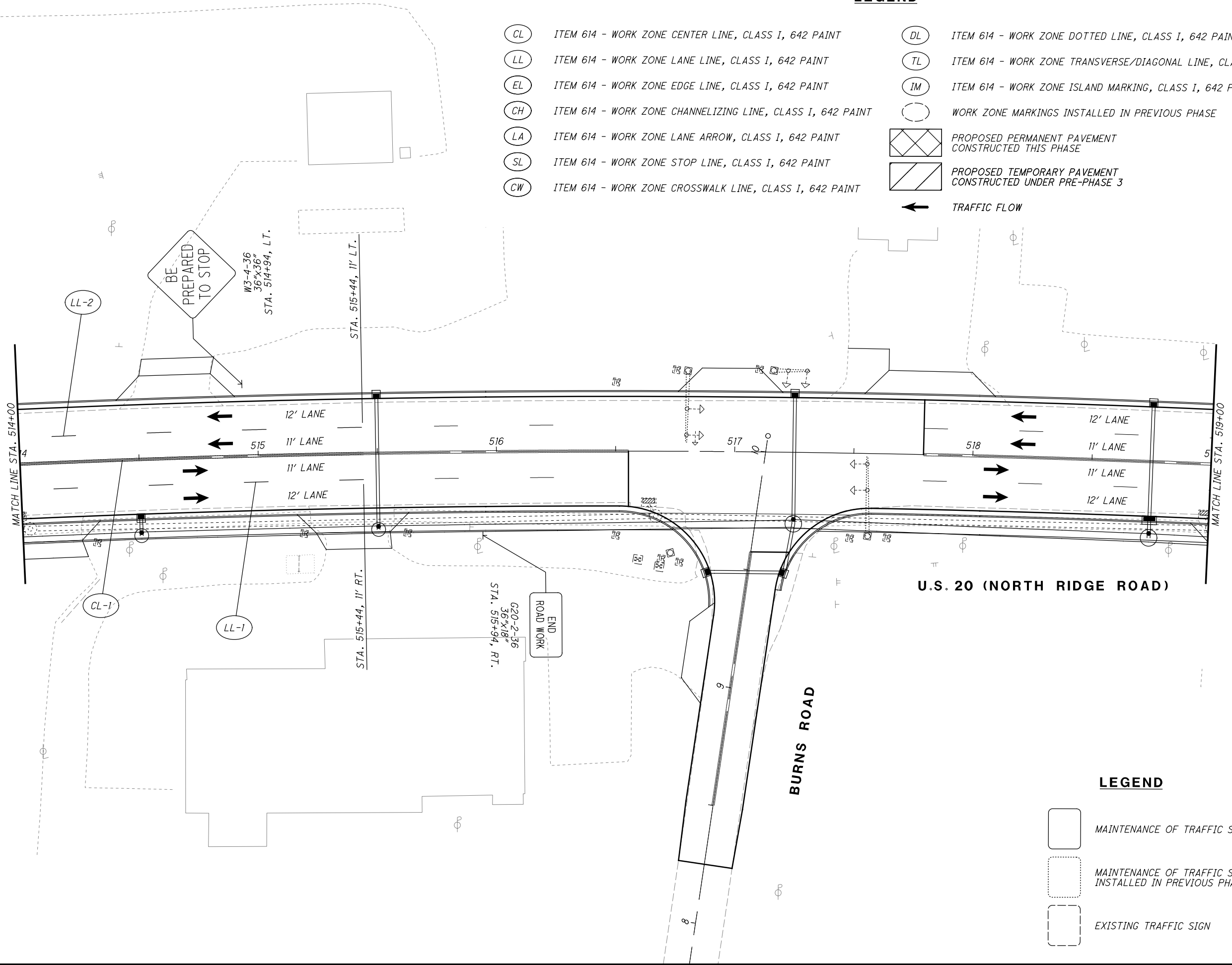


LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP082.dgn Sheet 7/15/2022 2:52:16 PM troyer



LEGEND

- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- () WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- (X) PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- (/) PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- (←) TRAFFIC FLOW

LEGEND

- () MAINTENANCE OF TRAFFIC SIGN
- () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- () EXISTING TRAFFIC SIGN

CALCULATED
EJT
CHECKED
WDB

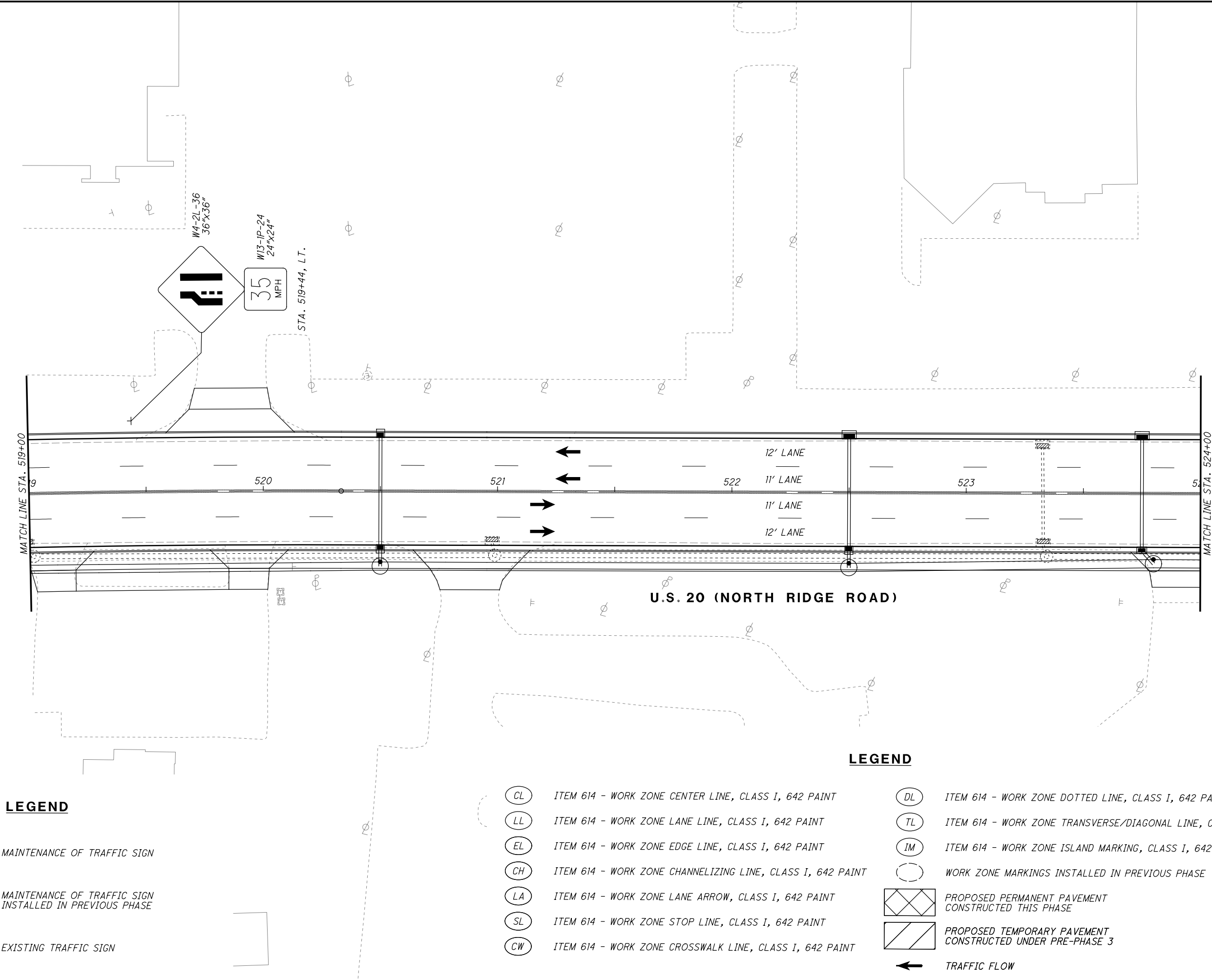
MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 514+00 TO STA. 519+00

LAK-US-20-24.99
PART 2




167
697










H:\2018\80743\0DOT\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP082A.dgn Sheet 7/15/2022 2:52:17 PM Troyer





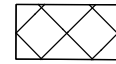
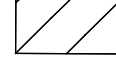



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW

CALCULATED
EJT
CHECKED
WDB

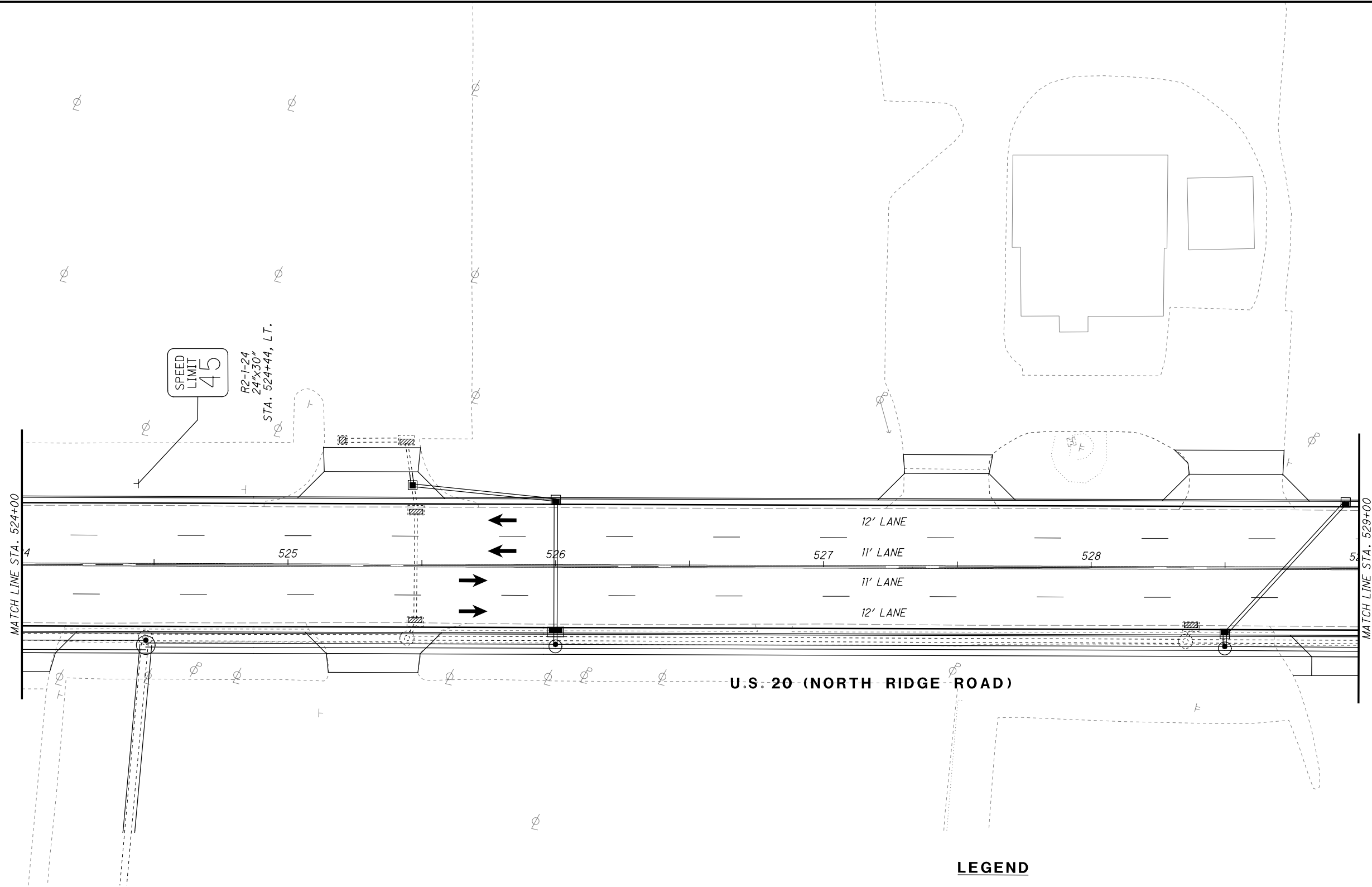


0 20 40
HORIZONTAL
SCALE IN FEET




MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 519+00 TO STA. 524+00








LAK-US-20-24.99
PART 2

H:\2018\80743\0DOT\ProjectData\09270_LAK_20_EAST\Design\DOT\Sheets\09270_MP082B.dgn Sheet 7/15/2022 2:52:18 PM Troyer





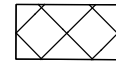
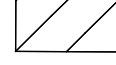



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW

CALCULATED
EJT
CHECKED
WDB



0 10 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 524+00 TO STA. 529+00

LAK-US-20-24.99
PART 2

169
697

H:\2018\180743\DOT\ProjectData\09270_LAK_20_EAST\Design\DOT\Sheets\09270_MP082C.dgn Sheet 7/15/2022 2:52:18 PM Troyer

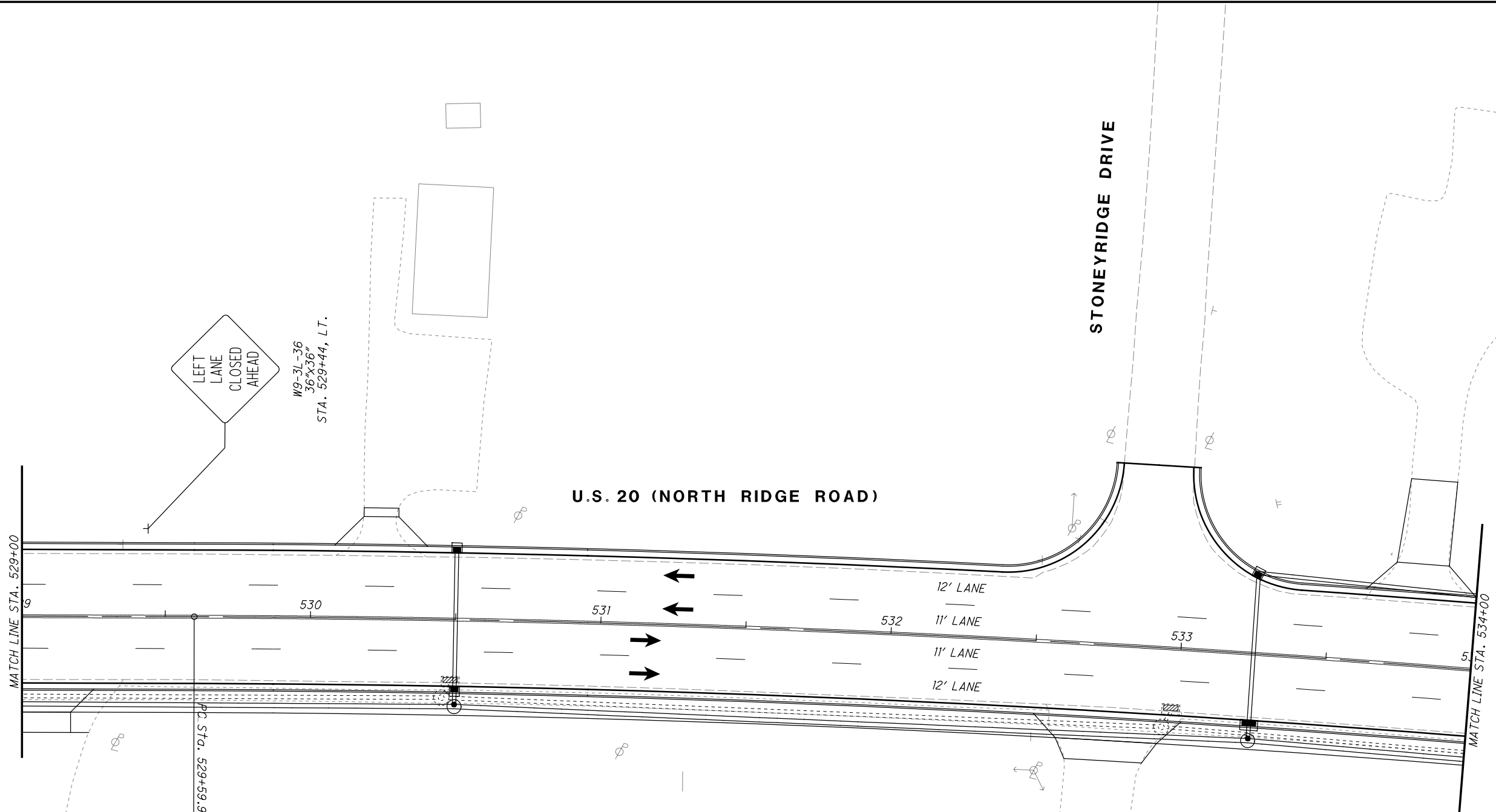
CALCULATED
EJT
CHECKED
WDB

0 10 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 529+00 TO STA. 534+00

LAK-US-20-24.99
PART 2

170
697



LEGEND

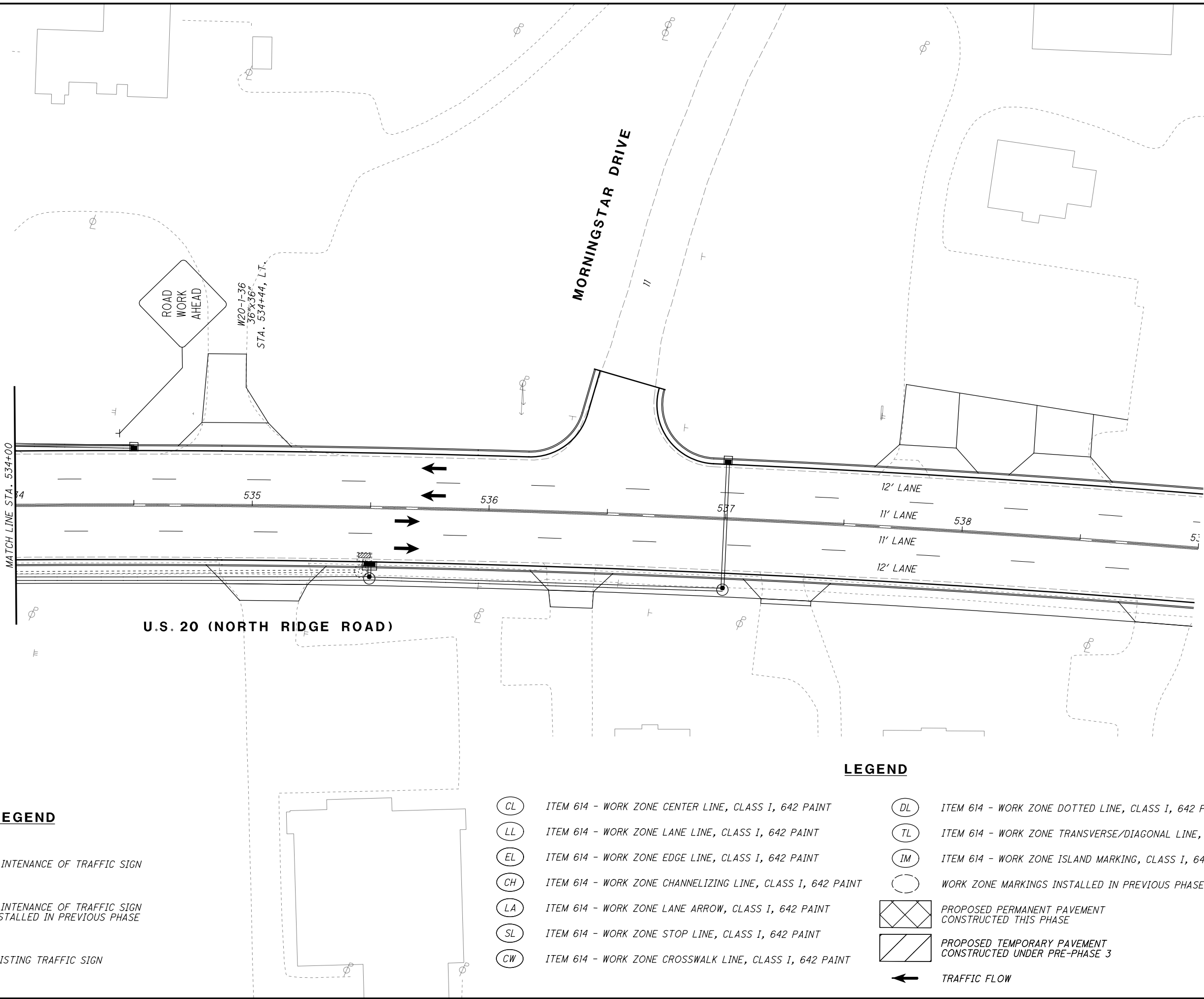
- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP083.dgn_Sheet 7/15/2022 2:52:19 PM troyer






U.S. 20 (NORTH RIDGE ROAD)








MORNINGSTAR DRIVE

ROAD WORK AHEAD
 W20-1-36
 36°x36°
 STA. 534+44, LT.








MATCH LINE STA. 534+00

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW

CALCULATED EJT
 CHECKED WDB




HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
STA. 534+00 TO STA. 539+00

LAK-US-20-24.99
PART 2

171
 697

H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP083A.dgn Sheet 7/15/2022 2:52:20 PM troyer





 HORIZONTAL SCALE IN FEET

CALCULATED	EJT
CHECKED	WDB




MAINTENANCE OF TRAFFIC PLAN - PHASE 3
TOWNLINE ROAD



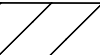

LAK-US-20-24.99
PART 2

172
697



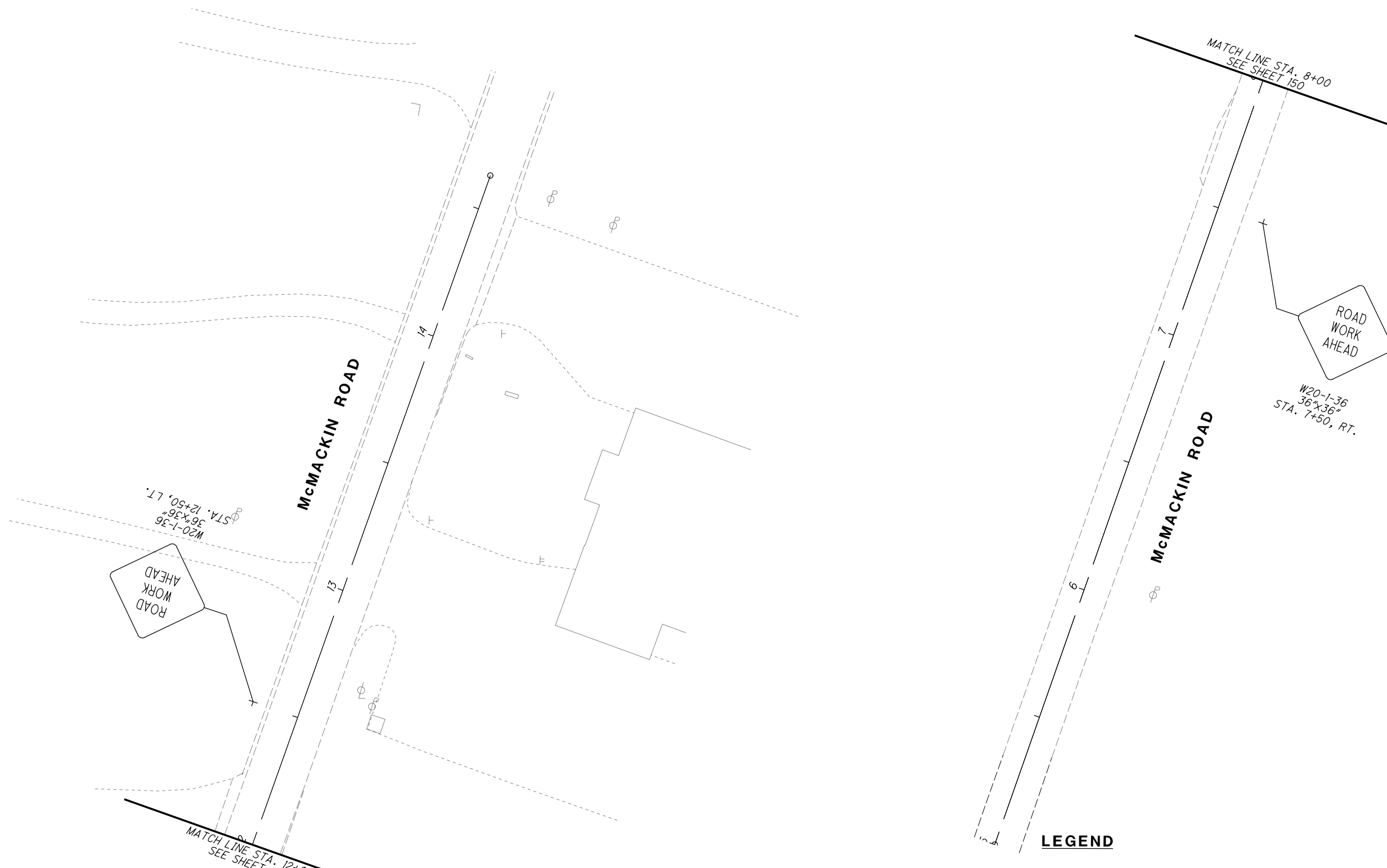
LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN




- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW















LEGEND

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP083B.dgn Sheet 7/15/2022 2:52:21PM troyer



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

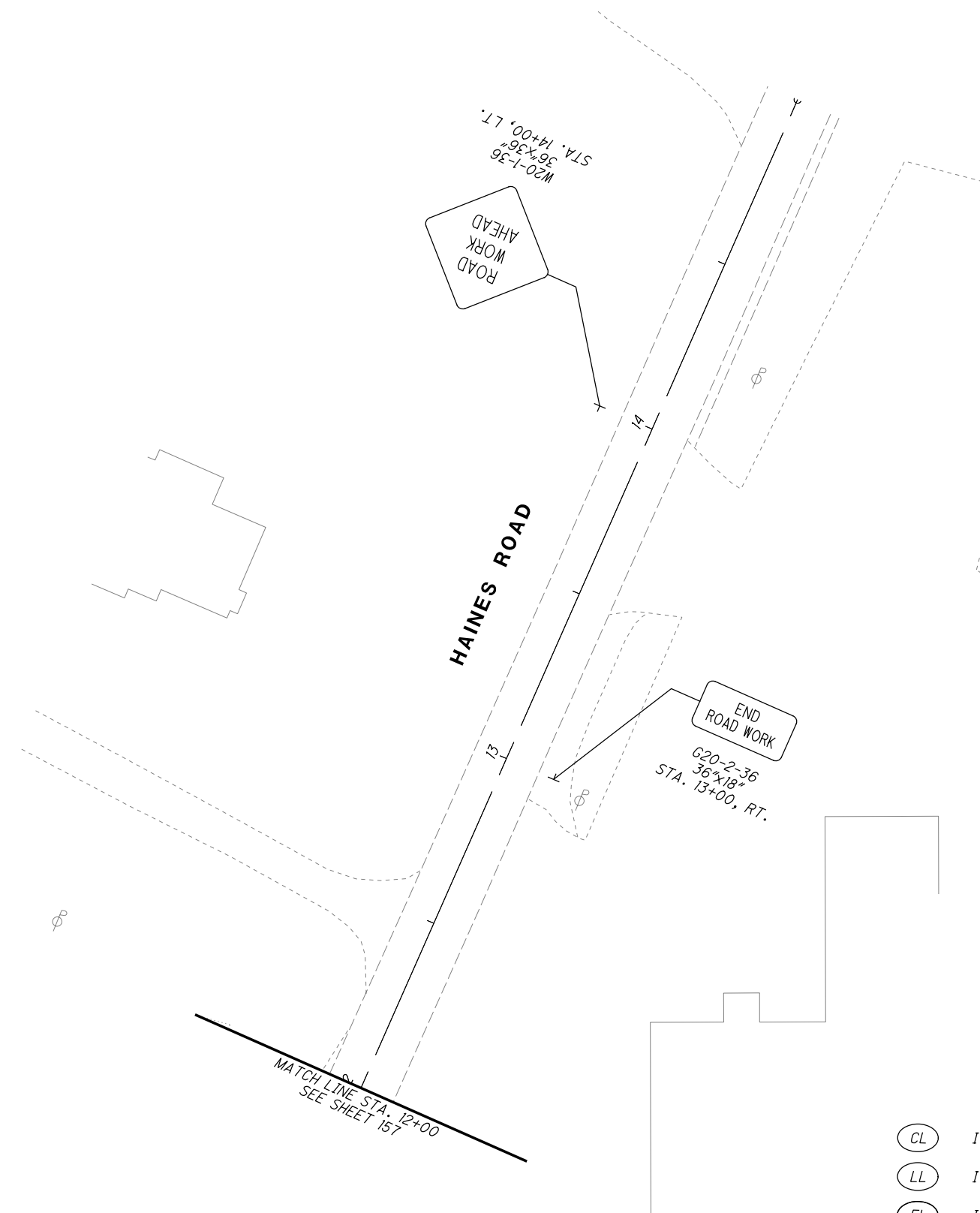
- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

CALCULATED
EJT
CHECKED
WDB




0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 3
McMACKIN ROAD

LAK-US-20-24.99
PART 2



LEGEND












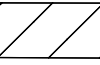


-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

CALCULATED
EJT
CHECKED
WDB

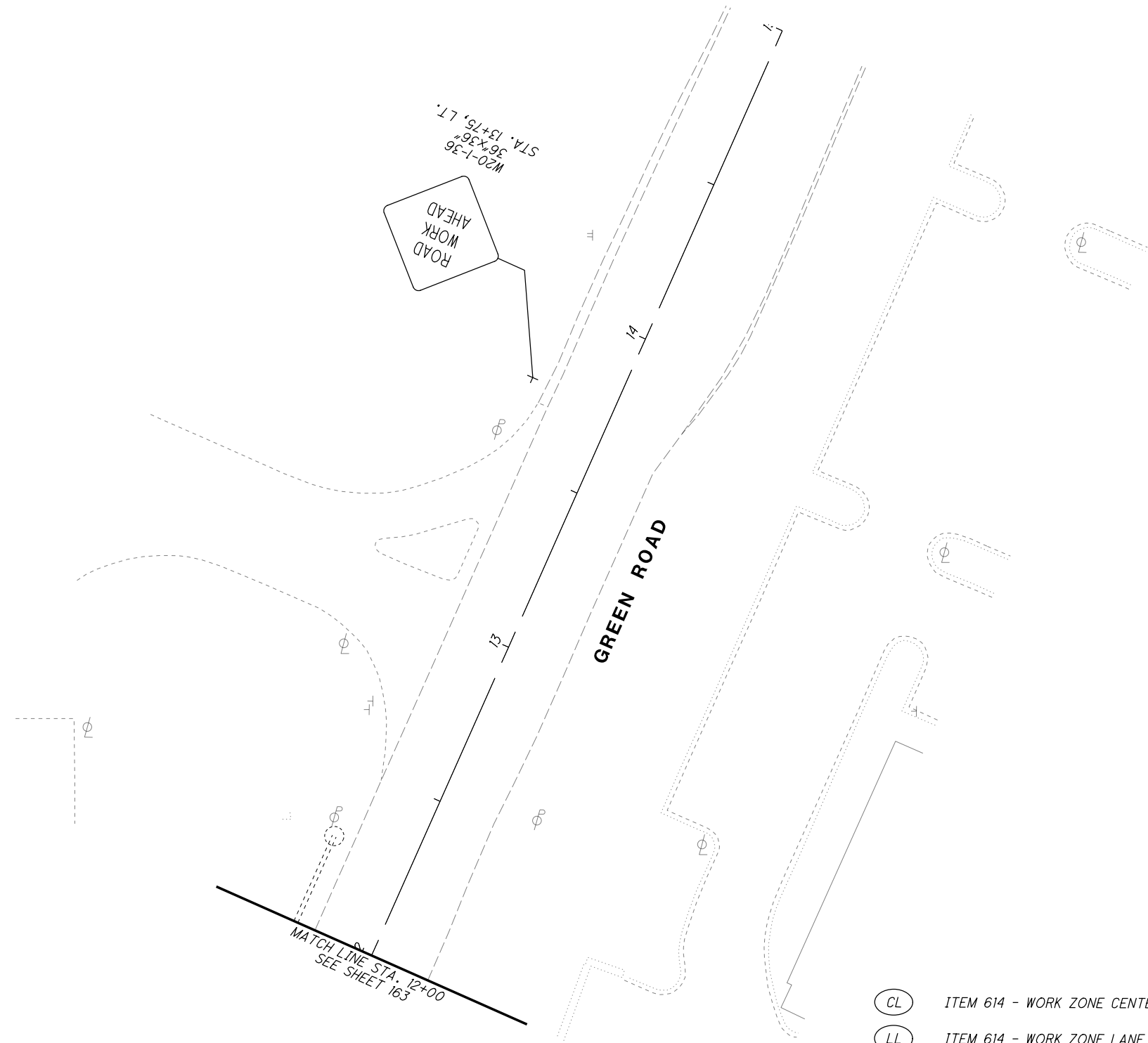
0 20 40
HORIZONTAL
SCALE IN FEET

**MAINTENANCE OF TRAFFIC PLAN - PHASE 3
HAINES ROAD**

LEGEND

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |




**LAK-US-20-24.99
PART 2**



ROAD WORK AHEAD
W20-1-36
36"x36"
STA. 13+75, LT.

MATCH LINE STA. 12+00
SEE SHEET 163

LEGEND












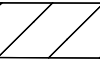


-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

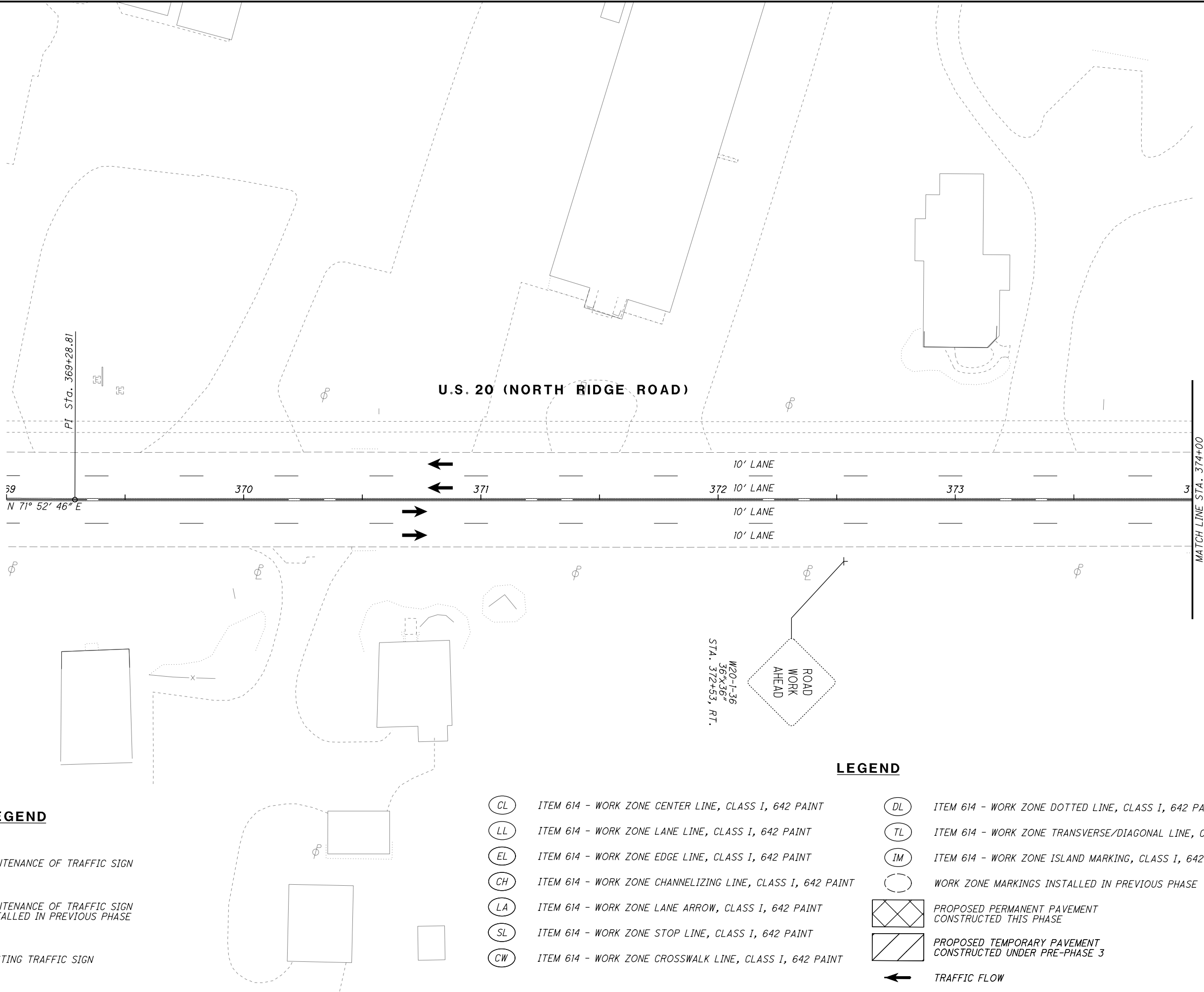
**MAINTENANCE OF TRAFFIC PLAN - PHASE 3
GREEN ROAD**

LEGEND




- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |








**LAK-US-20-24.99
PART 2**

H:\2018\180743\ODOT\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP265F.dgn Sheet 7/15/2022 2:52:23 PM troyer










LEGEND

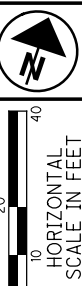
-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW

CALCULATED
EJT
CHECKED
WDB



0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 369+00 TO STA. 374+00

LAK-US-20-24.99
PART 2

176
697

H:\2018\180743\ODOT\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP265G.dgn Sheet 7/15/2022 2:52:24 PM troyer

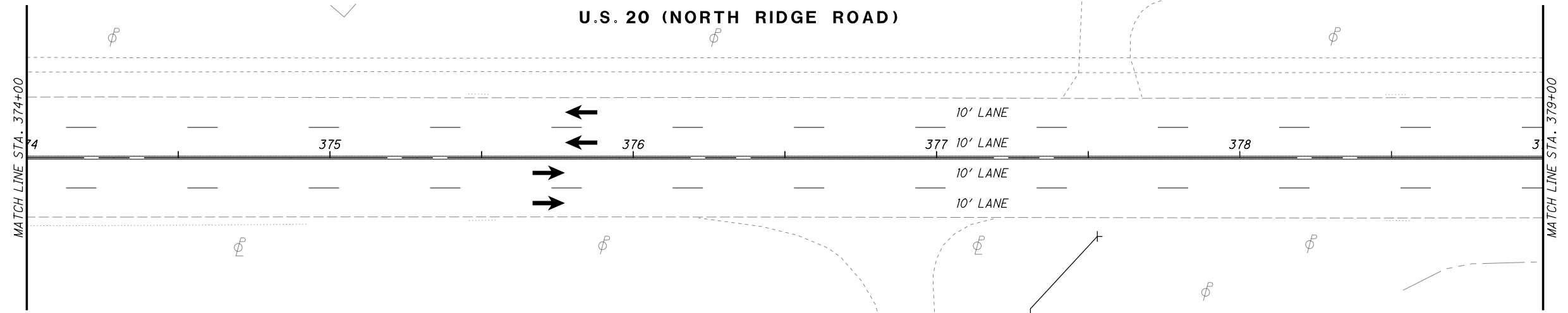
CALCULATED
EJT
CHECKED
WDB

HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 374+00 TO STA. 379+00

LAK-US-20-24.99
PART 2

177
697



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\M0T\Sheets\09270_MP266.dgn_Sheet 7/15/2022 2:52:24 PM Troyer

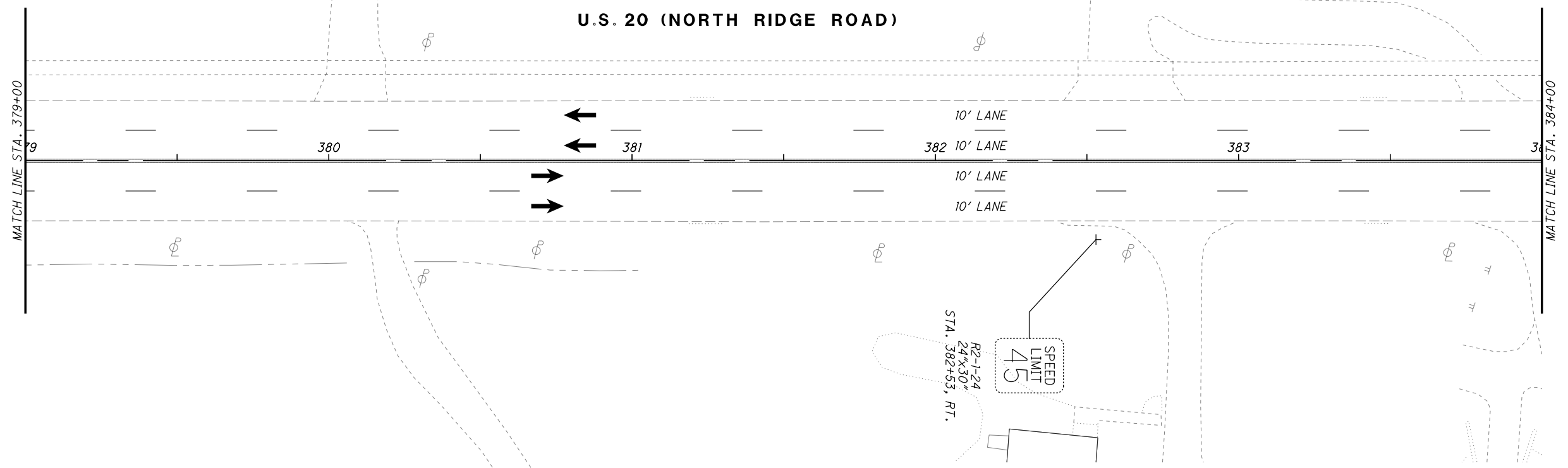
CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 379+00 TO STA. 384+00

LAK-US-20-24.99
PART 2

178
697



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

LEGEND

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP266A.dgn Sheet 7/15/2022 2:52:25 PM troyer

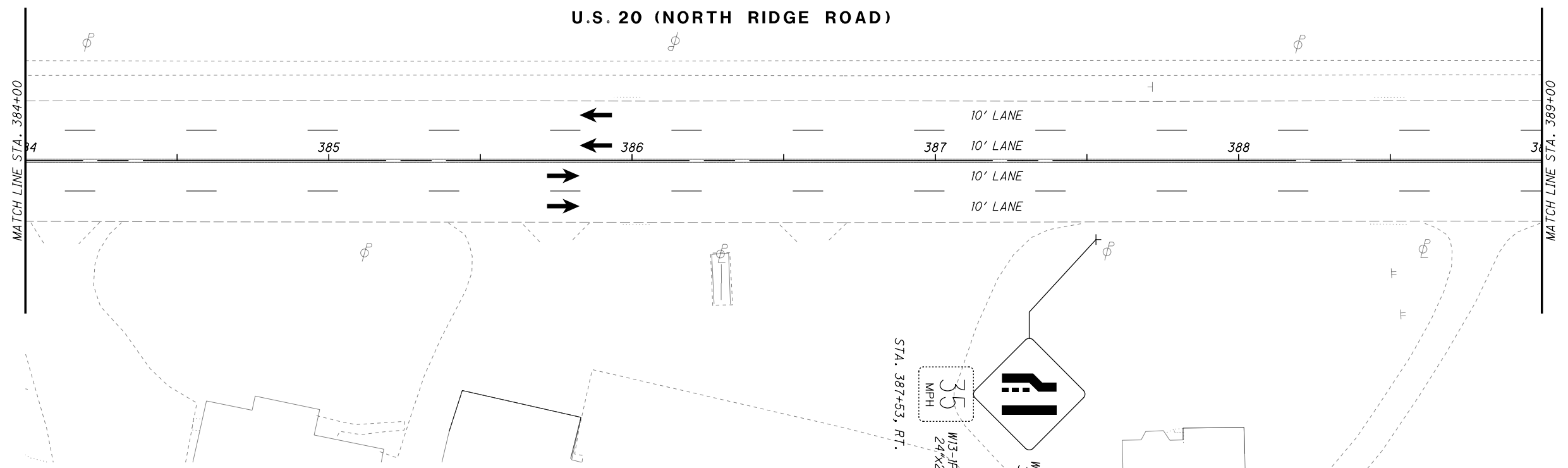
CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 384+00 TO STA. 389+00

LAK-US-20-24.99
PART 2

179
697



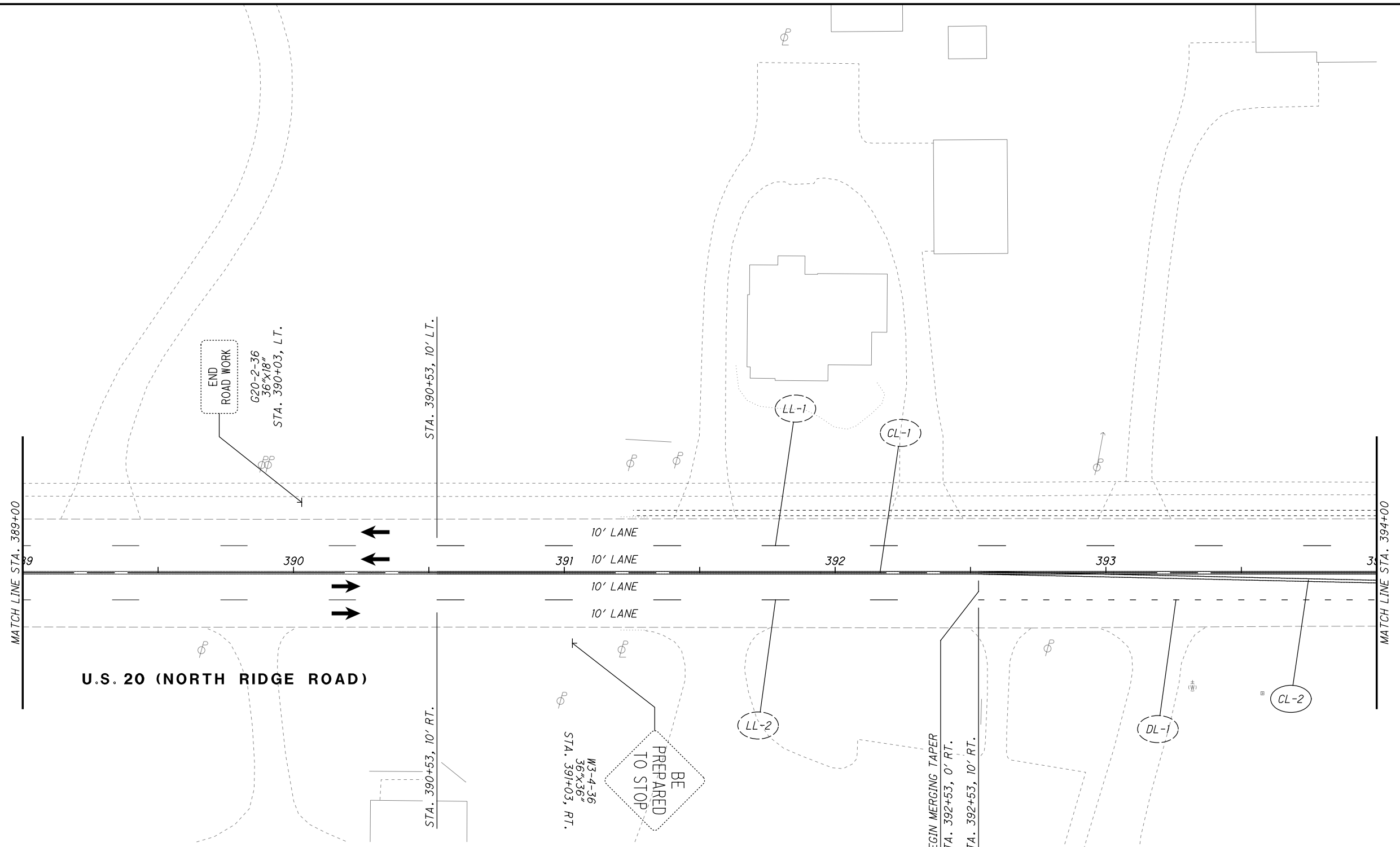
LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | () | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT
CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT
CONSTRUCTED UNDER PRE-PHASE 3 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

LEGEND

- | | |
|-----|------------------------------------------------------------|
| () | MAINTENANCE OF TRAFFIC SIGN |
| () | MAINTENANCE OF TRAFFIC SIGN
INSTALLED IN PREVIOUS PHASE |
| () | EXISTING TRAFFIC SIGN |

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP266B.dgn Sheet 7/15/2022 2:52:25 PM troyer



- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- () WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- ← TRAFFIC FLOW

- LEGEND**
- MAINTENANCE OF TRAFFIC SIGN
 - MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
 - EXISTING TRAFFIC SIGN

CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 389+00 TO STA. 394+00

LAK-US-20-24.99
PART 2

H:\2018\180743\0D0T\ProjectData\092TO_LAK_20_EAST\Design\MOT\Sheets\092TO_MP266C.dgn Sheet 7/15/2022 2:52:26 PM troyer

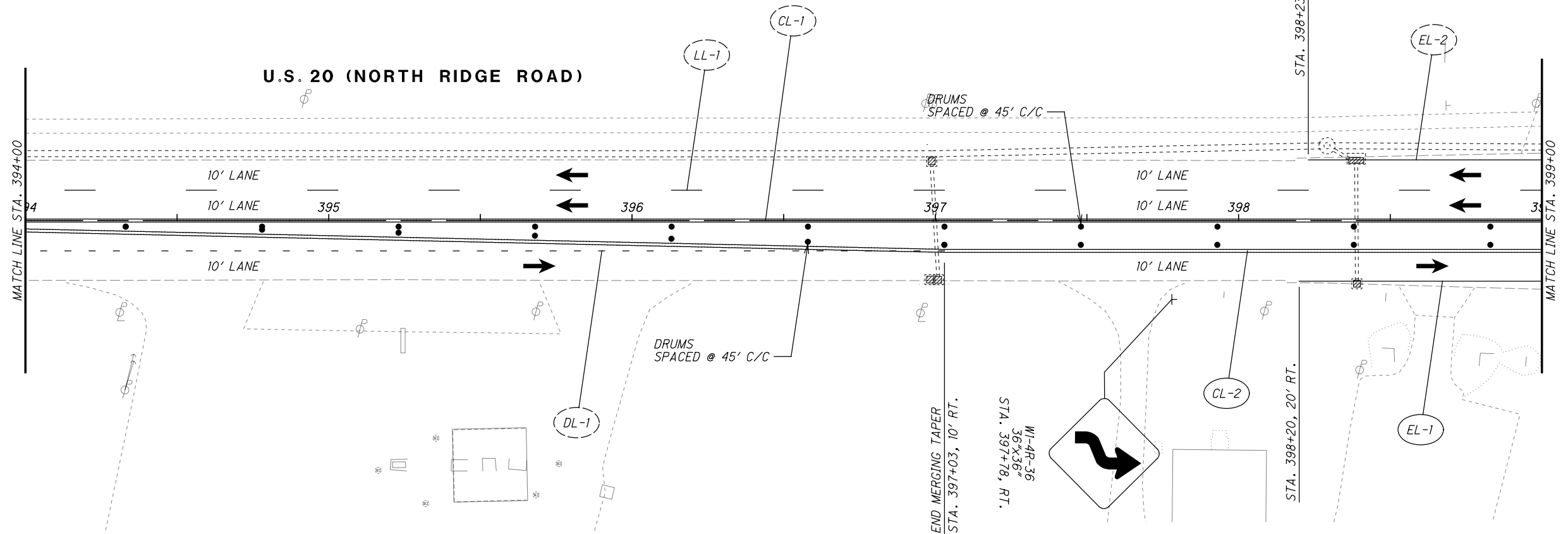
CALCULATED
 EJT
 CHECKED
 WDB

0 20 40
 HORIZONTAL
 SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 394+00 TO STA. 399+00

LAK-US-20-24.99
PART 2

181
697



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\01\Sheets\09270_MP266D.dgn Sheet 7/15/2022 2:52:26 PM troyer

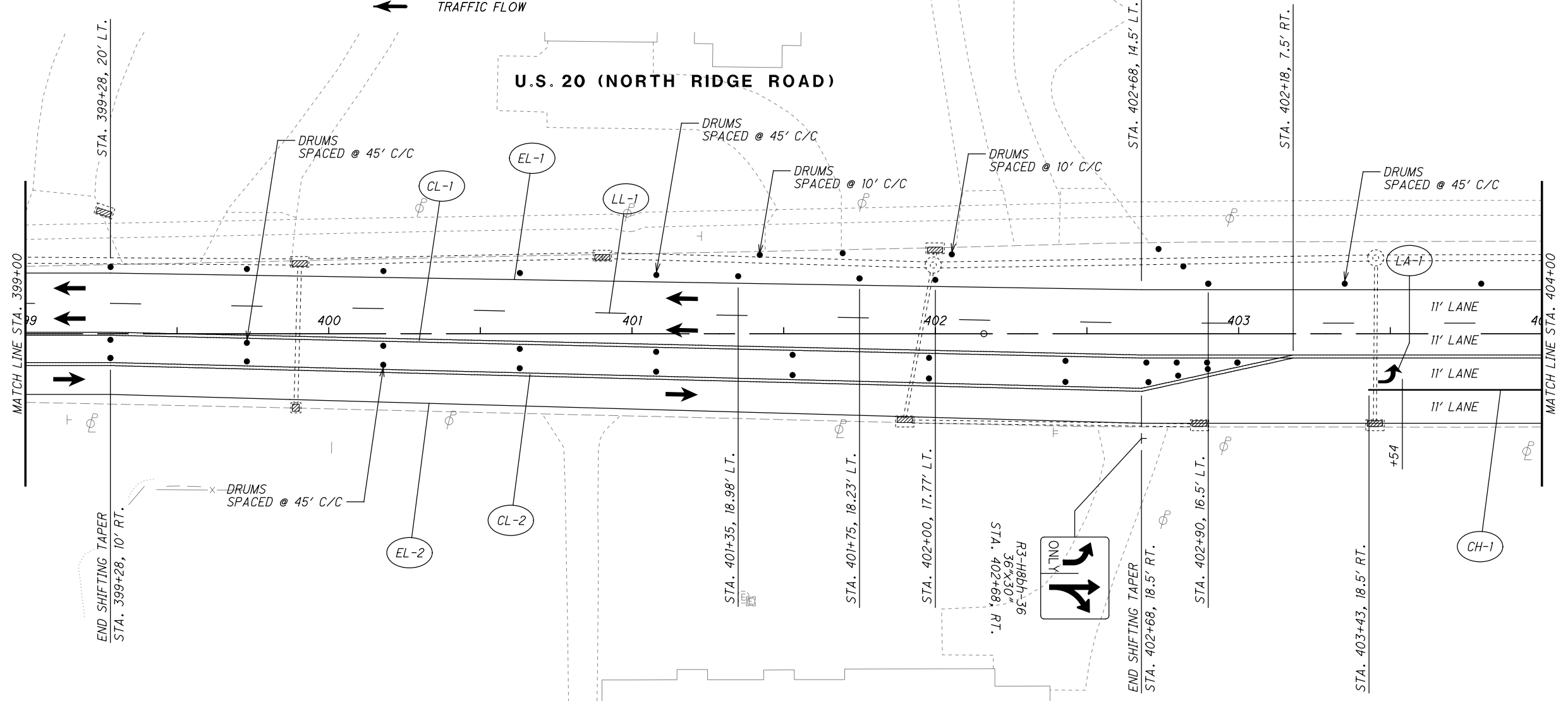
- LEGEND**
- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
 - (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
 - (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
 - (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
 - (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
 - (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
 - (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
 - (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
 - (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
 - (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
 - () WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
 - [X] PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
 - [/] PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
 - ← TRAFFIC FLOW

CALCULATED
EJT
CHECKED
WDB

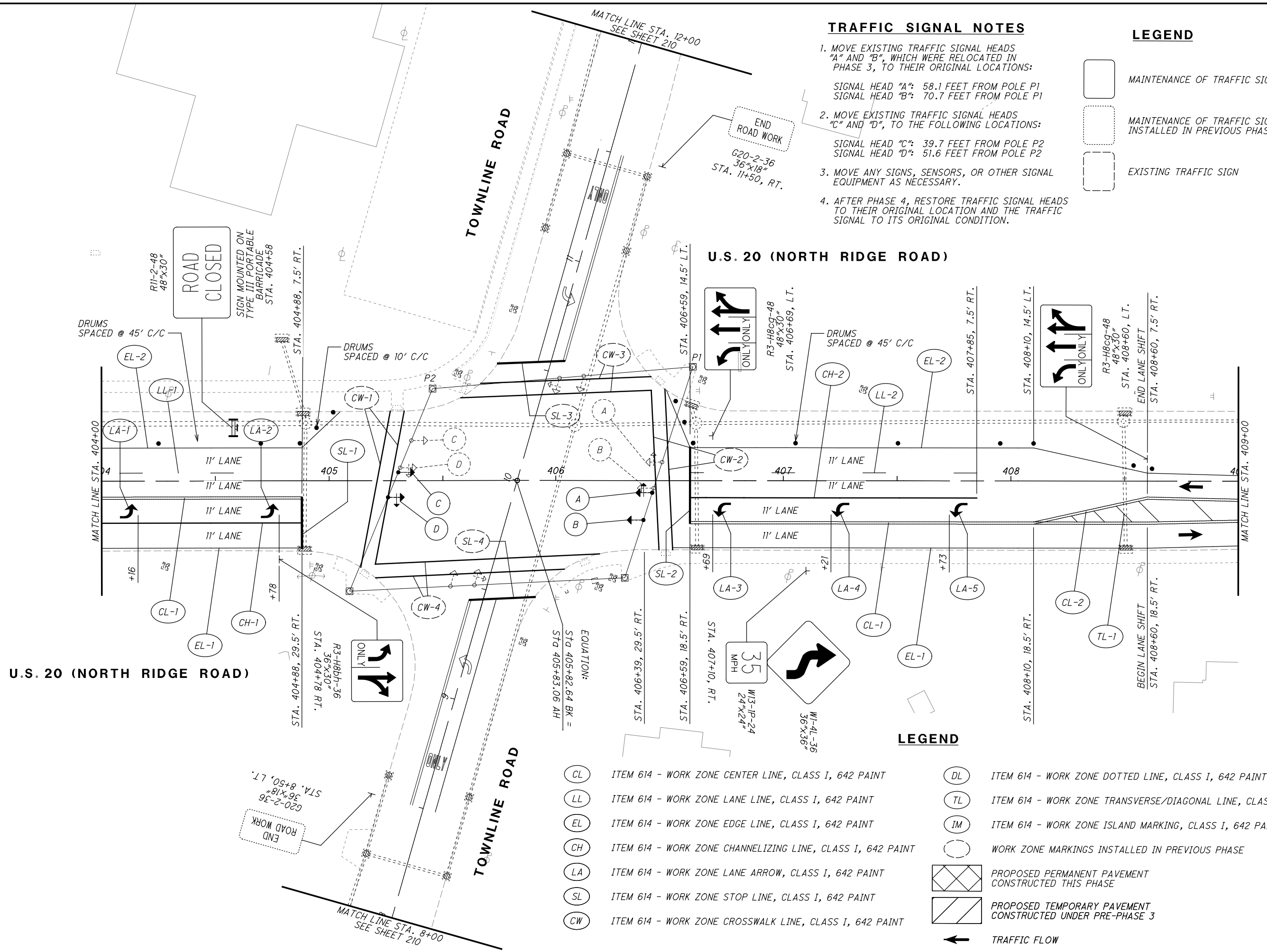
0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 399+00 TO STA. 404+00

LAK-US-20-24.99
PART 2



- LEGEND**
- [] MAINTENANCE OF TRAFFIC SIGN
 - [] MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
 - [] EXISTING TRAFFIC SIGN



TRAFFIC SIGNAL NOTES

1. MOVE EXISTING TRAFFIC SIGNAL HEADS "A" AND "B", WHICH WERE RELOCATED IN PHASE 3, TO THEIR ORIGINAL LOCATIONS:
 SIGNAL HEAD "A": 58.1 FEET FROM POLE P1
 SIGNAL HEAD "B": 70.7 FEET FROM POLE P1
2. MOVE EXISTING TRAFFIC SIGNAL HEADS "C" AND "D", TO THE FOLLOWING LOCATIONS:
 SIGNAL HEAD "C": 39.7 FEET FROM POLE P2
 SIGNAL HEAD "D": 51.6 FEET FROM POLE P2
3. MOVE ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.
4. AFTER PHASE 4, RESTORE TRAFFIC SIGNAL HEADS TO THEIR ORIGINAL LOCATION AND THE TRAFFIC SIGNAL TO ITS ORIGINAL CONDITION.

LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

0 20 40
 HORIZONTAL SCALE IN FEET
 CALCULATED EJT
 CHECKED WDB

U.S. 20 (NORTH RIDGE ROAD)

LEGEND

- | | |
|------------------------------------------------------------|-------------------------------------------------------------------|
| ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
| ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | TRAFFIC FLOW |

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP266F.dgn Sheet 7/15/2022 2:52:28 PM troyer



0 10 20 40
HORIZONTAL
SCALE IN FEET

CALCULATED
EJT
CHECKED
WDB

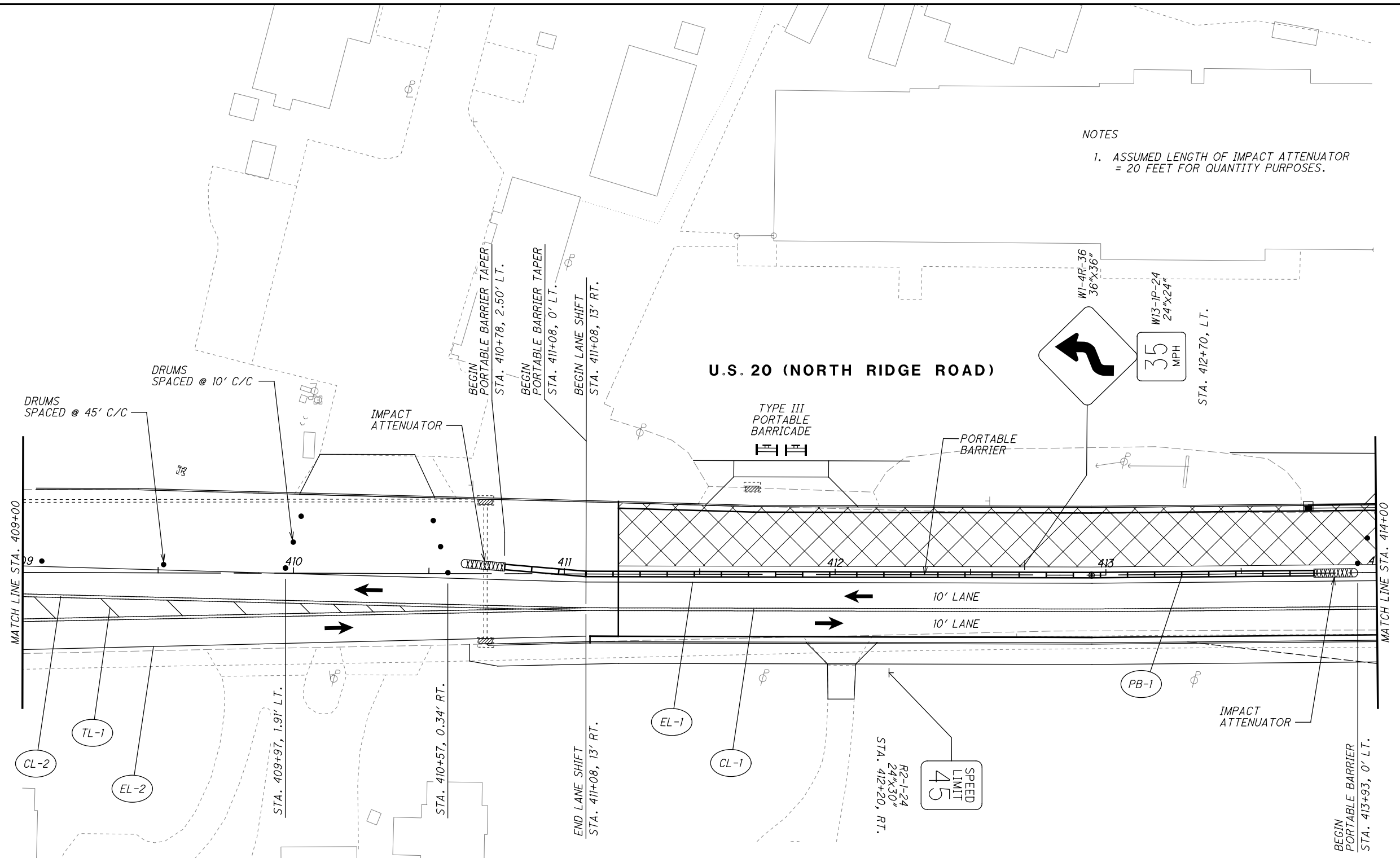
MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 409+00 TO STA. 414+00

LAK-US-20-24.99
PART 2

184
697

NOTES

1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

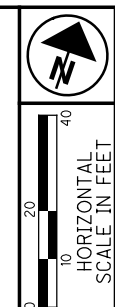
- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP266G.dgn Sheet 7/15/2022 2:52:28 PM troyer



CALCULATED	EJT	CHECKED	WDB
------------	-----	---------	-----

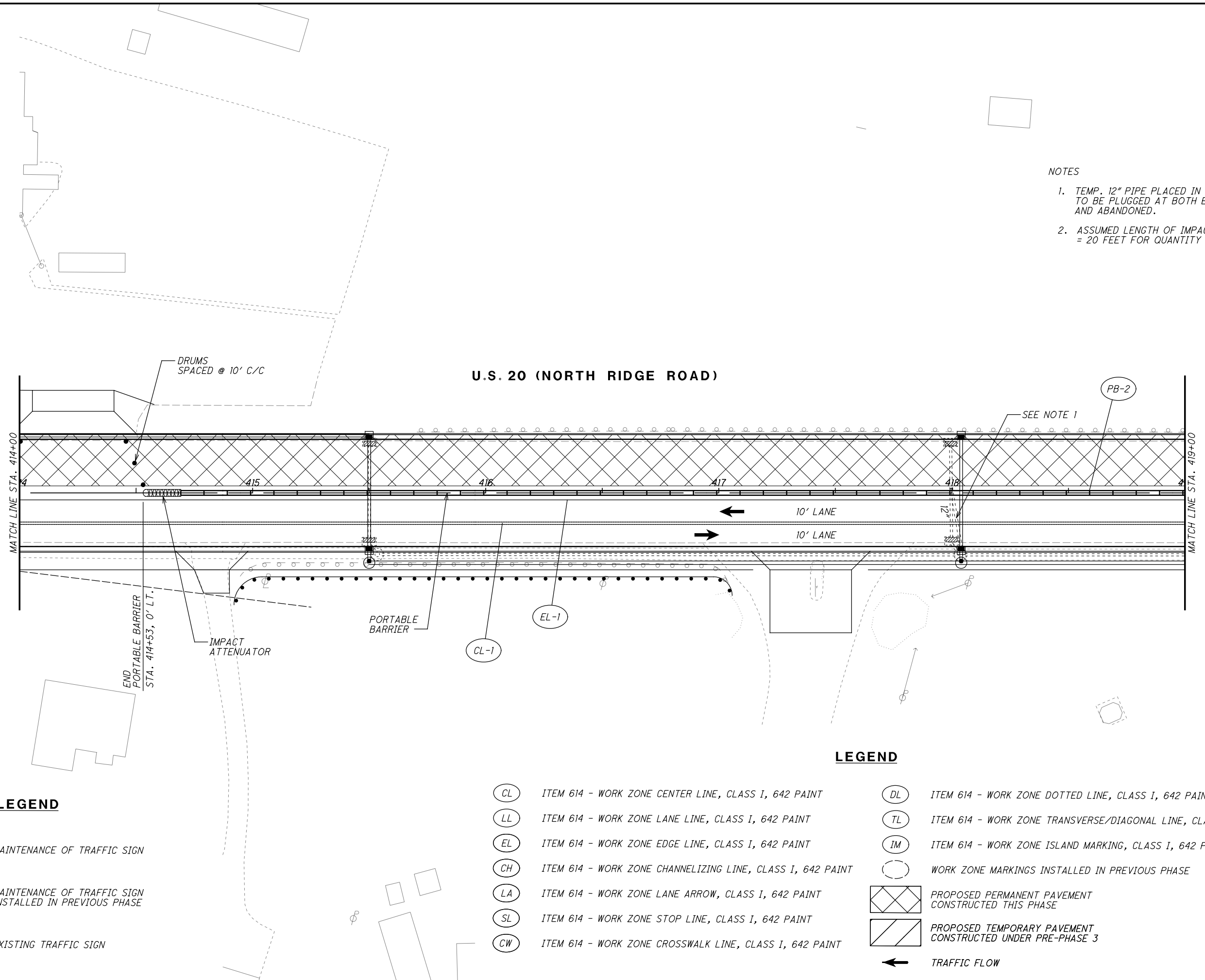
MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 414+00 TO STA. 419+00

LAK-US-20-24.99
PART 2

185
697

NOTES

1. TEMP. 12" PIPE PLACED IN PHASE 3 TO BE PLUGGED AT BOTH ENDS, FILLED AND ABANDONED.
2. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN


- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP266H.dgn Sheet 7/15/2022 2:52:29 PM troyer





 CALCULATED: EJT

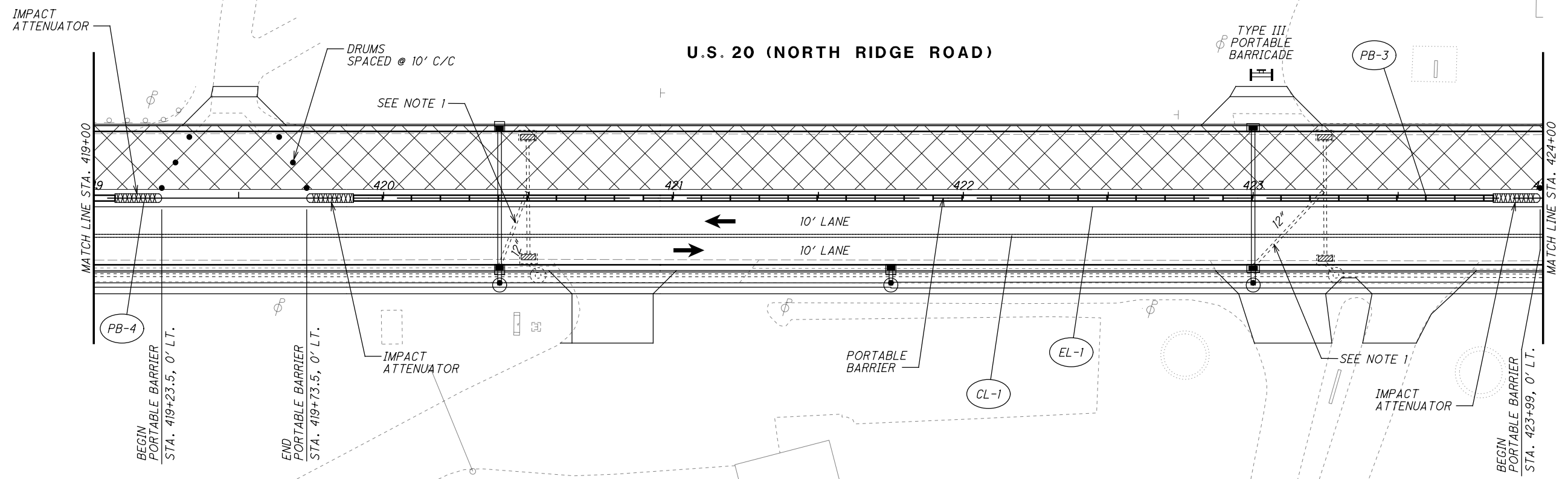
 CHECKED: WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 419+00 TO STA. 424+00




LAK-US-20-24.99
PART 2








NOTES

1. TEMP. 12" PIPE PLACED IN PHASE 3 TO BE PLUGGED AT BOTH ENDS, FILLED AND ABANDONED.
2. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.










LEGEND

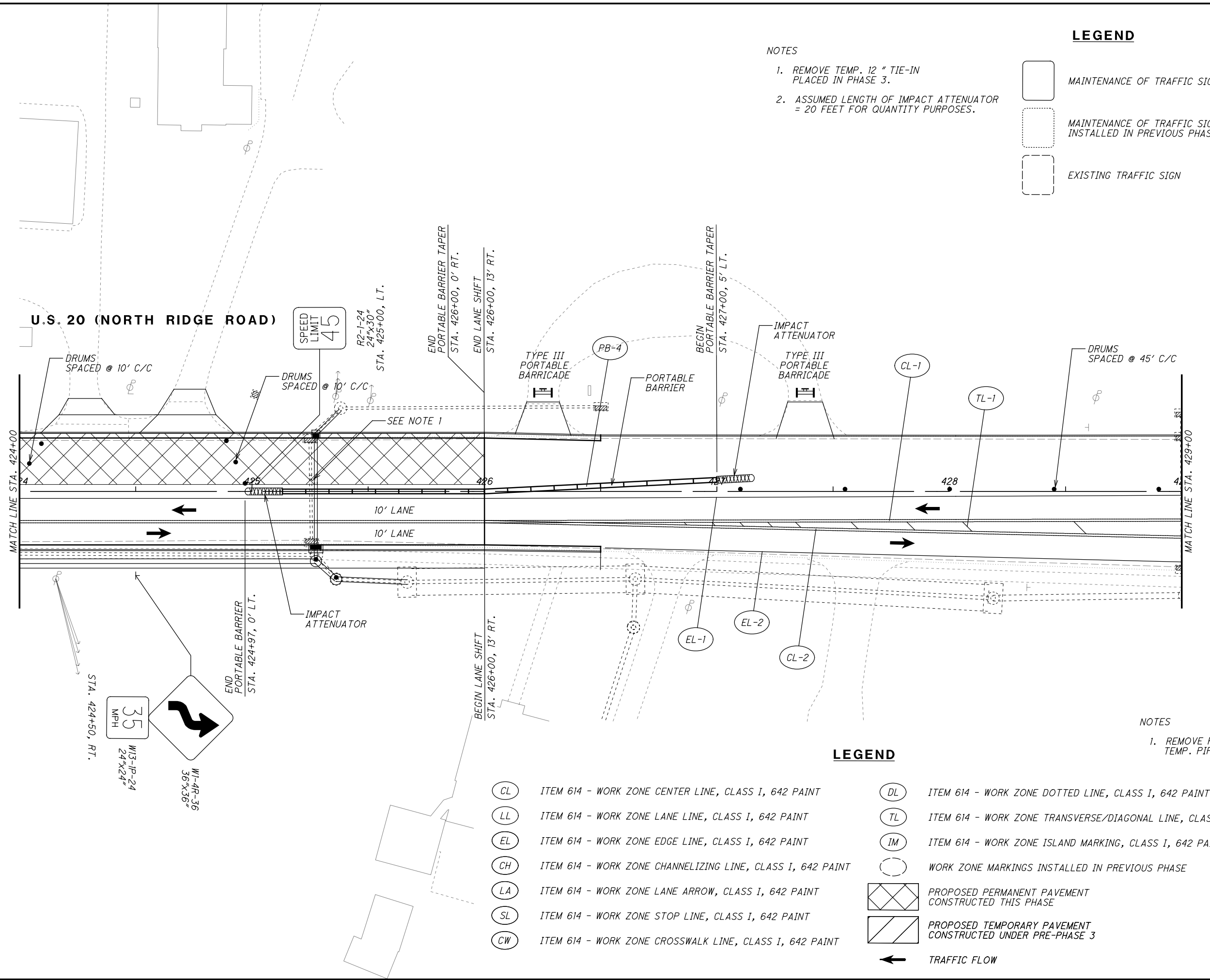
-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  CL ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  LL ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  EL ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  CH ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  LA ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  SL ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  CW ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  DL ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  TL ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  IM ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW




H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP2661.dgn Sheet 7/15/2022 2:52:29 PM Troyer



NOTES

1. REMOVE TEMP. 12" TIE-IN PLACED IN PHASE 3.
2. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.












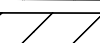


LEGEND


-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN


NOTES

1. REMOVE PHASE 3 TEMP. PIPE TIE-IN

LEGEND

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |





HORIZONTAL SCALE IN FEET

CALCULATED	EJT
CHECKED	WDB

LAK-US-20-24.99

MAINTENANCE OF TRAFFIC PLAN - PHASE 4

STA. 424+00 TO STA. 429+00

PART 2

187

697

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP266.dgn Sheet 7/15/2022 2:52:30 PM troyer

CALCULATED EJT
CHECKED WDB

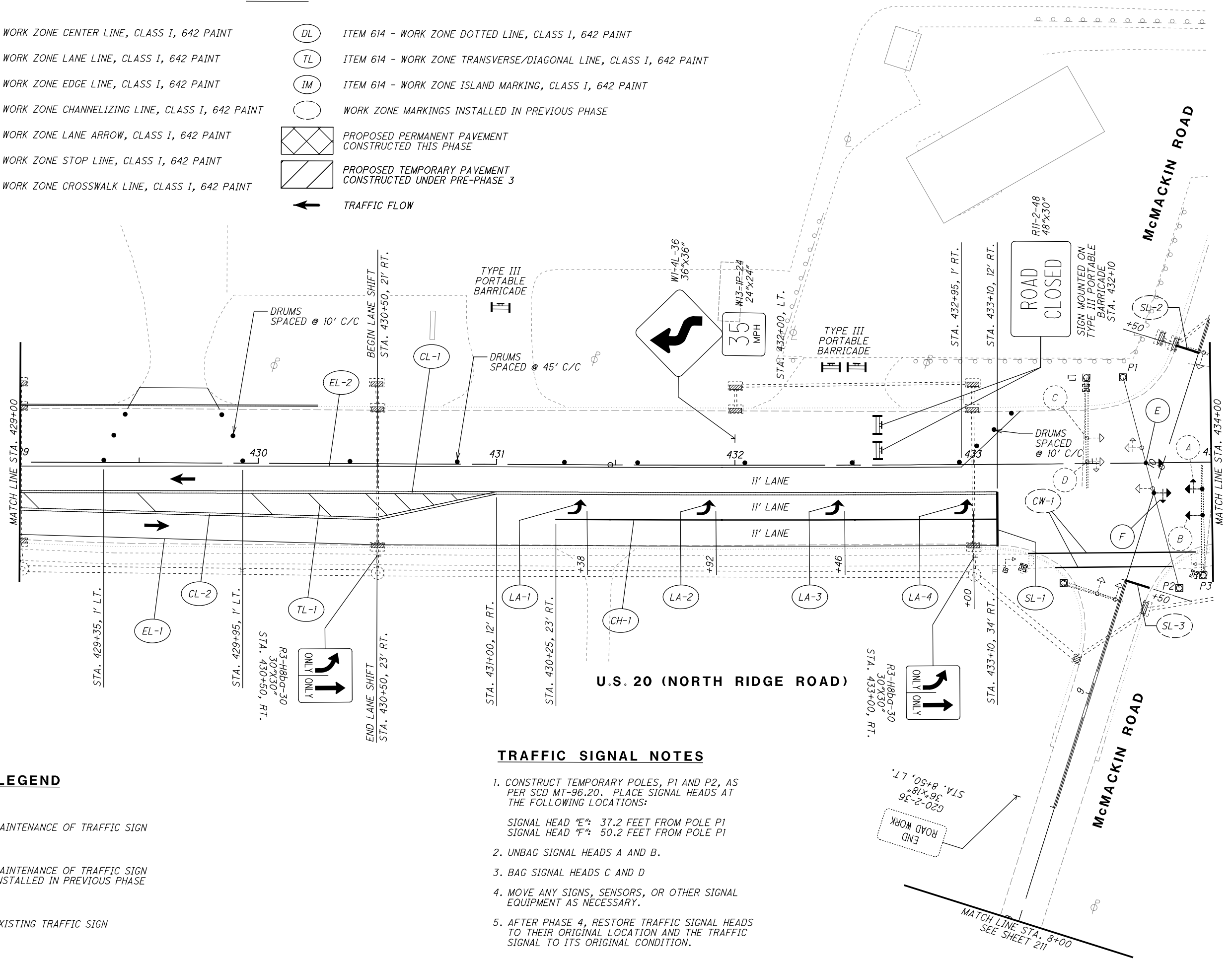
0 10 20 40
HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 429+00 TO STA. 434+00

LAK-US-20-24.99
PART 2

LEGEND

- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- () WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- (X) PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- (/) PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- ← TRAFFIC FLOW

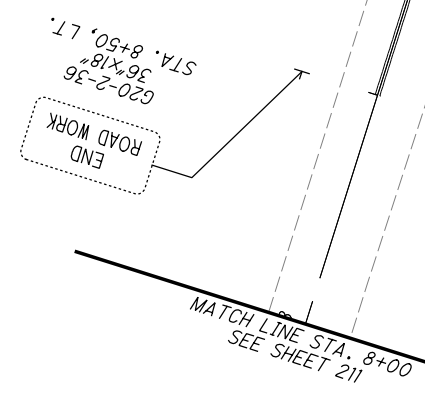


LEGEND

- () MAINTENANCE OF TRAFFIC SIGN
- () MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- () EXISTING TRAFFIC SIGN

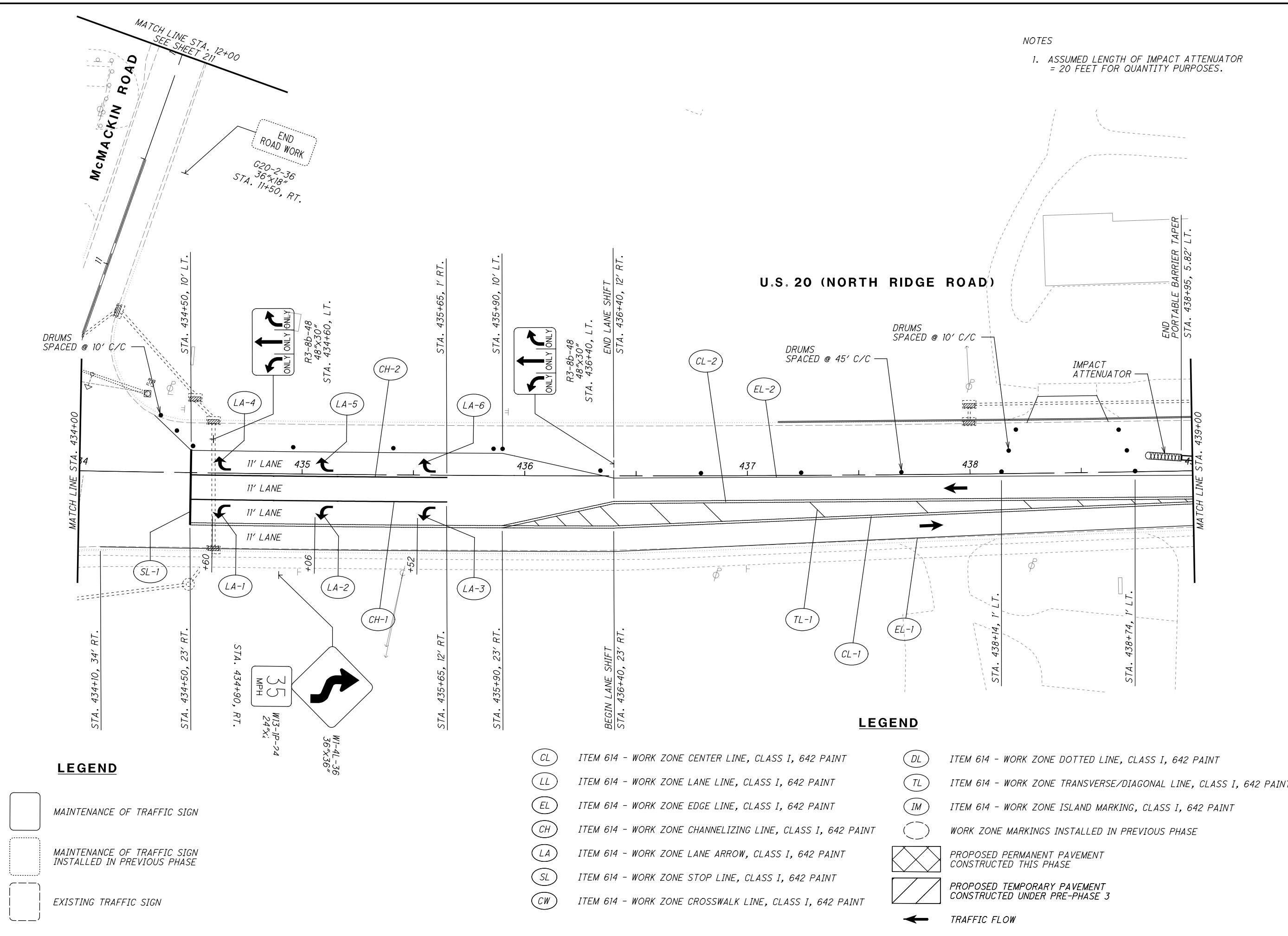
TRAFFIC SIGNAL NOTES

1. CONSTRUCT TEMPORARY POLES, P1 AND P2, AS PER SCD MT-96.20. PLACE SIGNAL HEADS AT THE FOLLOWING LOCATIONS:
 SIGNAL HEAD "E": 37.2 FEET FROM POLE P1
 SIGNAL HEAD "F": 50.2 FEET FROM POLE P1
2. UNBAG SIGNAL HEADS A AND B.
3. BAG SIGNAL HEADS C AND D
4. MOVE ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.
5. AFTER PHASE 4, RESTORE TRAFFIC SIGNAL HEADS TO THEIR ORIGINAL LOCATION AND THE TRAFFIC SIGNAL TO ITS ORIGINAL CONDITION.



H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\01\Sheets\09270_MP266k.dgn Sheet 7/15/2022 2:52:31PM troyer

NOTES
 1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

MATCH LINE STA. 12+00
SEE SHEET 211

McMACKIN ROAD

END ROAD WORK
 G20-2-36
 36"x18"
 STA. 11+50, RT.

U.S. 20 (NORTH RIDGE ROAD)

END PORTABLE BARRIER TAPER
 STA. 438+95, 5.82' LT.

DRUMS SPACED @ 10' C/C

DRUMS SPACED @ 45' C/C

DRUMS SPACED @ 10' C/C

IMPACT ATTENUATOR

MATCH LINE STA. 434+00

MATCH LINE STA. 439+00

STA. 434+10, 34' RT.

STA. 434+50, 23' RT.

STA. 434+90, RT.

STA. 435+65, 12' RT.

STA. 435+90, 23' RT.

BEGIN LANE SHIFT
 STA. 436+40, 23' RT.

STA. 438+14, 1' LT.

STA. 438+74, 1' LT.

R3-8b-48
 48"x30"
 STA. 434+60, LT.

R3-8b-48
 48"x30"
 STA. 436+40, LT.

LA-4

LA-5

LA-6

CH-2

CL-2

EL-2

SL-1

LA-1

LA-2

CH-1

LA-3

TL-1

CL-1

EL-1

35
 MPH
 W13-IP-24
 36"x36"

W1-4L-36
 36"x36"

W13-IP-24
 36"x36"

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP267.dgn Sheet 7/15/2022 2:52:31PM Troyer





 CALCULATED EJT
 CHECKED WDB

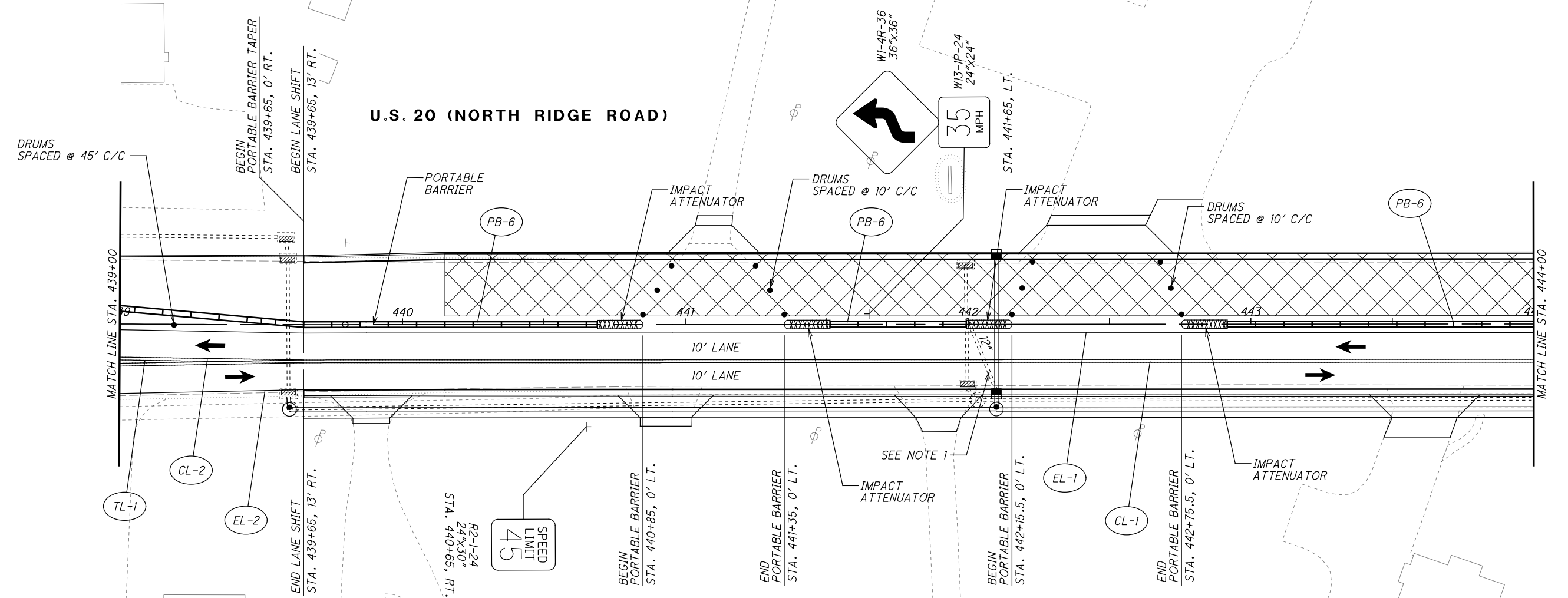
MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 439+00 TO STA. 444+00

LAK-US-20-24.99
PART 2




190
697










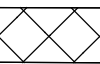

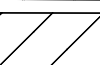


- NOTES**
- TEMP. 12" PIPE PLACED IN PHASE 3 TO BE PLUGGED AT BOTH ENDS, FILLED AND ABANDONED.
 - ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

U.S. 20 (NORTH RIDGE ROAD)



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

LEGEND



CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 444+00 TO STA. 449+00

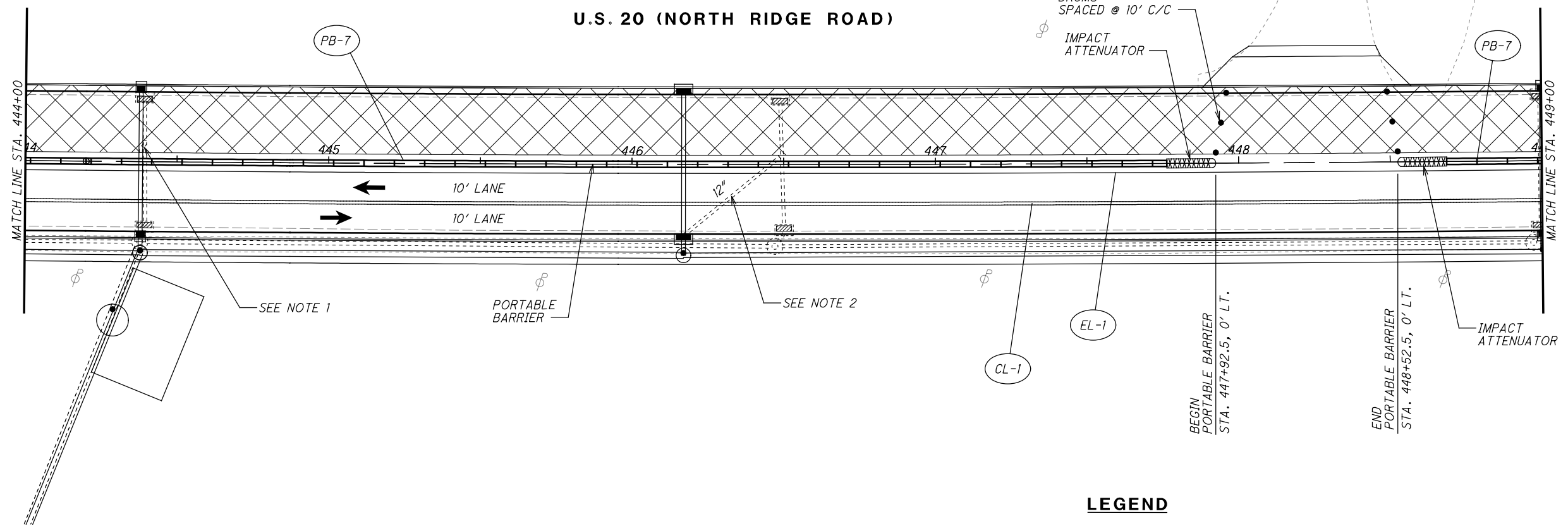
LAK-US-20-24.99
PART 2

191
697

NOTES

1. REMOVE PHASE 3 TEMP. 12" PIPE TIE-IN.
2. TEMP. 12" PIPE PLACED IN PHASE 3 TO BE PLUGGED AT BOTH ENDS, FILLED AND ABANDONED.
3. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

U.S. 20 (NORTH RIDGE ROAD)



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\M0T\Sheets\09270_MP268.dgn Sheet 7/15/2022 2:52:32 PM troyer

H:\2018\80743\0DOT\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP269.dgn Sheet 7/15/2022 2:52:33 PM troyer



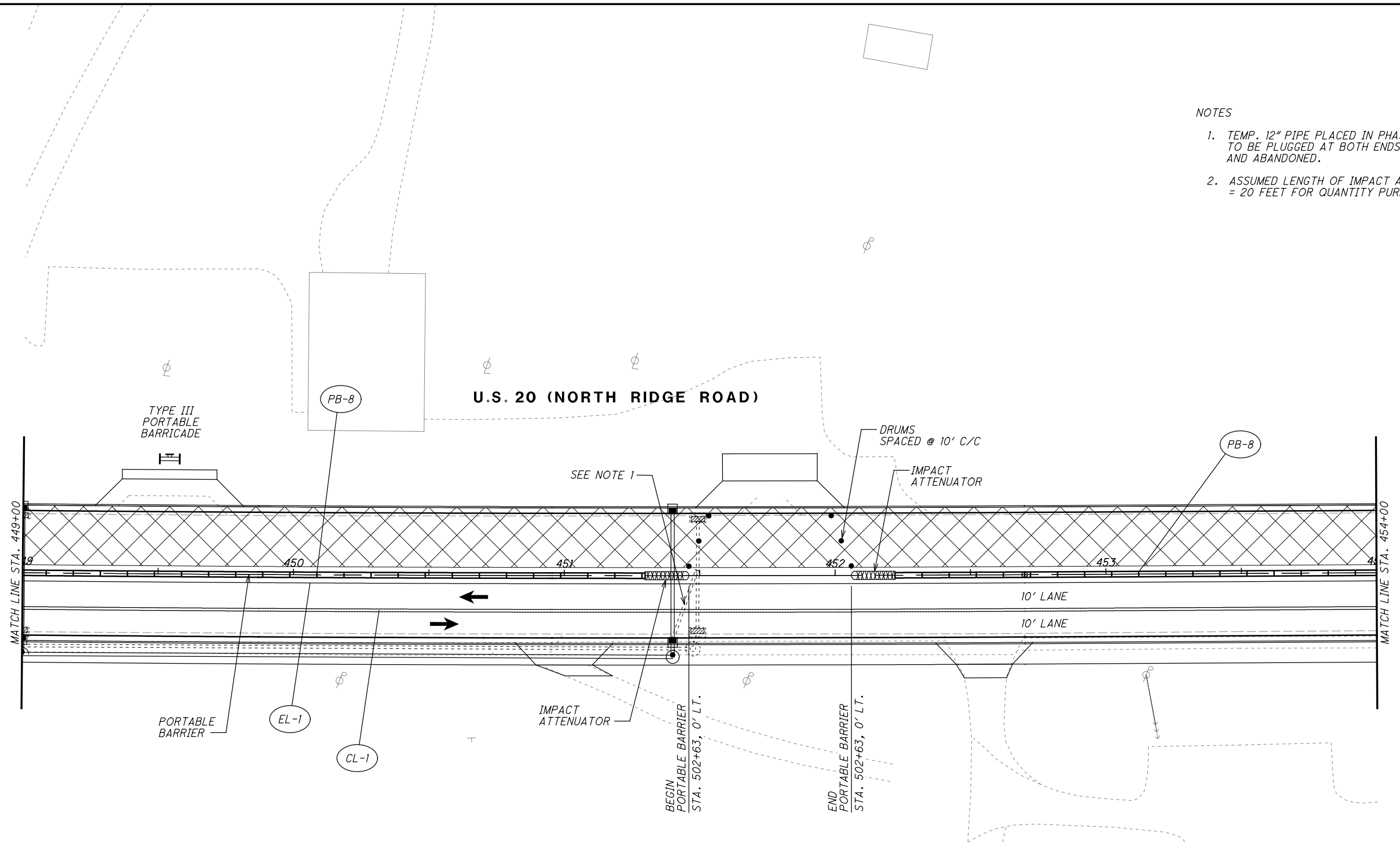
CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 449+00 TO STA. 454+00

LAK-US-20-24.99
PART 2

192
697

- NOTES**
1. TEMP. 12" PIPE PLACED IN PHASE 3 TO BE PLUGGED AT BOTH ENDS, FILLED AND ABANDONED.
 2. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\001\Sheets\09270_MP270.dgn Sheet 7/15/2022 2:52:33 PM troyer

CALCULATED
EJT
CHECKED
WDB

0 10 20 40
HORIZONTAL
SCALE IN FEET

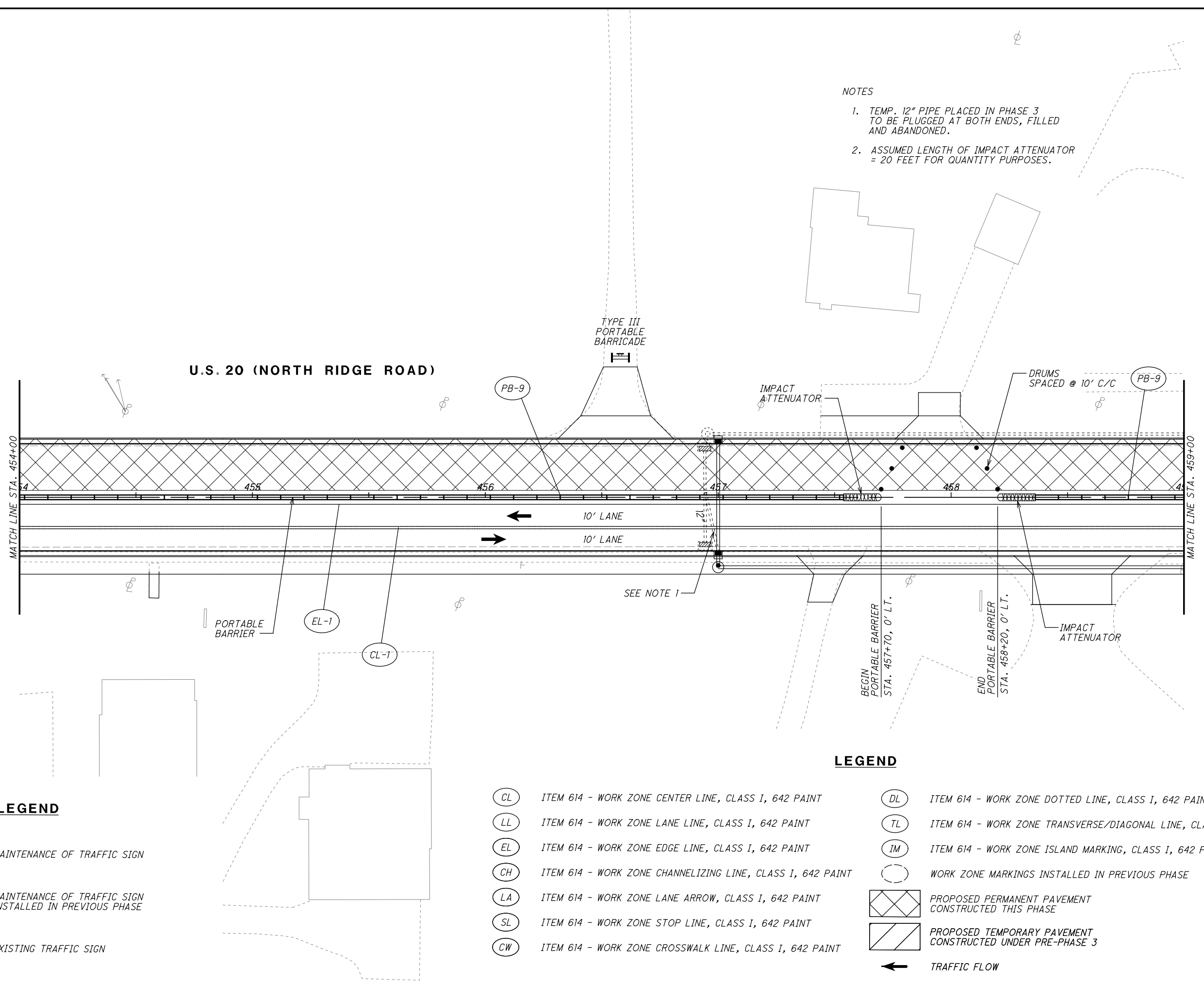
MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 454+00 TO STA. 459+00

LAK-US-20-24.99
PART 2

193
697

NOTES

1. TEMP. 12" PIPE PLACED IN PHASE 3 TO BE PLUGGED AT BOTH ENDS, FILLED AND ABANDONED.
2. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

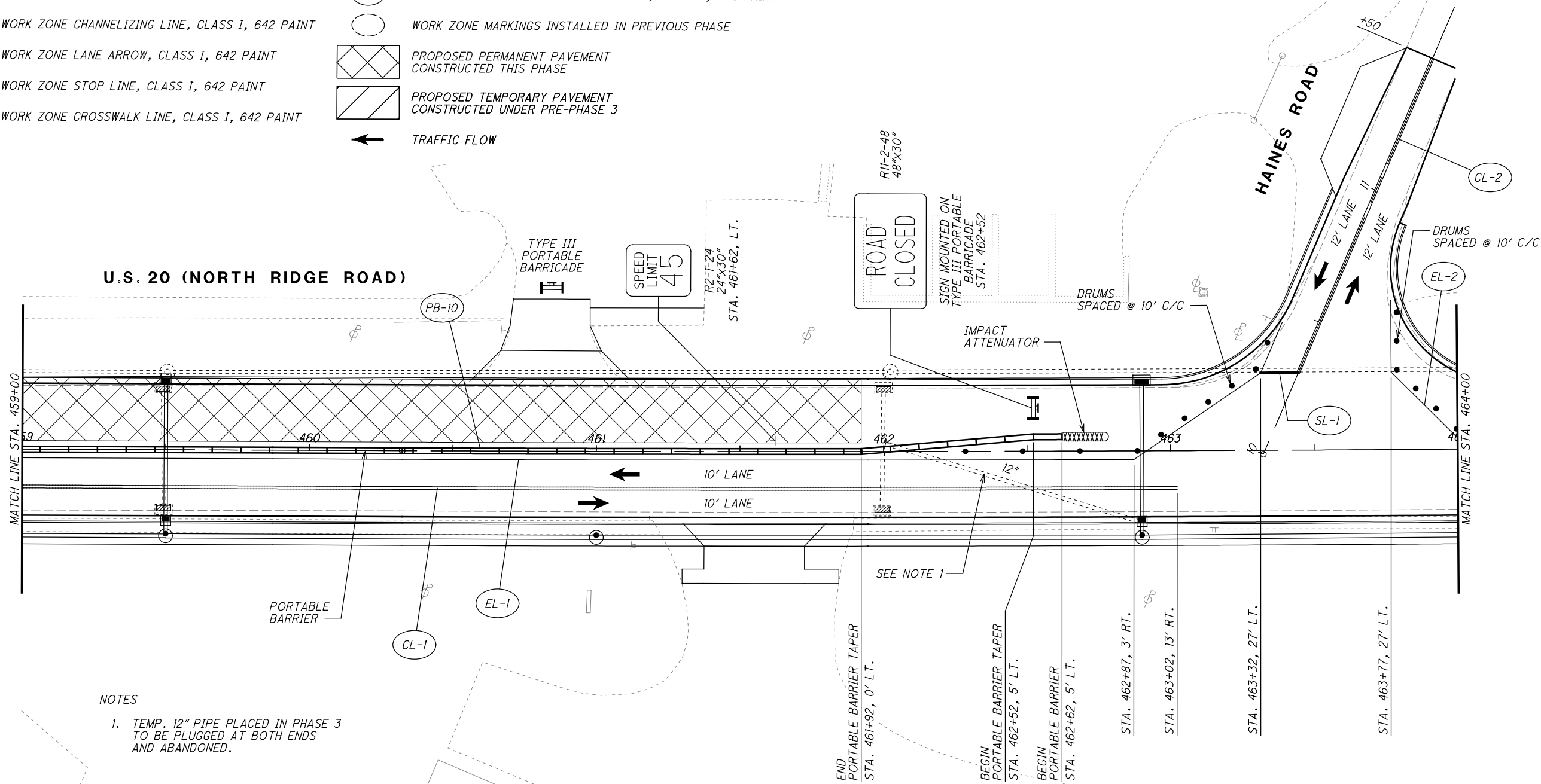
H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP27L.dgn Sheet 7/15/2022 2:52:34 PM troyer

LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | () | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

NOTES

1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



NOTES

1. TEMP. 12" PIPE PLACED IN PHASE 3 TO BE PLUGGED AT BOTH ENDS AND ABANDONED.

LEGEND

- | | |
|-----|---------------------------------------------------------|
| () | MAINTENANCE OF TRAFFIC SIGN |
| () | MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE |
| () | EXISTING TRAFFIC SIGN |

NOTE:

THE CONTRACTOR SHALL BE ALLOWED TO CLOSE HAINES ROAD AT STA. 463+33, RT. FOR SEVEN (7) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT HAINES ROAD REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT SIDE STREETS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

THE FOLLOWING ADJACENT SIDE STREETS WILL REMAIN OPEN DURING THE CLOSURE OF THIS SIDE STREET.

McMACKIN ROAD STA. 433+80
GREEN ROAD STA. 494+73

FOR CLOSURE OF HAINES ROAD SEE DETAILS ON SHEET 214.
FOR DETOUR ROUTE SEE SHEET 27.

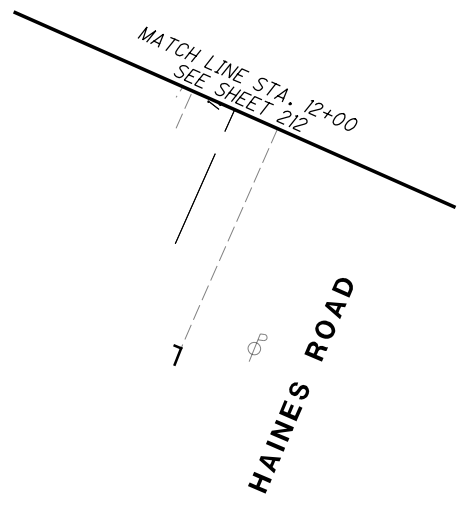


CALCULATED EJT
CHECKED WDB

**MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 459+00 TO STA. 464+00**

**LAK-US-20-24.99
PART 2**

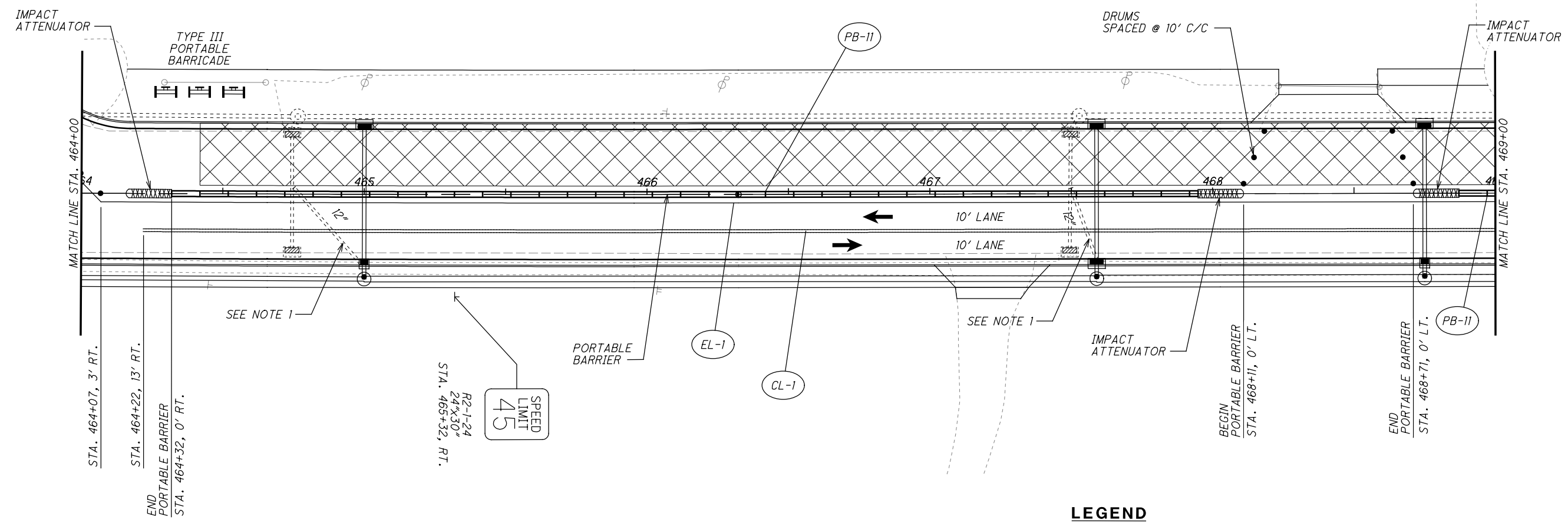
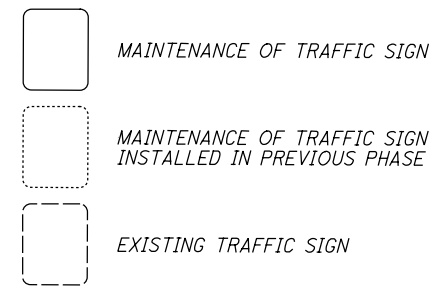
H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP272.dgn Sheet 7/15/2022 2:52:34 PM troyer



NOTES

- 1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

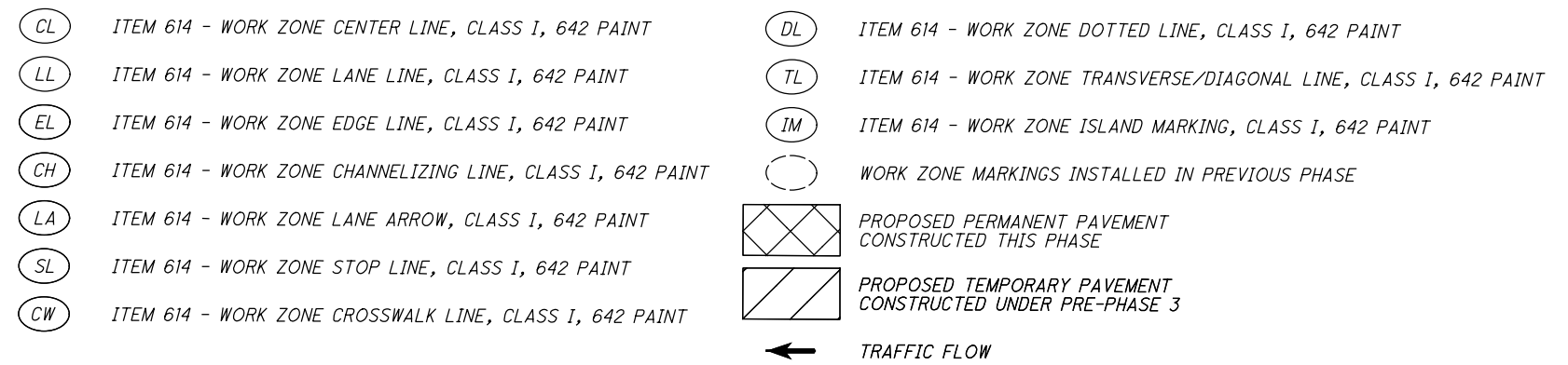
LEGEND



NOTES

- 1. TEMP. 12" PIPE PLACED IN PHASE 3 TO BE PLUGGED AT BOTH ENDS AND ABANDONED.

LEGEND



LAK-US-20-24.99 PART 2 MAINTENANCE OF TRAFFIC PLAN - PHASE 4 STA. 464+00 TO STA. 469+00

LAK-US-20-24.99 PART 2

195
697

H:\2018\80743\0DOT\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP273.dgn Sheet 7/15/2022 2:52:35 PM troyer



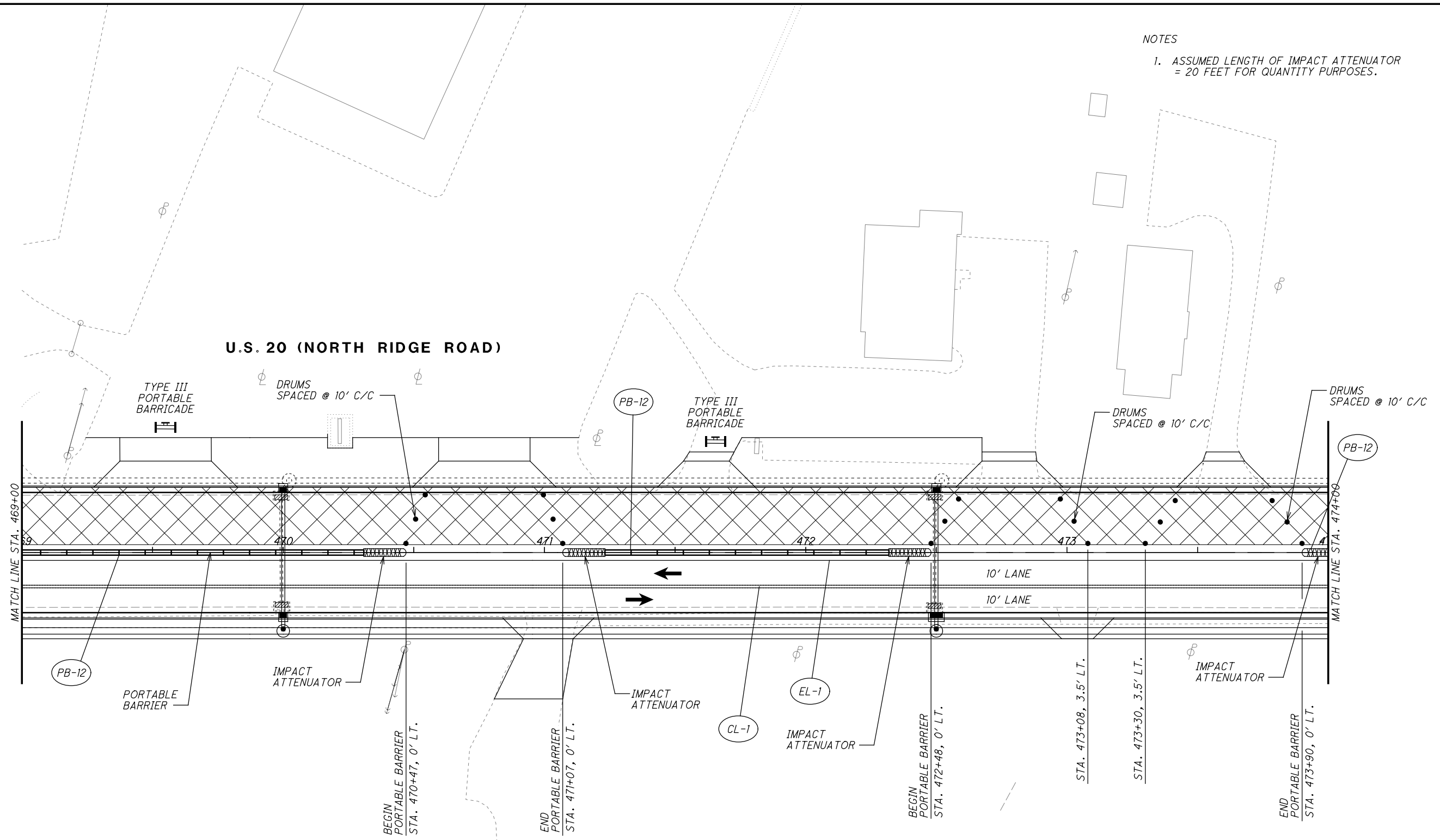
CALCULATED
EJT
CHECKED
WDB

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 469+00 TO STA. 474+00

LAK-US-20-24.99
PART 2

196
697

NOTES
1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND


- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN


- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP274.dgn Sheet 7/15/2022 2:52:36 PM troyer





 HORIZONTAL SCALE IN FEET

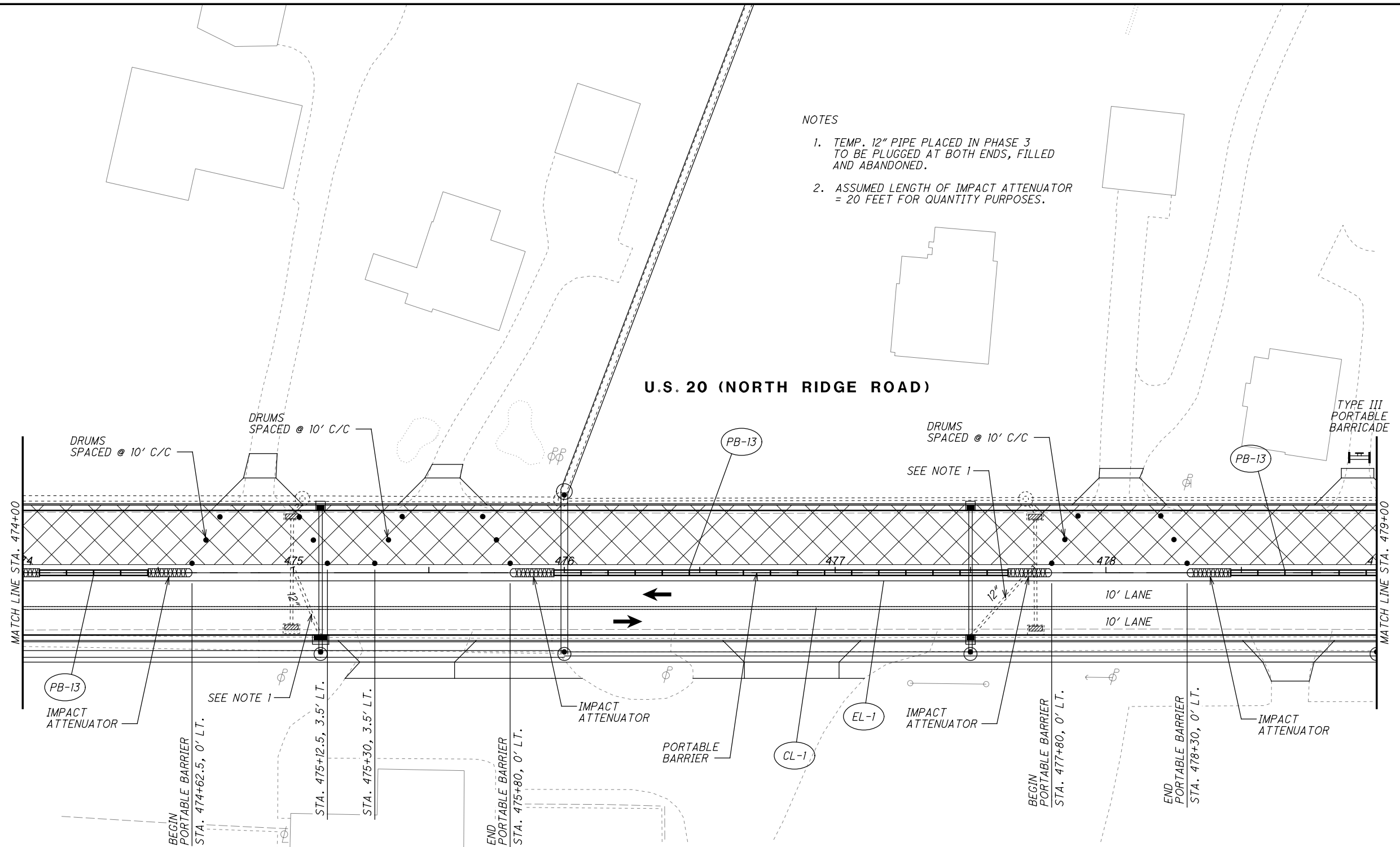
CALCULATED EJT
 CHECKED WDB
MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 474+00 TO STA. 479+00

LAK-US-20-24.99
PART 2




197
 697








NOTES

- TEMP. 12" PIPE PLACED IN PHASE 3 TO BE PLUGGED AT BOTH ENDS, FILLED AND ABANDONED.
- ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.










LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

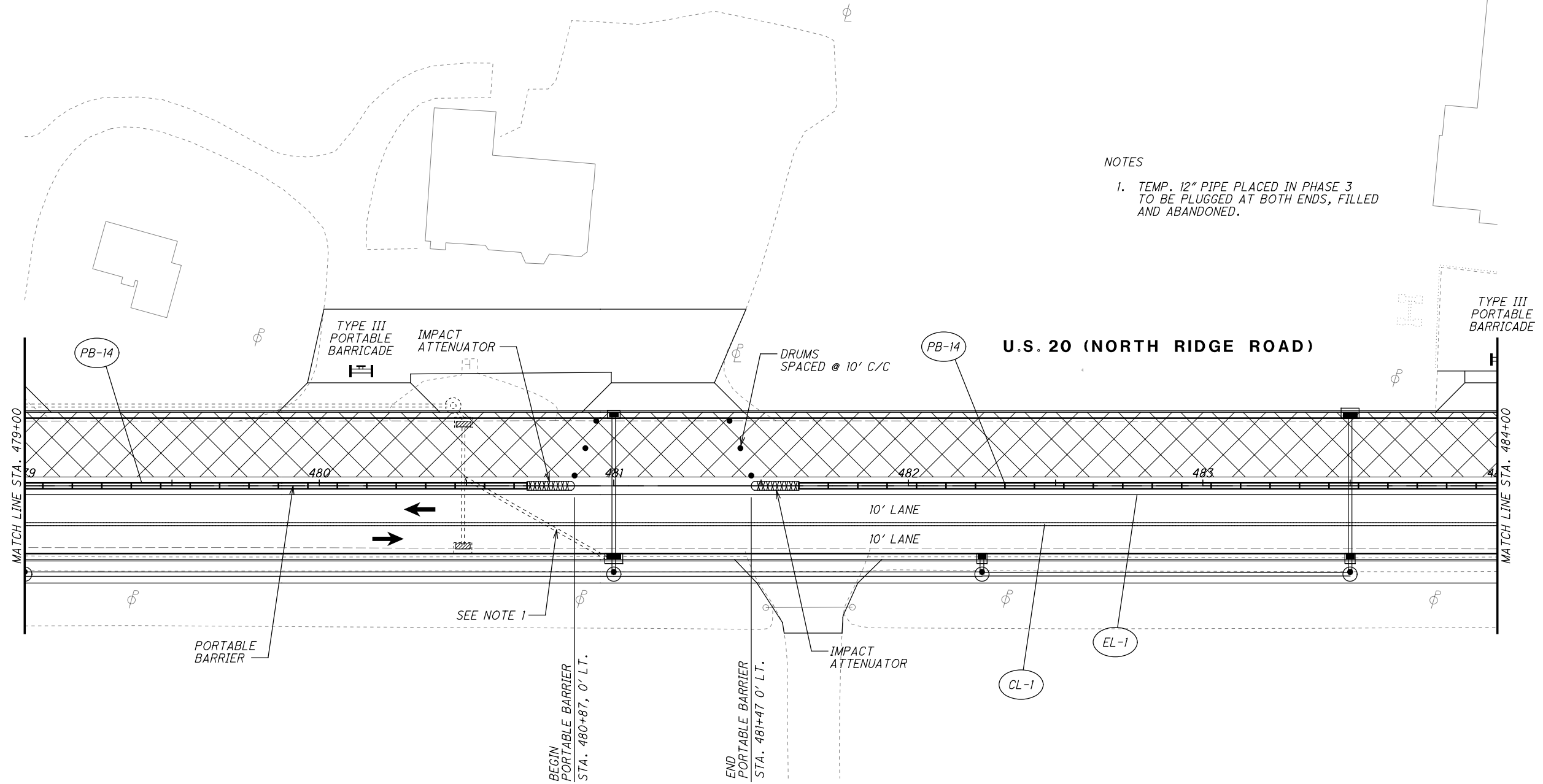
LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW

H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP275.dgn Sheet 7/15/2022 2:52:36 PM troyer

NOTES
 1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.

NOTES
 1. TEMP. 12" PIPE PLACED IN PHASE 3 TO BE PLUGGED AT BOTH ENDS, FILLED AND ABANDONED.













CALCULATED EJT
 CHECKED WDB
MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 479+00 TO STA. 484+00

LAK-US-20-24.99
PART 2





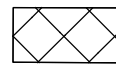
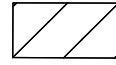

198
 697

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

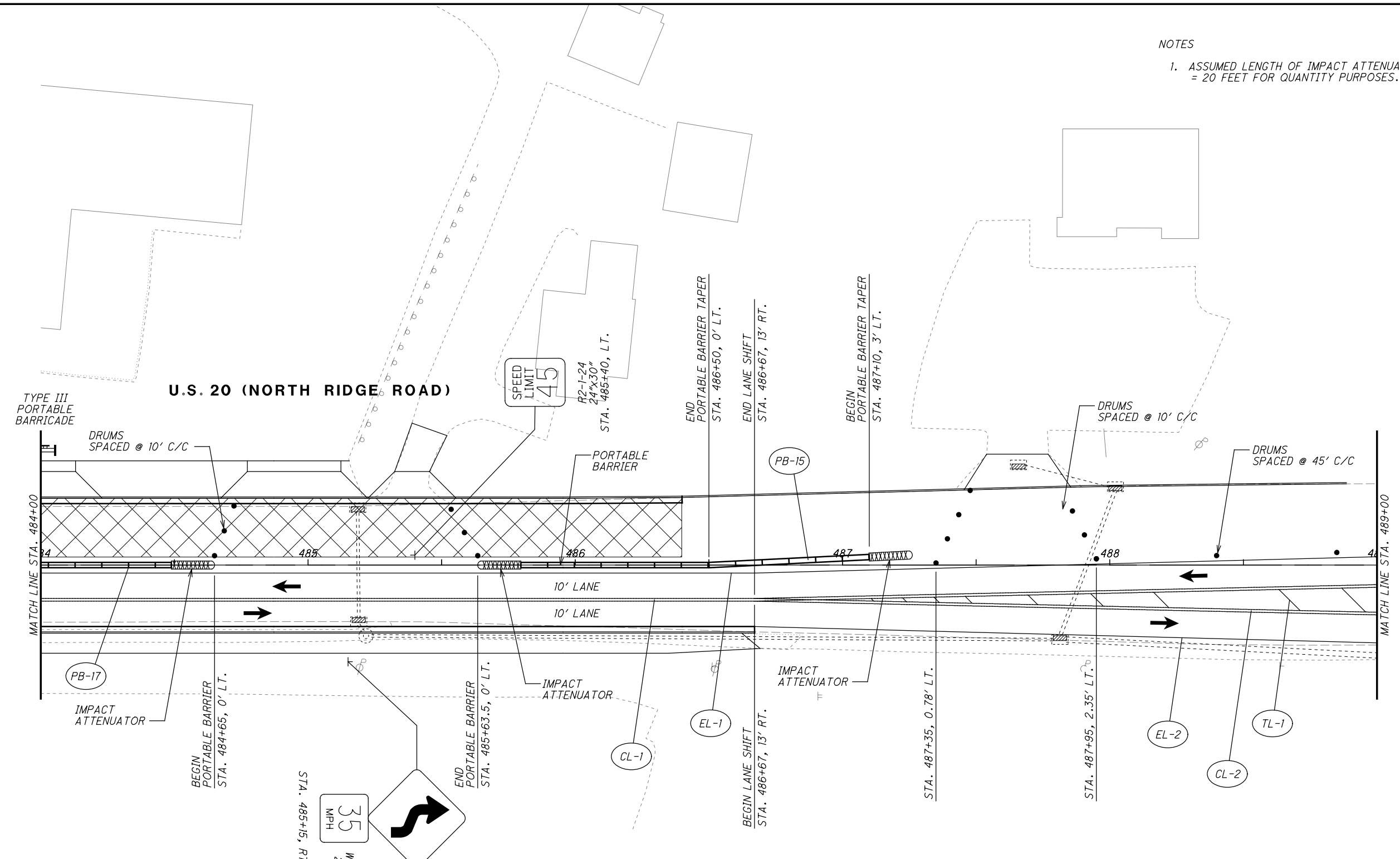
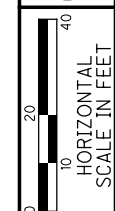
-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP276.dgn Sheet 7/15/2022 2:52:37 PM troyer

NOTES
1. ASSUMED LENGTH OF IMPACT ATTENUATOR = 20 FEET FOR QUANTITY PURPOSES.



LEGEND

MAINTENANCE OF TRAFFIC SIGN

MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE

EXISTING TRAFFIC SIGN

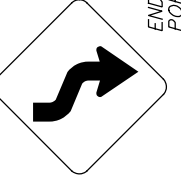
LEGEND

ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT	ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT	ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT	ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT	WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT	PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT	PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT	TRAFFIC FLOW

U.S. 20 (NORTH RIDGE ROAD)

SPEED LIMIT 45

35 MPH



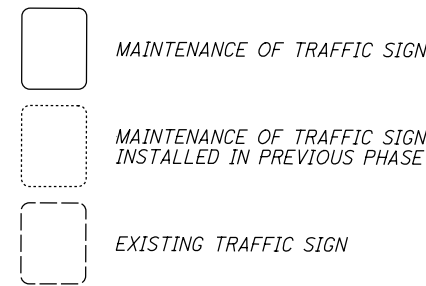
W13-1P-24 24'x24'

W1-41-36 36'x36'

- (CL) ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- (LL) ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- (EL) ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- (CH) ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- (LA) ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- (SL) ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- (CW) ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

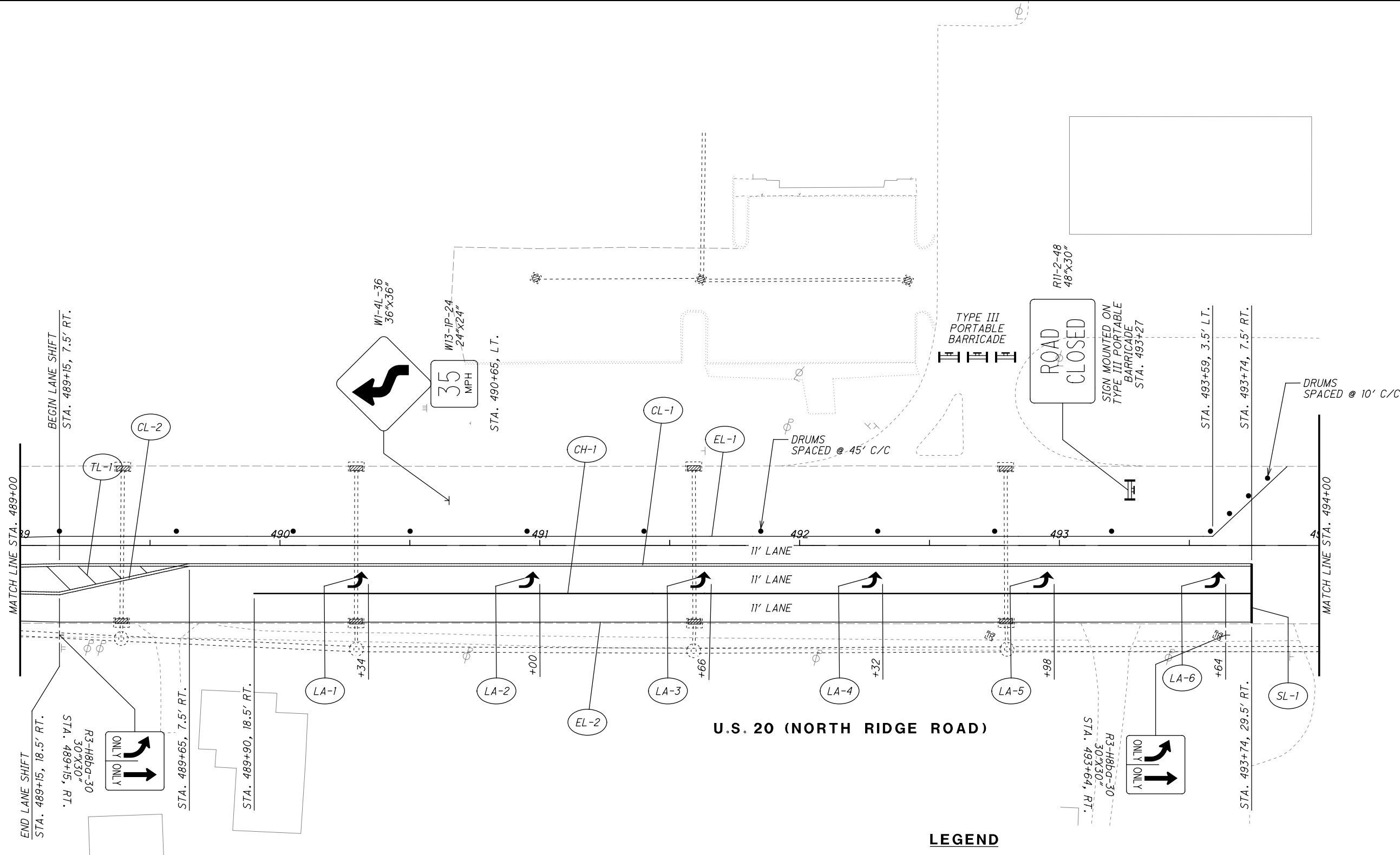
- (DL) ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- (TL) ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- (IM) ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- () WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP277.dgn Sheet 7/15/2022 2:52:38 PM troyer

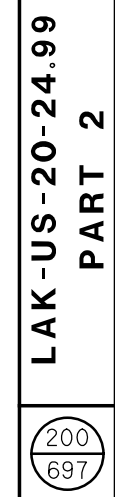


LEGEND

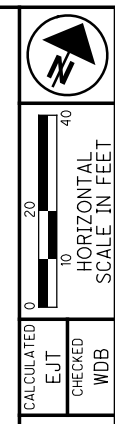
- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | (○) | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |



LAK-US-20-24.99
PART 2
MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 489+00 TO STA. 494+00



H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP278.dgn Sheet 7/15/2022 2:52:38 PM troyer

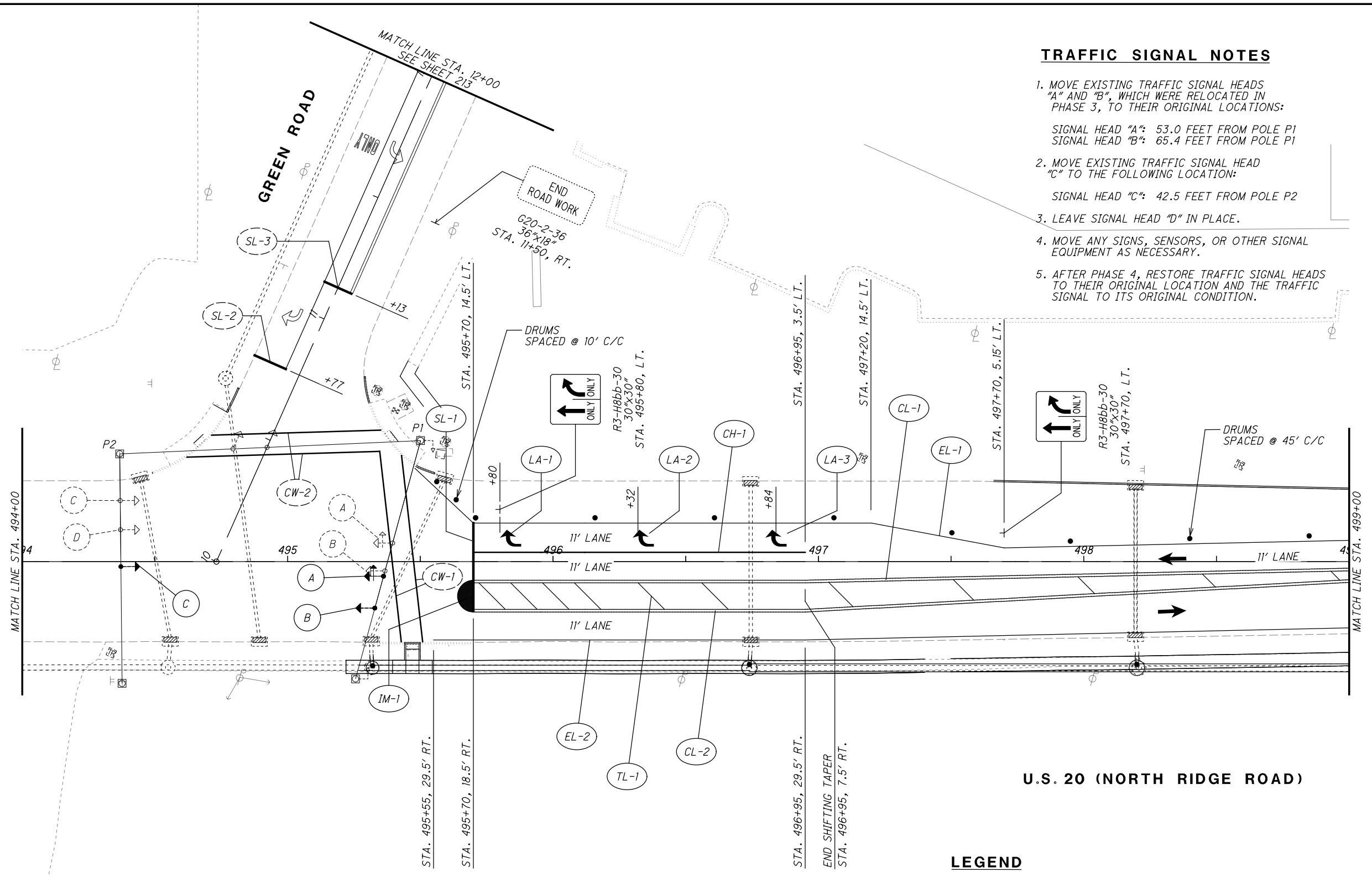


CALCULATED	EJT
CHECKED	WDB

201
697

TRAFFIC SIGNAL NOTES

1. MOVE EXISTING TRAFFIC SIGNAL HEADS "A" AND "B", WHICH WERE RELOCATED IN PHASE 3, TO THEIR ORIGINAL LOCATIONS:
 SIGNAL HEAD "A": 53.0 FEET FROM POLE P1
 SIGNAL HEAD "B": 65.4 FEET FROM POLE P1
2. MOVE EXISTING TRAFFIC SIGNAL HEAD "C" TO THE FOLLOWING LOCATION:
 SIGNAL HEAD "C": 42.5 FEET FROM POLE P2
3. LEAVE SIGNAL HEAD "D" IN PLACE.
4. MOVE ANY SIGNS, SENSORS, OR OTHER SIGNAL EQUIPMENT AS NECESSARY.
5. AFTER PHASE 4, RESTORE TRAFFIC SIGNAL HEADS TO THEIR ORIGINAL LOCATION AND THE TRAFFIC SIGNAL TO ITS ORIGINAL CONDITION.



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

LEGEND

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP279.dgn Sheet 7/15/2022 2:52:39 PM troyer

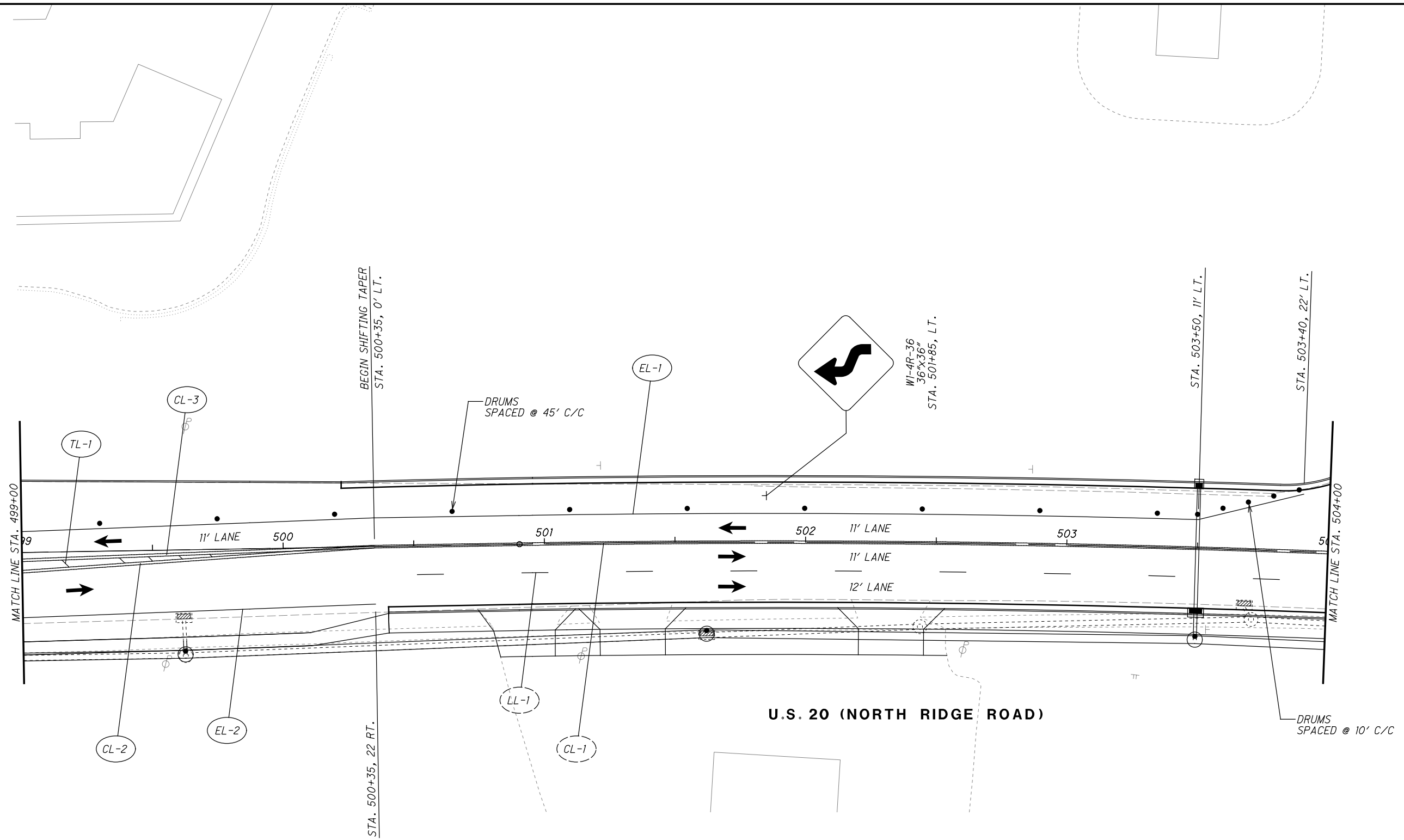
CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 499+00 TO STA. 504+00

LAK-US-20-24.99
PART 2

202
697



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP280.dgn Sheet 7/15/2022 2:52:39 PM troyer

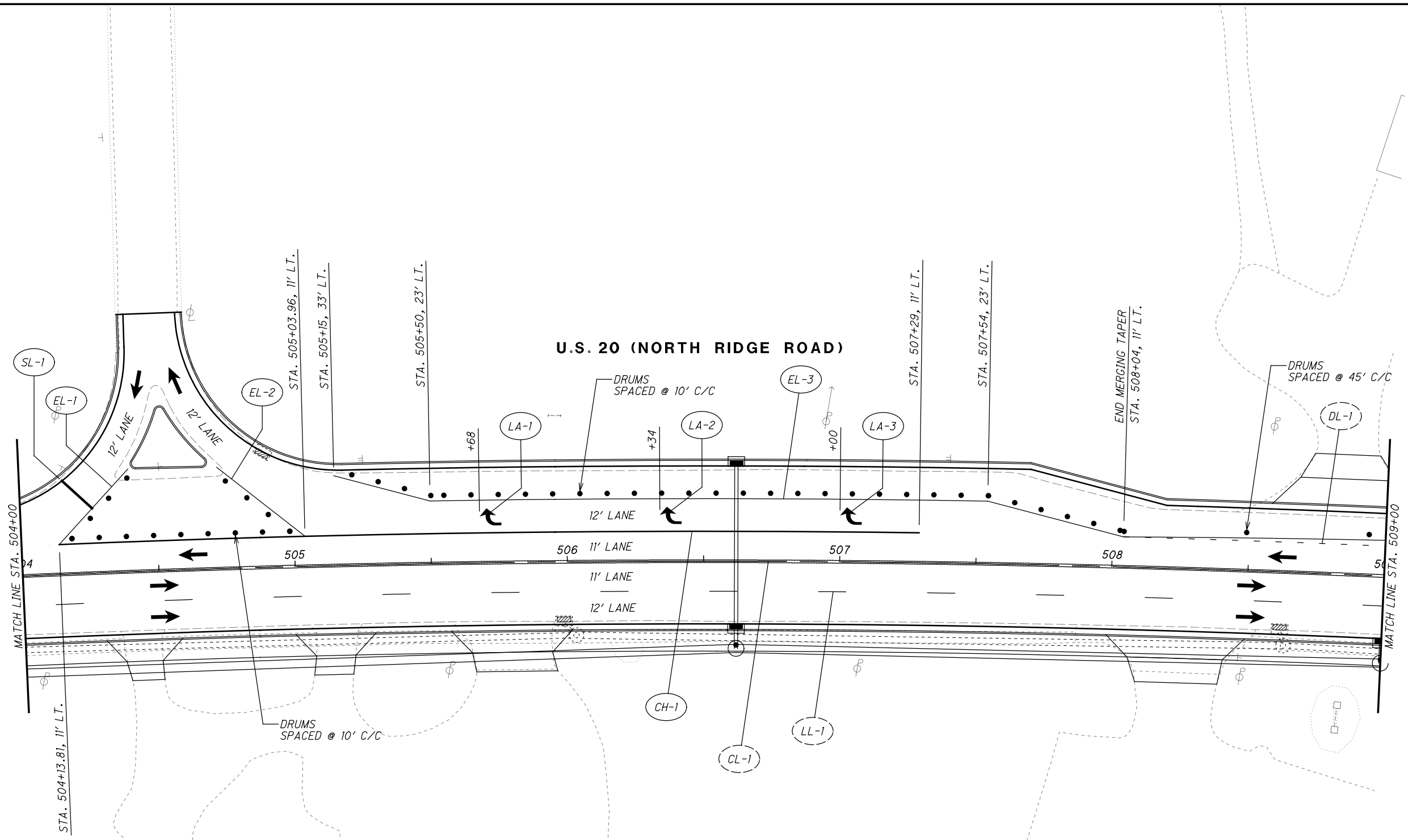
CALCULATED
EJT
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 504+00 TO STA. 509+00

LAK-US-20-24.99
PART 2

203
697



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP28.dgn Sheet 7/15/2022 2:52:40 PM troyer

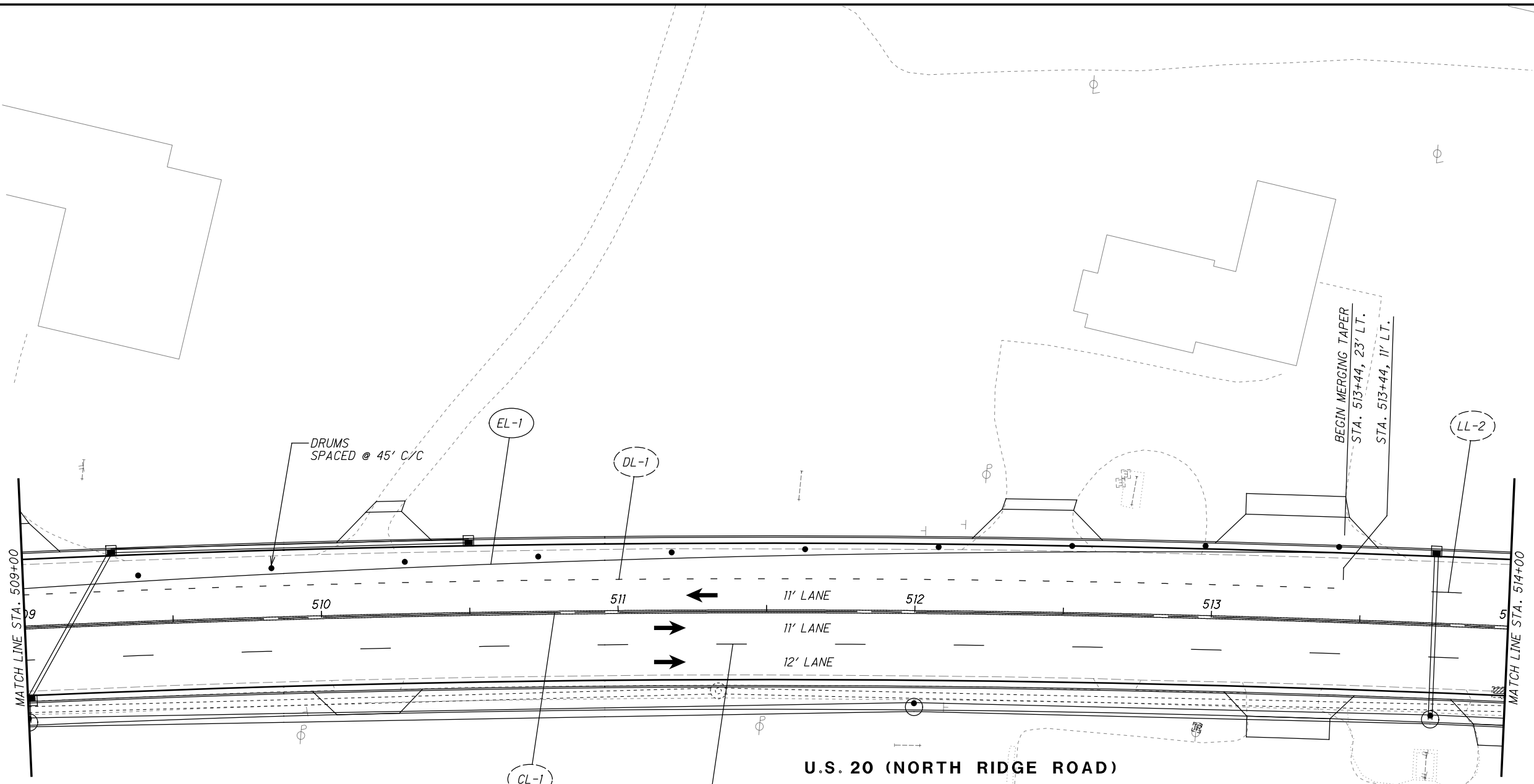
CALCULATED
EJT
CHECKED
WDB

0 10 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 509+00 TO STA. 514+00




LAK-US-20-24.99
PART 2

204
697












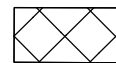
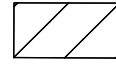



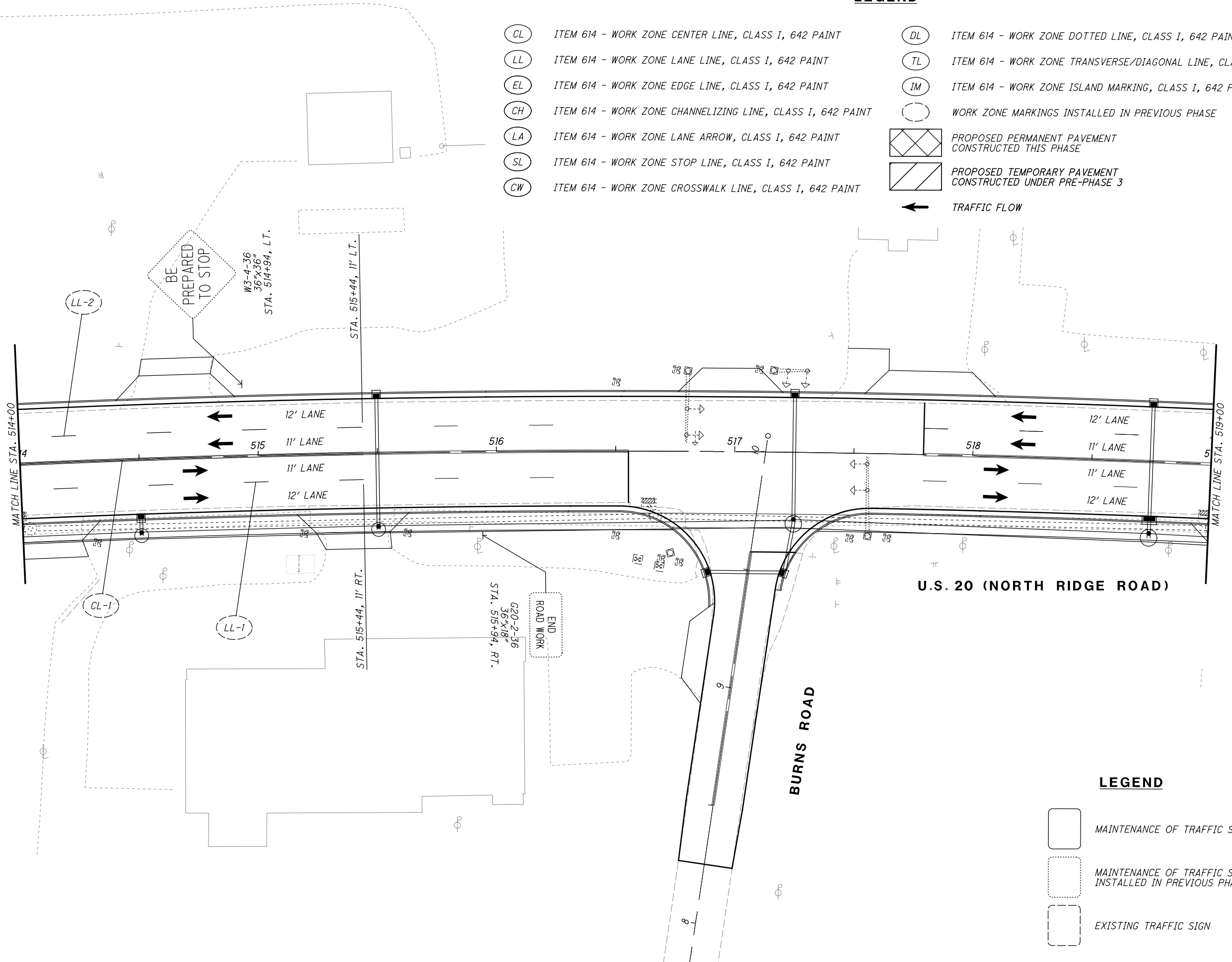
U.S. 20 (NORTH RIDGE ROAD)

LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

LEGEND

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW



LEGEND

- | | | | |
|------|------------------------------------------------------------|------|-------------------------------------------------------------------|
| (CL) | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | (DL) | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| (LL) | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | (TL) | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| (EL) | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | (IM) | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| (CH) | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | (○) | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| (LA) | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | (X) | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| (SL) | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | (/) | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
| (CW) | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | (←) | TRAFFIC FLOW |

LEGEND

- | | |
|-----|---------------------------------------------------------|
| (□) | MAINTENANCE OF TRAFFIC SIGN |
| (□) | MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE |
| (□) | EXISTING TRAFFIC SIGN |

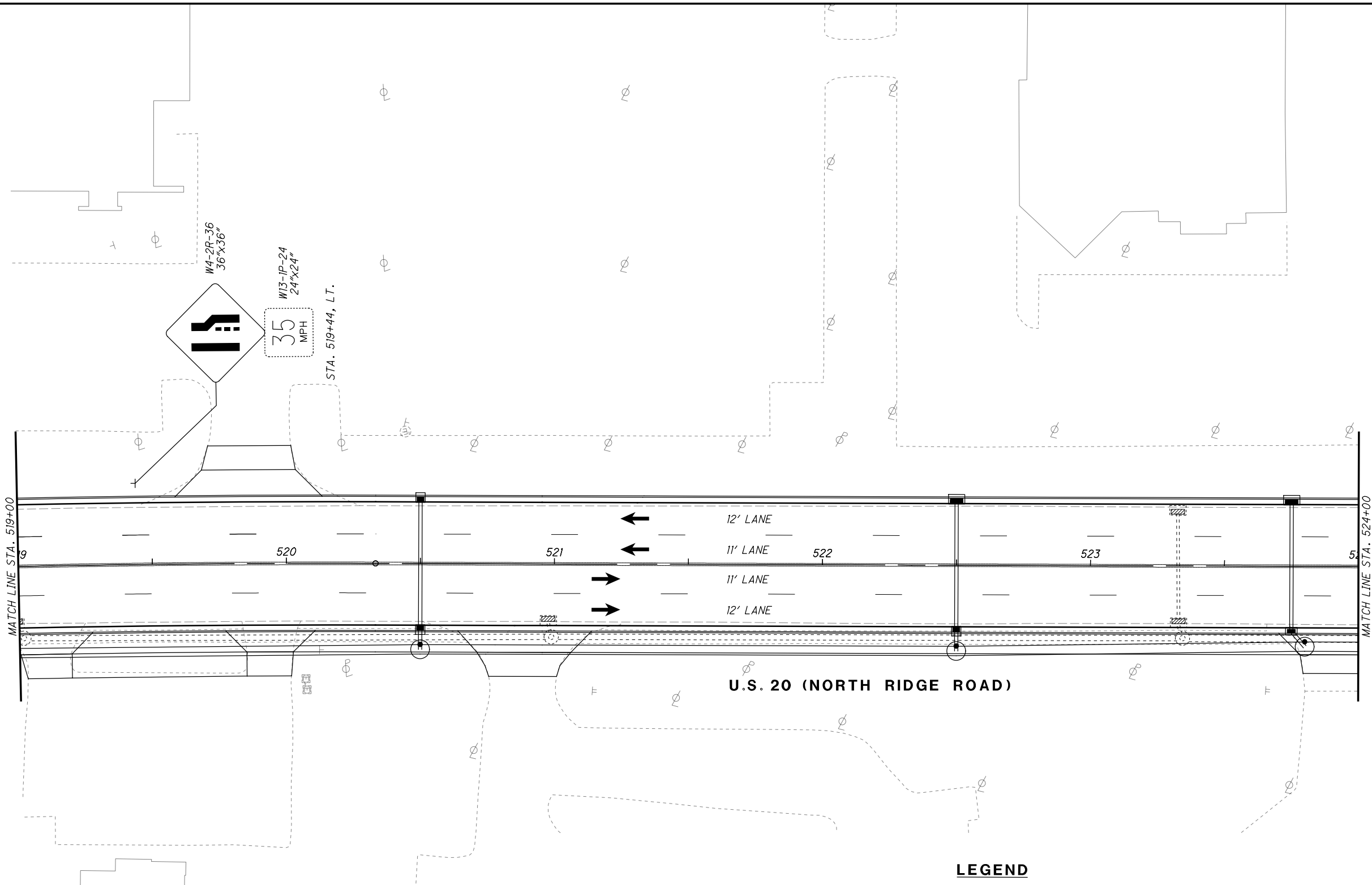


CALCULATED	EJT
CHECKED	WDB




MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 514+00 TO STA. 519+00








LAK-US-20-24.99
PART 2








H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP282A.dgn Sheet 7/15/2022 2:52:41PM troyer



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW



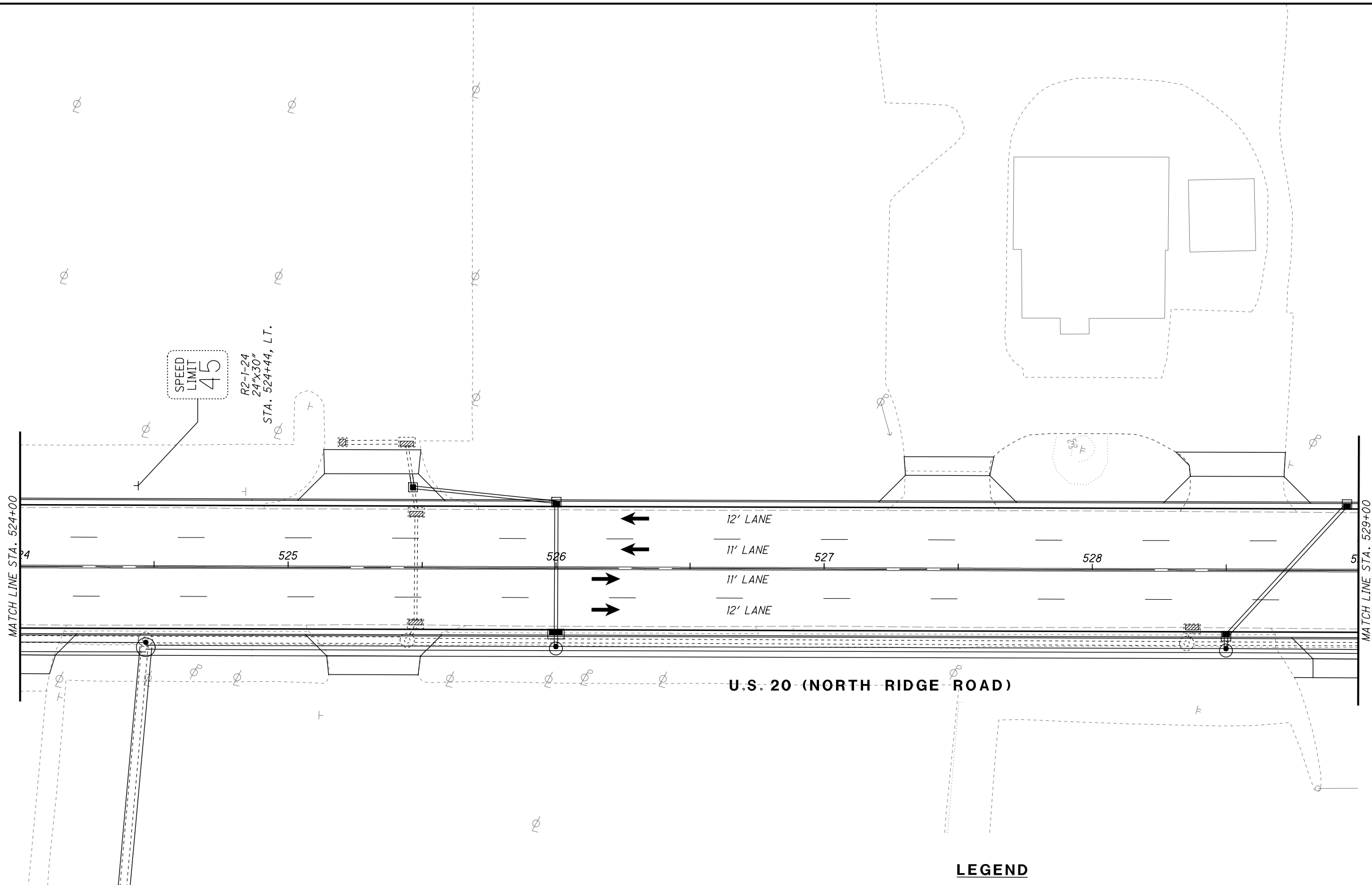
CALCULATED
EJT
CHECKED
WDB

**MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 519+00 TO STA. 524+00**




**LAK-US-20-24.99
PART 2**








206
697








H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP282B.dgn Sheet 7/15/2022 2:52:42 PM Troyer



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW



CALCULATED
EJT
CHECKED
WDB

**MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 524+00 TO STA. 529+00**

**LAK-US-20-24.99
PART 2**

207
697

H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP282C.dgn Sheet 7/15/2022 2:52:42 PM troyer

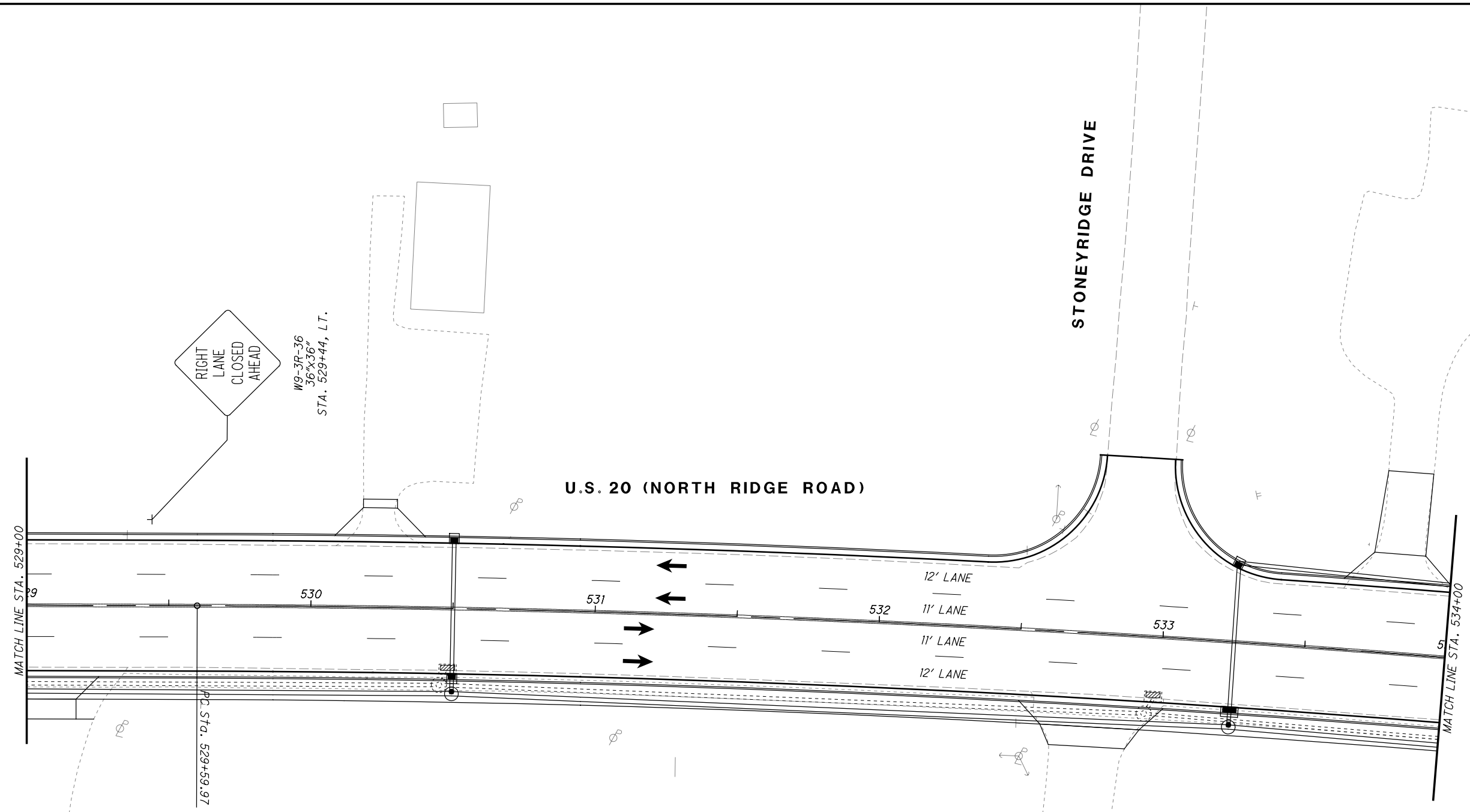
CALCULATED
EJT
CHECKED
WDB

0 10 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 529+00 TO STA. 534+00

LAK-US-20-24.99
PART 2

208
697



LEGEND

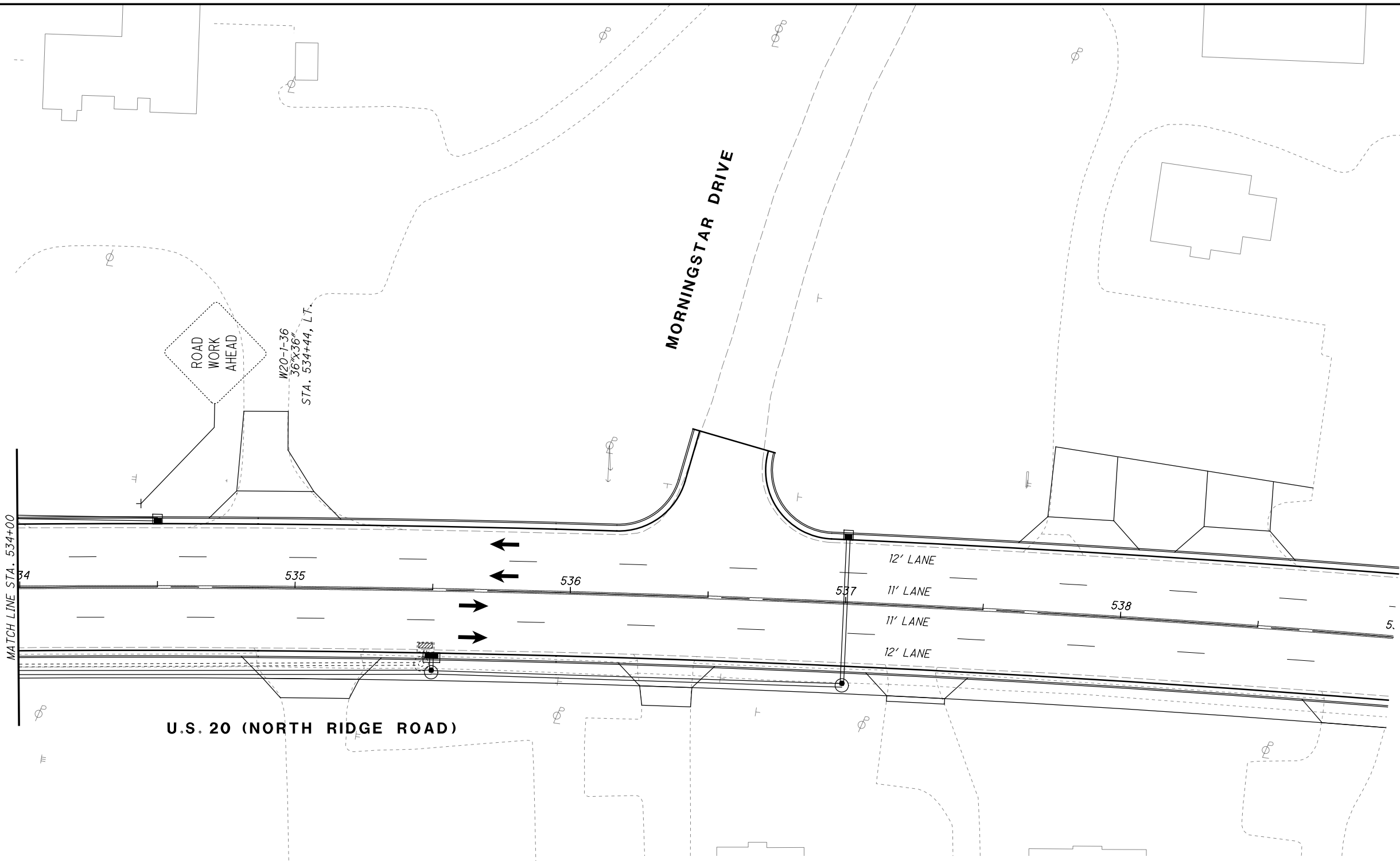
- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

- ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT




LEGEND








- ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
- ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
- WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
- PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
- TRAFFIC FLOW

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP283.dgn Sheet 7/15/2022 2:52:43 PM troyer






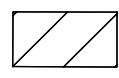



LEGEND

-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

-  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT

LEGEND

-  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
-  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
-  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
-  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
-  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
-  TRAFFIC FLOW

CALCULATED
EJT
CHECKED
WDB




HORIZONTAL SCALE IN FEET


MAINTENANCE OF TRAFFIC PLAN - PHASE 4
STA. 534+00 TO STA. 539+00

LAK-US-20-24.99
PART 2

209
697

H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\M01\Sheets\09270_MP283A.dgn Sheet 7/15/2022 2:52:44 PM troyer





 HORIZONTAL SCALE IN FEET

CALCULATED	EJT
CHECKED	WDB




MAINTENANCE OF TRAFFIC PLAN - PHASE 4
TOWNLINE ROAD












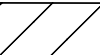


LAK-US-20-24.99
PART 2

210
697

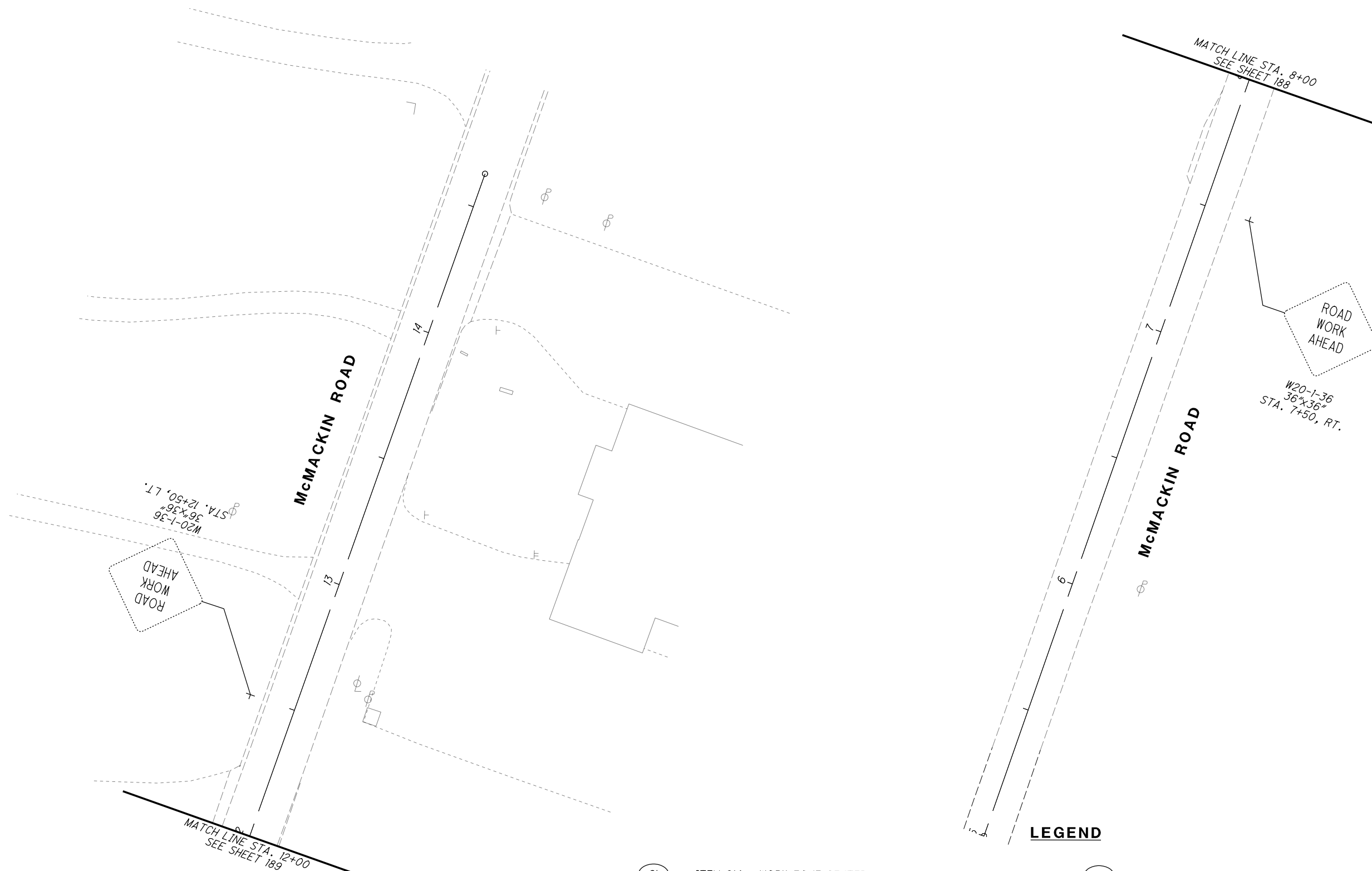


LEGEND




-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |















LEGEND

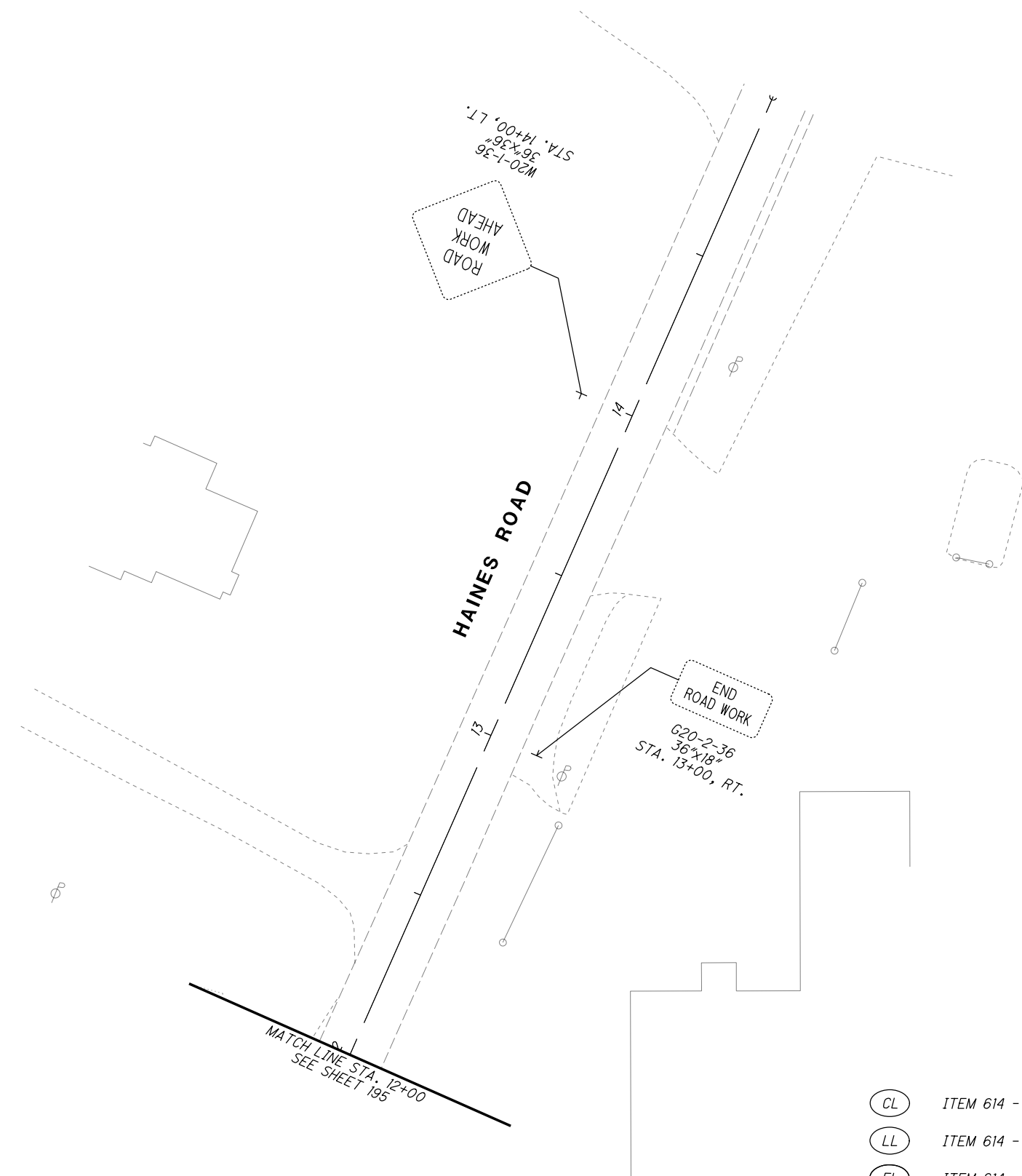


LEGEND




-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

LEGEND

 ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT	 ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT
 ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT	 ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT
 ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT	 ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT
 ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT	 WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE
 ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT	 PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
 ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT	 PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3
 ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT	 TRAFFIC FLOW



LEGEND












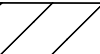


-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN

CALCULATED EJT
CHECKED WDB

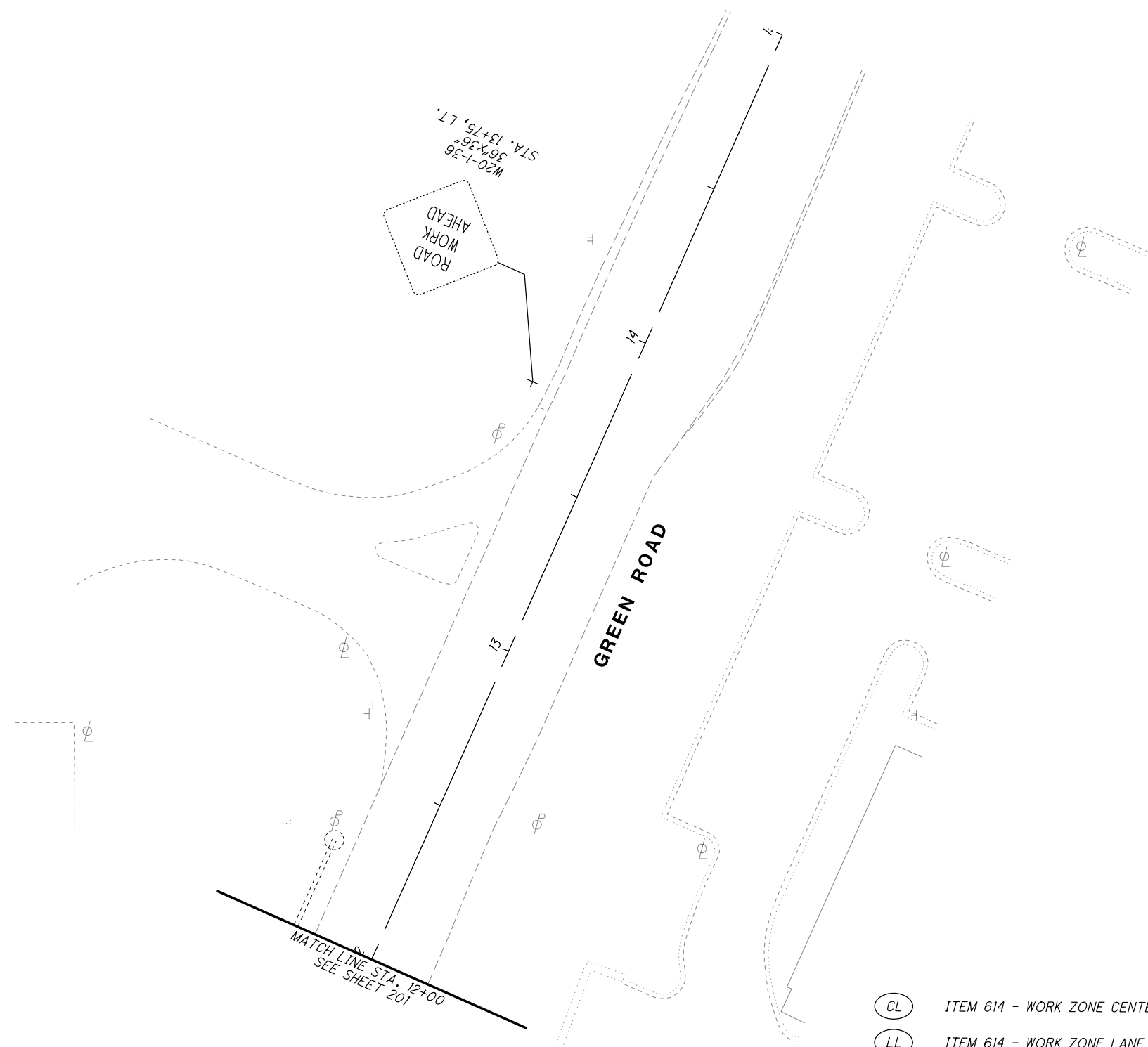
0 20 40
HORIZONTAL SCALE IN FEET

**MAINTENANCE OF TRAFFIC PLAN - PHASE 4
HAINES ROAD**




LEGEND

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

**LAK-US-20-24.99
PART 2**



LEGEND












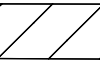


-  MAINTENANCE OF TRAFFIC SIGN
-  MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
-  EXISTING TRAFFIC SIGN



CALCULATED	EJT
CHECKED	WDB

LAK-US-20-24.99 PART 2 MAINTENANCE OF TRAFFIC PLAN - PHASE 4 GREEN ROAD

LEGEND

- | | |
|--------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------|
|  ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT |  ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
|  ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT |  WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
|  ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT |  PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
|  ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT |  PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
|  ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT |  TRAFFIC FLOW |

LAK-US-20-24.99 PART 2

NOTE:
 THE CONTRACTOR SHALL BE ALLOWED TO CLOSE HAINES ROAD AT STA. 463+33, RT. FOR FOURTEEN (14) CONSECUTIVE CALENDAR DAYS. LIQUIDATED DAMAGES SHALL BE ASSESSED IN THE AMOUNT OF \$1,500 FOR EACH CALENDAR DAY THAT HAINES ROAD REMAINS CLOSED TO TRAFFIC BEYOND THIS SPECIFIED TIME LIMIT.

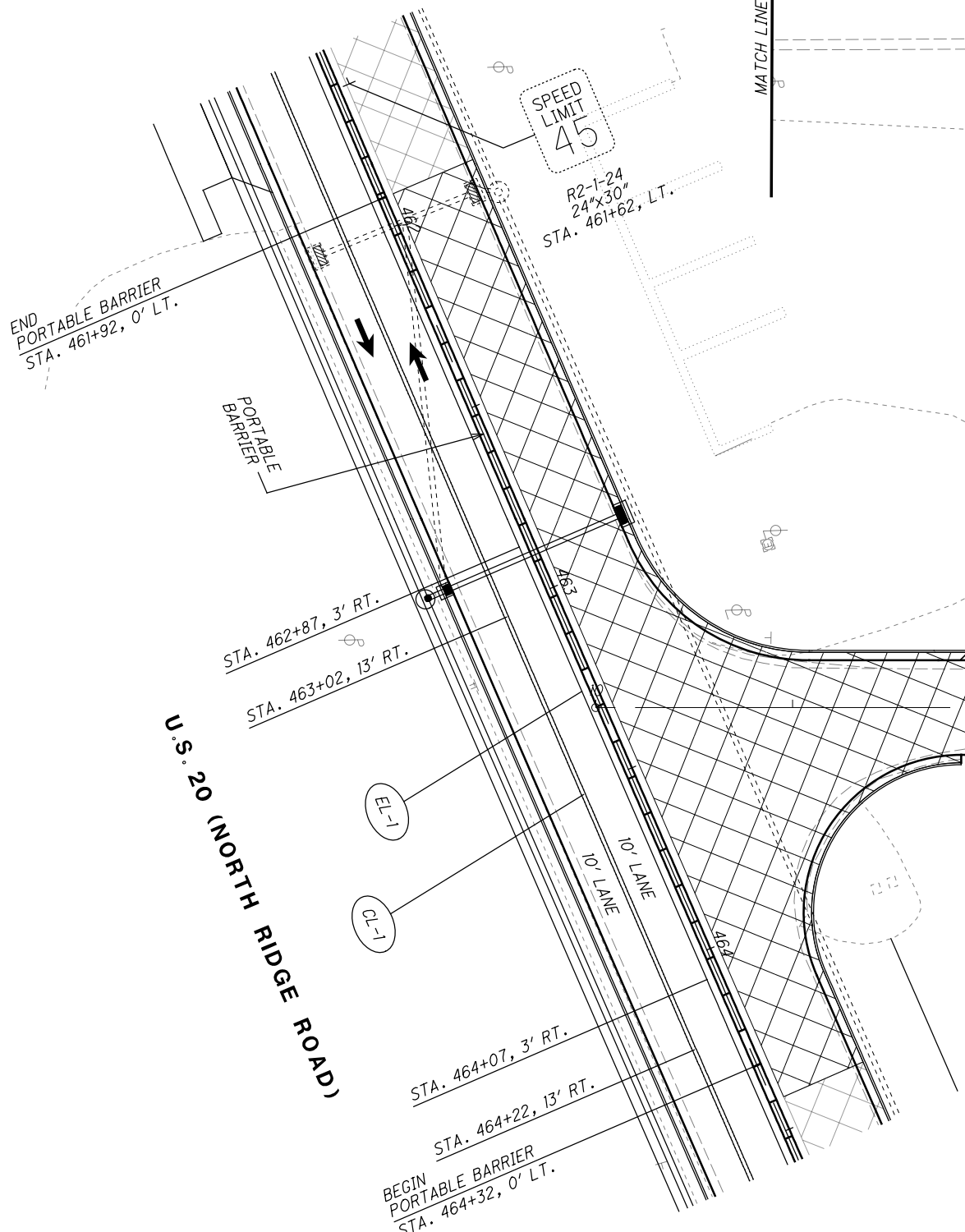
THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE TWO (2) ADJACENT SIDE STREETS AT THE SAME TIME WITHIN THIS SECTION OF THE PROJECT.

THE FOLLOWING ADJACENT SIDE STREETS WILL REMAIN OPEN DURING THE CLOSURE OF THIS SIDE STREET.

McMACKIN ROAD STA. 433+80
 GREEN ROAD STA. 494+73

FOR DETOUR ROUTE SEE SHEET 27.

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\MOT\Sheets\09270_MP2221.dgn Sheet 7/15/2022 2:52:46 PM Troyer



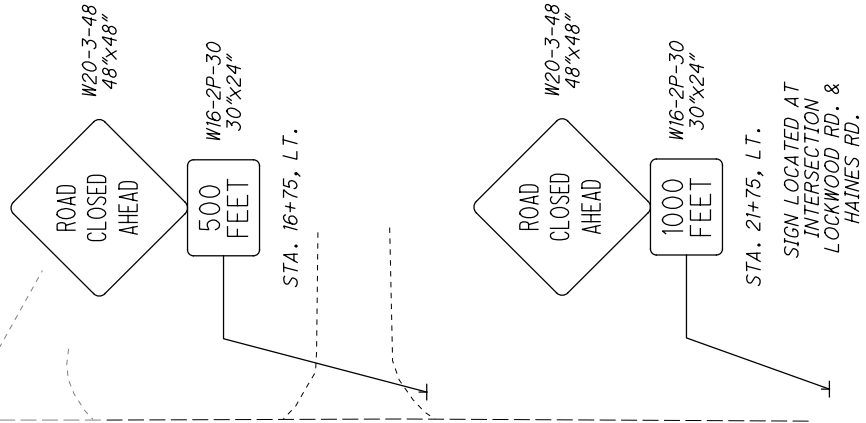
NOTE:
 FOR DETAILS OF ADVANCED WARNING SIGNS ON HAINES ROAD SEE SCD MT-101.60.

MATCH LINE STA. 14+25

MATCH LINE STA. 14+25

HAINES ROAD

HAINES ROAD



LEGEND

- MAINTENANCE OF TRAFFIC SIGN
- MAINTENANCE OF TRAFFIC SIGN INSTALLED IN PREVIOUS PHASE
- EXISTING TRAFFIC SIGN

LEGEND

- | | | | |
|----|------------------------------------------------------------|----|-------------------------------------------------------------------|
| CL | ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT | DL | ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 642 PAINT |
| LL | ITEM 614 - WORK ZONE LANE LINE, CLASS I, 642 PAINT | TL | ITEM 614 - WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT |
| EL | ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 642 PAINT | IM | ITEM 614 - WORK ZONE ISLAND MARKING, CLASS I, 642 PAINT |
| CH | ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT | | WORK ZONE MARKINGS INSTALLED IN PREVIOUS PHASE |
| LA | ITEM 614 - WORK ZONE LANE ARROW, CLASS I, 642 PAINT | | PROPOSED PERMANENT PAVEMENT CONSTRUCTED THIS PHASE |
| SL | ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT | | PROPOSED TEMPORARY PAVEMENT CONSTRUCTED UNDER PRE-PHASE 3 |
| CW | ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 642 PAINT | | TRAFFIC FLOW |



MAINTENANCE OF TRAFFIC PLAN - PHASE 4
HAINES ROAD

LAK-US-20-24.99
PART 2

214
697

H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CG001.dgn Sheet 7/15/2022 2:52:46 PM troyer

SHEET NUM.													PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE	CALCULATED	EUT	CHECKED	WDB
16	17	18	19		225	226	229		233	236	240	425	607	01/NHS/ PV	EXT	TOTAL			NO.				
LS														LS	201	11000	LS						
					2									2	202	20010	2	EACH					
						63,867				79,353		11,397		90,750	202	23001	90,750	SY				19	
					40									63,867	202	30000	63,867	SF					
					75									40	202	30700	40	FT					
														75	202	30800	75	SY					
						35				2,040				2,075	202	32000	2,075	FT					
					12,980									12,980	202	35100	12,980	FT					
					834									834	202	35200	834	FT					
					592									592	202	38000	592	FT					
					29									29	202	53100	29	EACH					
					54									54	202	58000	54	EACH					
					84									84	202	58300	84	EACH					
					1									1	202	60010	1	EACH					
					255									255	SPECIAL	20270000	255	FT				18	
														1,300	SPECIAL	20270110	1,300	FT				19	
			1,300											750	SPECIAL	20270120	750	FT				19	
			750											2,110	202	75000	2,110	FT					
					2,110																		
					1									1	202	98100	1	EACH				17	
					33									33	202	98100	33	EACH				17	
					23									23	202	98100	23	EACH				17	
					8									8	202	98100	8	EACH				17	
									12,483					13,152	203	10000	13,152	CY					
									1,439					1,439	203	20000	1,439	CY					
										81,438				89,977	204	10000	89,977	SY					
1,600														1,600	204	13000	1,600	CY					
1,600														1,600	204	30010	1,600	CY					
														30	204	45000	30	HOUR					
														3,200	204	50000	3,200	SY					
														3,200	204	51000	3,200	SY					
														200	606	15050	200	FT					
														3	606	26550	3	EACH					
														94,151	608	10000	94,151	SF					
														2,260	608	52000	2,260	SF					
														20	623	38500	20	EACH					
														4	626	00110	4	EACH					
														29	SPECIAL	69050000	29	EACH					17
														3,800	SPECIAL	69065016	3,800	TON					18
														1,900	SPECIAL	69065022	1,900	GAL					18
														1,900	SPECIAL	69065024	1,900	GAL					18

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_G0004.dgn Sheet 7/15/2022 2:52:59 PM troyer

SHEET NUM.												PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.	
						582	584	589	590	592	595	01/NHS/ PV		EXT	TOTAL				
TRAFFIC SIGNALS																			
						63	16	49		102	84		314	625	25400	314	FT	CONDUIT, 2", 725.04	
								72					72	625	25500	72	FT	CONDUIT, 3", 725.04	
						61		150					211	625	25600	211	FT	CONDUIT, 4", 725.04	
						124	16	257		102	84		583	625	29000	583	FT	TRENCH	
						2		3			3		8	625	30700	8	EACH	PULL BOX, 725.08, 18"	
								1					1	625	30706	1	EACH	PULL BOX, 725.08, 24"	
								4					4	625	31510	4	EACH	PULL BOX REMOVED	
						4	2	7		8	6		27	625	32000	27	EACH	GROUND ROD	
								2	10		6		18	630	09102	18	EACH	SURFACE PREPARATION, NEW SUPPORT SECTION	
								2	10		6		18	630	09106	18	EACH	COATING, EPOXY INTERMEDIATE COAT, SUPPORT SECTION	
								2	10		6		18	630	09108	18	EACH	COATING, URETHANE TOP COAT, SUPPORT SECTION	
								2	10		6		18	630	09120	18	EACH	COATING, ORGANIC ZINC PRIME COAT, SUPPORT SECTION	
						3.8	1.9	3.8		7.5	5.6		22.6	630	80100	22.6	SF	SIGN, FLAT SHEET	
						2				8	2		12	630	87500	12	EACH	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL	
									8				8	632	05006	8	EACH	VEHICULAR SIGNAL HEAD, (LED), 3-SECTION, 12" LENS, 1-WAY, POLYCARBONATE , BLACK	
						4	2		4	8	6		24	632	20731	24	EACH	PEDESTRIAN SIGNAL HEAD (LED), TYPE D2, COUNTDOWN, AS PER PLAN	577
						4	2		4	8	6		24	632	20750	24	EACH	ACCESSIBLE PEDESTRIAN PUSHBUTTON	
									8				8	632	25000	8	EACH	COVERING OF VEHICULAR SIGNAL HEAD	
						4	2		4	8	6		24	632	25010	24	EACH	COVERING OF PEDESTRIAN SIGNAL HEAD	
									710				710	632	40400	710	FT	SIGNAL CABLE, 4 CONDUCTOR, NO. 14 AWG	
						472	175		618	1,656	738		3,659	632	40500	3,659	FT	SIGNAL CABLE, 5 CONDUCTOR, NO. 14 AWG	
									774				774	632	40700	774	FT	SIGNAL CABLE, 7 CONDUCTOR, NO. 14 AWG	
								3					3	632	64010	3	EACH	SIGNAL SUPPORT FOUNDATION	
						4	2	3		8	6		23	632	64020	23	EACH	PEDESTAL FOUNDATION	
						452	165		598	1,616	708		3,539	632	65200	3,539	FT	LOOP DETECTOR LEAD-IN CABLE	
									210				210	632	68100	210	FT	POWER CABLE, 1 CONDUCTOR, NO. 6 AWG	
									113				113	632	69800	113	FT	SERVICE CABLE, 3 CONDUCTOR, NO. 6 AWG	
									1				1	632	70000	1	EACH	POWER SERVICE	
									1				1	632	70300	1	EACH	CONDUIT RISER, 1-1/2" DIAMETER	
								1					1	632	71240	1	EACH	SIGNAL SUPPORT, TYPE TC-12.31 DESIGN 6 POLE, WITH MAST ARMS TC-81.22 DESIGN 12 AND DESIGN 2	
									1				1	632	72100	1	EACH	SIGNAL SUPPORT, TYPE TC-81.22, DESIGN 2	
									1				1	632	72110	1	EACH	SIGNAL SUPPORT, TYPE TC-81.22, DESIGN 4	
						4	2	3		8	6		23	632	89900	23	EACH	PEDESTAL, 8', TRANSFORMER BASE	
						4				16	8		28	632	90020	28	EACH	REMOVAL OF MISCELLANEOUS TRAFFIC SIGNAL ITEM, (PEDESTRIAN SIGNAL SYSTEM)	
								1					1	632	90100	1	EACH	REMOVAL OF TRAFFIC SIGNAL INSTALLATION	
						93				293			386	632	90500	386	FT	SIGNALIZATION, MISC.: UNLASH AND RELASH MESSENGER WIRE	577
													1	633	65523	1	EACH	CABINET, TYPE 332L, AS PER PLAN	577
													1	633	67100	1	EACH	CABINET FOUNDATION	
													1	633	67200	1	EACH	CONTROLLER WORK PAD	
													1	633	75001	1	EACH	UNINTERRUPTIBLE POWER SUPPLY (UPS), 1000 WATT, AS PER PLAN	579
						2	2		2	2	6		14	809	69001	14	EACH	ADVANCE RADAR DETECTION, AS PER PLAN	580
						3	3		4	4	10		24	809	69101	24	EACH	STOP LINE RADAR DETECTION, AS PER PLAN	580
								1					1	809	69123	1	EACH	ATC CONTROLLER, AS PER PLAN	577
									1				1	809	69200	1	EACH	EMERGENCY VEHICLE PREEMPTION	
									4				4	809	69210	4	EACH	PREEMPT RECEIVING UNIT	
									710				710	809	69220	710	FT	PREEMPT DETECTOR CABLE	
									1				1	809	69230	1	EACH	PREEMPT PHASE SELECTOR	
									4				4	809	69240	4	EACH	PREEMPT CONFIRMATION LIGHT	
									1				1	815	30000	1	EACH	SPREAD SPECTRUM RADIO	

GENERAL SUMMARY

LAK-US-20-24.99 PART 2

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CG005.dgn Sheet 7/15/2022 2:53:00 PM Troyer

SHEET NUM.											PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.	CALCULATED	EJTB	CHECKED	WDB
21	22	23			37	41	46	50			01/NHS/ PV		EXT	TOTAL							
		1,560										1,560	410	12000	1,560	CY	TRAFFIC COMPACTED SURFACE, TYPE A OR B				
							325					325	606	15051	325	FT	GUARDRAIL, TYPE MGS, AS PER PLAN				23
							1					1	606	26151	1	EACH	ANCHOR ASSEMBLY, MGS TYPE E, AS PER PLAN (MASH 2016)				23
							1					1	606	26551	1	EACH	ANCHOR ASSEMBLY, MGS TYPE T, AS PER PLAN				23
86,000												86,000	608	21200	86,000	SF	TEMPORARY ASPHALT CONCRETE WALK				
					461		734					1,195	611	04401	1,195	FT	12" CONDUIT, TYPE B, AS PER PLAN				22
					11		24					35	611	98371	35	EACH	CATCH BASIN, NO. 6, AS PER PLAN				22
					11		25					36	611	98635	36	EACH	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN				22
							10					10	611	99654	10	EACH	MANHOLE ADJUSTED TO GRADE				
	1,000											1,000	614	11110	1,000	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE				
20					12	12	32	36				20	SPECIAL	61411300	20	EACH	WORK ZONE TRAFFIC SIGNAL				20
												92	614	12384	92	EACH	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (BIDIRECTIONAL)				
		LS										LS	614	12420	LS		DETOUR SIGNING				
		300										300	614	13000	300	CY	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC				
					18	17	83	63				181	614	13310	181	EACH	BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL)				
							10					10	614	13312	10	EACH	BARRIER REFLECTOR, TYPE 2 (BIDIRECTIONAL)				
					18	17	88	63				186	614	13360	186	EACH	OBJECT MARKER, TWO WAY				
120												120	614	18601	120	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN				20
3.61					0.36	0.15	0.67	0.14				4.93	614	20100	4.93	MILE	WORK ZONE LANE LINE, CLASS I, 4", 642 PAINT				
6.19												6.19	614	20550	6.19	MILE	WORK ZONE LANE LINE, CLASS III, 4", 642 PAINT				
2.71					2.95	2.61	3.11	2.61				13.99	614	21100	13.99	MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT				
4.33												4.33	614	21550	4.33	MILE	WORK ZONE CENTER LINE, CLASS III, 642 PAINT				
1.33					4.73	3.34	4.13	2.97				16.5	614	22110	16.5	MILE	WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT				
2.13												2.13	614	22360	2.13	MILE	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT				
1,506					337	657	1,356	1,520				5,376	614	23200	5,376	FT	WORK ZONE CHANNELIZING LINE, CLASS I, 8", 642 PAINT				
2,016												2,016	614	23680	2,016	FT	WORK ZONE CHANNELIZING LINE, CLASS III, 8", 642 PAINT				
					1,018	300	990					2,308	614	24200	2,308	FT	WORK ZONE DOTTED LINE, CLASS I, 4", 642 PAINT				
648					251	177	501	536				2,113	614	25200	2,113	FT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I, 642 PAINT				
838												838	614	25620	838	FT	WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS III, 642 PAINT				
566					448	346	278	179				1,817	614	26200	1,817	FT	WORK ZONE STOP LINE, CLASS I, 642 PAINT				
669												669	614	26610	669	FT	WORK ZONE STOP LINE, CLASS III, 642 PAINT				
1,582					274		1,016					2,872	614	27050	2,872	FT	WORK ZONE CROSSWALK LINE, CLASS I, 12", 642 PAINT				
1,695												1,695	614	27250	1,695	FT	WORK ZONE CROSSWALK LINE, CLASS III, 12", 642 PAINT				
23					7	13	25	28				96	614	30200	96	EACH	WORK ZONE ARROW, CLASS I, 642 PAINT				
31												31	614	30650	31	EACH	WORK ZONE ARROW, CLASS III, 642 PAINT				
51					51	51	51	51				255	614	32700	255	SF	WORK ZONE ISLAND MARKING, CLASS I				
51												51	614	32800	51	SF	WORK ZONE ISLAND MARKING, CLASS III, 642 PAINT				
	65											65	614	40051	65	EACH	BUSINESS ENTRANCE SIGN, AS PER PLAN				21
LS												LS	615	10000	LS		ROADS FOR MAINTAINING TRAFFIC				
					6,720		8,647					15,367	615	20001	15,367	SY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN				22
550												550	616	10000	550	MGAL	WATER				
					1,186	1,650	6,179	4,713				13,728	622	41100	13,728	FT	PORTABLE BARRIER, UNANCHORED				
												LS	108	10000	LS		CPM PROGRESS SCHEDULE				
LS	LS	LS										LS	614	11000	LS		MAINTAINING TRAFFIC				
												24	619	16020	24	MNTH	FIELD OFFICE, TYPE C				
												LS	623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING				
												LS	624	10000	LS		MOBILIZATION				
												LS	SPECIAL	69098400	LS		PRECONSTRUCTION VIDEO DOCUMENTATION				16

GENERAL SUMMARY

**LAK-US-20-24.99
PART 2**

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CS002.dgn Sheet 7/5/2022 2:53:02 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION	SIDE	202	202	202	202	202	202	202	202	202	202	202	202	202	202	202	606	606	441	SPECIAL	SPECIAL	
				HEADWALL REMOVED EACH	PIPE REMOVED, 24" AND UNDER FT	PIPE REMOVED, OVER 24" FT	GUARDRAIL REMOVED FT	CONCRETE BARRIER REMOVED FT	MAILBOX REMOVED EACH	MANHOLE REMOVED EACH	CATCH BASIN OR INLET REMOVED EACH	MONUMENT ASSEMBLY REMOVED EACH	FENCE REMOVED FT	REMOVAL MISC.:SHED EACH	REMOVAL MISC.:BUSINESS SIGN EACH	REMOVAL MISC.:BOULDER EACH	REMOVAL MISC.:LIGHT POLE EACH	TRAFFIC ISLAND REMOVED SY	GUARDRAIL, TYPE MGS FT	ANCHOR ASSEMBLY, MGS TYPE T EACH	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (449), (UNDER GUARDRAIL), AS PER PLAN CY	FILL AND PLUG EXISTING CONDUIT, 12" FT	MAILBOX SUPPORT SYSTEM, SINGLE EACH	
R51	247	NOT USED																						
R52	247		TO																					
R53	247				439+59.38	LT																		
R54	247				439+60.19	RT																		
R55	247				439+60.19	RT																		
R56	247				439+98.76	RT																		
R57	247	NOT USED																						
R58	247				440+90.97	LT																		
R59	247				441+99.13	LT																		
R60	247				441+99.81	RT																		
R61	247	442+03.46			444+00.00	RT																		
R62	247				443+82.44	RT																		
R63	248				444+39.21	LT																		
R64	248	443+76.78			444+38.00	RT																		
R65	248				444+39.39	RT																		
R66	248	NOT USED																						
R67	248				446+48.92	LT																		
R68	248	444+37.91			446+47.14	RT																		
R69	248				446+50.13	RT																		
R70	248	NOT USED																						
R71	248				448+99.40	LT																		
R72	248				448+99.43	RT																		
R73	248	446+47.14			448+97.09	RT																		
R74	249	NOT USED																						
R75	249				451+49.33	LT																		
R76	249				451+49.18	RT																		
R77	249	449+00.00			451+47.50	RT																		
R78	249	NOT USED																						
R79	250	NOT USED																						
R80	250				454+79.70	RT																		
R81	250	NOT USED																						
R82	250	456+95.49			459+00.00	LT																		
R83	250				456+94.23	LT																		
R84	250				456+94.46	RT																		
R85	250				457+36.81	LT																		
R86	250				458+12.77	RT																		
R87	251	459+00.00			459+50.43	LT																		
R88	251				459+48.88	LT																		
R89	251				459+49.29	RT																		
R90	251	460+28.15			460+66.55	LT																		
R91	251	462+02.38			464+00.00	LT																		
R92	251				462+00.04	LT																		
R93	251				461+99.42	RT																		
R94	251	NOT USED																						
R95	251	NOT USED																						
R96	251	460+08.32			463+12.54	LT																		
R97	252	NOT USED																						
R98	252				4647+66.45	LT																		
R99	252				464+74.45	LT																		
R100	252				464+74.52	RT																		
R101	252	464+00.00			469+09.78	LT																		
R102	252	NOT USED																						
R103	252	467+52.93			469+00.00	LT																		
SUBTOTALS CARRIED TO SHEET 225																								
					3034			40		4	11	19			757		2	1					4	

ROADWAY SUBSUMMARY

**LAK-US-20-24.99
PART 2**

CALCULATED
LIME
CHECKED
JMP

221
697

H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CS003.dgn Sheet 7/5/2022 2:53:03 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION	SIDE	202	202	202	202	202	202	202	202	202	202	202	202	202	202	202	606	606	441	SPECIAL	SPECIAL			
				HEADWALL REMOVED EACH	PIPE REMOVED, 24" AND UNDER FT	PIPE REMOVED, OVER 24" FT	GUARDRAIL REMOVED FT	CONCRETE BARRIER REMOVED FT	MAILBOX REMOVED EACH	MANHOLE REMOVED EACH	CATCH BASIN OR INLET REMOVED EACH	MONUMENT ASSEMBLY REMOVED EACH	FENCE REMOVED FT	REMOVAL MISC.:SHED EACH	REMOVAL MISC.:BUSINESS SIGN EACH	REMOVAL MISC.:BOULDER EACH	REMOVAL MISC.:LIGHT POLE EACH	TRAFFIC ISLAND REMOVED SY	GUARDRAIL, TYPE MGS FT	ANCHOR ASSEMBLY, MGS TYPE T EACH	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (449), (UNDER GUARDRAIL), AS PER PLAN CY	FILL AND PLUG EXISTING CONDUIT, 12" FT	MAILBOX SUPPORT SYSTEM, SINGLE EACH			
		TO																								
R104	252	467+49.86	LT		4																					
R105	252	467+49.47	RT		40																					
R106	253	NOT USED																								
R107	253	NOT USED																								
R108	253	472+52.10	LT		346						1															
R109	253	469+99.20	LT		4																					
R110	253	469+99.41	RT		40																					
R111	253	470+21.65	LT																							
R112	253	471+17.58	RT																							
R113	253	471+81.24	LT																							
R114	253	472+52.10	LT		146																					
R115	253	471+49.35	LT		3																					
R116	253	472+49.32	RT		41																					
R117	254	NOT USED																								
R118	254	474+65.47	LT																							
R119	254	474+00.00	LT		193																					
R120	254	474+99.50	LT		3																					
R121	254	474+99.23	RT		40																					
R122	254	475+70.80	LT																							
R123	254	475+98.70	LT	1	551																					
R124	254	477+70.41	LT		126																					
R125	254	477+73.87	LT		3																					
R126	254	477+74.11	RT		40																					
R127	254	477+80.04	LT																							
R128	254	477+27.75	RT																							
R129	254	478+55.11	LT																							
R130	255	479+11.84	LT																							
R131	255	NOT USED																								
R132	255	NOT USED																								
R133	255	479+00.00	LT		143																					
R134	255	480+48.97	LT		4																					
R135	255	480+48+75.00	RT		40																					
R136	255	480+60.21	LT																							
R137	255	479+00.00	RT																							
R138	256	NOT USED																								
R139	256	484+00.00	RT																							
R140	256	485+18.58	LT/RT		48																					
R141	256	485+55.34	LT																							
R142	256	NOT USED																								
R143	256	487+98.21	LT																							
R293	256	485+21.68	RT																							
R144	257	NOT USED																								
R145	258	NOT USED																								
R146	258	495+31.65	RT		143																					
R147	258	496+74.00	RT		143																					
R148	258	498+20.00	RT		77																					
R149	259	NOT USED																								
R150	259	499+00.00	RT		255																					
R151	259	NOT USED																								
R152	259	501+62.00	RT		277																					
R153	259	502+44.10	RT		123																					
SUBTOTALS CARRIED TO SHEET 225					1	2833					8	12	15			927						3			255	8

ROADWAY SUBSUMMARY

LAK-US-20-24.99 PART 2

CALCULATED
LIME
CHECKED
JMP

222
697

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CS004.dgn Sheet 7/15/2022 2:53:03 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION		SIDE	202	202	202	202	202	202	202	202	202	202	202	202	202	202	606	606	441	SPECIAL	SPECIAL	
					HEADWALL REMOVED EACH	PIPE REMOVED, 24" AND UNDER FT	PIPE REMOVED, OVER 24" FT	GUARDRAIL REMOVED FT	CONCRETE BARRIER REMOVED FT	MAILBOX REMOVED EACH	MANHOLE REMOVED EACH	CATCH BASIN OR INLET REMOVED EACH	MONUMENT ASSEMBLY REMOVED EACH	FENCE REMOVED FT	REMOVAL MISC.:SHED EACH	REMOVAL MISC.:BUSINESS SIGN EACH	REMOVAL MISC.:BOULDER EACH	REMOVAL MISC.:LIGHT POLE EACH	TRAFFIC ISLAND REMOVED SY	GUARDRAIL, TYPE MGS FT	ANCHOR ASSEMBLY, MGS TYPE T EACH	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (449); (UNDER GUARDRAIL), AS PER PLAN CY	FILL AND PLUG EXISTING CONDUIT, 12" FT	MAILBOX SUPPORT SYSTEM, SINGLE EACH
R154	259		TO	503+68.53		3																		
R155	259	503+71.41		504+00.00		149				1														
R156	259	NOT USED																						
R157	260	504+35.11		504+74.55																				
R158	260			504+66.00						1													75	
R159	260			504+89.20																				
R160	260			505+98.62		4																		
R161	260	504+00.00		506+03.19		200																		
R162	260	NOT USED																						
R163	260			508+62.36		3																		
R164	260	506+03.19		509+00.00		289				1														
R165	261	NOT USED																						
R166	261	509+00.00		514+00.00		495																		
R167	261			511+61.32																				
R168	261	512+59.10		512+83.96																				
R169	261			512+73.08																				
R170	261	513+50.43		513+59.23																				
R171	261	513+54.62		513+88.82																				
R172	261			513+74.08																				
R173	261			513+98.63		4																		
R174	261	NOT USED																						
R175	262	NOT USED																						
R176	262	514+03.18		516+66.93		258																		
R177	262			514+84.21																				
R178	262			515+16.08																				
R179	262			516+63.97		4																		
R180	262	516+66.93		519+00.00		230																		
R181	262	NOT USED																						
R182	262			518+98.71		3																		
R183	263	NOT USED																						
R184	263	519+01.57		520+99.45		193																		
R185	263			520+07.66																				
R186	263			520+97.59		3																		
R187	263	520+99.45		523+34.41		232																		
R188	263	NOT USED																						
R189	263			523+32.54		40																		
R190	263	523+32.94		523+32.94		5																		
R191	263	523+32.94		524+00.00																				
R192	264	NOT USED																						
R193	264			524+14.83																				
R194	264	524+00.00		525+44.53		1																		
R195	264			524+47.53																				
R196	264			524+80.89																				
R197	264	NOT USED																						
R198	264	NOT USED																						
R199	264			525+47.60		58																		
R200	264	525+44.53		528+35.34		287																		
R201	264			525+60.41																				
R202	264			525+97.21																				
R203	264			526+39.97																				
R204	264	NOT USED																						
R205	264			527+95.70																				
SUBTOTALS CARRIED TO SHEET 225						1	2460	834					2	11	12									

ROADWAY SUBSUMMARY

**LAK-US-20-24.99
PART 2**

CALCULATED
LIME
CHECKED
JMP

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CS005.dgn Sheet 7/15/2022 2:53:04 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION		SIDE	202	202	202	202	202	202	202	202	202	202	202	202	202	202	606	606	441	SPECIAL	SPECIAL
					HEADWALL REMOVED EACH	PIPE REMOVED, 24" AND UNDER FT	PIPE REMOVED, OVER 24" FT	GUARDRAIL REMOVED FT	CONCRETE BARRIER REMOVED FT	MAILBOX REMOVED EACH	MANHOLE REMOVED EACH	CATCH BASIN OR INLET REMOVED EACH	MONUMENT ASSEMBLY REMOVED EACH	FENCE REMOVED FT	REMOVAL MISC.:SHED EACH	REMOVAL MISC.:BUSINESS SIGN EACH	REMOVAL MISC.:BOULDER EACH	REMOVAL MISC.:LIGHT POLE EACH	TRAFFIC ISLAND REMOVED SY	GUARDRAIL, TYPE MGS FT	ANCHOR ASSEMBLY, MGS TYPE T EACH	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (449), (UNDER GUARDRAIL), AS PER PLAN CY	FILL AND PLUG EXISTING CONDUIT, 12" FT
R206	264	527+53.41	TO	528+67.30	RT										120								
R207	264			528+35.34	RT		63			1													
R208	264			528+36.98	RT		3				1												
R211	265	NOT USED																					
R212	265			530+48.47	RT		3					1											
R213	265			530+45.51	RT		143			1													
R214	265	530+73.02		532+36.22	LT								165										
R215	265			531+29.93	RT										1								
R216	265			532+67.58	LT											1							
R217	265			532+97.40	RT		4					1											
R218	265	530+45.51		534+00.00	RT		347			1													
R219	265			533+69.97	LT											1							
R220	265	NOT USED																					
R221	266	NOT USED																					
R222	266			534+42.31	LT											1							
R223	266			534+74.75	LT											1							
R224	266	NOT USED																					
R225	266	534+00.00		535+46.29	RT		144			1													
R226	266			535+47.78	RT		3				1												
R227	266			536+69.01	RT											1							
R228	266			536+81.04	LT											1							
R229	266			537+61.17	RT					1													1
R230	266			537+63.35	LT											1							
R231	267	NOT USED																					
R232	267			539+08.97	RT					1													1
R233	267	NOT USED																					
R234	267			539+97.83	RT		5					1											
R235	267			540+00.36	RT											1							
R236	267			540+01.25	RT		220			1													
R237	267			540+22.14	LT											1							
R238	267			540+68.15	LT											1							
R239	267			541+11.60	LT											1							
R240	267			542+02.15	LT											1							
R241	267			542+23.09	RT		4					1											
R242	267			542+26.64	RT		171			1													
R243	267			543+14.65	LT											1							
R244	268	NOT USED																					
R245	268			544+47.48	RT		4					1											
R246	268			544+50.35	RT		295			1													
R247	268	NOT USED																					
R248	268			545+37.20	LT					1													1
R249	268			546+97.29	LT		40					1											
R250	268			546+97.59	RT		4					1											
R251	268			547+00.74	RT		197			1													
R252	268	NOT USED																					
R253	269	NOT USED																					
R254	269			549+47.39	LT		40					1											
R255	269			549+47.36	RT		3					1											
R256	269	549+49.13		551+97.62	RT		292			1													
R257	269	NOT USED																					
R258	269			551+82.38	LT		40					1											
R259	269			551+82.53	RT		18					1											
R260	269	551+97.62		554+00.00	RT		200			1													
SUBTOTALS CARRIED TO SHEET 225						2243				3	10	13			285		13	1					3

ROADWAY SUBSUMMARY
LAK-US-20-24.99
PART 2

CALCULATED
LIME
CHECKED
JMP

224
697

H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CS006.dgn Sheet 7/15/2022 2:53:05 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION	SIDE	202	202	202	202	202	202	202	202	202	202	202	202	202	202	202	606	606	441	SPECIAL	SPECIAL	
				HEADWALL REMOVED EACH	PIPE REMOVED, 24" AND UNDER FT	PIPE REMOVED, OVER 24" FT	GUARDRAIL REMOVED FT	CONCRETE BARRIER REMOVED FT	MAILBOX REMOVED EACH	MANHOLE REMOVED EACH	CATCH BASIN OR INLET REMOVED EACH	MONUMENT ASSEMBLY REMOVED EACH	FENCE REMOVED FT	REMOVAL MISC.:SHED EACH	REMOVAL MISC.:BUSINESS SIGN EACH	REMOVAL MISC.:BOULDER EACH	REMOVAL MISC.:LIGHT POLE EACH	TRAFFIC ISLAND REMOVED SY	GUARDRAIL, TYPE MGS FT	ANCHOR ASSEMBLY, MGS TYPE T EACH	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (449), (UNDER GUARDRAIL), AS PER PLAN CY	FILL AND PLUG EXISTING CONDUIT, 12" FT	MAILBOX SUPPORT SYSTEM, SINGLE EACH	
R261	270	NOT USED																						
R262	270		TO																					
R263	270		554+51.08	LT																				
R264	270	554+00.00	554+52.84	RT																				
R265	270	NOT USED																						
R266	270		557+17.29	LT																				
R267	270		557+17.38	RT																				
R268	270		557+14.89	RT																				
R269	270		557+99.84	LT																				
R270	271	NOT USED																						
R271	271	NOT USED																						
R272	271		562+36.73																					
R273	271		562+40.38																					
R274	272	NOT USED																						
R275	272	NOT USED																						
R276	272		565+08.91	LT																				
R277	272		565+05.20	LT																				
R278	272	564+00.00	565+52.43	RT																				
R279	272		565+52.43	RT																				
R280	272		567+18.82	LT																				
R281	272	568+38.17	569+00.00	RT																				
R282	272	NOT USED																						
R283	273		568+99.97	LT																				
R284	273		569+00.25	RT																				
R285	273	NOT USED																						
R286	273	NOT USED																						
R287	273		571+49.70	LT																				
R288	273		571+50.38	RT																				
R289	274	NOT USED																						
R290	278		11+32.03	RT																				
R291	278	NOT USED																						
R292	278		12+44.50	RT																				
R209	473		524+22.98	RT																				
R210	473		524+20.86	RT																				
GR1	242	414+92.94	417+05.75	RT																				
GR2	280	92+51.50	92+64.00	LT																				
SUBTOTALS THIS SHEET																								
SUBTOTALS FROM SHEET 220																								
SUBTOTALS FROM SHEET 221																								
SUBTOTALS FROM SHEET 222																								
SUBTOTALS FROM SHEET 223																								
SUBTOTALS FROM SHEET 224																								
TOTALS CARRIED TO GENERAL SUMMARY																								

ROADWAY SUBSUMMARY

LAK-US-20-24.99 PART 2

CALCULATED
LIME
CHECKED
JMP

225
697

H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CS008.dgn Sheet 7/5/2022 2:53:06 PM Troyer

SHEET NO.	REFERENCE NO.	STATION	STATION	SIDE	LENGTH	WIDTH	CALCULATED SURFACE AREA	CADD GENERATED SURFACE AREA	202	608	608	202	609
									WALK REMOVED	4" CONCRETE WALK	CURB RAMP	CURB REMOVED	CURB TYPE 6
					FT	FT	SF	SF	SF	SF	SF	FT	LF
241	SW-1	410+65.00	411+89.75	R	124.75	7.5	935.7		496.7	935.7			
241	SW-2	412+15.25	414+67.50	R	252.25	7.5	1891.9		1337.1	1891.9			
242	SW-3	414+97.50	417+15.00	R	217.50	7.5	1631.3		951.8	1631.3			
242	SW-4	417+65.00	420+57.50	R	292.50	7.5	2193.8		1372.6	2193.8			
243	SW-5	421+00.50	422+87.50	R	187.00	7.5	1402.5		814.8	1402.5			
243	SW-6	423+32.50	423+33.50	R	1.00	7.5	7.5			7.5			
243	SW-7	423+76.50	426+50.00	R	273.50	7.5	2051.3		1213.8	2051.3			
247	SW-8	439+65.00	439+75.00	R	10.00	7.5	75.0		78.5	75.0			
247	SW-9	440+03.00	440+76.50	R	73.50	7.5	551.3		375.6	551.3			
247	SW-10	441+09.50	441+73.00	R	63.50	7.5	476.3		337.1	476.3			
247	SW-11	442+06.00	443+42.50	R	136.50	7.5	1023.8		738.0	1023.8			
247	SW-12	443+80.50	450+82.50	R	702.00	7.5	5265.0		3258.9	5265.0			
249	SW-13	451+17.50	452+37.50	R	120.00	7.5	900.0		725.3	900.0			
249	SW-14	452+72.50	457+34.25	R	461.75	7.5	3463.2		2069.0	3463.2			
250	SW-15	457+61.75	458+27.50	R	65.75	7.5	493.2		273.2	493.2			
250	SW-16	458+76.50	461+30.50	R	254.00	7.5	1905.0		1114.8	1905.0			
251	SW-17	461+79.50	467+02.00	R	522.50	7.5	3918.8		1993.8	3918.8			
252	SW-18	467+42.00	470+84.50	R	342.50	7.5	2568.8		1633.0	2568.8			
253	SW-19	471+18.50	472+90.50	R	172.00	7.5	1290.0		807.7	1290.0			
253	SW-20	473+17.50	475+17.00	R	199.50	7.5	1496.3		982.8	1496.3			
254	SW-21	475+67.00	476+59.00	R	92.00	7.5	690.0		512.3	690.0			
254	SW-22	477+09.00	478+51.00	R	142.00	7.5	1065.0		952.2	1065.0			
254	SW-23	478+84.00	481+41.50	R	257.50	7.5	1931.3		1419.7	1931.3			
255	SW-24	481+90.50	486+67.00	R	476.50	7.5	3573.8		2429.5	3573.8			
258	SW-25	495+49.00	495+82.00	L				171.3	171.3		167.6	17.5	17.5
258	SW-26	495+29.00	495+55.00	L				205.4	176.1	159.1	205.4	17.5	17.5
258	SW-27	495+22.00	500+40.00	R	518.00	5	2590.0		2161.8	2590.0	31.1		
259	SW-28	500+40.00	500+72.50	R	32.50	7.5	243.8		178.3	243.8			
259	SW-29	501+11.50	501+13.50	R	2.00	7.5	15.0		69.7	15.0			
259	SW-30	501+53.50	502+13.00	R	59.50	7.5	446.3		318.9	446.3			
259	SW-31	502+53.00	504+29.00	R	176.90	7.5	1326.8		892.6	1326.8			
260	SW-32	504+58.00	504+99.75	R	41.75	7.5	313.2		283.8	313.2			
260	SW-33	505+29.25	505+57.75	R	28.50	7.5	213.8		185.1	213.8			
260	SW-34	506+02.25	507+99.00	R	196.75	7.5	1475.7		861.1	1475.7			
260	SW-35	508+49.00	509+96.50	R	147.50	7.5	1106.3		679.8	1106.3			
261	SW-36	510+32.50	513+04.50	R	272.00	7.5	2040.0		1533.5	2040.0			
261	SW-37	513+47.50	513+82.00	R	34.50	7.5	258.8		412.7	258.8			
3+3	SW-38	514+32.00	515+16.50	R	84.50	7.5	633.8		464.3	633.8			
262	SW-39	515+62.50	516+58.08	R	102.58	7.5	769.4		635.7	769.4	67.9		
262	SW-40	517+51.18	518+93.50	R	142.32	7.5	1067.4		719.7	1067.4	67.5		
263	SW-41	519+27.50	519+78.00	R	50.50	7.5	378.8		300.9	378.8			
263	SW-42	520+10.00	520+64.00	R	54.00	7.5	405.0		301.5	405.0			
263	SW-43	521+14.00	523+71.00	R	257.00	7.5	1927.5		1336.8	1927.5			
264	SW-44	524+21.00	525+07.00	R	86.00	7.5	645.0		464.3	645.0			
264	SW-45	525+57.00	528+75.00	R	318.00	7.5	2385.0		1508.7	2385.0			
265	SW-46	529+25.00	532+51.00	R	326.00	7.5	2445.0		1491.5	2445.0			
265	SW-47	533+01.00	534+82.00	R	181.00	7.5	1357.5		876.9	1357.5			
266	SW-48	535+32.00	536+18.80	R	86.80	7.5	651.1		463.6	651.1			
266	SW-49	536+52.80	537+08.50	R	55.70	7.5	417.8		346.8	417.8			
266	SW-50	537+44.50	538+68.50	R	124.00	7.5	930.0		661.4	930.0			
266	SW-51	539+07.50	540+24.50	R	117.00	7.5	877.5		611.6	877.5			
267	SW-52	540+65.50	542+53.50	R	188.00	7.5	1410.0		922.8	1410.0			
267	SW-53	542+86.50	543+31.50	R	45.00	7.5	337.5		246.0	337.5			
267	SW-54	543+71.50	546+67.50	R	296.00	7.5	2220.0		1360.3	2220.0			
268	SW-55	547+42.50	549+50.00	R	207.5	7.5	1556.3	60.0	872.9	1616.3			
TOTALS COLUMN 1									47399	71465	540	35	35

SHEET NO.	REFERENCE NO.	STATION	STATION	SIDE	LENGTH	WIDTH	CALCULATED SURFACE AREA	CADD GENERATED SURFACE AREA	202	608	608	202	609
									WALK REMOVED	4" CONCRETE WALK	CURB RAMP	CURB REMOVED	CURB TYPE 6
					FT	FT	SF	SF	SF	SF	SF	FT	LF
269	SW-56	549+50.00	550+07.53	R	57.53	5	287.7						
269	SW-57	550+58.99	552+82.00	R	223.01	5	1115.1		266.3	287.7	45.8		
269	SW-58	553+34.00	555+62.87	R	228.87	5	1144.4		1028.8	1115.1	46.2		
270	SW-59	555+72.06		R					1322.8	1144.4	77.2		
270	SW-60	556+25.44	556+67.99	R							61.1		
270	SW-61	556+95.50	557+52.00	R	56.50	7.5	423.8	165.5	432.1	165.5	178.1		
270	SW-62	557+87.00	558+06.00	R	19.00	7.5	142.5		310.0	423.8			
270	SW-63	558+45.00	558+87.00	R	42.00	7.5	315.0		148.1	142.5			
271	SW-64	559+34.00	559+62.50	R	28.50	7.5	213.8		320.5	315.0			
271	SW-65	559+96.50	560+51.50	R	55.00	7.5	412.5		193.7	213.8			
271	SW-66	560+88.50	561+39.00	R	50.50	7.5	378.8		334.5	412.5			
271	SW-67	561+89.00	564+79.05	R	290.05	7.5	2175.4		287.1	378.8			
272	SW-68	564+79.05	565+38.09	R			442.2		537.3	2175.4			
272	SW-69	565+38.09	565+50.00	R					351.6	442.2	45.7		
272	SW-70	566+17.95		R							42.3		
268	SW-71	545+78.50	550+00.00	L	421.50	7.5	3161.3				70.0		
269	SW-72	550+00.00	550+07.55	L					2060.6	3161.3			
269	SW-73	550+44.32	550+52.69	L							82.0		
269	SW-74	550+52.69	551+74.50	L	121.81	7.5	913.6				82.0		
269	SW-75	552+19.50	552+73.00	L	53.50	7.5	401.3		600.5	913.6			
269	SW-76	553+16.00	555+05.62	L	189.62	7.5	1422.2		278.0	401.3			
270	SW-77	555+05.62	555+42.05	L					914.2	1422.2			
270	SW-78	556+09.27	556+54.50	L					274.7		229.2		
270	SW-79	556+54.50	558+04.15	L	149.65	7.5	1122.4		387.0		295.2		
270	SW-80	558+38.21	558+56.40	L					763.6	1122.4	71.2		
270	SW-81	558+56.40	561+25.50	L	269.10	7.5	2018.3		186.1	149.2	102.8		
271	SW-82	561+75.50	563+62.50	L	187.00	7.5	1402.5		1348.4	2018.3			
272	SW-83	564+12.50	564+47.50	L	35.00	7.5	262.5		935.2	1402.5			
272	SW-84	564+78.99	565+21.24	L			56.4		262.5				
272	SW-85	565+94.94	566+30.52	L		7.5	159.3		56.4		231.8		
272	SW-86	568+50.00	569+76.33	L					176.6	159.3	59.4		
272	SW-87	568+58.50	568+74.95	L	16.45	7.5	123.4		509.9				
273	SW-88	569+11.05	569+68.15	L	57.10	7.5	428.3				123.4		
273	SW-89	570+07.85	570+44.25	L	36.40	7.5	273.1				428.3		
273	SW-90	570+85.75	572+66.40	L	180.65	7.5	1354.9		197.2	273.1			
273	SW-91	573+06.40	574+55.00	L	148.60	7.5	1114.5		898.3	1354.9			
274	SW-92	574+82.00	575+86.50	L	104.5	7.5	783.8		695.9	1114.5			
274	SW-93	576+13.50	576+22.00	L	8.5	7.5	63.8		492.1	783.8			
274	SW-94	576+72.00	577+06.40	L	34.4	7.5	258.1		91.0	63.8			
TOTALS THIS COLUMN									16468	22686	1720		
TOTALS COLUMN 1									47399	71465	540	35	35
TOTALS CARRIED TO GENERAL SUMMARY									63867	94151	2260	35	35

ROADWAY SUBSUMMARY

LAK-US-20-24.99 PART 2

CALCULATED
LIME
CHECKED
JMP

H:\2018\180743\000\ProjectData\09270.LAK_20_EAST\Design\Roadway\Sheets\09270_CC001.dgn Sheet 7/15/2022 2:53:11 PM Troyer

SHEET NO.	STATION		203	203	659
	FROM	TO	EXCAVATION CY	EMBANKMENT CY	SEEDING AND MULCHING, CLASS 1 SY
282	410+00.00	411+00.00			477
283	411+20.00	412+00.00	46	2	519
284	412+50.00	413+50.00	183	1	1019
285	414+00.00	415+00.00	218	6	909
286	415+50.00	416+50.00	367	375	1000
287	417+00.00	418+00.00	225	91	1320
288	418+50.00	420+00.00	292	297	1325
289	420+50.00	421+50.00	96	9	891
290	422+00.00	423+00.00	101	7	948
291	423+50.00	425+00.00	110	16	1090
292	425+50.00	426+50.00	63	13	495
293	427+00.00	427+50.00			209
294	428+00.00	428+50.00			200
295	429+00.00	429+50.00			203
296	430+00.00	430+50.00			164
297	431+00.00	432+00.00			
298	432+50.00	433+50.00			
299	434+00.00	435+00.00			
300	435+50.00	436+50.00			
301	437+00.00	438+00.00			172
302	438+50.00	439+50.00			285
303	440+00.00	440+50.00	23		229
304	441+00.00	442+00.00	78	4	495
305	442+50.00	443+50.00	100		501
306	444+00.00	445+00.00	134		467
307	445+50.00	446+50.00	174		432
308	447+00.00	448+00.00	119	3	432
309	448+50.00	449+50.00	141	5	417
310	450+00.00	451+00.00	134	2	378
311	451+50.00	452+50.00	96	10	484
312	453+00.00	454+00.00	227	6	729
313	454+50.00	455+50.00	337		768
314	456+00.00	457+00.00	166	39	728
315	457+50.00	458+50.00	31	50	589
316	459+00.00	460+00.00	55	26	468
317	460+50.00	461+50.00	86	6	705
318	462+00.00	463+00.00	87	1	648
319	463+50.00	464+50.00	220	7	536
320	465+00.00	466+00.00	128	10	536
321	466+50.00	467+50.00	131	5	516
322	468+00.00	469+00.00	157	4	516
323	469+50.00	470+50.00	181	2	586
324	471+00.00	472+00.00	185		658
325	472+50.00	473+50.00	165		549
326	474+00.00	475+00.00	142		549
327	475+50.00	476+50.00	91	9	549
328	477+00.00	478+00.00	90	8	486
329	478+50.00	479+50.00	94	1	413
330	480+00.00	481+00.00	347	2	770
331	481+50.00	482+50.00	459		725
332	483+00.00	484+00.00	303	1	512
333	484+50.00	485+50.00	86	20	577
334	486+00.00	487+00.00	15	11	472
335	487+50.00	489+00.00			323
336	489+50.00	490+00.00			
337	490+50.00	491+00.00			
338	491+50.00	492+00.00			
TOTALS CARRIED TO COLUMN 2			6483	1049	28969

SHEET NO.	STATION		203	203	659
	FROM	TO	EXCAVATION CY	EMBANKMENT CY	SEEDING AND MULCHING, CLASS 1 SY
339	492+50.00	493+00.00			
340	493+50.00	494+00.00			
341	494+50.00	495+50.00			217
342	496+00.00	497+00.00			392
343	497+50.00	498+50.00			520
344	499+00.00	499+50.00			460
345	500+00.00	500+50.00	4	1	444
346	501+00.00	502+00.00	46	27	604
347	502+50.00	503+50.00	67	15	618
348	504+00.00	505+00.00	55	12	461
349	505+50.00	506+50.00	134	4	441
350	507+00.00	508+00.00	239		370
351	508+50.00	509+50.00	203	2	520
352	510+00.00	511+00.00	206		534
353	511+50.00	512+50.00	115	21	534
354	513+00.00	514+00.00	80	17	534
355	514+50.00	515+50.00	91	15	534
356	516+00.00	517+00.00	77	6	500
357	517+50.00	518+50.00	65	4	500
358	519+00.00	520+00.00	75	12	534
359	520+50.00	521+50.00	92	11	519
360	522+00.00	523+00.00	74	19	516
361	523+50.00	524+50.00	86	12	516
362	525+00.00	526+00.00	86	6	516
363	526+50.00	527+50.00	144	1	516
364	528+00.00	529+00.00	270	1	544
365	529+50.00	530+50.00	246		598
366	531+00.00	532+00.00	190	1	756
367	532+50.00	533+50.00	139	1	694
368	534+00.00	535+00.00	154		908
369	535+50.00	536+50.00	132		789
370	537+00.00	538+00.00	89	7	819
371	538+50.00	539+50.00	106	16	772
372	540+00.00	541+00.00	153	2	699
373	541+50.00	542+50.00	143		699
374	543+00.00	544+00.00	116	20	731
375	544+50.00	545+50.00	120	1	505
376	546+00.00	547+00.00	114	6	516
377	547+50.00	548+50.00	132	1	516
378	549+00.00	550+00.00	118		478
379	550+50.00	551+50.00	146	2	603
380	552+00.00	553+00.00	201	8	713
381	553+50.00	554+50.00	146	7	689
382	555+00.00	556+00.00	98	3	415
383	556+50.00	557+50.00	95		575
384	558+00.00	559+00.00	111	2	508
385	559+50.00	560+50.00	96	14	492
386	561+00.00	562+00.00	64	25	413
387	562+50.00	563+50.00	80	6	359
388	564+00.00	565+00.00	79	32	516
389	565+50.00	566+00.00	25	4	155
390	566+50.00	567+50.00	80	3	228
391	568+00.00	569+00.00	89	7	386
392	569+50.00	570+50.00	136		486
TOTALS COLUMN 2			5607	354	27862
TOTALS FROM COLUMN 1			6483	1049	28969
TOTALS CARRIED TO SHEET 233			12090	1403	56831

CALCULATED	EJT	CHECKED	WDB		
CALCULATIONS					
LAK-US-20-24.99					
PART 2					
<table border="1" style="margin: auto;"> <tr> <td style="text-align: center;">232</td> </tr> <tr> <td style="text-align: center;">697</td> </tr> </table>				232	697
232					
697					

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CC002.dgn_Sheet 7/5/2022 2:53:12 PM troyer

SHEET NO.	STATION		203	203	659
	FROM	TO	EXCAVATION CY	EMBANKMENT CY	SEEDING AND MULCHING, CLASS 1 SY
393	571+00.00	571+50.00	70	4	267
394	572+00.00	572+50.00	43	7	311
395	573+00.00	573+50.00	7	1	233
396	574+00.00				83
HAINES ROAD					
297	10+26.62	11+20.00	18		185
298	11+50.00		12		93
BURNS ROAD					
299	7+50.00	8+50.00	7		56
300	9+00.00	9+76.54	24	2	316
DERUBERTIS					
401	10+23.30	11+00.00	74	4	244
402	11+50.00	12+00.00	42	4	286
HUBBARD ROAD					
403	92+30.00	93+50.00	4		261
404	94+00.00	95+50.00	16		356
405	96+00.00	97+50.00	17		331
406	98+00.00	99+00.00	18	4	227
407	99+50.00	100+50.00	25	10	304
408	101+00.00	102+00.00	16		122
TOTALS CARRIED TO COLUMN 2					
			393	36	3675

SHEET NO.	STATION		203	203	659
	FROM	TO	EXCAVATION CY	EMBANKMENT CY	SEEDING AND MULCHING, CLASS 1 SY
TOTALS COLUMN 2					
TOTALS COLUMN 1					
TOTALS SHEET 232					
TOTALS CARRIED TO GENERAL NOTES					
TOTALS CARRIED TO GENERAL SUMMARY					

EROSION CONTROL CALCULATIONS

ITEM 659 TOPSOIL

60506 SY SEEDING AND MULCHING
 60506 SY x 0.111 CY/SY = 6716.2 CY
 QUANTITY CARRIED TO GENERAL NOTES = 6717 CY

ITEM 659 SOILS ANALYSIS TEST

6717 CY SEEDING AND MULCHING
 6717 CY x 1 TEST / 10000 CY = 0.7 EACH (2 TESTS MINIMUM)
 QUANTITY CARRIED TO GENERAL NOTES = 2 EACH

ITEM 659 REPAIR SEEDING AND MULCHING

60506 SY SEEDING AND MULCHING
 60506 SY x 5% = 3025.3 SY
 QUANTITY CARRIED TO GENERAL NOTES = 3026 SY

ITEM 659 INTER-SEEDING

60506 SY SEEDING AND MULCHING
 60506 SY x 5% = 3025.3 SY
 QUANTITY CARRIED TO GENERAL NOTES = 3026 SY

ITEM 659 COMMERCIAL FERTILIZER

60506 SY SEEDING AND MULCHING
 60506 SY x 1 TON / 7410 SY = 8.17 TON
 3026 SY INTER-SEEDING
 3026 SY x 1 TON / 11111 SY = 0.27 TON
 QUANTITY CARRIED TO GENERAL NOTES = 8.44 TON

ITEM 659 LIME

60506 SY SEEDING AND MULCHING
 60506 SY x 1 ACRE / 4840 SY = 12.50 ACRE
 QUANTITY CARRIED TO GENERAL NOTES = 12.50 ACRE

ITEM 659 WATER

60506 SY SEEDING AND MULCHING
 60506 SY x 0.0027 MGAL/SY X 2 = 327 MGAL
 3026 SY INTER-SEEDING
 3026 SY x 0.0027 MGAL/SY X 2 = 17 MGAL
 QUANTITY CARRIED TO GENERAL NOTES = 344 MGAL

MISCELLANEOUS CALCULATIONS

ITEM 204 PROOF ROLLING

89902 SY SUBGRADE COMPACTION
 89902 SY / 3000 SY / HR = 30.0 HOUR
 QUANTITY CARRIED TO GENERAL NOTES = 30 MGAL

ITEM 626 BARRIER REFLECTOR, TYPE 2 (BIDIRECTIONAL)

GUARDRAIL LENGTH = 225 FT
 225 FT / 100 +1 = 4 EACH
 QUANTITY CARRIED TO GENERAL SUMMARY = 4 EACH

CALCULATED EJT CHECKED WDB	CALCULATIONS
LAK-US-20-24.99 PART 2	
<div style="border: 1px solid black; border-radius: 50%; width: 30px; height: 30px; display: flex; align-items: center; justify-content: center; margin: 0 auto;"> 233 697 </div>	

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST_Design\Roadway\Sheets\09270_CC005.dgn Sheet 7/5/2022 2:53:15 PM troyer

STATION		SIDE	LENGTH	AVERAGE WIDTH	CALCULATED SURFACE AREA	CADD GENERATED SURFACE AREA	202	202	204	252	254	254	301	301	304	407	441	441	441		441		441	609	609	
FROM	TO						SY	FT	SY	FT	SY	SY	CY	CY	CY	GAL	CY	CY	CY	CY	CY	CY	CY	CY	CY	CY
MCMACKIN																										
10+27.20	10+75.75	L/R	48.55		1766.8	196.3						196.3			23.6		6.8				9.5					
9+20.95	9+64.92	L/R	43.97		1531.4	170.2						170.2			20.4		5.9				8.3					
HAINES																										
10+26.62	10+92.79	L/R	66.17		2630.2	292.2				292.2		48.7		60.1	35.1		10.2					24.4	177.0			
10+92.79	11+50.00	L/R	57.21		1249.5	138.8			20	138.8		23.8		24.2	16.7		4.8					11.6				
GREEN																										
10+33.66	11+03.83	L/R	70.17		3659.4	406.6						406.6			48.8		14.1				19.8				96.0	
WALMART																										
10+34.74	10+94.59	L/R	59.85		2800.6	311.2			24	311.2		51.9		64.8	37.3		10.8					25.9	200.0			
BURNS																										
8+24.93	9+33.02	L/R	108.09		2606.5	289.6			23	289.6		49.6		50.3	34.8		10.1					24.1				
9+33.02	9+76.54	L/R	43.52		1734.1	192.7				192.7		32.1		40.3	23.1		6.7					16.1	126.0			
STONERIDGE																										
10+22.77	10+62.77	L/R	40.00		1646.6	183.0			24	183.0		30.5		38.6	22.0		6.4					15.2	125.0			
MORNINGSTAR																										
10+23.70	10+59.02	L/R	35.32		1225.5	136.2			26	136.2		22.7		29.0	16.3		4.7					11.3	98.0			
DERUBERTIS																										
10+23.30	11+00.00	L/R	76.7		2250.8	250.1				250.1		41.7		53.3	30.0		8.7					20.8	180.0			
11+00.00	11+50.00	L/R	50.0		1202.4	133.6				133.6		22.6		26.0	16.0		4.6					11.1	50.0			
11+50.00	11+75.00	L/R	25.0		559.9	62.2			21	62.2		10.7		13.6	7.5		2.2					5.2				
SOUTH DERUBERTIS																										
9+45.00	9+55.50	R	10.5		322.5	35.8			37	35.8		6.1		6.2			1.2					3.0				
9+55.50	9+77.00	R	21.5		730.68	81.2				81.2		13.5		17.8			2.8					6.8	66.0			
SHOP STA 556																										
10+23.29	10+79.97	L	56.68		3371.52	374.6			41	374.6		62.4		71.1			13.0					31.2	135.0			
9+08.00	9+77.00	R	69.00		3737.94	415.3			46	415.3		69.2		79.1			14.4					34.6	153.0			
SHOP 558+31																										
9+23.17	9+66.10	L	42.93		1892.28	210.3			30	210.3		35.0		41.5			7.3					17.5	119.0			
HUBBARD																										
92+96.00	96+00.00	L	304.0	14.50	4408.0				319	489.8		85.4		84.9	58.8		17.0					23.8				
96+00.00	99+00.00	L	300.0	16.25	4875.0				300	541.7		94.0		93.5	65.0		18.8					26.3				
99+00.00	99+71.43	L/R	71.43		3559.2	395.5				395.5		65.9		78.2	47.5		13.7					33.0	190.0			
100+29.13	101+00.00	L/R	70.87		3684.7	409.4				409.4		68.2		80.7	49.1		14.2					34.1	192.0			
							4684.8			911.0	4943.2	773.1	834.0		953.2	552.0		198.4			87.7	325.9	1811.0	96.0		
TOTALS THIS SHEET							4685			911	4944	774	834		954	552		199			414			1811	96	
TOTALS FROM SHEET 234							33815	1903	40982	123	35064	8308		5844	6708	5205	1507			3326			10677	12594	1898	
TOTALS FROM SHEET 235							40853	137	40456	147	34538	9612		5757	6620	5298	1534			3346			10635	12966	95	
TOTALS CARRIED TO GENERAL SUMMARY							79353	2040	81438	1181	74546	18694	834	11601	14282	11055	3041		199	6672			414	21312	27371	2089

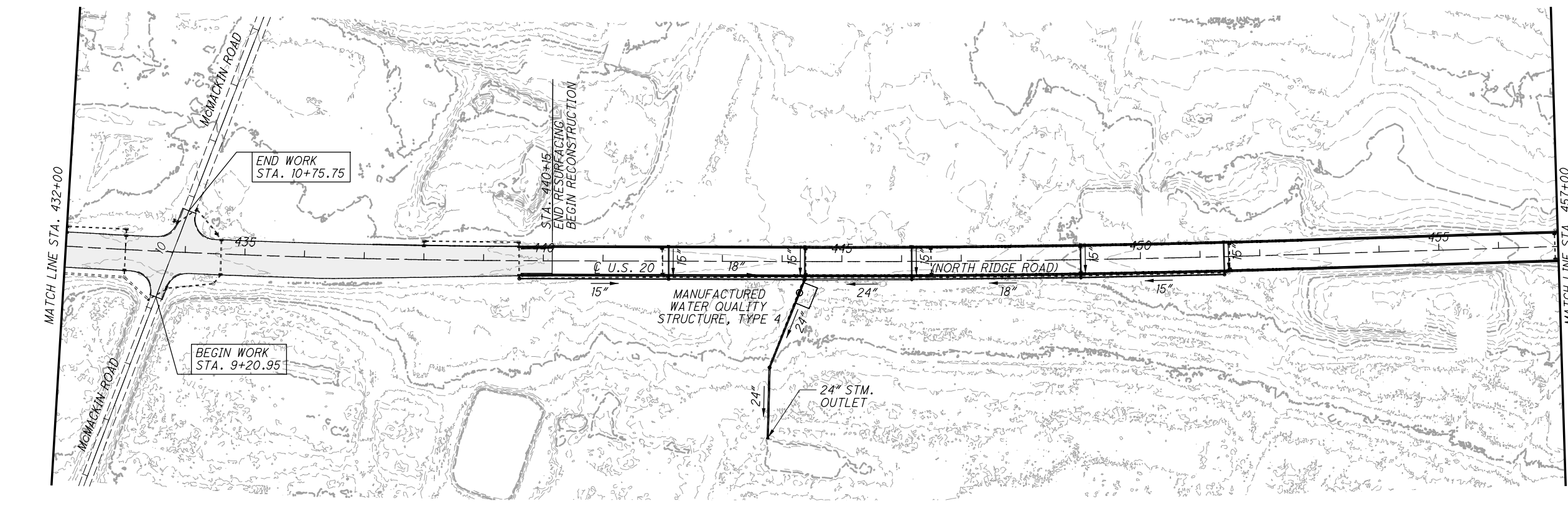
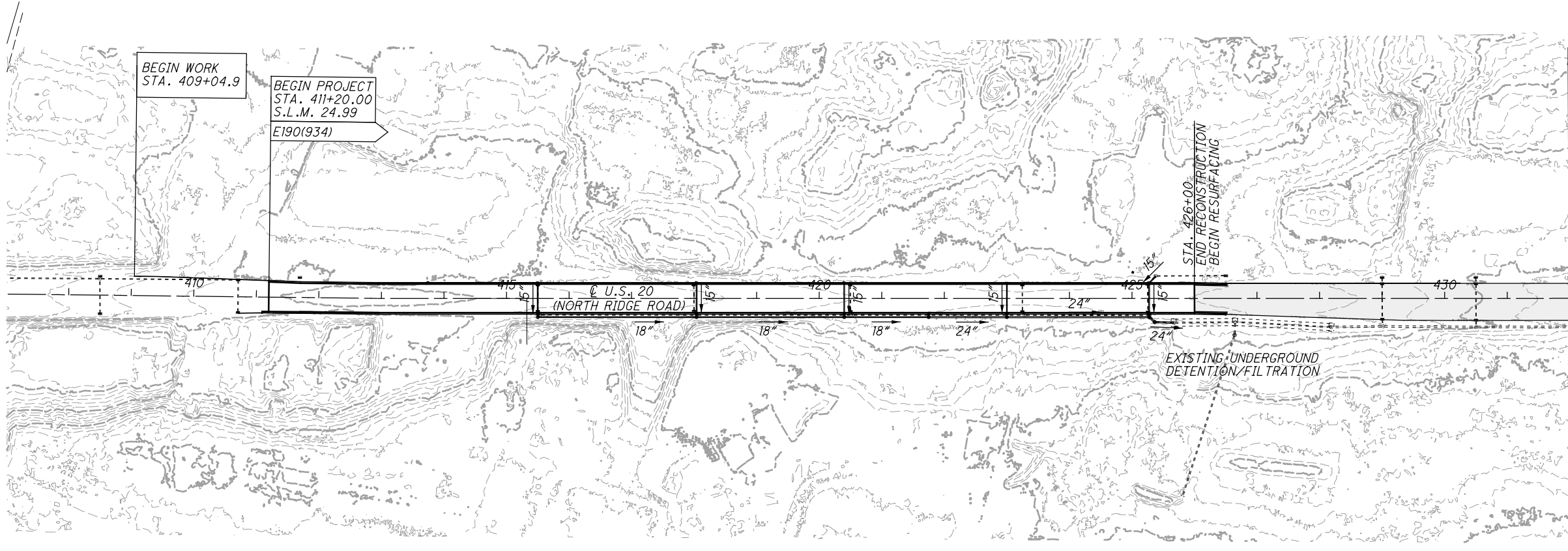
PAVEMENT CALCULATIONS

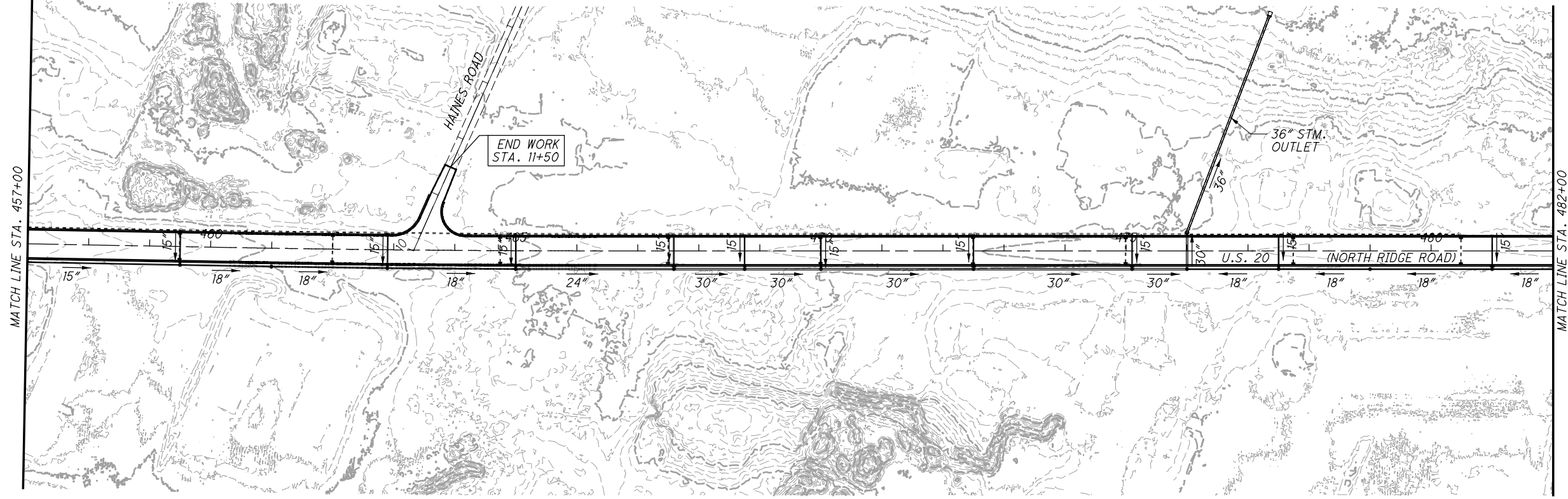
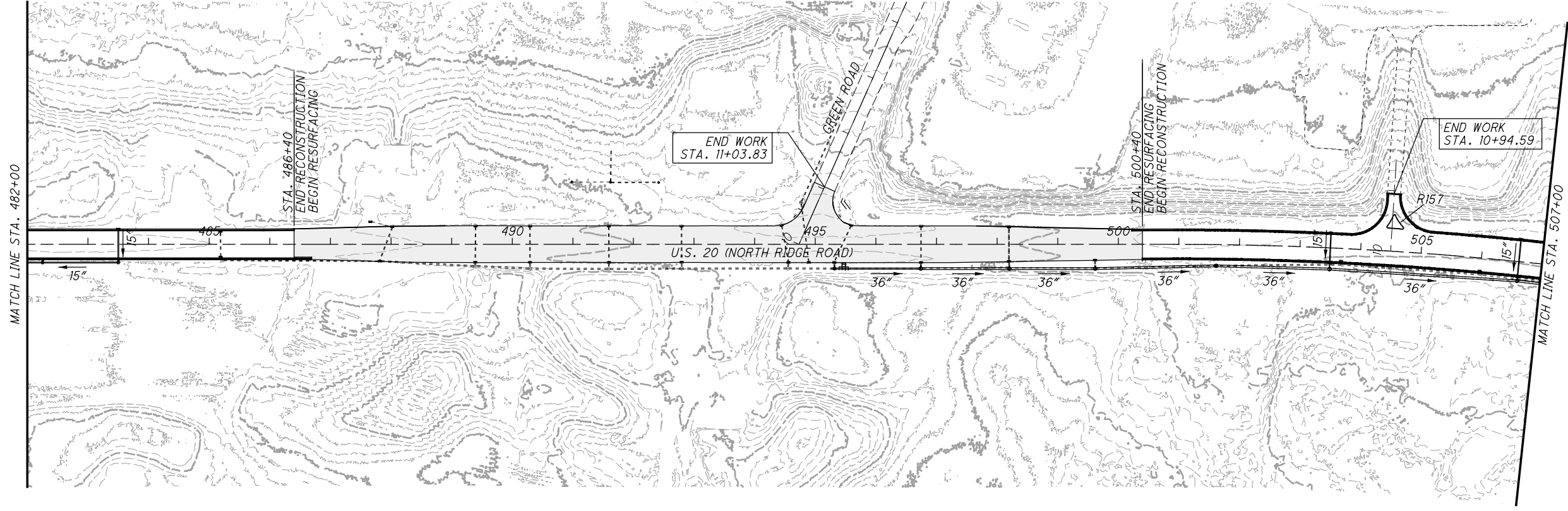
LAK-US-20-24.99 PART 2

CALCULATED
JMP
CHECKED
WDB

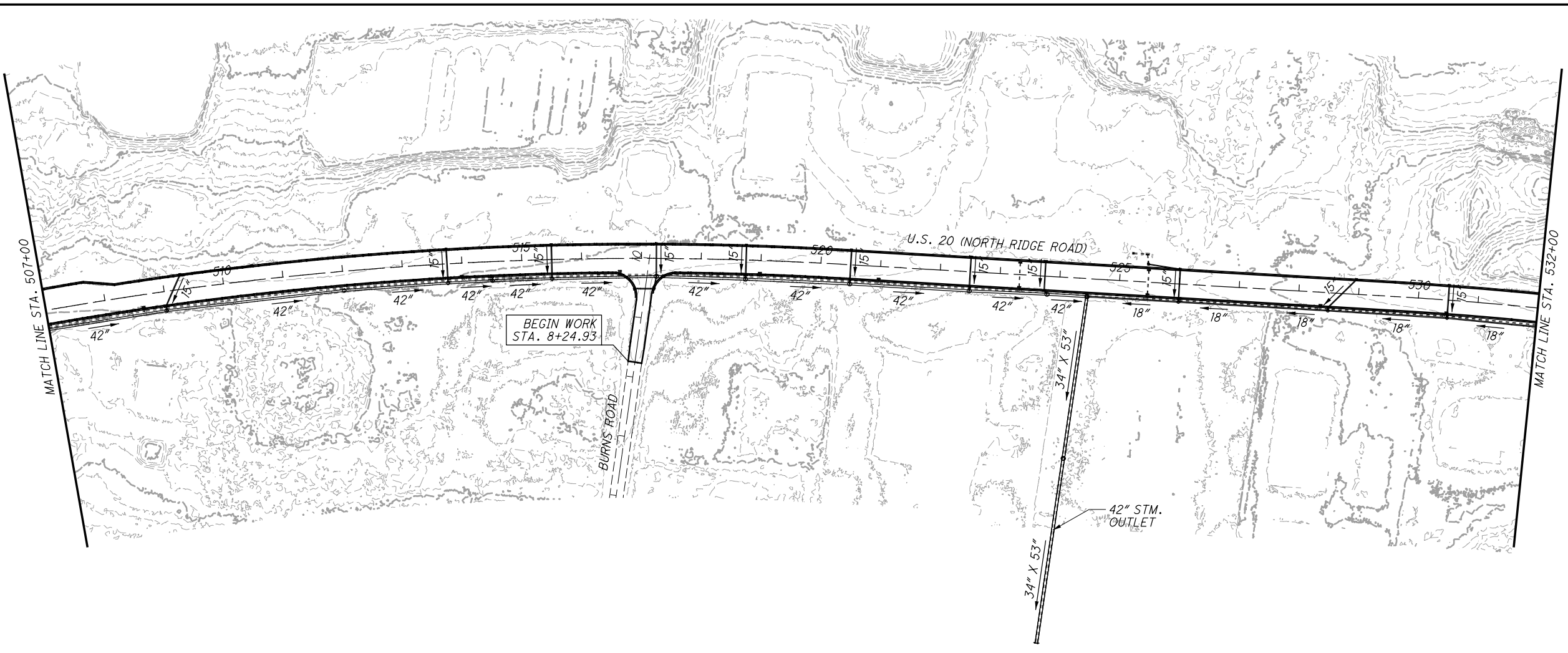
236
697

H:\2018\80743\001\ProjectData\092TO_LAK_20_EAST\Design\Roadway\Sheets\092TO_CB107.dgn Sheet 7/15/2022 2:53:21PM troyer





H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\Roadway_Sheets\09270_CB109.dgn Sheet 7/15/2022 2:53:37 PM Troyer

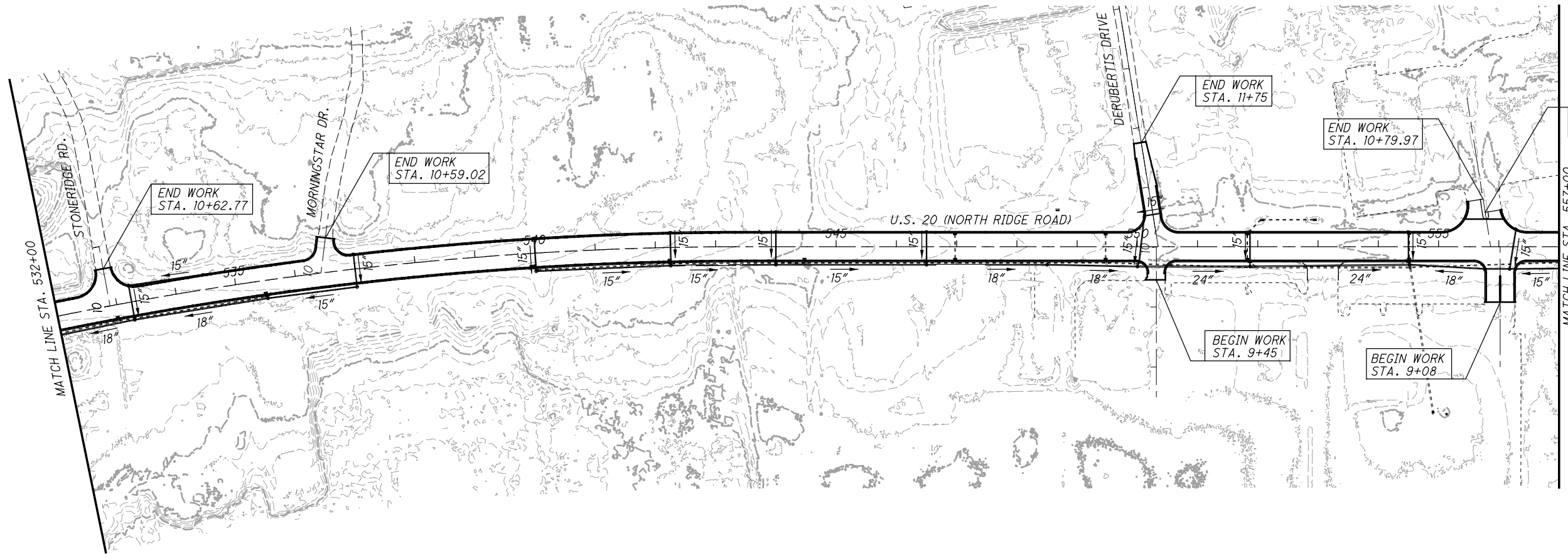


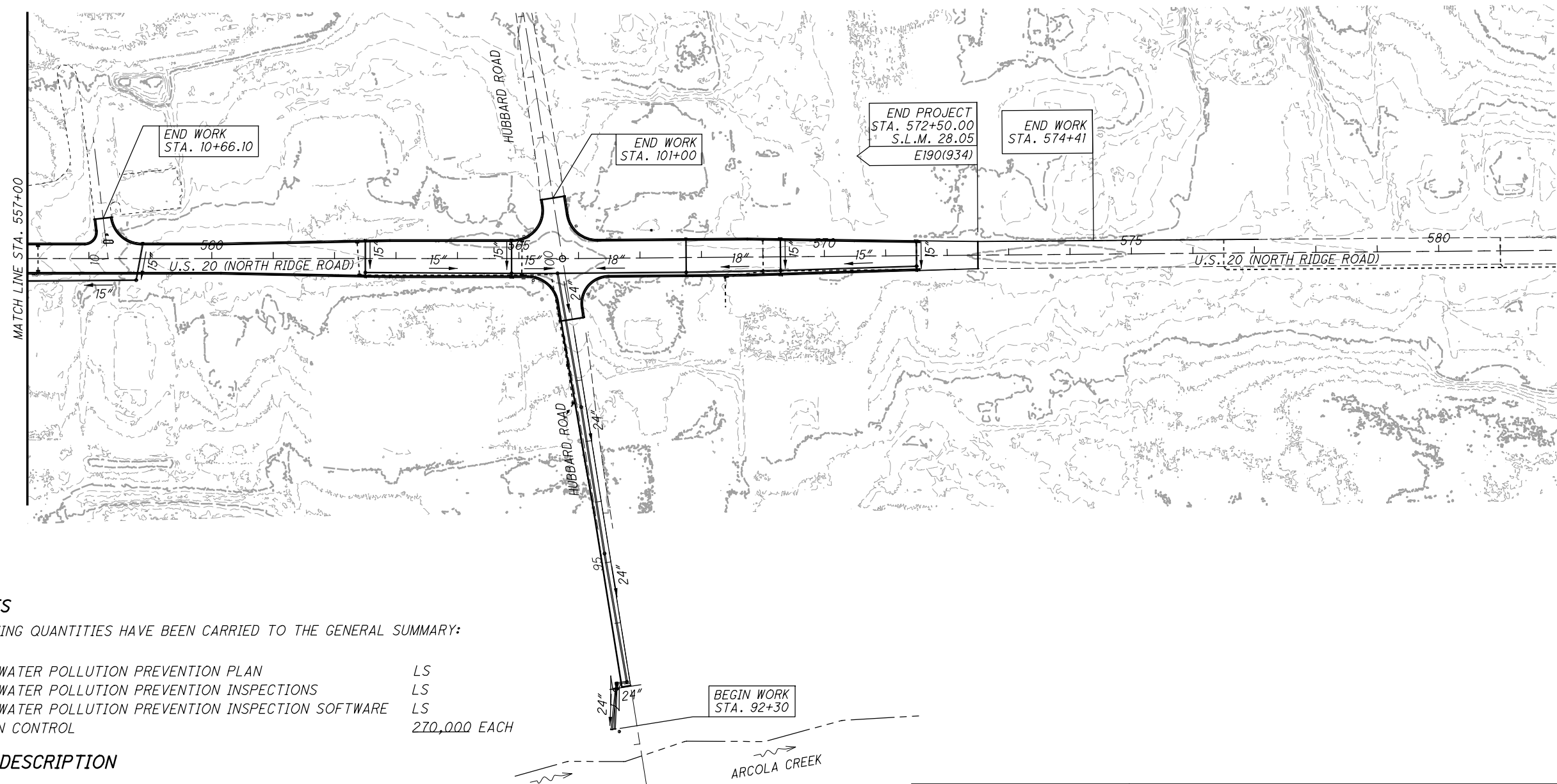
CALCULATED	PSL
CHECKED	JMP

PROJECT SITE PLAN
STA. 507+00 TO STA. 557+00

LAK-US-20-24.99
PART 2

239
697





H:\2018\180743\0D01\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CB10.dgn Sheet 7/15/2022 2:53:44 PM Troyer

QUANTITIES

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

832 STORM WATER POLLUTION PREVENTION PLAN	LS
832 STORM WATER POLLUTION PREVENTION INSPECTIONS	LS
832 STORM WATER POLLUTION PREVENTION INSPECTION SOFTWARE	LS
832 EROSION CONTROL	270,000 EACH

PROJECT DESCRIPTION

MAJOR REHABILITATION OF 3.06 MILES OF NORTH RIDGE ROAD (U.S. ROUTE 20) FROM TOWNLINE ROAD TO HUBBARD ROAD IN MADISON TOWNSHIP. WORK WILL INCLUDE ROADWAY WIDENING, RESURFACING, SIDEWAL, DRAINAGE, AND TRAFFIC SIGNAL IMPROVEMENTS.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	28.34 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	1.00 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	29.40 ACRES

PROJECT DATA		
TOTAL AREA (RIGHT-OF-WAY)	26.8 Ac	RUNOFF COEFFICIENT FOR PRE-CONSTRUCTION SITE 0.90
PROJECT EARTH DISTURBED AREA	28.4 Ac	RUNOFF COEFFICIENT FOR POST CONSTRUCTION SITE 0.90
ESTIMATED CONTRACTOR EARTH DISTURBED AREA	1.0 Ac	A MANUFACTURED WATER QUALITY SYSTEM AND AN EXISTING UNDERGROUND DETENTION/INFILTRATION SYSTEM ARE PROVIDED TO MEET NPDES POST CONSTRUCTION REQUIREMENTS.
NOTICE OF INTENT EARTH DISTURBED AREA	29.40 Ac	
IMPERVIOUS (PAVED) AREA FOR PRE-CONSTRUCTION SITE	26.8 Ac	IMMEDIATE RECEIVING WATERS : ARCOLA CREEK & UNNAMED STREAMS
IMPERVIOUS (PAVED) AREA FOR POST CONSTRUCTION SITE	28.6 Ac	SUBSEQUENT RECEIVING WATER: LAKE ERIE

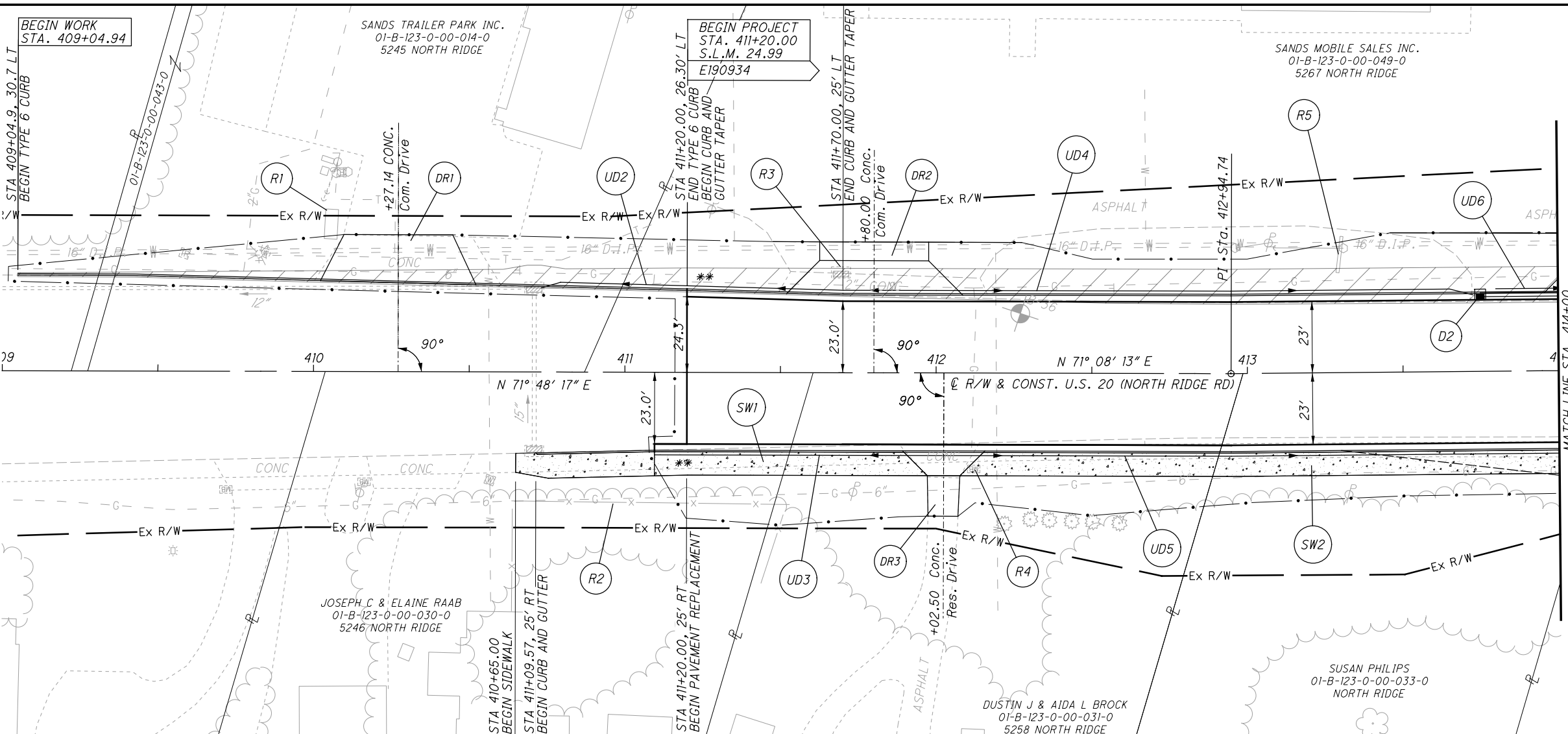
LEGEND

- M.H. NO. 3
- C.B. 3
- C.B. 3A

USGS QUADRANT: MADISON, OHIO
 LATITUDE: 41°45'58.7" N
 LONGITUDE: 81°10'51.7" W

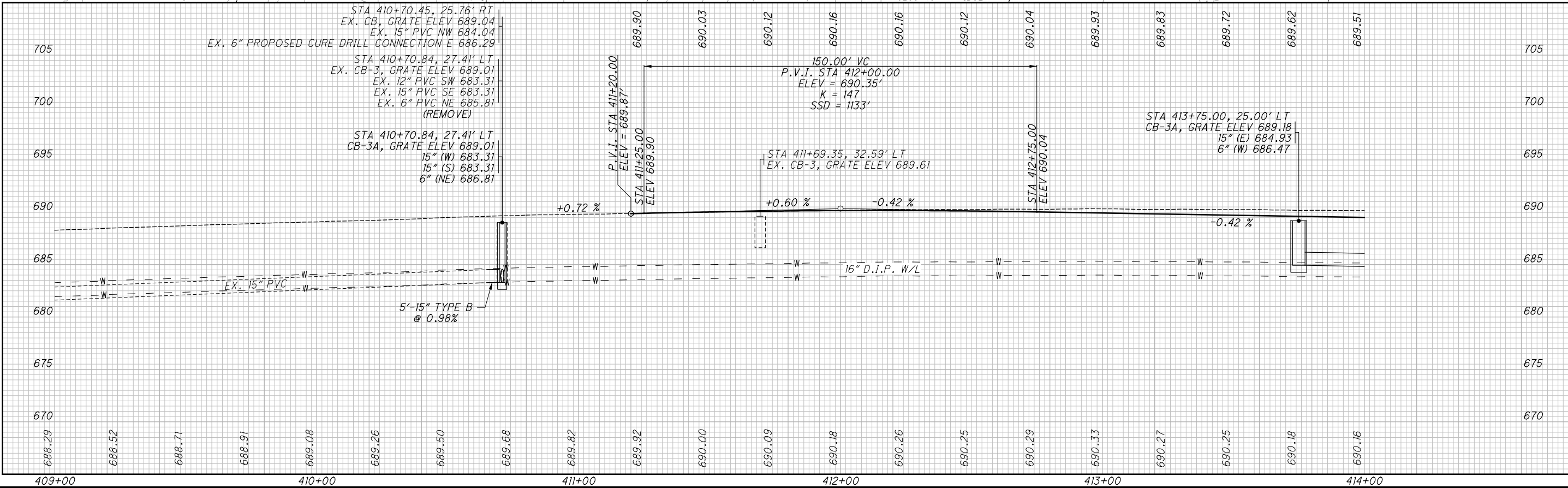
BMP TYPE	LATITUDE / LONGITUDE				EDA TREATMENT CREDIT (ACRES)
	BEGIN		END		
EXISTING UNDERGROUND DETENTION / INFILTRATION	41.793550	-81.097191	41.793745	-81.096304	3.85
MANUFACTURED SYSTEM 4	41.795229	-81.090731			3.08
EDA TREATMENT CREDIT FOR PROJECT					6.93
EDA TREATMENT REQUIRED					5.68

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP001.dgn Sheet 7/15/2022 2:53:47 PM troyer



** VARY GUTTER CROSS SLOPE FROM 0.083 TO 0.016 IN 30.0'

- RESURFACING AREA
- TEMPORARY PAVEMENT

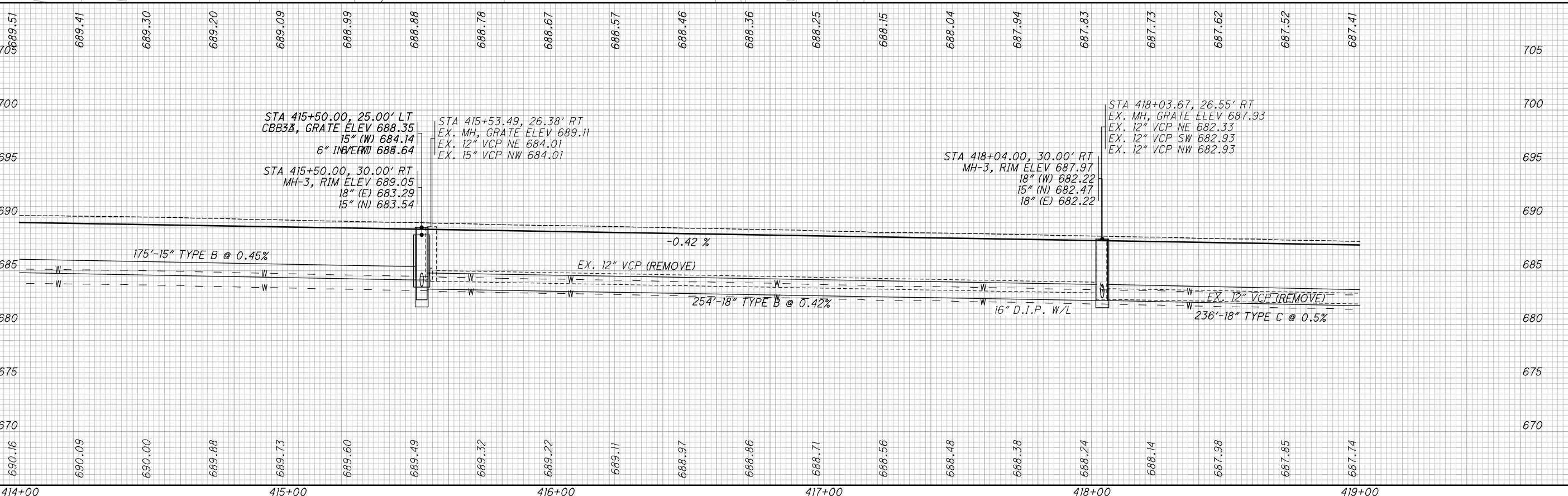
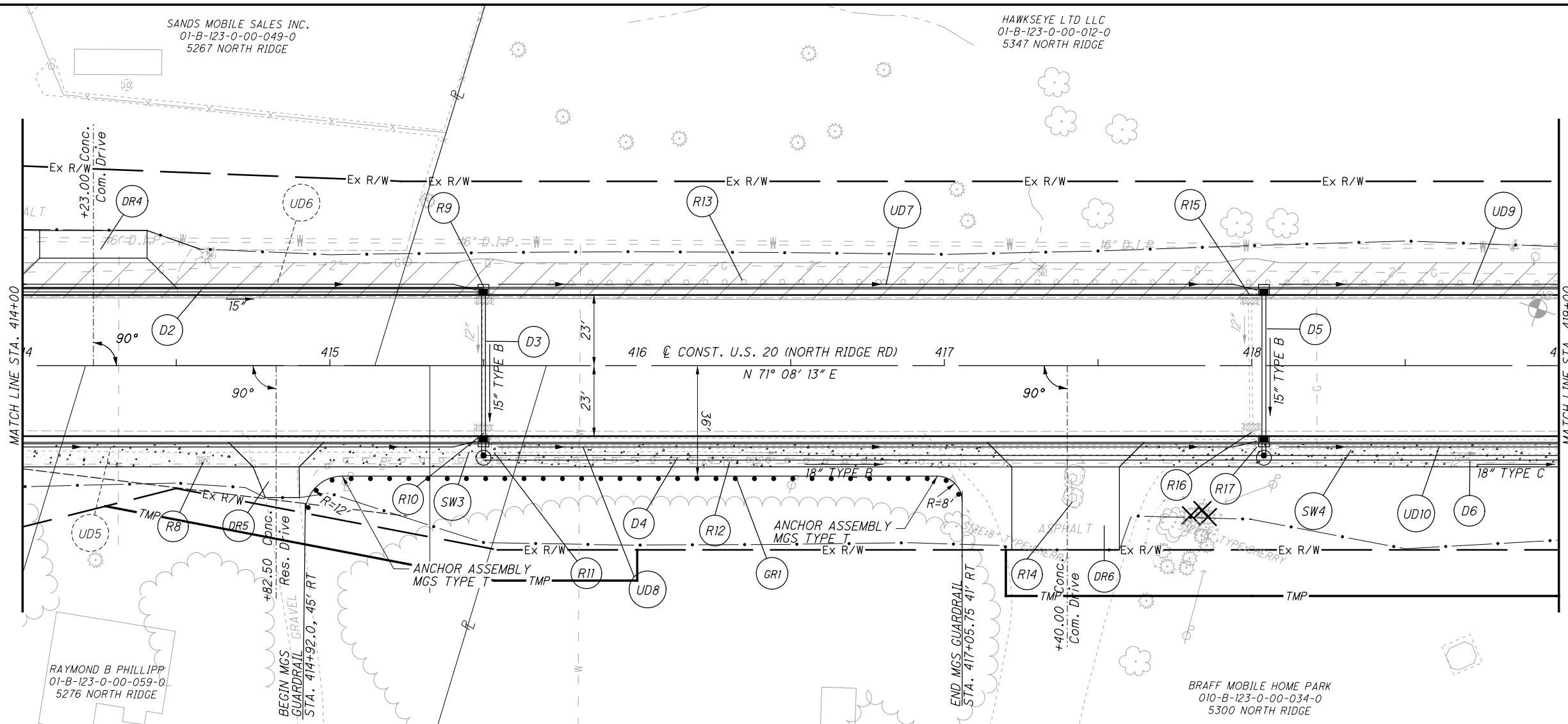


CALCULATED
JMP
CHECKED
WDB

**PLAN AND PROFILE U.S. 20
STA. 409+00 TO STA. 414+00**

**LAK-US-20-24.99
PART 2**

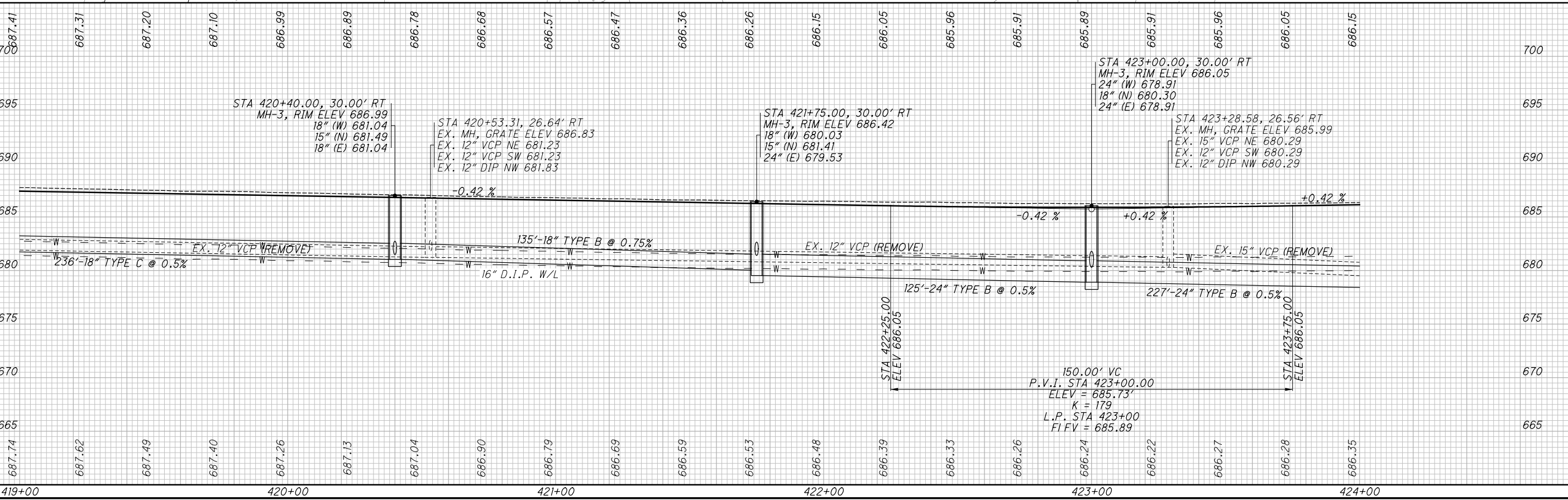
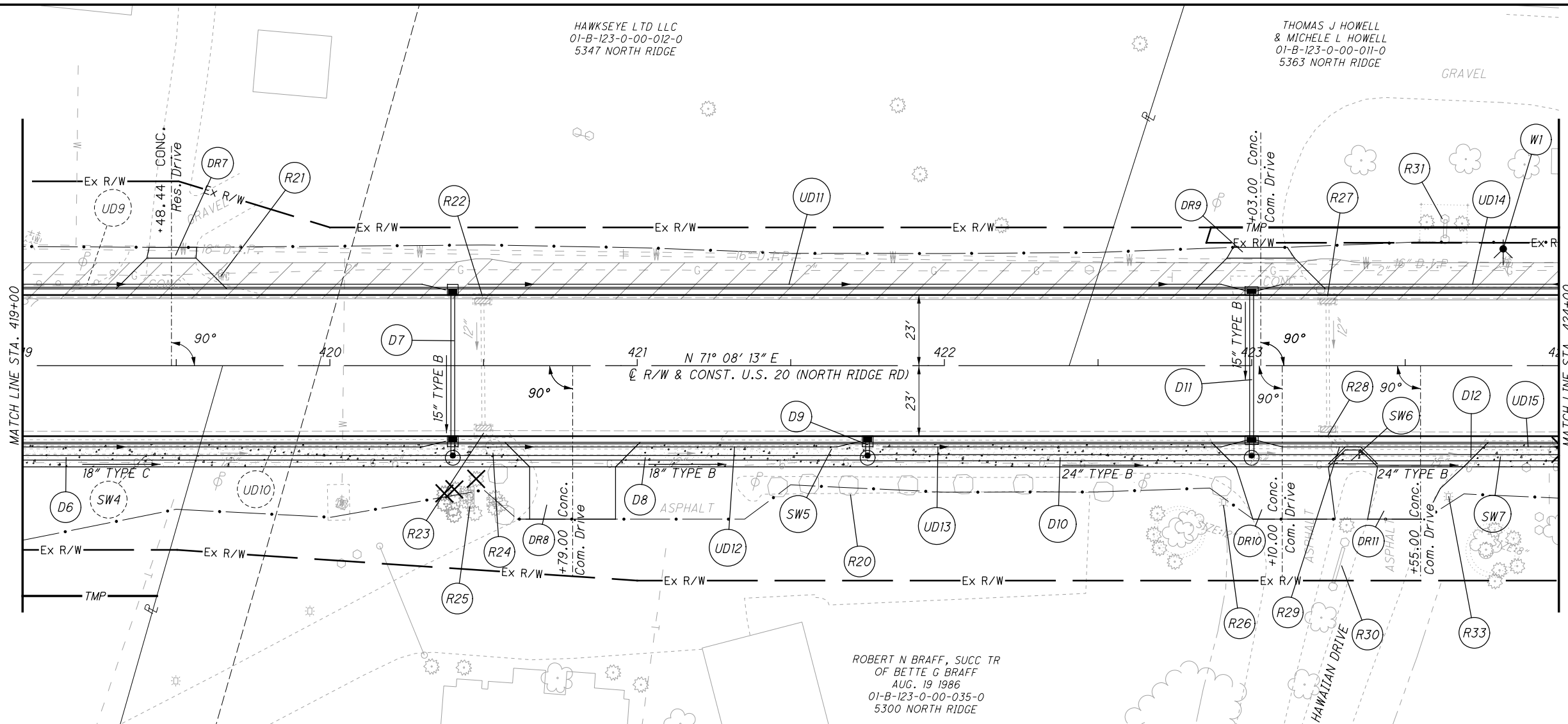
H:\2018\80743\0001\ProjectData\0922\0_LAK_20_EAST\Design\Roadway\Sheets\0922\0_CP002.dgn Sheet 7/15/2022 2:53:47 PM troyer



**PLAN AND PROFILE U.S. 20
STA. 414+00 TO STA. 419+00**

**LAK-US-20-24.99
PART 2**

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_GP003.dgn Sheet 7/15/2022 2:53:48 PM troyer



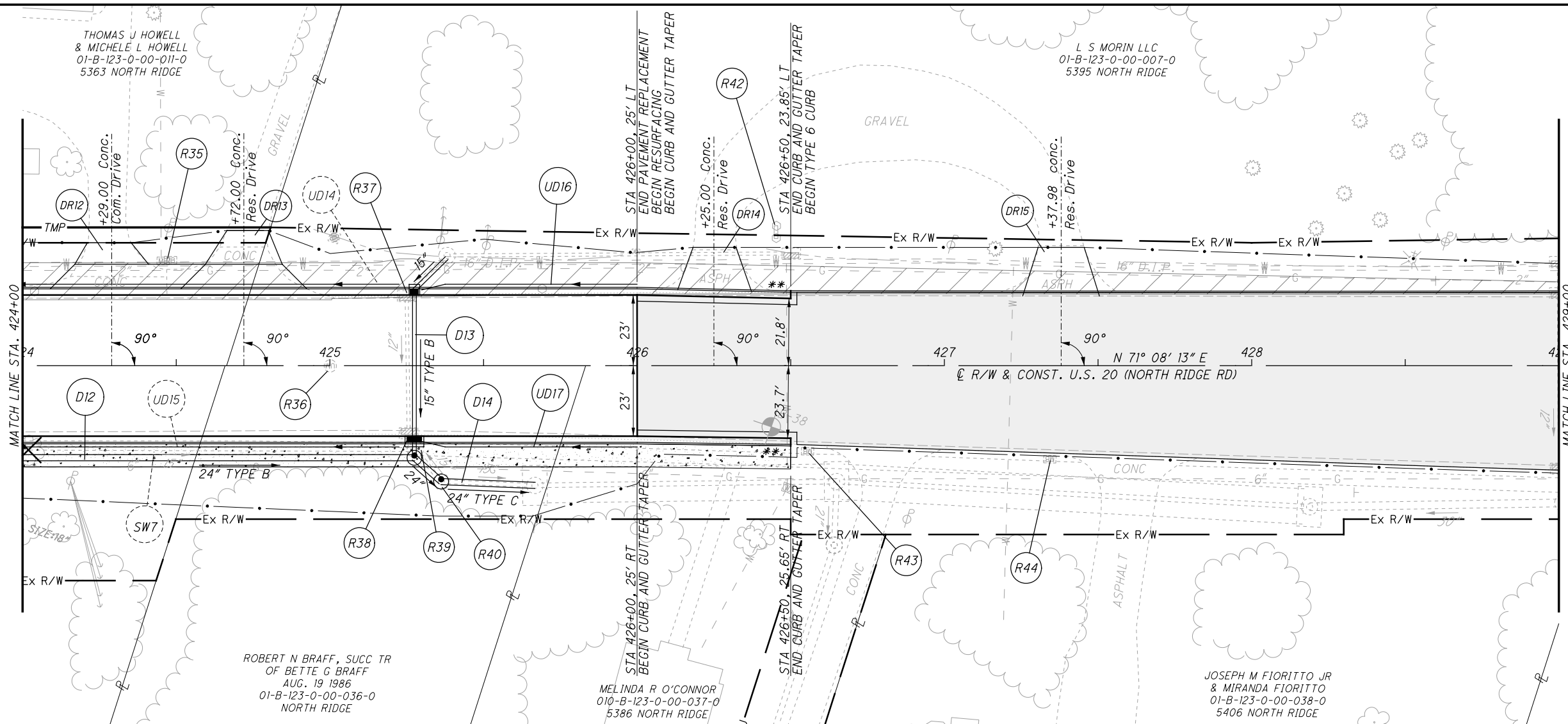
CALCULATED JIM
CHECKED WDB

PLAN AND PROFILE U.S. 20
STA. 419+00 TO STA. 424+00

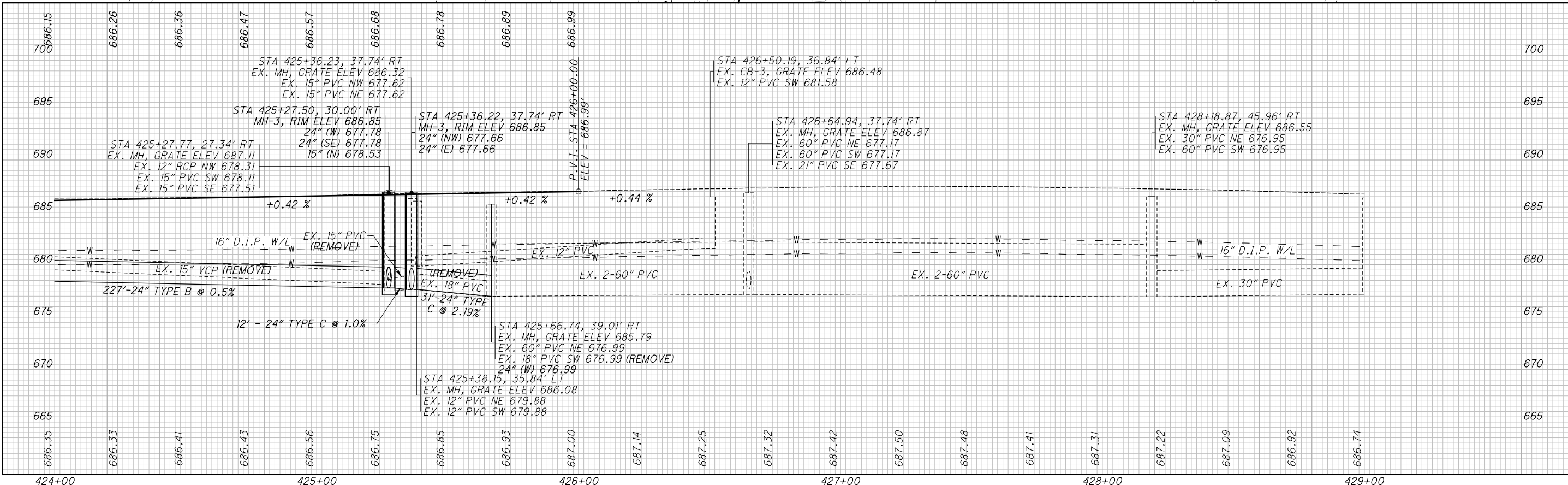
LAK-US-20-24.99
PART 2

243
697

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP004.dgn Sheet 7/15/2022 2:53:49 PM troyer



RESURFACING AREA
TEMPORARY PAVEMENT



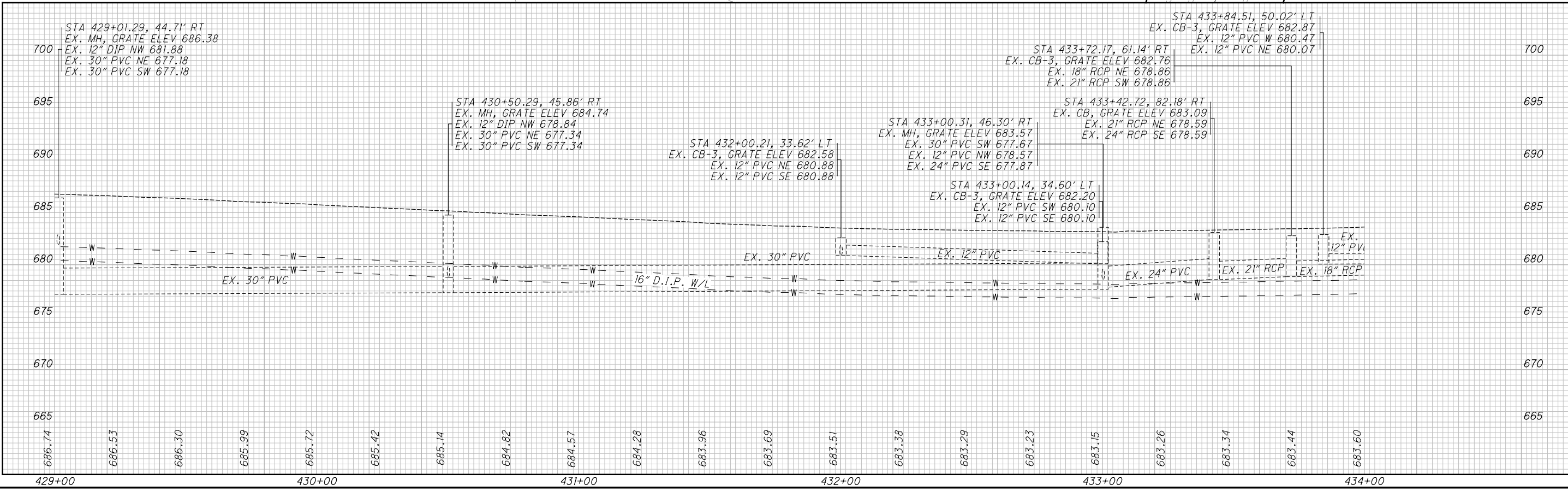
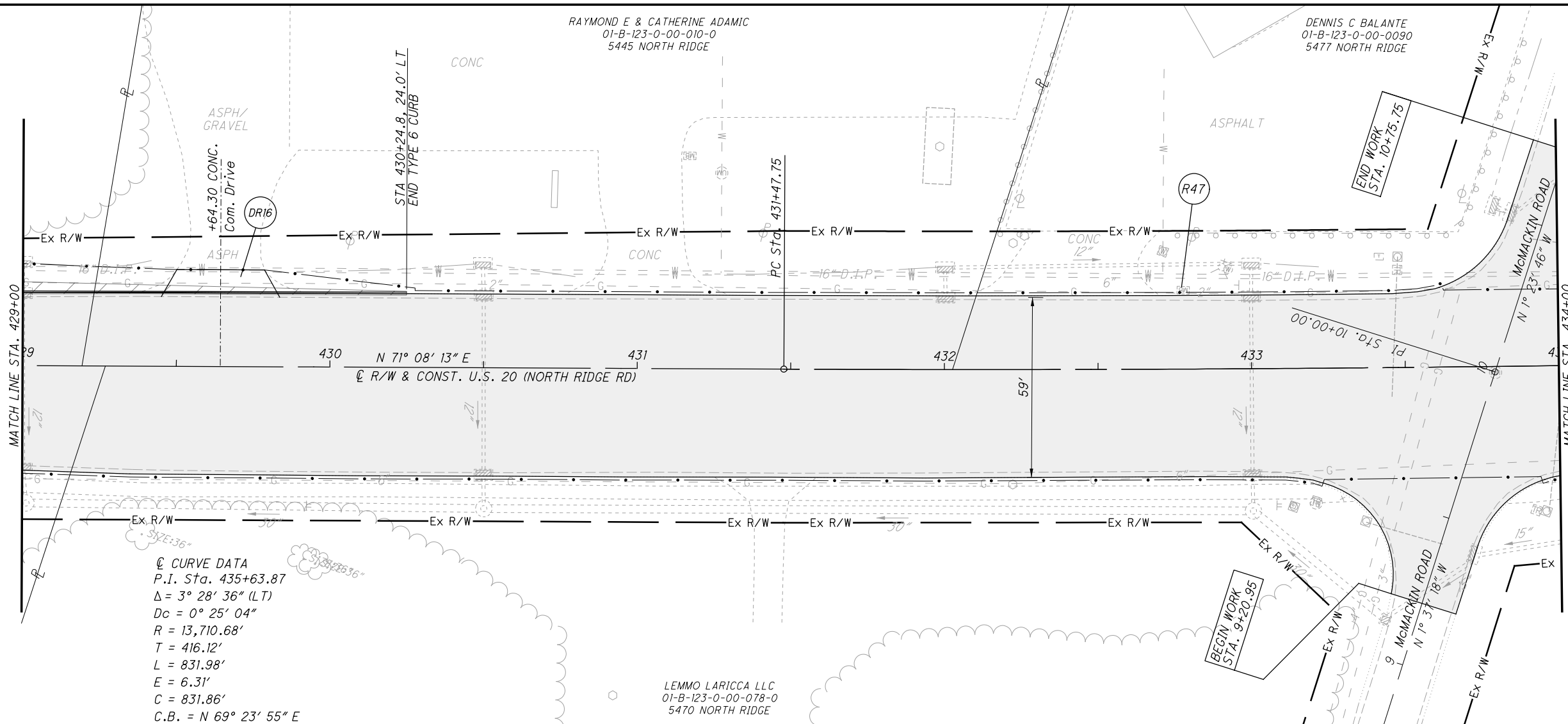
CALCULATED
JMP
CHECKED
WDB

PLAN AND PROFILE U.S. 20
STA. 424+00 TO STA. 429+00

LAK-US-20-24.99
PART 2

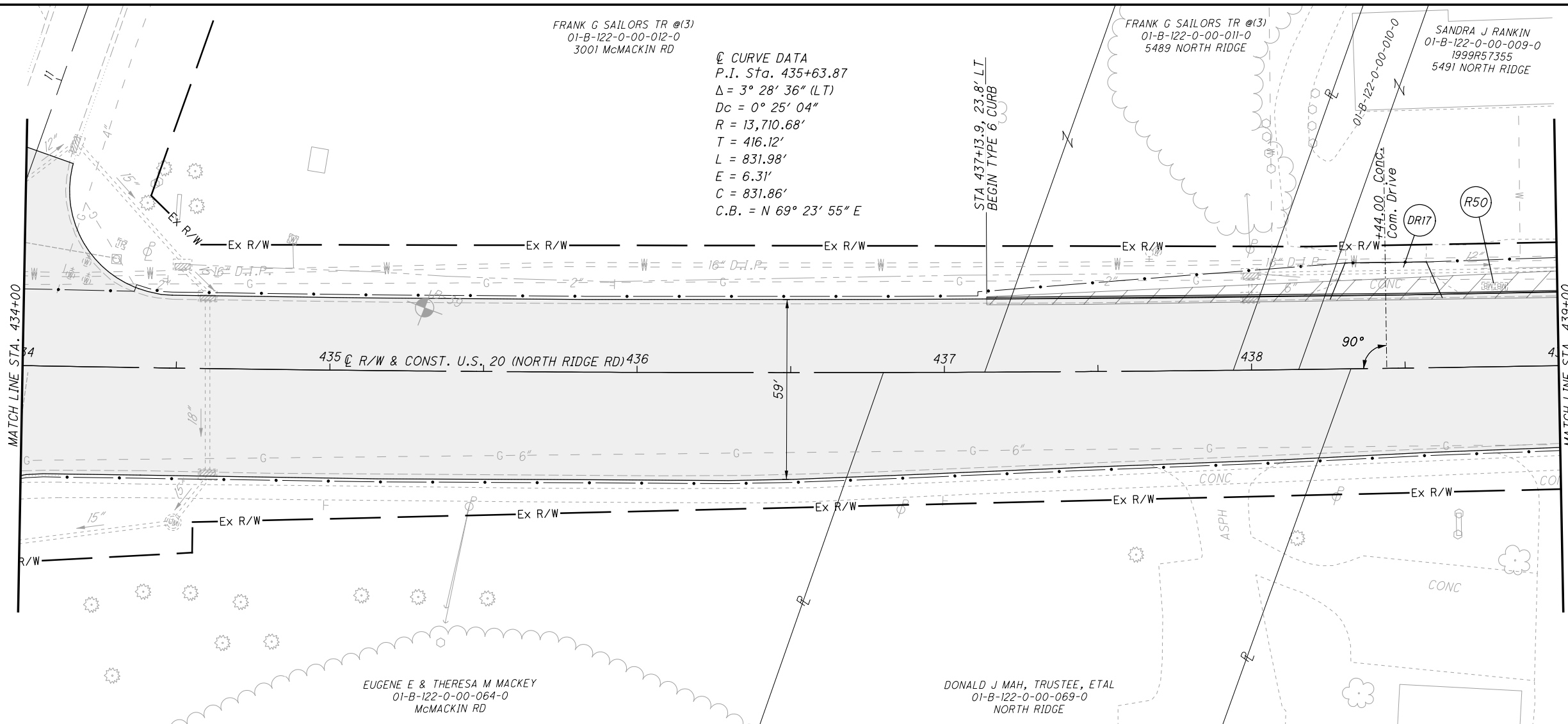
244
697

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_GP005.dgn Sheet 7/15/2022 2:53:50 PM Troyer



PLAN AND PROFILE U.S. 20
 STA. 429+00 TO STA. 434+00
 LAK-US-20-24.99
 PART 2
 245
 697

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP006.dgn Sheet 7/15/2022 2:53:50 PM Troyer



☉ CURVE DATA
 P.I. Sta. 435+63.87
 $\Delta = 3^\circ 28' 36''$ (LT)
 $Dc = 0^\circ 25' 04''$
 $R = 13,710.68'$
 $T = 416.12'$
 $L = 831.98'$
 $E = 6.31'$
 $C = 831.86'$
 $C.B. = N 69^\circ 23' 55'' E$

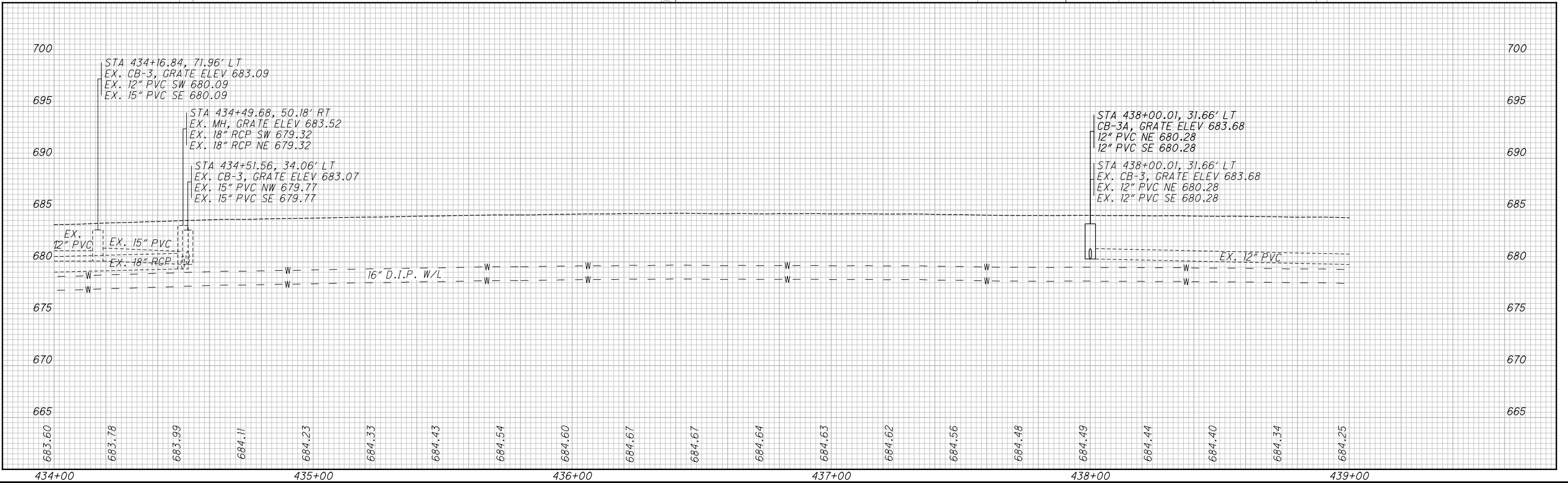
STA 437+13.9, 23.8' LT
 BEGIN TYPE 6 CURB

+44.00 Conc.
 Com. Drive

DR17

R50

RESURFACING AREA



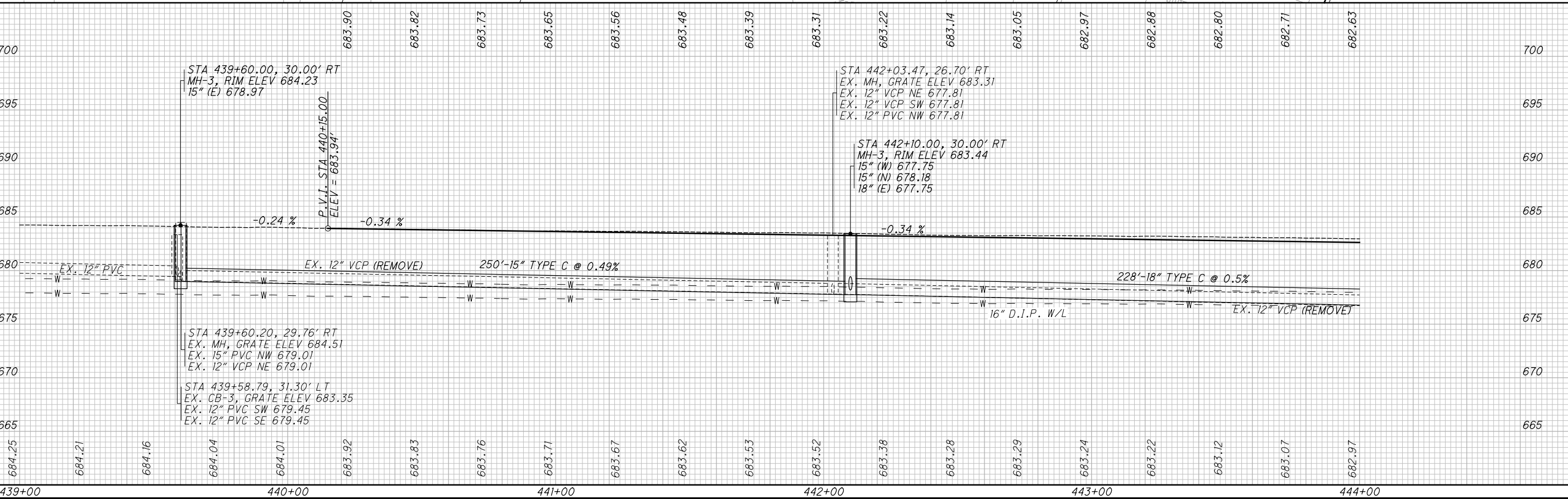
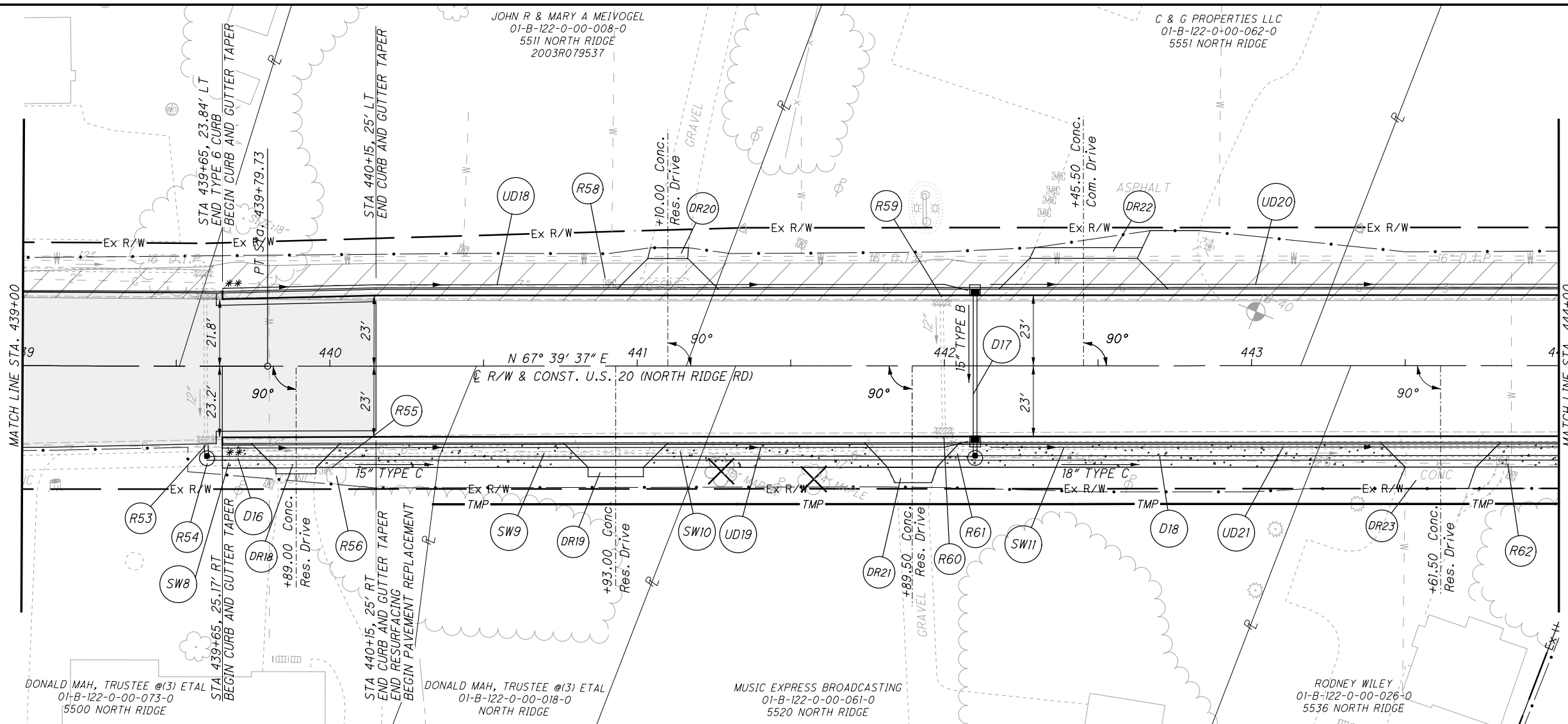
CALCULATED
 JUMP
 CHECKED
 WDB

PLAN AND PROFILE U.S. 20
STA. 434+00 TO STA. 439+00

LAK-US-20-24.99
PART 2

246
 697

H:\2018\80743\001\ProjectData\092TO_LAK_20_EAST\Design\Roadway\Sheets\092TO_CP007.dgn Sheet 7/15/2022 2:53:51PM Troyer



** VARY GUTTER CROSS SLOPE FROM 0.083 TO 0.016 IN 10.0'

RESURFACING AREA
TEMPORARY PAVEMENT



CALCULATED
JMP
CHECKED
WDB

PLAN AND PROFILE U.S. 20
STA. 439+00 TO STA. 444+00

LAK-US-20-24.99
PART 2

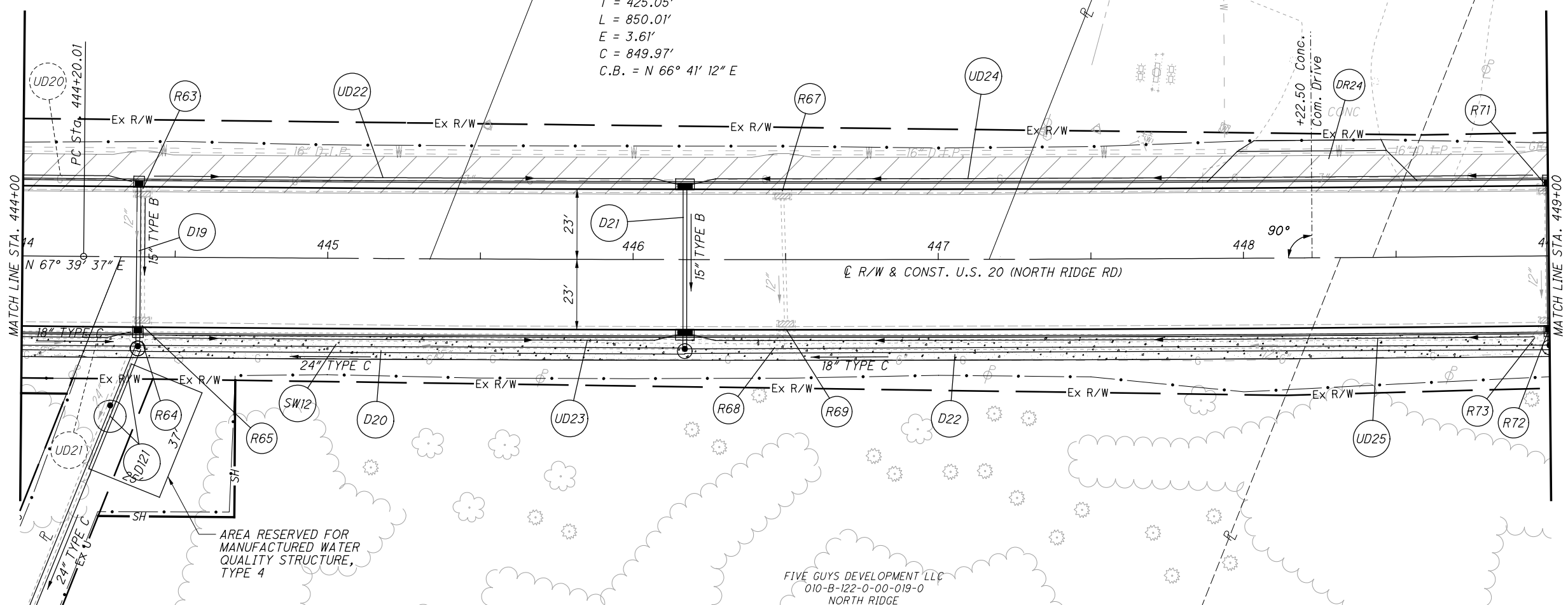
H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP008.dgn Sheet 7/15/2022 2:53:52 PM troyer

C & G PROPERTIES LLC
01-B-122-0-00-007-0
NORTH RIDGE

☉ CURVE DATA
P.I. Sta. 448+45.05
 $\Delta = 1^\circ 56' 50''$ (LT)
 $D_c = 0^\circ 13' 45''$
 $R = 25,009.74'$
 $T = 425.05'$
 $L = 850.01'$
 $E = 3.61'$
 $C = 849.97'$
C.B. = $N 66^\circ 41' 12'' E$

EDWIN & MARTHA IRVIN
01-B-122-0-00-074-0
NORTH RIDGE

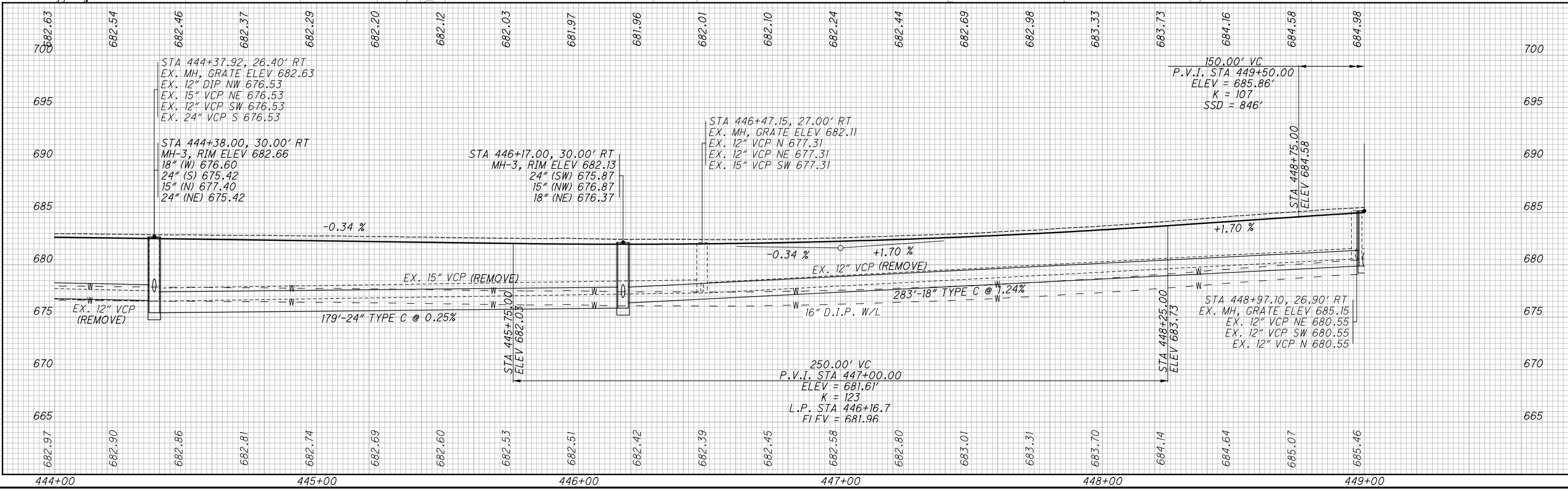
BELDING PROPERTIES LLC
01-B-122-0-00-068-0
NORTH RIDGE



AREA RESERVED FOR
MANUFACTURED WATER
QUALITY STRUCTURE,
TYPE 4

FIVE GUYS DEVELOPMENT LLC
010-B-122-0-00-019-0
NORTH RIDGE

TEMPORARY
PAVEMENT



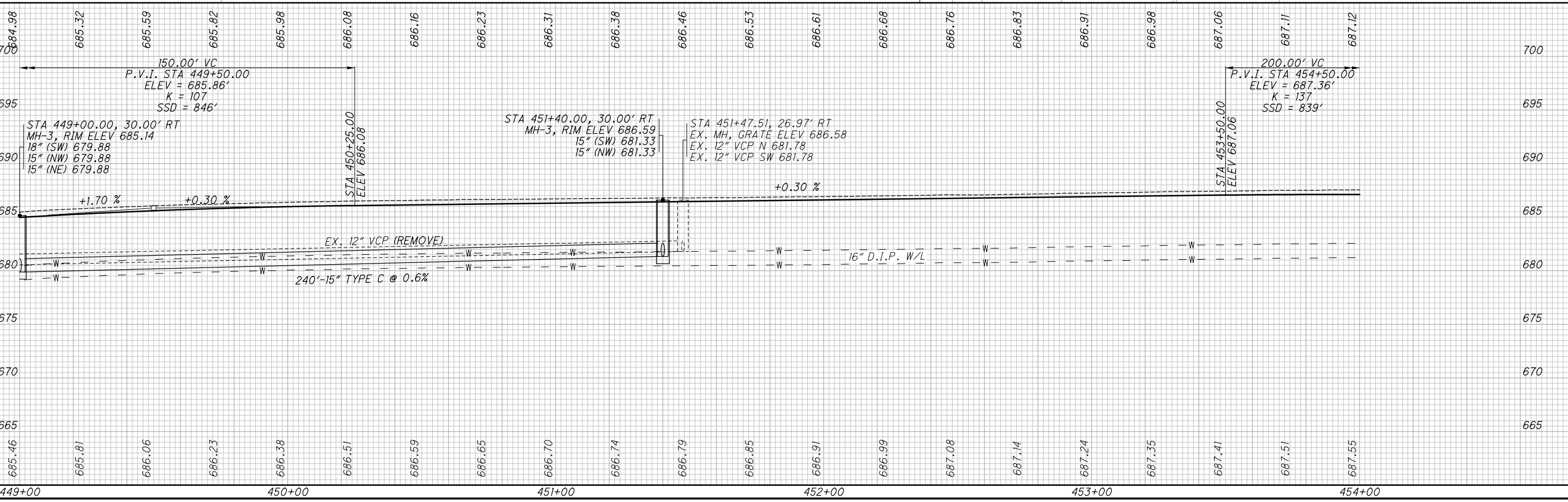
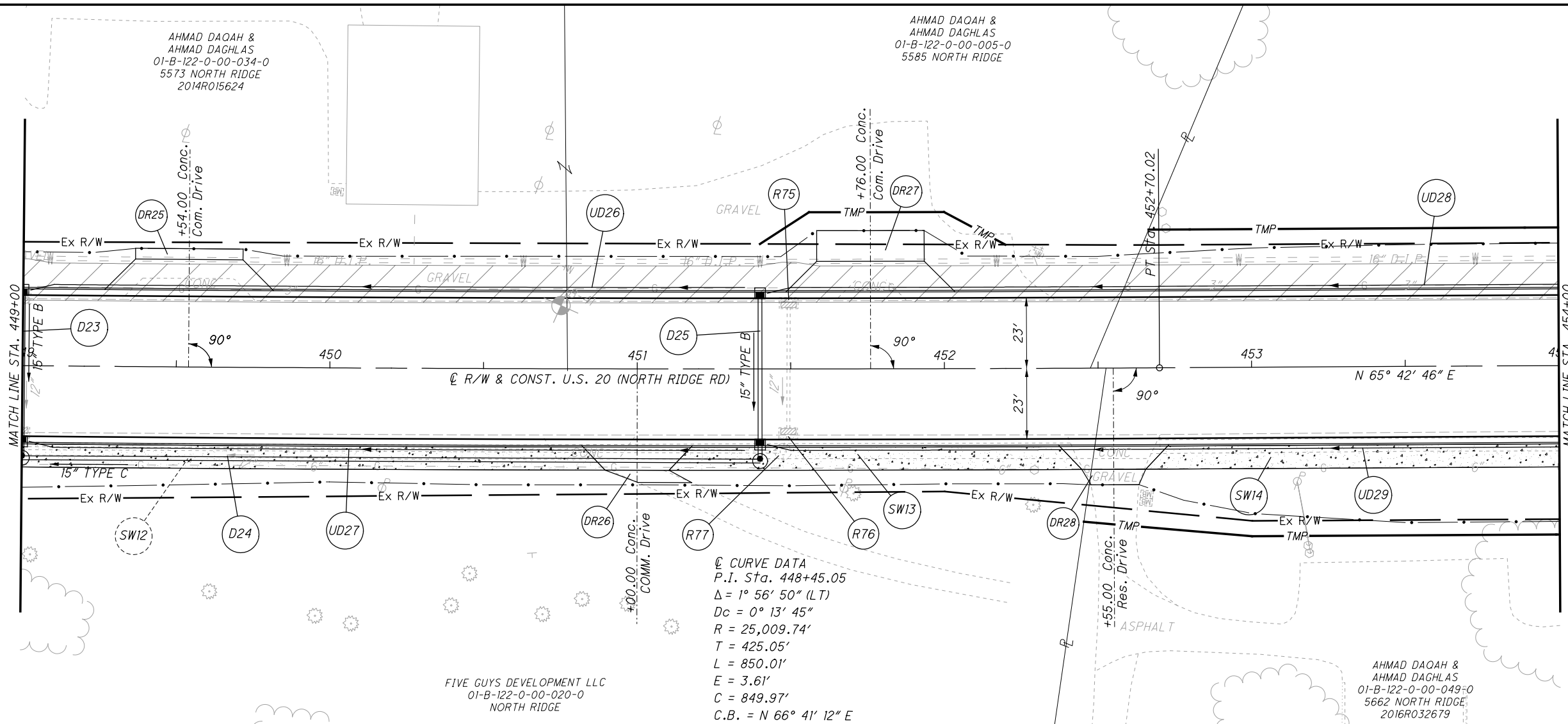
CALCULATED
JMP
CHECKED
WDB

PLAN AND PROFILE U.S. 20
STA. 444+00 TO STA. 449+00

LAK-US-20-24.99
PART 2

248
697

H:\2018\80743\0001\Project\Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP009.dgn Sheet 7/15/2022 2:53:52 PM troyer



PLAN AND PROFILE U.S. 20
STA. 449+00 TO STA. 454+00

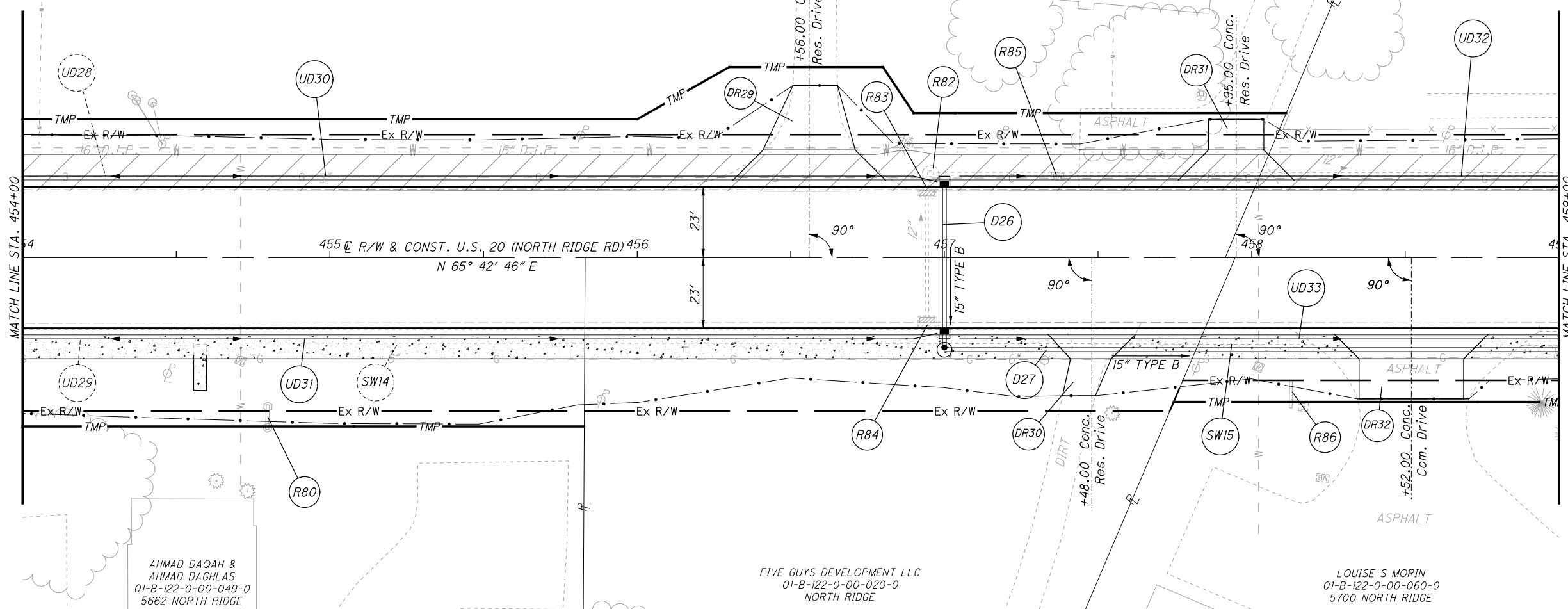
LAK-US-20-24.99
PART 2

THOMAS A & NANCY M LASKO
01-B-122-0-00-004-0
5673 NORTH RIDGE

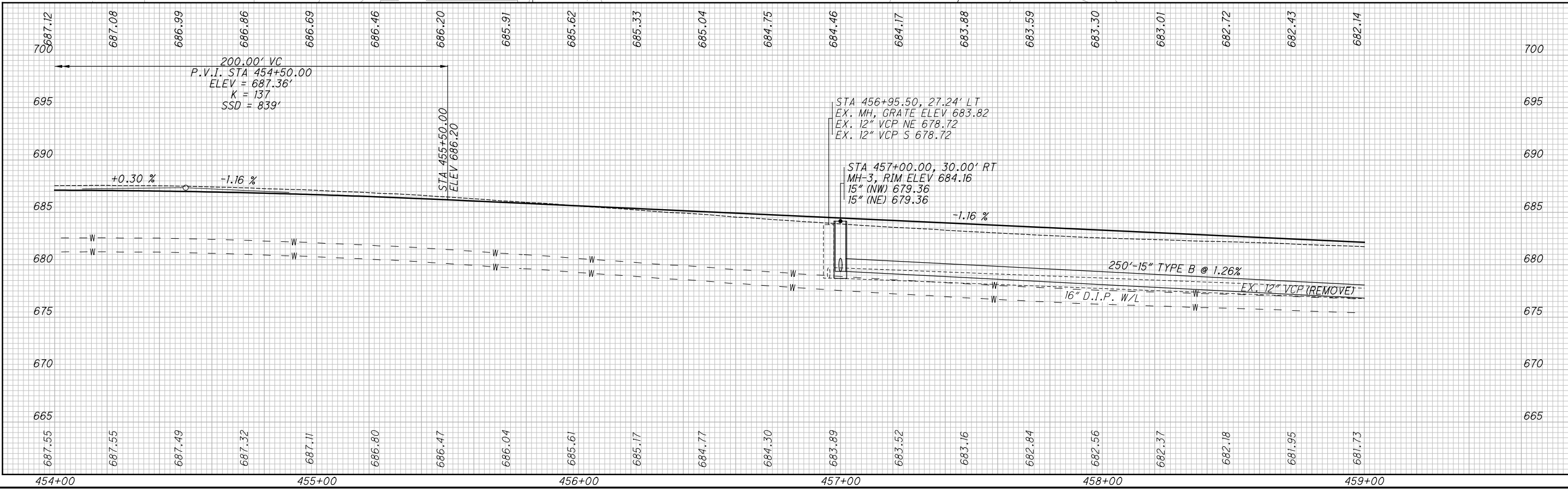
THOMAS J BUKKY
01-B-122-0-00-003-0
NORTH RIDGE



CALCULATED
JMP
CHECKED
WDB



TEMPORARY PAVEMENT



**PLAN AND PROFILE U.S. 20
STA. 454+00 TO STA. 459+00**

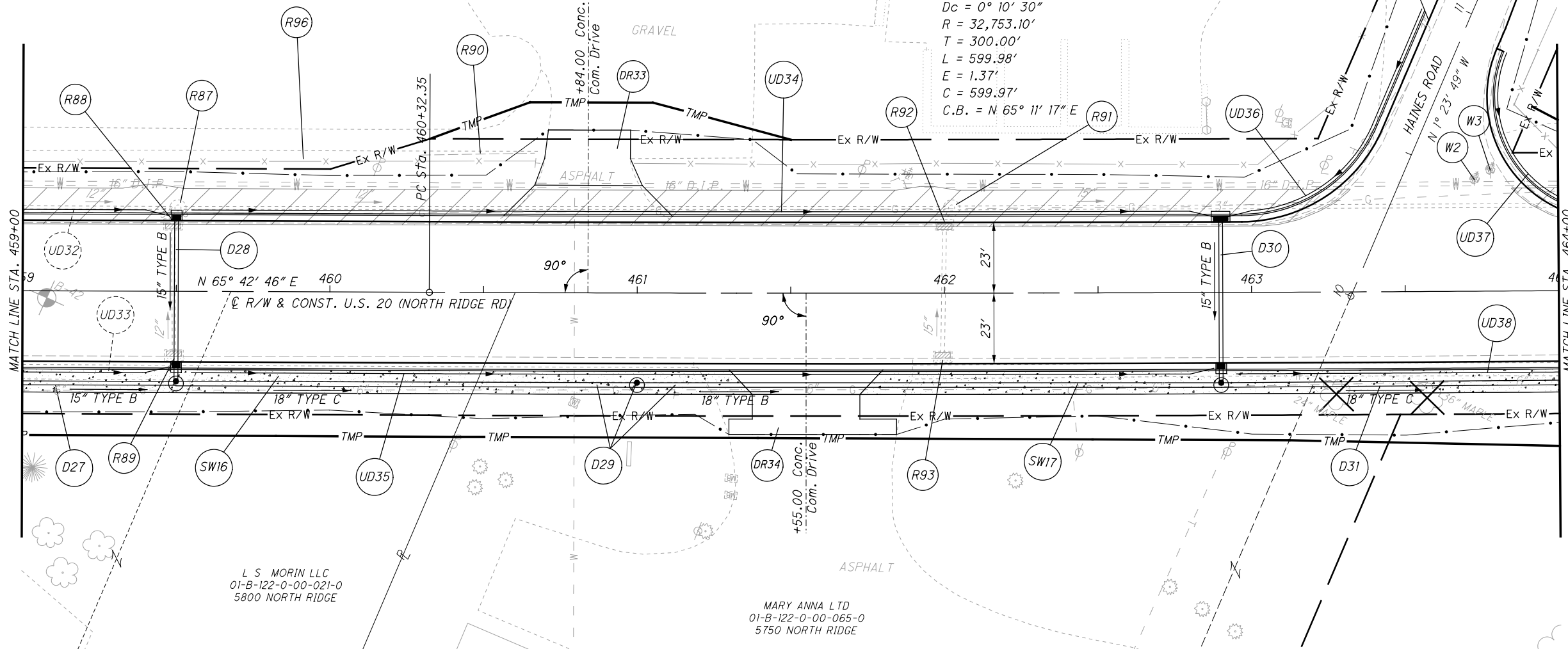
**LAK-US-20-24.99
PART 2**

250
697

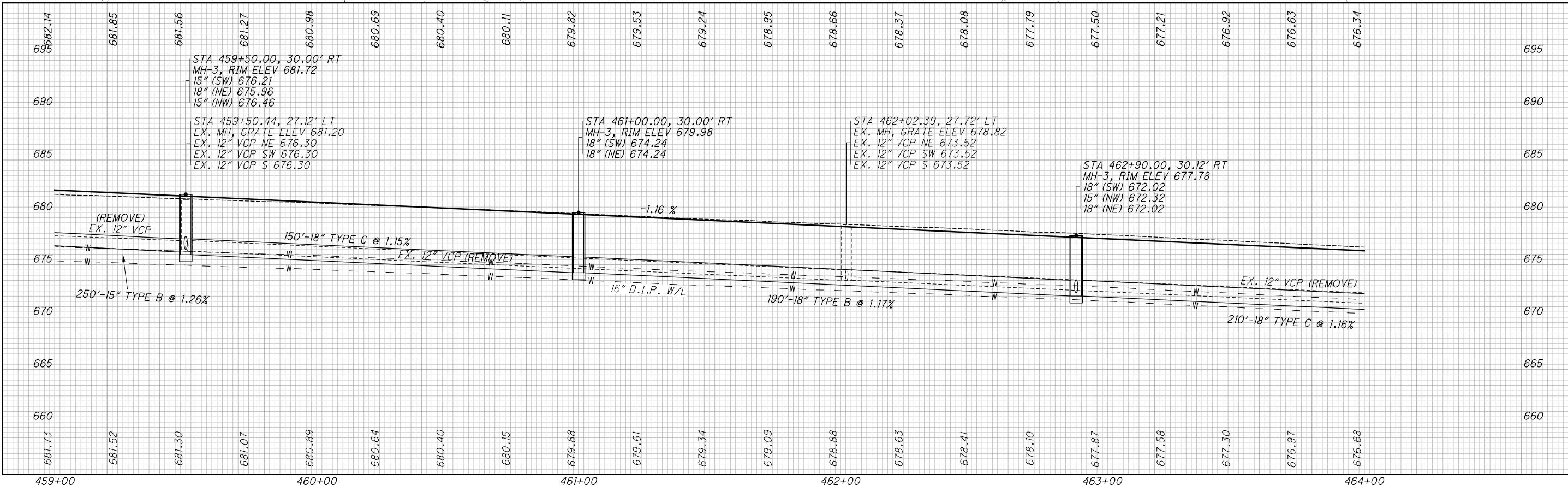
H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP010.dgn Sheet 7/15/2022 2:53:53 PM troyer

THOMAS J BUKKY
01-B-122-0-00-003-0
NORTH RIDGE

☉ CURVE DATA
P.I. Sta. 463+32.35
 $\Delta = 1^\circ 02' 58''$ (LT)
 $D_c = 0^\circ 10' 30''$
 $R = 32,753.10'$
 $T = 300.00'$
 $L = 599.98'$
 $E = 1.37'$
 $C = 599.97'$
C.B. = $N 65^\circ 11' 17'' E$



TEMPORARY PAVEMENT



CALCULATED
JMP
CHECKED
WDB

PLAN AND PROFILE U.S. 20
STA. 459+00 TO STA. 464+00

LAK-US-20-24.99
PART 2

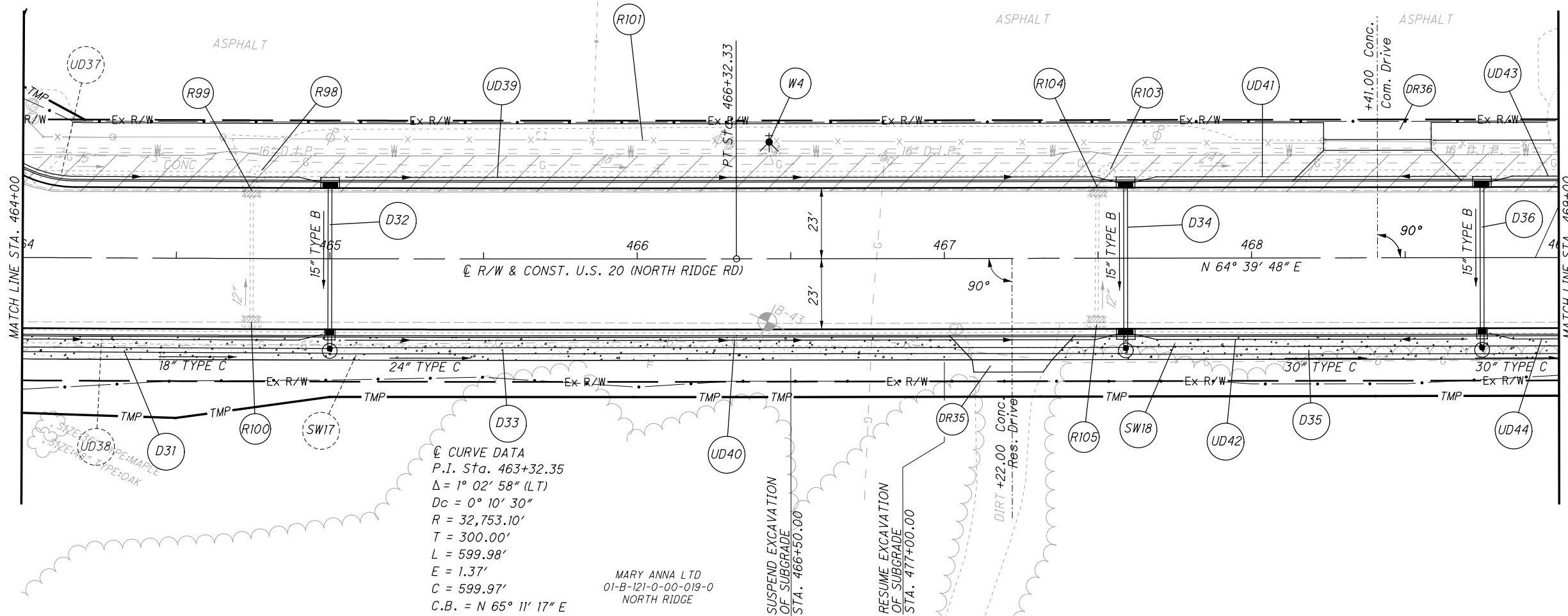
251
697

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP01.dgn Sheet 7/15/2022 2:53:54 PM troyer

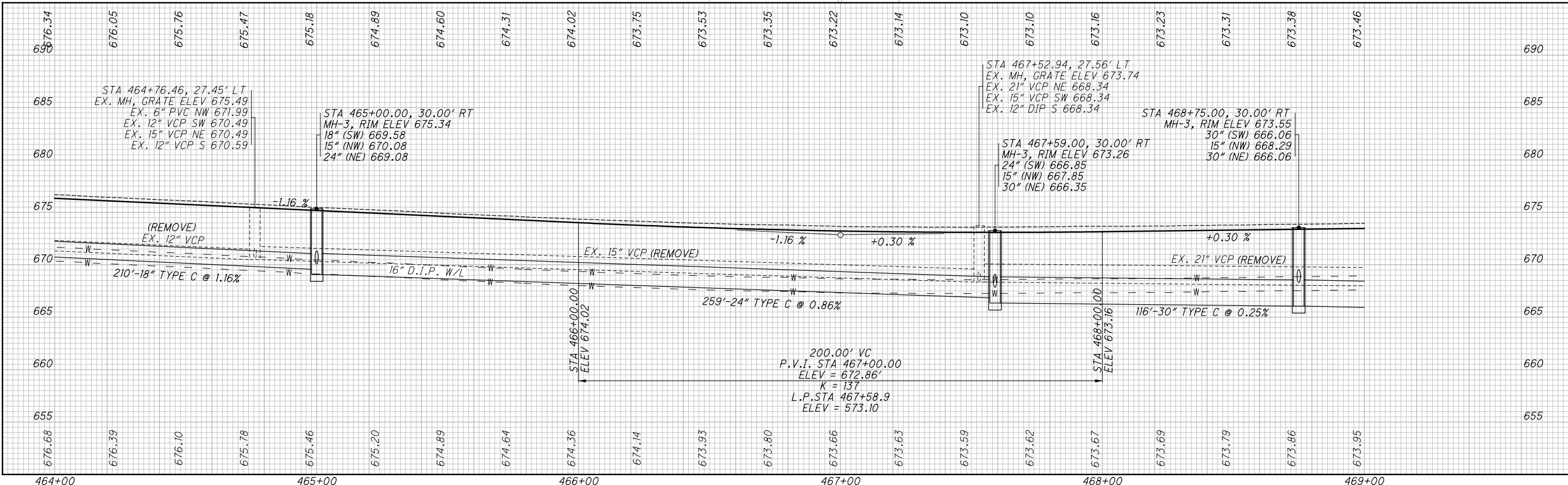
LAKE MADISON LLC
01-B-126-0-00-021-0
5745 NORTH RIDGE



0 10 20 40
HORIZONTAL SCALE IN FEET
CALCULATED JIM
CHECKED WDB



TEMPORARY PAVEMENT



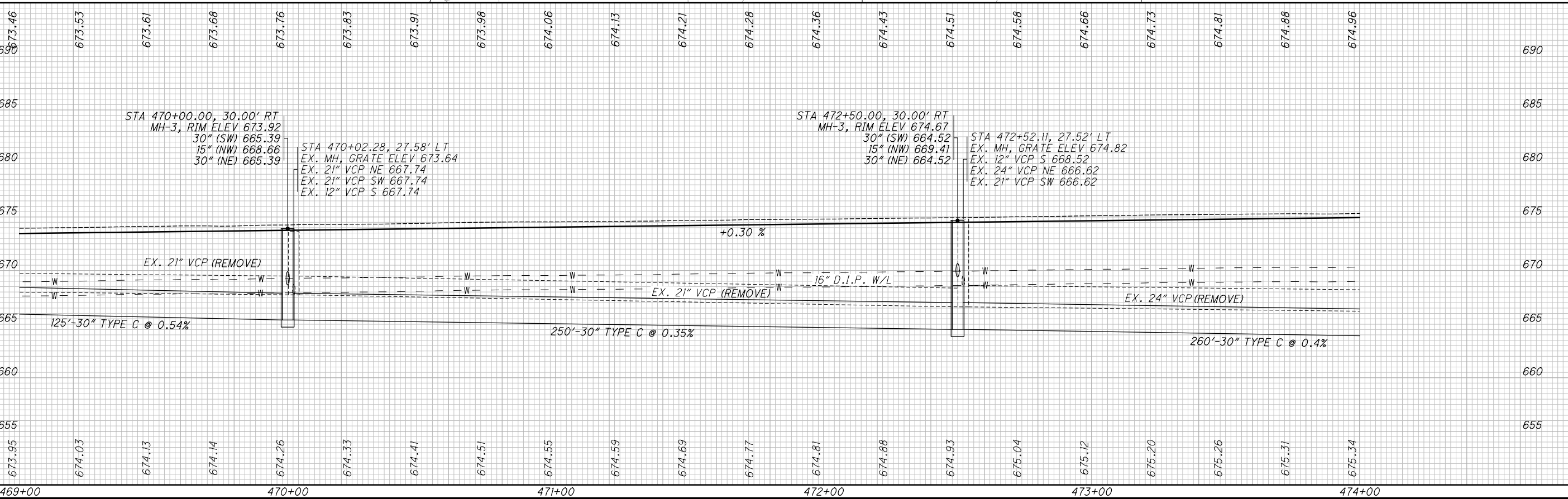
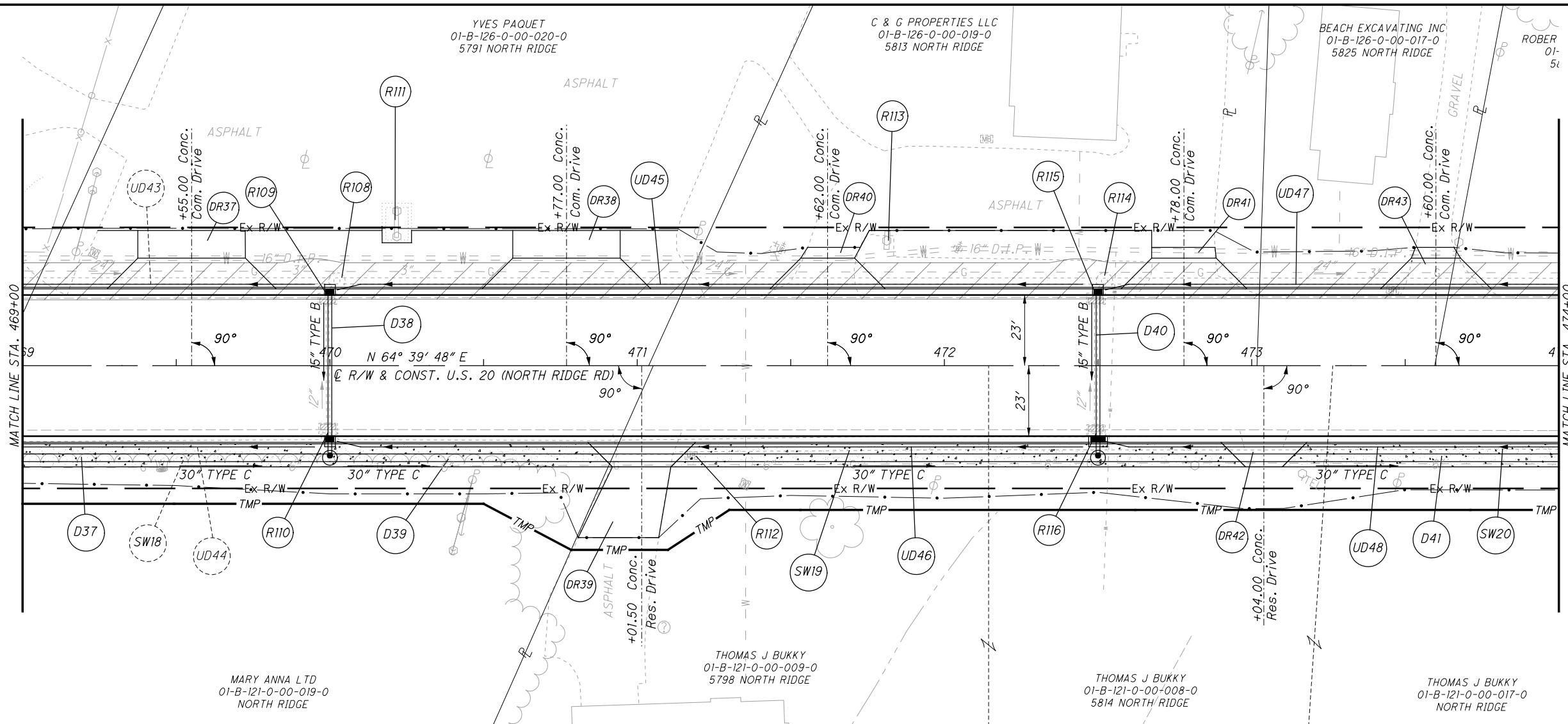
PLAN AND PROFILE U.S. 20
STA. 464+00 TO STA. 469+00

LAK-US-20-24.99
PART 2

252
697

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP012.dgn Sheet 7/15/2022 2:53:54 PM Troyer

H:\2018\80743\000\ProjectData\0922\0_LAK_20_EAST\Design\Roadway\Sheets\0922\0_CP013.dgn Sheet 7/15/2022 2:53:55 PM Troyer



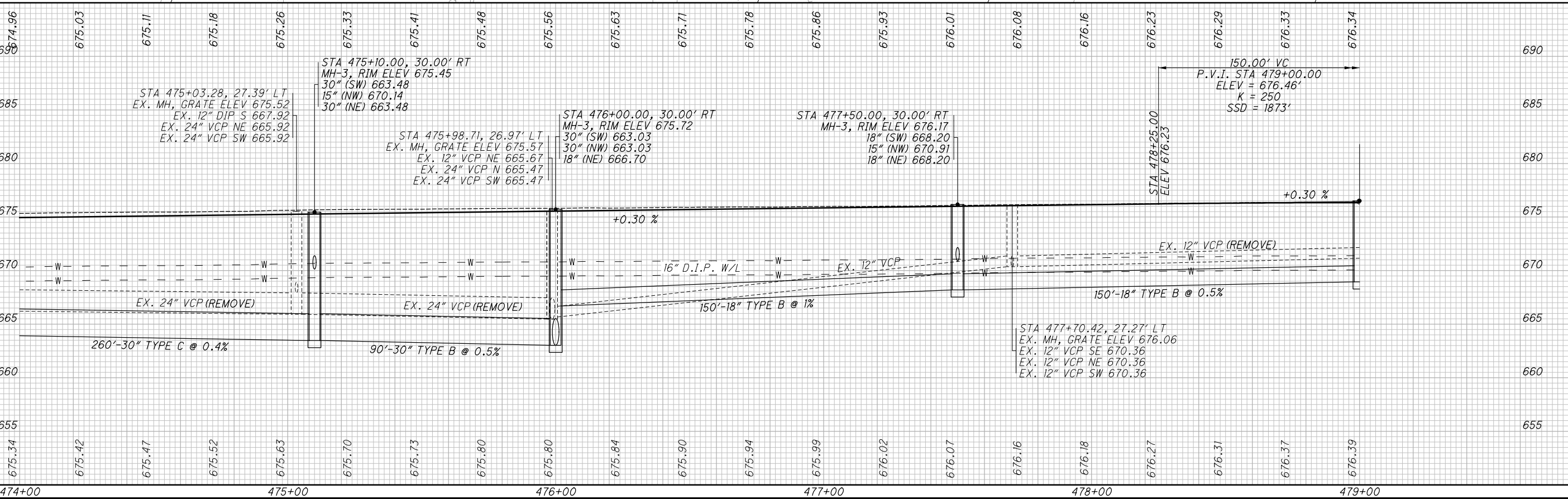
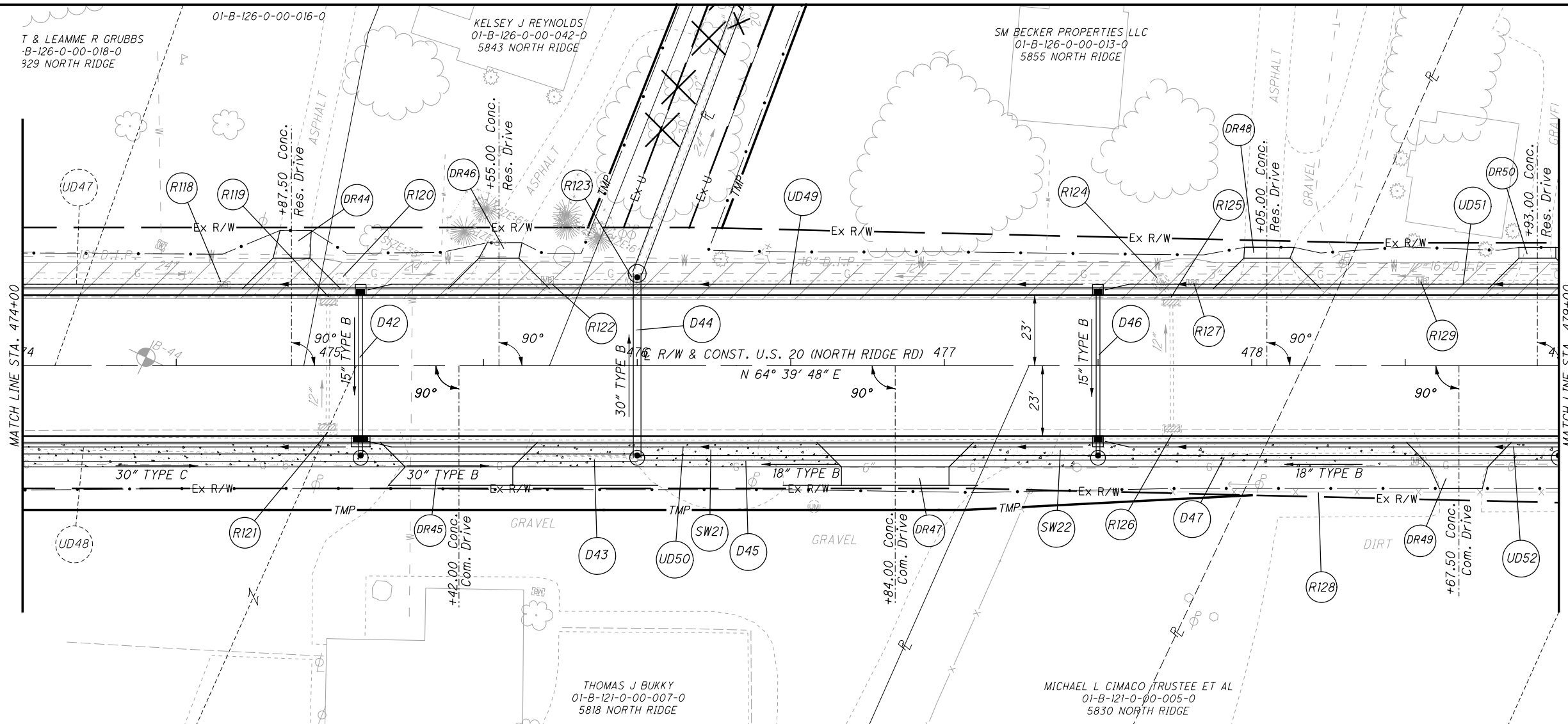
CALCULATED
JMP
CHECKED
WDB

**PLAN AND PROFILE U.S. 20
STA. 469+00 TO STA. 474+00**

**LAK-US-20-24.99
PART 2**

TEMPORARY PAVEMENT

H:\2018\80743\0001\ProjectData\092270_LAK_20_EAST\Design\Roadway\Sheets\092270_CP014.dgn Sheet 7/15/2022 2:53:56 PM troyer



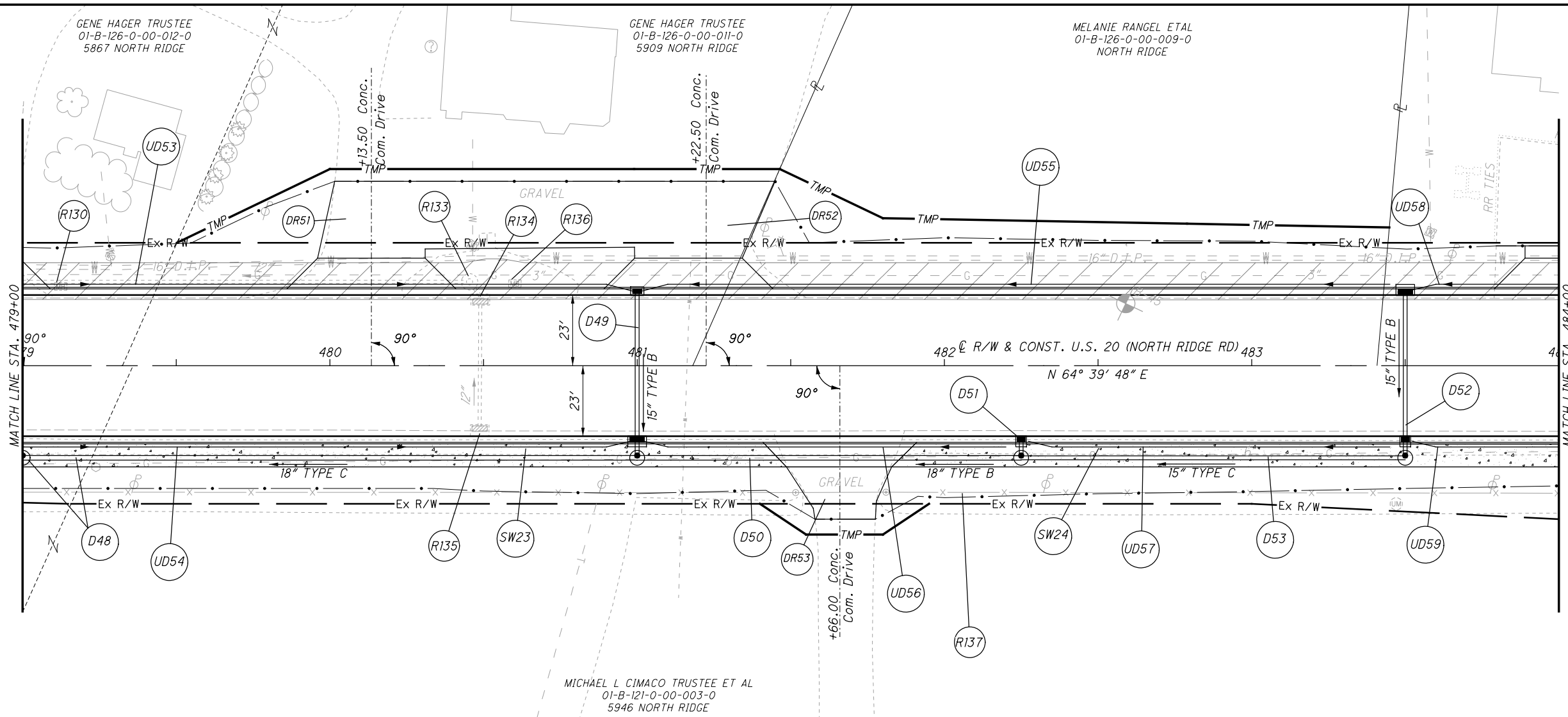
CALCULATED
JMP
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

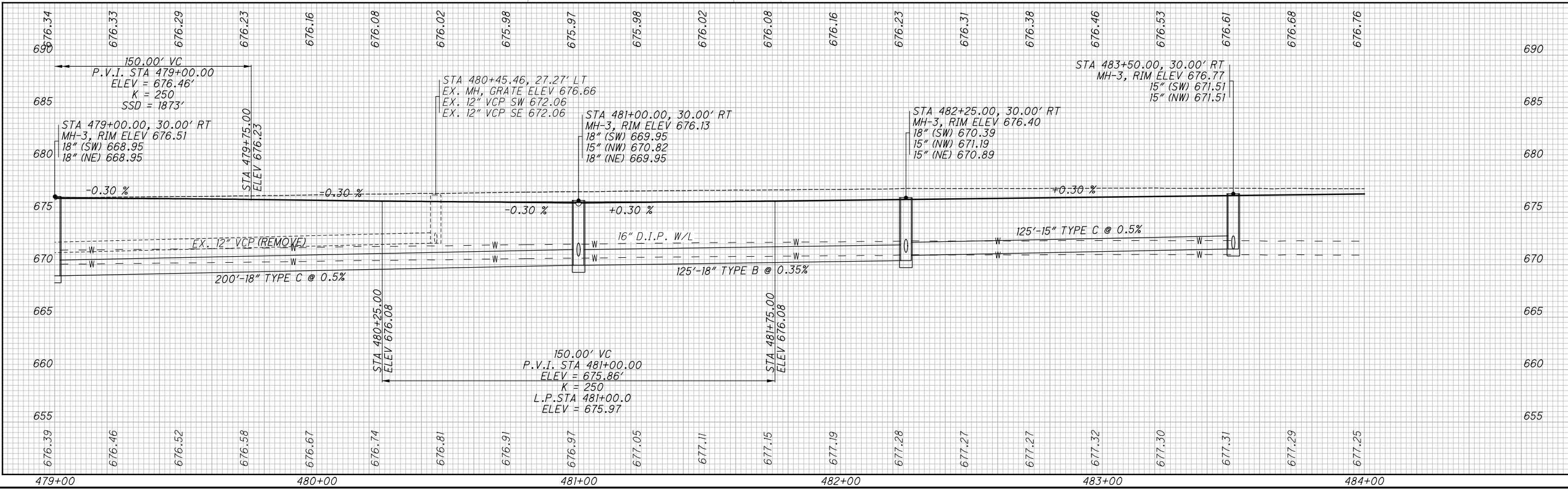
**PLAN AND PROFILE U.S. 20
STA. 474+00 TO STA. 479+00**

**LAK-US-20-24.99
PART 2**

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP015.dgn Sheet 7/15/2022 2:53:57 PM Troyer



TEMPORARY PAVEMENT



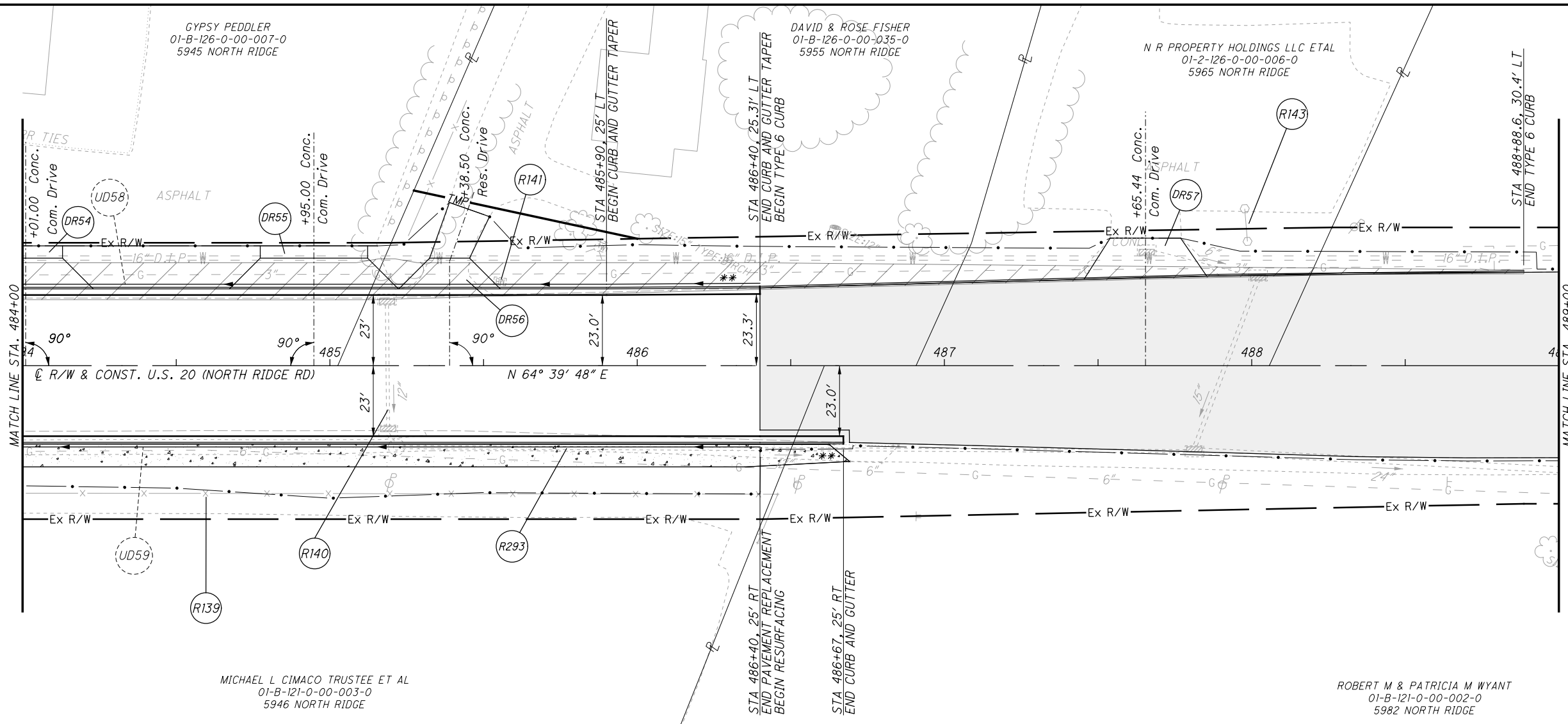
CALCULATED	0
JMP	
CHECKED	
WDB	

**PLAN AND PROFILE U.S. 20
STA. 479+00 TO STA. 484+00**

**LAK-US-20-24.99
PART 2**

255
697

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP016.dgn Sheet 7/15/2022 2:53:57 PM troyer



** VARY GUTTER CROSS SLOPE FROM 0.083 TO 0.016 IN 40.0'

RESURFACING AREA
TEMPORARY PAVEMENT



CALCULATED 0
JMP
CHECKED
WDB

PLAN AND PROFILE U.S. 20
STA. 484+00 TO STA. 489+00

LAK-US-20-24.99
PART 2

256
697

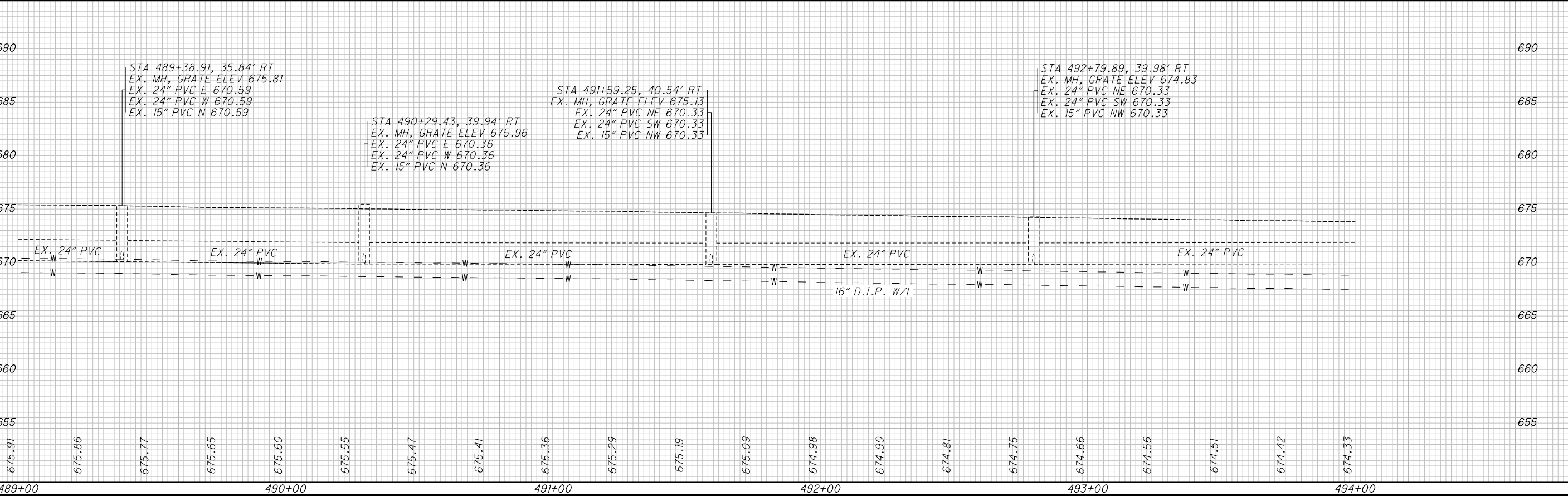
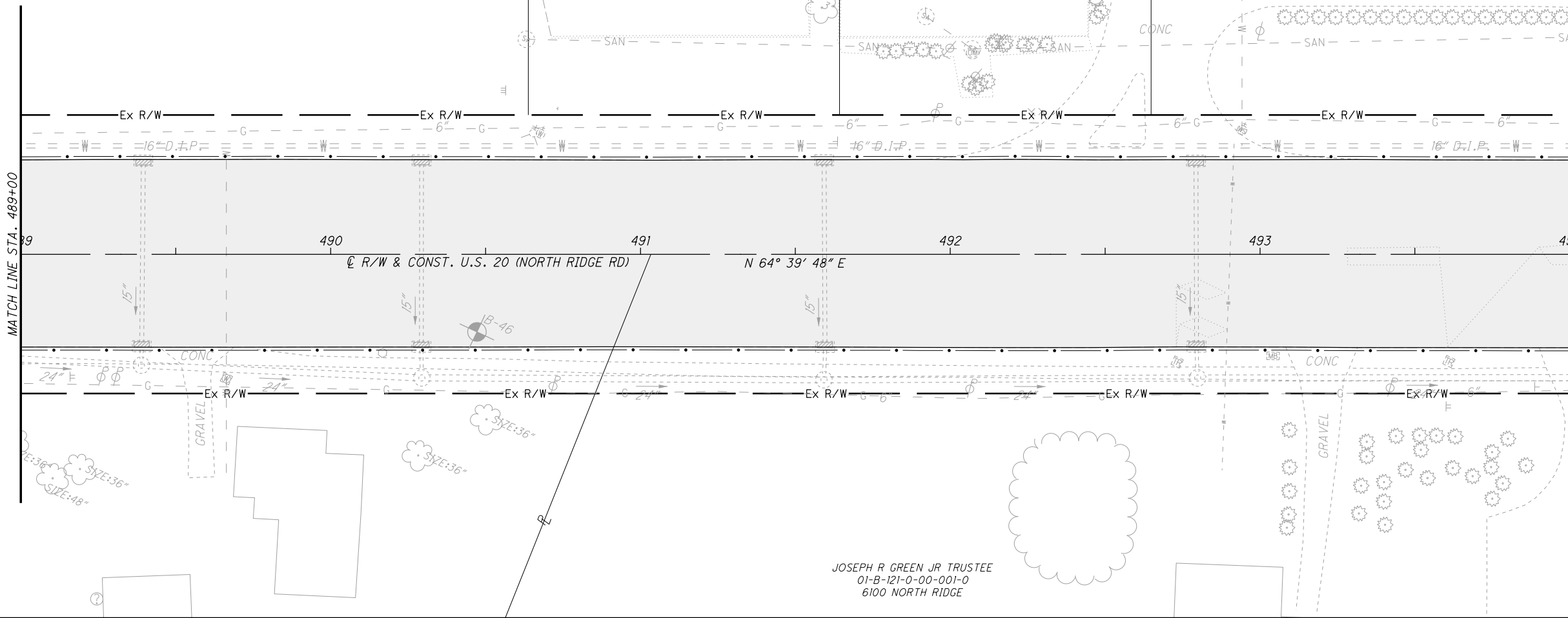
GREEN MADISON LLC
01-B-126-0-00-005-0
GREEN ROAD

GRHH CSOK LLC ET AL
01-B-126-0-00-059-0
NORTH RIDGE RD

HRM CSOK LLC ET AL
01-B-126-0-00-060-0
5995 NORTH RIDGE RD

MACS CONVENIENCE STORES LLC
01-B-126-0-00-0056-0
2014R024865
GREEN ROAD

JOSEPH R GREEN JR TRUSTEE
01-B-121-0-00-001-0
6100 NORTH RIDGE



CALCULATED
JMP
CHECKED
WDB

**PLAN AND PROFILE U.S. 20
STA. 489+00 TO STA. 494+00**

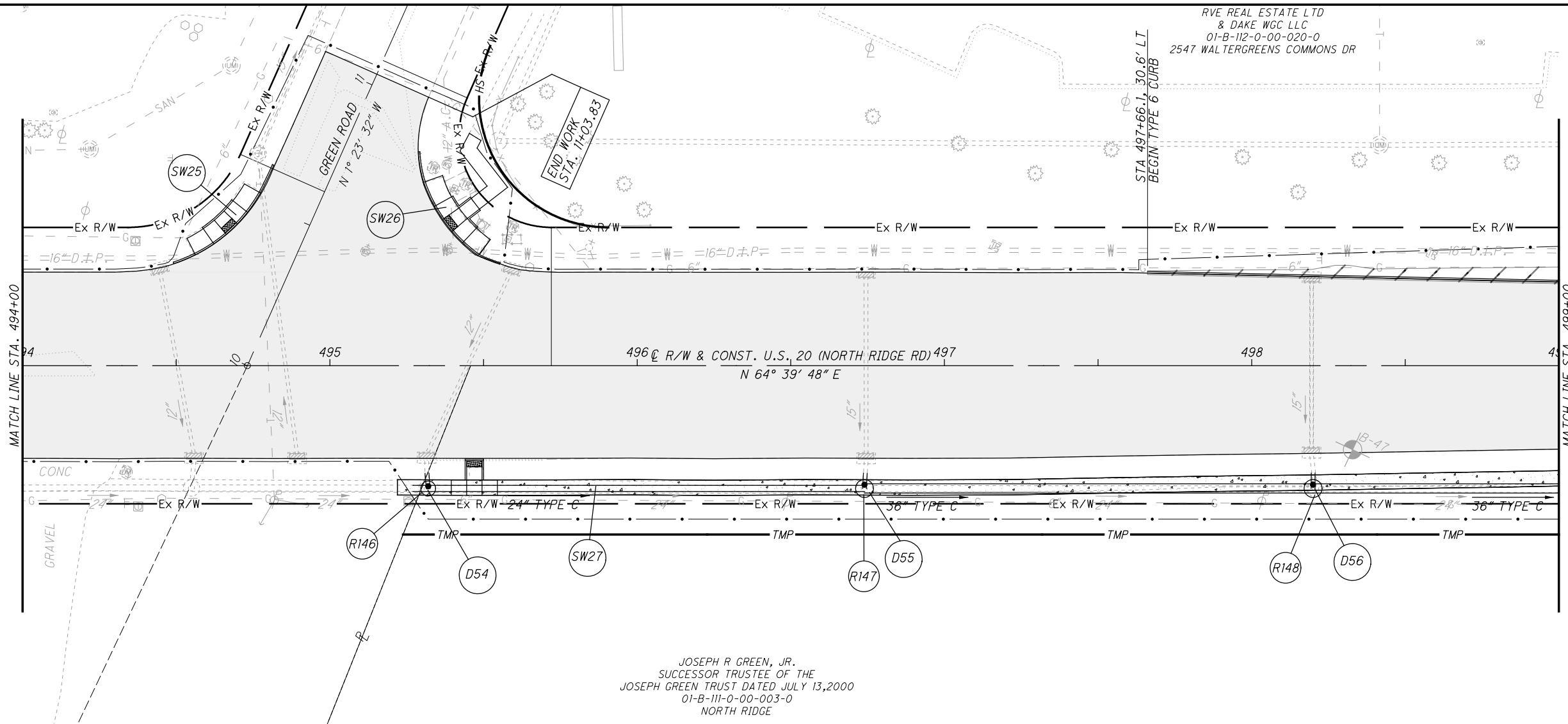
**LAK-US-20-24.99
PART 2**

257
697

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP017.dgn Sheet 7/15/2022 2:53:58 PM Troyer

RESURFACING AREA

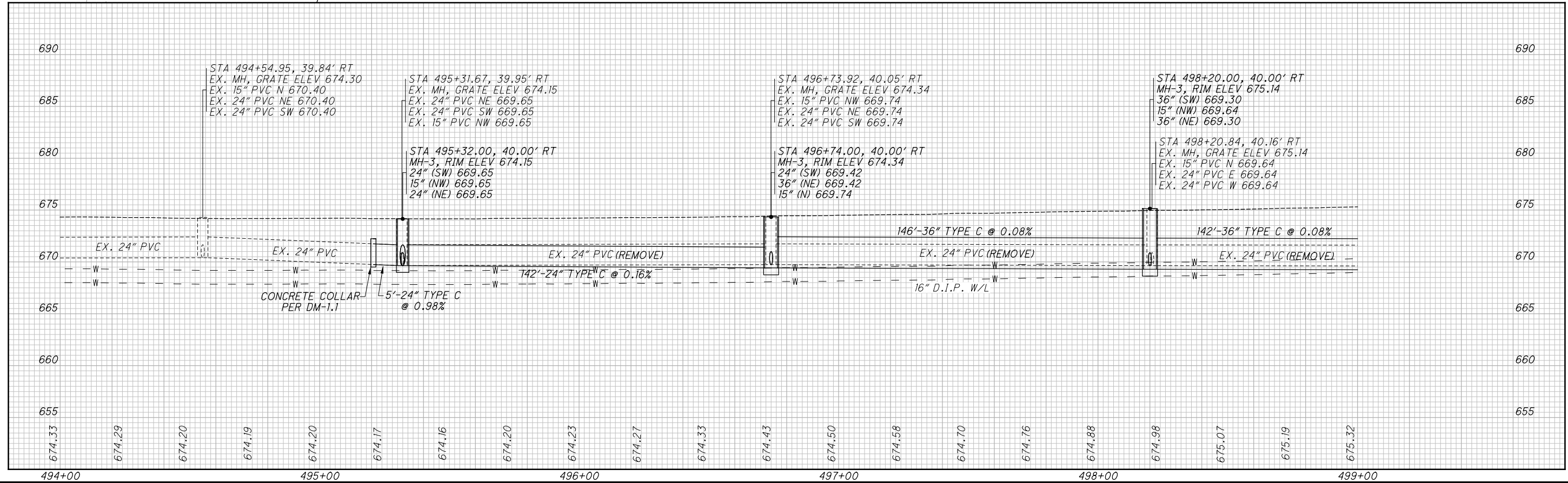
H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP08.dgn Sheet 7/15/2022 2:54:04 PM troyer



JOSEPH R GREEN, JR.
 SUCCESSOR TRUSTEE OF THE
 JOSEPH GREEN TRUST DATED JULY 13, 2000
 01-B-111-0-00-003-0
 NORTH RIDGE

RVE REAL ESTATE LTD
 & DAKE WGC LLC
 01-B-112-0-00-020-0
 2547 WALTERGREENS COMMONS DR

RESURFACING AREA
 TEMPORARY PAVEMENT



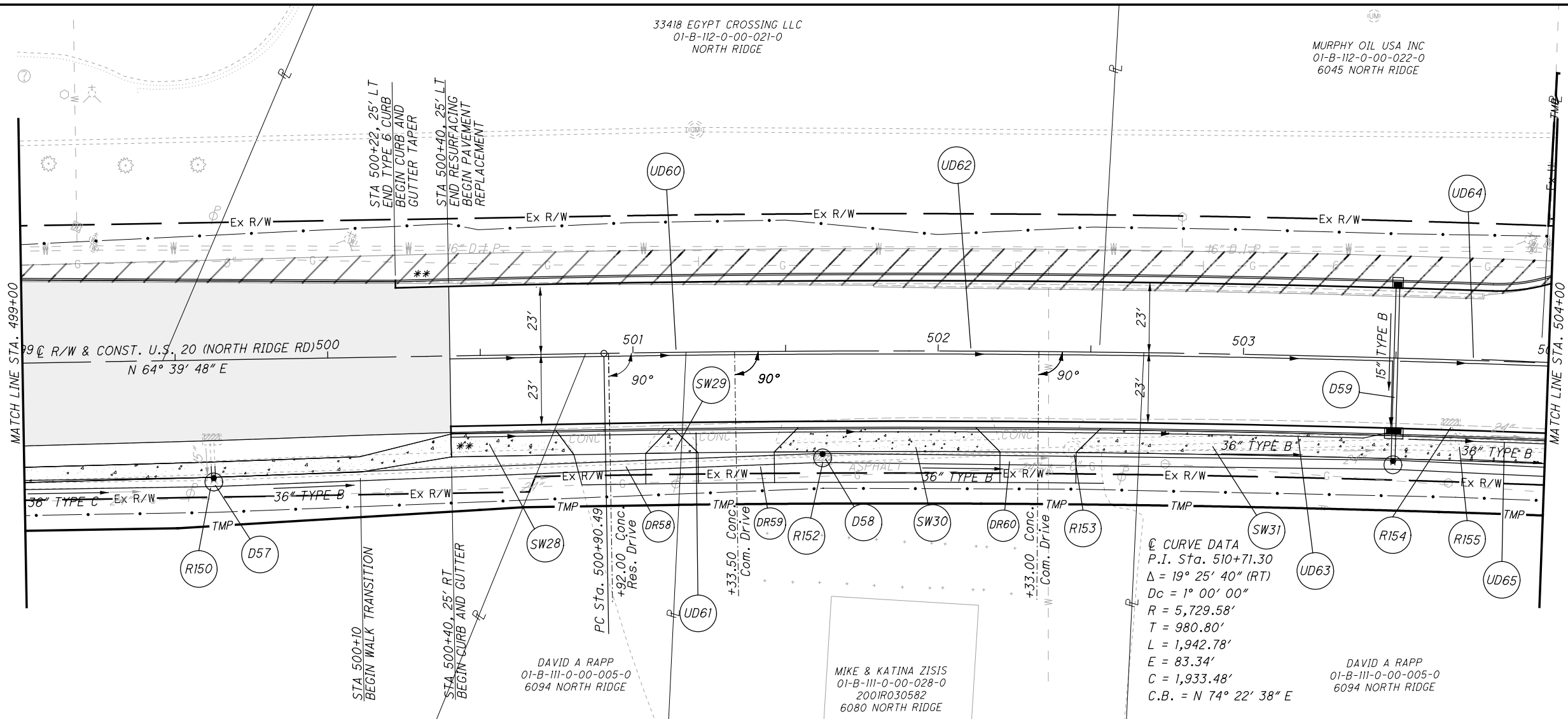
CALCULATED
 JUMP
 CHECKED
 WDB

PLAN AND PROFILE U.S. 20
 STA. 494+00 TO STA. 499+00

LAK-US-20-24.99
 PART 2

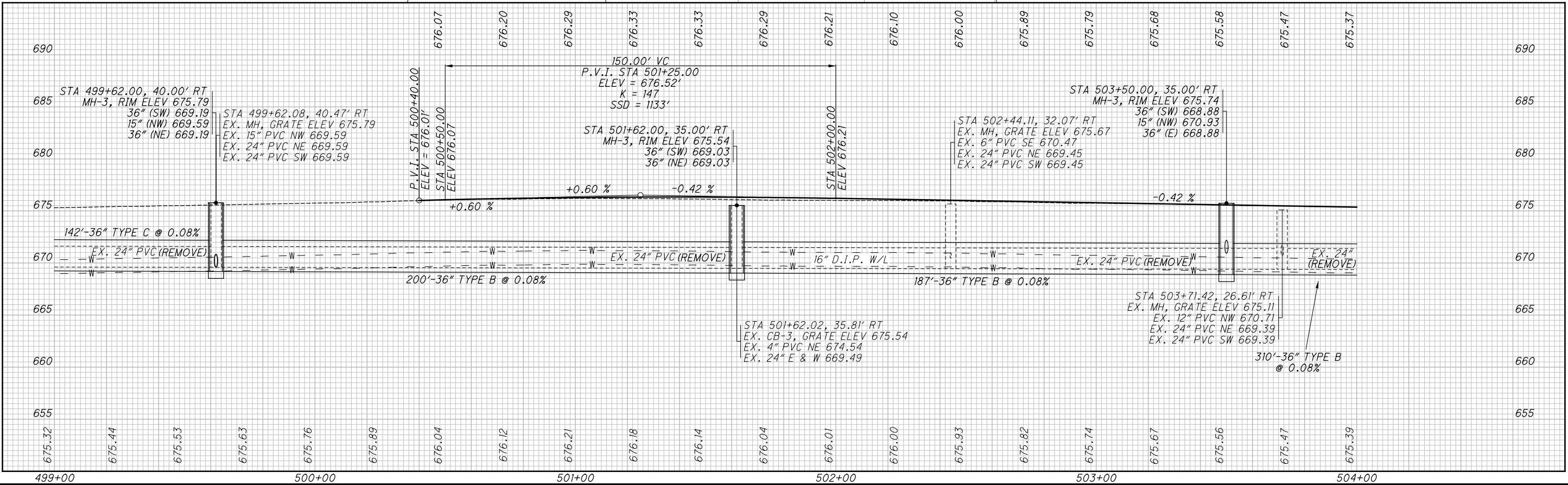
258
 697

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP019.dgn Sheet 7/15/2022 2:54:05 PM troyer



** VARY GUTTER CROSS SLOPE FROM 0.083 TO 0.016 IN 30.0'

RESURFACING AREA
TEMPORARY PAVEMENT

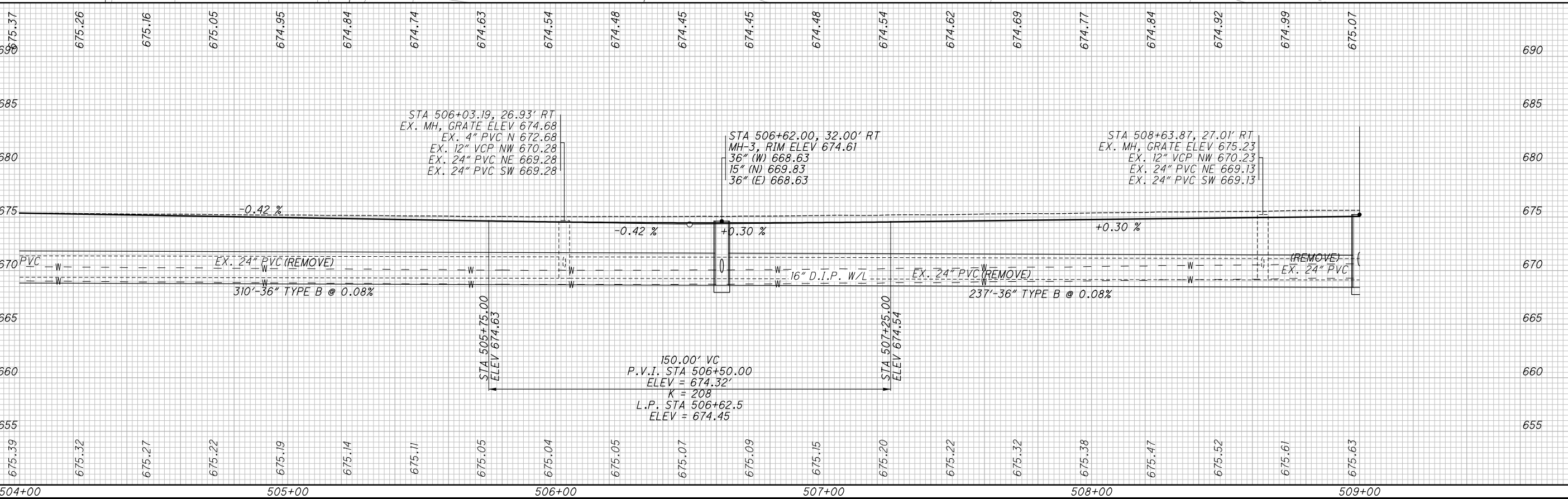
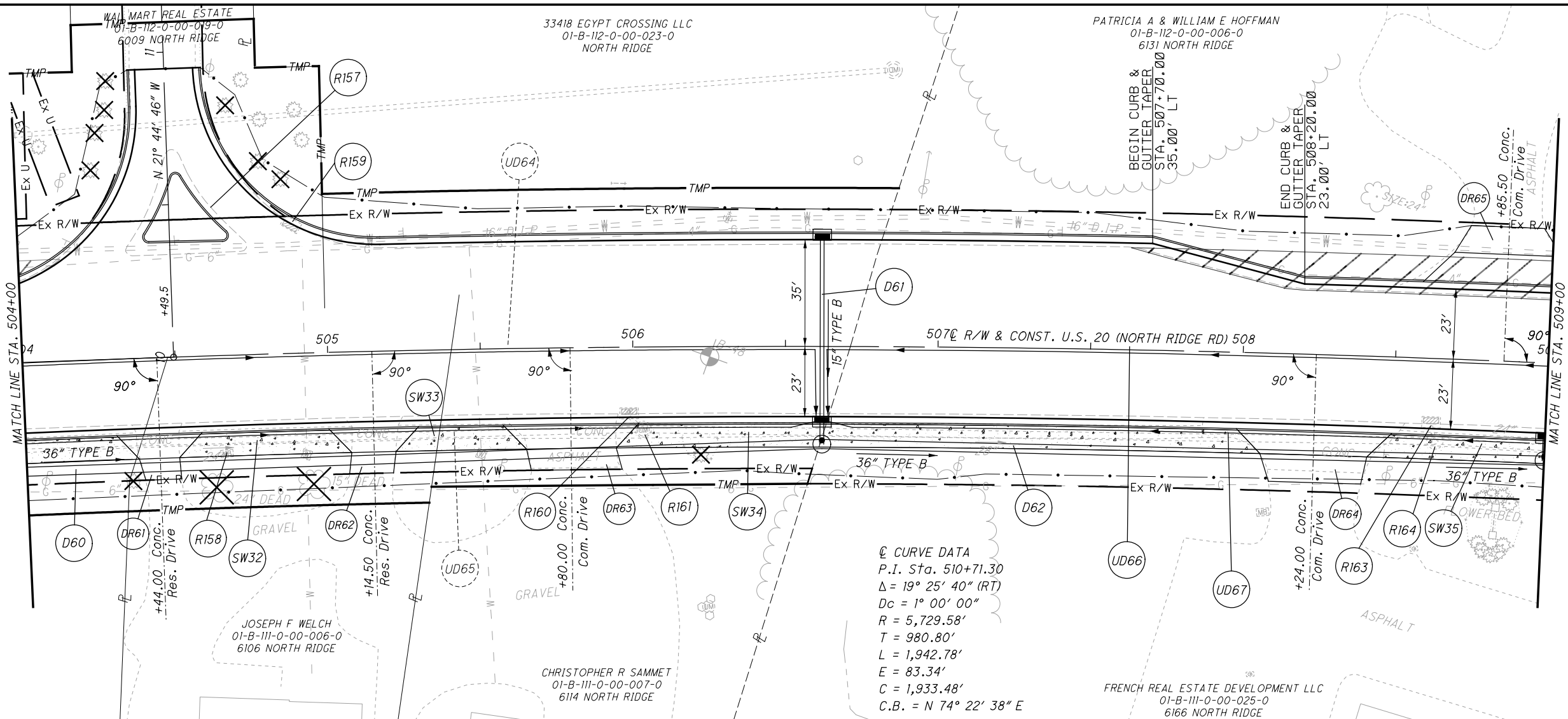


CALCULATED JWP
CHECKED WDB

PLAN AND PROFILE U.S. 20
STA. 499+00 TO STA. 504+00

LAK-US-20-24.99
PART 2

H:\2018\80743\0001\ProjectData\092270_LAK_20_EAST\Design\Roadway\Sheets\092270_GP020.dgn Sheet 7/15/2022 2:54:06 PM troyer



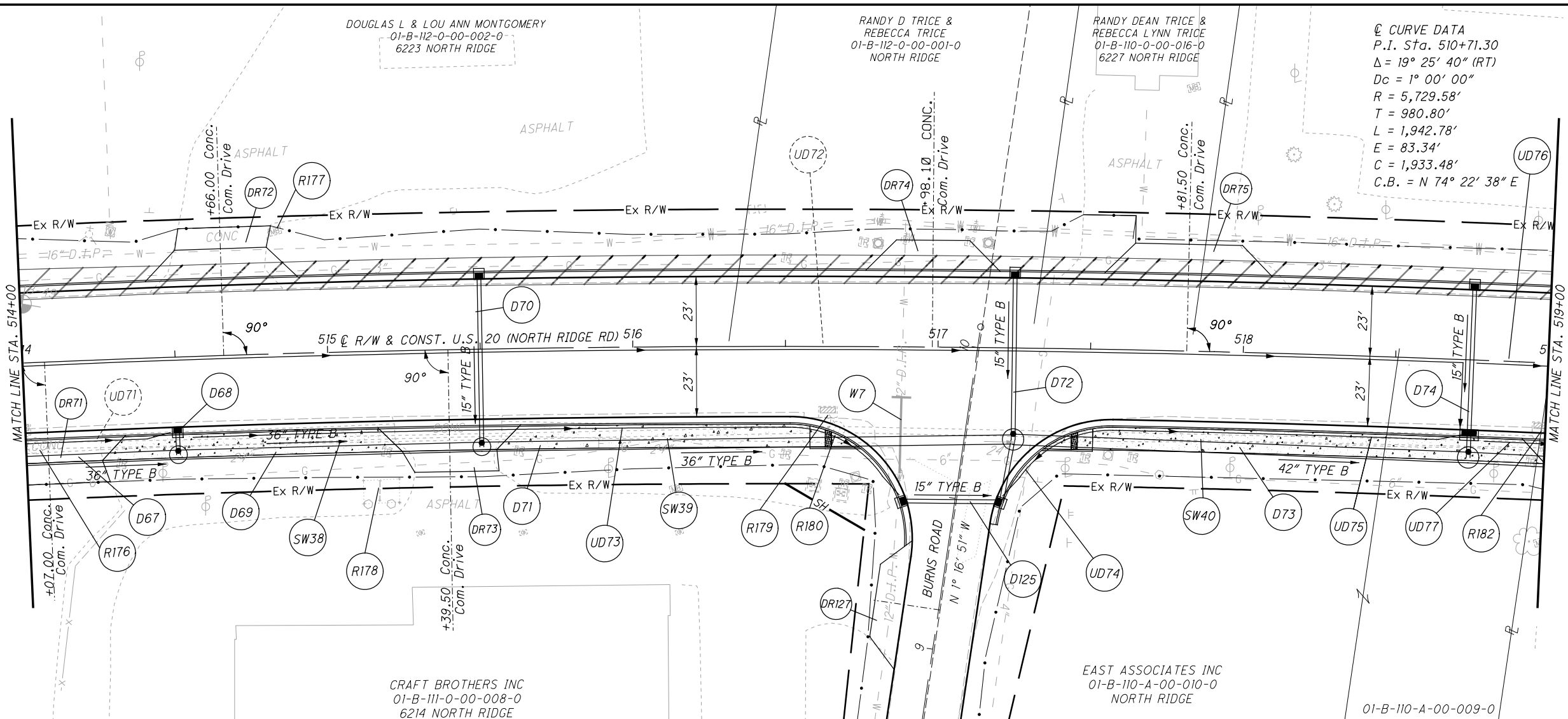
CALCULATED
JMP
CHECKED
WDB

**PLAN AND PROFILE U.S. 20
STA. 504+00 TO STA. 509+00**

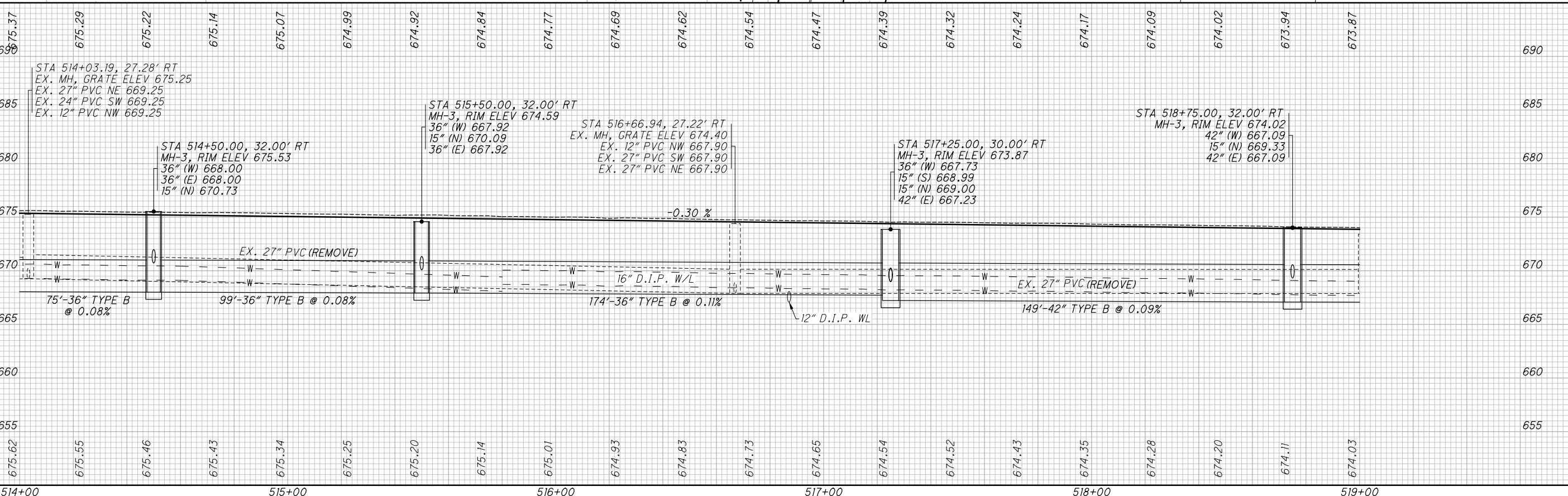
**LAK-US-20-24.99
PART 2**

260
697

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP022.dgn Sheet 7/15/2022 2:54:07 PM troyer



CURVE DATA
 P.I. Sta. 510+71.30
 $\Delta = 19^\circ 25' 40''$ (RT)
 $D_c = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 980.80'$
 $L = 1,942.78'$
 $E = 83.34'$
 $C = 1,933.48'$
 $C.B. = N 74^\circ 22' 38'' E$



PLAN AND PROFILE U.S. 20
 STA. 514+00 TO STA. 519+00

LAK-US-20-24.99
 PART 2

262
 697

CALCULATED
 JUMP
 CHECKED
 WDB

TEMPORARY PAVEMENT

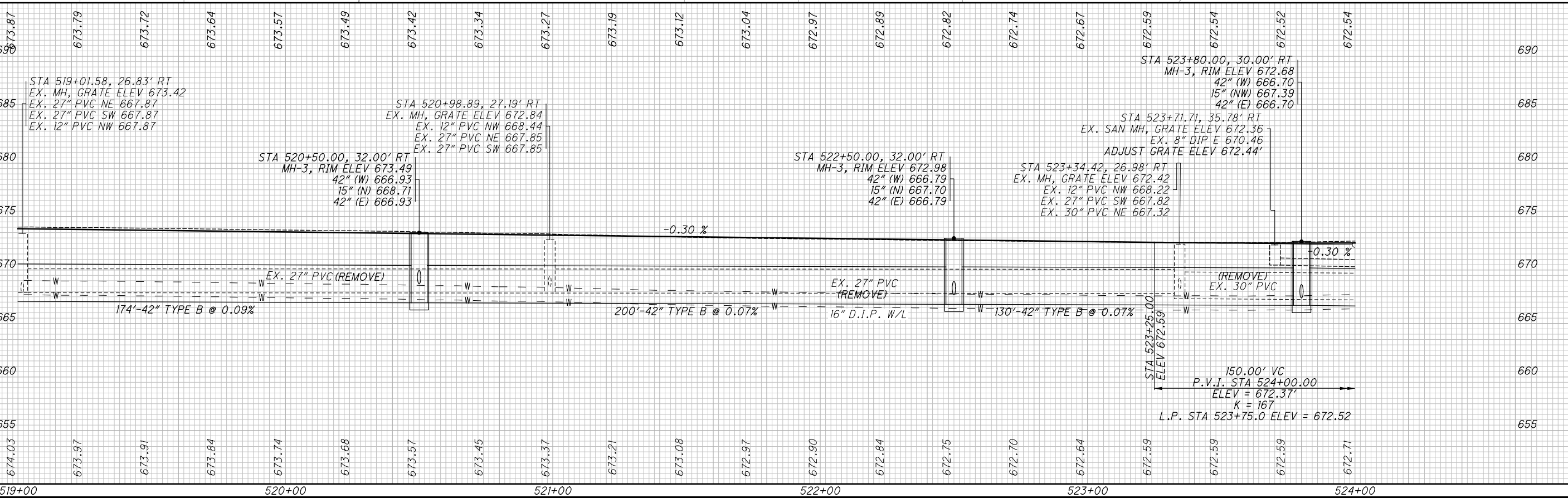
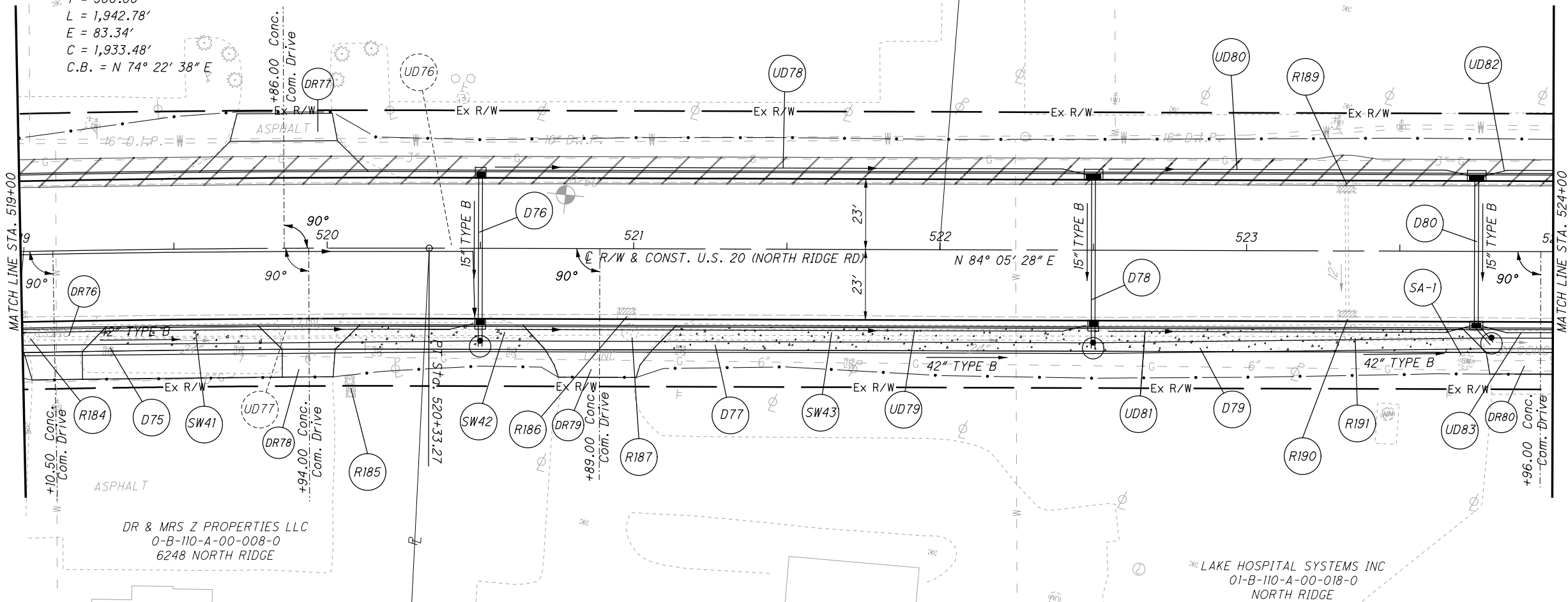
@ CURVE DATA
 P.I. Sta. 510+71.30
 $\Delta = 19^\circ 25' 40''$ (RT)
 $D_c = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 980.80'$
 $L = 1,942.78'$
 $E = 83.34'$
 $C = 1,933.48'$
 $C.B. = N 74^\circ 22' 38'' E$

@ MADISON ROUTE 20 PROPERTIES LLC
 01-B-110-0-00-013-0
 6251 NORTH RIDGE

@ MADISON ROUTE 20 PROPERTIES LLC
 01-B-110-0-00-011-0
 6277 NORTH RIDGE

@ DR & MRS Z PROPERTIES LLC
 0-B-110-A-00-008-0
 6248 NORTH RIDGE

@ LAKE HOSPITAL SYSTEMS INC
 01-B-110-A-00-018-0
 NORTH RIDGE



CALCULATED
 JUMP
 CHECKED
 WDB

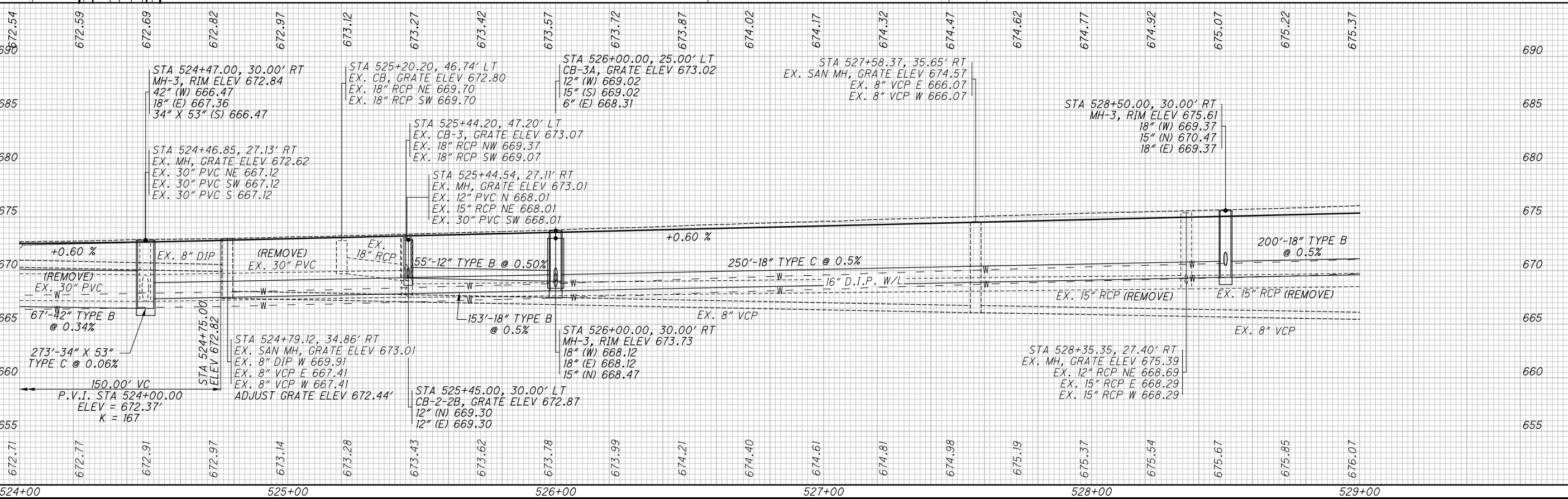
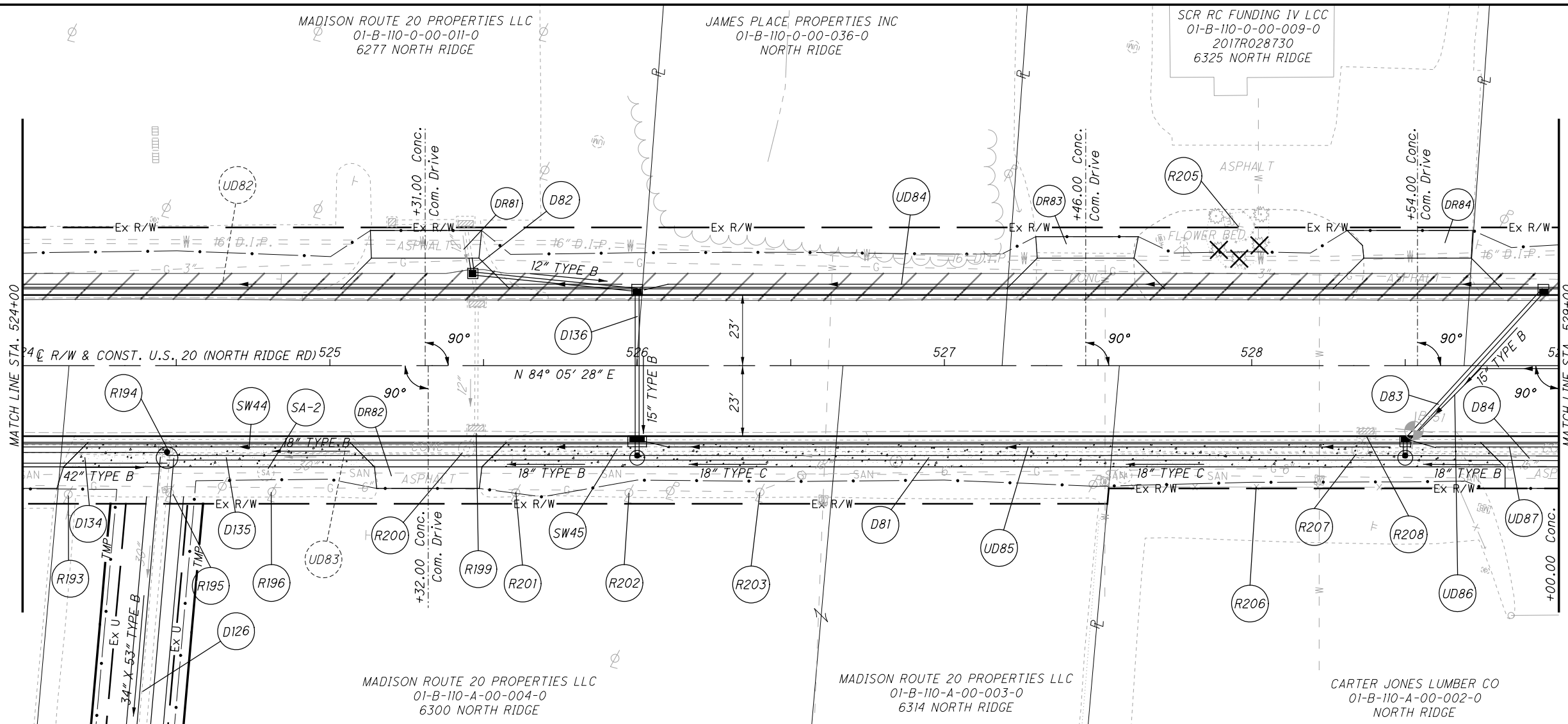
PLAN AND PROFILE U.S. 20
STA. 519+00 TO STA. 524+00

LAK-US-20-24.99
PART 2

263
 697

H:\2018\80743\000\ProjectData\0922TO_LAK_20_EAST\Design\Roadway\Sheets\0922TO_GP023.dgn Sheet 7/15/2022 2:54:08 PM troyer

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_GPO24.dgn Sheet 7/15/2022 2:54:09 PM troyer



CALCULATED
JMP
CHECKED
WDB

**PLAN AND PROFILE U.S. 20
STA. 524+00 TO STA. 529+00**

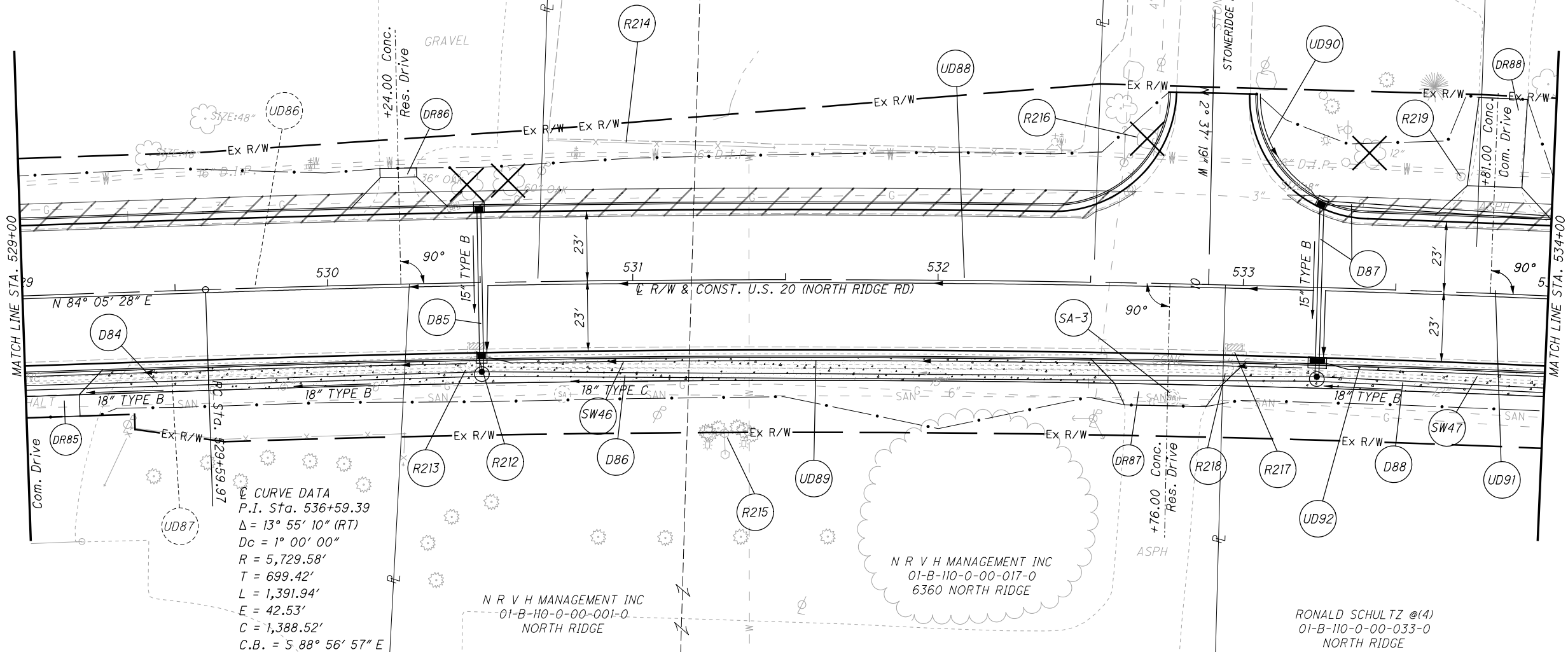
**LAK-US-20-24.99
PART 2**

264
697

JAMES PLACE PROPERTIES INC
01-B-110-0-00-037-0
6337 NORTH RIDGE

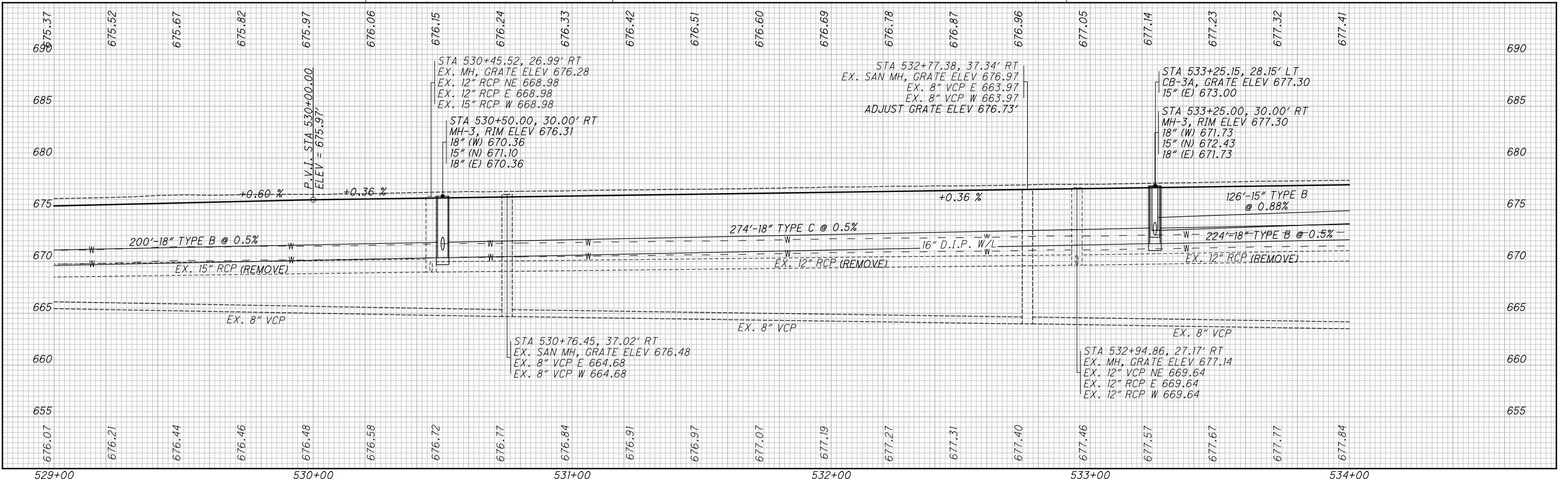
MADISON BOARD OF TRUSTEES CEMETERIES
01-B-110-0-00-007-0
NORTH RIDGE

JAMES PLACE PROPERTIES INC
01-B-110-0-00-035-0
NORTH RIDGE



CURVE DATA
 P.I. Sta. 536+59.39
 $\Delta = 13^\circ 55' 10''$ (RT)
 $D_c = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 699.42'$
 $L = 1,391.94'$
 $E = 42.53'$
 $C = 1,388.52'$
 $C.B. = S 88^\circ 56' 57'' E$

TEMPORARY PAVEMENT



H:\2018\80743\0D01\ProjectData\092270_LAK_20_EAST\Design\Roadway\Sheets\092270_GPO25.dgn Sheet 7/15/2022 2:54:10 PM troyer



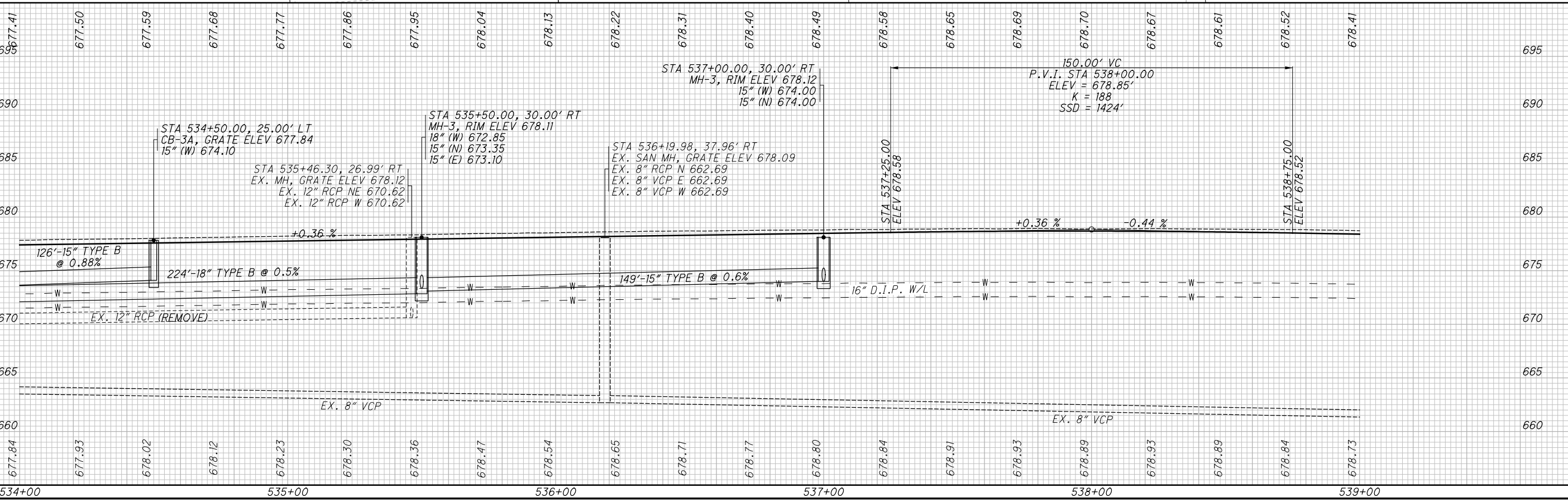
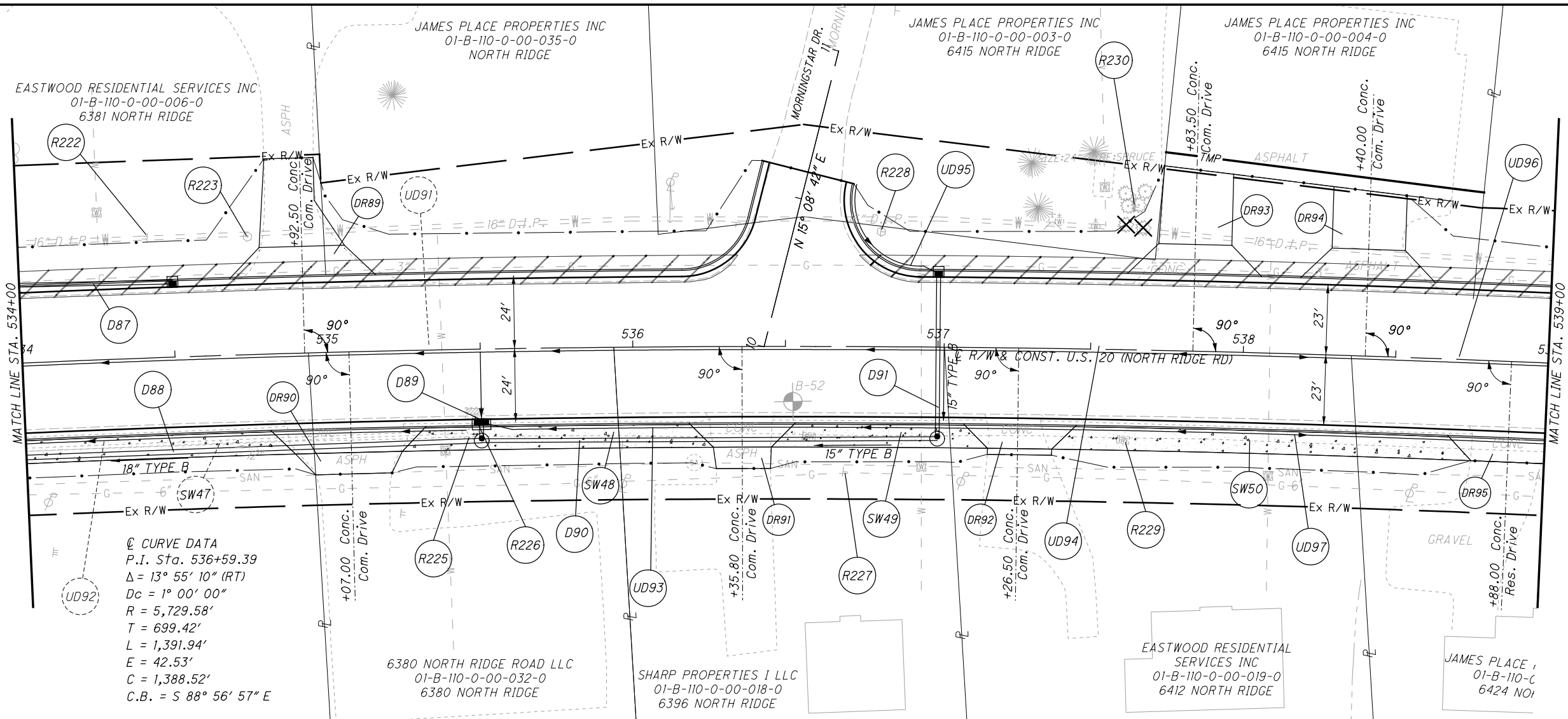
CALCULATED: []
 JUMP: []
 CHECKED: []
 WDB: []

PLAN AND PROFILE U.S. 20
 STA. 529+00 TO STA. 534+00

LAK-US-20-24.99
 PART 2

265
 697

H:\2018\80743\001\Project\Data\092TO_LAK_20_EAST\Design\Roadway\Sheets\092TO_GP026.dgn Sheet 7/15/2022 2:54:10 PM troyer

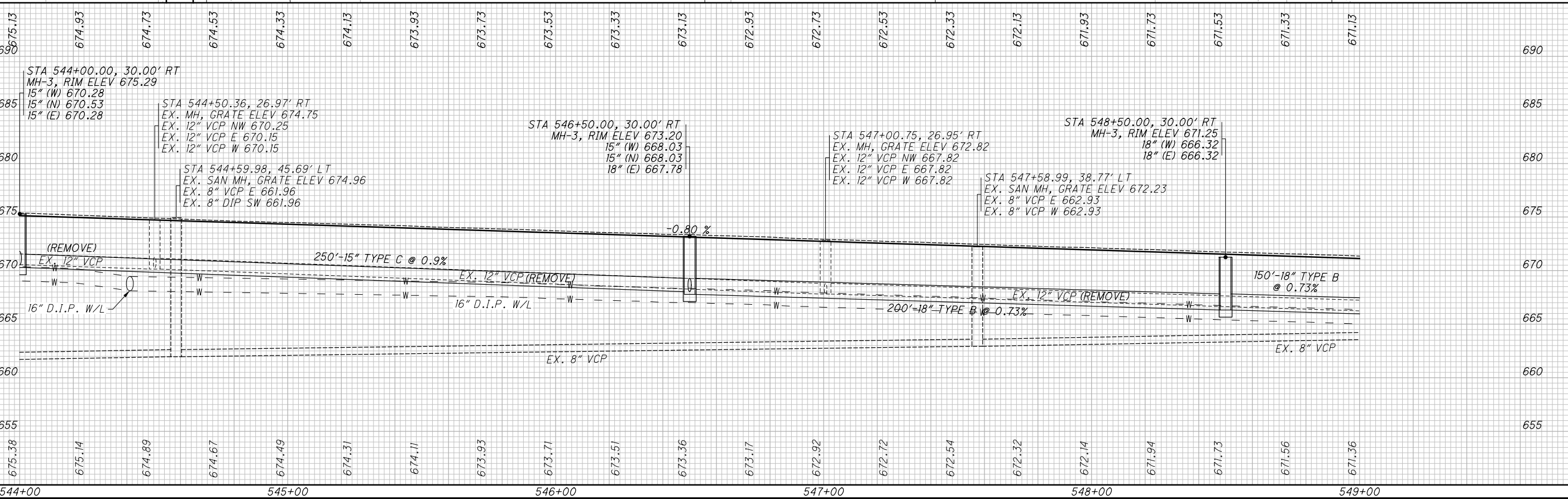
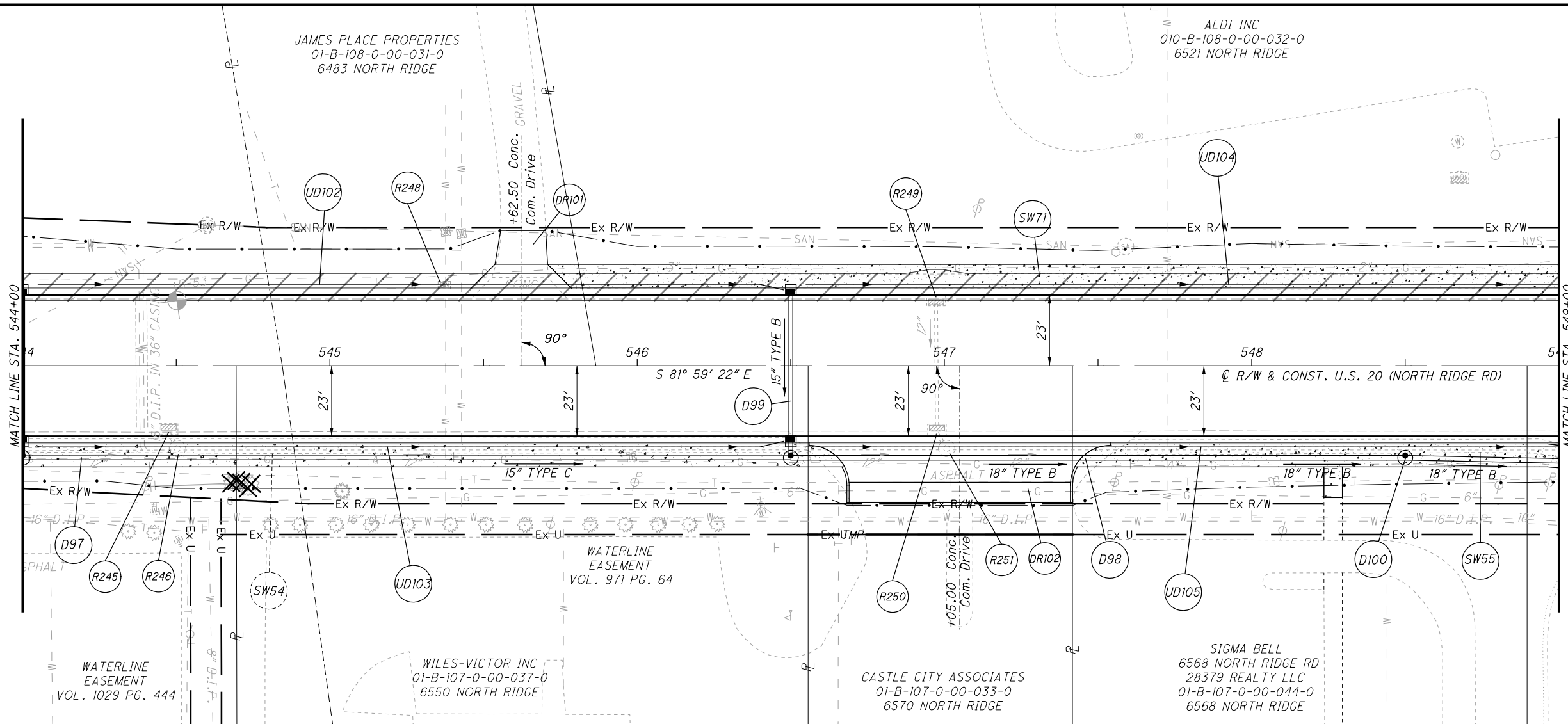


PLAN AND PROFILE U.S. 20
 STA. 534+00 TO STA. 539+00

LAK-US-20-24.99
 PART 2

266
 697

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP028.dgn Sheet 7/15/2022 2:54:42 PM Troyer



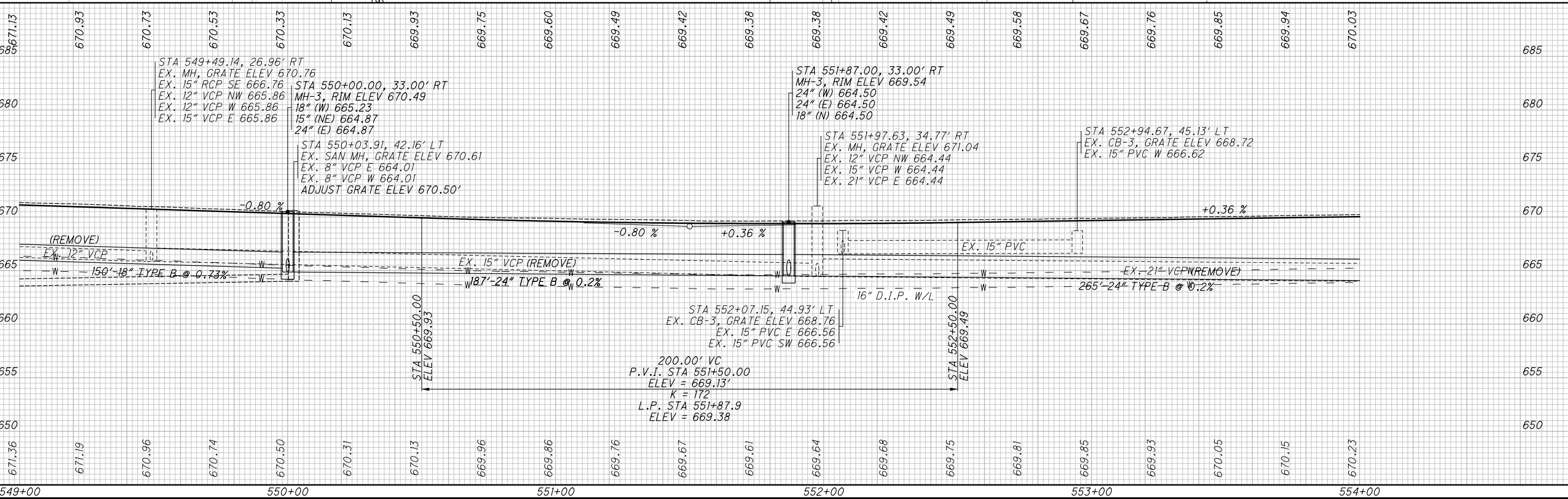
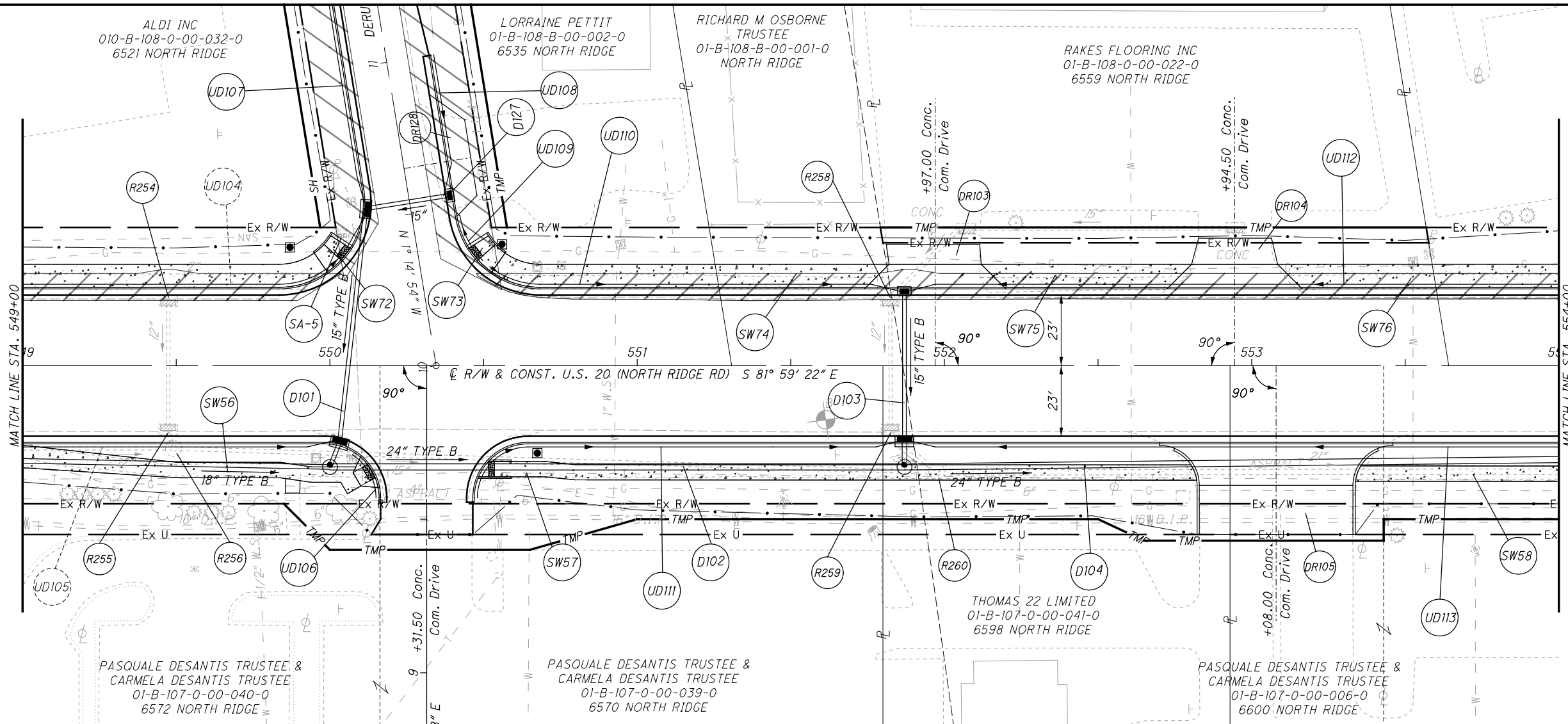
CALCULATED
JMP
CHECKED
WDB

**PLAN AND PROFILE U.S. 20
STA. 544+00 TO STA. 549+00**

**LAK-US-20-24.99
PART 2**

268
697

H:\2018\80743\001\ProjectData\092270_LAK_20_EAST\Design\Roadway\Sheets\092270_GP029.dgn Sheet 7/15/2022 2:54:13 PM Troyer

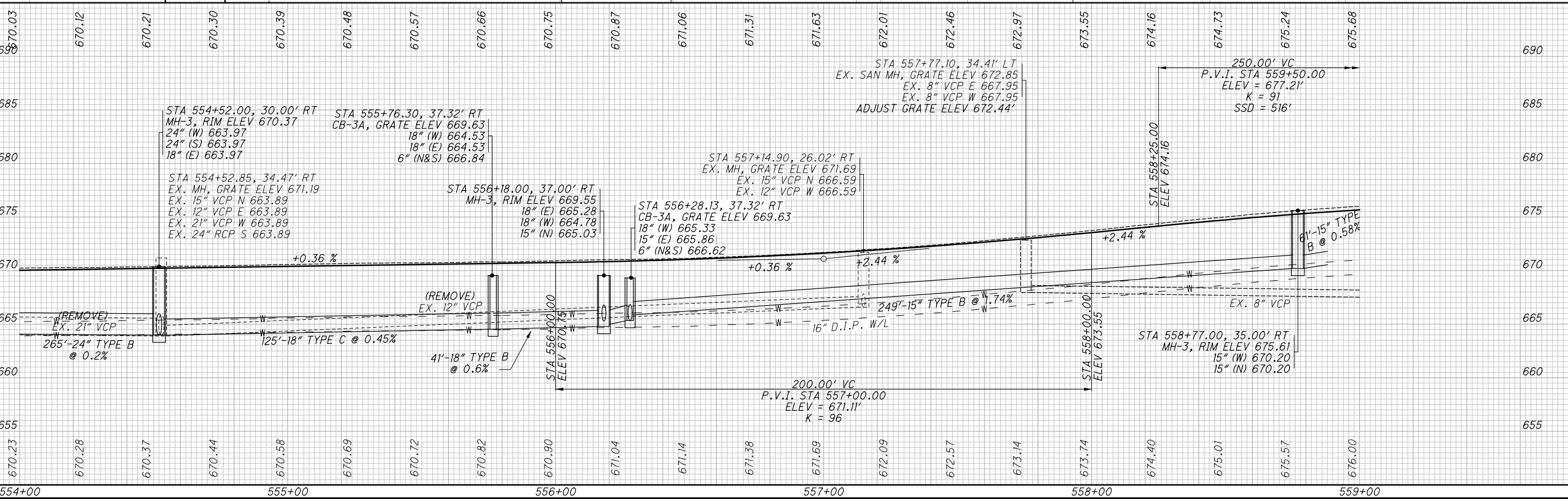
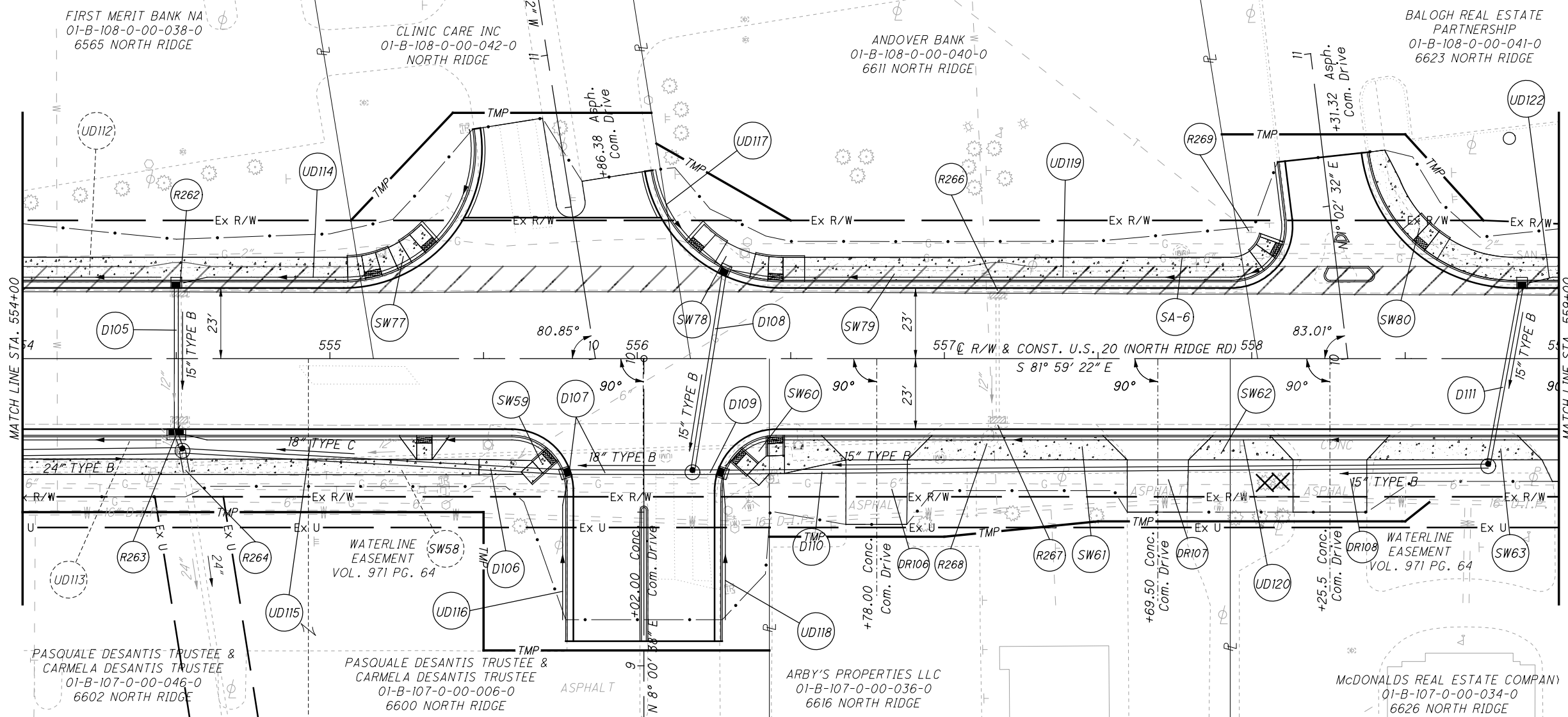


CALCULATED JIMP CHECKED WDB

**PLAN AND PROFILE U.S. 20
STA. 549+00 TO STA. 554+00**

**LAK-US-20-24.99
PART 2**

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP030.dgn Sheet 7/15/2022 2:54:13 PM troyer



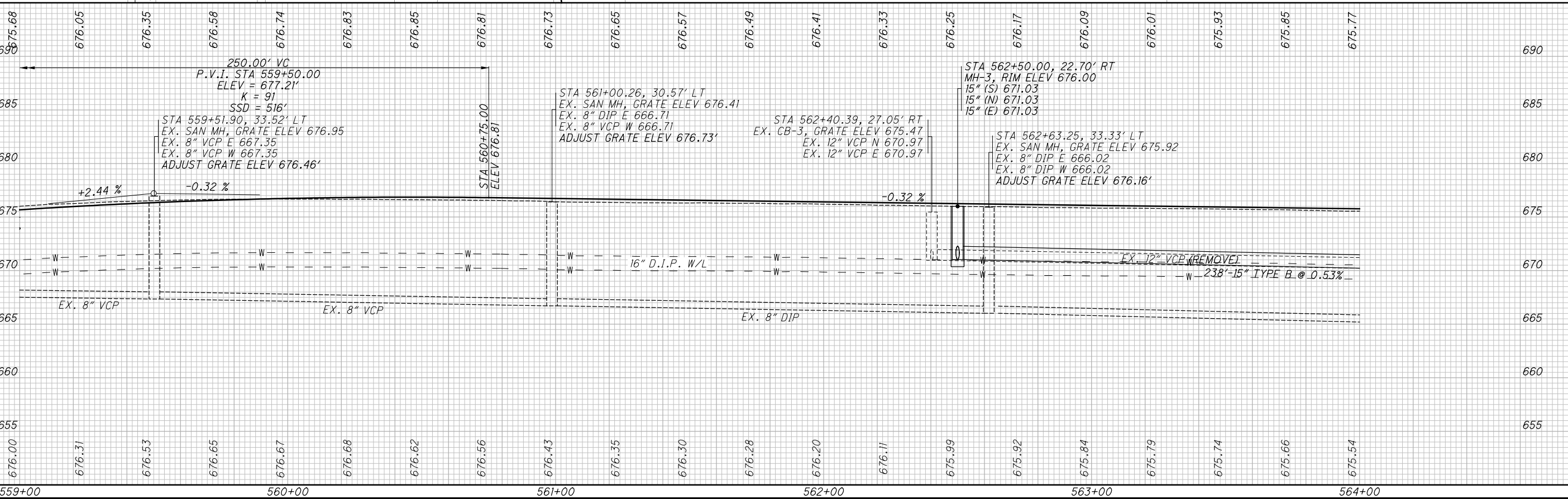
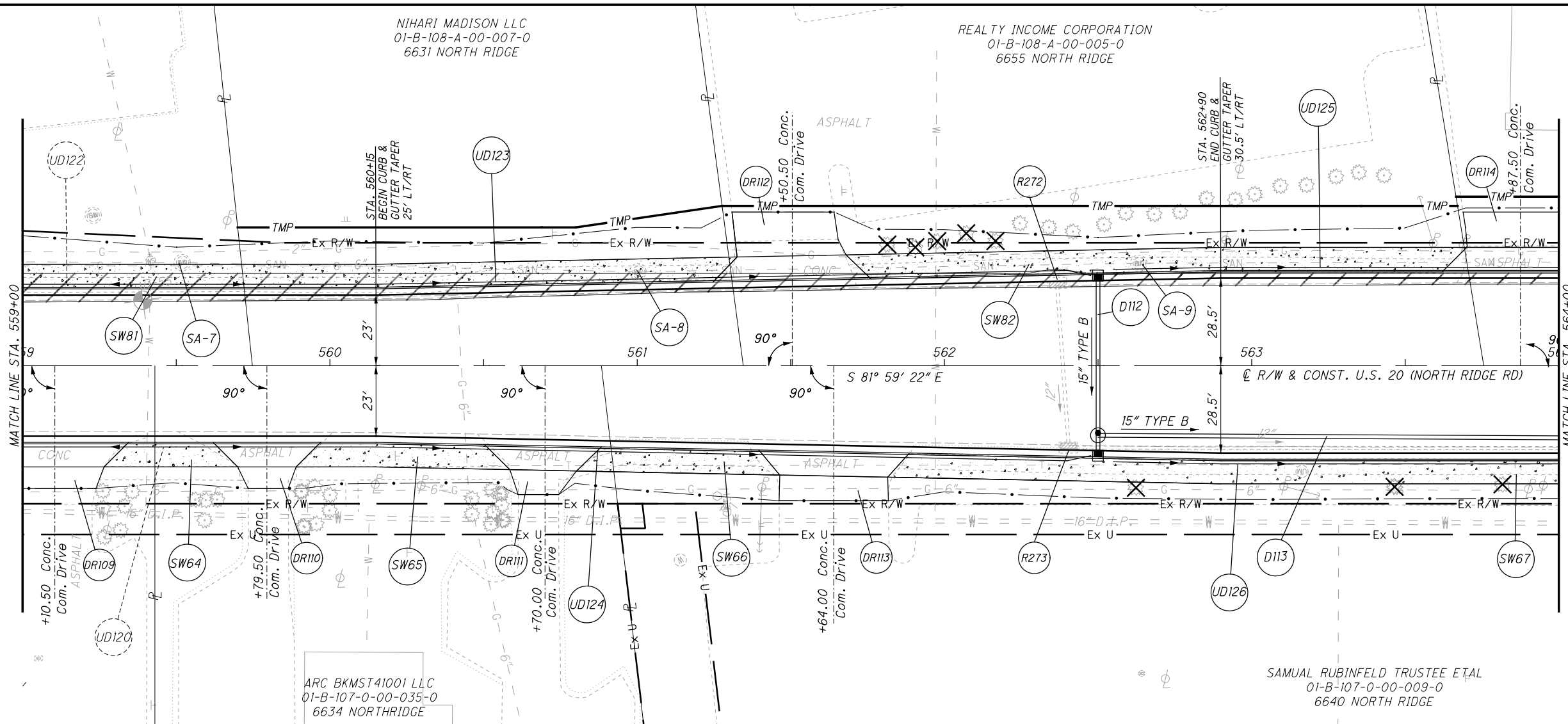
CALCULATED JIMP CHECKED WDB

PLAN AND PROFILE U.S. 20
STA. 554+00 TO STA. 559+00

LAK-US-20-24.99
PART 2

270
697

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP031.dgn Sheet 7/15/2022 2:54:44 PM troyer



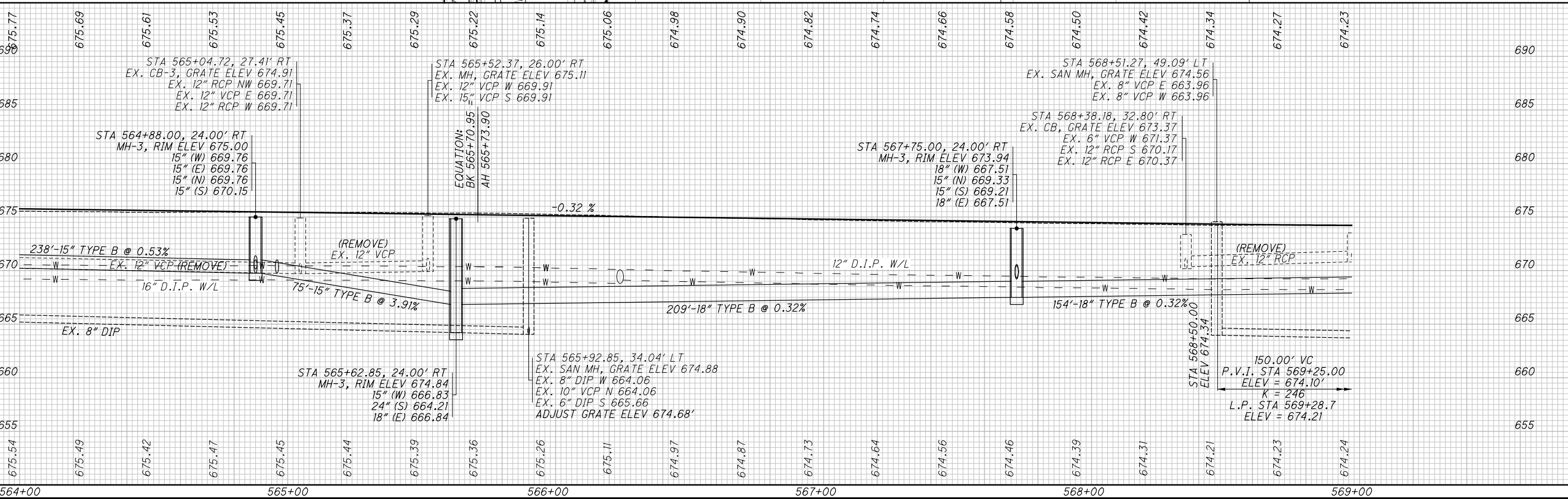
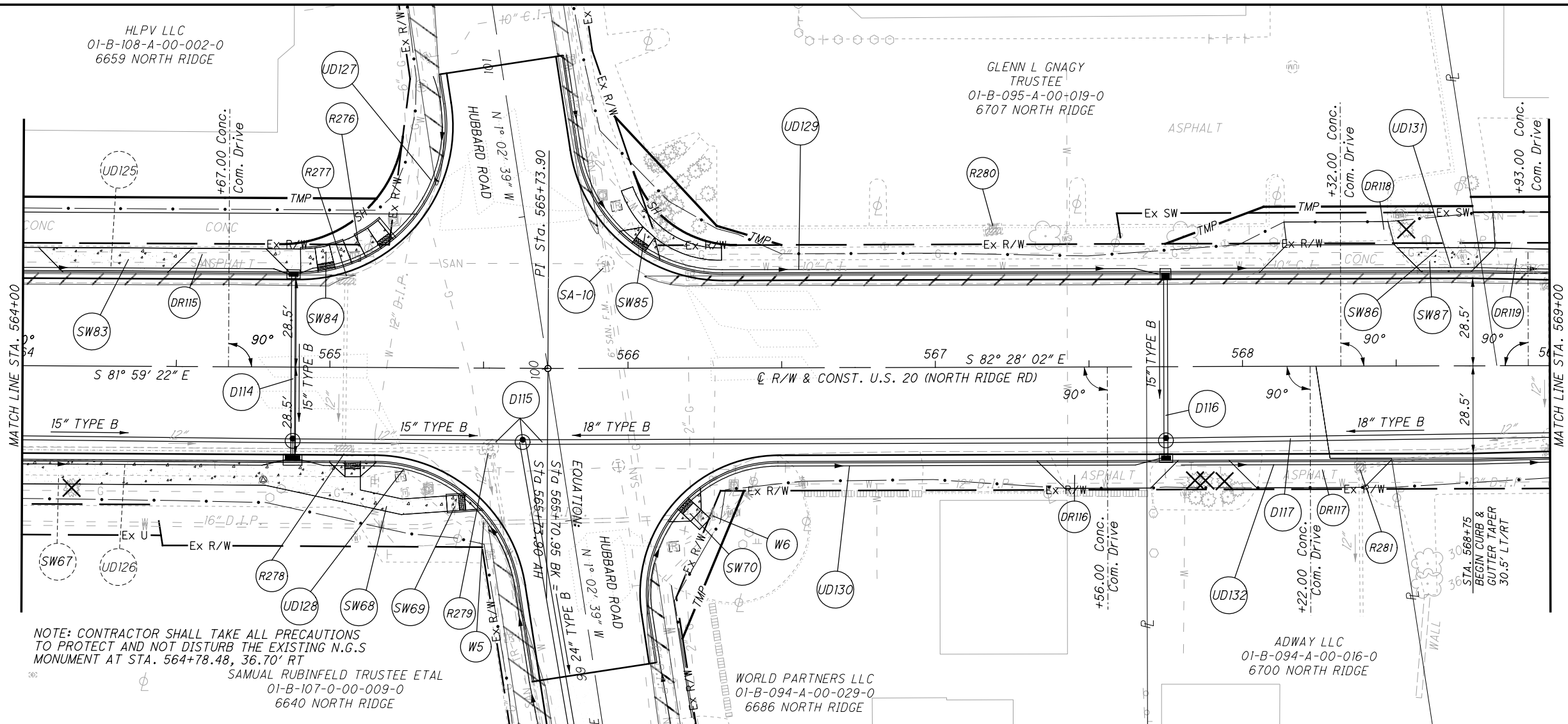
CALCULATED
JMP
CHECKED
WDB

**PLAN AND PROFILE U.S. 20
STA. 559+00 TO STA. 564+00**

**LAK-US-20-24.99
PART 2**

271
697

H:\2018\80743\0D01\ProjectData\092270_LAK_20_EAST\Design\Roadway\Sheets\092270_CP032.dgn Sheet 7/15/2022 2:54:15 PM Troyer

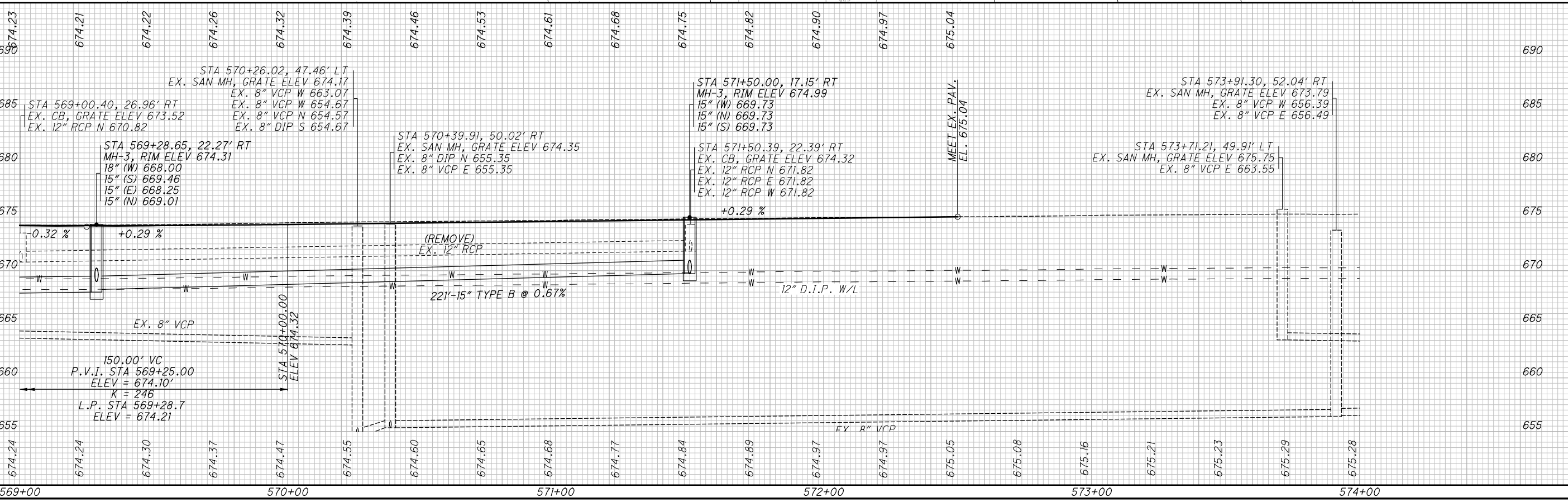
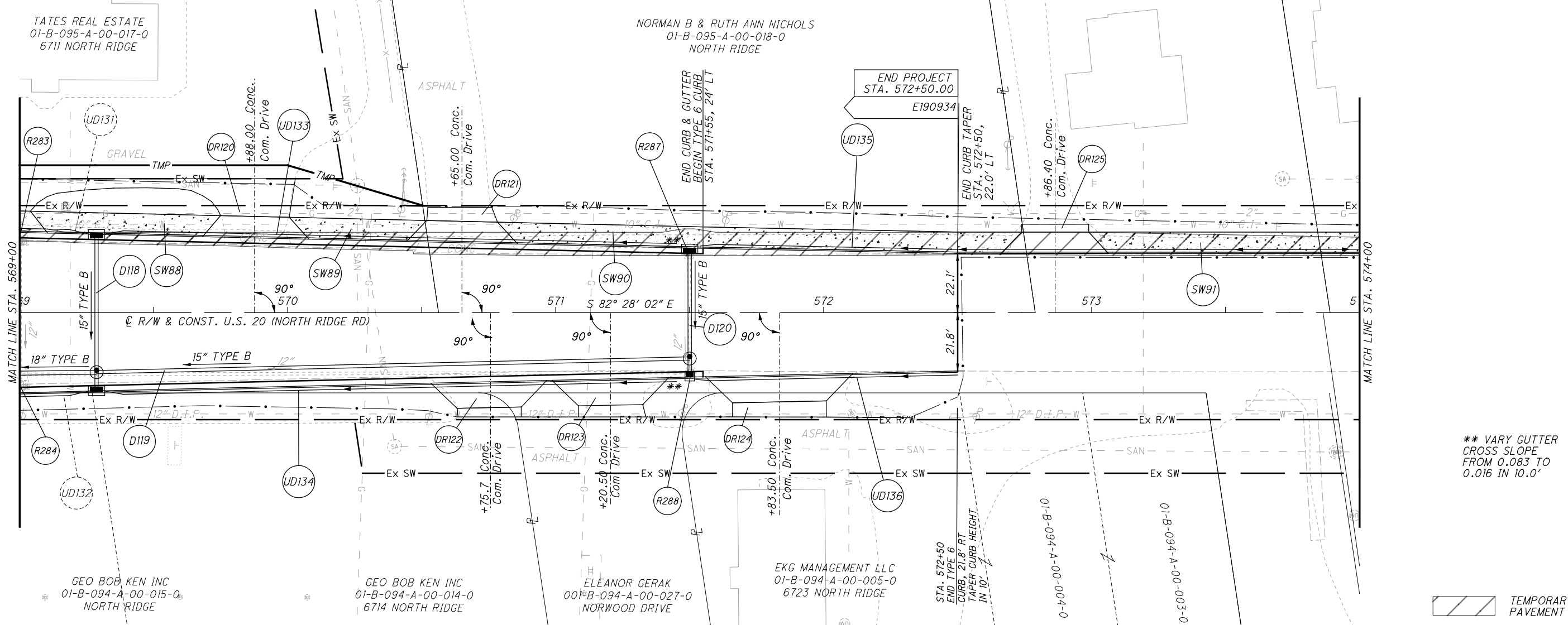


CALCULATED JIM
CHECKED WDB

**PLAN AND PROFILE U.S. 20
STA. 564+00 TO STA. 569+00**

**LAK-US-20-24.99
PART 2**

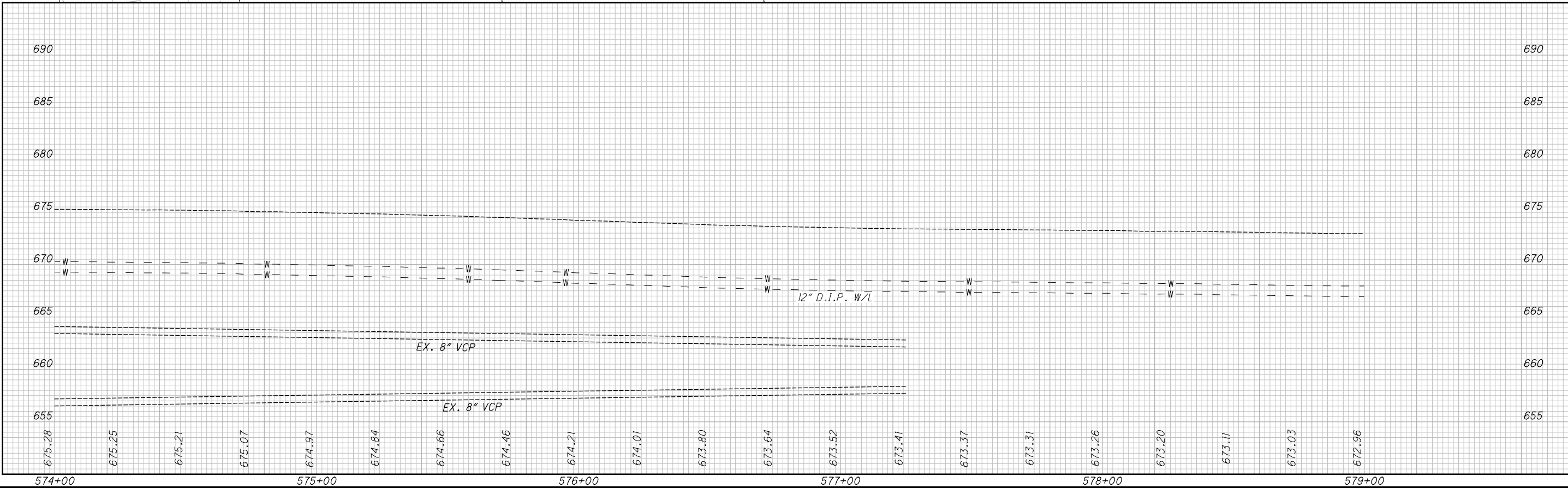
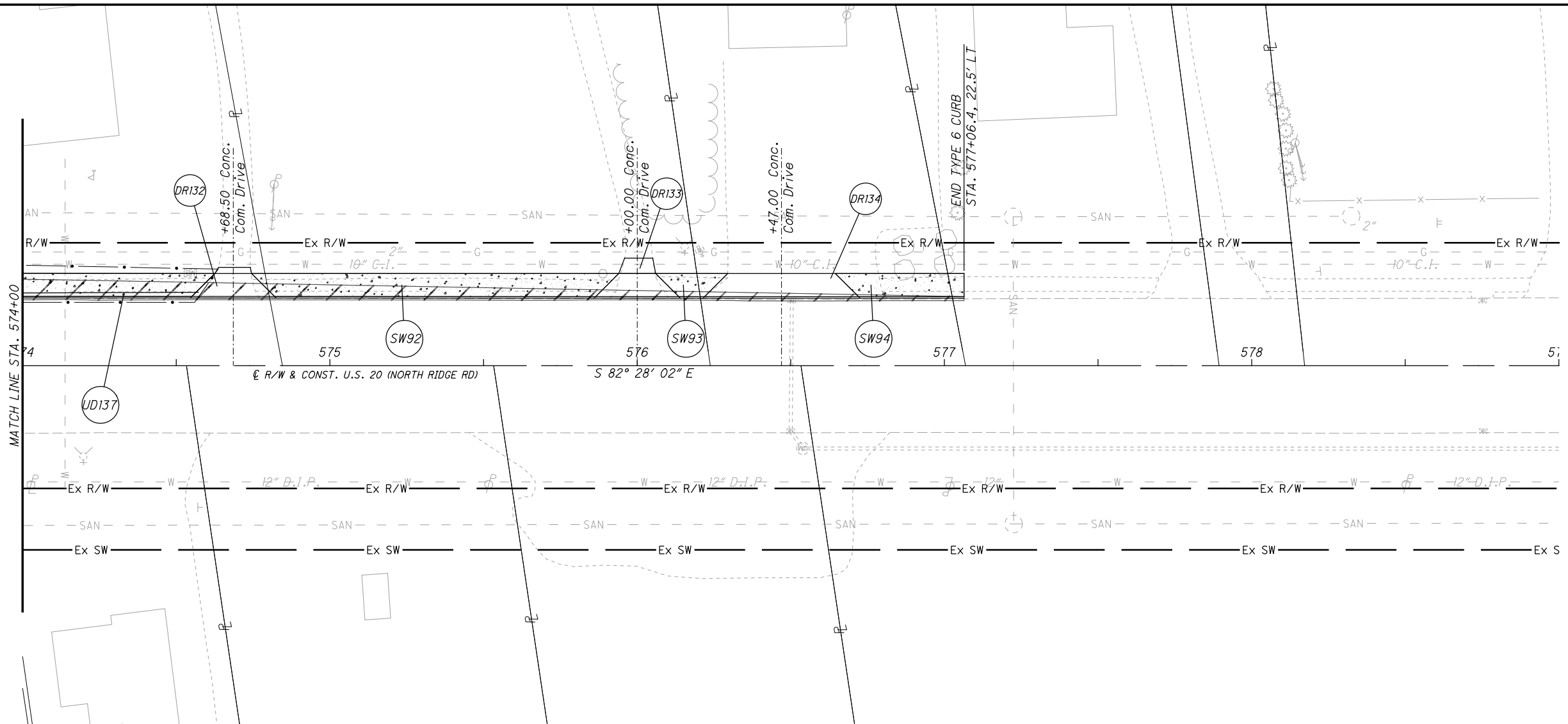
H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP033.dgn Sheet 7/15/2022 2:54:16 PM Troyer



**PLAN AND PROFILE U.S. 20
STA. 569+00 TO STA. 574+00**

**LAK-US-20-24.99
PART 2**

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP034.dgn Sheet 7/15/2022 2:54:16 PM troyer



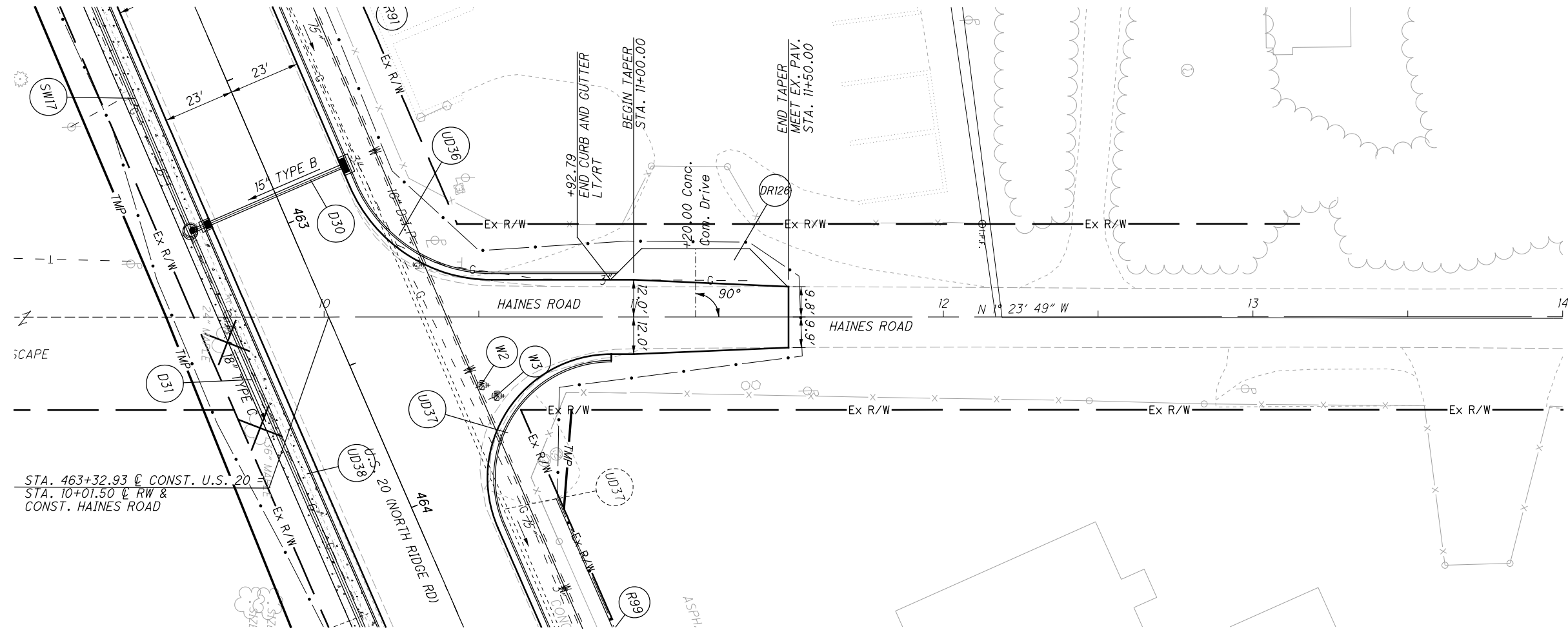
TEMPORARY PAVEMENT

CALCULATED
JMP
CHECKED
WDB

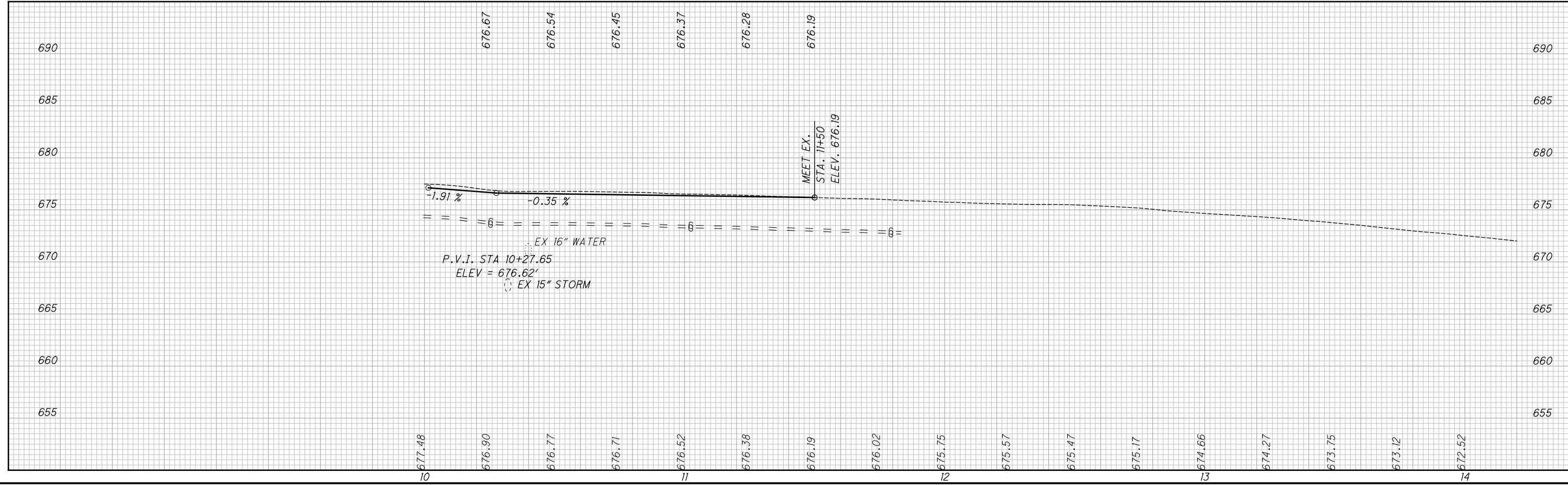
0 20 40
HORIZONTAL SCALE IN FEET

**PLAN AND PROFILE U.S. 20
STA. 574+00 TO STA. 579+00**

**LAK-US-20-24.99
PART 2**



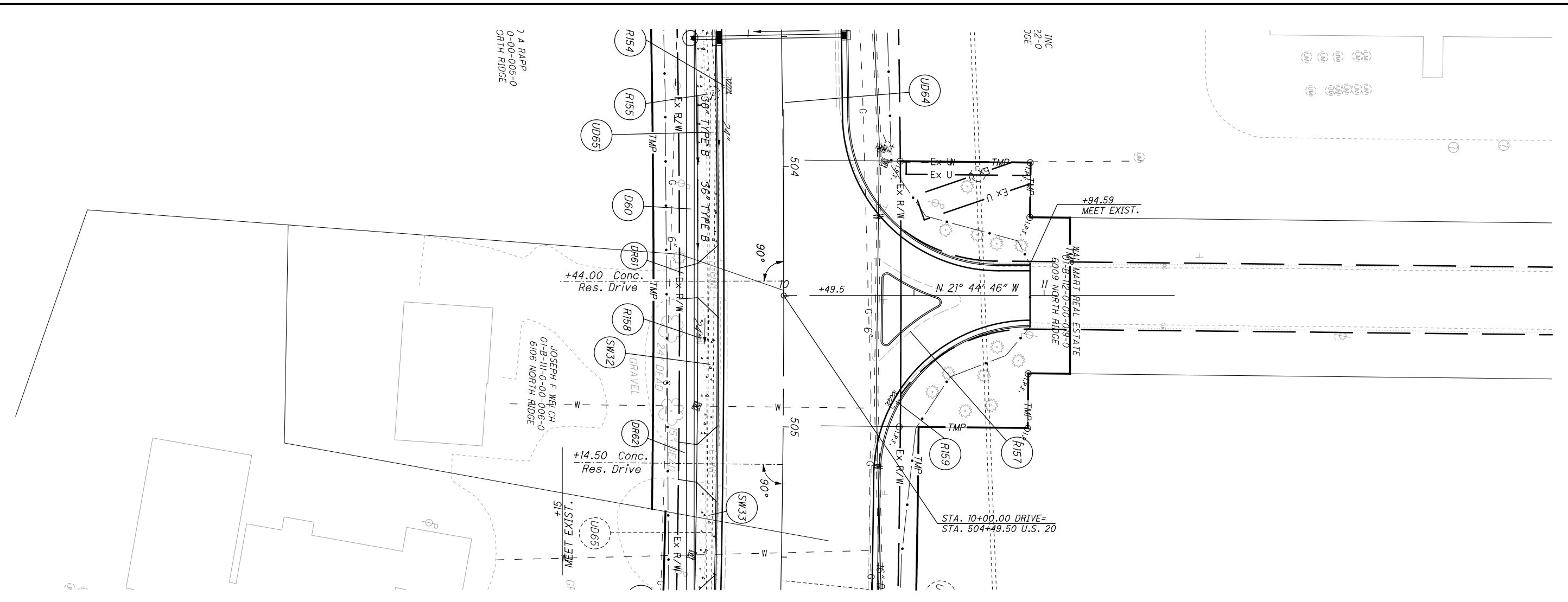
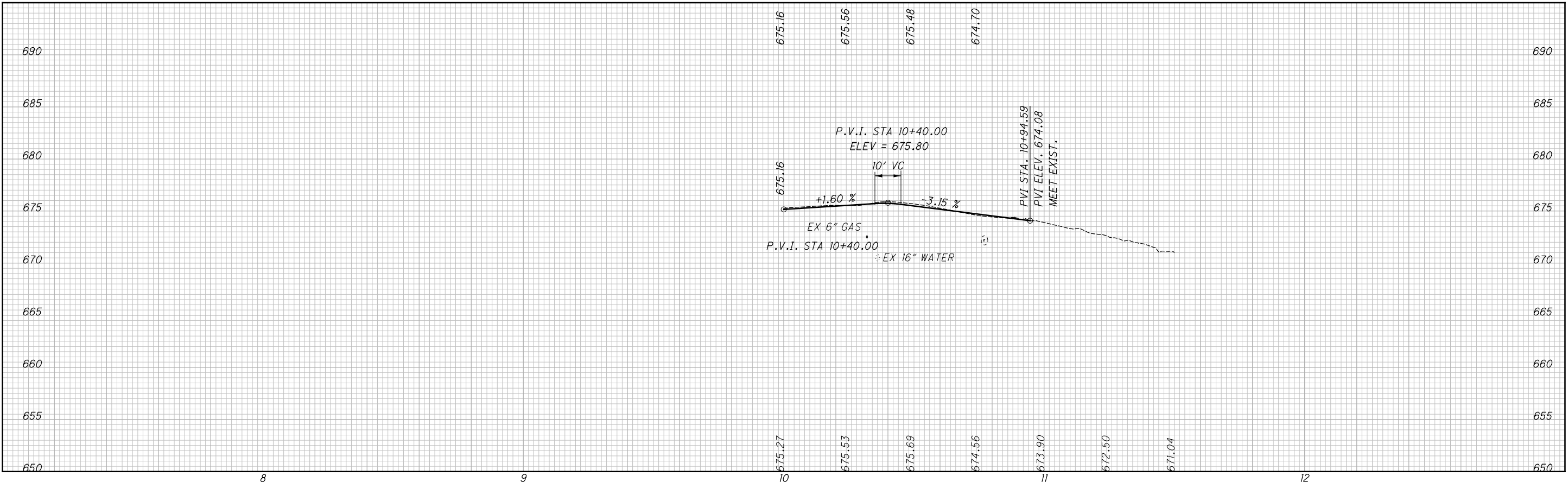
STA. 463+32.93 @ CONST. U.S. 20
 STA. 10+01.50 @ RW &
 CONST. HAINES ROAD



CALCULATED
 JMP
 CHECKED
 WDB

LAK-US-20-24.99
PART 2
PLAN AND PROFILE - HAINES RD.
STA. 10+00 TO STA. 14+00

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP203_DRIVE_504+49 LT 7/15/2022 2:54:28 PM Troyer



CALCULATED
JMP
CHECKED
WDB

0 20 40
HORIZONTAL
SCALE IN FEET

N

PLAN AND PROFILE
DRIVE STA. 504+49.50

LAK-US-20-24.99
PART 2

276
697

CRAFT BROTHERS INC
01-B-111-0-00-008-0
6214 NORTH RIDGE

EAST ASSOCIATES INC
01-B-110-A-00-011-0
1997R011111
BURNS ROAD

EAST ASSOCIATES INC
01-B-110-A-00-010-0
1997R011111
NORTH RIDGE

RANDY D TRICE &
REBECCA TRICE
01-B-112-0-00-001-0
1996R040632
NORTH RIDGE

RANDY DEAN TRICE &
REBECCA LYNN TRICE
01-B-110-0-00-016-0
VOL 498 PG 28
6227 NORTH RIDGE

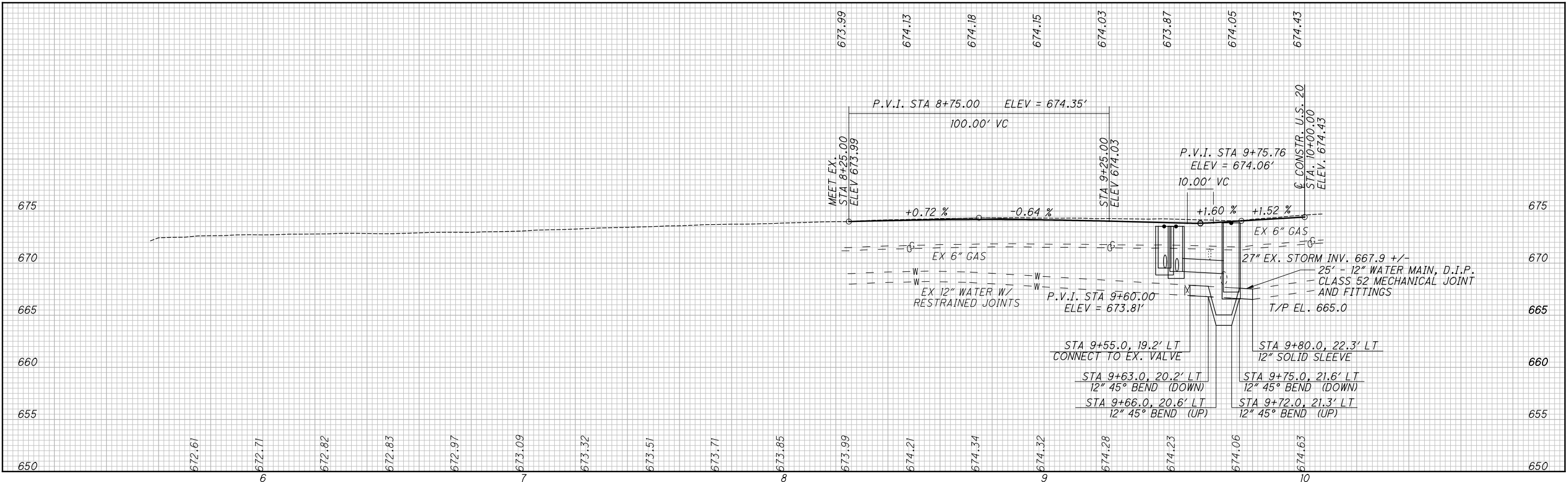
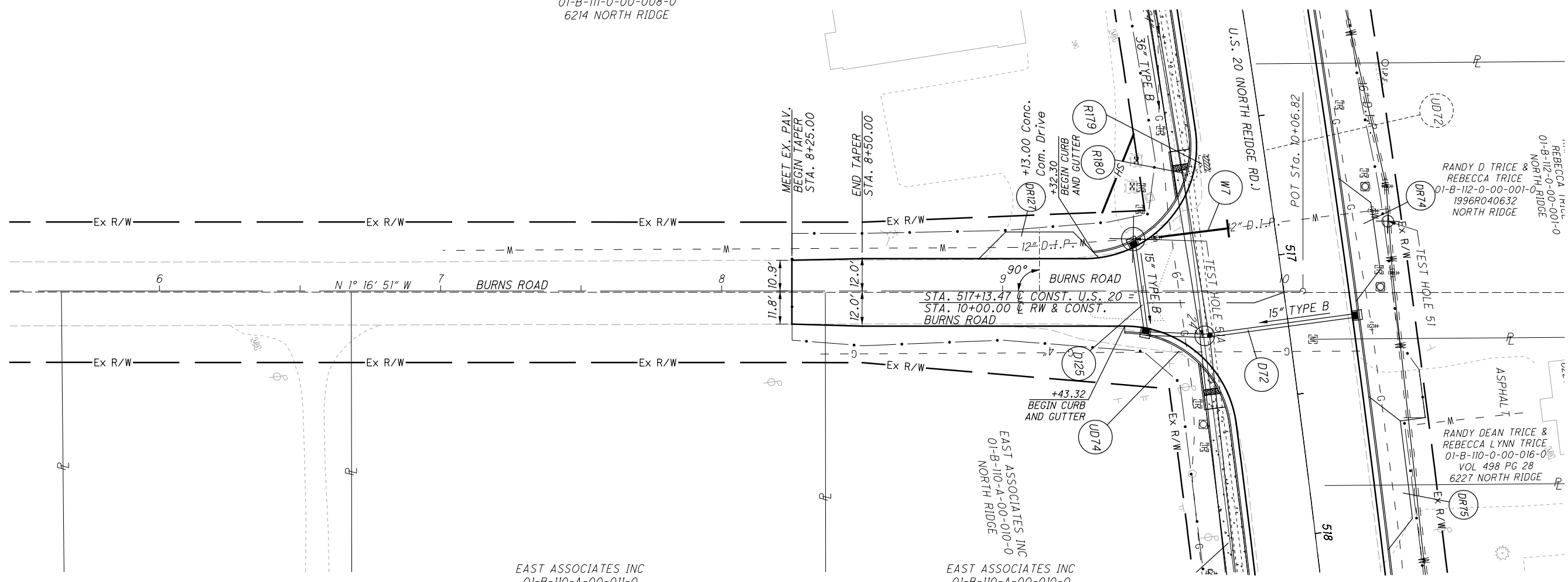


CALCULATED
JMP
CHECKED
WDB

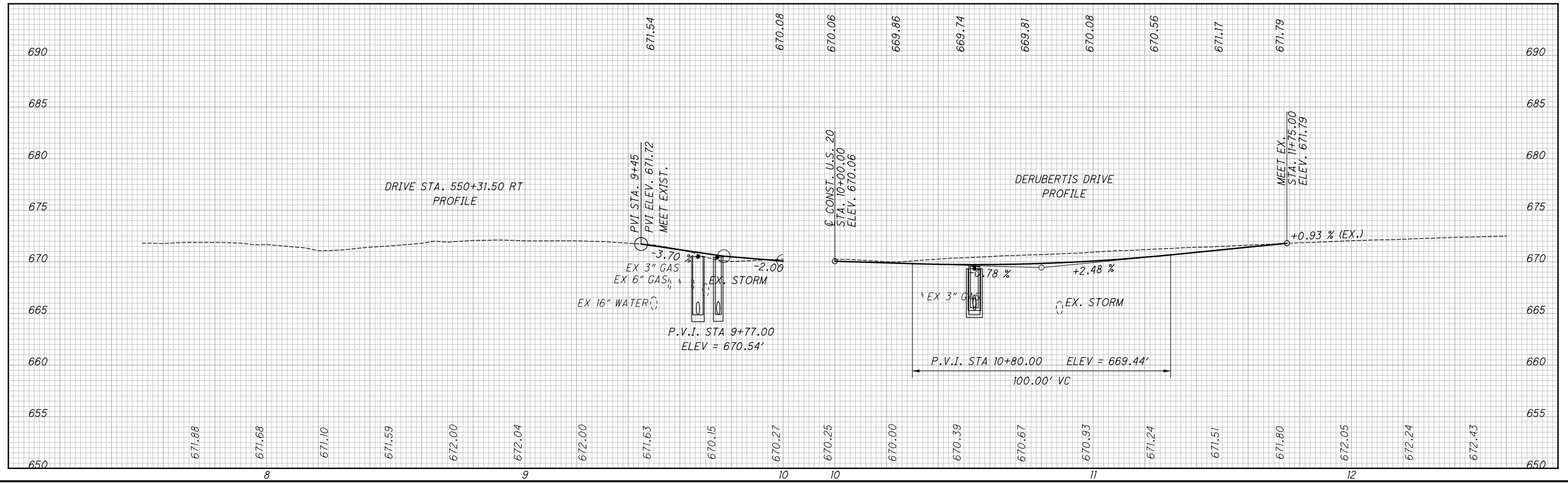
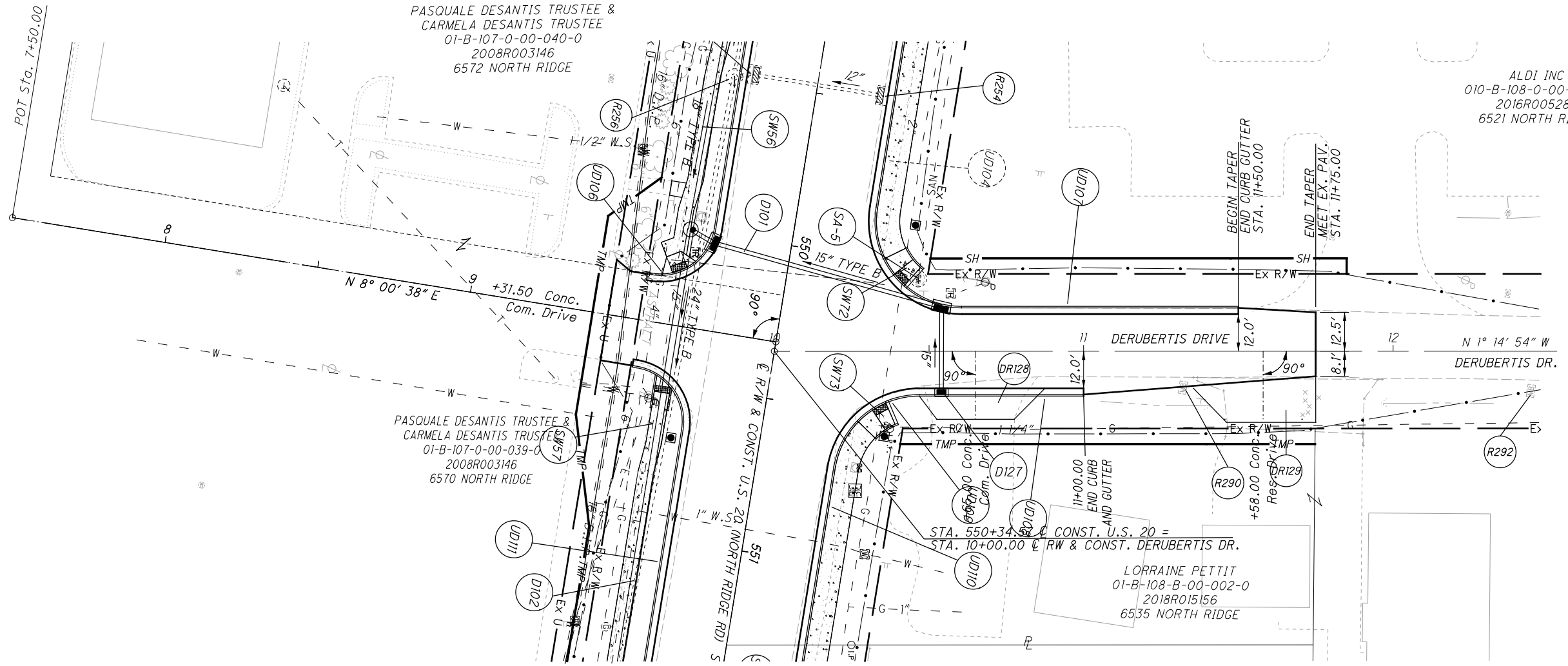
**PLAN AND PROFILE - BURNS RD.
STA. 6+00 STA. TO STA. 10+00**

**LAK-US-20-24.99
PART 2**

277
697



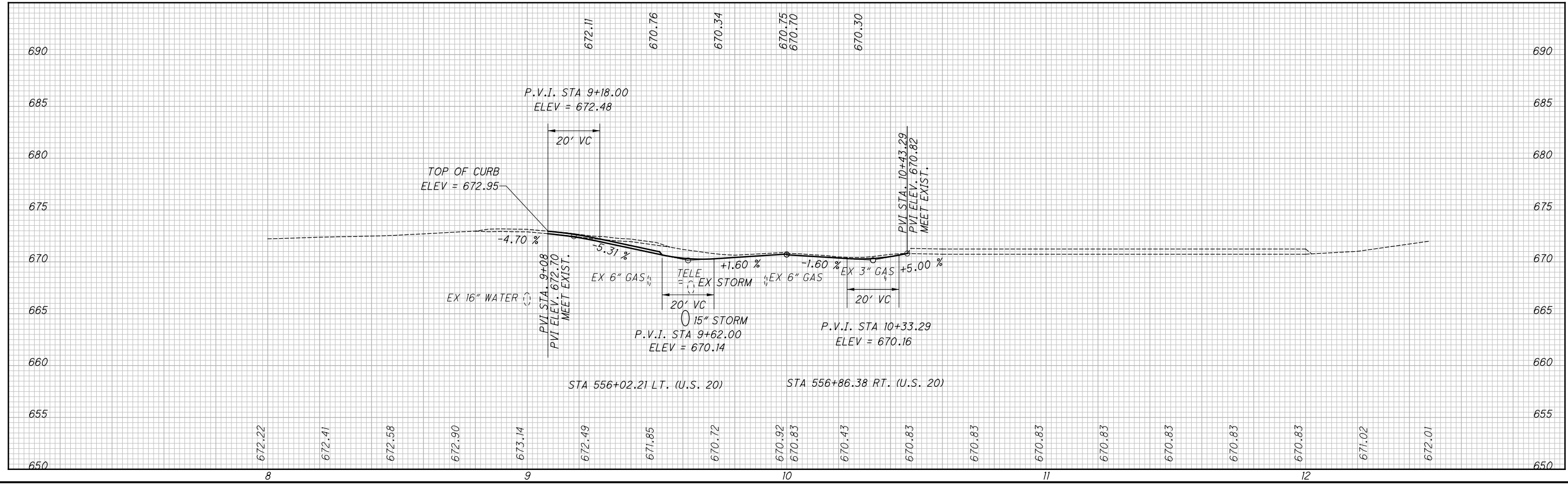
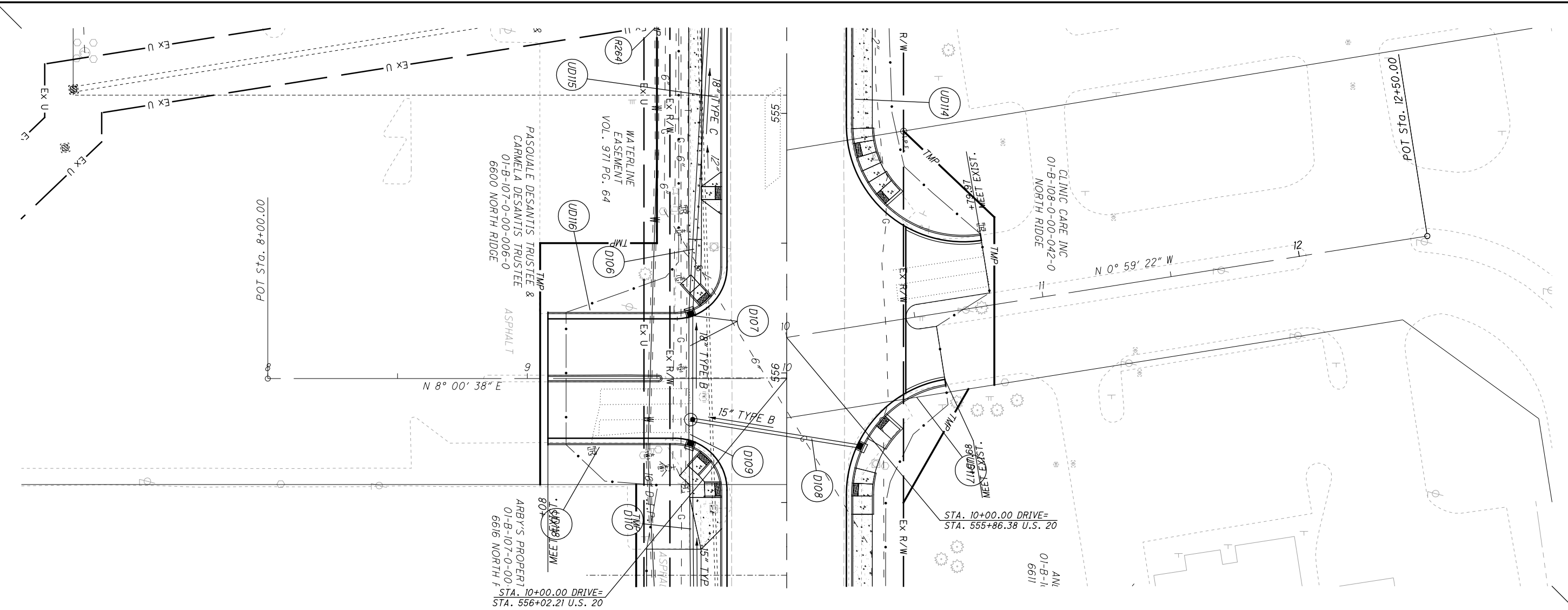
H:\2018\80743\0D01\Project\Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP202_BURNS.dgn Sheet 7/15/2022 2:54:29 PM troyer



PLAN AND PROFILE - DERBERTIS DR.
STA. 10+00 TO STA. 14+00

LAK-US-20-24.99
PART 2

CALCULATED	JMP
CHECKED	WDB

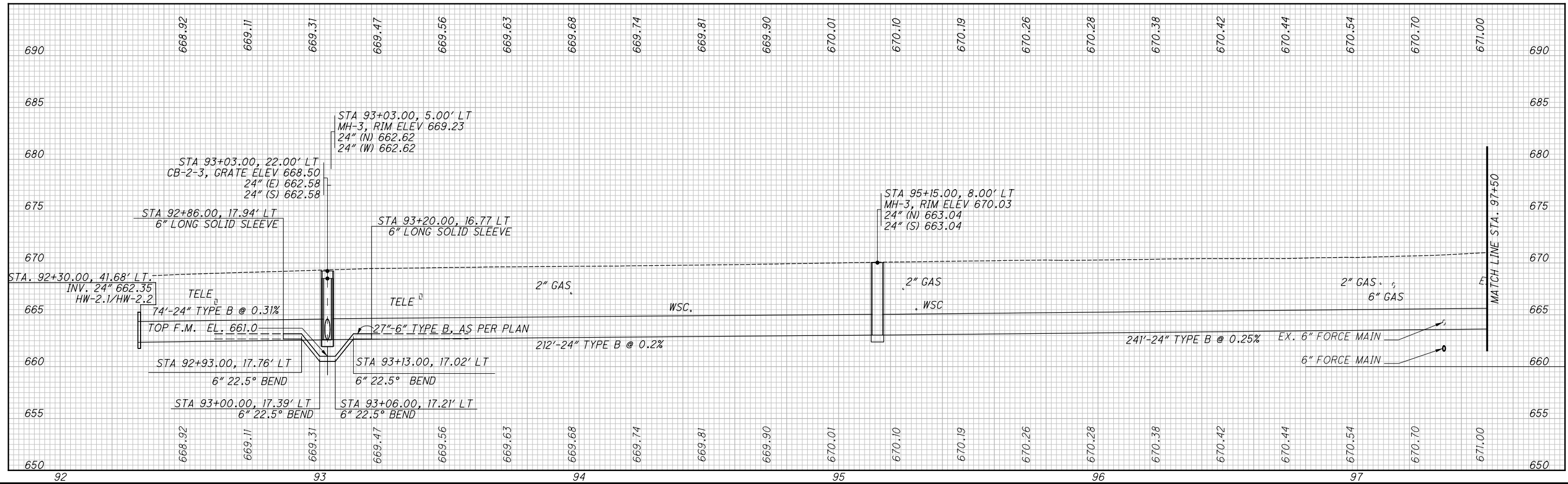
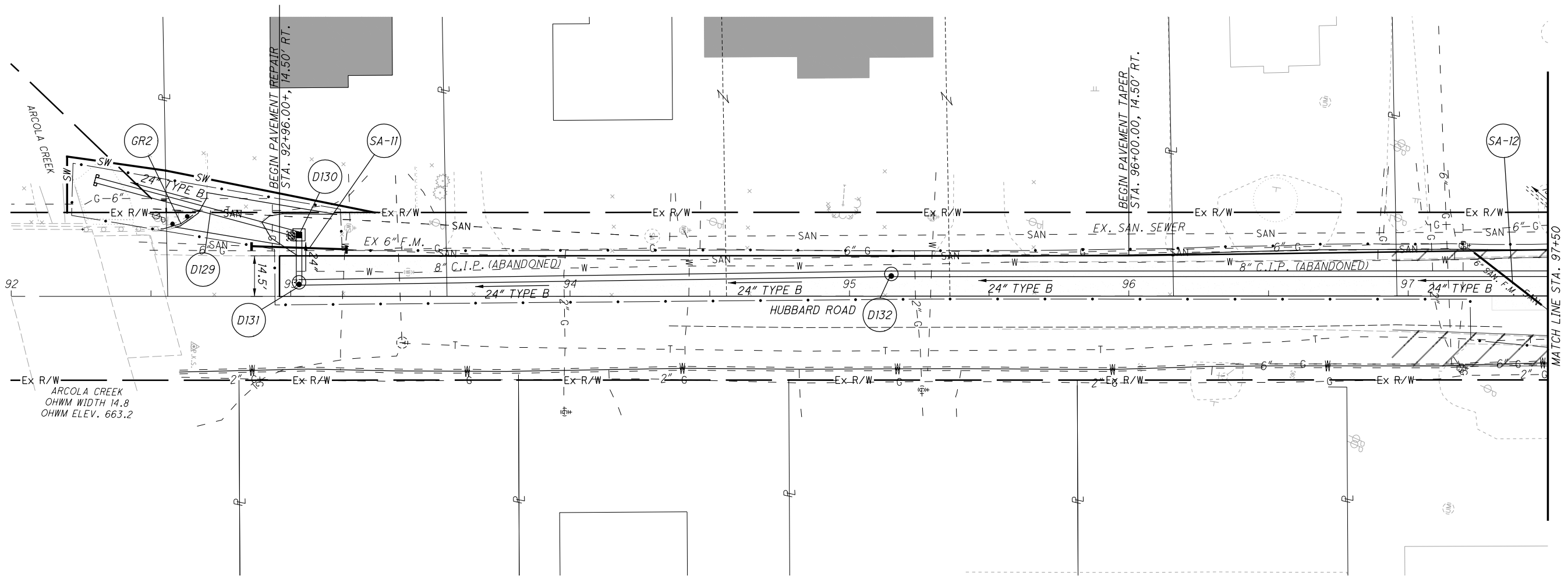


CALCULATED JIM
 CHECKED WDB
PLAN AND PROFILE - DRIVES * STA 556+02.21
STA. 8+00 TO STA. 12+00

LAK-US-20-24.99
PART 2

279
 697

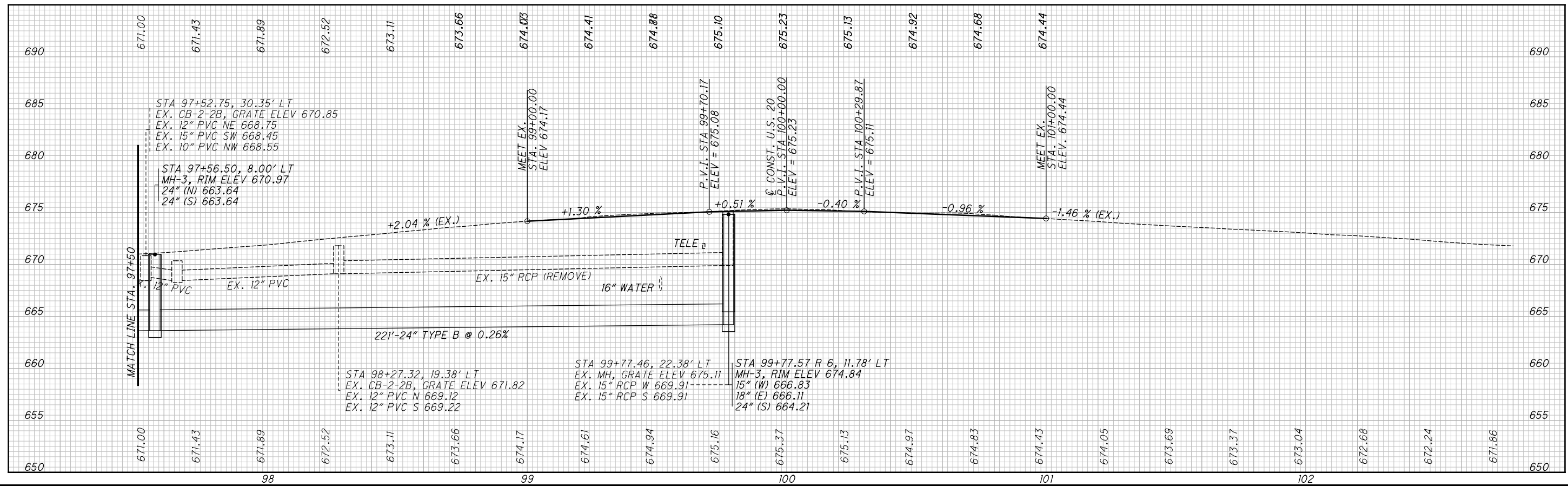
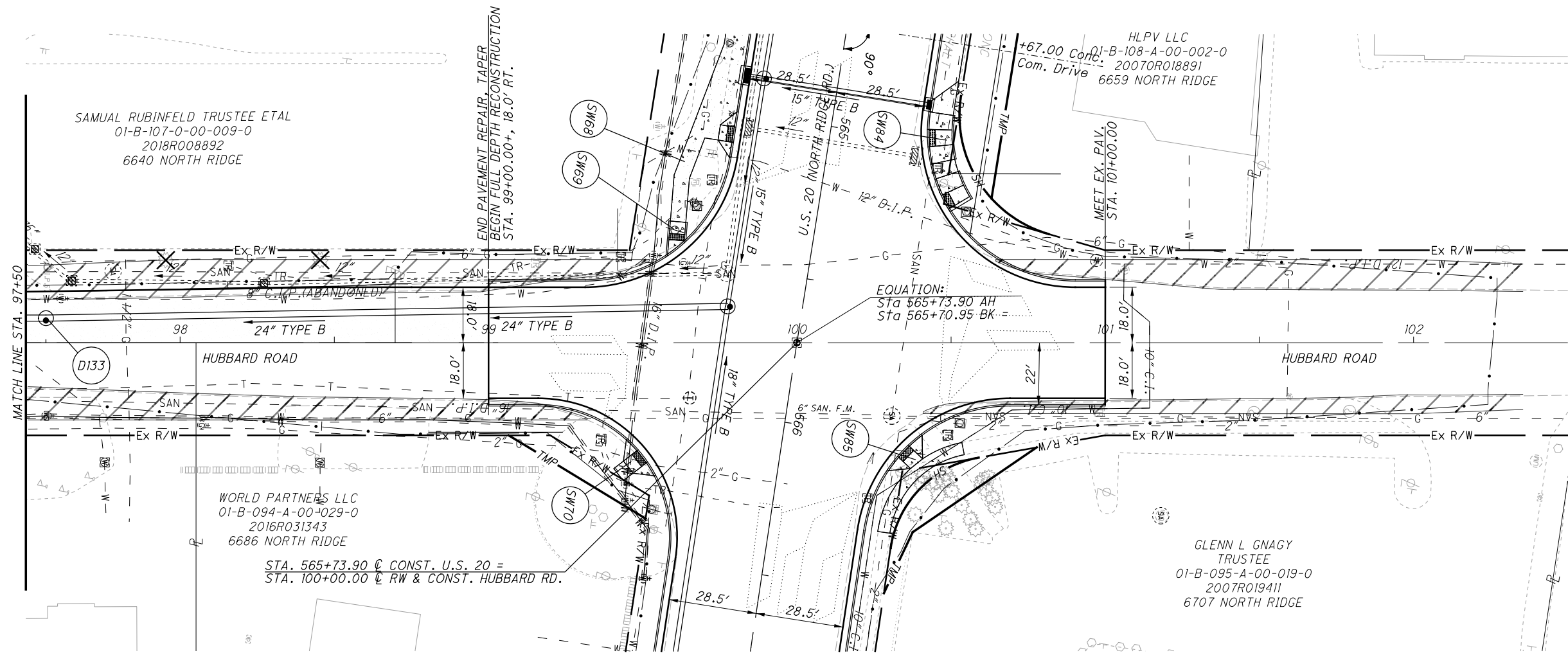
H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CP204_HUBBARD.dgn_Sheet-2 7/15/2022 2:54:32 PM Troyer



PLAN AND PROFILE - HUBBARD RD, 92+00 TO STA. 97+50

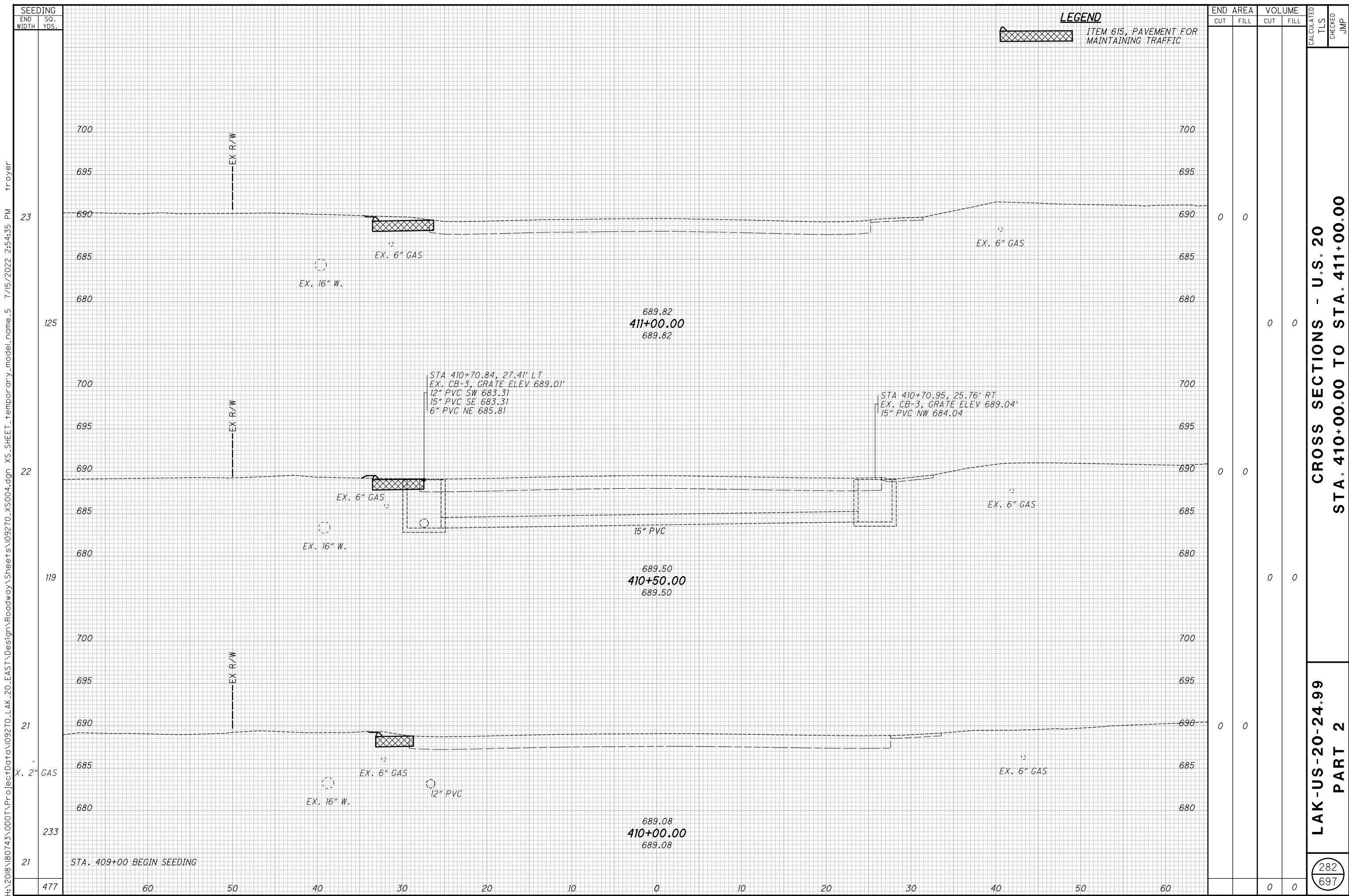
LAK-US-20-24.99 PART 2





LAK-US-20-24.99
PART 2
PLAN AND PROFILE - HUBBARD RD,
98+00 TO STA. 102+00

CALCULATED
JMP
CHECKED
WDB



SEEDING
 END WIDTH SO. YDS.
 23
 125
 22
 119
 21
 233
 21
 477

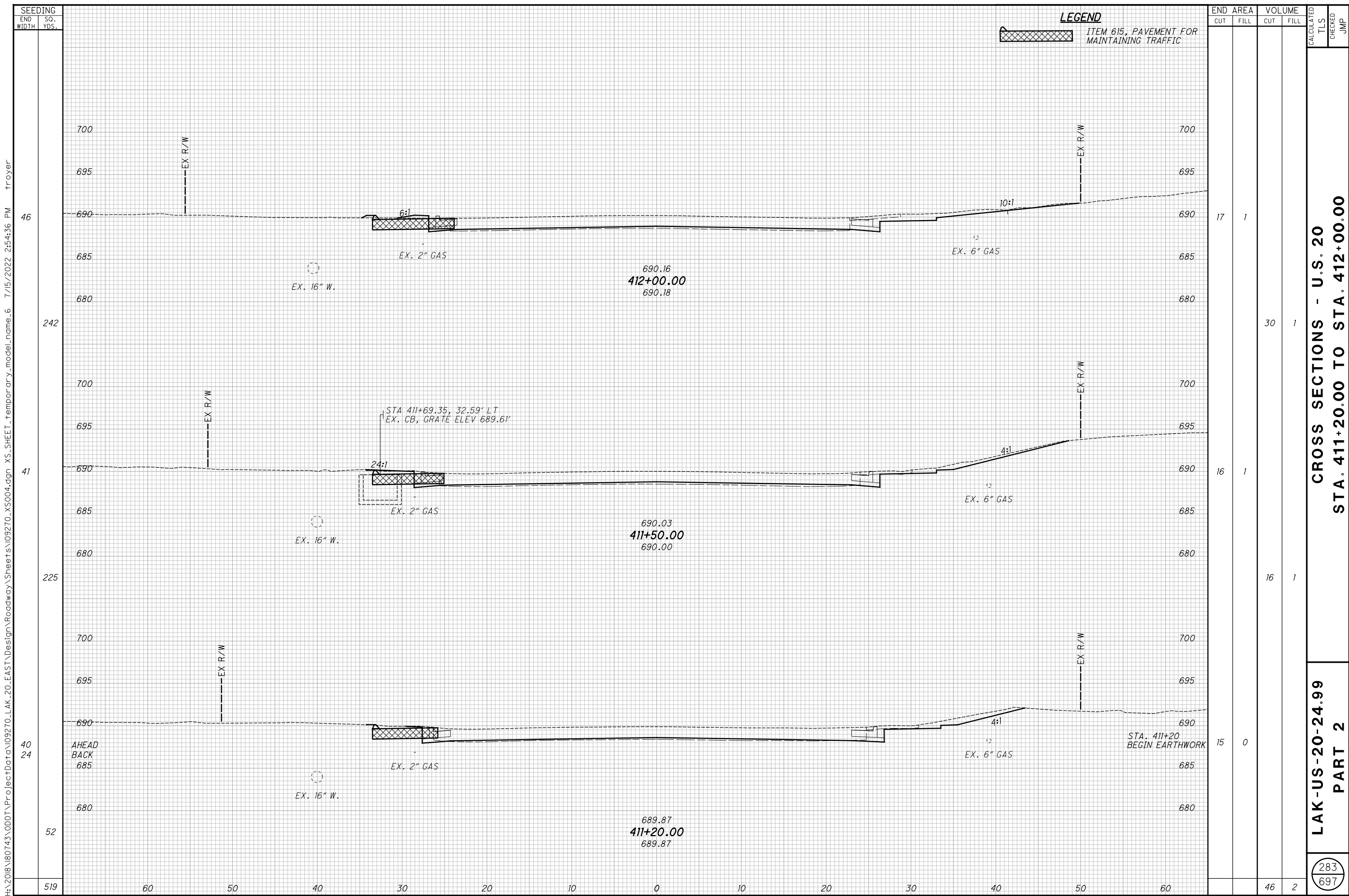
LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

CROSS SECTIONS - U.S. 20
 STA. 410+00.00 TO STA. 411+00.00

LAK-US-20-24.99
 PART 2

282
 697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_5 7/15/2022 2:54:35 PM Troyer

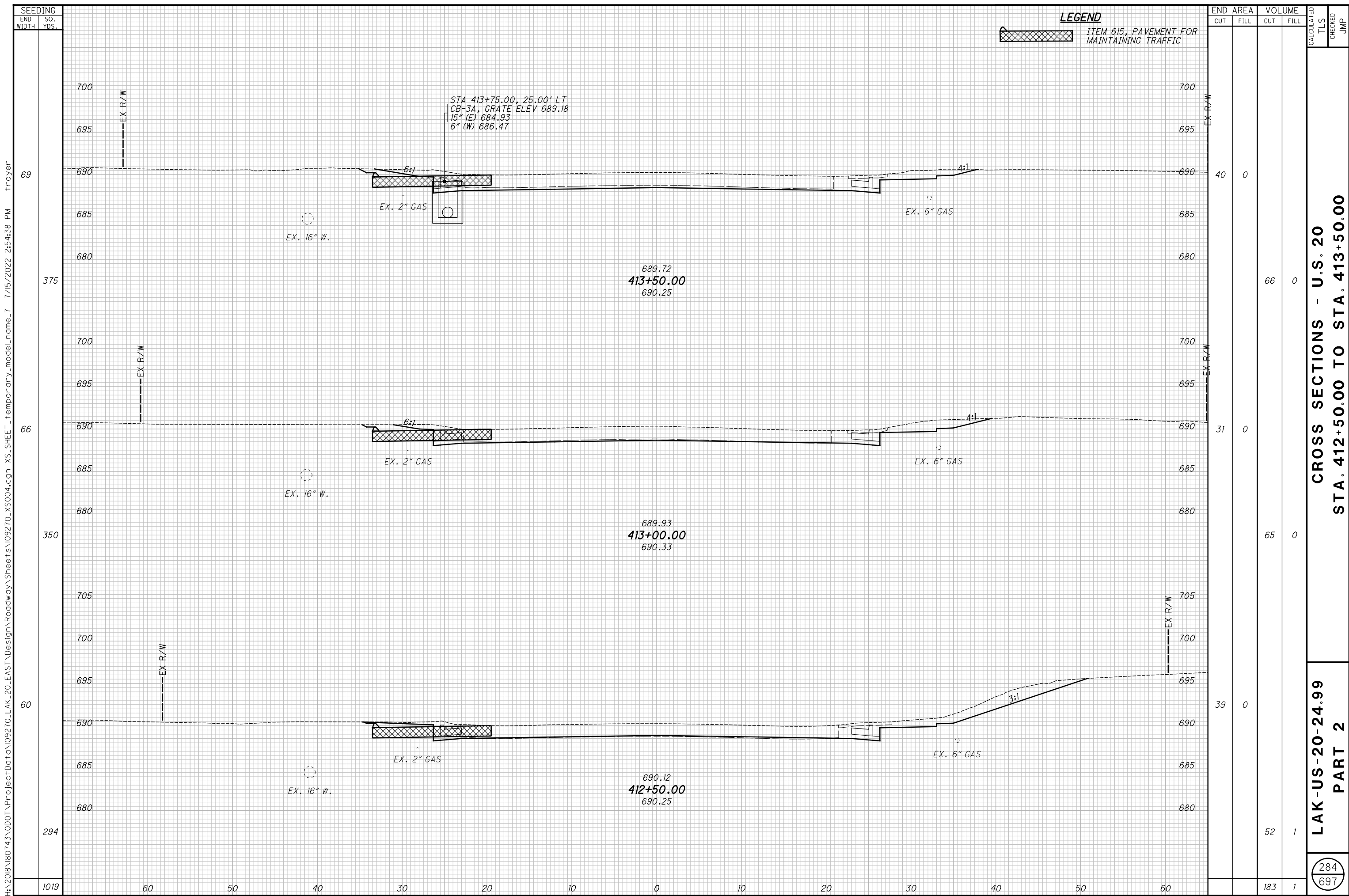


H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_6 7/15/2022 2:54:36 PM Troyer

CROSS SECTIONS - U.S. 20
STA. 411+20.00 TO STA. 412+00.00

LAK-US-20-24.99
PART 2

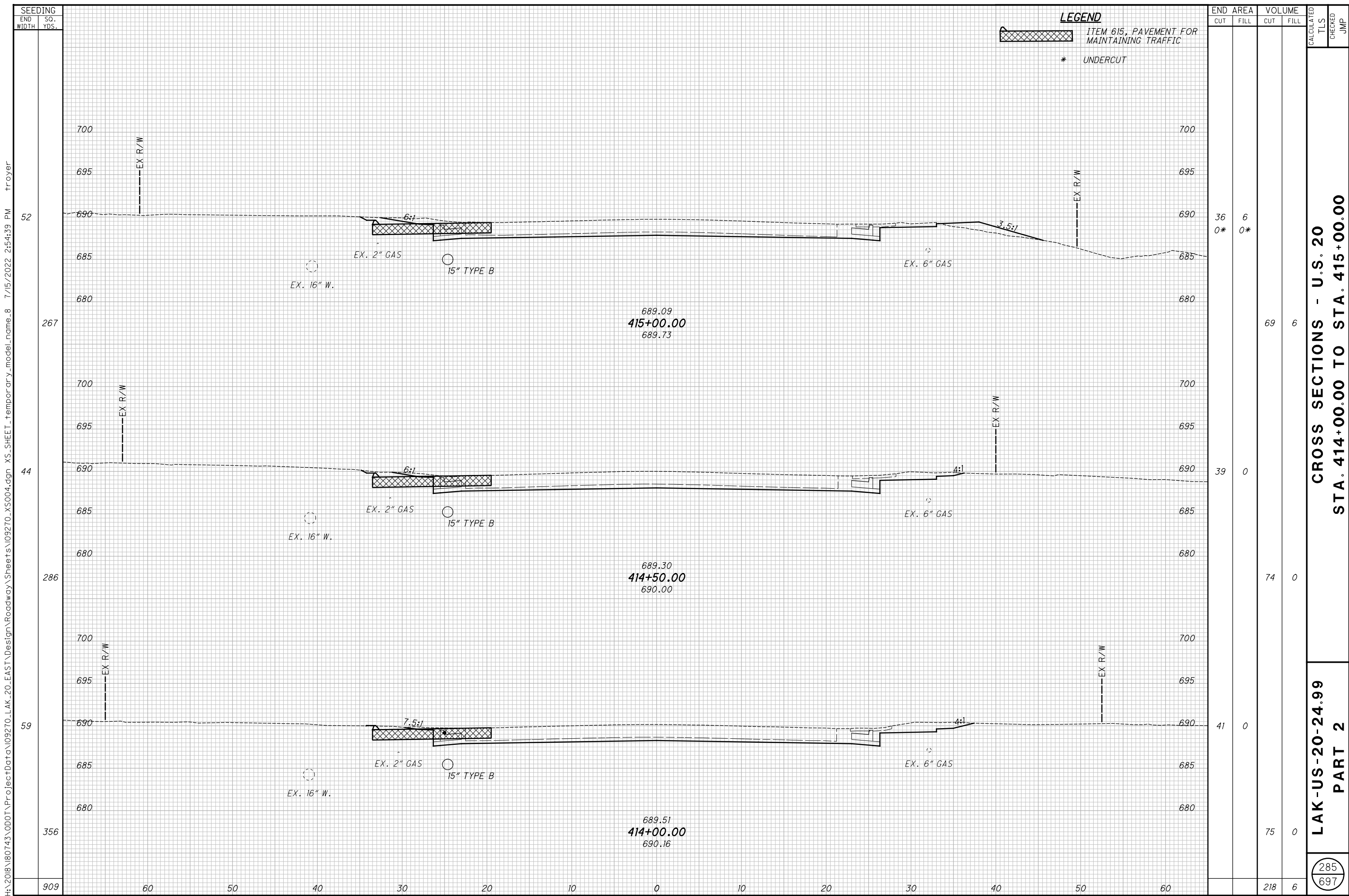
283
 697



CROSS SECTIONS - U.S. 20
STA. 412+50.00 TO STA. 413+50.00

LAK-US-20-24.99
PART 2

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_7 7/15/2022 2:54:38 PM Troyer



SEEDING	
END WIDTH	SO. YDS.
52	267
44	286
59	356
909	

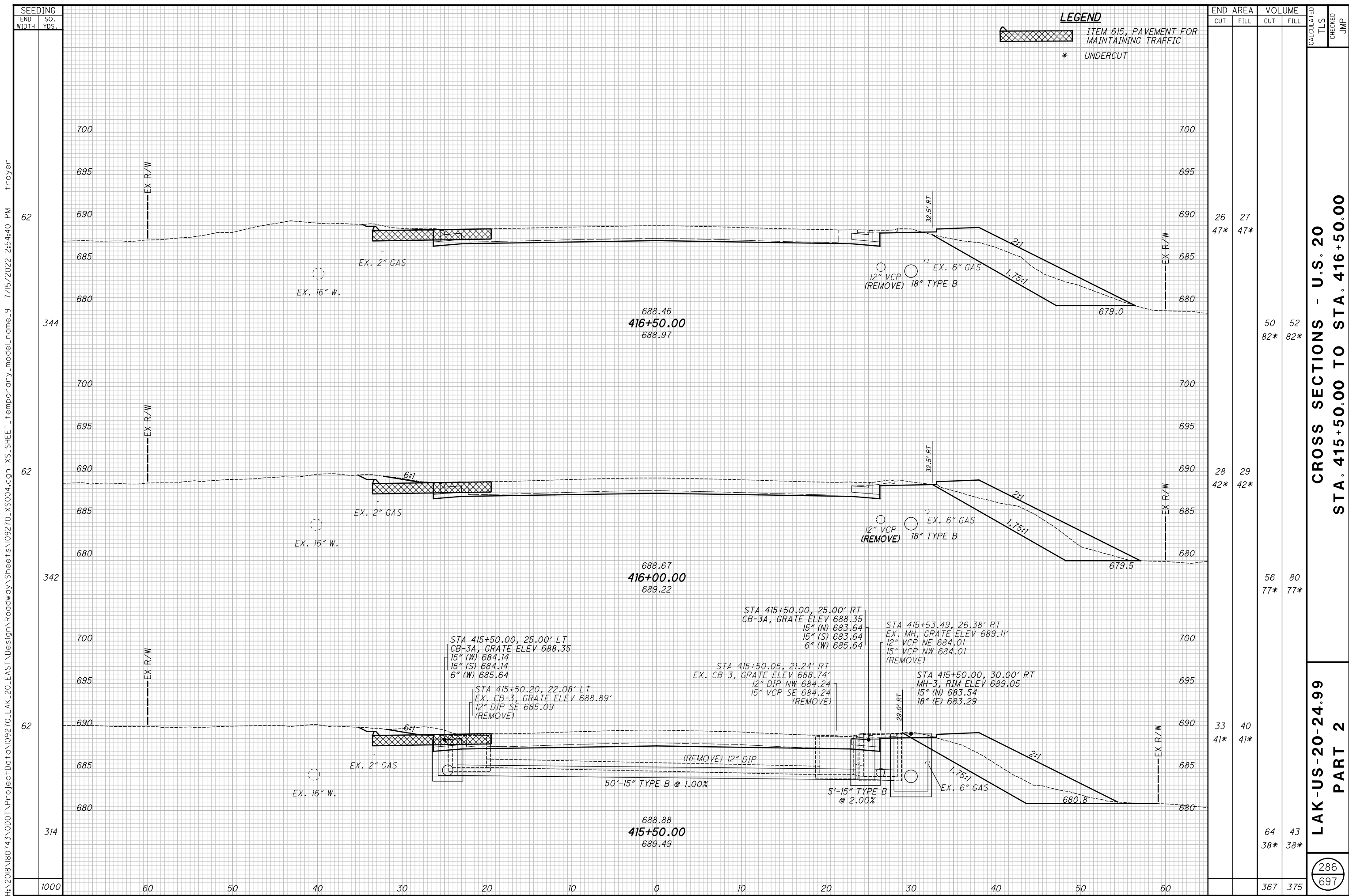
END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
36	6	0*	0*		
39	0	74	0		
41	0	75	0		
		218	6		

CROSS SECTIONS - U.S. 20
STA. 414+00.00 TO STA. 415+00.00

LAK-US-20-24.99
PART 2

(285)
697

H:\2018\180743\Projec+Data\09220_LAK_20_EAST\Design\Roadway\Sheets\09220_XS004.dgn XS_SHEET_temporary_model_name_8 7/15/2022 2:54:39 PM Troyer



LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC
 * UNDERCUT

END AREA	VOLUME	CALCULATED	CHECKED	JMP
26	27	47*	47*	
50	52	82*	82*	
28	29	42*	42*	
56	80	77*	77*	
33	40	41*	41*	
64	43	38*	38*	
367	375			

CROSS SECTIONS - U.S. 20
STA. 415+50.00 TO STA. 416+50.00

LAK-US-20-24.99
PART 2

286
 697

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_9 7/15/2022 2:54:40 PM Troyer

SEEDING	SO. YDS.	END WIDTH													
			62	344	342	62	314								
		1000	60	50	40	30	20	10	0	10	20	30	40	50	60

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_10 7/15/2022 2:54:41PM troyer

SEEDING	
END WIDTH	SO. YDS.
61	339
61	339
61	339
61	342
1320	

LEGEND

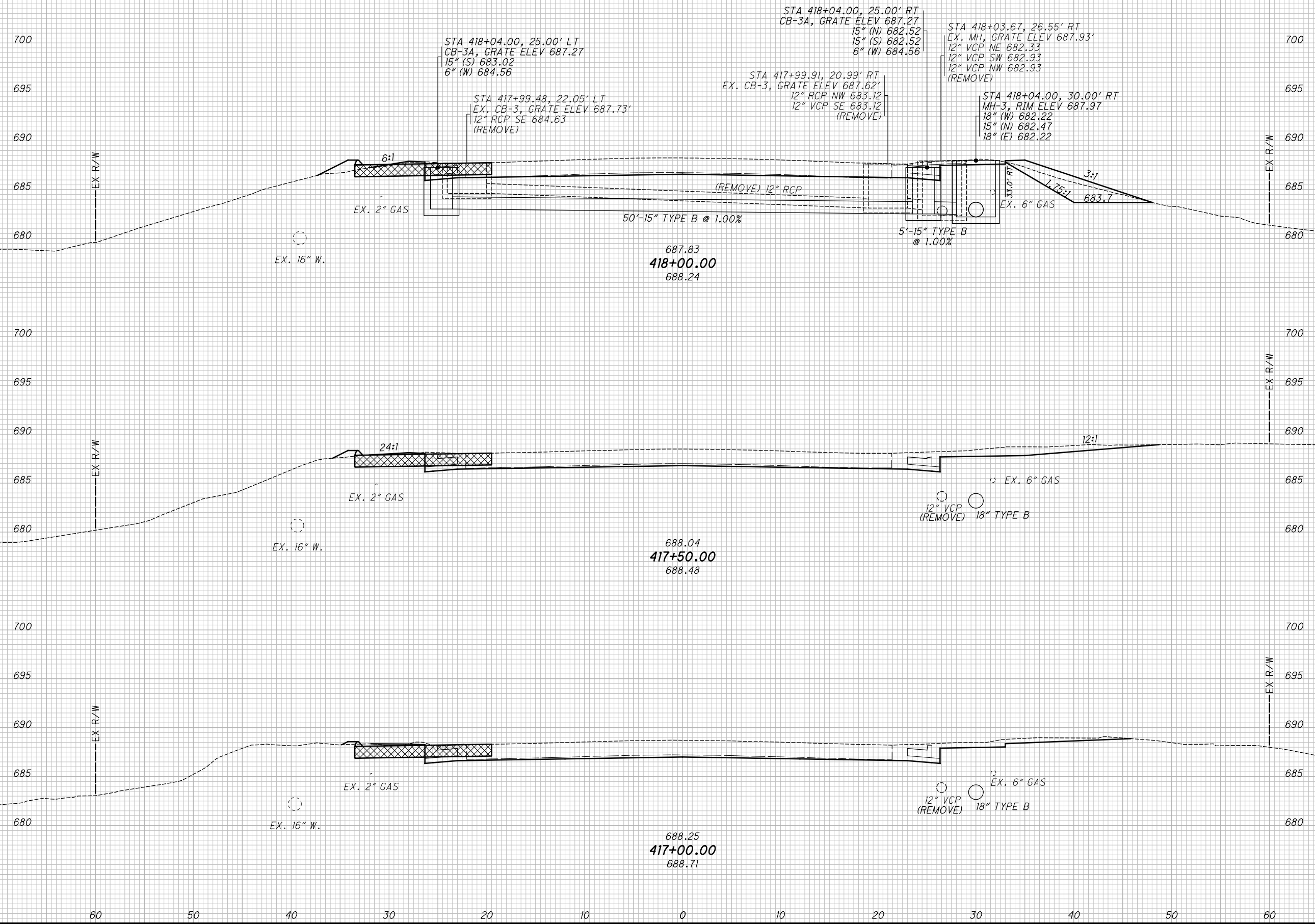
-  ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC
- * UNDERCUT

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
20	7	51	6		
17*	17*	16*	16*		
35	0	61	0		
0*	0*	0*	0*		
31	0	53	25		
0*	0*	44*	44*		
		225	91		

CROSS SECTIONS - U.S. 20
STA. 417+00.00 TO STA. 418+00.00

LAK-US-20-24.99
PART 2

287
697



STA 418+04.00, 25.00' LT
 CB-3A, GRATE ELEV 687.27
 15" (N) 682.52
 15" (S) 682.52
 6" (W) 684.56

STA 418+04.00, 25.00' RT
 CB-3A, GRATE ELEV 687.27
 15" (N) 682.52
 15" (S) 682.52
 6" (W) 684.56

STA 417+99.91, 20.99' RT
 EX. CB-3, GRATE ELEV 687.62'
 12" RCP NW 683.12
 12" VCP SE 683.12
 (REMOVE)

STA 418+03.67, 26.55' RT
 EX. MH, GRATE ELEV 687.93'
 12" VCP NE 682.33
 12" VCP SW 682.93
 12" VCP NW 682.93
 (REMOVE)

STA 417+99.48, 22.05' LT
 EX. CB-3, GRATE ELEV 687.73'
 12" RCP SE 684.63
 (REMOVE)

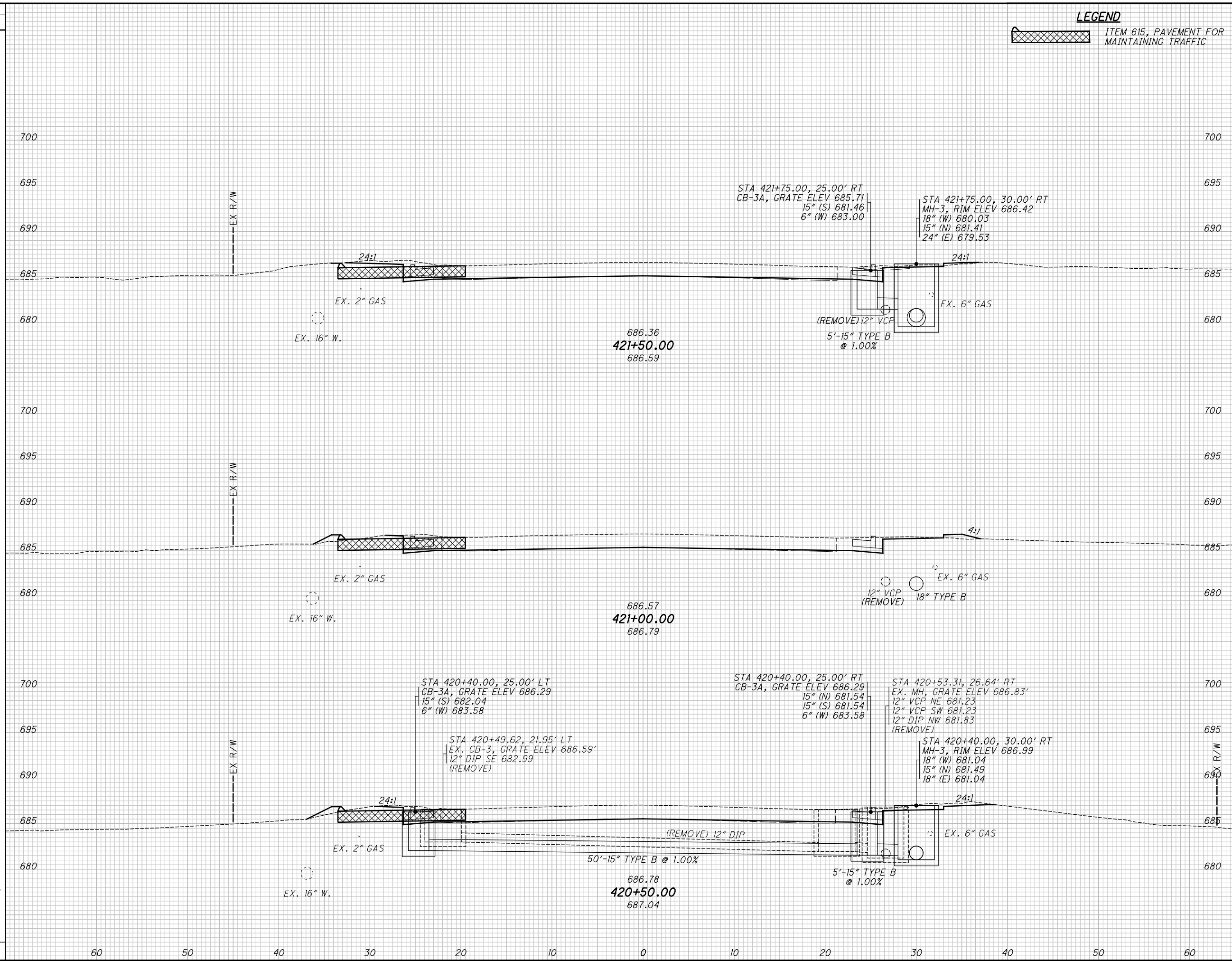
STA 418+04.00, 30.00' RT
 MH-3, RIM ELEV 687.97
 18" (W) 682.22
 15" (N) 682.47
 18" (E) 682.22

687.83
418+00.00
 688.24

688.04
417+50.00
 688.48

688.25
417+00.00
 688.71

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_12 7/15/2022 2:54:44 PM Troyer



LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
17	1			
16	1			
19	0			
96	9			

CROSS SECTIONS - U.S. 20
STA. 420+50.00 TO STA. 421+50.00
LAK-US-20-24.99
PART 2

289
 697

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_13 7/15/2022 2:54:46 PM troyer

SEEDING	
END WIDTH	SO. YDS.
948	60
57	50
57	40
57	30
57	20
57	10
57	0
57	10
57	20
57	30
57	40
57	50
57	60

LEGEND

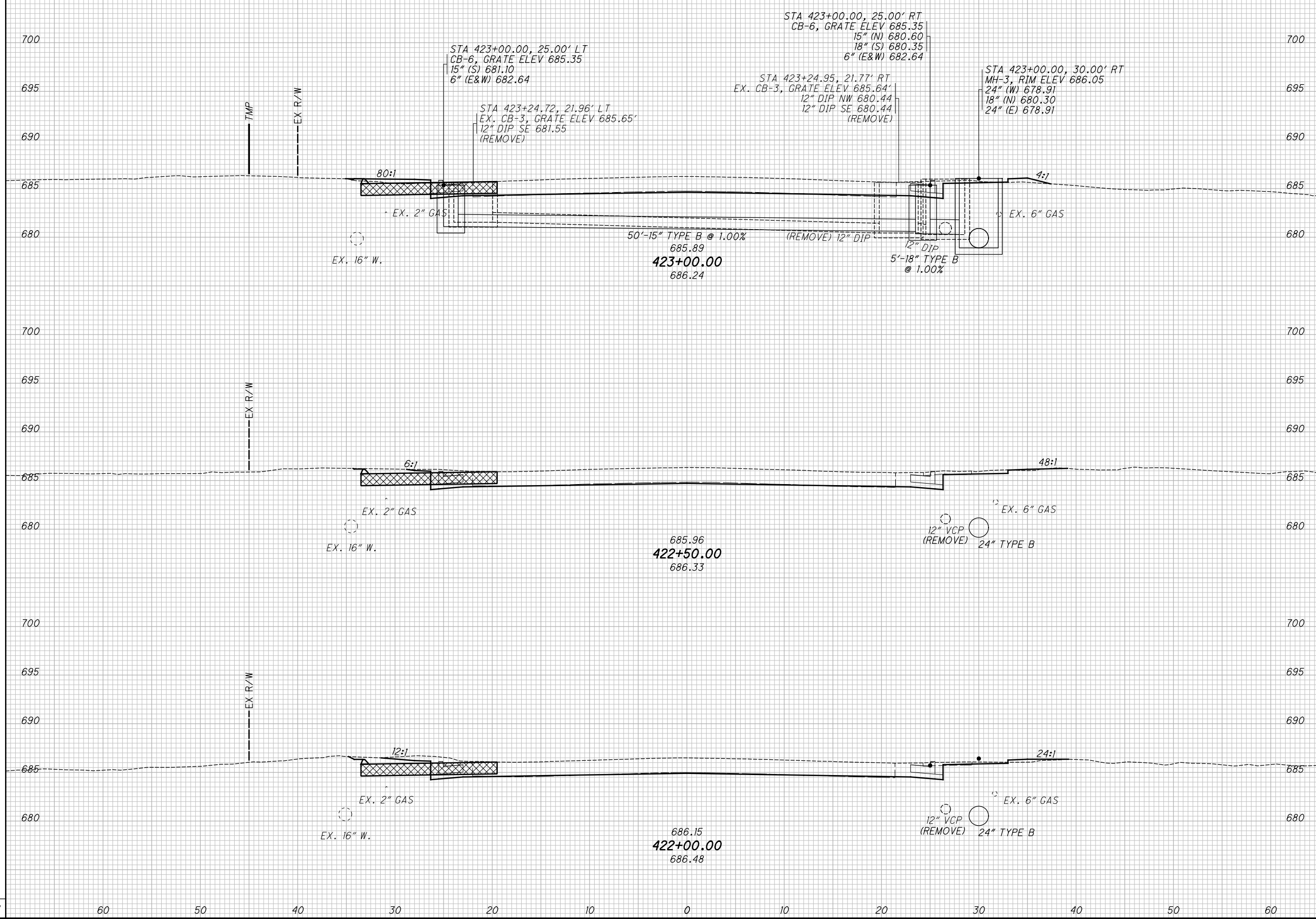
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

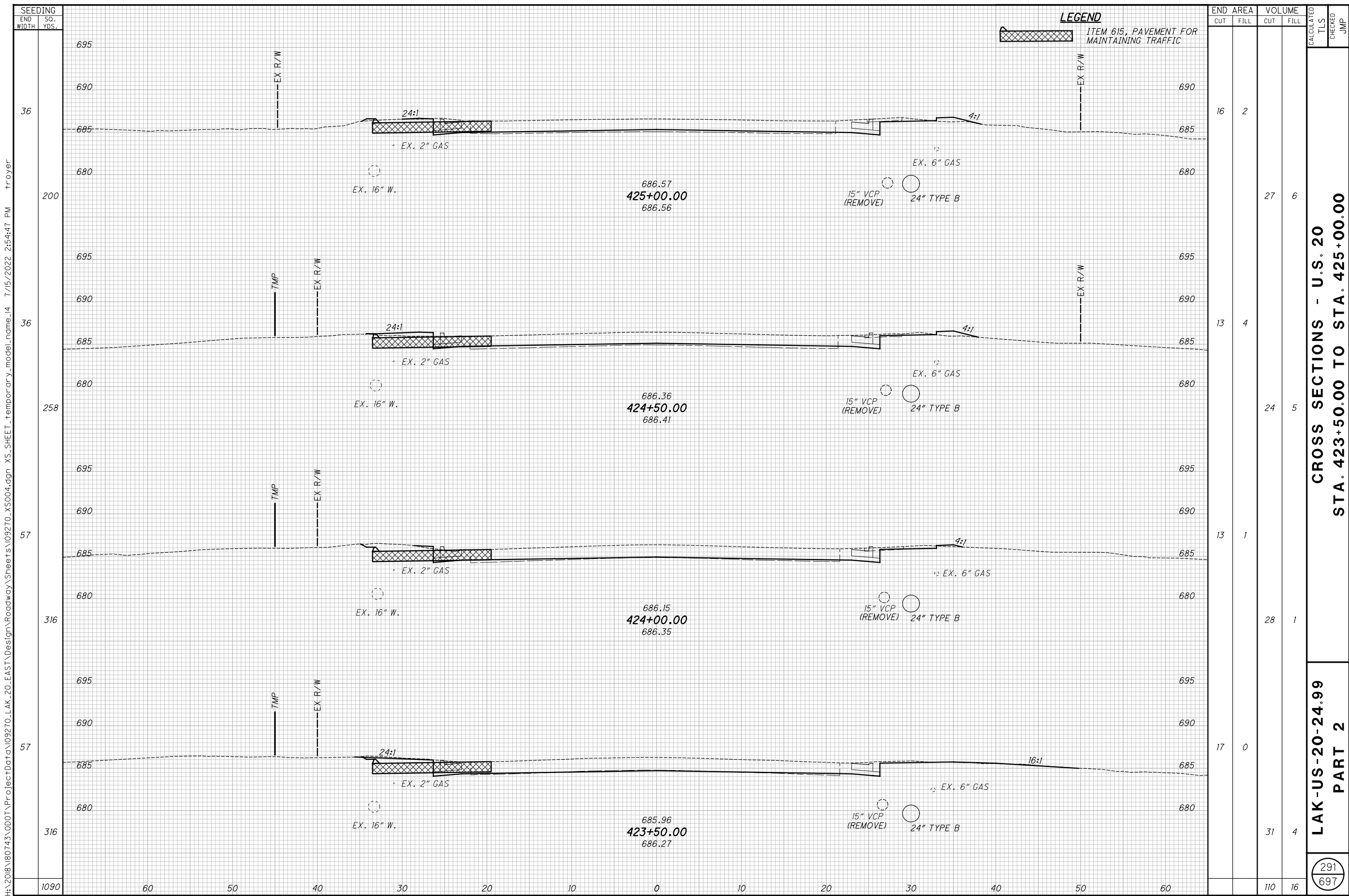
END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
17	4				
		33	4		
19	0				
		35	1		
19	1				
		33	2		
		101	7		

CROSS SECTIONS - U.S. 20
STA. 422+00.00 TO STA. 423+00.00

LAK-US-20-24.99
PART 2

290
697





LEGEND
 [Hatched Box] ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END STA.	AREA		VOLUME		CALCULATED	CHECKED
	CUT	FILL	CUT	FILL		
423+50.00	17	0	31	4		
424+00.00	13	1	28	1		
424+50.00	13	4	24	5		
425+00.00	16	2	27	6		
TOTAL	57	7	110	16		

CROSS SECTIONS - U.S. 20
STA. 423+50.00 TO STA. 425+00.00

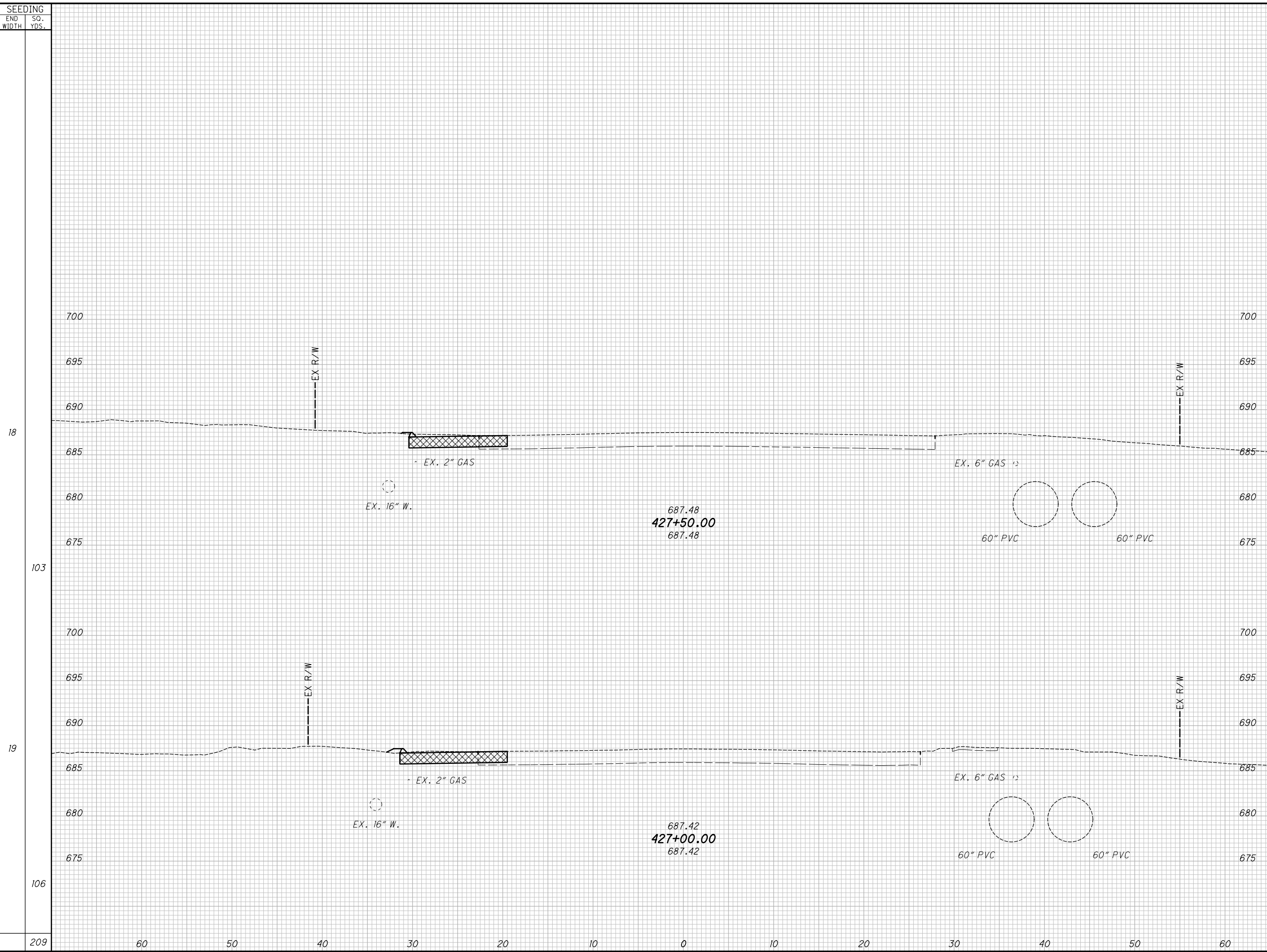
LAK-US-20-24.99
PART 2

291
 697

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_14 7/15/2022 2:54:47 PM Troyer

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_16 7/15/2022 2:54:50 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
	CUT	FILL	CUT	FILL		
END WIDTH						
SO. YDS.						



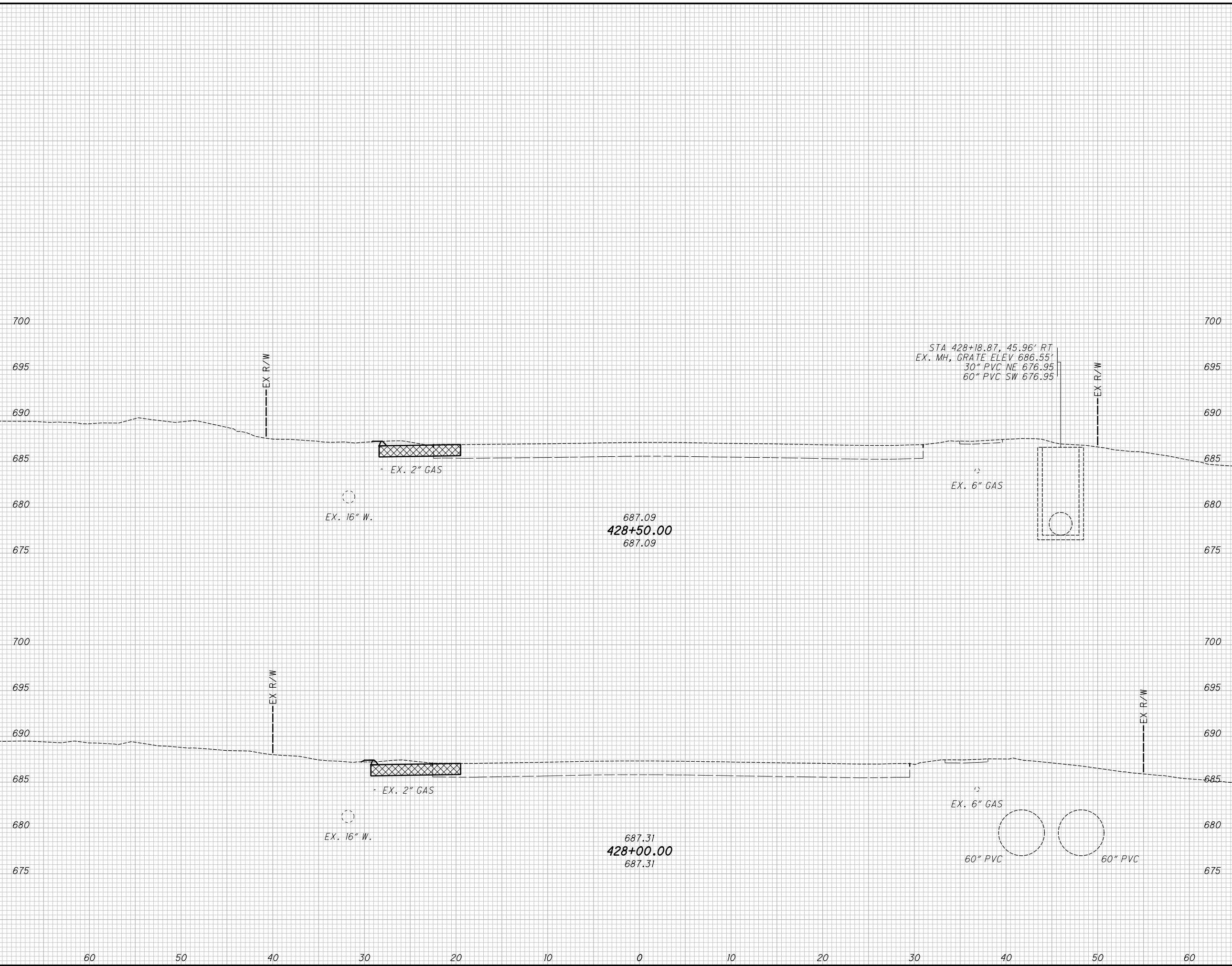
SEEDING	END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
	CUT	FILL	CUT	FILL		
END WIDTH						
SO. YDS.						

LAK-US-20-24.99
PART 2
CROSS SECTIONS - U.S. 20
STA. 427+00.00 TO STA. 427+50.00

293
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_17 7/15/2022 2:54:51PM troyer

SEEDING	
END WIDTH	SO. YDS.
200	
60	
50	
40	
30	
20	
10	
0	
10	
20	
30	
40	
50	
60	



END AREA		VOLUME	
CUT	FILL	CUT	FILL
		0	0

CALCULATED	CHECKED
TLS	JMP

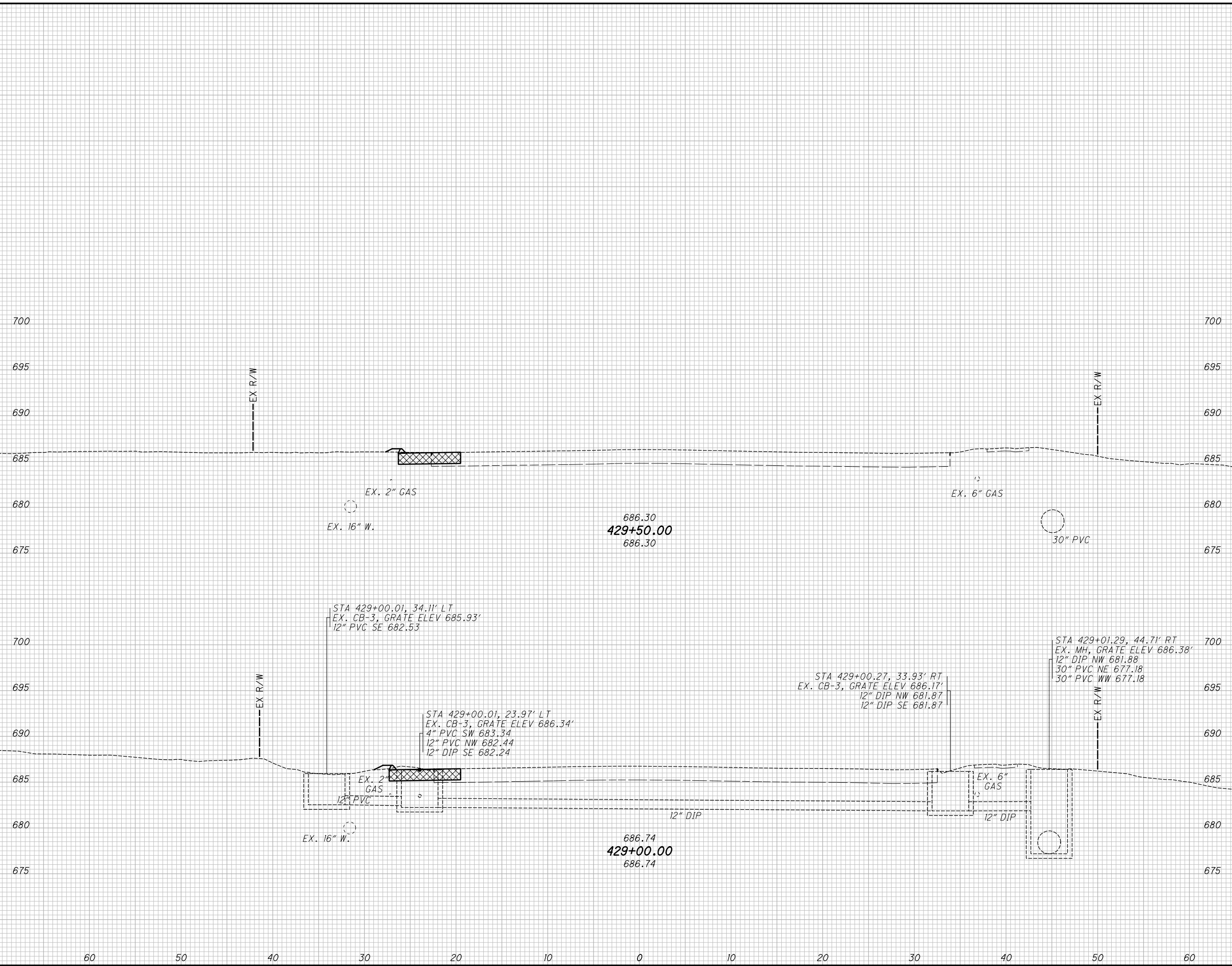
CROSS SECTIONS - U.S. 20
STA. 428+00.00 TO STA. 428+50.00

LAK-US-20-24.99
PART 2

294
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_18 7/15/2022 2:54:51PM troyer

SEEDING	
END WIDTH	SO. YDS.
203	
100	
103	
19	



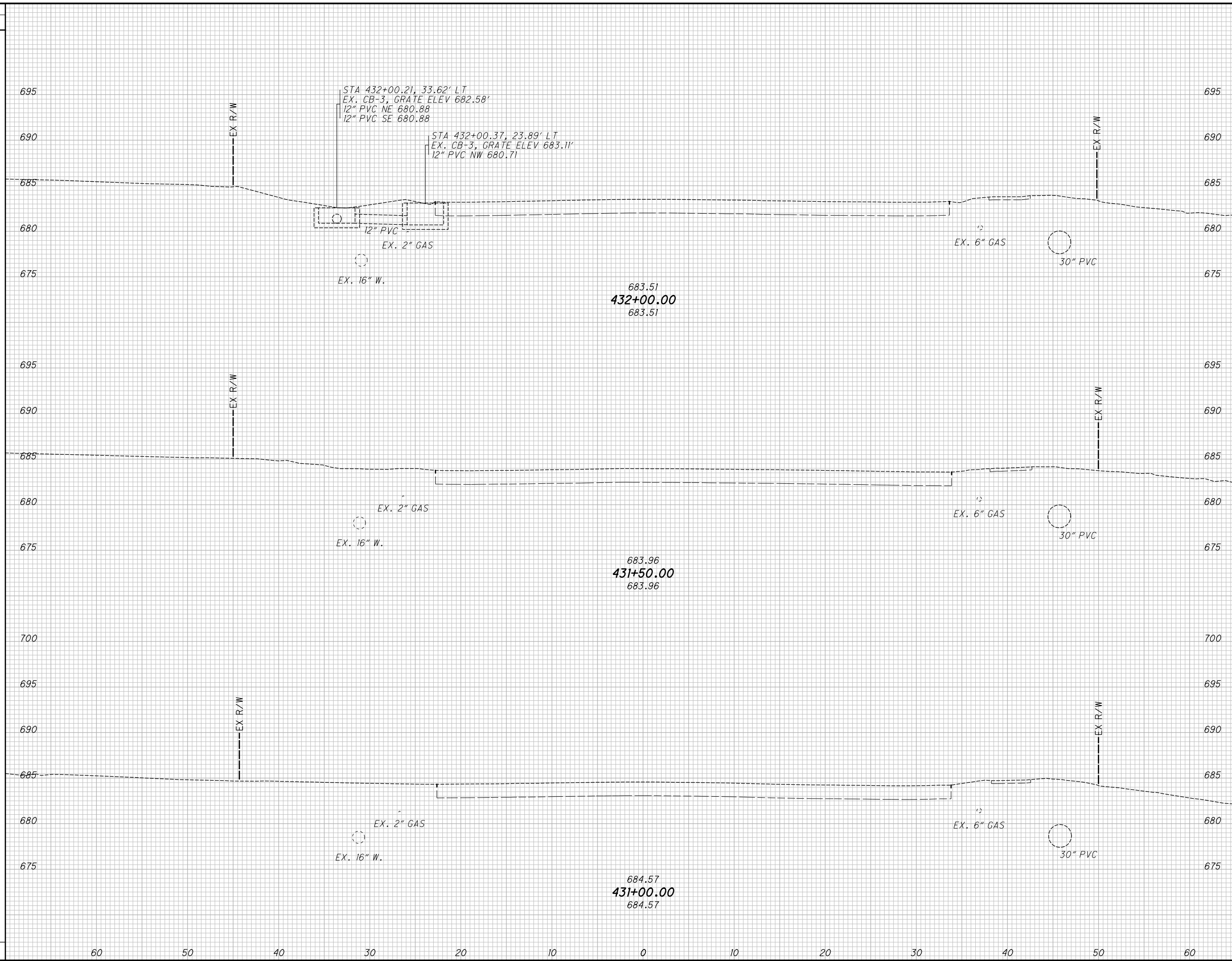
END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
		0	0		

LAK-US-20-24.99
PART 2
CROSS SECTIONS - U.S. 20
STA. 429+00.00 TO STA. 429+50.00

295
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_20 7/15/2022 2:54:53 PM Troyer

SEEDING	
END WIDTH	SO. YDS.
0	



END AREA		VOLUME	
CUT	FILL	CUT	FILL
		0	0

CALCULATED	CHECKED
TLS	JMP

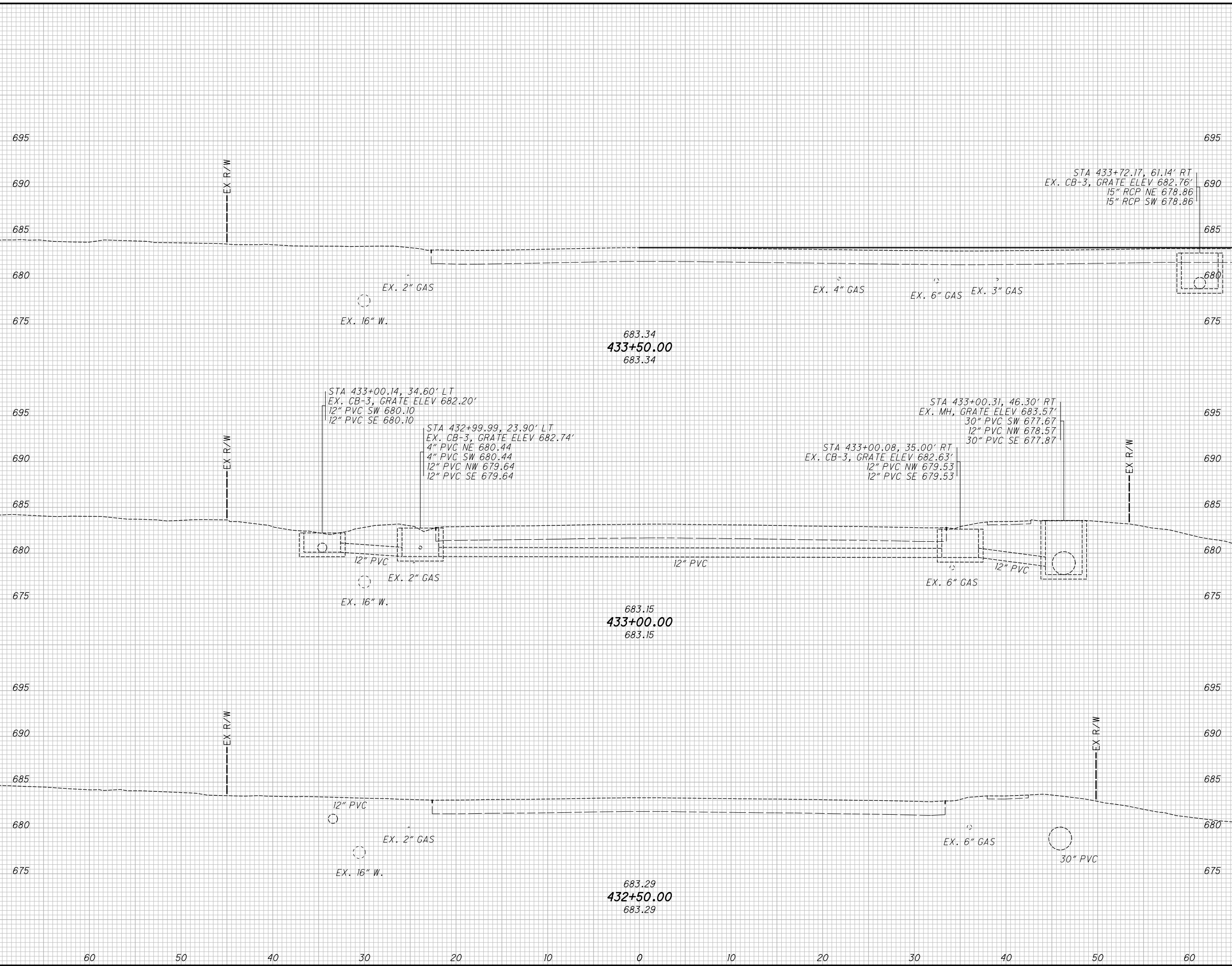
CROSS SECTIONS - U.S. 20
STA. 431+00.00 TO STA. 432+00.00

LAK-US-20-24.99
PART 2

297
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_21 7/15/2022 2:54:55 PM troyer

SEEDING	
END WIDTH	SO. YDS.
0	



END AREA		VOLUME	
CUT	FILL	CUT	FILL
		0	0

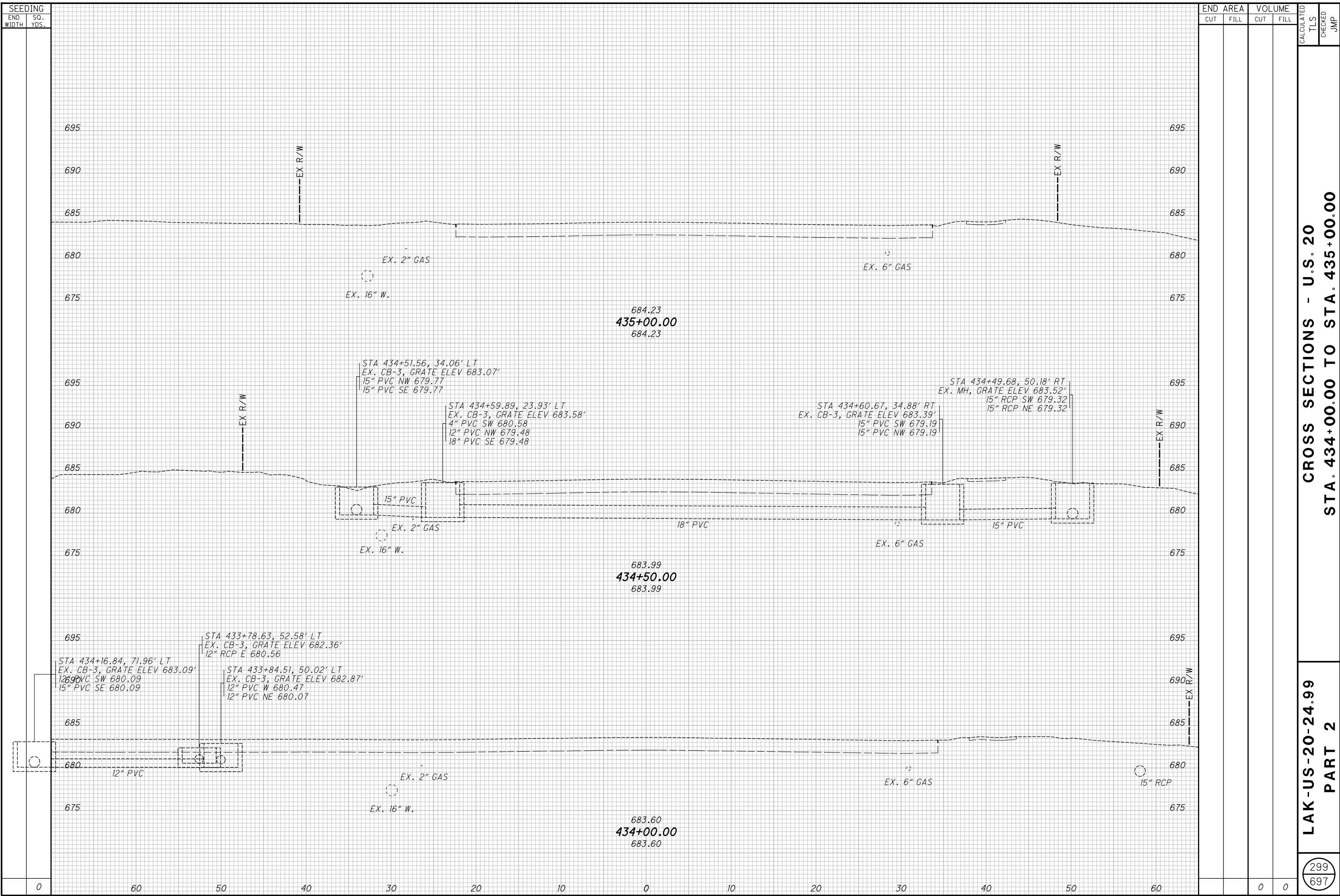
CALCULATED	CHECKED
TLS	JMP

CROSS SECTIONS - U.S. 20
 STA. 432+50.00 TO STA. 433+50.00

LAK-US-20-24.99
 PART 2

298
 697

H:\2018\180743\0001\ProjectData\092TO_LAK_20_EAST\Design\Roadway\Sheets\092TO_XS004.dgn XS_SHEET_temporary_model_name_22 7/15/2022 2:54:56 PM troyer



SEEDING		END AREA		VOLUME		CALCULATED		CHECKED	
END WIDTH	SO. YDS.	CUT	FILL	CUT	FILL	TLS	JMP	TLS	JMP
0				0	0				

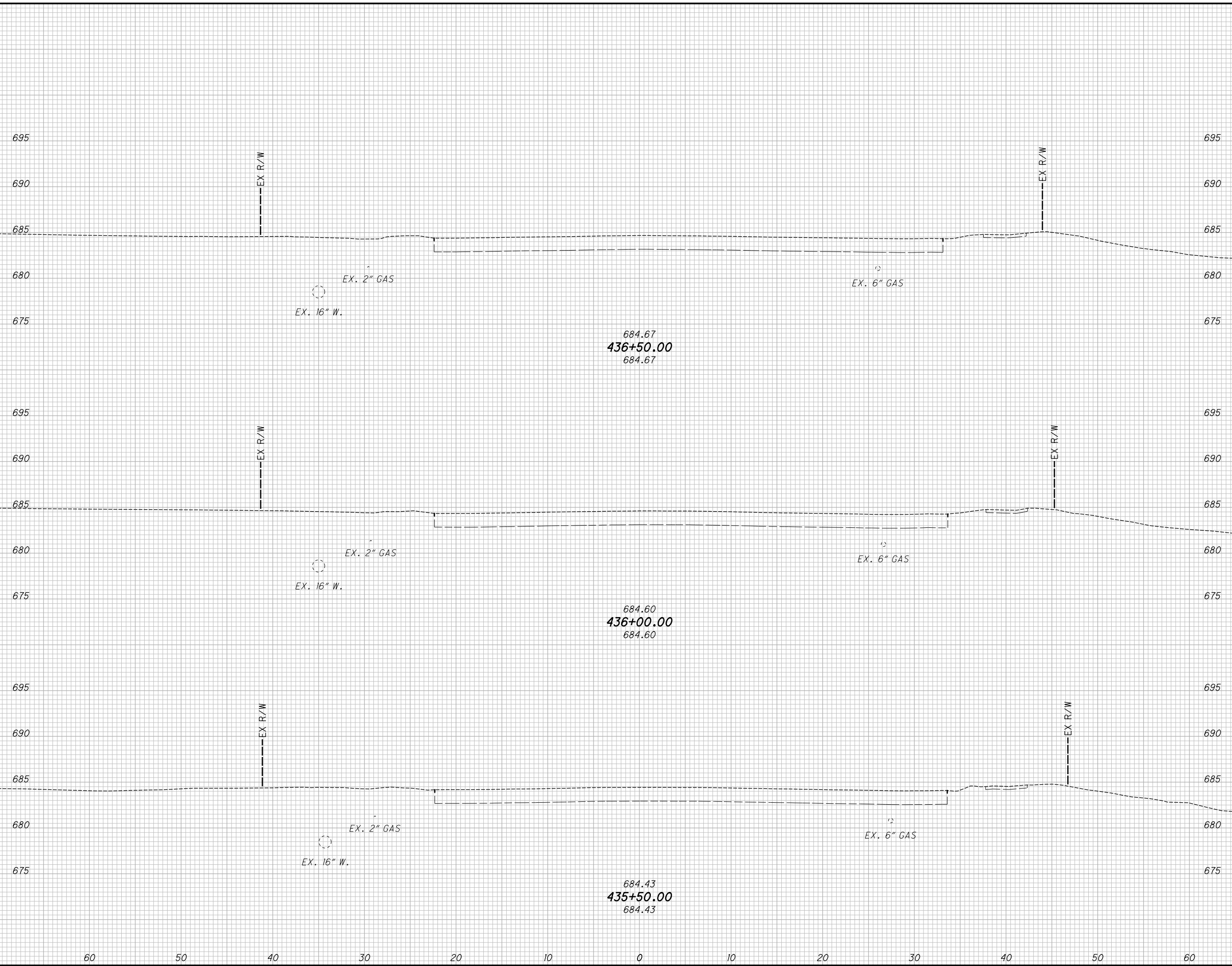
CROSS SECTIONS - U.S. 20
STA. 434+00.00 TO STA. 435+00.00

LAK-US-20-24.99
PART 2

299
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_23 7/15/2022 2:54:57 PM troyer

SEEDING	
END WIDTH	SO. YDS.
0	



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
		0	0		

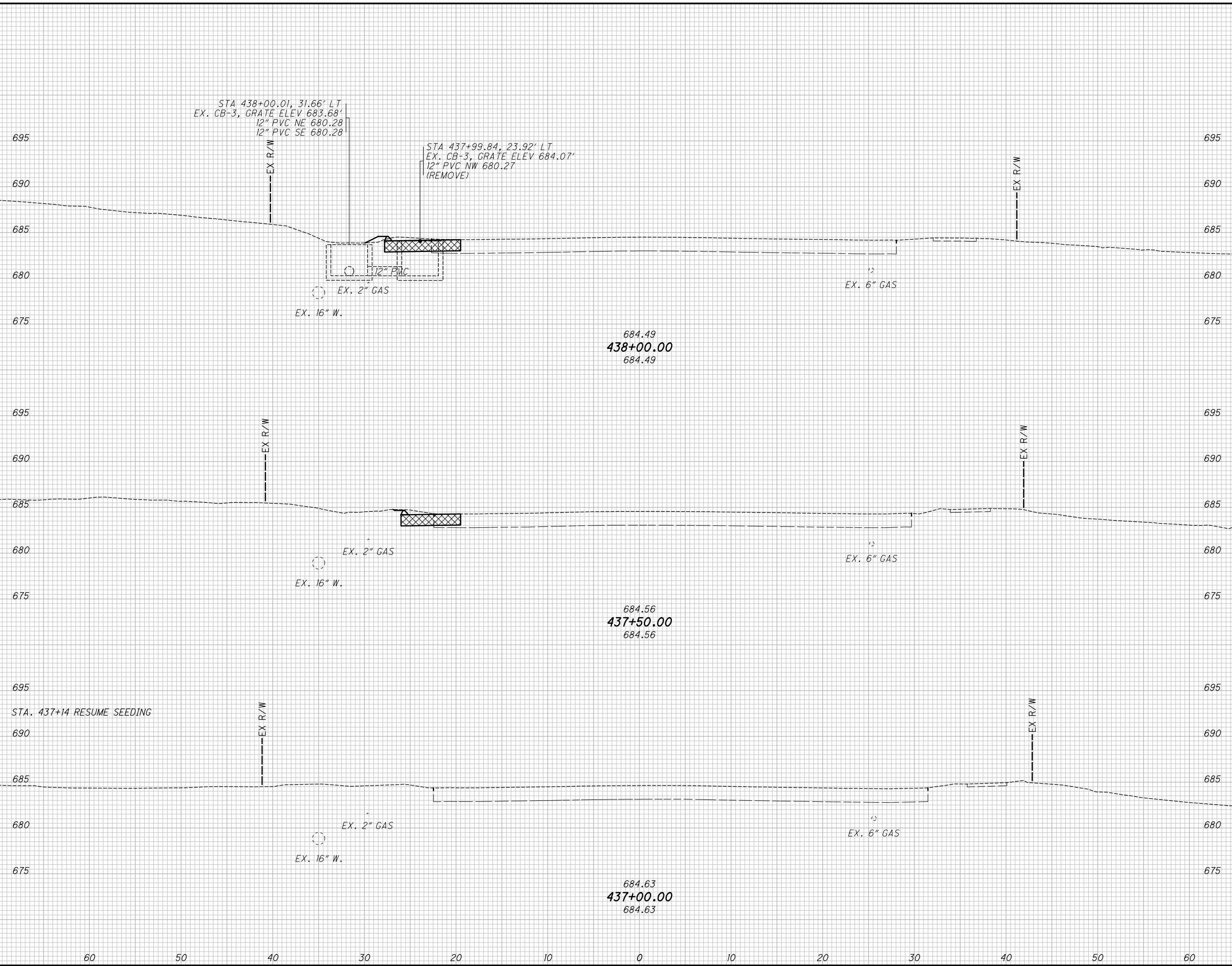
LAK-US-20-24.99
PART 2

CROSS SECTIONS - U.S. 20
STA. 435+50.00 TO STA. 436+50.00

300
697

H:\2018\180743\ProjectData\092TO_LAK_20_EAST\Design\Roadway\Sheets\092TO_XS004.dgn XS_SHEET_temporary_model_name_24 7/15/2022 2:54:58 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED
	CUT	FILL	CUT	FILL		
END WIDTH						
SO. YDS.						
172			0	0		



END AREA		VOLUME		CALCULATED	CHECKED
CUT	FILL	CUT	FILL		
		0	0		

LAK-US-20-24.99
PART 2

CROSS SECTIONS - U.S. 20
STA. 437+00.00 TO STA. 438+00.00

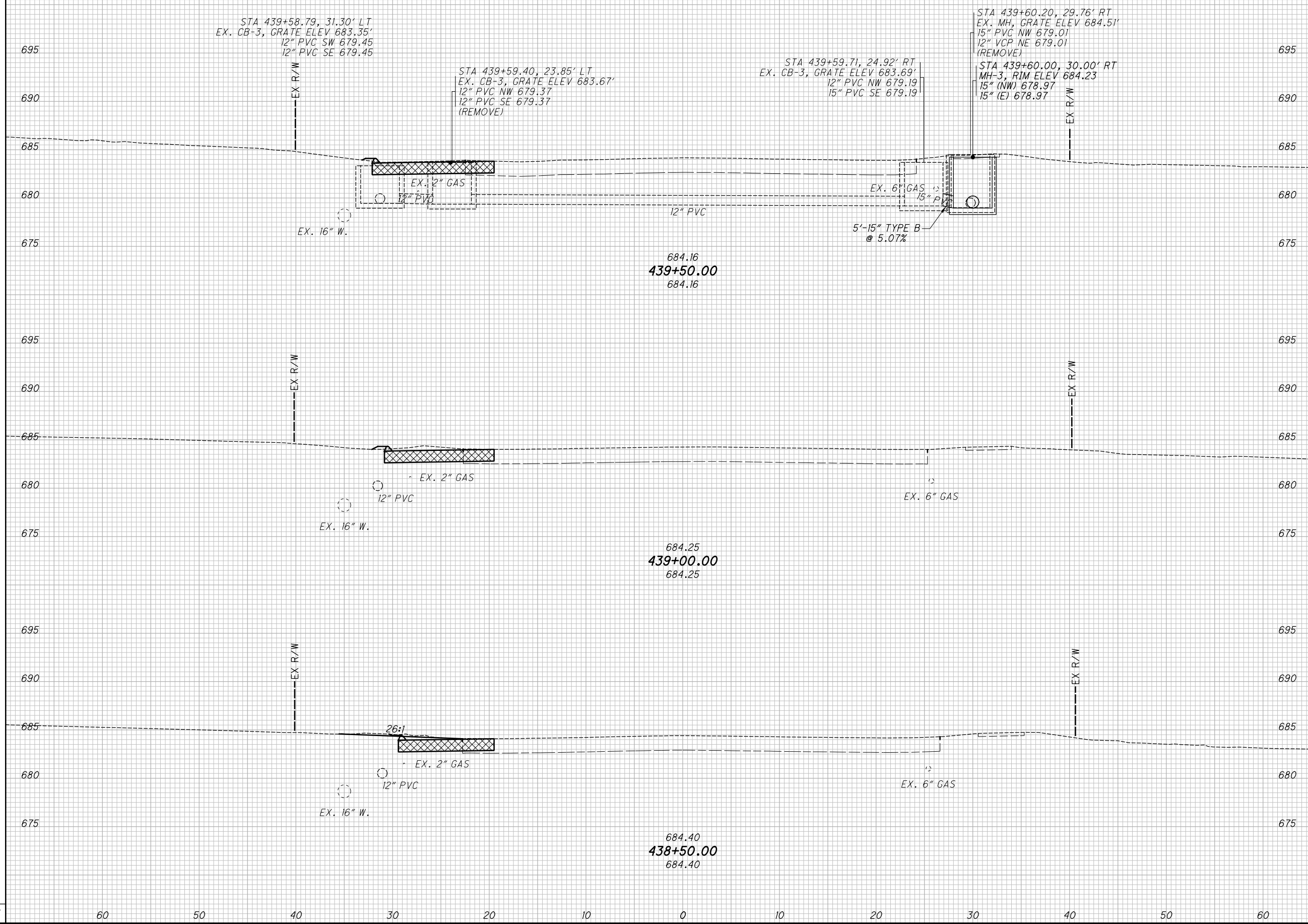
301
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_25 7/15/2022 2:54:59 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
END WIDTH							
SO. YDS.							
17							
94							
17							
94							
17							
97							
285							

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 438+50.00 TO STA. 439+50.00

LAK-US-20-24.99
PART 2

302
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_27 7/15/2022 2:55:02 PM troyer

SEEDING	
END WIDTH	SO. YDS.
30	695
30	690
167	685
30	680
30	675
167	670
30	665
30	660
161	655
495	650

LEGEND

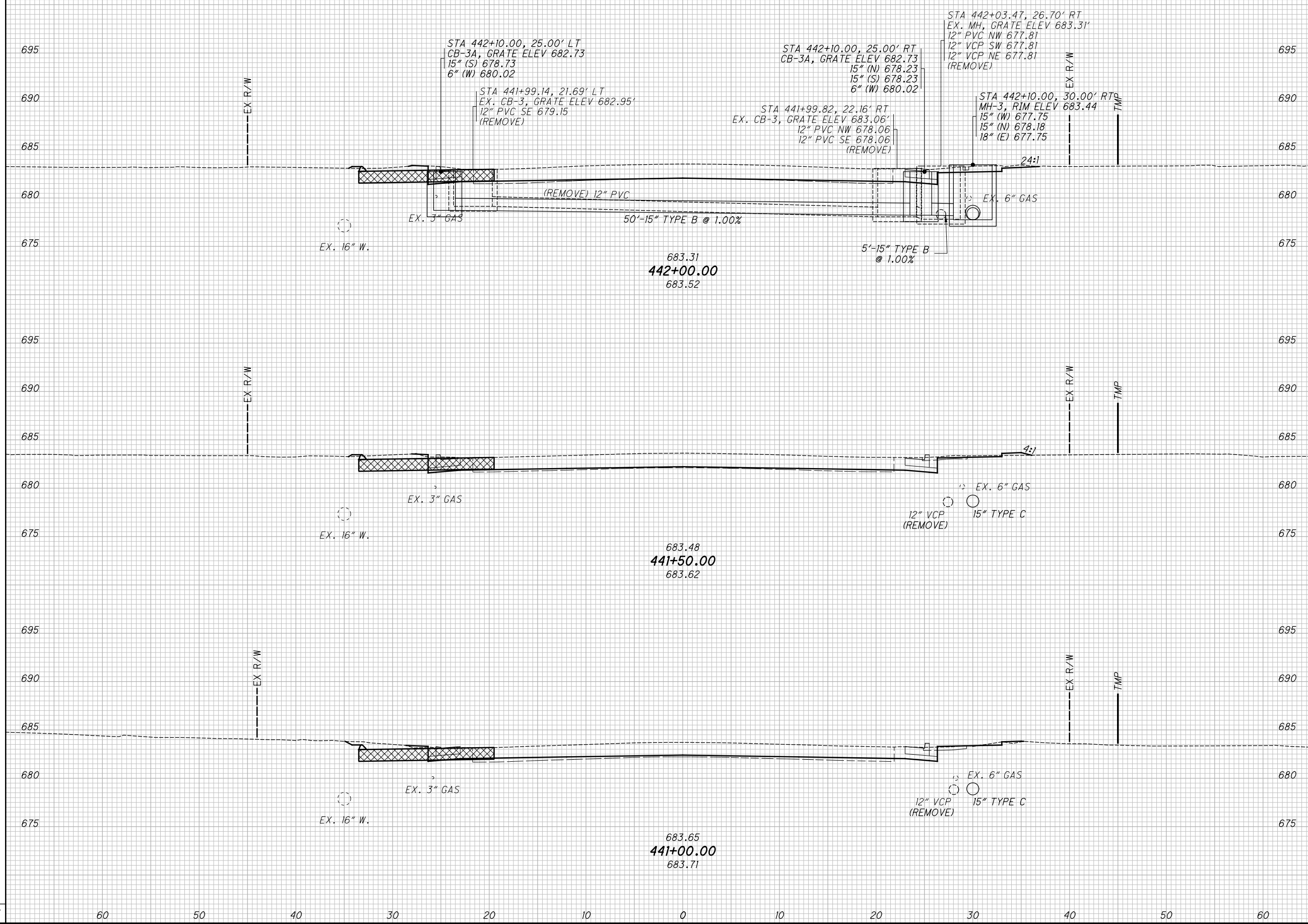
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
18	0	29	1		
13	1	22	2		
11	1	27	1		
		78	4		

**CROSS SECTIONS - U.S. 20
STA. 441+00.00 TO STA. 442+00.00**

**LAK-US-20-24.99
PART 2**

304
697



STA 442+10.00, 25.00' LT
CB-3A, GRATE ELEV 682.73
15" (S) 678.73
6" (W) 680.02

STA 442+10.00, 25.00' RT
CB-3A, GRATE ELEV 682.73
15" (N) 678.23
15" (S) 678.23
6" (W) 680.02

STA 442+03.47, 26.70' RT
EX. MH, GRATE ELEV 683.31'
12" PVC NW 677.81
12" VCP SW 677.81
12" VCP NE 677.81
(REMOVE)

STA 441+99.14, 21.69' LT
EX. CB-3, GRATE ELEV 682.95'
12" PVC SE 679.15
(REMOVE)

STA 441+99.82, 22.16' RT
EX. CB-3, GRATE ELEV 683.06'
12" PVC NW 678.06
12" PVC SE 678.06
(REMOVE)

STA 442+10.00, 30.00' RT
MH-3, RIM ELEV 683.44
15" (W) 677.75
15" (N) 678.18
18" (E) 677.75

683.31
442+00.00
683.52

683.48
441+50.00
683.62

683.65
441+00.00
683.71

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_28 7/15/2022 2:55:03 PM troyer

SEEDING	
END WIDTH	SO. YDS.
30	685
167	675
30	685
167	675
30	685
167	675
501	675

LEGEND

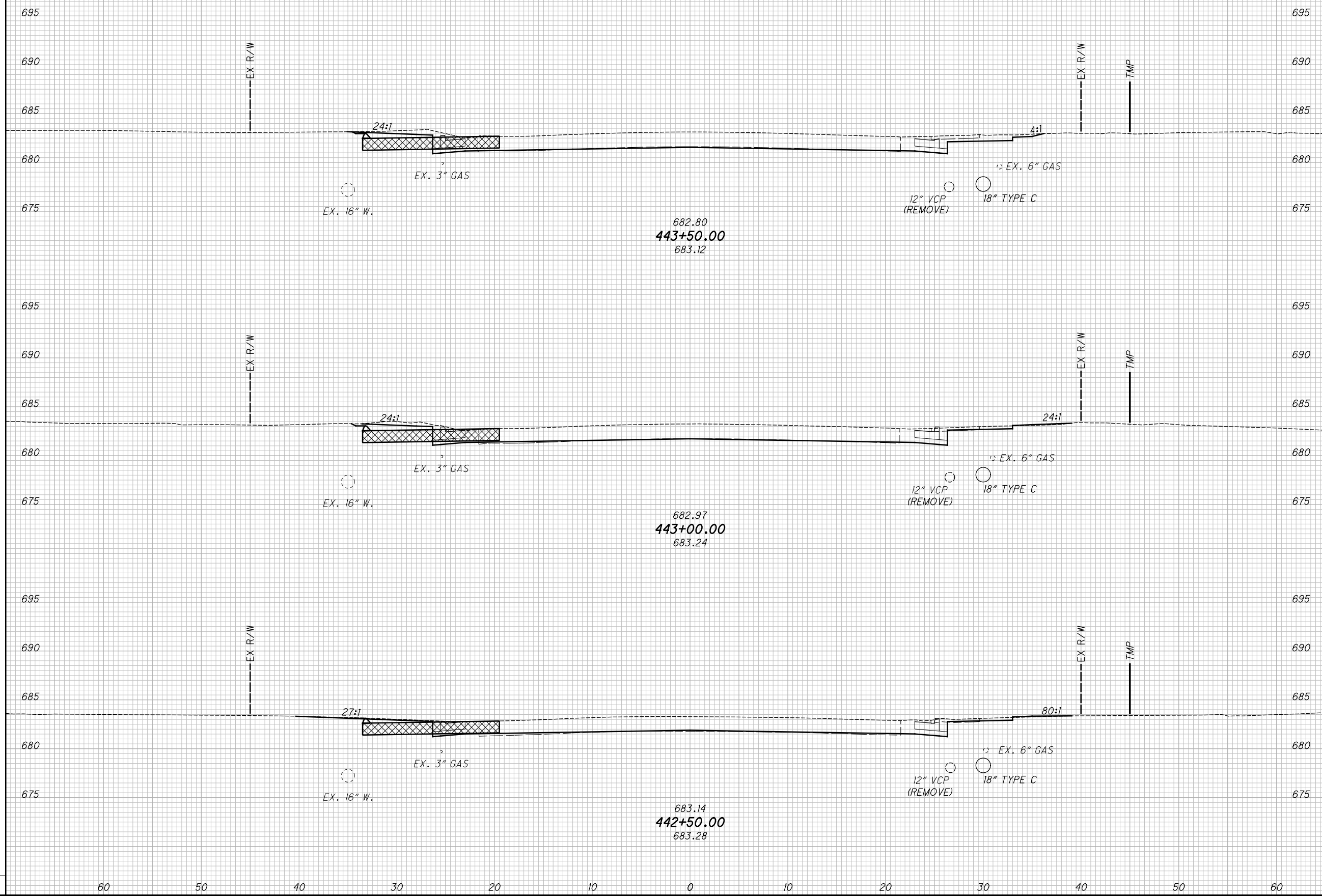
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
23	0				
18	0				
15	0				
		100	0		

**CROSS SECTIONS - U.S. 20
STA. 442+50.00 TO STA. 443+50.00**

**LAK-US-20-24.99
PART 2**

305
697

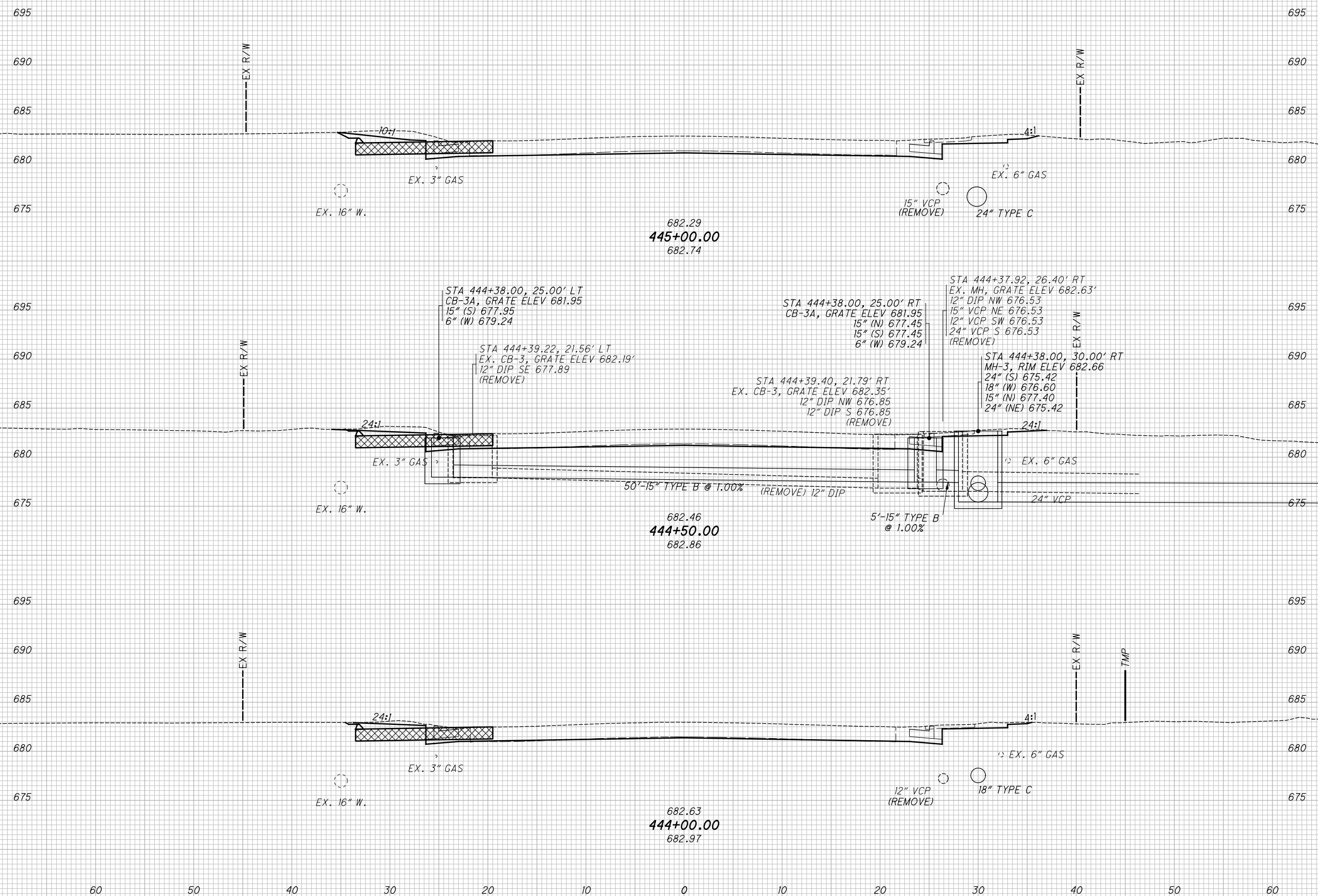


H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_29 7/15/2022 2:55:04 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
26	31	0					
144			52	0			
26							
156			42	0			
30							
167			40	0			
467			134	0			

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 444+00.00 TO STA. 445+00.00

LAK-US-20-24.99
PART 2

306
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_30 7/15/2022 2:55:06 PM Troyer

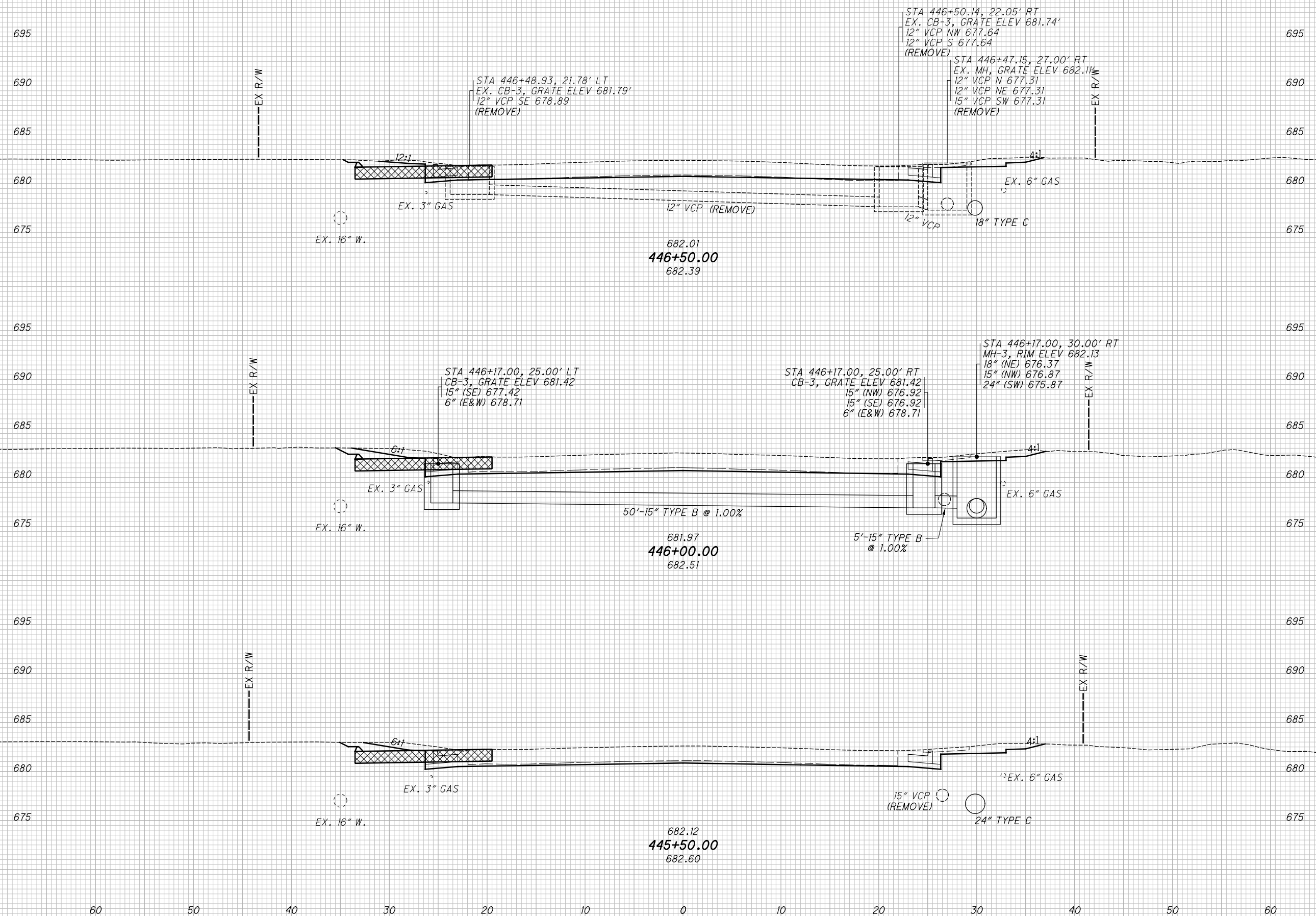
SEEDING	
END WIDTH	SO. YDS.
26	695
144	680
26	685
144	675
26	695
144	680
26	685
144	675
432	680

LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
23	0	53	0		
34	0	62	0		
33	0	59	0		
		174	0		

CROSS SECTIONS - U.S. 20
STA. 445+50.00 TO STA. 446+50.00
LAK-US-20-24.99
PART 2

307
697



H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_31 7/15/2022 2:55:07 PM troyer

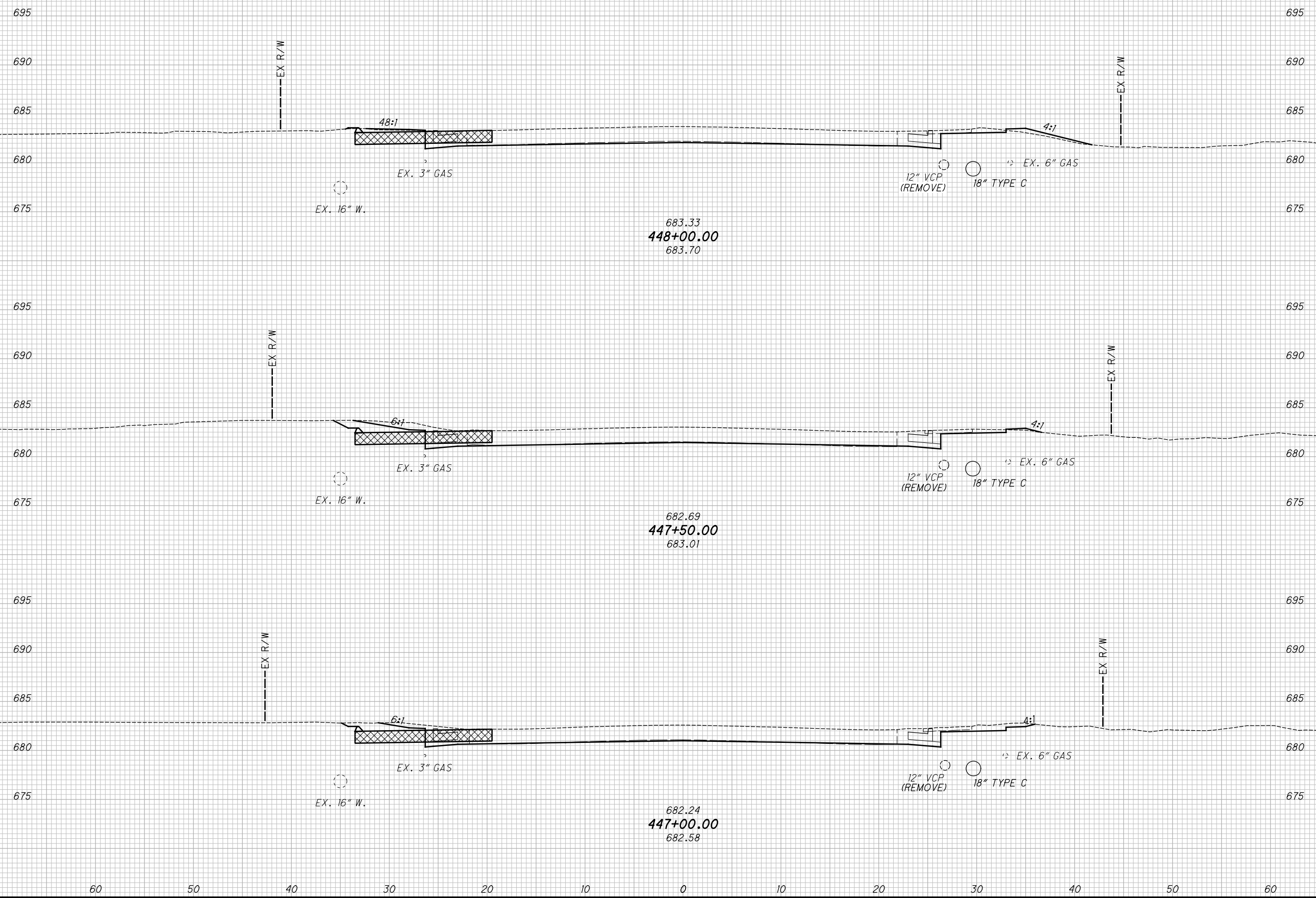
SEEDING	
END WIDTH	SO. YDS.
28	144
26	144
26	144
432	

LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED		
				CUT	FILL
19	3				
21	0				
22	0				
	119		3		

CROSS SECTIONS - U.S. 20
STA. 447+00.00 TO STA. 448+00.00
LAK-US-20-24.99
PART 2

308
697

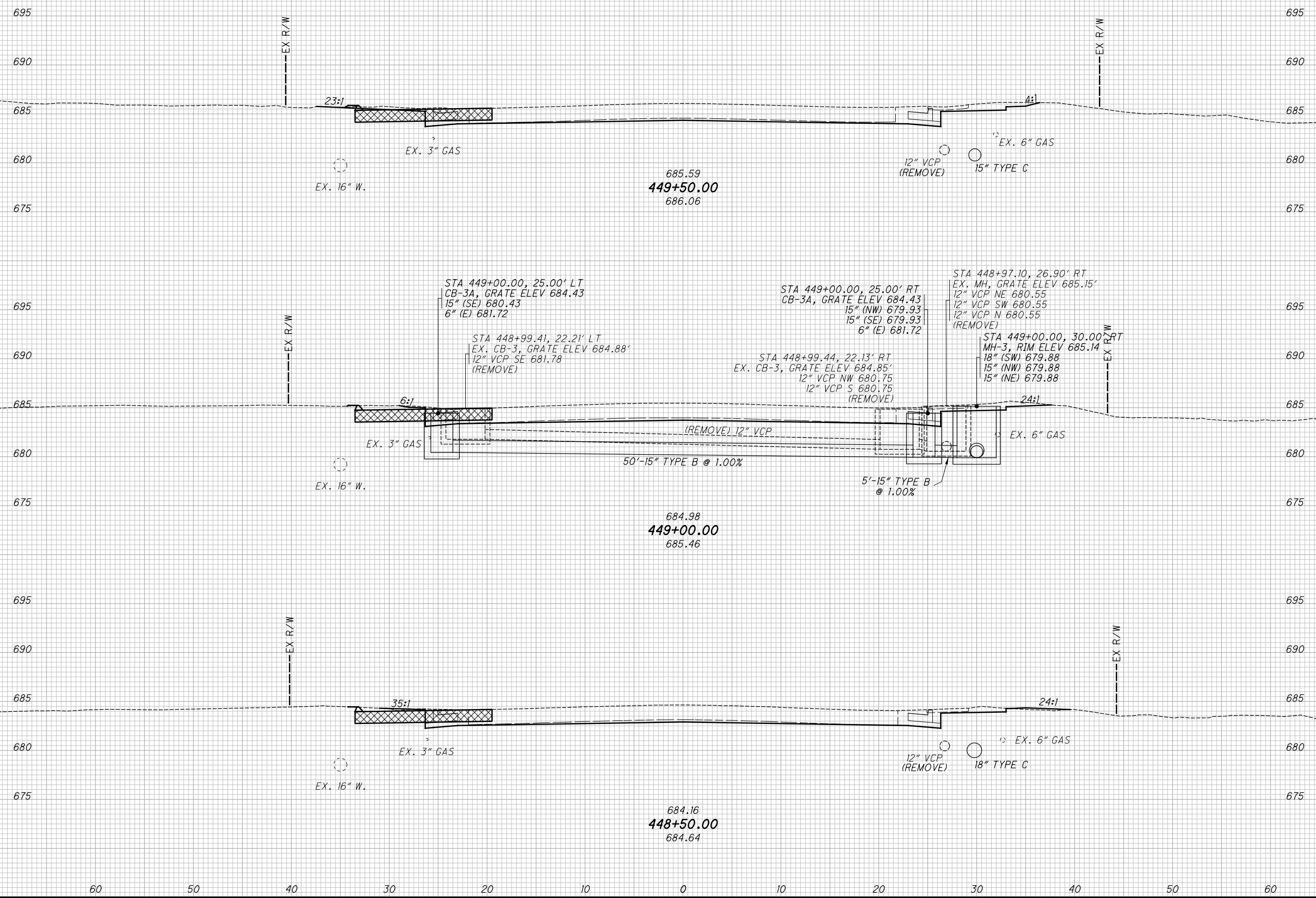


H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_32 7/15/2022 2:55:08 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
24		0					
136			53	0			
25		0					
139			48	1			
25		1					
142			40	4			
417			141	5			

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 448+50.00 TO STA. 449+50.00

LAK-US-20-24.99
PART 2

309
697

SEEDING
END SO.
WIDTH YDS.
378
131
23
125
22
22

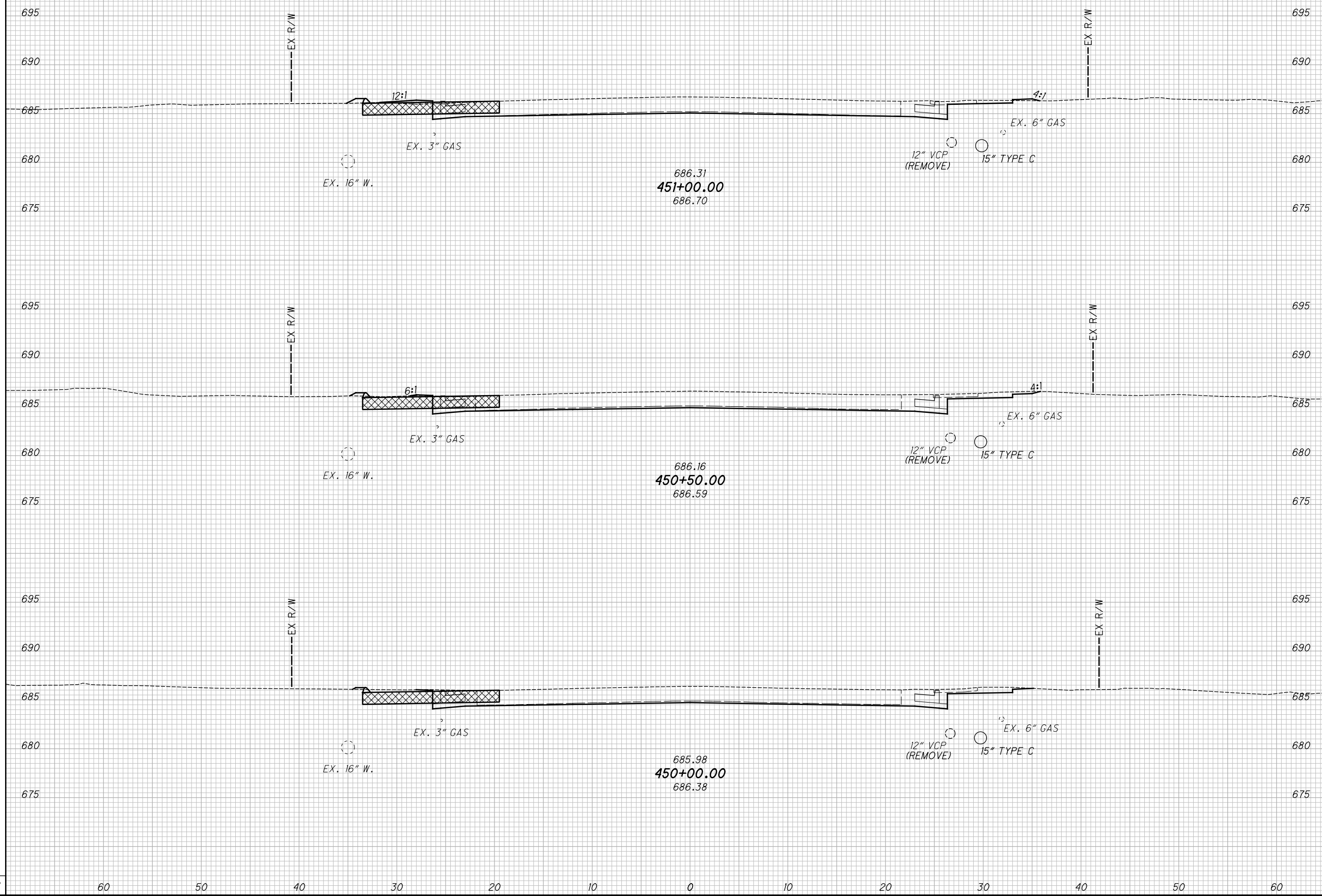
H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_33 7/15/2022 2:55:09 PM troyer

LEGEND
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
20	2			
24	0	41	2	
24	0	44	0	
24	0	49	0	
		134	2	

CROSS SECTIONS - U.S. 20
STA. 450+00.00 TO STA. 451+00.00
LAK-US-20-24.99
PART 2

310
697

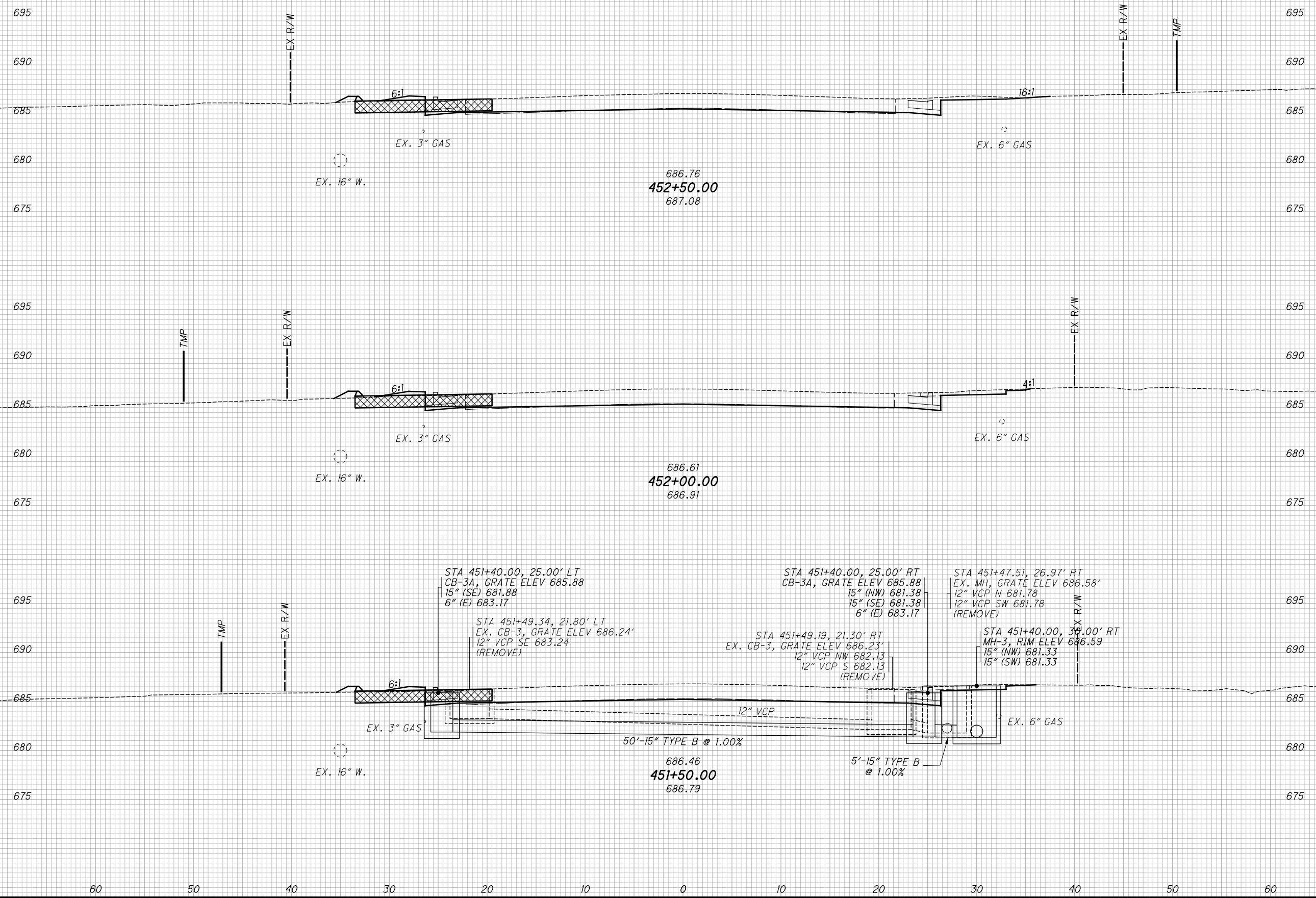


H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_34 7/15/2022 2:55:11 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
32	16	2	30	3			
178	16	1	31	3			
167	18	2	35	4			
139			96	10			
484							

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 451+50.00 TO STA. 452+50.00

LAK-US-20-24.99
PART 2

311
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_35 7/15/2022 2:55:12 PM Troyer

SEEDING	
END WIDTH	SO. YDS.
46	256
46	256
46	217
729	

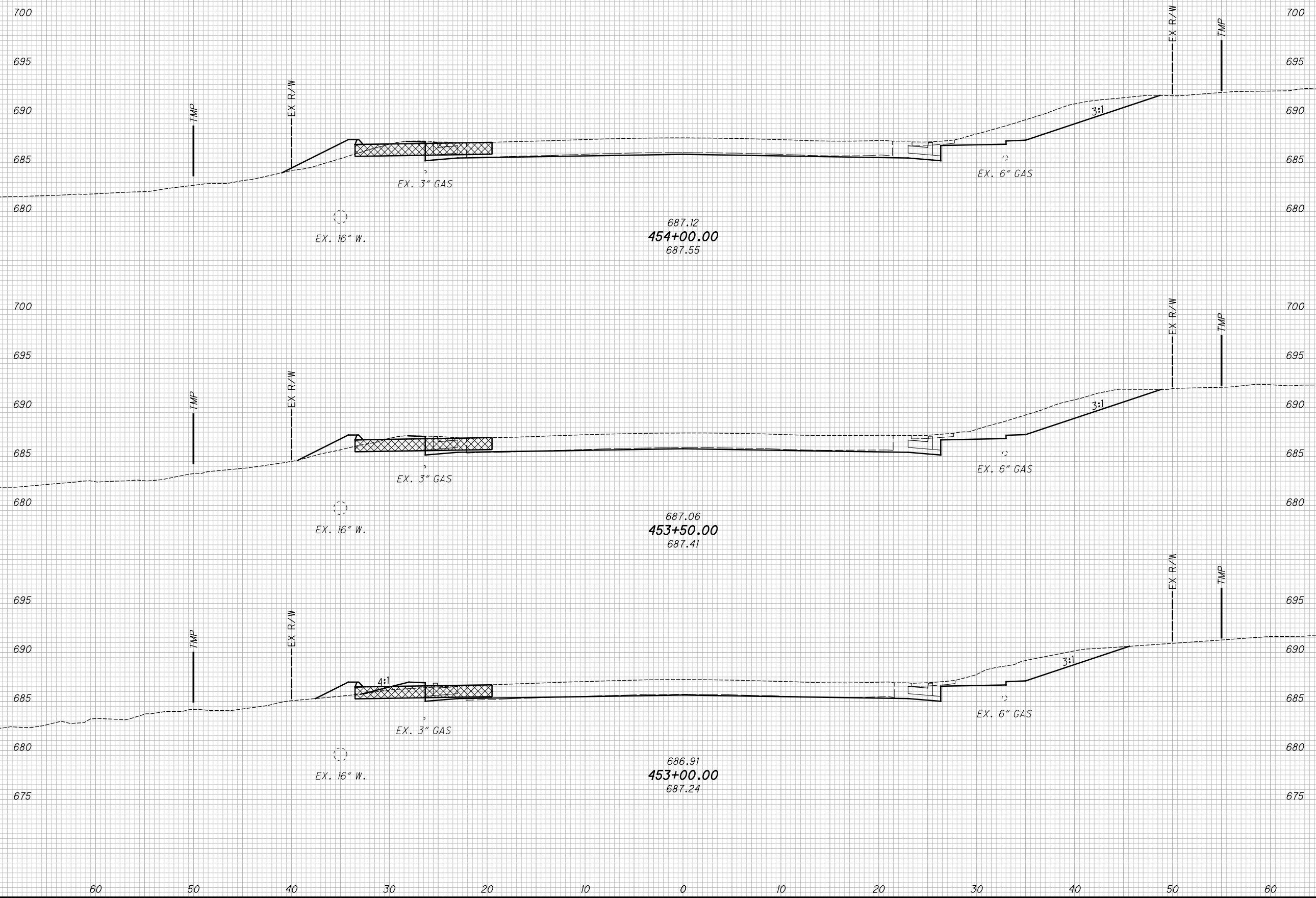
LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
52	0	94	0		
50	0	82	2		
39	2	51	4		
		227	6		

CROSS SECTIONS - U.S. 20
STA. 453+00.00 TO STA. 454+00.00

LAK-US-20-24.99
PART 2

312
697



H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_36 7/15/2022 2:55:13 PM troyer

SEEDING	
END WIDTH	SO. YDS.
768	
60	
50	
40	
30	
20	
10	
0	
10	
20	
30	
40	
50	
60	

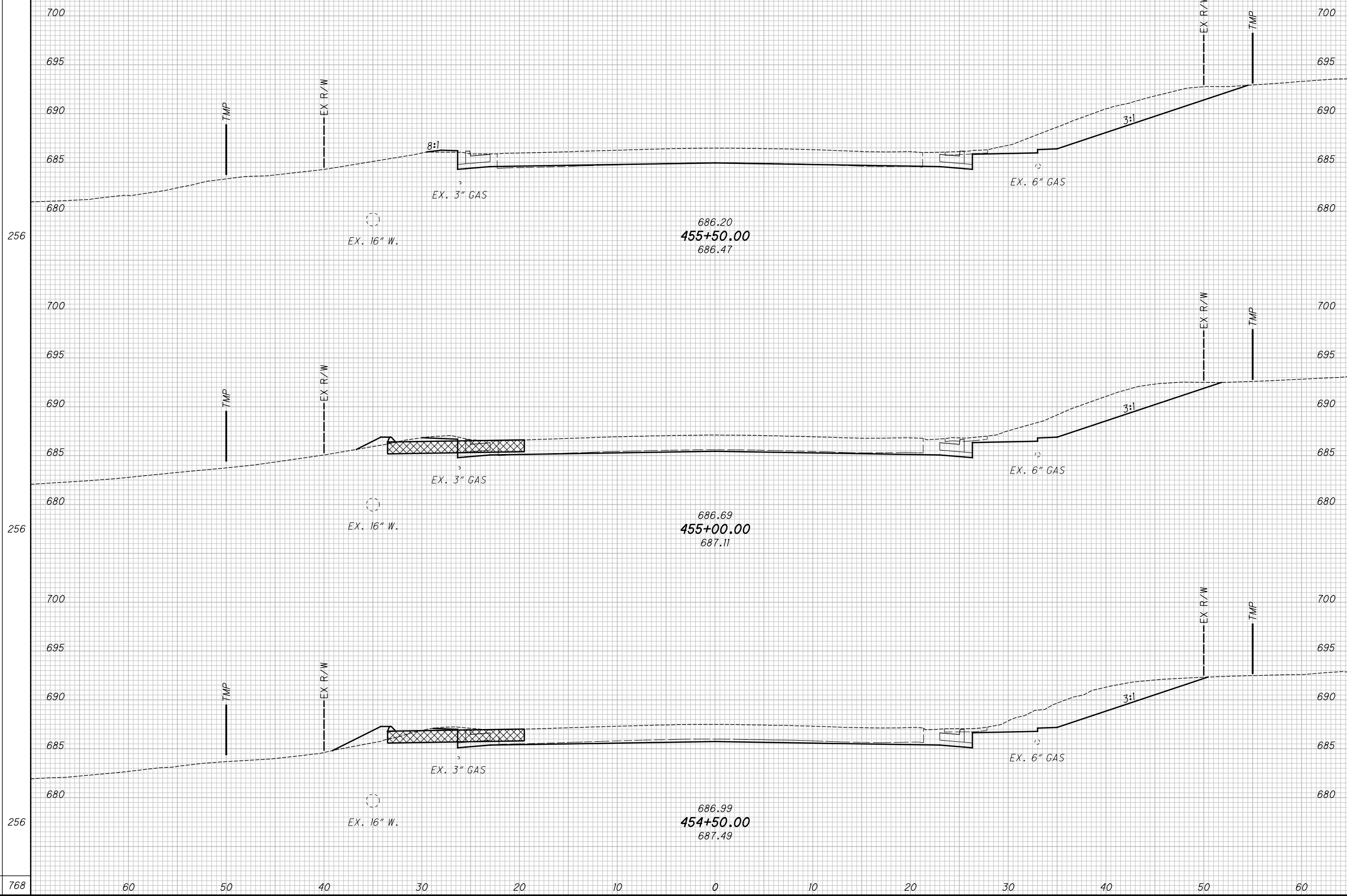
LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
58	0				
64	0	113	0		
63	0	118	0		
		106	0		
		337	0		

CROSS SECTIONS - U.S. 20
STA. 454+50.00 TO STA. 455+50.00

LAK-US-20-24.99
PART 2

313
697



H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_37 7/15/2022 2:55:14 PM Troyer

SEEDING	END	
	WIDTH	SO. YDS.
	728	60
	242	50
	41	40
	247	30
	48	20
	239	10
	38	0

LEGEND

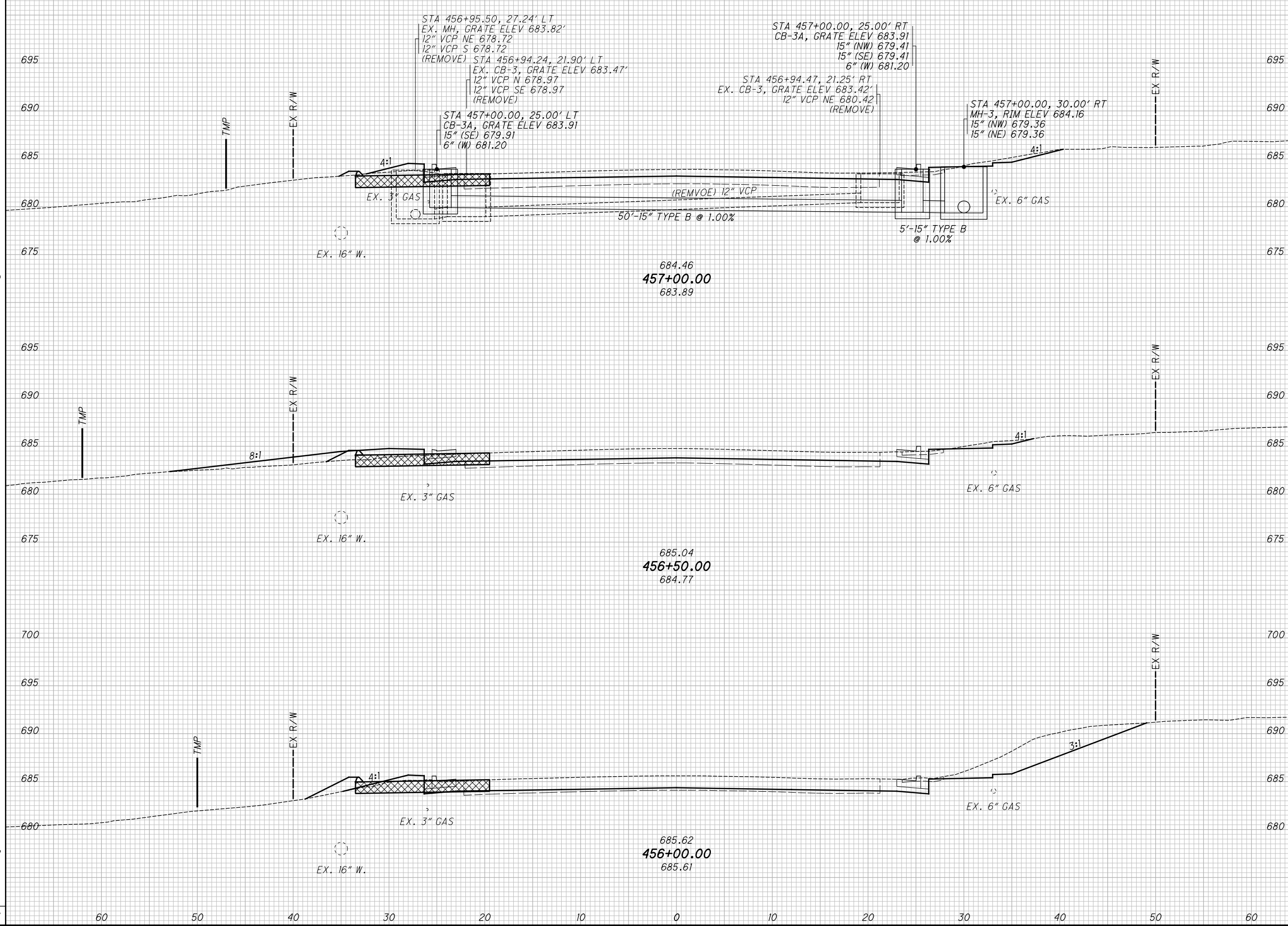
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED				
				CUT	FILL	CUT	FILL
9	4	19	19				
11	17	52	18				
45	2	95	2				
		166	39				

**CROSS SECTIONS - U.S. 20
STA. 456+00.00 TO STA. 457+00.00**

**LAK-US-20-24.99
PART 2**

314
697

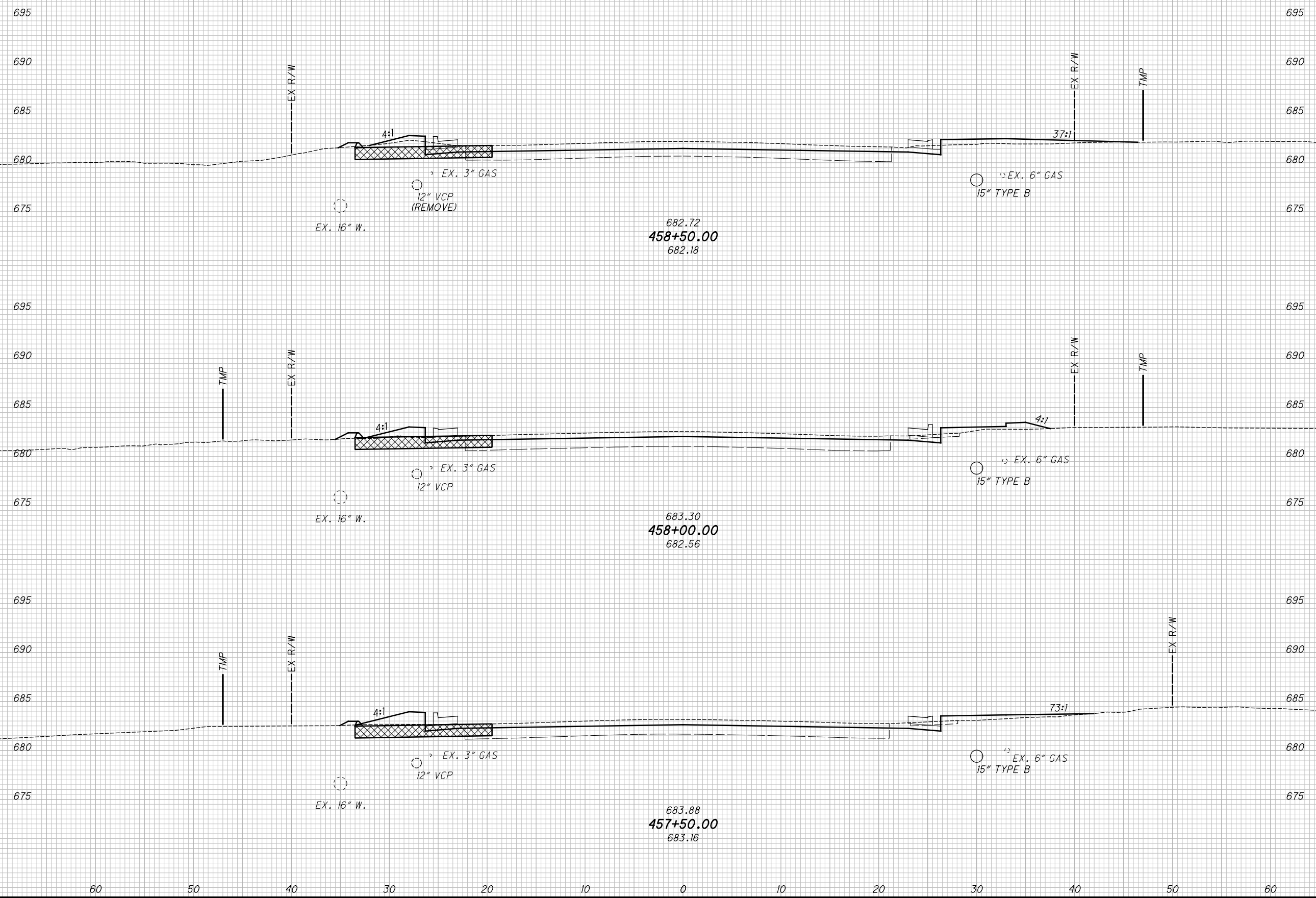


H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_38 7/15/2022 2:55:16 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
28	7	9					
175			10	17			
35	4	9					
203			8	19			
38	5	11					
211			13	14			
589			31	50			

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



**CROSS SECTIONS - U.S. 20
STA. 457+50.00 TO STA. 458+50.00**

**LAK-US-20-24.99
PART 2**

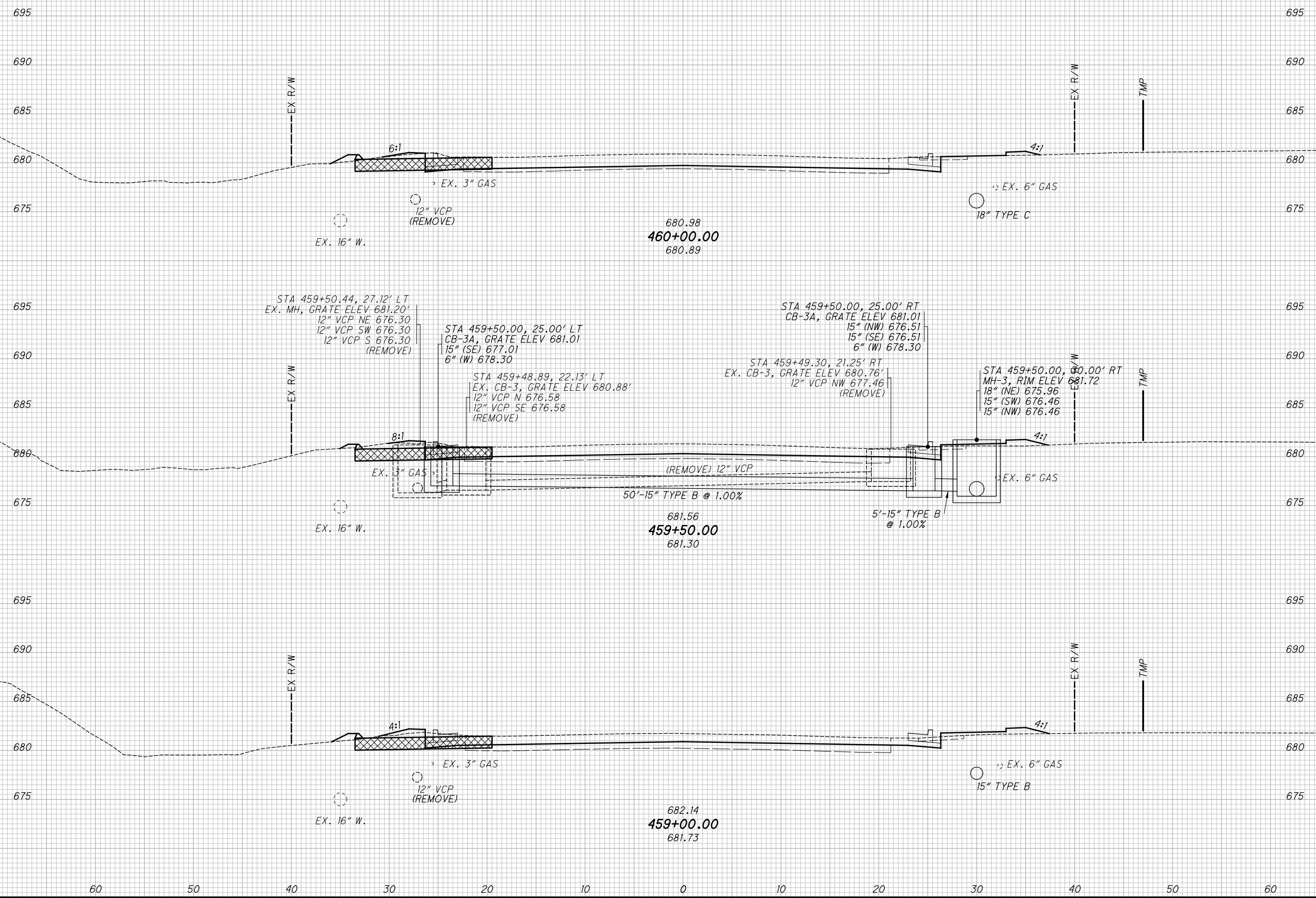
315
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_39 7/15/2022 2:55:17 PM Troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
END WIDTH	12	1	21	5			
SO. YDS.	28	11	19	8			
	156	9	15	13			
	468	55	26				

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 459+00.00 TO STA. 460+00.00

LAK-US-20-24.99
PART 2

316
697

H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_40 7/15/2022 2:55:18 PM troyer

SEEDING	
END WIDTH	SO. YDS.
705	
200	
44	
261	
50	
244	
38	

LEGEND

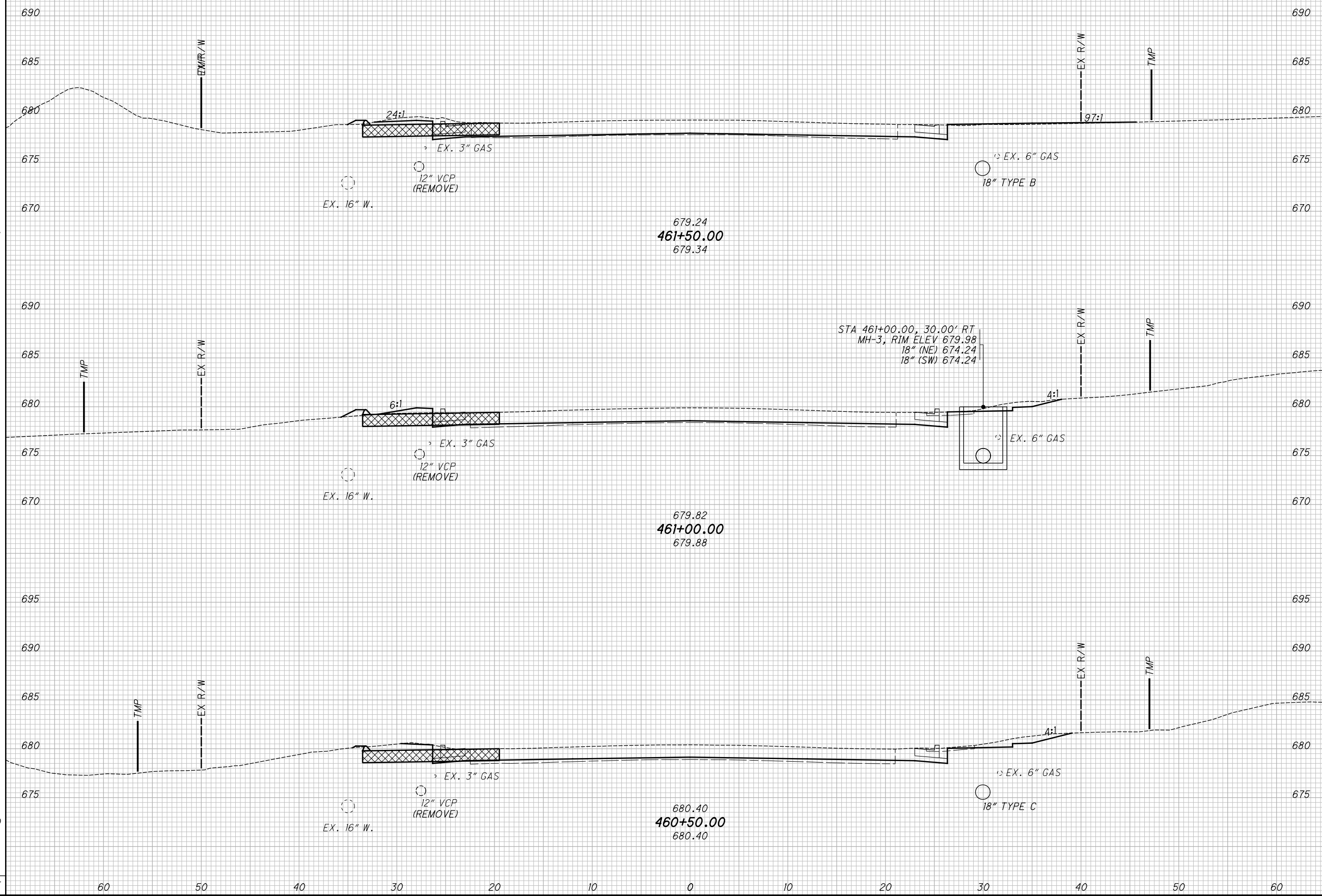
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
16	1				
		29	3		
15	2				
		30	2		
17	0				
		27	1		
		86	6		

**CROSS SECTIONS - U.S. 20
STA. 460+50.00 TO STA. 461+50.00**

**LAK-US-20-24.99
PART 2**

317
697

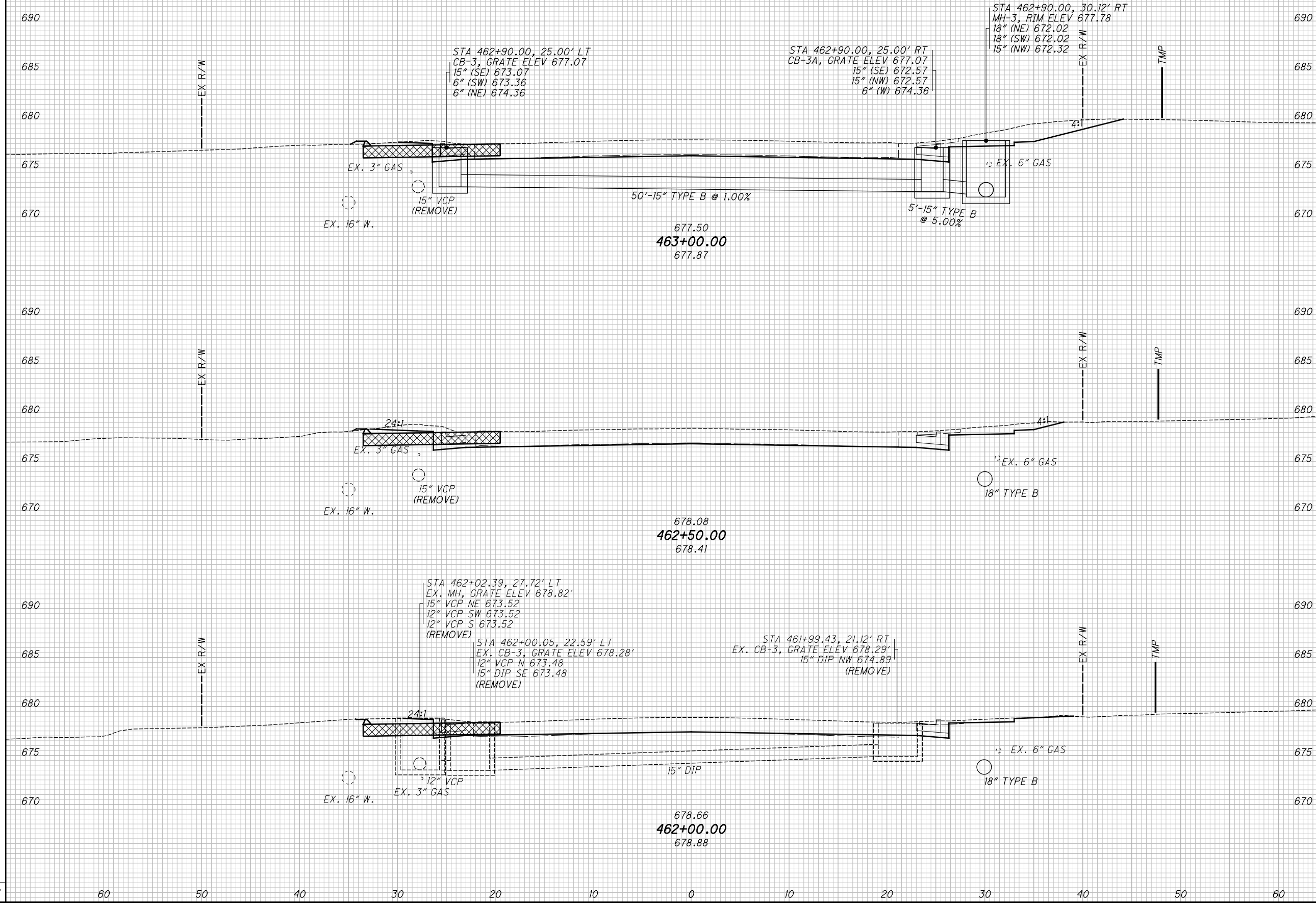


H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_41 7/15/2022 2:55:19 PM Troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
END WIDTH							
SO. YDS.							
39			42	0			
217			66	0			
39			29	0			
217			42	0			
39			16	0			
214			30	1			
648			87	1			

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
 STA. 462+00.00 TO STA. 463+00.00

LAK-US-20-24.99
 PART 2

318
 697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_42 7/15/2022 2:55:20 PM troyer

SEEDING	
END WIDTH	SO. YDS.
38	
222	
42	
161	
16	
153	
536	

LEGEND

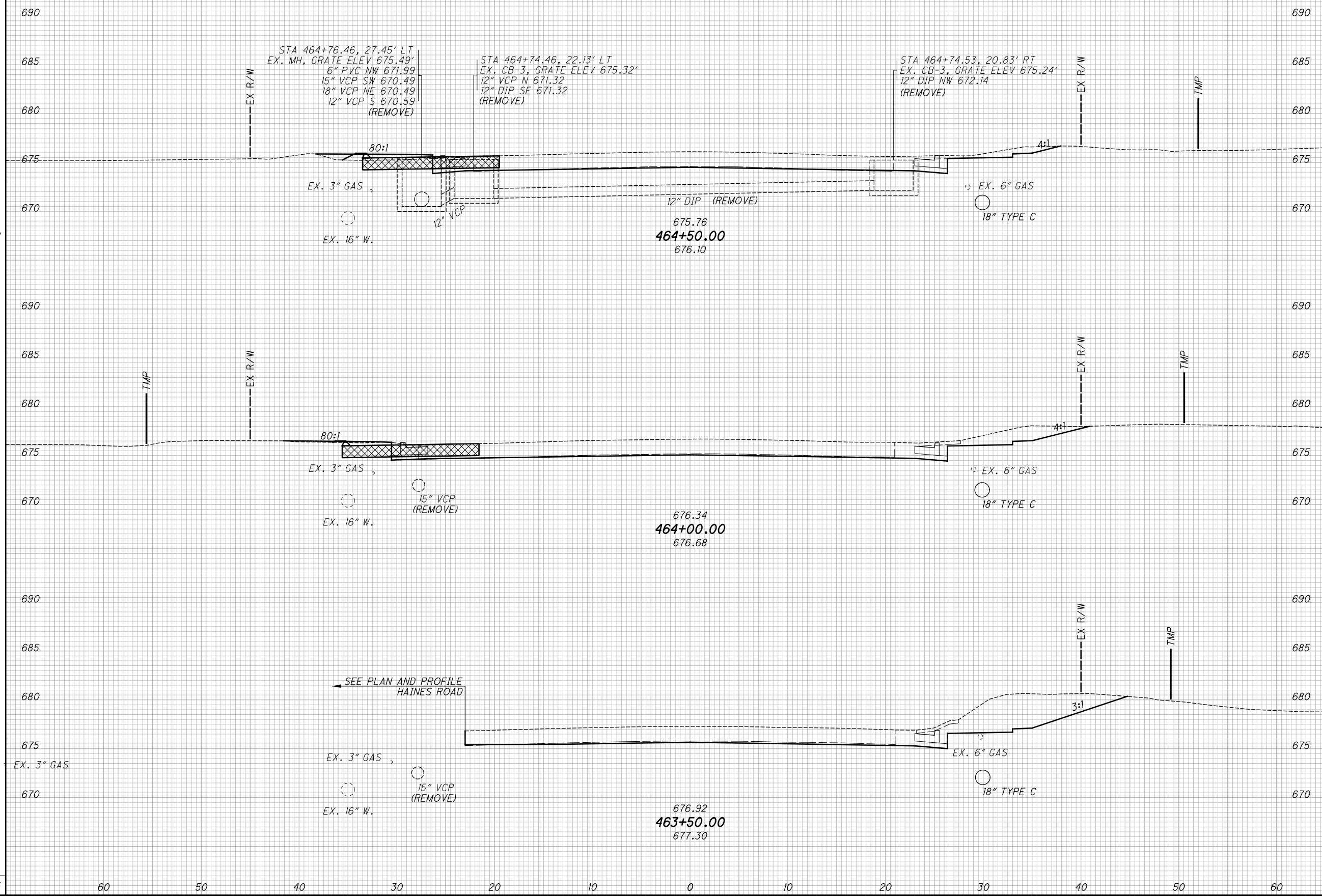
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
21	6			
47	6			
30	1			
81	1			
57	0			
92	0			
	220			7

**CROSS SECTIONS - U.S. 20
STA. 463+50.00 TO STA. 464+50.00**

**LAK-US-20-24.99
PART 2**

319
697



STA 464+76.46, 27.45' LT
EX. MH, GRATE ELEV 675.49'
6" PVC NW 671.99
15" VCP SW 670.49
18" VCP NE 670.49
12" VCP S 670.59
(REMOVE)

STA 464+74.46, 22.13' LT
EX. CB-3, GRATE ELEV 675.32'
12" VCP N 671.32
12" DIP SE 671.32
(REMOVE)

STA 464+74.53, 20.83' RT
EX. CB-3, GRATE ELEV 675.24'
12" DIP NW 672.14
(REMOVE)

EX. 3" GAS
EX. 16" W.

12" VCP

675.76
464+50.00
676.10

676.34
464+00.00
676.68

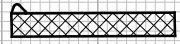

676.92
463+50.00
677.30

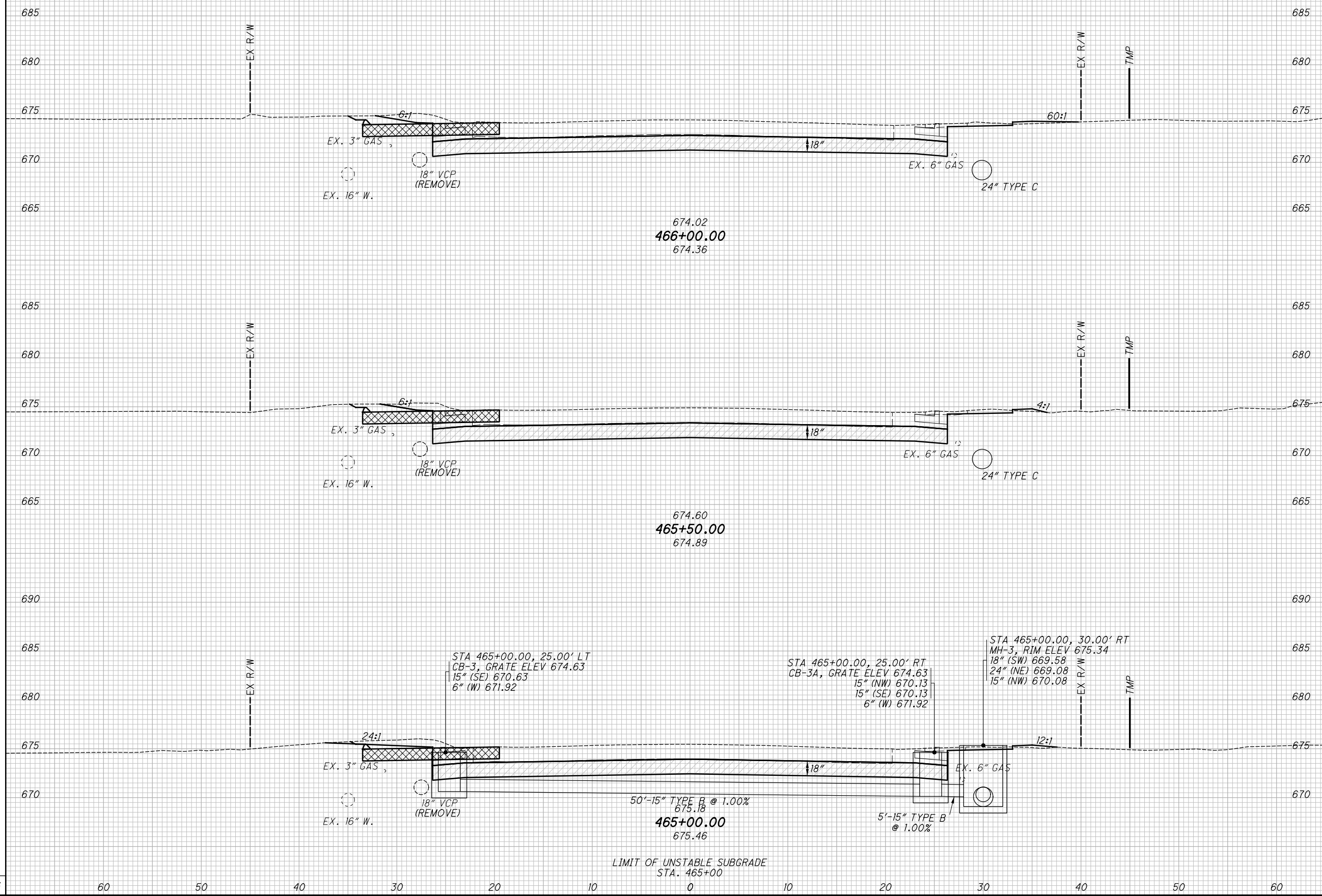
SEE PLAN AND PROFILE
HAINES ROAD

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_43 7/15/2022 2:55:22 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	END WIDTH	SO. YDS.	CUT	FILL			
31	60	675	24	1			
172	60	665	44	2			
31	60	675	23	1			
172	60	665	43	2			
31	60	675	23	1			
192	60	670	41	6			
536	60	670	128	10			

LEGEND

-  ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC
-  UNSTABLE SUBGRADE



**CROSS SECTIONS - U.S. 20
STA. 465+00.00 TO STA. 466+00.00**

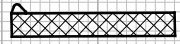

**LAK-US-20-24.99
PART 2**

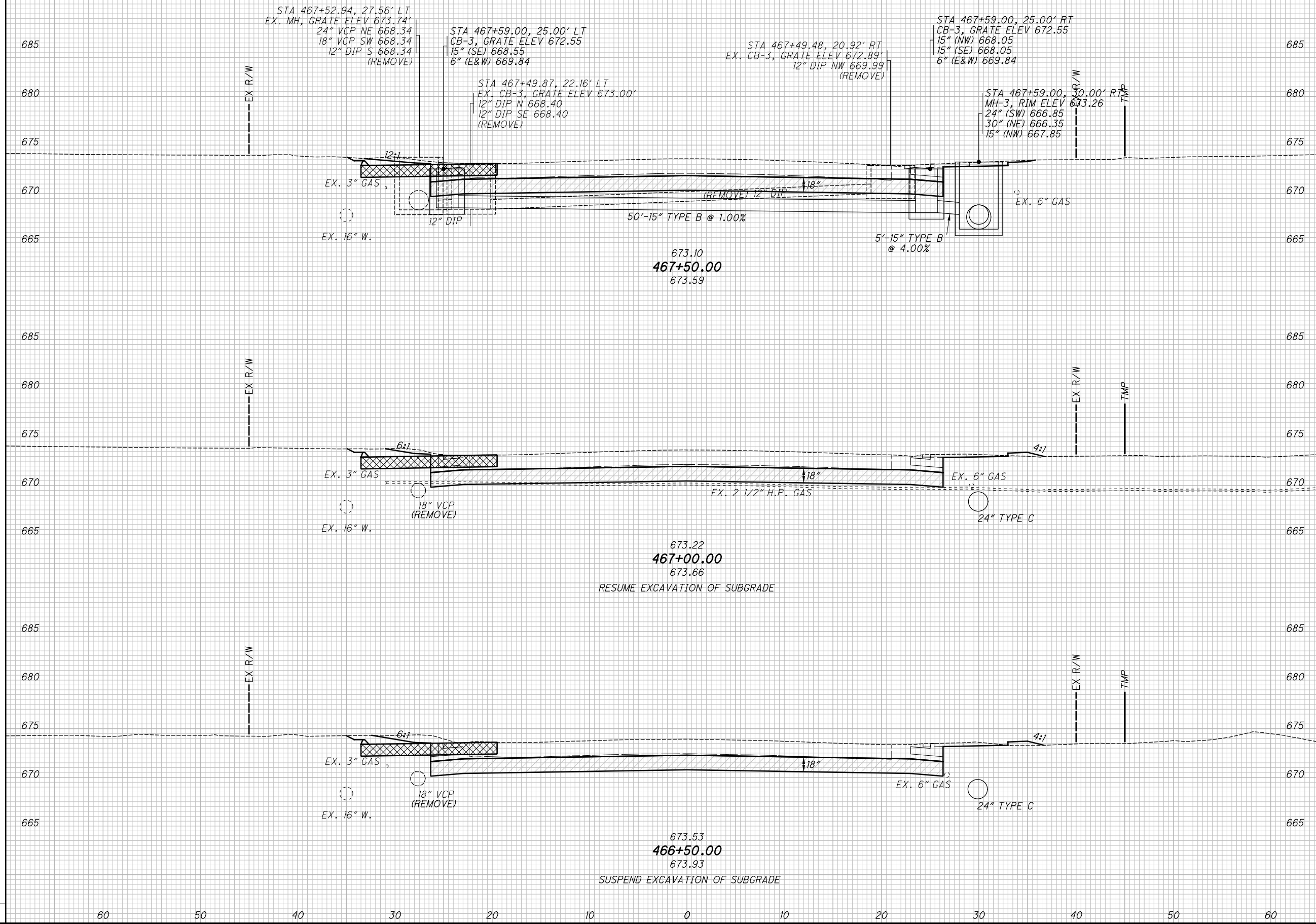
320
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_44 7/15/2022 2:55:23 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	END WIDTH	SO. YDS.	CUT	FILL			
	31	675	25	0			
	172	665			43	1	
	31	675	21	1			
	172	665			43	2	
	31	675	25	1			
	172	665			45	2	
	516	665			131	5	

LEGEND

-  ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC
-  UNSTABLE SUBGRADE

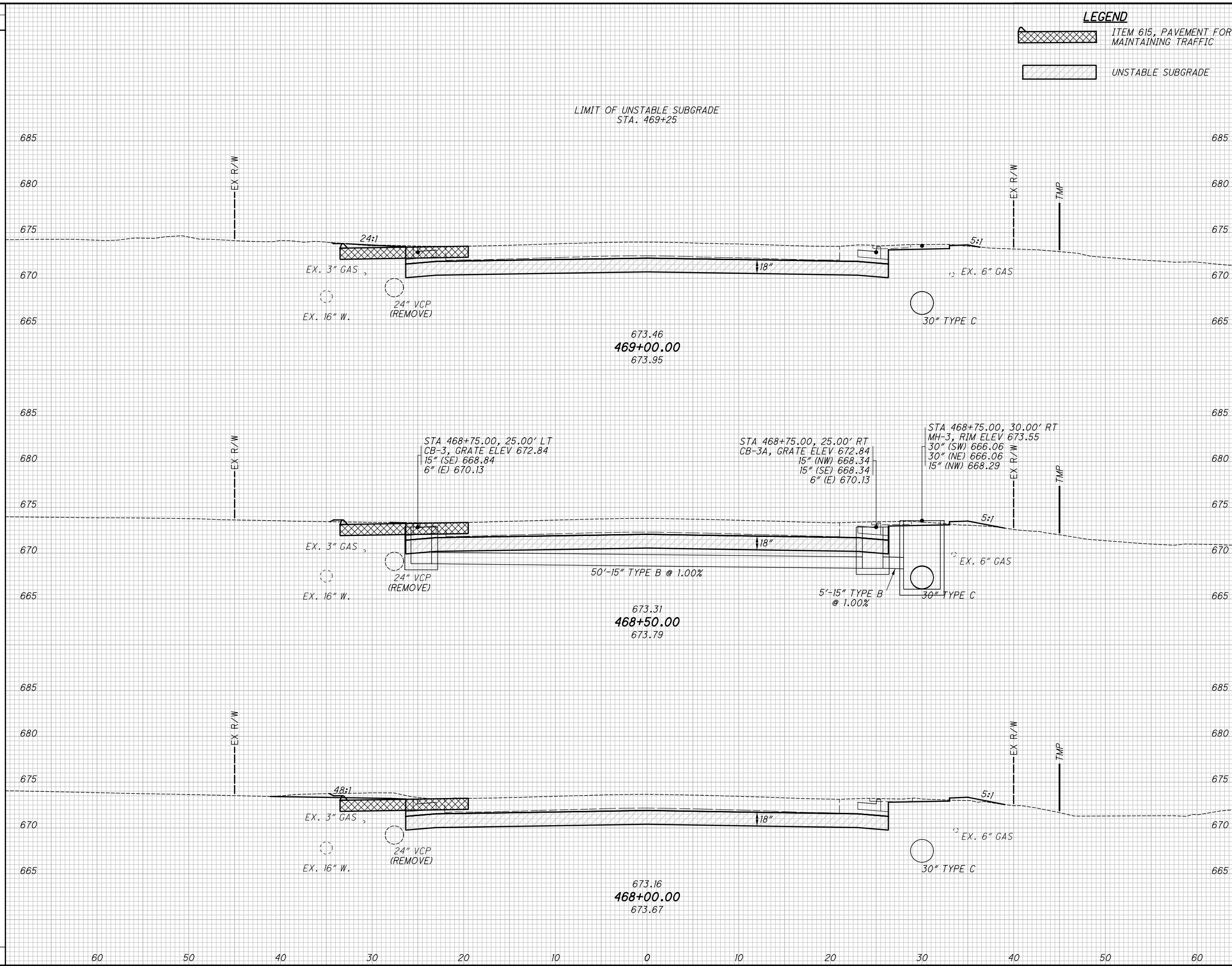


CROSS SECTIONS - U.S. 20
STA. 466+50.00 TO STA. 467+50.00

LAK-US-20-24.99
PART 2

321
 697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_45 7/15/2022 2:55:24 PM troyer



LEGEND

	ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC
	UNSTABLE SUBGRADE

END STA.	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
469+00.00	28	0	50	1			
468+50.00	26	1	54	2			
468+00.00	32	1	53	1			
TOTAL			157	4			

CROSS SECTIONS - U.S. 20
STA. 468+00.00 TO STA. 469+00.00

LAK-US-20-24.99
PART 2

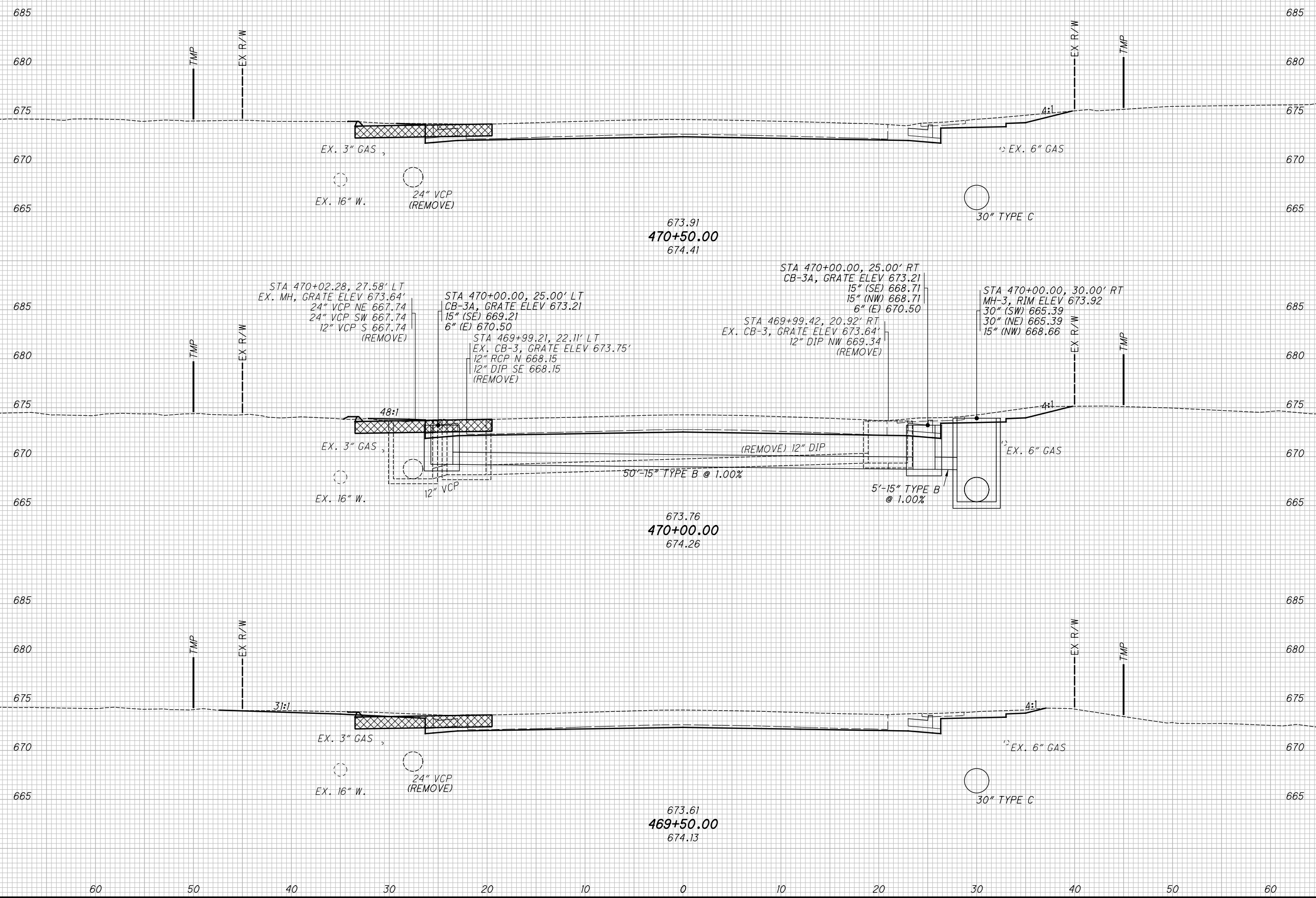
322
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_46 7/15/2022 2:55:25 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
END WIDTH	36	36	34	33	34	34	34
SO. YDS.	200	200	62	62	62	62	62
	186	186	57	0	57	0	57
	586	586	181	2	181	2	181

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 469+50.00 TO STA. 470+50.00

LAK-US-20-24.99
PART 2

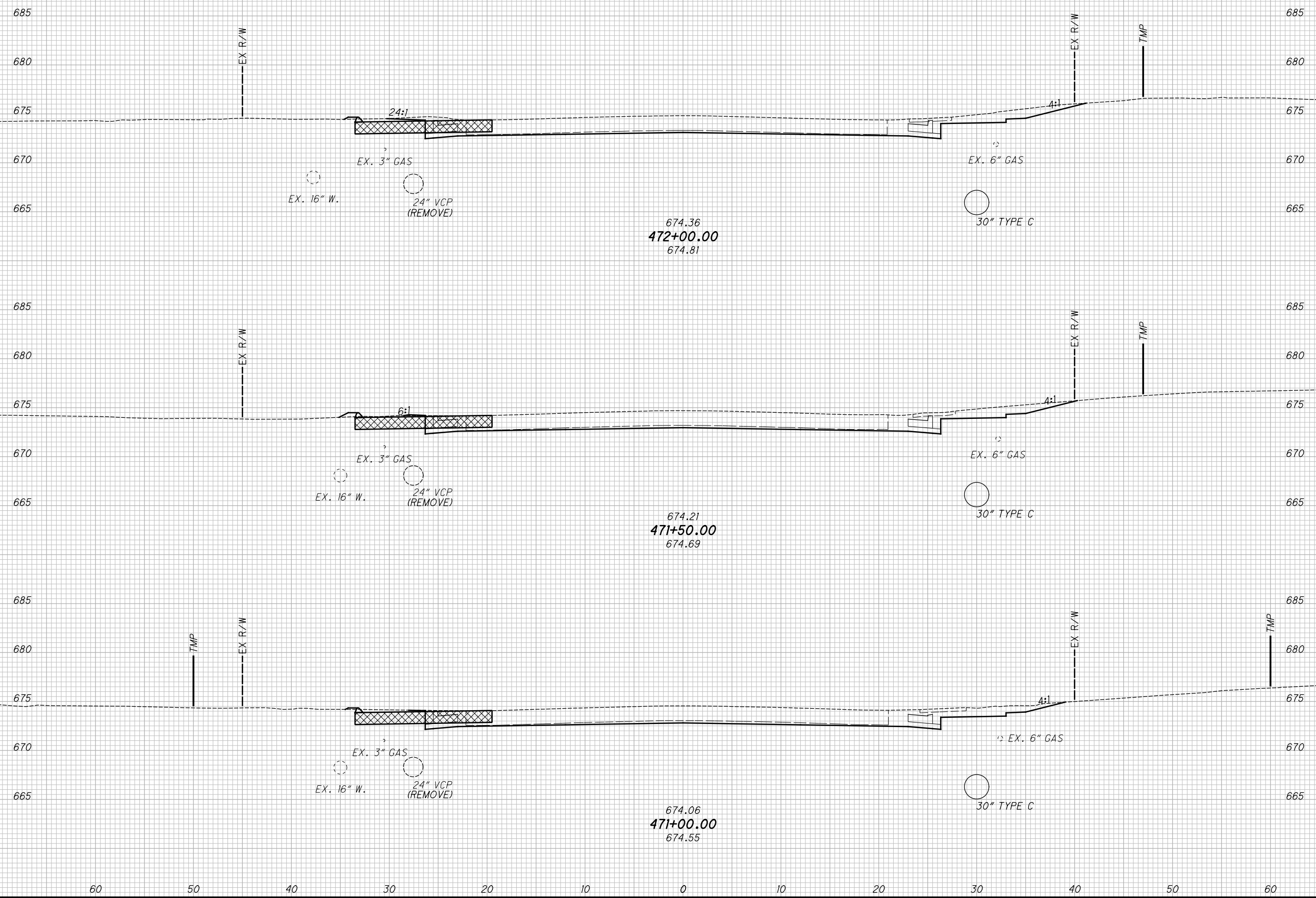
323
697

H:\2018\80743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_47 7/15/2022 2:55:27 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
END WIDTH	33	33	33	33			
SO. YDS.	183	233	51	242			
	658	658	658	658	185	0	0

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



**CROSS SECTIONS - U.S. 20
STA. 471+00.00 TO STA. 472+00.00**

**LAK-US-20-24.99
PART 2**

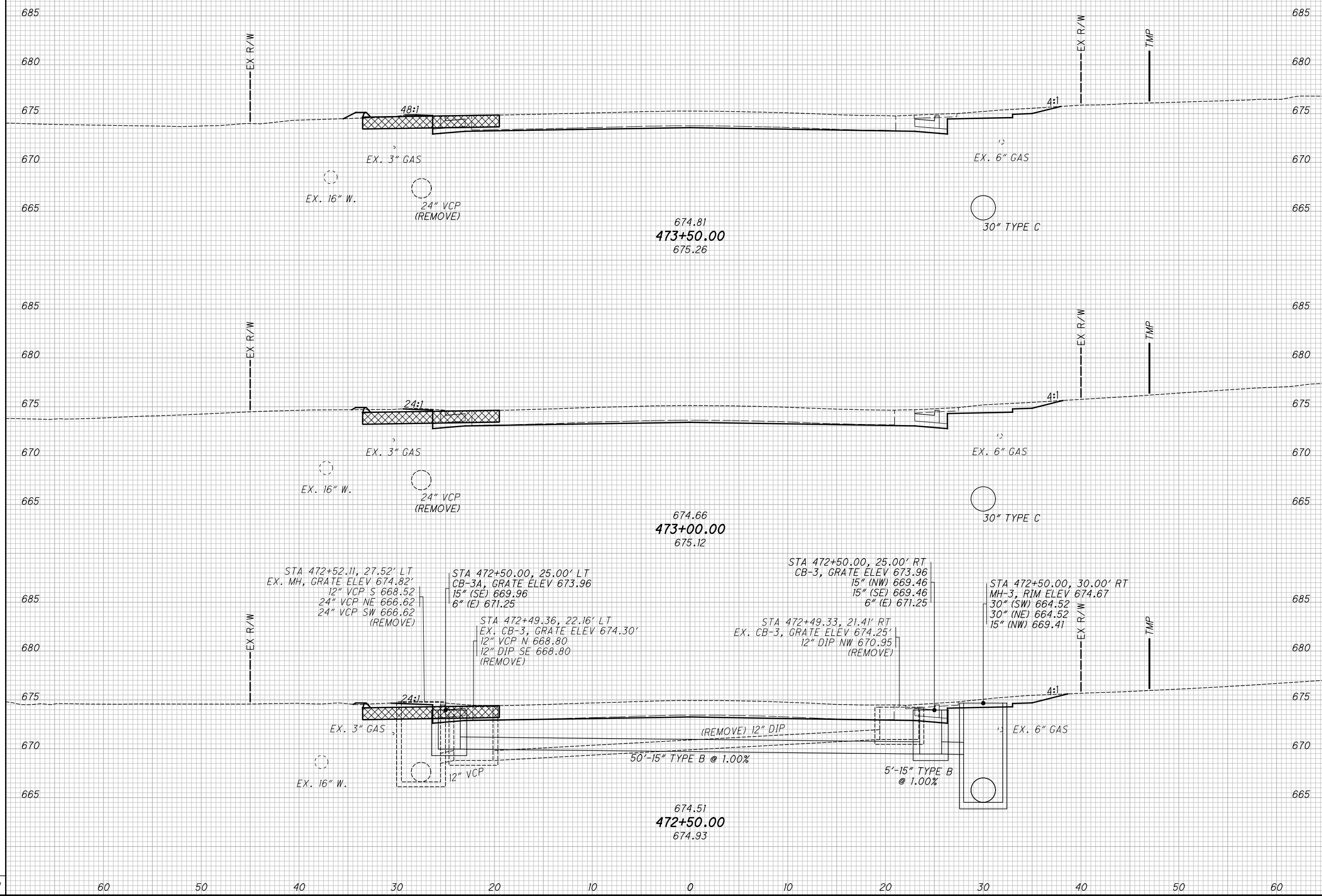
324
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_48 7/15/2022 2:55:28 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
	END WIDTH	SO. YDS.	CUT	FILL		
	33		28	0		
	183			53	0	
	33		29	0		
	183			54	0	
	33		29	0		
	183			58	0	
	549			165	0	

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 472+50.00 TO STA. 473+50.00

LAK-US-20-24.99
PART 2

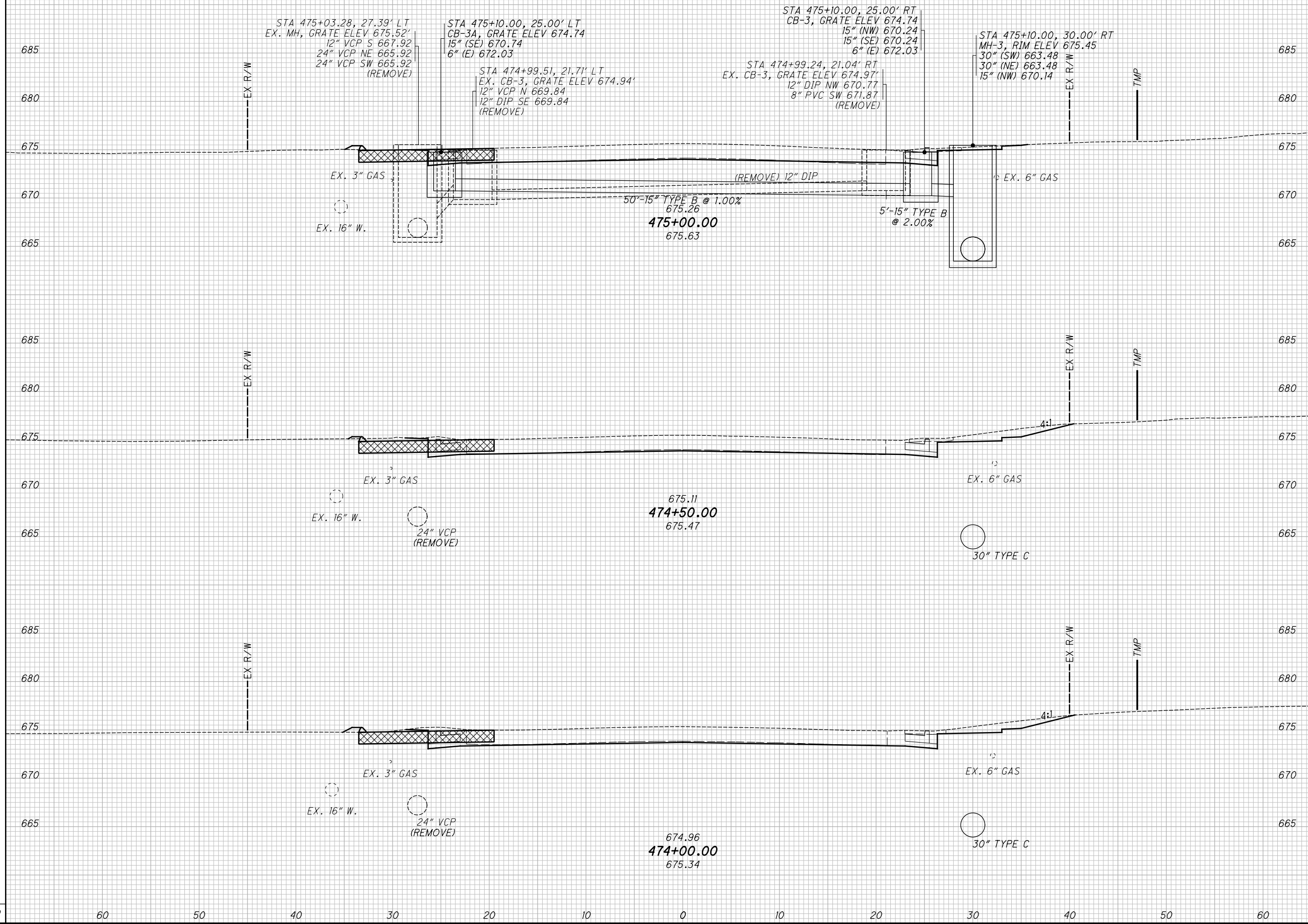
325
697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_49 7/15/2022 2:55:29 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	END WIDTH	SO. YDS.	CUT	FILL			
	33	675	17	0			
	183	665		40			
	33	675	26	0			
	183	665		50			
	33	675	28	0			
	183	665		52			
	549			142			

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



**CROSS SECTIONS - U.S. 20
STA. 474+00.00 TO STA. 475+00.00**

**LAK-US-20-24.99
PART 2**

326
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_51 7/15/2022 2:55:32 PM troyer

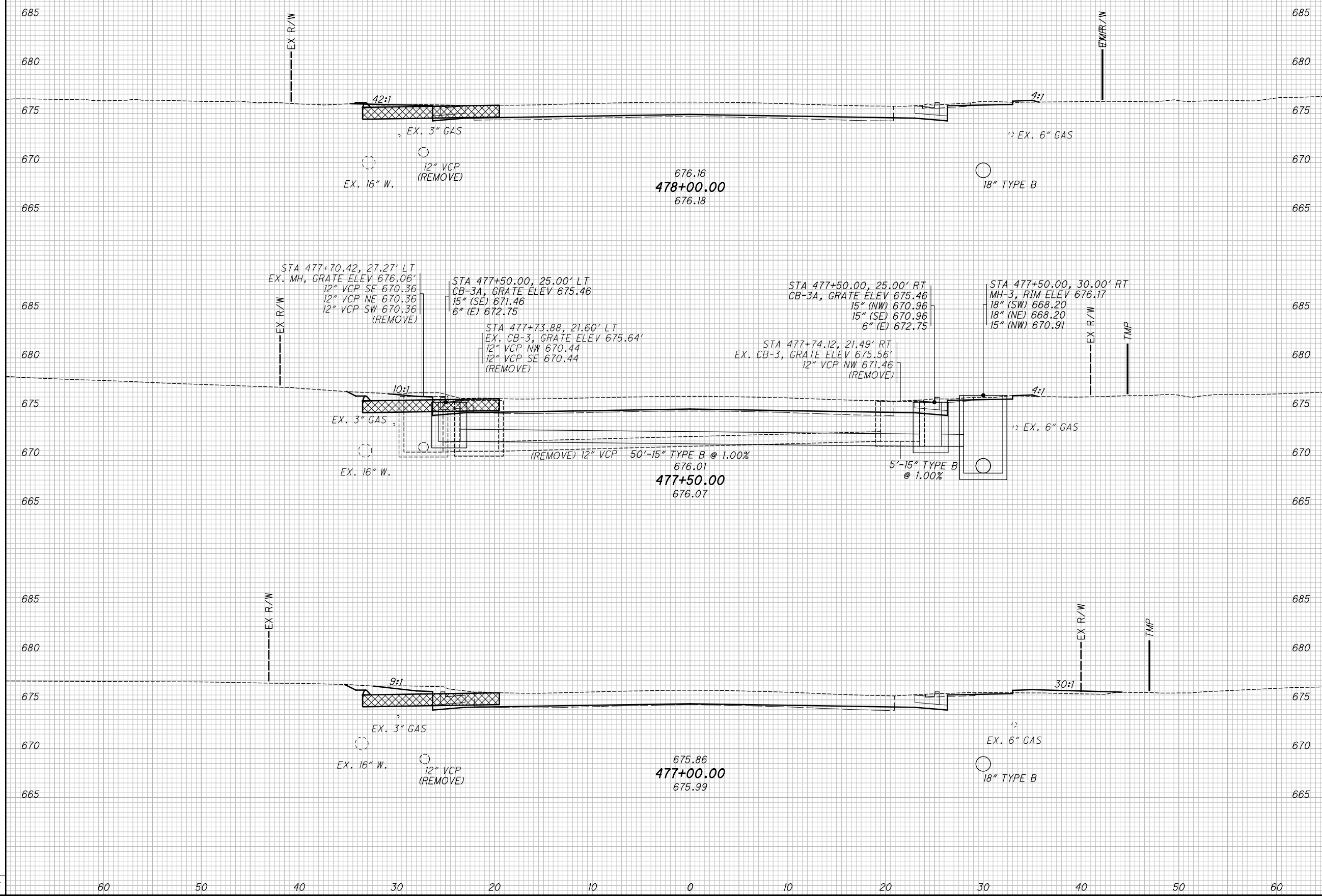
SEEDING	END	
	WIDTH	SO. YDS.
	24	144
	28	164
	31	178
	486	

LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
14	1			
16	0			
18	3			
90	8			

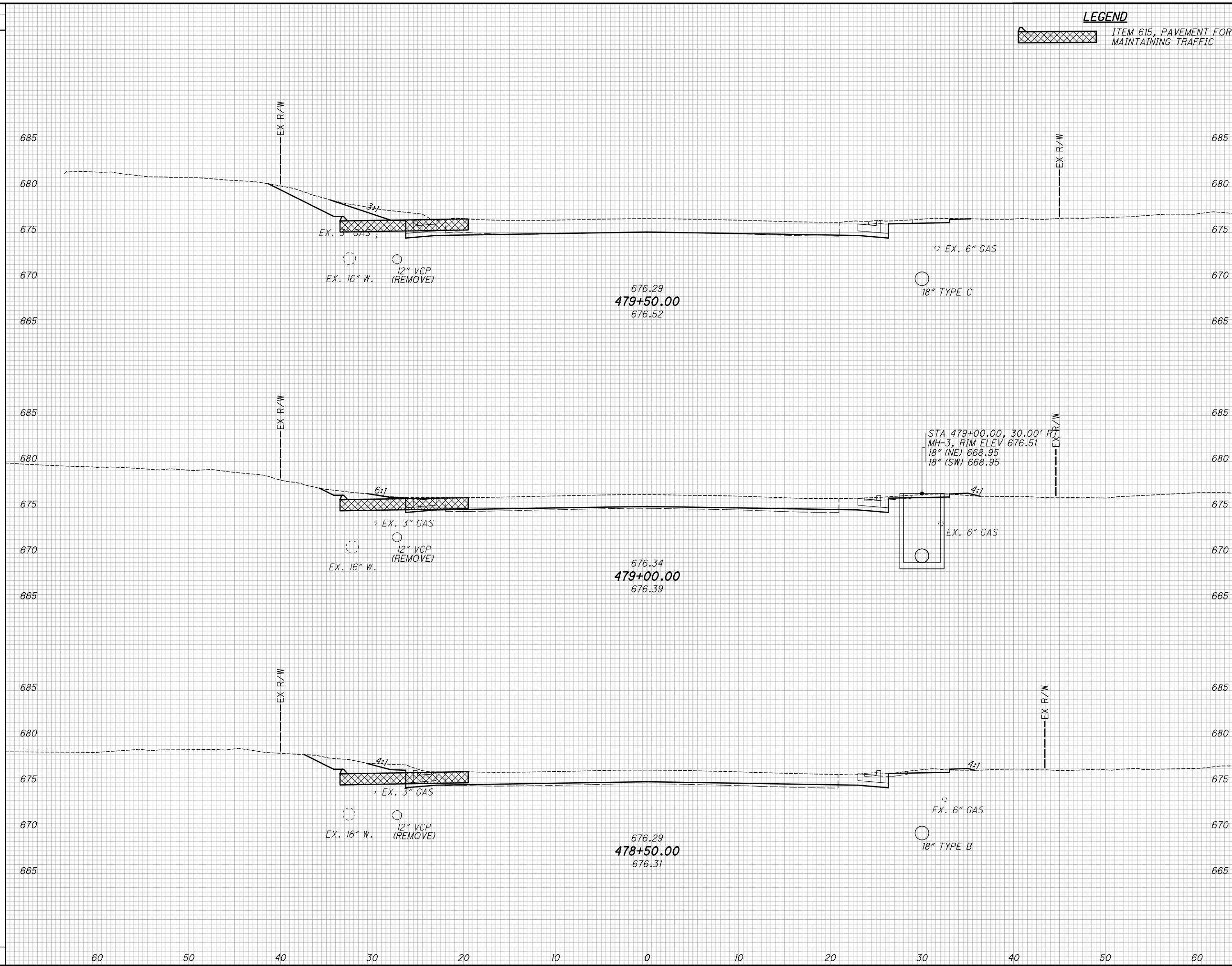
LAK-US-20-24.99
PART 2
CROSS SECTIONS - U.S. 20
STA. 477+00.00 TO STA. 478+00.00

328
697



H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_52 7/15/2022 2:55:33 PM troyer

SEEDING	
END WIDTH	SO. YDS.
413	133
	24
	136
	25
	144
	27



LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED		CHECKED	JMP
		CUT	FILL		
25	0				
14	0				
17	0				
94	1				

CROSS SECTIONS - U.S. 20
STA. 478+50.00 TO STA. 479+50.00

LAK-US-20-24.99
PART 2

329
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_53 7/15/2022 2:55:34 PM troyer

SEEDING	
END WIDTH	SO. YDS.
770	50
278	50
278	50
214	50
770	60
770	50
770	40
770	30
770	20
770	10
770	0
770	10
770	20
770	30
770	40
770	50
770	60

LEGEND

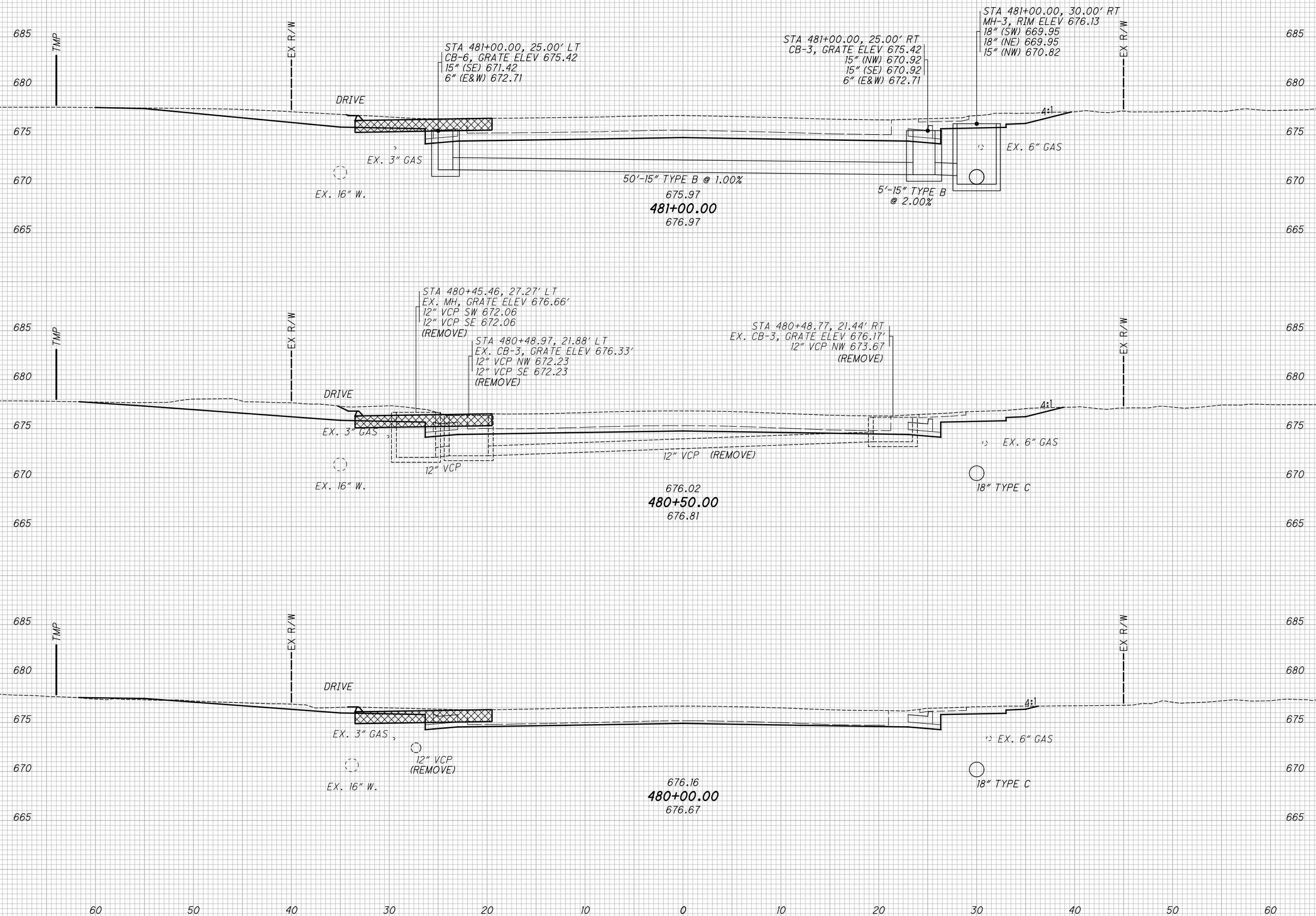
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
90	0	164	0		
87	0	120	1		
43	1	63	1		
		347	2		

**CROSS SECTIONS - U.S. 20
STA. 480+00.00 TO STA. 481+00.00**

**LAK-US-20-24.99
PART 2**

330
697



H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_54 7/15/2022 2:55:35 PM troyer

SEEDING	
END WIDTH	SO. YDS.
725	
60	33
50	186
40	34
30	247
20	55
10	292
0	
10	
20	
30	
40	
50	
60	

LEGEND

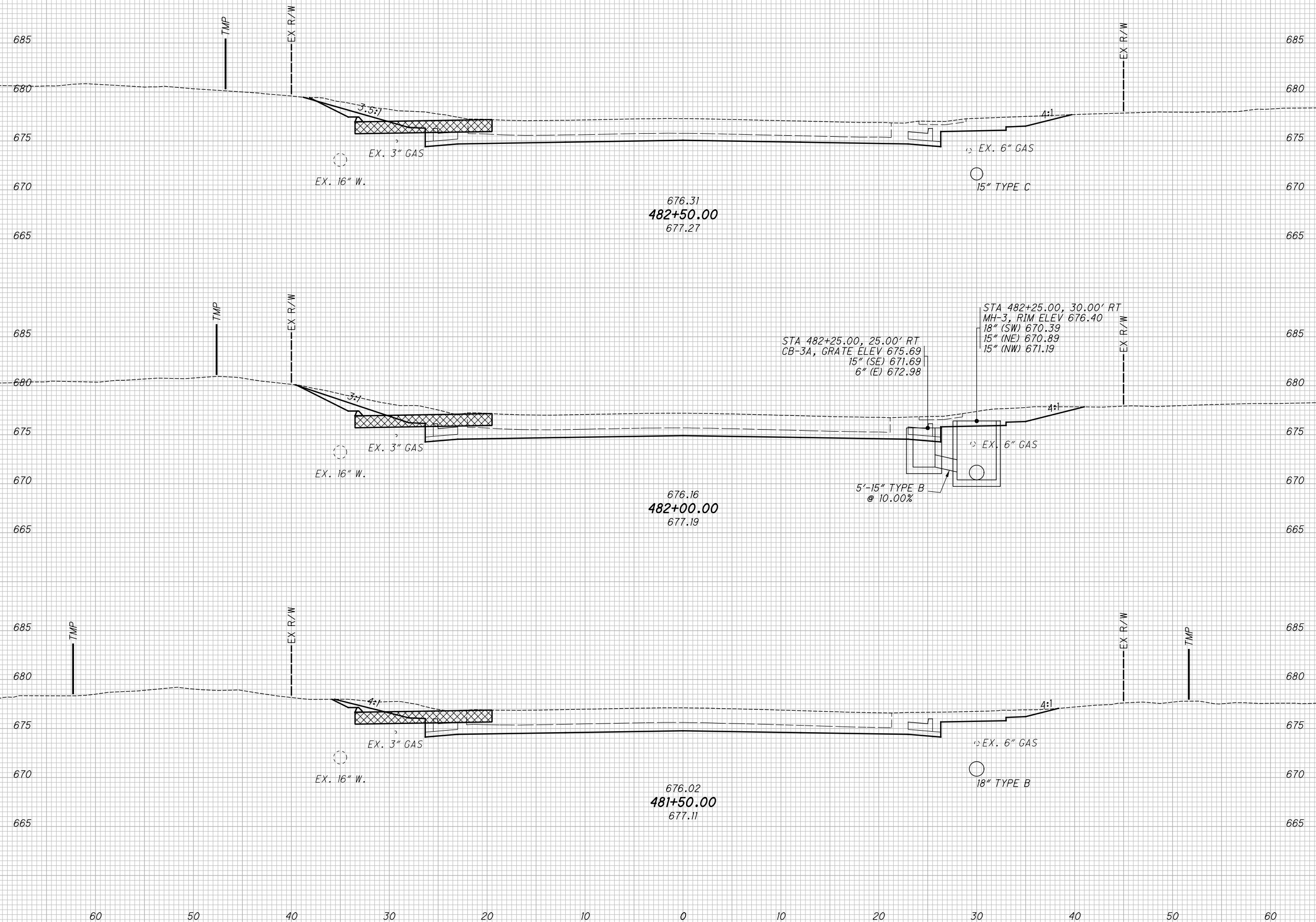
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
79	0	154	0		
87	0	151	0		
76	0	154	0		
		459	0		

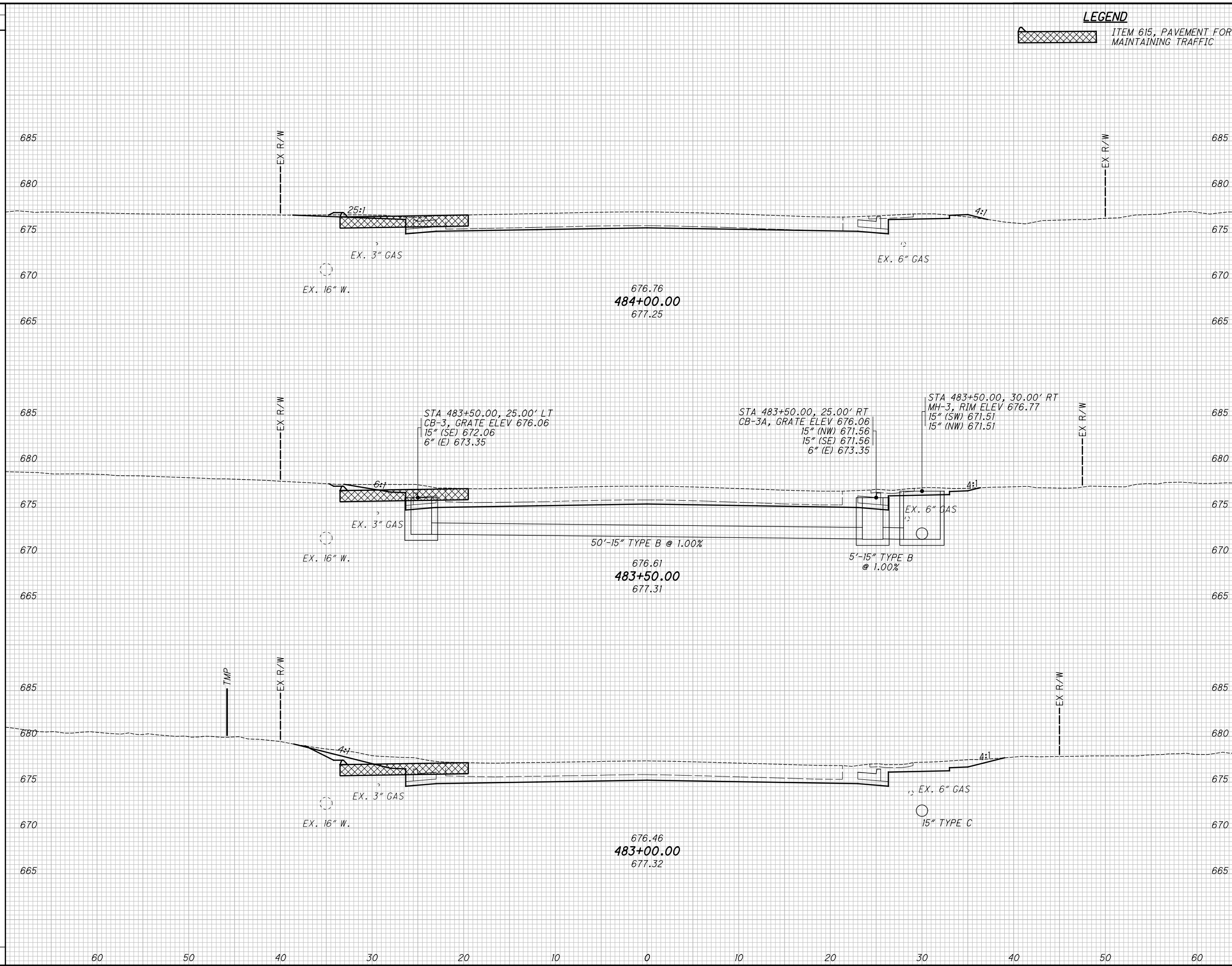
**CROSS SECTIONS - U.S. 20
STA. 481+50.00 TO STA. 482+50.00**

**LAK-US-20-24.99
PART 2**

331
697



H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_55 7/15/2022 2:55:37 PM troyer



LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
29	1			
46	0			
64	0			
303	1			

LAK-US-20-24.99
PART 2
CROSS SECTIONS - U.S. 20
STA. 483+00.00 TO STA. 484+00.00

332
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_56 7/15/2022 2:55:38 PM troyer

SEEDING	
END WIDTH	SO. YDS.
43	219
36	186
31	172
577	

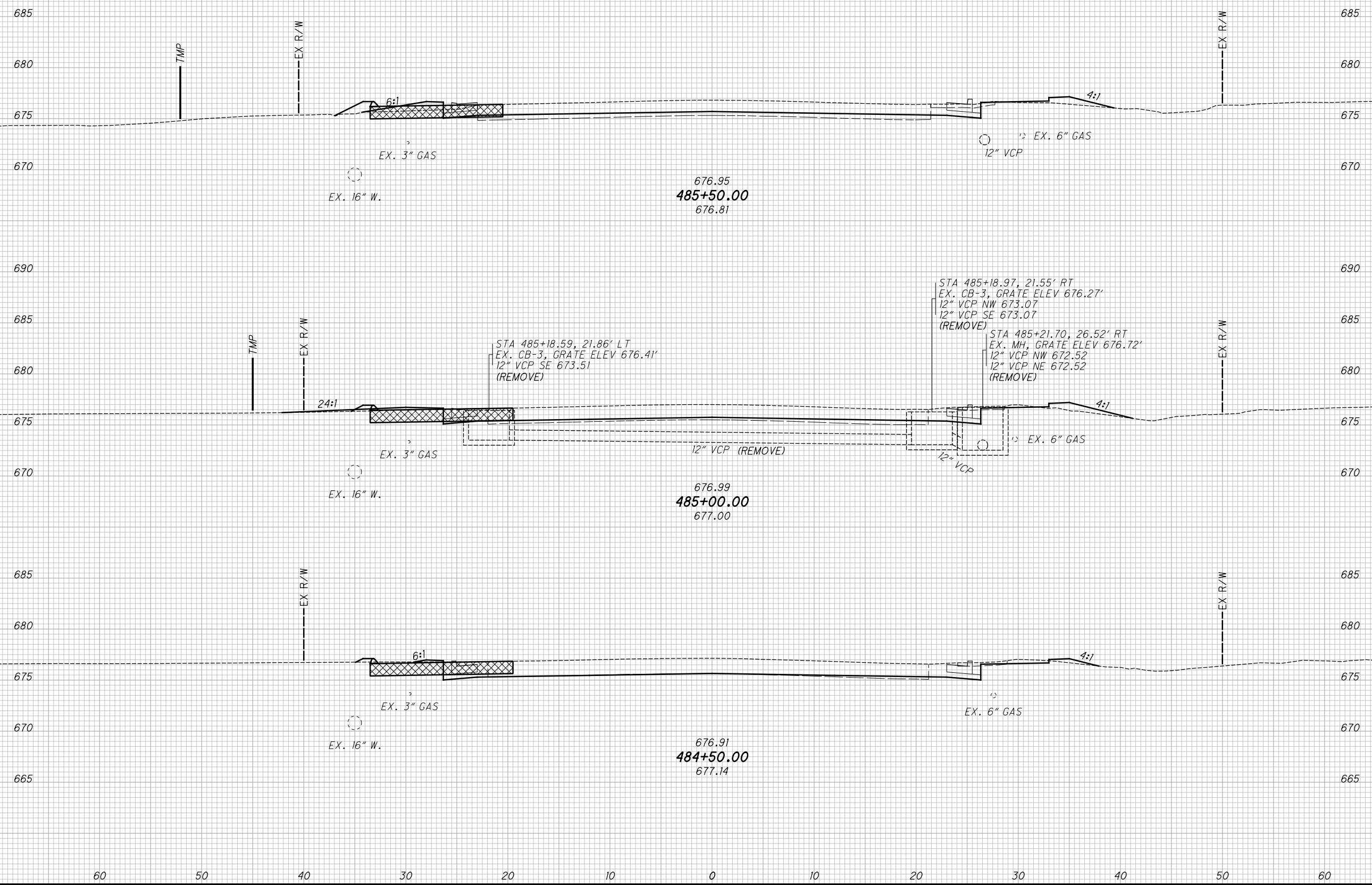
LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
7	7	19	12		
13	6	26	6		
15	1	41	2		
		86	20		

CROSS SECTIONS - U.S. 20
STA. 484+50.00 TO STA. 485+50.00

LAK-US-20-24.99
PART 2

333
697

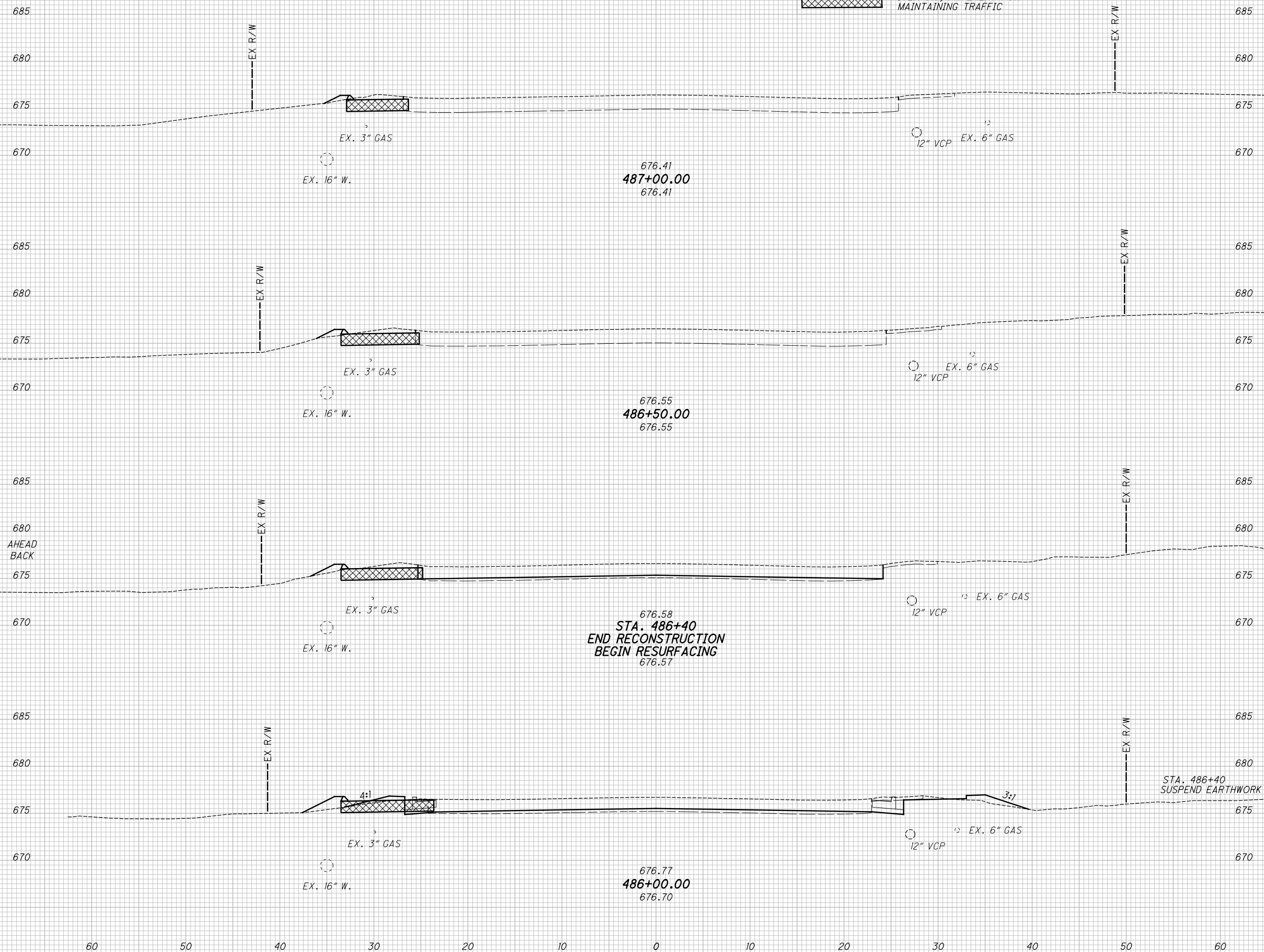


H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_57 7/15/2022 2:55:39 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
16							
17							
17							
153							
32	9	5					
208			15	11			
472			15	11			

LEGEND

 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



676.41
487+00.00
 676.41

676.55
486+50.00
 676.55

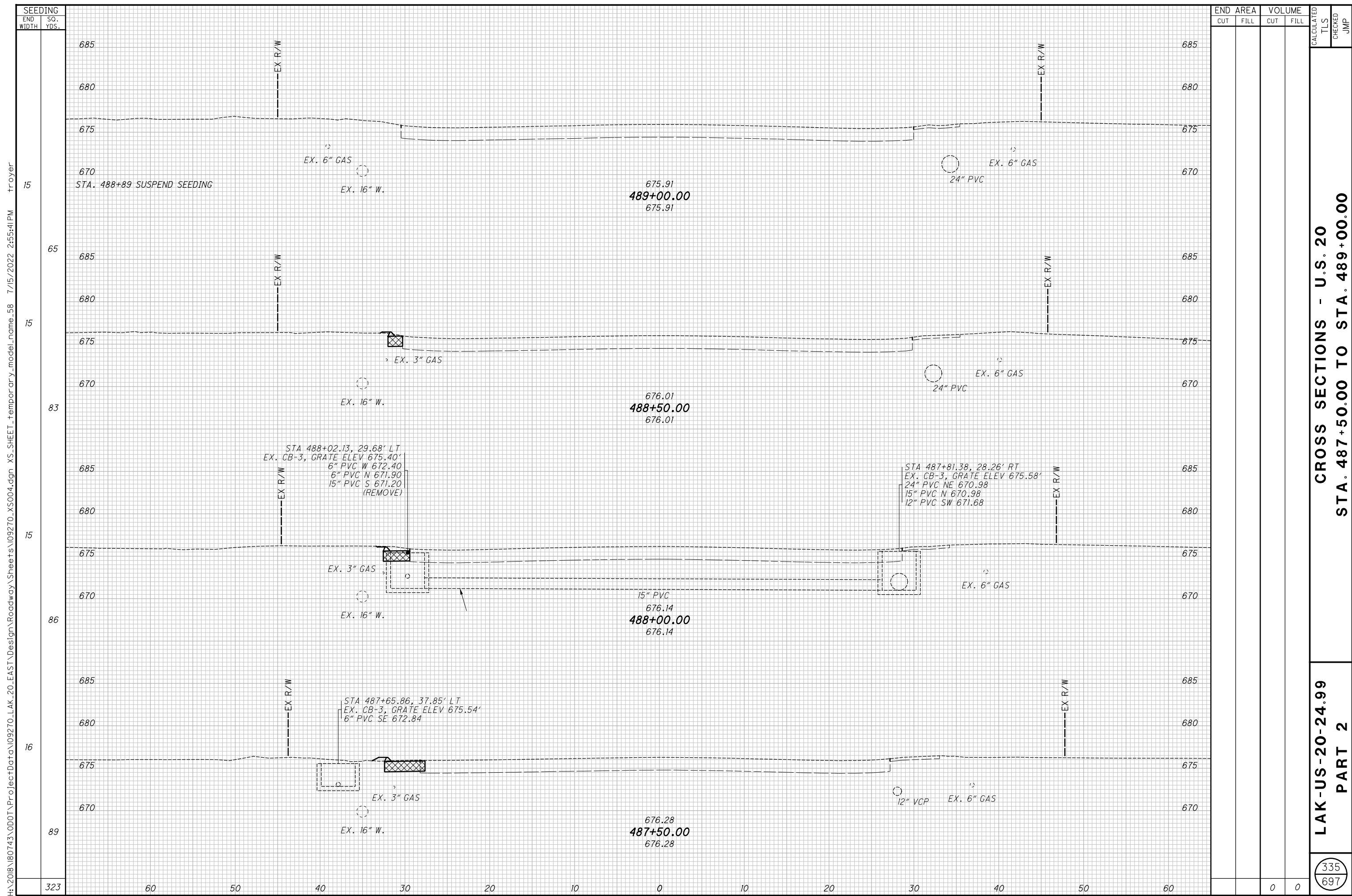
676.58
STA. 486+40
 END RECONSTRUCTION
 BEGIN RESURFACING
 676.57

676.77
486+00.00
 676.70

CROSS SECTIONS - U.S. 20
STA. 486+00.00 TO STA. 487+00.00

LAK-US-20-24.99
PART 2

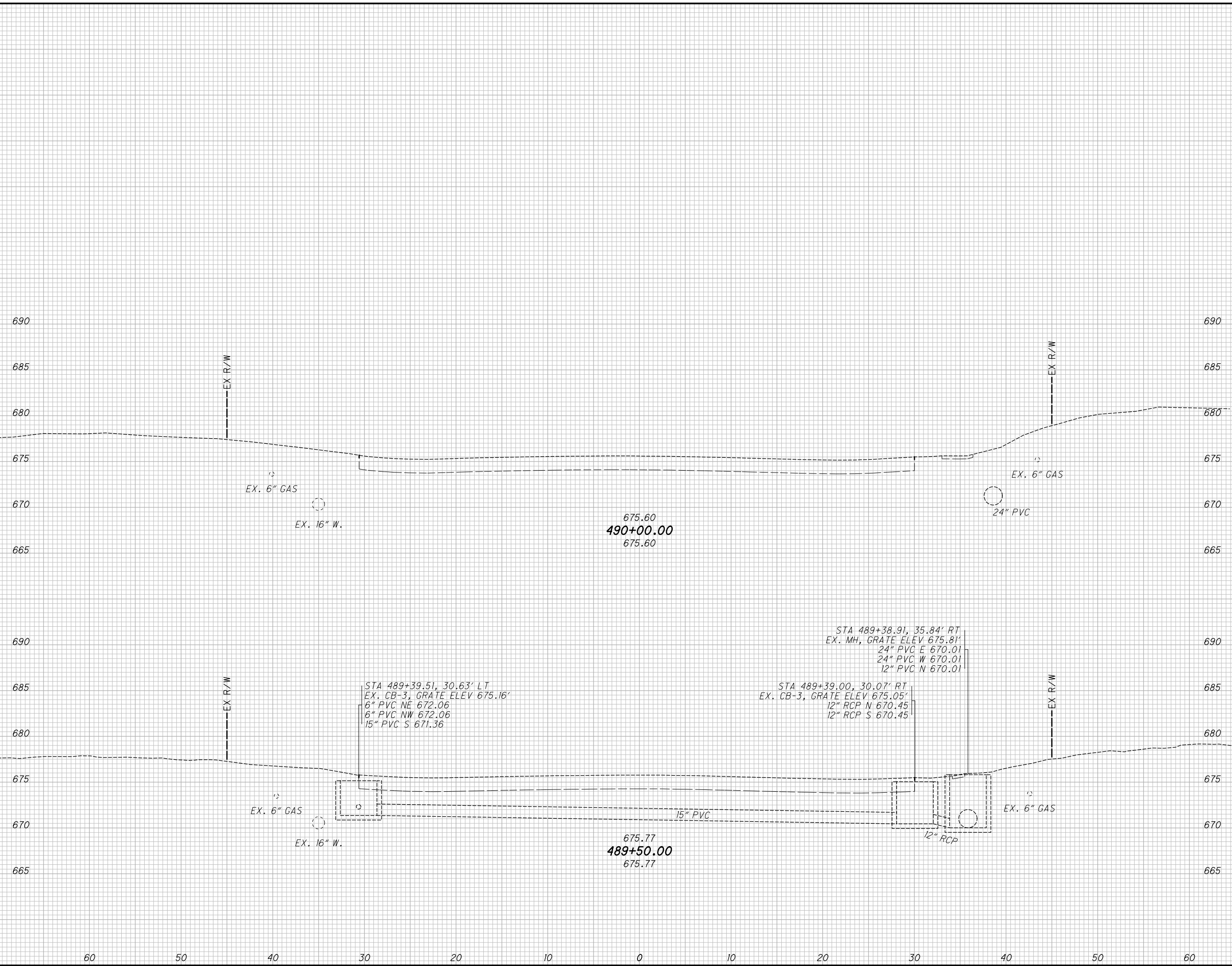
334
 697



H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_58 7/15/2022 2:55:41 PM Troyer

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_59 7/15/2022 2:55:42 PM troyer

SEEDING	
END WIDTH	SO. YDS.
0	



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

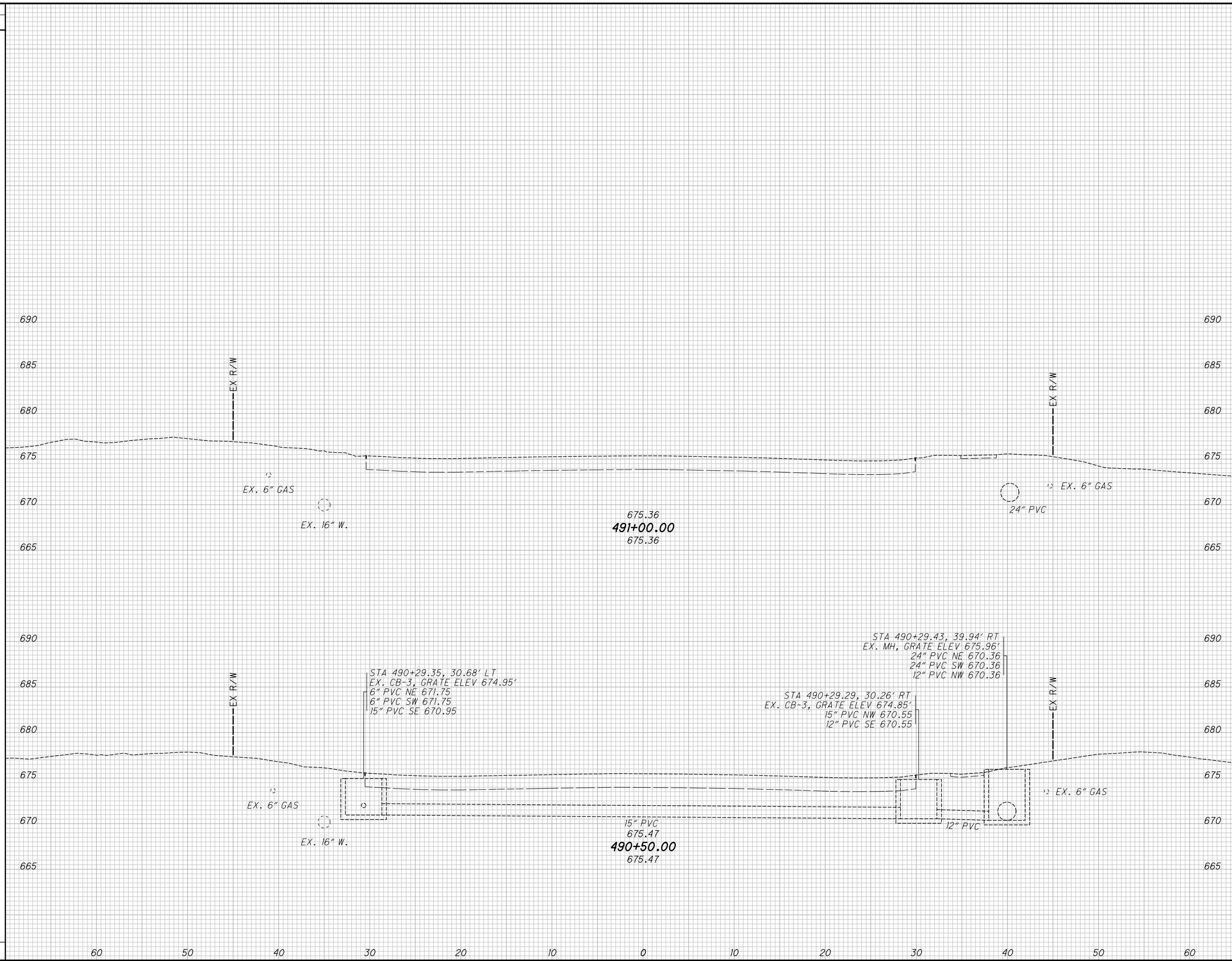
LAK-US-20-24.99
PART 2

CROSS SECTIONS - U.S. 20
STA. 489+50.00 TO STA. 490+00.00

336
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_60 7/15/2022 2:55:43 PM Troyer

SEEDING	
END WIDTH	SO. YDS.
0	



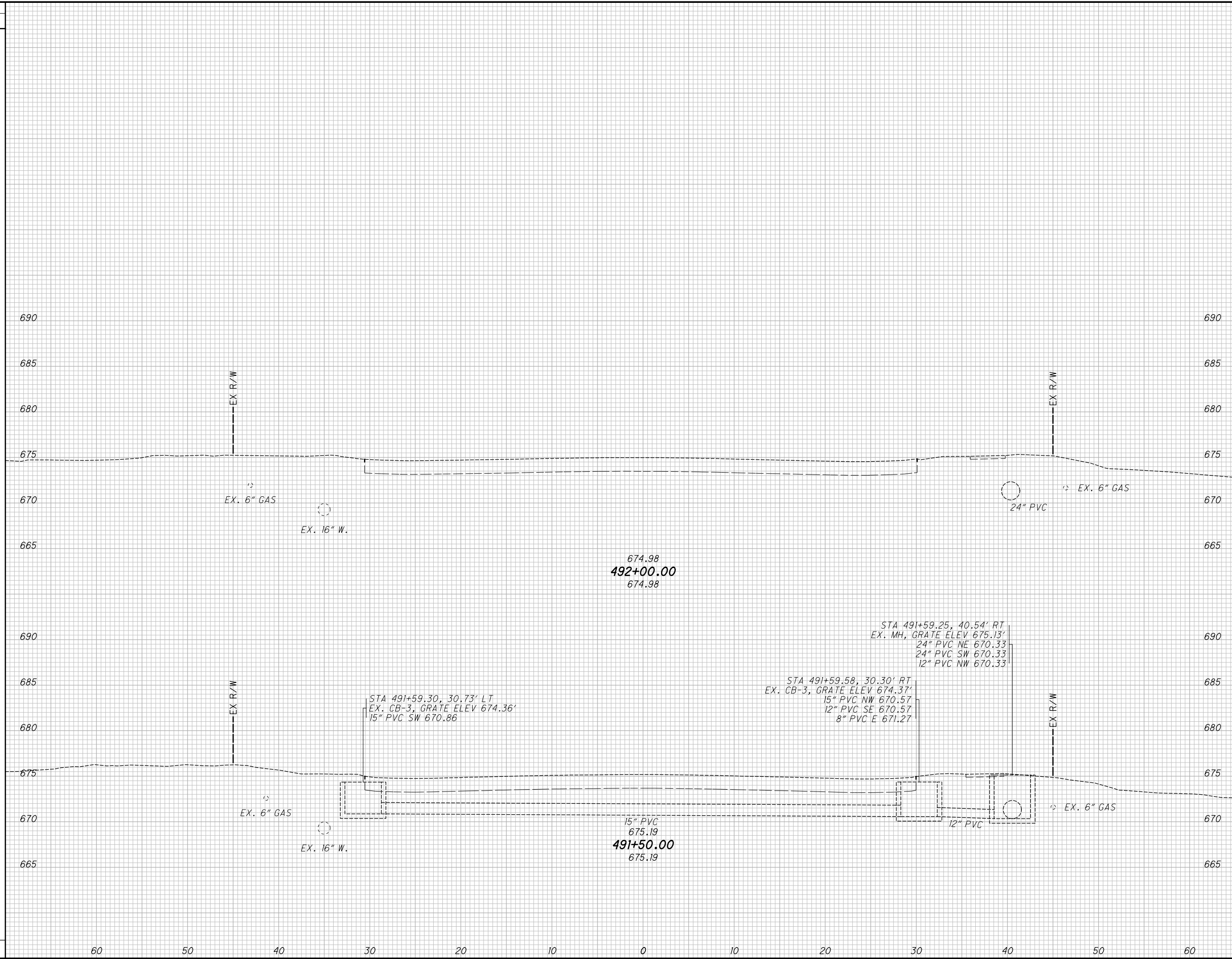
END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

LAK-US-20-24.99	
PART 2	
CROSS SECTIONS - U.S. 20	
STA. 490+50.00 TO STA. 491+00.00	

337
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_61 7/15/2022 2:55:44 PM troyer

SEEDING	
END WIDTH	SO. YDS.
0	



END AREA		VOLUME	
CUT	FILL	CUT	FILL
		0	0

CALCULATED	CHECKED
TLS	JMP

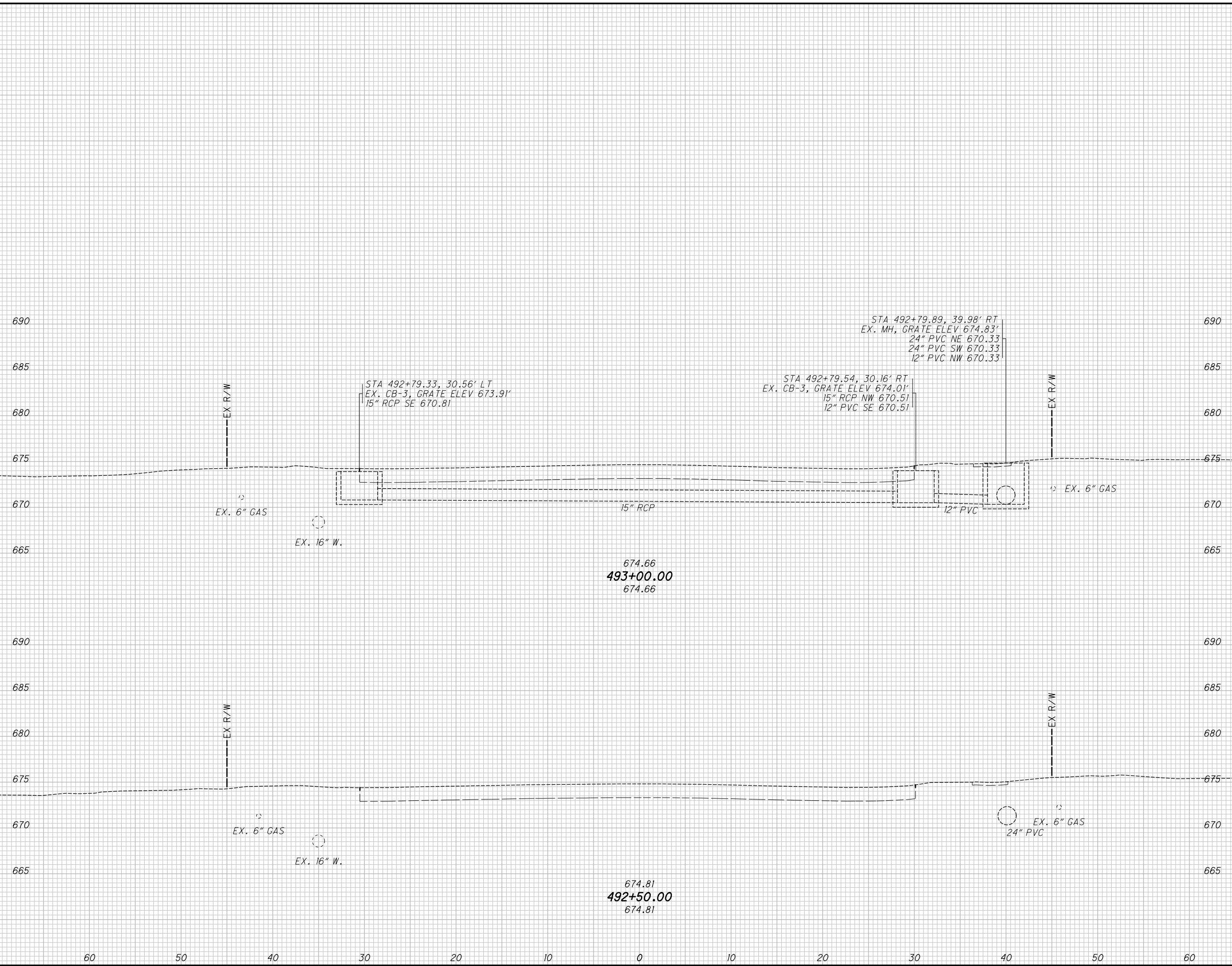
CROSS SECTIONS - U.S. 20
STA. 491+50.00 TO STA. 492+00.00

LAK-US-20-24.99
PART 2

338
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_62 7/15/2022 2:55:45 PM troyer

SEEDING	
END WIDTH	SO. YDS.
0	



END AREA		VOLUME	
CUT	FILL	CUT	FILL
		0	0

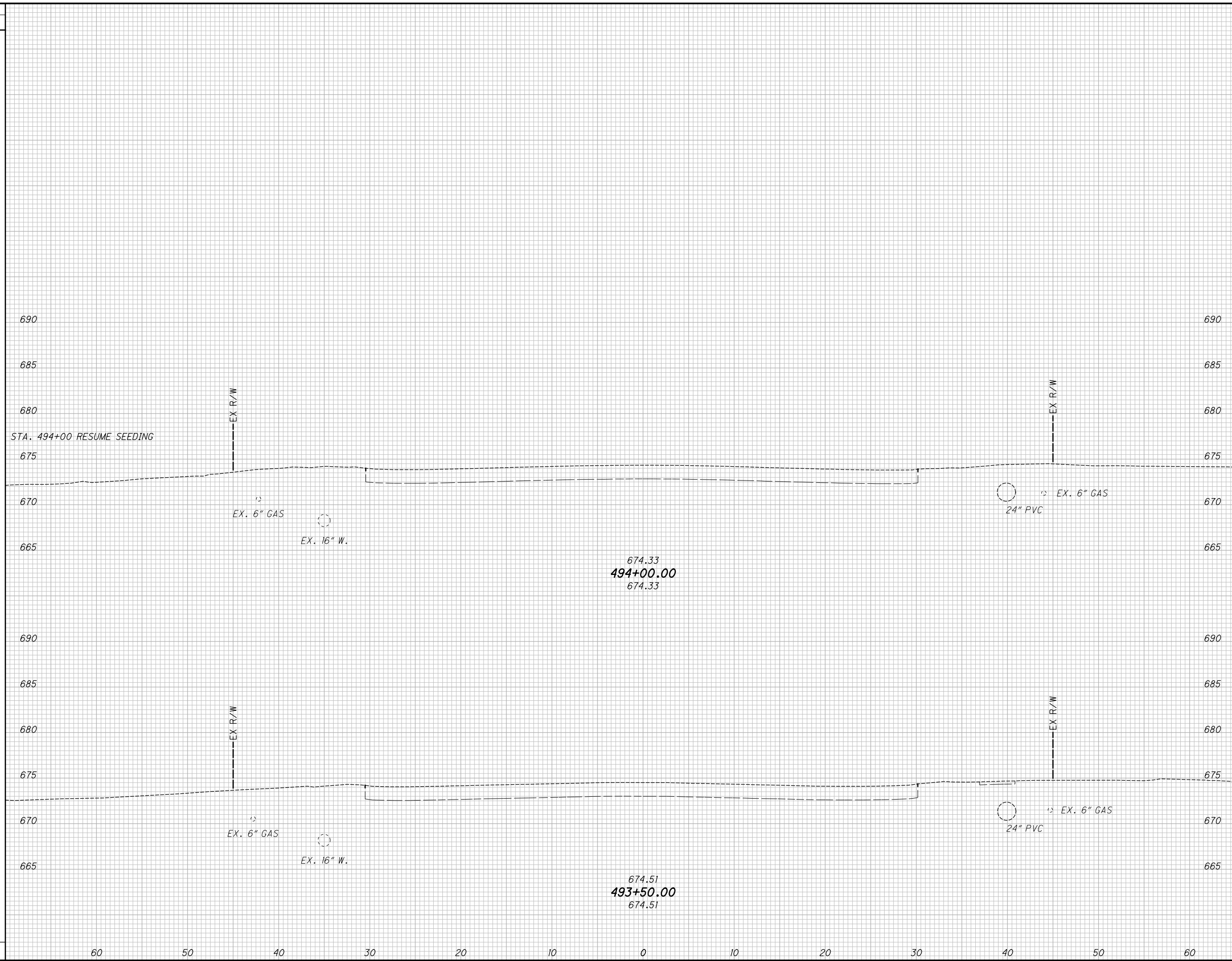
CROSS SECTIONS - U.S. 20
STA. 492+50.00 TO STA. 493+00.00

LAK-US-20-24.99
PART 2

339
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_63 7/15/2022 2:55:46 PM troyer

SEEDING	
END WIDTH	SO. YDS.
0	



END AREA		VOLUME	
CUT	FILL	CUT	FILL
		0	0

CALCULATED	CHECKED
TLS	JMP

CROSS SECTIONS - U.S. 20
STA. 493+50.00 TO STA. 494+00.00

LAK-US-20-24.99
PART 2

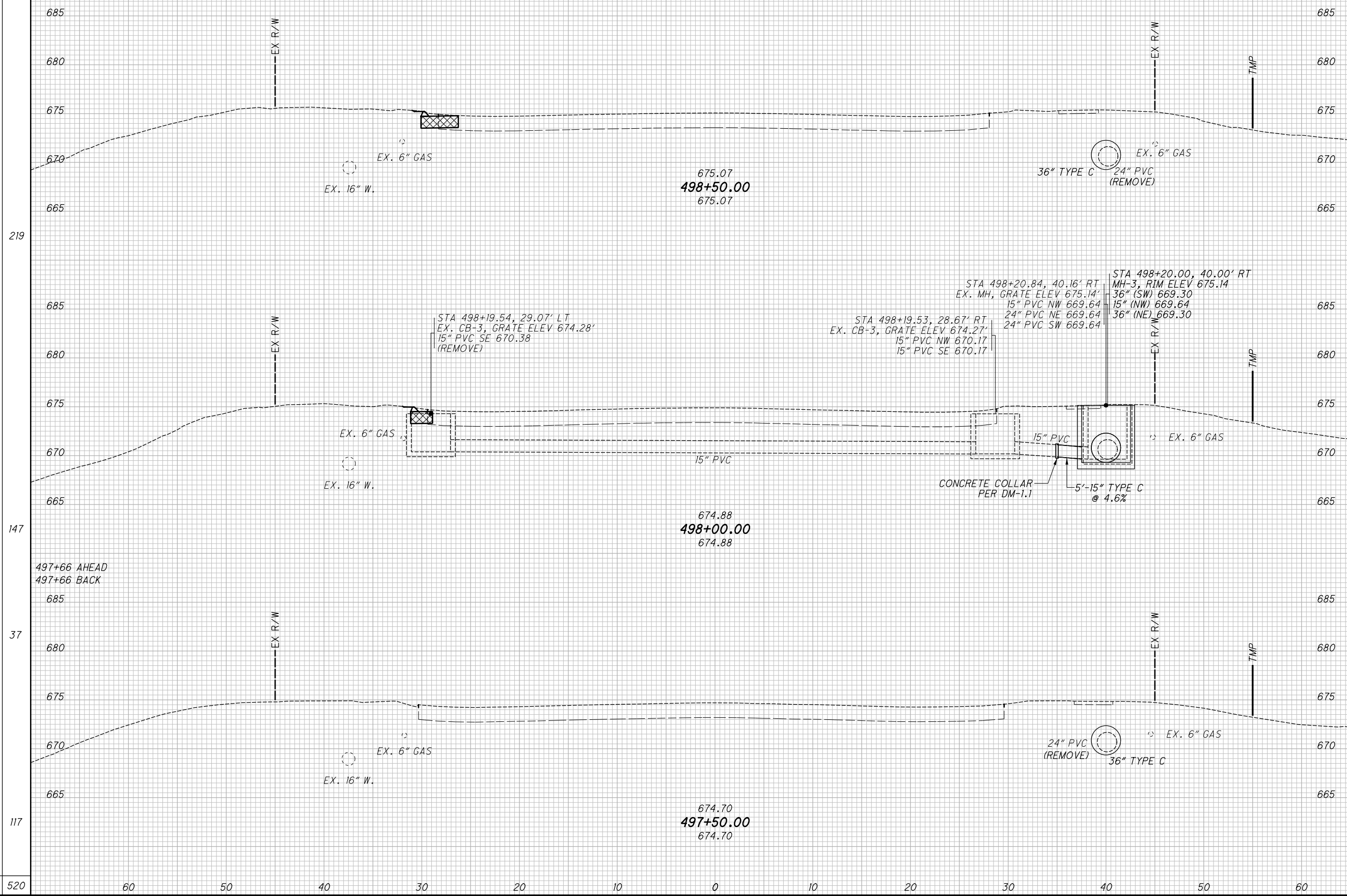
340
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS004.dgn XS_SHEET_temporary_model_name_66 7/15/2022 2:55:50 PM Troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
END WIDTH							
SO. YDS.							
40							
219							
39							
147							
39							
21							
37							
21							
117							
520							

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

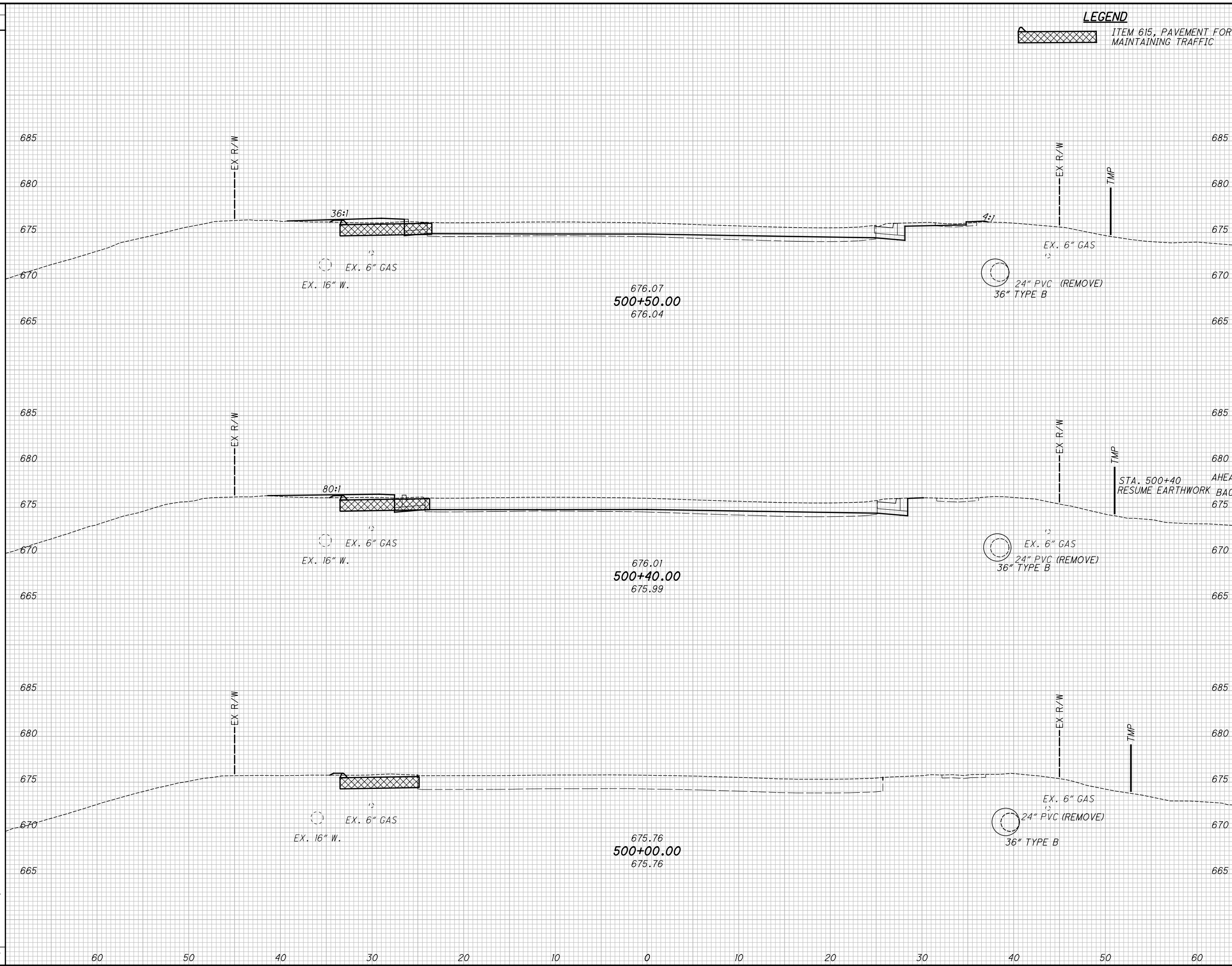


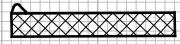
CROSS SECTIONS - U.S. 20
STA. 497+50.00 TO STA. 498+50.00

LAK-US-20-24.99
PART 2

343
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_1 7/15/2022 2:55:53 PM troyer



LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
10	4			
10	4			
0	0			
0	0			
4	1			

CROSS SECTIONS - U.S. 20
STA. 500+00.00 TO STA. 500+50.00

LAK-US-20-24.99
PART 2

345
697



LEGEND
 [Hatched Box] ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

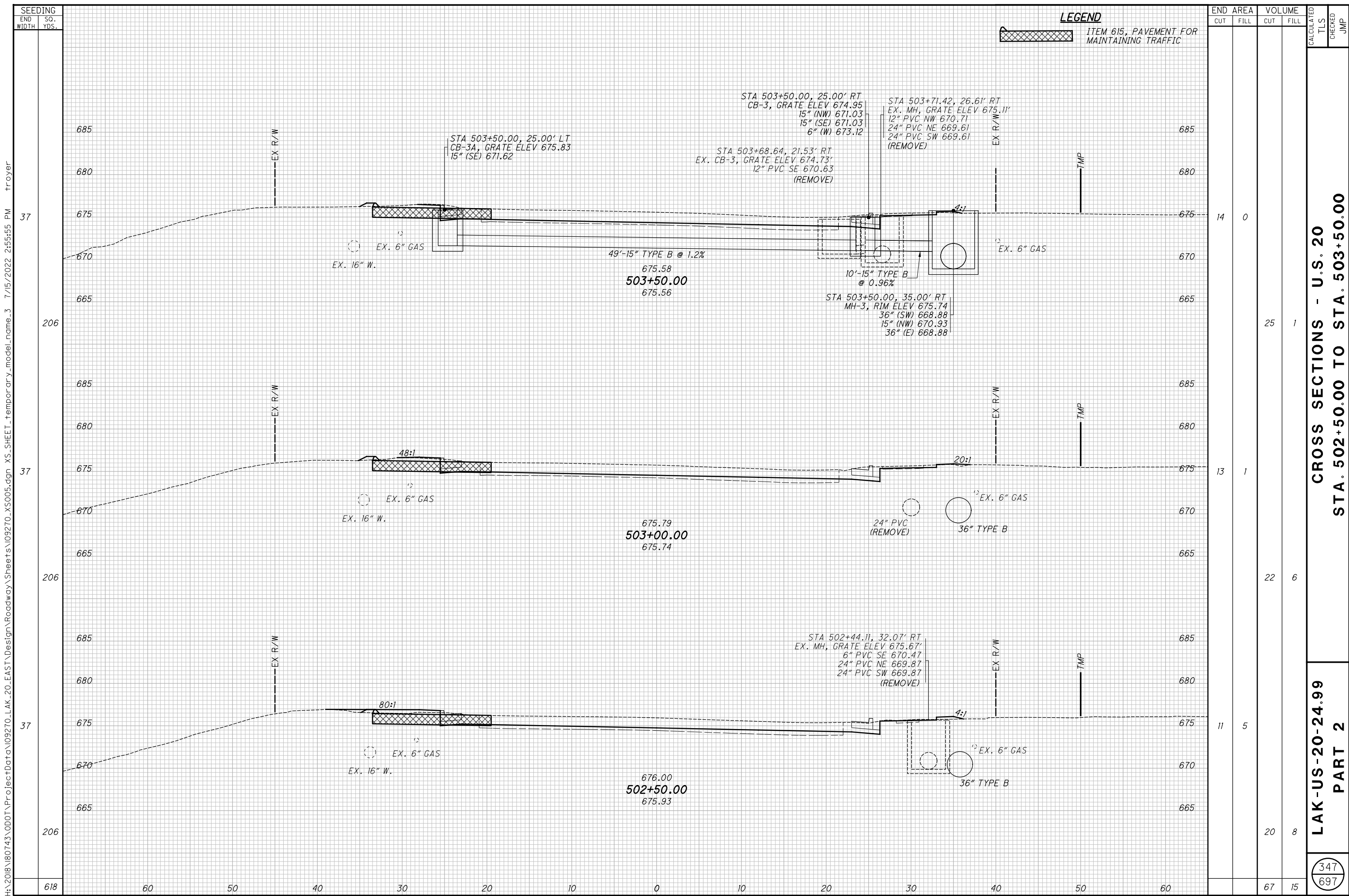
END STA.	AREA		VOLUME		CALCULATED TLS	CHECKED JMP
	CUT	FILL	CUT	FILL		
501+00.00	11	4	18	9		
501+50.00	8	6	13	10		
502+00.00	6	5	15	8		
TOTAL	25	15	46	27		

CROSS SECTIONS - U.S. 20
STA. 501+00.00 TO STA. 502+00.00

LAK-US-20-24.99
PART 2

346
 697

SEEDING
 END WIDTH SO. YDS.
 604 60 50 40 30 20 10 0 10 20 30 40 50 60
 192 37 206 37
 H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_2 7/15/2022 2:55:54 PM Troyer



LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
14	0			
13	1			
11	5			
20	8			
67	15			

CROSS SECTIONS - U.S. 20
STA. 502+50.00 TO STA. 503+50.00

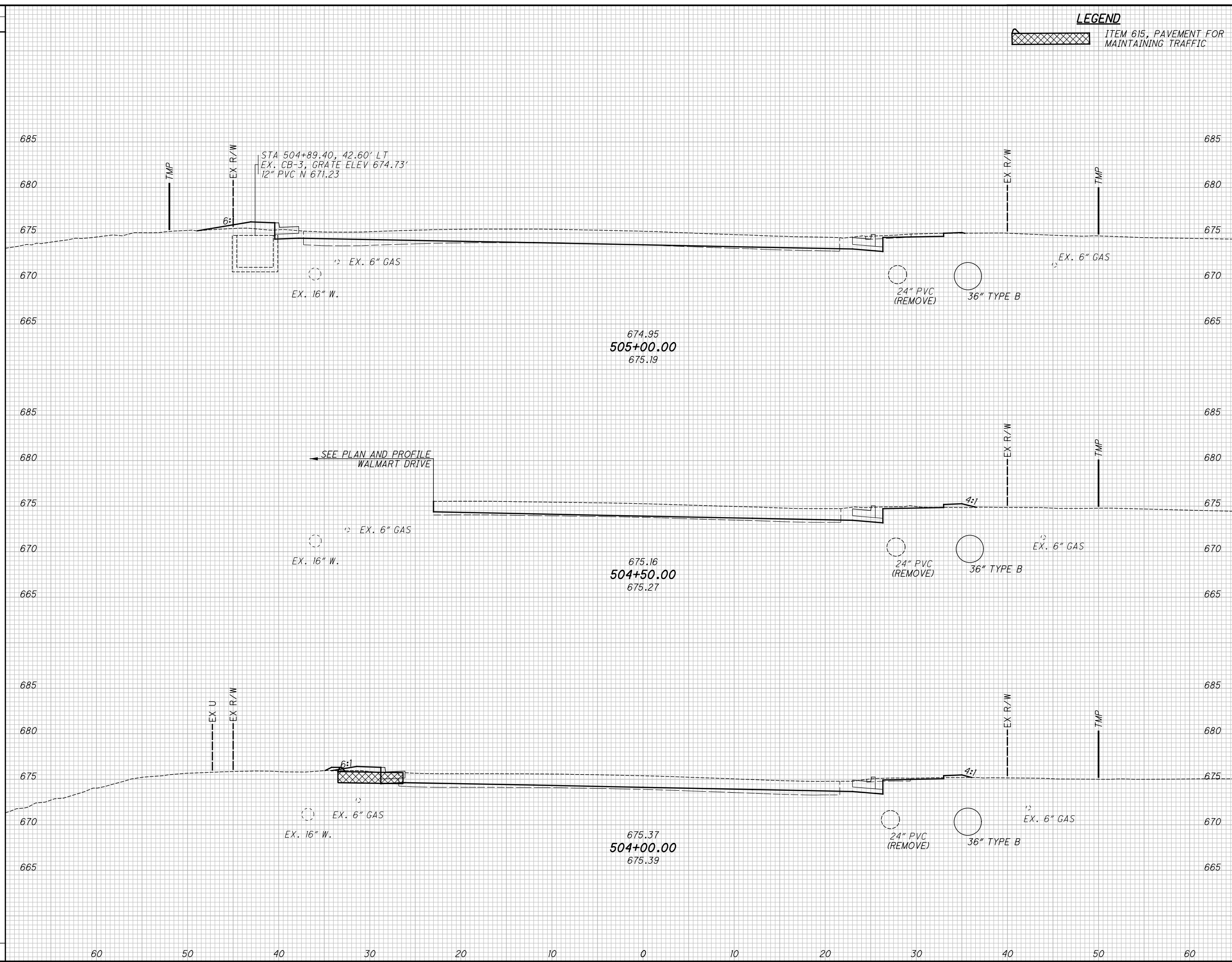
LAK-US-20-24.99
PART 2

347
 697

SEEDING
 END WIDTH SO. YDS.
 618 60 50 40 30 20 10 0 10 20 30 40 50 60
 37 685 680 675 670 665
 206 685 680 675 670 665
 37 685 680 675 670 665
 206 685 680 675 670 665
 37 685 680 675 670 665
 206 685 680 675 670 665
 618 60 50 40 30 20 10 0 10 20 30 40 50 60

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_3 7/15/2022 2:55:55 PM Troyer

SEEDING
END SO.
WIDTH YDS.
29
128
17
139
33
194
461



LEGEND
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
11	4	18	5		
8	1	16	4		
9	3	21	3		
		55	12		

CROSS SECTIONS - U.S. 20
STA. 504+00.00 TO STA. 505+00.00

LAK-US-20-24.99
PART 2

348
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_4 7/15/2022 2:55:56 PM Troyer

SEEDING
END WIDTH SO. YDS.
26
144
144
26
153
441

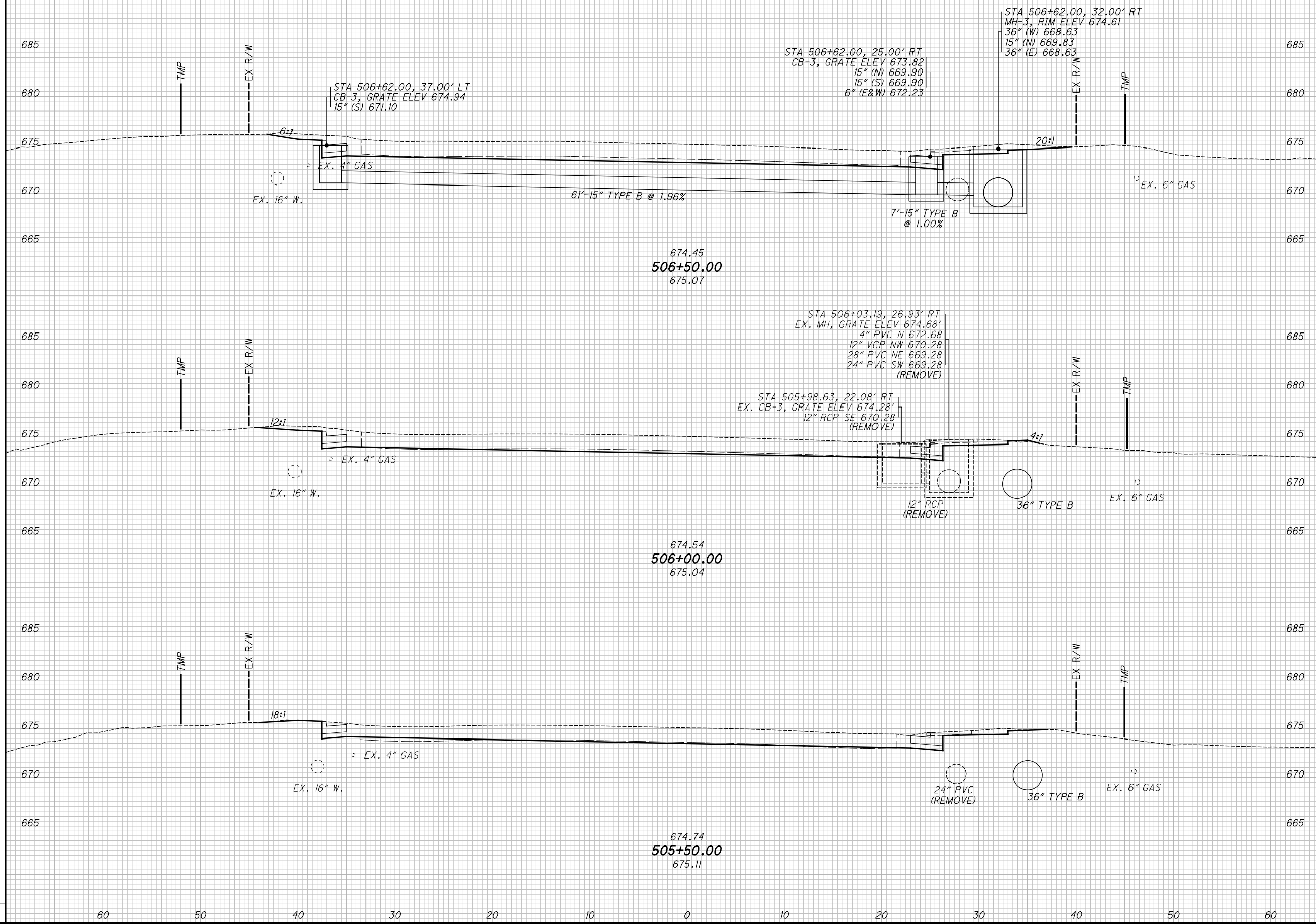
LEGEND
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
39	0	62	0		
28	0	44	0		
19	0	28	4		
		134	4		

CROSS SECTIONS - U.S. 20
STA. 505+50.00 TO STA. 506+50.00

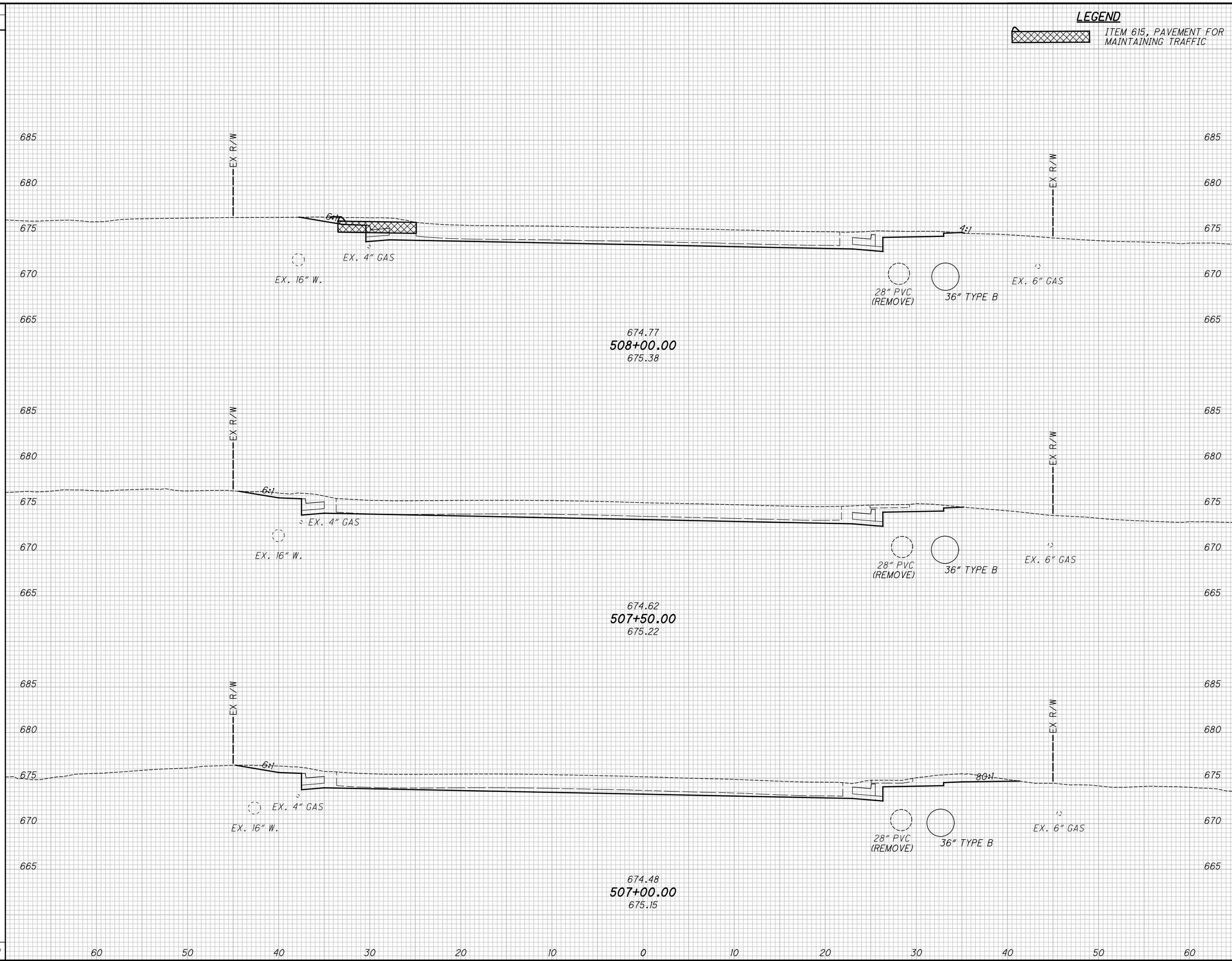
LAK-US-20-24.99
PART 2

349
697



H:\2018\80743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_5 7/15/2022 2:55:58 PM Troyer

H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_6 7/15/2022 2:55:59 PM Troyer

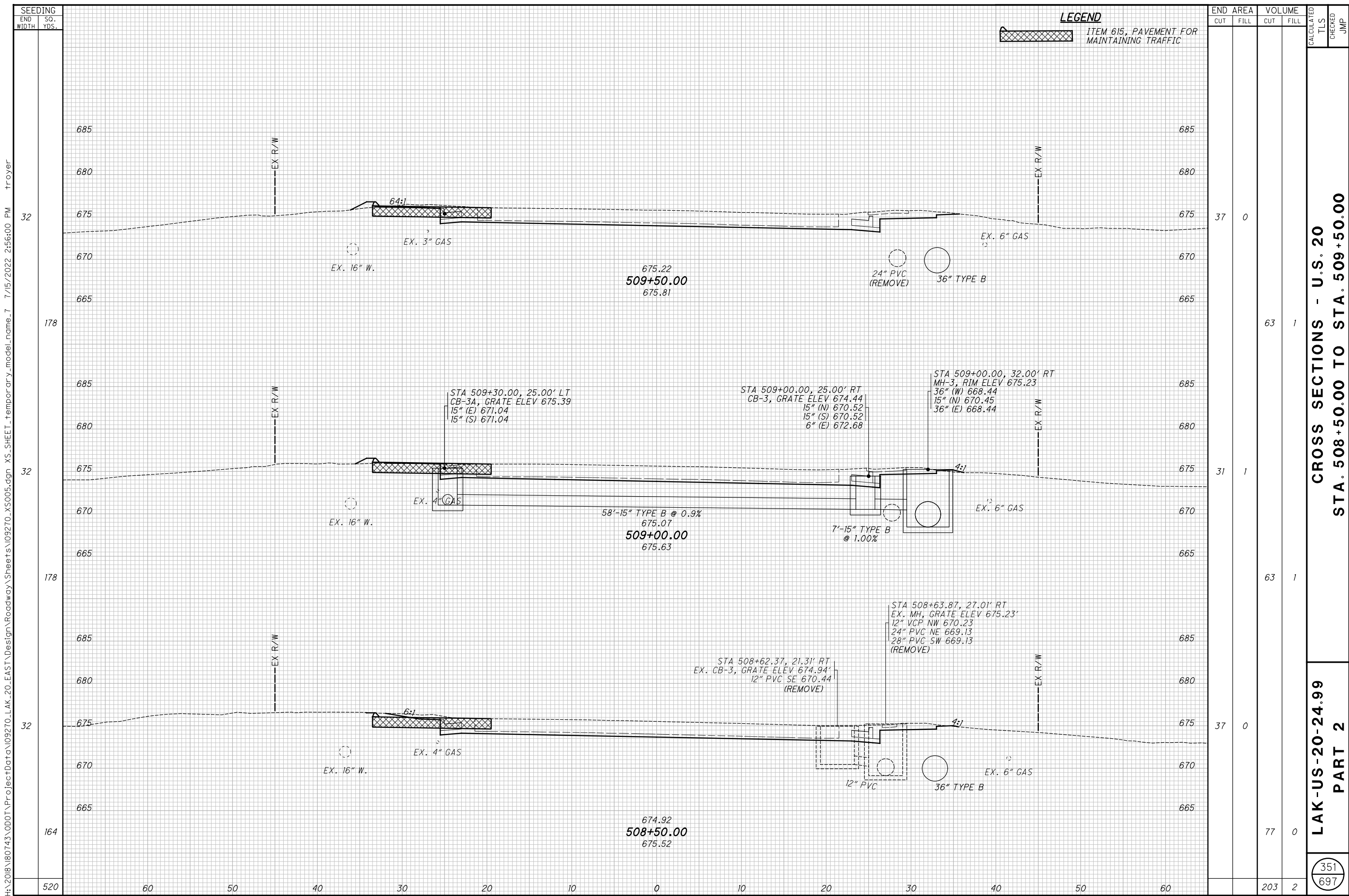


LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
46	0			
39	0			
47	0			
	239	0		

LAK-US-20-24.99
PART 2
CROSS SECTIONS - U.S. 20
STA. 507+00.00 TO STA. 508+00.00

350
697



CROSS SECTIONS - U.S. 20
STA. 508+50.00 TO STA. 509+50.00

LAK-US-20-24.99
PART 2

351
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_7 7/15/2022 2:56:00 PM Troyer

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_8 7/15/2022 2:56:01PM troyer

SEEDING	END	
	WIDTH	SO. YDS.
	32	685
	178	665
	32	675
	178	665
	32	675
	178	665
	32	675
	178	665
534	60	665

LEGEND

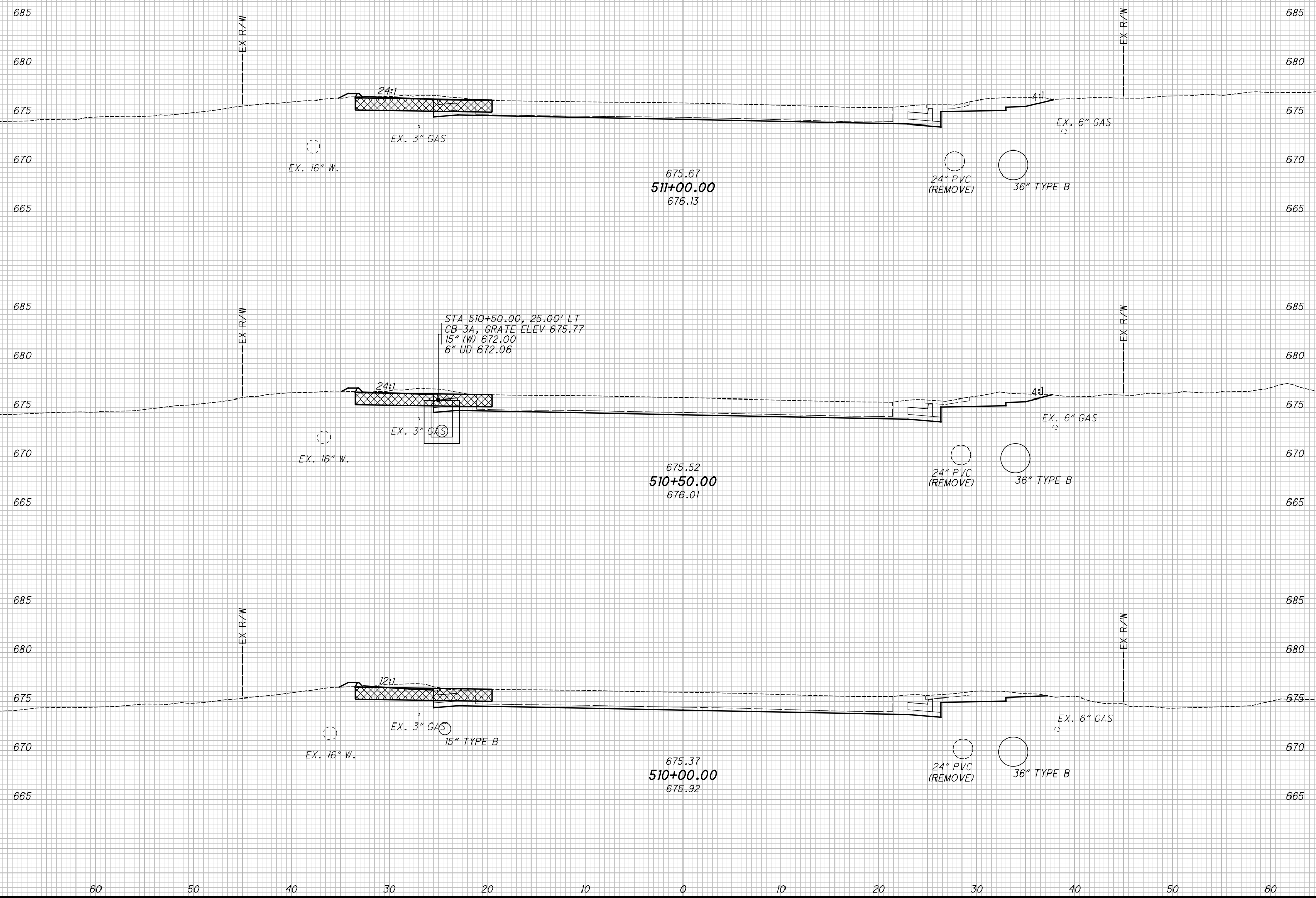
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
34	0			
38	0			
38	0			
206	0			

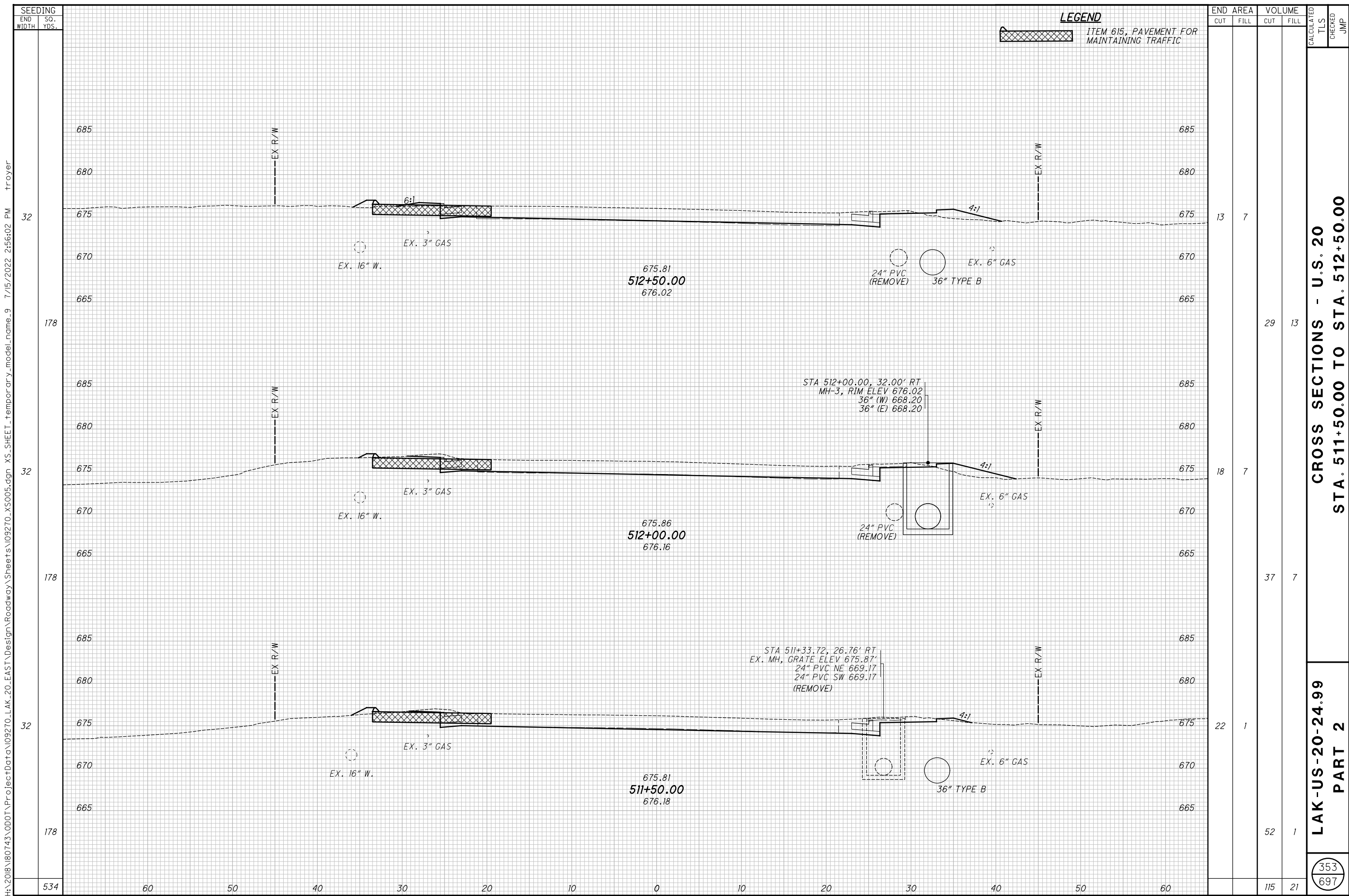
CROSS SECTIONS - U.S. 20
STA. 510+00.00 TO STA. 511+00.00

LAK-US-20-24.99
PART 2

352
697



STA 510+50.00, 25.00' LT
 CB-3A, GRATE ELEV 675.77
 15" (W) 672.00
 6" UD 672.06



SEEDING	
END WIDTH	SO. YDS.
32	178
32	178
32	178
534	

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
13	7	29	13		
18	7	37	7		
22	1	52	1		
		115	21		

CROSS SECTIONS - U.S. 20
STA. 511+50.00 TO STA. 512+50.00

LAK-US-20-24.99
PART 2

353
 697

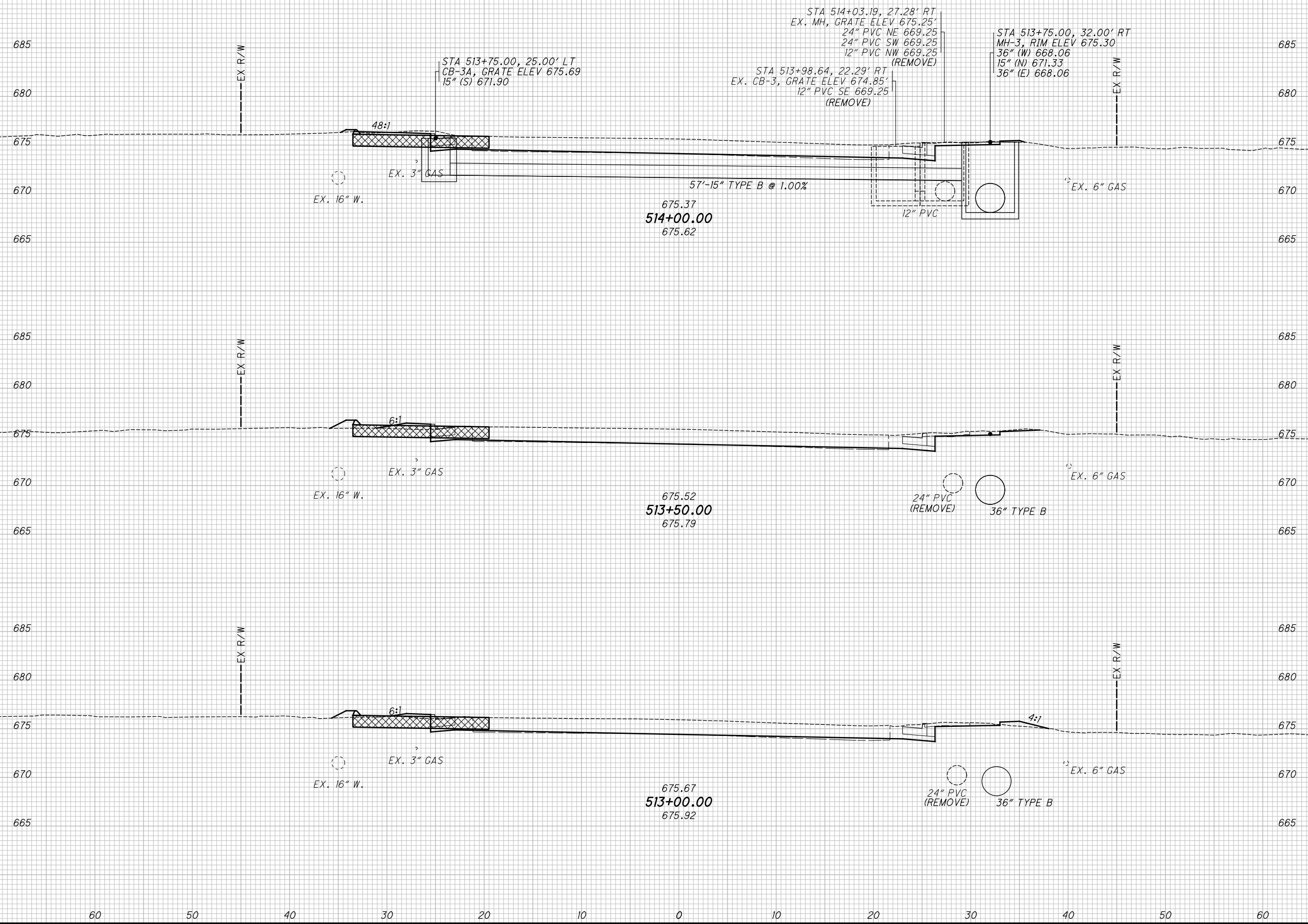
H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_9 7/15/2022 2:56:02 PM troyer

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_10 7/15/2022 2:56:03 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
END WIDTH	17	1	30	3			
SO. YDS.	15	2	26	5			
	13	3	24	9			
			80	17			

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



STA 513+75.00, 25.00' LT
CB-3A, GRATE ELEV 675.69
15" (S) 671.90

STA 513+98.64, 22.29' RT
EX. CB-3, GRATE ELEV 674.85'
12" PVC SE 669.25
(REMOVE)

STA 513+75.00, 32.00' RT
MH-3, RIM ELEV 675.30
36" (W) 668.06
15" (N) 671.33
36" (E) 668.06

675.37
514+00.00
675.62

675.52
513+50.00
675.79

675.67
513+00.00
675.92

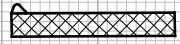
CROSS SECTIONS - U.S. 20
STA. 513+00.00 TO STA. 514+00.00

LAK-US-20-24.99
PART 2

354
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_11/7/15/2022 2:56:05 PM Troyer

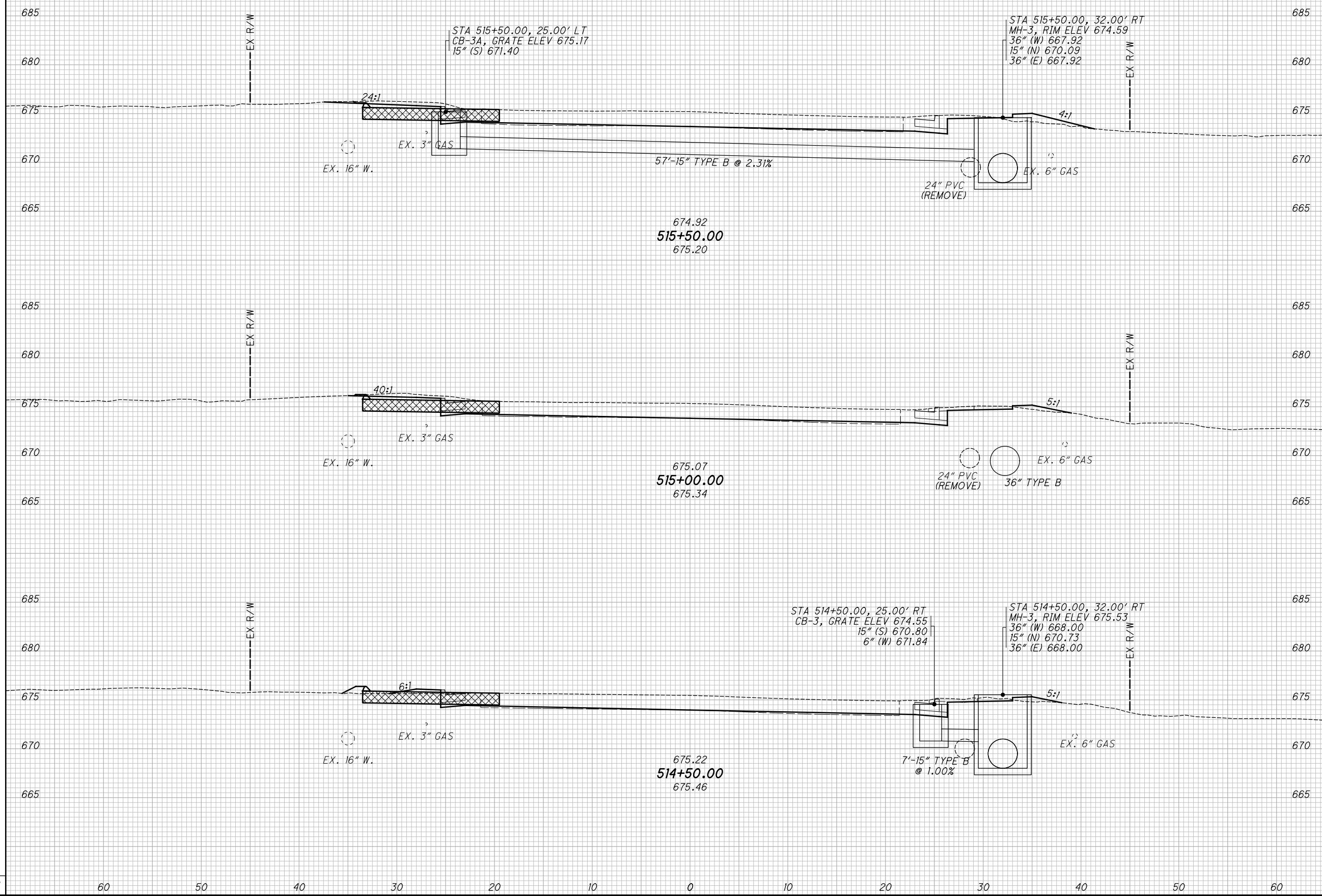
SEEDING	
END WIDTH	SO. YDS.
32	
178	
32	
178	
32	
178	
534	

LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

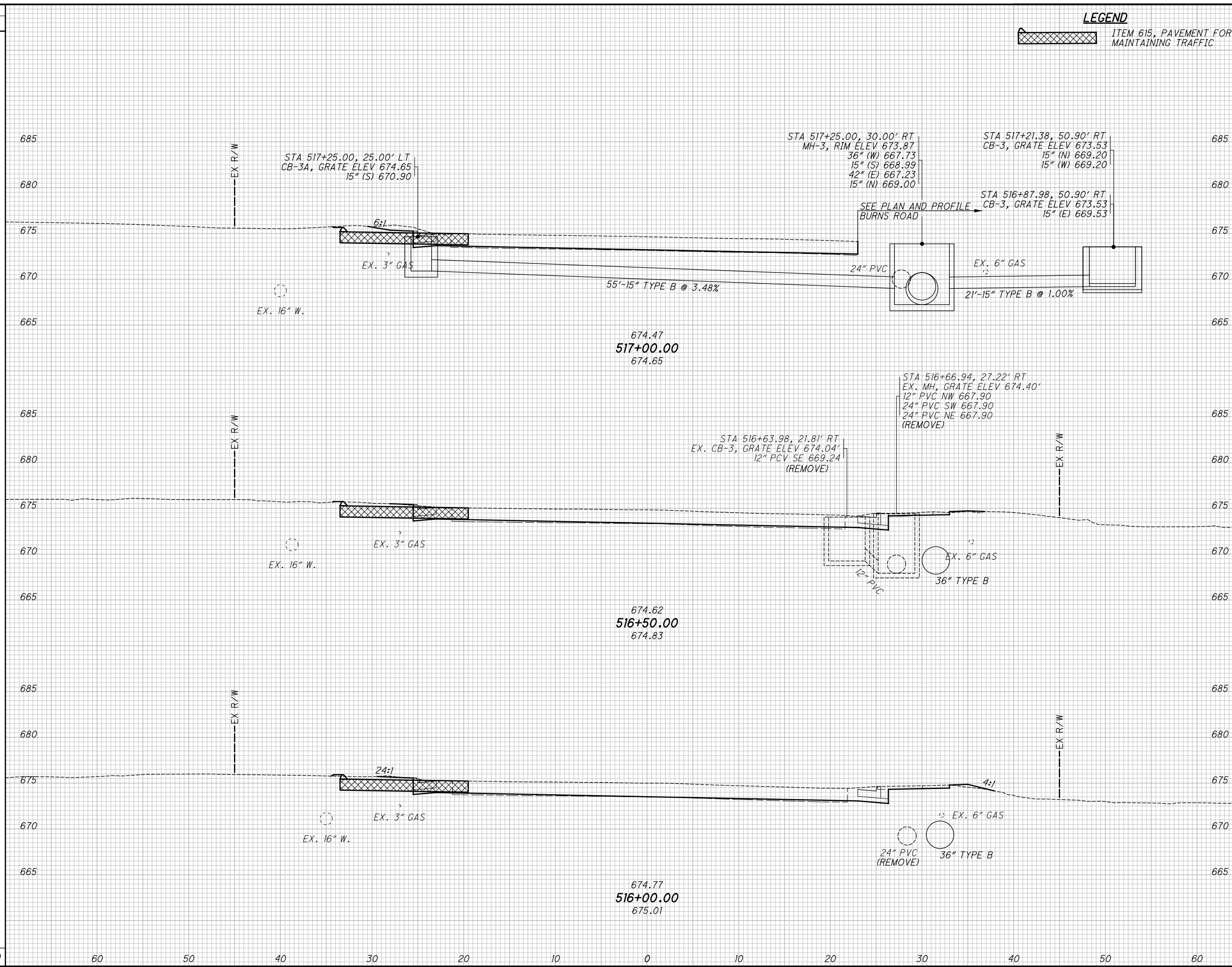
END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
19	4				
		33	6		
17	2				
		29	5		
14	3				
		29	4		
		91	15		

CROSS SECTIONS - U.S. 20
STA. 514+50.00 TO STA. 515+50.00
LAK-US-20-24.99
PART 2

355
697



SEEDING
 END SO.
 WIDTH YDS.
 20
 144
 32
 178
 32
 178
 500



LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
8	0			
14	0			
14	1			
31	5			
77	6			

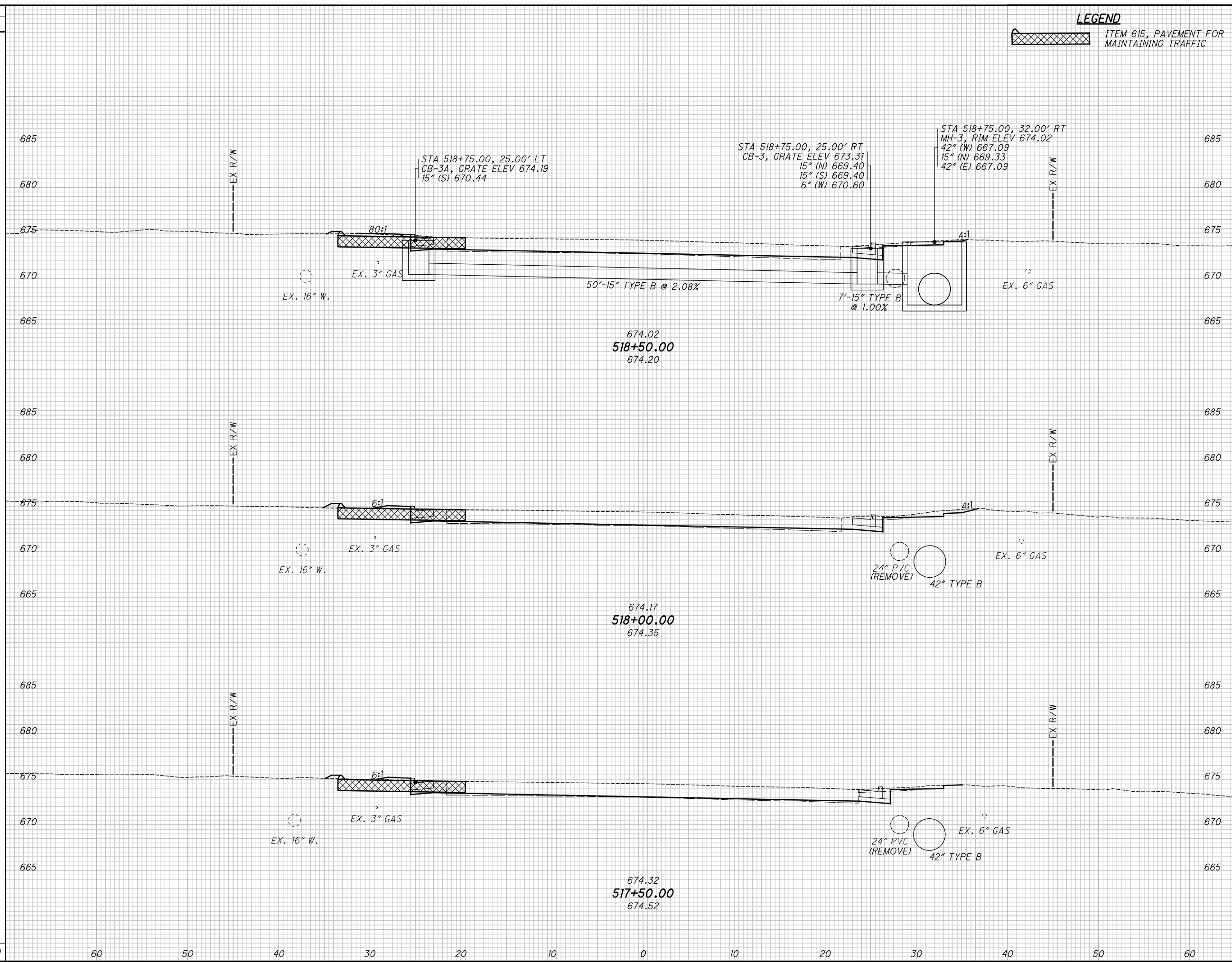
CROSS SECTIONS - U.S. 20
STA. 516+00.00 TO STA. 517+00.00

LAK-US-20-24.99
PART 2

356
 697

H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_12 7/15/2022 2:56:11 PM Troyer

SEEDING
 END WIDTH SO. YDS.
 32
 178
 178
 32
 144
 500

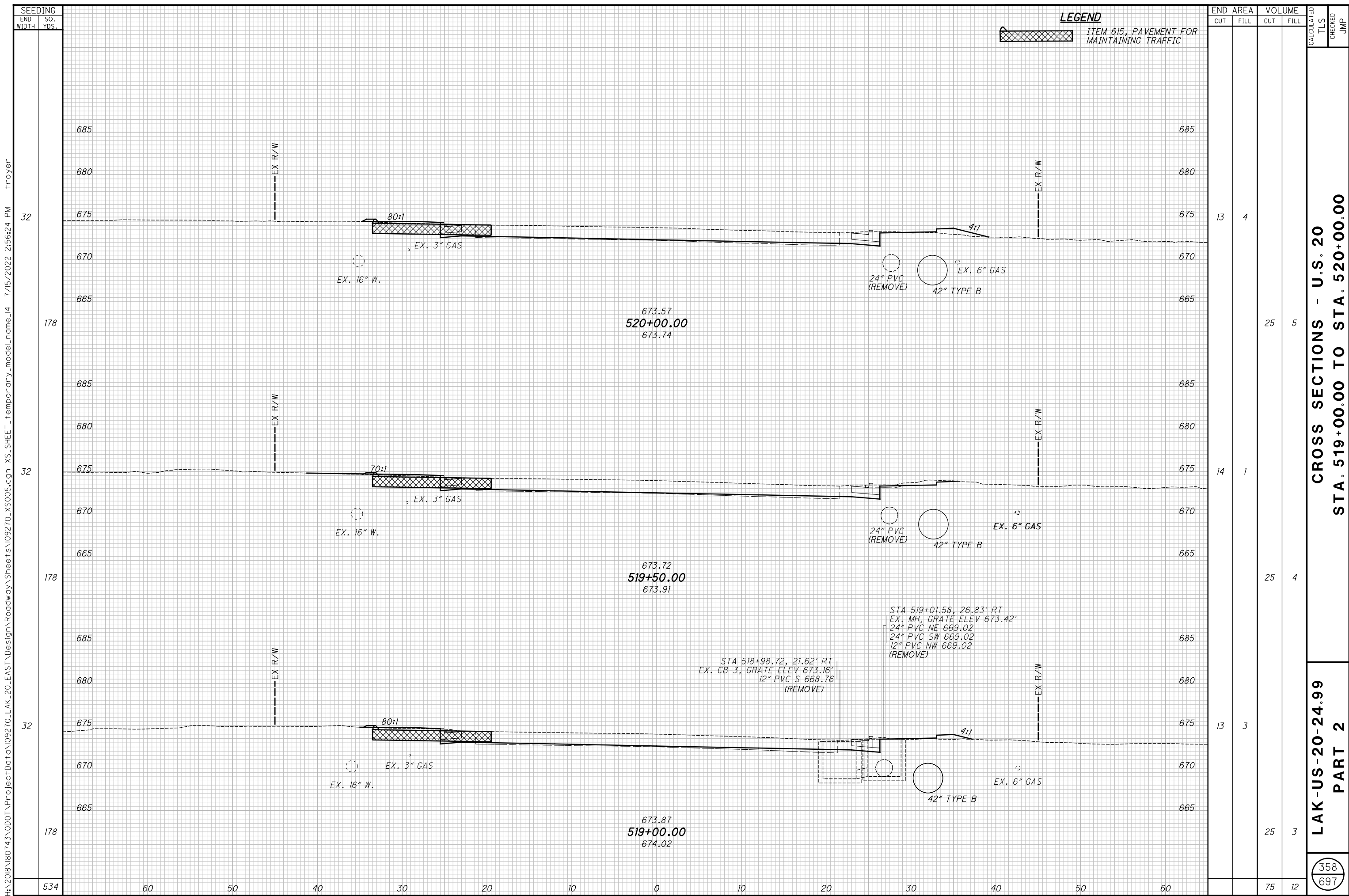


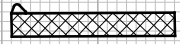
LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
14	0			
14	1	26	1	
10	1	22	2	
17	1	65	4	

CROSS SECTIONS - U.S. 20
STA. 517+50.00 TO STA. 518+50.00
LAK-US-20-24.99
PART 2
 357
 697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_13 7/15/2022 2:56:22 PM troyer



LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
13	4			
14	1			
13	3			
75	12			

CROSS SECTIONS - U.S. 20
STA. 519+00.00 TO STA. 520+00.00

LAK-US-20-24.99
PART 2

358
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_14 7/15/2022 2:56:24 PM troyer

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_15 7/15/2022 2:56:25 PM Troyer

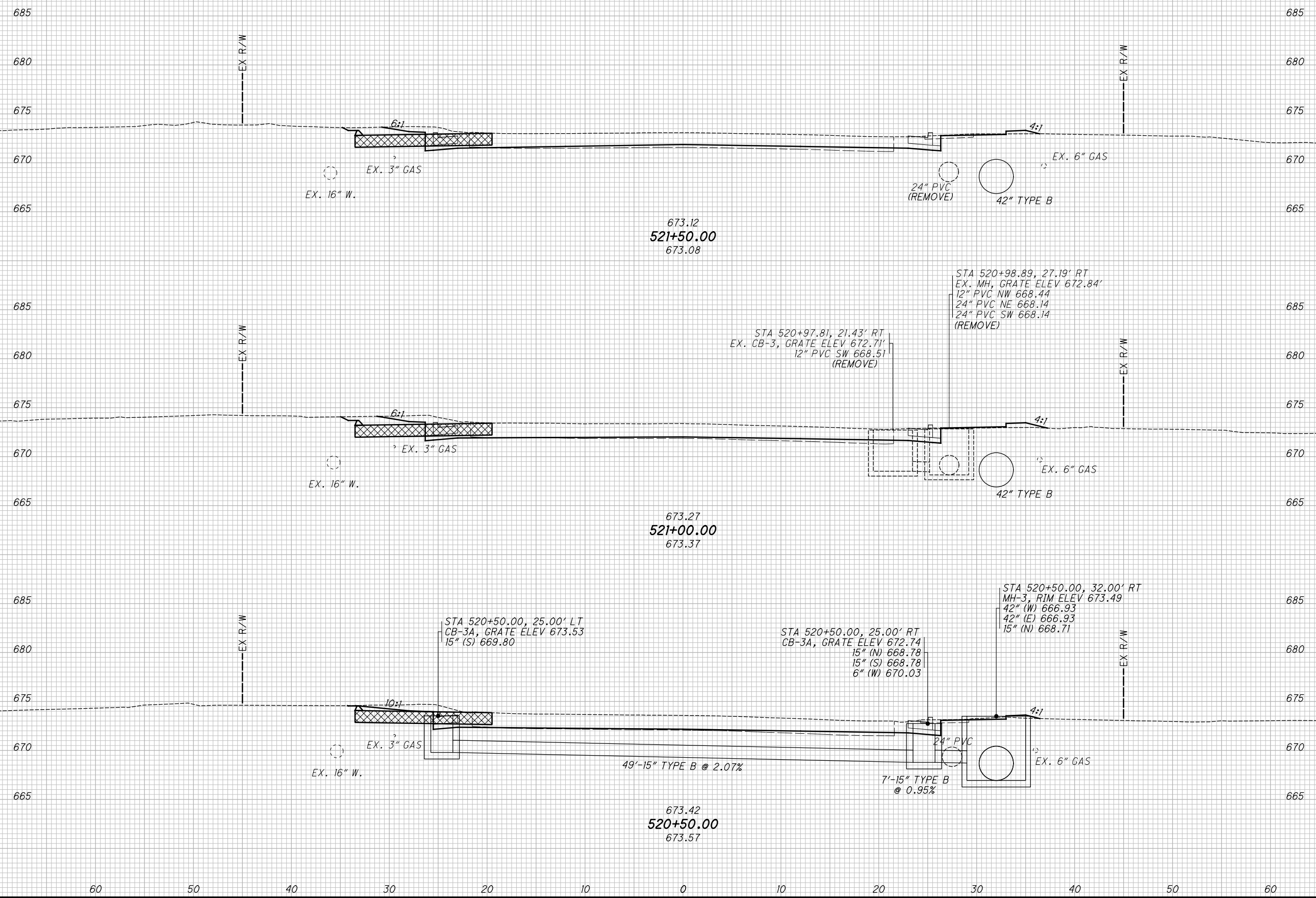
SEEDING	
END WIDTH	SO. YDS.
31	675
172	665
31	675
172	665
31	675
175	665
519	665

LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
17	1				
		32	3		
18	2				
		32	3		
17	1				
		28	5		
		92	11		

LAK-US-20-24.99
PART 2
CROSS SECTIONS - U.S. 20
STA. 520+50.00 TO STA. 521+50.00

359
697



H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_16 7/15/2022 2:56:26 PM troyer

SEEDING	
END WIDTH	SO. YDS.
31	172
31	172
31	172
516	

LEGEND

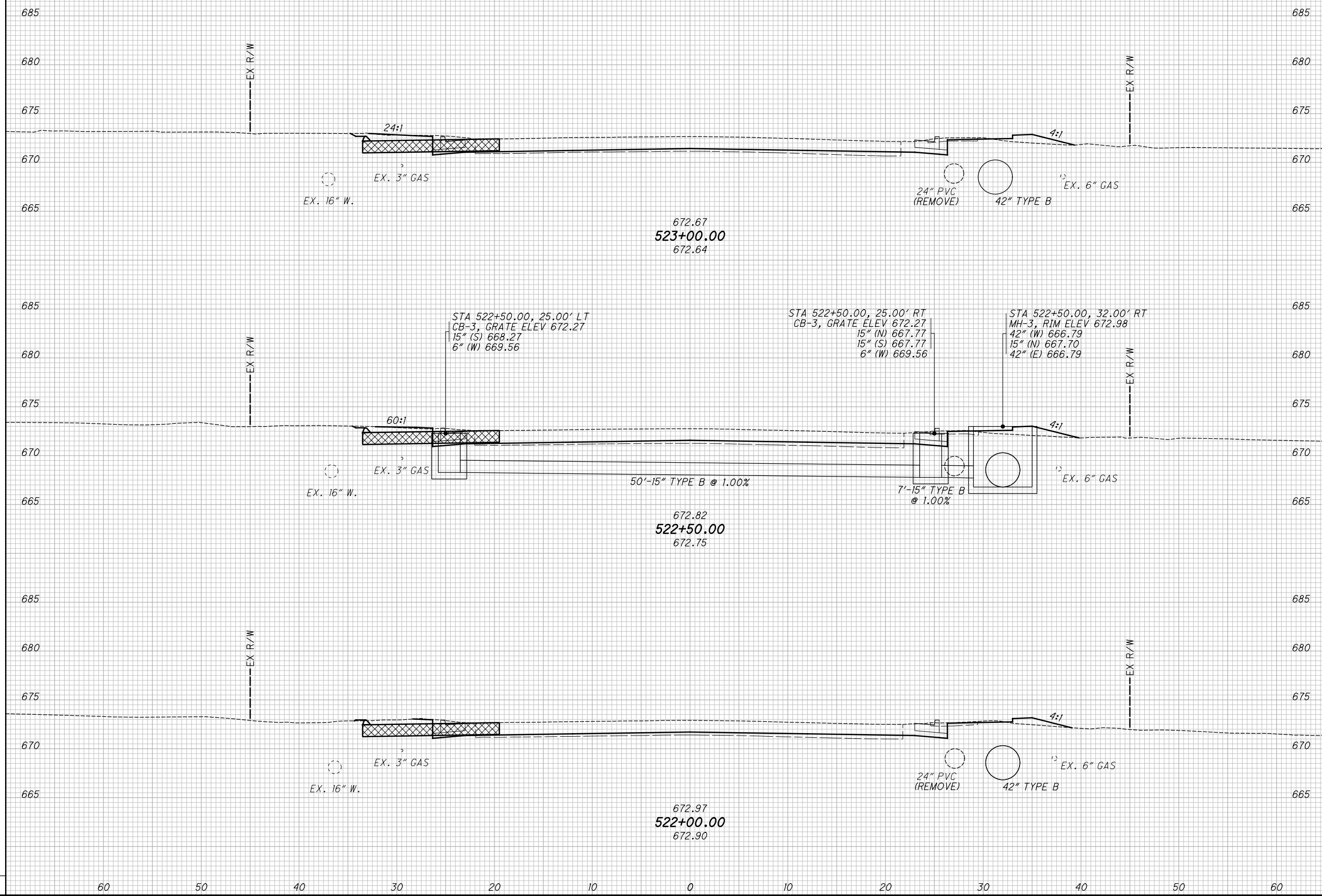
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
13	4	23	8		
12	5	23	7		
13	3				
		74	19		

**CROSS SECTIONS - U.S. 20
STA. 522+00.00 TO STA. 523+00.00**

**LAK-US-20-24.99
PART 2**

360
697



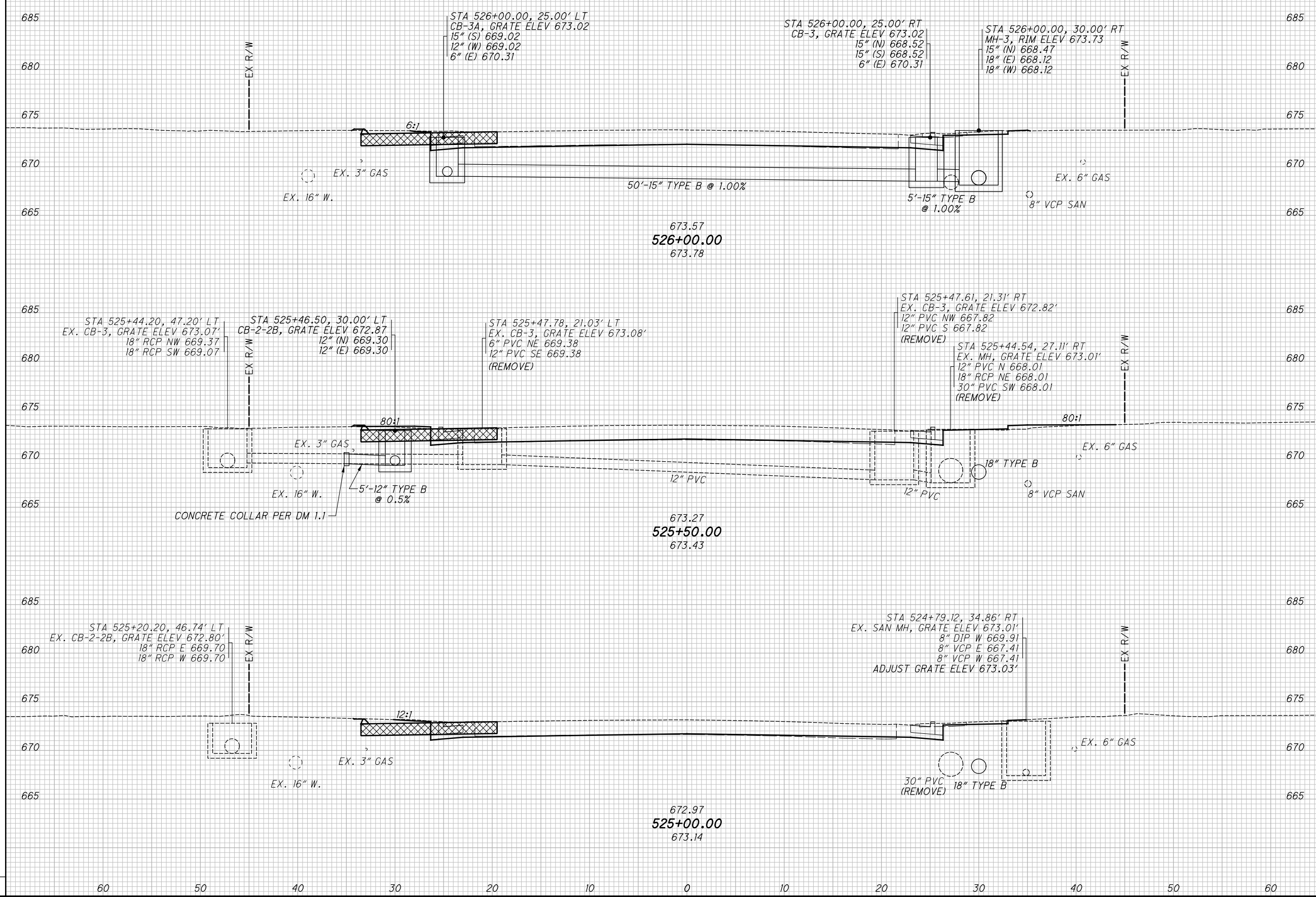
SEEDING
END WIDTH SO. YDS.
31
172
31
172
31
172
516

LEGEND
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
17	0			
28	3			
13	3			
27	3			
16	0			
31	0			
86	6			

CROSS SECTIONS - U.S. 20
STA. 525+00.00 TO STA. 526+00.00
LAK-US-20-24.99
PART 2

362
697



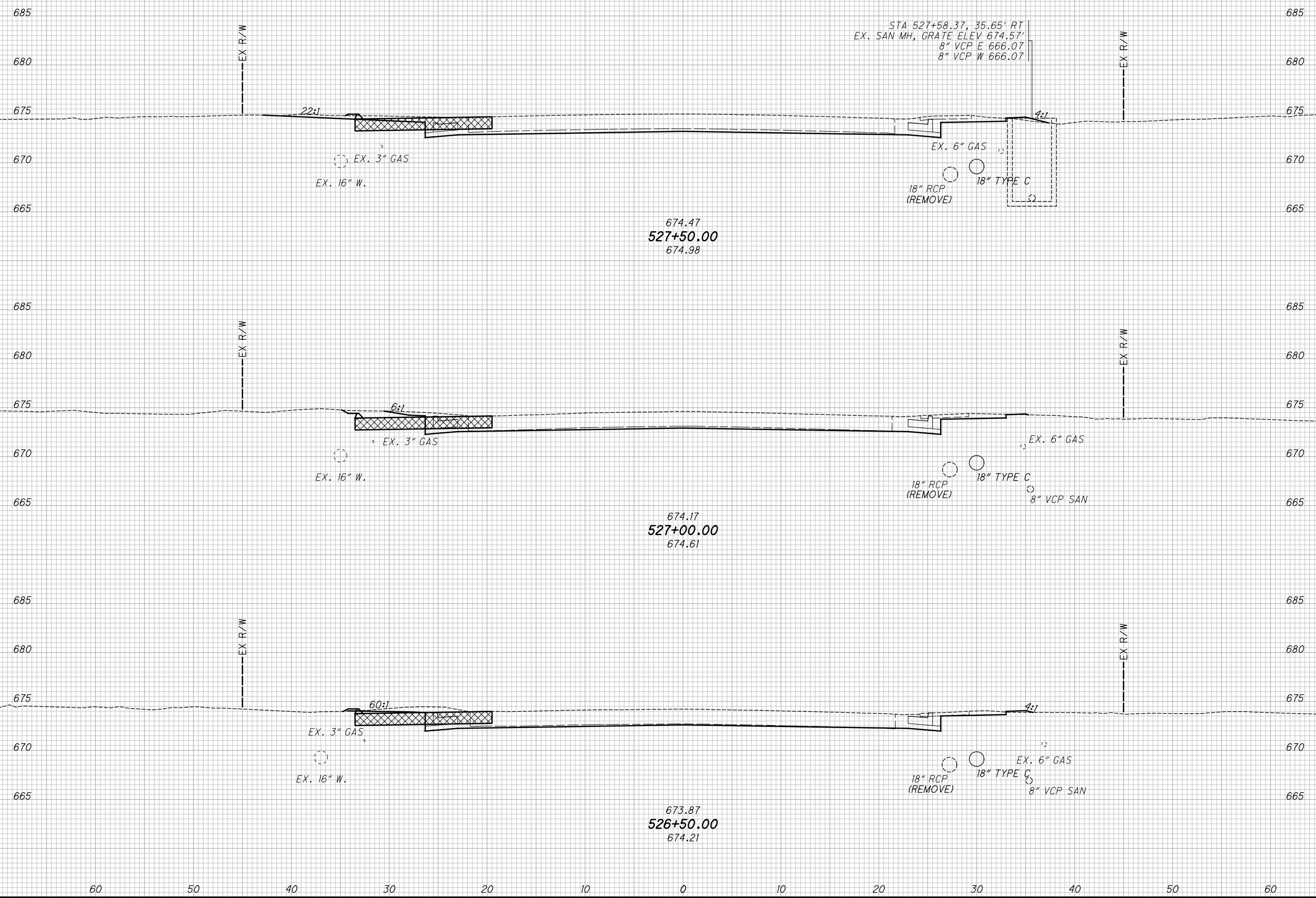
H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_19 7/15/2022 2:56:30 PM troyer

SEEDING	
END WIDTH	SO. YDS.
31	685
172	675
31	675
172	665
31	675
172	665
31	675
172	665
516	665

LEGEND

 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

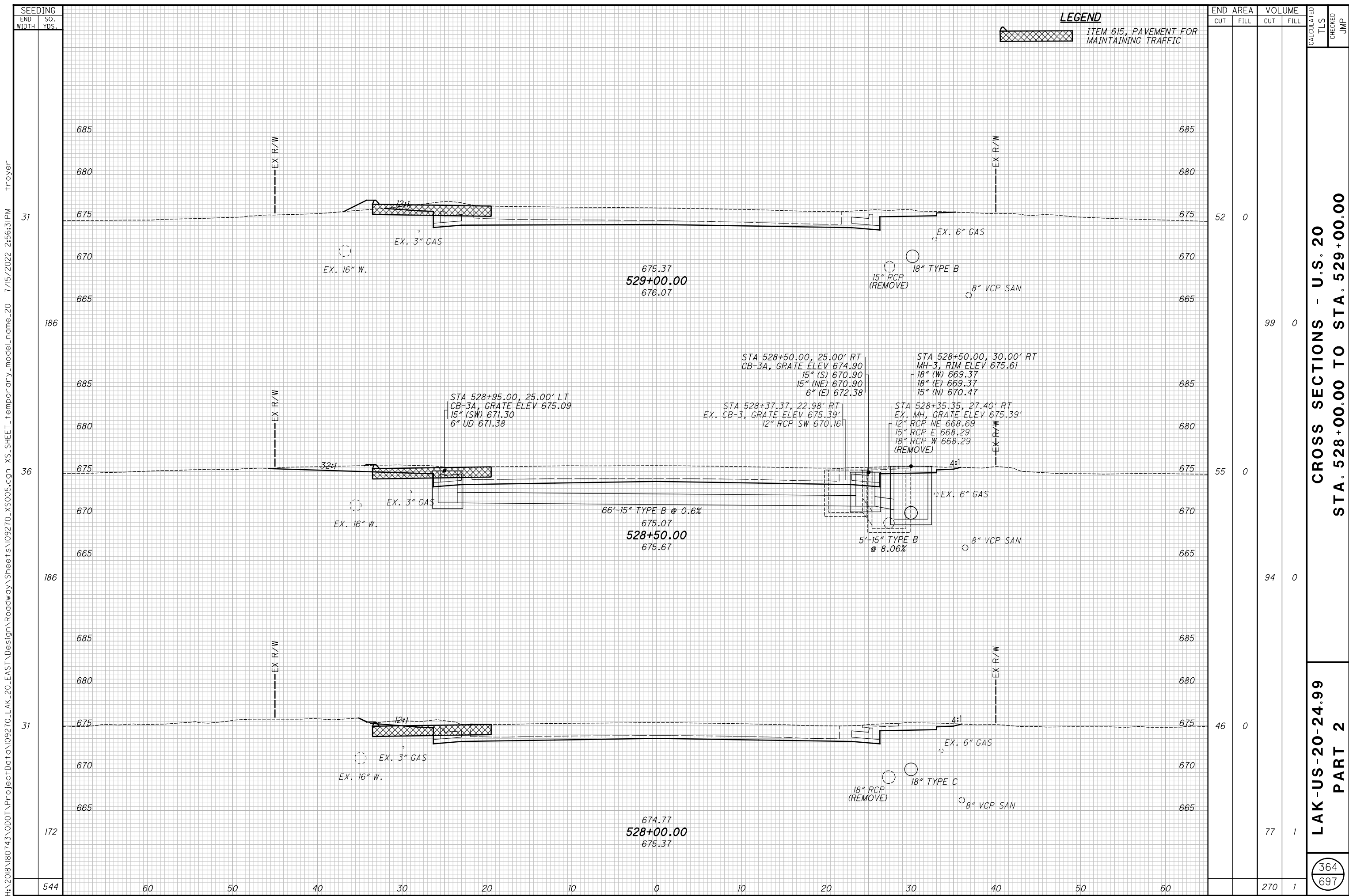
END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
37	1				
		58	1		
26	0				
		47	0		
25	0				
		39	0		
		144	1		



CROSS SECTIONS - U.S. 20
STA. 526+50.00 TO STA. 527+50.00

LAK-US-20-24.99
PART 2

363
697



SEEDING	
END WIDTH	SO. YDS.
31	60
186	50
36	40
186	30
31	20
172	10
544	0

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
52	0	99	0		
55	0	94	0		
46	0	77	1		
		270	1		

CROSS SECTIONS - U.S. 20
STA. 528+00.00 TO STA. 529+00.00

LAK-US-20-24.99
PART 2

364
 697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_20 7/15/2022 2:56:31PM Troyer

SEEDING
END WIDTH SO. YDS.
40
217
38
200
34
181
598

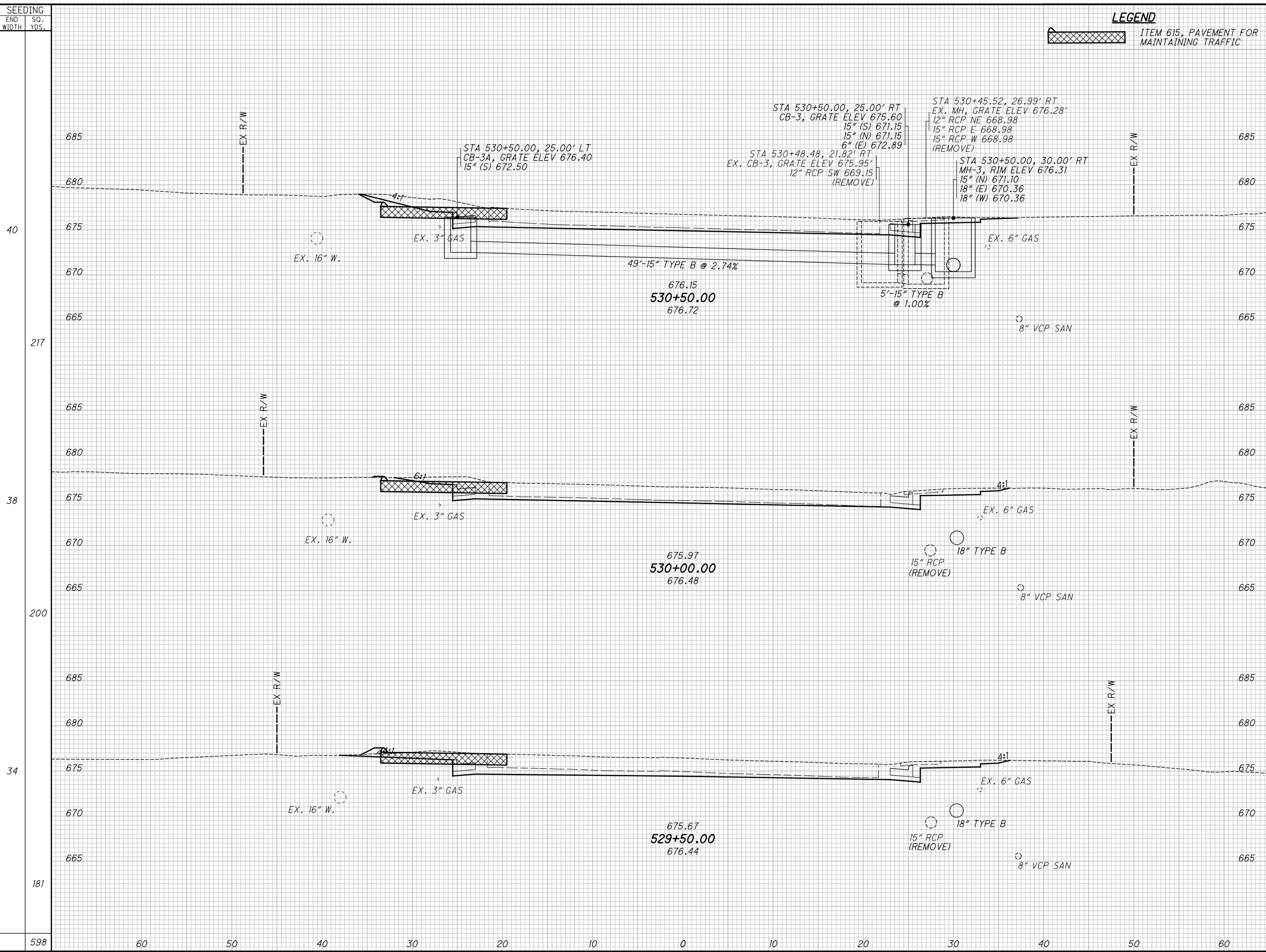
LEGEND
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
45	0	74	0		
35	0	78	0		
49	0	94	0		
		246	0		

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_21 7/15/2022 2:56:33 PM troyer

CROSS SECTIONS - U.S. 20
STA. 529+50.00 TO STA. 530+50.00
LAK-US-20-24.99
PART 2

365
697

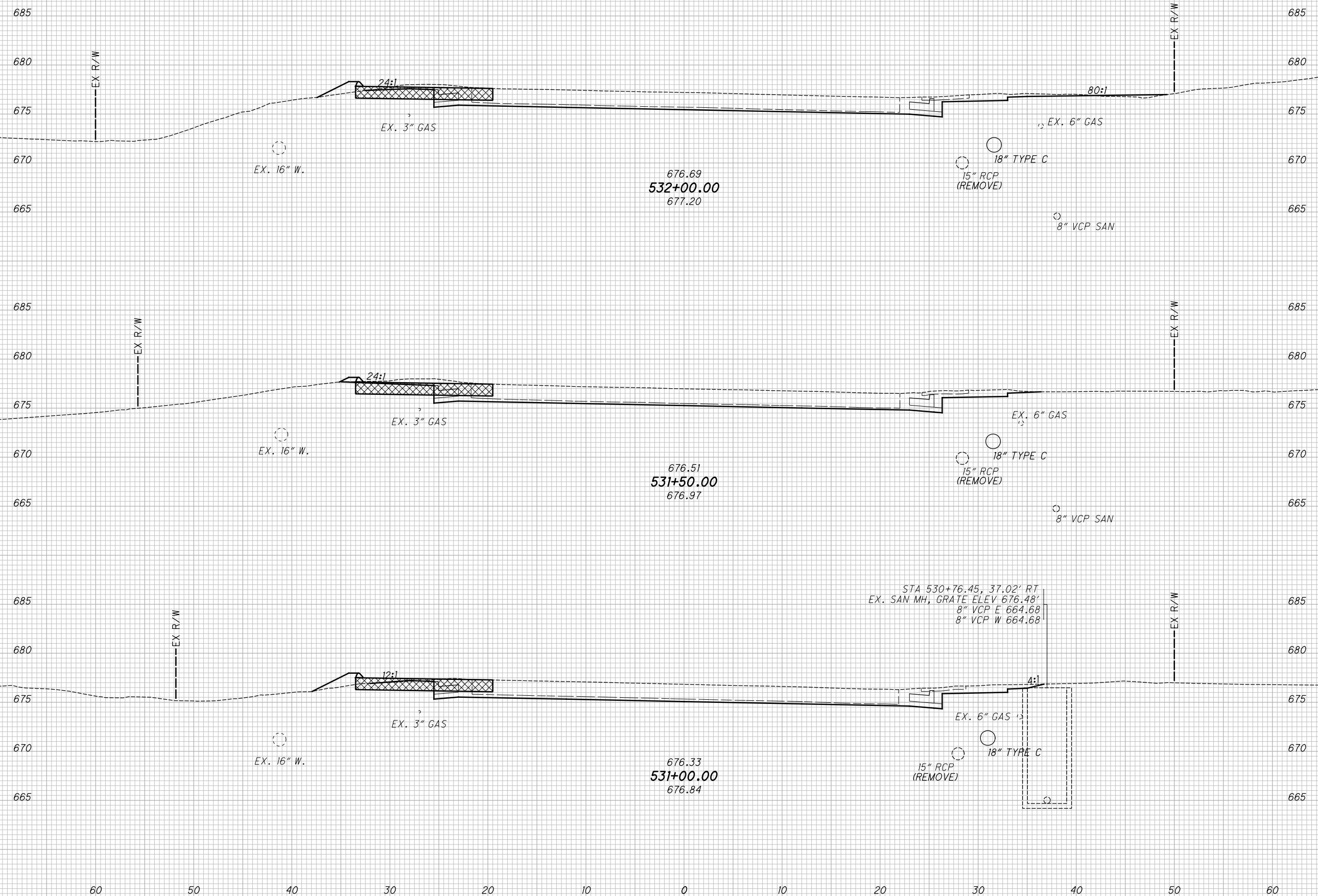


H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_22 7/15/2022 2:56:34 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
52		33	1				
275		33	0	61			
47		33	0				
250		31	0	59			
43		31	0				
231		70	0				
756		190	1				

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 531+00.00 TO STA. 532+00.00

LAK-US-20-24.99
PART 2

366
697

SEEDING
END WIDTH SO. YDS.
694 60 50 40 30 20 10 0 10 20 30 40 50 60
294 60 50 40 30 20 10 0 10 20 30 40 50 60
197 60 50 40 30 20 10 0 10 20 30 40 50 60
17 60 50 40 30 20 10 0 10 20 30 40 50 60
56 60 50 40 30 20 10 0 10 20 30 40 50 60
7/15/2022 2:56:35 PM troyer

LEGEND

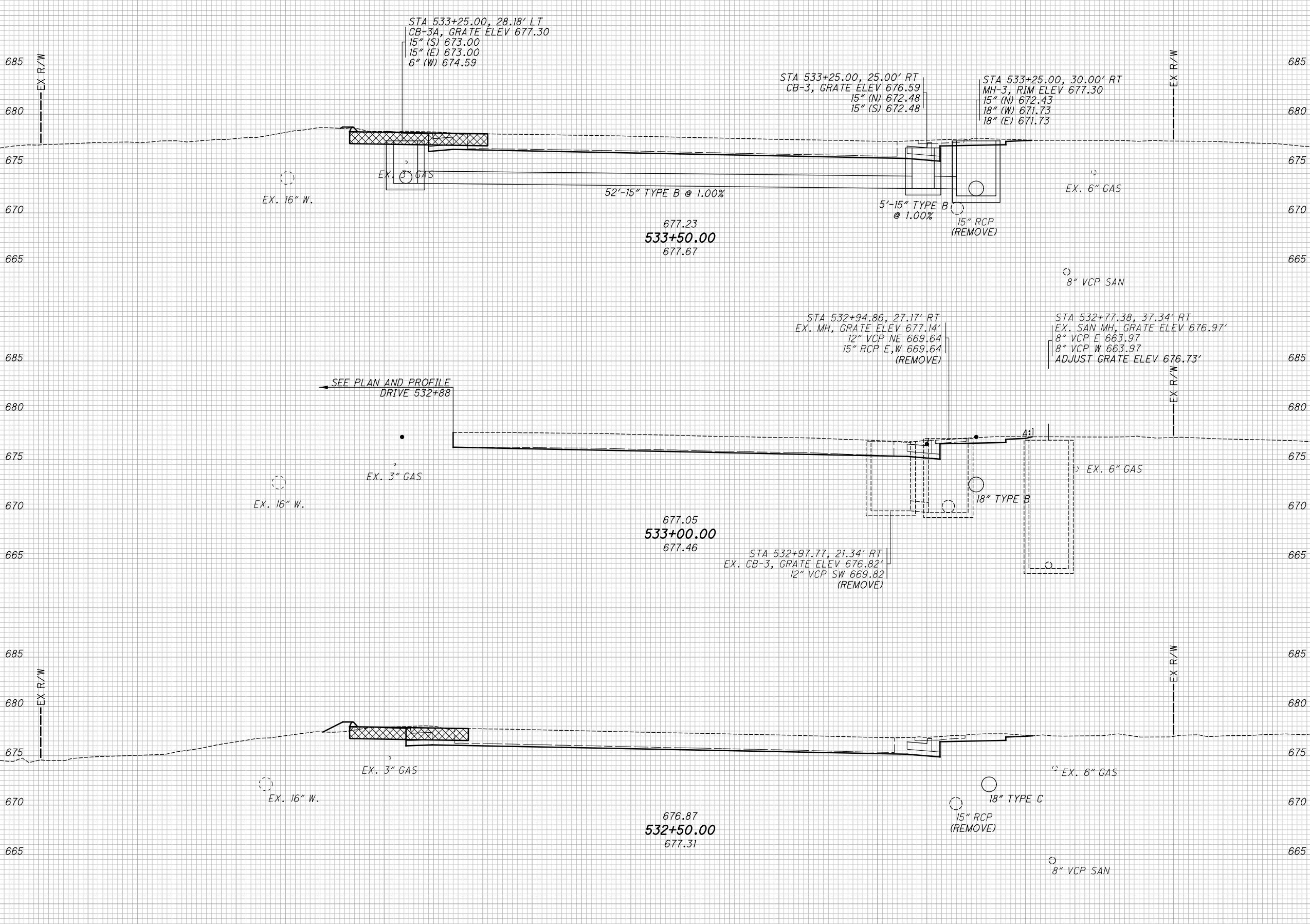
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
27	0			
16	0			
29	0			
57	1			
139	1			

**CROSS SECTIONS - U.S. 20
STA. 532+50.00 TO STA. 533+50.00**

**LAK-US-20-24.99
PART 2**

367
697



STA 533+25.00, 28.18' LT
CB-3A, GRATE ELEV 677.30
15" (S) 673.00
15" (E) 673.00
6" (W) 674.59

STA 533+25.00, 25.00' RT
CB-3, GRATE ELEV 676.59
15" (N) 672.48
15" (S) 672.48

STA 533+25.00, 30.00' RT
MH-3, RIM ELEV 677.30
15" (N) 672.43
18" (W) 671.73
18" (E) 671.73

STA 532+94.86, 27.17' RT
EX. MH, GRATE ELEV 677.14'
12" VCP NE 669.64
15" RCP E,W 669.64
(REMOVE)

STA 532+77.38, 37.34' RT
EX. SAN MH, GRATE ELEV 676.97'
8" VCP E 663.97
8" VCP W 663.97
ADJUST GRATE ELEV 676.73'

STA 532+97.77, 21.34' RT
EX. CB-3, GRATE ELEV 676.82'
12" VCP SW 669.82
(REMOVE)

677.23
533+50.00
677.67

677.05
533+00.00
677.46

676.87
532+50.00
677.31

SEE PLAN AND PROFILE
DRIVE 532+88

EX R/W

EX R/W

EX R/W

EX R/W

EX. 16" W.

EX. 6" GAS

52'-15" TYPE B @ 1.00%

5'-15" TYPE B @ 1.00%
15" RCP (REMOVE)

EX. 6" GAS

8" VCP SAN

EX. 16" W.

EX. 3" GAS

18" TYPE B

EX. 6" GAS

EX R/W

EX R/W

EX. 16" W.

EX. 3" GAS

15" RCP (REMOVE)
18" TYPE C

EX. 6" GAS

8" VCP SAN

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_24 7/15/2022 2:56:37 PM troyer

SEEDING	END SO.	
	WIDTH	YDS.
	47	286
	56	311
	56	311
	908	

LEGEND

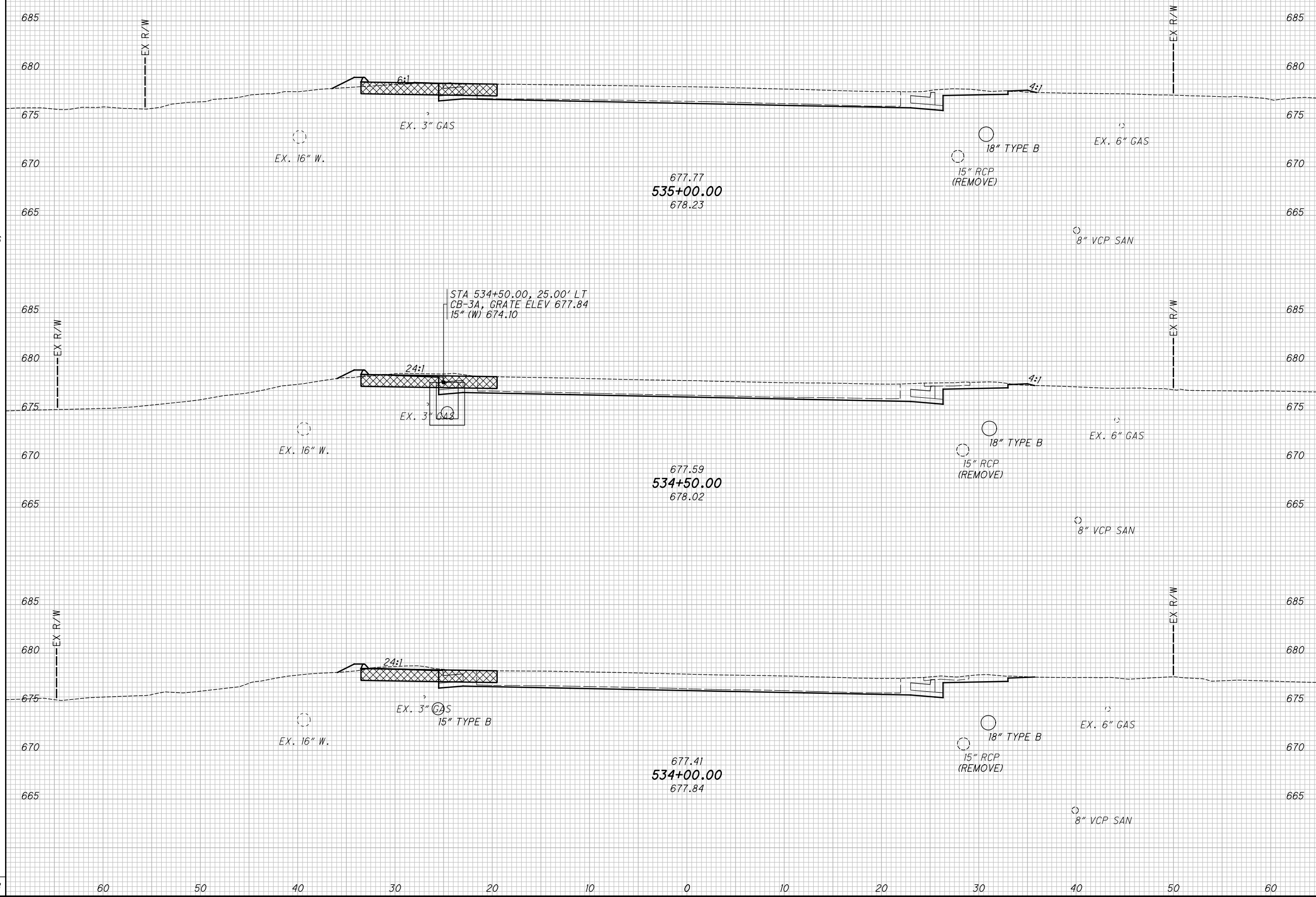
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED
25	0		
29	0		
28	0		
	154	0	

CROSS SECTIONS - U.S. 20
STA. 534+00.00 TO STA. 535+00.00

LAK-US-20-24.99
PART 2

368
697



STA 534+50.00, 25.00' LT
 CB-3A, GRATE ELEV 677.84
 15" (W) 674.10

677.77
535+00.00
 678.23

677.59
534+50.00
 678.02

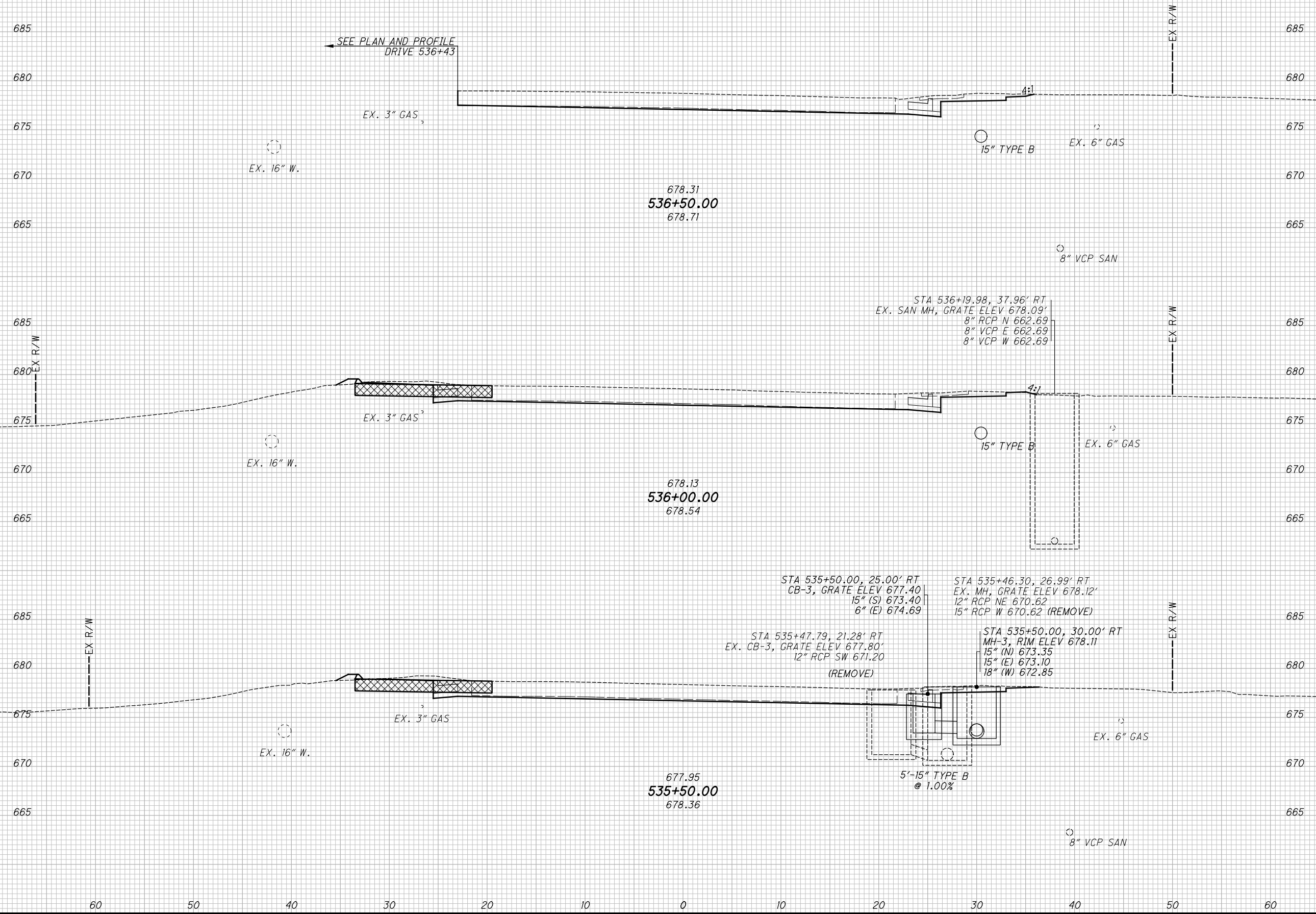
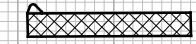
677.41
534+00.00
 677.84

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_25 7/15/2022 2:56:38 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
17		16	0				
208		24	0	37			
58		47	0				
306		27	0				
52		27	0				
275		48	0				
789		132	0				

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 535+50.00 TO STA. 536+50.00

LAK-US-20-24.99
PART 2

369
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_26 7/15/2022 2:56:39 PM troyer

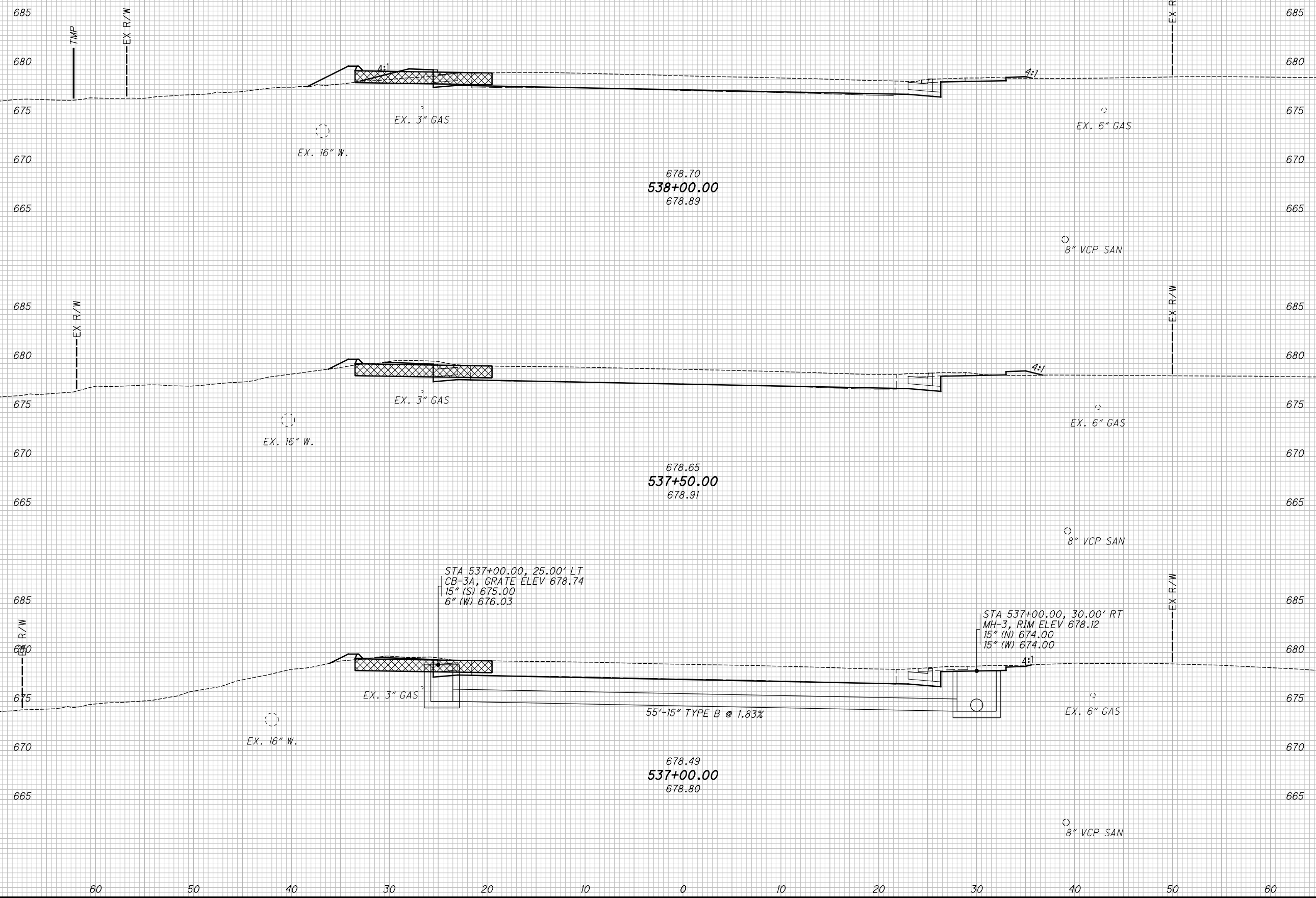
SEEDING	END	
	WIDTH	SO. YDS.
	60	54
	50	297
	40	53
	30	311
	20	59
	10	211
	0	819

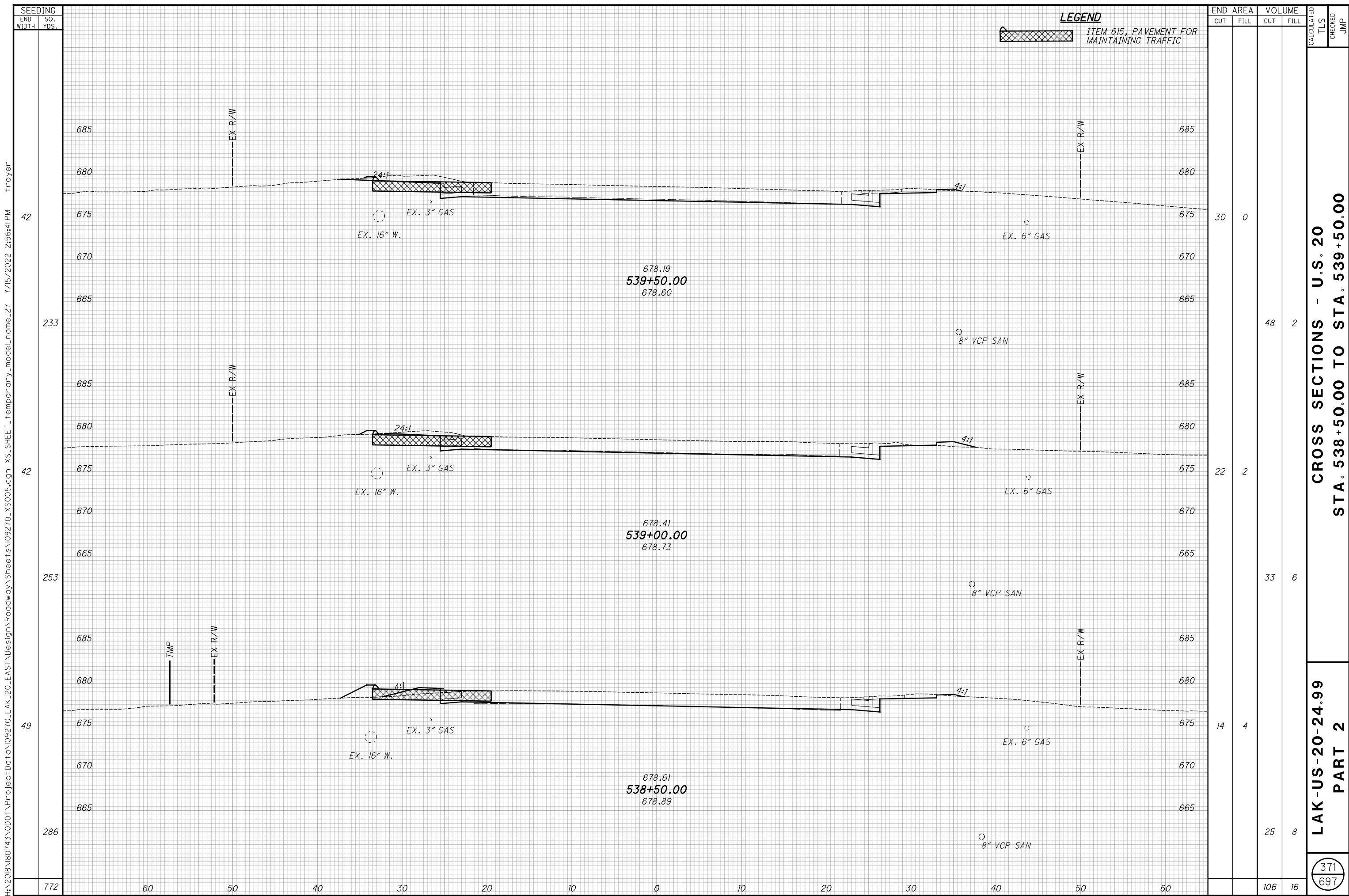
LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL			
	13	5			
			26	6	
	15	1			
			31	1	
	19	0			
			32	0	
			89	7	

LAK-US-20-24.99
PART 2
CROSS SECTIONS - U.S. 20
STA. 537+00.00 TO STA. 538+00.00

370
697





SEEDING	END	
	WIDTH	SO. YDS.
	42	233
	42	253
	49	286
	772	

LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED
30	0		
22	2	48	2
14	4	33	6
		25	8
		106	16

LAK-US-20-24.99
CROSS SECTIONS - U.S. 20
PART 2
STA. 538+50.00 TO STA. 539+50.00

371
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_27 7/15/2022 2:56:41PM Troyer

SEEDING
END SO.
WIDTH YDS.
42
233
42
233
42
233
699

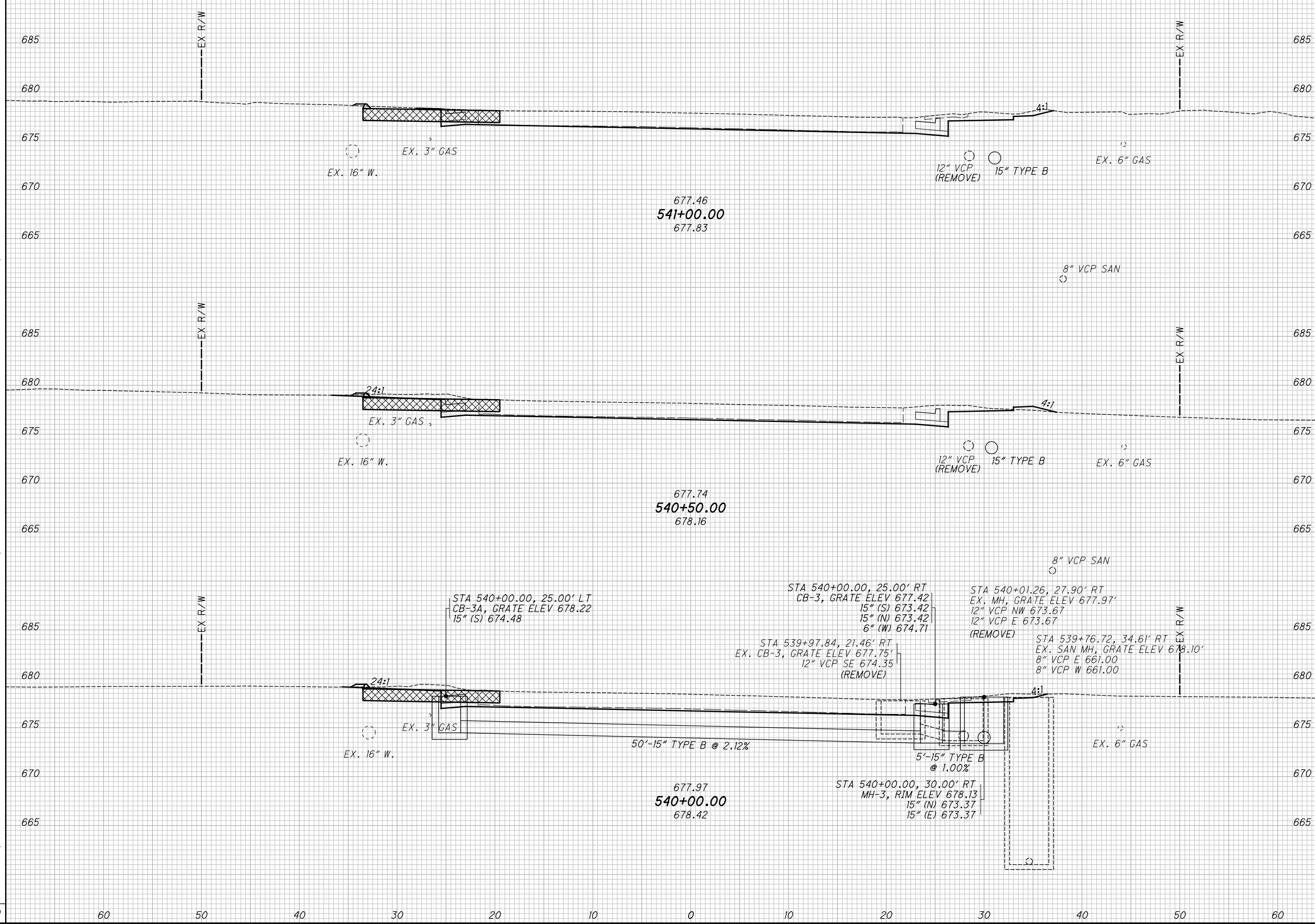
LEGEND
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
23	0	47	1		
28	1	52	1		
28	0	54	0		
		153	2		

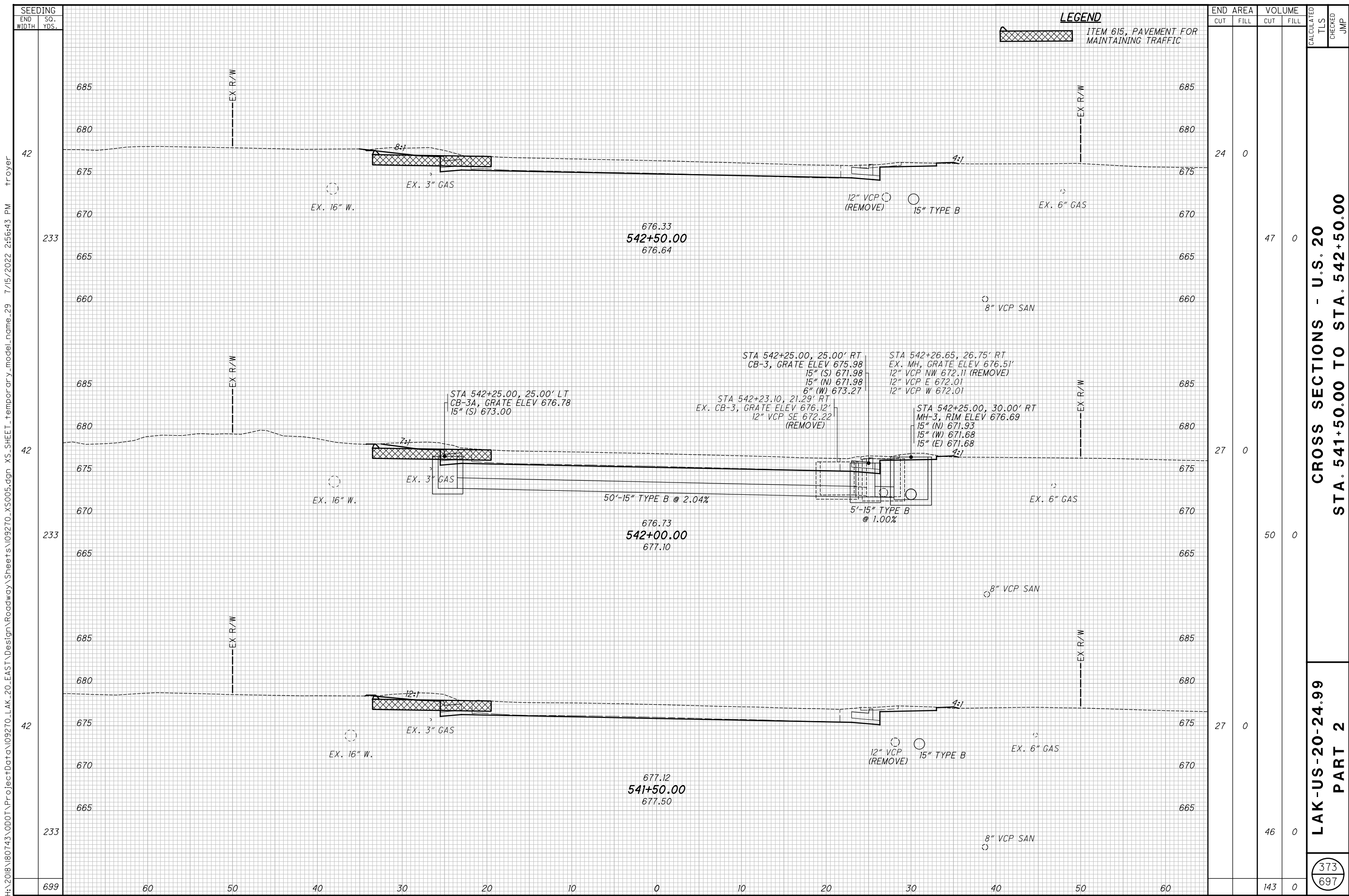
CROSS SECTIONS - U.S. 20
STA. 540+00.00 TO STA. 541+00.00

LAK-US-20-24.99
PART 2

372
697



H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_28 7/15/2022 2:56:42 PM troyer



LEGEND
 [Hatched Box] ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED		
				CUT	FILL
24	0				
27	0				
27	0				
143	0				

CROSS SECTIONS - U.S. 20
STA. 541+50.00 TO STA. 542+50.00

LAK-US-20-24.99
PART 2

373
697

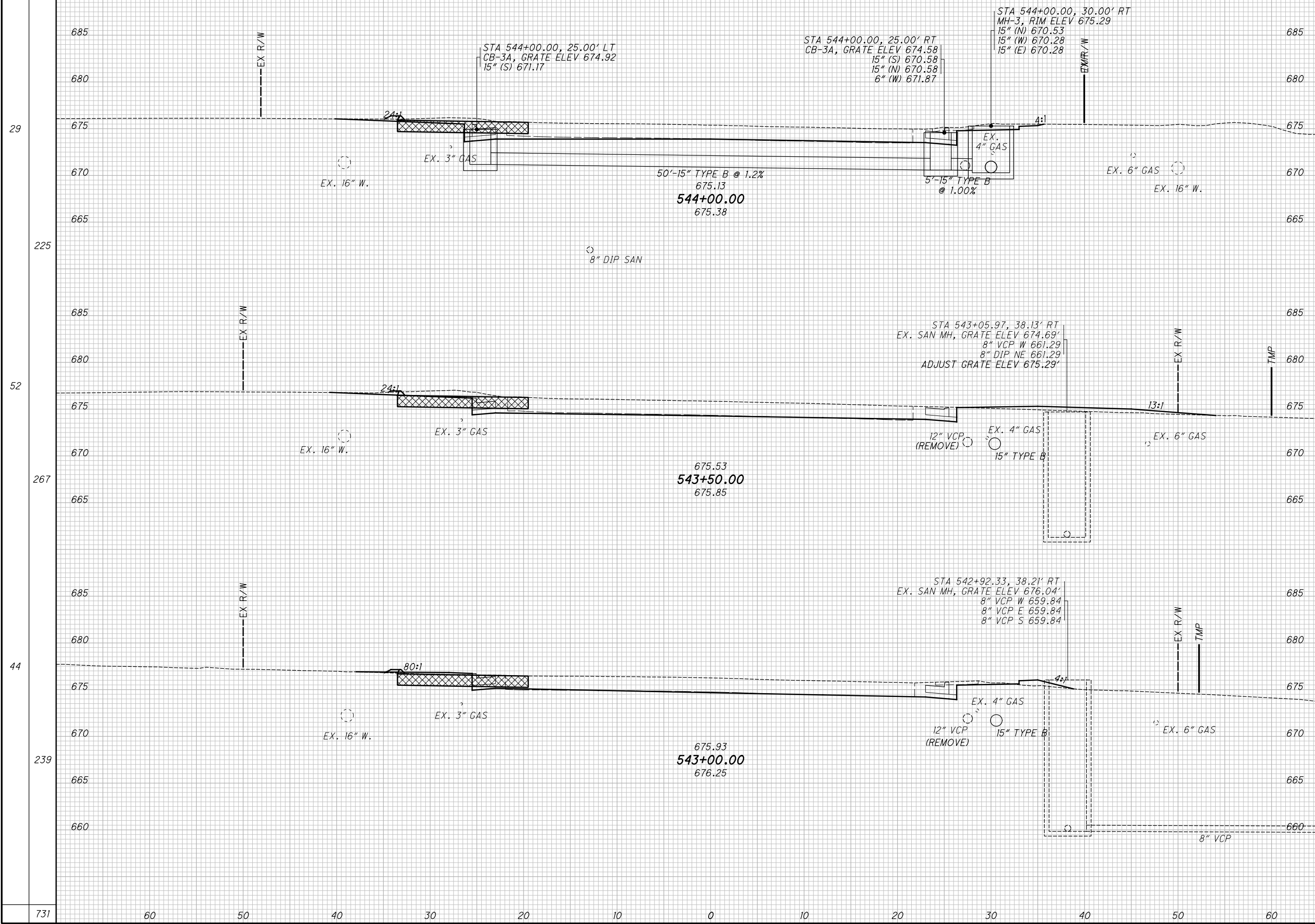
H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_29 7/15/2022 2:56:43 PM troyer

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST_Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_30 7/15/2022 2:56:44 PM Troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
END WIDTH							
SO. YDS.							
29		28	0				
225			47	6			
52		23	7				
267			34	10			
44		14	4				
239			35	4			
731			116	20			

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 543+00.00 TO STA. 544+00.00

LAK-US-20-24.99
PART 2

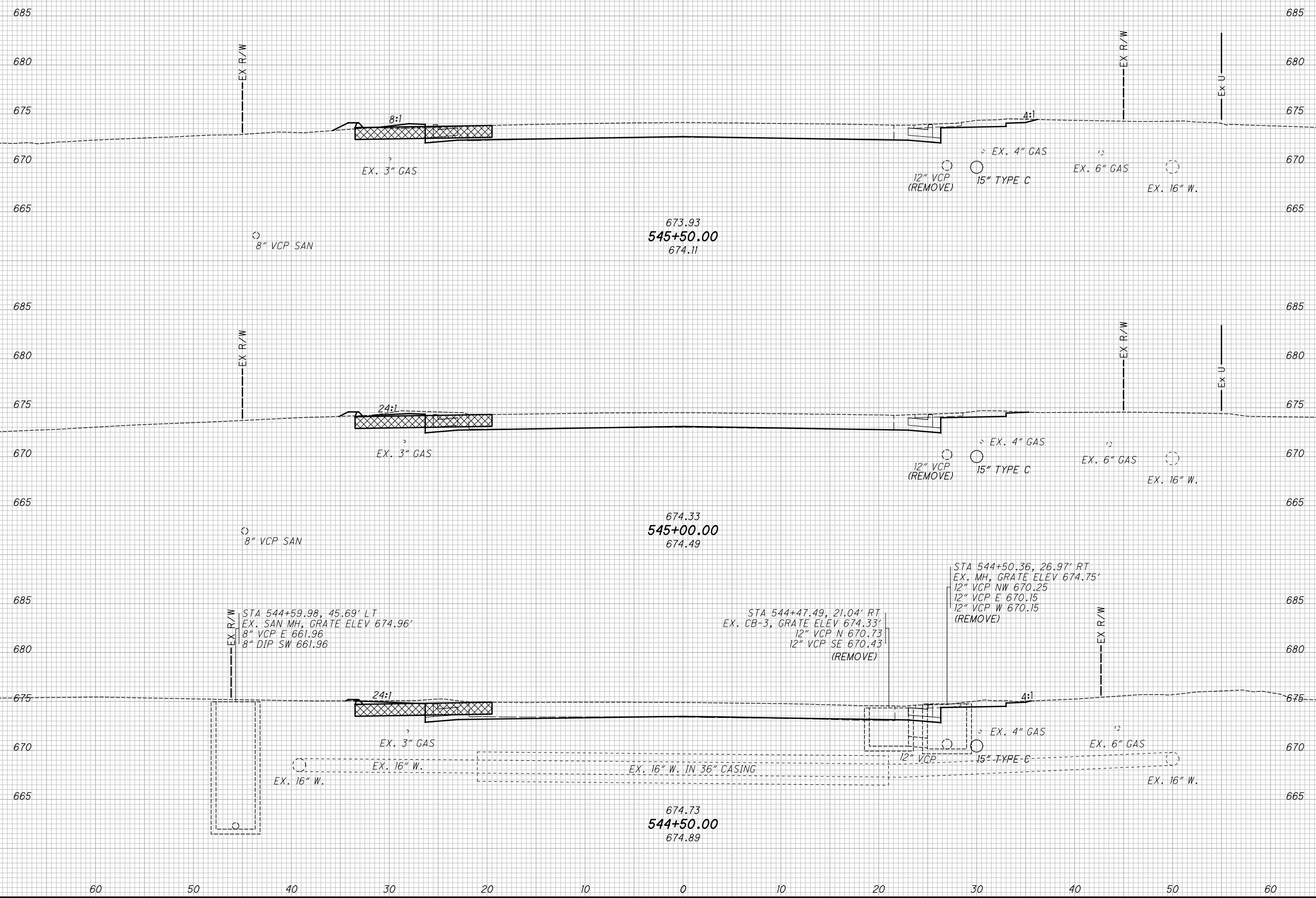
374
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_31 7/15/2022 2:56:46 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
31	18	1					
172			35	1			
31	20	0					
169			39	0			
30	22	0					
164			46	0			
505			120	1			

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 544+50.00 TO STA. 545+50.00

LAK-US-20-24.99
PART 2

375
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_32 7/15/2022 2:56:47 PM troyer

SEEDING	
END WIDTH	SO. YDS.
31	172
31	172
31	172
516	

LEGEND

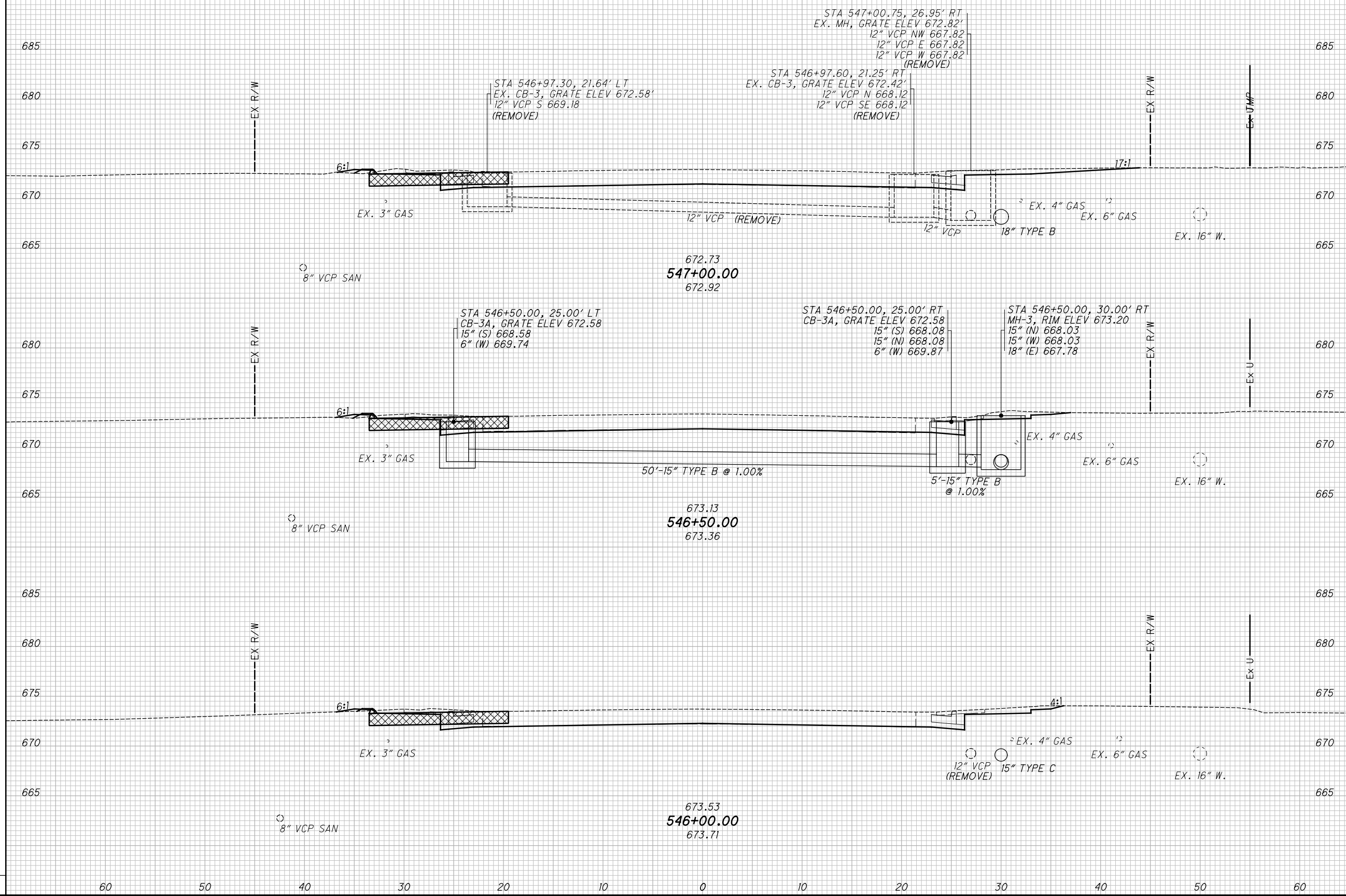
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
23	1	41	2		
21	1	38	2		
20	1	35	2		
		114	6		

**CROSS SECTIONS - U.S. 20
STA. 546+00.00 TO STA. 547+00.00**

**LAK-US-20-24.99
PART 2**

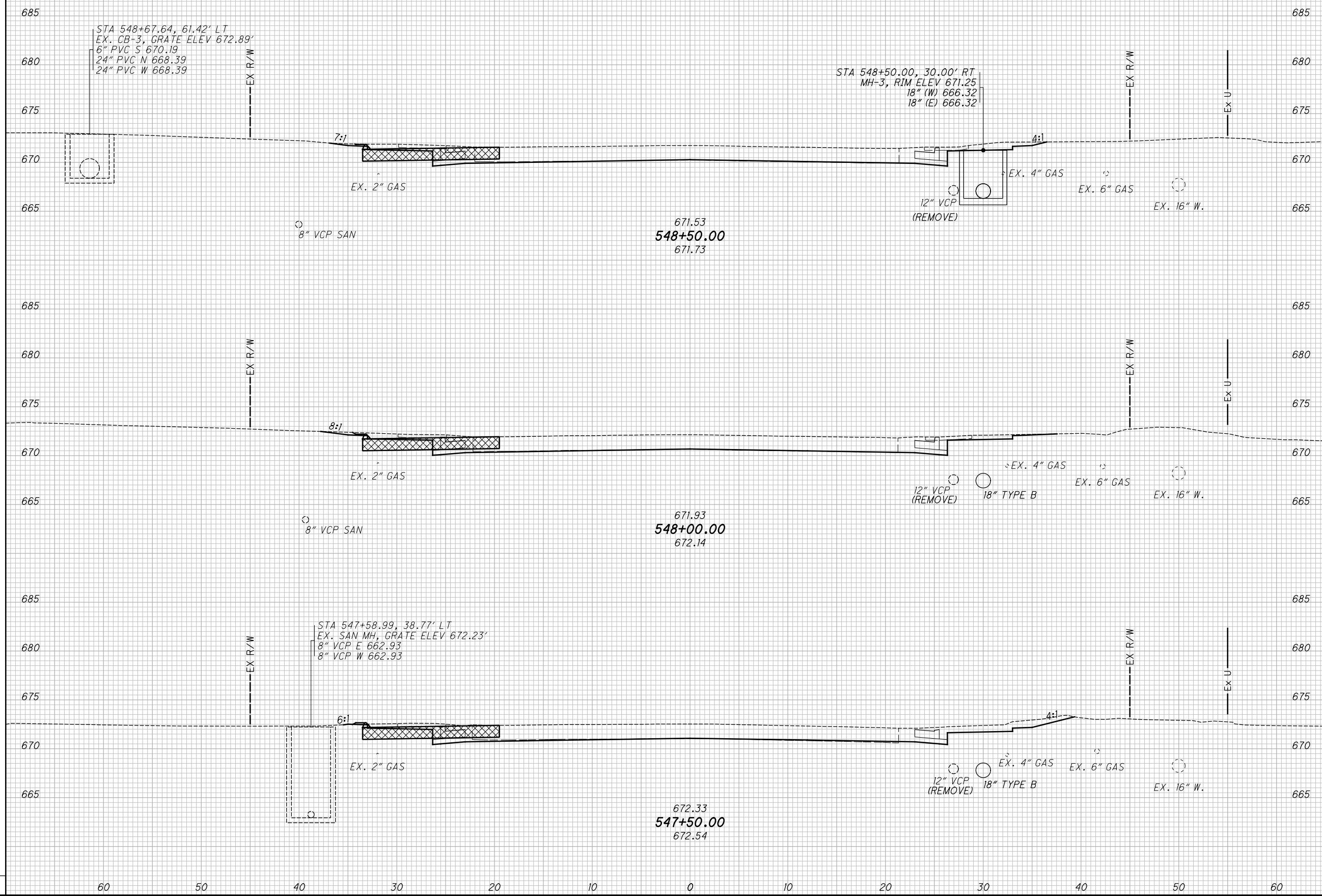
376
697



H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_33 7/15/2022 2:56:48 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
31	24	0	42	0			
172	21	0	44	0			
31	27	0	46	1			
172			132	1			
516							

LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



STA 548+67.64, 61.42' LT
 EX. CB-3, GRATE ELEV 672.89'
 6" PVC S 670.19
 24" PVC N 668.39
 24" PVC W 668.39

STA 548+50.00, 30.00' RT
 MH-3, RIM ELEV 671.25
 18" (W) 666.32
 18" (E) 666.32

STA 547+58.99, 38.77' LT
 EX. SAN MH, GRATE ELEV 672.23'
 8" VCP E 662.93
 8" VCP W 662.93

CROSS SECTIONS - U.S. 20
STA. 547+50.00 TO STA. 548+50.00

LAK-US-20-24.99
PART 2

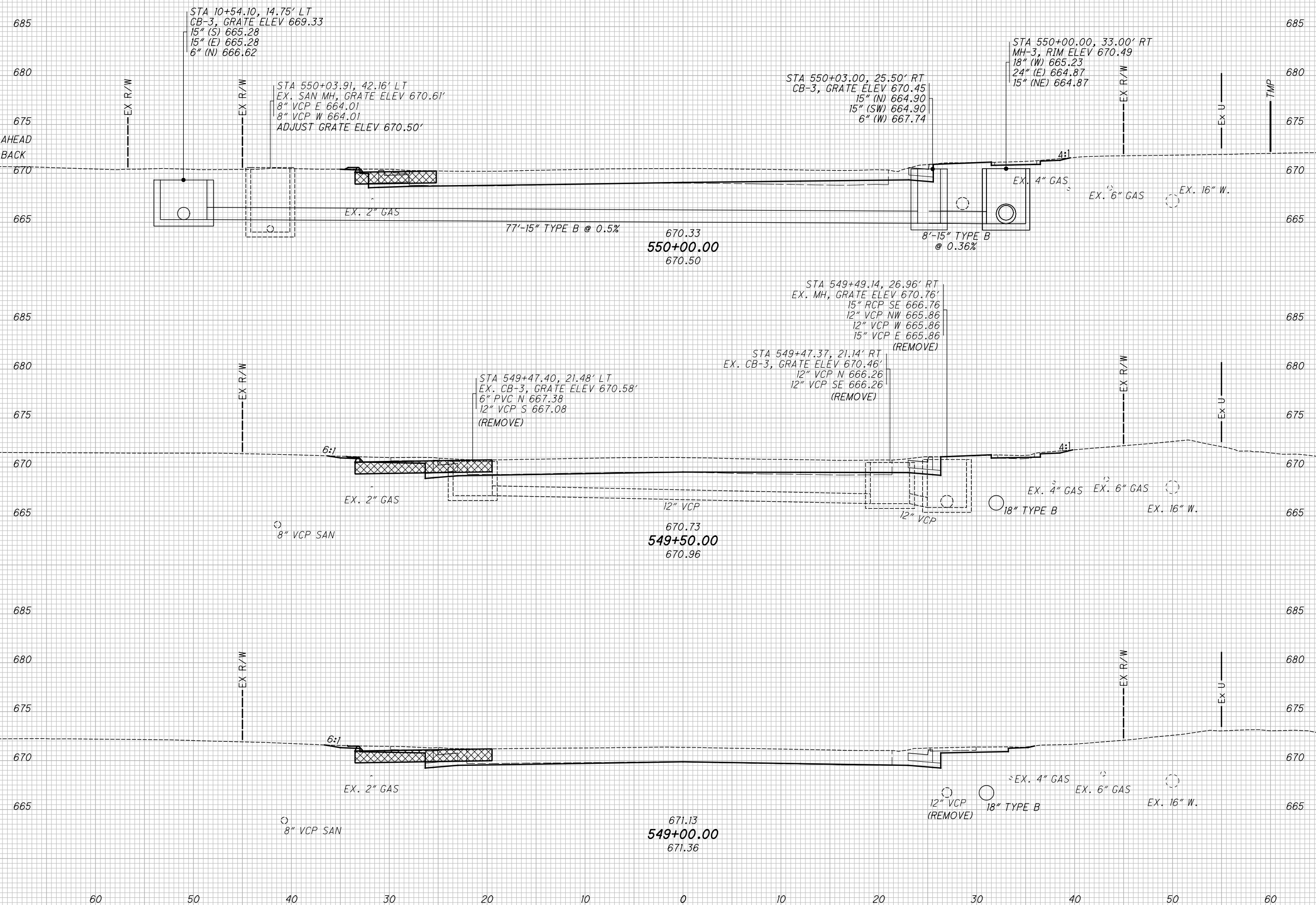
377
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_34 7/15/2022 2:56:49 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	END WIDTH	SO. YDS.	CUT	FILL			
	33	21	14	0			
	139				32	0	
	29		21	0			
	167				42	0	
	31		24	0			
	172				44	0	
	478				118	0	

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 549+00.00 TO STA. 550+00.00

LAK-US-20-24.99
PART 2

378
697



SEEDING	
END WIDTH	SO. YDS.
603	189
60	35
60	203
60	211
60	38

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
40	0	62	0		
27	0	48	1		
25	1	36	1		
		146	2		

CROSS SECTIONS - U.S. 20
STA. 550+50.00 TO STA. 551+50.00

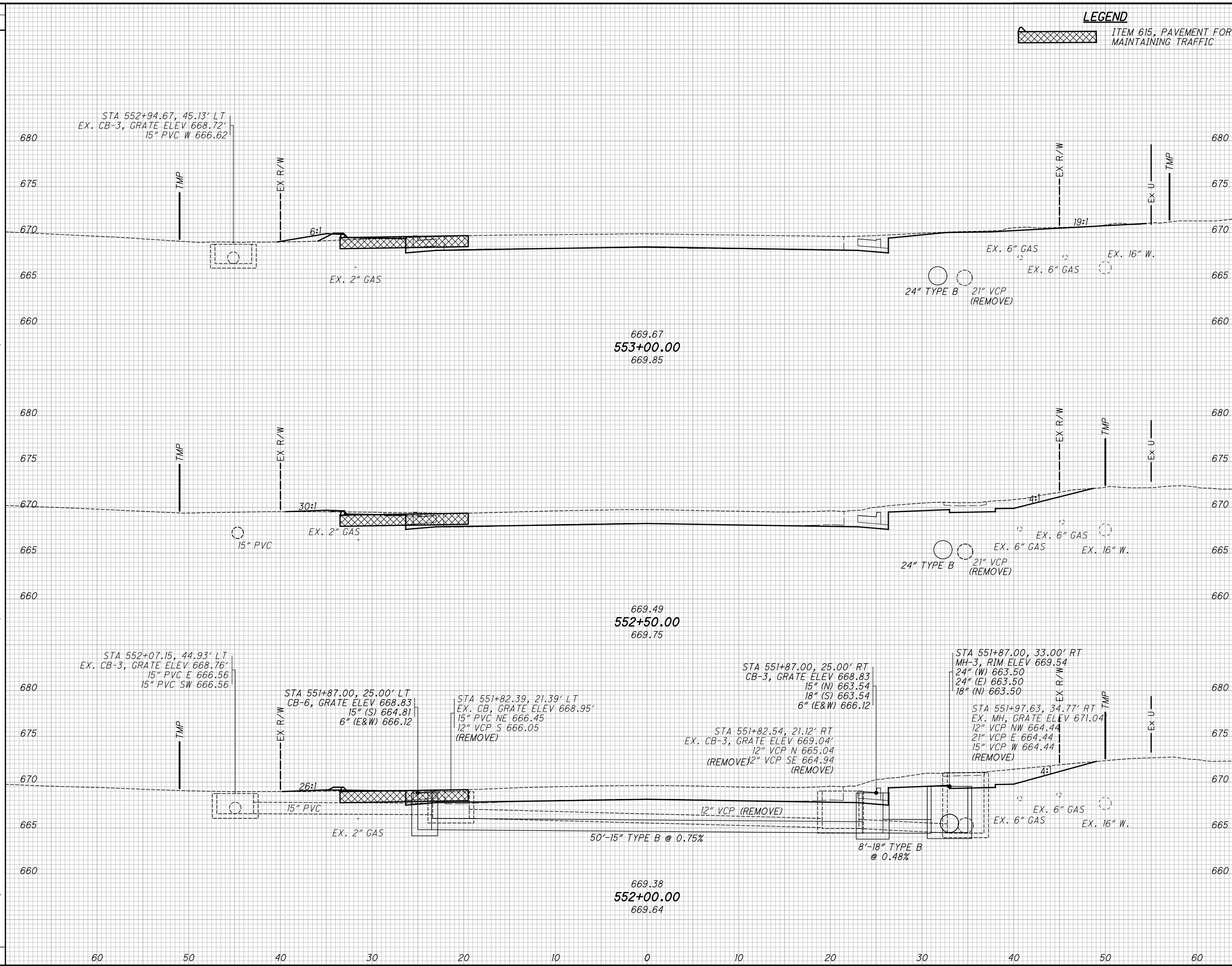
LAK-US-20-24.99
PART 2

379
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_35 7/15/2022 2:56:51 PM troyer

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_36 7/15/2022 2:56:52 PM troyer

SEEDING	
END WIDTH	SO. YDS.
51	19
258	49
42	34
233	73
42	45
222	79
713	201



LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
19	4				
49	5				
34	1				
73	2				
45	1				
79	1				
		201	8		

LAK-US-20-24.99
CROSS SECTIONS - U.S. 20
PART 2
STA. 552+00.00 TO STA. 553+00.00

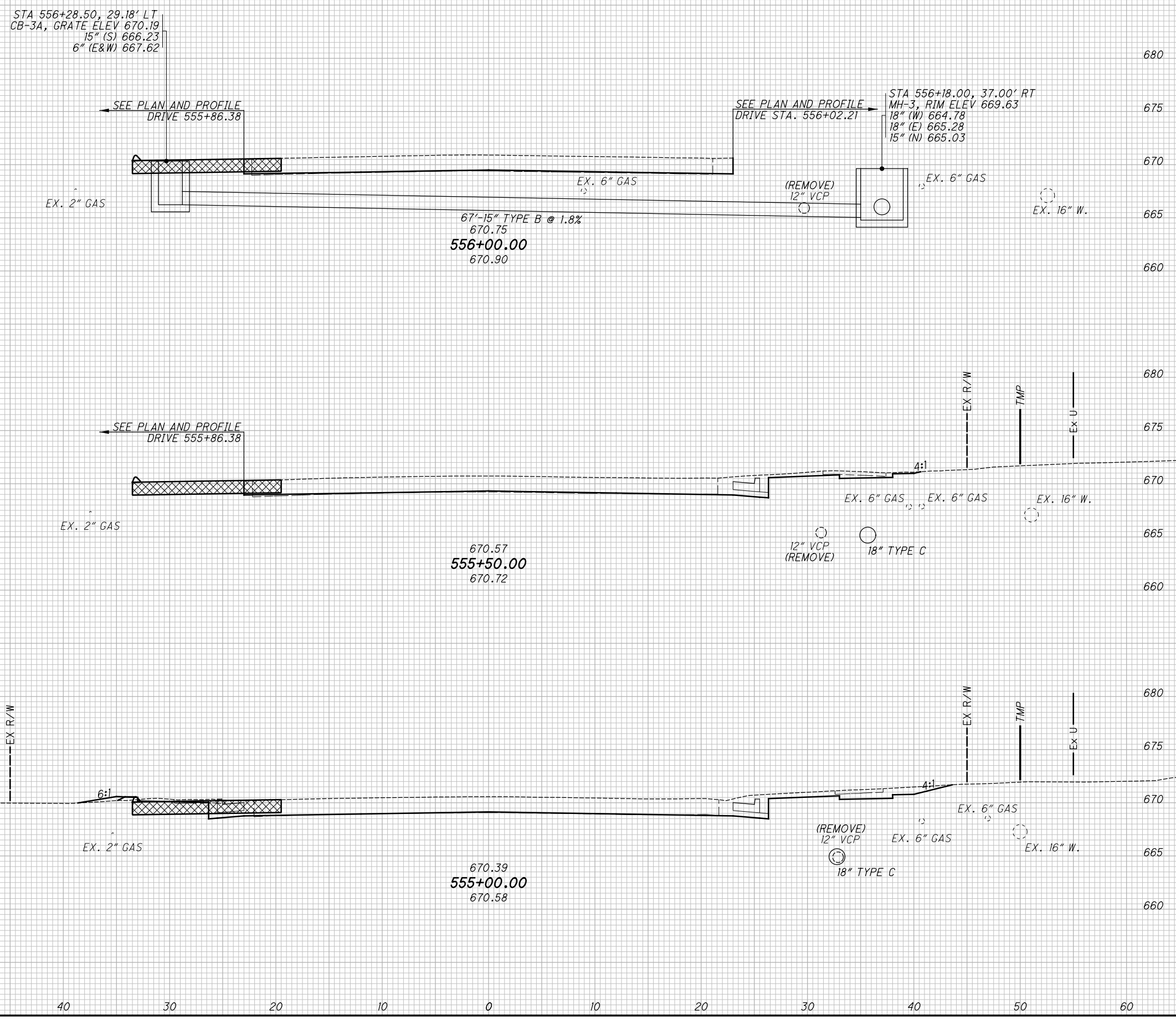
380
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_38 7/15/2022 2:56:55 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
	END WIDTH	SO. YDS.	CUT	FILL		
0			4	0		
53				16	0	
19			13	0		
156				34	1	
37			24	1		
206				48	2	
415	60			98	3	

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

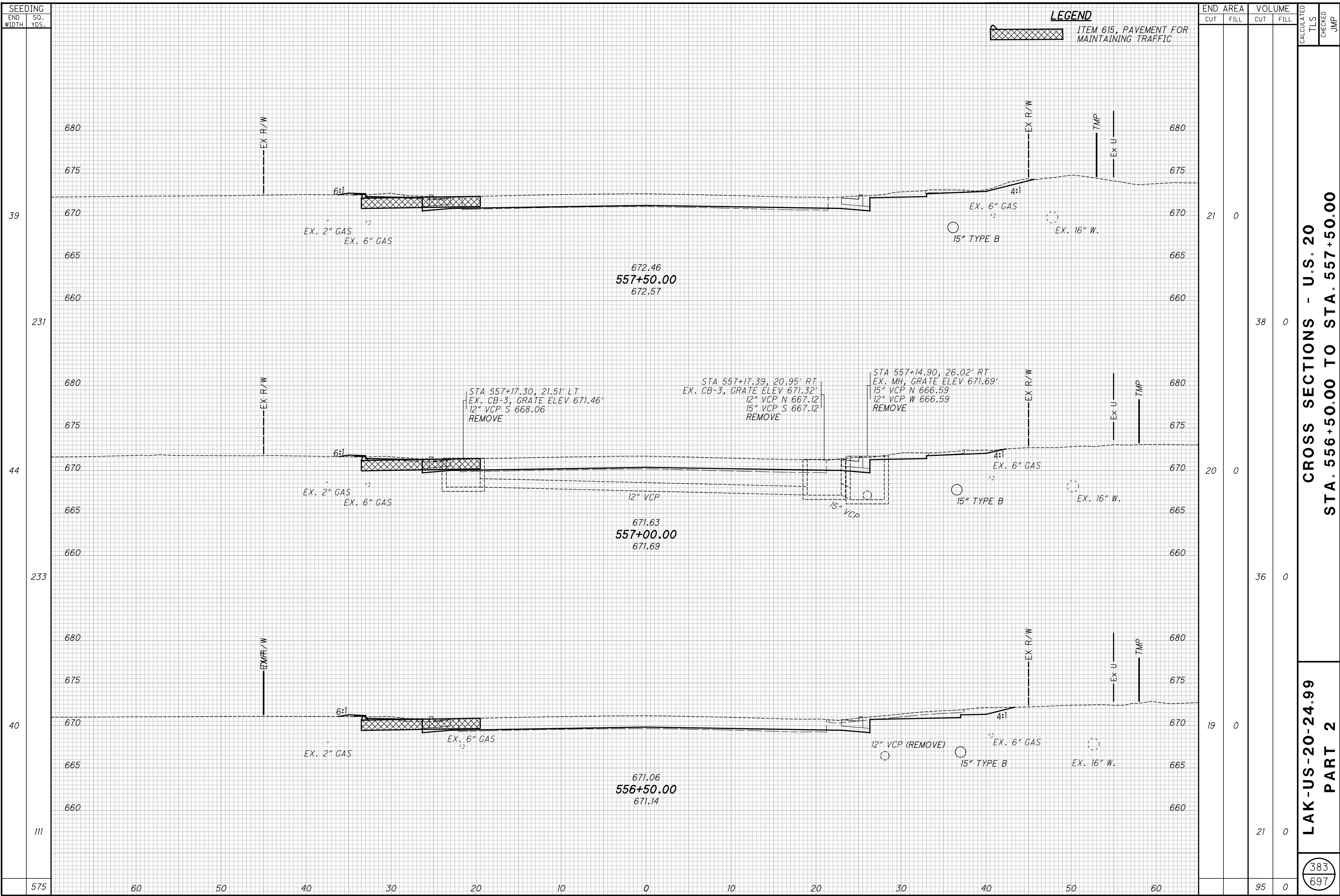


CROSS SECTIONS - U.S. 20
STA. 555+00.00 TO STA. 556+00.00

LAK-US-20-24.99
PART 2

382
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_39 7/15/2022 2:56:56 PM troyer



LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED	JMP
21	0			
20	0			
19	0			
95	0			

CROSS SECTIONS - U.S. 20
STA. 556+50.00 TO STA. 557+50.00

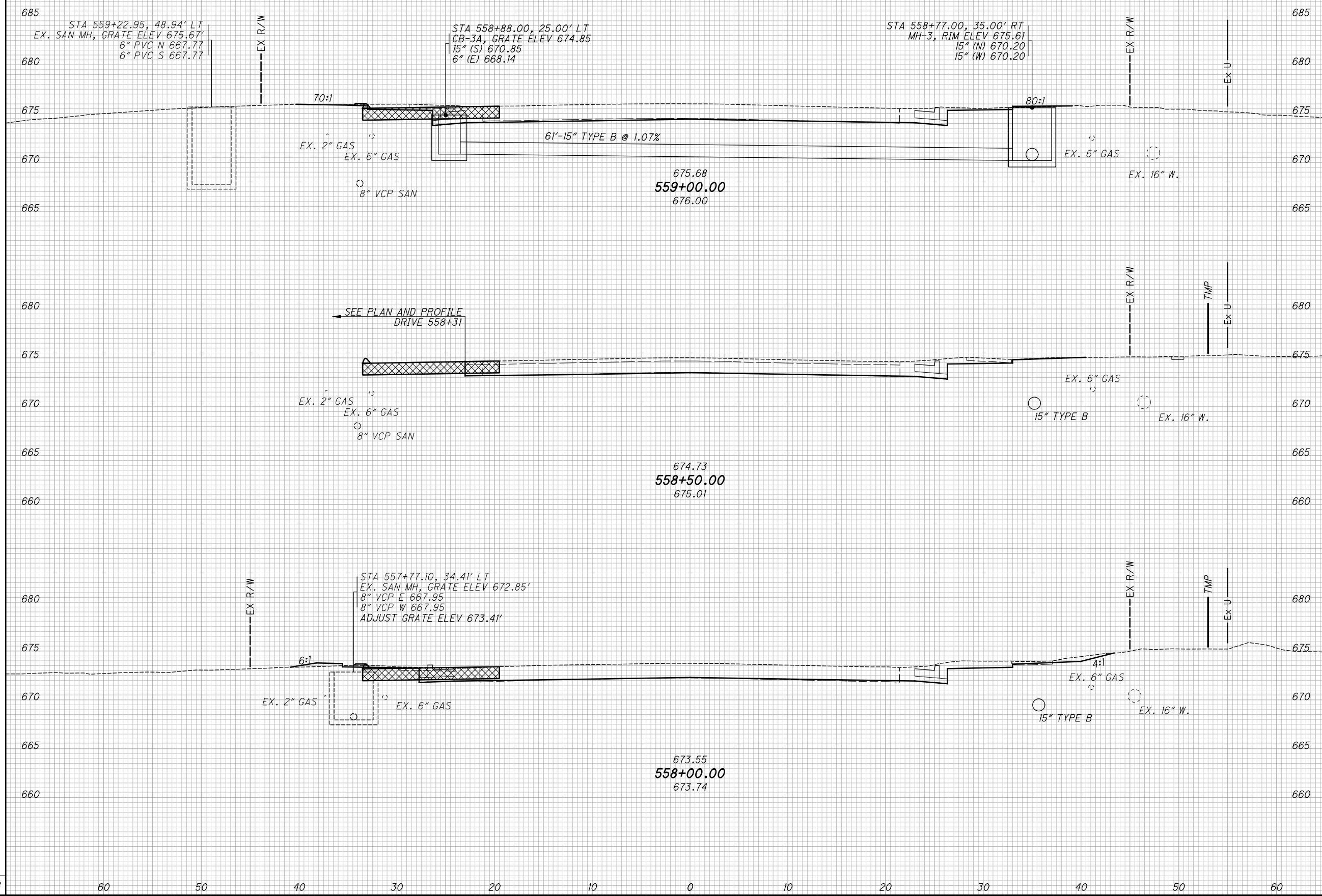
LAK-US-20-24.99
PART 2

383
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_40 7/15/2022 2:56:57 PM Troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	END WIDTH	SO. YDS.	CUT	FILL			
	60	50	25	0			
	139			36	0		
	20		14	0			
	158			34	1		
	37		23	1			
	211			41	1		
508				111	2		

LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



LAK-US-20-24.99
PART 2
CROSS SECTIONS - U.S. 20
STA. 558+00.00 TO STA. 559+00.00

384
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_41 7/15/2022 2:56:58 PM troyer

SEEDING	END	
	WIDTH	SO. YDS.
	492	
	161	
	28	
	164	
	31	
	167	
	29	

LEGEND

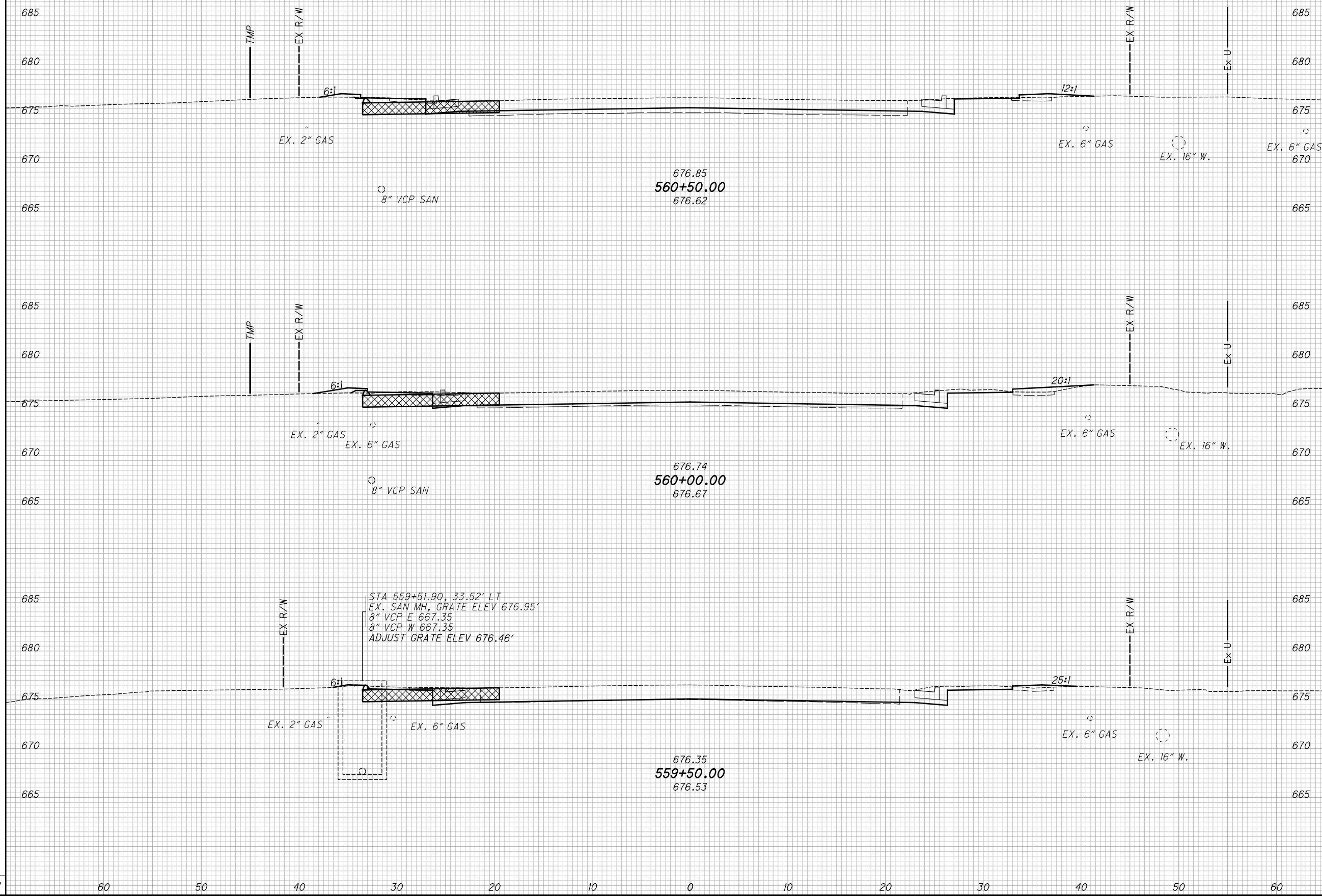
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED		CHECKED	JMP
		CUT	FILL		
12	3				
15	5				
18	1				
96	14				

**CROSS SECTIONS - U.S. 20
STA. 559+50.00 TO STA. 560+50.00**

**LAK-US-20-24.99
PART 2**

385
697



STA 559+51.90, 33.52' LT
EX. SAN MH, GRATE ELEV 676.95'
8" VCP E 667.35
8" VCP W 667.35
ADJUST GRATE ELEV 676.46'

676.85
560+50.00
676.62

676.74
560+00.00
676.67

676.35
559+50.00
676.53

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_42 7/15/2022 2:57:00 PM Troyer

SEEDING	
END WIDTH	SO. YDS.
24	136
25	133
23	144
413	

LEGEND

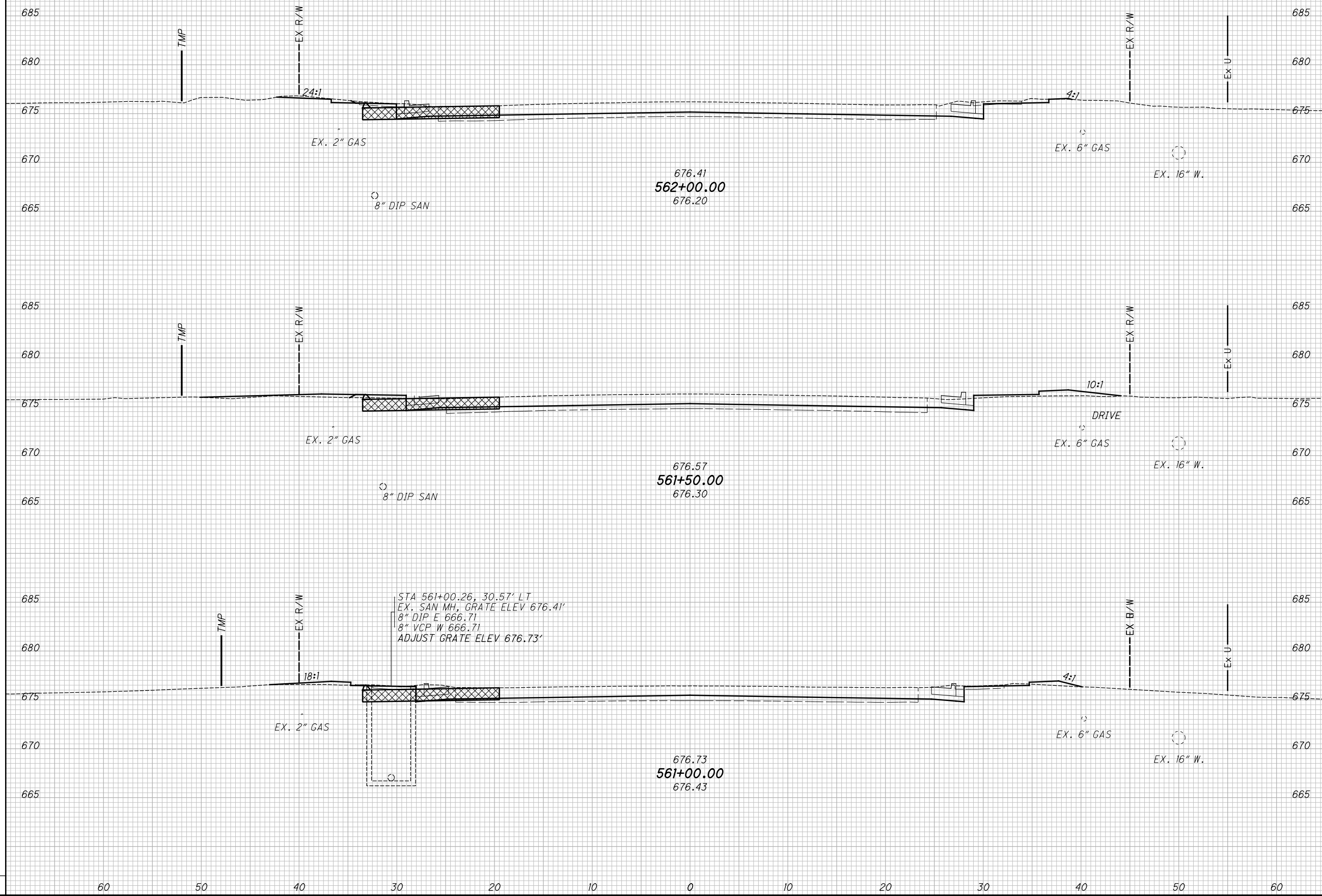
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
14	0	21	8		
9	9	20	11		
13	3	23	6		
		64	25		

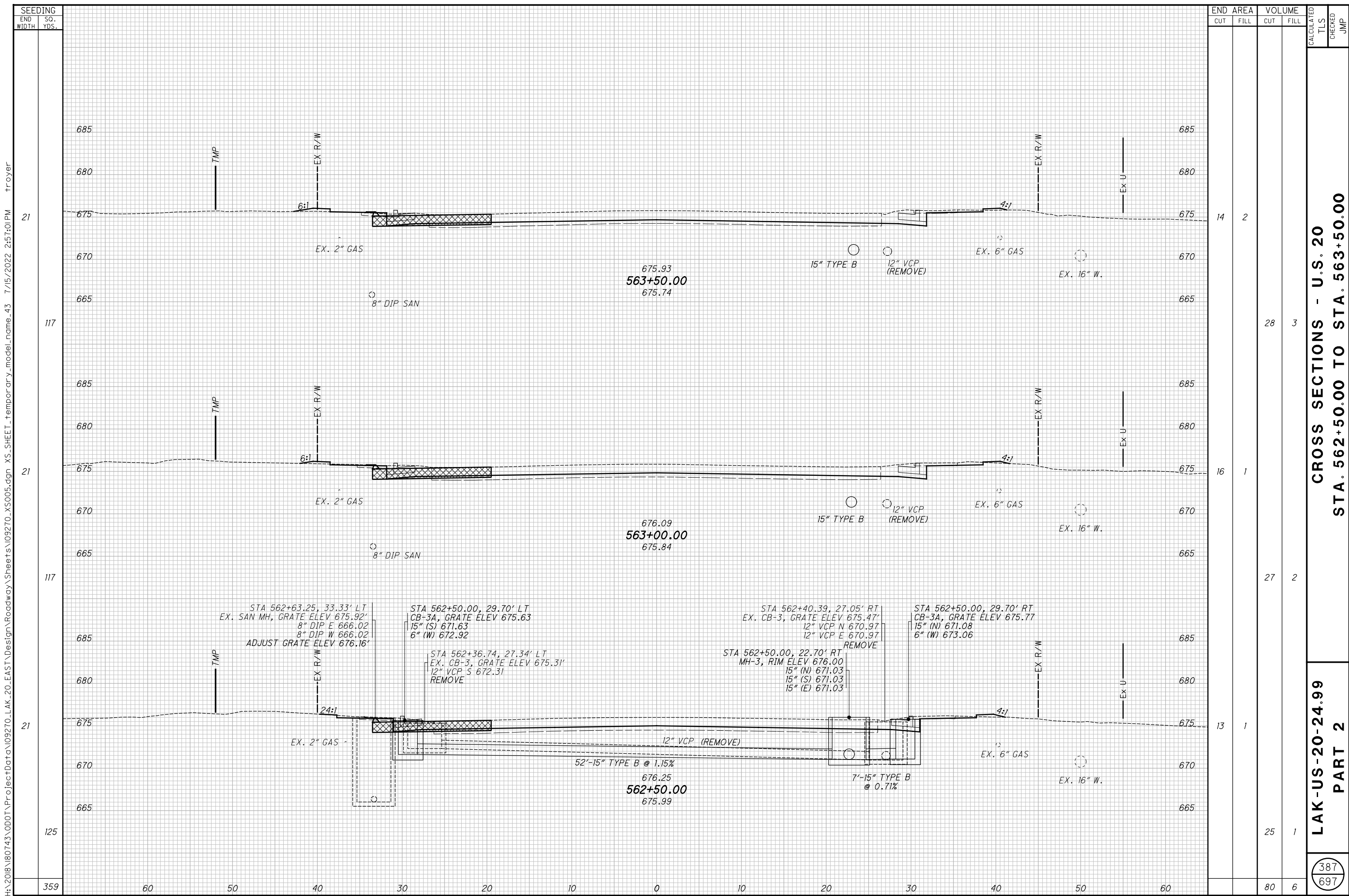
**CROSS SECTIONS - U.S. 20
STA. 561+00.00 TO STA. 562+00.00**

**LAK-US-20-24.99
PART 2**

386
697



STA 561+00.26, 30.57' LT
EX. SAN MH, GRATE ELEV 676.41'
8" DIP E 666.71
8" VCP W 666.71
ADJUST GRATE ELEV 676.73'



SEEDING	
END WIDTH	SO. YDS.
21	117
21	117
21	125
359	

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
14	2				
16	1				
13	1				
		80	6		

CROSS SECTIONS - U.S. 20
STA. 562+50.00 TO STA. 563+50.00

LAK-US-20-24.99
PART 2

387
 697

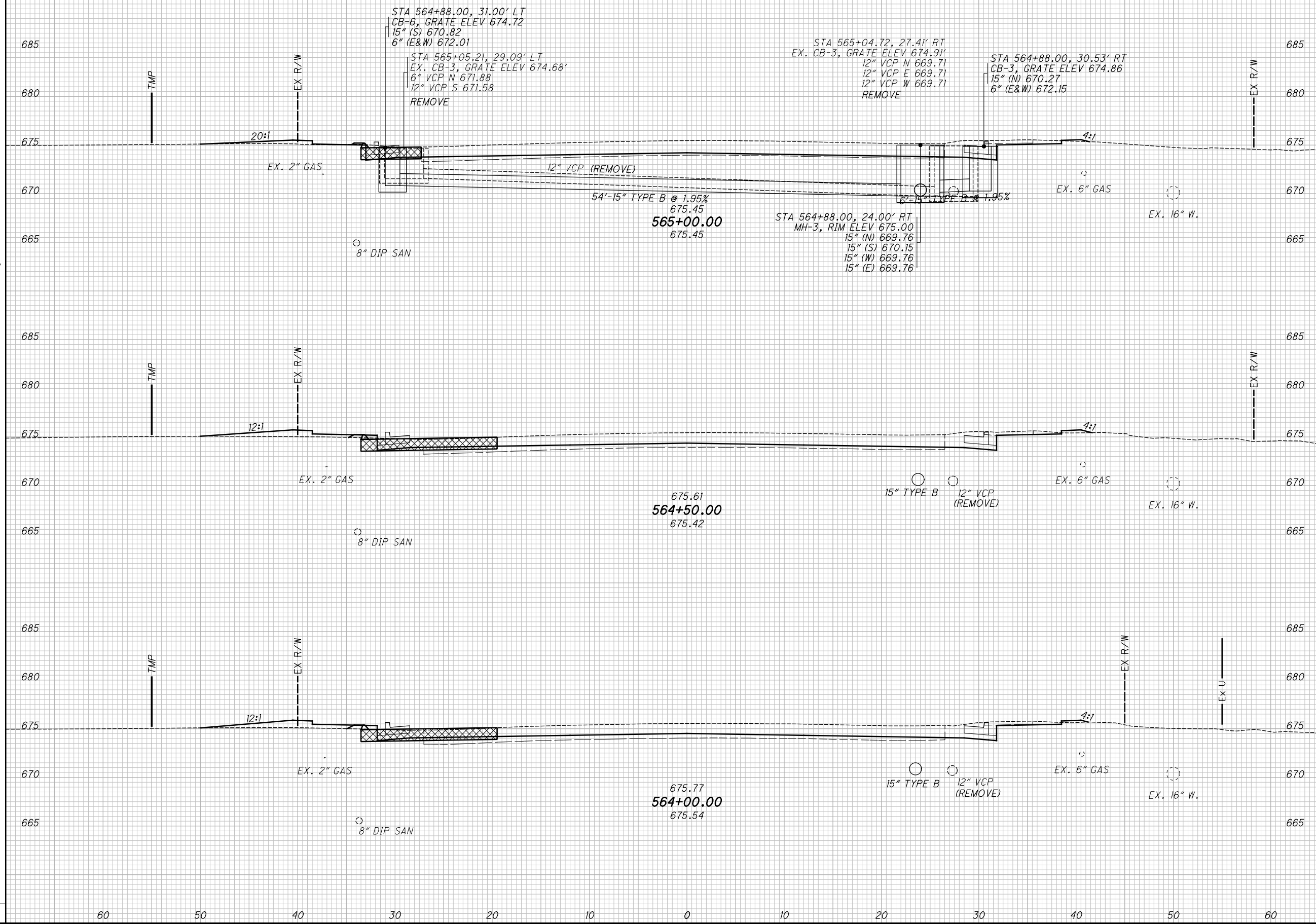
H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_43 7/15/2022 2:57:01 PM Troyer

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_44 7/15/2022 2:57:02 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	END WIDTH	SO. YDS.	CUT	FILL			
43	60	50	17	3			
222	50	40	14	7			
37	40	30	14	7			
169	30	20	13	8			
24	20	10	13	8			
125	10	0	25	9			
516	60	50	79	32			

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



**CROSS SECTIONS - U.S. 20
STA. 564+00.00 TO STA. 565+00.00**

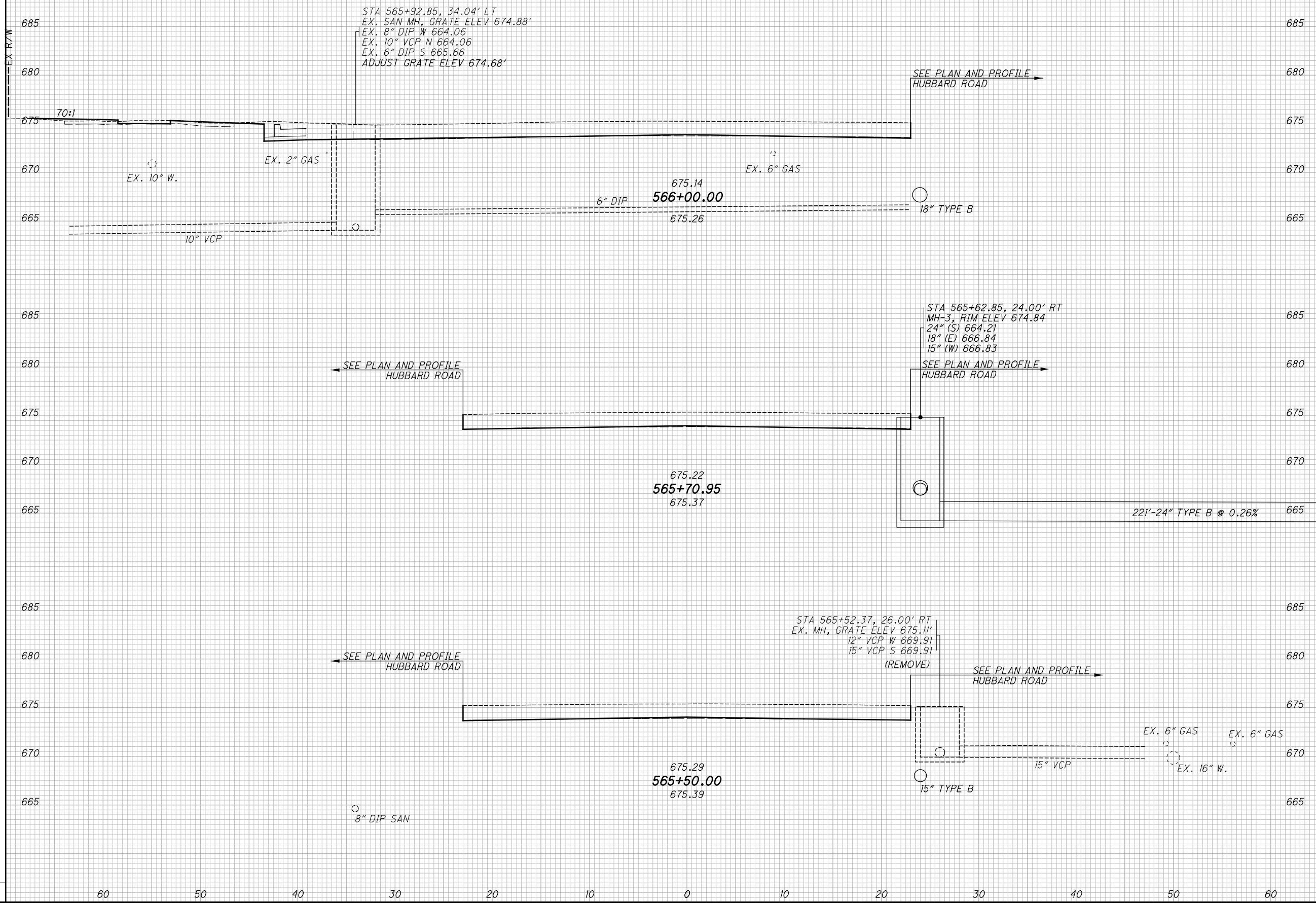
**LAK-US-20-24.99
PART 2**

388
697

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_45 7/15/2022 2:57:03 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
22	17	2					
36			9	1			
0	0	0	0	0			
0	0	0	0	0			
119			16	3			
155			25	4			

LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 565+50.00 TO STA. 566+00.00
LAK-US-20-24.99
PART 2

389
697

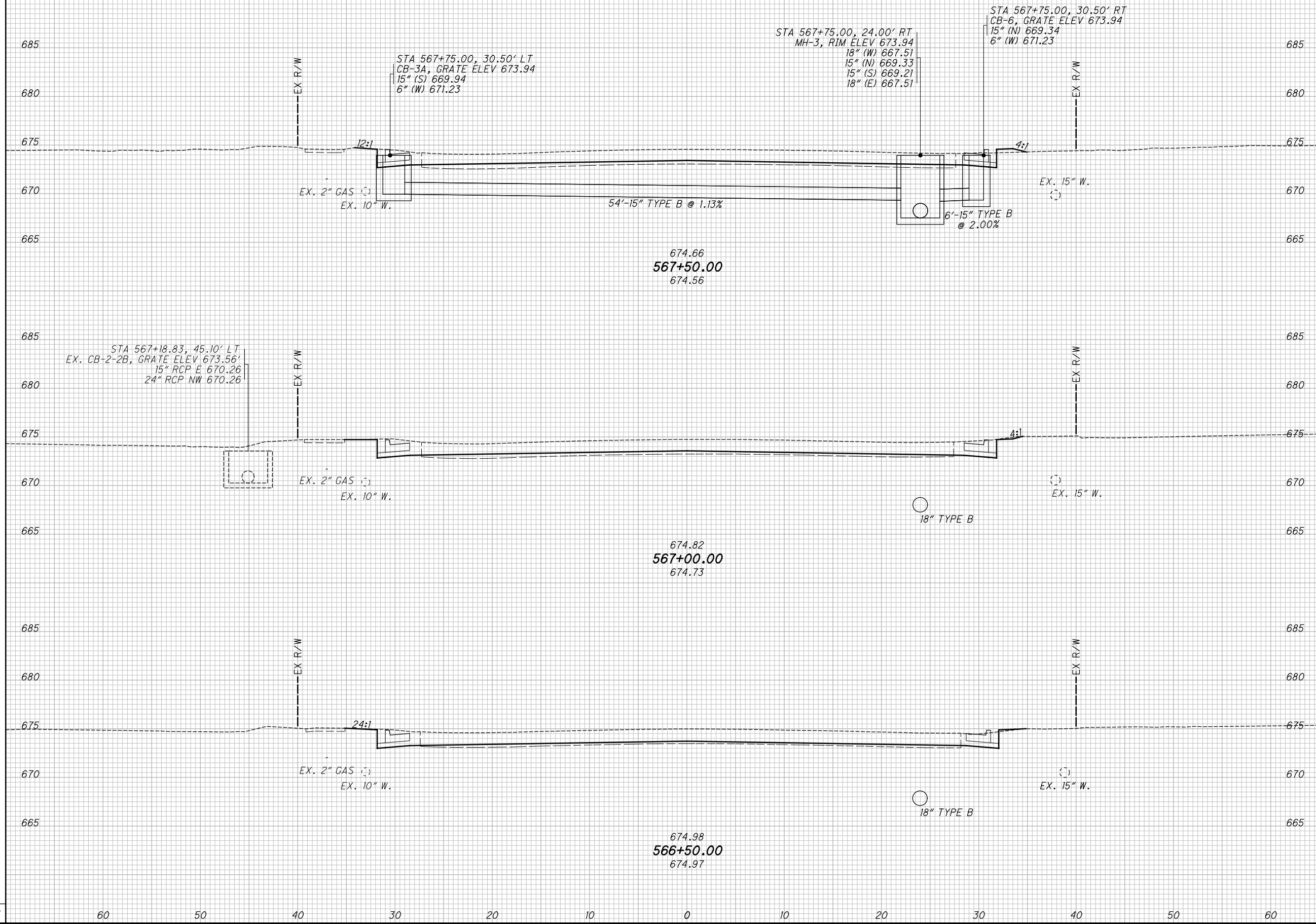
H:\2018\80743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_46 7/15/2022 2:57:04 PM Troyer

SEEDING	END	
	WIDTH	SO. YDS.
	12	685
	12	680
	12	675
	67	670
	67	665
	12	685
	12	680
	12	675
	67	670
	67	665
	12	685
	12	680
	12	675
	94	670
	94	665
228	60	665

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL			
13	1				
15	0				
13	0				
80	3				



CROSS SECTIONS - U.S. 20
STA. 566+50.00 TO STA. 567+50.00

LAK-US-20-24.99
PART 2

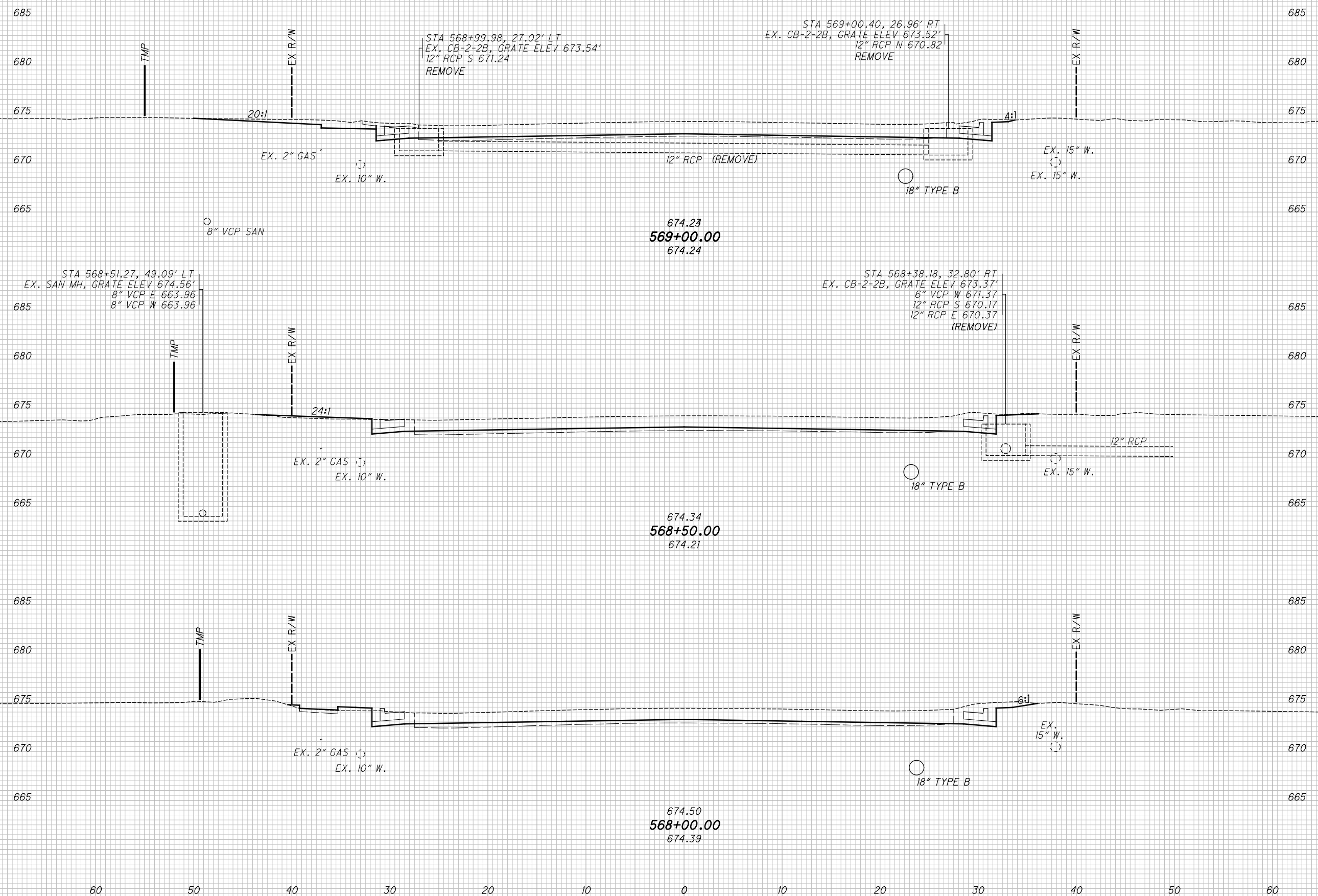
390
697

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_47 7/15/2022 2:57:06 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
27	21	0					
153			33	2			
28	15	2					
139			29	3			
22	16	1					
94			27	2			
386			89	7			

LEGEND

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



**CROSS SECTIONS - U.S. 20
STA. 568+00.00 TO STA. 569+00.00**

**LAK-US-20-24.99
PART 2**

391
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_49 7/15/2022 2:57:08 PM troyer

SEEDING	
END WIDTH	SO. YDS.
26	139
24	128
267	

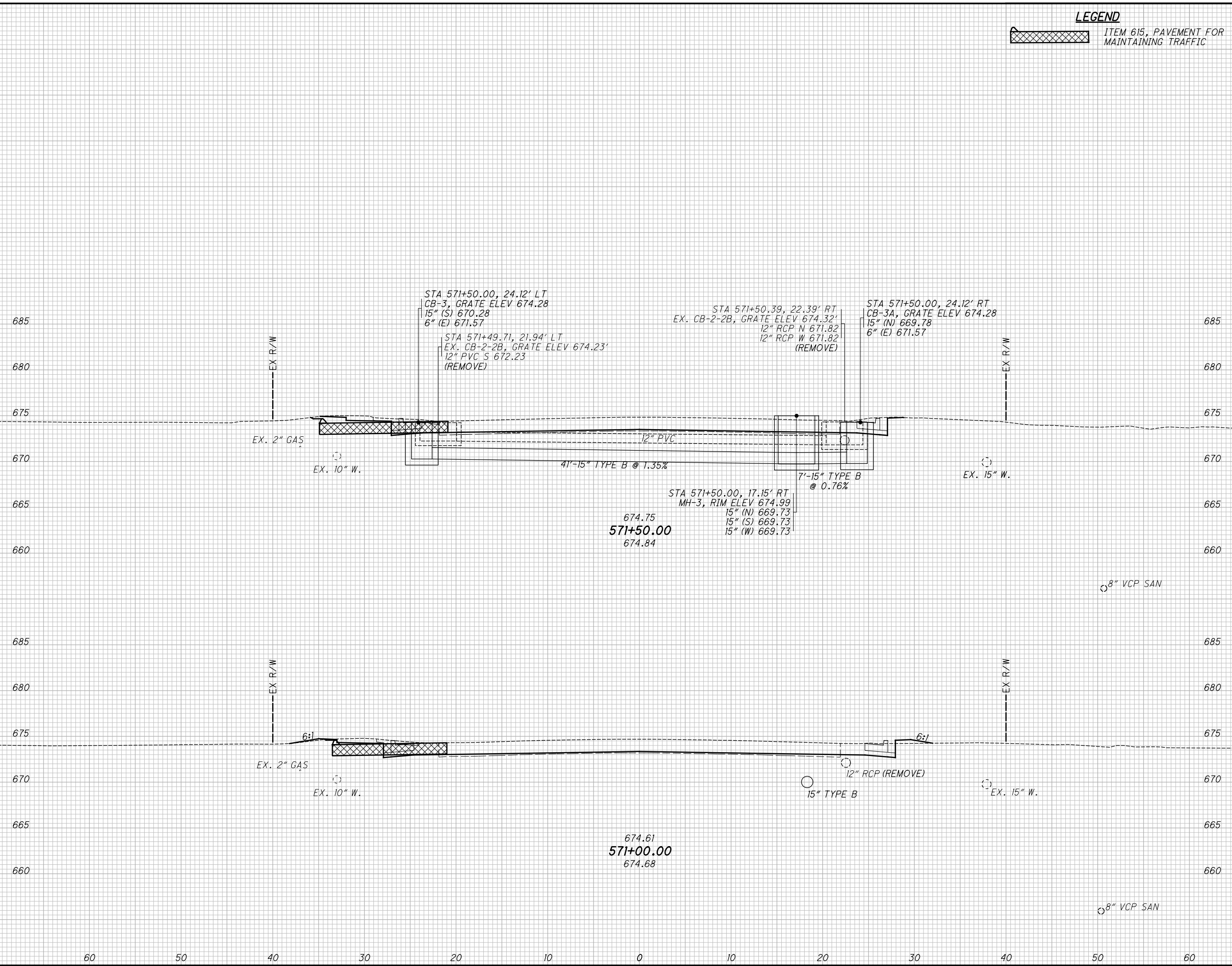
LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
16	0	31	2		
18	2	39	2		
		70	4		

CROSS SECTIONS - U.S. 20
STA. 571+00.00 TO STA. 571+50.00

LAK-US-20-24.99
PART 2

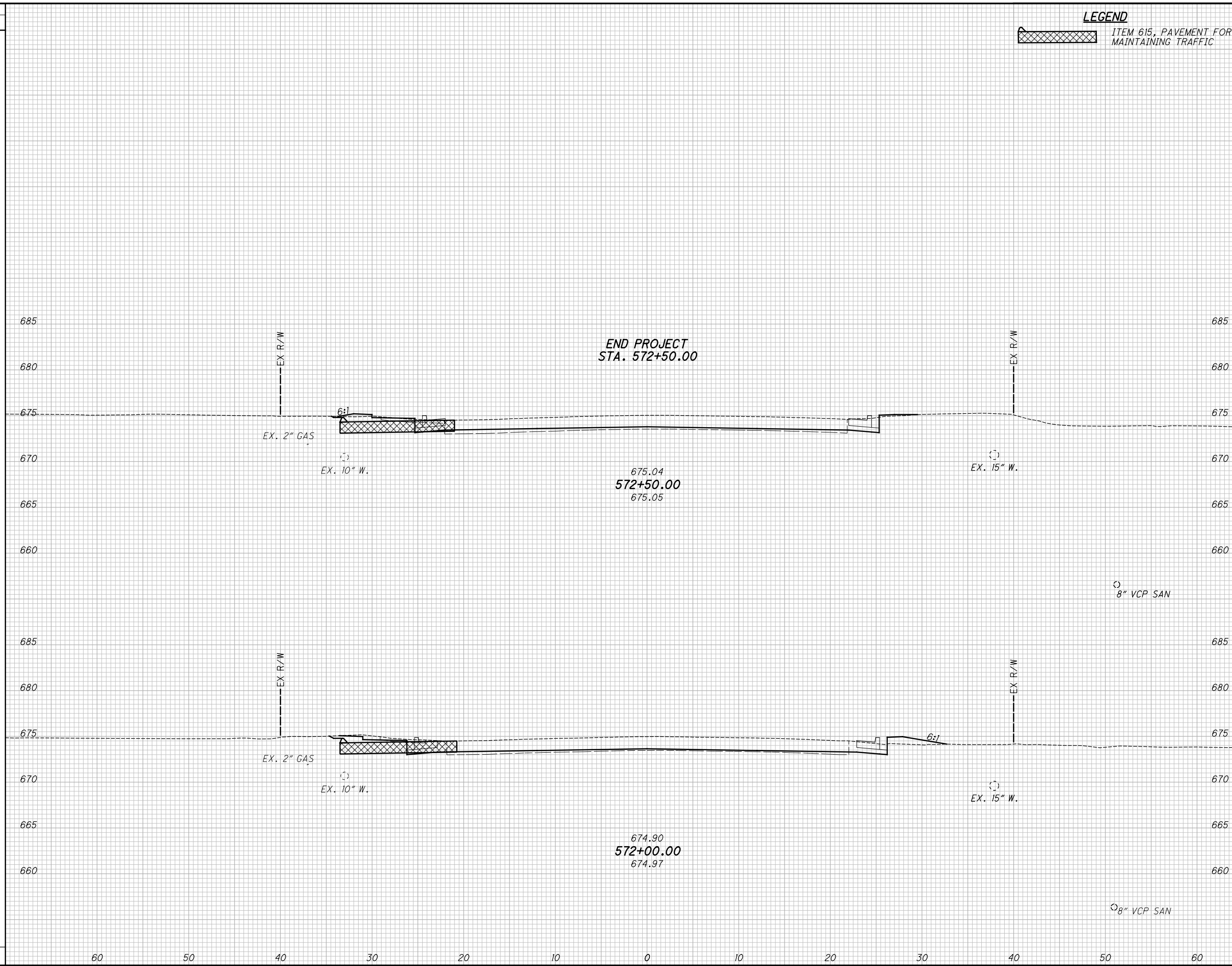
393
697



H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_50 7/15/2022 2:57:09 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
END WIDTH							
SO. YDS.							
30			8	1			
161			18	4			
28			11	3			
150			25	3			
311			43	7			

LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



CROSS SECTIONS - U.S. 20
STA. 572+00.00 TO STA. 572+50.00

LAK-US-20-24.99
PART 2

394
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_51 7/15/2022 2:57:10 PM troyer

SEEDING	END	
	WIDTH	SO. YDS.
	18	100
	18	133
	233	

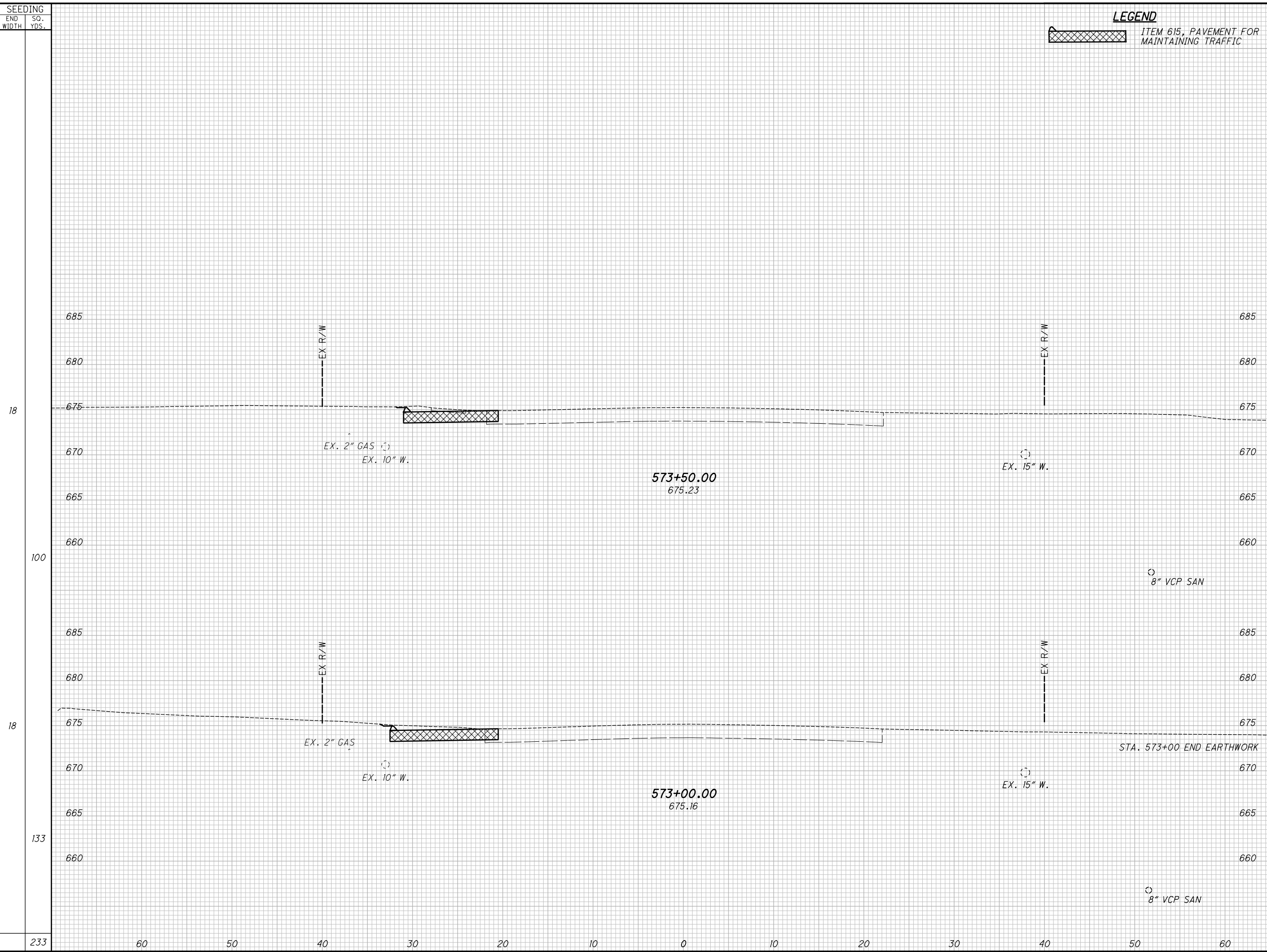
LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA	VOLUME	CALCULATED	CHECKED
0	0		
0	0		
0	7	1	
0	7	1	

CROSS SECTIONS - U.S. 20
STA. 573+00.00 TO STA. 573+50.00

LAK-US-20-24.99
PART 2

395
697



H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_52 7/15/2022 2:57:11 PM troyer

SEEDING	
END WIDTH	SO. YDS.
12	83
83	83

LEGEND

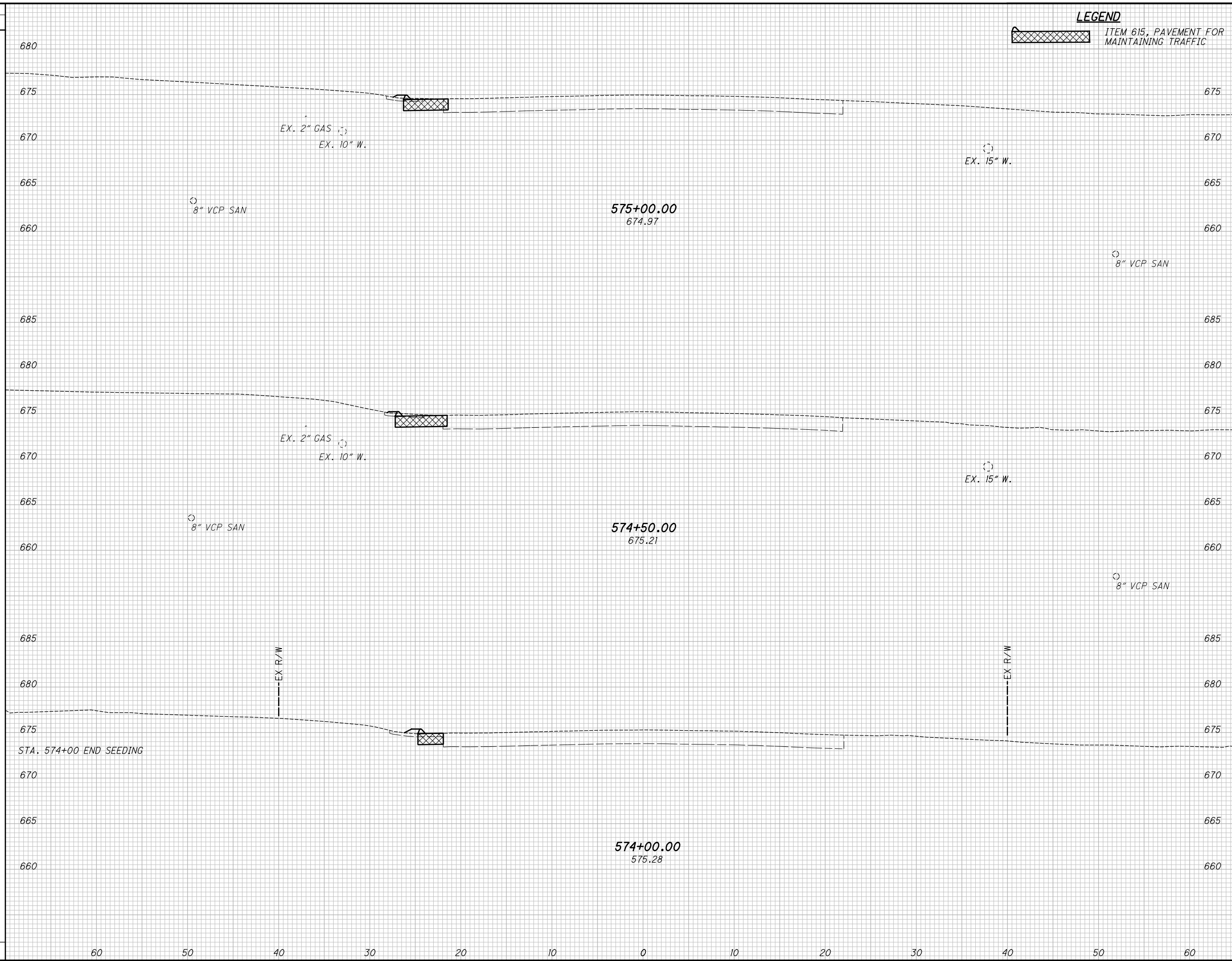
ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
0	0	0	0		
		0	0		
		0	0		

**CROSS SECTIONS - U.S. 20
STA. 574+00.00 TO STA. 575+00.00**

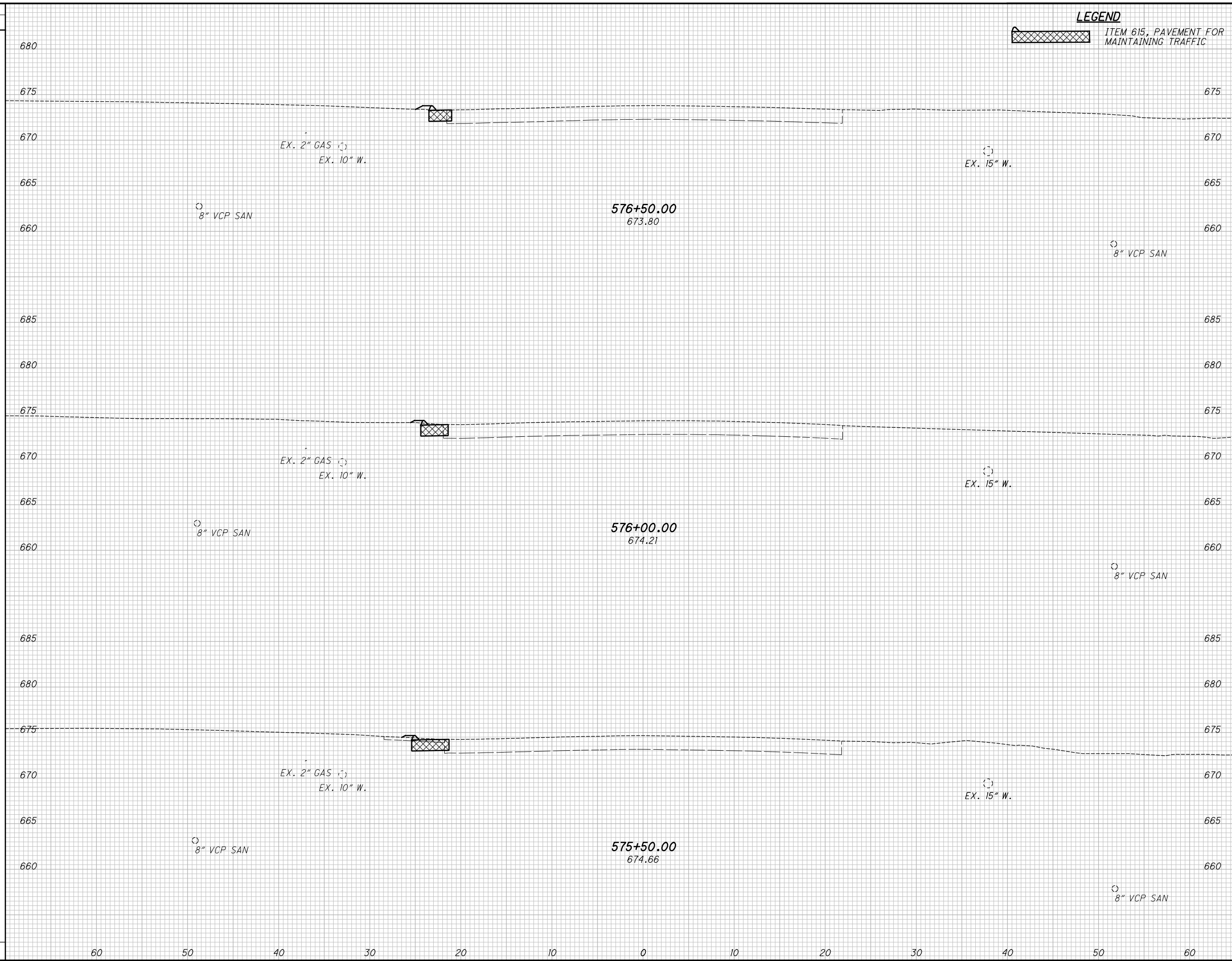
**LAK-US-20-24.99
PART 2**

396
697



H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_53 7/15/2022 2:57:42 PM Troyer

SEEDING	
END WIDTH	SO. YDS.
83	



LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME	
CUT	FILL	CUT	FILL

LAK-US-20-24.99
PART 2
CROSS SECTIONS - U.S. 20
STA. 575+50.00 TO STA. 576+50.00

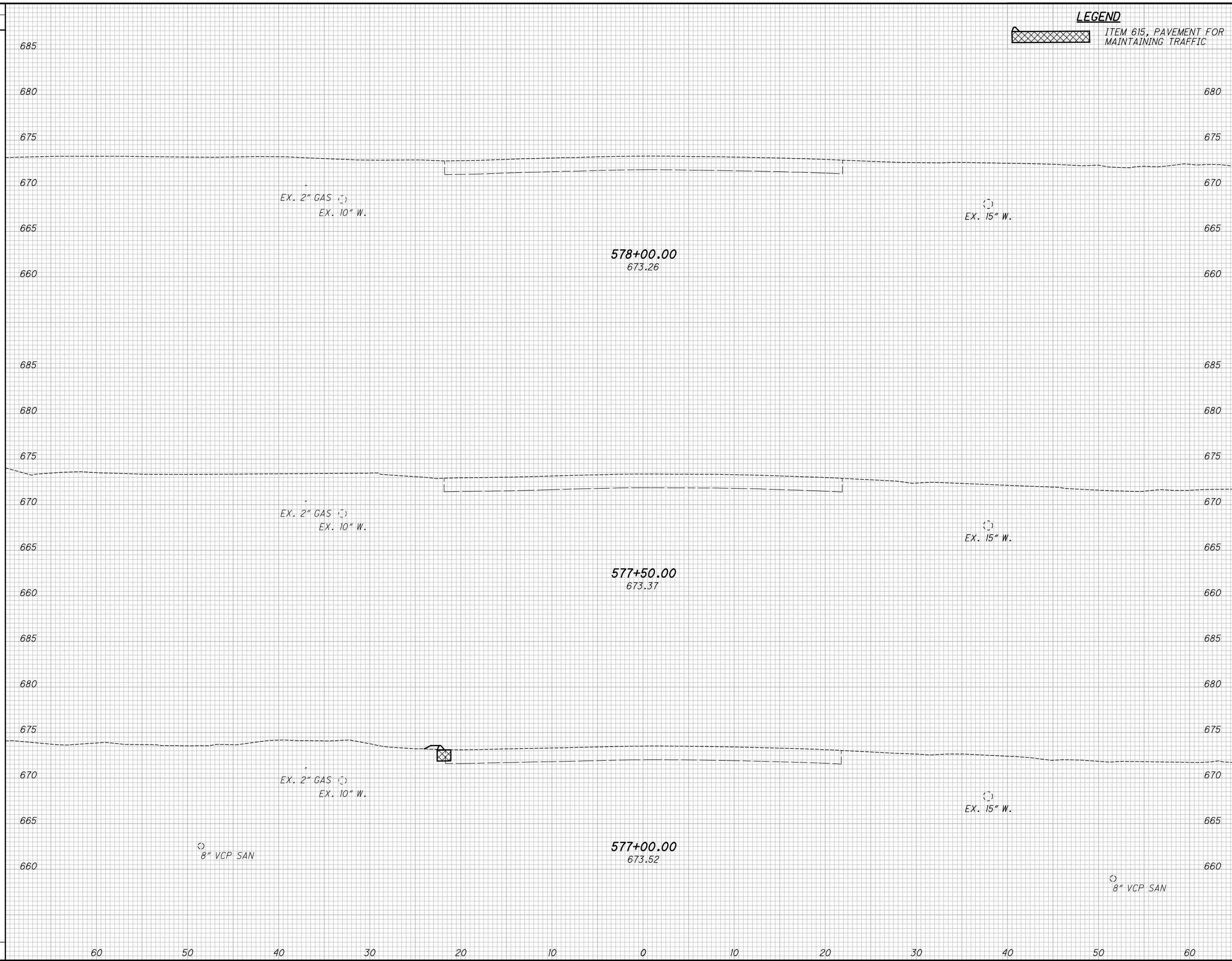
CALCULATED	TLS
CHECKED	JMP
397	697

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS005.dgn XS_SHEET_temporary_model_name_54 7/15/2022 2:57:14 PM Troyer

SEEDING	
END WIDTH	SO. YDS.
83	

LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		



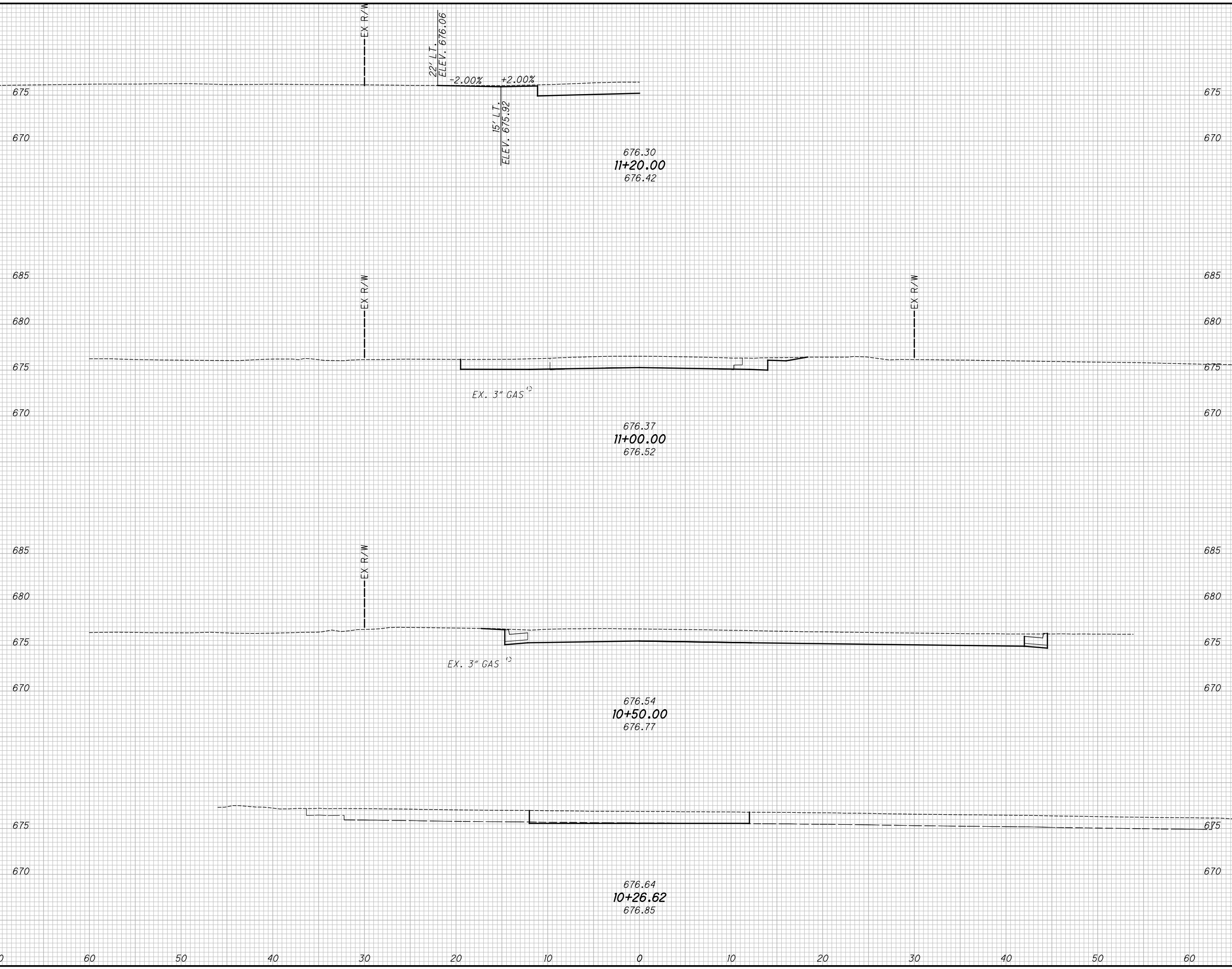
CROSS SECTIONS - U.S. 20
STA. 577+00.00 TO STA. 578+00.00

LAK-US-20-24.99
PART 2

398
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS200.dgn XS_SHEET_temporary_model_name_1 7/15/2022 2:57:15 PM troyer

SEEDING	END	
	WIDTH	SO. YDS.
16	675	675
48	670	670
27	685	685
	680	680
	675	675
117	670	670
	685	685
	680	680
15	675	675
	670	670
20	675	675
	670	670
0	675	675
	670	670
185	60	60

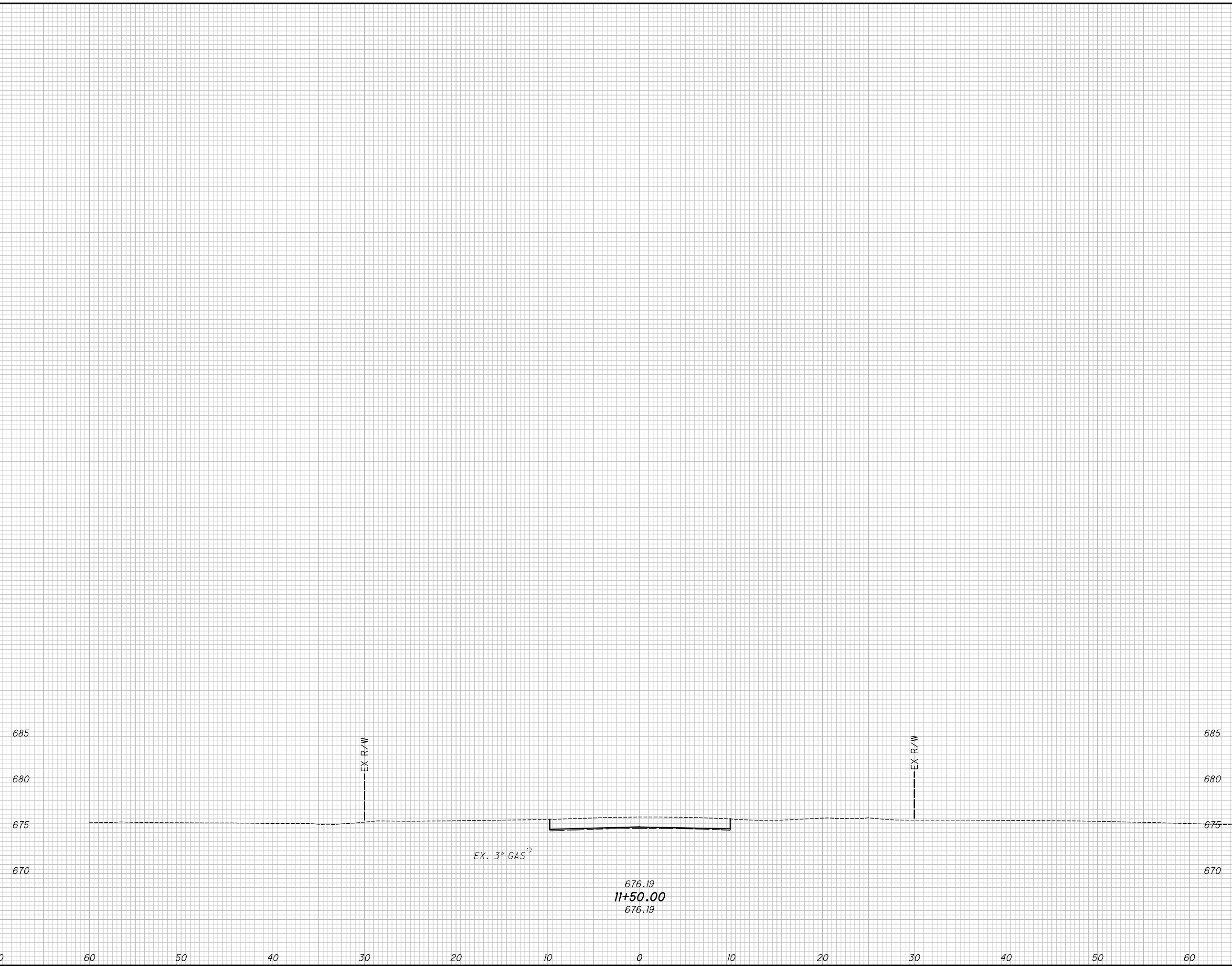


END AREA	VOLUME	
	CUT	FILL
	13	0
	18	0
	6	0
	0	0
	0	0
	18	0

CROSS SECTIONS HAINES ROAD
STA. 10+26.62 TO STA. 11+20.00
LAK-US-20-24.99
PART 2
 CALCULATED: 399
 CHECKED: 697
 JMP

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS200.dgn XS_SHEET_temporary_model_name_2 7/15/2022 2:57:17 PM troyer

SEEDING	
END WIDTH	SO. YDS.
40	93
93	70

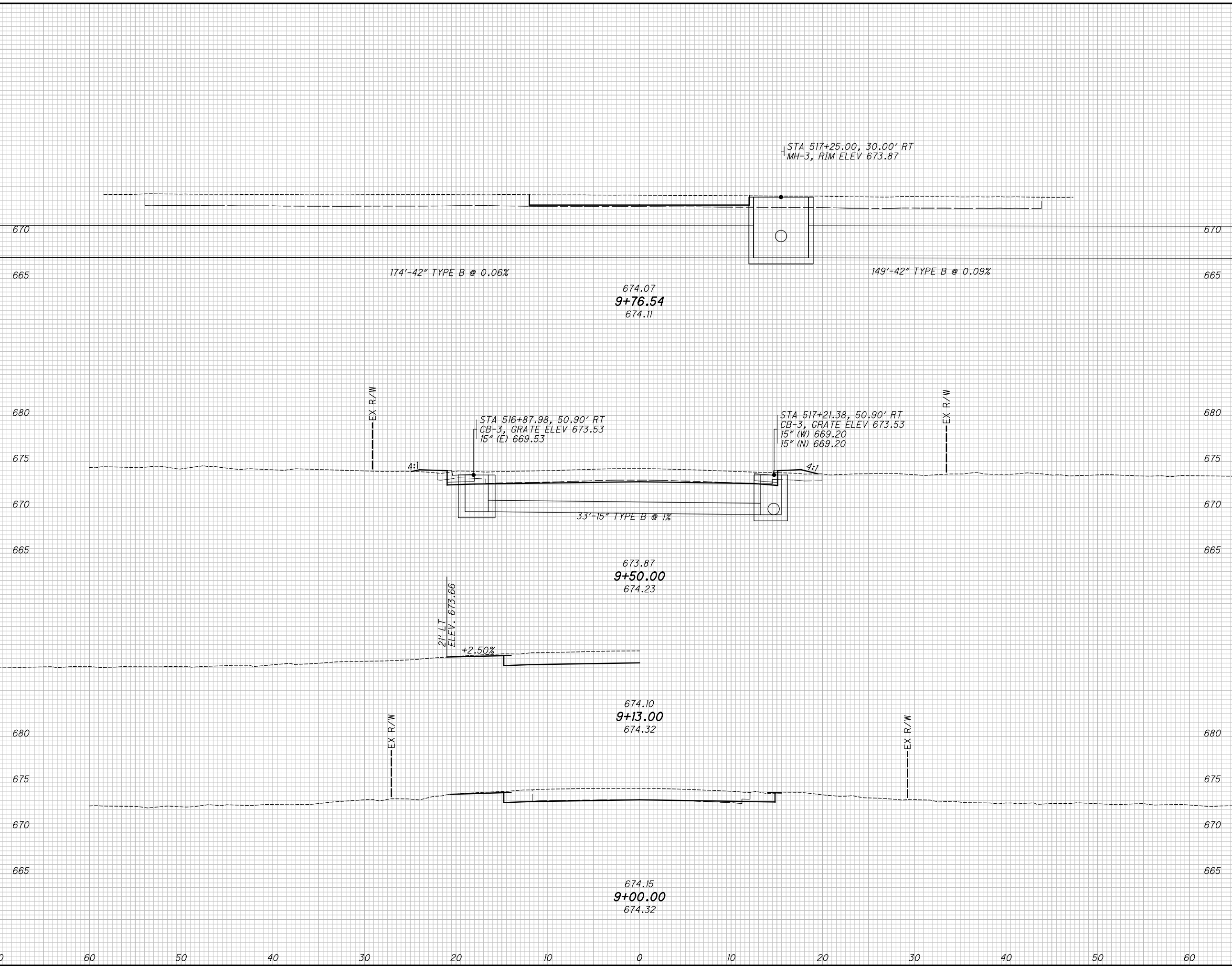


END AREA		VOLUME	
CUT	FILL	CUT	FILL
0	0	12	0

CALCULATED TLS CHECKED JMP	LAK-US-20-24.99 PART 2	CROSS SECTIONS HAINES ROAD STA. 11+50.00 TO STA. 12+00.00
	400 697	

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS201.dgn XS_SHEET_temporary_model_name_2 7/15/2022 2:57:19 PM Troyer

SEEDING	
END WIDTH	SO. YDS.
316	70
26	128
26	29
14	14
82	665
26	670
26	675
77	680
26	665
26	670



END AREA		VOLUME	
CUT	FILL	CUT	FILL
6	0	13	0
6	2	11	2
6	2	6	2
24	2		

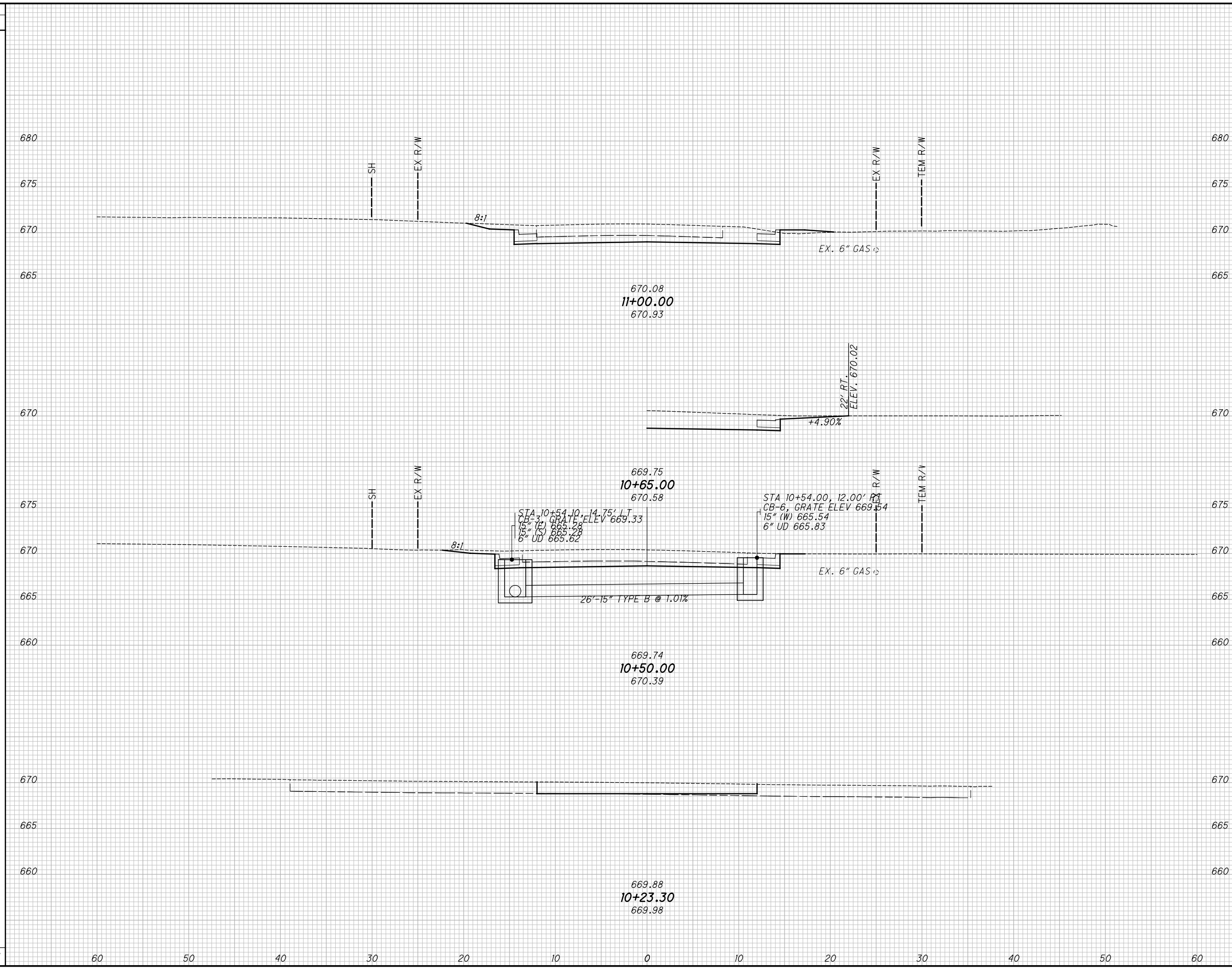
CROSS SECTIONS BURNS ROAD
STA. 9+00.00 TO STA. 9+76.54

LAK-US-20-24.99
PART 2

402
697

SEEDING
 END SO.
 WIDTH YDS.
 30
 161
 28
 83
 28
 244

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS202.dgn XS_SHEET_temporary_model_name_1 7/5/2022 2:57:21PM Troyer



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		
30	2	50	4		
24	0	24	0		
24	0	74	4		

CROSS SECTIONS DERUBERTIS ROAD
STA. 10+23.30 TO STA. 11+00.00
LAK-US-20-24.99
PART 2
 403
 697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS202.dgn XS_SHEET_temporary_model_name.2 7/15/2022 2:57:23 PM Troyer

SEEDING	END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
	CUT	FILL	CUT	FILL		
END WIDTH						
SO. YDS.						
38	4	0				
103			6	1		
36	9	1				
183			36	3		
286			42	4		



END AREA	VOLUME		CALCULATED TLS	CHECKED JMP
	CUT	FILL		
4	0			
		6	1	
9	1			
		36	3	
		42	4	

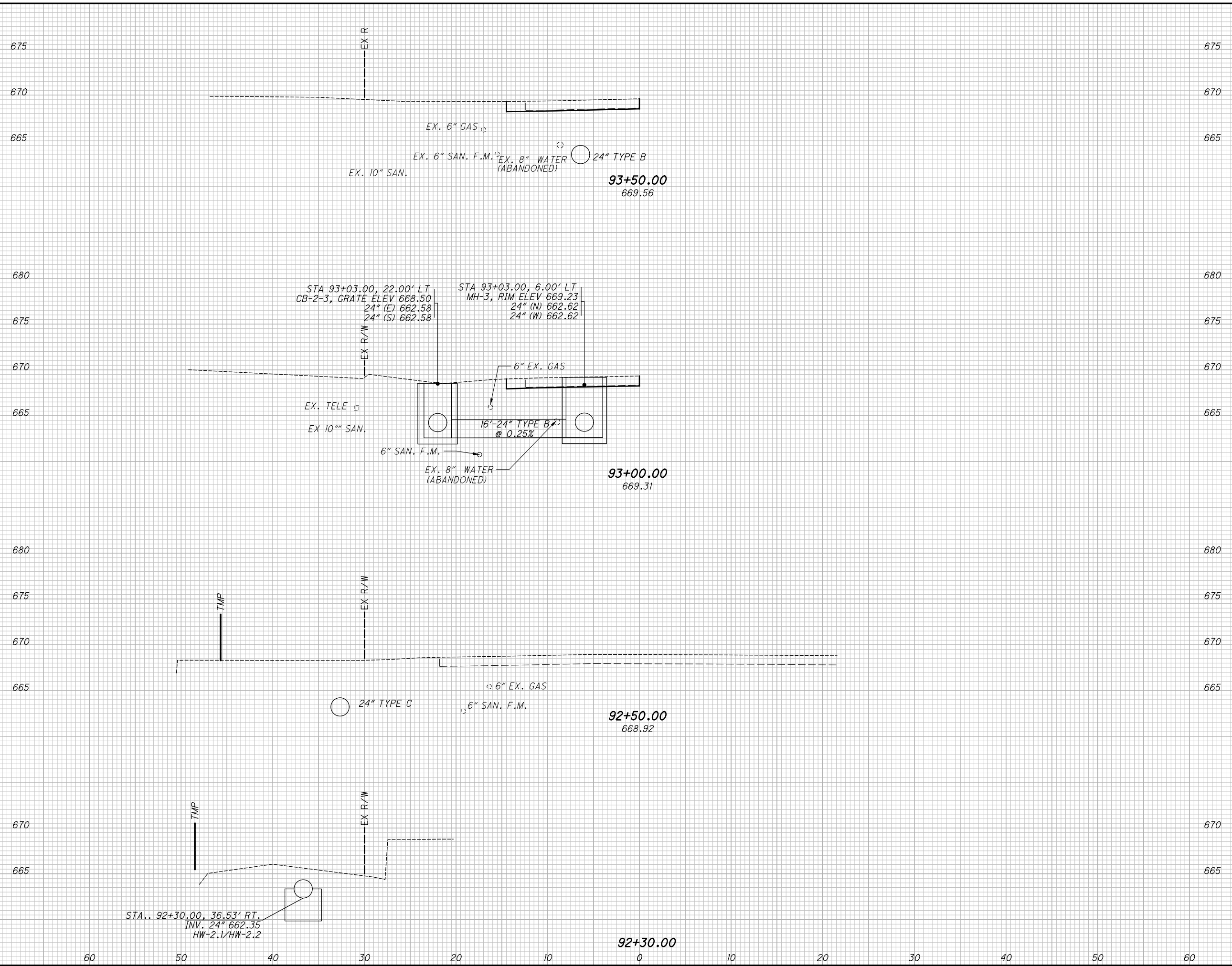
**CROSS SECTIONS DERUBERTIS ROAD
STA. 11+50.00 TO STA. 12+00.00**

**LAK-US-20-24.99
PART 2**

(404)
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS203.dgn STORM SEWER XS_SHEET_3 7/15/2022 2:57:25 PM Troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
16	2	0					
94			4	0			
18	2	0					
117							
24							
50							
21							
261			4	0			



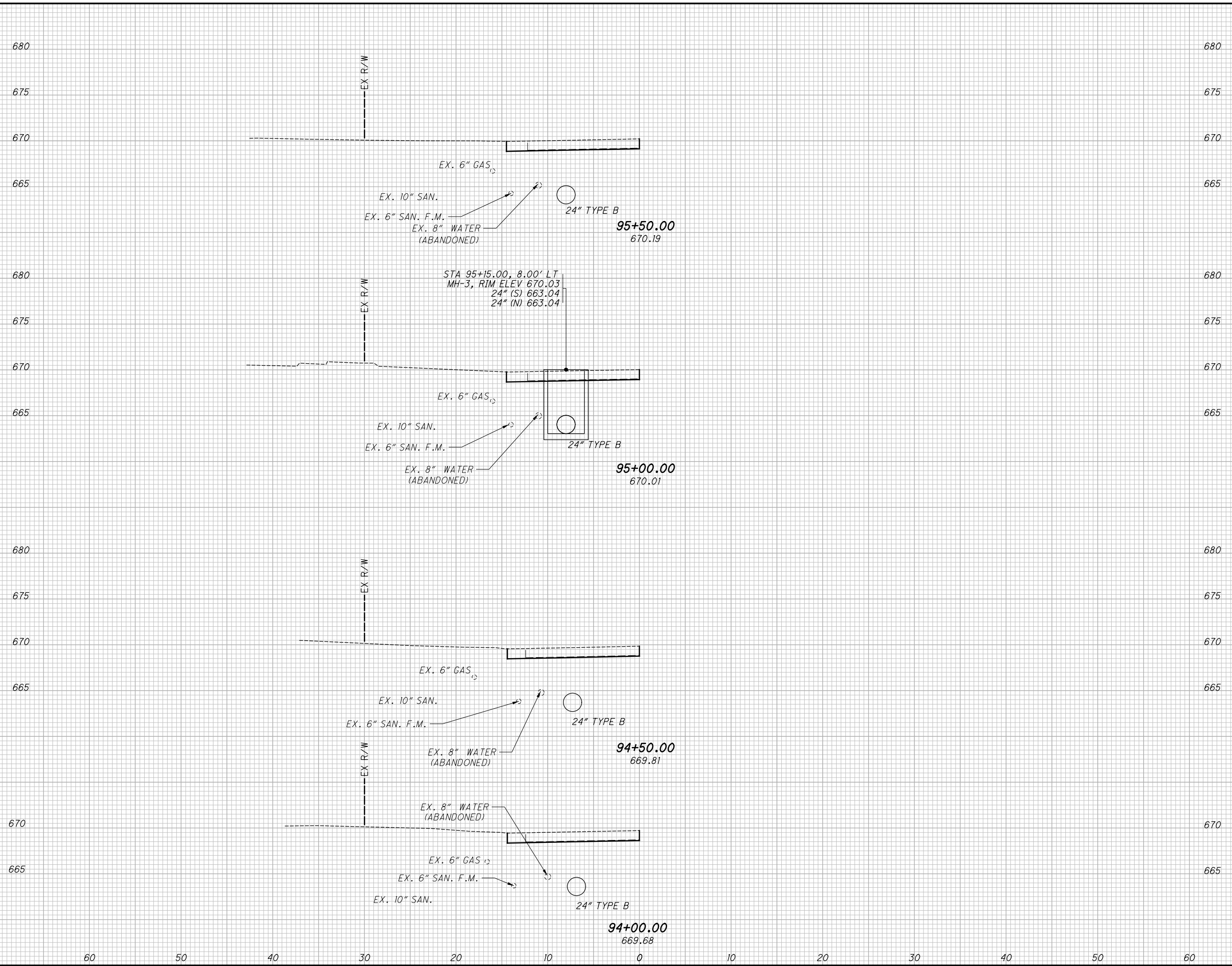
**CROSS SECTIONS HUBBARD ROAD
STA. 92+30.00 TO STA. 93+50.00**

**LAK-US-20-24.99
PART 2**

405
697

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS203.dgn STORM_SEWER_XS_SHEET_2 7/15/2022 2:57:27 PM Troyer

SEEDING	END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
	CUT	FILL	CUT	FILL		
16	2	0				
89			4	0		
16	2	0				
89			4	0		
16	2	0				
89			4	0		
16	2	0				
89			4	0		
356			16	0		



**CROSS SECTIONS HUBBARD ROAD
STA. 94+00.00 TO STA. 95+50.00**

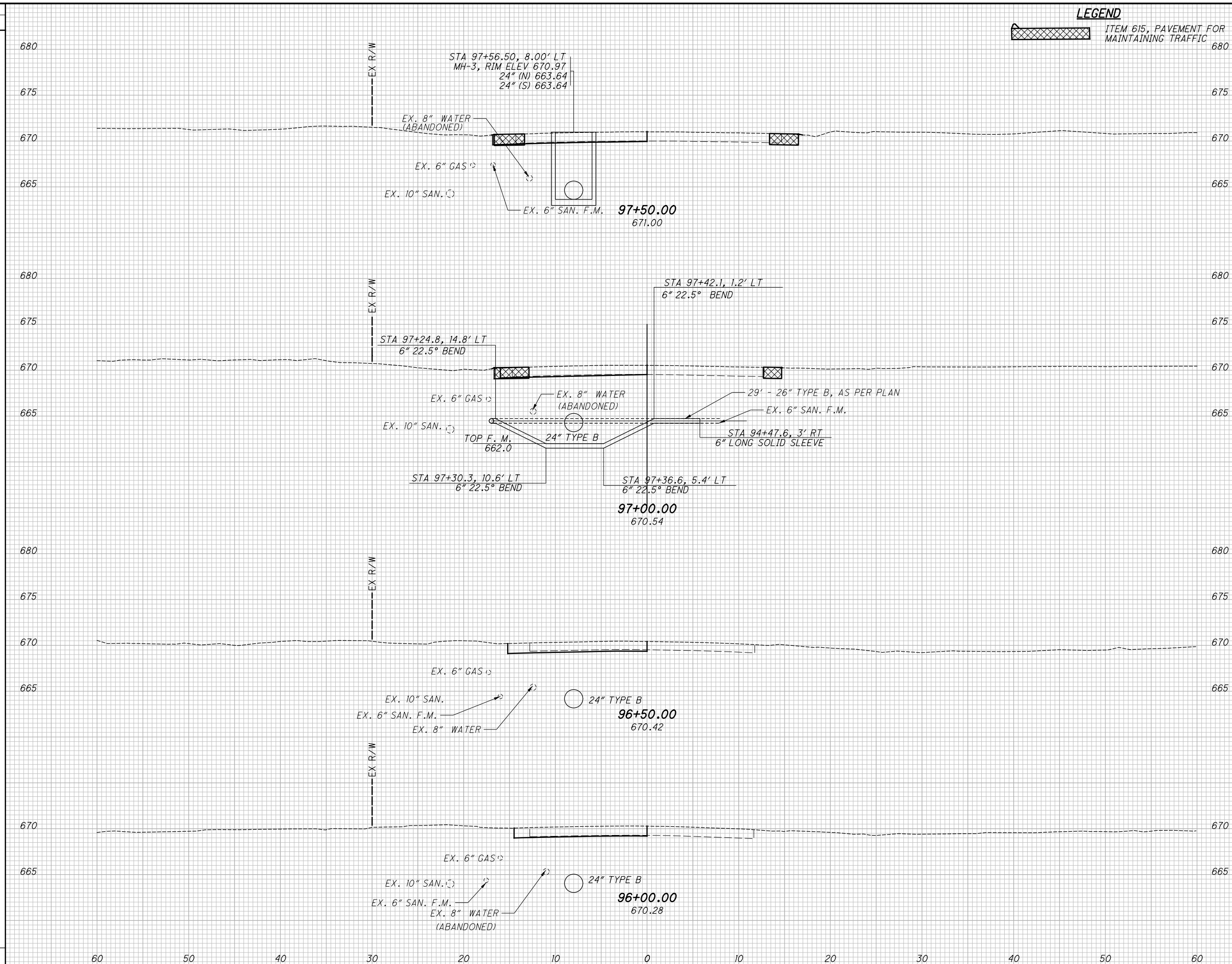
**LAK-US-20-24.99
PART 2**

406
697

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS203.dgn STORM SEWER XS_SHEET_1 7/15/2022 2:57:28 PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	END WIDTH	SO. YDS.	CUT	FILL			
13			5	0			
75				6			
14			1	0			
81				3			
15			2	0			
86				4			
16			2	0			
89				4			
331	60			17			

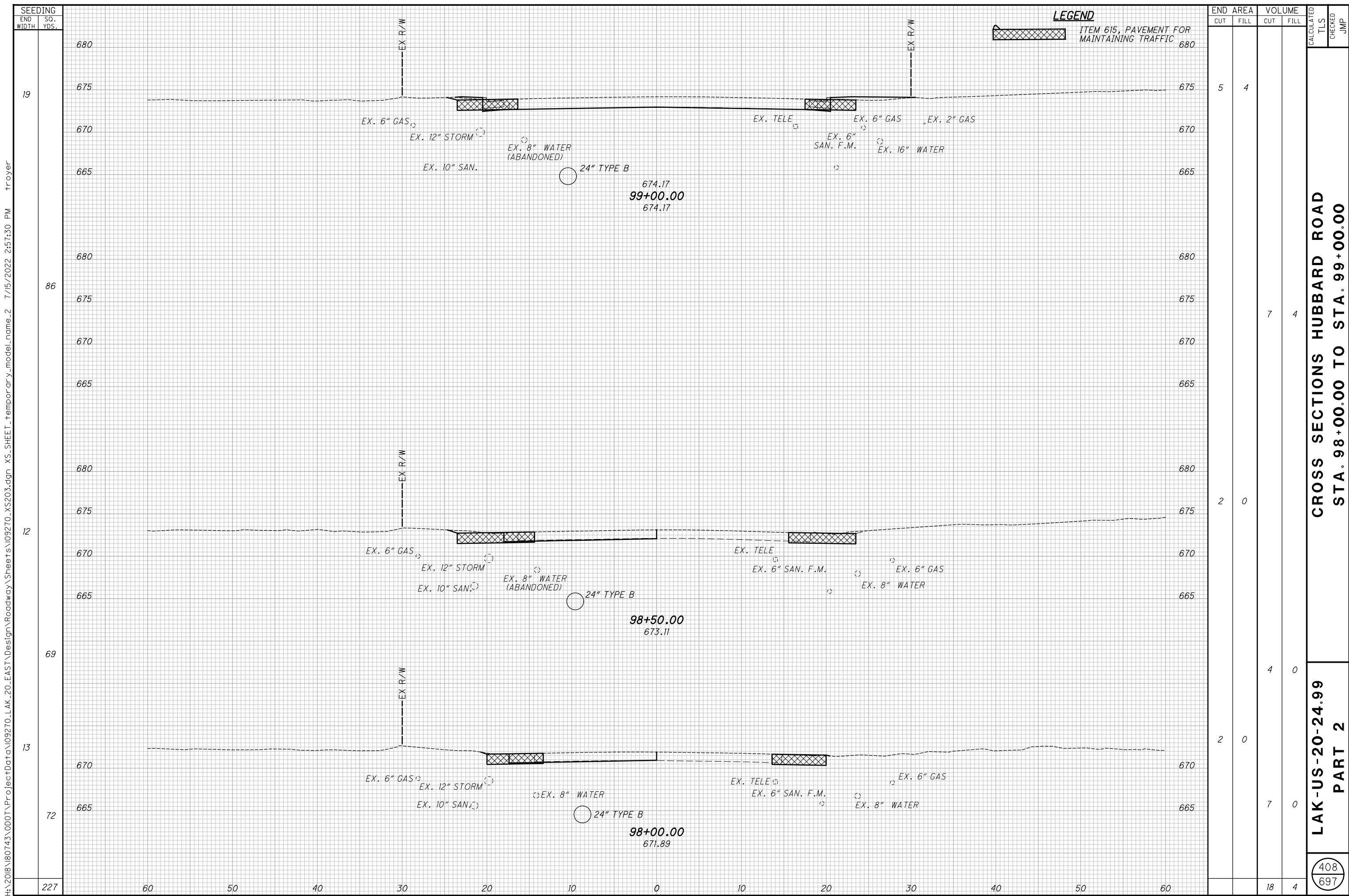
LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC



**CROSS SECTIONS HUBBARD ROAD
 STA. 96+00.00 TO STA. 97+50.00**

**LAK-US-20-24.99
 PART 2**

407
 697



H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS203.dgn XS_SHEET_temporary_model_name_2 7/15/2022 2:57:30 PM Troyer

**CROSS SECTIONS HUBBARD ROAD
STA. 98+00.00 TO STA. 99+00.00**

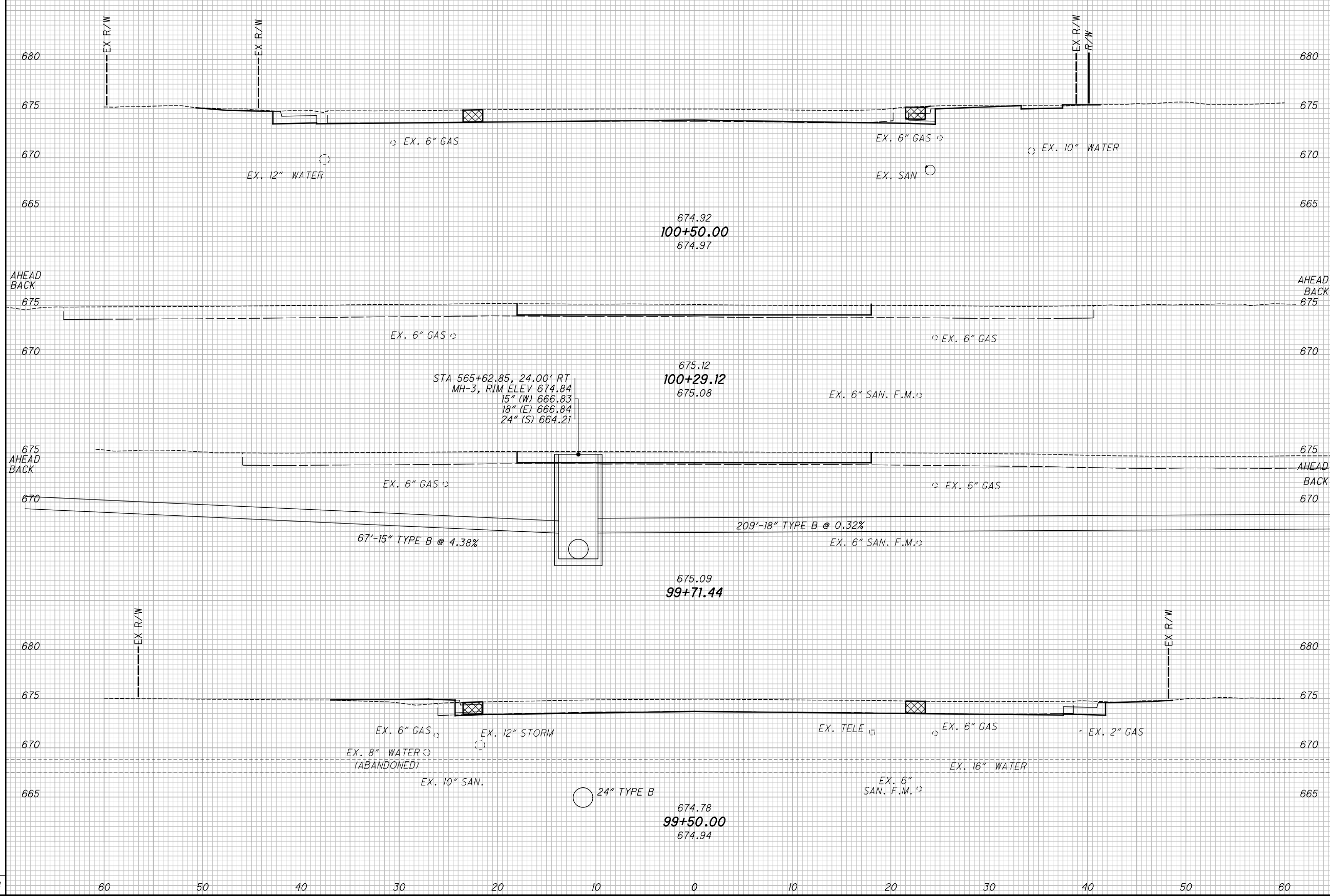
**LAK-US-20-24.99
PART 2**

408
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS203.dgn XS_SHEET_temporary_model_name_3 7/15/2022 2:57:31PM troyer

SEEDING	END AREA		VOLUME		CALCULATED	CHECKED	JMP
	END WIDTH	SO. YDS.	CUT	FILL			
	304						
	25		15	0			
	58				12	0	
	25		15	0			
	70						
	0		0	0			
	19		5	4			
	70				4	3	
	19		5	4			
	106				9	7	
	304				25	10	

LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

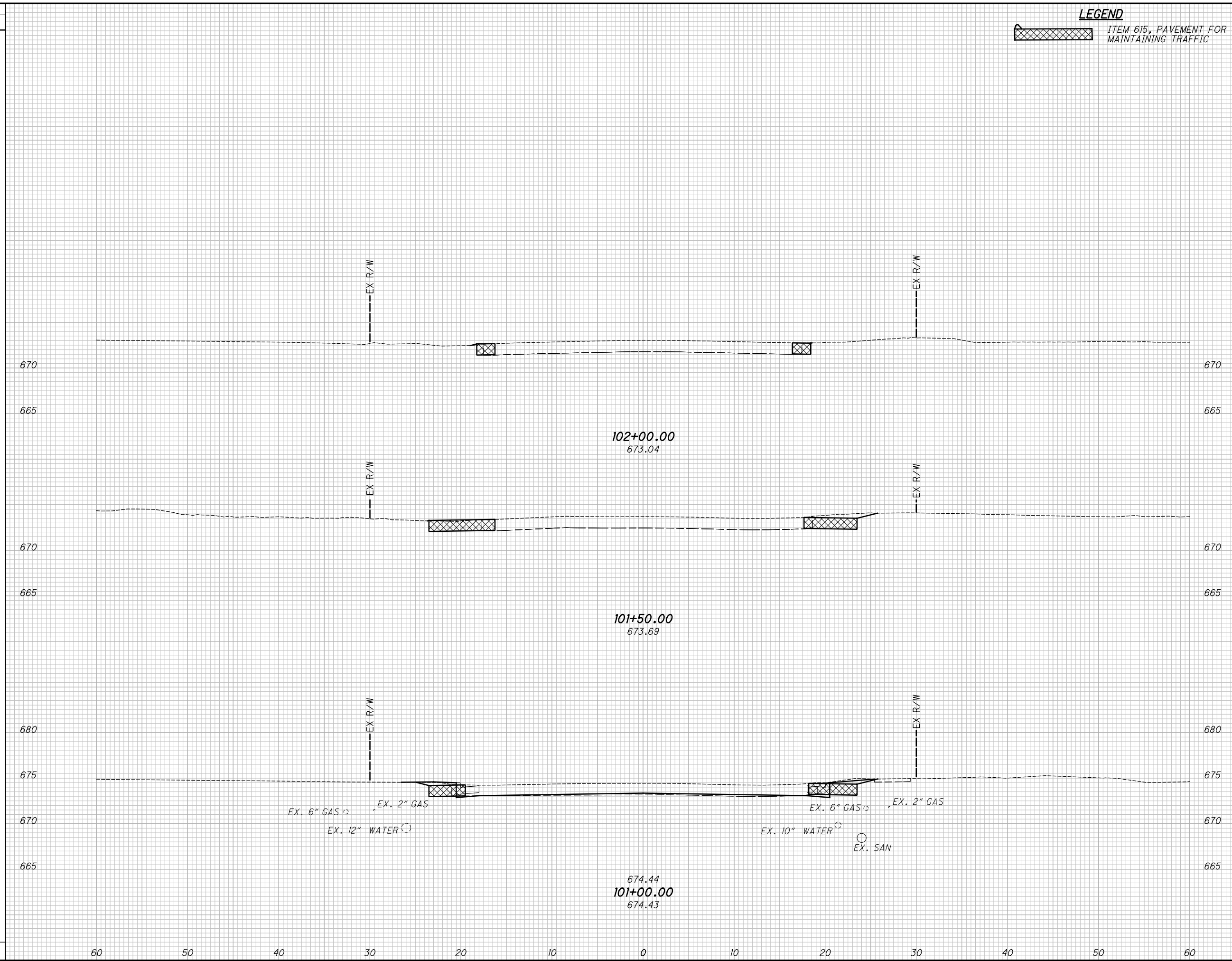


CROSS SECTIONS HUBBARD ROAD
STA. 99+50.00 TO STA. 100+50.00

LAK-US-20-24.99
PART 2

409
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS203.dgn XS_SHEET_temporary_model_name_4 7/15/2022 2:57:33 PM Troyer



LEGEND
 ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC

END	AREA		VOLUME		CALCULATED	CHECKED	JMP
	CUT	FILL	CUT	FILL			
122	2	0	16	0			

LAK-US-20-24.99
CROSS SECTIONS HUBBARD ROAD
STA. 101+00.00 TO STA. 102+00.00
PART 2

410
697

SUPERELEVATION TABLE

P.I. STA. 510+71.30

Dc=1°00'00"

LEFT SIDE					CENTERLINE CONTROL		RIGHT SIDE					REMARKS
EDGE ELEVATION	TRANSITION RATE	ELEVATION CORRECTION	CROSS SLOPE	WIDTH	STATION	PROFILE GRADE	WIDTH	CROSS SLOPE	ELEVATION CORRECTION	TRANSITION RATE	EDGE ELEVATION	
675.92		-0.02	-0.0007	23	500+40.00	675.94	23	-0.0160	-0.37		675.57	
676.03		0.00	0.0000	23	500+42.49	676.03	23	-0.0160	-0.37		675.66	HALF
676.12		0.05	0.0021	23	500+50.00	676.07	23	-0.0160	-0.37		675.71	
676.41		0.21	0.0090	23	500+75.00	676.20	23	-0.0160	-0.37		675.83	
676.57		0.31	0.0133	23	500+90.50	676.26	23	-0.0160	-0.37		675.89	PC
676.66		0.37	0.0160	23	501+00.00	676.29	23	-0.0160	-0.37		675.92	
676.66		0.37	0.0160	23	501+00.09	676.29	23	-0.0160	-0.37		675.92	RC
676.78		0.46	0.0200	23	501+14.49	676.32	23	-0.0200	-0.46		675.86	FULL
676.79		0.46	0.0200	23	501+25.00	676.33	23	-0.0200	-0.46		675.87	
676.79		0.46	0.0200	23	501+50.00	676.33	23	-0.0200	-0.46		675.87	
676.75		0.46	0.0200	23	501+75.00	676.29	23	-0.0200	-0.46		675.83	
676.67		0.46	0.0200	23	502+00.00	676.21	23	-0.0200	-0.46		675.75	
676.56		0.46	0.0200	23	502+25.00	676.10	23	-0.0200	-0.46		675.64	
676.46		0.46	0.0200	23	502+50.00	676.00	23	-0.0200	-0.46		675.54	
676.35		0.46	0.0200	23	502+75.00	675.89	23	-0.0200	-0.46		675.43	
676.25		0.46	0.0200	23	503+00.00	675.79	23	-0.0200	-0.46		675.33	
676.14		0.46	0.0200	23	503+25.00	675.68	23	-0.0200	-0.46		675.22	
676.04		0.46	0.0200	23	503+50.00	675.58	23	-0.0200	-0.46		675.12	
675.93		0.46	0.0200	23	503+75.00	675.47	23	-0.0200	-0.46		675.01	
675.89		0.52	0.0200	26.14	504+00.00	675.37	23	-0.0200	-0.46		674.91	
675.96		0.70	0.0200	35	504+25.00	675.26	23	-0.0200	-0.46		674.80	
675.86		0.70	0.0200	35	504+50.00	675.16	23	-0.0200	-0.46		674.70	
675.75		0.70	0.0200	35	504+75.00	675.05	23	-0.0200	-0.46		674.59	
675.65		0.70	0.0200	35	505+00.00	674.95	23	-0.0200	-0.46		674.49	
675.54		0.70	0.0200	35	505+25.00	674.84	23	-0.0200	-0.46		674.38	
675.44		0.70	0.0200	35	505+50.00	674.74	23	-0.0200	-0.46		674.28	
675.33		0.70	0.0200	35	505+75.00	674.63	23	-0.0200	-0.46		674.17	
675.24		0.70	0.0200	35	506+00.00	674.54	23	-0.0200	-0.46		674.08	
675.18		0.70	0.0200	35	506+25.00	674.48	23	-0.0200	-0.46		674.02	
675.15		0.70	0.0200	35	506+50.00	674.45	23	-0.0200	-0.46		673.99	
675.15		0.70	0.0200	35	506+75.00	674.45	23	-0.0200	-0.46		673.99	
675.18		0.70	0.0200	35	507+00.00	674.48	23	-0.0200	-0.46		674.02	
675.24		0.70	0.0200	35	507+25.00	674.54	23	-0.0200	-0.46		674.08	
675.32		0.70	0.0200	35	507+50.00	674.62	23	-0.0200	-0.46		674.16	
675.37		0.68	0.0200	33.93	507+75.00	674.69	23	-0.0200	-0.46		674.23	
675.33		0.56	0.0200	27.92	508+00.00	674.77	23	-0.0200	-0.46		674.31	
675.30		0.46	0.0200	23	508+25.00	674.84	23	-0.0200	-0.46		674.38	
675.38		0.46	0.0200	23	508+50.00	674.92	23	-0.0200	-0.46		674.46	
675.45		0.46	0.0200	23	508+75.00	674.99	23	-0.0200	-0.46		674.53	
675.53		0.46	0.0200	23	509+00.00	675.07	23	-0.0200	-0.46		674.61	
675.60		0.46	0.0200	23	509+25.00	675.14	23	-0.0200	-0.46		674.68	
675.68		0.46	0.0200	23	509+50.00	675.22	23	-0.0200	-0.46		674.76	
675.75		0.46	0.0200	23	509+75.00	675.29	23	-0.0200	-0.46		674.83	
675.83		0.46	0.0200	23	510+00.00	675.37	23	-0.0200	-0.46		674.91	
675.90		0.46	0.0200	23	510+25.00	675.44	23	-0.0200	-0.46		674.98	
675.98		0.46	0.0200	23	510+50.00	675.52	23	-0.0200	-0.46		675.06	
676.05		0.46	0.0200	23	510+75.00	675.59	23	-0.0200	-0.46		675.13	
676.13		0.46	0.0200	23	511+00.00	675.67	23	-0.0200	-0.46		675.21	
676.20		0.46	0.0200	23	511+25.00	675.74	23	-0.0200	-0.46		675.28	
676.27		0.46	0.0200	23	511+50.00	675.81	23	-0.0200	-0.46		675.35	
676.30		0.46	0.0200	23	511+75.00	675.84	23	-0.0200	-0.46		675.38	
676.32		0.46	0.0200	23	512+00.00	675.86	23	-0.0200	-0.46		675.40	
676.30		0.46	0.0200	23	512+25.00	675.84	23	-0.0200	-0.46		675.38	
676.27		0.46	0.0200	23	512+50.00	675.81	23	-0.0200	-0.46		675.35	
676.20		0.46	0.0200	23	512+75.00	675.74	23	-0.0200	-0.46		675.28	
676.13		0.46	0.0200	23	513+00.00	675.67	23	-0.0200	-0.46		675.21	
676.05		0.46	0.0200	23	513+25.00	675.59	23	-0.0200	-0.46		675.13	
675.98		0.46	0.0200	23	513+50.00	675.52	23	-0.0200	-0.46		675.06	

H:\2018\80743\00T\Project\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CE001.dgn Sheet 7/15/2022 2:57:34 PM Troyer

SUPERELEVATION TABLE

P.I. STA. 510+71.30

Dc=1°00'00"

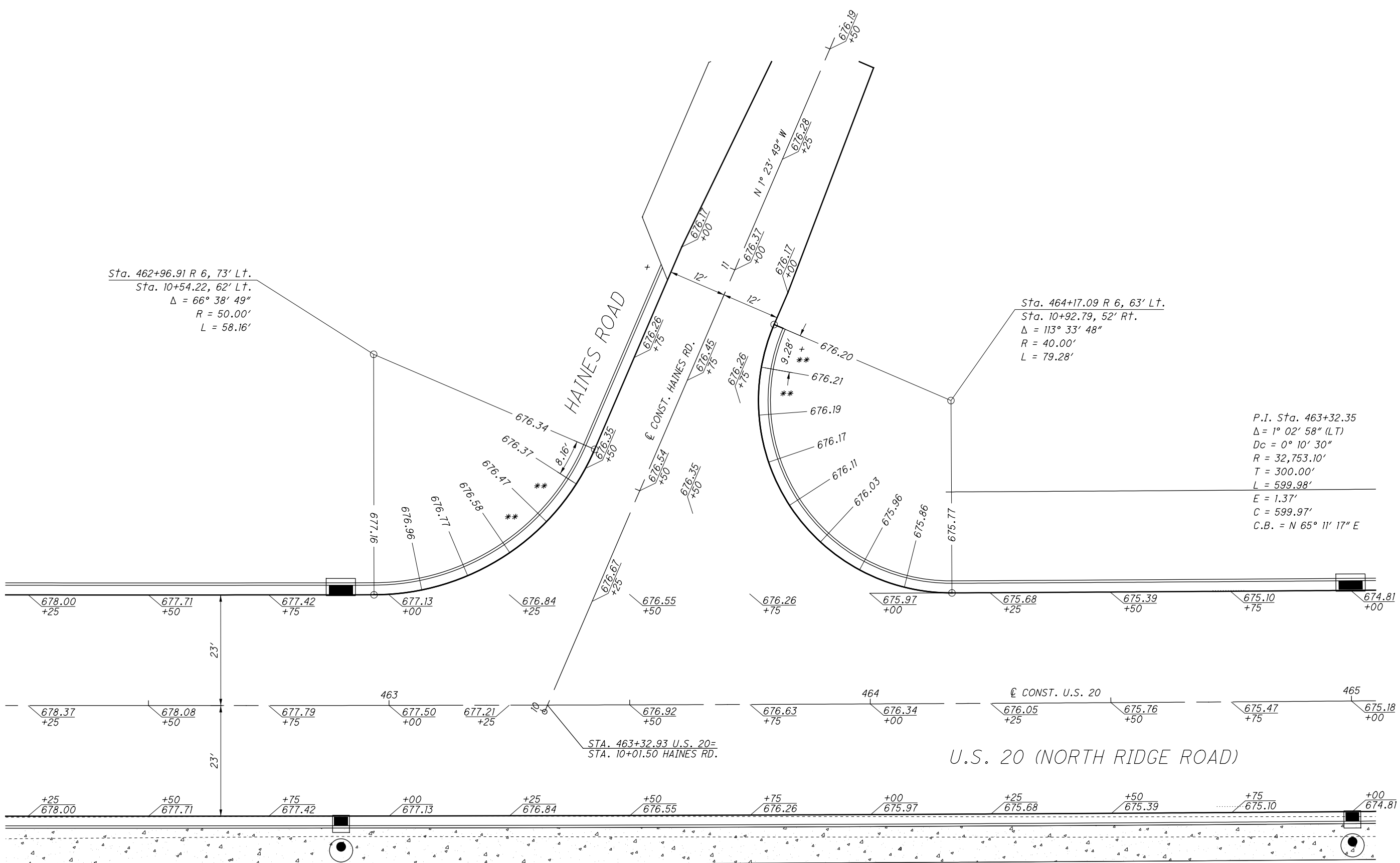
LEFT SIDE					CENTERLINE CONTROL		RIGHT SIDE					REMARKS
EDGE ELEVATION	TRANSITION RATE	ELEVATION CORRECTION	CROSS SLOPE	WIDTH	STATION	PROFILE GRADE	WIDTH	CROSS SLOPE	ELEVATION CORRECTION	TRANSITION RATE	EDGE ELEVATION	
675.90		0.46	0.0200	23	513+75.00	675.44	23	-0.0200	-0.46		674.98	
675.83		0.46	0.0200	23	514+00.00	675.37	23	-0.0200	-0.46		674.91	
675.75		0.46	0.0200	23	514+25.00	675.29	23	-0.0200	-0.46		674.83	
675.68		0.46	0.0200	23	514+50.00	675.22	23	-0.0200	-0.46		674.76	
675.60		0.46	0.0200	23	514+75.00	675.14	23	-0.0200	-0.46		674.68	
675.53		0.46	0.0200	23	515+00.00	675.07	23	-0.0200	-0.46		674.61	
675.45		0.46	0.0200	23	515+25.00	674.99	23	-0.0200	-0.46		674.53	
675.38		0.46	0.0200	23	515+50.00	674.92	23	-0.0200	-0.46		674.46	
675.30		0.46	0.0200	23	515+75.00	674.84	23	-0.0200	-0.46		674.38	
675.23		0.46	0.0200	23	516+00.00	674.77	23	-0.0200	-0.46		674.31	
675.15		0.46	0.0200	23	516+25.00	674.69	23	-0.0200	-0.46		674.23	
675.08		0.46	0.0200	23	516+50.00	674.62	23	-0.0200	-0.46		674.16	
675.00		0.46	0.0200	23	516+75.00	674.54	23	-0.0200	-0.46		674.08	
674.93		0.46	0.0200	23	517+00.00	674.47	23	-0.0200	-0.46		674.01	
674.85		0.46	0.0200	23	517+25.00	674.39	23	-0.0200	-0.46		673.93	
674.78		0.46	0.0200	23	517+50.00	674.32	23	-0.0200	-0.46		673.86	
674.70		0.46	0.0200	23	517+75.00	674.24	23	-0.0200	-0.46		673.78	
674.63		0.46	0.0200	23	518+00.00	674.17	23	-0.0200	-0.46		673.71	
674.55		0.46	0.0200	23	518+25.00	674.09	23	-0.0200	-0.46		673.63	
674.48		0.46	0.0200	23	518+50.00	674.02	23	-0.0200	-0.46		673.56	
674.40		0.46	0.0200	23	518+75.00	673.94	23	-0.0200	-0.46		673.48	
674.33		0.46	0.0200	23	519+00.00	673.87	23	-0.0200	-0.46		673.41	
674.25		0.46	0.0200	23	519+25.00	673.79	23	-0.0200	-0.46		673.33	
674.18		0.46	0.0200	23	519+50.00	673.72	23	-0.0200	-0.46		673.26	
674.10		0.46	0.0200	23	519+75.00	673.64	23	-0.0200	-0.46		673.18	
674.03		0.46	0.0200	23	520+00.00	673.57	23	-0.0200	-0.46		673.11	
674.00		0.46	0.0200	23	520+09.28	673.54	23	-0.0200	-0.46		673.08	FULL
673.87		0.37	0.0160	23	520+23.68	673.50	23	-0.0160	-0.37		673.13	RC
673.85		0.36	0.0156	23	520+25.00	673.49	23	-0.0160	-0.37		673.13	
673.78		0.31	0.0133	23	520+33.28	673.47	23	-0.0160	-0.37		673.10	PT
673.62		0.20	0.0087	23	520+50.00	673.42	23	-0.0160	-0.37		673.05	
673.38		0.04	0.0017	23	520+75.00	673.34	23	-0.0160	-0.37		672.98	
673.32		0.00	0.0000	23	520+81.28	673.32	23	-0.0160	-0.37		672.95	HALF
673.15		-0.12	-0.0052	23	521+00.00	673.27	23	-0.0160	-0.37		672.90	
672.91		-0.28	-0.0121	23	521+25.00	673.19	23	-0.0160	-0.37		672.83	
672.78		-0.37	-0.0160	23	521+38.88	673.15	23	-0.0160	-0.37		672.78	NC

CALCULATED
JMP
CHECKED
WDB

SUPERELEVATION TABLE

LAK-US-20-24.99
PART 2

H:\2018\180743\0010\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_0100.dgn Sheet 7/15/2022 2:57:37 PM troyer



NOTE:
 ALL CURB RETURN ELEVATIONS ARE MEASURED AT TEN FOOT (10') INTERVALS, UNLESS OTHERWISE SHOWN.
 ALL ELEVATIONS ARE GIVEN AT THE OUTSIDE EDGE OF GUTTER.

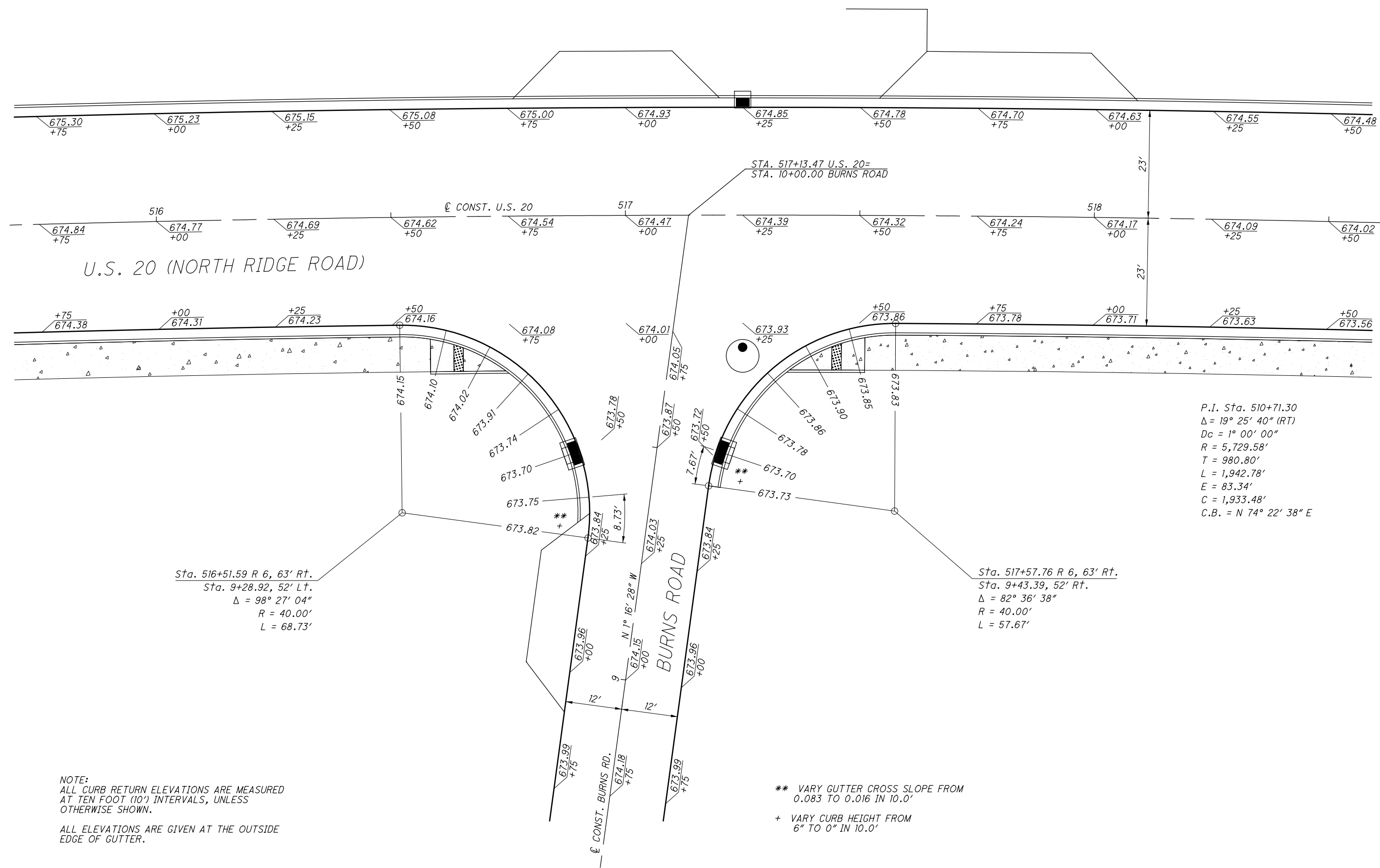
** VARY GUTTER CROSS SLOPE FROM 0.083 TO 0.016 IN 20.0'
 + VARY CURB HEIGHT FROM 6" TO 0" IN 10.0'

CALCULATED
 JIMP
 CHECKED
 WDB

0 5 10 20
 HORIZONTAL SCALE IN FEET

INTERSECTION DETAIL
 U.S. 20 AND HAINES ROAD

H:\2018\180743\0D01\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_G1003.dgn Sheet 7/15/2022 2:57:39 PM Troyer



U.S. 20 (NORTH RIDGE ROAD)

BURNS ROAD

Sta. 516+51.59 R 6, 63' Rt.
 Sta. 9+28.92, 52' Lt.
 $\Delta = 98^\circ 27' 04''$
 $R = 40.00'$
 $L = 68.73'$

Sta. 517+57.76 R 6, 63' Rt.
 Sta. 9+43.39, 52' Rt.
 $\Delta = 82^\circ 36' 38''$
 $R = 40.00'$
 $L = 57.67'$

P.I. Sta. 510+71.30
 $\Delta = 19^\circ 25' 40''$ (RT)
 $Dc = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 980.80'$
 $L = 1,942.78'$
 $E = 83.34'$
 $C = 1,933.48'$
 $C.B. = N 74^\circ 22' 38'' E$

NOTE:
 ALL CURB RETURN ELEVATIONS ARE MEASURED
 AT TEN FOOT (10') INTERVALS, UNLESS
 OTHERWISE SHOWN.
 ALL ELEVATIONS ARE GIVEN AT THE OUTSIDE
 EDGE OF GUTTER.

** VARY GUTTER CROSS SLOPE FROM
 0.083 TO 0.016 IN 10.0'
 + VARY CURB HEIGHT FROM
 6" TO 0" IN 10.0'

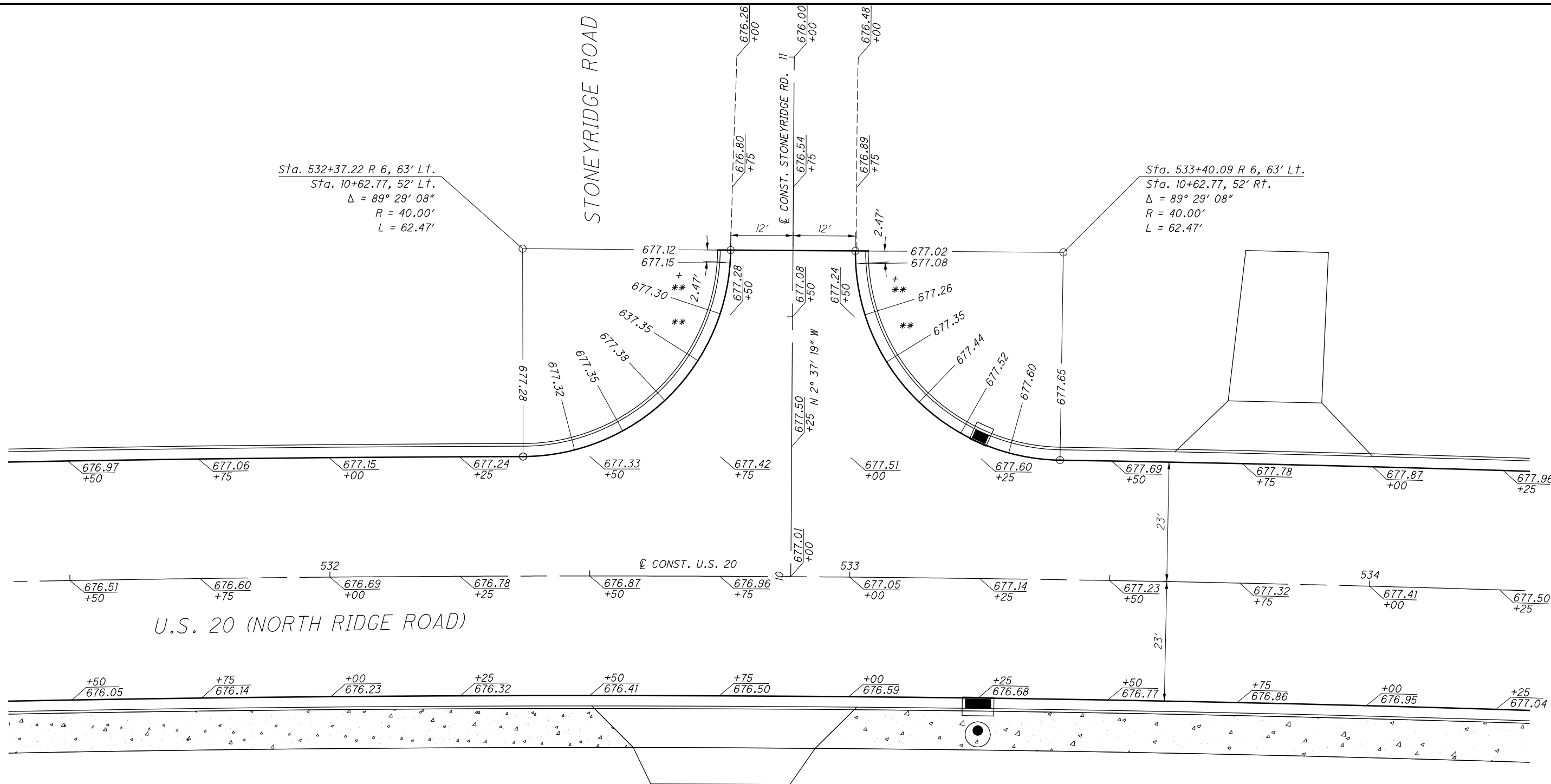


CALCULATED
 JMP
 CHECKED
 WDB

INTERSECTION DETAIL
 U.S. 20 AND BURNS ROAD

LAK-US-20-24.99
 PART 2

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_GI004.dgn Sheet 7/15/2022 2:57:39 PM troyer



Sta. 532+37.22 R 6, 63' Lt.
 Sta. 10+62.77, 52' Lt.
 $\Delta = 89^\circ 29' 08''$
 $R = 40.00'$
 $L = 62.47'$

Sta. 533+40.09 R 6, 63' Lt.
 Sta. 10+62.77, 52' Rt.
 $\Delta = 89^\circ 29' 08''$
 $R = 40.00'$
 $L = 62.47'$

U.S. 20 (NORTH RIDGE ROAD)

CONST. U.S. 20

** VARY GUTTER CROSS SLOPE FROM
 0.083 TO 0.016 IN 20.0'
 + VARY CURB HEIGHT FROM
 6" TO 0" IN 10.0'

P.I. Sta. 536+59.39
 $\Delta = 13^\circ 55' 10''$ (RT)
 $D_c = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 699.42'$
 $L = 1,391.94'$
 $E = 42.53'$
 $C = 1,388.52'$
 $C.B. = S 88^\circ 56' 57'' E$

NOTE:
 ALL CURB RETURN ELEVATIONS ARE MEASURED
 AT TEN FOOT (10') INTERVALS, UNLESS
 OTHERWISE SHOWN.
 ALL ELEVATIONS ARE GIVEN AT THE OUTSIDE
 EDGE OF GUTTER.

CALCULATED
 JMP
 CHECKED
 WDB

0 5 10
 HORIZONTAL
 SCALE IN FEET

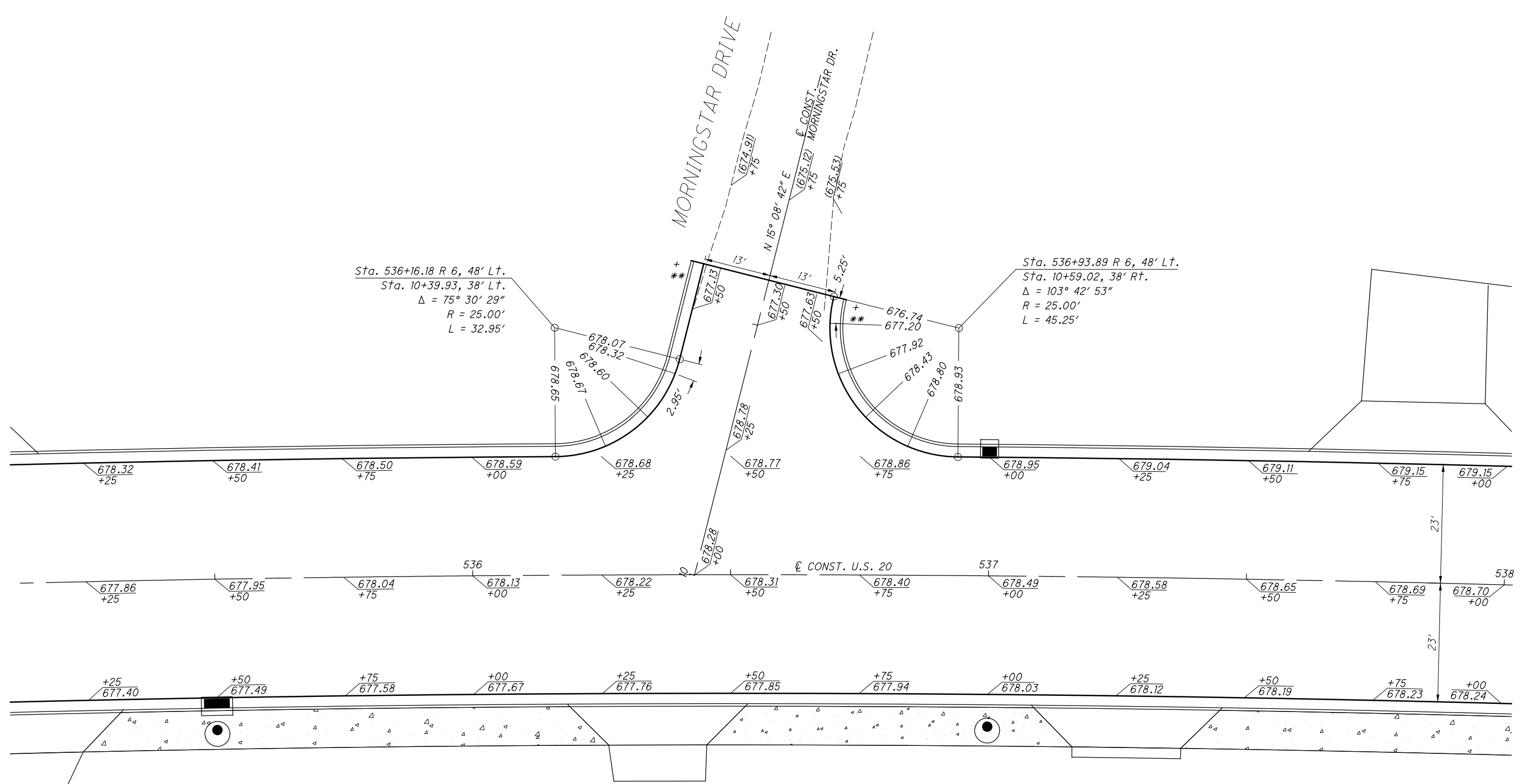
INTERSECTION DETAIL
 U.S. 20 AND STONEYRIDGE ROAD

LAK-US-20-24.99
 PART 2

H:\2018\180743\001\Project\Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_01005.dgn Sheet 7/15/2022 2:51:40 PM troyer

CALCULATED
JMP
CHECKED
WDB

0 5 10 20
HORIZONTAL
SCALE IN FEET



NOTE:
ALL CURB RETURN ELEVATIONS ARE MEASURED AT TEN FOOT (10') INTERVALS, UNLESS OTHERWISE SHOWN.
ALL ELEVATIONS ARE GIVEN AT THE OUTSIDE EDGE OF GUTTER.

** VARY GUTTER CROSS SLOPE FROM 0.083 TO 0.016 IN 10.0'
+ VARY CURB HEIGHT FROM 6" TO 0" IN 10.0'

P.I. Sta. 536+59.39
Delta = 13° 55' 10" (RT)
Dc = 1° 00' 00"
R = 5,729.58'
T = 699.42'
L = 1,391.94'
E = 42.53'
C = 1,388.52'
C.B. = S 88° 56' 57" E

INTERSECTION DETAIL
U.S. 20 AND MORNINGSTAR DRIVE

LAK-US-20-24.99
PART 2

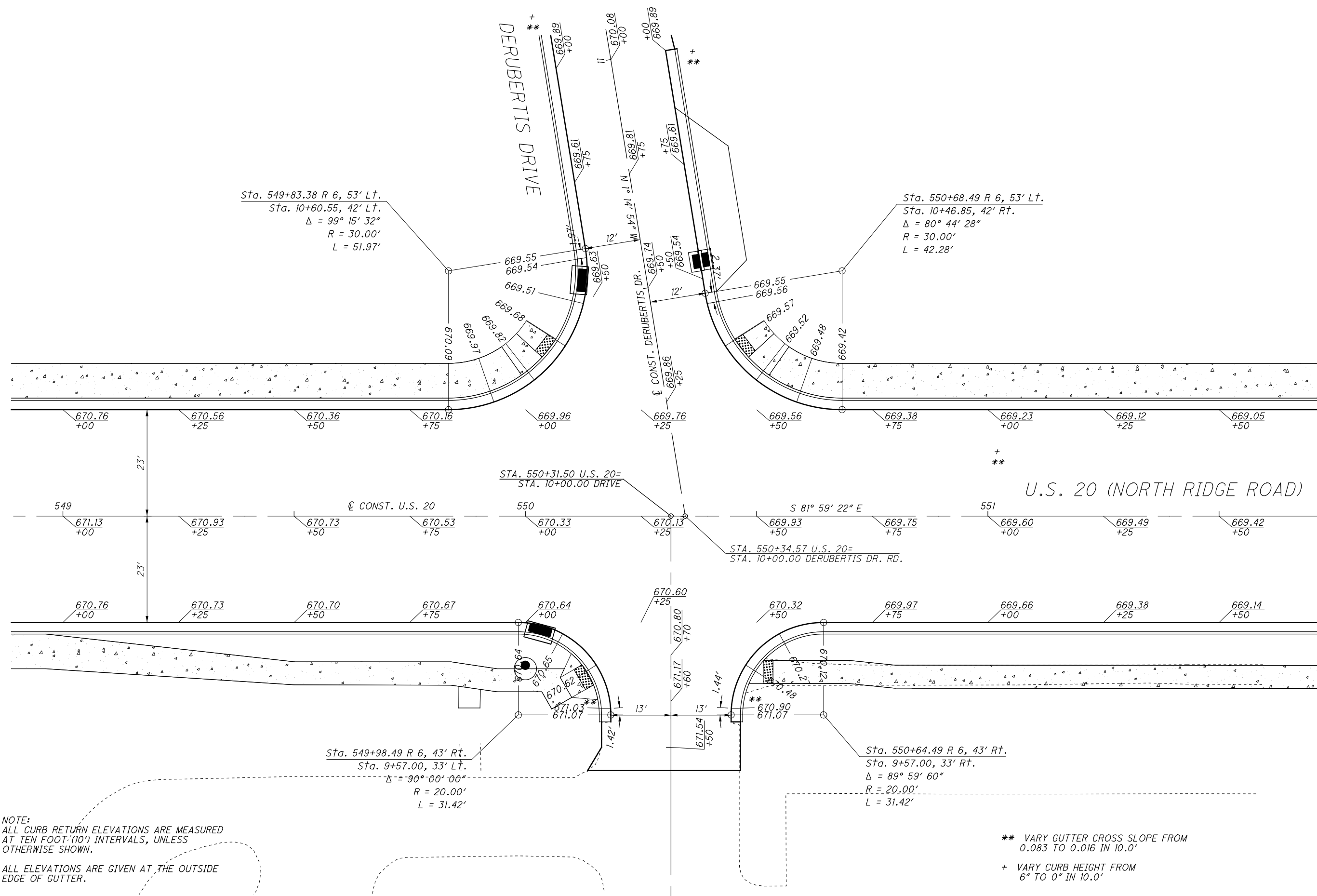
H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_G1006.dgn Sheet 7/15/2022 2:57:41PM troyer

CALCULATED
JMP
CHECKED
WDB

0 5 10 20
HORIZONTAL
SCALE IN FEET

INTERSECTION DETAIL
U.S. 20 AND DERUBERTIS DRIVE ROAD

LAK-US-20-24.99
PART 2



NOTE:
ALL CURB RETURN ELEVATIONS ARE MEASURED
AT TEN FOOT (10') INTERVALS, UNLESS
OTHERWISE SHOWN.

ALL ELEVATIONS ARE GIVEN AT THE OUTSIDE
EDGE OF GUTTER.

** VARY GUTTER CROSS SLOPE FROM
0.083 TO 0.016 IN 10.0'

+ VARY CURB HEIGHT FROM
6" TO 0" IN 10.0'

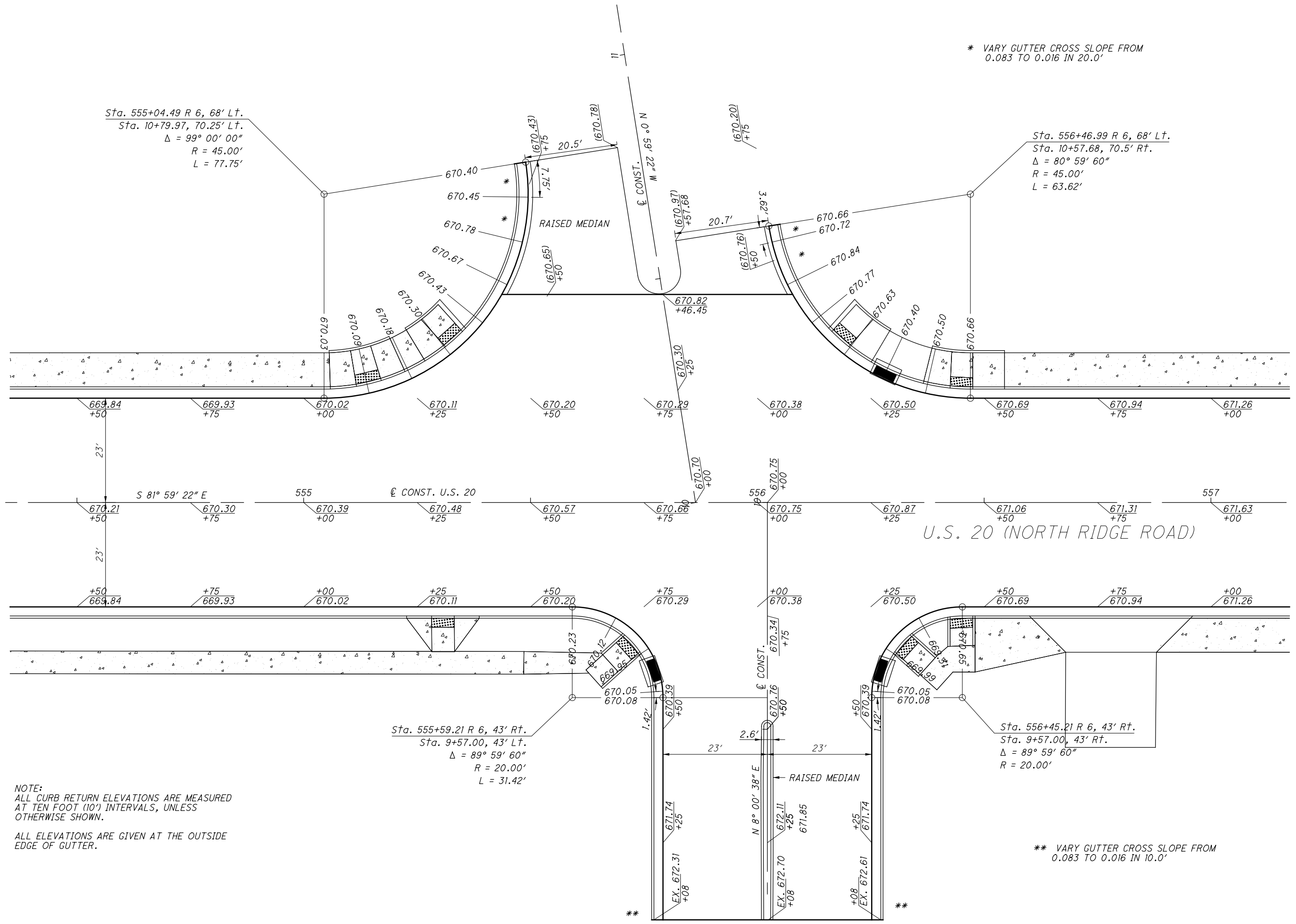
H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_c007.dgn Sheet 7/15/2022 2:57:42 PM Troyer



INTERSECTION DETAIL
 U.S. 20 AND HUNTINGTON / ANDOVER BANK DR.

LAK-US-20-24.99
 PART 2

419
697



* VARY GUTTER CROSS SLOPE FROM 0.083 TO 0.016 IN 20.0'

Sta. 555+04.49 R 6, 68' Lt.
 Sta. 10+79.97, 70.25' Lt.
 $\Delta = 99^\circ 00' 00''$
 $R = 45.00'$
 $L = 77.75'$

Sta. 556+46.99 R 6, 68' Lt.
 Sta. 10+57.68, 70.5' Rt.
 $\Delta = 80^\circ 59' 60''$
 $R = 45.00'$
 $L = 63.62'$

Sta. 555+59.21 R 6, 43' Rt.
 Sta. 9+57.00, 43' Lt.
 $\Delta = 89^\circ 59' 60''$
 $R = 20.00'$
 $L = 31.42'$

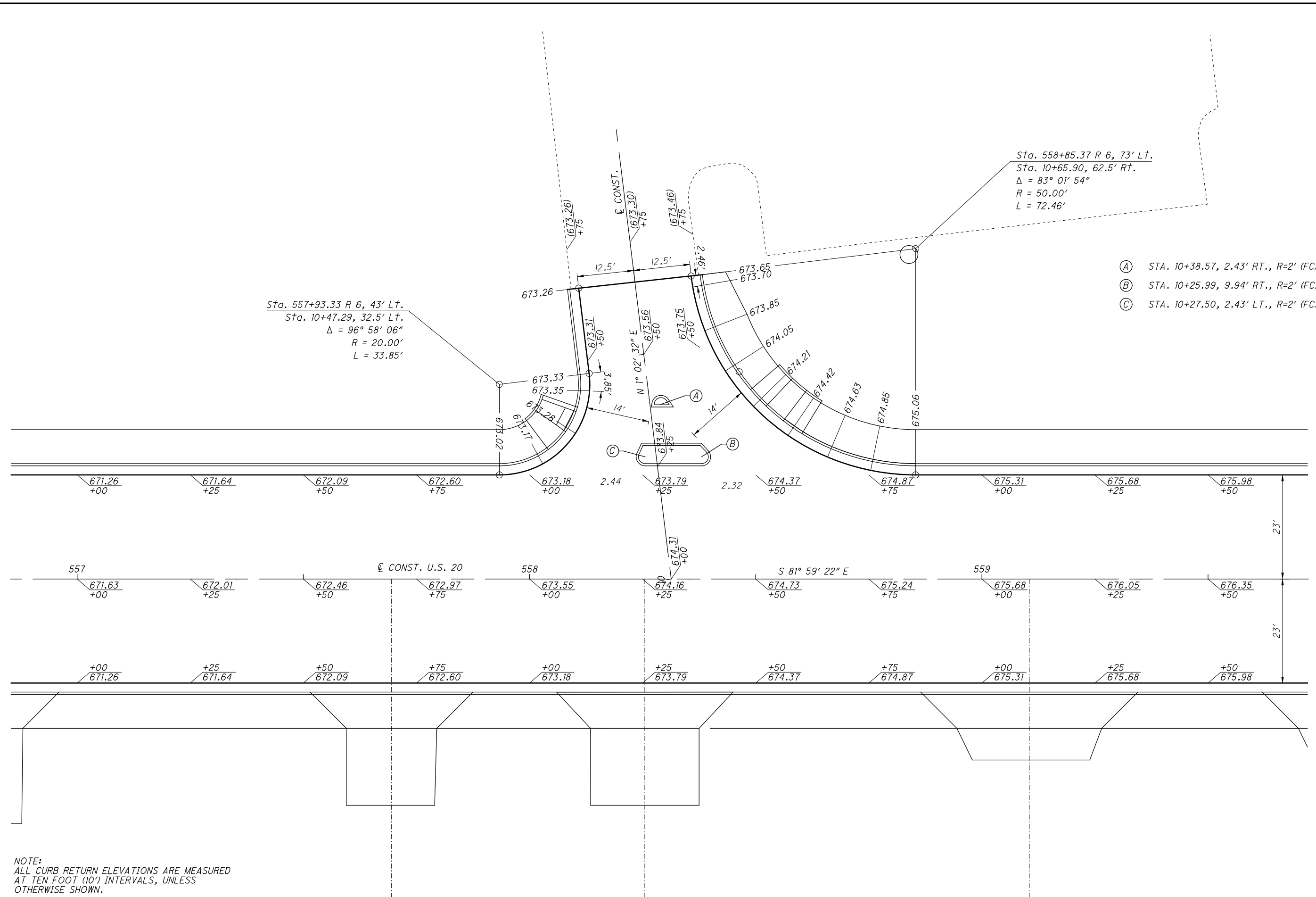
Sta. 556+45.21 R 6, 43' Rt.
 Sta. 9+57.00, 43' Rt.
 $\Delta = 89^\circ 59' 60''$
 $R = 20.00'$

NOTE:
 ALL CURB RETURN ELEVATIONS ARE MEASURED AT TEN FOOT (10') INTERVALS, UNLESS OTHERWISE SHOWN.

ALL ELEVATIONS ARE GIVEN AT THE OUTSIDE EDGE OF GUTTER.

** VARY GUTTER CROSS SLOPE FROM 0.083 TO 0.016 IN 10.0'

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_G1008.dgn Sheet 7/15/2022 2:51:43 PM troyer



NOTE:
 ALL CURB RETURN ELEVATIONS ARE MEASURED
 AT TEN FOOT (10') INTERVALS, UNLESS
 OTHERWISE SHOWN.
 ALL ELEVATIONS ARE GIVEN AT THE OUTSIDE
 EDGE OF GUTTER.

CALCULATED
 JUMP
 CHECKED
 WDB

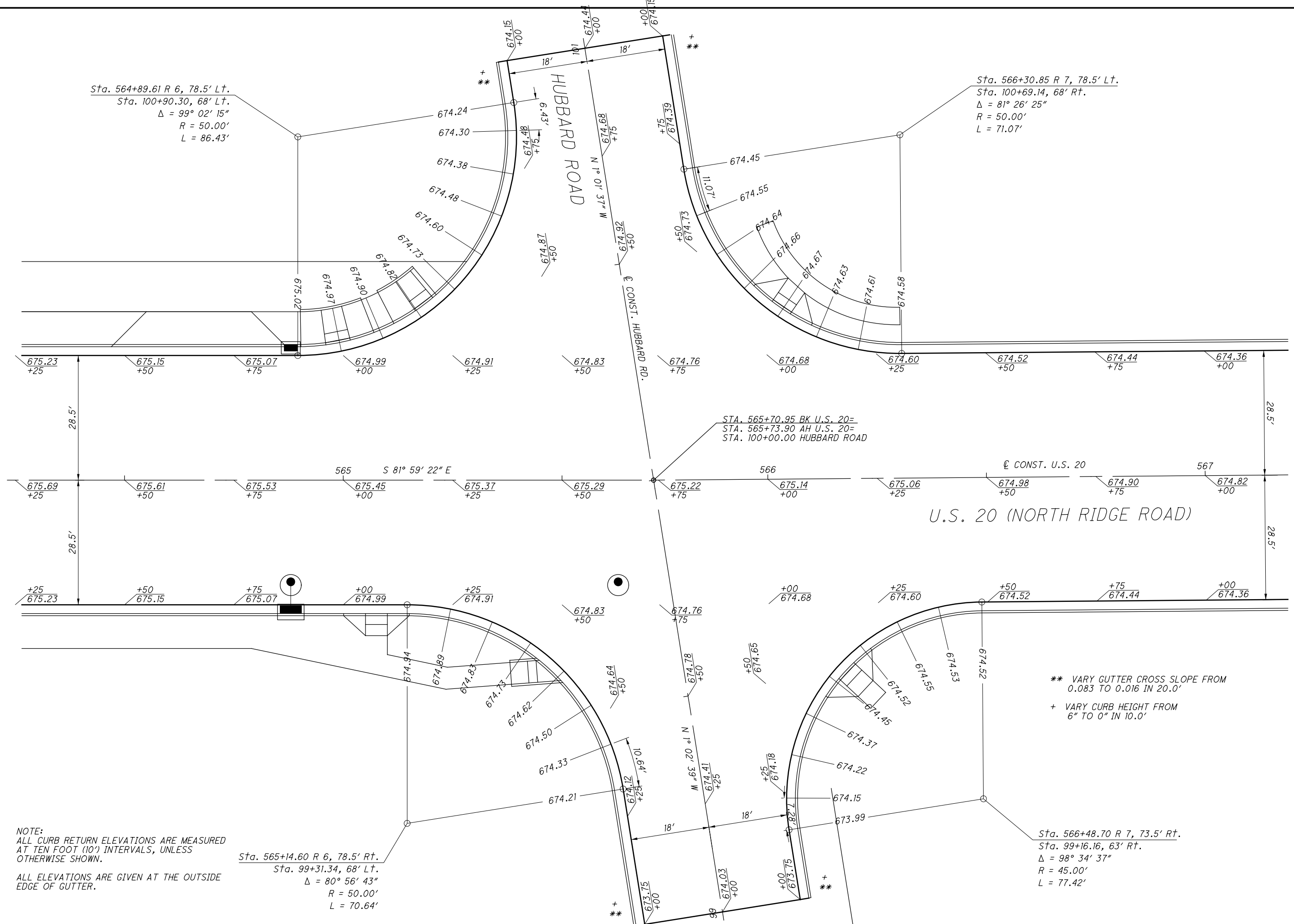
0 5 10 20
 HORIZONTAL
 SCALE IN FEET

INTERSECTION DETAIL
 U.S. 20 AND TRACTOR SUPPLY DRIVE

LAK-US-20-24.99
 PART 2

420
 697

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\Roadway_Sheets\09270_G1009.dgn Sheet 7/15/2022 2:51:44 PM troyer



Sta. 564+89.61 R 6, 78.5' Lt.
 Sta. 100+90.30, 68' Lt.
 $\Delta = 99^\circ 02' 15''$
 $R = 50.00'$
 $L = 86.43'$

Sta. 566+30.85 R 7, 78.5' Lt.
 Sta. 100+69.14, 68' Rt.
 $\Delta = 81^\circ 26' 25''$
 $R = 50.00'$
 $L = 71.07'$

STA. 565+70.95 BK U.S. 20=
 STA. 565+73.90 AH U.S. 20=
 STA. 100+00.00 HUBBARD ROAD

** VARY GUTTER CROSS SLOPE FROM
 0.083 TO 0.016 IN 20.0'
 + VARY CURB HEIGHT FROM
 6" TO 0" IN 10.0'

Sta. 566+48.70 R 7, 73.5' Rt.
 Sta. 99+16.16, 63' Rt.
 $\Delta = 98^\circ 34' 37''$
 $R = 45.00'$
 $L = 77.42'$

Sta. 565+14.60 R 6, 78.5' Rt.
 Sta. 99+31.34, 68' Lt.
 $\Delta = 80^\circ 56' 43''$
 $R = 50.00'$
 $L = 70.64'$

NOTE:
 ALL CURB RETURN ELEVATIONS ARE MEASURED
 AT TEN FOOT (10') INTERVALS, UNLESS
 OTHERWISE SHOWN.
 ALL ELEVATIONS ARE GIVEN AT THE OUTSIDE
 EDGE OF GUTTER.

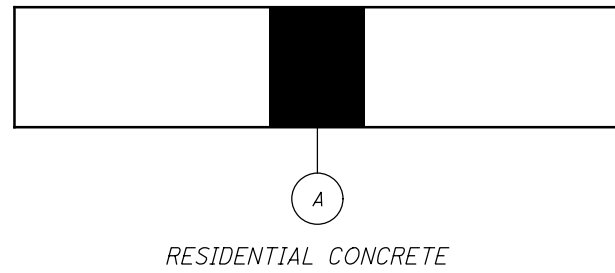
CALCULATED
 JIMP
 CHECKED
 WDB

0 5 10 20
 HORIZONTAL
 SCALE IN FEET

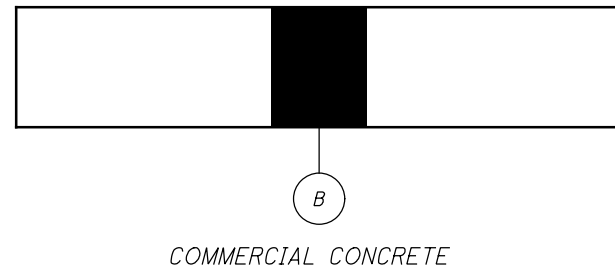
INTERSECTION DETAIL
 U.S. 20 AND HUBBARD ROAD

LAK-US-20-24.99
 PART 2

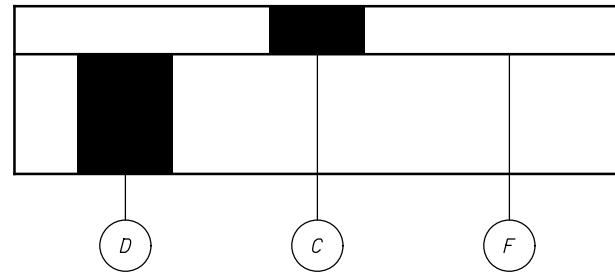
H:\2018\180743\000T\Proj\ec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CD001.dgn DETAILS 7/15/2022 2:57:45 PM Troyer



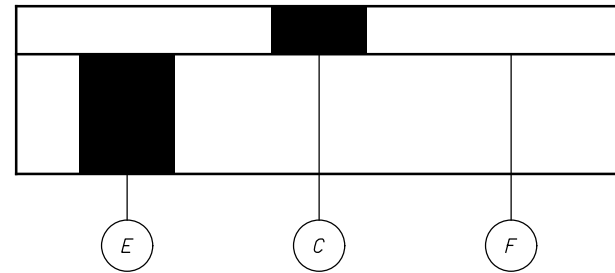
RESIDENTIAL CONCRETE



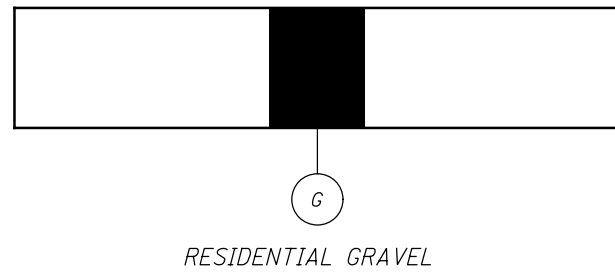
COMMERCIAL CONCRETE



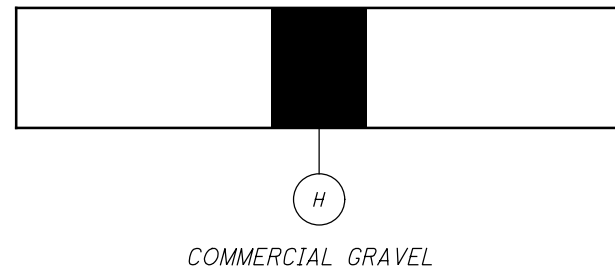
RESIDENTIAL ASPHALT



COMMERCIAL ASPHALT



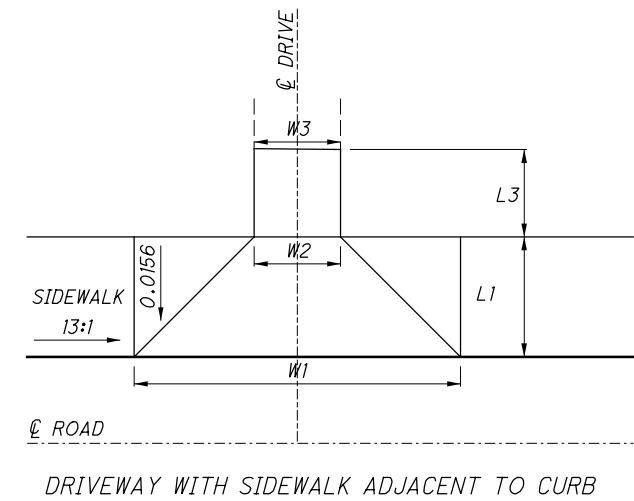
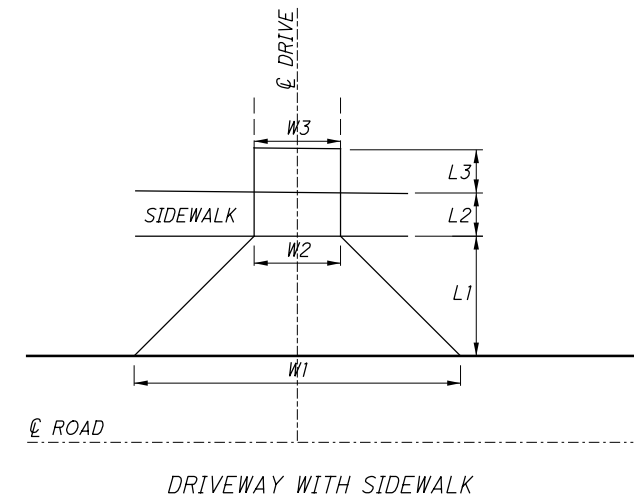
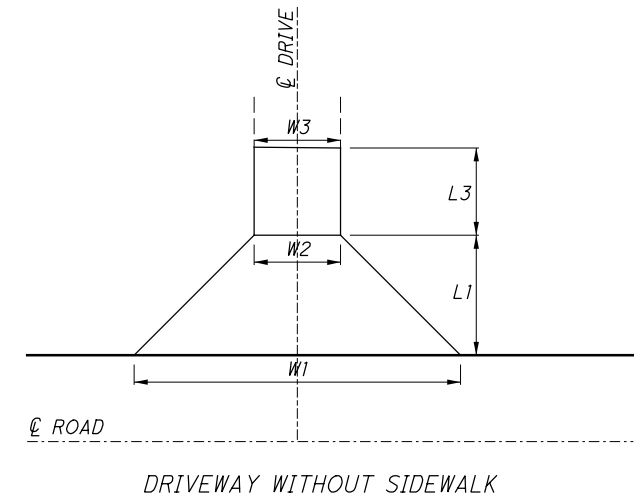
RESIDENTIAL GRAVEL



COMMERCIAL GRAVEL

LEGEND

- (A) ITEM 452 - 6" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS
- (B) ITEM 452 - 8" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS
- (C) ITEM 441 - 1 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), (DRIVEWAYS)
- (D) ITEM 301 - 3 1/2" ASPHALT CONCRETE BASE PG64-22, (449), (DRIVEWAYS)
- (E) ITEM 301 - 5" ASPHALT CONCRETE BASE PG64-22, (449), (DRIVEWAYS)
- (F) ITEM 407 - NON-TRACKING TACK COAT
- (G) ITEM 304 - 8" AGGREGATE BASE
- (H) ITEM 304 - 10" AGGREGATE BASE



CALCULATED
JMP
CHECKED
WDB

DRIVEWAY DETAILS

**LAK-US-20-24.99
PART 2**

422
697

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_C0002.dgn Sheet 7/15/2022 2:57:47 PM Troyer

SHEET NO.	REFERENCE NO.	STATION	SIDE	COMMERCIAL "C" RESIDENTIAL "R" CONCRETE "C" ASPHALT "A" GRAVEL "G"	DRIVE ANGLE		APRON LENGTH "L1" FT	WALK DRIVEWAY LENGTH "L2" FT	DRIVEWAY LENGTH "L3" FT	APRON WIDTH "W1" FT	APRON WIDTH "W2" FT	DRIVE WIDTH "W3" FT	CALCULATED APRON SURFACE AREA SF	CADD GENERATED APRON SURFACE AREA SF	CALCULATED DRIVE SURFACE AREA SF	CADD GENERATED DRIVE SURFACE AREA SF	202	203	204	252	301		304		407	441	452	452	609																		
					PAVEMENT REMOVED, AS PER PLAN SY	EXCAVATION CY											SUBGRADE COMPACTION SY	FULL DEPTH PAVEMENT SAWING FT	3.5" ASPHALT CONCRETE BASE, PG64-22, (449), (DIVEWAYS) CY	5" ASPHALT CONCRETE BASE, PG64-22, (449), (DIVEWAYS) CY	8" AGGREGATE BASE CY	10" AGGREGATE BASE CY	NON-TRACKING TACK COAT GAL	1.25" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), (DIVEWAYS) CY	6" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS SY	8" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS SY	CURB, TYPE 6 FT																				
256	DR-56	485+38.50	L	R	A		9.5		17	32	13	13.9	213.8			221.0	51.7		48.3	13.0	2.4			1.5	0.9	23.8																					
256	DR-57	487+65.44	L	C	A		12.4		17	39	23		384.4				50.6	1.1	42.7	23.0							42.7																				
259	DR-58	500+92.00	R	C	A		7.5		10	39	24	21	292.5		225.0		65.9	0.6	57.5	24.0				1.5	0.9		32.5																				
259	DR-59	501+33.50	R	C	A		7.5		10	40	25	25	300.0		250.0		148.1		61.1	25.0				1.7	1.0		33.3																				
259	DR-60	502+33.00	R	C	A		7.5		10	40	25	25	300.0		250.0		71.7	0.4	61.1	25.0				1.7	1.0		33.3																				
260	DR-61	504+44.00	R	R	C		7.5		7	28	13	11.8	210.0		86.8		25.8	1.2	33.0	13.0						33.0																					
260	DR-62	505+14.50	R	R	C		7.5		7	29.5	14.5	14.4	221.3		101.2		31.0	0.8	35.8	14.5						35.8																					
260	DR-63	505+80.00	R	C	C		7.5		7	44.5	29.5	29.8	333.8		207.6		63.6	2.8	60.2	29.5							60.2																				
260	DR-64	508+24.00	R	C	A		7.5		9	50	35	30.3	375.0		293.9		102.3		74.3	35.0				2.0	1.1		41.7																				
260	DR-65	508+85.50	L	C	A		9.5		9	54	35	24	422.8		265.5		123.4		76.5	35.0				1.8	1.0		47.0																				
261	DR-66	510+14.50	R	R	C		7.5		11	36	21		213.8		115.5		9.0	4.6	36.6	21.0						36.6																					
261	DR-67	510+21.71	L	C	G		9.5		3.5	27	12	9.7	185.3		38.0		24.6	5.8	24.8								20.6																				
261	DR-68	512+40.95	L	C	G		9.5		3.5	23	24	23.9	223.3		83.8		48.6	8.1	34.1				1.2				24.8																				
261	DR-69	513+26.00	R	C	G		7.5		7	43	28	28	266.3		196.0		23.2	12.6	51.4				2.6				29.6																				
261	DR-70	513+28.00	L	C	G		9.5		7	52	35	33.5	413.3		239.8		86.1	17.6	72.6				6.0				45.9																				
262	DR-71	514+07.00	R	C	G		7.5		7	50	35	35.4	318.8		246.4		33.6	15.5	62.8				7.4				35.4																				
262	DR-72	514+66.00	L	C	A		9.5		7	49	30	30.4	375.3		211.4		74.6	0.9	65.2	30.0							41.7																				
262	DR-73	515+39.50	R	C	A		7.5		7	46	31	27.6	288.8		205.1		63.7	0.5	54.9	31.0				1.4	0.8		32.1																				
262	DR-74	516+98.00	L	C			9.5			43	24		318.3					7.9	35.4								35.4																				
262	DR-75	517+81.50	L	C	A		9.5			54	35		422.8				115.9		47.0	35.0							47.0																				
263	DR-76	519+10.50	R	C	A		7.5		9	34	19	16.5	198.8		159.8		55.1		39.8	19.0			2.5			1.1	0.6	22.1																			
263	DR-77	519+86.00	L	C	C		9.5		9	54	35	31	422.8		297.0		120.2		80.0	35.0							80.0																				
263	DR-78	519+94.00	R	C	A		7.5		9	32	17	16.6	183.8		151.2		48.2		37.2	17.0			2.3			1.0	0.6	20.4																			
263	DR-79	520+89.00	R	C	C		7.5		9	50	35	25.4	318.8		271.8		93.1		65.6	35.0							65.6																				
263	DR-80	523+96.00	R	C	C		7.5		7	50	35	31.7	318.8		233.5		74.6	1.2	61.4	35.0							61.4																				
264	DR-81	525+31.00	L	C	A		9.5		9	54	35	36.2	422.8		320.4		126.4		82.6	35.0			4.9			2.1	1.2	47.0																			
264	DR-82	525+32.00	R	C	A		7.5		7	50	35	33.4	318.8		239.4		83.6		62.0	35.0			3.7			1.6	0.9	35.4																			
264	DR-83	527+46.00	L	C	A		9.5		7	50.5	31.5	33.4	389.5		227.2		74.8	1.6	68.5	31.5			3.5			1.5	0.9	43.3																			
264	DR-84	528+54.00	L	C	A		9.5		9	54	35	40.8	422.8		341.1		91.7	1.8	84.9	35.0			5.3			2.3	1.3	47.0																			
264	DR-85	529+00.00	R	C	A		7.5		7	50	35	35	318.8		245.0		119.7		62.6	35.0			3.8			1.6	0.9	35.4																			
265	DR-86	530+24.00	L	R	G		9.5		3	31	12	12	204.3		36.0		26.8	4.7	26.7				0.9					22.7																			
265	DR-87	532+76.00	R	R	A		9.5		7	50	35	26.8	403.8		216.3		76.4		68.9	35.0	2.3					1.4	0.8	44.9																			
265	DR-88	533+81.00	L	C	A		9.5		29	37	18	15.9	261.3		491.6		93.0	0.4	83.7	18.0			7.6			3.3	1.9	29.0																			
266	DR-89	534+92.50	L	C	A		9.5		29	47	28	16.1	356.3		639.5		123.4	0.6	110.6	28.0			9.9			4.3	2.5	39.6																			
266	DR-90	535+07.00	R	C	A		7.5		7	50	35	25	318.8		210.0		69.0	0.4	58.8	35.0			3.2			1.4	0.8	35.4																			
266	DR-91	536+35.80	R	C	A		7.5		7	34	19	17.8	198.8		128.8		39.4	0.8	36.4	19.0			2.0			0.9	0.5	22.1																			
266	DR-92	537+26.50	R	C	A		7.5		2	36	21	21.1	213.8		42.1		27.5	1.6	28.4	21.0			0.7			0.3	0.2	23.8																			
266	DR-93	537+83.50	L	C	A		9.5		25	43	24	23	318.3		570.2		200.8		98.7	24.0			8.8			3.8	2.2	35.4																			
266	DR-94	538+40.00	L	C	A		9.5		19	43	24	24	318.3		458.5		137.0		86.3	24.0			7.1			3.1	1.8	35.4																			
266	DR-95	538+88.00	R	R	G		7.5			39	24		236.3				21.4	4.4	26.3								26.3																				
267	DR-96	540+45.00	R	C	G		7.5		4	41	26	26	251.3		104.0		21.0	9.4	39.5					3.2				27.9																			
267	DR-97	540+90.50	L	C	A		9.5			50.7	31.7		391.4				57.6	0.1	43.5	31.7							43.5																				
267	DR-98	542+70.00	R	C	A		7.5		4	33	18	15.1	191.3		66.2		32.5	0.6	28.6	18.0			1.0			0.4	0.3	21.3																			
267	DR-99	542+89.00	L	C	A		9.5			48	29		365.8				60.4		40.6	29.0							40.6																				
267	DR-100	543+51.50	R	C	A		7.5			40	25	25	243.8		500.0		99.3		82.6	25.0			7.7			3.3	1.9	27.1	40.0																		
268	DR-101	545+62.50	L	C	C		7.5			11	32	17	183.8		173.8		47.3	0.9	39.7	17.0							39.7																				
268	DR-102	547+05.00	R	C	C				6.7		72	72		987.0	482.4		231.4		163.3	72.0							163.3	24.0																			
269	DR-103	551+97.00	L	C	C		7.5		6.5	45	30	30.4	281.3		196.3		63.2	1.3	53.1	30.0							53.1																				
269	DR-104	552+94.50	L	C	C		7.5		8.5	43	28	23.8	266.3		220.2		65.1	1.2	54.1	28.0							54.1																				
269	DR-105	553+08.00	R	C	C				15.8		50	50		763.5	790.0		219.0	1.9	172.6	50.0							172.6	70.0																			
270	DR-106	556+78.00	R	C	C		7.5		21	35	20	20	206.3		420.0		90.7	0.4	69.6	20.0							69.6																				
270	DR-107	557+69.50	R	C	C		7.5		17	35	20	19.5	206.3		335.8		76.2	0.7	60.2	20.0							60.2																				
270	DR-108	558+25.50	R	C	C		7.5		17	39	24	24	236.3		408.0		91.4	0.7	71.6	24.0							71.6																				
271	DR-109	559+10.50	R	C	C		7.5		7	47	32	26	296.3		203.0		715.3		55.5	32.0							55.5																				
271	DR-110	559+79.50	R	C	C		7.5		7	34	19	13.1	198.8		112.4		57.3		34.6	19.0							34.6																				
TOTALS																	4843.0	115.1	3313.8	1286.2	4.7	100.4	0.9	28.0	46.4	26.8	223.1	2216.2	134.0																		
TOTALS CARRIED TO SHEET 425																	4843	116	3314	1287	5	101	1	28	47	27	224	2217	134																		

DRIVEWAY QUANTITIES

LAK-US-20-

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_C0003.dgn Sheet 7/15/2022 2:57:48 PM Troyer

SHEET NO.	REFERENCE NO.	STATION	SIDE	COMMERCIAL "C" RESIDENTIAL "R" CONCRETE "C" ASPHALT "A" GRAVEL "G"	DRIVE ANGLE	APRON LENGTH "L1"	WALK DRIVEWAY LENGTH "L2"	DRIVEWAY LENGTH "L3"	APRON WIDTH "W1"	APRON WIDTH "W2"	DRIVE WIDTH "W3"	CALCULATED APRON SURFACE AREA	CADD GENERATED APRON SURFACE AREA	CALCULATED DRIVE SURFACE AREA	CADD GENERATED DRIVE SURFACE AREA	202	203	204	252	301		304		407	441	452	452	609																		
																PAVEMENT REMOVED, AS PER PLAN	EXCAVATION	SUBGRADE COMPACTION	FULL DEPTH PAVEMENT SAWING	3.5" ASPHALT CONCRETE BASE, PG64-22, (449), (DIVEWAYS)	5" ASPHALT CONCRETE BASE, PG64-22, (449), (DIVEWAYS)	8" AGGREGATE BASE	10" AGGREGATE BASE	NON-TRACKING TACK COAT	1.25" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), (DIVEWAYS)	6" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS	8" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS	CURB, TYPE 6																		
DEG.	FT	FT	FT	FT	FT	FT	SF	SF	SF	SF	SY	CY	SY	FT	CY	CY	CY	CY	GAL	CY	SY	SY	SY	FT																						
271	DR-111	560+70.00	R	C	C	7.5		8	37	22	13.2	221.3		140.8		56.3		40.2	22.0								40.2																			
271	DR-112	561+50.50	L	C	C	7.5		14.5	50	35	33.2	318.8		494.5		99.2	3.6	90.4	35.0								90.4																			
271	DR-113	561+64.00	R	C	C	7.5		8	50	35	35.6	318.8		282.4		89.8		66.8	35.0								66.8																			
271	DR-114	563+87.50	L	C	C	7.5		11.5	50	35		318.8			1635.8	201.2	14.8	217.2	35.0								217.2																			
272	DR-115	564+67.00	L	C	C	7.5		11.5	39	24		236.3				162.7		26.3	24.0								26.3																			
272	DR-116	567+56.00	R	C	C	8.5			46	28		314.5				45.2	0.2	34.9	28.0								34.9																			
272	DR-117	568+22.00	R	C	C	8.5			53	35		374.0				44.3	1.9	41.6	35.0								41.6																			
272	DR-118	568+32.00	L	C	C	8.5		7.5	53	35	33.8	374.0		258.0		91.2	0.4	70.2	35.0								70.2																			
272	DR-119	568+93.00	L	C	C	7.5			36.1	21.8		217.1			998.5	71.5	18.1	135.1	21.8								135.1																			
273	DR-120	569+88.00	L	C	C	7.5			39.7	25.7		245.3				89.7		27.3	25.7								27.3																			
273	DR-121	570+65.00	L	C	C	7.5		5	41.5	26.3	23.9	254.3		125.5		58.0		42.2	26.3								42.2																			
273	DR-122	570+75.70	R	C	C	9.2		3.5	42.4	24	24	305.4		84.0		73.3		43.3	24.0								43.3																			
273	DR-123	571+20.50	R	C	C	9.3		4.4	42.6	24	25.2	309.7		108.2		71.6		46.4	24.0								46.4																			
273	DR-124	571+83.50	R	C	C	9.5		5.6	54	35	35	422.8		196.0		109.6		68.8	35.0								68.8																			
273	DR-125	572+86.40	L	C	C	7.5		3	40	25	25	243.8		75.0		40.0	1.2	35.4	25.0								35.4																			
274	DR-132	574+68.50	L	R	G	7.5		2	27	12	10.4	146.3		22.4		40.0	3.3	18.7			0.6			16.3			16.3																			
274	DR-133	576+00.00	L	C	G	7.5		5	27	12	9.1	146.3		52.8		40.0	5.2	22.1				1.6					22.1																			
274	DR-134	576+47.00	L	C	C	7.5			50	35		318.8				40.0	1.2	35.4	35.0								35.4																			
275	DR-126	Haines 11+20.00	L	C	G	10.9			56	35		496.0				123.8	12.3	55.1									55.1																			
277	DR-127	Bums 9+13.00	L	C	A	9.5			43	24		318.3				51.4		35.4	24.0								35.4																			
278	DR-128	De Rubertis 10+65.00	R	C	C	7.5			40	25		243.8				42.1		27.1	25.0								27.1																			
278	DR-129	11+58.00	R	R	G	13.5			39.7	25.9		442.8				56.6	8.2	49.2						49.2			49.2																			
280	DR-130	Lake Health 524+47.00	R	C	C	25			40	40		1000.0				111.0	24.7	111.1	40.0								111.1																			
280	DR-131	Classic 524+47.00	R	C	A			356		12	12		4272.0			475.0	82.5	474.7	712.0		66.0		28.5	16.5			712.0																			
TOTALS																2283.5	177.6	1814.9	1266.8	66.0	0.6	1.6	28.5	16.5	65.5	1266.5																				
TOTALS THIS SHEET																2284	178	1815	1267	66	1	2	29	17	66	1267																				
TOTALS SHEET 423																4252	375	3410	788	12	74	37	39	23	711	1300																				
TOTALS SHEET 424																4843	116	3314	1287	5	101	1	28	47	27	224	134																			
TOTALS CARRIED TO GENERAL SUMMARY																11379	669	8539	3342	258	233	115	67	1001	4784	134																				

DRIVEWAY QUANTITIES

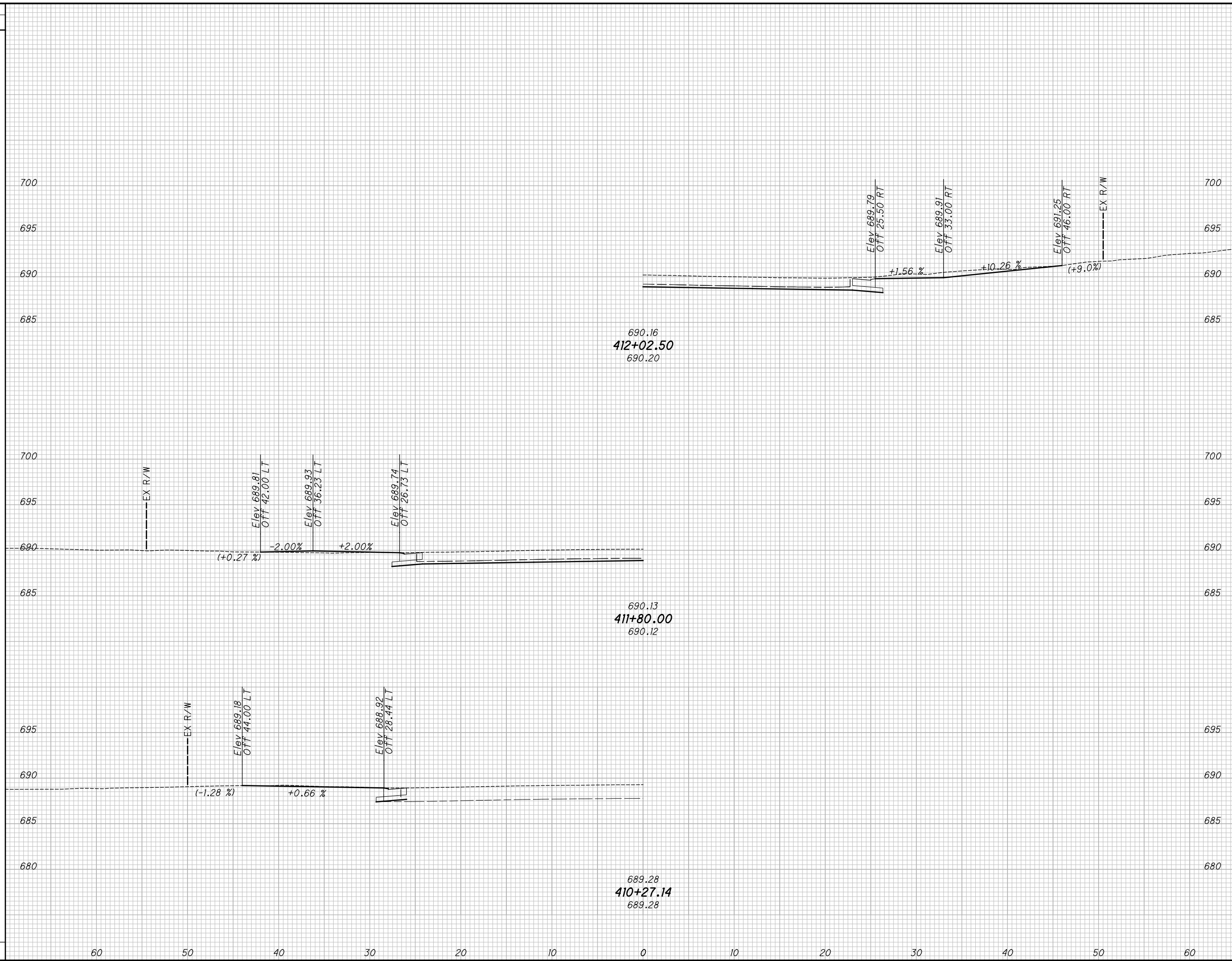
LAK-US-20-24.99 PART 2

CALCULATED
LIME
CHECKED
JMP

425
697

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_1 7/15/2022 2:57:49 PM Troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

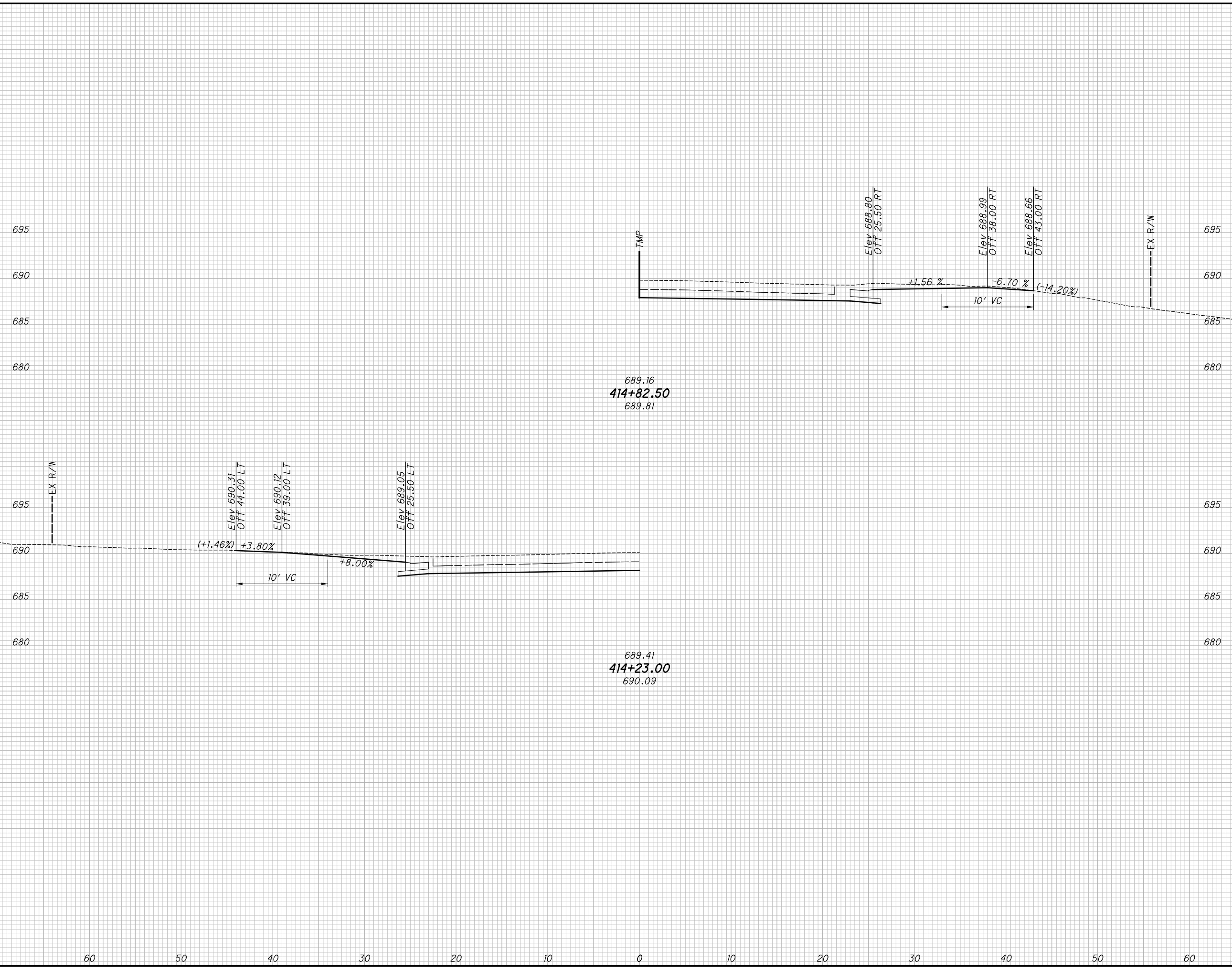
DRIVEWAY PROFILES
STA. 411+80.00 TO STA. 412+02.50

LAK-US-20-24.99
PART 2

426
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_2 7/15/2022 2:57:51PM troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

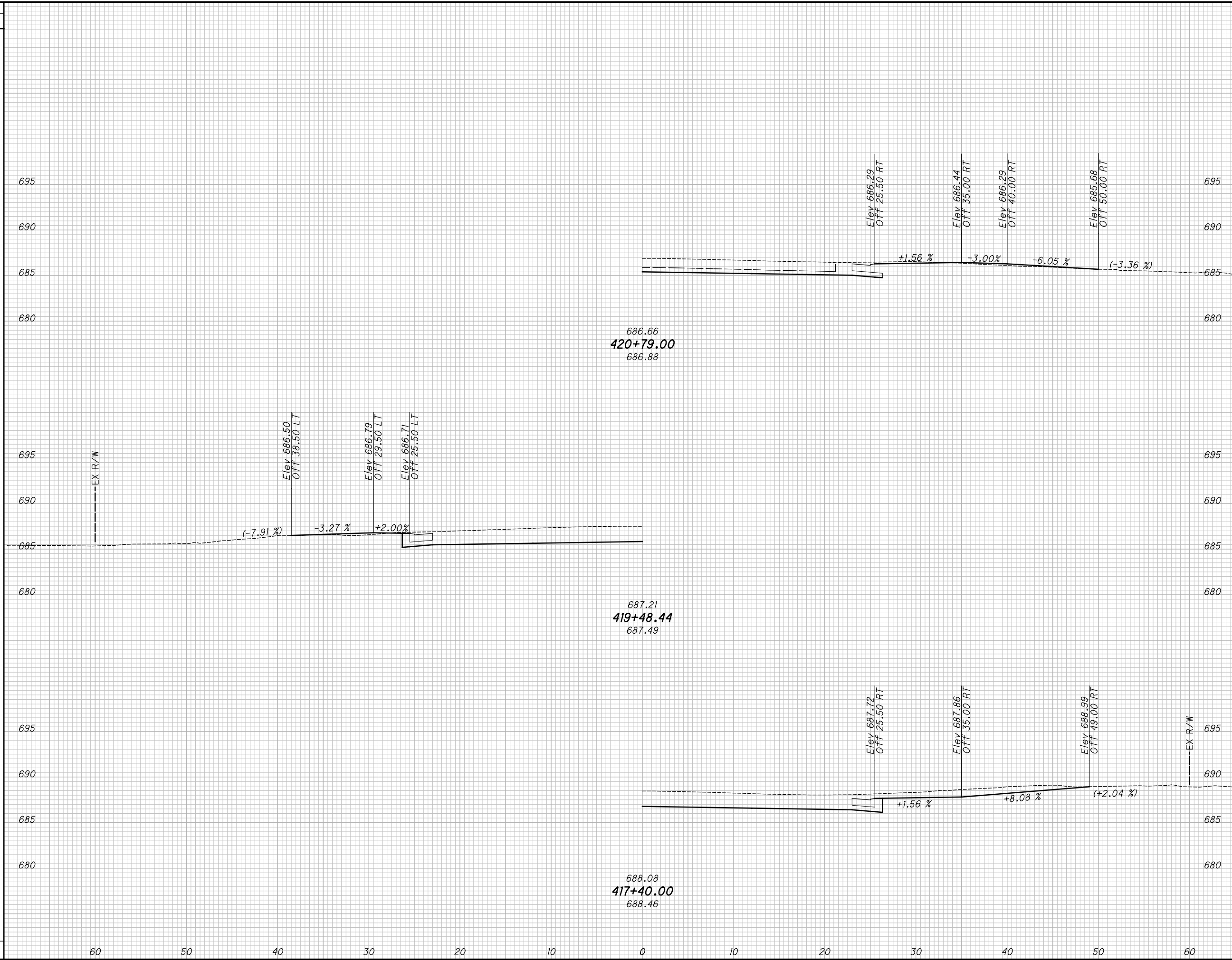
DRIVEWAY PROFILES
STA. 414+23.00 TO STA. 414+82.50

LAK-US-20-24.99
PART 2

427
697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_3 7/15/2022 2:57:52 PM Troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

DRIVEWAY PROFILES
STA. 417+27.00 TO STA. 419+48.50

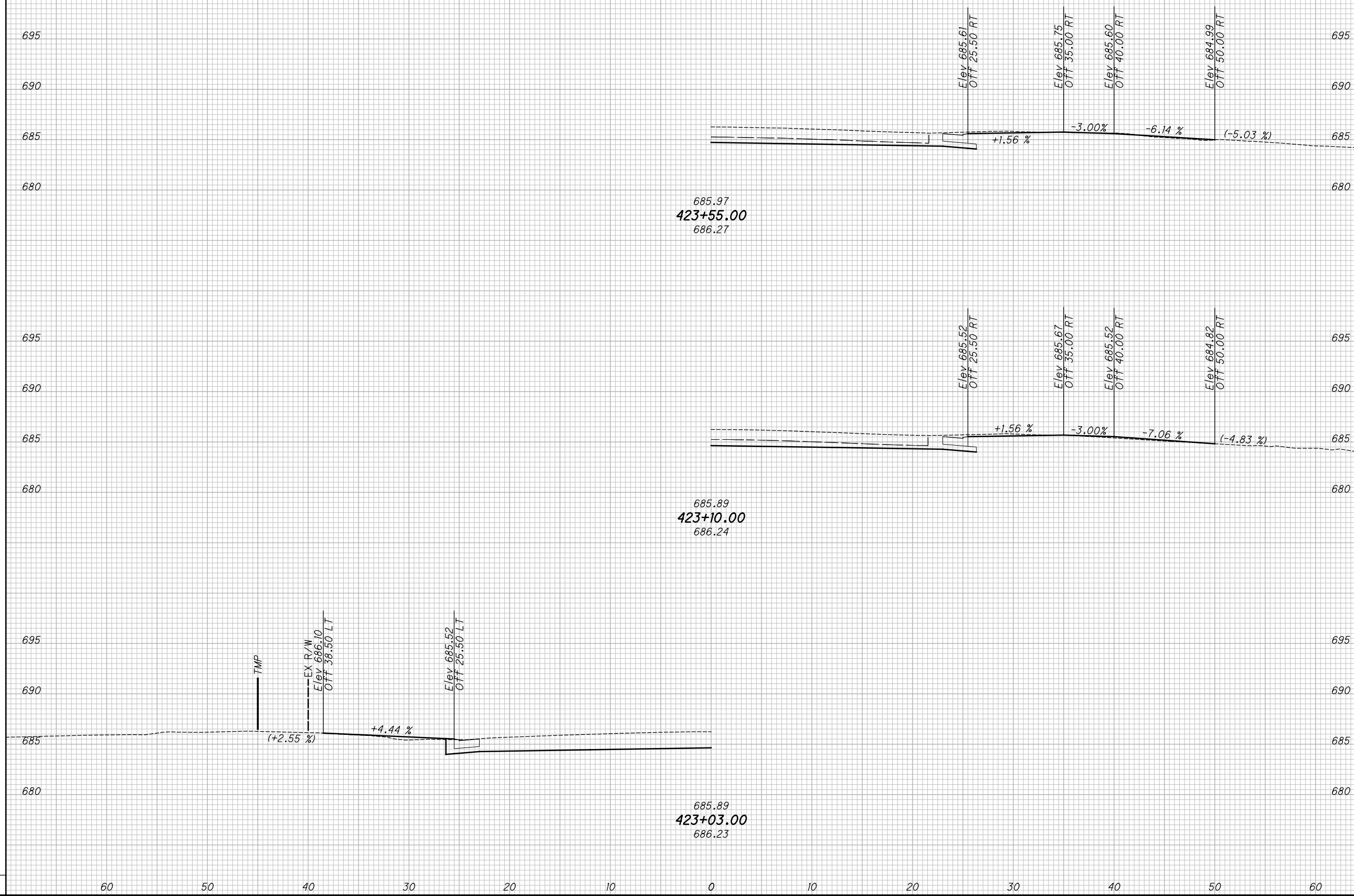
LAK-US-20-24.99
PART 2

428
697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_4 7/15/2022 2:57:53 PM troyer

SEEDING
END SO.
WIDTH YDS.

END AREA VOLUME
CUT FILL CUT FILL
CALCULATED
CHECKED
TLS
JMP



DRIVEWAY PROFILES
STA. 420+79.00 TO STA. 423+03.00

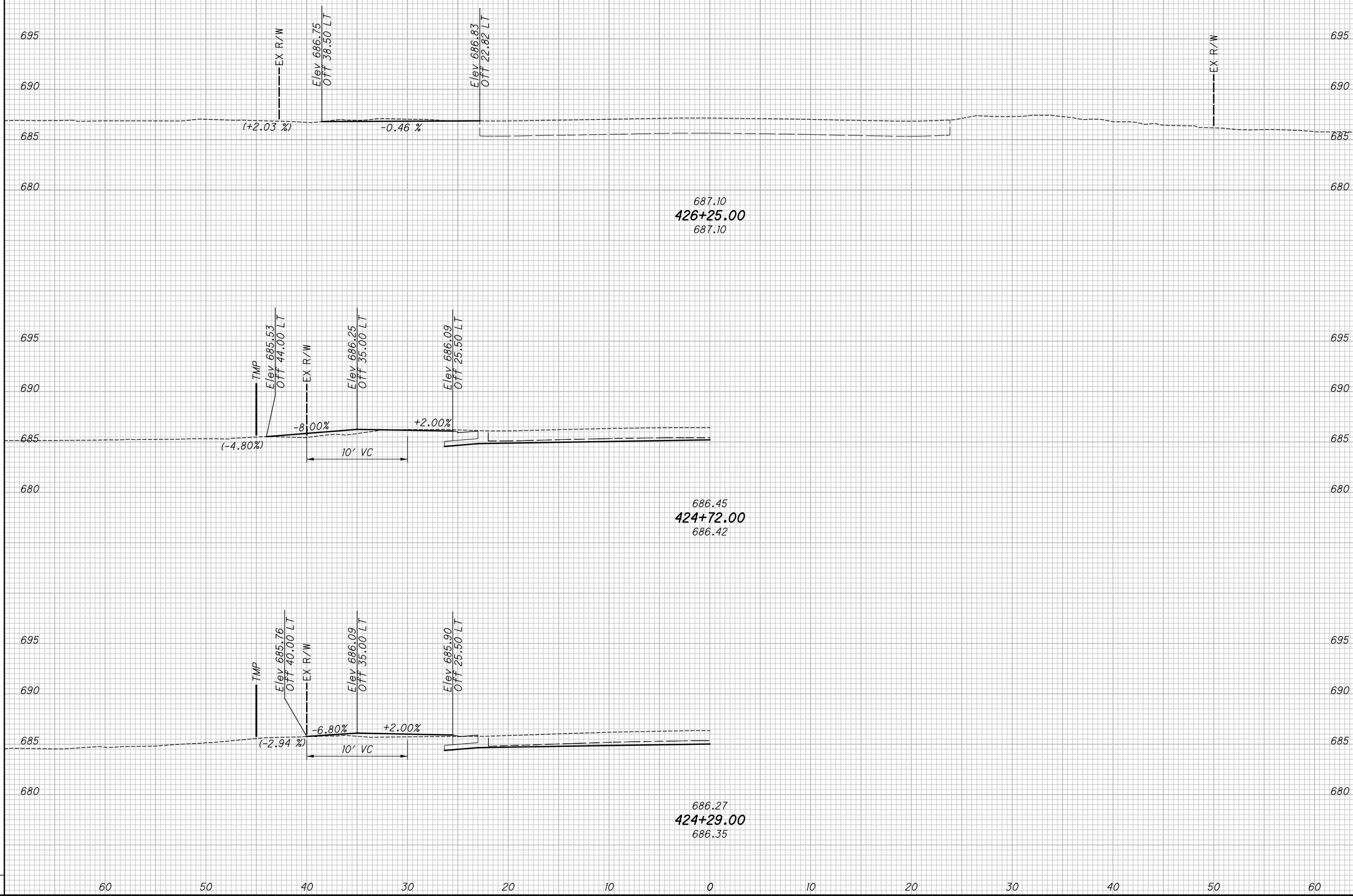
LAK-US-20-24.99
PART 2

429
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_5 7/15/2022 2:57:54 PM troyer

SEEDING
END SO.
WIDTH YDS.

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		



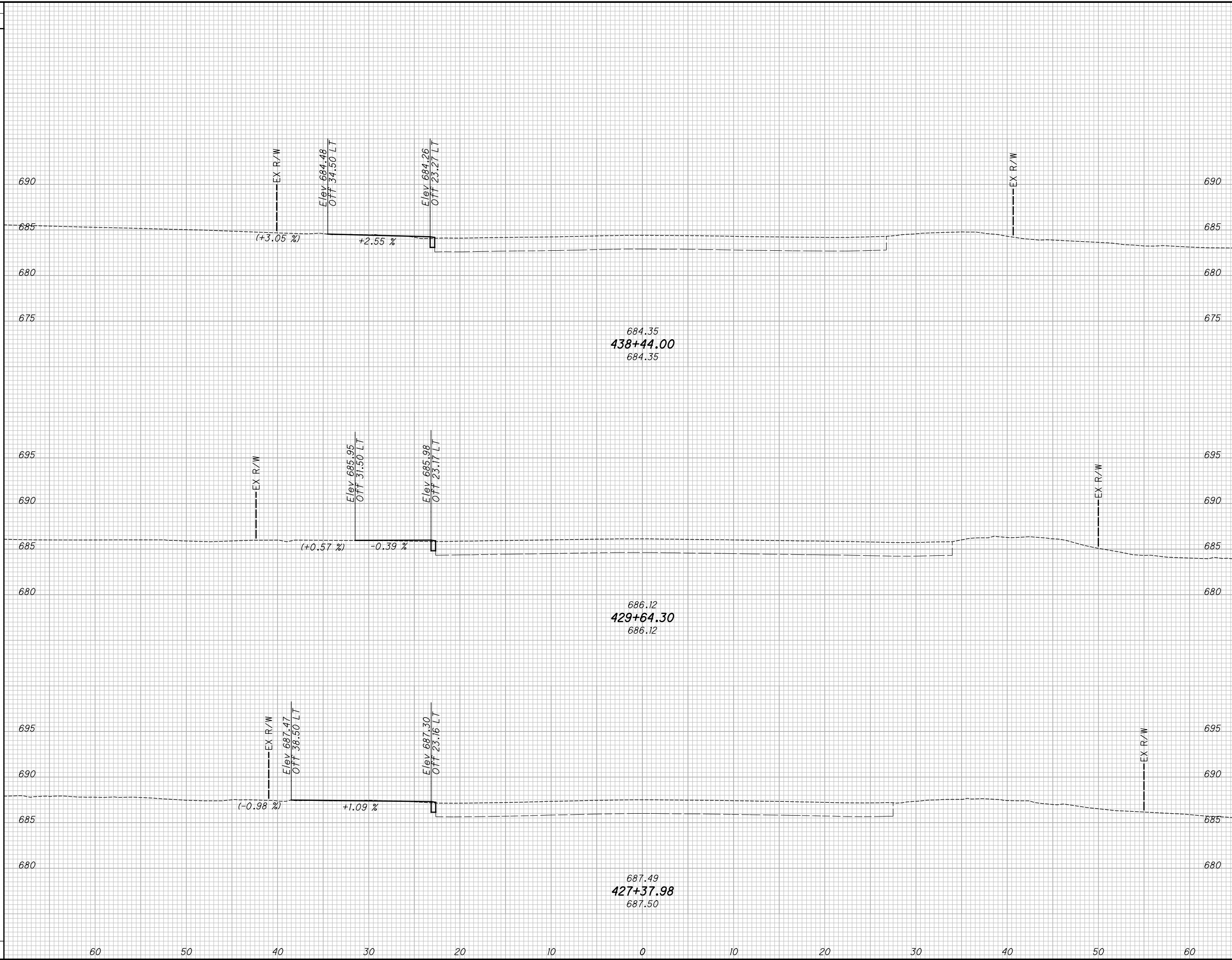
DRIVEWAY PROFILES
STA. 423+10.00 TO STA. 424+29.00

LAK-US-20-24.99
PART 2

430
697

H:\2018\180743\000\ProjectData\092TO_LAK_20_EAST\Design\Roadway\Sheets\092TO_XS10.dgn XS_SHEET_temporary_model_name_6 7/15/2022 2:57:55 PM Troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

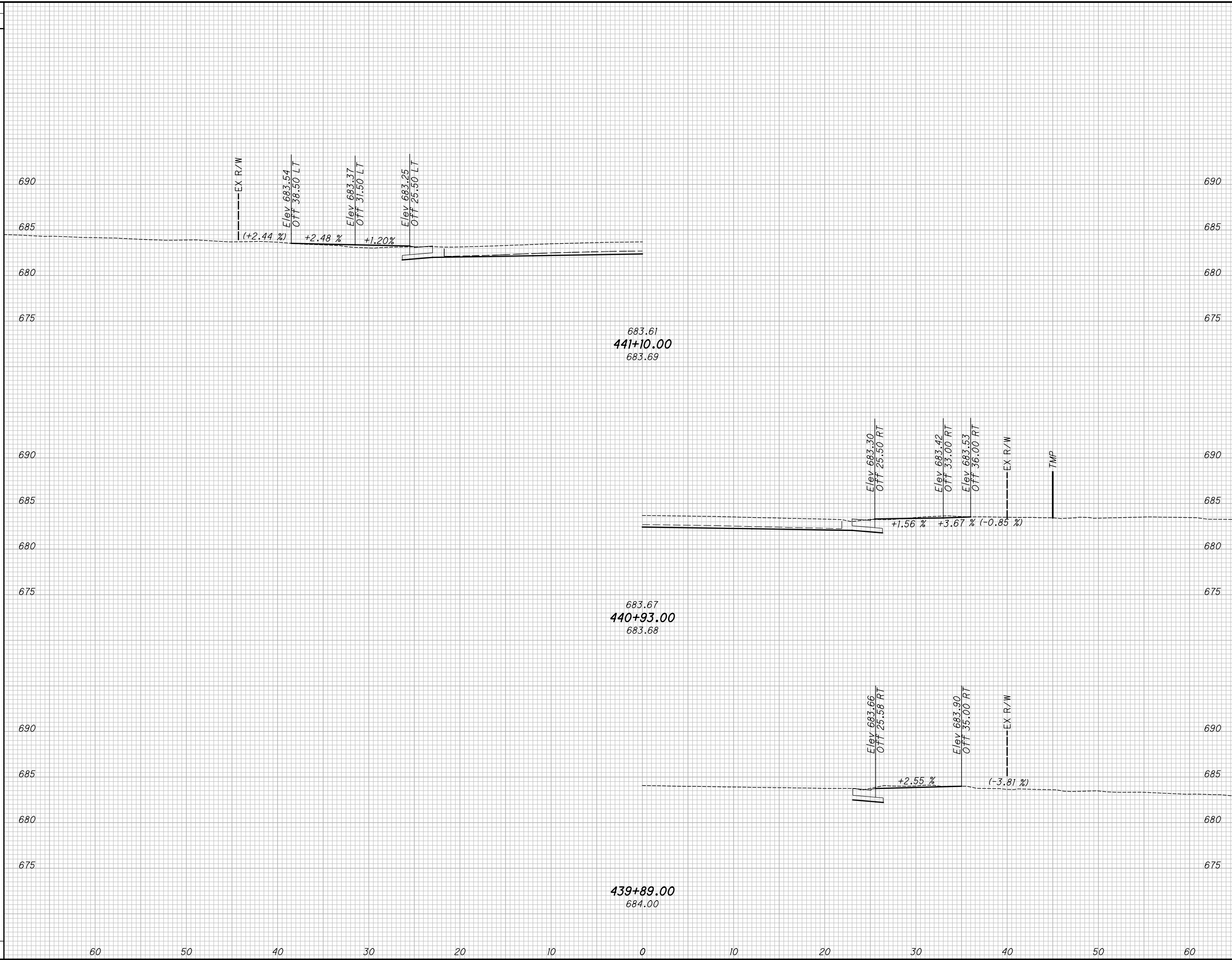
DRIVEWAY PROFILES
STA. 424+72.00 TO STA. 441+89.50

LAK-US-20-24.99
PART 2

431
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_7 7/15/2022 2:57:57 PM troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA	VOLUME	CALCULATED	CHECKED

DRIVEWAY PROFILES
STA. 442+45.50 TO STA. 448+22.50

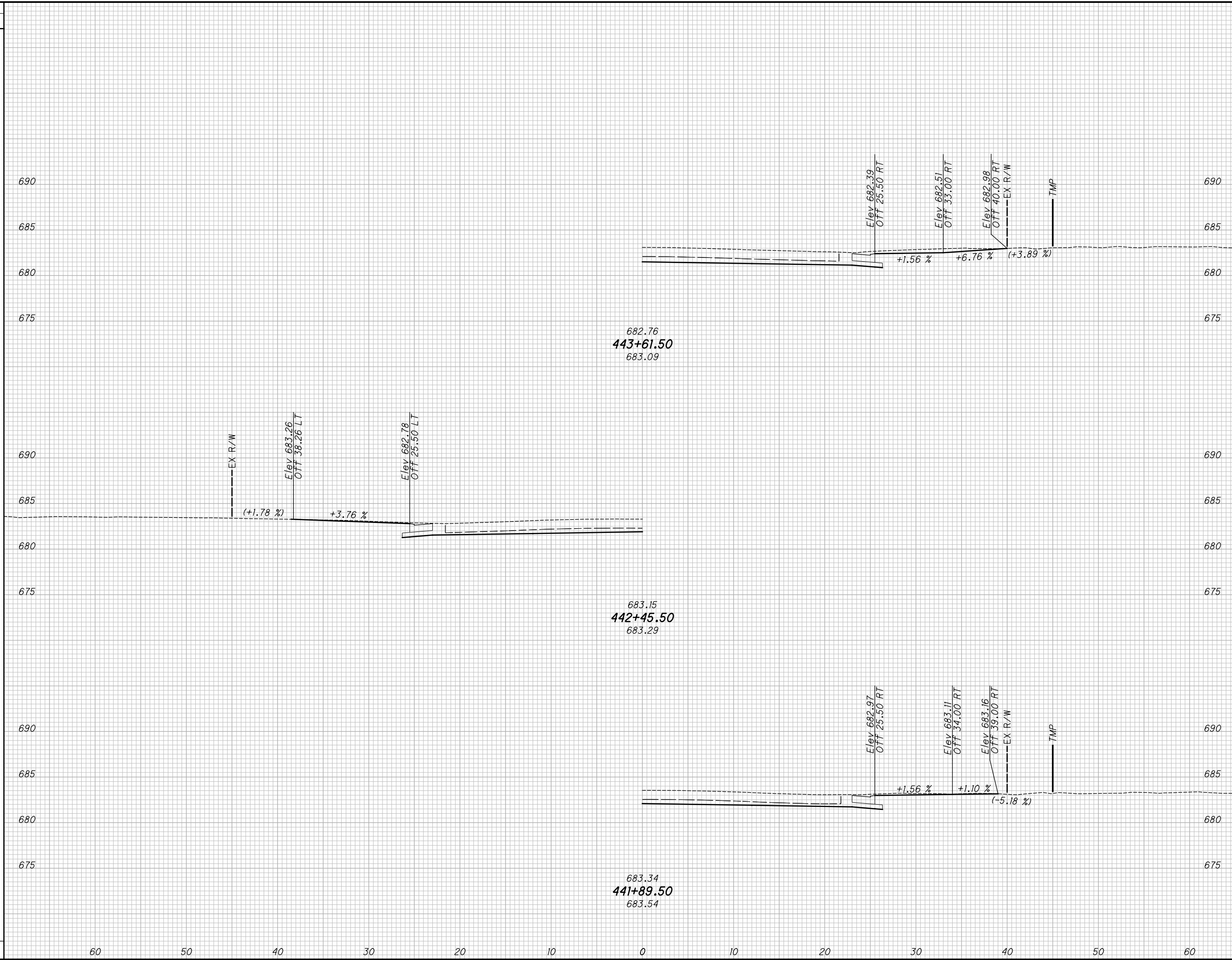
LAK-US-20-24.99
PART 2

432
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_8 7/15/2022 2:57:58 PM Troyer

SEEDING

END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

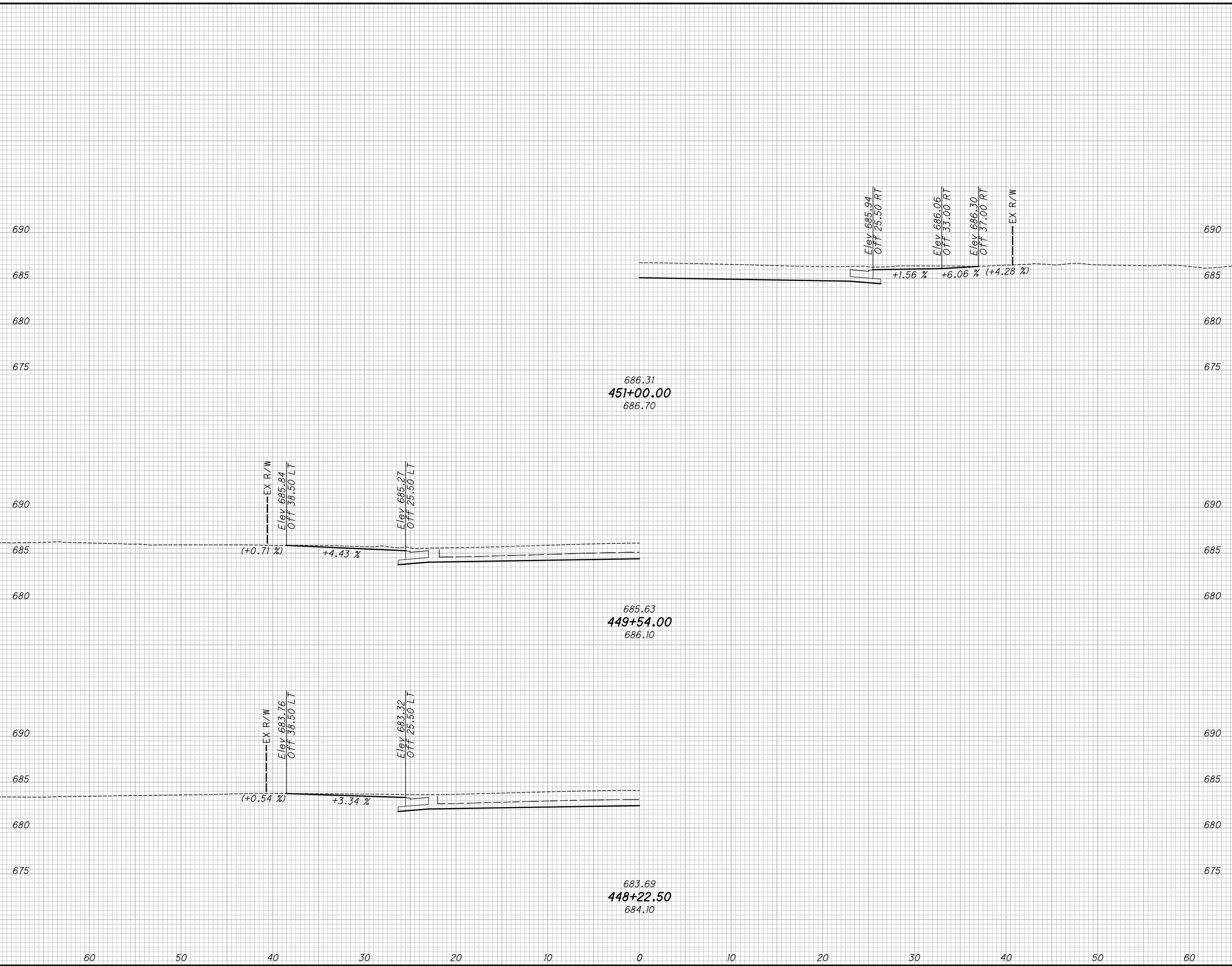
DRIVEWAY PROFILES
STA. 449+54.00 TO STA. 452+55.00

LAK-US-20-24.99
PART 2

433
697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_9 7/15/2022 2:57:59 PM Troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

DRIVEWAY PROFILES
STA. 456+56.00 TO STA. 457+48.00

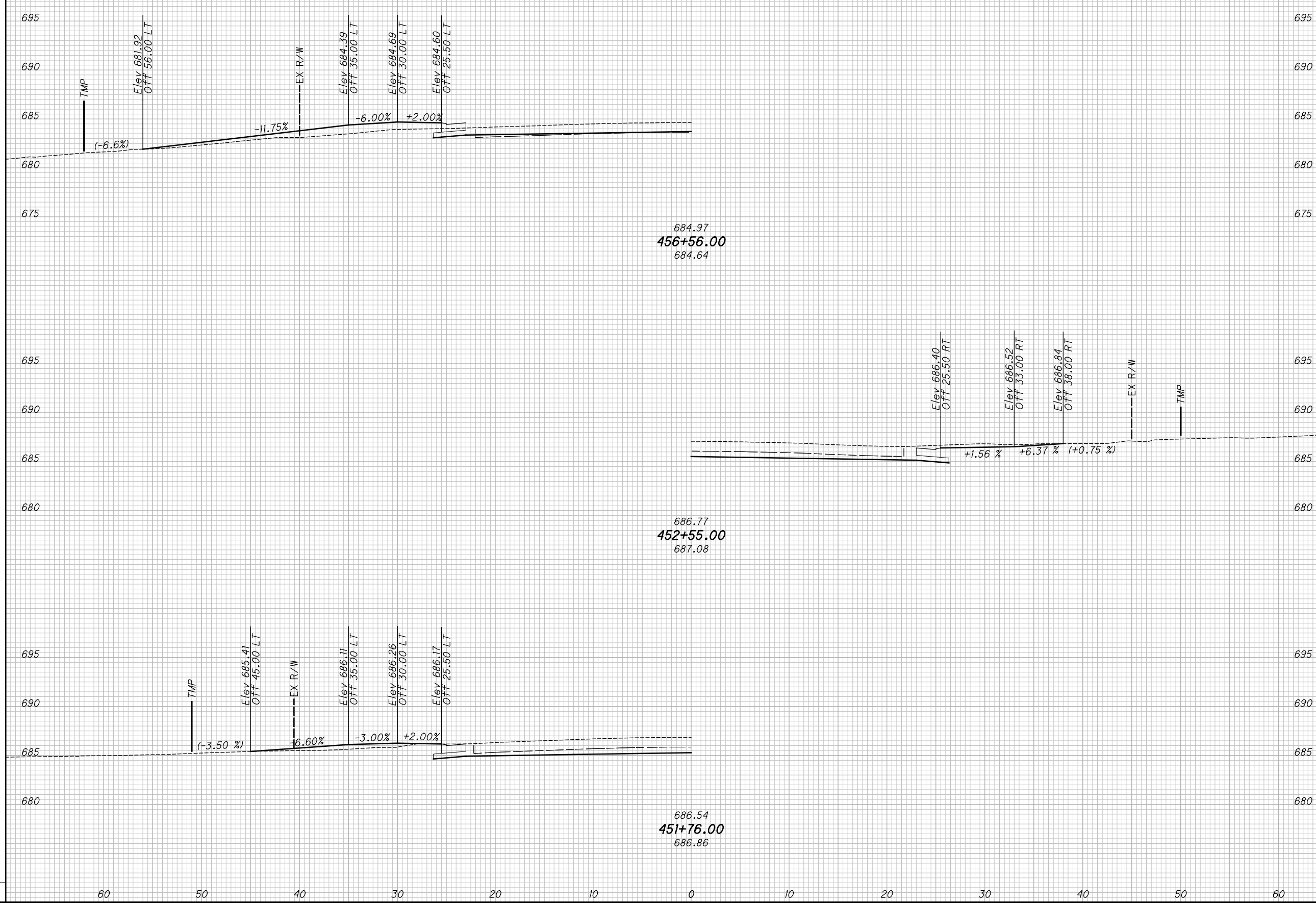
LAK-US-20-24.99
PART 2

434
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_10 7/15/2022 2:58:00 PM troyer

SEEDING
END SO.
WIDTH YDS.

END AREA VOLUME
CUT FILL CUT FILL
CALCULATED
CHECKED
TLS
JMP



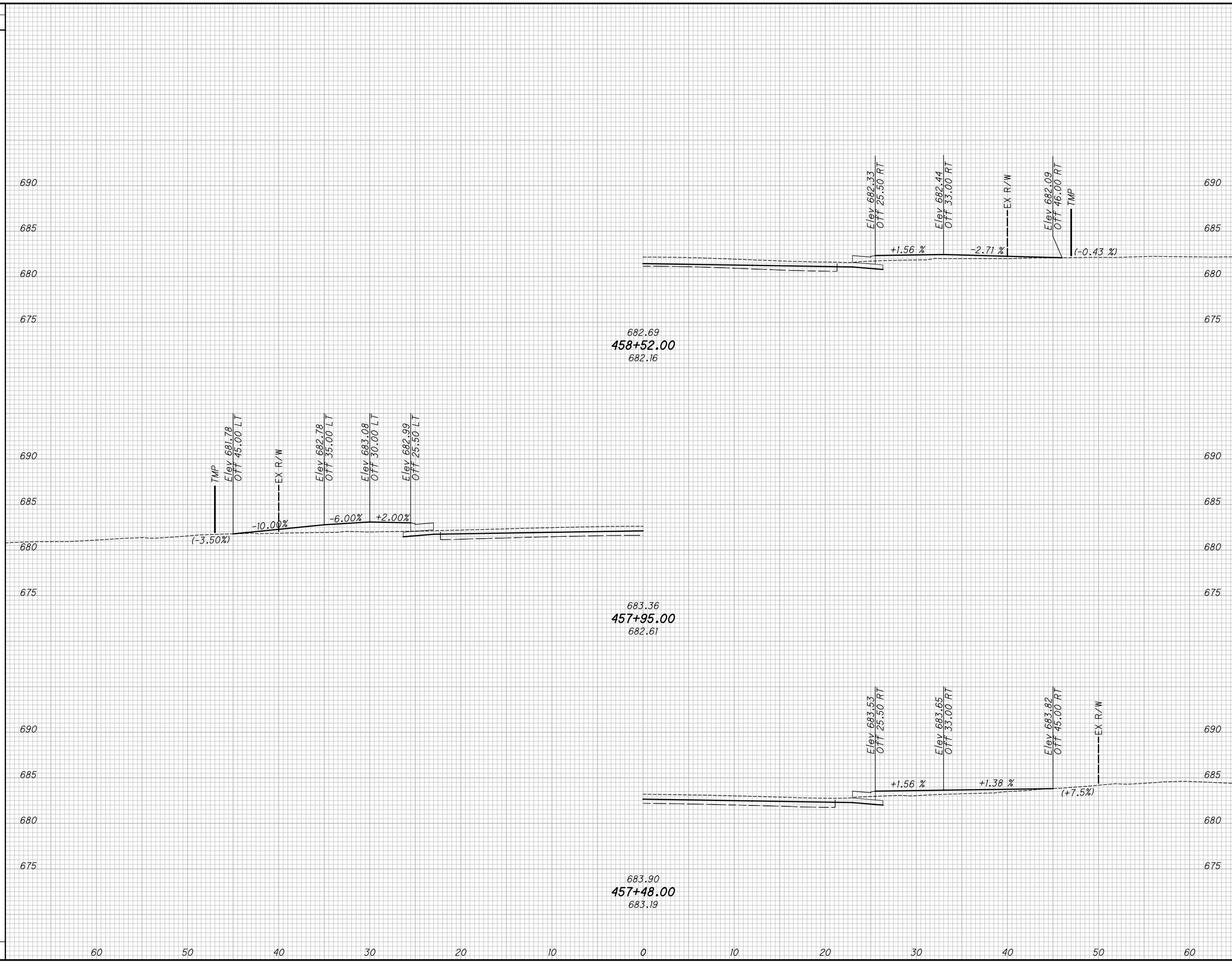
DRIVEWAY PROFILES
STA. 457 +95.00 TO STA. 460 +84.00

LAK-US-20-24.99
PART 2

435
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_11 7/15/2022 2:58:02 PM troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

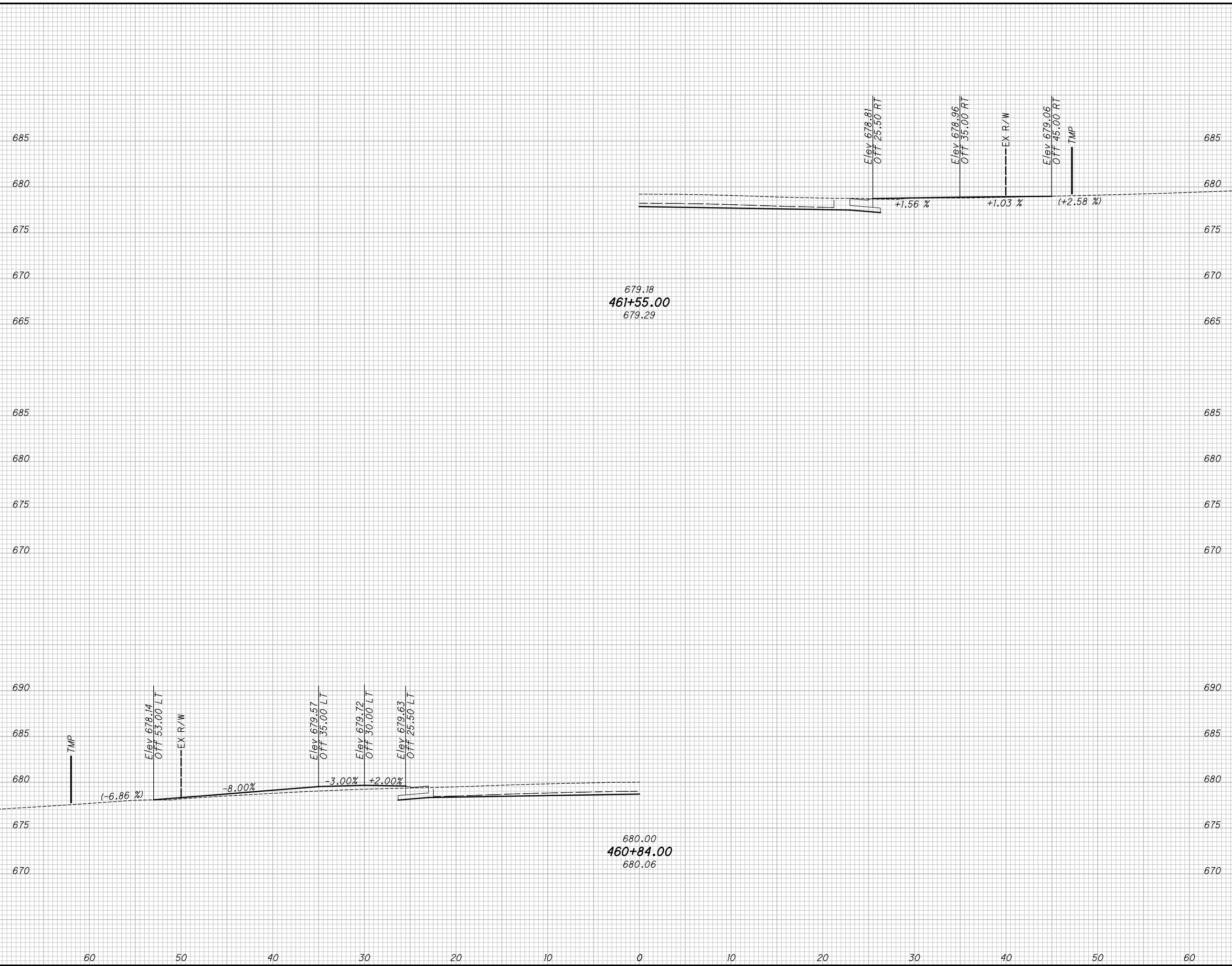
DRIVEWAY PROFILES
STA. 461+55.00 TO STA. 467+22.00

LAK-US-20-24.99
PART 2

436
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_12 7/15/2022 2:58:03 PM troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

DRIVEWAY PROFILES
STA. 468+41.00 TO STA. 470+77.00

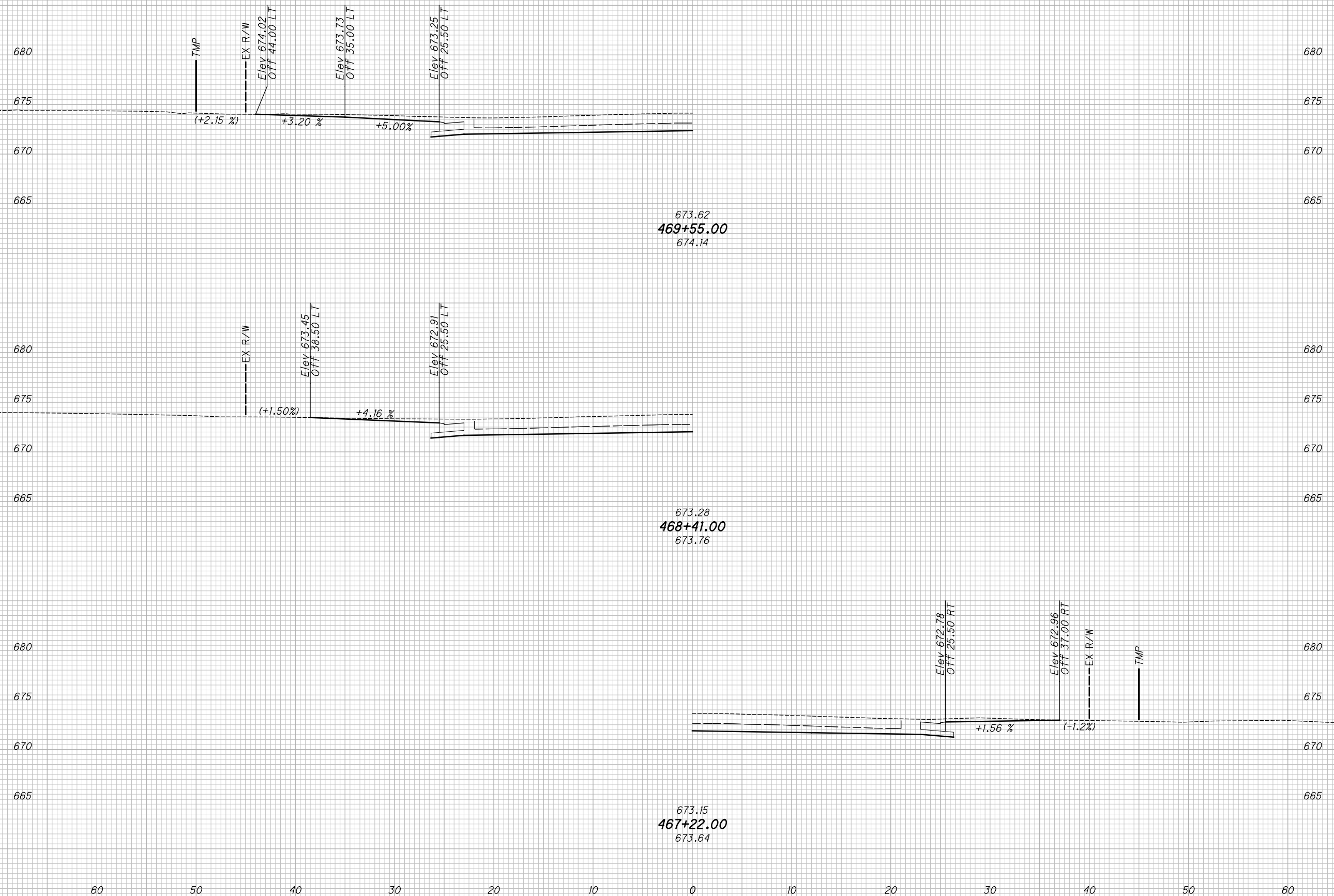
LAK-US-20-24.99
PART 2

437
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_13 7/15/2022 2:58:04 PM troyer

SEEDING
END SO.
WIDTH YDS.

END AREA VOLUME
CUT FILL CUT FILL
CALCULATED
CHECKED
TLS
JMP



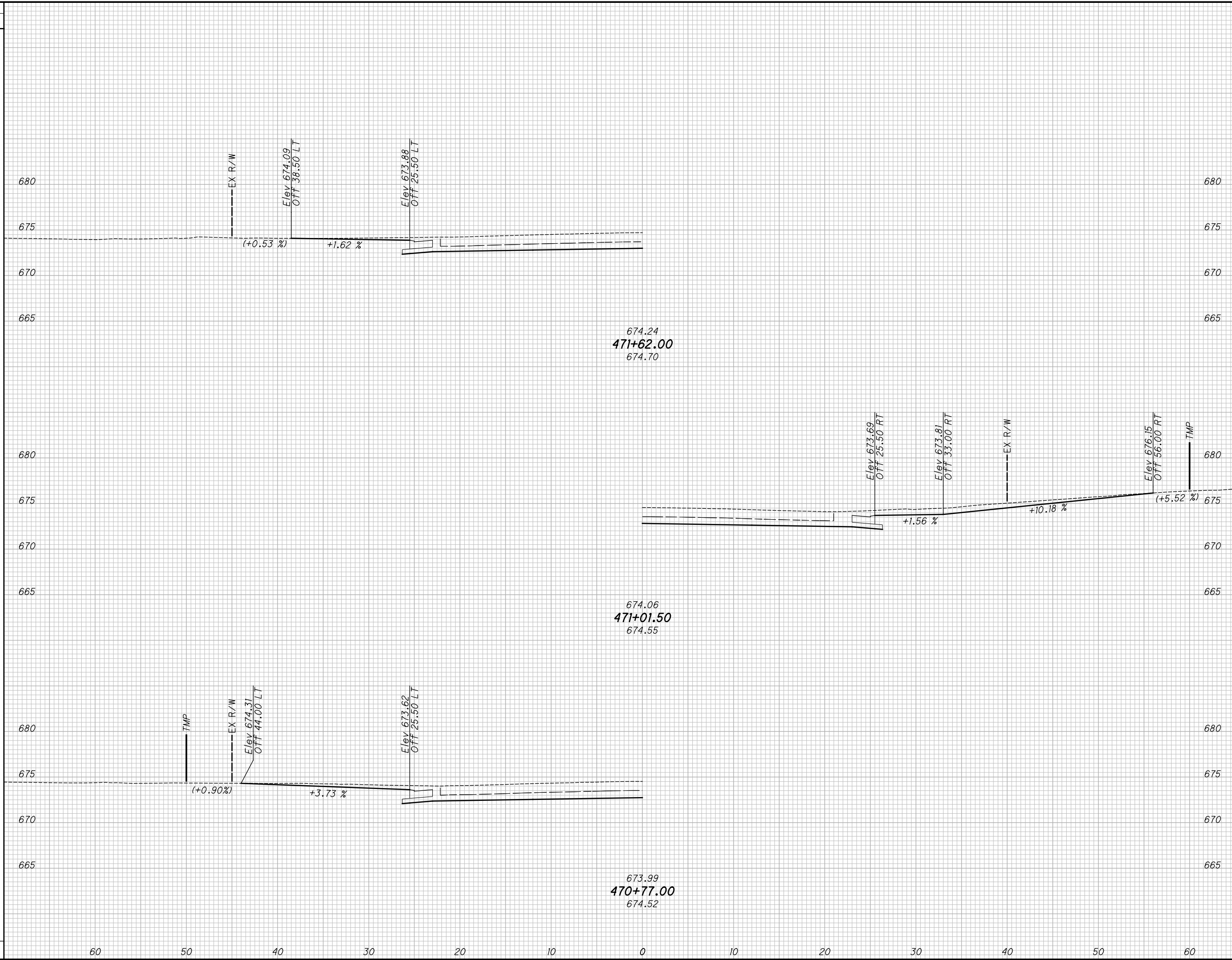
DRIVEWAY PROFILES
STA. 471+01.50 TO STA. 471+62.00

LAK-US-20-24.99
PART 2

438
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_14 7/15/2022 2:58:05 PM troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME	
CUT	FILL	CUT	FILL

DRIVEWAY PROFILES
STA. 472+78.00 TO STA. 473+60.00

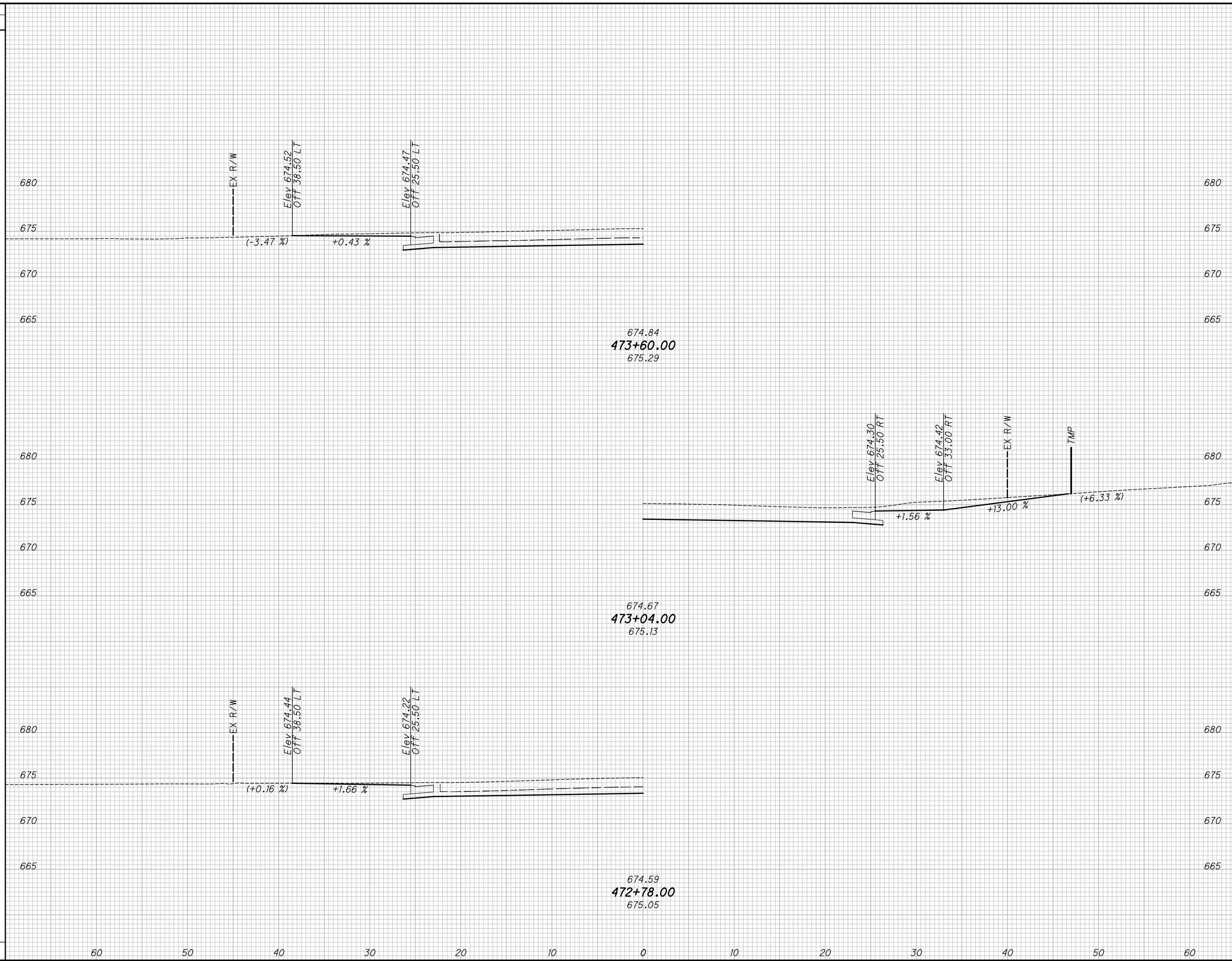
LAK-US-20-24.99
PART 2

CALCULATED
 T L S
 CHECKED
 J M P

439
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_15 7/15/2022 2:58:06 PM troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

DRIVEWAY PROFILES
STA. 474+87.50 TO STA. 475+55.00

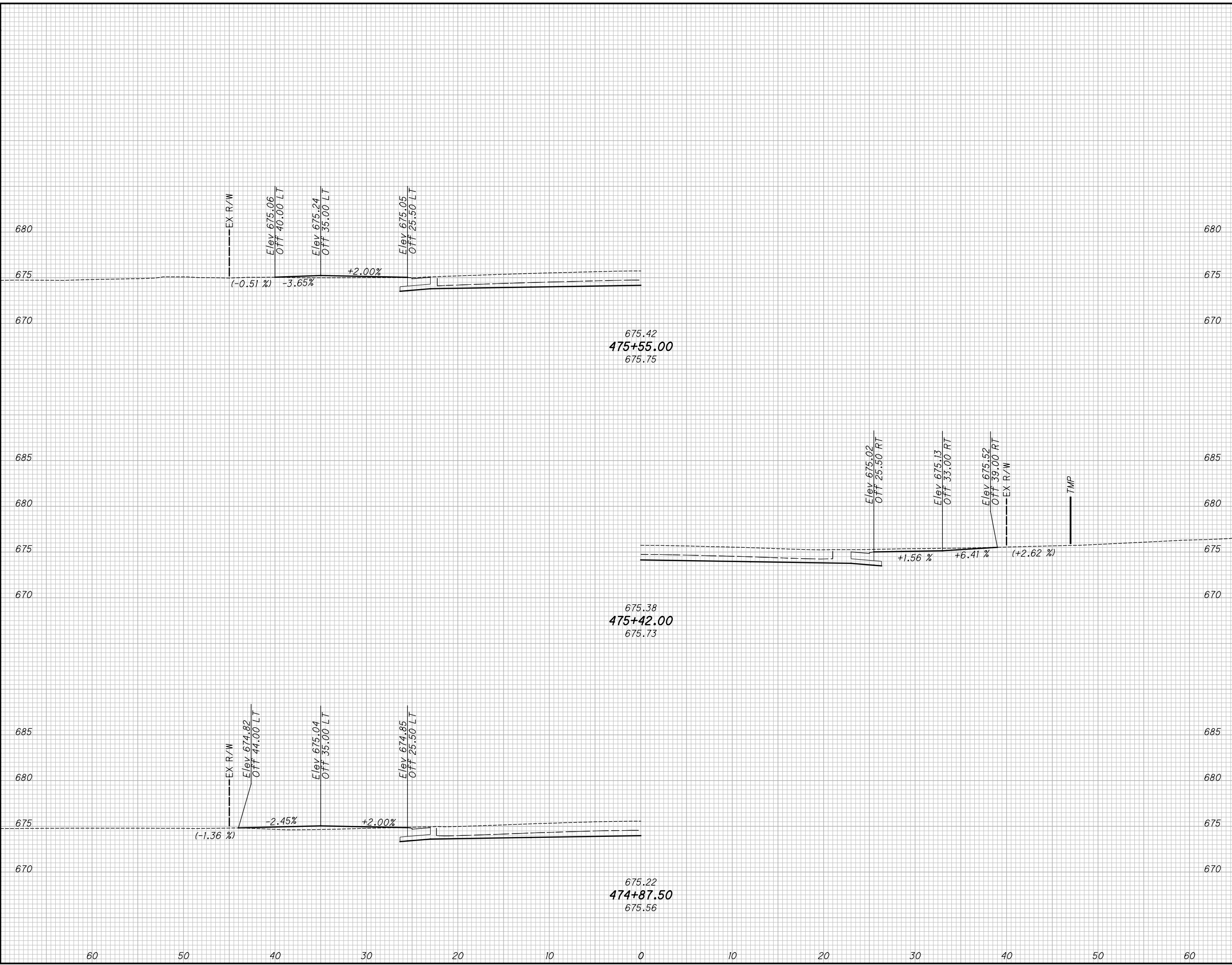
LAK-US-20-24.99
PART 2

440
697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_16 7/15/2022 2:58:07 PM troyer

SEEDING
END SO.
WIDTH YDS.

END AREA VOLUME
CUT FILL CUT FILL
CALCULATED
TLS
CHECKED
JMP



DRIVEWAY PROFILES
STA. 476+84.00 TO STA. 478+67.50

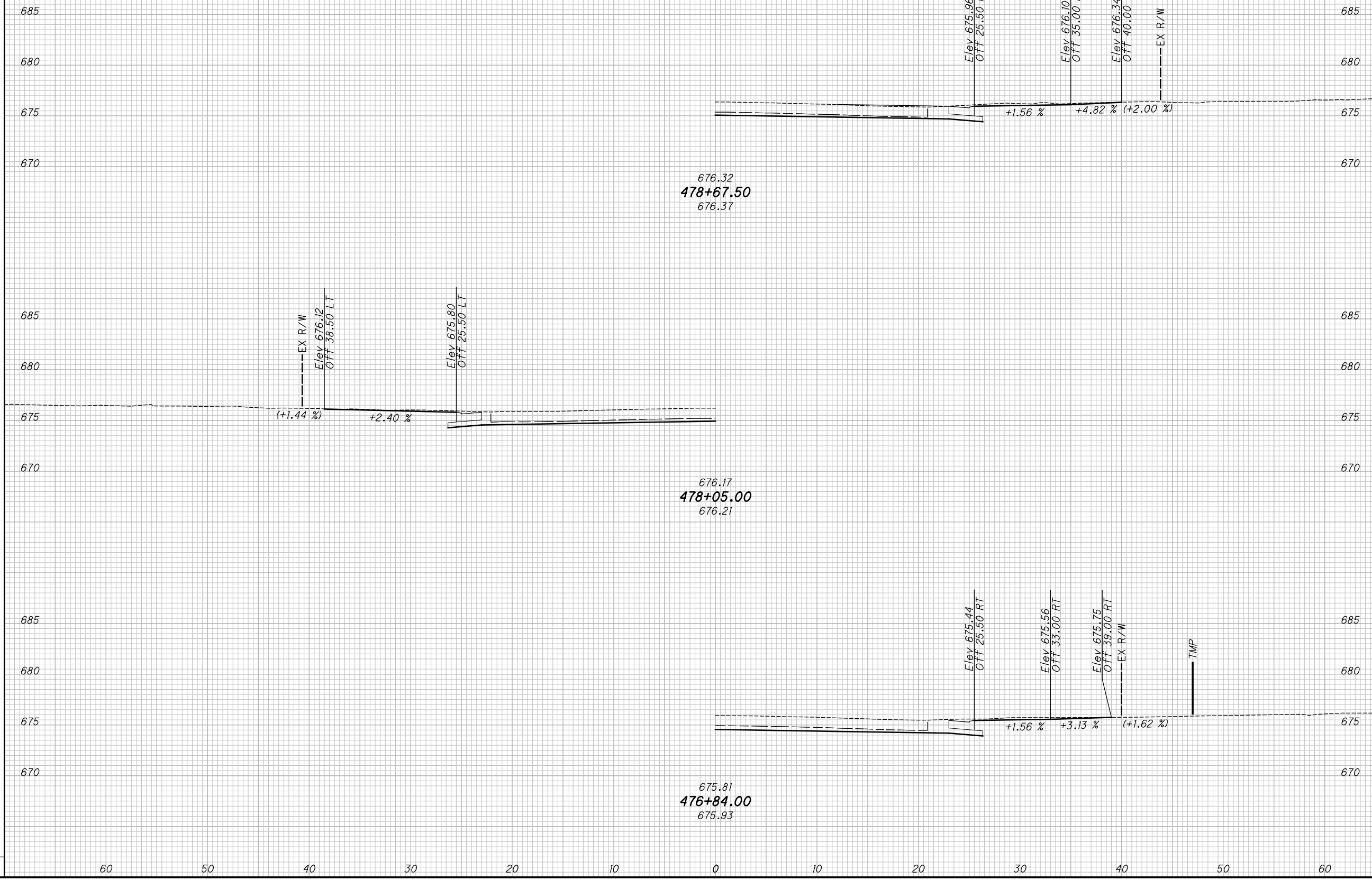
LAK-US-20-24.99
PART 2

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_17 7/15/2022 2:58:09 PM troyer

SEEDING

END WIDTH	SO. YDS.

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		



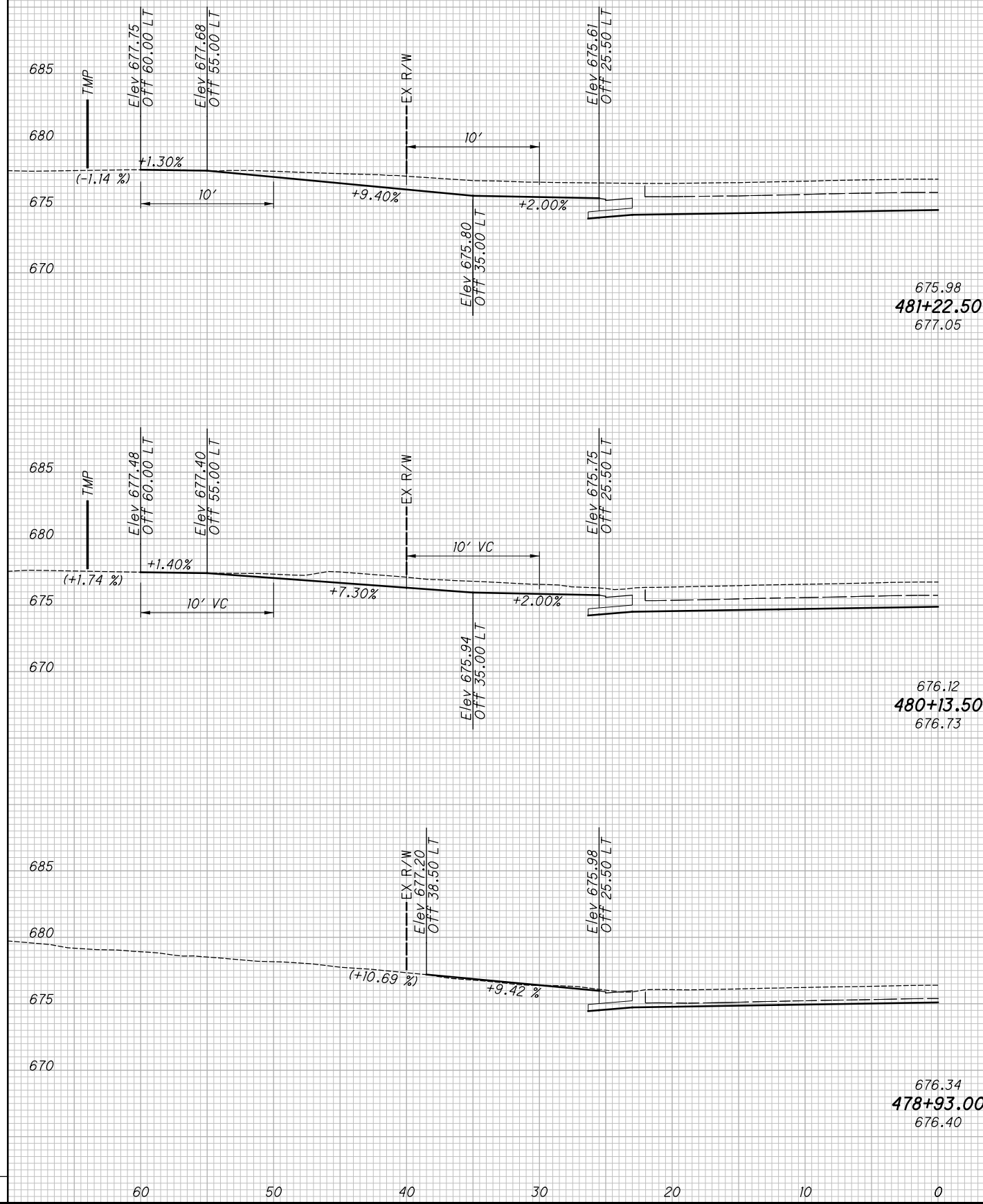
DRIVEWAY PROFILES
STA. 478+93.00 TO STA. 481+22.50

LAK-US-20-24.99
PART 2

442
697

H:\2018\180743\Projec+Data\092TO_LAK_20_EAST\Design\Roadway\Sheets\092TO_XS10.dgn XS_SHEET_temporary_model_name_18 7/15/2022 2:58:10 PM Troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA	VOLUME	CALCULATED	CHECKED	JMP						
					CUT	FILL	CUT	FILL	TLS	

DRIVEWAY PROFILES
STA. 481+66.00 TO STA. 484+95.00

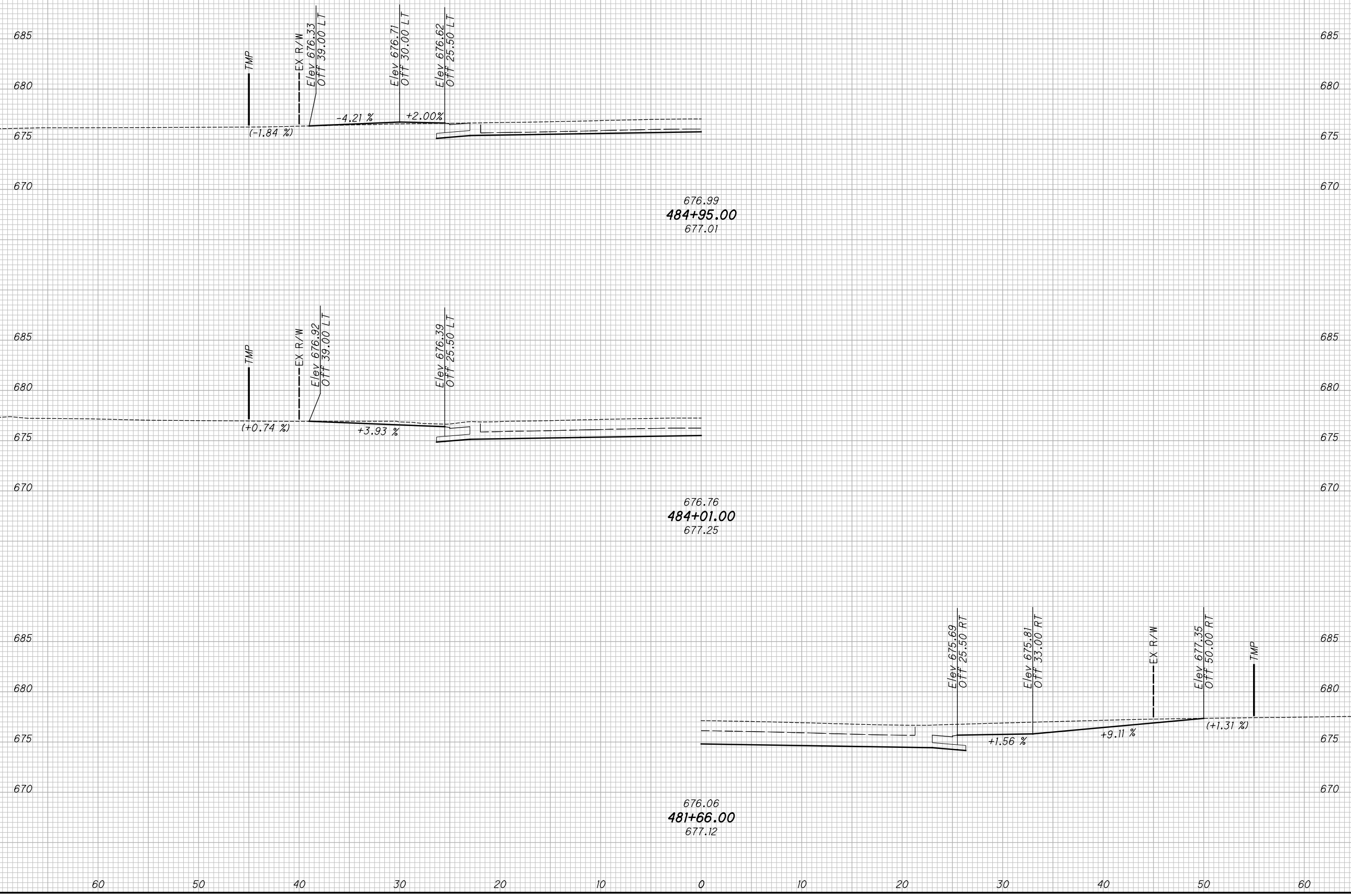
LAK-US-20-24.99
PART 2

443
697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_19 7/15/2022 2:58:11PM troyer

SEEDING	
END WIDTH	SO. YDS.

END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		



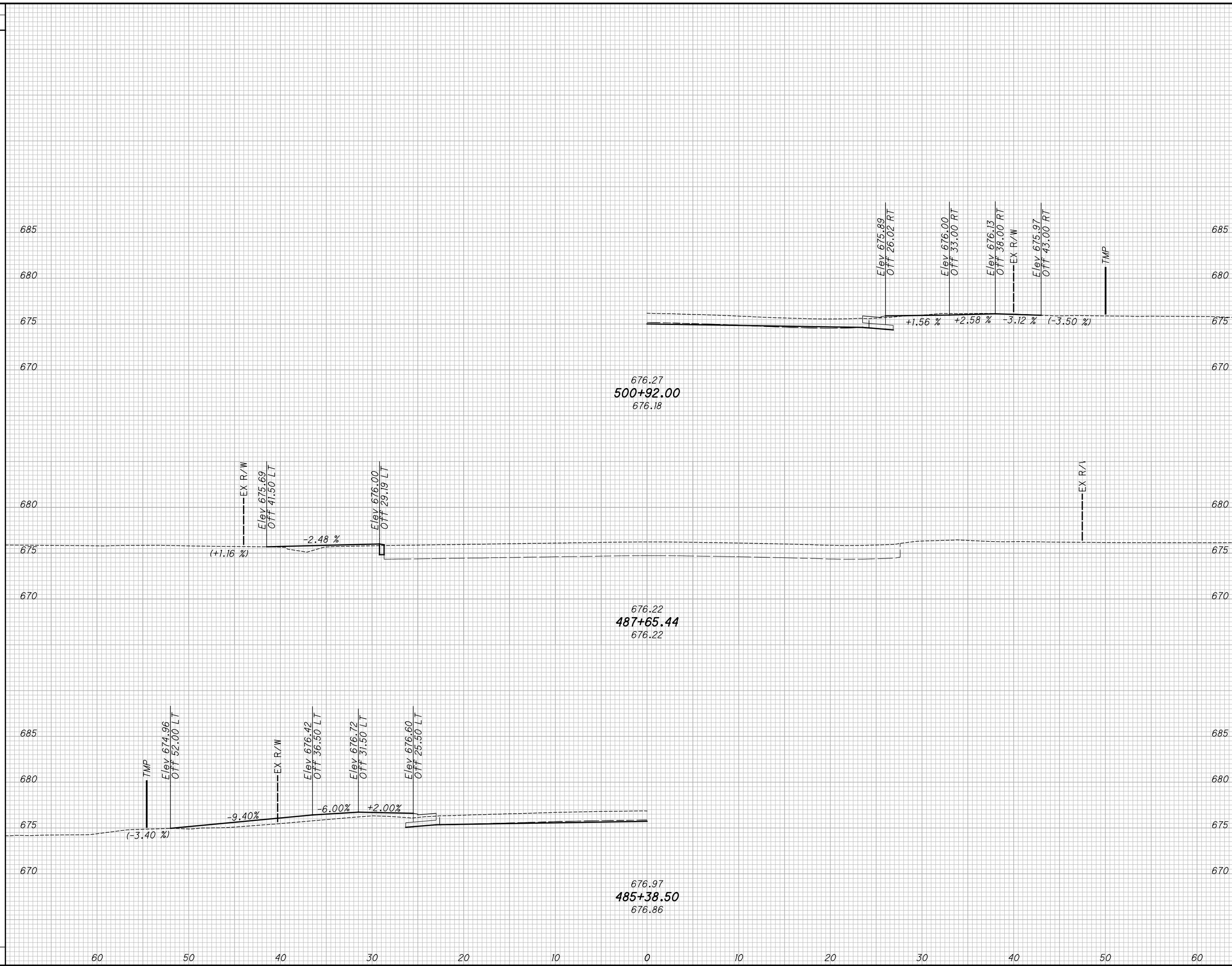
DRIVEWAY PROFILES
STA. 485+38.50 TO STA. 500+92.00

LAK-US-20-24.99
PART 2

H:\2018\180743\ProjectData\092TO_LAK_20_EAST\Design\Roadway\Sheets\092TO_XS10.dgn XS_SHEET_temporary_model_name_20 7/15/2022 2:58:42 PM troyer

SEEDING
END SO.
WIDTH YDS.

END AREA VOLUME
CUT FILL CUT FILL
CALCULATED
CHECKED
TLS
JMP



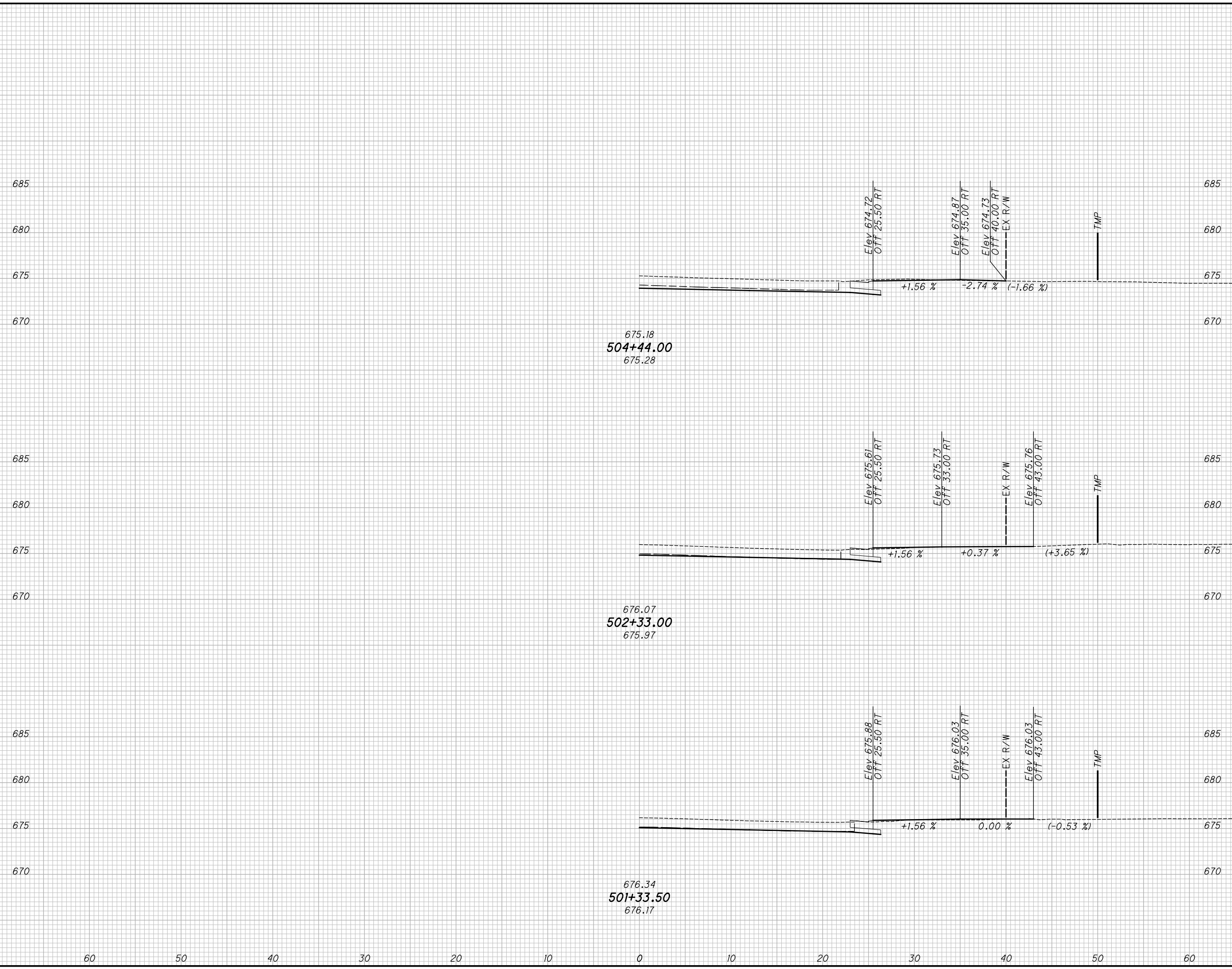
DRIVEWAY PROFILES
STA. 501+33.50 TO STA. 502+33.00

LAK-US-20-24.99
PART 2

445
697

H:\2018\80743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_21 7/5/2022 2:58:14 PM troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME	
CUT	FILL	CUT	FILL

DRIVEWAY PROFILES
STA. 504+44.00 TO STA. 505+80.00

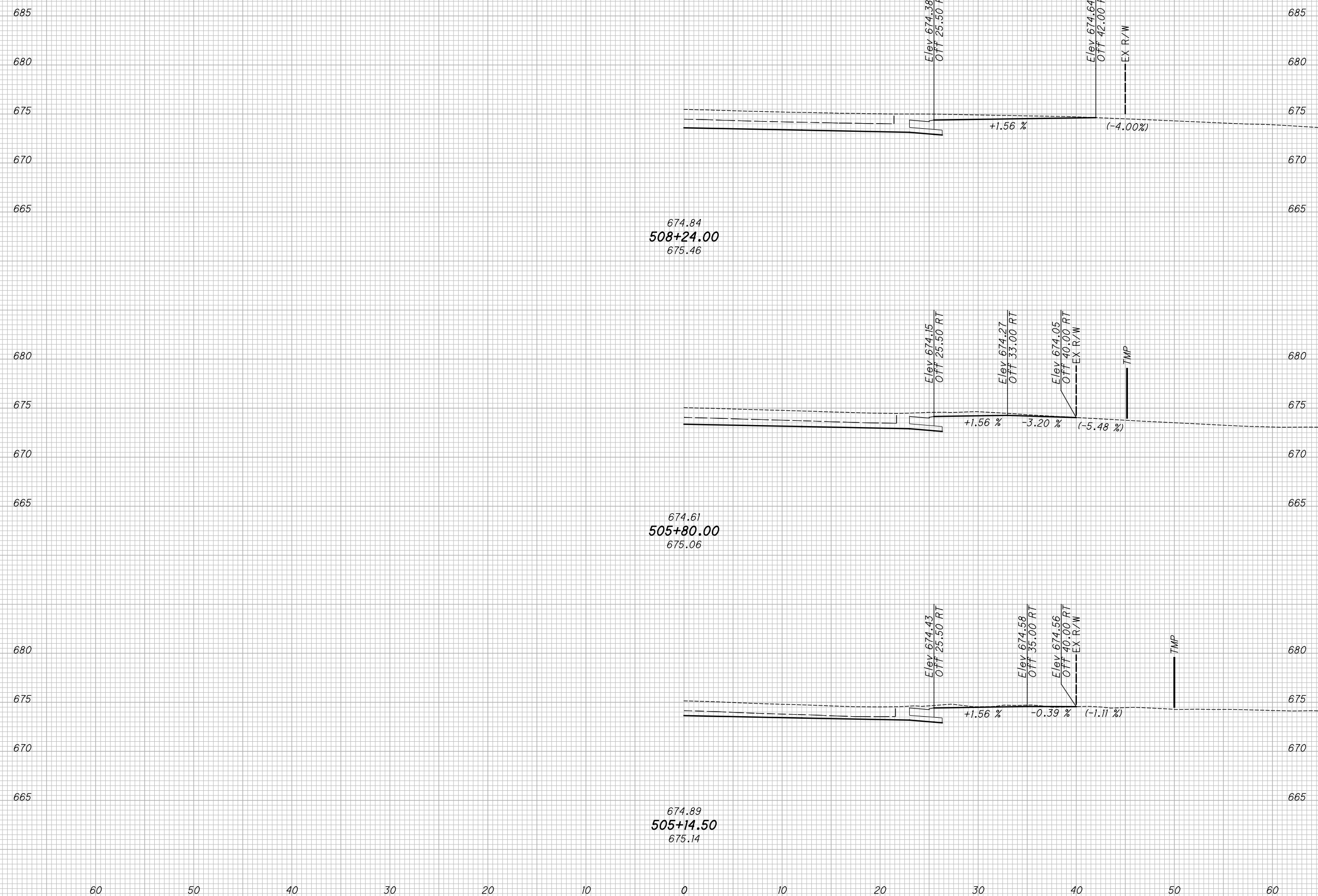
LAK-US-20-24.99
PART 2

446
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_22 7/15/2022 2:58:15 PM troyer

SEEDING
END SO.
WIDTH YDS.

END AREA VOLUME
CUT FILL CUT FILL
CALCULATED
CHECKED
TLS
JMP



DRIVEWAY PROFILES
STA. 508+24.00 TO STA. 508+85.50

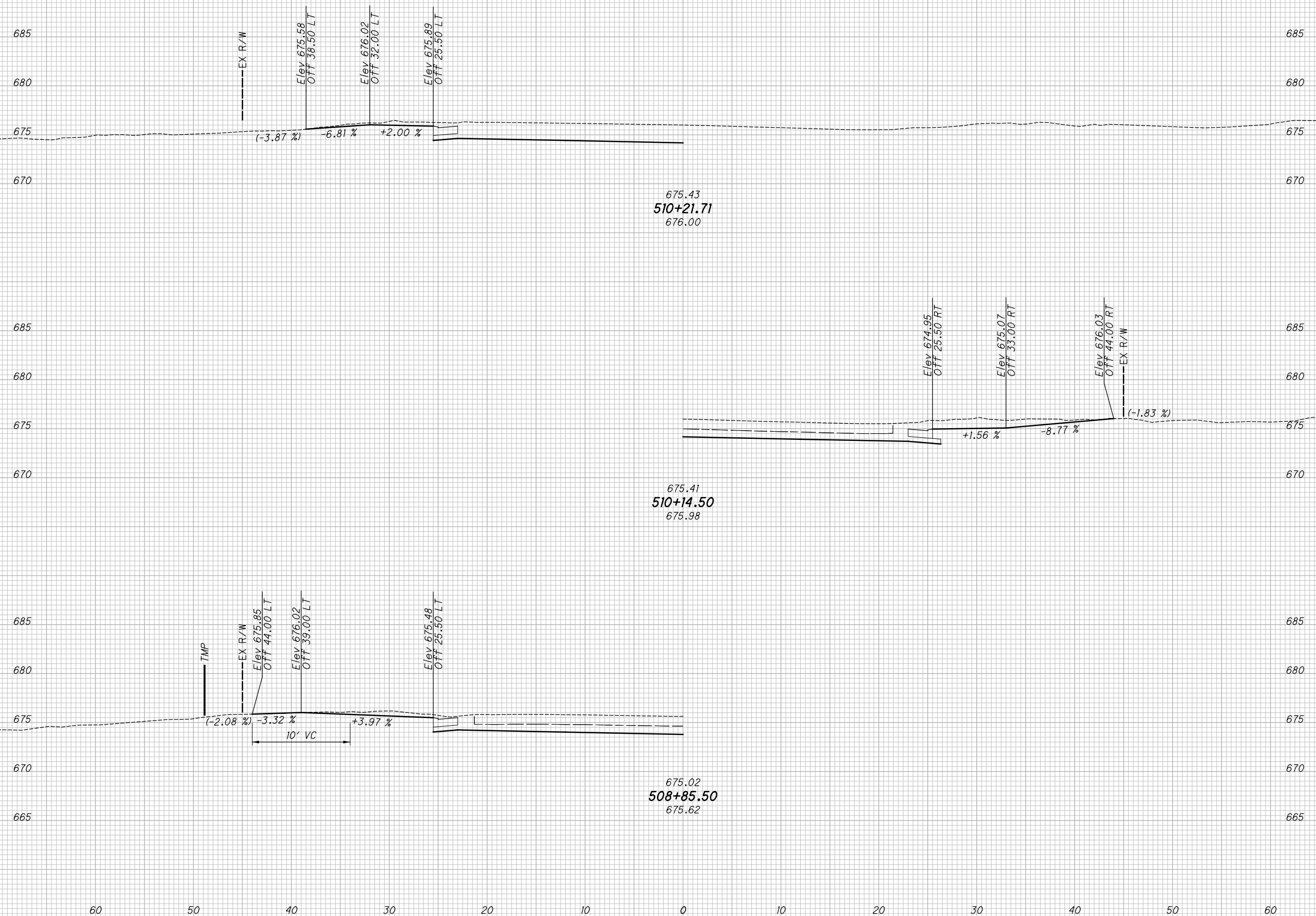
LAK-US-20-24.99
PART 2

447
697

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_23 7/15/2022 2:58:16 PM troyer

SEEDING
END SO.
WIDTH YDS.

END AREA VOLUME
CUT FILL CUT FILL
CALCULATED
CHECKED
TLS
JMP



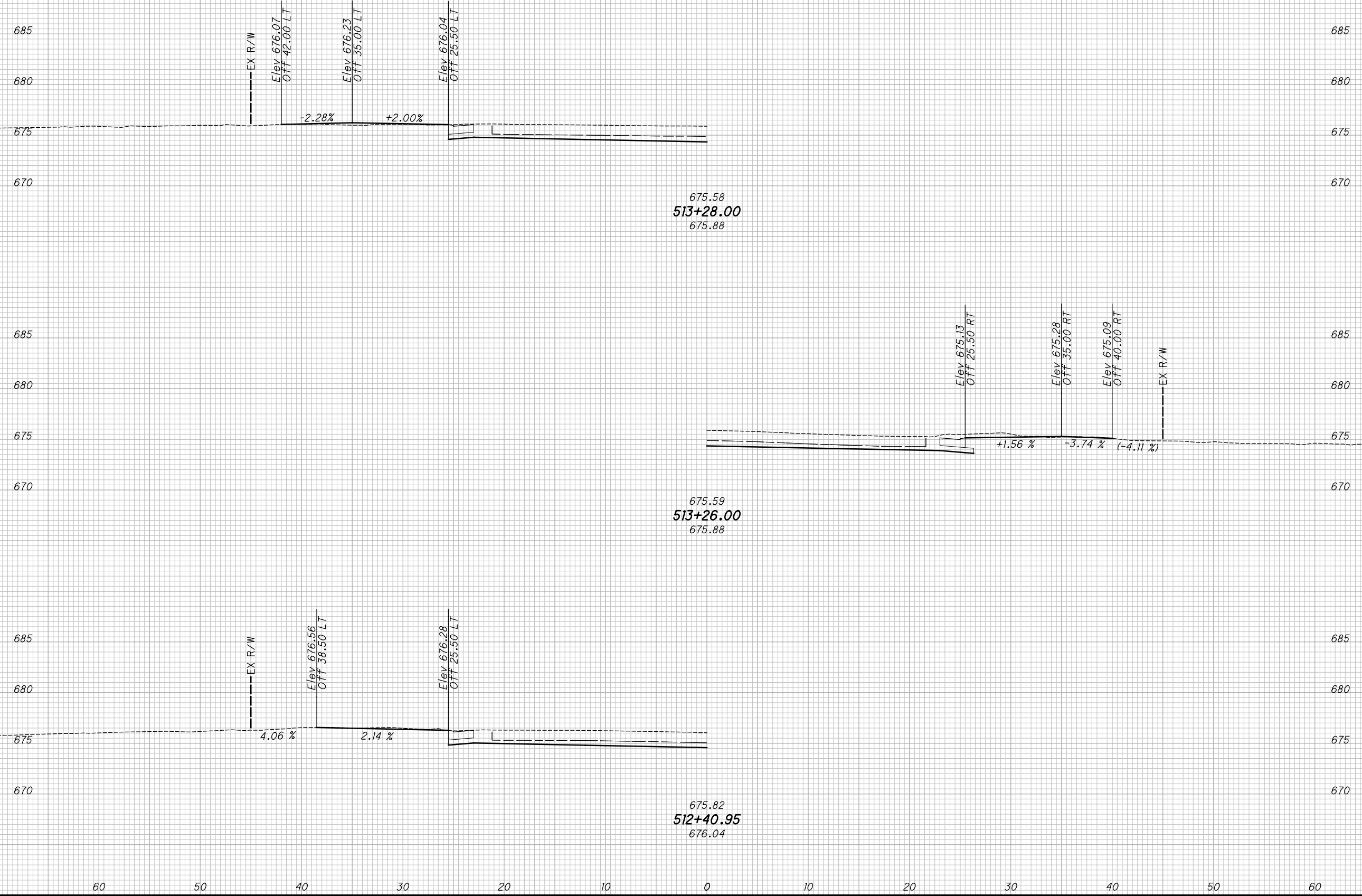
DRIVEWAY PROFILES
STA. 510+14.50 TO STA. 513+25.99

LAK-US-20-24.99
PART 2

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_24 7/15/2022 2:58:17 PM troyer

SEEDING
END SO.
WIDTH YDS.

END AREA VOLUME
CUT FILL CUT FILL
CALCULATED
TLS
CHECKED
JMP

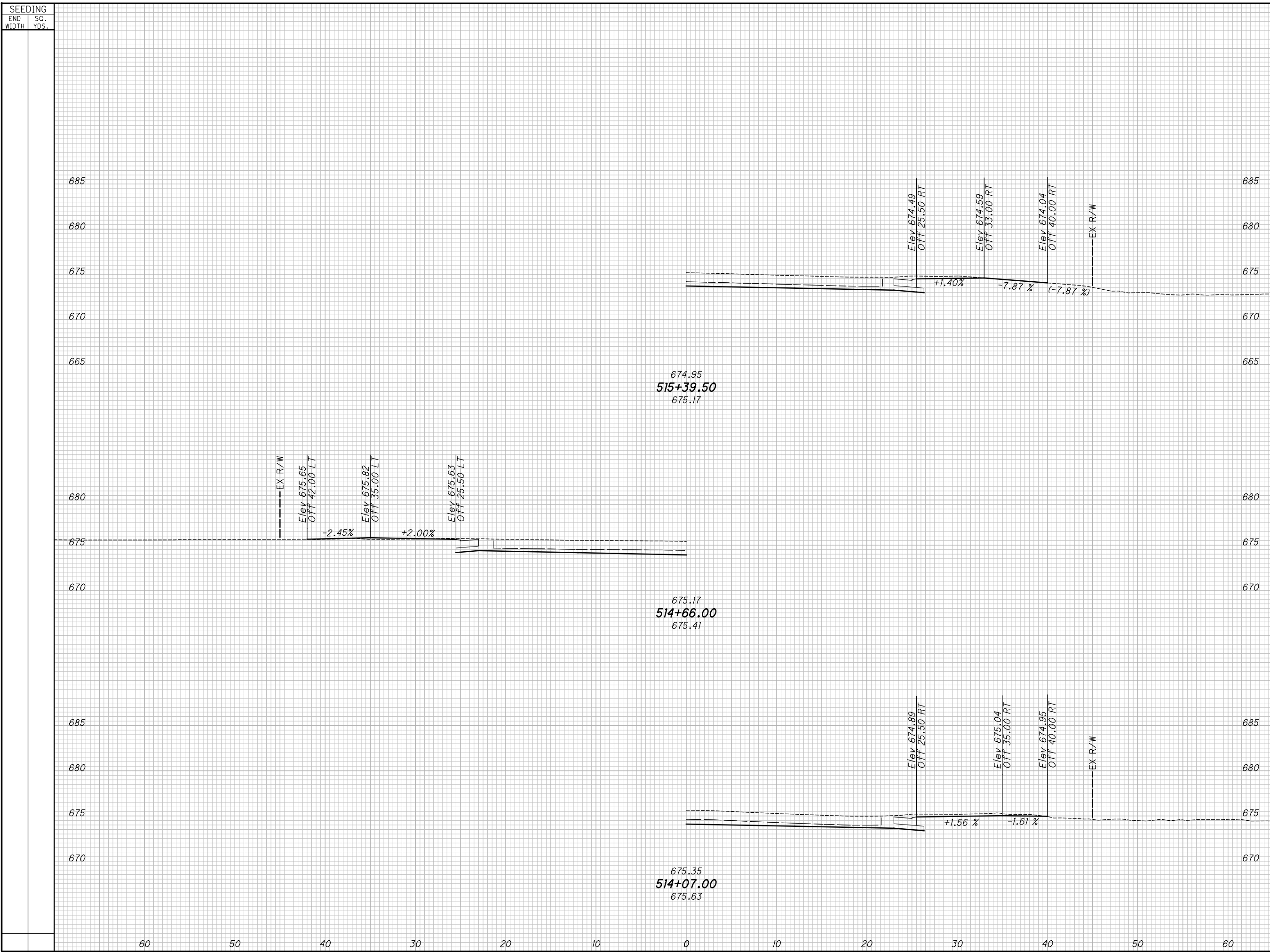


DRIVEWAY PROFILES
STA. 513+28.00 TO STA. 514+66.00

LAK-US-20-24.99
PART 2

449
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_25 7/15/2022 2:58:19 PM troyer

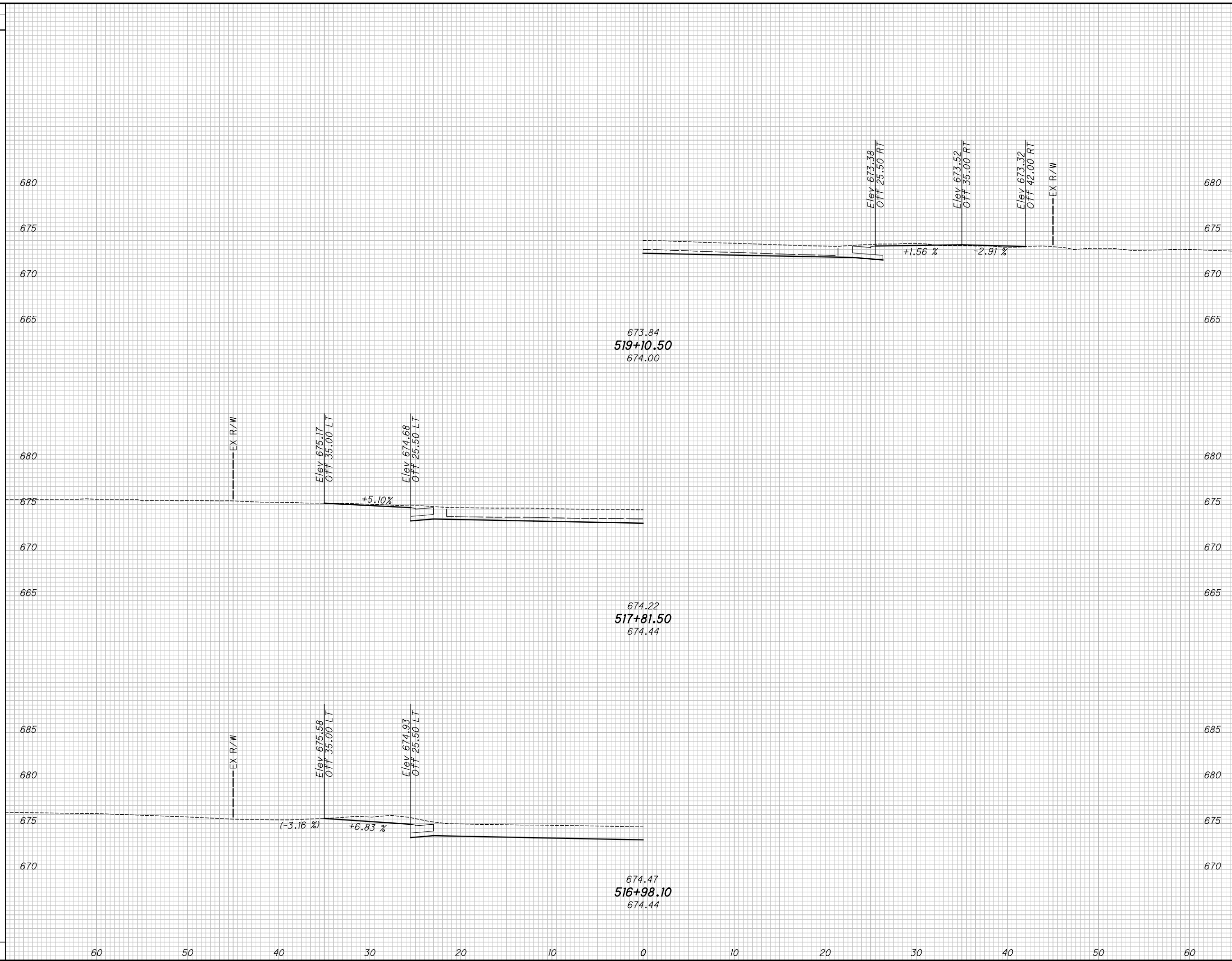


SEEDING		END AREA		VOLUME		CALCULATED	
END WIDTH	SO. YDS.	CUT	FILL	CUT	FILL	TLS	CHECKED
							JMP

DRIVEWAY PROFILES	
STA. 515+39.50 TO STA. 517+81.50	
LAK-US-20-24.99	PART 2
450	697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_26 7/15/2022 2:58:20 PM Troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

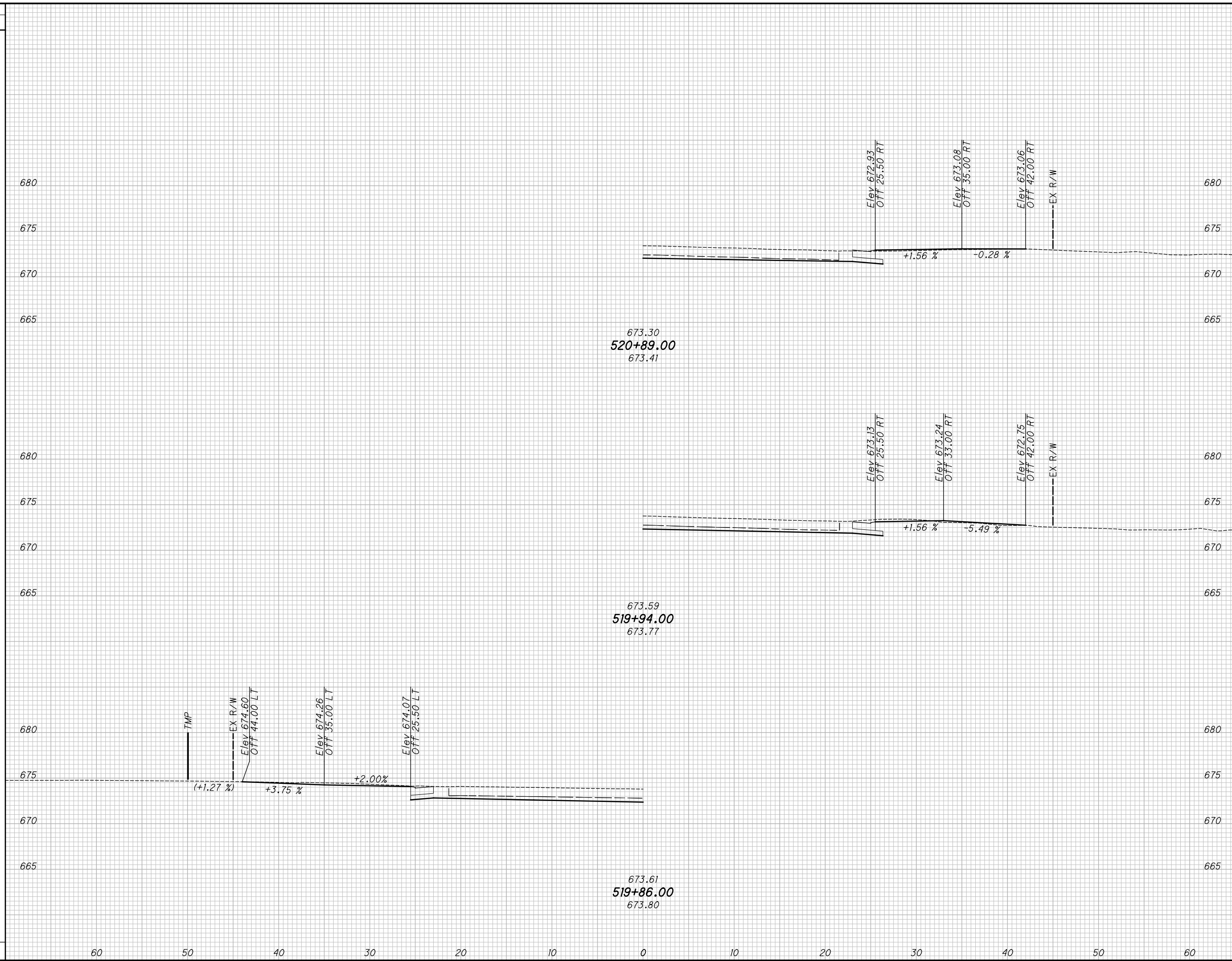
DRIVEWAY PROFILES
STA. 519+10.50 TO STA. 519+94.00

LAK-US-20-24.99
PART 2

451
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_27 7/15/2022 2:58:21PM Troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

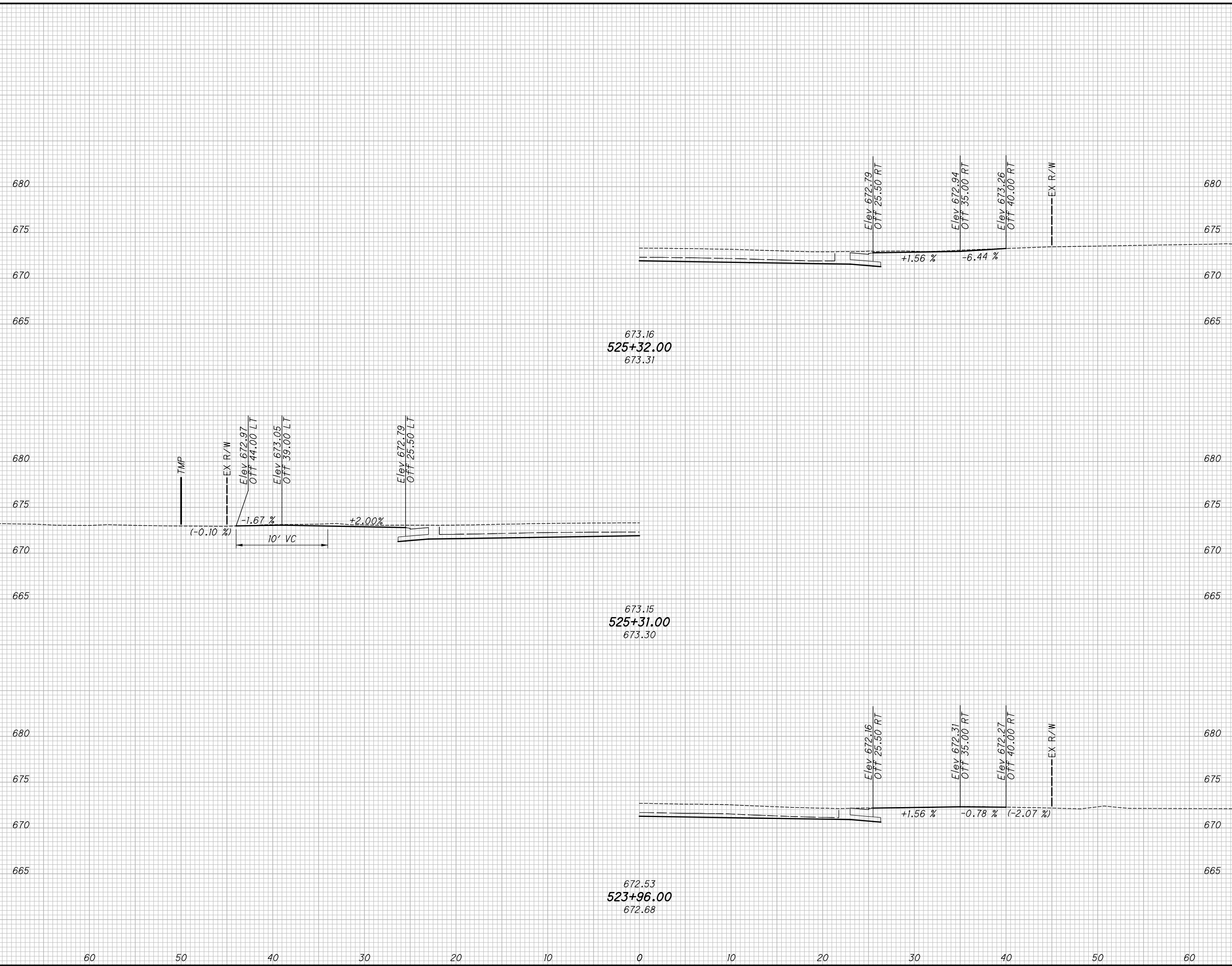
DRIVEWAY PROFILES
STA. 520+89.00 TO STA. 525+31.00

LAK-US-20-24.99
PART 2

(452 / 697)

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_28 7/15/2022 2:58:22 PM troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

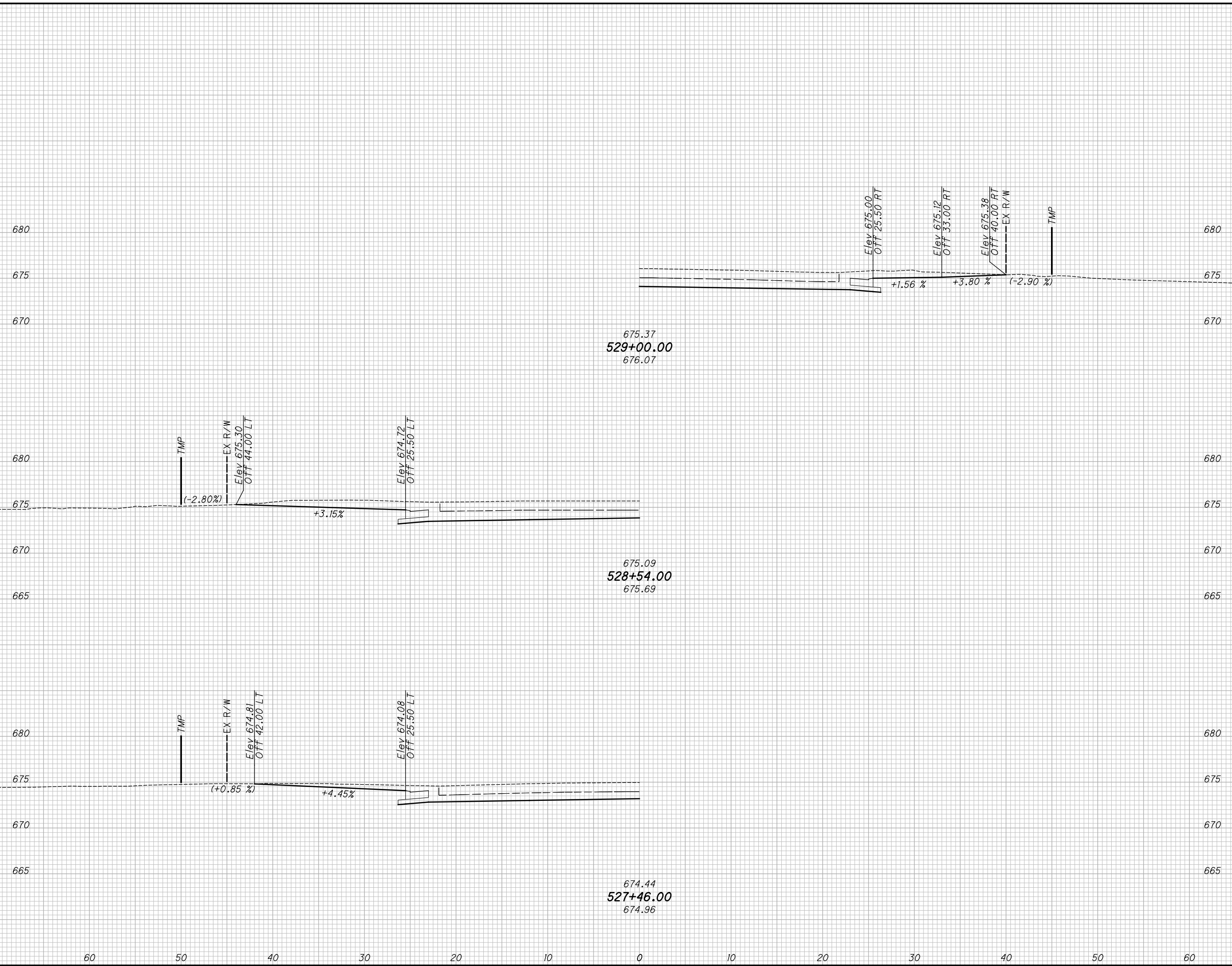
DRIVEWAY PROFILES
STA. 525+32.00 TO STA. 528+54.00

LAK-US-20-24.99
PART 2

453
697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_29 7/15/2022 2:58:23 PM troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

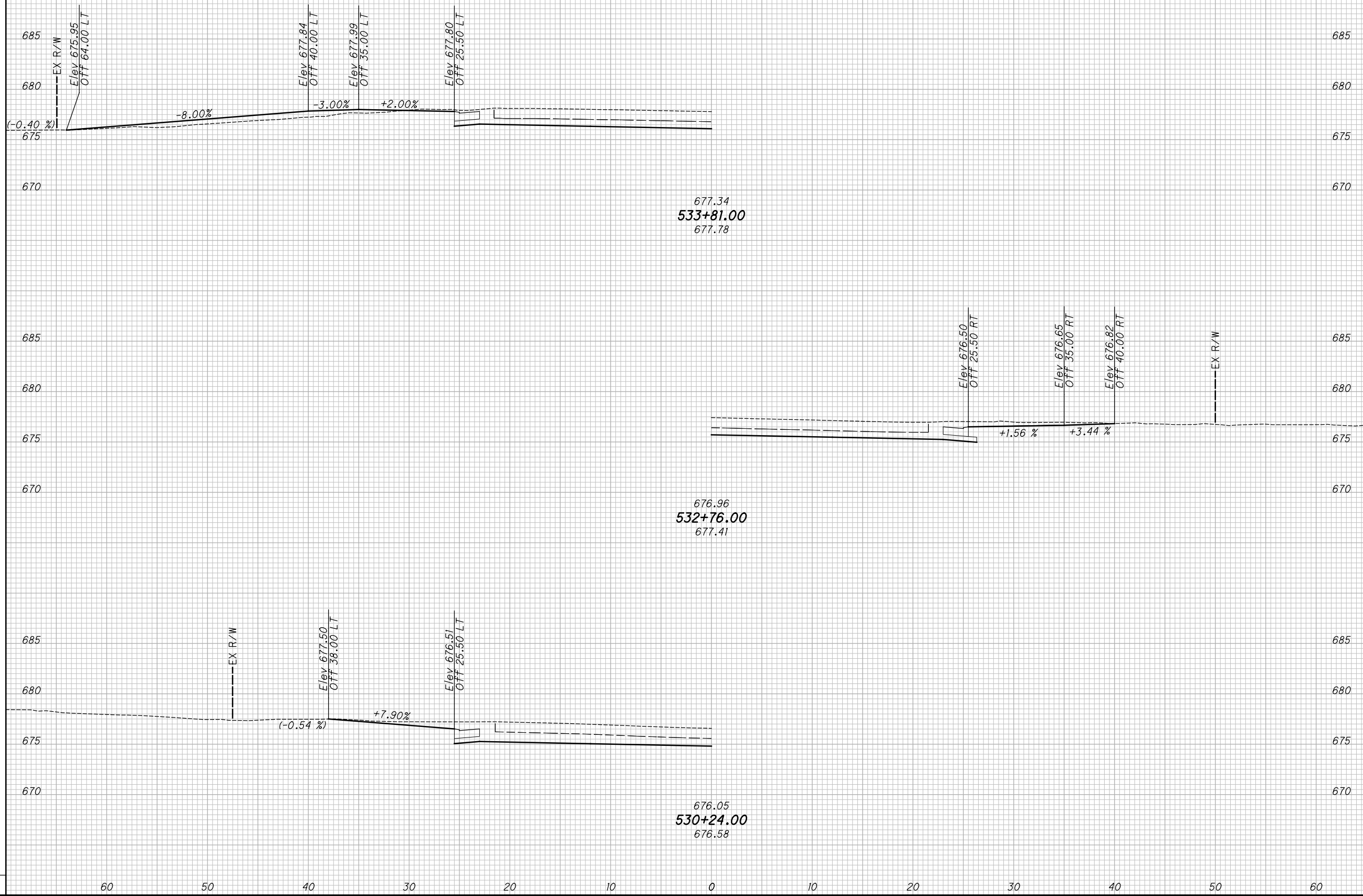
DRIVEWAY PROFILES
STA. 529+00.00 TO STA. 532+76.00

LAK-US-20-24.99
PART 2

454
697

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_30 7/15/2022 2:58:25 PM Troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

DRIVEWAY PROFILES
STA. 533+81.00 TO STA. 535+07.00

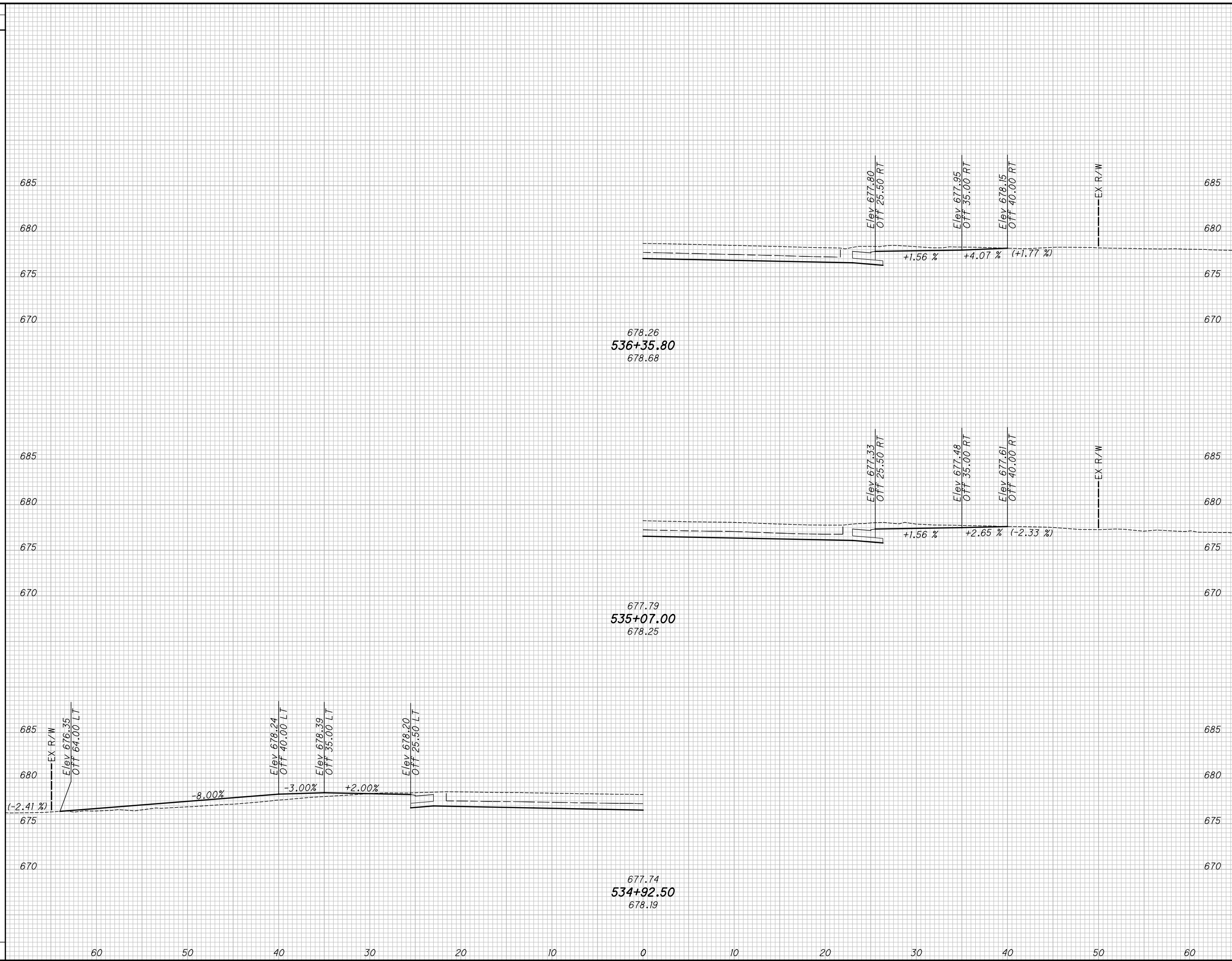
LAK-US-20-24.99
PART 2

455
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_31 7/5/2022 2:58:31PM troyer

SEEDING

END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

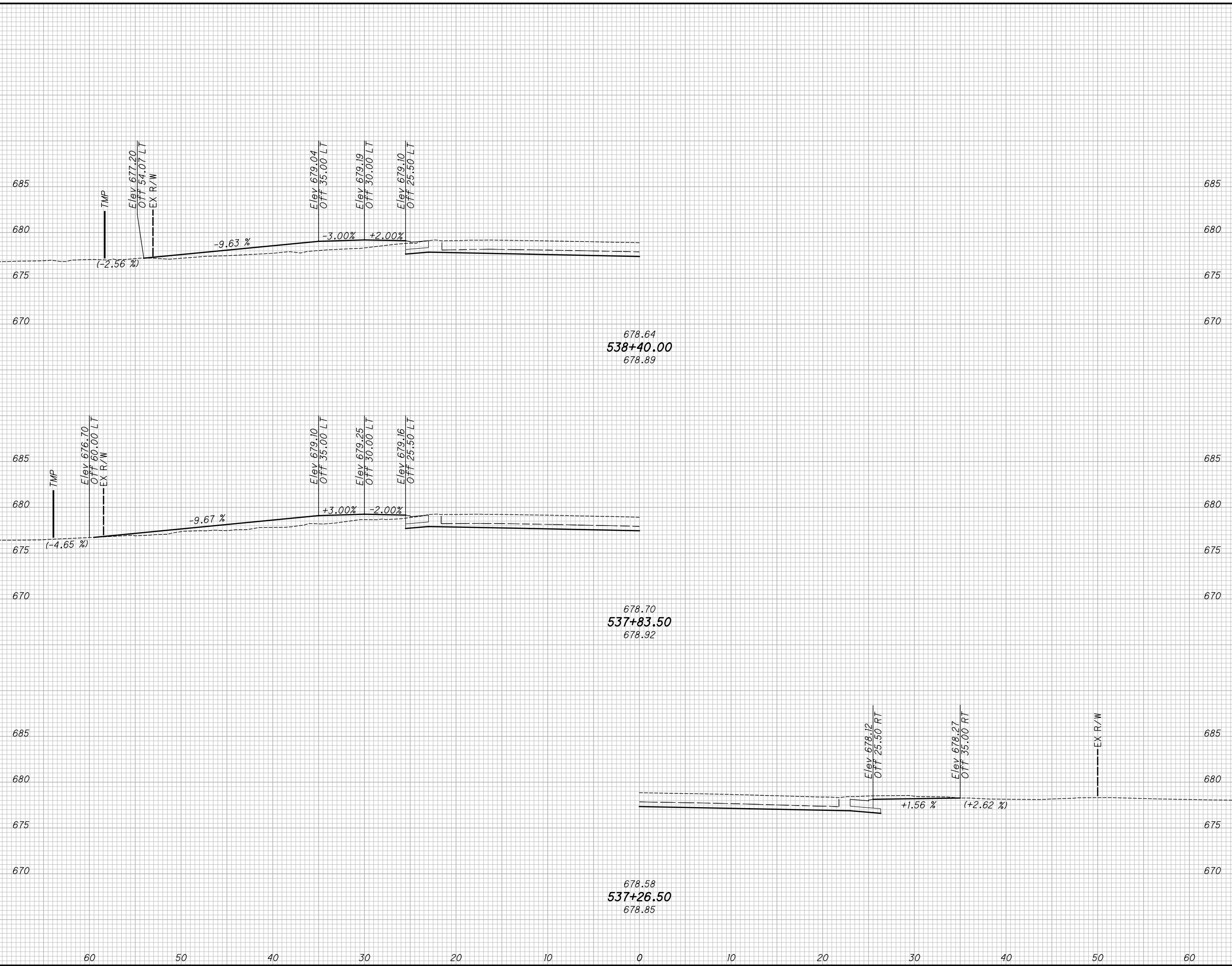
DRIVEWAY PROFILES
STA. 536+35.80 TO STA. 537+83.50

LAK-US-20-24.99
PART 2

456
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_32 7/15/2022 2:58:42 PM troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

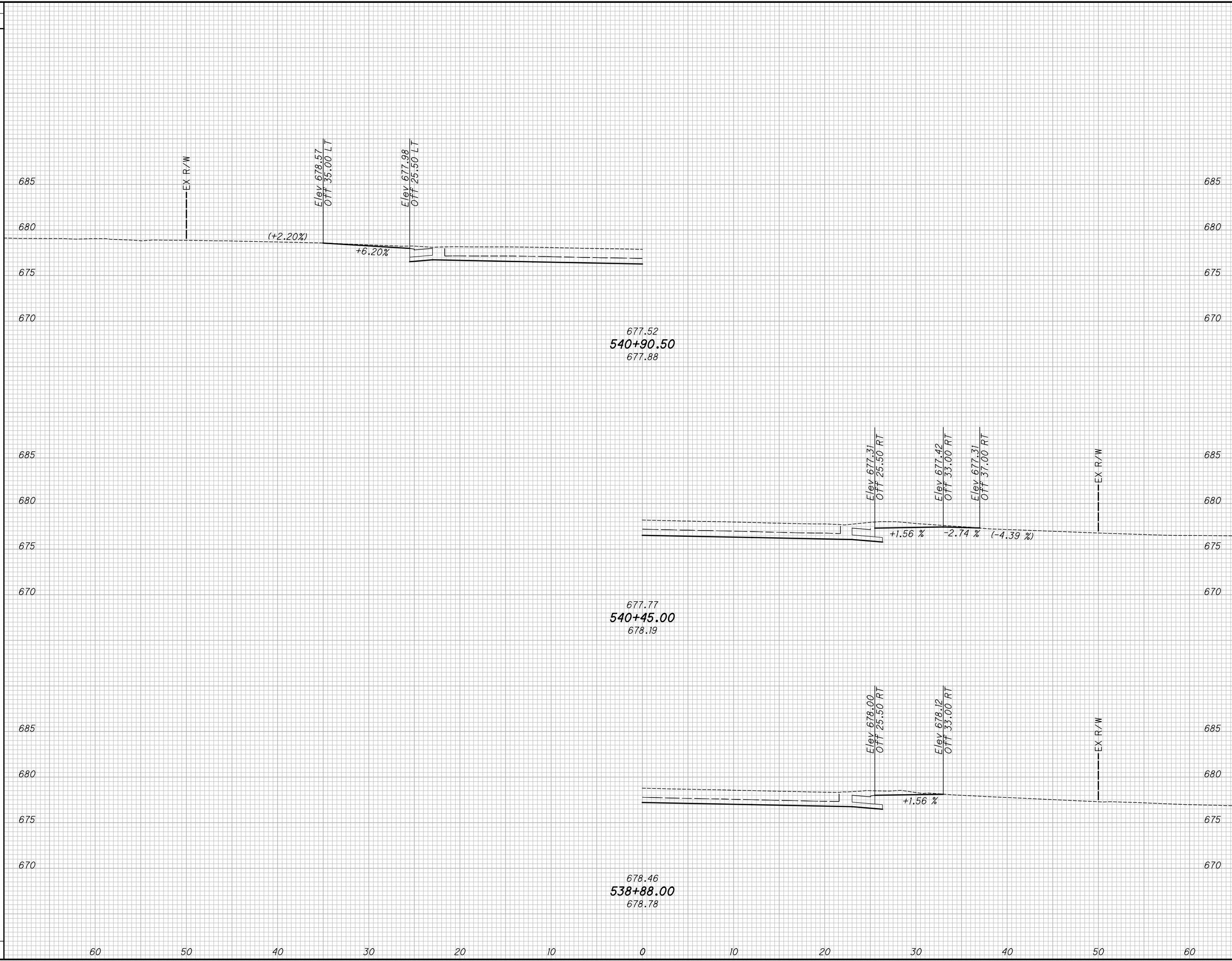
DRIVEWAY PROFILES
STA. 538+40.00 TO STA. 540+45.00

LAK-US-20-24.99
PART 2

457
697

H:\2018\180743\Projec+Data\092TO_LAK_20_EAST\Design\Roadway\Sheets\092TO_XS10.dgn XS_SHEET_temporary_model_name_33 7/15/2022 2:58:43 PM Troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

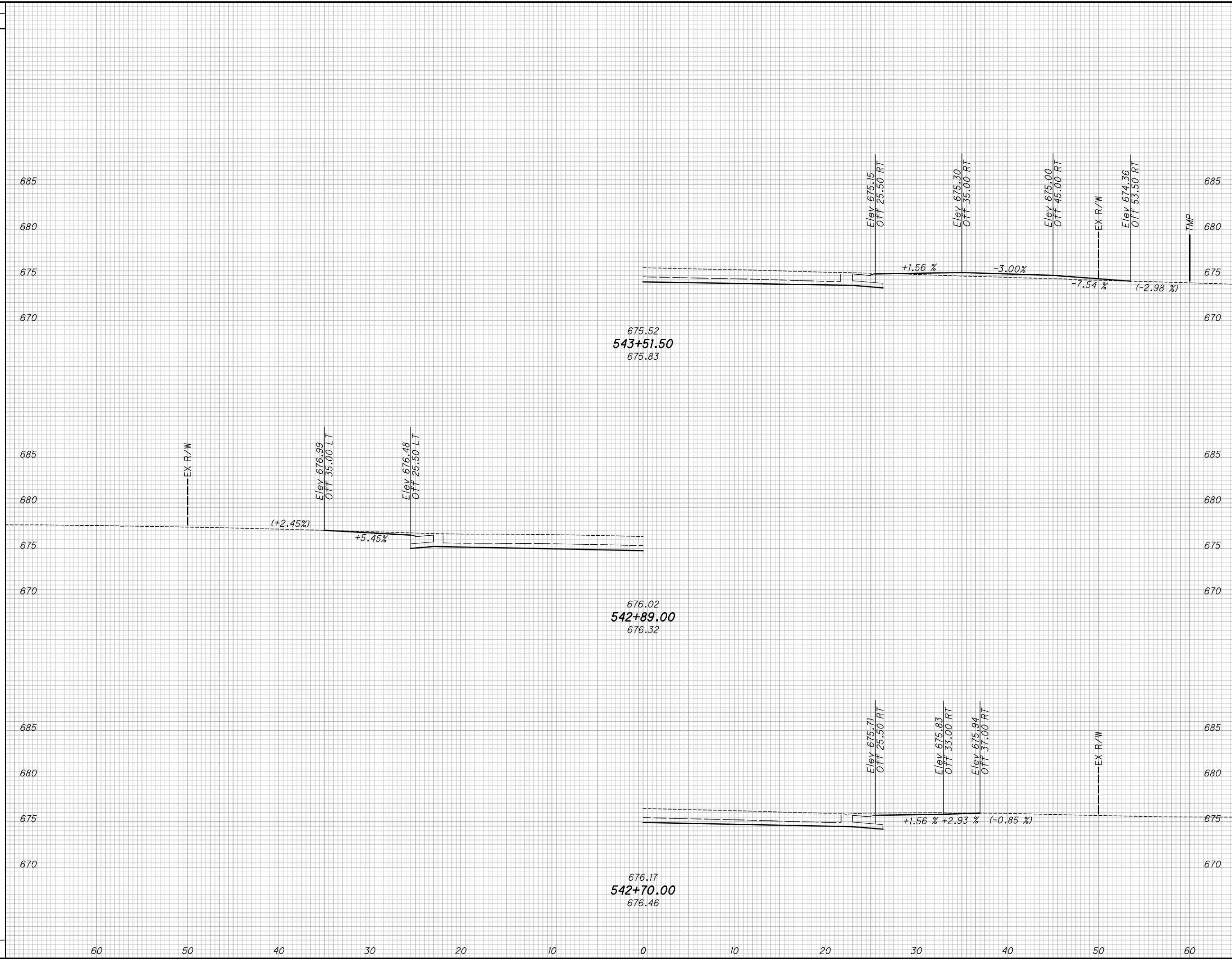
DRIVEWAY PROFILES
STA. 540+90.50 TO STA. 542+89.00

LAK-US-20-24.99
PART 2

458
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_34 7/15/2022 2:58:45 PM troyer

SEEDING
END SO.
WIDTH YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

DRIVEWAY PROFILES
STA. 543+51.50 TO STA. 547+05.00

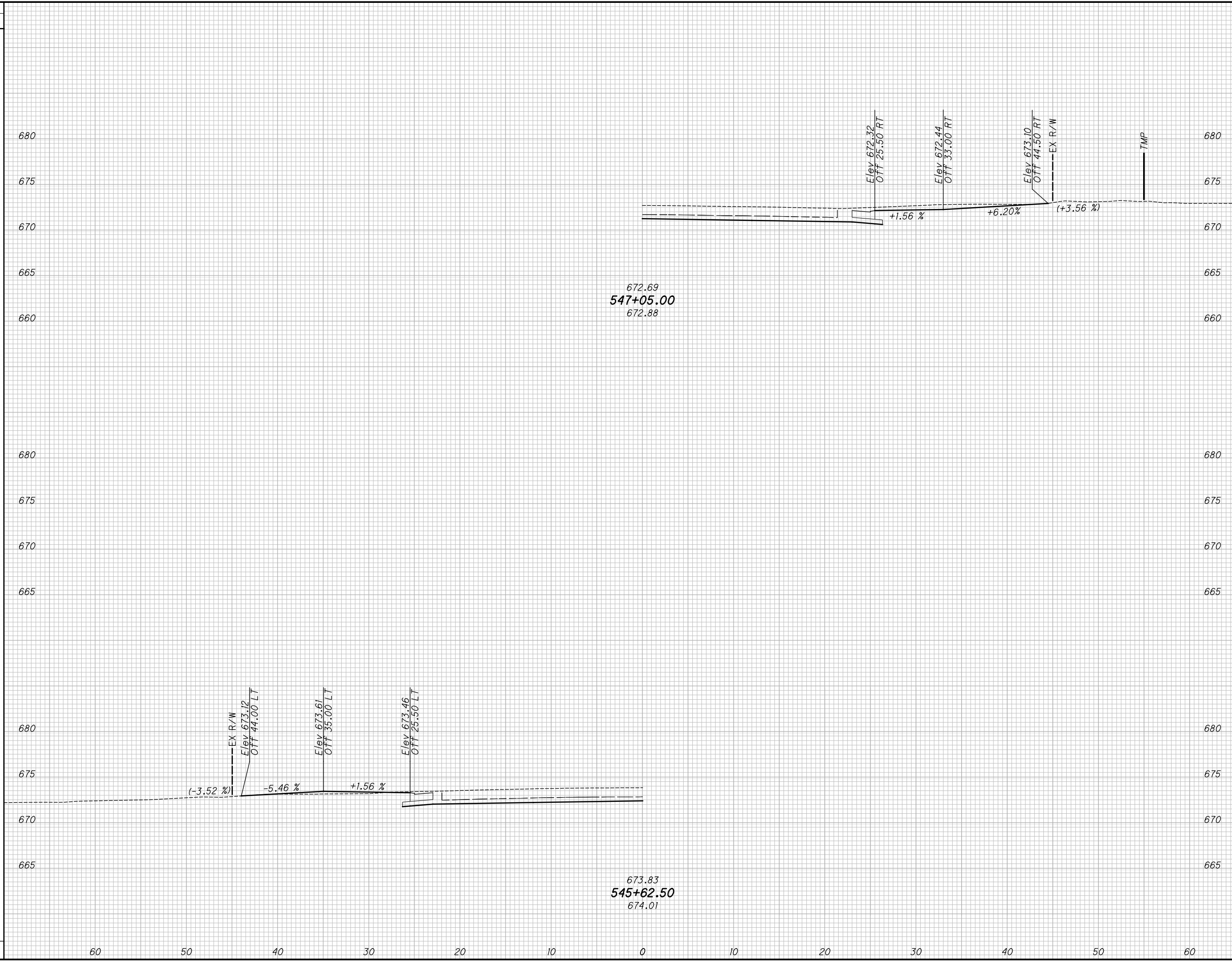
LAK-US-20-24.99
PART 2

459
697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_35 7/15/2022 2:58:46 PM Troyer

SEEDING

END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

DRIVEWAY PROFILES
STA. 550+31.50 TO STA. 551+97.00

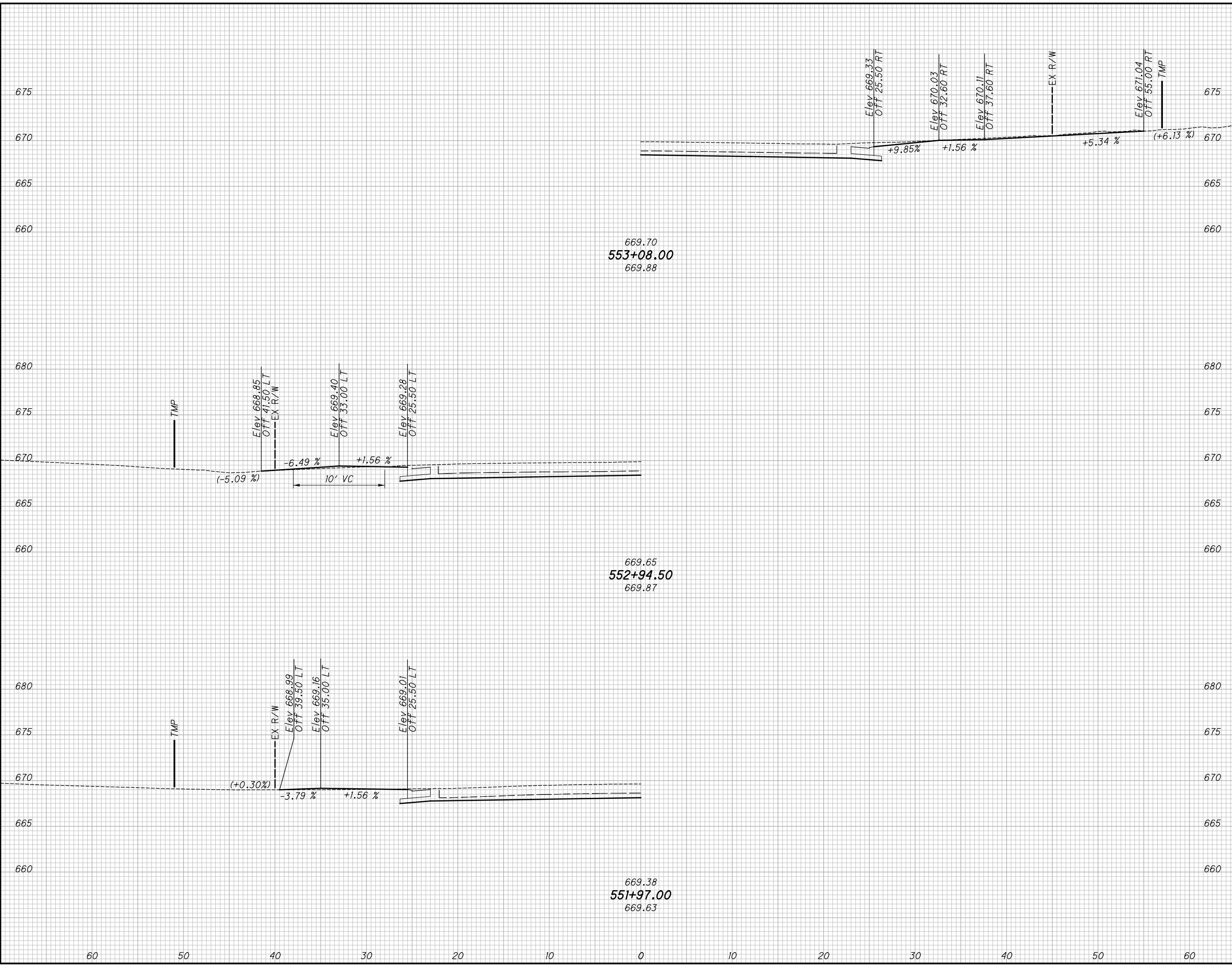
LAK-US-20-24.99
PART 2

460
697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_36 7/15/2022 2:58:47 PM Troyer

SEEDING

END WIDTH	SO. YDS.
60	
50	
40	
30	
20	
10	
0	
10	
20	
30	
40	
50	
60	



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

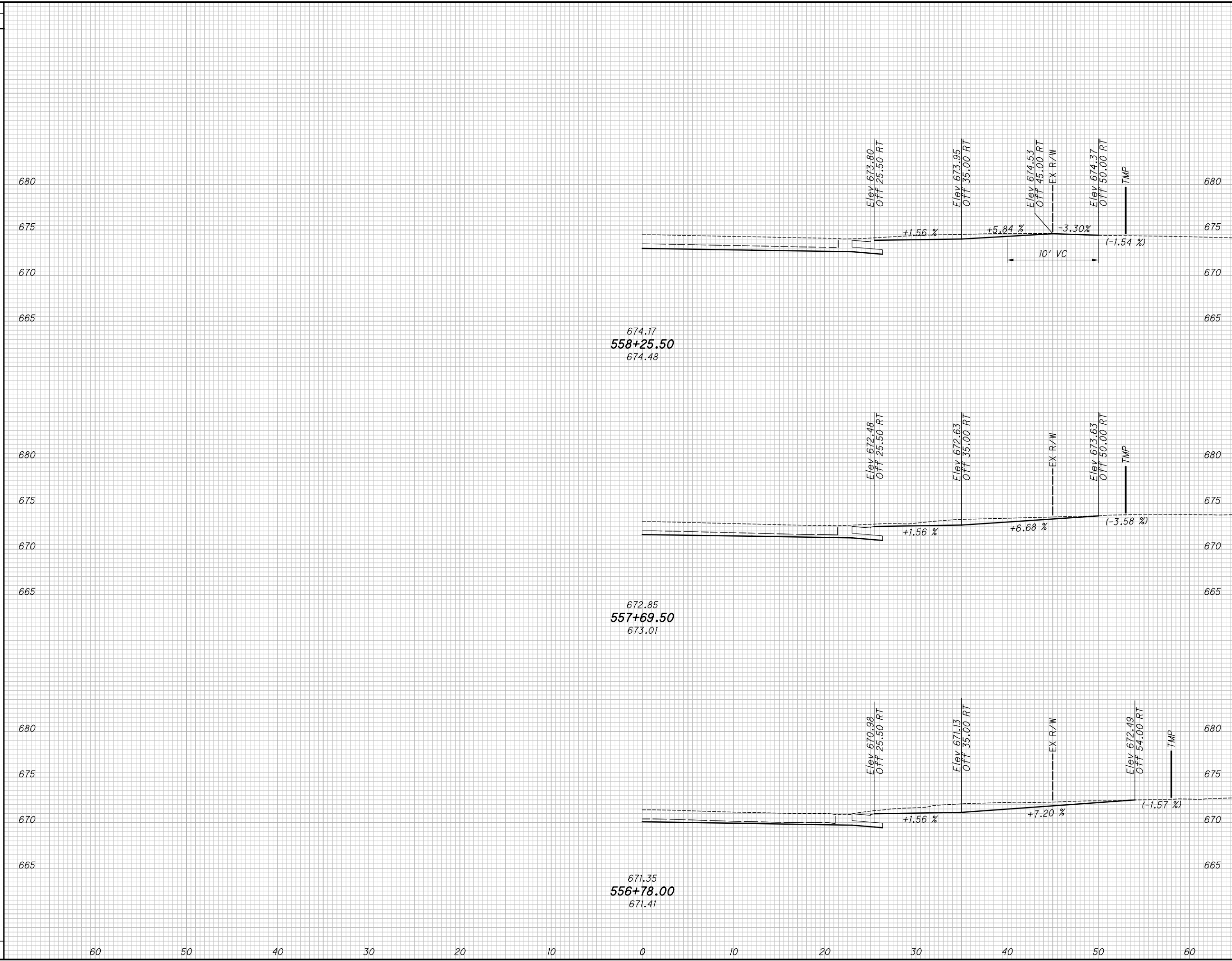
DRIVEWAY PROFILES
STA. 552+94.50 TO STA. 553+08.00

LAK-US-20-24.99
PART 2

461
697

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_37 7/15/2022 2:58:48 PM troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED	
CUT	FILL	CUT	FILL	TLS	JMP

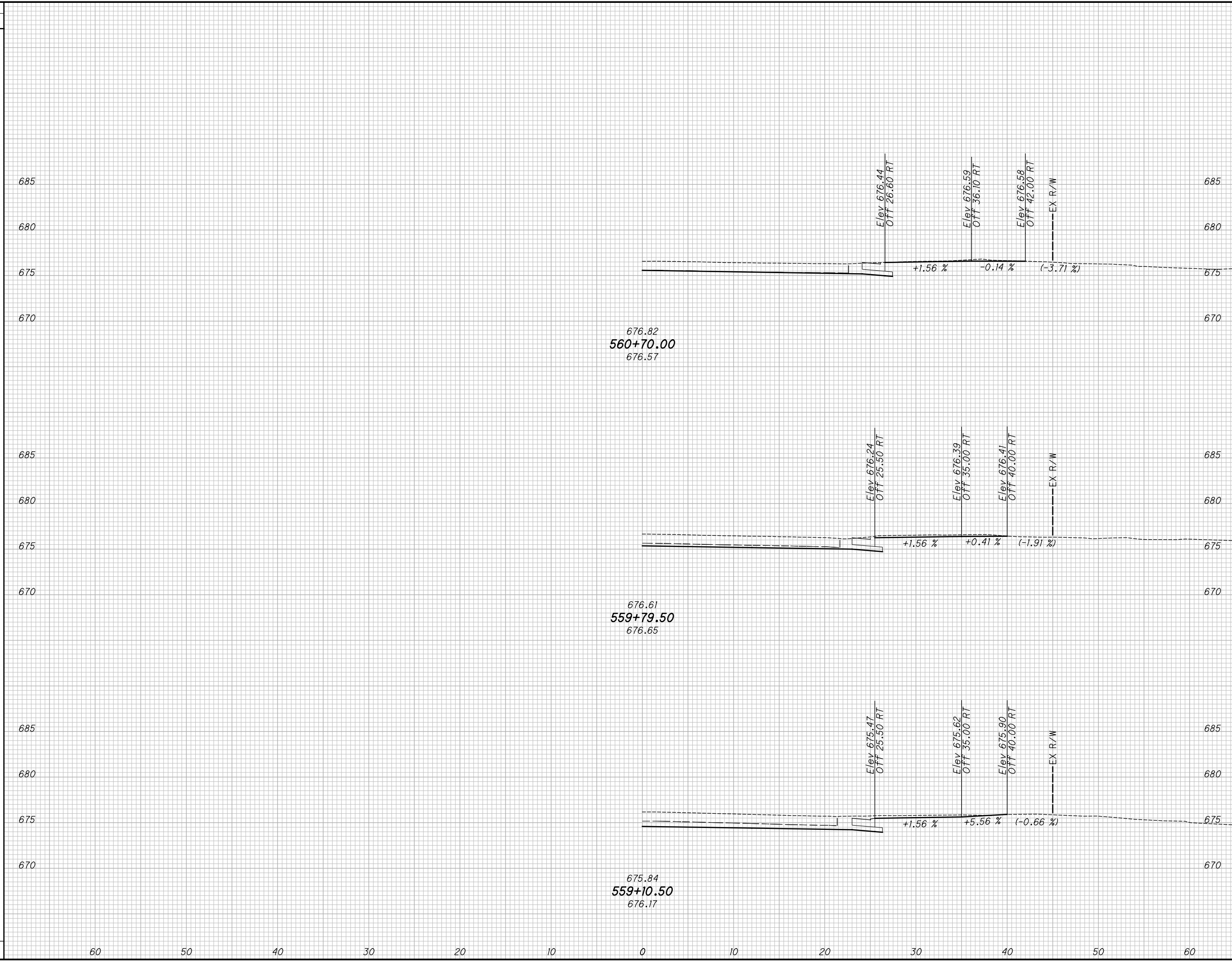
DRIVEWAY PROFILES
STA. 556+78.00 TO STA. 558+28.00

LAK-US-20-24.99
PART 2

462
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_38 7/15/2022 2:58:49 PM troyer

SEEDING	
END WIDTH	SO. YDS.



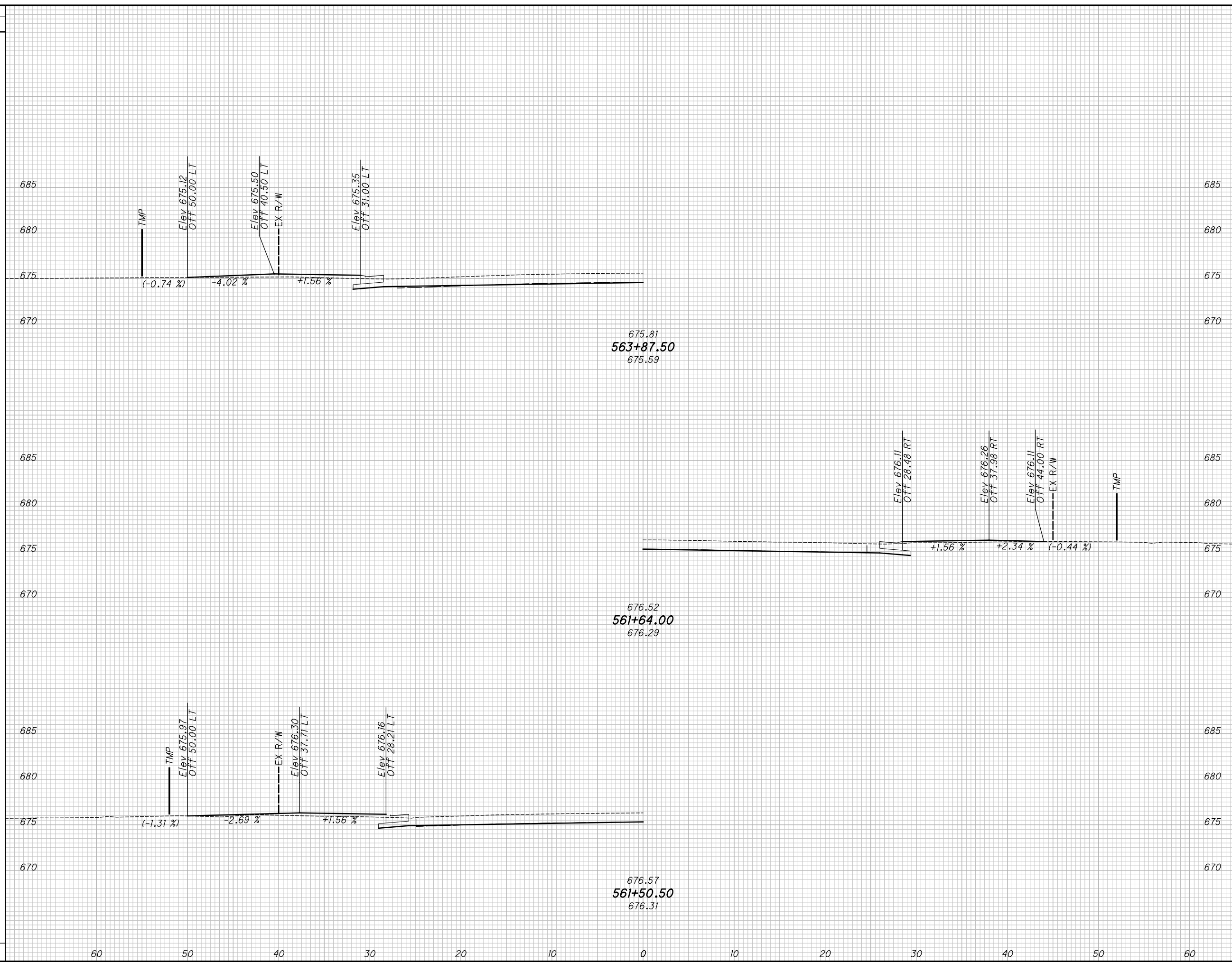
END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

DRIVEWAY PROFILES
STA. 559 + 10.50 TO STA. 560 + 70.00

LAK-US-20-24.99
PART 2

463
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_39 7/15/2022 2:58:50 PM Troyer



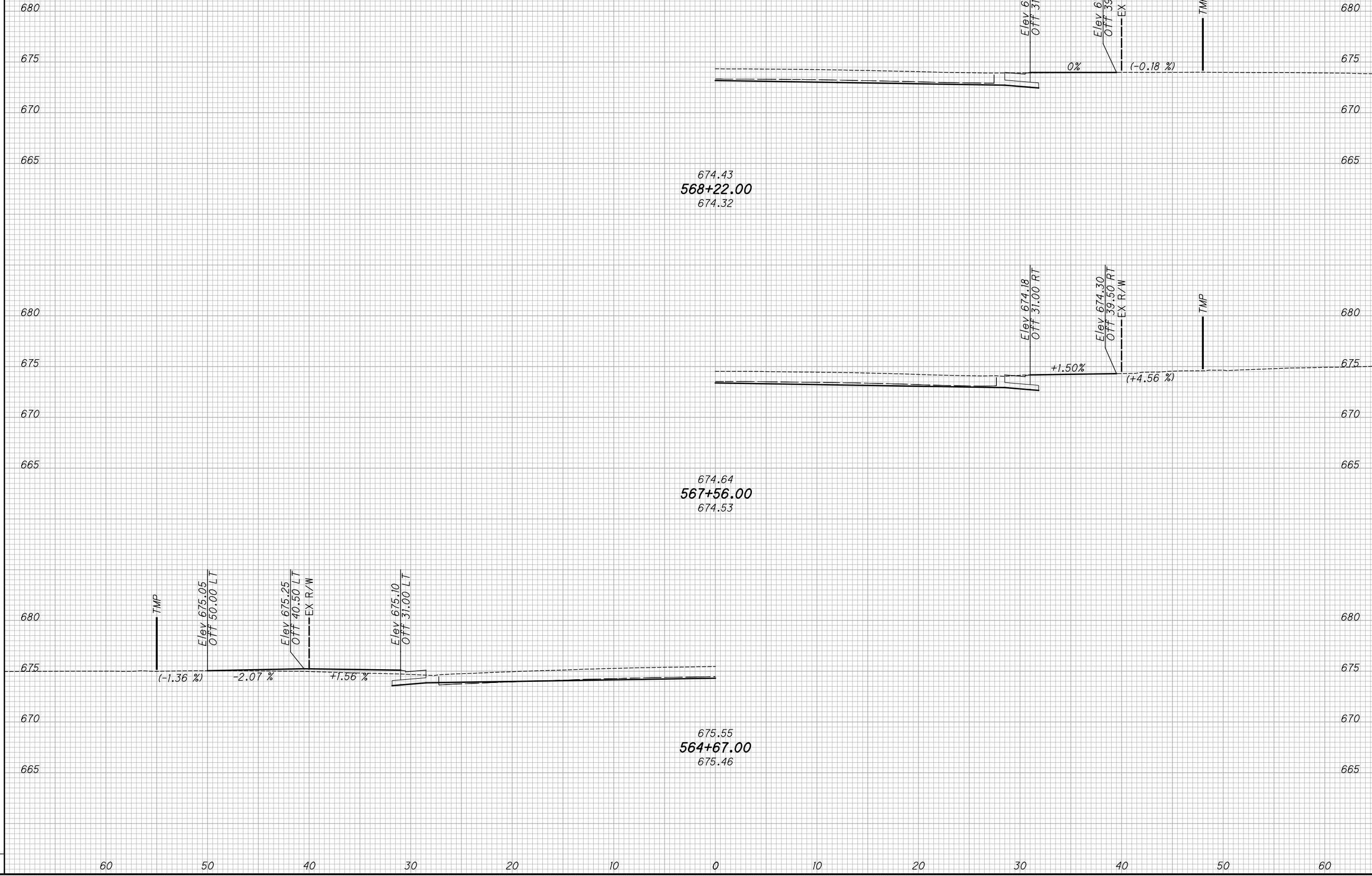
SEEDING		END AREA		VOLUME		CALCULATED	
END WIDTH	SO. YDS.	CUT	FILL	CUT	FILL	TLS	CHECKED
							JMP

DRIVEWAY PROFILES	
STA. 561+50.50 TO STA. 563+87.50	
LAK-US-20-24.99	
PART 2	

464
697

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_40 7/15/2022 2:58:52 PM Troyer

SEEDING
END SO.
WIDTH YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

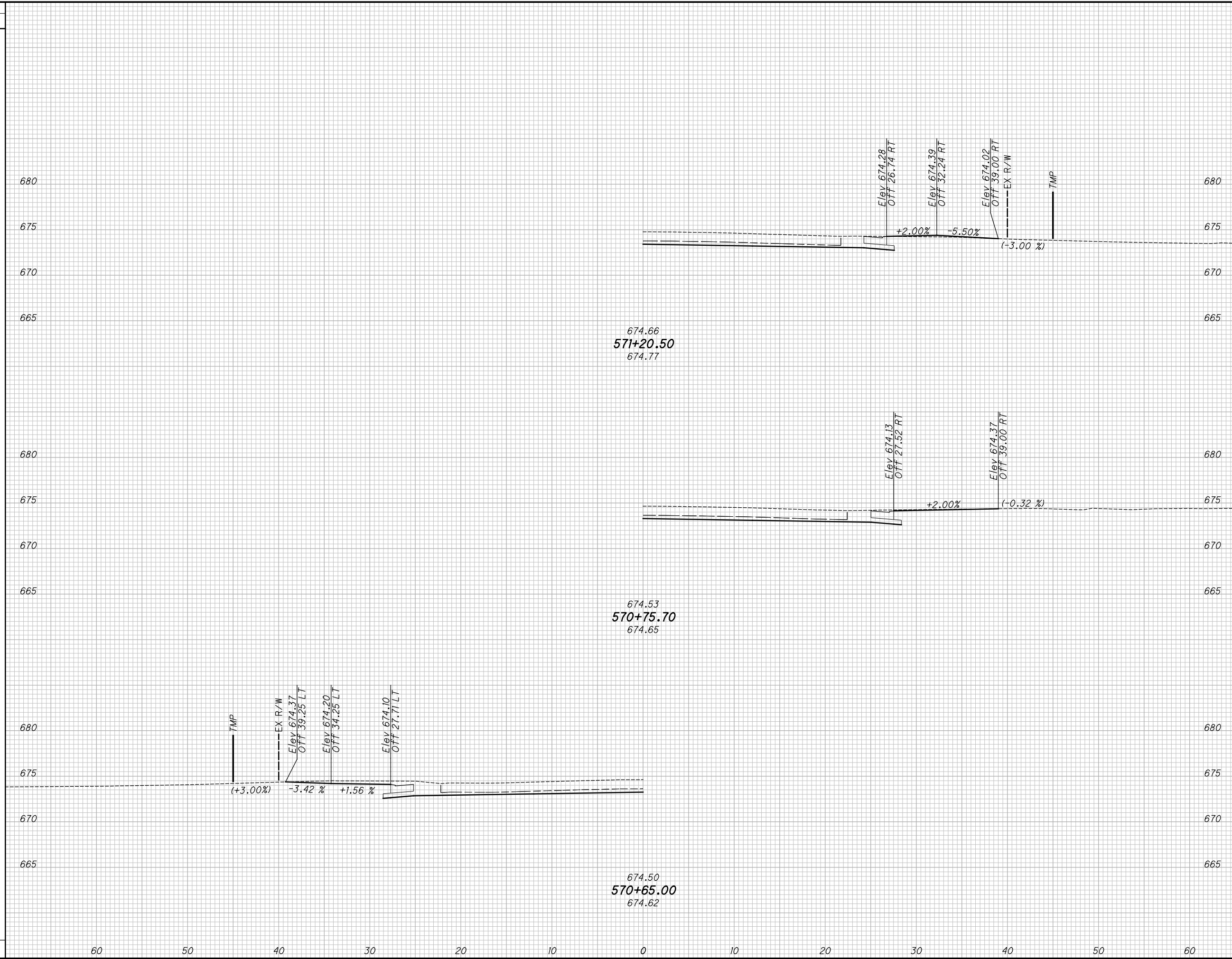
**LAK-US-20-24.99
PART 2**

**DRIVEWAY PROFILES
STA. 564+67.00 TO STA. 568+22.00**

465
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn 7/15/2022 2:58:54 PM troyer

SEEDING	
END WIDTH	SO. YDS.



END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		

DRIVEWAY PROFILES
STA. 570+65.00 TO STA. 571+20.50

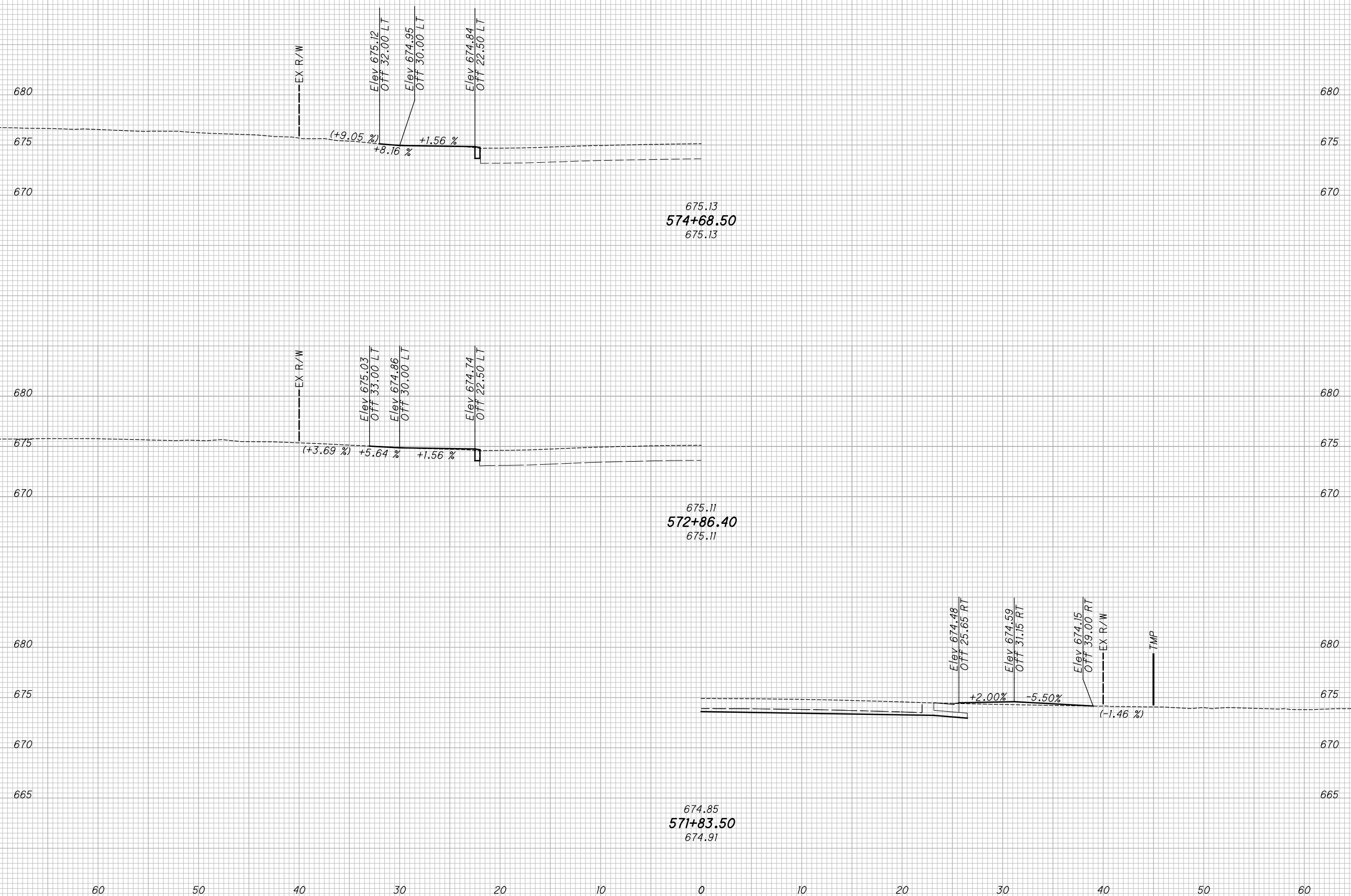
LAK-US-20-24.99
PART 2

(467)
(697)

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_43 7/15/2022 2:58:55 PM troyer

SEEDING
END SO.
WIDTH YDS.

END AREA
CUT FILL
VOLUME
CUT FILL
CALCULATED
TLS
CHECKED
JMP



DRIVEWAY PROFILES
STA. 571+83.50 TO STA. 574+68.50

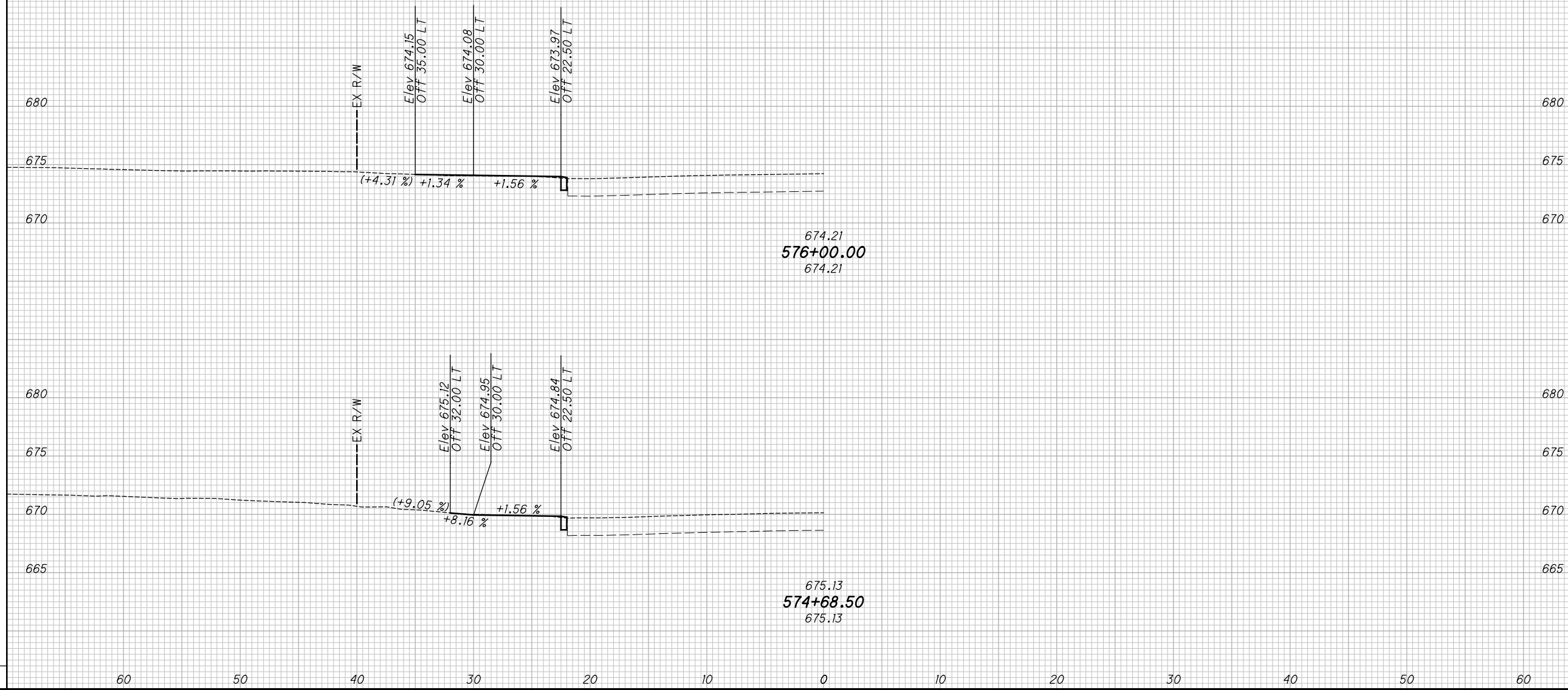
LAK-US-20-24.99
PART 2

468
697

H:\2018\180743\Projec+Data\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_XS10.dgn XS_SHEET_temporary_model_name_44 7/15/2022 2:58:57 PM troyer

SEEDING	
END WIDTH	SO. YDS.

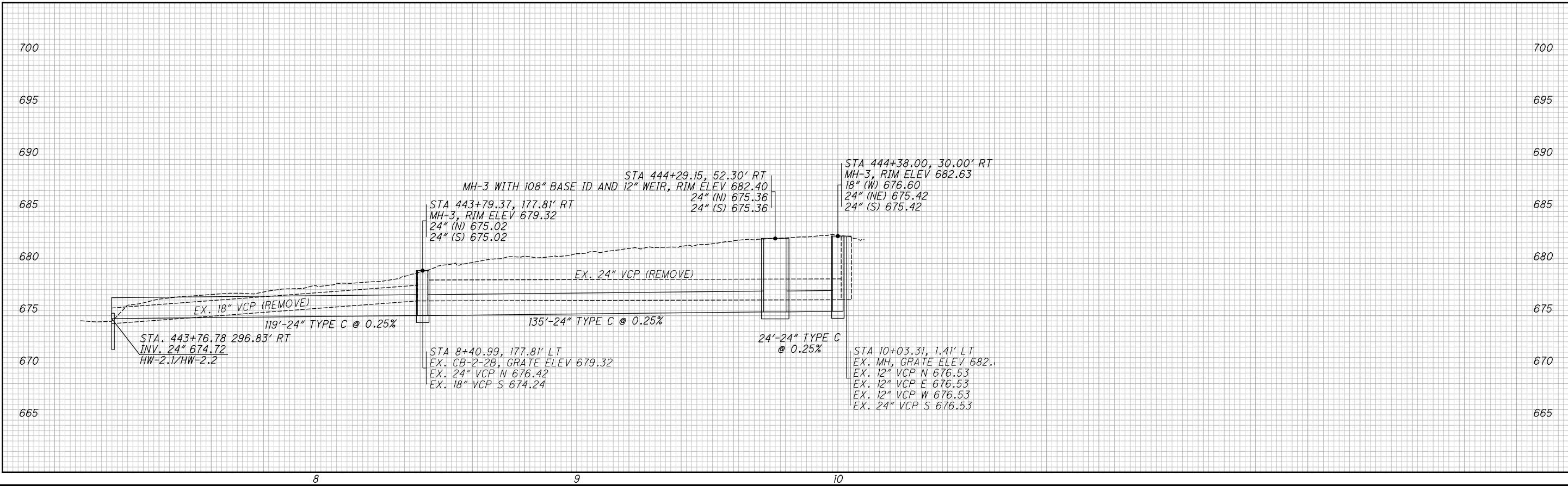
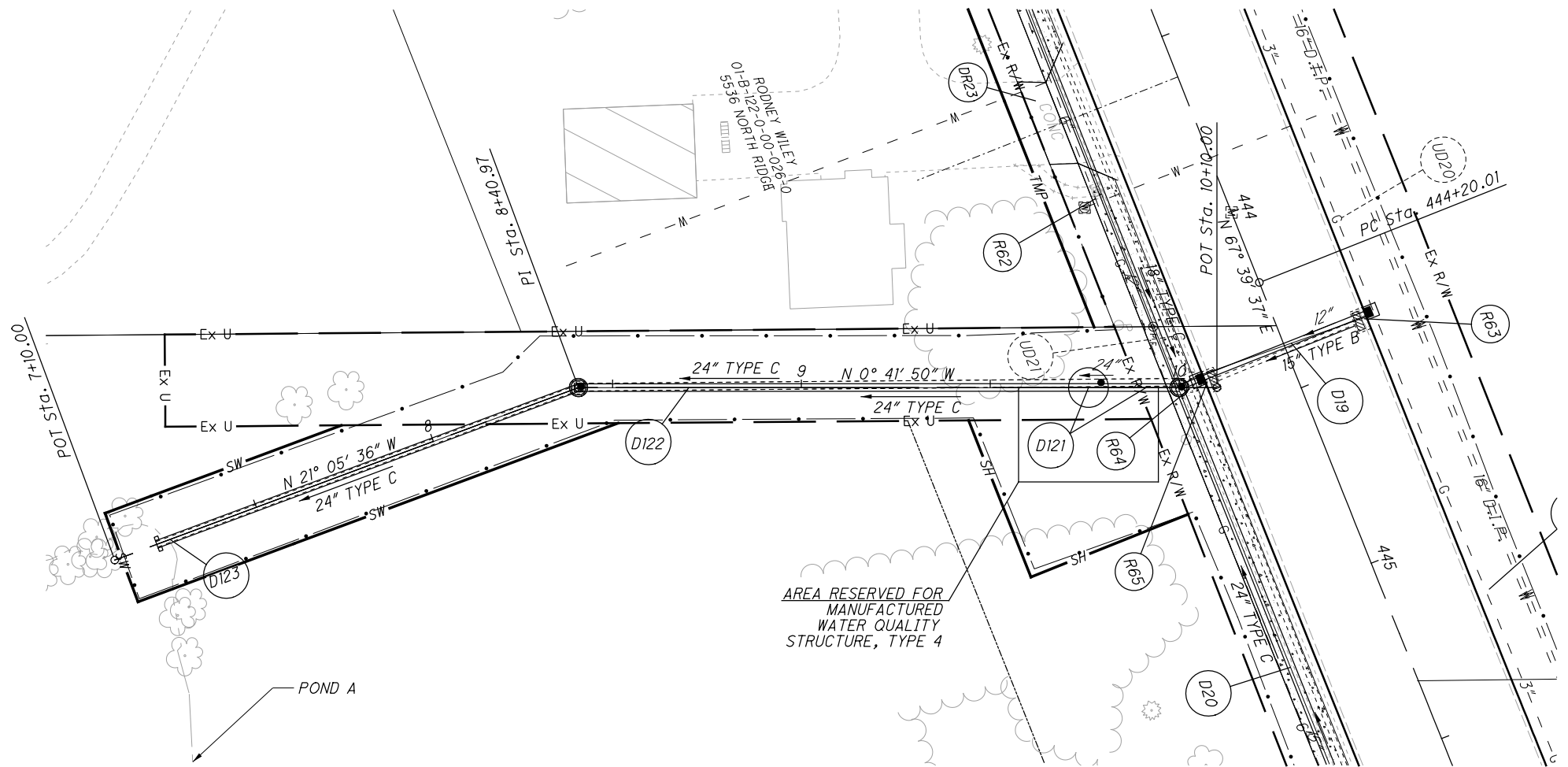
END AREA		VOLUME		CALCULATED TLS	CHECKED JMP
CUT	FILL	CUT	FILL		



LAK-US-20-24.99
PART 2
DRIVEWAY PROFILES
STA. 574+68.50 TO STA. 576+00.00

469
697

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Drainage\Sheets\09270_DP002_STA_444+00.dgn Sheet 7/15/2022 2:58:59 PM Troyer



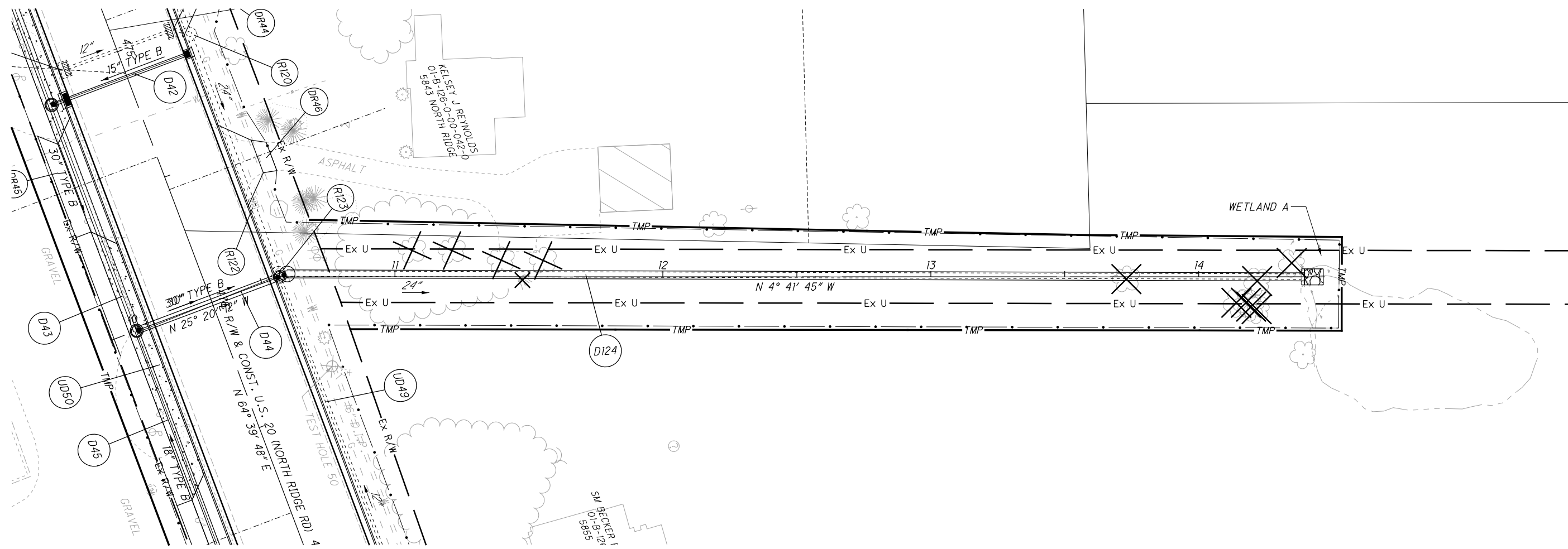
CALCULATED
TJS
CHECKED
JMP

PLAN AND PROFILE - STORM SEWER
STA. 444+38

LAK-US-20-24.99
PART 2

470
697

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Drainage\Sheets\09270_DP0002_STA_476+00.dgn Sheet 7/15/2022 2:59:00 PM troyer



CALCULATED
TLS
CHECKED
JMP

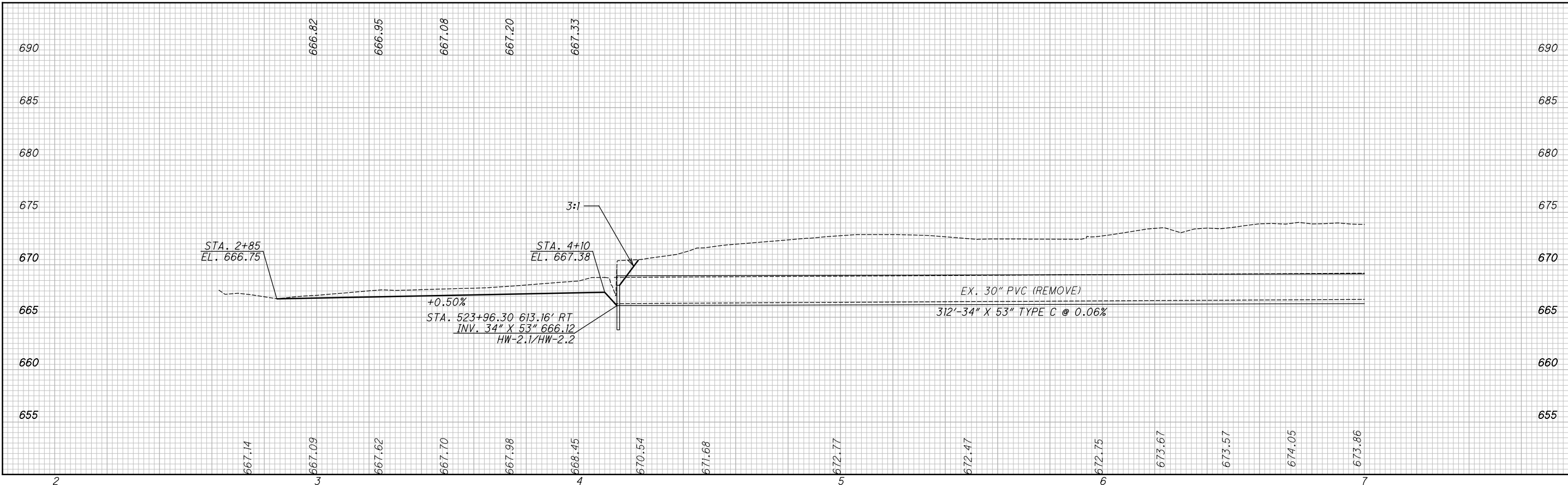
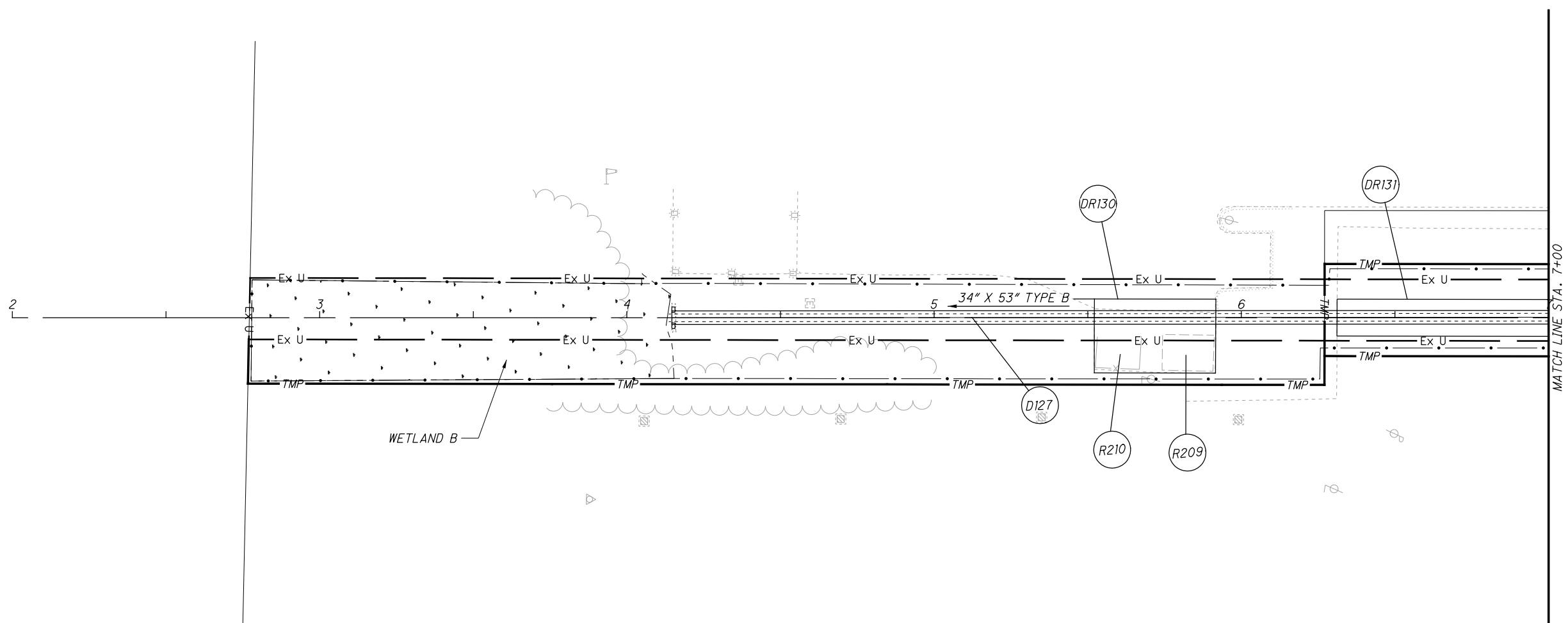
0 20 40
HORIZONTAL
SCALE IN FEET

PLAN AND PROFILE - STORM SEWER
STA. 476+00

LAK-US-20-24.99
PART 2

471
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Drainage\Sheets\09270_DP003_STA_524+50.dgn Sheet 1 7/15/2022 2:59:01 PM troyer

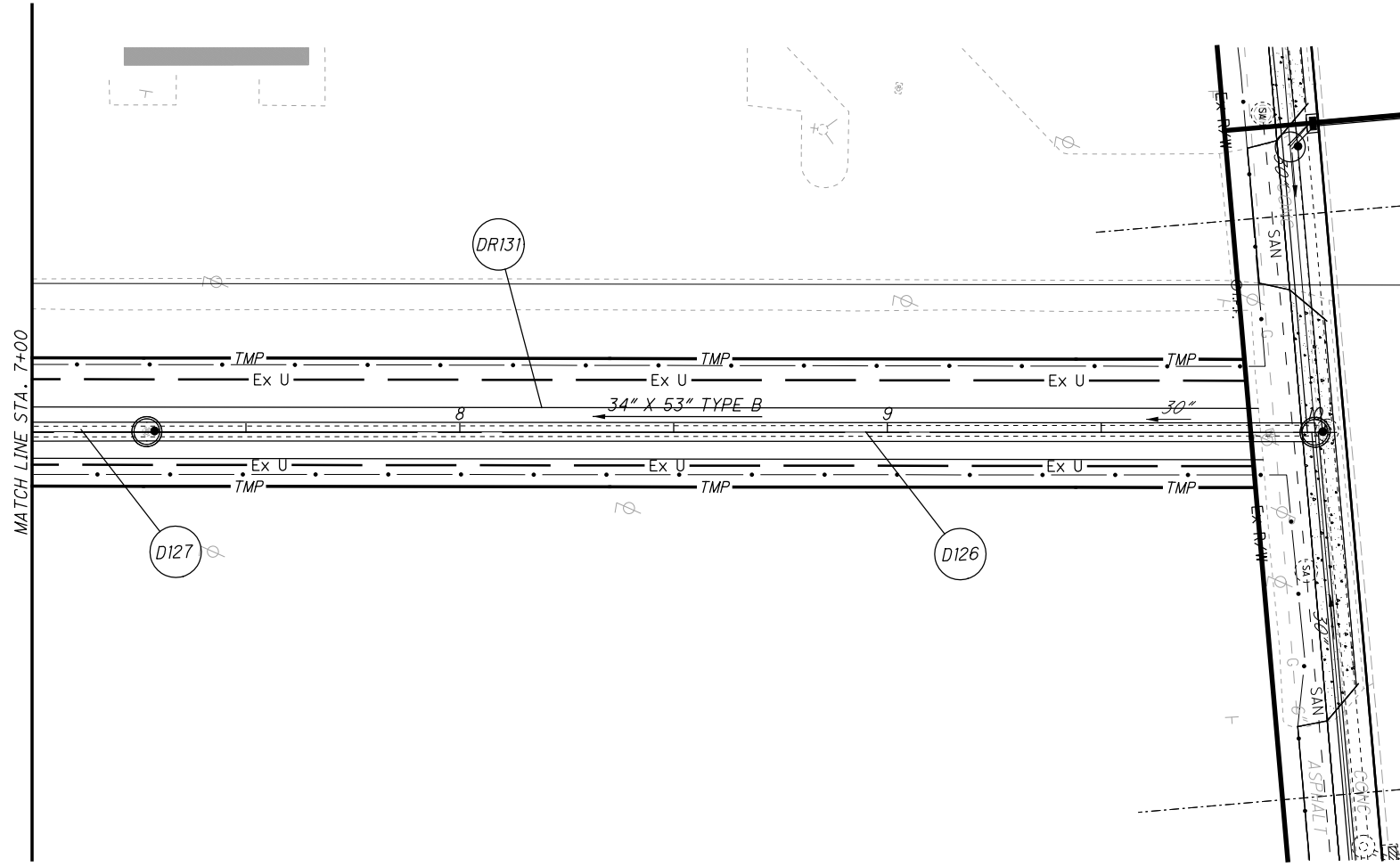
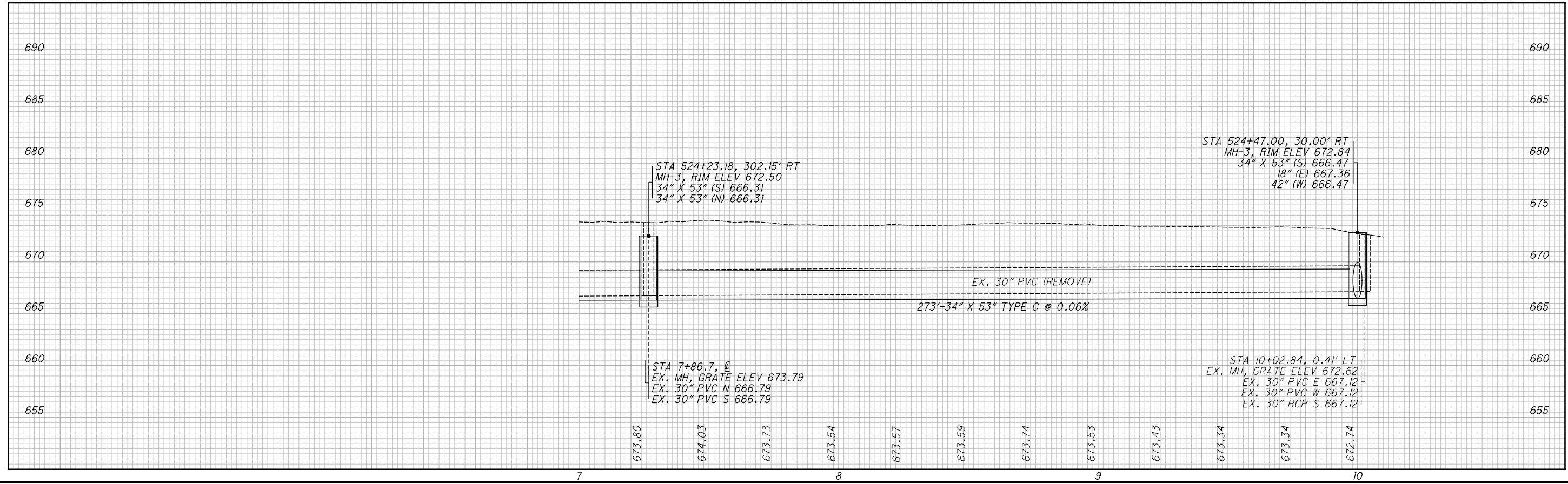


CALCULATED
TLS
CHECKED
JMP

**PLAN AND PROFILE - STORM SEWER
OUTLET STA. 524+50**

**LAK-US-20-24.99
PART 2**

473
697



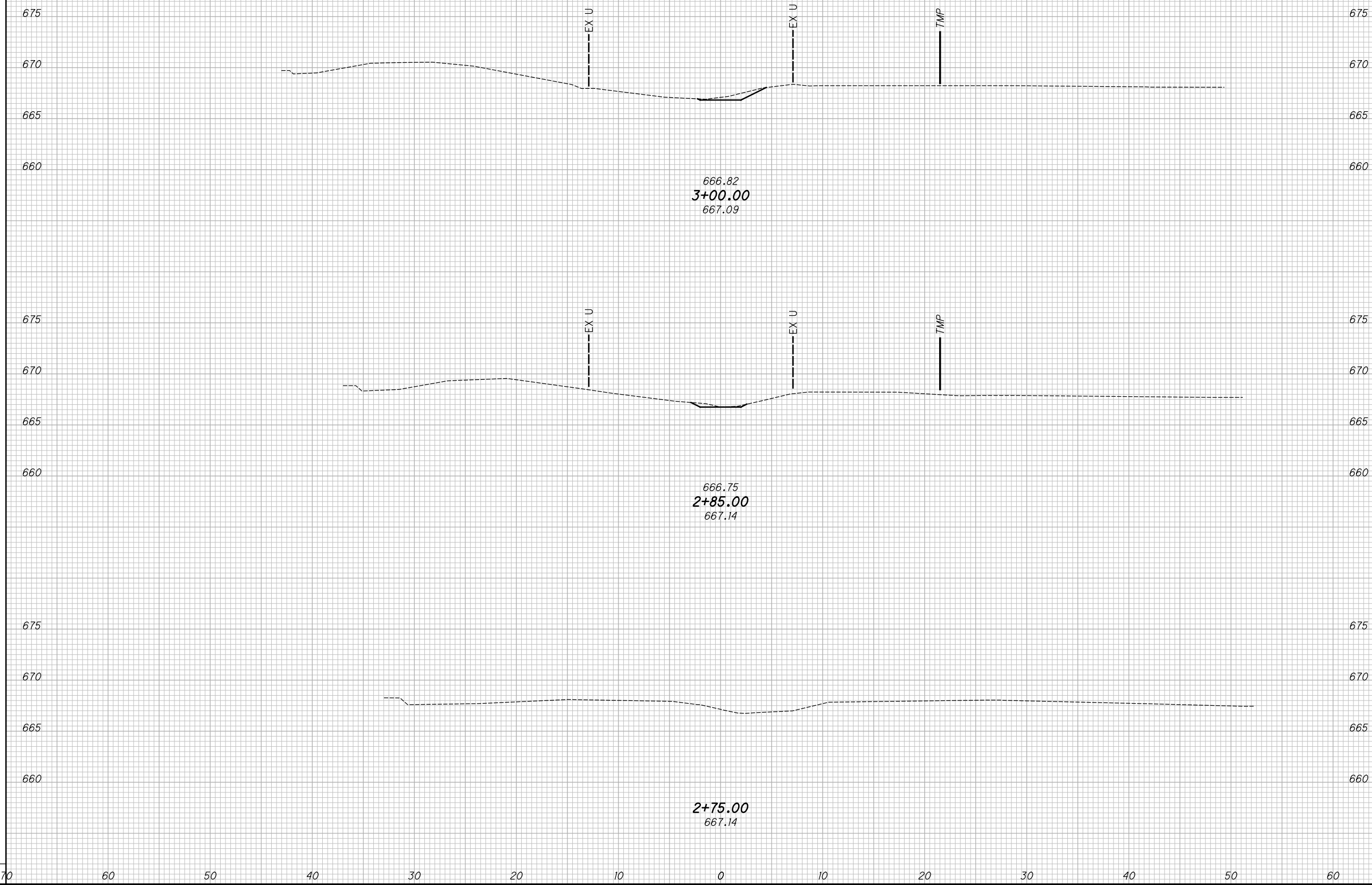
PLAN AND PROFILE - STORM SEWER
OUTLET STA. 524+50

LAK-US-20-24.99
PART 2

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Drainage\Sheets\09270_XD001.dgn XS_SHEET_Temporary_model_name_1 7/5/2022 2:59:03 PM troyer

SEEDING	
END WIDTH	SO. YDS.
70	

END AREA		VOLUME		CALCULATED JUMP	CHECKED WDB
CUT	FILL	CUT	FILL		



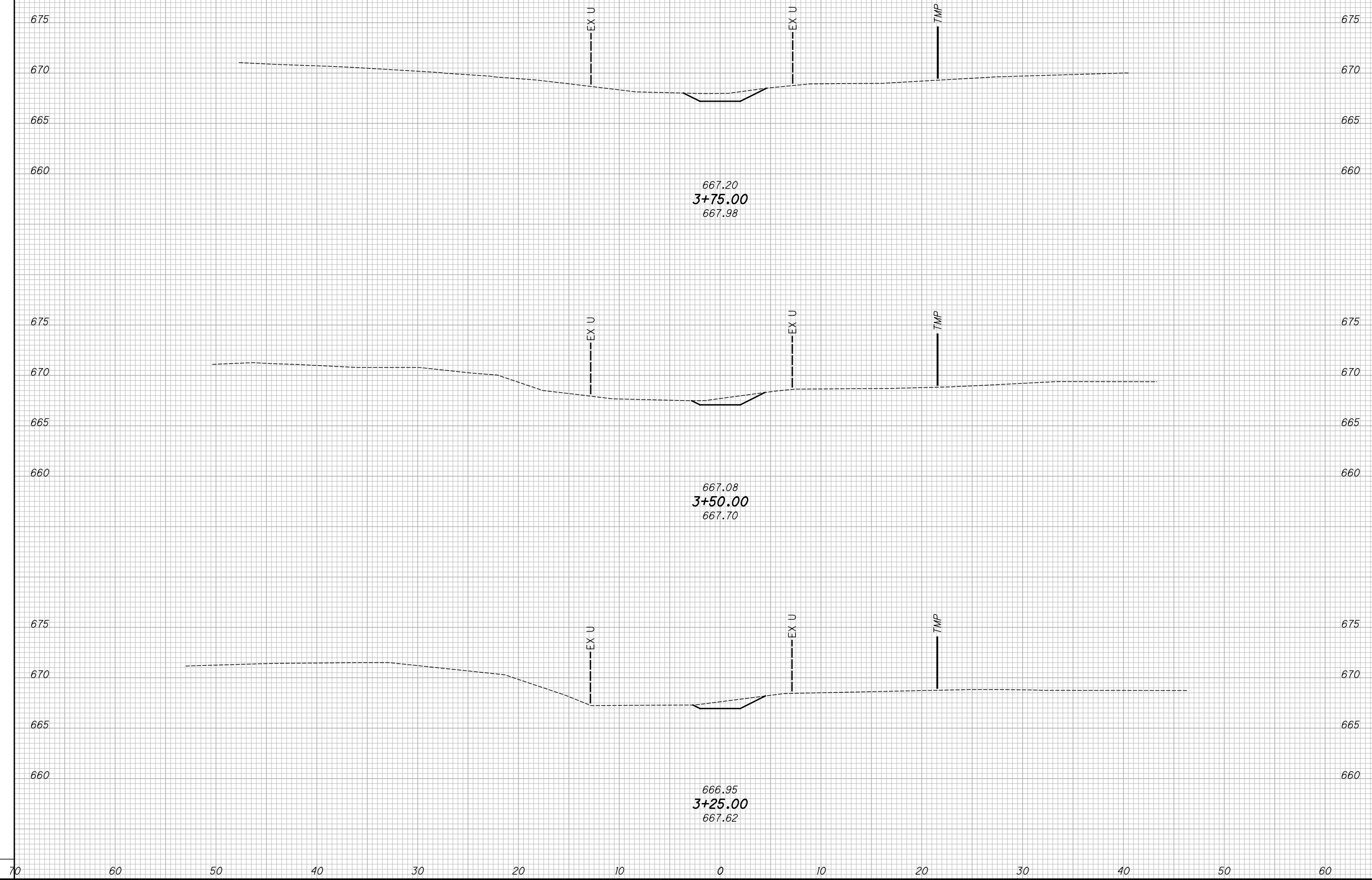
LAK-US-20-24.99
PART 2
CROSS SECTIONS - STORM OUTLET 524+50
STA. 2+75.00 TO STA. 3+00.00

474
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Drainage\Sheets\09270_XD001.dgn XS_SHEET_temporary_model_name.2 7/15/2022 2:59:04 PM Troyer

SEEDING	
END WIDTH	SO. YDS.

END AREA		VOLUME		CALCULATED JMP	CHECKED WDB
CUT	FILL	CUT	FILL		



CROSS SECTIONS - STORM OUTLET 524+50
STA. 3+25.00 TO STA. 3+75.00

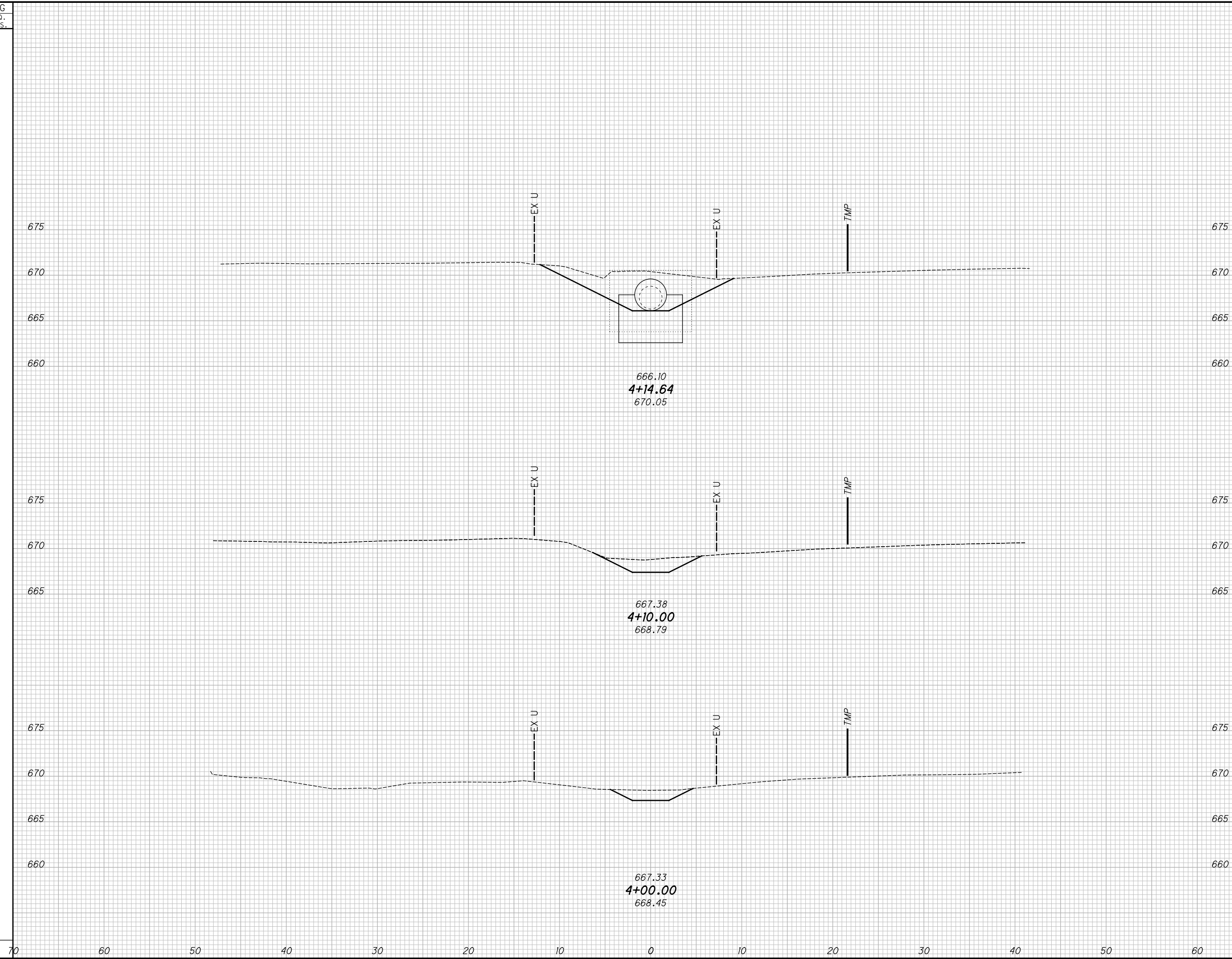
LAK-US-20-24.99
PART 2

475
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Drainage\Sheets\09270_XD001.dgn XS_SHEET_Temporary_model_name_3 7/15/2022 2:59:05 PM Troyer

SEEDING	
END WIDTH	SO. YDS.

END AREA		VOLUME		CALCULATED	CHECKED
CUT	FILL	CUT	FILL	JMP	WDB

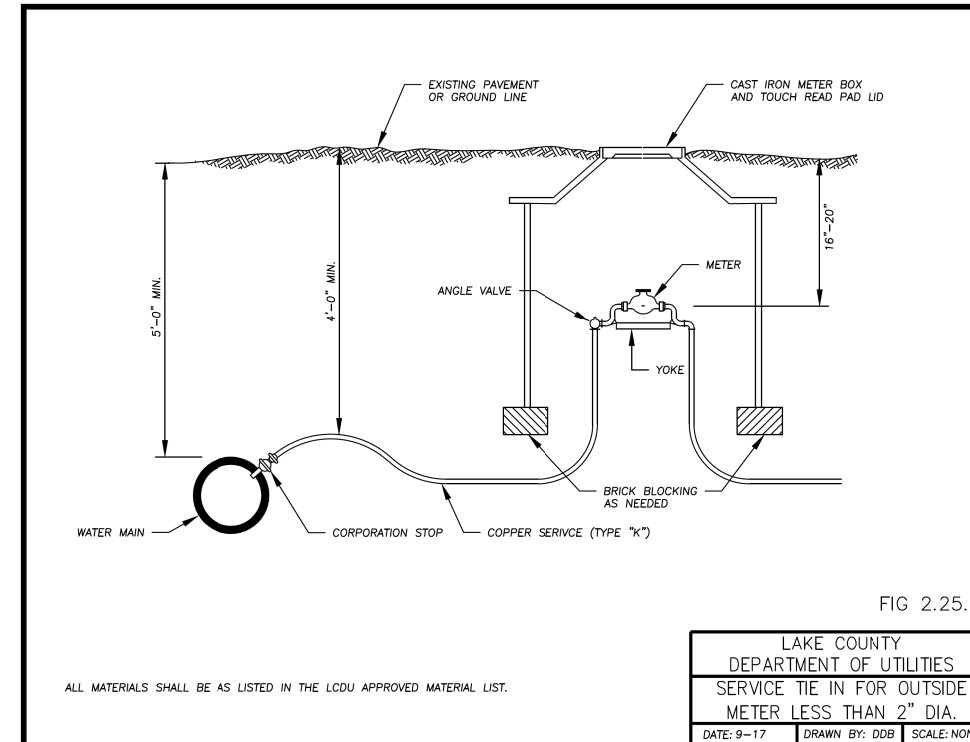
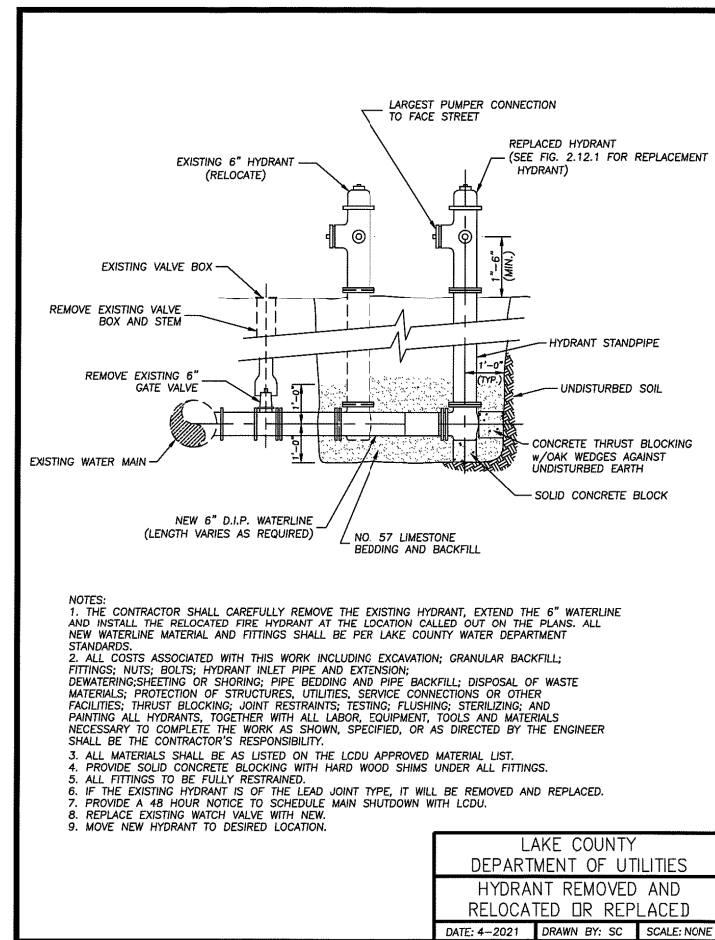
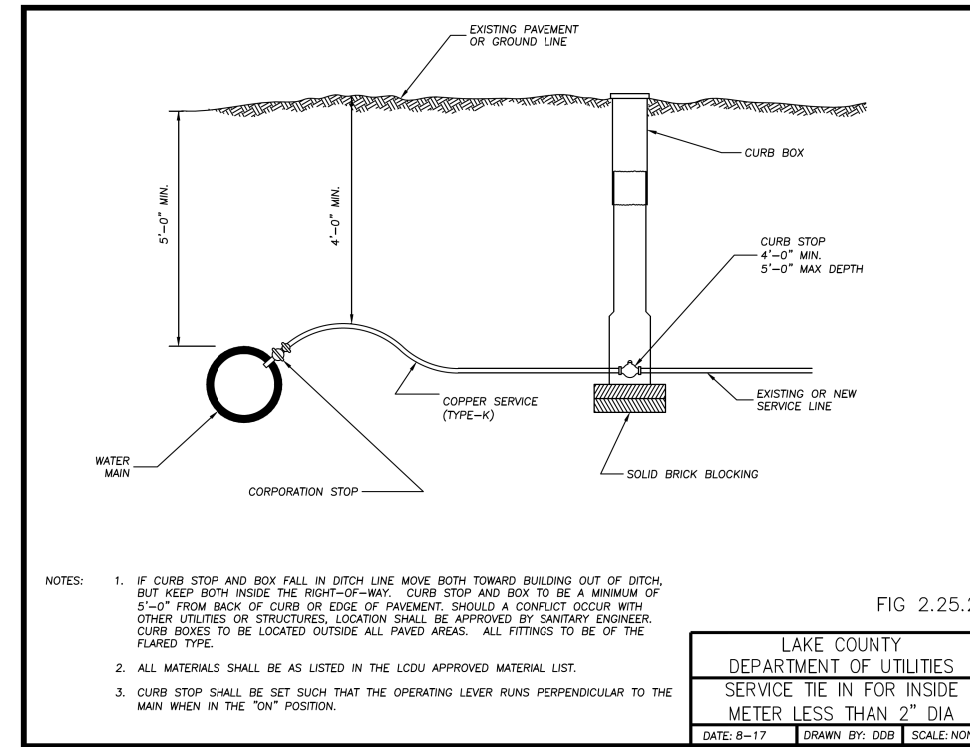
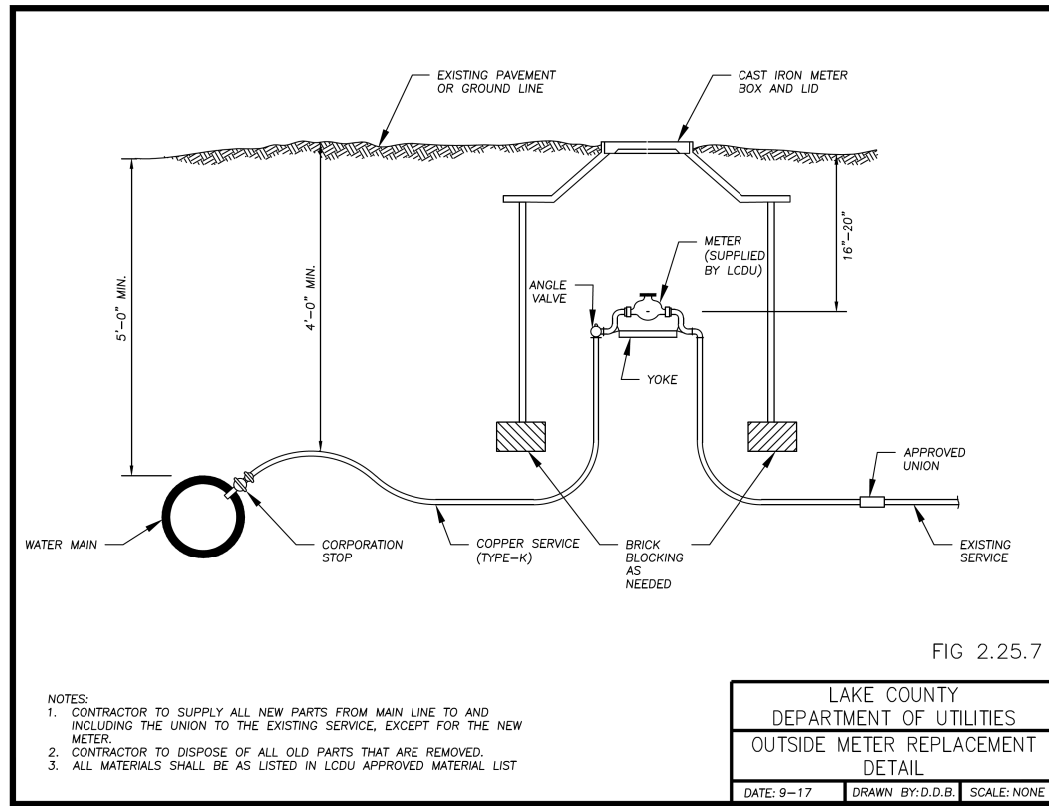


CROSS SECTIONS - STORM OUTLET 524+50
STA. 4+00.00 TO STA. 4+14.64

LAK-US-20-24.99
PART 2

(476)
 (697)

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_UD001.dgn Sheet 7/15/2022 2:59:07 PM troyer



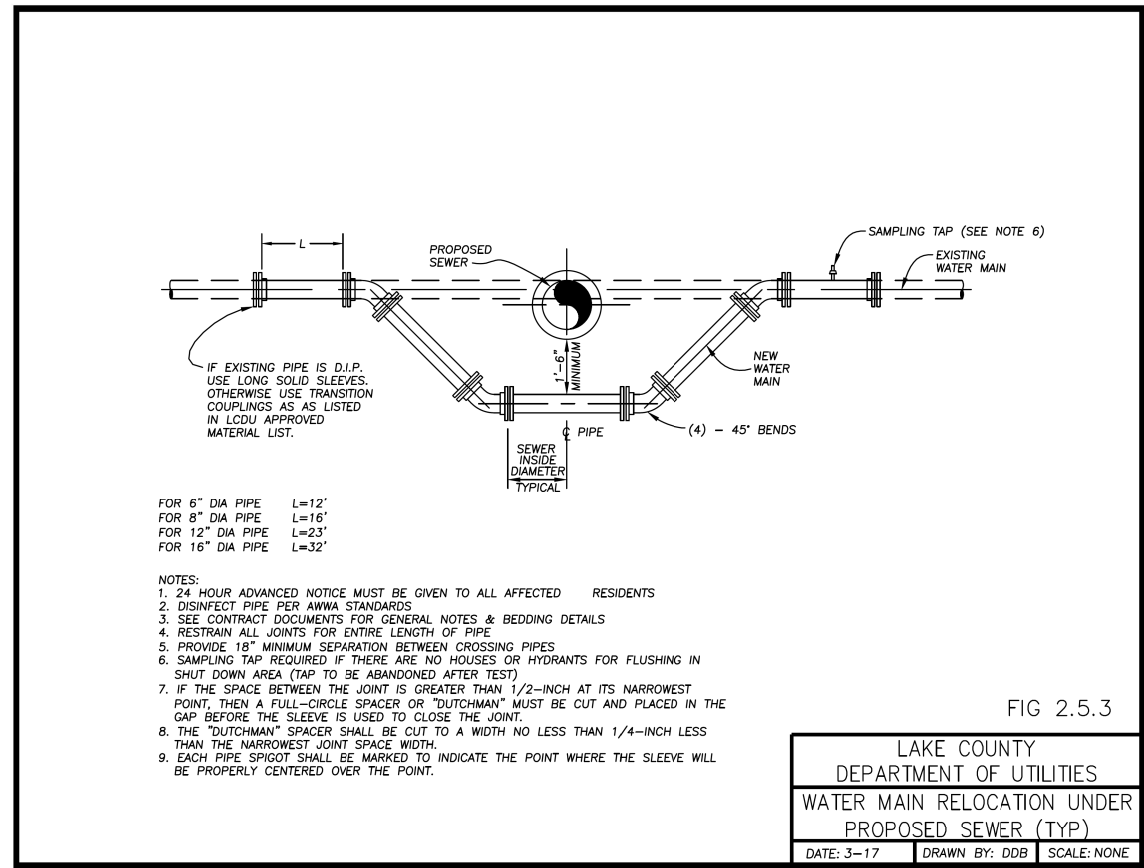
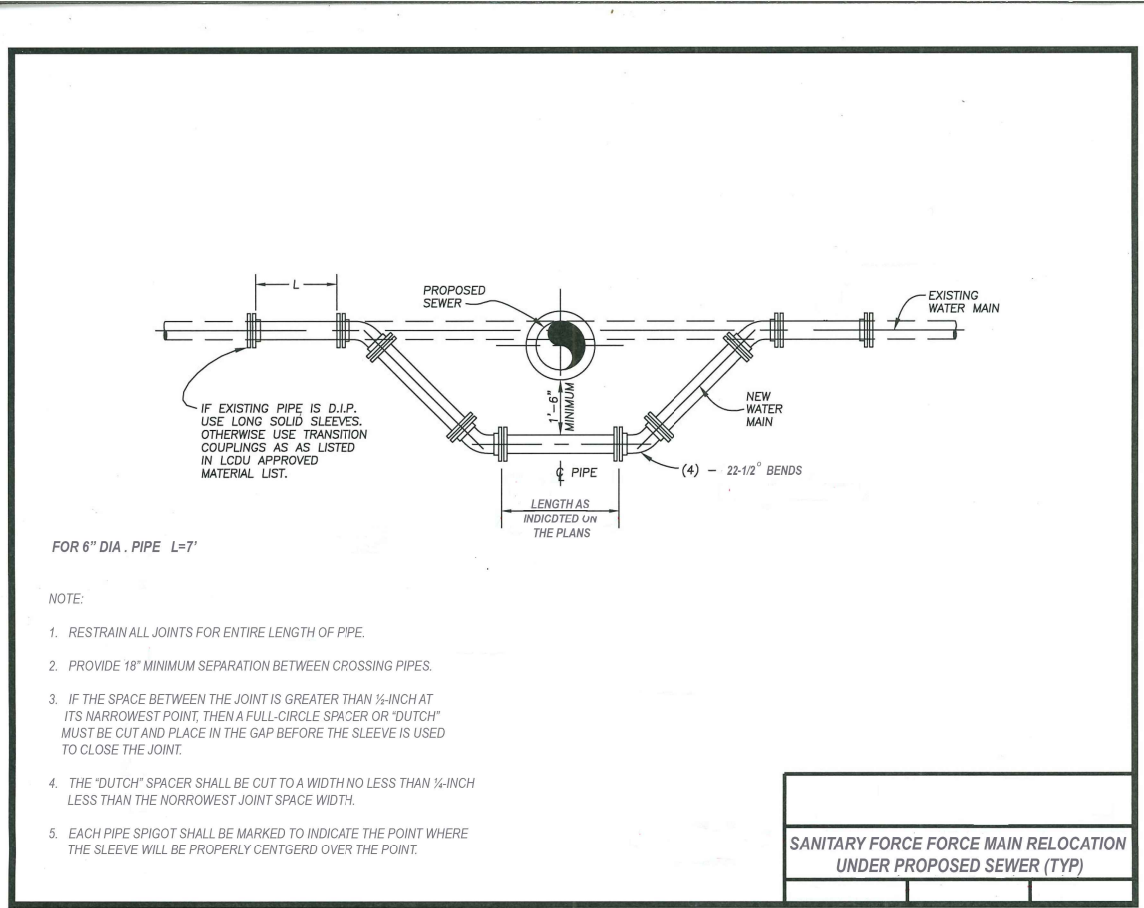
CALCULATED
T.L.S.
CHECKED
J.M.P.

WATER WORK AND SANITARY DETAILS

LAK-US-20-24.99
PART 2

477
697

H:\2018\80743\001\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_UD002.dgn Sheet 7/15/2022 2:59:14 PM troyer



SHEET NO.	REFERENCE NO.	STATION TO STATION	SIDE	611	611	SPECIAL	SPECIAL	SPECIAL	SPECIAL
				6" CONDUIT, TYPE B, AS PER PLAN, 748.01	MANHOLE ADJUSTED TO GRADE, AS PER PLAN	12" WATER MAIN DIP CLASS 52 MECHANICAL JOINTS AND FITTINGS, LCDU STANDARD	VALVE BOX ADJUSTED TO GRADE, LCDU STANDARD	6" FIRE HYDRANT FURNISHED, LCDU STANDARD	FIRE HYDRANT REMOVED FOR STORAGE, LCDU STANDARD
				FT	EACH	FT	EACH	EACH	EACH
263	SA-1	523+71.70	R		1				
264	SA-2	524+79.10	R		1				
265	SA-3	532+77.40	R		1				
267	SA-4	543+06.00	R		1				
269	SA-5	550+03.90	L		1				
270	SA-6	557+77.10	L		1				
271	SA-7	559+51.90	L		1				
271	SA-8	561+00.30	L		1				
271	SA-9	562+63.30	L		1				
272	SA-10	565+92.90	L		1				
<i>Hubbard Road</i>									
280	SA-11	92+86.00	L	27					
280	SA-12	97+24.80	L	29					
<i>Burns Road</i>									
277	W-7	9+55.00	L			22			
TOTALS COLUMN 1				56	10	22	4	2	2

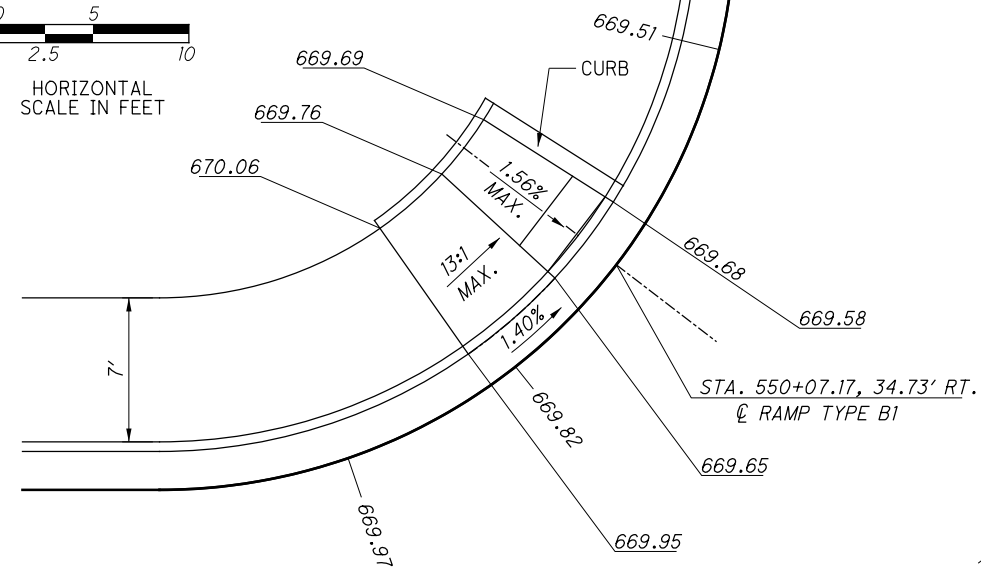
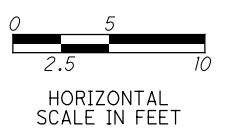


CALCULATED
CWH
CHECKED
EJT

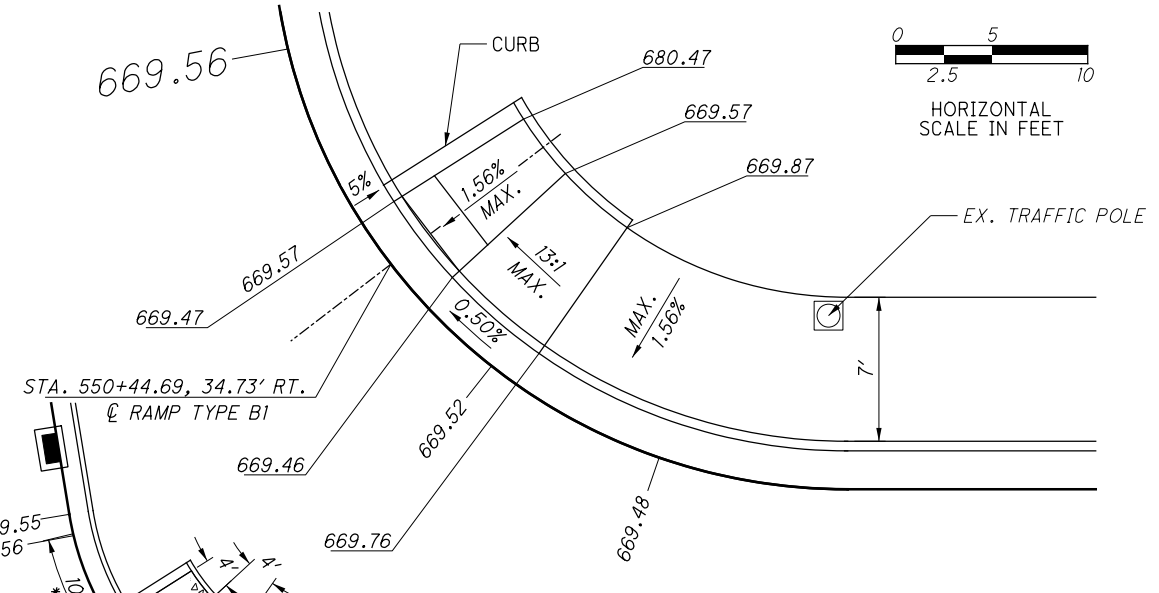
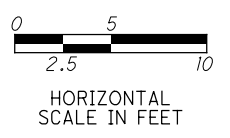
CURB RAMP SHEET
U.S. 20 AND DERUBERTIS DRIVE ROAD

LAK-US-20-24.99
PART 2

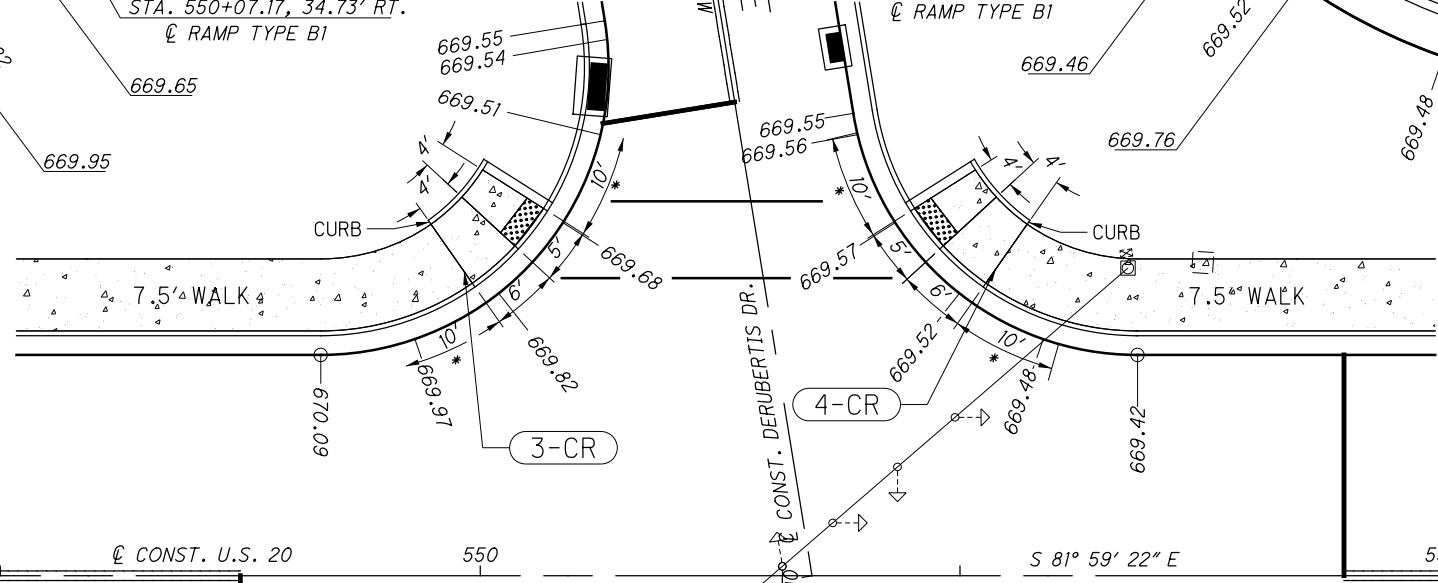
480
697



REMOVE AND REPLACE EXISTING WALK AND CURB RAMP WITH TYPE B1 CURB RAMP.



REMOVE AND REPLACE EXISTING WALK AND CURB RAMP WITH TYPE B1 CURB RAMP.



549

CONST. U.S. 20

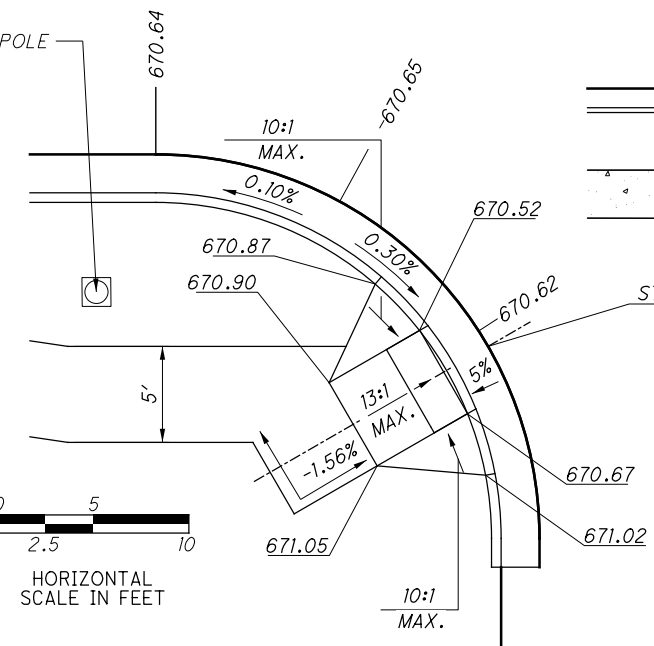
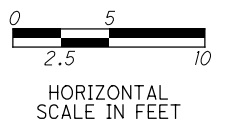
550

S 81° 59' 22" E

551

U.S. 20 (NORTH RIDGE ROAD)

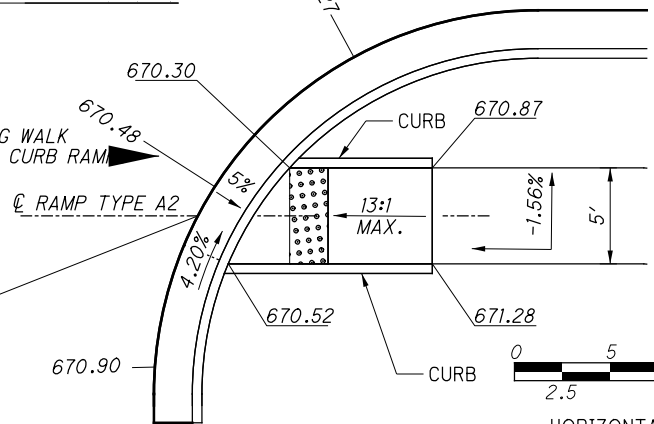
EX. TRAFFIC POLE



REMOVE AND REPLACE EXISTING WALK AND CURB RAMP WITH TYPE A1 CURB RAMP.

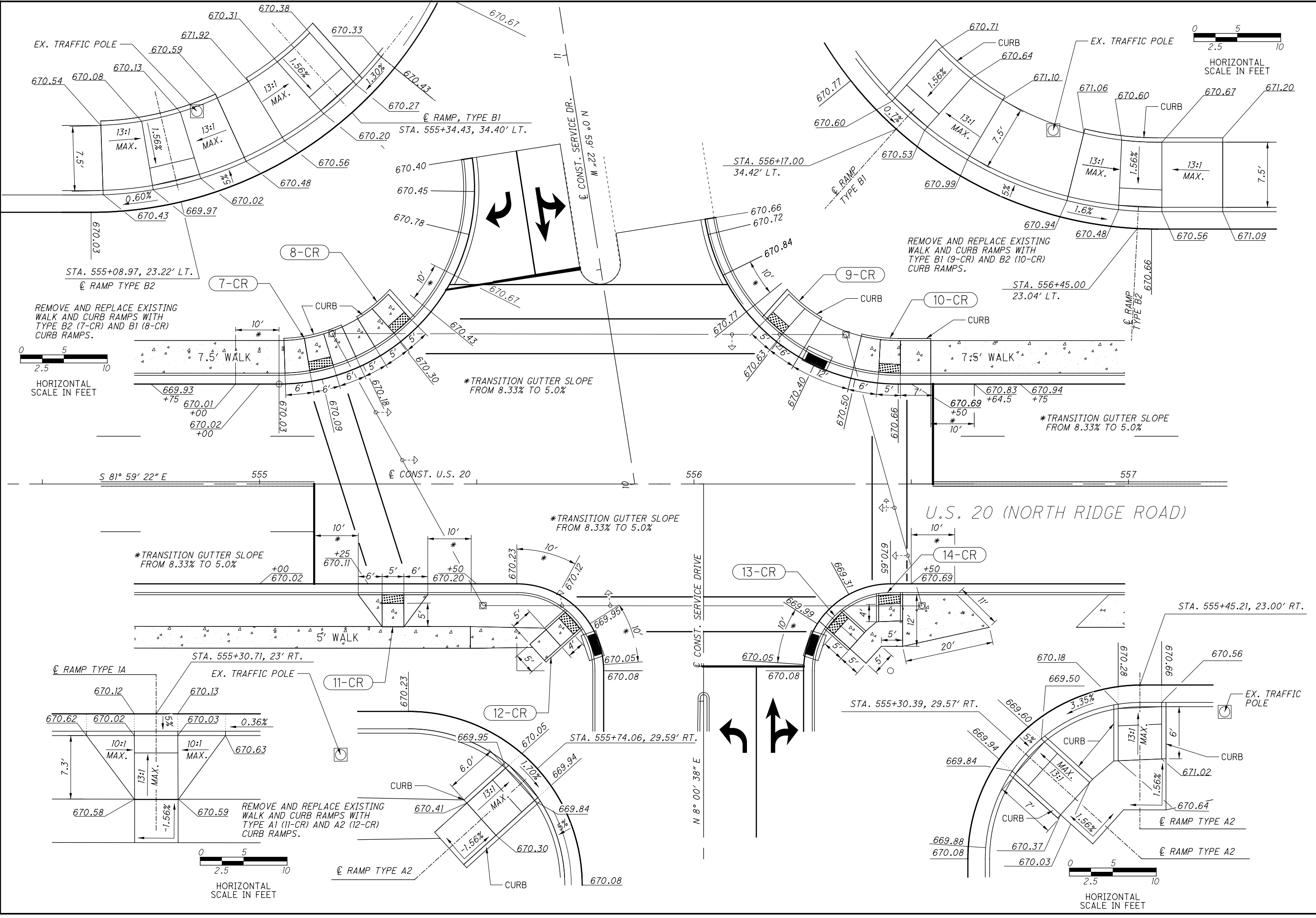
REMOVE AND REPLACE EXISTING WALK AND CURB RAMP WITH TYPE A2 CURB RAMP

STA. 550+46.77, 33.71' RT.



H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CM002.dgn Sheet 7/15/2022 2:59:21PM Troyer

H:\2018\80743\0DOT\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_GM003.dgn Sheet 7/15/2022 2:59:22 PM troyer

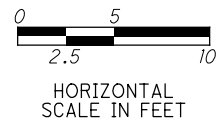


CALCULATED
CWH
CHECKED
EJT

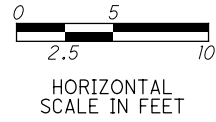
CURB RAMP SHEET
SERVICE DRIVE

LAK-US-20-24.99
PART 2

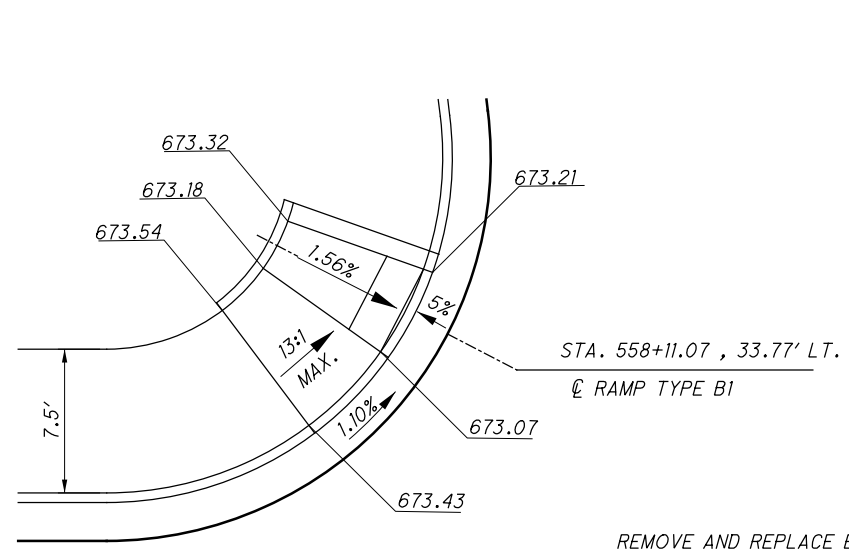
H:\2018\80743\000T\Project\Data\092TO_LAK_20_EAST\Design\Roadway\Sheets\092TO_CM004.dgn Sheet 7/15/2022 2:59:23 PM Troyer



HORIZONTAL SCALE IN FEET



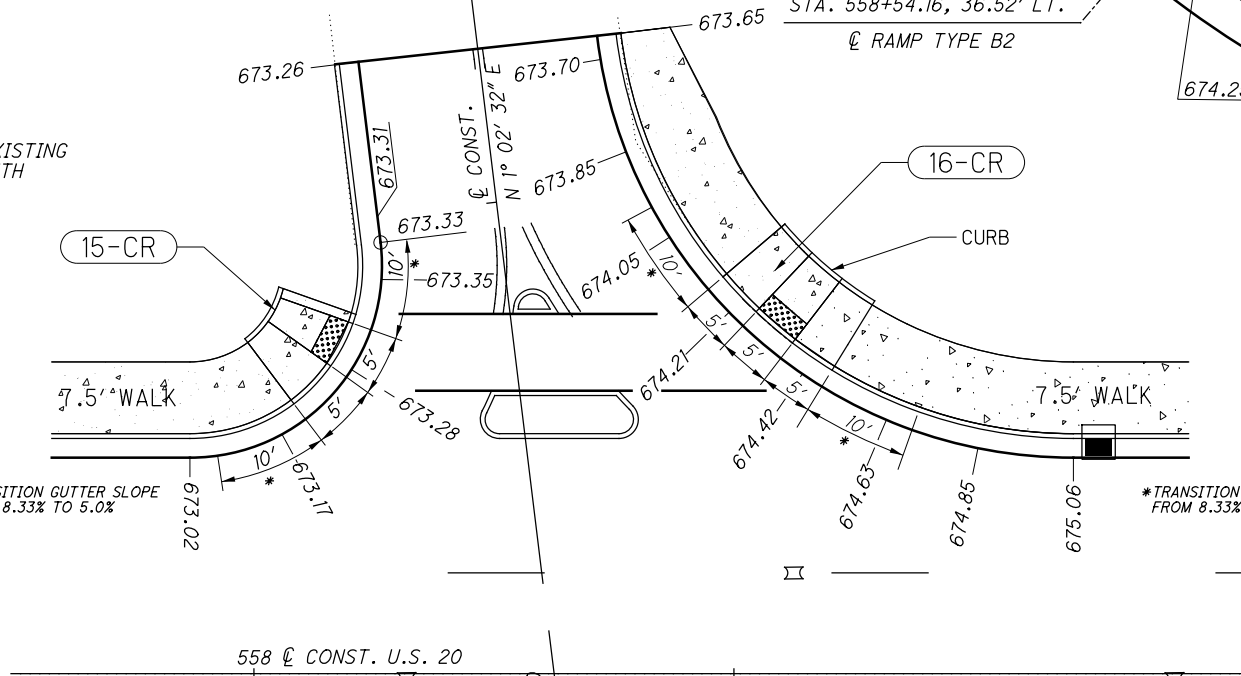
HORIZONTAL SCALE IN FEET



STA. 558+11.07 , 33.77' LT.
RAMP TYPE B1

REMOVE AND REPLACE EXISTING WALK AND CURB RAMP WITH TYPE B1 CURB RAMP.

*TRANSITION GUTTER SLOPE FROM 8.33% TO 5.0%

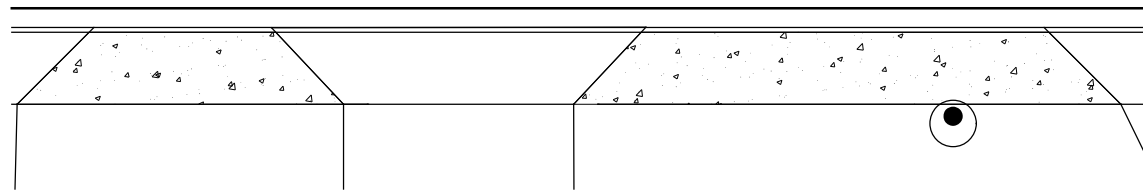


STA. 558+54.16, 36.52' LT.
RAMP TYPE B2

REMOVE AND REPLACE EXISTING WALK AND CURB RAMP WITH TYPE B2 CURB RAMP.

*TRANSITION GUTTER SLOPE FROM 8.33% TO 5.0%

558 RAMP CONST. U.S. 20

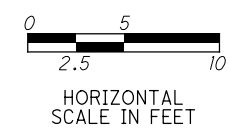
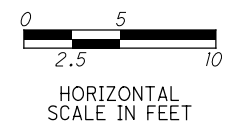
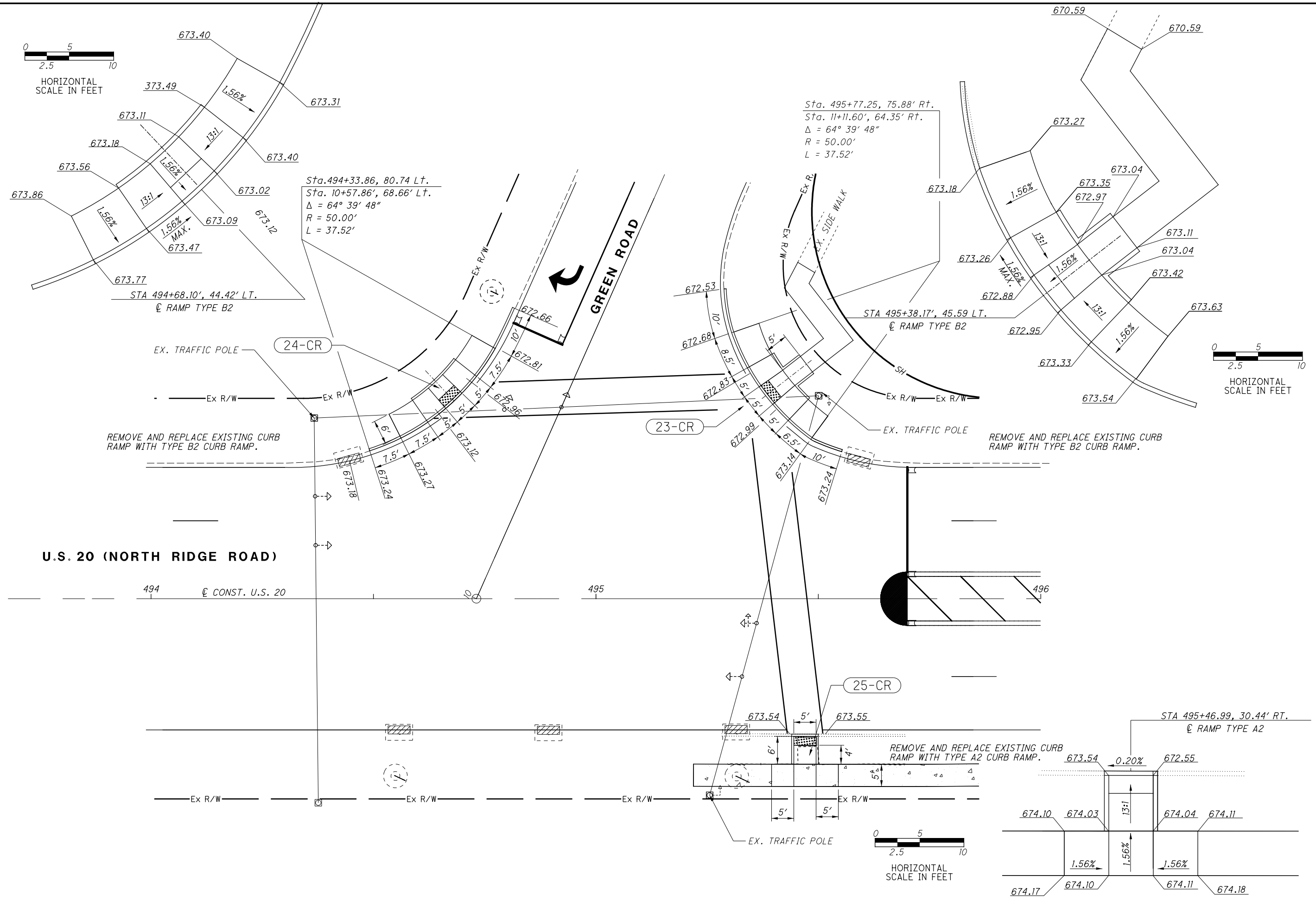
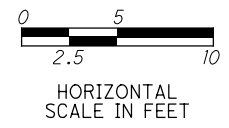


CALCULATED	0
CWH	0
CHECKED	0
EJT	0

CURB RAMP SHEET
U.S. 20 AND BALOGH REAL ESTATE DRIVE

LAK-US-20-24.99
PART 2

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\Roadway\Sheets\09270_CM006.dgn_Sheet 7/15/2022 2:59:25 PM Troyer



CURB RAMP SHEET
U.S. 20 GREEN ROAD

LAK-US-20-24.99
PART 2

484
697

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_I000.dgn Sheet 7/15/2022 2:59:25 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION		SIDE	621	642	642	642	642	642	642	642	646	646	646	646	646	646	646	646	646	
					RPM	CENTER LINE, TYPE 1	LANE LINE, 6", TYPE 1	STOP LINE, TYPE 1	CROSSWALK LINE, 12", TYPE 1	CHANNELIZING LINE, 12", TYPE 1	TRANSVERSE/DIAGONAL LINE, TYPE 1	LANE ARROW, TYPE 1	CENTER LINE	EDGE LINE, 6"	LANE LINE, 6"	STOP LINE	CROSSWALK LINE, 12"	CHANNELIZING LINE, 12"	TRANSVERSE/DIAGONAL LINE	ISLAND MARKING	LANE ARROW	
					EACH	MILE	MILE	FT	FT	FT	FT	EACH	MILE	MILE	MILE	FT	FT	FT	FT	SF	EACH	
CL-1	493	396+28	TO	399+00	C/L		0.052															
CL-2	493	396+18	TO	399+00	RT		0.016															
LL-1	493	396+28	TO	399+00	LT			0.052														
LL-2	493	396+28	TO	399+00	RT			0.052														
CL-1	494	399+00	TO	404+00	C/L		0.095															
CL-2	494	399+00	TO	403+08	RT		0.077															
LL-1	494	399+00	TO	404+00	LT			0.095														
LL-2	494	399+00	TO	404+00	RT			0.095														
CH-1	494	403+33	TO	404+00	RT					67												
LA-1	494	403+45			RT							1										
LA-2	494	403+95			RT							1										
TL-1	494	399+00	TO	403+08	RT					112												
CL-1	495	404+00	TO	409+00	C/L		0.095															
CL-2	495	408+57	TO	409+00	LT		0.057															
CL-3	495	8+50	TO	11+50	LT/RT		0.057															
LL-1	495	404+00	TO	409+00	LT			0.095														
LL-2	495	404+00	TO	409+00	RT			0.095														
CW-1	495	405+20	TO	405+28	LT & RT					138												
CW-2	495	406+43	TO	406+51	LT & RT					141												
CW-3	495	405+16	TO	406+10	RT					182												
CW-4	495	405+63	TO	406+45	LT					159												
SL-1	495	405+05			RT				39													
SL-2	495	406+60			LT				40													
SL-3	495	9+44			RT				28													
SL-4	495	10+56			LT				29													
CH-1	495	404+00	TO	405+05	RT						105											
CH-2	495	406+60	TO	408+32	LT						172											
CH-3	495	8+50	TO	9+47	RT						97											
CH-4	495	10+53	TO	11+50	LT						98											
LA-1	495	404+45			RT							1										
LA-2	495	404+95			RT							1										
LA-3	495	406+70			LT							1										
LA-4	495	407+20			LT							1										
LA-5	495	407+70			LT							1										
LA-6	495	408+20			LT							1										
LA-7	495	8+20			RT							1										
LA-8	495	8+77			RT							1										
LA-9	495	9+34			RT							1										
LA-10	495	10+99			LT							1										
LA-11	495	11+04			LT							1										
LA-12	495	11+42			LT							1										
TL-1	495	408+57	TO	409+00	LT					18												
CL-1	496	409+00	TO	411+20	LT		0.061															
CL-2	496	409+00	TO	411+20	RT		0.061															
CL-3	496	411+20	TO	414+00	C/L	3						0.053										
CL-4	496	411+20	TO	411+82	RT							0.012										
LL-1	496	409+00	TO	411+20	LT			0.042														
LL-2	496	409+00	TO	411+20	RT			0.042														
LL-3	496	411+20	TO	414+00	LT	3									0.053							
LL-4	496	411+20	TO	414+00	RT	3								0.053								
TL-1	496	409+00	TO	411+82	RT					71												
CL-1	497	414+00	TO	419+00	C/L	7						0.095										
LL-1	497	414+00	TO	419+00	LT	7									0.095							
LL-2	497	414+00	TO	419+00	RT	7								0.095								
SUBTOTAL SHEET 485						30	0.571	0.568	136	620	539	201	14	0.160		0.296						

PAVEMENT MARKING SUBSUMMARY

**LAK-US-20-24.99
PART 2**

CALCULATED
ARK
CHECKED
EJT

485
697

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_T0002.dgn Sheet 7/15/2022 2:59:26 PM troyer

REF NO.	SHEET NO.	STATION TO STATION			SIDE	621	642	642	642	642	642	642	642	642	646	646	646	646	646	646	646	646	646
						RPM	CENTER LINE, TYPE 1	LANE LINE, 6", TYPE 1	STOP LINE, TYPE 1	CROSSWALK LINE, 12", TYPE 1	CHANNELIZING LINE, 12", TYPE 1	TRANSVERSE/DIAGONAL LINE, TYPE 1	LANE ARROW, TYPE 1	CENTER LINE	EDGE LINE, 6"	LANE LINE, 6"	STOP LINE	CROSSWALK LINE, 12"	CHANNELIZING LINE, 12"	TRANSVERSE/DIAGONAL LINE	ISLAND MARKING	LANE ARROW	
						EACH	MILE	MILE	FT	FT	FT	FT	MILE	MILE	MILE	FT	FT	FT	FT	FT	FT	EACH	
CL-1	498	419+00	TO	424+00	C/L	6								0.095									
LL-1	498	419+00	TO	424+00	LT	6																	
LL-2	498	419+00	TO	424+00	RT																		
CL-1	499	424+00	TO	429+00	C/L	6								0.095									
CL-2	499	426+00	TO	429+00	RT	3								0.057									
LL-1	499	424+00	TO	429+00	LT	6																	
LL-2	499	424+00	TO	429+00	RT	6																	
EL-1	499	426+50	TO	429+00	LT																		
EL-2	499	426+50	TO	429+00	RT	3																	
TL-1	499	426+52	TO	429+00	RT																		
CL-1	500	429+00	TO	434+00	C/L	6								0.095									
CL-2	500	429+00	TO	429+90	RT	3								0.017									
CL-3	500	8+50	TO	11+50	RT	4								0.057									
CH-1	500	430+15	TO	433+20	RT	8																	
CW-1	500	433+33	TO	433+84	RT																		
LL-1	500	429+00	TO	434+00	LT	5																	
LL-2	500	429+00	TO	434+00	RT	5																	
LA-1	500	430+46			RT																		1
LA-2	500	431+12			RT																		1
LA-3	500	431+78			RT																		1
LA-4	500	432+44			RT																		1
LA-5	500	433+10			RT																		1
EL-1	500	429+00	TO	434+00	LT																		
EL-2	500	429+00	TO	434+00	RT	11																	
EL-3	500	8+50	TO	11+50	LT	3																	
EL-4	500	8+50	TO	11+50	RT	3																	
SL-1	500	433+20			RT																		
SL-2	500	9+50			RT																		
SL-3	500	10+50			LT																		
TL-1	500	429+00	TO	429+90	RT																		
CL-1	501	435+95	TO	439+00	C/L	5								0.058									
CL-2	501	434+00	TO	439+00	RT	6								0.095									
CH-1	501	434+40	TO	436+95	RT	4																	
EL-1	501	434+00	TO	439+00	LT	12																	
EL-2	501	434+00	TO	439+00	RT																		
LL-1	501	434+00	TO	439+65	LT	6																	
LL-2	501	434+00	TO	439+65	RT	6																	
LA-1	501	434+50			RT																		
LA-2	501	435+04			RT																		
LA-3	501	435+58			RT																		
SL-1	501	434+40			LT																		
TL-1	501	435+95	TO	439+00	RT																		
CL-1	502	439+00	TO	444+00	C/L	6								0.095									
CL-2	502	439+00	TO	439+95	RT	1								0.018									
EL-1	502	439+00	TO	444+00	LT																		
EL-2	502	439+00	TO	444+00	RT																		
LL-1	502	439+00	TO	444+00	LT	6																	
LL-2	502	439+00	TO	444+00	RT	6																	
TL-1	502	439+00	TO	439+63	RT																		
SUBTOTAL SHEET 486						142								0.682	0.780	0.786	89	113	510	190		8	

PAVEMENT MARKING SUBSUMMARY

LAK-US-20-24.99 PART 2

CALCULATED
ARK
CHECKED
EJT

486
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_T0003.dgn Sheet 1/15/2022 2:59:27 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION			SIDE	621	642	642	642	642	642	642	642	646	646	646	646	646	646	646	646	646
						RPM	CENTER LINE, TYPE 1	LANE LINE, 6", TYPE 1	STOP LINE, TYPE 1	CROSSWALK LINE, 12", TYPE 1	CHANNELIZING LINE, 12", TYPE 1	TRANSVERSE/DIAGONAL LINE, TYPE 1	LANE ARROW, TYPE 1	CENTER LINE	EDGE LINE, 6"	LANE LINE, 6"	STOP LINE	CROSSWALK LINE, 12"	CHANNELIZING LINE, 12"	TRANSVERSE/DIAGONAL LINE	ISLAND MARKING	LANE ARROW
						EACH	MILE	MILE	FT	FT	FT	FT	EACH	MILE	MILE	MILE	FT	FT	FT	FT	SF	EACH
CL-1	503	444+00	TO	449+00	C/L	6								0.095								
LL-1	503	444+00	TO	449+00	LT	6										0.095						
LL-2	503	444+00	TO	449+00	RT	6										0.095						
CL-1	504	449+00	TO	454+00	C/L	6								0.095								
LL-1	504	449+00	TO	454+00	LT	7										0.095						
LL-2	504	449+00	TO	454+00	RT	7										0.095						
CL-1	505	454+00	TO	459+00	C/L	7								0.095								
LL-1	505	454+00	TO	459+00	LT	6										0.095						
LL-2	505	454+00	TO	459+00	RT	6										0.095						
CL-1	506	459+00	TO	464+00	C/L	5								0.095								
CL-2	506	10+00	TO	11+50	LT	2								0.029								
EL-1	506	10+92	TO	11+50	LT	3									0.011							
EL-2	506	10+92	TO	11+50	RT										0.011							
LL-1	506	459+00	TO	464+00	LT	5										0.095						
LL-2	506	459+00	TO	464+00	RT	5										0.095						
SL-1	506	10+40															14					
CL-1	507	464+00	TO	469+00	C/L	6								0.095								
LL-1	507	464+00	TO	469+00	LT	6										0.095						
LL-2	507	464+00	TO	469+00	RT	6										0.095						
CL-1	508	469+00	TO	474+00	C/L	7								0.095								
LL-1	508	469+00	TO	474+00	LT	7										0.095						
LL-2	508	469+00	TO	474+00	RT	7										0.095						
CL-1	509	474+00	TO	479+00	C/L	6								0.095								
LL-1	509	474+00	TO	479+00	LT	6										0.095						
LL-2	509	474+00	TO	479+00	RT	6										0.095						
CL-1	510	479+00	TO	484+00	C/L	6								0.095								
LL-1	510	479+00	TO	484+00	LT	6										0.095						
LL-2	510	479+00	TO	484+00	RT	6										0.095						
CL-1	511	484+00	TO	489+00	C/L	6								0.095								
CL-2	511	486+05	TO	489+00	RT	4								0.056								
EL-1	511	486+40	TO	489+00	LT										0.050							
EL-2	511	486+42	TO	489+00	RT	2									0.045							
LL-1	511	484+00	TO	489+00	LT	6										0.095						
LL-2	511	484+00	TO	489+00	RT	6										0.095						
TL-1	511	486+05	TO	489+00	RT														85			
SUBTOTAL SHEET 487						176								0.940	0.117	1.710	14			85		

PAVEMENT MARKING SUBSUMMARY

**LAK-US-20-24.99
PART 2**

CALCULATED
ARK
CHECKED
EJT

487
697

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_I0004.dgn Sheet 1/15/2022 2:59:28 PM troyer

REF NO.	SHEET NO.	STATION TO STATION		SIDE	621	642	642	642	642	642	642	642	642	646	646	646	646	646	646	646	646	646
					RPM	CENTER LINE, TYPE 1	LANE LINE, 6", TYPE 1	STOP LINE, TYPE 1	CROSSWALK LINE, 12", TYPE 1	CHANNELIZING LINE, 12", TYPE 1	TRANSVERSE/DIAGONAL LINE, TYPE 1	LANE ARROW, TYPE 1	CENTER LINE	EDGE LINE, 6"	LANE LINE, 6"	STOP LINE	CROSSWALK LINE, 12"	CHANNELIZING LINE, 12"	TRANSVERSE/DIAGONAL LINE	ISLAND MARKING	LANE ARROW	
					EACH	MILE	MILE	FT	FT	FT	FT	MILE	MILE	MILE	FT	FT	FT	FT	SF	EACH		
CL-1	512	489+00	TO 494+00	C/L	8								0.095									
CL-2	512	489+00	TO 489+30	RT	1								0.0057									
CH-1	512	489+55	TO 493+90	RT	11													433				
EL-1	512	489+00	TO 494+00	LT										0.095								
EL-2	512	489+00	TO 494+00	RT	12									0.095								
LL-1	512	489+00	TO 494+00	LT	7									0.095								
LL-2	512	489+00	TO 494+00	RT	7									0.095								
LA-1	512	489+84		RT																		1
LA-2	512	490+50		RT																		1
LA-3	512	491+16		RT																		1
LA-4	512	491+82		RT																		1
LA-5	512	492+48		RT																		1
LA-6	512	493+14		RT																		1
LA-7	512	493+80		RT																		1
SL-1	512	493+90		RT												35						
TL-1	512	489+00	TO 489+30	RT														9				
CL-1	513	494+00	TO 499+00	C/L	5								0.095									
CL-2	513	495+70	TO 499+00	RT	5								0.063									
CL-3	513	10+00	TO 11+50	LT	1								0.029									
CH-1	513	10+60	TO 11+50	LT	3													90				
CW-1	513	495+39	TO 495+47	LT & RT														134				
CW-2	513	494+69	TO 495+39	LT														141				
EL-1	513	494+00	TO 494+69	LT										0.013								
EL-2	513	494+00	TO 495+47	RT										0.0278								
EL-3	513	495+70	TO 499+00	LT	9									0.0625								
EL-4	513	495+70	TO 499+00	RT										0.0625								
EL-5	513	10+60	TO 11+50	LT	3									0.017								
EL-6	513	10+60	TO 11+50	LT										0.017								
LL-1	513	494+00	TO 499+00	LT	4										0.095							
LL-2	513	494+00	TO 499+00	RT	4										0.095							
LA-1	513	10+70		LT																		1
IM-1	513	495+70		LT & RT																		
TL-1	513	495+70	TO 499+00	RT														144				51
SL-1	513	495+70		LT												24						
SL-2	513	10+60		LT												12						
SL-3	513	11+10		LT												11						
CL-1	514	499+00	TO 504+00	C/L	6								0.095									
CL-2	514	499+00	TO 500+25	RT	1								0.024									
EL-1	514	499+00	TO 500+22	LT	2									0.023								
EL-2	514	499+00	TO 500+38	RT										0.026								
LL-1	514	499+00	TO 504+00	LT	6										0.095							
LL-2	514	499+00	TO 504+00	RT	6										0.095							
TL-1	514	499+00	TO 500+25	RT														15				
SUBTOTAL SHEET 488					101								0.407	0.629	0.380	82	275	523	168	51	8	

CALCULATED
 ARK
 CHECKED
 EJT
PAVEMENT MARKING SUBSUMMARY
LAK-US-20-24.99
PART 2

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_I0005.dgn Sheet 7/15/2022 2:59:29 PM troyer

REF NO.	SHEET NO.	STATION TO STATION			SIDE	621	642	642	642	642	642	642	642	646	646	646	646	646	646	646	646	646
						RPM	CENTER LINE, TYPE 1	LANE LINE, 6", TYPE 1	STOP LINE, TYPE 1	CROSSWALK LINE, 12", TYPE 1	CHANNELIZING LINE, 12", TYPE 1	TRANSVERSE/DIAGONAL LINE, TYPE 1	LANE ARROW, TYPE 1	CENTER LINE	EDGE LINE, 6"	LANE LINE, 6"	STOP LINE	CROSSWALK LINE, 12"	CHANNELIZING LINE, 12"	TRANSVERSE/DIAGONAL LINE	ISLAND MARKING	LANE ARROW
						EACH	MILE	MILE	FT	FT	FT	FT	EACH	MILE	MILE	MILE	FT	FT	FT	FT	SF	EACH
CL-1	515	504+00	TO	509+00	C/L	6								0.095								
CH-1	515	504+22	TO	507+46	LT	7														324		
CH-2	515	504+22	TO	504+40	LT															18		
CH-3	515	504+68	TO	505+06	LT															38		
LL-1	515	504+00	TO	509+00	LT	7									0.095							
LL-2	515	504+00	TO	509+00	RT	7									0.095							
LA-1	515	505+28			LT																	
LA-2	515	505+94			LT																	
LA-3	515	506+60			LT																	
LA-4	515	507+26			LT																	
SL-1	515	504+27	TO	504+37	LT												13					
TL-1	515	504+22	TO	505+06	LT															89		
CL-1	516	509+00	TO	514+00	C/L	6								0.095								
LL-1	516	509+00	TO	514+00	LT	6									0.095							
LL-2	516	509+00	TO	514+00	RT	6									0.095							
CL-1	517	514+00	TO	516+45	C/L	6								0.095								
CL-2	517	8+25	TO	10+00	RT	2								0.034								
CW-1	517	516+71	TO	517+39	RT														127			
LL-1	517	514+00	TO	519+00	LT	4									0.095							
LL-2	517	514+00	TO	519+00	RT	4									0.095							
SL-1	517	516+45			RT												23					
SL-2	517	518+05			LT												23					
SL-3	517	9+57			RT												16					
EL-1	517	8+25	TO	9+34	LT										0.021							
EL-2	517	8+25	TO	9+57	RT	3									0.025							
CL-1	518	519+00	TO	524+00	C/L	6								0.095								
LL-1	518	519+00	TO	524+00	LT	7									0.095							
LL-2	518	519+00	TO	524+00	RT	7									0.095							
CL-1	519	524+00	TO	529+00	C/L	6								0.095								
LL-1	519	524+00	TO	529+00	LT	6									0.095							
LL-2	519	524+00	TO	529+00	RT	6									0.095							
CL-1	520	529+00	TO	532+48	C/L	6								0.095								
LL-1	520	529+00	TO	532+48	LT	5									0.095							
LL-2	520	529+00	TO	534+00	RT	6									0.095							
SL-1	520	532+70	TO	532+90	LT												20					
CL-1	521	534+00	TO	536+12	C/L	6								0.095								
LL-1	521	534+00	TO	536+12	LT	6									0.095							
LL-2	521	534+00	TO	539+00	RT	6									0.095							
SL-1	521	536+41	TO	536+55	LT												14					
CL-1	522	539+00	TO	544+00	C/L	6								0.095								
LL-1	522	539+00	TO	544+00	LT	6									0.095							
LL-2	522	539+00	TO	544+00	RT	6									0.095							
SUBTOTAL SHEET 489						155								0.794	0.046	1.520	109	127	380	89		

PAVEMENT MARKING SUBSUMMARY

LAK-US-20-24.99

PART 2

CALCULATED
ARK
CHECKED
EJT

489
697

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_I0006.dgn Sheet 7/15/2022 2:59:29 PM troyer

REF NO.	SHEET NO.	STATION TO STATION			SIDE	621	642	642	642	642	642	642	642	646	646	646	646	646	646	646	646	646
						RPM	CENTER LINE, TYPE 1	LANE LINE, 6", TYPE 1	STOP LINE, TYPE 1	CROSSWALK LINE, 12", TYPE 1	CHANNELIZING LINE, 12", TYPE 1	TRANSVERSE/DIAGONAL LINE, TYPE 1	LANE ARROW, TYPE 1	CENTER LINE	EDGE LINE, 6"	LANE LINE, 6"	STOP LINE	CROSSWALK LINE, 12"	CHANNELIZING LINE, 12"	TRANSVERSE/DIAGONAL LINE	ISLAND MARKING	LANE ARROW
						EACH	MILE	MILE	FT	FT	FT	FT	EACH	MILE	MILE	MILE	FT	FT	FT	FT	SF	EACH
CL-1	523	544+00	TO	549+00	C/L	7								0.095								
LL-1	523	544+00	TO	549+00	LT	6										0.095						
LL-2	523	544+00	TO	549+00	RT	7										0.095						
SL-1	523	547+05	TO	547+41	RT												36					
CL-1	524	549+00	TO	554+00	C/L	5								0.095								
CL-2	524	10+00	TO	11+75	LT	2								0.034								
CW-1	524	550+14	TO	550+49	RT													62				
CW-2	524	550+06	TO	550+46	LT													75				
LL-1	524	549+00	TO	554+00	LT	5										0.095						
LL-2	524	549+00	TO	554+00	RT	4										0.095						
SL-1	524	549+75			RT												23					
SL-2	524	550+90			LT												23					
SL-3	524	550+31	TO	550+44	RT												13					
SL-4	524	10+50			LT												14					
SL-5	524	553+07	TO	553+32	RT												25					
EL-1	524	11+50	TO	11+75	LT										0.005							
EL-2	524	11+00	TO	11+75	RT										0.014							
CL-1	525	554+00	TO	559+00	C/L	6								0.095								
CL-2	525	0+00	TO	0+65	LT									0.0123								
CL-3	525	0+00	TO	0+65	LT									0.0123								
CH-1	525	0+50	TO	0+80	LT	3													30			
CH-2	525	0+15	TO	0+58	RT	3													43			
CW-1	525	555+15	TO	555+31	LT & RT													108				
CW-2	525	556+45	TO	556+45	LT & RT													101				
CW-3	525	555+72		556+33	RT													126				
CW-4	525	555+31	TO	556+21	LT													176				
CW-5	525	558+09	TO	558+57	LT													98				
LA-1	525	0+62			LT																	1
LA-2	525	0+62			LT																	1
LA-3	525	0+46			RT																	1
LA-4	525	0+46			RT																	1
LL-1	525	554+00	TO	559+00	LT	4										0.095						
LL-2	525	554+00	TO	559+00	RT	4										0.095						
SL-1	525	555+12			RT												23					
SL-2	525	556+55			LT												23					
SL-3	525	556+00			RT												23					
SL-4	525	555+50			LT												31					
CL-1	526	559+00	TO	564+00	C/L	7								0.095								
CL-2	526	560+15	TO	563+40	RT	5								0.062								
CH-1	526	563+65	TO	564+00	RT	1													35			
LA-1	526	563+65			RT																	1
LL-1	526	559+00	TO	564+00	LT	6										0.095						
LL-2	526	559+00	TO	564+00	RT	6										0.095						
TL-1	526	560+15	TO	583+40	RT															85		
SUBTOTAL SHEET 490						81								0.501	0.019	0.760	234	746	108	85		5

PAVEMENT MARKING SUBSUMMARY

CALCULATED
ARK
CHECKED
EJT

**LAK-US-20-24.99
PART 2**

490
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_T0007.dgn Sheet 7/15/2022 2:59:30 PM troyer

REF NO.	SHEET NO.	STATION TO STATION			SIDE	621	642	642	642	642	642	642	642	646	646	646	646	646	646	646	646	646
						RPM	CENTER LINE, TYPE 1	LANE LINE, 6", TYPE 1	STOP LINE, TYPE 1	CROSSWALK LINE, 12", TYPE 1	CHANNELIZING LINE, 12", TYPE 1	TRANSVERSE/DIAGONAL LINE, TYPE 1	LANE ARROW, TYPE 1	CENTER LINE	EDGE LINE, 6"	LANE LINE, 6"	STOP LINE	CROSSWALK LINE, 12"	CHANNELIZING LINE, 12"	TRANSVERSE/DIAGONAL LINE	ISLAND MARKING	LANE ARROW
						EACH	MILE	MILE	FT	FT	FT	FT	EACH	MILE	MILE	MILE	FT	FT	FT	FT	SF	EACH
CL-1	527	564+00	TO	569+00	C/L	6																
CL-2	527	568+25	TO	569+00	LT	2																
CL-3	527	98+50	TO	101+50	LT/RT	2																
CH-1	527	564+00	TO	564+96	RT	3																
CH-2	527	566+31	TO	568+00	LT	5																
CH-3	527	98+60	TO	99+43	RT	3															96	
CH-4	527	100+53	TO	101+50	LT	3															169	
CW-1	527	564+99	TO	565+08	LT & RT																83	
CW-2	527	565+47	TO	566+18	RT																97	
CW-3	527	565+16	TO	566+05	LT																125	
EL-1	527	98+50	TO	99+00	RT																136	
EL-2	527	98+50	TO	99+00	RT	2									0.010						173	
EL-3	527	101+00	TO	101+50	LT	2									0.010							
EL-4	527	101+00	TO	101+50	RT										0.010							
LA-1	527	564+24			RT																	1
LA-2	527	564+71			RT																	1
LA-3	527	566+56			LT																	1
LA-4	527	567+22			LT																	1
LA-5	527	567+88			LT																	1
LA-6	527	98+72			RT																	1
LA-7	527	99+13			RT																	1
LA-8	527	100+83			LT																	1
LA-9	527	101+33			LT																	1
LL-1	527	564+00	TO	564+81	LT	4											0.095					
LL-2	527	564+00	TO	564+96	RT	4											0.095					
SL-1	527	564+96			RT																23	
SL-2	527	564+81			RT																11	
SL-3	527	566+31			LT																23	
SL-4	527	566+46			LT																11	
SL-5	527	99+43			RT																22	
SL-6	527	99+23			RT																11	
SL-7	527	100+53			LT																29	
SL-8	527	100+73			LT																11	
TL-1	527	568+25	TO	569+00	LT																	42
CL-1	528	95+00	TO	98+50	C/L	5																
CL-2	528	96+10	TO	98+35	RT	4																
CL-3	528	91+50	TO	95+00	C/L	4																
CL-4	528	89+52	TO	91+50	C/L	3																
EL-1	528	95+00	TO	98+50	LT																	
EL-2	528	95+00	TO	98+50	RT	9																
EL-3	528	91+50	TO	95+00	LT																	
EL-4	528	91+50	TO	95+00	RT	4																
EL-5	528	89+52	TO	91+50	LT																	
EL-6	528	89+52	TO	91+50	RT	1																
TL-1	528	96+10	TO	98+35	RT																	
SUBTOTAL SHEET 491						66									0.380	0.380	0.190	141	434	445	98	9

PAVEMENT MARKING SUBSUMMARY

**LAK-US-20-24.99
PART 2**

CALCULATED
ARK
CHECKED
EJT

491
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_I0008.dgn Sheet 7/15/2022 2:59:31PM troyer

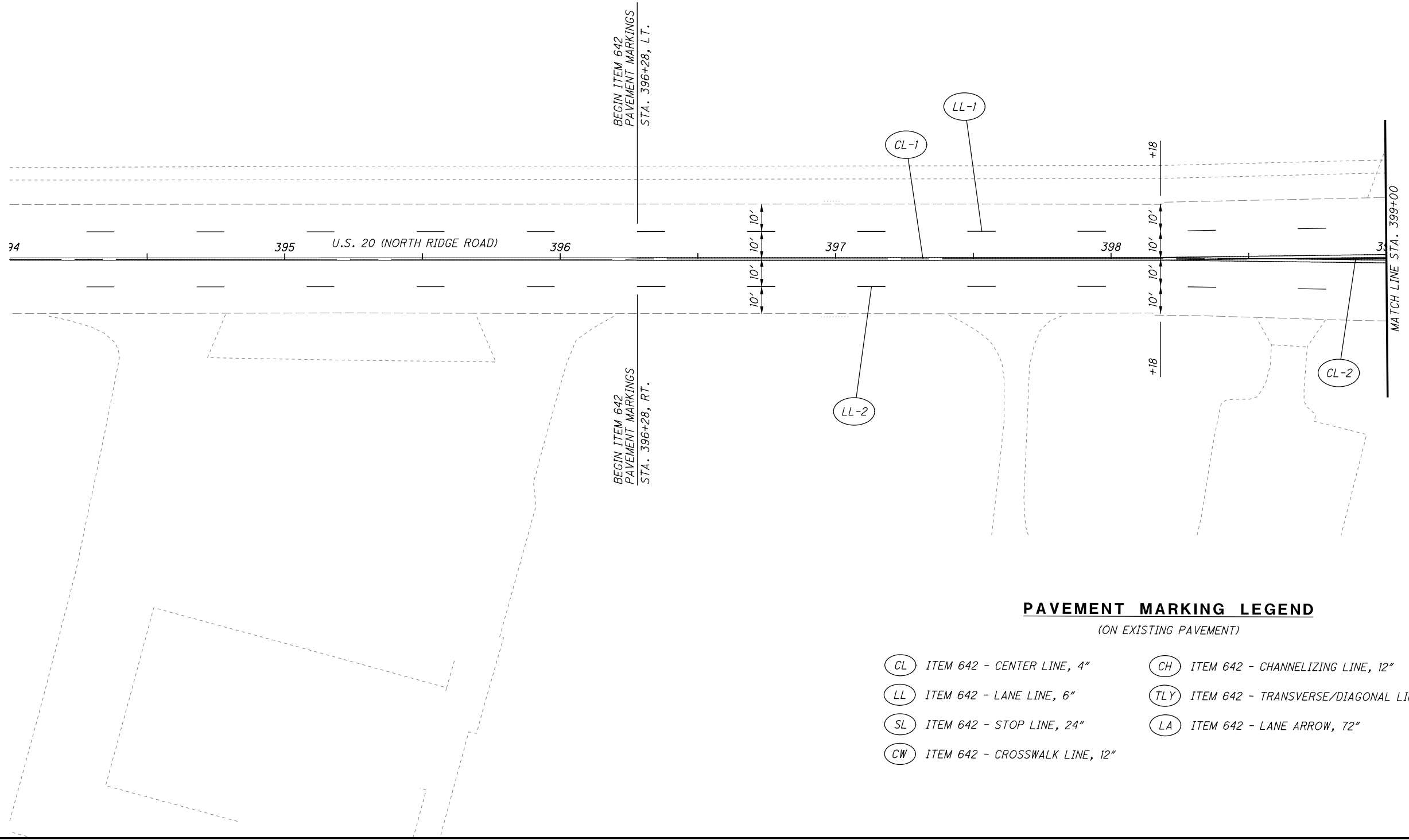
REF NO.	SHEET NO.	STATION TO STATION			SIDE	621	642	642	642	642	642	642	642	646	646	646	646	646	646	646	646	646			
						RPM	CENTER LINE, TYPE 1	LANE LINE, 6", TYPE 1	STOP LINE, TYPE 1	CROSSWALK LINE, 12", TYPE 1	CHANNELIZING LINE, 12", TYPE 1	TRANSVERSE/DIAGONAL LINE, TYPE 1	LANE ARROW, TYPE 1	CENTER LINE	EDGE LINE, 6"	LANE LINE, 6"	STOP LINE	CROSSWALK LINE, 12"	CHANNELIZING LINE, 12"	TRANSVERSE/DIAGONAL LINE	ISLAND MARKING	LANE ARROW			
						EACH	MILE	MILE	FT	FT	FT	FT	EACH	FT	MILE	MILE	MILE	FT	FT	FT	FT	SF	EACH		
CL-1	529	101+50	TO	105+00	C/L	5									0.066										
CL-2	529	102+25	TO	104+50	LT	4									0.043										
CL-3	529	105+00	TO	105+86	C/L	1									0.016										
CH-1	529	101+50	TO	102+00	LT	1													50						
EL-1	529	101+50	TO	105+00	RT											0.066									
EL-2	529	101+50	TO	105+00	LT	9										0.066									
EL-3	529	105+00	TO	105+86	RT											0.016									
EL-4	529	105+00	TO	105+86	LT	1										0.016									
LA-1	529	101+83			LT																		1		
TL-2	529	102+25	TO	104+50	LT																59				
CL-1	530	569+00	TO	574+00	C/L	4									0.095										
CL-2	530	569+00	TO	572+50	RT	4									0.066										
LL-1	530	569+00	TO	574+00	LT	5											0.095								
LL-2	530	569+00	TO	574+00	RT	5											0.095								
TL-1	530	569+00	TO	572+50	RT																64				
CL-1	531	574+00	TO	579+00	C/L										0.095										
LL-1	531	574+00	TO	579+00	LT												0.095								
LL-2	531	574+00	TO	579+00	RT												0.095								
CL-1	532	579+00	TO	583+49	C/L										0.085										
LL-1	532	579+00	TO	583+49	LT												0.085								
LL-2	532	579+00	TO	583+49	RT												0.085								
TOTALS FROM SHEET 492															0.466	0.164	0.550			50	123		1		
TOTALS FROM SHEET 485						30	0.571	0.568	136	620	539	201	14		0.160		0.296								
TOTALS FROM SHEET 486						142									0.682	0.780	0.786	89	113	510	190				8
TOTALS FROM SHEET 487						176									0.940	0.117	1.710	14		85					
TOTALS FROM SHEET 488						101									0.407	0.629	0.380	82	275	523	168	51			8
TOTALS FROM SHEET 489						155									0.794	0.046	1.520	109	127	380	89				
TOTALS FROM SHEET 490						81									0.501	0.019	0.760	234	746	108	85				5
TOTALS FROM SHEET 491						66									0.380	0.380	0.190	141	434	445	98				9
TOTAL						751	0.571	0.568	136	620	539	201	14		4.329	2.135	6.192	669	1695	2016	838	51		31	
TOTALS CARRIED TO GENERAL SUMMARY						751	0.57	0.57	136	620	539	201	14		4.33	2.13	6.19	669	1695	2016	838	51		31	

PAVEMENT MARKING SUBSUMMARY

LAK-US-20-24.99 PART 2

CALCULATED
ARK
CHECKED
EJT

492
697



PAVEMENT MARKING LEGEND

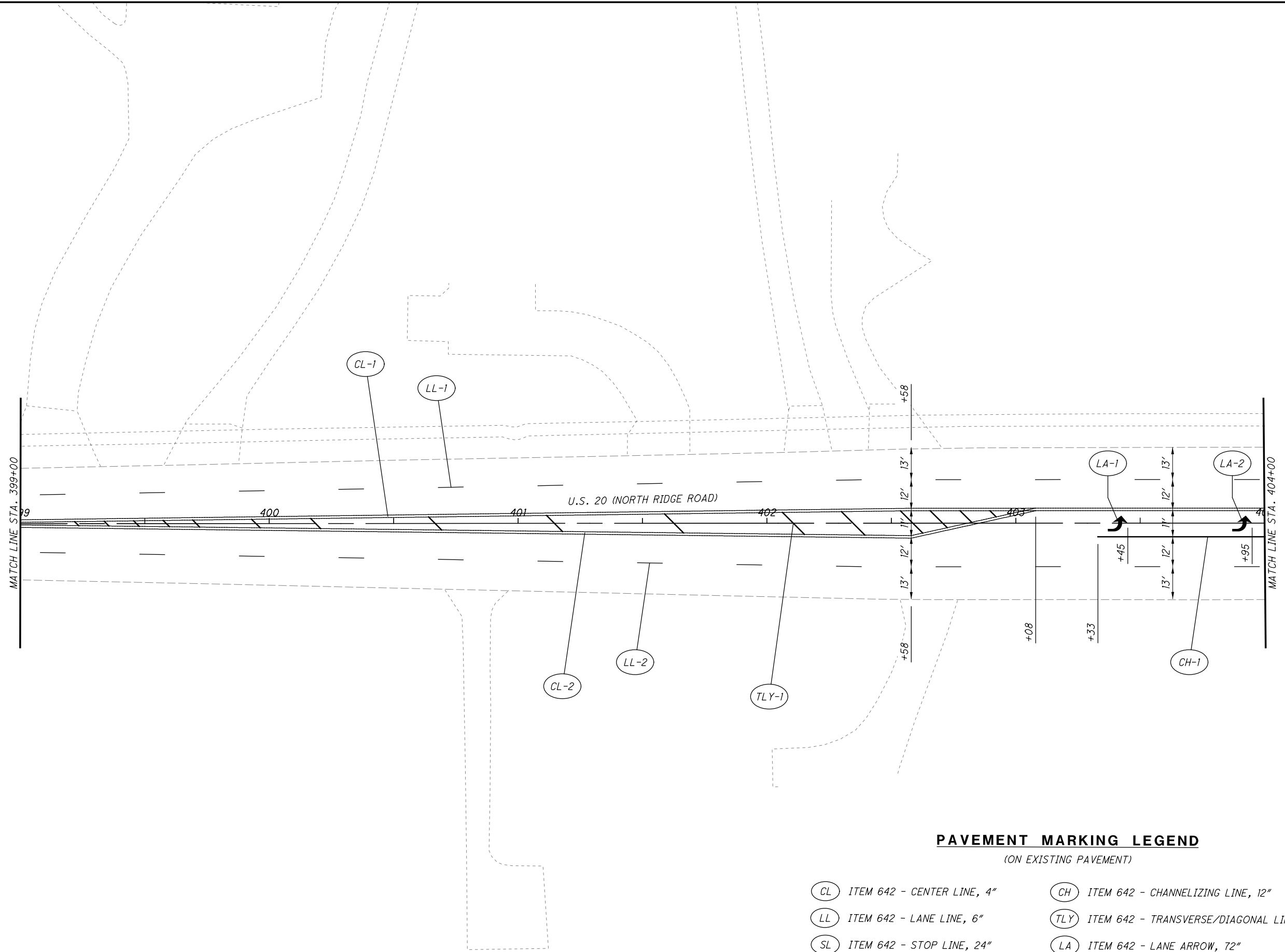
(ON EXISTING PAVEMENT)

- | | |
|-------------------------------------|---------------------------------------------------------|
| (CL) ITEM 642 - CENTER LINE, 4" | (CH) ITEM 642 - CHANNELIZING LINE, 12" |
| (LL) ITEM 642 - LANE LINE, 6" | (TLY) ITEM 642 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 642 - STOP LINE, 24" | (LA) ITEM 642 - LANE ARROW, 72" |
| (CW) ITEM 642 - CROSSWALK LINE, 12" | |



CALCULATED
JDC
CHECKED
EJT

**PAVEMENT MARKING PLAN
STA. 394+00 TO STA. 399+00**



PAVEMENT MARKING LEGEND

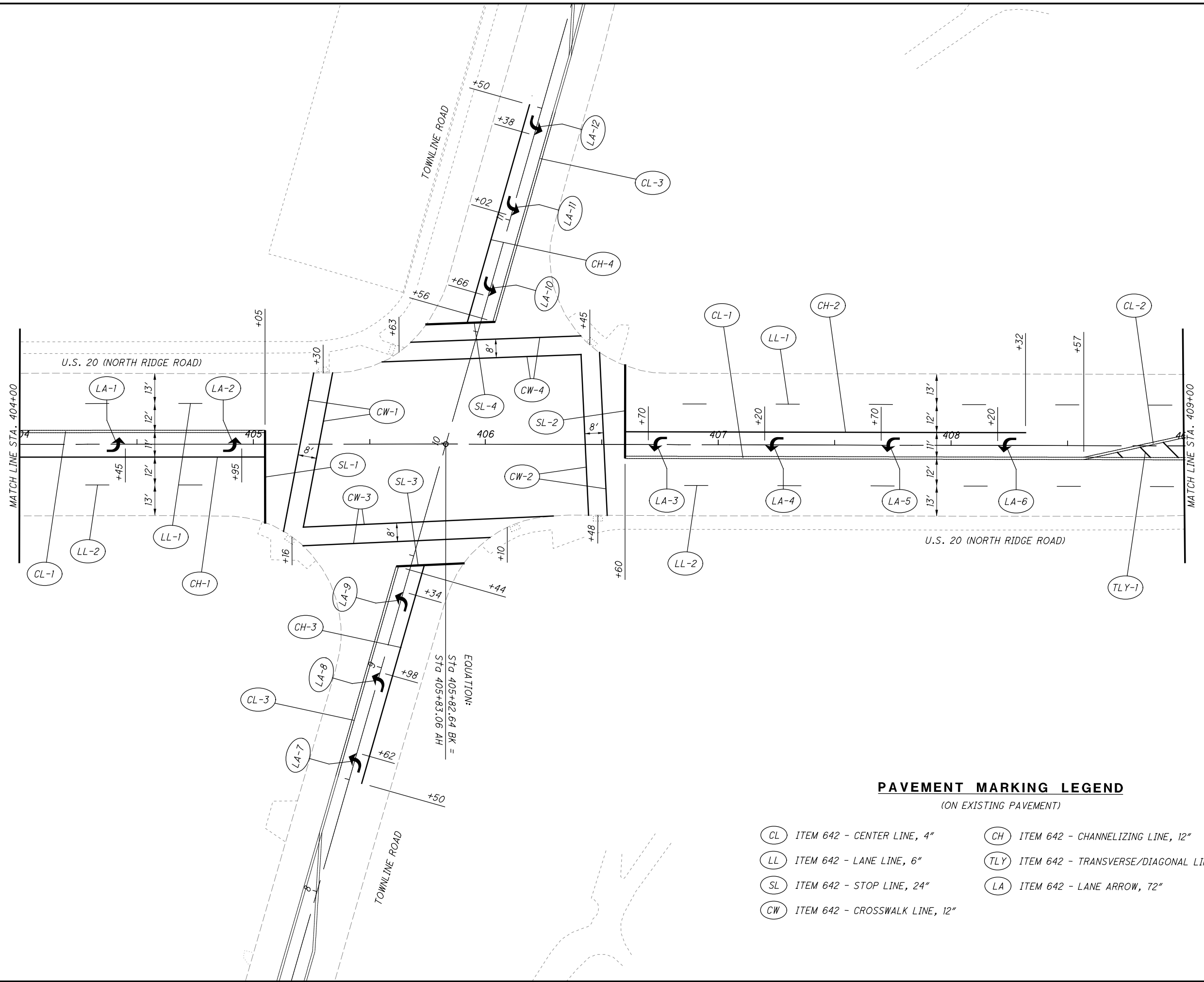
(ON EXISTING PAVEMENT)

- | | |
|-------------------------------------|---------------------------------------------------------|
| (CL) ITEM 642 - CENTER LINE, 4" | (CH) ITEM 642 - CHANNELIZING LINE, 12" |
| (LL) ITEM 642 - LANE LINE, 6" | (TLY) ITEM 642 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 642 - STOP LINE, 24" | (LA) ITEM 642 - LANE ARROW, 72" |
| (CW) ITEM 642 - CROSSWALK LINE, 12" | |

CALCULATED
JDC
CHECKED
EJT

HORIZONTAL SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 399+00 TO STA. 404+00



EQUATION:
 STA 405+82.64 BK =
 STA 404+83.06 AH =

PAVEMENT MARKING LEGEND
 (ON EXISTING PAVEMENT)

- | | |
|-------------------------------------|---------------------------------------------------------|
| (CL) ITEM 642 - CENTER LINE, 4" | (CH) ITEM 642 - CHANNELIZING LINE, 12" |
| (LL) ITEM 642 - LANE LINE, 6" | (TLY) ITEM 642 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 642 - STOP LINE, 24" | (LA) ITEM 642 - LANE ARROW, 72" |
| (CW) ITEM 642 - CROSSWALK LINE, 12" | |

CALCULATED JDC
 CHECKED EJT

0 20 40
 HORIZONTAL SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 404+00 TO STA. 409+00

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP261.dgn Sheet 7/15/2022 2:59:34 PM troyer

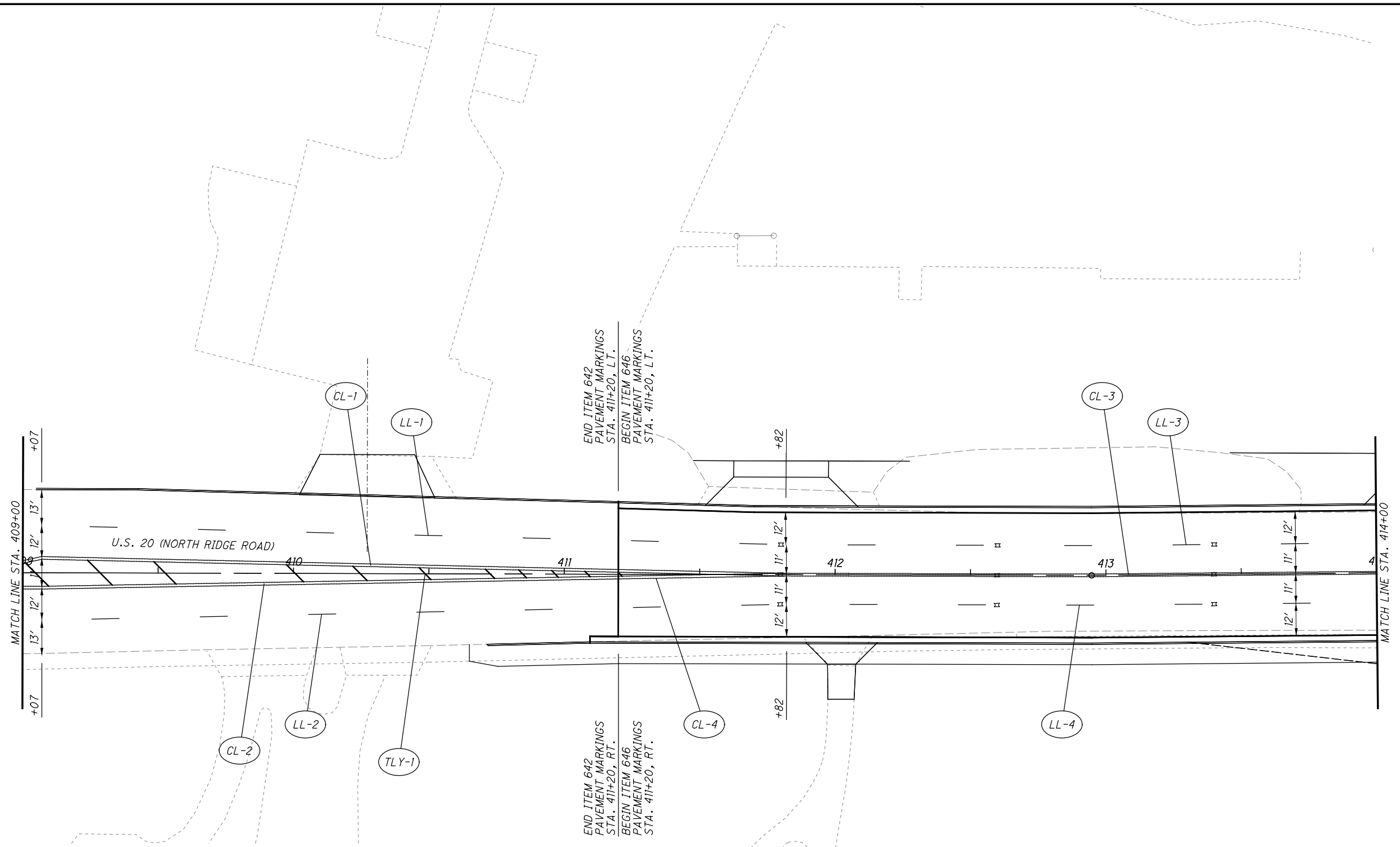


CALCULATED
JDC
CHECKED
EJT

PAVEMENT MARKING PLAN
STA. 409+00 TO STA. 414+00

LAK-US-20-24.99
PART 2

496
697

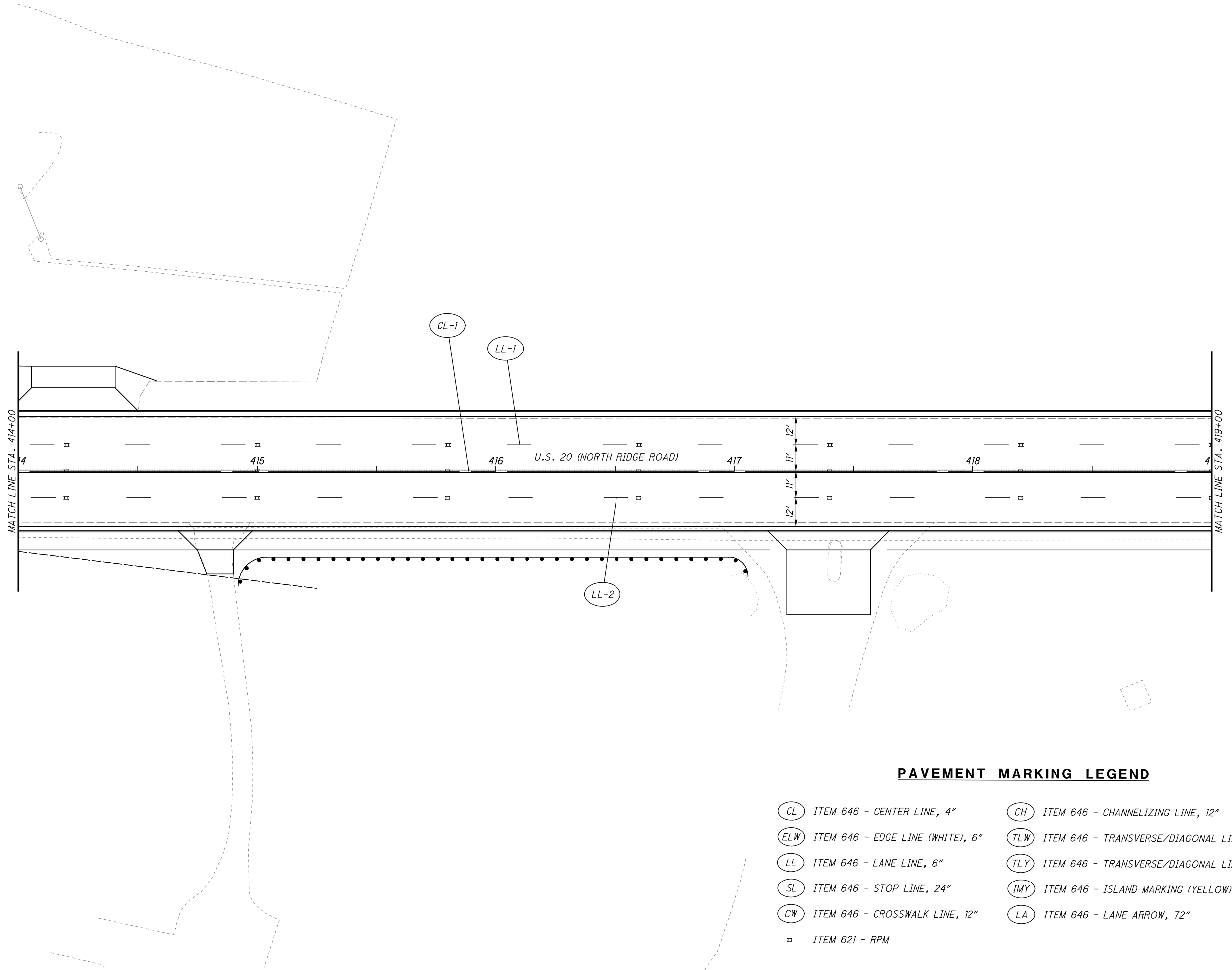


PAVEMENT MARKING LEGEND
(ON EXISTING PAVEMENT)

- | | |
|-------------------------------------|---------------------------------------------------------|
| (CL) ITEM 642 - CENTER LINE, 4" | (CH) ITEM 642 - CHANNELIZING LINE, 12" |
| (LL) ITEM 642 - LANE LINE, 6" | (TLY) ITEM 642 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 642 - STOP LINE, 24" | (LA) ITEM 642 - LANE ARROW, 72" |
| (CW) ITEM 642 - CROSSWALK LINE, 12" | |

PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |



PAVEMENT MARKING LEGEND

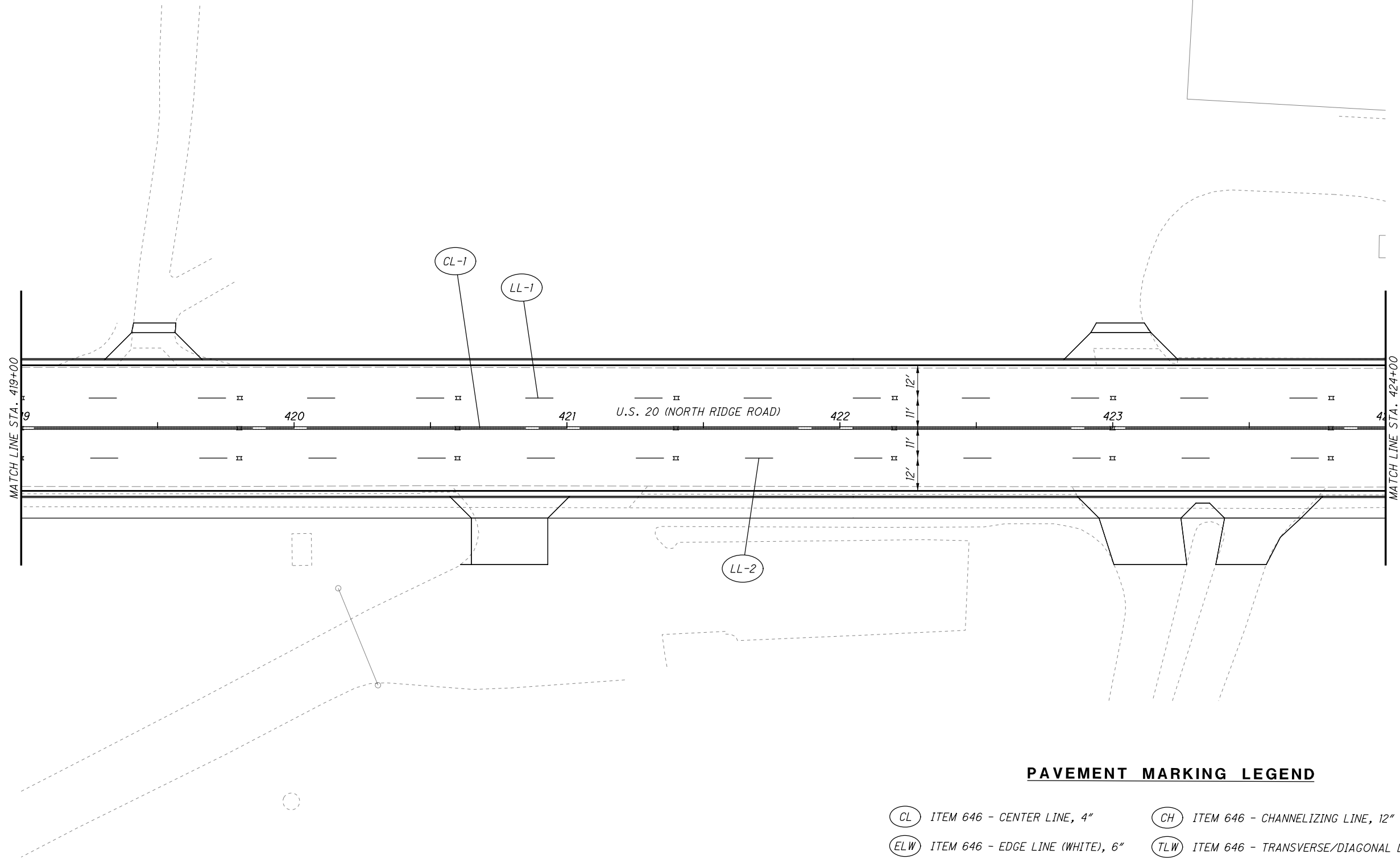
- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |



CALCULATED
JDC
CHECKED
EJT

**PAVEMENT MARKING PLAN
STA. 414+00 TO STA. 419+00**

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP263.dgn Sheet 7/15/2022 2:59:35 PM Troyer



PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▬ ITEM 621 - RPM | |



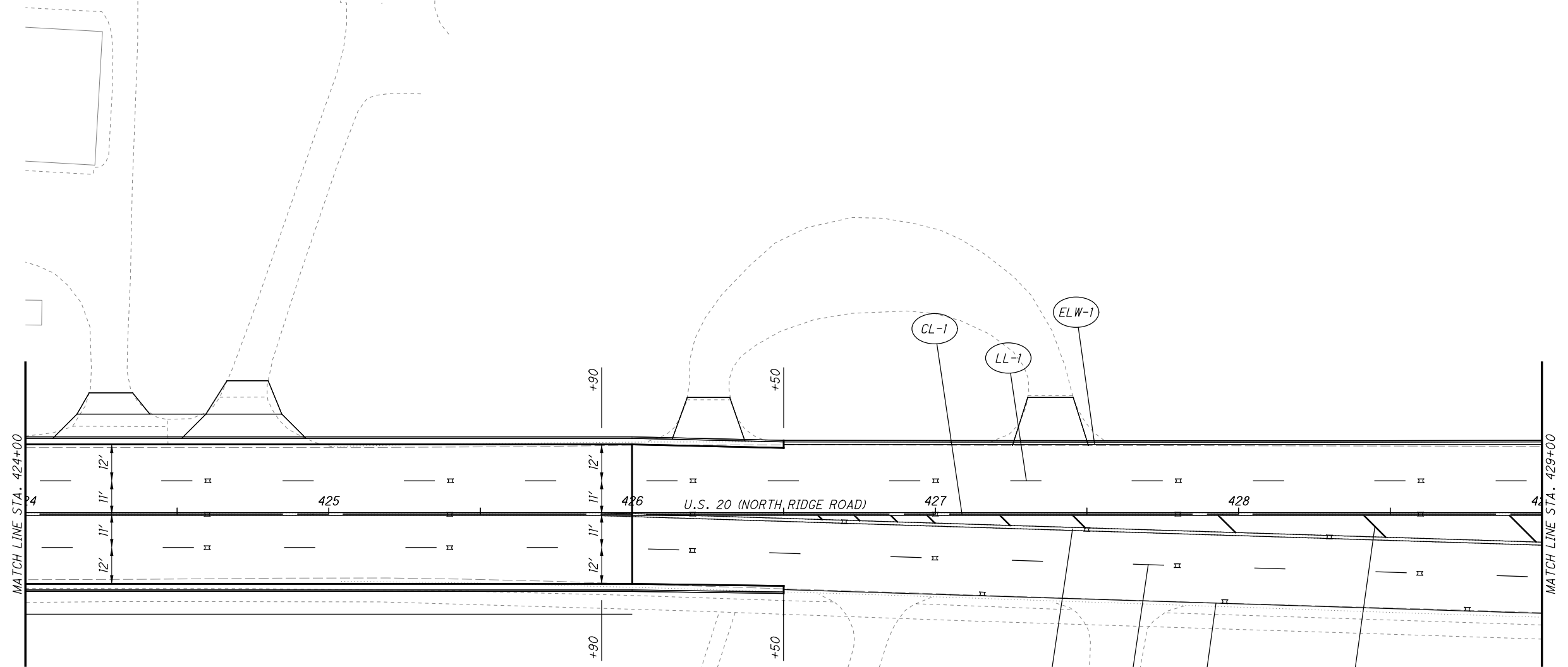
CALCULATED
JDC
CHECKED
EJT

**PAVEMENT MARKING PLAN
STA. 419+00 TO STA. 424+00**

**LAK-US-20-24.99
PART 2**

498
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP264.dgn Sheet 7/15/2022 2:59:35 PM Troyer



PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED JDC
CHECKED EJT

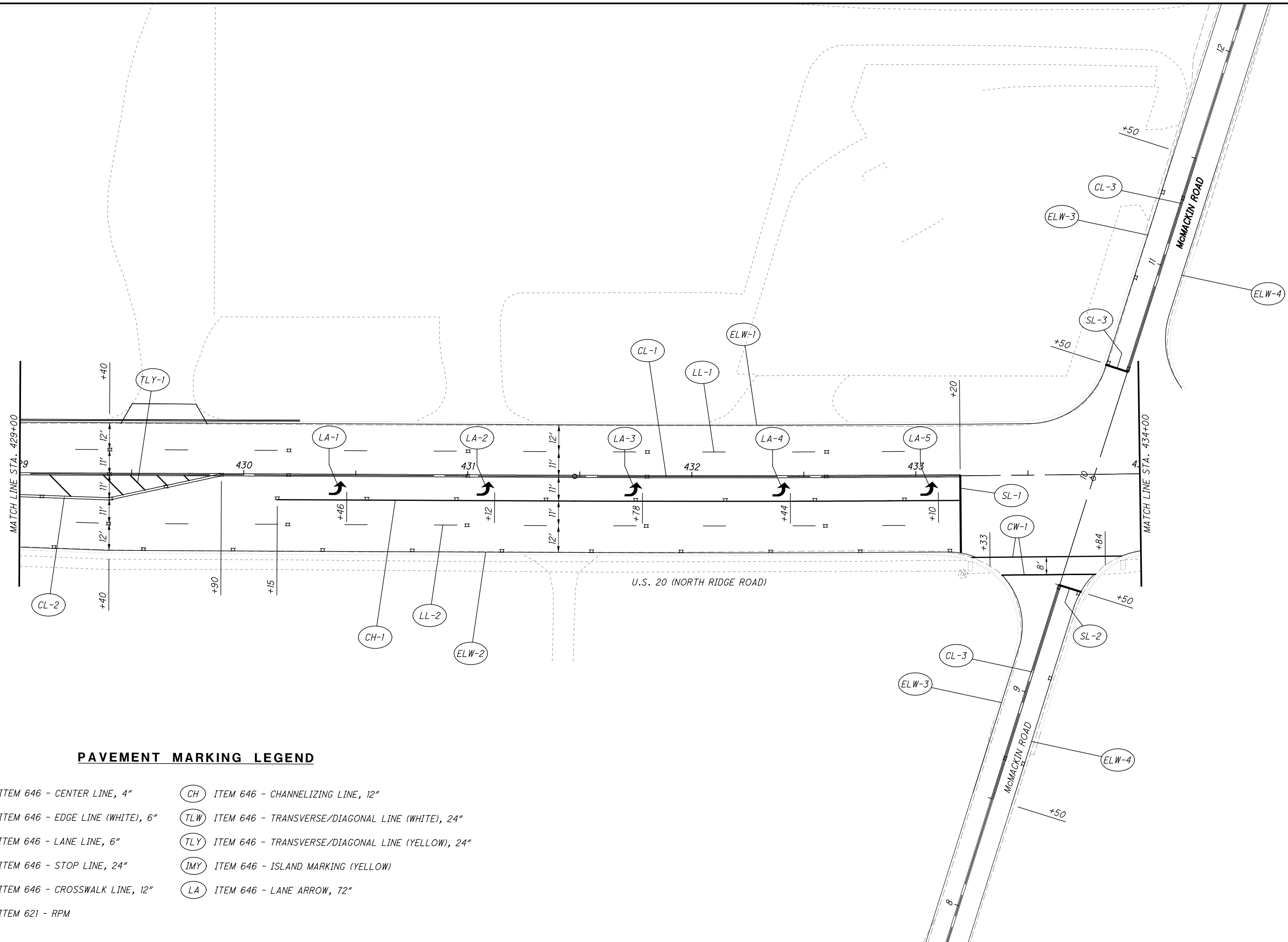
HORIZONTAL SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 424+00 TO STA. 429+00**

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_TP265.dgn Sheet 7/15/2022 2:59:36 PM Troyer

CALCULATED JDC CHECKED EJT

0 20 40
HORIZONTAL SCALE IN FEET



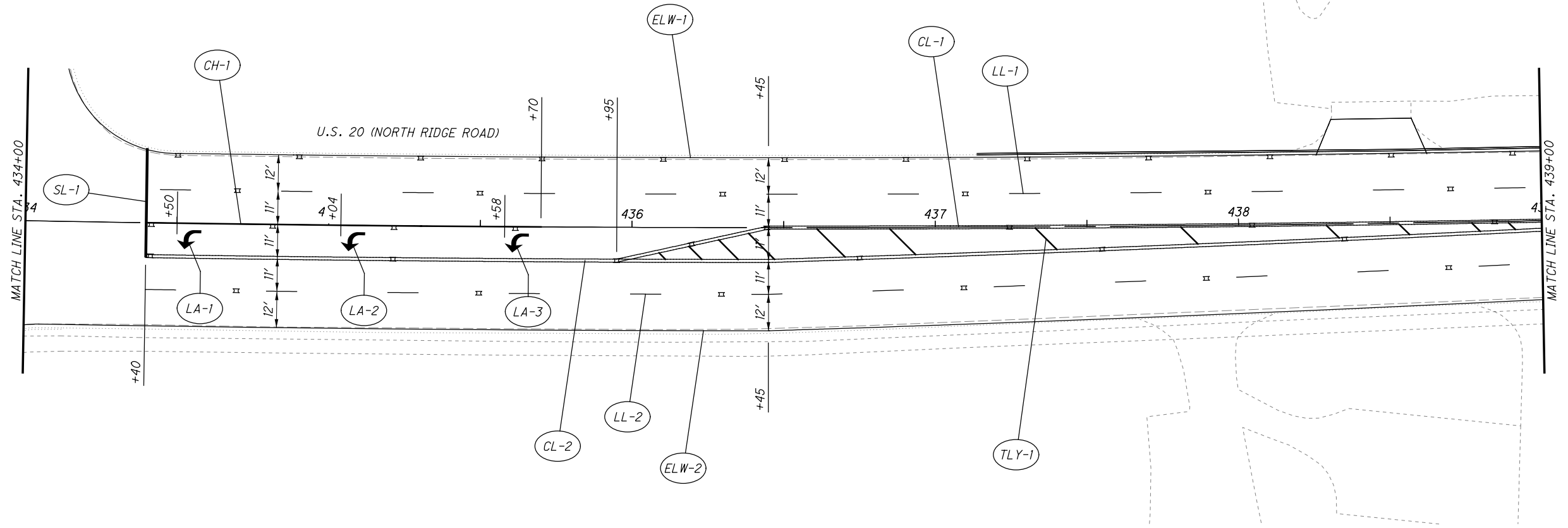
PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▬ ITEM 621 - RPM | |

PAVEMENT MARKING PLAN
STA. 429+00 TO STA. 434+00

LAK-US-20-24.99
PART 2

500
697



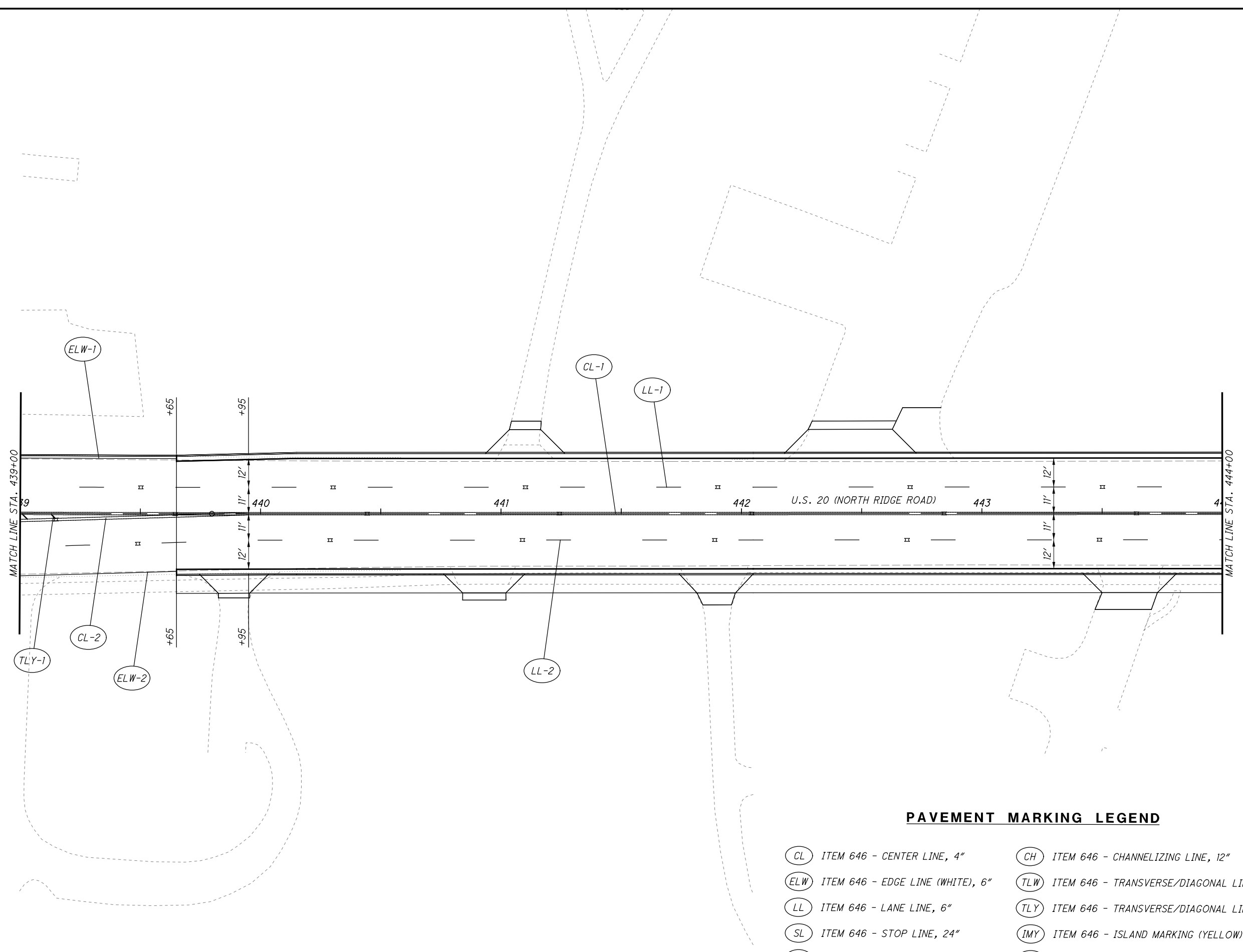
PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED
JDC
CHECKED
EJT

0 20 40
HORIZONTAL
SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 434+00 TO STA. 439+00**



PAVEMENT MARKING LEGEND

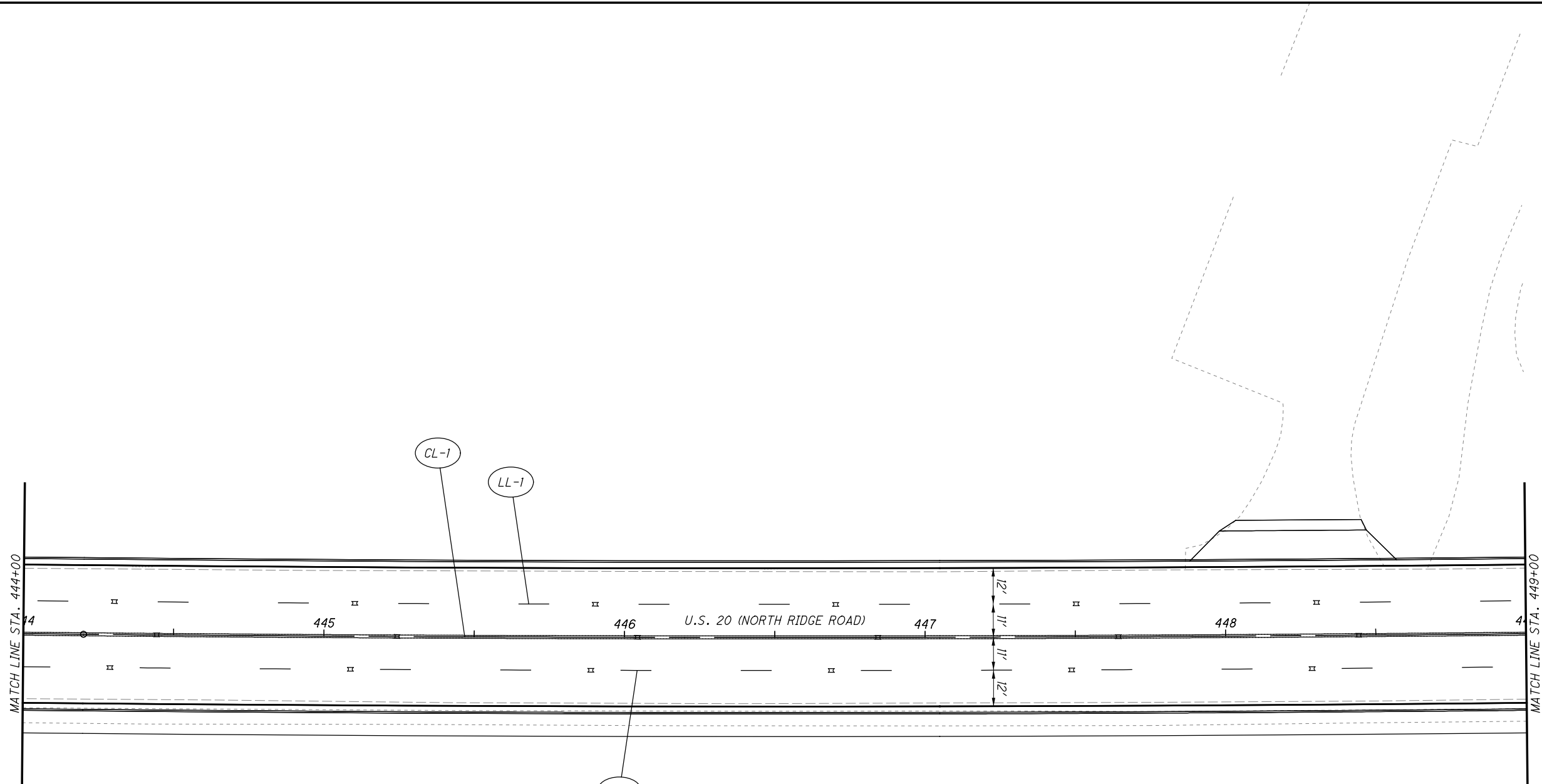
- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED JDC
CHECKED EJT

HORIZONTAL SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 439+00 TO STA. 444+00**

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_TP268.dgn Sheet 7/15/2022 2:59:37 PM Troyer



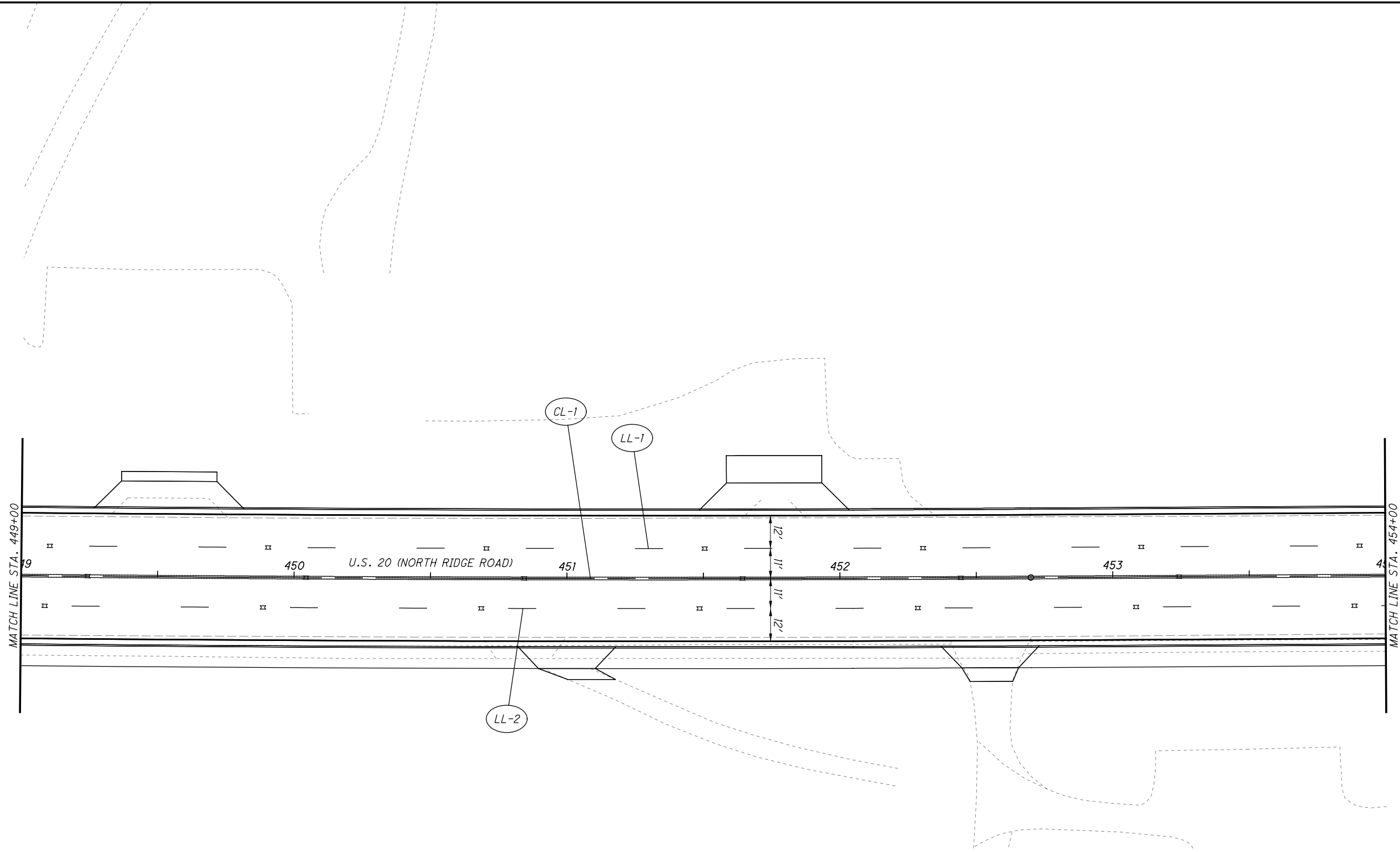
CALCULATED JDC
CHECKED EJT

0 20 40
HORIZONTAL SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 444+00 TO STA. 449+00

PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |



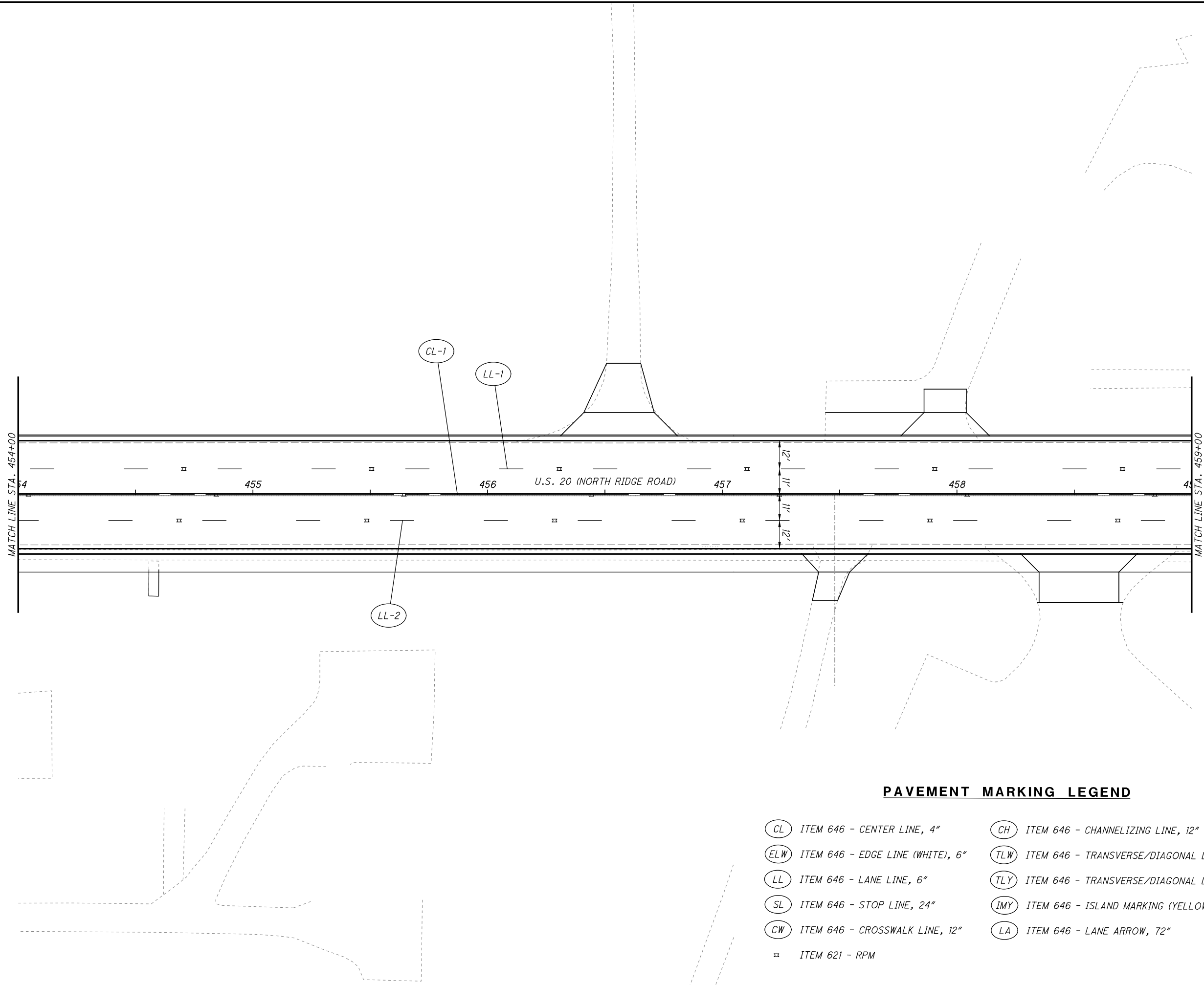
CALCULATED
JDC
CHECKED
EJT

0 20 40
HORIZONTAL
SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 419+00 TO STA. 424+00

PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |



PAVEMENT MARKING LEGEND

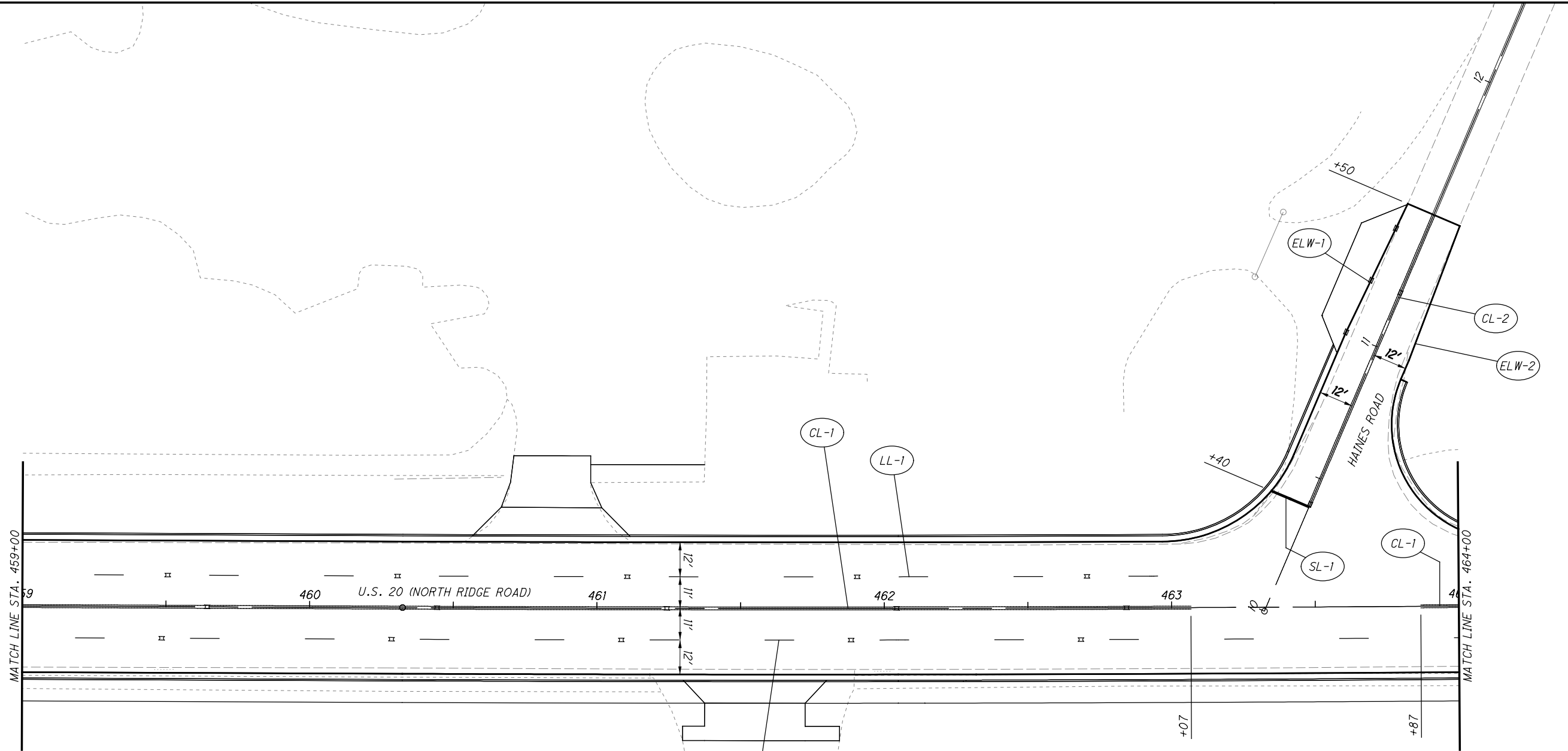
- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED
JDC
CHECKED
EJT

0 20 40
HORIZONTAL
SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 454+00 TO STA. 459+00**

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP271.dgn Sheet 7/15/2022 2:59:39 PM troyer



PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▬ ITEM 621 - RPM | |

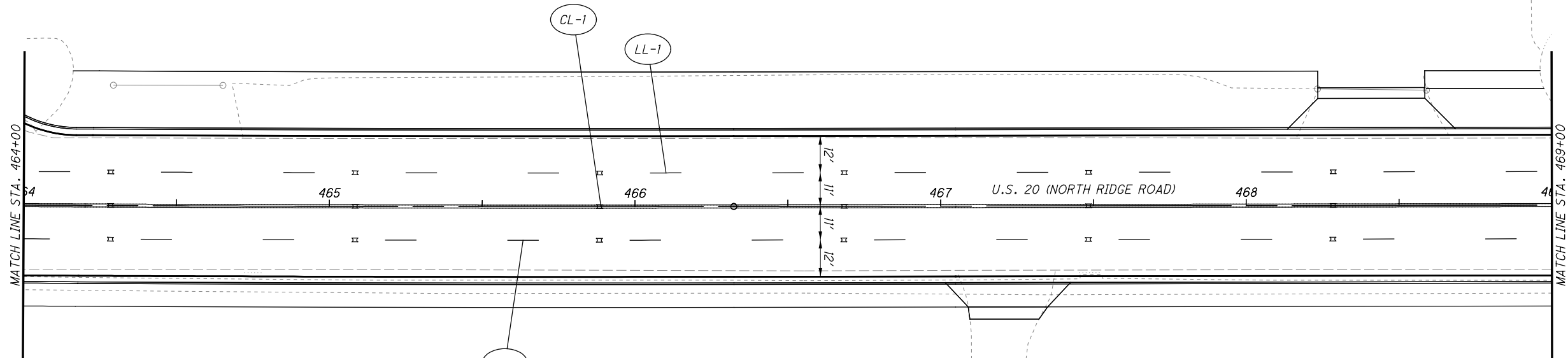
CALCULATED JDC
CHECKED EJT

0 20 40
10
HORIZONTAL
SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 459+00 TO STA. 464+00**

**LAK-US-20-24.99
PART 2**

506
697



PAVEMENT MARKING LEGEND

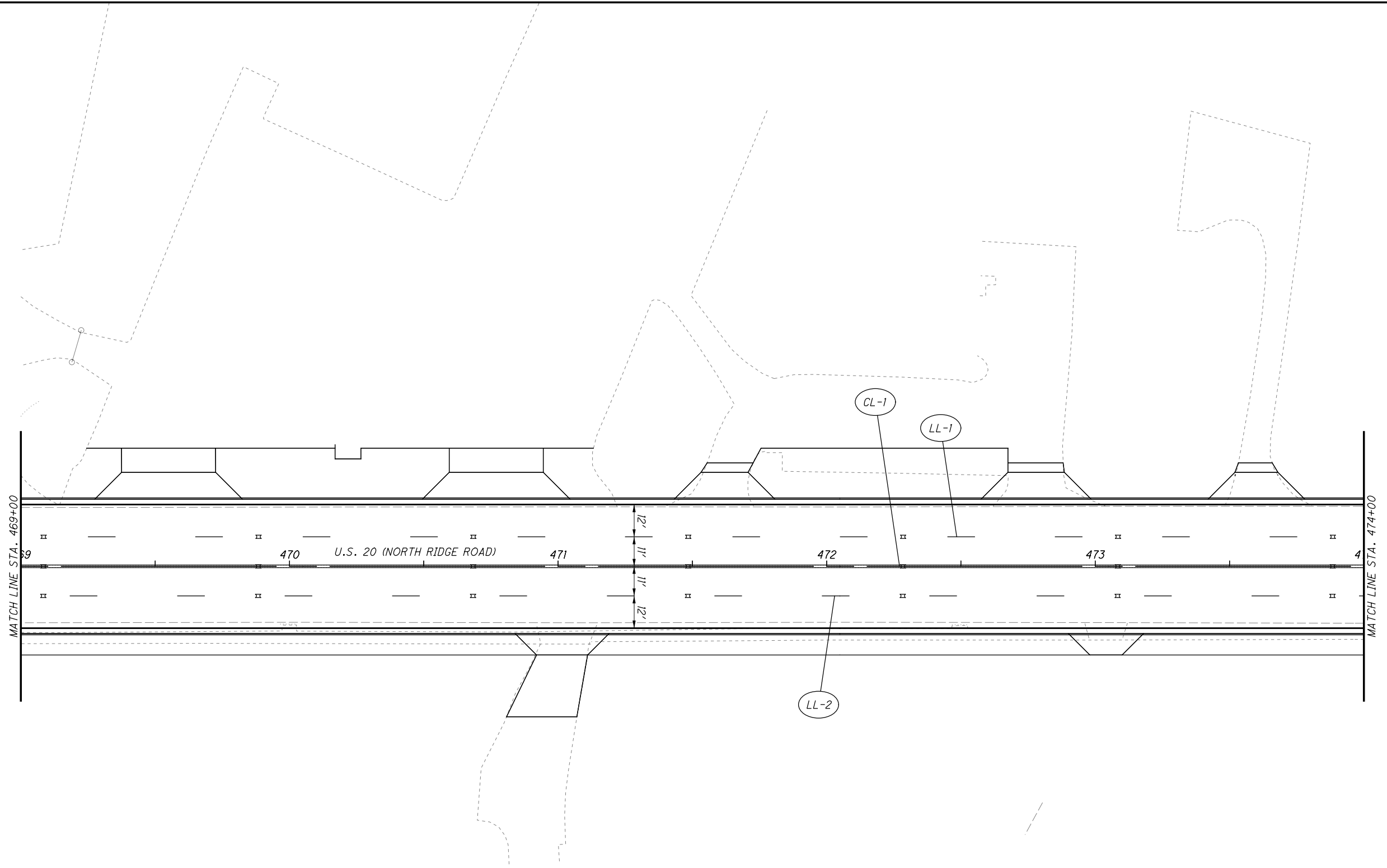
- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED
JDC
CHECKED
EJT

0 10 20 40
HORIZONTAL
SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 464+00 TO STA. 469+00

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP273.dgn Sheet 7/15/2022 2:59:40 PM troyer



CALCULATED JDC
CHECKED EJT

0 20 40
HORIZONTAL SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 469+00 TO STA. 474+00

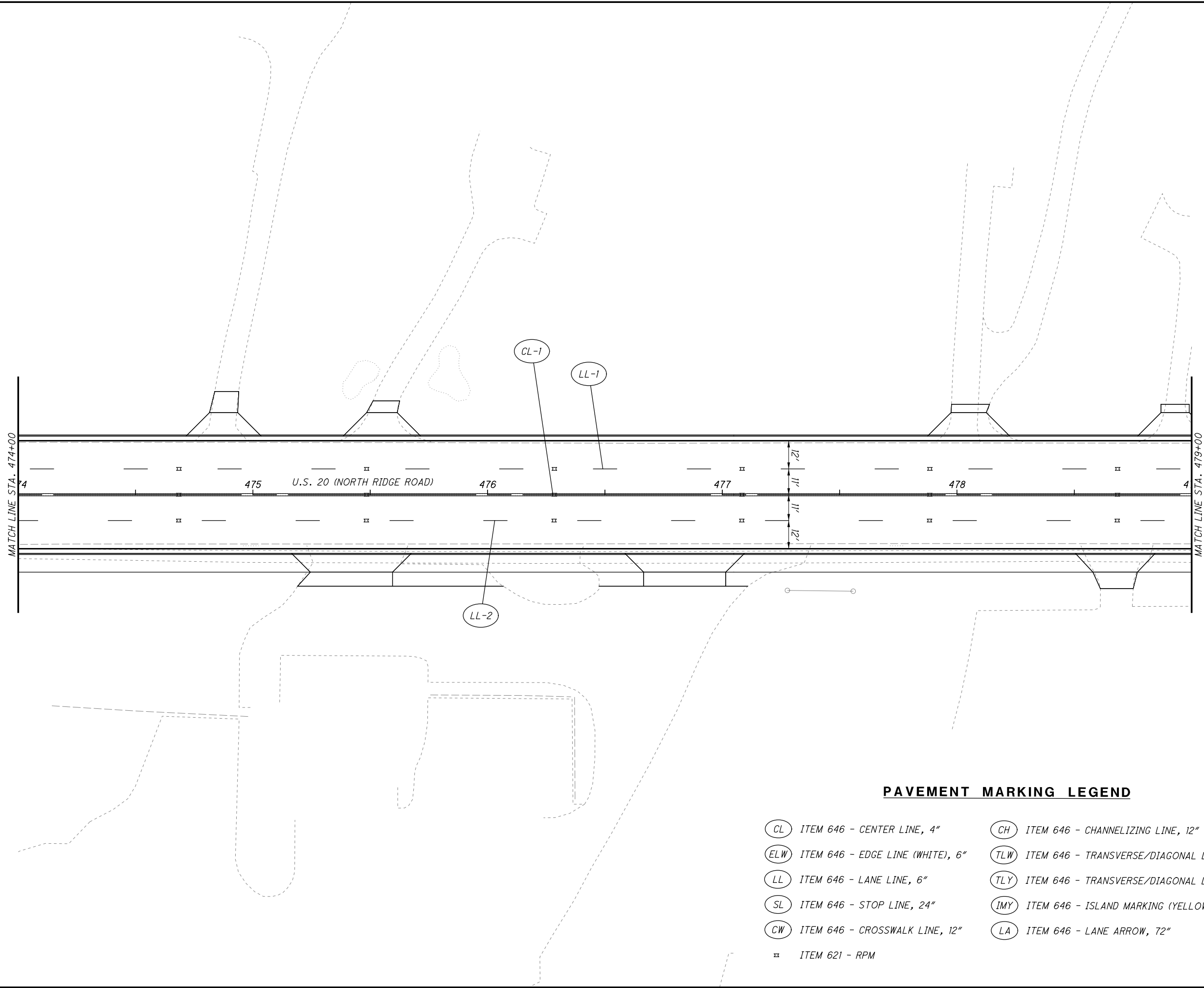
LAK-US-20-24.99
PART 2

508
697

PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP274.dgn Sheet 7/15/2022 2:59:41PM troyer



CALCULATED JDC
 CHECKED EJT

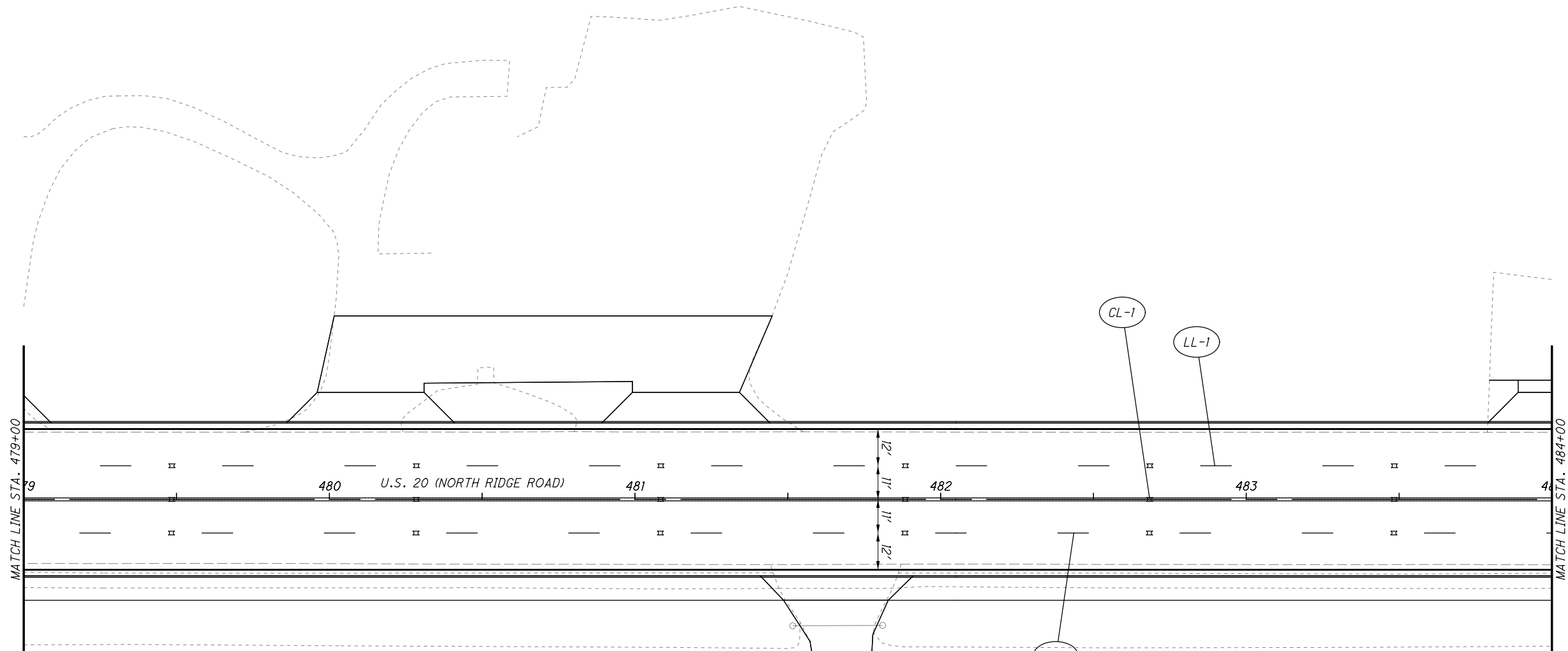
0 20 40
 HORIZONTAL
 SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 474+00 TO STA. 479+00

PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP275.dgn Sheet 7/15/2022 2:59:41PM Troyer



PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |



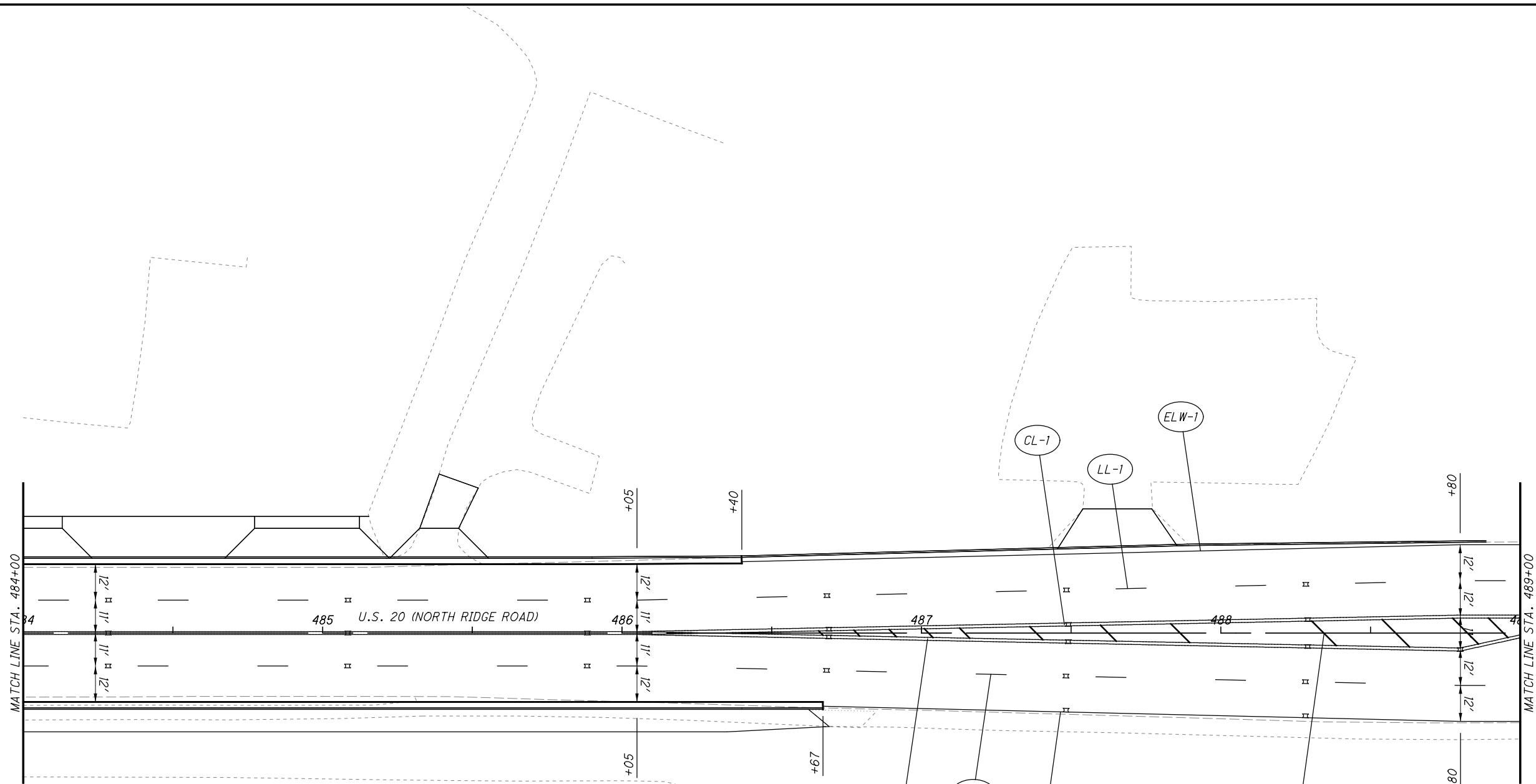
CALCULATED
JDC
CHECKED
EJT

**PAVEMENT MARKING PLAN
STA. 479+00 TO STA. 484+00**

**LAK-US-20-24.99
PART 2**

510
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP276.dgn Sheet 7/15/2022 2:59:42 PM Troyer



CALCULATED JDC CHECKED EJT

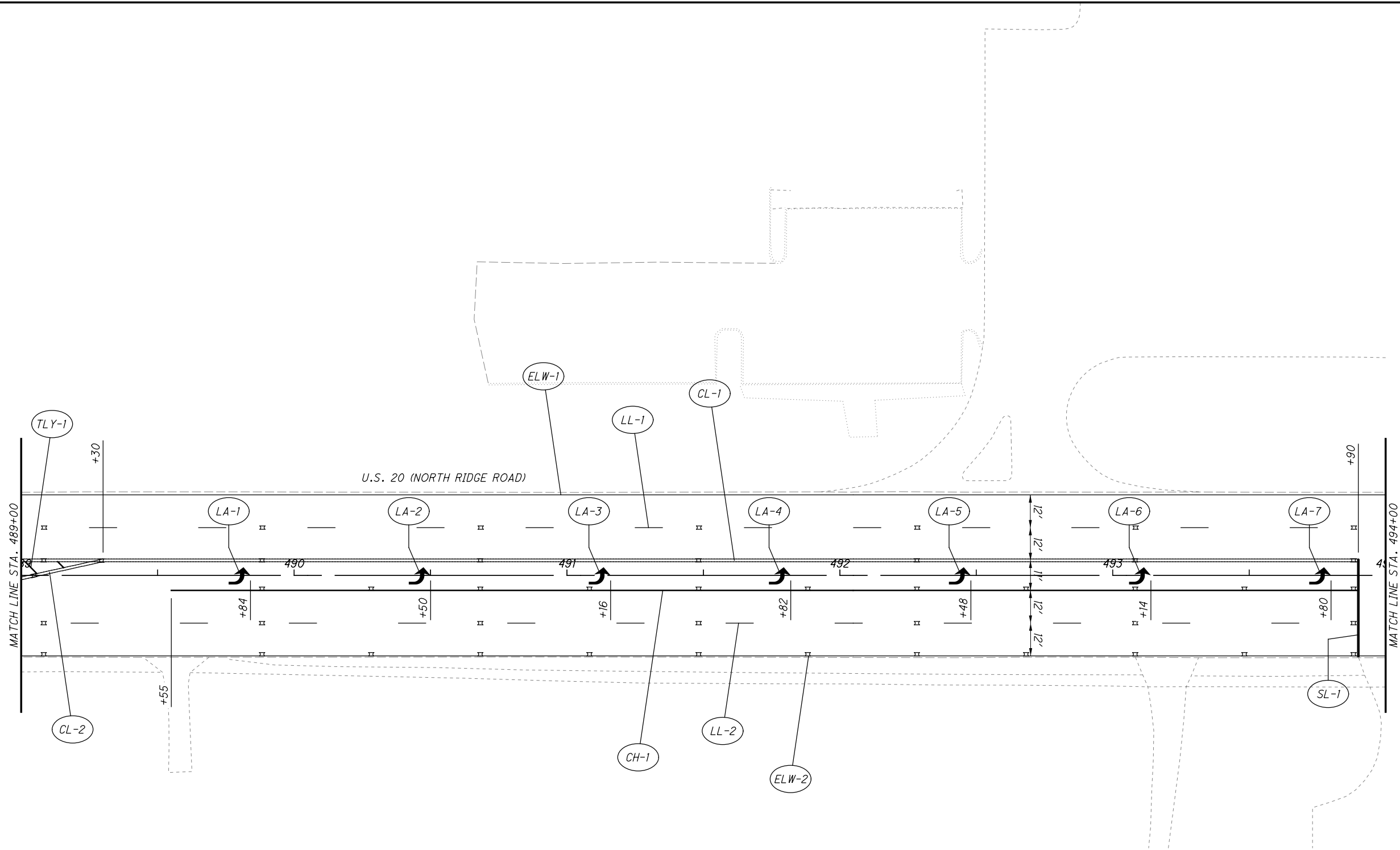
0 20 40
HORIZONTAL SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 484+00 TO STA. 489+00

PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP277.dgn Sheet 7/15/2022 2:59:42 PM Troyer



PAVEMENT MARKING LEGEND

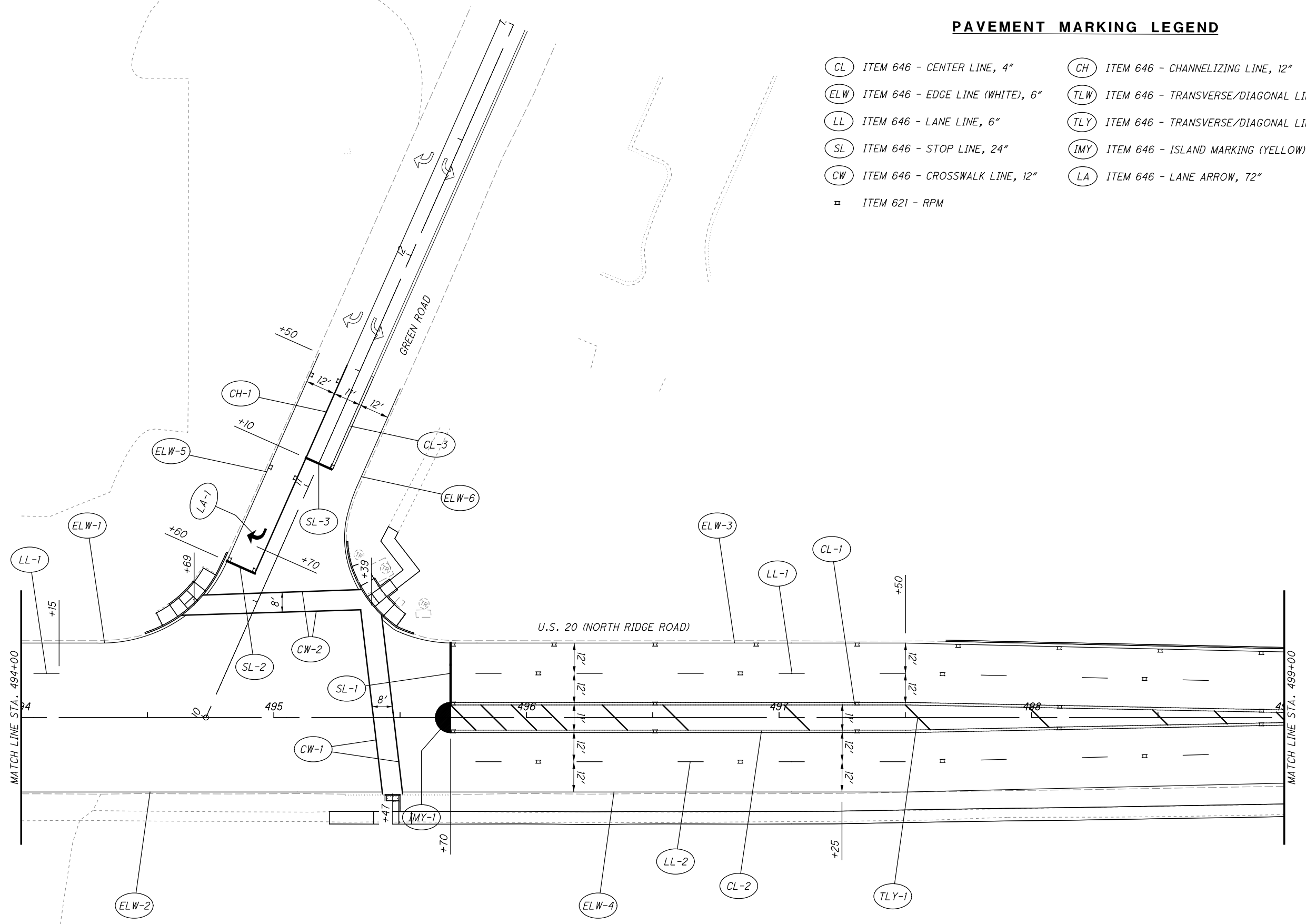
- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED JDC
CHECKED EJT

HORIZONTAL SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 489+00 TO STA. 494+00**

**LAK-US-20-24.99
PART 2**



PAVEMENT MARKING LEGEND

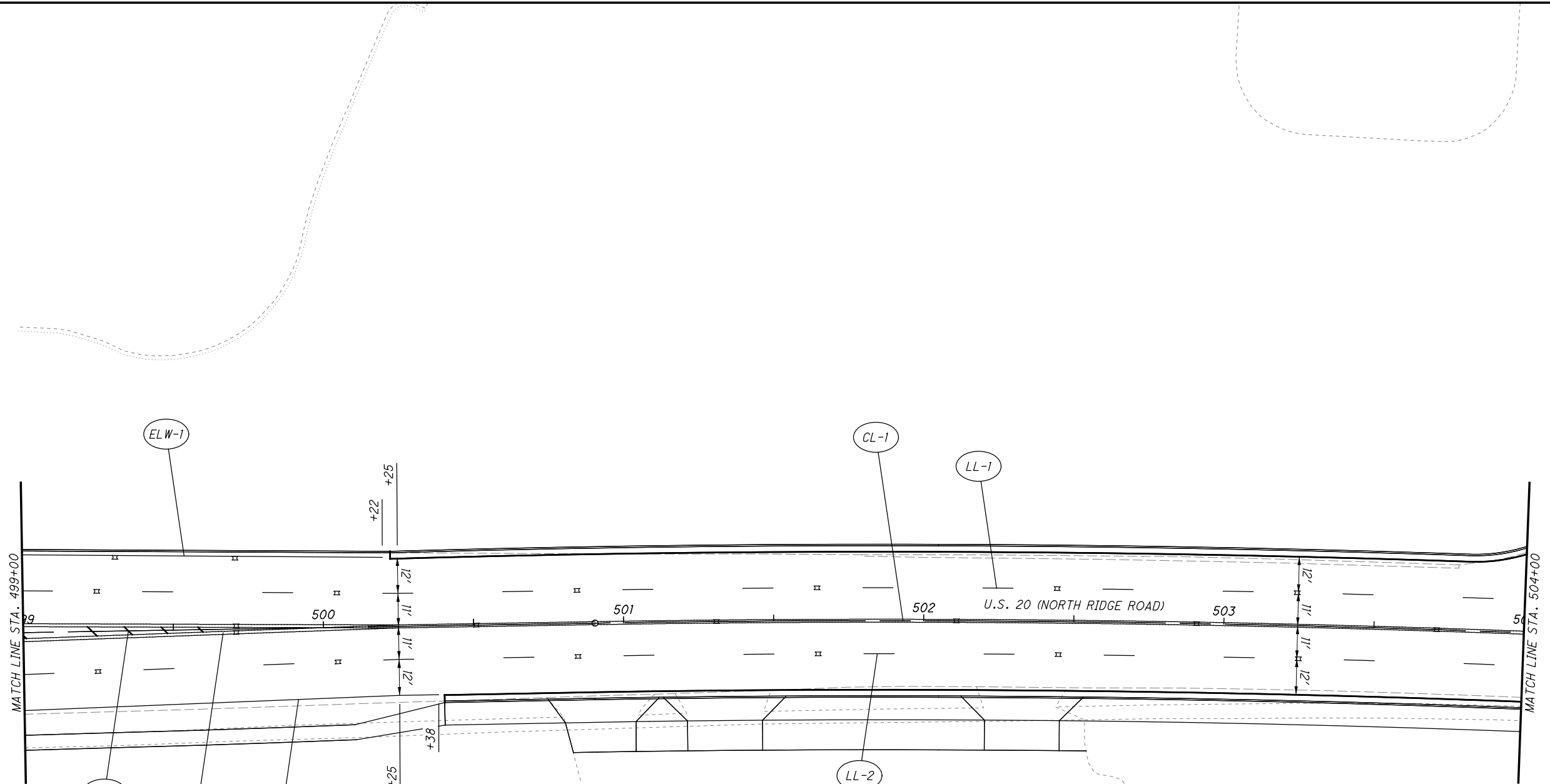
- | | | | |
|-----|----------------------------------|-----|---------------------------------------------------|
| CL | ITEM 646 - CENTER LINE, 4" | CH | ITEM 646 - CHANNELIZING LINE, 12" |
| ELW | ITEM 646 - EDGE LINE (WHITE), 6" | TLW | ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| LL | ITEM 646 - LANE LINE, 6" | TLY | ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| SL | ITEM 646 - STOP LINE, 24" | IMY | ITEM 646 - ISLAND MARKING (YELLOW) |
| CW | ITEM 646 - CROSSWALK LINE, 12" | LA | ITEM 646 - LANE ARROW, 72" |
| ▣ | ITEM 621 - RPM | | |

CALCULATED JDC
CHECKED EJT

0 20 40
HORIZONTAL SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 494+00 TO STA. 499+00**

**LAK-US-20-24.99
PART 2**



PAVEMENT MARKING LEGEND

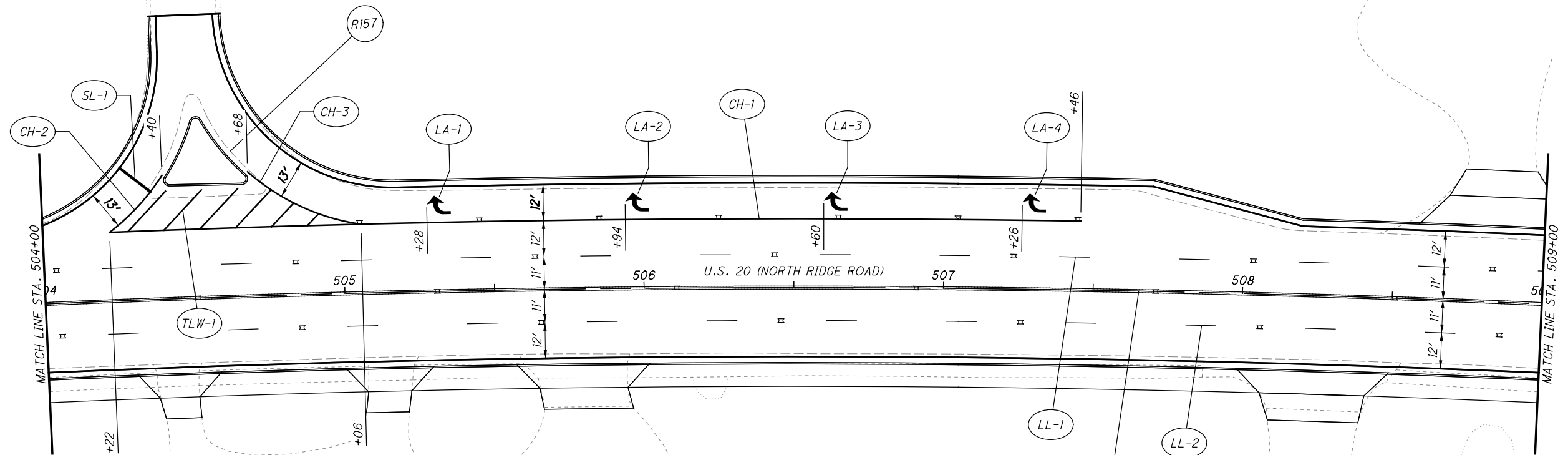
- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED
JDC
CHECKED
EJT

0 20 40
HORIZONTAL
SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 499+00 TO STA. 504+00**

H:\2018\180743\000\Project\Data\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_TP280.dgn Sheet 7/15/2022 2:59:44 PM Troyer



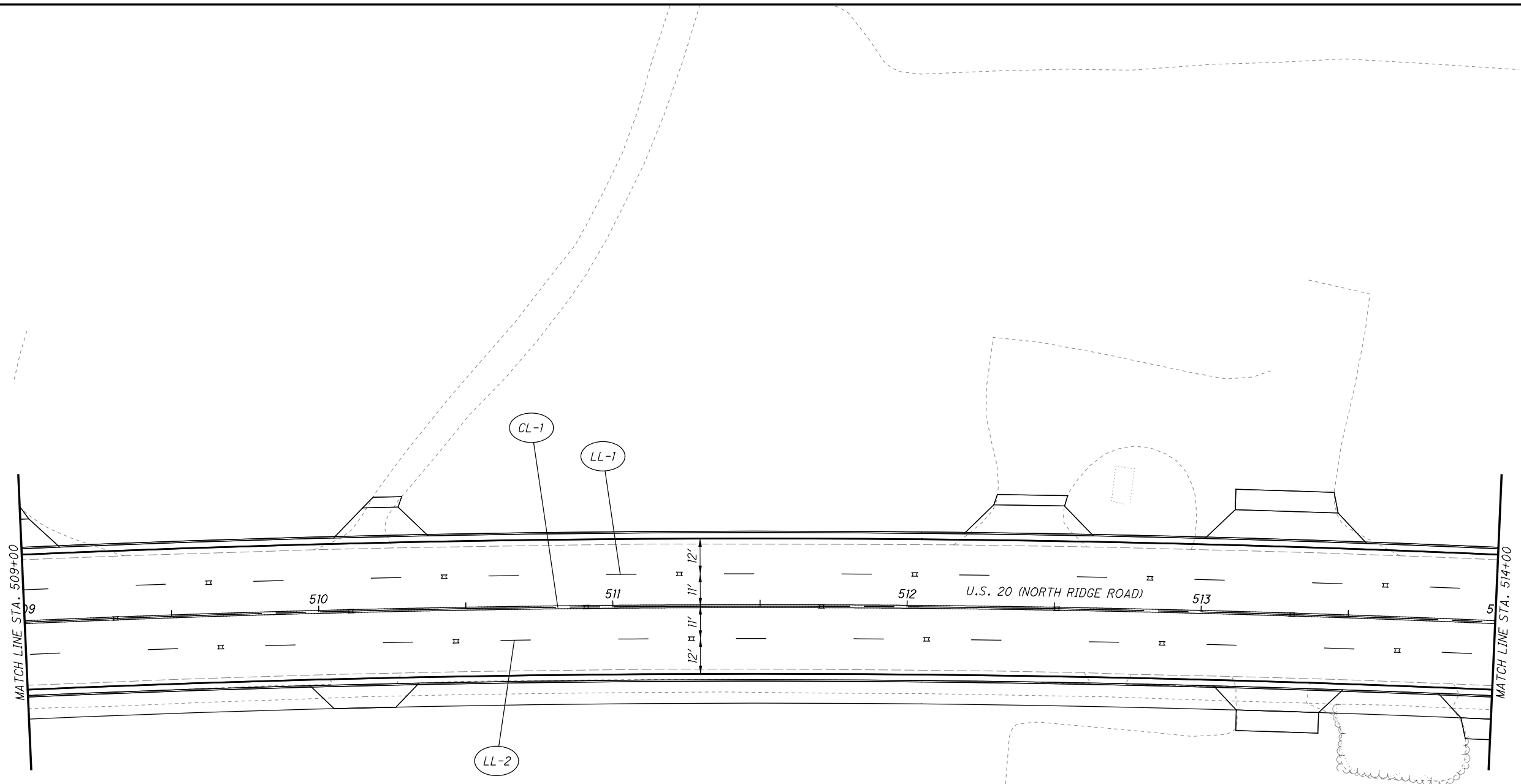
PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED JDC CHECKED EJT

**PAVEMENT MARKING PLAN
STA. 504+00 TO STA. 509+00**

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP281.dgn Sheet 7/15/2022 2:59:44 PM troyer



CALCULATED JDC
CHECKED EJT

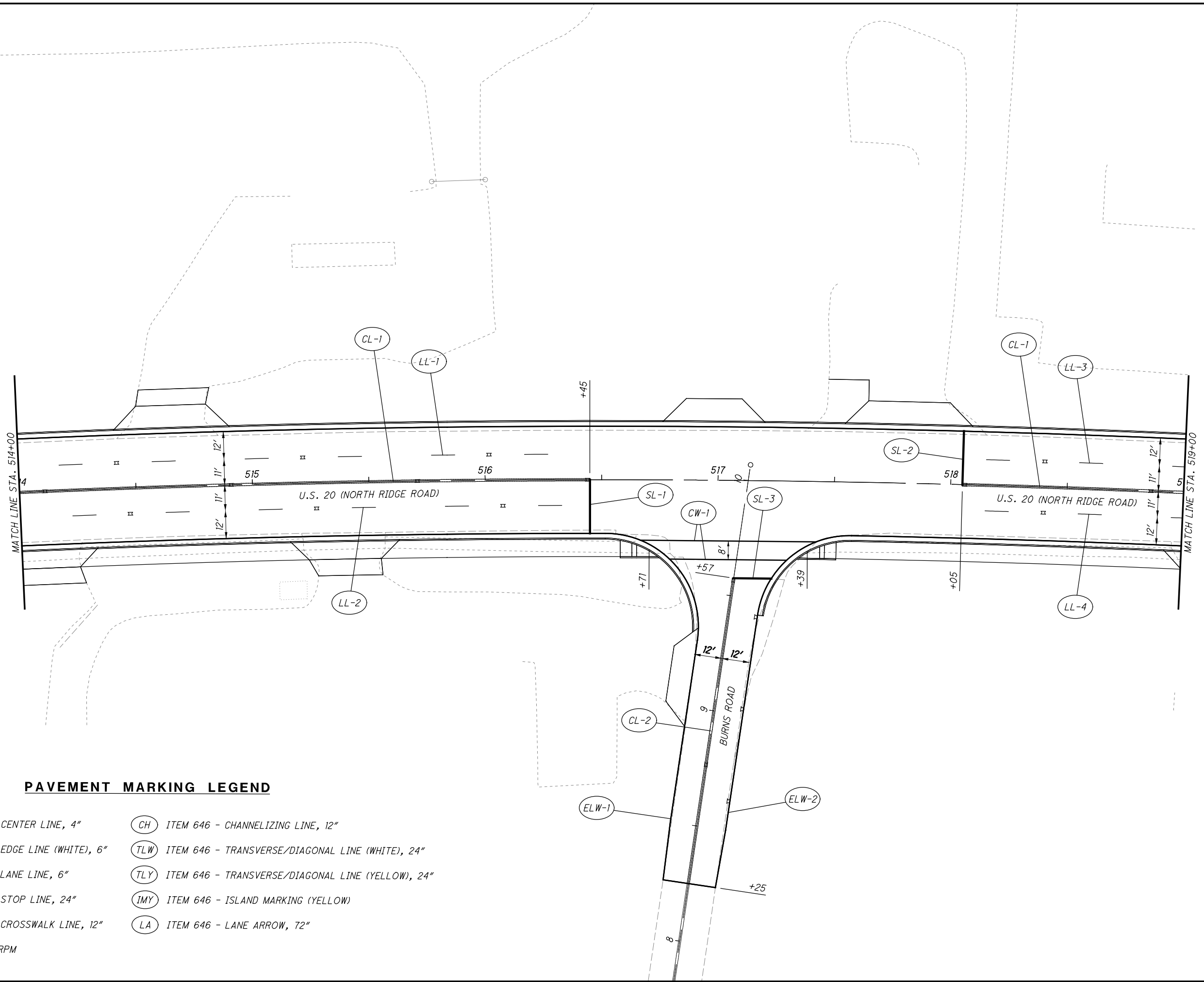
0 20 40
HORIZONTAL SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 509+00 TO STA. 514+00

PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP282.dgn Sheet 7/15/2022 2:59:44 PM Troyer



PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

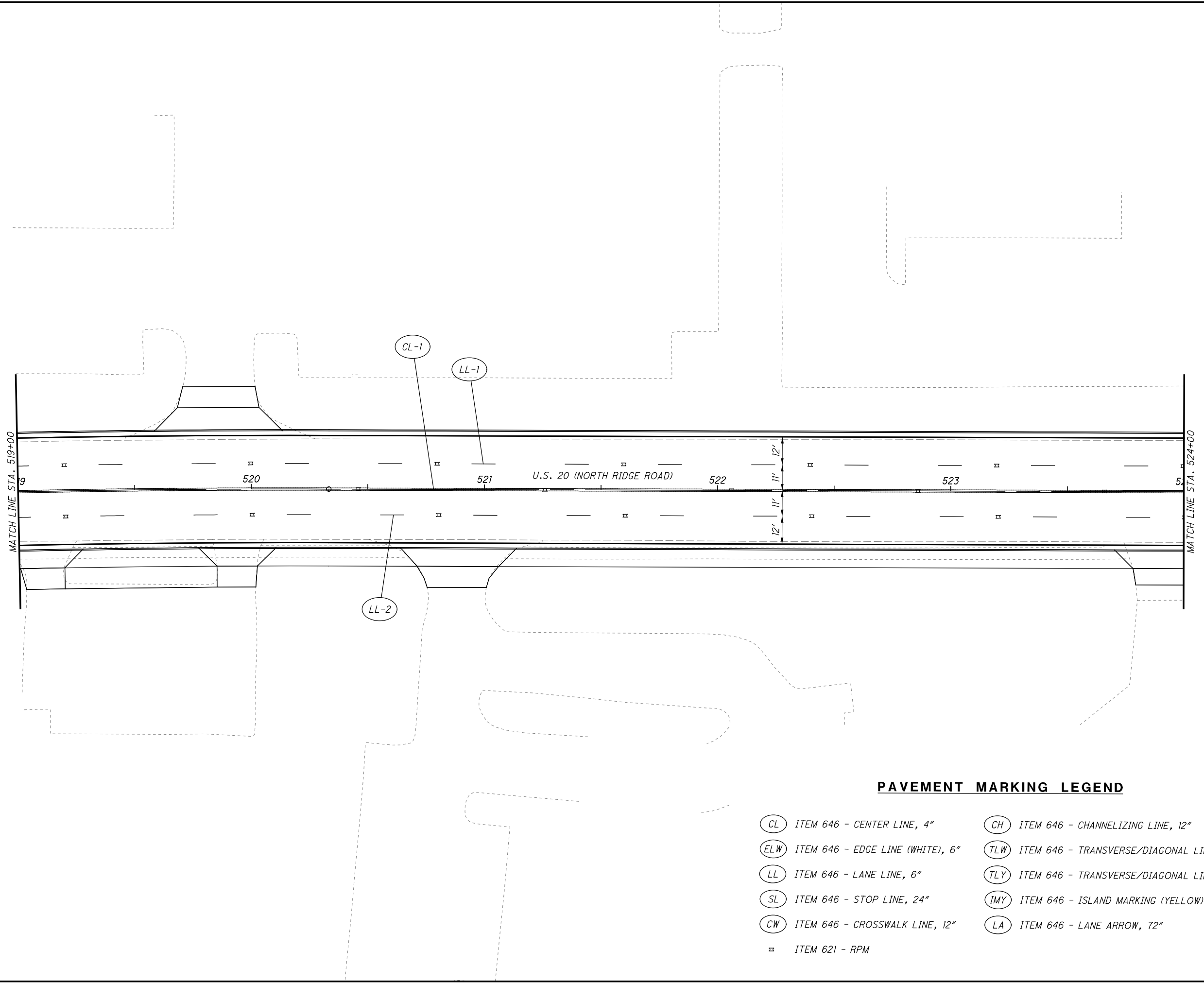


CALCULATED JDC
CHECKED EJT

**PAVEMENT MARKING PLAN
STA. 514+00 TO STA. 519+00**

**LAK-US-20-24.99
PART 2**

517
697



PAVEMENT MARKING LEGEND

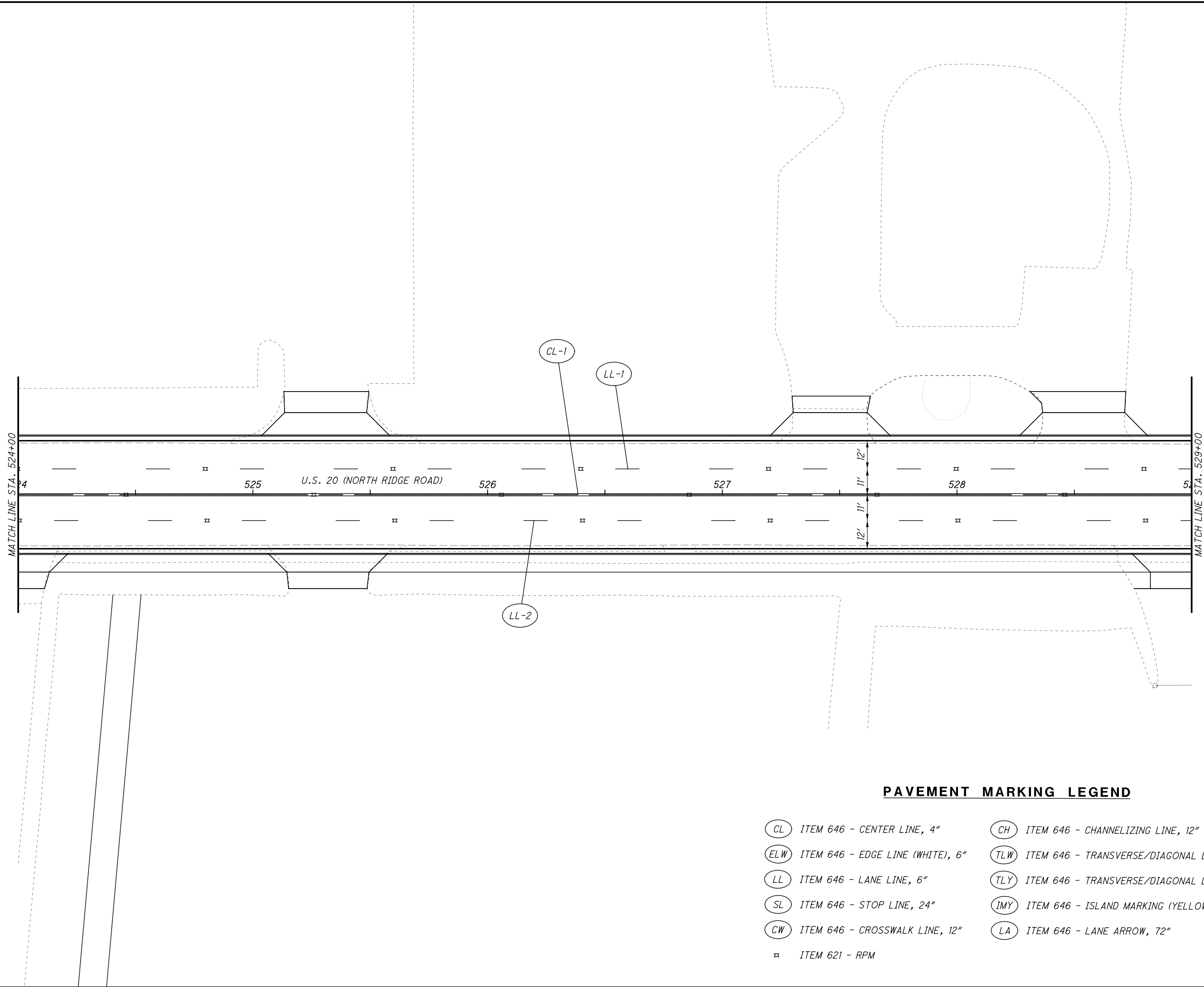
- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED
JDC
CHECKED
EJT

0 10 20 40
HORIZONTAL
SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 519+00 TO STA. 524+00**

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP284.dgn Sheet 7/15/2022 2:59:45 PM Troyer



CALCULATED JDC
CHECKED EJT

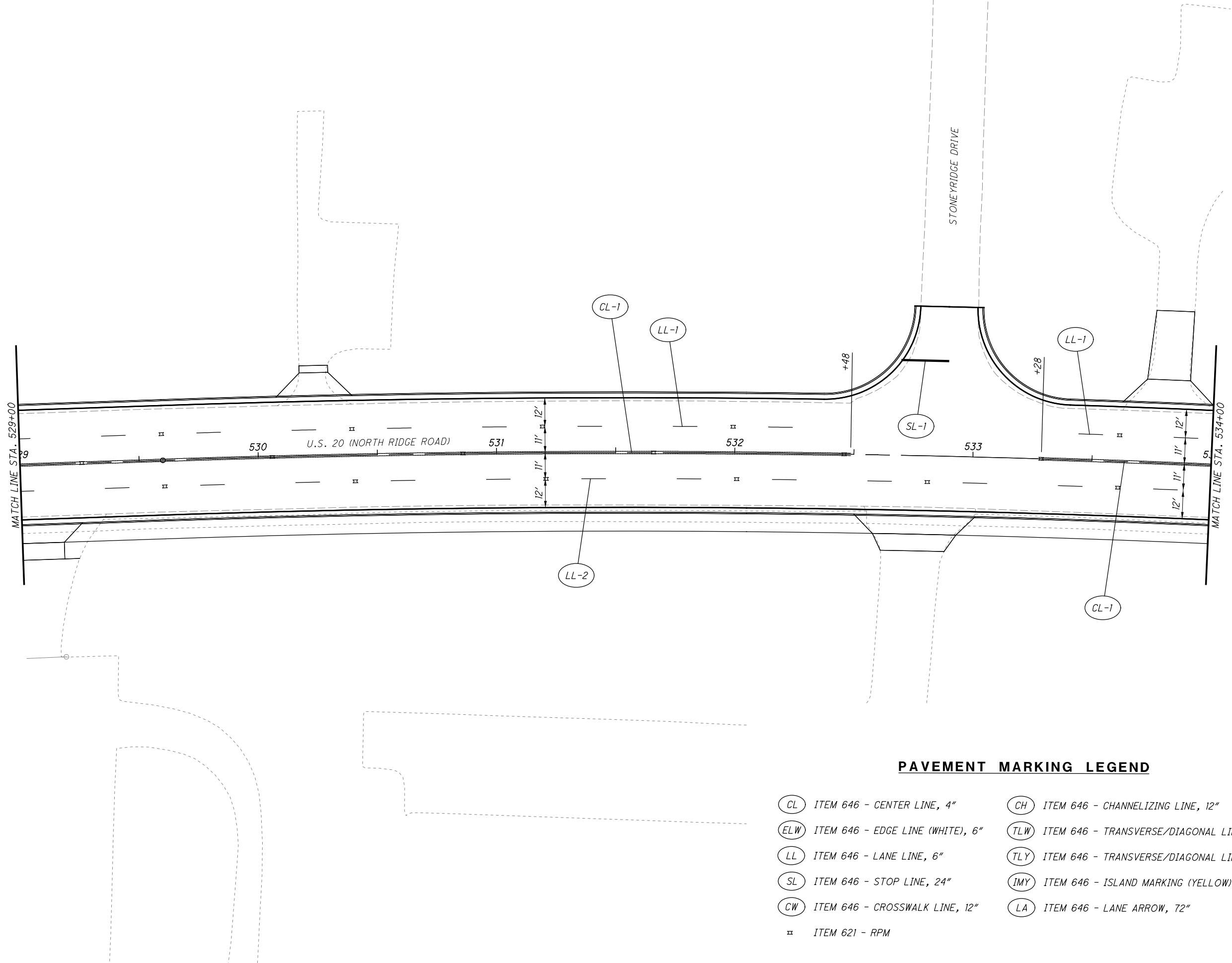
0 20 40
HORIZONTAL SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 524+00 TO STA. 529+00

PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP285.dgn Sheet 7/15/2022 2:59:46 PM Troyer



PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▬ ITEM 621 - RPM | |



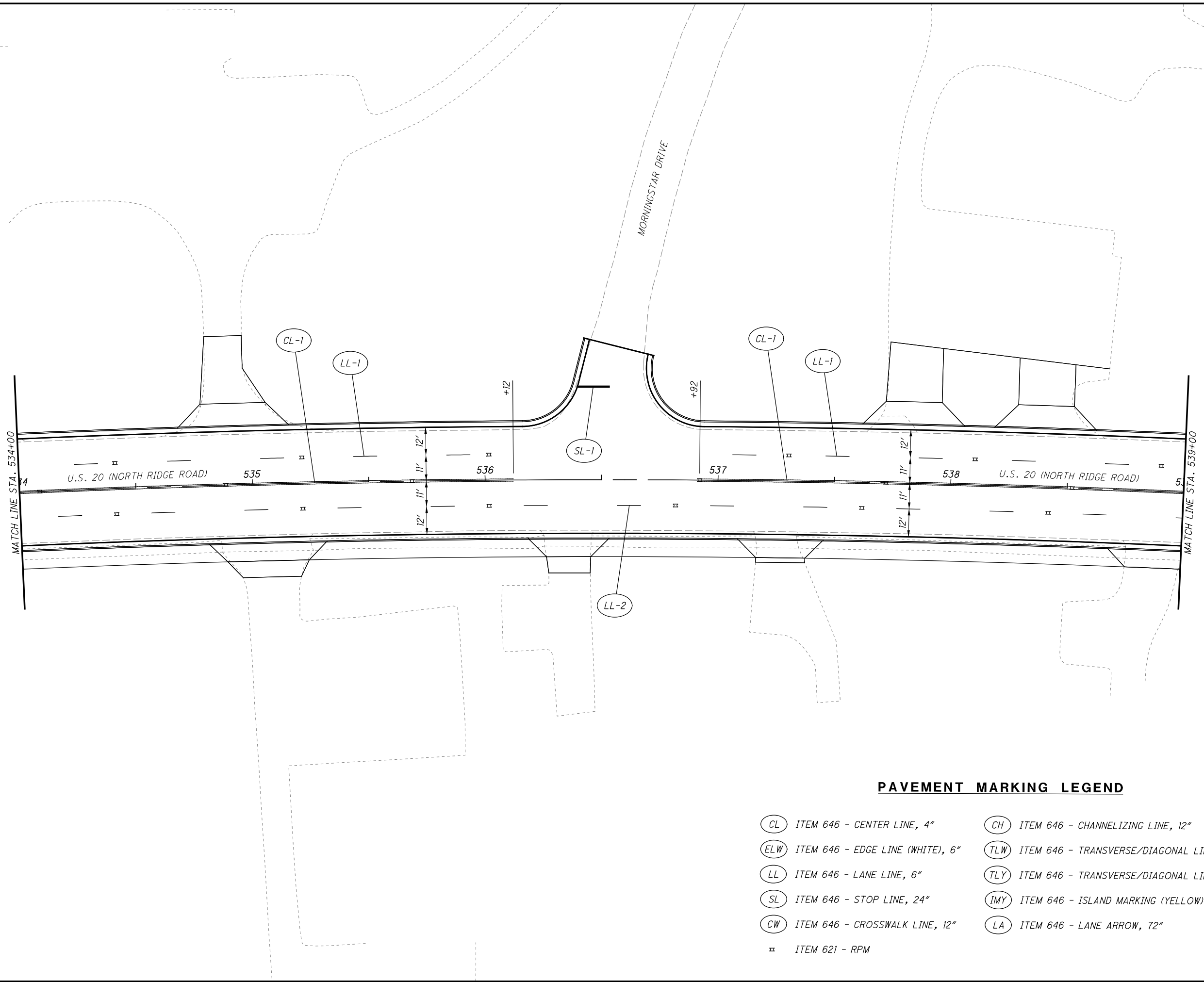
CALCULATED
JDC
CHECKED
EJT

**PAVEMENT MARKING PLAN
STA. 529+00 TO STA. 534+00**

**LAK-US-20-24.99
PART 2**

520
697

H:\2018\180743\000\Project\Data\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP286.dgn Sheet 7/15/2022 2:59:46 PM troyer



PAVEMENT MARKING LEGEND

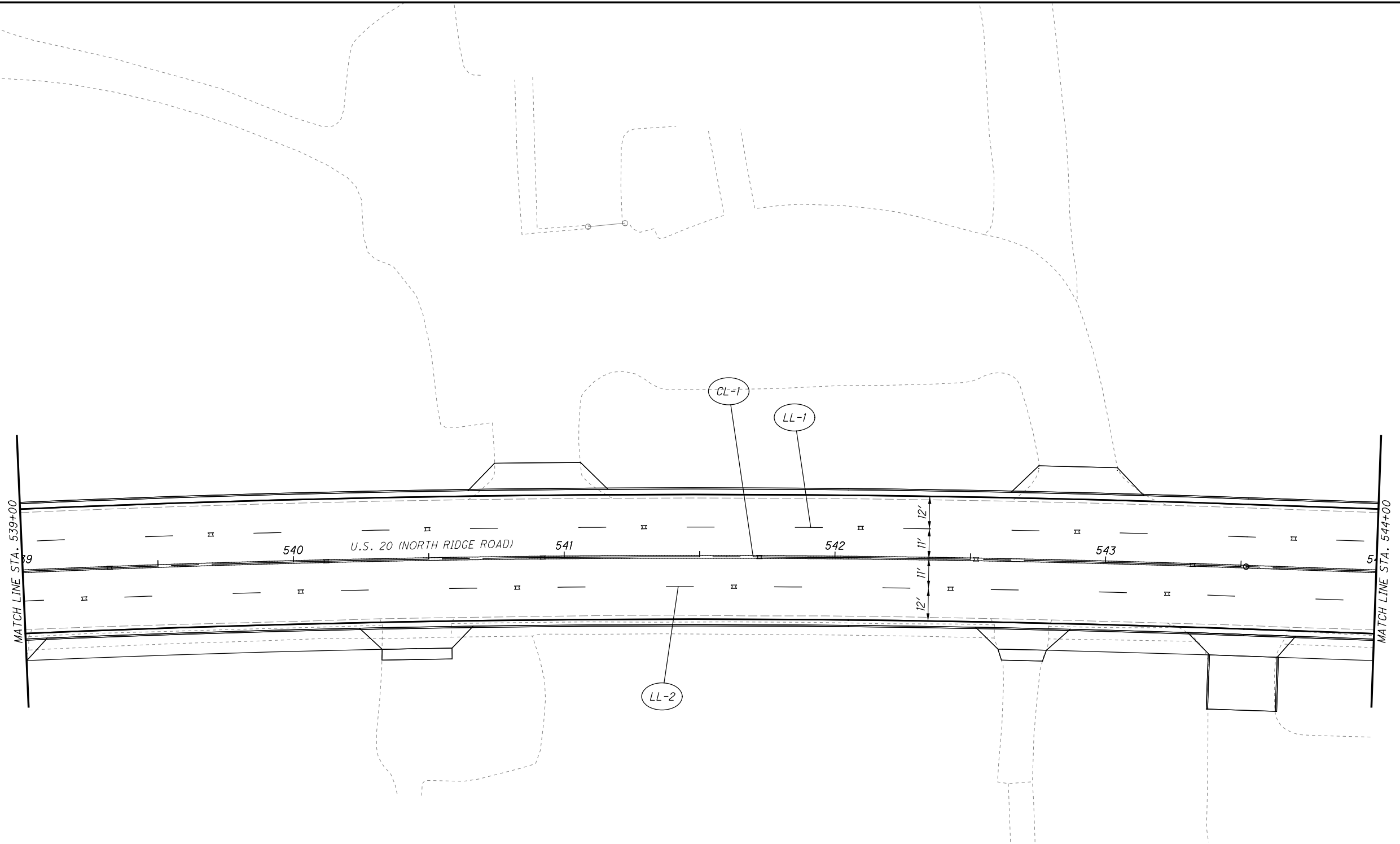
- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED JDC
CHECKED EJT

0 20 40
HORIZONTAL SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 534+00 TO STA. 539+00

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_TP287.dgn Sheet 7/15/2022 2:59:47 PM Troyer



CALCULATED JDC
CHECKED EJT

0 20 40
HORIZONTAL
SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 539+00 TO STA. 544+00

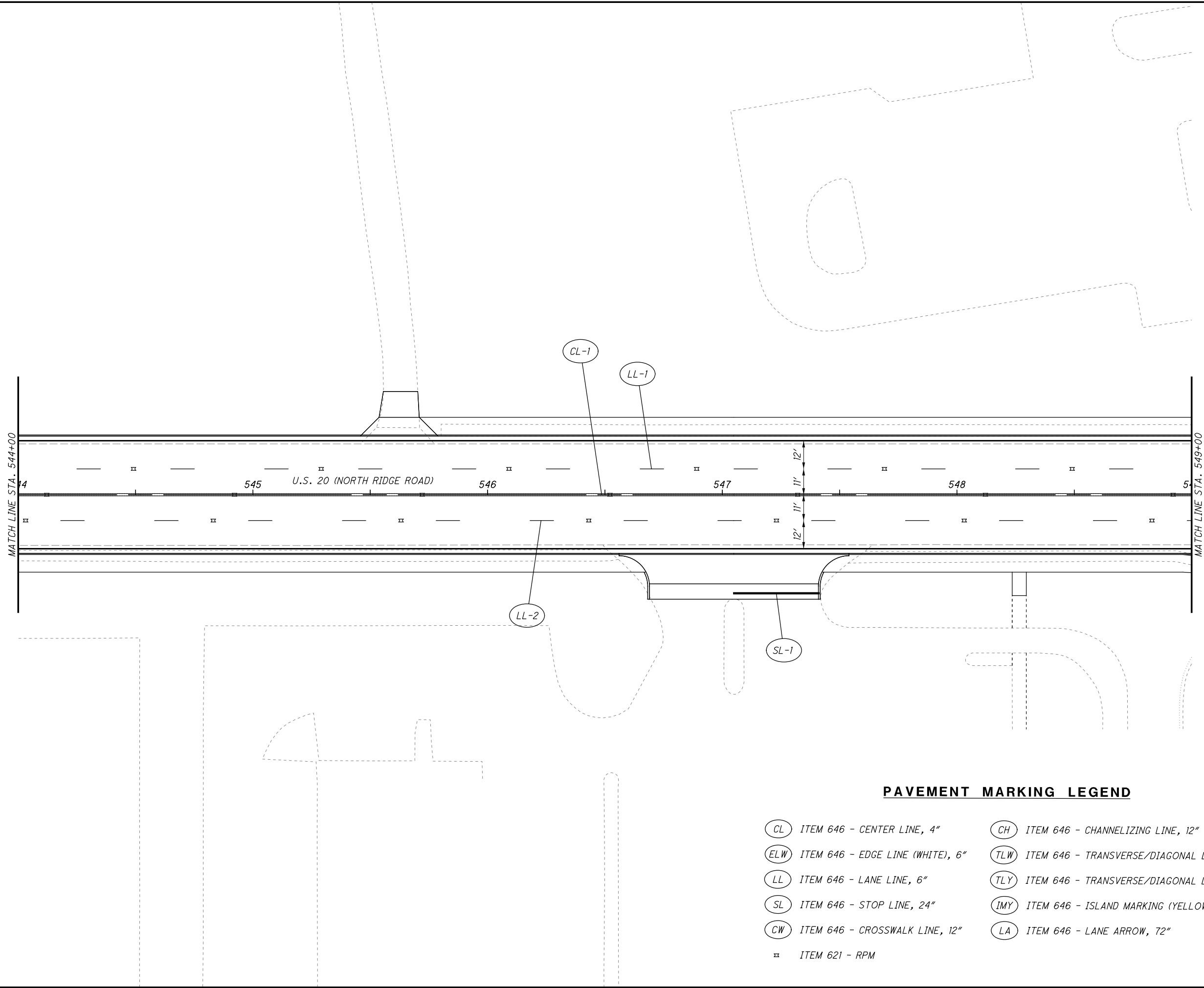
LAK-US-20-24.99
PART 2

522
697

PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP288.dgn Sheet 7/15/2022 2:59:47 PM Troyer



PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

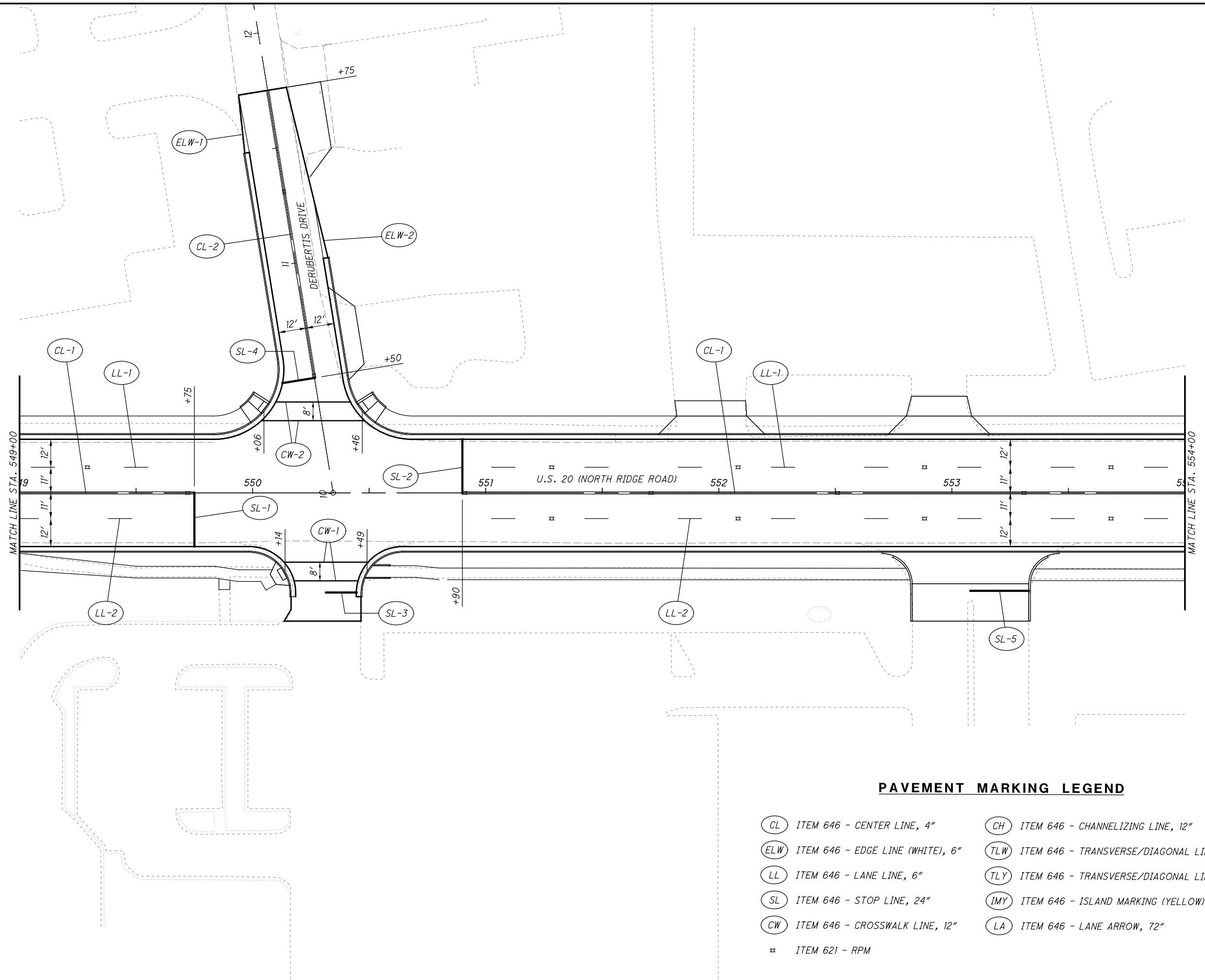
CALCULATED JDC
CHECKED EJT

HORIZONTAL SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 544+00 TO STA. 549+00**

**LAK-US-20-24.99
PART 2**

523
697



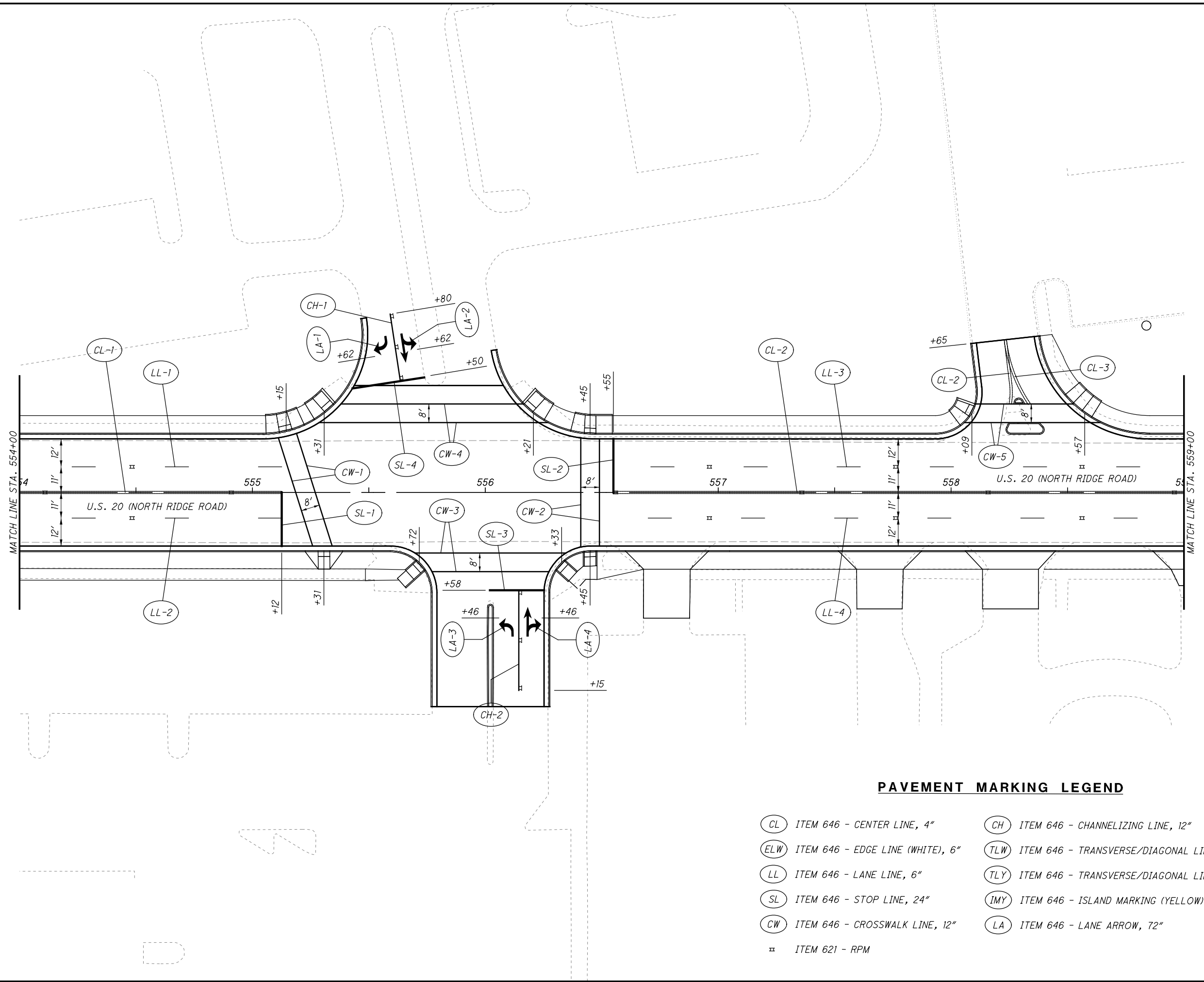
PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED JDC
CHECKED EJT

**PAVEMENT MARKING PLAN
STA. 549+00 TO STA. 554+00**

H:\2018\180743\0D01\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP290.dgn Sheet 7/15/2022 2:59:48 PM Troyer



PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED
JDC
CHECKED
EJT

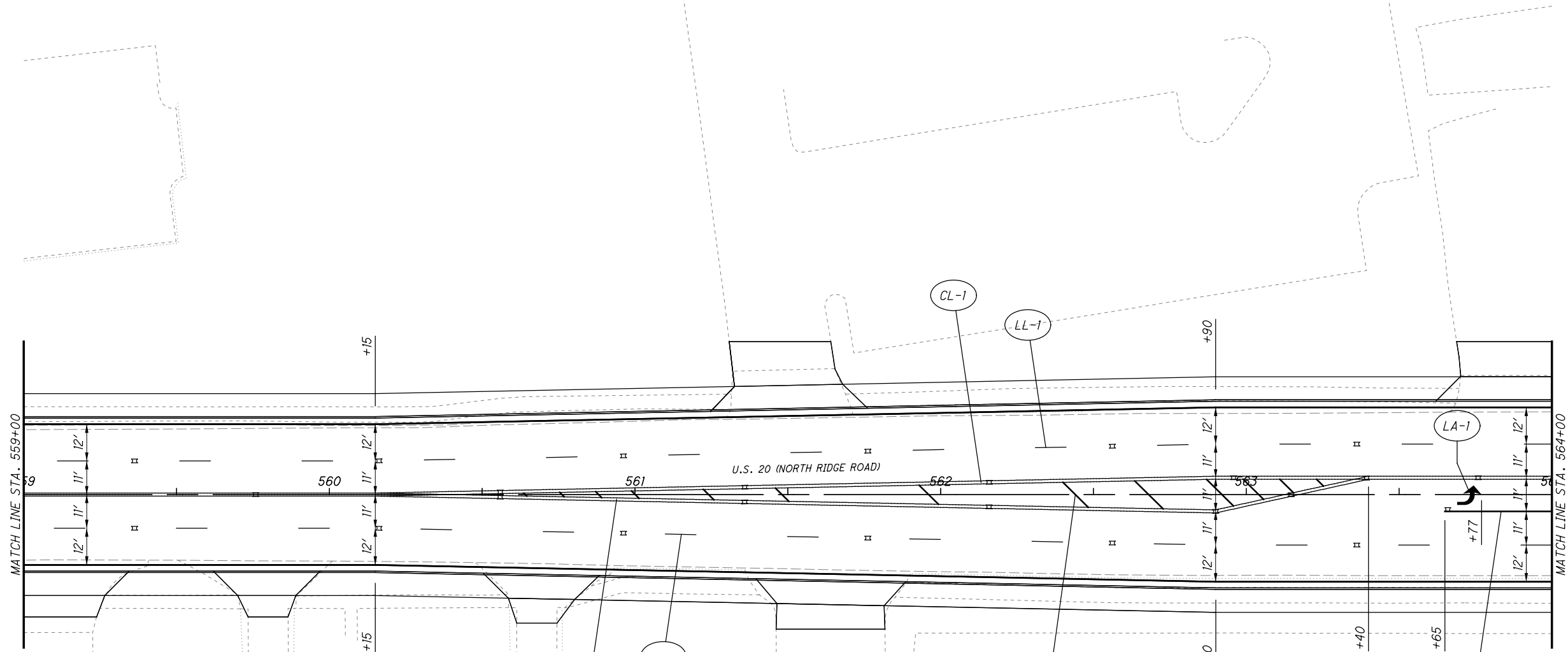
0 20 40
10
HORIZONTAL
SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 554+00 TO STA. 559+00**

**LAK-US-20-24.99
PART 2**

525
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP291.dgn Sheet 7/15/2022 2:59:49 PM troyer



PAVEMENT MARKING LEGEND

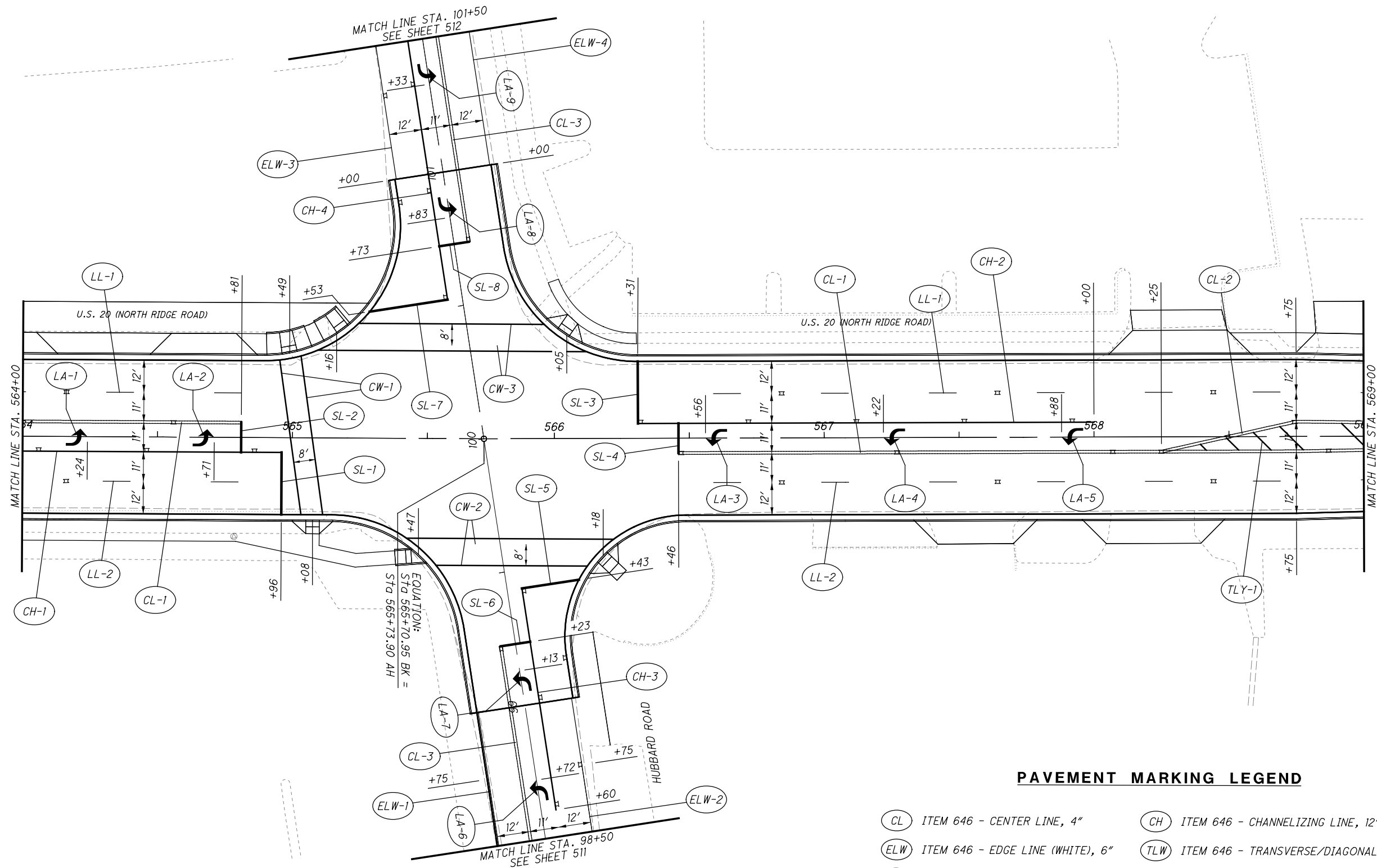
- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED
JDC
CHECKED
EJT

0 20 40
10
HORIZONTAL
SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 559+00 TO STA. 564+00**

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP292.dgn Sheet 7/15/2022 2:59:49 PM Troyer



PAVEMENT MARKING LEGEND

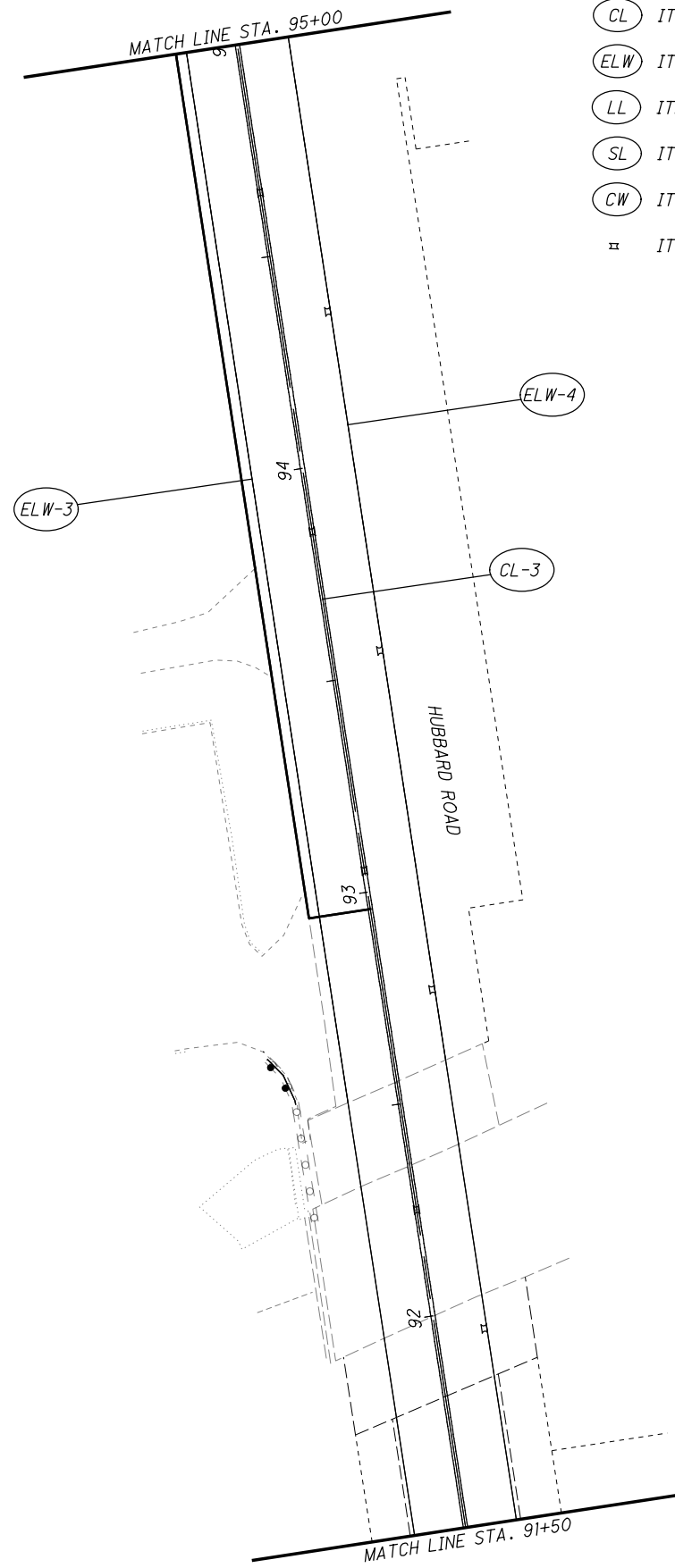
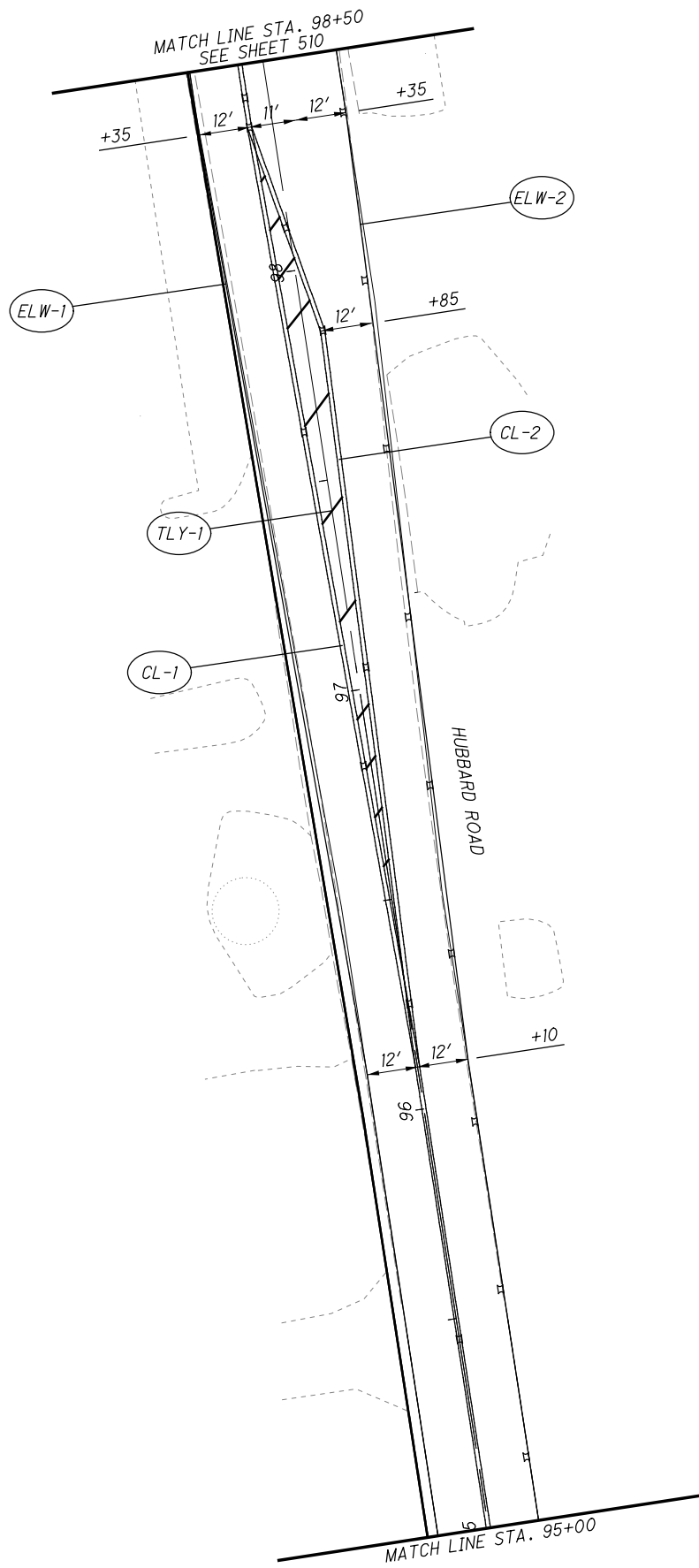
- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED JDC CHECKED EJT

10' HORIZONTAL SCALE IN FEET

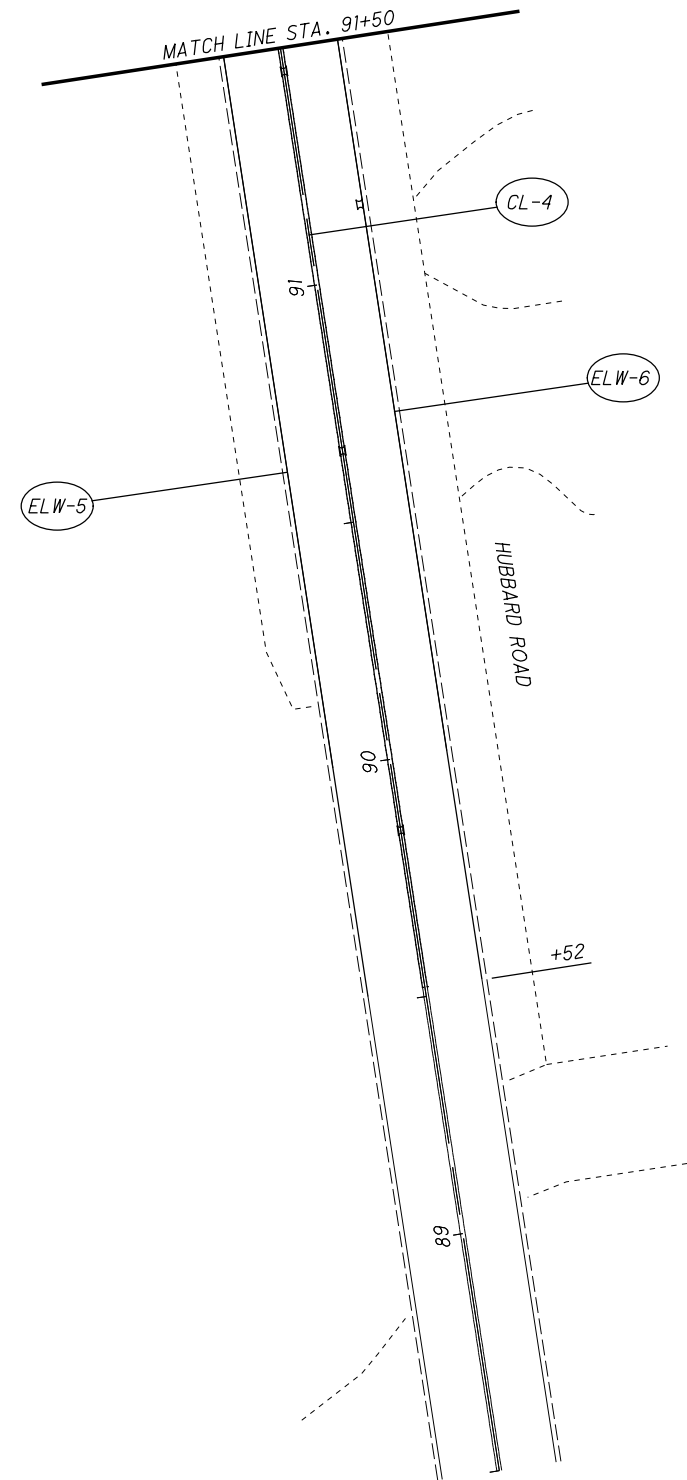
**PAVEMENT MARKING PLAN
STA. 564+00 TO STA. 569+00**

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP292A.dgn Sheet 7/15/2022 2:59:50 PM Troyer



PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |



CALCULATED JDC CHECKED EJT

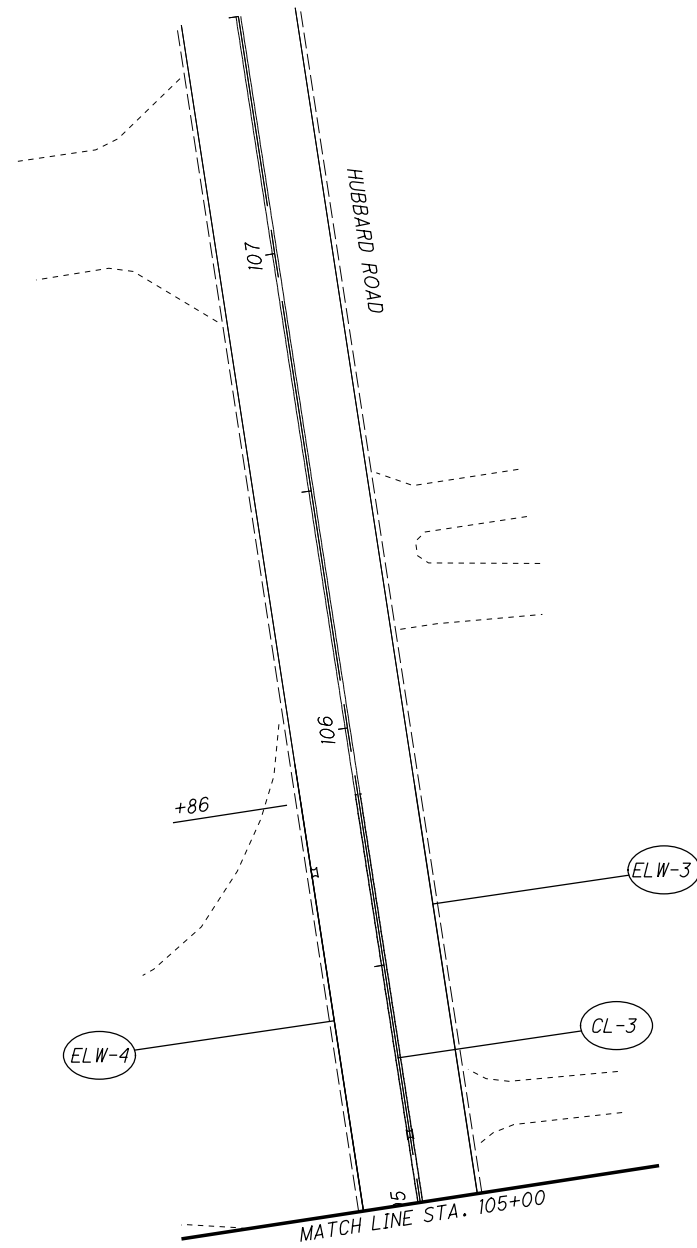
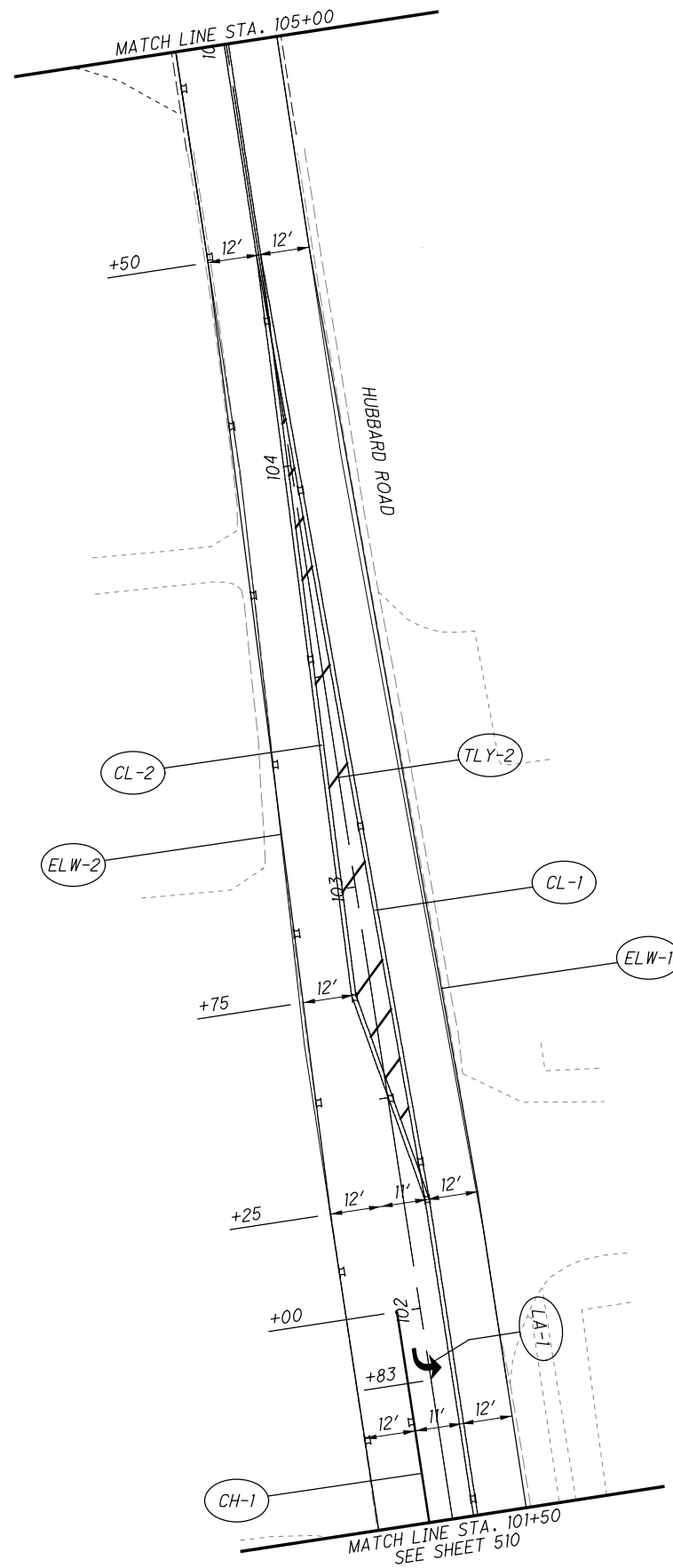
0 20 40
10 HORIZONTAL SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 89+50 TO STA. 98+50**

**LAK-US-20-24.99
PART 2**

528
697

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP292B.dgn_Sheet 7/15/2022 2:59:50 PM Troyer



PAVEMENT MARKING LEGEND

- (CL) ITEM 646 - CENTER LINE, 4"
- (ELW) ITEM 646 - EDGE LINE (WHITE), 6"
- (LL) ITEM 646 - LANE LINE, 6"
- (SL) ITEM 646 - STOP LINE, 24"
- (CW) ITEM 646 - CROSSWALK LINE, 12"
- (CH) ITEM 646 - CHANNELIZING LINE, 12"
- (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24"
- (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24"
- (IMY) ITEM 646 - ISLAND MARKING (YELLOW)
- (LA) ITEM 646 - LANE ARROW, 72"
- ▣ ITEM 621 - RPM

N

0 20 40
HORIZONTAL
SCALE IN FEET

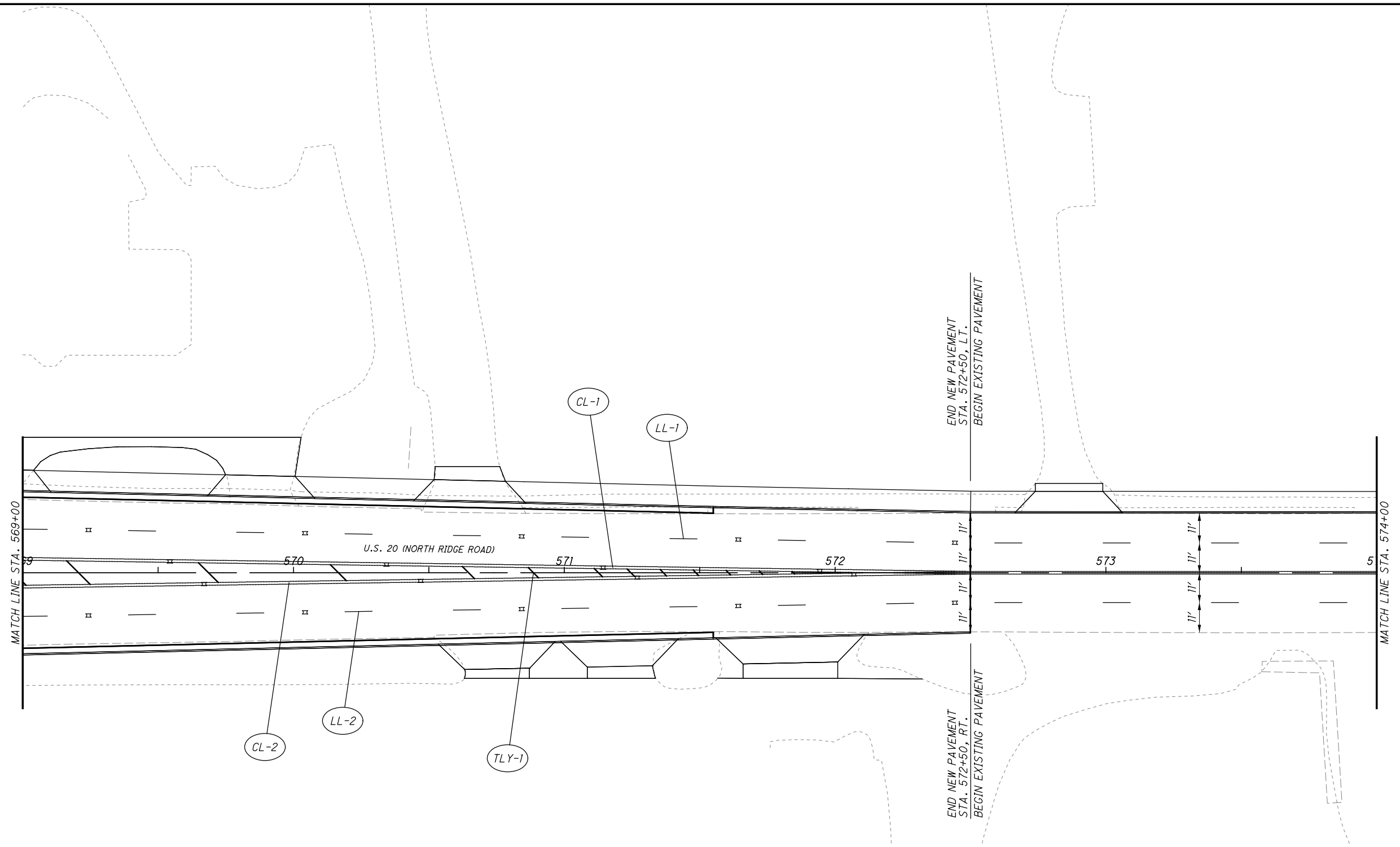
CALCULATED JDC
CHECKED EJT

**PAVEMENT MARKING PLAN
STA. 101+50 TO STA. 106+00**

**LAK-US-20-24.99
PART 2**

529
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP293.dgn Sheet 7/15/2022 2:59:51PM troyer



PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------|---------------------------------------------------------|
| (CL) ITEM 646 - CENTER LINE, 4" | (CH) ITEM 646 - CHANNELIZING LINE, 12" |
| (ELW) ITEM 646 - EDGE LINE (WHITE), 6" | (TLW) ITEM 646 - TRANSVERSE/DIAGONAL LINE (WHITE), 24" |
| (LL) ITEM 646 - LANE LINE, 6" | (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24" |
| (SL) ITEM 646 - STOP LINE, 24" | (IMY) ITEM 646 - ISLAND MARKING (YELLOW) |
| (CW) ITEM 646 - CROSSWALK LINE, 12" | (LA) ITEM 646 - LANE ARROW, 72" |
| ▣ ITEM 621 - RPM | |

CALCULATED JDC
CHECKED EJT

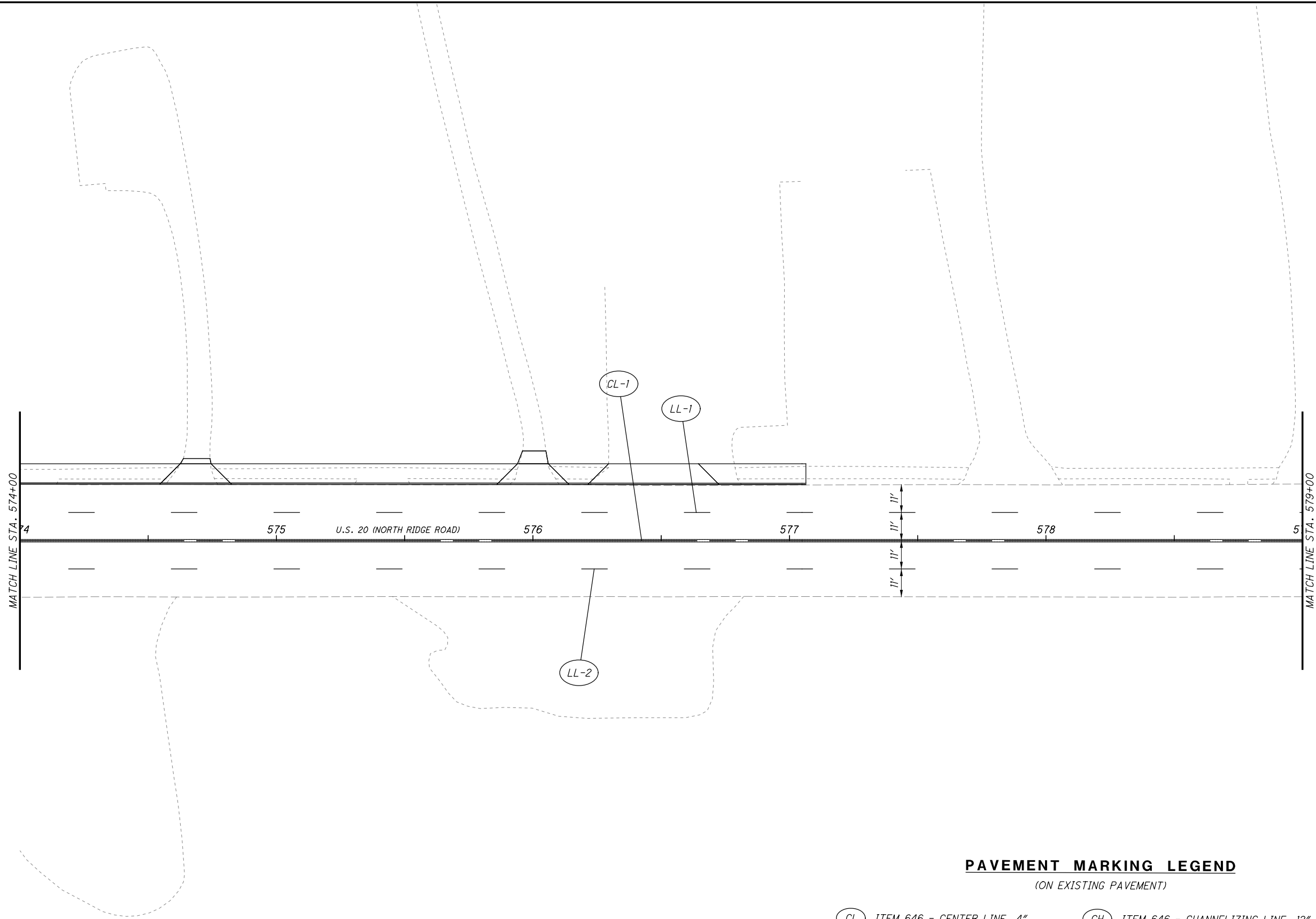
HORIZONTAL SCALE IN FEET

**PAVEMENT MARKING PLAN
STA. 569+00 TO STA. 574+00**

**LAK-US-20-24.99
PART 2**

530
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP294.dgn Sheet 7/15/2022 2:59:51PM troyer



CALCULATED JDC
CHECKED EJT

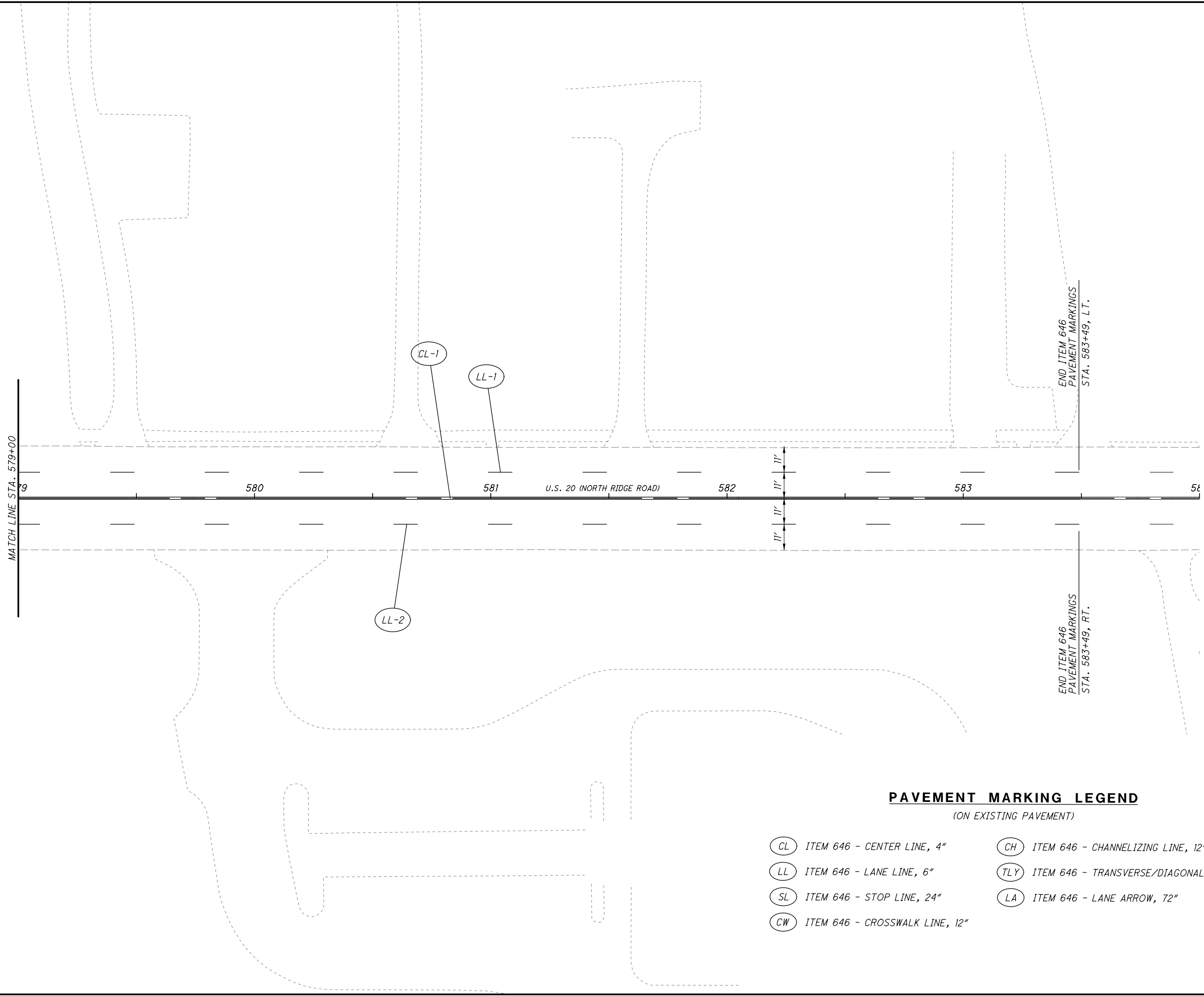
0 20 40
HORIZONTAL
SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 574+00 TO STA. 579+00

PAVEMENT MARKING LEGEND
(ON EXISTING PAVEMENT)

- (CL) ITEM 646 - CENTER LINE, 4"
- (LL) ITEM 646 - LANE LINE, 6"
- (SL) ITEM 646 - STOP LINE, 24"
- (CW) ITEM 646 - CROSSWALK LINE, 12"
- (CH) ITEM 646 - CHANNELIZING LINE, 12"
- (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24"
- (LA) ITEM 646 - LANE ARROW, 72"

H:\2018\180743\000\Project\Data\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP295.dgn Sheet 7/15/2022 2:59:52 PM Troyer



PAVEMENT MARKING LEGEND

(ON EXISTING PAVEMENT)

- (CL) ITEM 646 - CENTER LINE, 4"
- (LL) ITEM 646 - LANE LINE, 6"
- (SL) ITEM 646 - STOP LINE, 24"
- (CW) ITEM 646 - CROSSWALK LINE, 12"
- (CH) ITEM 646 - CHANNELIZING LINE, 12"
- (TLY) ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW), 24"
- (LA) ITEM 646 - LANE ARROW, 72"

CALCULATED JDC	CHECKED EJT
-------------------	----------------

0 20 40
HORIZONTAL
SCALE IN FEET

PAVEMENT MARKING PLAN
STA. 579+00 TO STA. 584+00

H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IS001.dgn Sheet 7/15/2022 2:59:52 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION		SIDE	SIGN CODE	SIGN SIZE	630	630			630	630	630	630	630	630	630	630	630								
							GROUND MOUNTED SUPPORT, NO. 2 POST	GROUND MOUNTED SUPPORT, NO. 3 POST			SIGN POST REFLECTOR		SIGN SUPPORT ASSEMBLY, POLE MOUNTED	SIGN, FLAT SHEET		REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	REMOVAL OF GROUND MOUNTED SIGN AND REERECTION	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL	REMOVAL OF POLE MOUNTED SIGN AND REERECTION							
							FT	FT			EACH		EACH	SF		EACH	EACH	EACH	EACH	EACH							
1E	540	412+57, 27' LT		LT	W3-3-36	36" X 36"										1											
1E	540	412+57, 27' LT		LT	W16-H8P-36	36" X 8"										1											
1P	540	412+57, 31' LT		LT	D3-2-78	78" X 36"																					
201E	540	410+66, 33' LT		LT	SPECIAL	12"X12"	12																				
2P	541	416+07, 30' LT		LT	W3-3-36	36" X 36"																					
2E	542	419+20, 34' LT		LT	SPECIAL-36	36" X 42"																					
3E	542	419+63, 38' RT		RT	SPECIAL-36	36" X 42"																					
3P	542	422+40, 36' RT		RT	W3-3-36	36" X 36"																					
202E	542	421+38, 38' RT		RT	SPECIAL	12"X12"																					
203E	542	422+74, 29' LT		LT	SPECIAL	12"X12"	12																				
4E	543	428+33, 41' RT		RT	W2-1-36	36" X 36"																					
4E	543	428+33, 41' RT		RT	W16-H8P-48	48" X 8"																					
5E	543	428+60, 28' LT		LT	M3-4-24	24" X 12"																					
5E	543	428+60, 28' LT		LT	M1-4-24-2	24" X 24"																					
4P	543	425+90, 38' RT		RT	D3-2-84	84" X 36"																					
6E	544	429+77, 43' RT		RT	R3-H8cg-48	48" X 30"																					
7E	544	433+08, 45' RT		RT	R3-H8cg-48	48" X 30"																					
8E	544	433+48, 36' LT		LT	R9-3-18	18" X 18"																					
9E	544	10+50, 15' LT		LT	R10-6-24	24" X 36"																					
5P	544	429+40, 45' RT		RT	R3-H8cg-48	48" X 30"																					
6P	544	430+00, 28' LT		LT	R2-1-30	30" X 36"																					
7P	544	432+00, 28' LT		LT	M3-4-24	12" X 24"																					
7P	544	432+00, 28' LT		LT	M1-4-24-2	24" X 24"																					
8P	544	433+05, 45' RT		RT	R3-H8cg-48	48" X 30"																					
9P	544	433+48, 36' LT		LT	R9-3-18	18" X 18"																					
10P	544	10+50, 15' LT		LT	R10-6-24	24" X 36"																					
10E	545	434+30, 35' LT		LT	R9-3-18	18" X 18"																					
11E	545	434+47, 29' LT		LT	R3-Hcg-48	48" X 30"																					
12E	545	434+98, 44' RT		RT	R2-1-30	30" X 36"																					
13E	545	435+93, 29' LT		LT	R3-Hcg-48	48" X 30"																					
14E	545	436+99, 44' RT		RT	M3-2-24	12" X 24"																					
14E	545	436+99, 44' RT		RT	M1-4-24-2	24" X 24"																					
11P	545	434+30, 35' LT		LT	R9-3-18	18" X 18"																					
12P	545	434+55, 29' LT		LT	R3-Hcg-48	48" X 30"																					
13P	545	435+40, 44' RT		RT	M3-2-24	12" X 24"																					
13P	545	435+40, 44' RT		RT	M1-4-24-2	24" X 24"																					
14P	545	436+45, 29' LT		LT	R3-Hcg-48	48" X 30"																					
15P	545	437+40, 40' RT		RT	R2-1-30	30" X 36"																					
15E	546	439+80, 29' LT		LT	W2-1-36	36" X 36"																					
15E	546	439+80, 29' LT		LT	W16-H8P-48	48" X 8"																					
16P	546	439+95, 31' LT		LT	D3-2-84	84" X 36"																					
17P	546	443+45, 30' LT		LT	W3-3-36	36" X 36"																					
16E	547	456+15, 29' RT		RT	R2-1-30	30" X 36"																					
17E	547	458+63, 40' LT		LT	D10-H8-12	12" X 12"																					
18E	547	458+63, 40' RT		RT	WW11-10-36	36"X36"																					
18E	547	458+99, 31' RT		RT	D10-H8-12	12" X 12"																					
18P	547	455+40, 36' RT		RT	W11-10-36	36" X 36"																					
19P	547	458+90, 36' RT		RT	W16-H8P-36	36" X 8"																					
19P	547	458+90, 36' RT		RT	W2-2L-36	36" X 36"																					
SUBTOTAL SHEET 533							24.0	335.2			6		3	186		19	17	4	3	1							

SIGNING SUBSUMMARY

LAK-US-20-24.99 PART 2

CALCULATED
ARK
CHECKED
EJT

533
697

H:\2018\80743\ProjectData\09270.LAK_20_EAST\Design\Traffic\Sheets\09270_IS002.dgn Sheet 7/15/2022 2:59:53 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION	SIDE	SIGN CODE	SIGN SIZE	630	630			630			630	630		630					
						GROUND MOUNTED SUPPORT, NO. 2 POST	GROUND MOUNTED SUPPORT, NO. 3 POST			SIGN POST REFLECTOR			SIGN, FLAT SHEET	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL		REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL				
						FT	FT			EACH			SF	EACH	EACH						
19E	550	461+12, 33' RT	RT	W16-H8P-36	36" X 8"									1	2						
19E	550	461+12, 33' RT	RT	W2-2L-48	48" X 48"									1							
20E	550	463+15, 27' RT	RT	W1-7-48	48" X 24"									1	2						
21E	550	463+24, 41' LT	LT	D3-1-36	36" X 9"													1			
22E	550	10+44, 18' LT	LT	R1-1-30	30" X 30"									1	1						
20P	550	459+69, 29' LT	LT	D10-H8-12	12" X 12"	12							1								
21P	550	459+69, 35' RT	RT	D10-H8-12	12" X 12"	12							1								
22P	550	461+40, 29' LT	LT	R2-1-30	30" X 36"		13.5						7.5								
23P	550	463+16, 35' RT	RT	W1-7-48	48" X 24"					2			8								
24P	550	463+17, 38' LT	LT	D3-1-66	50" X 18"								6.25								
24P	550	463+17, 38' LT	LT	D3-1-66	50" X 18"								6.25								
25P	550	10+00, 18' LT	LT	R1-1-36	36" X 36"		13.5			1			9								
23E	551	464+45, 32' RT	RT	M3-2-24	24" X 12"									1	1						
23E	551	464+45, 32' RT	RT	M1-4-24-2	24" X 24"									1							
24E	551	466+04, 34' RT	RT	W2-2R-48	48" X 48"									1	2						
24E	551	466+04, 34' RT	RT	W16-H8P-36	36" X 8"									1							
25E	551	466+07, 29' LT	LT	W2-2R-48	48" X 48"									1	2						
25E	551	466+07, 29' LT	LT	W16-H8P-36	36" X 8"									1							
26P	551	465+40, 35' RT	RT	R2-1-30	30" X 36"		13.5						7.5								
27P	551	467+90, 30' LT	LT	W2-2R-36	36" X 36"		14.25, 14.25			1			9								
27P	551	467+90, 30' LT	LT	W16-H8P-36	36" X 8"					1			2								
26E	553	478+30, 33' LT	LT	R2-1-30	30" X 36"									1	1						
28P	554	481+30, 36' RT	RT	W3-3-36	36" X 36"		13.5			1			9								
27E	555	486+52, 39' RT	RT	W3-3-36	36" X 36"									1	1						
27E	555	486+52, 39' RT	RT	SPECIAL-24	24" X 36"									1							
28E	555	488+64, 38' RT	RT	W2-2L-36	36" X 36"									1	1						
28E	555	488+64, 38' RT	RT	W16-H8P-36	36" X 8"									1							
29P	555	485+05, 38' RT	RT	D3-2-78	78" X 36"		13.5, 13.5						19.5								
30P	555	488+80, 39' RT	RT	R3-H8cg-48	48" X 30"		13, 13						10								
29E	556	489+16, 40' RT	RT	R3-H8cg-48	48" X 30"									1	2						
30E	556	491+64, 36' LT	LT	M3-4-24	24" X 12"									1	1						
30E	556	491+64, 36' LT	LT	M1-4-24-2	24" X 24"									1							
31E	556	492+26, 46' LT	LT	R1-1-30	30" X 30"									1	1						
31E	556	492+26, 46' LT	LT	R5-1-30	30" X 30"									1							
32E	556	492+30, 46' LT	LT	R3-5R-30	30" X 36"									1	1						
33E	556	493+89, 43' RT	RT	R10-H7a-24	24" X 30"									1	1						
31P	556	491+15, 35' LT	LT	R2-1-30	30" X 36"		13.5						7.5								
32P	556	492+22, 41' LT	LT	R5-1-36	36" X 36"		13.5						9								
33P	556	492+26, 44' LT	LT	R1-1-36	36" X 36"		16, 16			2			9								
33P	556	492+26, 44' LT	LT	R3-2-30	30" X 30"								6.25								
34P	556	493+15, 36' LT	LT	M3-4-24	12" X 24"		14.6						2								
34P	556	493+15, 36' LT	LT	M1-4-24-2	24" X 24"								4								
35P	556	493+75, 44' RT	RT	R3-H8ca-48	48" X 30"		13.5, 13.5						10								
36P	556	493+85, 34' RT	RT	R10-H7a-24	24" X 30"		13						5								
SUBTOTAL SHEET 534						24.0	299.1			8			149	22	19						

SIGNING SUBSUMMARY	CALCULATED ARK CHECKED EJT
LAK-US-20-24.99 PART 2	534 697

H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IS003.dgn Sheet 7/15/2022 2:59:54 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION	SIDE	SIGN CODE	SIGN SIZE	630	630			630	630	630	630	630	630	630	630	630	630	630		
						GROUND MOUNTED SUPPORT, NO. 2 POST	GROUND MOUNTED SUPPORT, NO. 3 POST			SIGN POST REFLECTOR		SIGN SUPPORT ASSEMBLY, POLE MOUNTED	SIGN, FLAT SHEET	SIGN, DOUBLE FACED, STREET NAME	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL		REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL				
						FT	FT			EACH		EACH	SF	EACH	EACH	EACH		EACH				
34E	557	494+33, 45' RT	RT	R3-H8cg-48	48" X 30"										1	2						
35E	557	494+37' 41' LT	LT	R9-2-12	12" X 18"															1		
35E	557	494+37' 41' LT	LT	R9-3bP-18	18" X 12"															1		
36E	557	494+37, 41' LT	LT	D3-1-36	36" X 12"															1		
37E	557	494+38, 46' RT	RT	R9-2-12	12" X 18"															1		
37E	557	494+38, 46' RT	RT	R9-3bP-18	18" X 12"															1		
38E	557	496+48, 44' RT	RT	M3-2-24	24" X 12"															1		
38E	557	496+48, 44' RT	RT	M1-4-24-2	24" X 24"															1		
39E	557	498+23, 35' LT	LT	R3-H8bk-36	36" X 30"										1	2						
40E	557	11+13, 22' LT	LT	R10-SPECIAL-24	24" X 36"										1	1						
41E	557	11+47, 30' LT	LT	R3-H8bd-30	30" X 30"															1		
37P	557	494+37, 41' LT	LT	R9-2-12	12" X 18"							1	1.5									
37P	557	494+37, 41' LT	LT	R9-3bP-18	18" X 12"							1	1.5									
38P	557	494+37, 41' LT	LT	D3-1-46	46" X 18"							1		1								
39P	557	494+38, 46' RT	RT	R9-2-12	12" X 18"								1.5									
39P	557	494+38, 46' RT	RT	R9-3bP-18	18" X 12"								1.5									
40P	557	495+85, 35' LT	LT	R3-H8bk-36	36" X 30"		13						7.5									
41P	557	496+95, 34' RT	RT	M3-2-24	12" X 24"		13.58						2									
41P	557	496+95, 34' RT	RT	M1-4-24-2	24" X 24"								4									
42P	557	497+50, 35' LT	LT	R3-H8bk-36	36" X 30"		13						7.5									
43P	557	498+95, 32' RT	RT	R2-1-30	30" X 36"		13.5						7.5									
44P	557	10+65, 22' LT	LT	R3-H8bd-30	30" X 30"		13						6.25									
45P	557	11+15, 22' LT	LT	R10-H6bL-24	24" X 36"		13.5						6									
42E	558	501+22, 30' LT	LT	W2-2R-36	36" X 36"										1	1						
43E	558	502+86, 29' LT	LT	W3-3-36	36" X 36"										1	1						
44E	558	503+54, 31' RT	RT	R3-2-30	30" X 30"										1	1						
46P	558	501+00, 31' LT	LT	D3-2-78	78" X 36"		13.5, 13.5						19.5									
47P	558	503+50, 29' LT	LT	W3-3-36	36" X 36"		13.5			1			9									
48P	558	503+55, 36' RT	RT	R3-2-30	30" X 30"		13						6.25									
45E	559	504+16, 40' LT	LT	R1-1-30	30" X 30"										1	1						
46E	559	504+35, 160' LT	LT	R2-2-24	24" X 24"										1	1						
47E	559	504+15, 38' LT	LT	R3-2-30	30" X 30"										1	1						
48E	559	505+26, 39' LT	LT	R3-H8cb-48	48" X 30"										1	2						
48E	559	505+26, 39' LT	LT	R5-2a-24	24" X 24"										1							
49E	559	508+47, 32' LT	LT	R3-H8cb-48	48" X 30"										1	2						
50E	559	508+47, 32' RT	RT	R2-H5D-24	24" X 30"										1	1						
49P	559	504+22, 48' LT	LT	R1-1-36	36" X 36"		13.5			1			9									
50P	559	504+35, 155' LT	LT	R3-2-24	24" X 24"	13							4									
51P	559	504+50, 42' LT	LT	R3-2-36	36" X 36"		13.5						9									
52P	559	505+10, 42' LT	LT	R3-H8cb-48	48" X 30"		13, 13						10									
52P	559	505+10, 42' LT	LT	R5-2-30	30" X 30"								6.25									
53P	559	507+50, 36' RT	RT	W3-5-36	36" X 36"		13.5			1			9									
54P	559	507+60, 42' LT	LT	R3-H8cb-48	48" X 30"		13, 13						10									
54P	559	507+60, 42' LT	LT	R5-2-30	30" X 30"								6.25									
51E	560	509+94, 32' RT	RT	W3-3-36	36" X 36"										1	1						
52E	560	512+03, 27' LT	LT	R2-1-30	30" X 36"										1	1						
53E	560	512+10, 32' RT	RT	D10-H8-12	12" X 12"										1	1						
53E	560	512+10, 32' RT	RT	R2-1-30	30" X 36"										1							
54E	560	512+17, 30' LT	LT	D10-H8-12	12" X 12"										1	1						
SUBTOTAL SHEET 535						13.0	225.6			3		3	145	1	18	20			8			

SIGNING SUBSUMMARY	LAK-US-20-24.99 PART 2	<table border="1" style="border-collapse: collapse; width: 100%;"> <tr> <td style="text-align: center;">CALCULATED</td> <td style="text-align: center;">ARK</td> </tr> <tr> <td style="text-align: center;">CHECKED</td> <td style="text-align: center;">EJT</td> </tr> </table>	CALCULATED	ARK	CHECKED	EJT
CALCULATED	ARK					
CHECKED	EJT					

H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IS004.dgn Sheet 7/15/2022 2:59:55 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION	SIDE	SIGN CODE	SIGN SIZE	630	630			630	630	630	630		630	630	630	630	630								
						GROUND MOUNTED SUPPORT, NO. 2 POST	GROUND MOUNTED SUPPORT, NO. 3 POST			SIGN POST REFLECTOR	SIGN ATTACHMENT ASSEMBLY, MAST ARM	SIGN SUPPORT ASSEMBLY, POLE MOUNTED	SIGN, FLAT SHEET		REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	REMOVAL OF GROUND MOUNTED SIGN AND REERECTION	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL	REMOVAL OF POLE MOUNTED SIGN AND REERECTION								
						FT	FT			EACH	EACH	EACH	SF		EACH	EACH	EACH	EACH	EACH								
55P	560	509+50, 36' RT	RT	W3-3-36	36" X 36"		13.5			1			9														
56P	560	512+00, 29' LT	LT	R2-1-30	30" X 36"		13.5						7.5														
57P	560	512+00, 36' RT	RT	R2-1-30	30" X 36"		13.5						7.5														
58P	560	512+50, 38' RT	RT	D3-2-78	78" X 36"		13.5, 13.5						19.5														
59P	560	512+74, 29' LT	LT	D10-H8-12	12" X 12"	12							1														
60P	560	512+74, 35' RT	RT	D10-H8-12	12" X 12"	12							1														
55E	561	517+42, 39' RT	RT	D3-1-36	36" X 9"																						
56E	561	9+41, 38' RT	RT	I-8-24	24" X 30"										1	1											
56E	561	9+41, 38' RT	RT	I-H12-24	24" X 6"										1												
56E	561	9+41, 38' RT	RT	I-8-24	24" X 30"										1												
56E	561	9+41, 38' RT	RT	I-H12-24	24" X 6"										1												
57E	561	9+59, 26' RT	RT	R10-6-24	24" X 36"										1	1											
61P	561	516+80, LT	LT	D3-1-58	58" X 24"						2		9.67														
62P	561	517+50, RT	RT	D3-1-58	58" X 24"						2		9.67														
63P	561	9+43, 38' RT	RT	I-8-24	24" X 24"		13.75, 13.75						4														
63P	561	9+43, 38' RT	RT	M6-1-21	21" X 15"								2.19														
63P	561	9+43, 38' RT	RT	I-8-24	24" X 24"								4														
63P	561	9+43, 38' RT	RT	M6-1-21	21" X 15"								2.19														
64P	561	9+60, 23' RT	RT	R10-H6bL-24	24" X 36"		13.5						6														
204E	561	515+92, 40' RT	RT	SPECIAL	12"X12"	12						1													1		
65P	562	521+50, 31' LT	LT	D3-2-78	78" X 36"		13.5, 13.5						19.5														
205E	562	520+22, 39' RT	RT	SPECIAL	12"X12"							1														1	
66P	563	524+50, 30' LT	LT	W3-3-36	36" X 36"		13.5			1			9														
58E	564	529+35, 25' LT	LT	R2-1-30	30" X 36"										1	1											
59E	564	529+61, 35' RT	RT	R2-1-30	30" X 36"										1	1											
60E	564	532+63, 36' LT	LT	D3-1-48	48" X 9"										1	1											
61E	564	532+71, 43' LT	LT	R1-1-30	30" X 30"										1	1											
67P	564	531+00, 29' LT	LT	R2-1-30	30" X 36"		13.5						7.5														
68P	564	531+00, 36' RT	RT	R2-1-30	30" X 36"		13.5						7.5														
69P	564	534+54, 36' LT	LT	D3-1-54	54" X 12"		11.5, 11.5						4.5														
69P	564	534+54, 36' LT	LT	D3-1-54	54" X 12"								4.5														
70P	564	532+65, 45' LT	LT	R1-1-36	36" X 36"		13.5			1			9														
206E	564	532+50, 33' RT	RT	SPECIAL	12"X12"	12																			1		
207E	564	532+51, 28' LT	LT	SPECIAL	12"X12"	12																			1		
62E	565	535+96, 32' RT	RT	R2-1-30	30" X 36"										1	1											
63E	565	536+34, 41' LT	LT	R1-1-30	30" X 30"										1	1											
64E	565	536+81, 38' LT	LT	D3-1-48	48" X 9"										1	1											
71P	565	536+28, 36' LT	LT	D3-1-54	54" X 12"		11.5, 11.5						4.5														
71P	565	536+28, 36' LT	LT	D3-1-54	54" X 12"								4.5														
72P	565	536+35, 45' LT	LT	R1-1-36	36" X 36"		13.5			1			9														
73P	565	538+40, 36' RT	RT	W3-5-36	36" X 36"		13.5			1			9														
65E	566	539+33, 32' RT	RT	W3-5-36	36" X 36"										1	1											
66E	566	541+85, 31' LT	LT	W11-2-36	36" X 36"										1	1											
67E	566	542+01, 33' RT	RT	R2-1-30	30" X 36"										1	1											
68E	566	543+36, 48' RT	RT	SPECIAL-24	24" X 12"										1	1											
68E	566	543+36, 48' RT	RT	R5-1-30	30" X 30"										1												
69E	566	543+47, 29' LT	LT	R2-1-30	30" X 36"										1	1											
70E	566	543+68, 45' RT	RT	SPECIAL-24	24" X 12"										1	1											
70E	566	543+68, 45' RT	RT	R5-1-30	30" X 30"										1												
SUBTOTAL SHEET 536						59.0	262.5			5	4	2	172		20	15	2	1	2								

SIGNING SUBSUMMARY

LAK-US-20-24.99 PART 2

CALCULATED
ARK
CHECKED
EJT

536
697

H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IS006.dgn Sheet 7/15/2022 2:59:57 PM Troyer

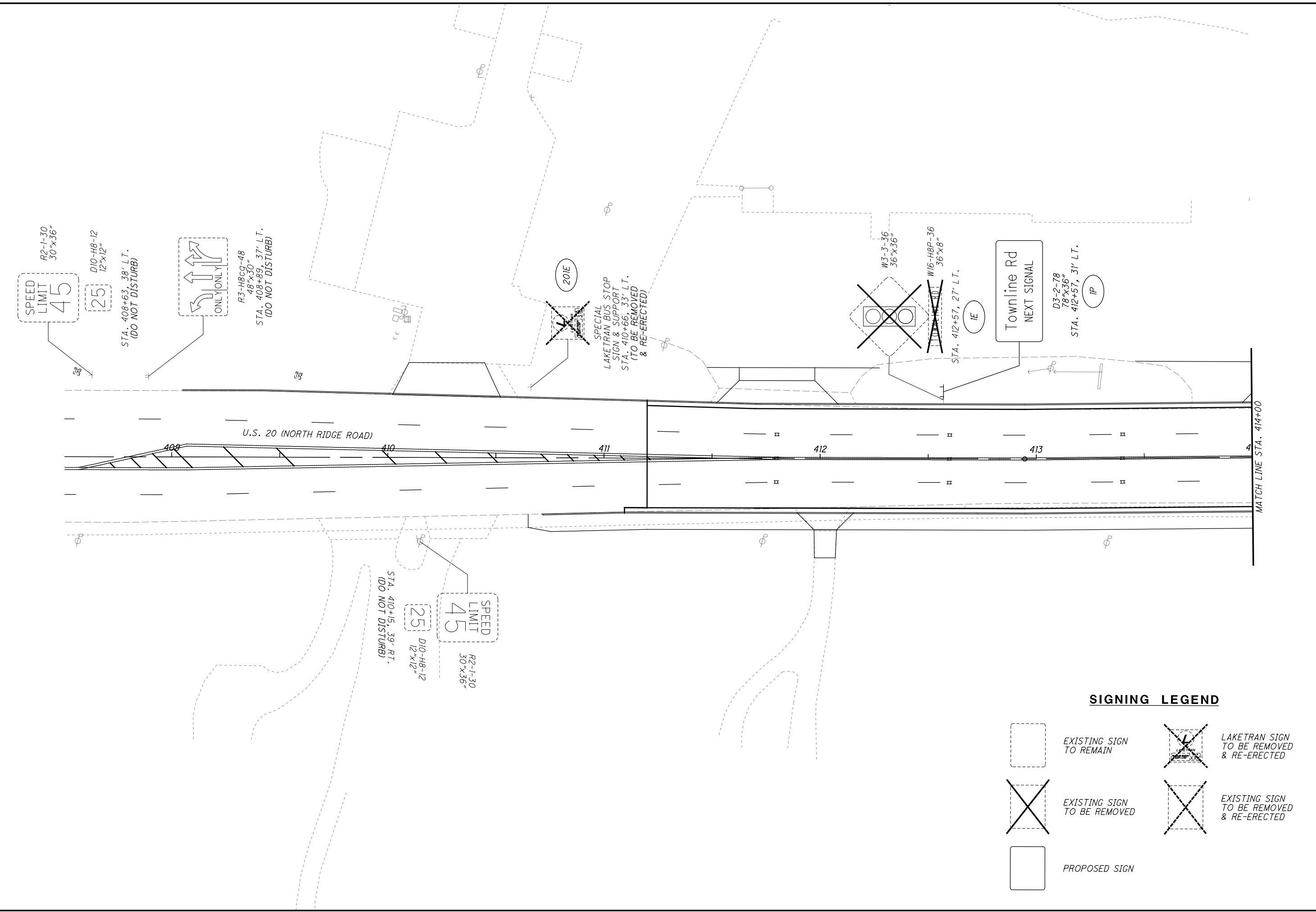
REF NO.	SHEET NO.	STATION TO STATION	SIDE	SIGN CODE	SIGN SIZE	630	630			630	630	630	630		630	630									
						GROUND MOUNTED SUPPORT, NO. 2 POST	GROUND MOUNTED SUPPORT, NO. 3 POST			SIGN POST REFLECTOR	SIGN ATTACHMENT ASSEMBLY, MAST ARM	SIGN SUPPORT ASSEMBLY, POLE MOUNTED	SIGN, FLAT SHEET		REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL		REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL							
						FT	FT			EACH	EACH	EACH	SF		EACH	EACH									
90E	571	566+41, 37' RT	RT	D10-H8-12	12" X 12"																				
91E	571	566+51, 33' RT	RT	R2-1-30	30" X 36"																				
92E	571	568+10, 40' LT	LT	R1-1-30	30" X 30"										1	1									
93E	571	568+60, 41' LT	LT	R10-H7a-24	24" X 30"										1	1									
94E	571	568+71, 36' RT	RT	M3-2-24	24" X 12"										1	1									
94E	571	568+71, 36' RT	RT	M1-4-24-2	24" X 22"										1										
95E	571	98+60, 27' RT	RT	M4-6-12	24" X 12"										1	1									
95E	571	98+60, 27' RT	RT	M1-5-30-3	30" X 24"										1										
96E	571	98+91, 25' LT	LT	M3-2-24	24" X 12"										1	1									
96E	571	98+91, 25' LT	LT	M1-5-30-3	30" X 24"										1										
97E	571	99+41, 36' RT	RT	R10-H6bL-24	24" X 36"										1	1									
98P	571	564+25, 41' LT	LT	M3-4-24	12" X 24"		14.58						2												
98P	571	564+25, 41' LT	LT	M1-4-24-2	24" X 24"								4												
99P	571	564+81, 42' RT	RT	R3-H8cg-48	48" X 30"		13, 13						10												
100P	571	565+20	LT	D3-1-72	72" X 24"					2			12												
101P	571	565+20, 48' LT	LT	D10-H8-12	12" X 12"								1												
102P	571	565+32, 40' RT	RT	D10-H8-12	12" X 12"								1												
103P	571	566+25, 42' LT	LT	R3-H8cg-48	48" X 30"			13, 13					10												
104P	571	566+28, 33' LT	LT	R9-3-18	18" X 18"	12							2.25												
105P	571	566+28, 40' RT	RT	R9-3-18	18" X 18"	12							2.25												
106P	571	566+34	RT	D3-1-72	72" X 24"					2			12												
107P	571	567+25, 34' RT	RT	M3-2-24	12" X 24"		14.58						2												
107P	571	567+25, 34' RT	RT	M1-4-24-2	24" X 24"								4												
108P	571	568+00, 42' LT	LT	R3-H8cg-48	48" X 30"		13, 13						10												
109P	571	568+12, 40' LT	LT	R1-1-30	30" X 30"					1			6.25												
110P	571	568+55, 40' LT	LT	R10-H7a-24	24" X 30"								5												
111P	571	568+75, 34' RT	RT	R2-1-30	30" X 36"								7.5												
112P	571	98+75, 27' RT	RT	M4-6-12	24" X 12"								2												
112P	571	98+75, 27' RT	RT	M1-5-30-3	30" X 24"								5												
113P	571	99+00, 25' LT	LT	M3-3-24	24" X 12"		13.58						2												
113P	571	99+00, 25' LT	LT	M1-5-30-3	30" X 24"								5												
114P	571	99+23, 25' RT	RT	R3-H8bh-36	36" X 30"			13					7.5												
115P	571	99+43, 34' RT	RT	R10-H6bL-24	24" X 36"			13.5					6												
116P	571	100+73, 28' LT	LT	R3-H8bh-36	36" X 30"			13					7.5												
98E	574	570+42, 39' LT	LT	R2-1-30	30" X 36"																				
99E	574	572+61, 26' RT	RT	R2-1-30	30" X 36"										1	1									
100E	574	573+69, 32' LT	LT	M1-5-30-3	30" X 24"										1	1									
100E	574	573+69, 32' LT	LT	M2-1-21	21" X 15"										1										
117P	574	571+00, 36' LT	LT	R2-1-30	30" X 36"			13.5					7.5												
118P	574	572+50, 34' LT	LT	D3-2-60	60" X 30"			13, 13					12.5												
101E	575	577+02, 39' RT	RT	R2-1-30	30" X 36"																				
102E	575	578+22, 31' LT	LT	R2-1-30	30" X 36"										1	1									
119P	575	575+00, 32' LT	LT	W3-3-36	36" X 36"			13.5		1			9												
120P	575	577+00, 31' LT	LT	M2-1-21	21" X 15"			13.83					2.19												
120P	575	577+00, 31' LT	LT	M1-5-30-3	30" X 24"								5												
121P	575	578+50, 31' LT	LT	R2-1-30	30" X 36"			13.5					7.5												
122P	575	578+50, 26' RT	RT	R2-1-30	30" X 36"			13.5					7.5												
103E	576	580+00, 32' LT	LT	W3-5-36	36" X 36"										1	1									
123P	576	582+00, 32' LT	LT	W3-5-36	36" X 36"			13.5		1			9												
SUBTOTAL SHEET 538						24.0	320.7			3	4	2	187		12	8									

SIGNING SUBSUMMARY	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="font-size: small;">CALCULATED</td> <td style="font-size: small;">ARK</td> </tr> <tr> <td style="font-size: small;">CHECKED</td> <td style="font-size: small;">EJT</td> </tr> </table>	CALCULATED	ARK	CHECKED	EJT
CALCULATED	ARK				
CHECKED	EJT				
LAK-US-20-24.99	PART 2				
538	697				

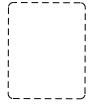

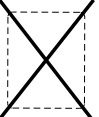
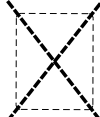
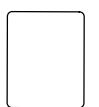
H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IS007.dgn Sheet 7/15/2022 2:59:58 PM Troyer

REF NO.	SHEET NO.	STATION TO STATION	SIDE	SIGN CODE	SIGN SIZE	630	630			630	630	630	630	630	630	630	630	630								
						GROUND MOUNTED SUPPORT, NO. 2 POST FT	GROUND MOUNTED SUPPORT, NO. 3 POST FT			SIGN POST REFLECTOR EACH	SIGN ATTACHMENT ASSEMBLY, MAST ARM EACH	SIGN SUPPORT ASSEMBLY, POLE MOUNTED EACH	SIGN, FLAT SHEET SF	SIGN, DOUBLE FACED, STREET NAME EACH	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL EACH	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL EACH	REMOVAL OF GROUND MOUNTED SIGN AND REERECTION EACH	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL EACH	REMOVAL OF POLE MOUNTED SIGN AND REERECTION EACH							
104E	572	92+52, 28' RT	RT	R2-1-30	30" X 36"										1	1										
105E	572	94+51, 27' LT	LT	R2-1-30	30" X 36"																					
106E	572	96+96, 28' RT	LT	R2-1-30	30" X 36"																					
107E	572	97+21, 25' LT	LT	R3-H8bh-36	36" X 30"										1	1										
108E	572	97+21, 33' LT	LT	M2-1-21	21" X 15"																					
108E	572	97+21, 33' LT	LT	M1-4-24-2	24" X 24"																					
124P	572	89+85, 28' RT	RT	M2-1-21	21" X 15"		13.83						2.19													
124P	572	89+85, 28' RT	RT	M1-4-24-2	24" X 24"								4													
125P	572	91+85, 28' RT	RT	W3-3-36	36" X 36"					1			9													
126P	572	92+36, 25' LT	RT	R2-1-30	30" X 36"		13.5						7.5													
127P	572	93+10, 25' LT	LT	R2-1-30	30" X 36"		13.5						7.5													
128P	572	97+60, 25' RT	RT	R3-H8bh-36	36" X 30"		13						7.5													
129P	572	98+00, 25' LT	LT	R2-1-30	30" X 36"		13.5						7.5													
109E	573	104+49, 18' LT	LT	R3-H8bh-36	36" X 30"										1	1										
110E	573	106+81, 19' LT	LT	W3-3-36	36" X 36"										1	1										
130P	573	101+75, 22' RT	RT	R2-1-30	30" X 36"		13.5						7.5													
131P	573	102+05, 25' LT	LT	R3-H8bh-36	36" X 30"		13						7.5													
132P	573	106+50, 20' LT	LT	W3-3-36	36" X 36"		13.5			1			9													
133P	573	108+50, 20' LT	LT	M2-1-21	21" X 15"		13.83						2.19													
133P	573	108+50, 20' LT	LT	M1-4-24-2	24" X 24"								4													
134P	573	108+50, 20' LT	LT	M2-1-21	21" X 15"		13.83						2.19													
134P	573	108+50, 20' LT	LT	M1-5-30-3	30" X 24"								5													
TOTALS FROM SHEET 539							148.5			2			82.57		4	4			4							
TOTALS FROM SHEET 533						24.0	335.2			6		3	186.00		19	17	4		3	1						
TOTALS FROM SHEET 534						24.0	299.1			8			149.00		22	19			1							
TOTALS FROM SHEET 535						13.0	225.6			3		3	145.00		18	20			8							
TOTALS FROM SHEET 536						59.0	262.5			5	4	2	172.00		20	15	2		1	2						
TOTALS FROM SHEET 537						89.0	305.3			1	4	1	211.00		15	16	2		7	1						
TOTALS FROM SHEET 538						24.0	320.7			3	4	2	187.00		12	8			2							
SUBTOTAL						233.0	1896.9			28	12	11	1132.57		110	99	8		26	4						
TOTALS CARRIED TO GENERAL SUMMARY						233	1897			28	12	11	1133		110	99	8		26	4						

SIGNING SUBSUMMARY	CALCULATED ARK CHECKED EJT
LAK-US-20-24.99 PART 2	539 697



SIGNING LEGEND

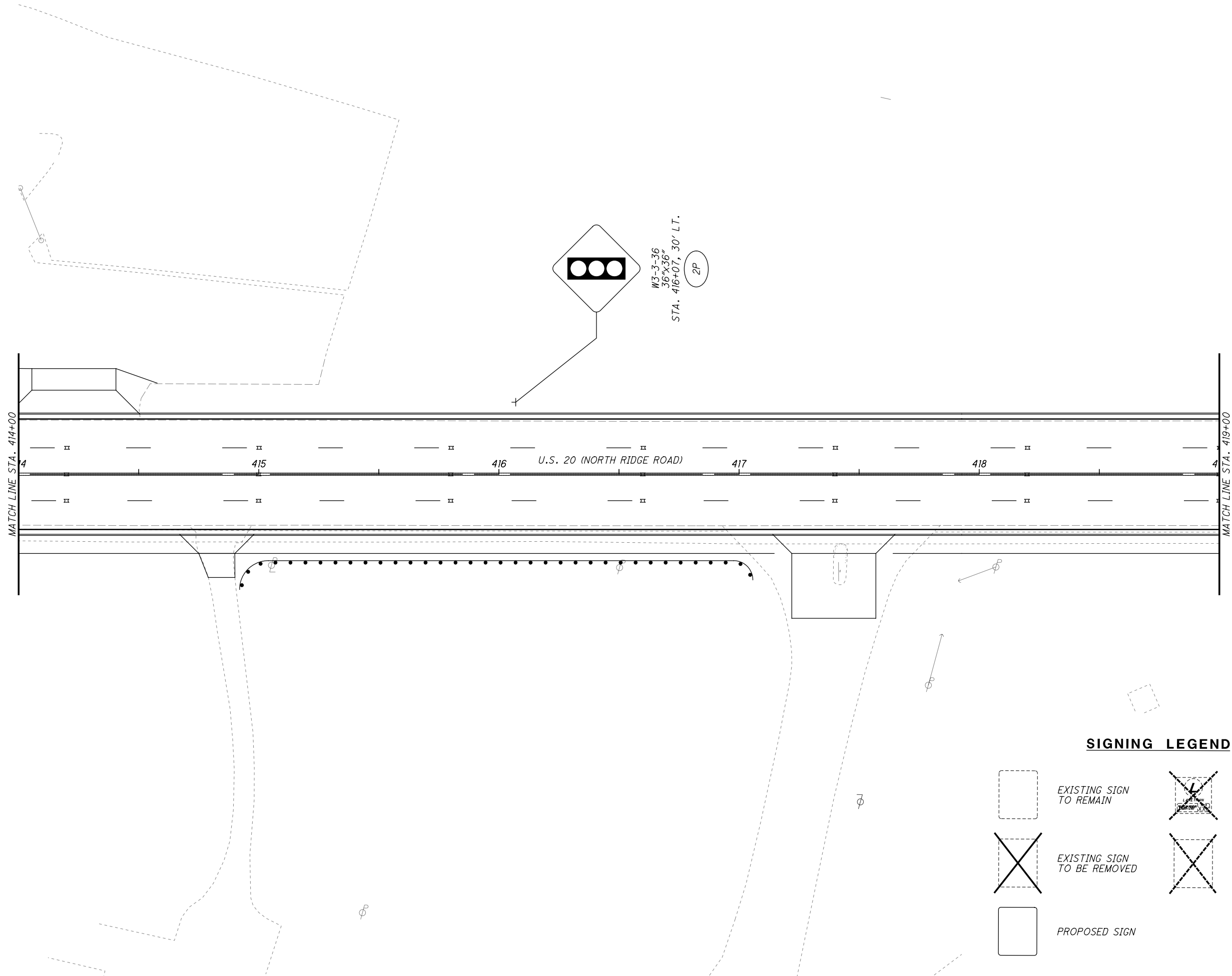
- | | | | |
|---------------------------------------------------------------------------------------|-----------------------------|---------------------------------------------------------------------------------------|------------------------------------------|
|  | EXISTING SIGN TO REMAIN |  | LAKETRAN SIGN TO BE REMOVED & RE-ERECTED |
|  | EXISTING SIGN TO BE REMOVED |  | EXISTING SIGN TO BE REMOVED & RE-ERECTED |
|  | PROPOSED SIGN | | |

CALCULATED JDC CHECKED EJT



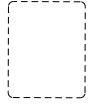
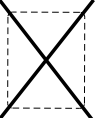
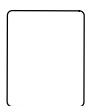

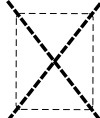

HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 409+00 TO STA. 414+00



W3-3-36
36" x 36"
STA. 416+07, 30' LT.
2P

SIGNING LEGEND

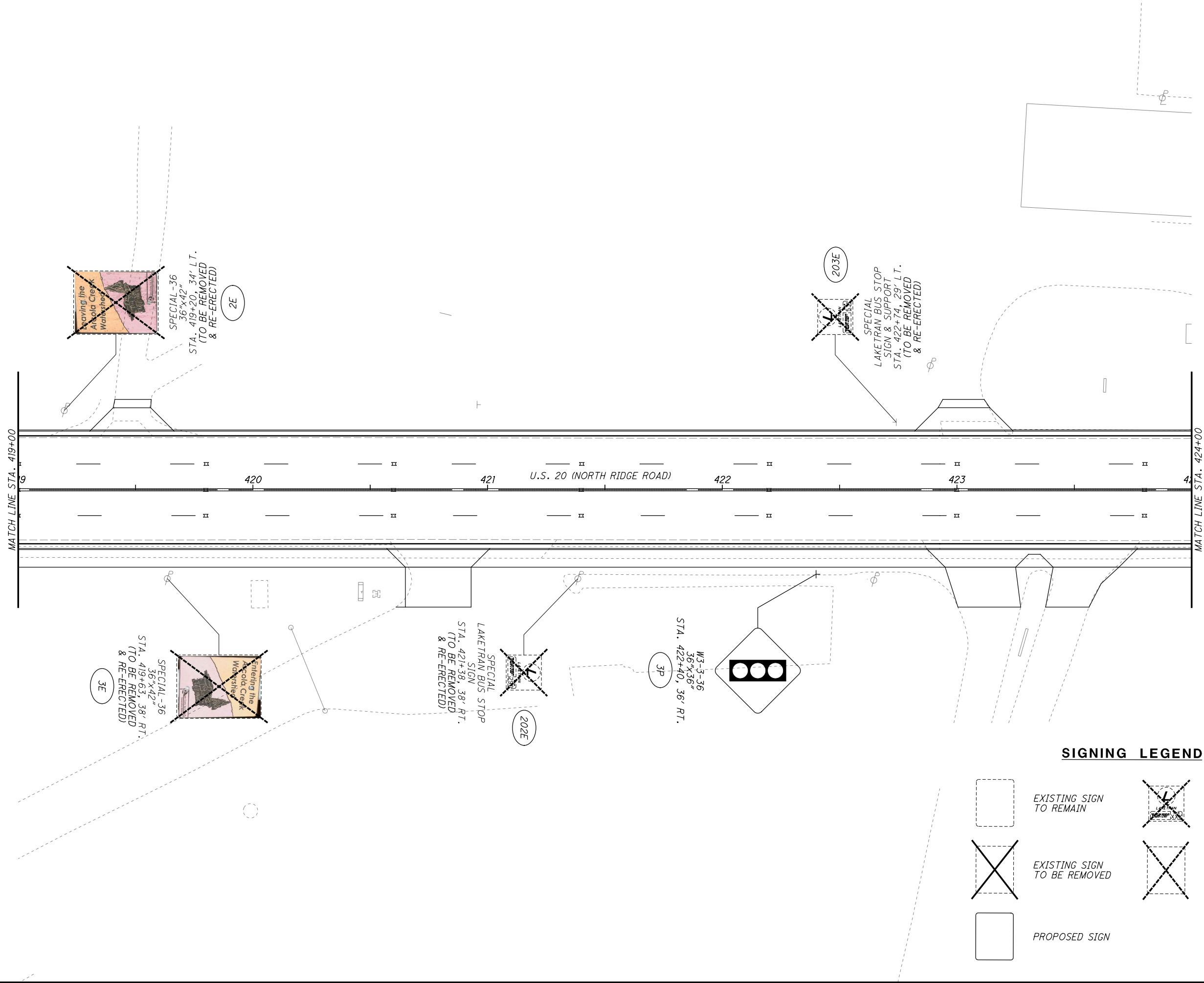
-  EXISTING SIGN TO REMAIN
-  EXISTING SIGN TO BE REMOVED
-  PROPOSED SIGN
-  LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
-  EXISTING SIGN TO BE REMOVED & RE-ERECTED

CALCULATED JDC
CHECKED EJT

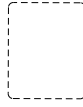



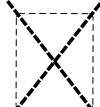



0 20 40
HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 414+00 TO STA. 419+00



SIGNING LEGEND

-  EXISTING SIGN TO REMAIN
-  EXISTING SIGN TO BE REMOVED
-  PROPOSED SIGN
-  LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
-  EXISTING SIGN TO BE REMOVED & RE-ERECTED




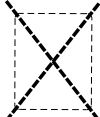

CALCULATED JDC CHECKED EJT

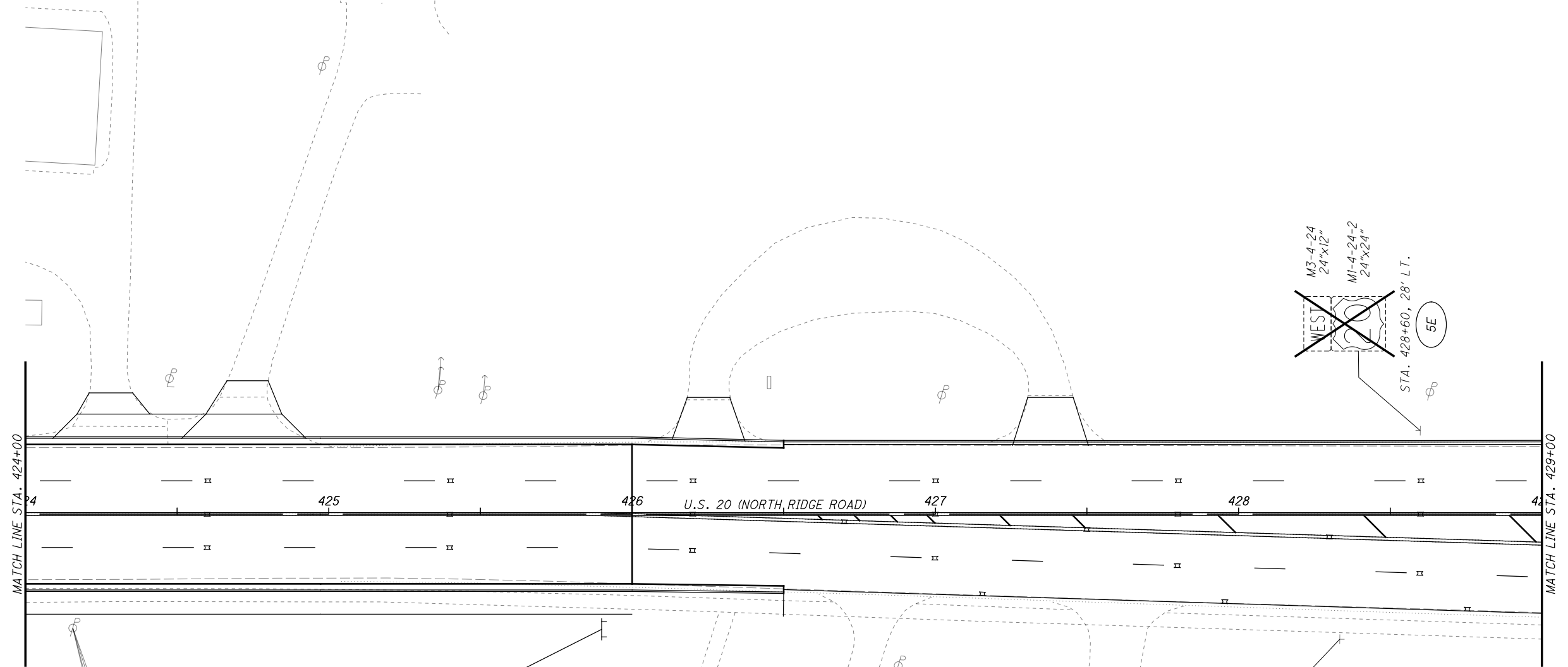
0 10 20 40
HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 419+00 TO STA. 424+00

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP064.dgn Sheet 7/15/2022 3:00:03 PM troyer

SIGNING LEGEND

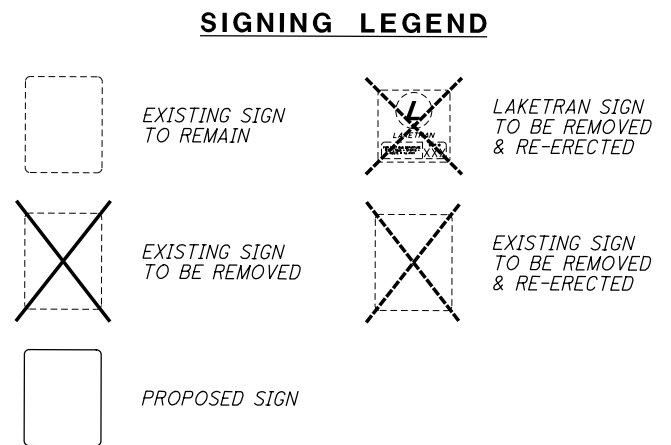
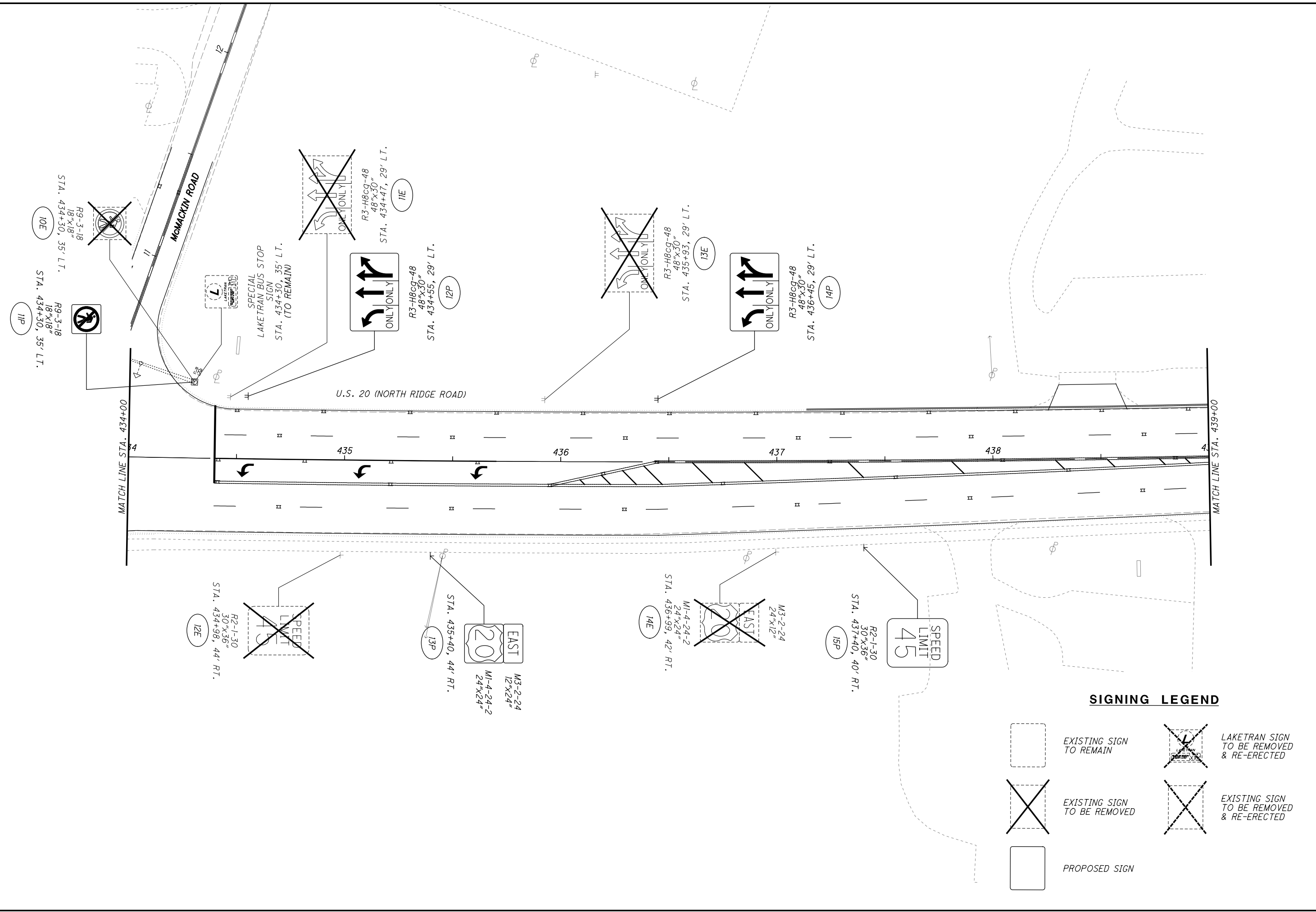
	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		



CALCULATED JDC
CHECKED EJT

0 20 40
10
HORIZONTAL SCALE IN FEET

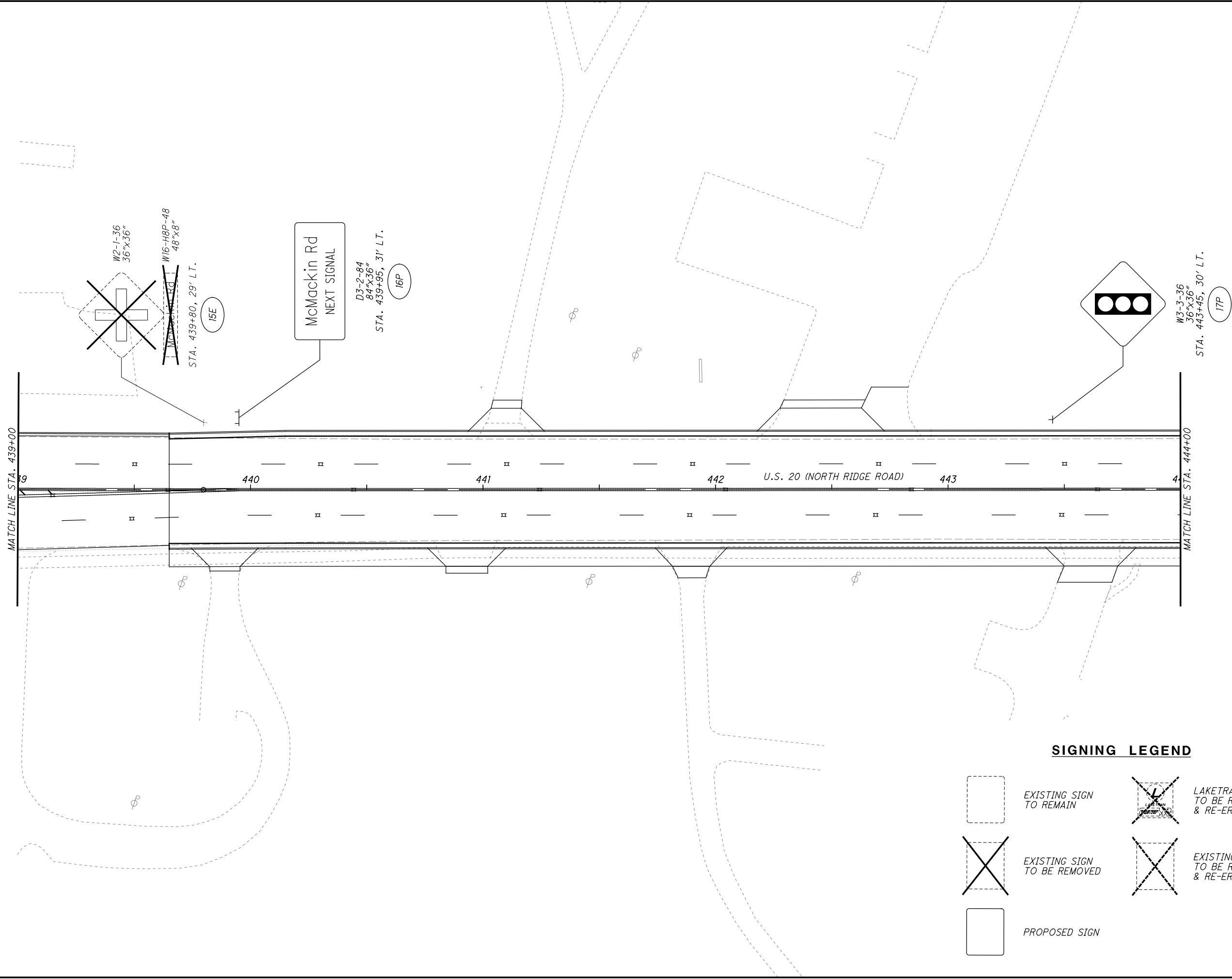
SIGNING PLAN
STA. 424+00 TO STA. 429+00





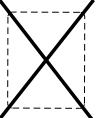
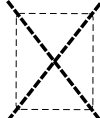

CALCULATED JDC
 CHECKED EJT
 0 20 40
 HORIZONTAL SCALE IN FEET

SIGNING PLAN

STA. 434+00 TO STA. 439+00



SIGNING LEGEND

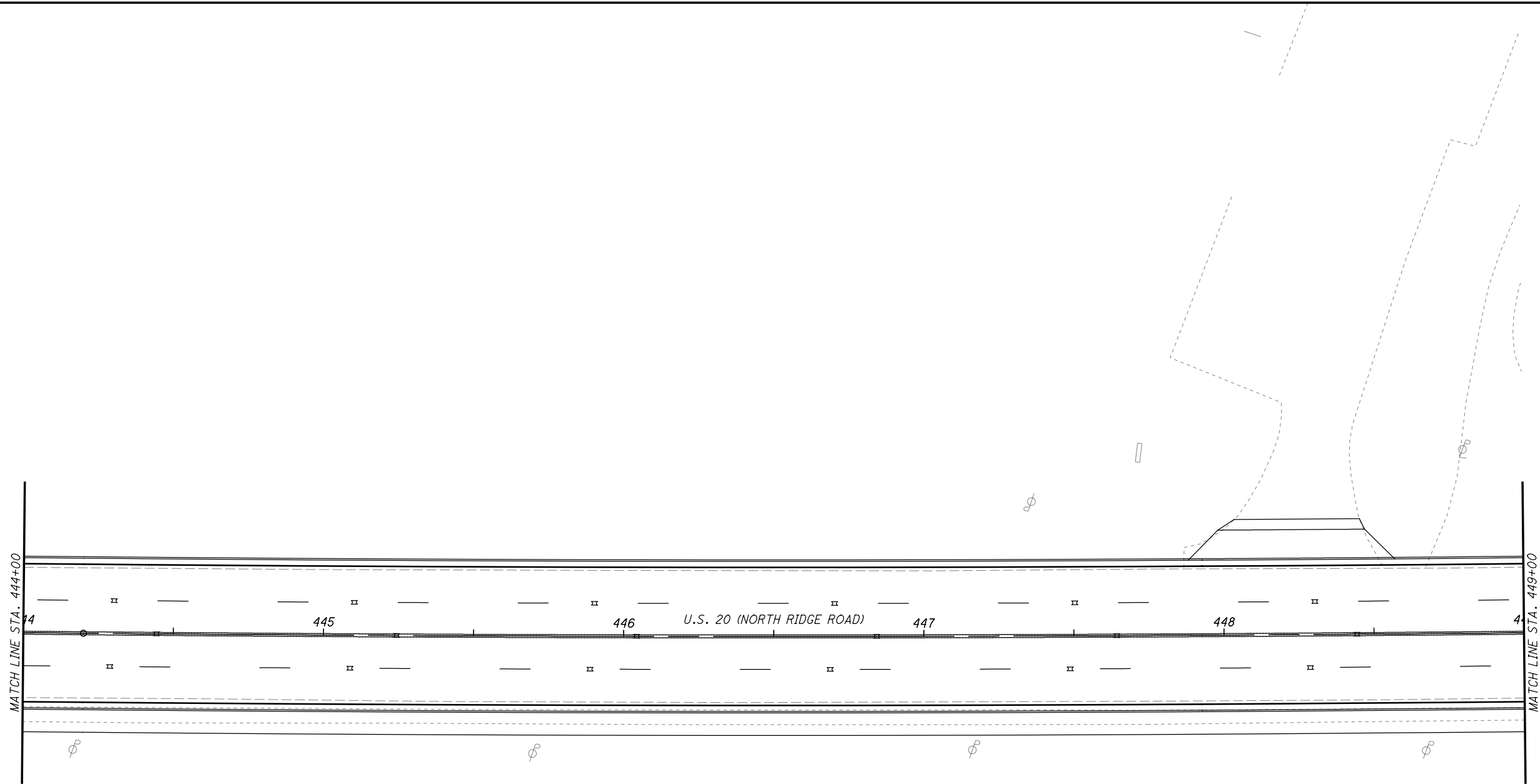
- | | | | |
|---------------------------------------------------------------------------------------|-----------------------------|---------------------------------------------------------------------------------------|------------------------------------------|
|  | EXISTING SIGN TO REMAIN |  | LAKETRAN SIGN TO BE REMOVED & RE-ERECTED |
|  | EXISTING SIGN TO BE REMOVED |  | EXISTING SIGN TO BE REMOVED & RE-ERECTED |
|  | PROPOSED SIGN | | |

CALCULATED JDC CHECKED EJT

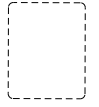
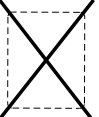
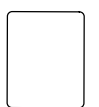

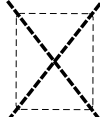



HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 439+00 TO STA. 444+00



SIGNING LEGEND

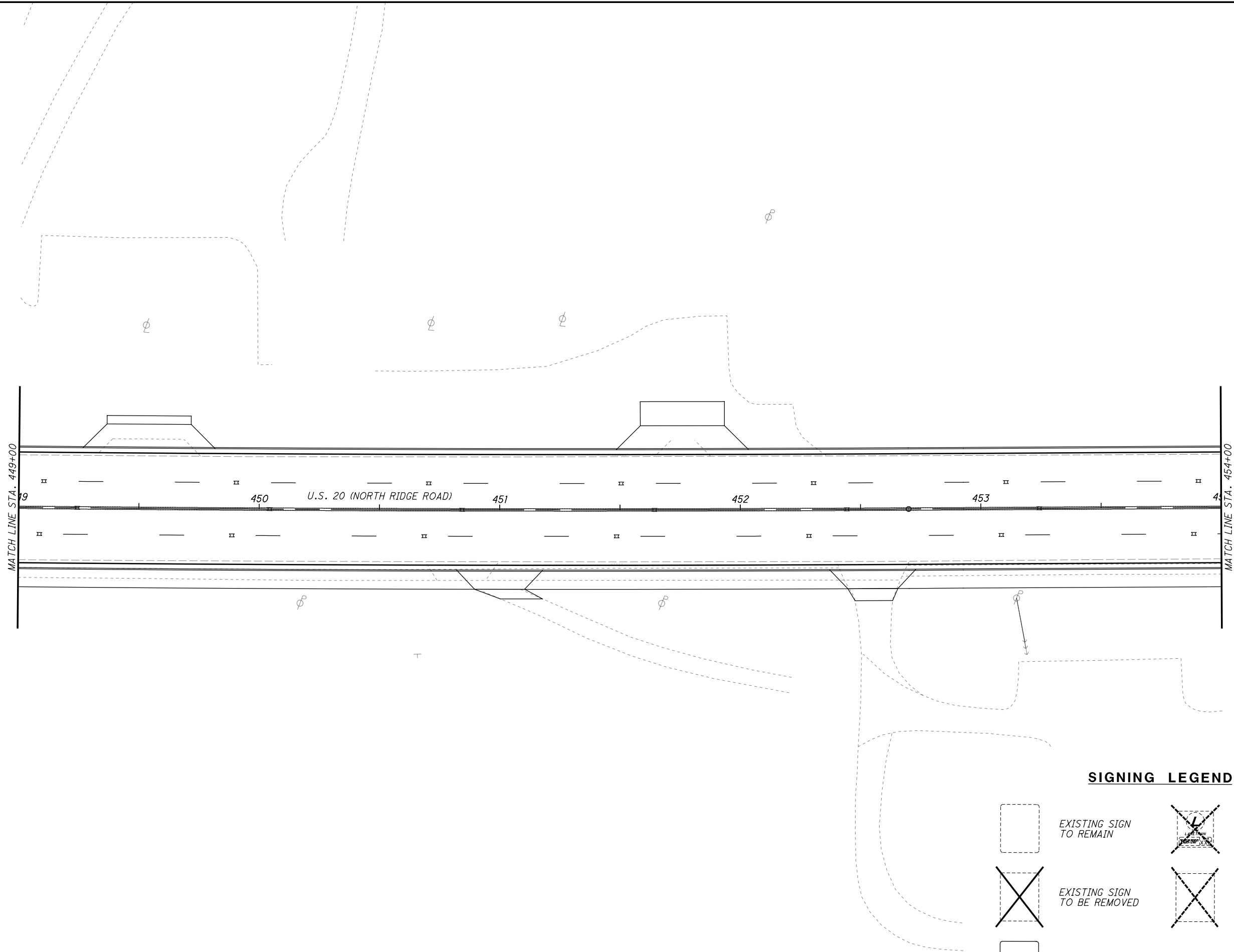
-  EXISTING SIGN TO REMAIN
-  EXISTING SIGN TO BE REMOVED
-  PROPOSED SIGN
-  LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
-  EXISTING SIGN TO BE REMOVED & RE-ERECTED

CALCULATED JDC
CHECKED EJT

0 10 20 40
HORIZONTAL SCALE IN FEET



SIGNING PLAN
STA. 444+00 TO STA. 449+00




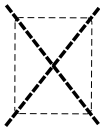
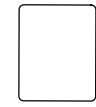


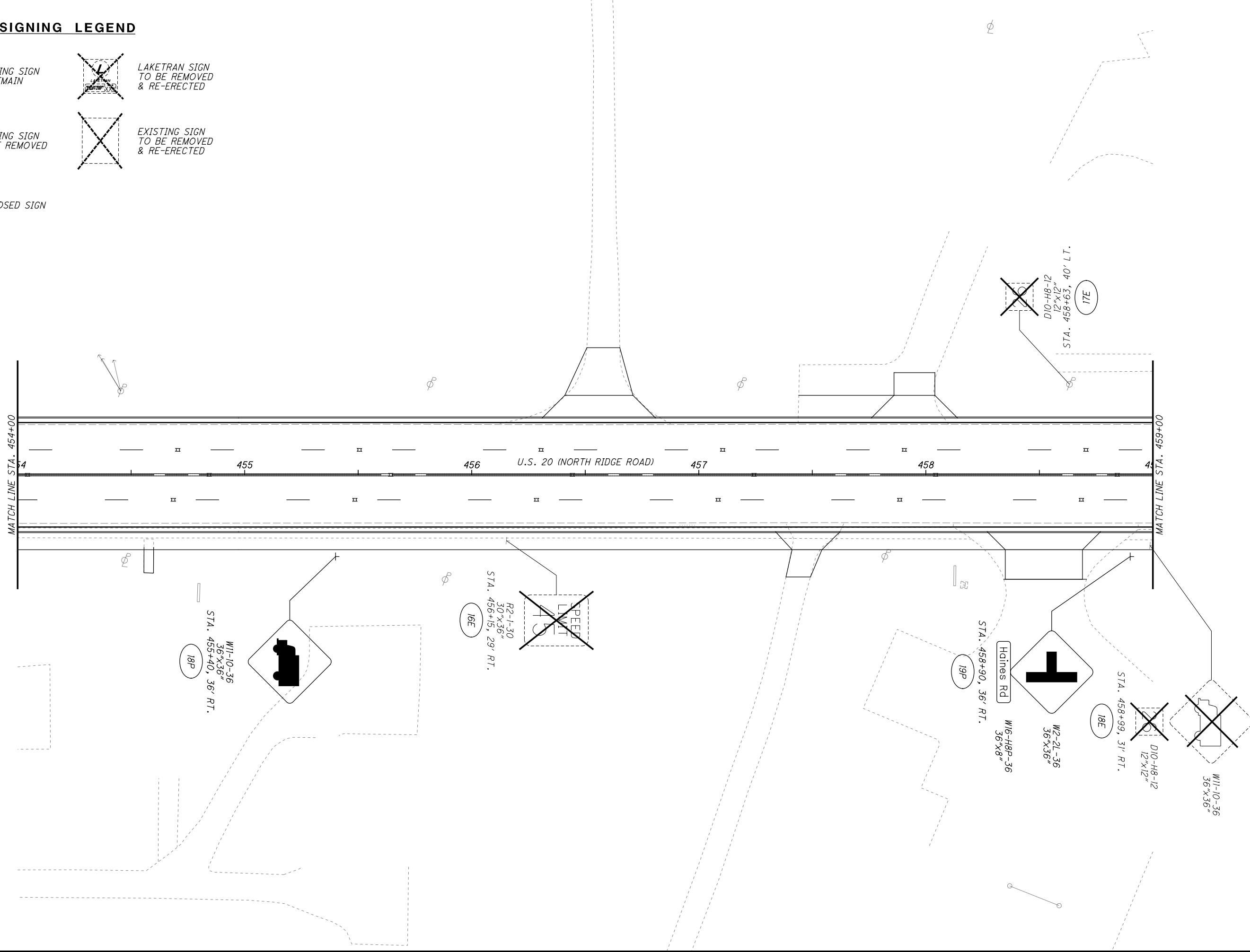
CALCULATED JDC CHECKED EJT

0 20 40
HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 449+00 TO STA. 454+00

SIGNING LEGEND

	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		



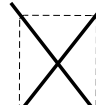
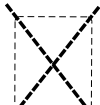
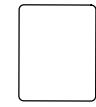


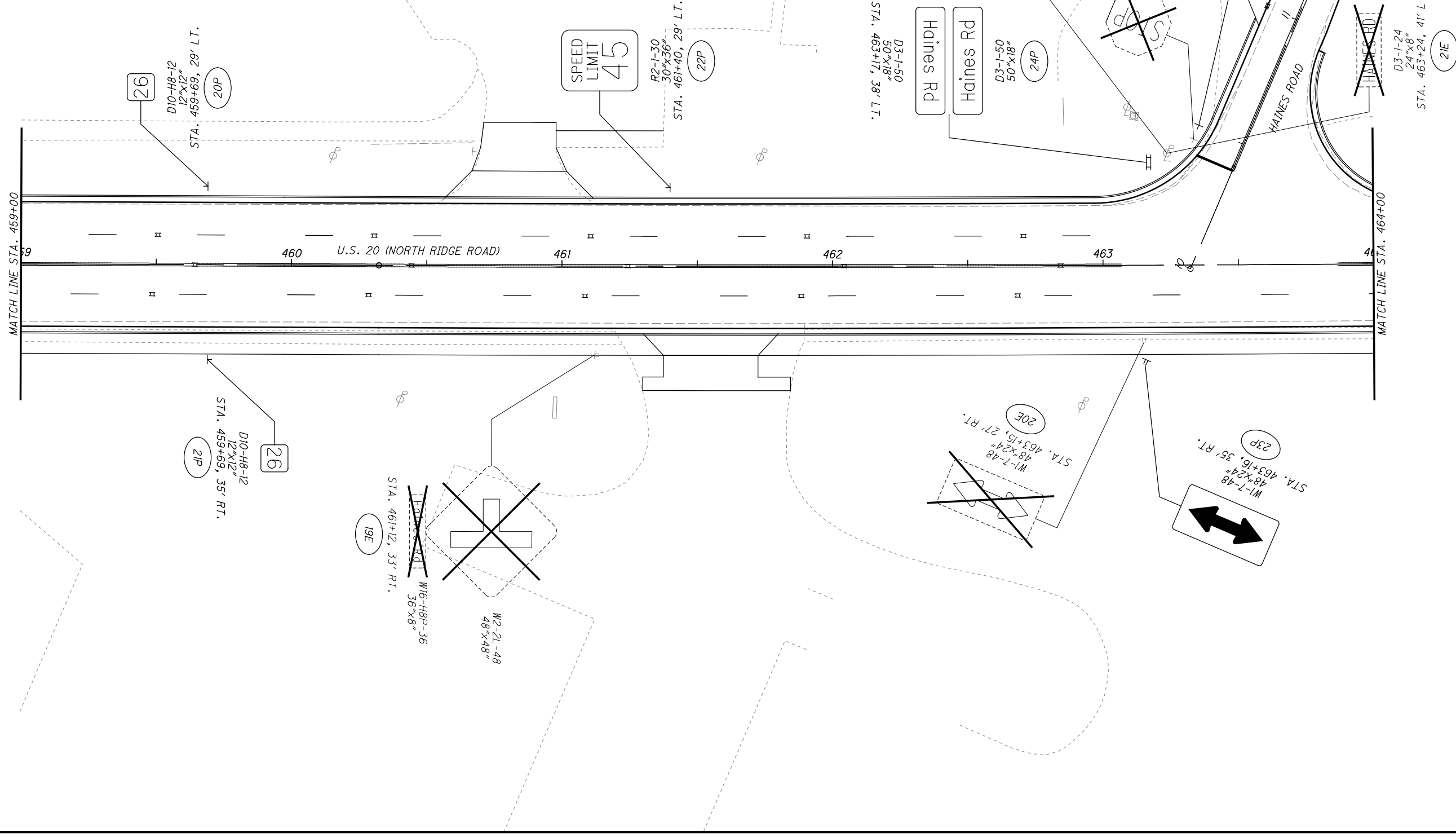
CALCULATED JDC
CHECKED EJT

SIGNING PLAN

STA. 454+00 TO STA. 459+00

SIGNING LEGEND

	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		



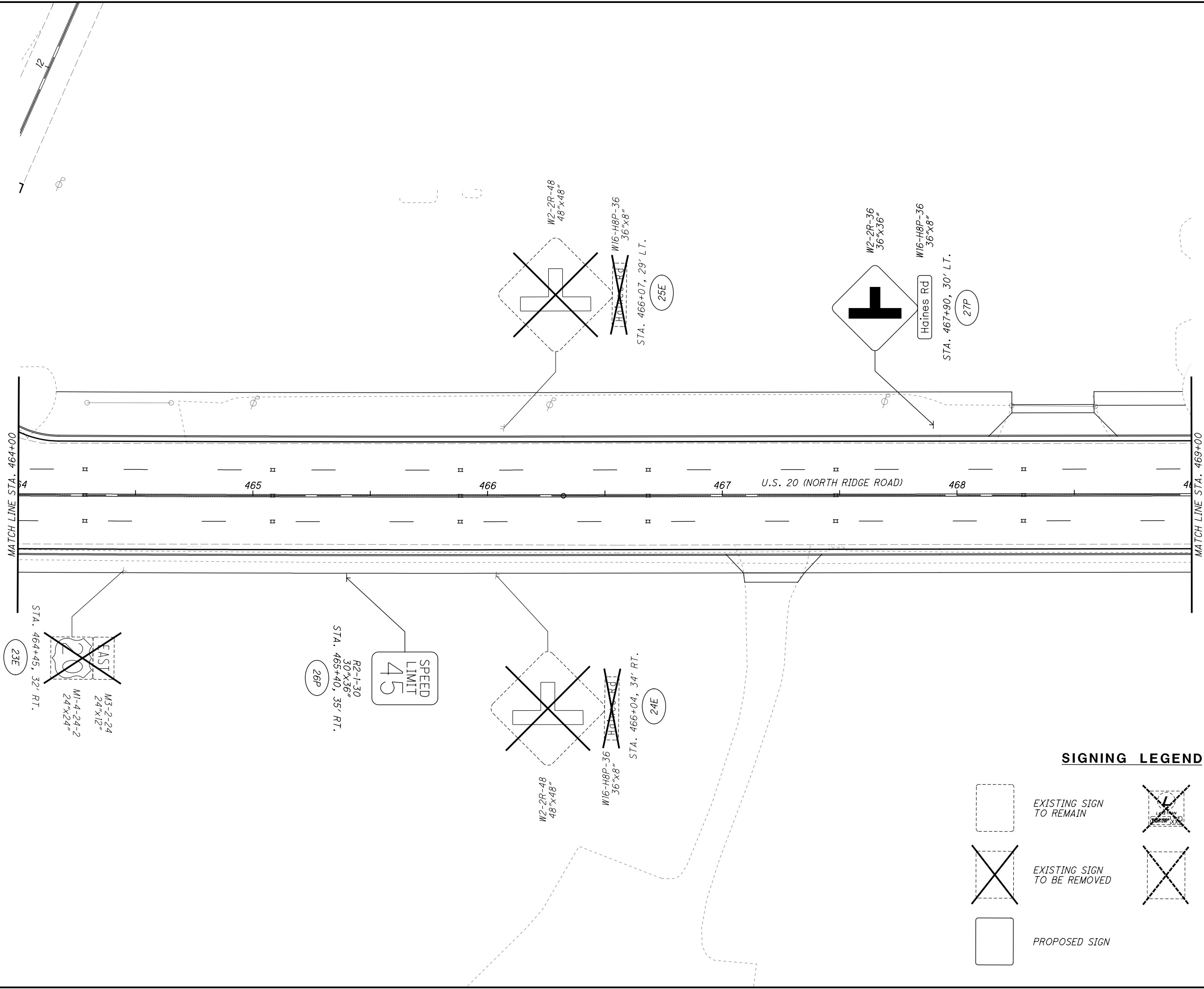
550
697

SCALE IN FEET
10
20
40



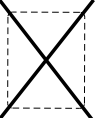
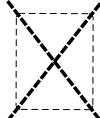

CALCULATED JDC
CHECKED EJT

SIGNING PLAN

STA. 459+00 TO STA. 464+00



SIGNING LEGEND

	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		

CALCULATED JDC CHECKED EJT

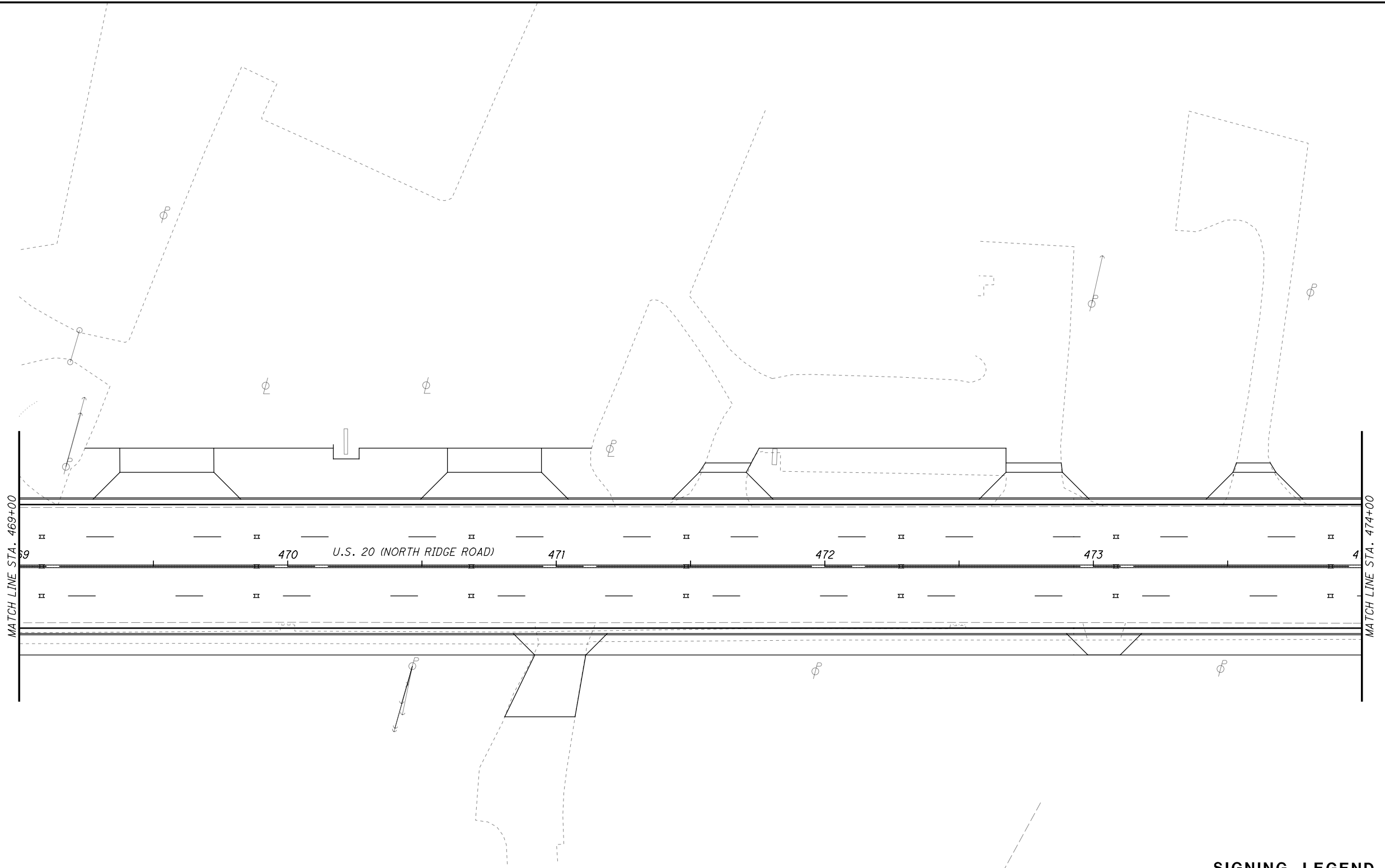
0 10 20 40

HORIZONTAL SCALE IN FEET


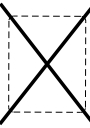
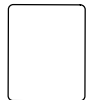

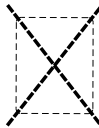
SIGNING PLAN

STA. 464+00 TO STA. 469+00

H:\2018\180743\0001\Project\Data\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP073.dgn Sheet 7/15/2022 3:00:07 PM troyer



SIGNING LEGEND

-  EXISTING SIGN TO REMAIN
-  EXISTING SIGN TO BE REMOVED
-  PROPOSED SIGN
-  LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
-  EXISTING SIGN TO BE REMOVED & RE-ERECTED

CALCULATED
JDC
CHECKED
EJT

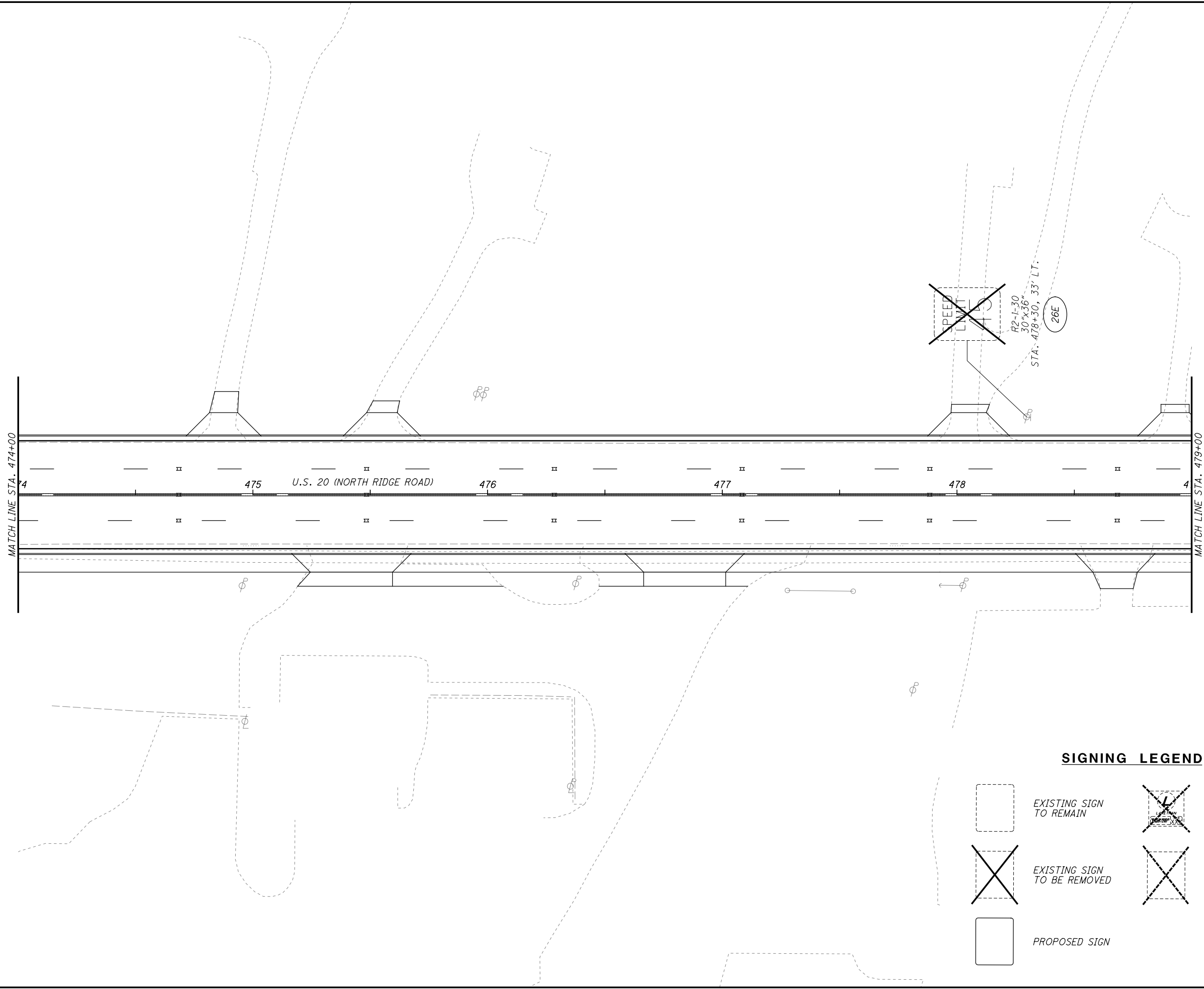



HORIZONTAL SCALE IN FEET



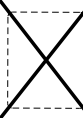
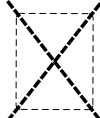

SIGNING PLAN
STA. 469+00 TO STA. 474+00

LAK-US-20-24.99
PART 2

552
697



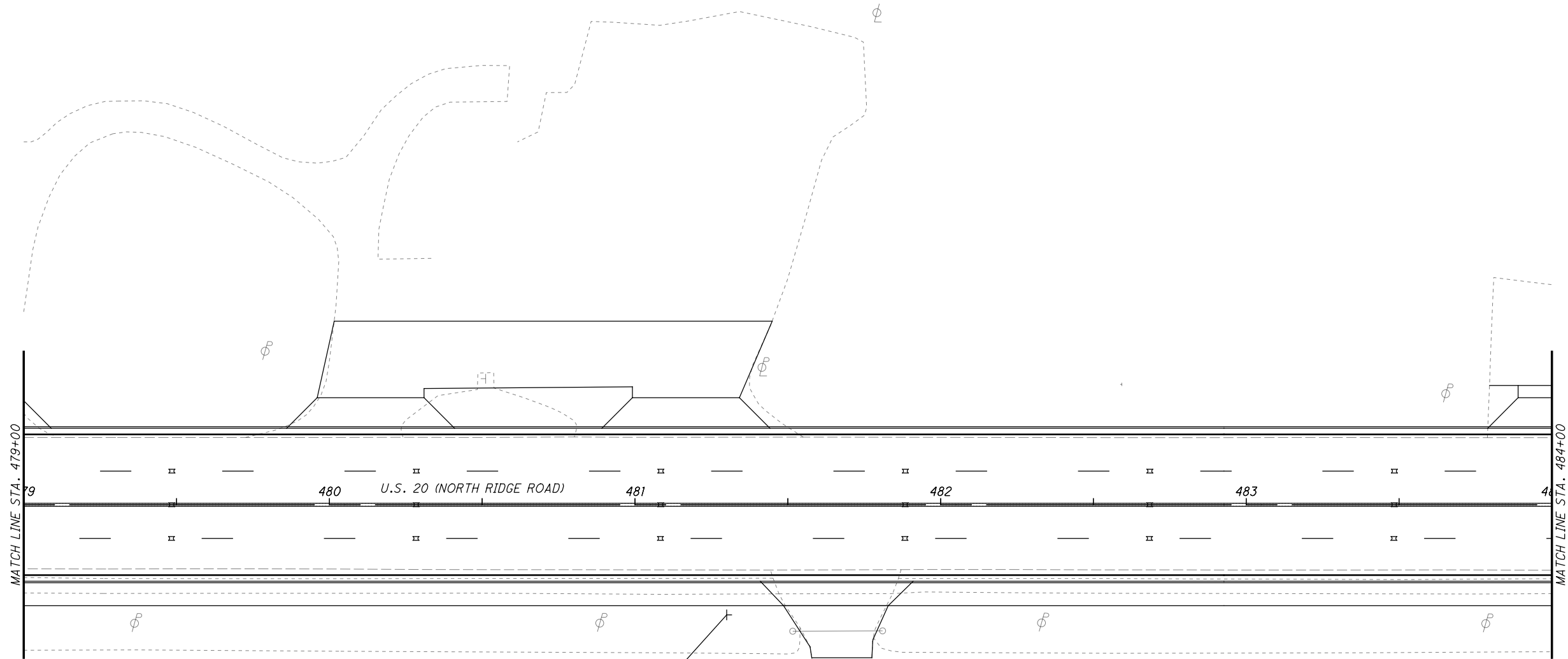
SIGNING LEGEND

- | | | | |
|---------------------------------------------------------------------------------------|-----------------------------|---------------------------------------------------------------------------------------|------------------------------------------|
|  | EXISTING SIGN TO REMAIN |  | LAKETRAN SIGN TO BE REMOVED & RE-ERECTED |
|  | EXISTING SIGN TO BE REMOVED |  | EXISTING SIGN TO BE REMOVED & RE-ERECTED |
|  | PROPOSED SIGN | | |

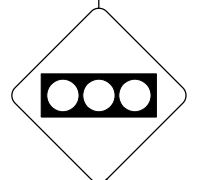
CALCULATED
JDC
CHECKED
EJT

HORIZONTAL SCALE IN FEET



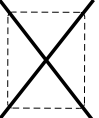
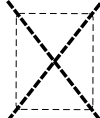

SIGNING PLAN
STA. 474+00 TO STA. 479+00



W3-3-36
36"x36"
STA. 481+30, 36' RT.
28P



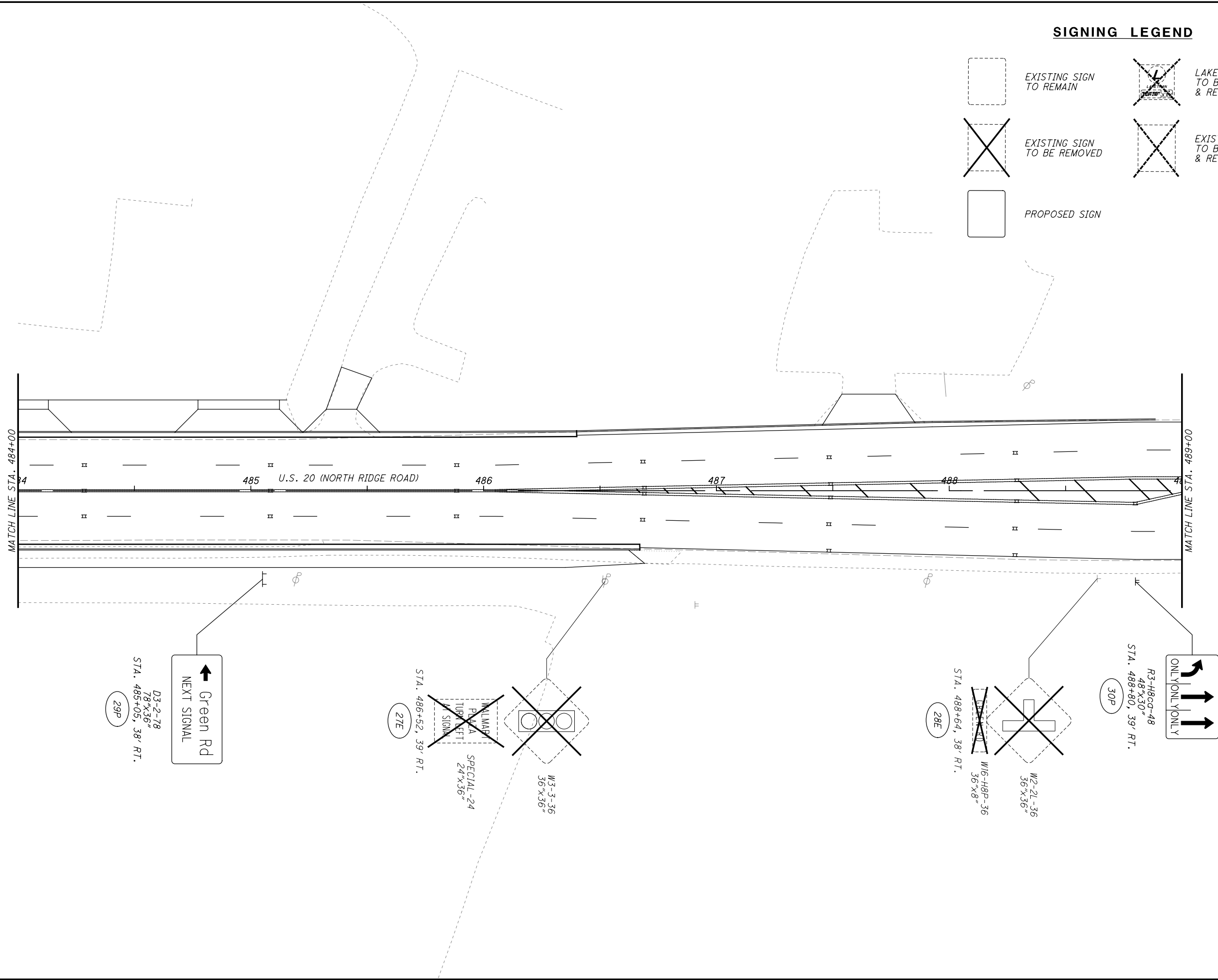
SIGNING LEGEND

- | | | | |
|---------------------------------------------------------------------------------------|-----------------------------|---------------------------------------------------------------------------------------|------------------------------------------|
|  | EXISTING SIGN TO REMAIN |  | LAKETRAN SIGN TO BE REMOVED & RE-ERECTED |
|  | EXISTING SIGN TO BE REMOVED |  | EXISTING SIGN TO BE REMOVED & RE-ERECTED |
|  | PROPOSED SIGN | | |

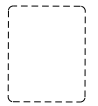
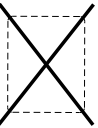
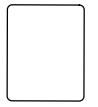

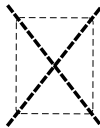
CALCULATED JDC CHECKED EJT

HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 479+00 TO STA. 484+00



SIGNING LEGEND



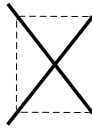
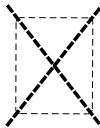

-  EXISTING SIGN TO REMAIN
-  EXISTING SIGN TO BE REMOVED
-  PROPOSED SIGN
-  LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
-  EXISTING SIGN TO BE REMOVED & RE-ERECTED

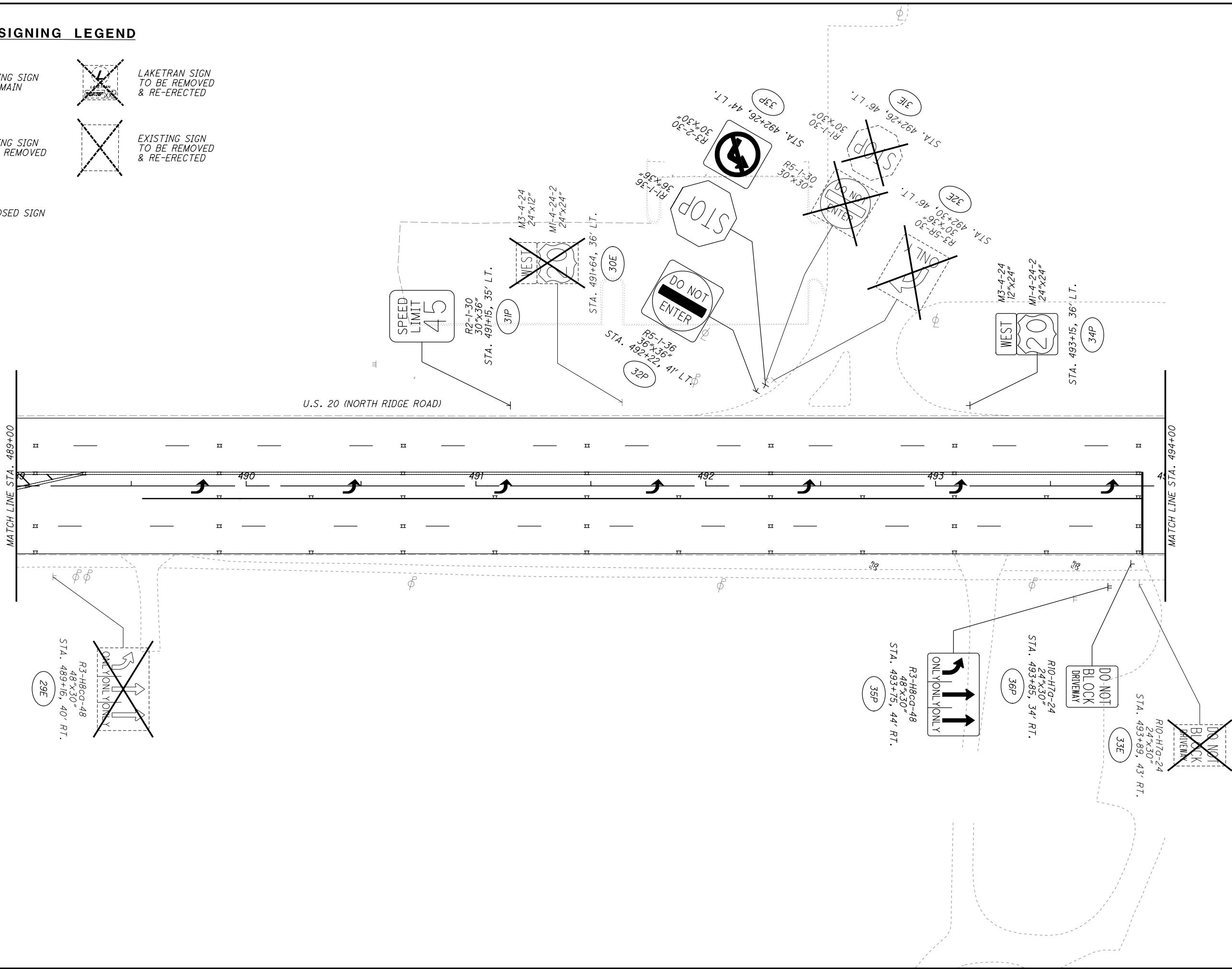
CALCULATED JDC CHECKED EJT

0 20 40
HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 484+00 TO STA. 489+00

SIGNING LEGEND

	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		

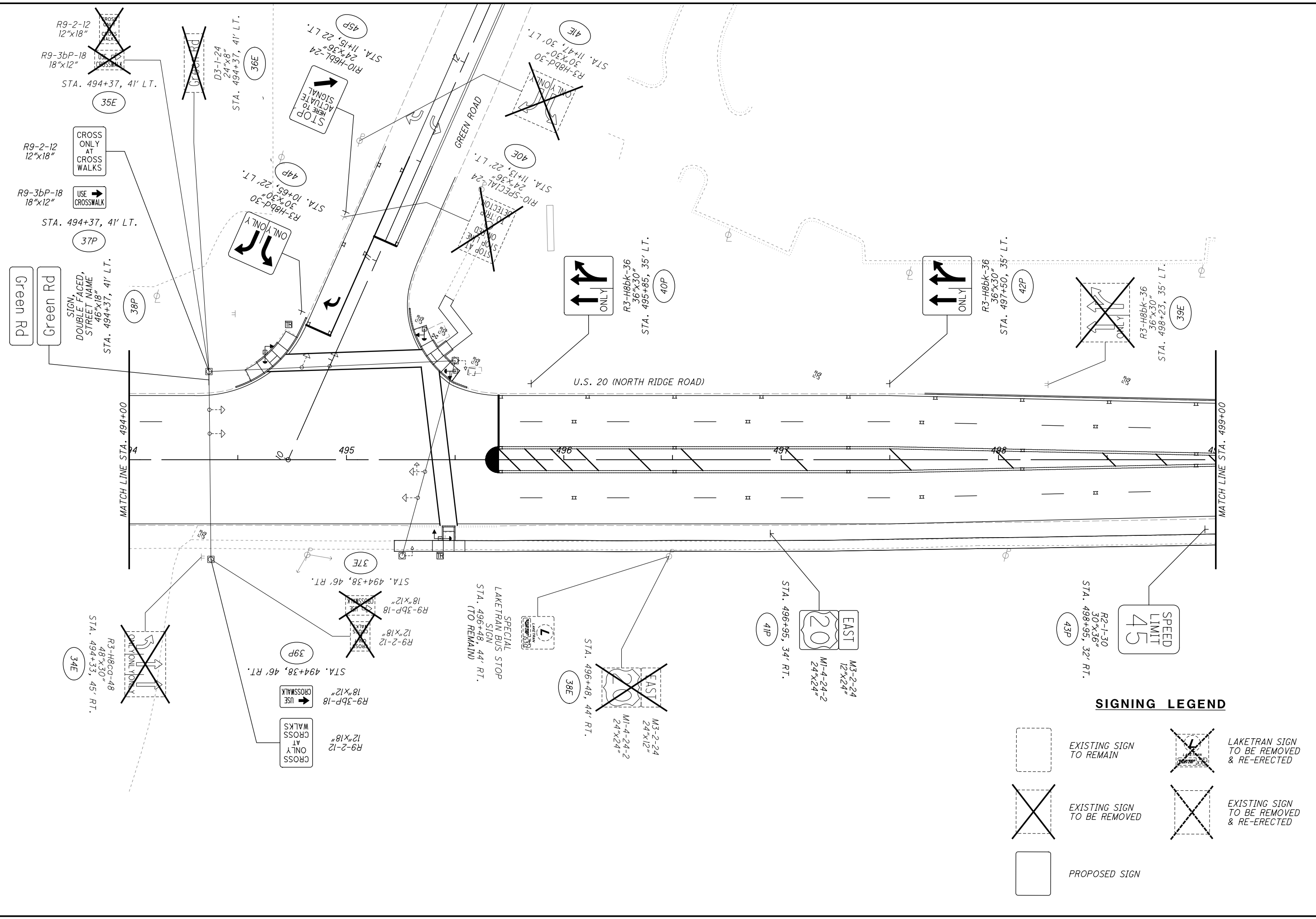


CALCULATED 0
 JDC
 CHECKED EJT

0 20 40
 HORIZONTAL SCALE IN FEET

SIGNING PLAN

STA. 489+00 TO STA. 494+00



SIGNING LEGEND




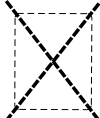

	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		

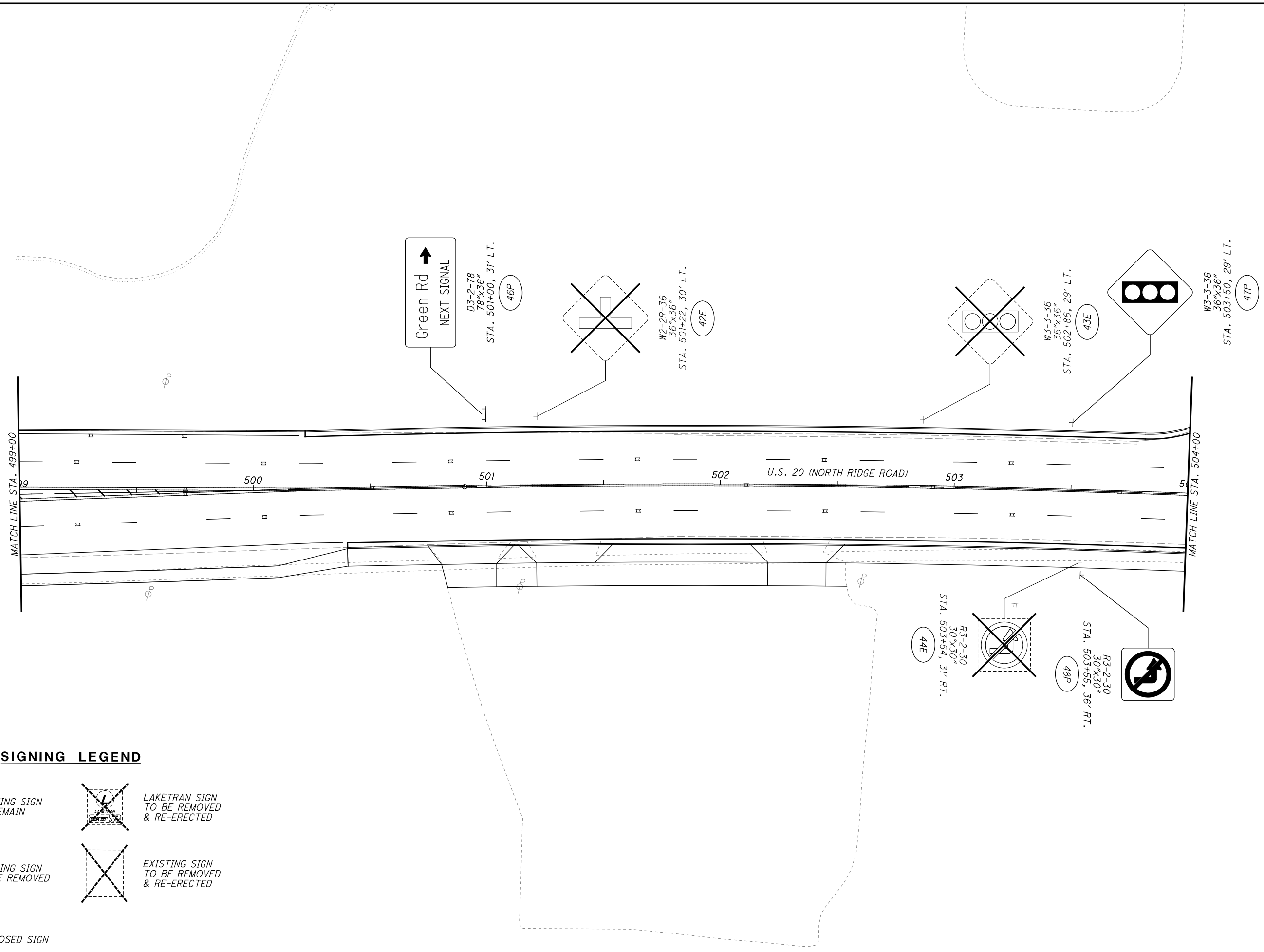
CALCULATED 0
 JDC 10
 CHECKED EJT
 HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 494+00 TO STA. 499+00

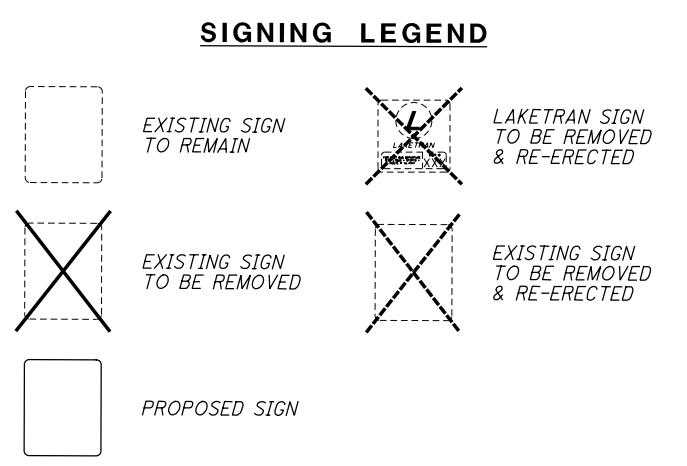
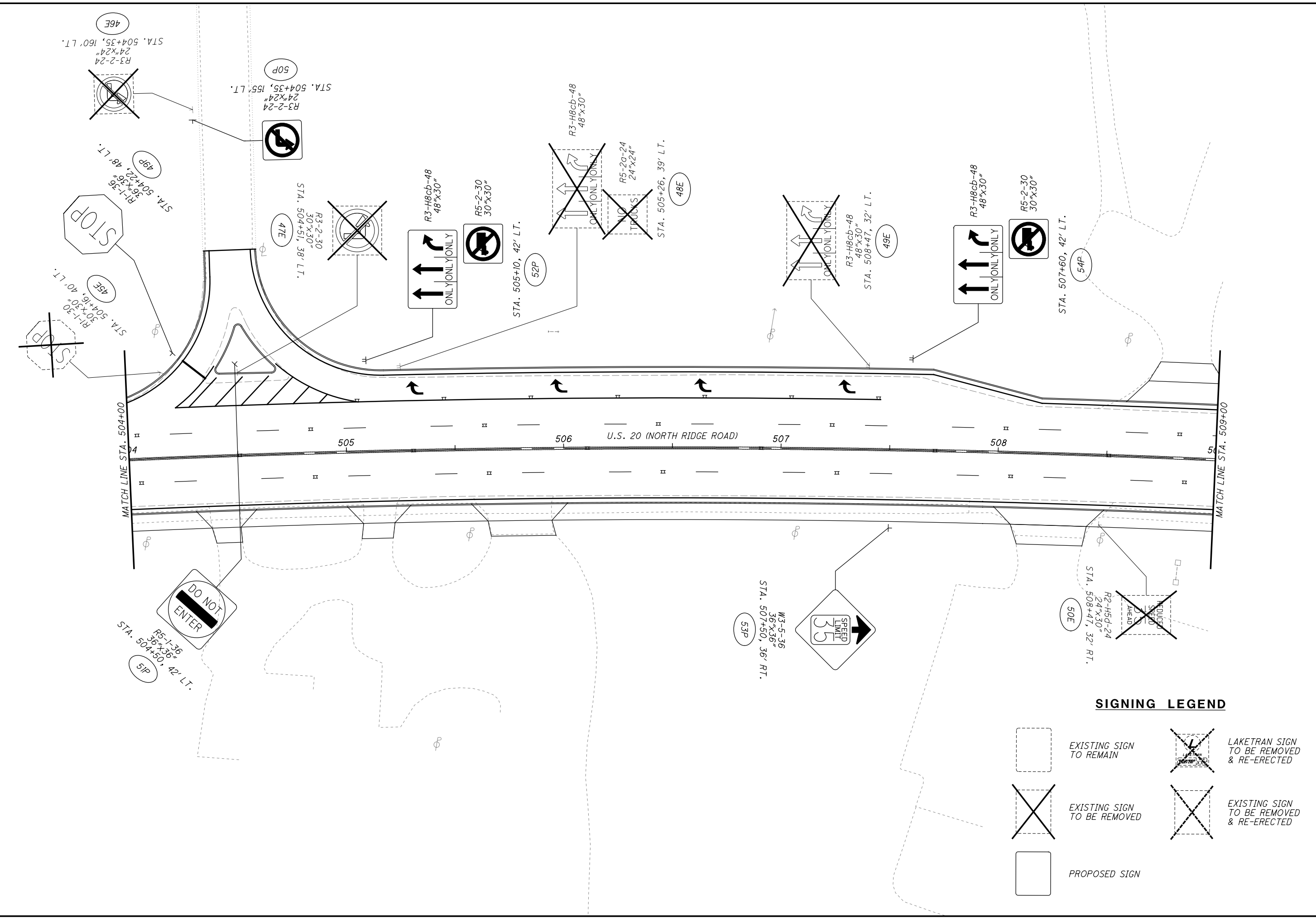
LAK-US-20-24.99
PART 2

SIGNING LEGEND

	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		



H:\2018\180743\0001\Project\Data\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP080.dgn Sheet 7/15/2022 3:00:16 PM Troyer

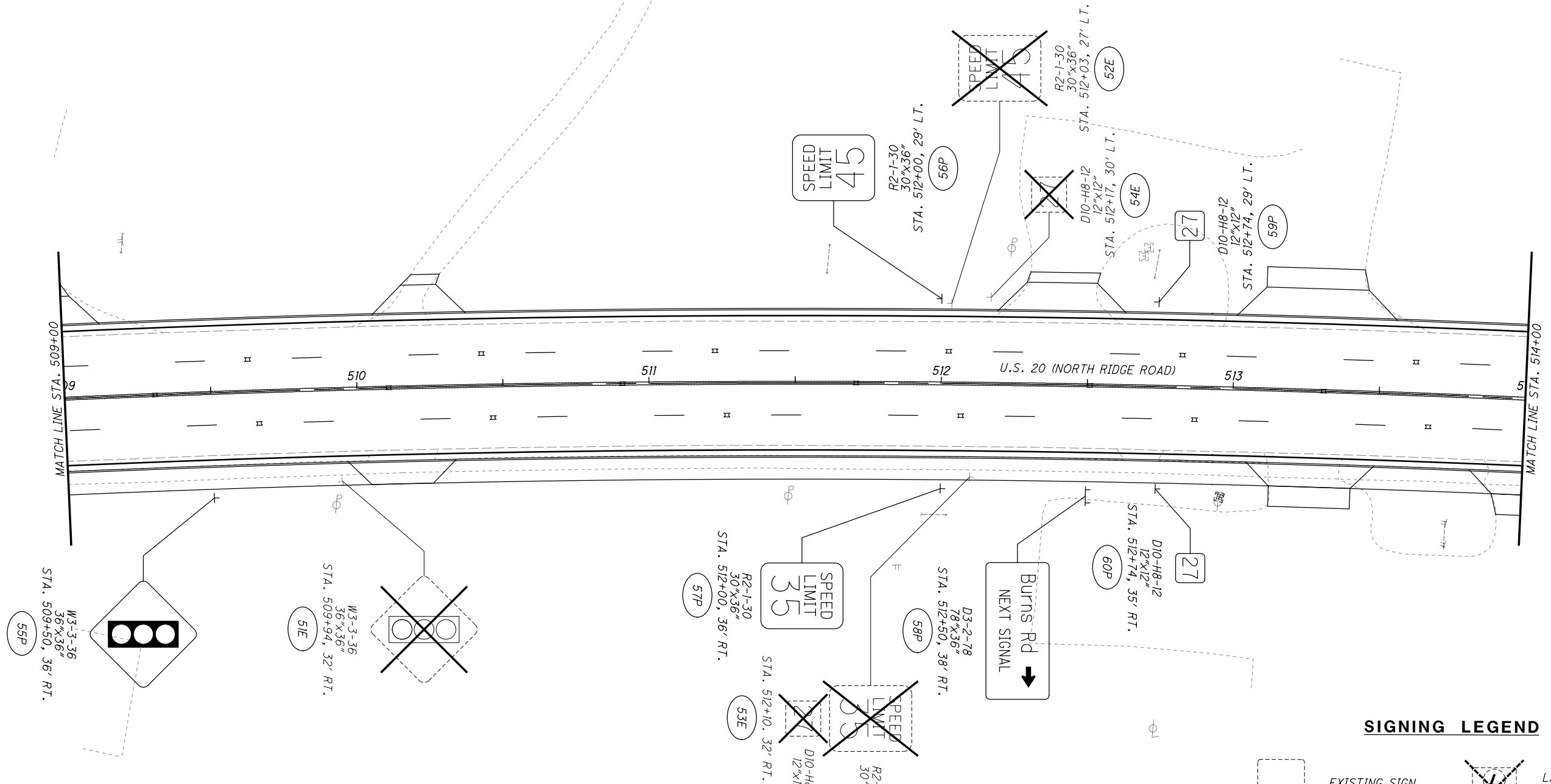


CALCULATED JDC CHECKED EJT




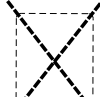

0 20 40
HORIZONTAL SCALE IN FEET

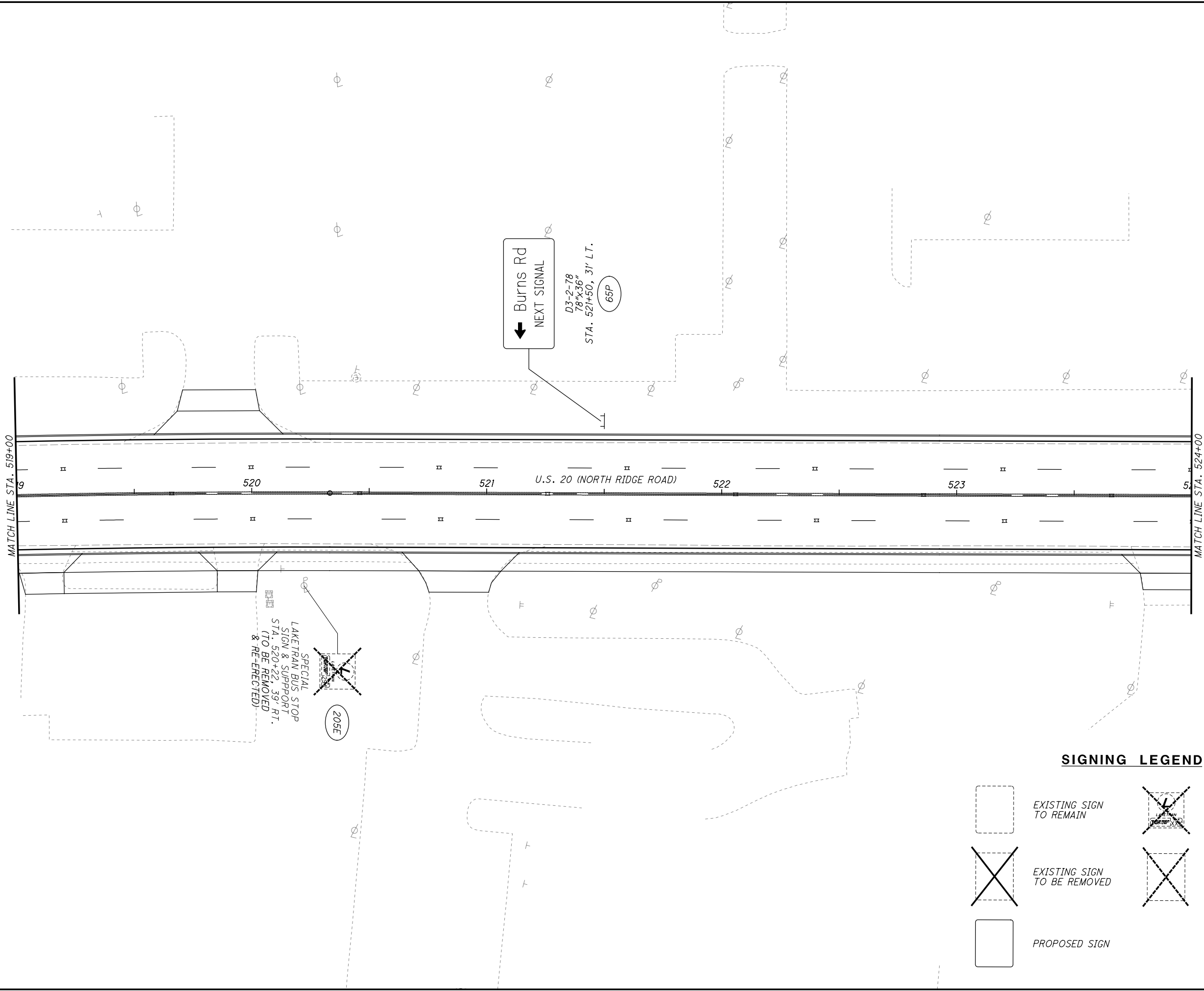


SIGNING PLAN
STA. 504+00 TO STA. 509+00


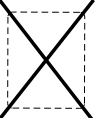


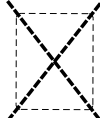


SIGNING LEGEND

	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		



SIGNING LEGEND

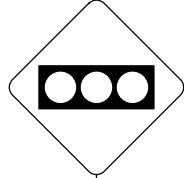
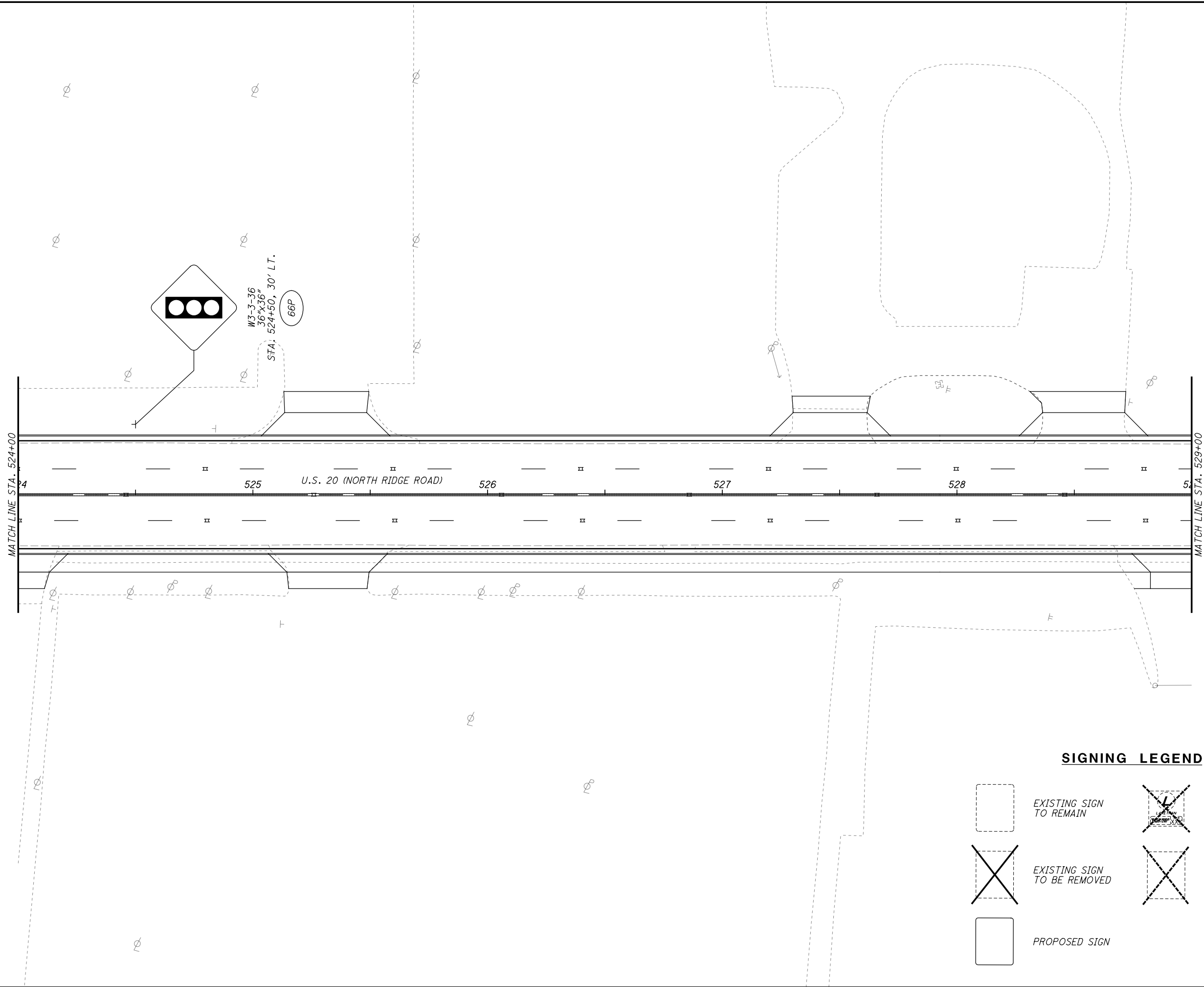
-  EXISTING SIGN TO REMAIN
-  EXISTING SIGN TO BE REMOVED
-  PROPOSED SIGN
-  LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
-  EXISTING SIGN TO BE REMOVED & RE-ERECTED

CALCULATED JDC
CHECKED EJT




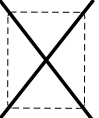


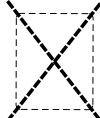

0 20 40
HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 519+00 TO STA. 524+00



W3-3-36
36"x36"
STA: 524+50, 30' L.T.
66P

SIGNING LEGEND

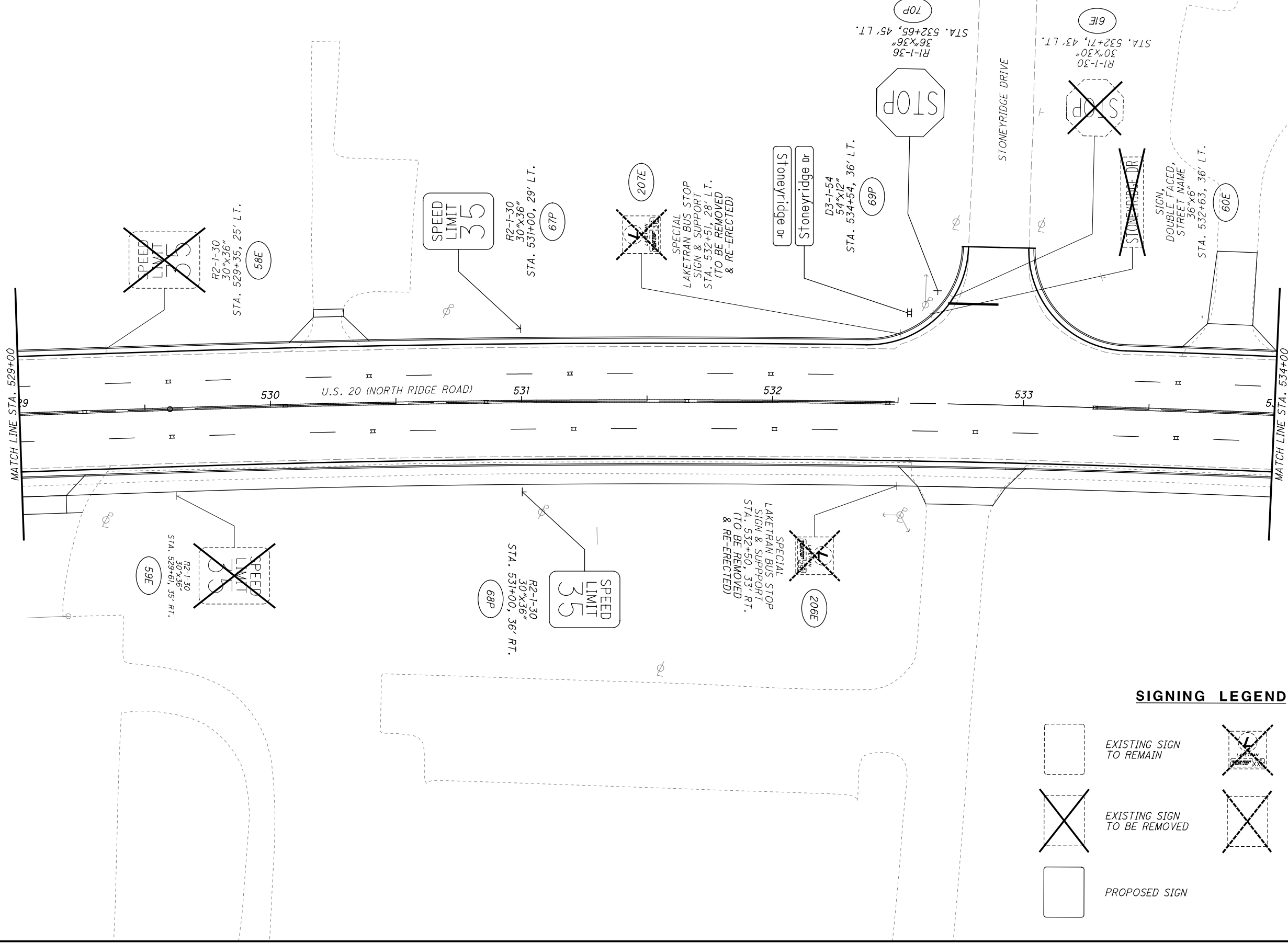
-  EXISTING SIGN TO REMAIN
-  EXISTING SIGN TO BE REMOVED
-  PROPOSED SIGN
-  LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
-  EXISTING SIGN TO BE REMOVED & RE-ERECTED

CALCULATED JDC
CHECKED EJT

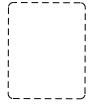

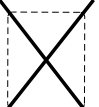
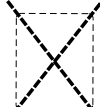
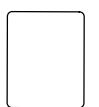



HORIZONTAL SCALE IN FEET

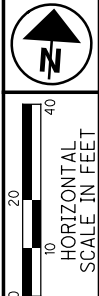
SIGNING PLAN
STA. 524+00 TO STA. 529+00



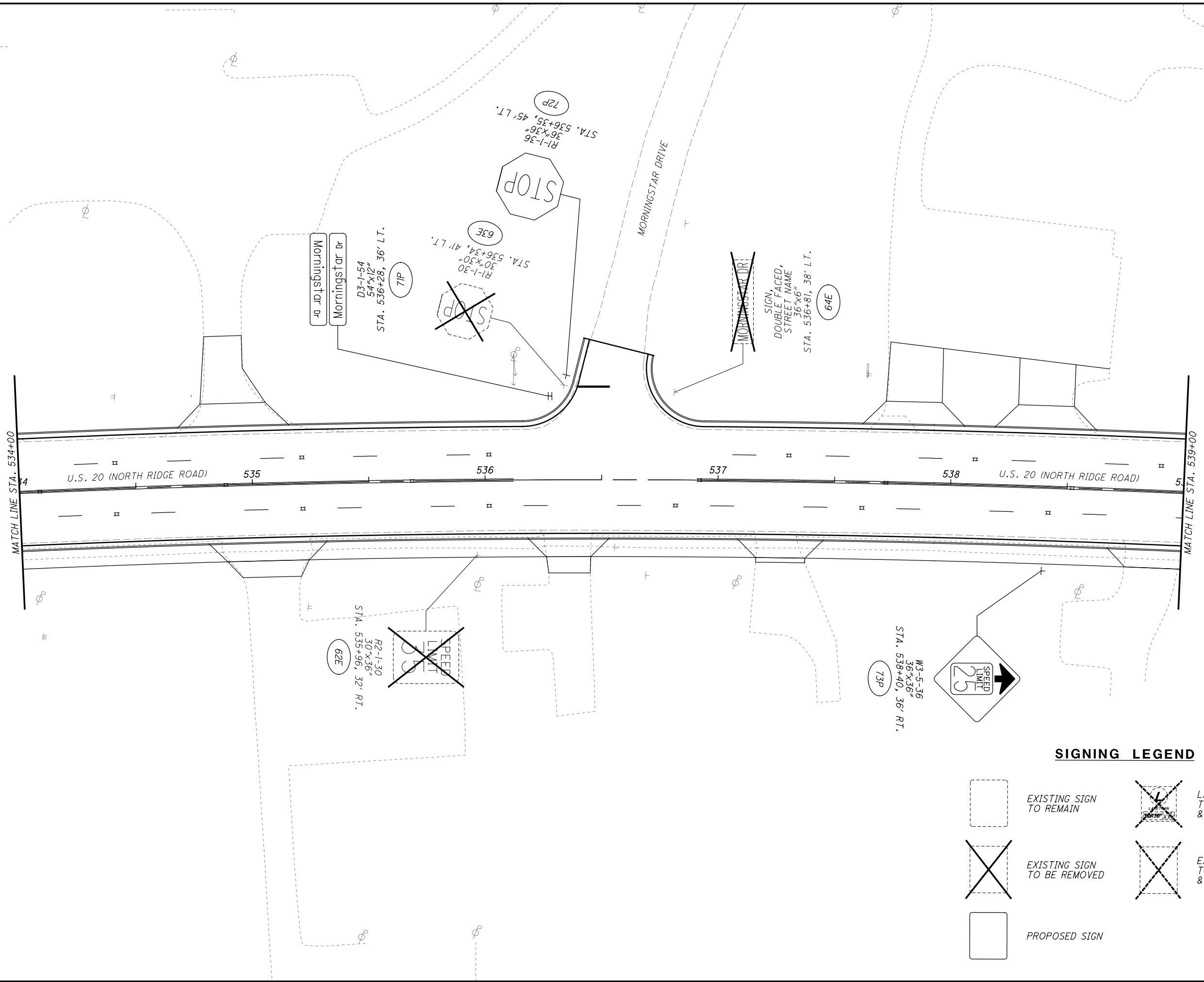
SIGNING LEGEND

- | | | | |
|---------------------------------------------------------------------------------------|-----------------------------|---------------------------------------------------------------------------------------|------------------------------------------|
|  | EXISTING SIGN TO REMAIN |  | LAKETRAN SIGN TO BE REMOVED & RE-ERECTED |
|  | EXISTING SIGN TO BE REMOVED |  | EXISTING SIGN TO BE REMOVED & RE-ERECTED |
|  | PROPOSED SIGN | | |

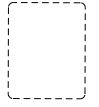

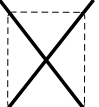
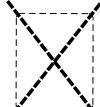
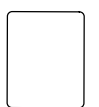
CALCULATED 0
 JDC
 CHECKED EJT



SIGNING PLAN
STA. 529+00 TO STA. 534+00



SIGNING LEGEND

	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		




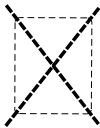
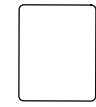
CALCULATED JDC CHECKED EJT

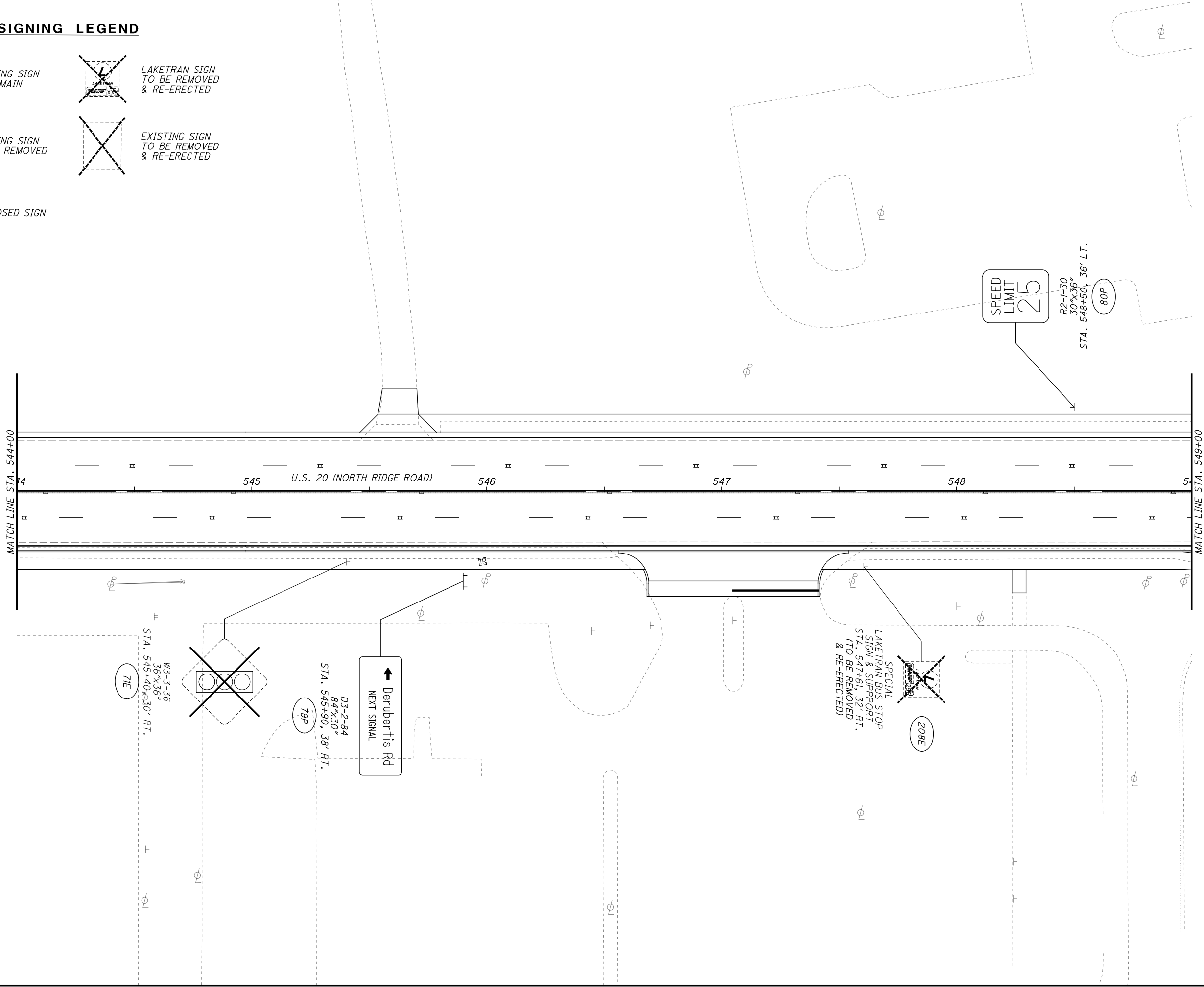



0 20 40
HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 534+00 TO STA. 539+00

SIGNING LEGEND

	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		



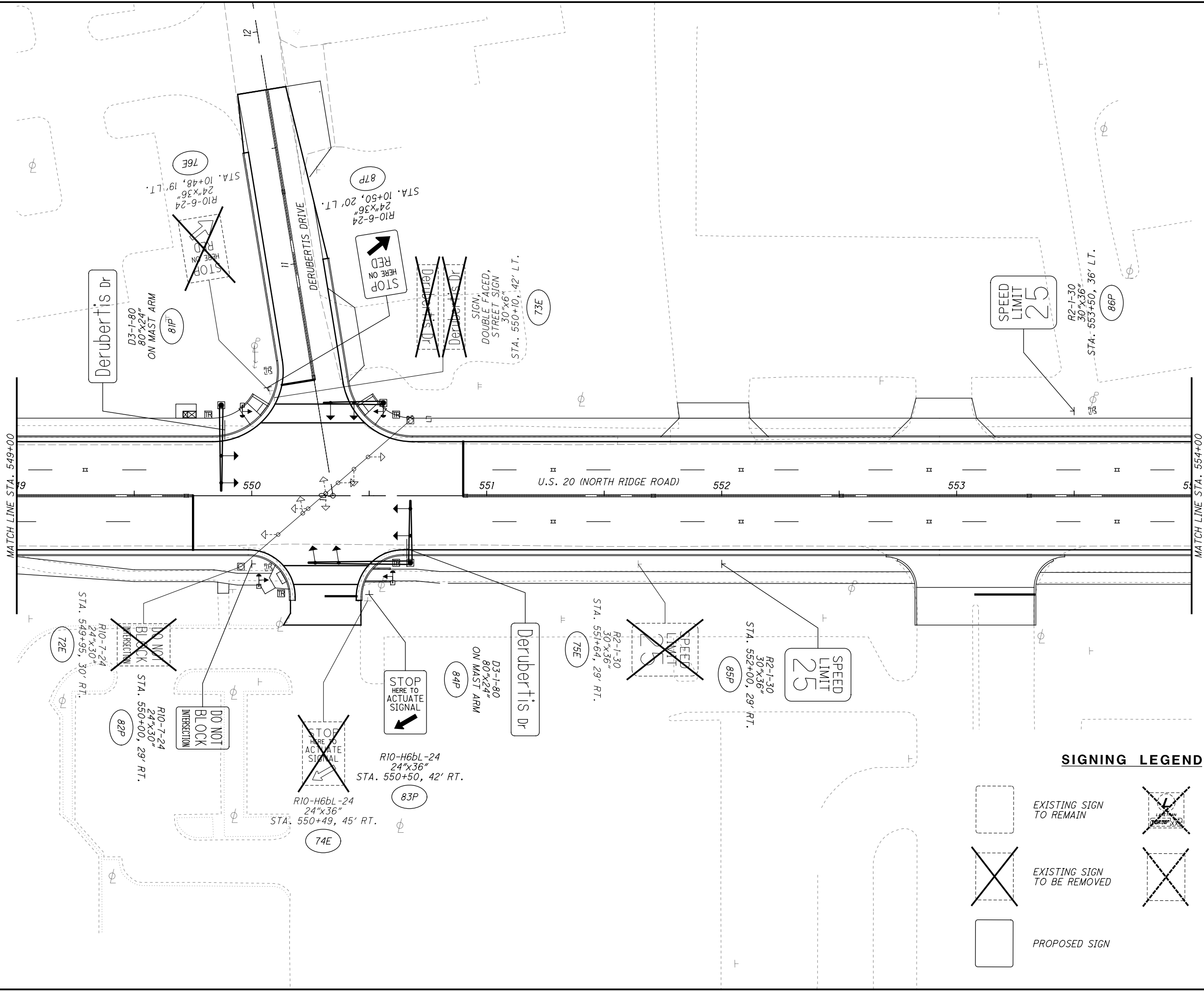
CALCULATED
JDC
CHECKED
EJT

0 20 40
10
HORIZONTAL
SCALE IN FEET






SIGNING PLAN

STA. 544+00 TO STA. 549+00

LAK-US-20-24.99
PART 2



SIGNING LEGEND

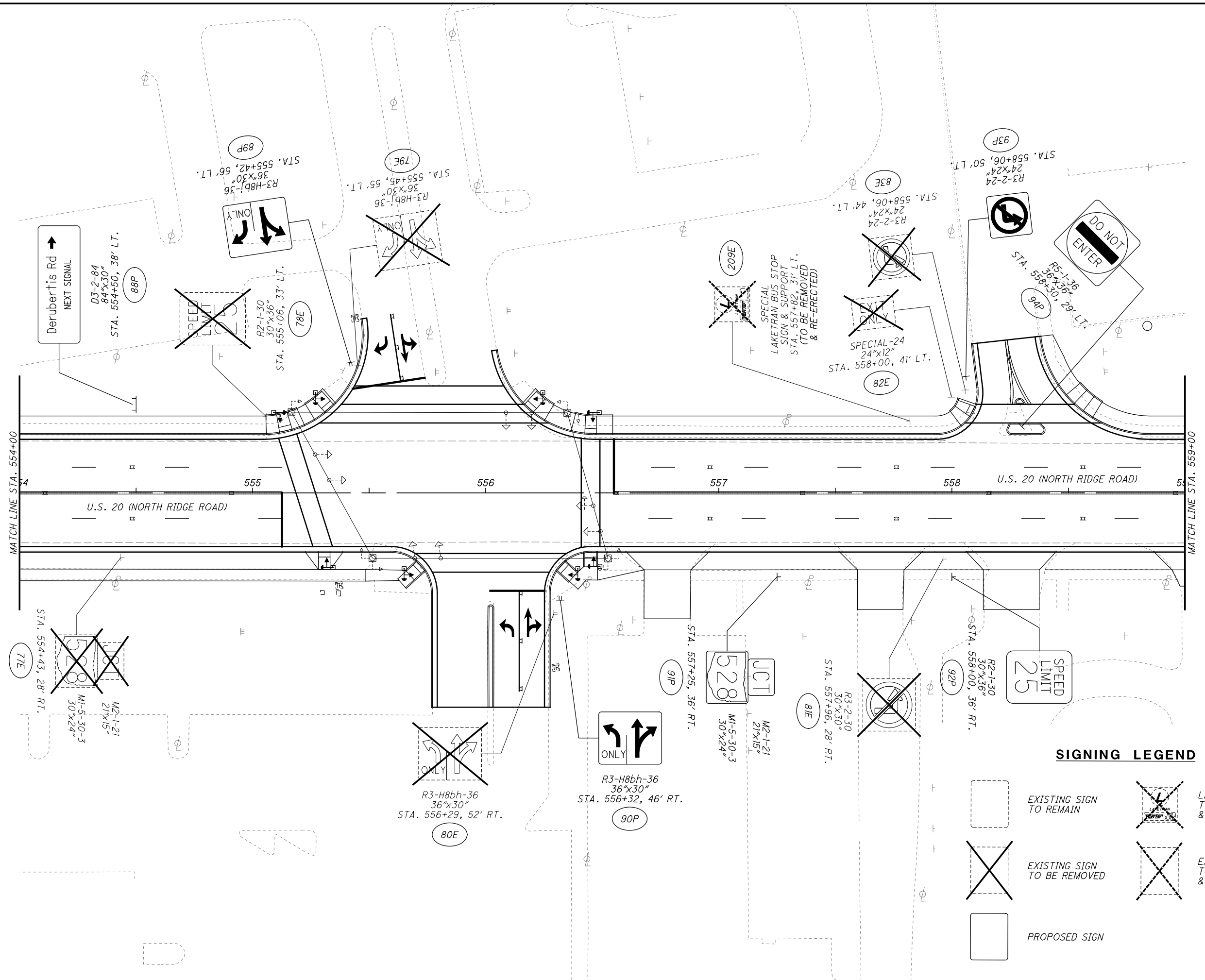
	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		

CALCULATED JDC CHECKED EJT




HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 549+00 TO STA. 554+00



SIGNING LEGEND

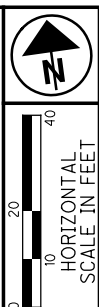
	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		

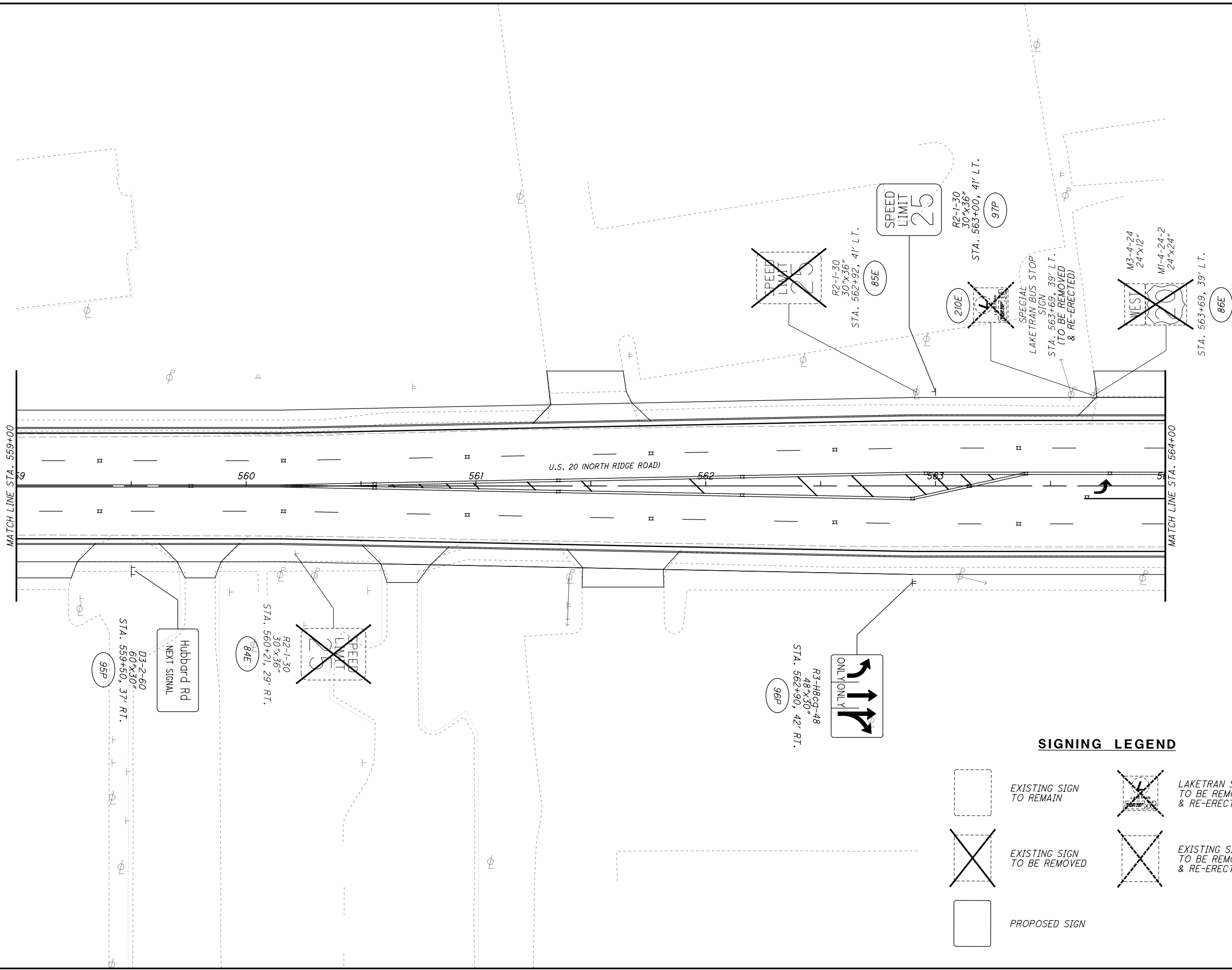
CALCULATED 0
 JDC
 CHECKED EJT

SIGNING PLAN
STA. 554+00 TO STA. 559+00






LAK-US-20-24.99
PART 2

569
697





SIGNING LEGEND

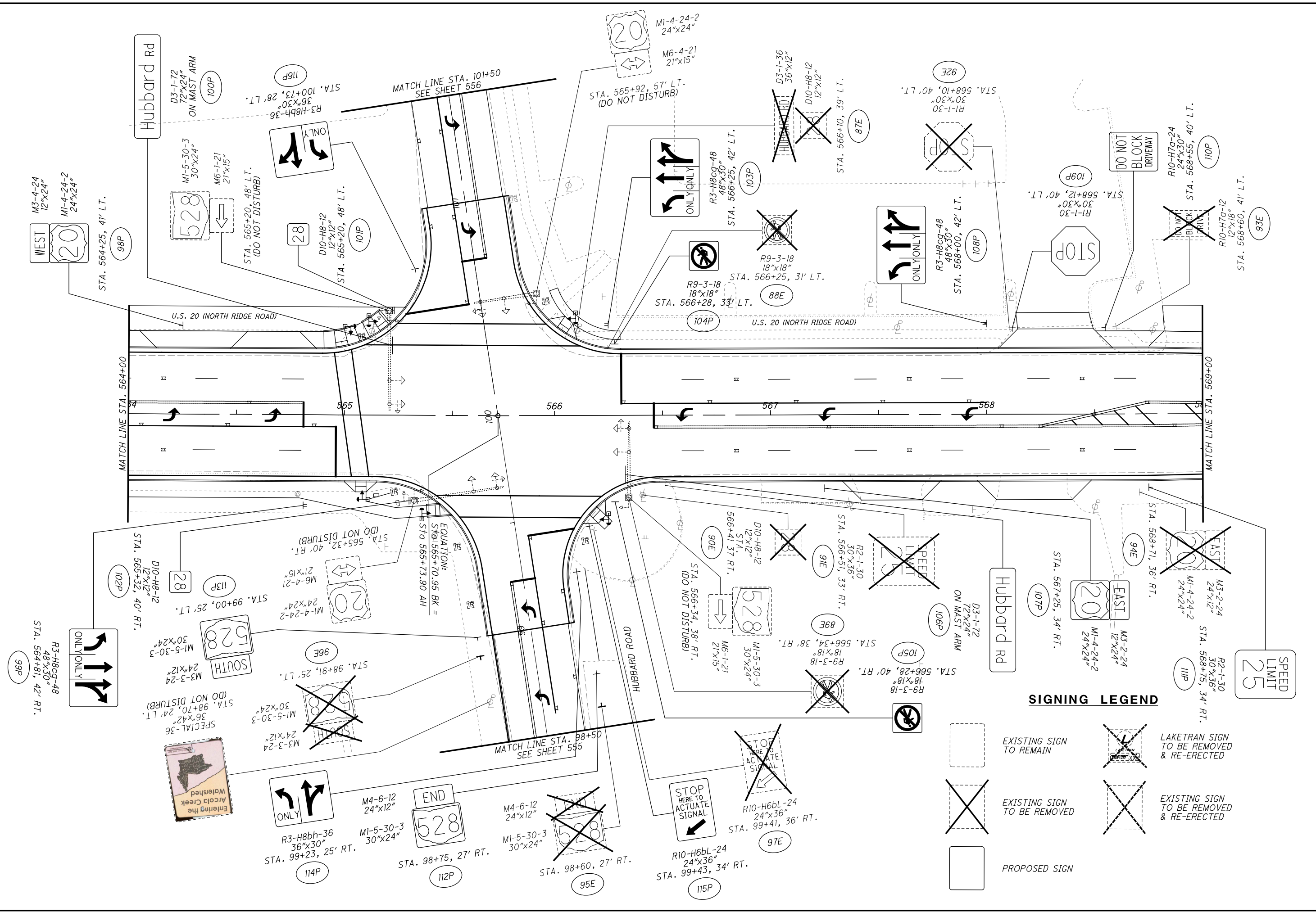
	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		

CALCULATED 0
JDC
CHECKED EJT

0 20 40
HORIZONTAL SCALE IN FEET



SIGNING PLAN
STA. 559+00 TO STA. 564+00



SIGNING LEGEND

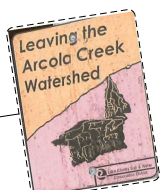
	EXISTING SIGN TO REMAIN		EXISTING SIGN TO BE REMOVED
	PROPOSED SIGN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\Traffic\Sheets\09270_IP092A.dgn Sheet 7/15/2022 3:00:23 PM Troyer

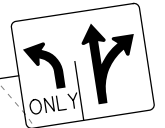
MATCH LINE STA. 98+50
SEE SHEET 554

MATCH LINE STA. 95+00

MATCH LINE STA. 91+50



SPECIAL-36
36"x42"
STA. 98+42, 35' RT.
(DO NOT DISTURB)



R3-H8bh-36
36"x30"
STA. 97+60, 25' RT.

128P

M2-1-21
21"x15"

M1-4-24-2
24"x24"

STA. 97+21, 33' LT.

108E

R3-H8bh-36
36"x30"
STA. 97+21, 25' LT.

107E

STA. 94+51, 27' LT.
R2-1-30
30"x36"

105E

STA. 93+10, 25' LT.
R2-1-30
30"x36"

127P

SPEED LIMIT
40

STA. 92+60, 28' LT.
(DO NOT DISTURB)

I-SPECIAL-30
30"x18"
ARCOLA
CREEK
I-3-30
30"x18"



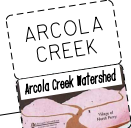
STA. 92+52, 28' RT.
R2-1-30
30"x36"

104E

SPEED LIMIT
35

STA. 98+00, 25' LT.
R2-1-30
30"x36"

126P

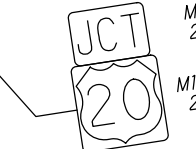


I-SPECIAL-30
30"x18"
ARCOLA
CREEK
I-3-30
30"x18"

STA. 91+90, 34' RT.
(DO NOT DISTURB)

STA. 91+85, 28' RT.
W3-3-36
36"x36"

125P



STA. 89+85, 28' RT.

124P

M2-1-21
21"x15"
M1-4-24-2
24"x24"

SIGNING LEGEND

	EXISTING SIGN TO REMAIN		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO BE REMOVED		EXISTING SIGN TO BE REMOVED & RE-ERECTED
	PROPOSED SIGN		

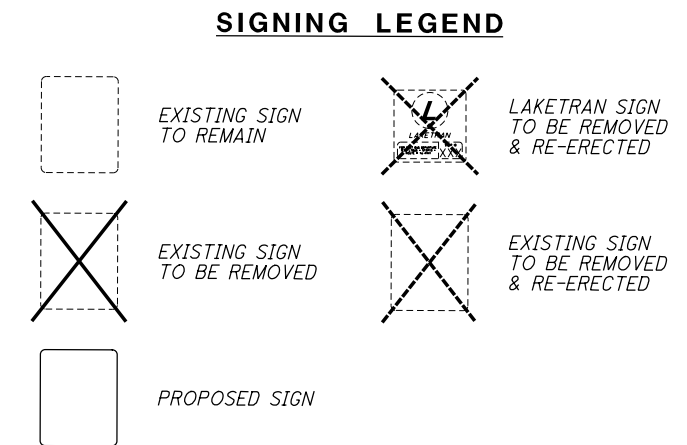
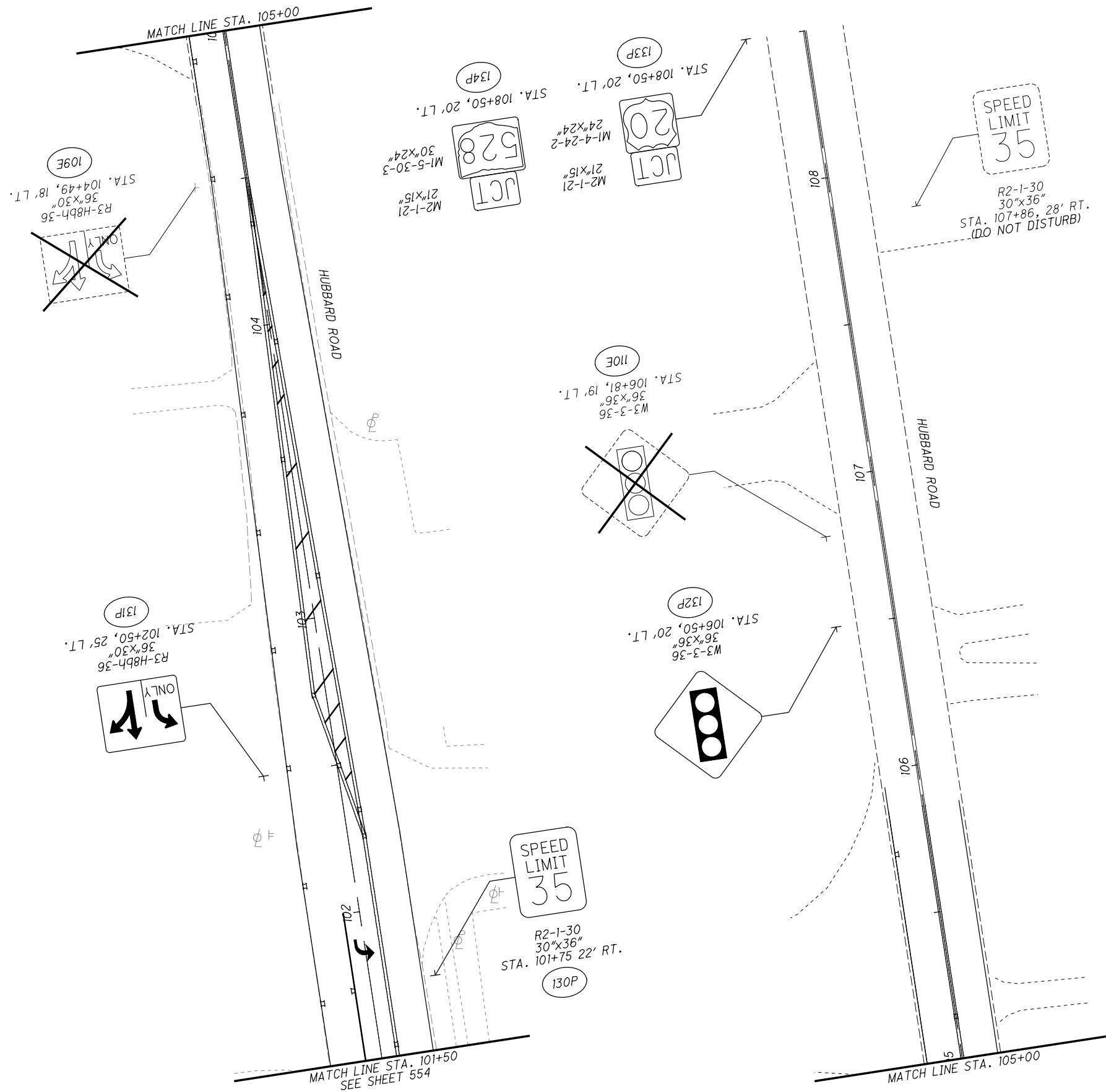
CALCULATED 0
JDC
CHECKED EJT

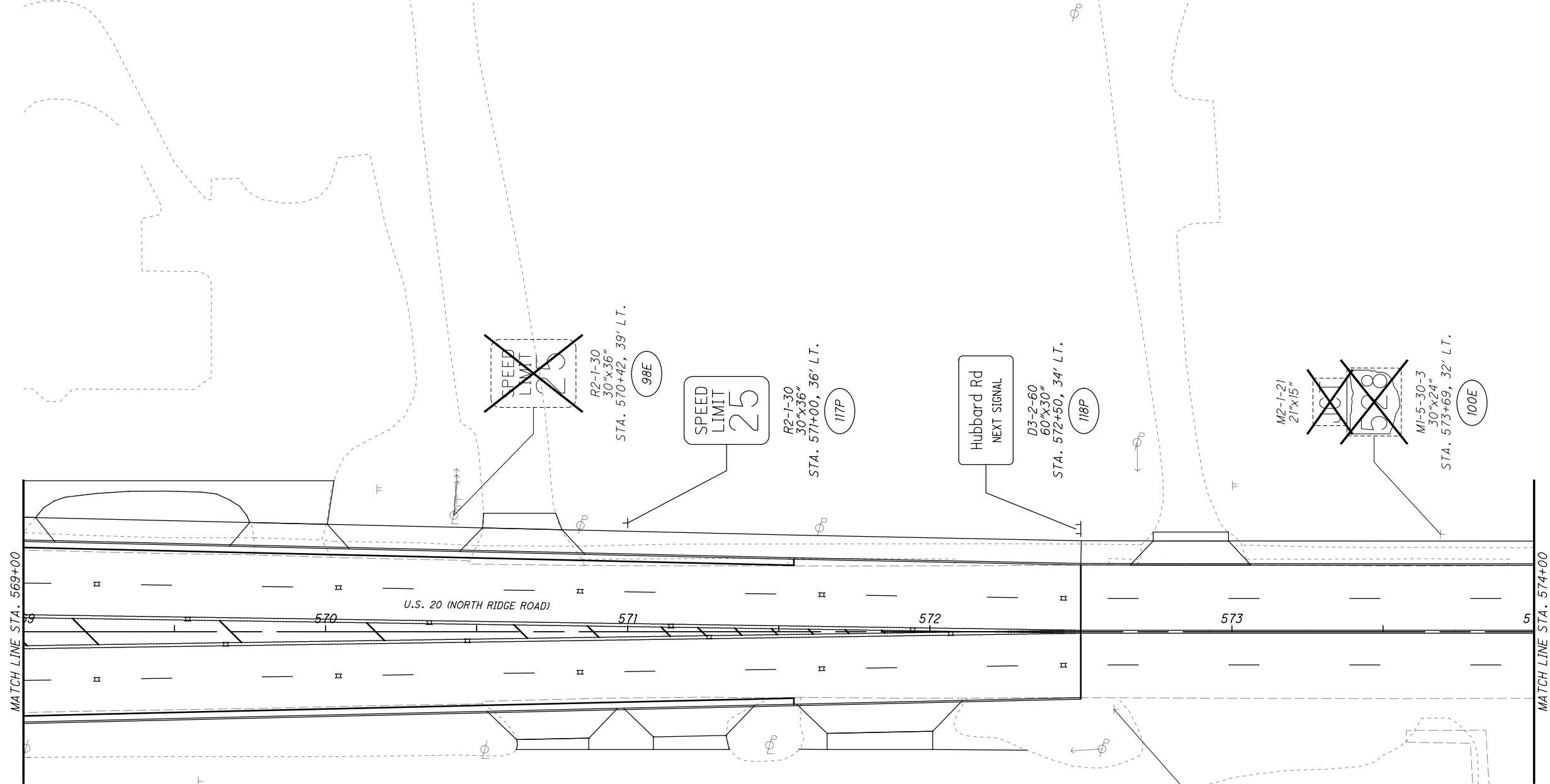
0 20 40
HORIZONTAL SCALE IN FEET

SIGNING PLAN STA. 89+50 TO STA. 91+50

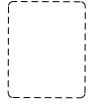
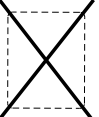
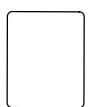

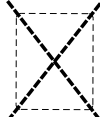
LAK-US-20-24.99 PART 2

572
697





SIGNING LEGEND

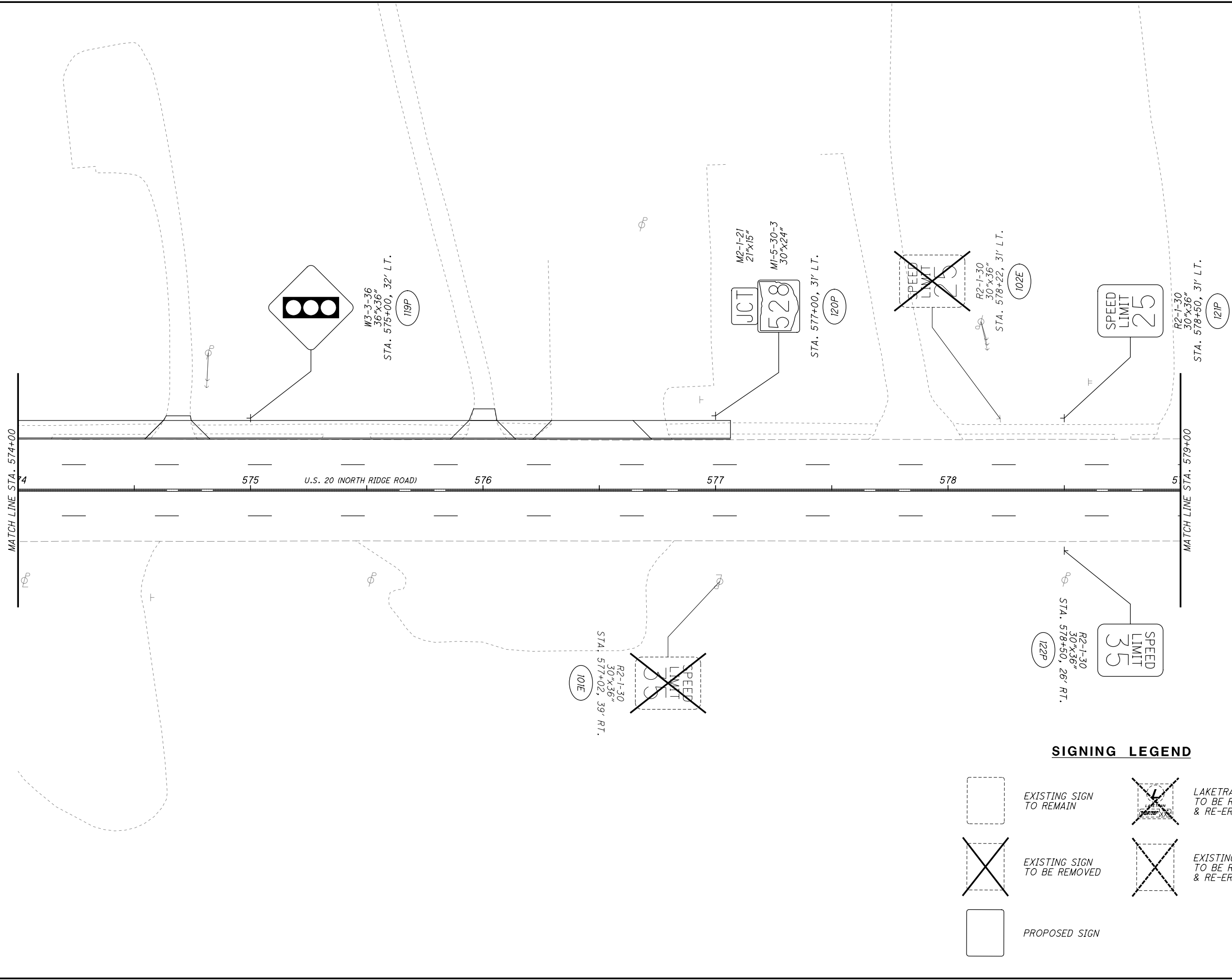
-  EXISTING SIGN TO REMAIN
-  EXISTING SIGN TO BE REMOVED
-  PROPOSED SIGN
-  LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
-  EXISTING SIGN TO BE REMOVED & RE-ERECTED

CALCULATED JDC
CHECKED EJT




HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 569+00 TO STA. 574+00



SIGNING LEGEND

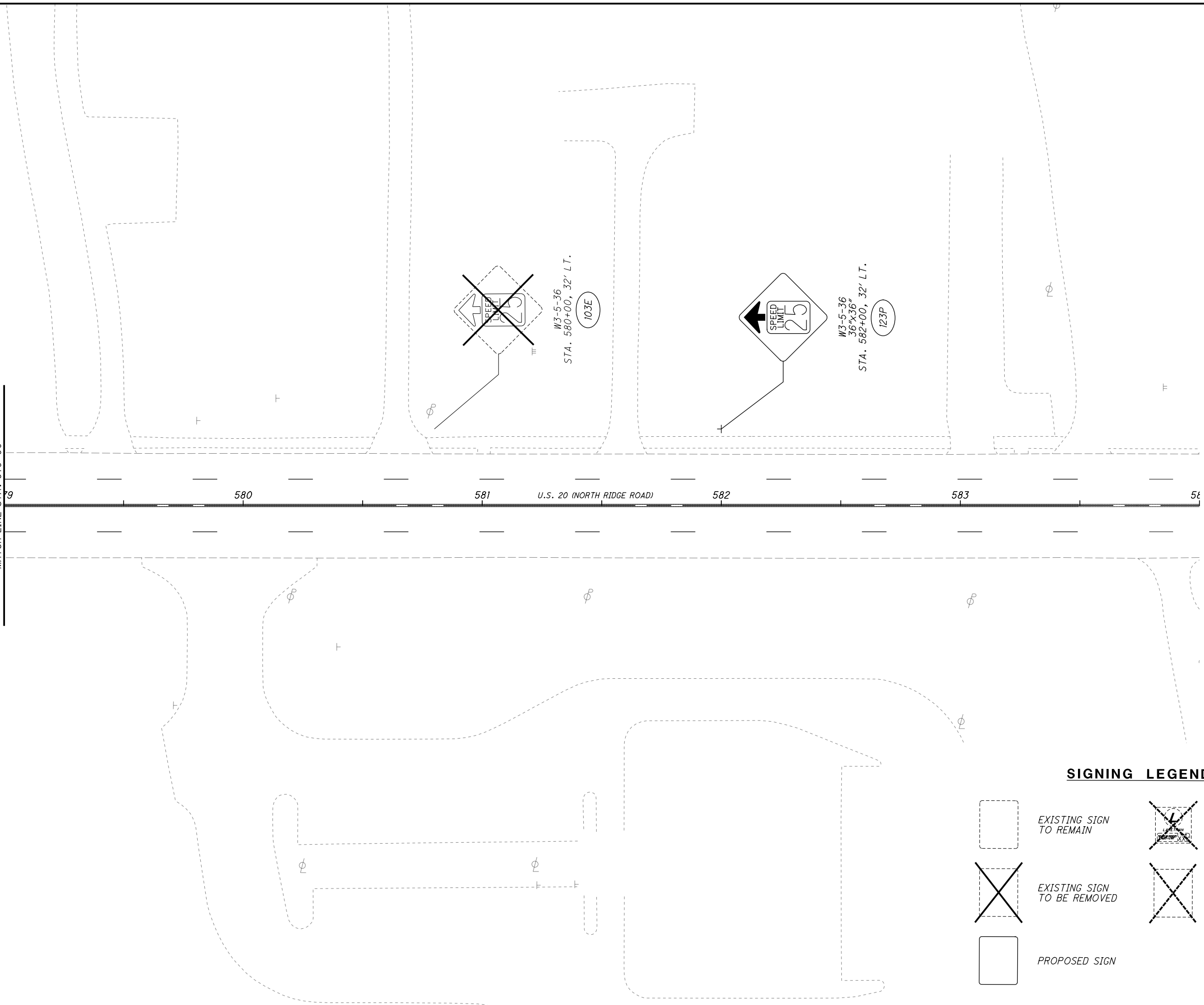
	PROPOSED SIGN		EXISTING SIGN TO BE REMOVED		LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
	EXISTING SIGN TO REMAIN		EXISTING SIGN TO BE REMOVED & RE-ERECTED		

CALCULATED JDC
CHECKED EJT


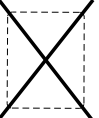


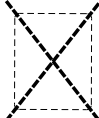
0 20 40
HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 574+00 TO STA. 579+00

MATCH LINE STA. 579+00



SIGNING LEGEND

-  EXISTING SIGN TO REMAIN
-  EXISTING SIGN TO BE REMOVED
-  PROPOSED SIGN
-  LAKETRAN SIGN TO BE REMOVED & RE-ERECTED
-  EXISTING SIGN TO BE REMOVED & RE-ERECTED

CALCULATED
JDC
CHECKED
EJT




HORIZONTAL SCALE IN FEET

SIGNING PLAN
STA. 579+00 TO STA. 584+00

POWER SUPPLY FOR TRAFFIC SIGNALS

ELECTRIC POWER SHALL BE OBTAINED FROM FIRST ENERGY AT THE LOCATION INDICATED ON THE PLANS. POWER SUPPLIED SHALL BE 120 VOLTS.

SIGNAL ACTIVATION

PRIOR TO ACTIVATING THE NEW TRAFFIC SIGNAL TO STOP-AND-GO MODE AND/OR REMOVING THE EXISTING TRAFFIC SIGNAL FROM SERVICE, ALL ITEMS IN THE PROPOSED SIGNAL PLAN SHALL BE FULLY COMPLETED, (I.E., VEHICLE DETECTION, PEDESTRIAN SIGNAL HEADS, ETC). IF THERE ARE CONSTRUCTABILITY ISSUES (I.E., ROADWAY WIDENING, ETC.) THAT PREVENT THE SIGNAL FROM BEING COMPLETED PRIOR TO ACTIVATION, IT SHALL BE BROUGHT TO THE ATTENTION OF THE PROJECT ENGINEER AND DISTRICT TRAFFIC ENGINEER. THE DISTRICT TRAFFIC ENGINEER WILL THEN REVIEW, APPROVE OR REJECT PROPOSALS TO ACTIVATE THE TRAFFIC SIGNAL PRIOR TO COMPLETION.

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AND DISTRICT TRAFFIC ENGINEER AT LEAST 10 WORKING DAYS PRIOR TO SCHEDULING THE FINAL INSPECTION OF THE SIGNAL INSTALLATION. FINAL INSPECTION IS NOT CONSIDERED COMPLETE UNTIL DESIGNATED DISTRICT TRAFFIC PERSONNEL INSPECT THE TRAFFIC SIGNAL AND ISSUE WRITTEN APPROVAL. IF ISSUES ARE FOUND DURING THE FINAL INSPECTION THAT EFFECT THE SAFETY OF THE TRAVELING PUBLIC AND/OR THE EFFICIENCY OF THE INTERSECTION, THE SIGNAL SHALL NOT BE ACTIVATED ON THE PROPOSED DATE. ANY PUNCH LIST ITEMS THAT ARE FOUND SHALL BE CORRECTED AND REINSPECTED BY DISTRICT TRAFFIC PERSONNEL PRIOR TO FINAL ACCEPTANCE. ODOT FORCES SHALL ONLY ASSUME DAY TO DAY MAINTENANCE OF THE TRAFFIC SIGNAL AFTER FINAL WRITTEN ACCEPTANCE HAS BEEN ISSUED.

GUARANTEE

THE CONTRACTOR SHALL GUARANTEE THAT THE TRAFFIC CONTROL SYSTEM INSTALLED AS PART OF THIS CONTRACT SHALL OPERATE SATISFACTORILY FOR A PERIOD OF 90 DAYS FOLLOWING COMPLETION OF THE 10-DAY PERFORMANCE TEST. IN THE EVENT OF UNSATISFACTORY OPERATION, THE CONTRACTOR SHALL CORRECT FAULTY INSTALLATIONS, MAKE REPAIRS AND REPLACE DEFECTIVE PARTS WITH NEW PARTS OF EQUAL OR BETTER QUALITY.

EQUIPMENT, MATERIAL AND LABOR COSTS INCURRED IN CORRECTING AN UNSATISFACTORY OPERATION SHALL BE BORNE BY THE CONTRACTOR.

THE GUARANTEE SHALL COVER THE FOLLOWING ITEMS OF THE TRAFFIC CONTROL SYSTEM: CONTROLLER, CABINET, UNINTERRUPTIBLE POWER SUPPLY, VEHICLE DETECTION EQUIPMENT, LED LAMP UNITS, NETWORK AND COMMUNICATION/ INTERCONNECT EQUIPMENT.

CUSTOMARY MANUFACTURER'S GUARANTEES FOR THE FOREGOING ITEMS SHALL BE TURNED OVER TO THE STATE OR THE MAINTAINING AGENCY FOLLOWING ACCEPTANCE OF THE EQUIPMENT.

THE COST OF GUARANTEEING THE TRAFFIC CONTROL SYSTEM WILL BE INCIDENTAL TO AND INCLUDED IN THE CONTRACT UNIT PRICE OF THE VARIOUS ITEMS MAKING UP THE SYSTEM.

DETECTION MAINTENANCE

IF VEHICLE DETECTION BECOMES UNEXPECTEDLY DISABLED, REQUIRES MODIFICATION, OR IS SCHEDULED TO BE TEMPORARILY REMOVED DURING THE CONSTRUCTION PROJECT, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE PROJECT ENGINEER AND DISTRICT TRAFFIC ENGINEER.

IF THE LOSS OF VEHICLE DETECTION IS KNOWN PRIOR TO THE START OF CONSTRUCTION, IT SHALL BE DISCUSSED AT THE PRECONSTRUCTION MEETING. AT SUCH TIME, THE DISTRICT TRAFFIC ENGINEER SHALL ADVISE THE PROJECT ENGINEER AND CONTRACTOR ON THE APPROPRIATE ACTION TO RECTIFY ANY LOSS OF VEHICLE DETECTION. THIS MAY INCLUDE PLACING THE TRAFFIC SIGNAL ON MINIMUM OR MAXIMUM RECALL, MODIFYING THE MINIMUM GREEN TIMES, AND REMOVING THE MALFUNCTIONING DETECTION FROM SERVICE. WHERE NON-INTRUSIVE DETECTION (I.E. VIDEO, RADAR) ALREADY EXISTS, THE CONTRACTOR SHALL INSURE THAT DETECTION IS OPERATING AND MAINTAINED BY RECONFIGURING THE DETECTION UNITS ACCORDINGLY DURING ALL CONSTRUCTION PHASES. THIS IS TO AVOID THE SIGNAL FROM MAXING OUT THE EFFECTED SIGNAL PHASE AND CREATING UNNECESSARY DELAYS.

LOCATIONS WHERE NON-INTRUSIVE DETECTION IS PROPOSED AND THE EXISTING VEHICLE DETECTION IS TO BE ABANDON, THE NON-INTRUSIVE VEHICLE DETECTION SHALL BE INSTALLED, CONFIGURED AND MADE FULLY FUNCTIONAL PRIOR TO THE EXISTING DETECTION BEING DISABLED. THE CONTRACTOR SHALL CONTINUE TO MAINTAIN AND MODIFY THE DETECTION UNTIL FINAL ACCEPTANCE OF THE TRAFFIC SIGNAL. THIS IS TO ENSURE VEHICLE DETECTION REMAINS FULLY FUNCTIONAL THROUGHOUT CONSTRUCTION.

632 REMOVAL OF TRAFFIC SIGNAL INSTALLATION

TRAFFIC SIGNAL INSTALLATIONS, INCLUDING SIGNAL HEADS, CABLE, MESSENGER WIRE, STRAIN POLES, CABINET, CONTROLLER, ETC., SHALL BE REMOVED IN ACCORDANCE WITH C&MS 632.26 AND AS INDICATED ON THE PLANS. REMOVED ITEMS SHALL BE REUSED AS PART OF A NEW INSTALLATION ON THE PROJECT OR STORED ON THE PROJECT FOR SALVAGE BY (NAME OF AGENCY RECEIVING STORED ITEMS) IN ACCORDANCE WITH THE LISTING GIVEN HEREIN.

ITEMS TO BE STORED:

- SIGNAL HEADS
- PEDESTRIAN SIGNAL HEADS
- PEDESTRIAN PUSHBUTTONS
- STRAIN POLES (DERUBERTIS)
- UPS & CABINET (DERUBERTIS)
- CONTROLLER & CABINET (DERUBERTIS)

REMOVED ITEMS SHALL BE DELIVERED TO THE NEAREST ODOT FACILITY WHOSE ADDRESS IS LISTED BELOW:

ODOT DISTRICT 12 SIGNAL SHOP, ATTN: LEONARD PAIGE
CONTACT PHONE NUMBER 216-584-2302

25609 EMERY ROAD
WARRENSVILLE, OHIO 44128

IN THE EVENT THE ITEMS STORED ON THE PROJECT FOR SALVAGE BY THE LOCAL AGENCY ARE NOT REMOVED, THE CONTRACTOR SHALL, WHEN DIRECTED BY THE ENGINEER IN WRITING, REMOVE AND DISPOSE OF THE ITEMS AT NO ADDITIONAL COST TO THE PROJECT.

WORK INSPECTION

THE CONTRACTOR SHALL PROVIDE THE PROJECT ENGINEER AND DISTRICT TRAFFIC ENGINEER WITH 72-HOUR NOTICE OF ANY SIGNAL WORK TO BE PERFORMED AT THE INTERSECTION SITE(S) SO THAT INSPECTION SERVICES CAN BE SUPPLIED.

632 PEDESTRIAN SIGNAL HEAD (LED), (COUNTDOWN), TYPE D2, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF C&MS 632 AND 732 THE FOLLOWING SHALL APPLY:

1. SIGNAL HEADS AND VISORS SHALL BE CONSTRUCTED OF BLACK POLYCARBONATE PLASTIC AND MEET ITE SPECIFICATIONS.
2. PROPER EXTERIOR COLORS SHALL BE OBTAINED BY USE OF COLORED PLASTIC MATERIAL RATHER THAN PAINTING.
3. PIPE, SPACERS AND FITTINGS CONSTRUCTED OF POLYCARBONATE PLASTIC MAY BE USED IN LIEU OF GALVANIZED STEEL OR ALUMINUM.
4. THE PEDESTRIAN SIGNAL HEAD SHALL BE OF THE LED COUNTDOWN TYPE.
5. NEW ATTACHMENT HARDWARE AND FITTINGS SHALL BE USED.
6. THE LIGHT EMITTING DIODE (LED) MODULES SHALL MEET THE REQUIREMENTS OF C&MS 732.04-C. THE CONTRACTOR SHALL PROVIDE ODOT, IN WRITING, WITH THE LED MANUFACTURER NAME, SERIAL NUMBER, PART NUMBER, DESCRIPTION OF LAMP, AND DATE OF MANUFACTURE FOR ALL LED UNITS THAT ARE TO BE USED IN THE SIGNAL HEAD PRIOR TO INSTALLATION, FOR ACCEPTANCE AND WARRANTY PURPOSES.

PAYMENT FOR ITEM 632 PEDESTRIAN SIGNAL HEAD (LED), (COUNTDOWN), TYPE D2, AS PER PLAN SHALL BE MADE FOR THE NUMBER OF COMPLETE SIGNAL HEAD FURNISHED AND INSTALLED, INCLUDING ALL LABOR, EQUIPMENT, MATERIALS AND NEW ATTACHMENT HARDWARE.

633 CABINET, TYPE 332L, AS PER PLAN

THE CABINET SHALL BE FURNISHED AND INSTALLED ACCORDING TO CMS 633 AND 733, AND BE LISTED ON THE TRAFFIC AUTHORIZED PRODUCTS LIST (TAP).

THE CABINET SHALL BE FURNISHED WITH AN EDI MONITOR AS ALLOWED ON THE TAP/APPROVED PRODUCTS LIST.

THE CONTRACTOR SHALL NOT REASSIGN THE CABINET DETECTOR INPUTS IN ORDER TO REDUCE THE NUMBER OF 2-CHANNEL DETECTOR UNITS SUPPLIED AND SHALL USE THE STANDARD CALTRANS INPUT FILE DESIGNATIONS FOLLOWING PLAN INSERT SHEET 203324.

PAYMENT FOR ITEM 633 CABINET, TYPE 332L, AS PER PLAN WILL BE AT THE CONTRACT BID PRICE PER EACH COMPLETE AND IN PLACE INCLUDING ALL CONNECTIONS TESTED AND ACCEPTED.

809 ATC CONTROLLER, AS PER PLAN

THE CONTROLLER UNIT SHALL BE FURNISHED AND INSTALLED PER SS 809 AND BE LISTED ON THE TRAFFIC AUTHORIZED PRODUCTS (TAP) LIST.

THE CONTROLLER SHALL BE AN ECONOLITE COBALT AND COMPATIBLE WITH THE CABINET TYPE BEING INSTALLED.

632 SIGNAL SUPPORT FOUNDATION

PRIOR TO ORDERING THE SIGNAL SUPPORTS, THE CONTRACTOR SHALL CONTACT OUPS TO HAVE ALL THE UTILITIES LOCATED IN THE FIELD. THEN THE CONTRACTOR SHALL MEET THE PROJECT ENGINEER TO LOCATE THE PROPOSED SUPPORT LOCATIONS TO INSURE THERE ARE NO CONFLICTS WITH UTILITIES. IF THERE ARE ISSUES, THE PROJECT ENGINEER SHALL PROVIDE GUIDANCE AS TO THE RELOCATION OF THE SUPPORTS.

DUE TO THE FURTHER POSSIBILITY OF CONFLICT WITH EXISTING OR PROPOSED UNDERGROUND OBSTRUCTIONS (INCLUDING THE POSSIBILITY OF UNRECORDED OBSTRUCTIONS) WHICH COULD AFFECT THE LOCATION OF THE FOUNDATION FOR THIS ITEM, AND CONSEQUENTLY, THE DESIGN OF THE SUPPORT AND/OR ARMS, THE CONTRACTOR SHALL NOT PLACE FINAL ORDERS FOR THE ITEM UNTIL THE FOUNDATIONS HAVE BEEN INSTALLED, AT FINAL GRADE, AND THE CONTRACTOR HAS RECEIVED, FROM ENGINEER, WRITTEN NOTICE TO PROCEED WITH THE ORDERS FOR THE ITEM.

IF ANY FOUNDATION LOCATIONS MUST BE ADJUSTED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND MAINTAINING AGENCY, WHO WILL DETERMINE THE REVISED LOCATION AND IF NEEDED, THE SUPPORT DESIGN. THE CONTRACTOR WILL NOT BE RESPONSIBLE FOR DETERMINING THE REVISED DESIGN. THE ENGINEER WILL INFORM THE CONTRACTOR OF ANY CHANGES NECESSARY AND AUTHORIZE THE CONTRACTOR TO ORDER THE SUPPORT.

THE CONTRACTOR SHALL, WHEN DEVELOPING THE PROGRESS SCHEDULE, AND THOSE OF SUBCONTRACTORS, ENSURE THAT THE FOUNDATIONS ARE INSTALLED AT THE EARLIEST TIME AS IS FEASIBLE AND PRACTICAL, AND SHALL INCLUDE SUFFICIENT TIME IN THE PROGRESS SCHEDULE FOR ORDERING, MANUFACTURING, DELIVERY, AND INSTALLATION OF THE SUPPORT ITEMS AFTER THE FOUNDATIONS ARE IN PLACE.

NO PAYMENTS FOR DELIVERED MATERIALS FOR THE FOUNDATION OR SUPPORT ITEMS SHALL BE MADE UNTIL THE FOUNDATIONS ARE IN PLACE, AND IF CHANGES IN THE DESIGN OF THIS ITEM ARE REQUIRED, NO PAYMENT SHALL BE MADE FOR THE ITEMS MANUFACTURED TO THE ORIGINAL DESIGN.

PAYMENT WILL BE AT THE CONTRACT UNIT PRICE AND WILL BE FULL COMPENSATION FOR ALL LABOR, MATERIALS, TOOLS, EQUIPMENT AND OTHER INCIDENTALS NECESSARY FOR EACH SUPPORT FURNISHED, IN PLACE, COMPLETE AND ACCEPTED.

H:\2018\180743\ODOT\ProjectData\09270_LAK_20_EAST\Design\Signals\Sheets\09270_CN001.dgn Sheet 7/15/2022 3:00:27 PM troyer

809 PREEMPTION

THIS ITEM OF WORK SHALL CONSIST OF FURNISHING AND INSTALLING OPTICOM PREEMPTION EQUIPMENT IN THE LOCATIONS AND LOCAL CONTROLLERS AS SHOWN IN THE PLAN. THE PREEMPTION SHALL CONFORM TO ODOT SUPPLEMENTAL SPECIFICATION 809 AND SHALL UTILIZE COMMUNICATIONS TO IDENTIFY THE PRESENCE OF AN EMERGENCY PRIORITY VEHICLE. IT SHALL CAUSE THE TRAFFIC SIGNAL CONTROLLER TO SELECT A PRE-PROGRAMMED PREEMPTION PLAN THAT WILL DISPLAY AND HOLD THE DESIRED SIGNAL PHASE FOR THE DIRECTION OF THE EMERGENCY VEHICLE.

THE COMMUNICATIONS MEDIUM SHALL EMPLOY LIGHT DETECTION DETECTION TECHNIQUES TO DETERMINE AND LOG THE PRESENCE OF THE EMERGENCY VEHICLE. THE SYSTEM SHALL DETECT THE PRESENCE OF THE VEHICLE THROUGH AN EMITTING DEVICE LOCATED ON THE EMERGENCY VEHICLE. THE SYSTEM SHALL ACTIVATE THE PREEMPTION SEQUENCE BY APPLYING A SIGNAL TO ONE OF THE CONTROLLER'S PREEMPT DISCRETE INPUTS. THE SYSTEM SHALL BE COMPLETELY COMPATIBLE WITH THE CONTROLLER.

THE EQUIPMENT SHALL BE SHELF OR RACK MOUNTED AND EASILY REMOVABLE AND REPLACEABLE WITHIN THE CABINET. SUPPLY EQUIPMENT COMPLETELY WIRED IN THE CONTROLLER CABINET AND TESTED. THE SYSTEM SHALL BE CAPABLE OF PREEMPTING AND RECEIVING PRIORITY FOR EACH APPROACH TO THE INTERSECTION. IT SHALL BE POSSIBLE TO DETECT THE EMERGENCY VEHICLE AT LEAST 2000 FEET FROM THE INTERSECTION IN AN 80dB-A NOISE ENVIRONMENT.

ALL PREEMPTION PLANS SHOULD BE PROGRAMMED TO PREVENT THE YELLOW TRAP, UNLESS AS DIRECTED BY THE DISTRICT TRAFFIC ENGINEER. YELLOW TRAP PREVENT WILL FORCE THE TRANSITION THROUGH YELLOW CHANGE AND RED CLEARANCE FOR RESOLUTION OF YELLOW TRAP IF ANY PHASE OPPOSING THE PREEMPTION CLEARANCE PHASE(S) IS ACTIVE AND DISPLAYING A GREEN OR FLASHING YELLOW ARROW INDICATION WHEN THE PREEMPTION PLAN IS ACTIVATED AND THE PREEMPTION CLEARANCE PHASE(S) ARE GREEN.

SUPPLY EACH INTERSECTION SHOWN IN THE PLANS WITH THE FOLLOWING COMPONENTS, EACH BID SEPARATELY:

1. PREEMPT RECEIVING UNIT.
2. PREEMPT DETECTOR CABLE.
3. PREEMPT PHASE SELECTOR ASSEMBLY AND INTERFACE WIRING PANEL.
4. CONFIRMATION LIGHT.

FOR THE LIGHT-ACTIVATED SYSTEM SPECIFIED, THE CONTRACTOR SHALL INVENTORY THE CITY'S EXISTING EMITTERS TO DETERMINE COMPATIBILITY WITH THE PROPOSED SYSTEM. IF EXISTING EMITTERS ARE FOUND TO BE NOT COMPATIBLE, THEN THE CITY SHALL BE SUPPLIED (AT COSTS INCIDENTAL TO THE SYSTEM) WITH THE EMITTERS, TRANSMITTERS, SWITCHES, WIRING AND ALL REQUIRED VEHICLE EQUIPMENT FOR THE FOLLOWING EMERGENCY VEHICLES. THE CITY SHALL BE RESPONSIBLE FOR INSTALLING VEHICLE EQUIPMENT.

THE CITY SHALL BE SUPPLIED WITH SOFTWARE REQUIRED TO CALIBRATE, LOG, AND OPERATE THE SYSTEM. TWO (2) OPERATING AND INSTRUCTION MANUALS SHALL BE SUPPLIED WITH THE SOFTWARE.

THE CONTRACTOR SHALL THOROUGHLY TEST THE INSTALLED SYSTEM. AS A MINIMUM, THE CONTRACTOR SHALL VERIFY THAT ALL CONNECTIONS ARE PROPERLY MADE TO THE CONTROLLER CABINETS. THE CONTRACTOR SHALL CHECK THAT THE RANGE SETTING IS PROPER FOR EACH INTERSECTION. THE CONTRACTOR SHALL DETERMINE THAT ALL PHASE SELECTORS ARE SELECTING THE PROPER PHASE AND TIMING ACCURATELY. THE CONTRACTOR SHALL VERIFY THAT ALL VEHICLE EMITTERS ARE BEING PROPERLY DETECTED.

IF THE PROPOSED PREEMPT SYSTEM IS NOT COMPATIBLE WITH THE EXISTING SYSTEM, THE CONTRACTOR SHALL PROVIDE TRAINING FOR UP TO FIFTEEN (15) PERSONS IN THE OPERATION OF THE SYSTEM. IT SHALL BE PROVIDED WITHIN 48 HOURS OF THE INSTALLATION OF THE SYSTEM. IT SHALL CONSIST OF HANDS-ON INSTRUCTION FOR A MINIMUM OF SIXTEEN (16) HOURS. THE CONTRACTOR SHALL PROVIDE TRAINING FOR UP TO FOUR (4) PERSONS IN THE INSTALLATION AND MAINTENANCE OF THE SYSTEM. IT SHALL CONSIST OF A MINIMUM OF EIGHT (8) HOURS OF INSTRUCTION. TRAINING SHALL BE SUPPLIED WITHIN SEVEN (7) DAYS OF THE INSTALLATION OF THE SYSTEM. ALL TRAINING SHALL BE HELD IN A CITY SUPPLIED LOCATION. TRAINING SHALL BE CONDUCTED BY SOMEONE WHO HAS PERFORMED THIS WITHIN THE LAST YEAR AND DOES IT ON A REGULAR BASIS. THE COST OF TRAINING, INCLUDING COURSE MATERIAL, TRAVEL SUBSISTENCE AND RELATED COSTS, SHALL BE ENTIRELY BORNE BY THE CONTRACTOR AND SHALL BE INCIDENTAL TO THE PREEMPTION EQUIPMENT.

PAYMENT FOR ITEM 809 PREEMPTION SHALL BE MADE AT THE CONTRACT UNIT PRICE FOR EACH PREEMPTION IN PLACE AND FULLY OPERATIONAL AS SHOWN IN THE PLANS, EXCEPT FOR THOSE ITEMS BID SEPARATELY.

809 PREEMPTION DETECTOR CABLE

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING PREEMPTION DETECTOR HOME RUN CABLE IN THE LOCATIONS SHOWN IN THE PLANS. IT SHALL CONNECT THE PREEMPT RECEIVING UNITS TO THE PHASE SELECTORS IN THE LOCAL CONTROLLER CABINET.

PREEMPTION DETECTOR CABLE SHALL CONFORM TO ODOT SPECIFICATION 632. ONLY ONE EXTERNAL SPLICE SHALL BE PERMITTED BETWEEN PREEMPTION RECEIVER UNIT AND CONTROLLER CABINET. THIS SPLICE SHALL MEET THE REQUIREMENTS OF C&MS 632.23 USING A WATERPROOF EPOXY SPLICE KIT. THE CABLE SHALL BE APPROVED FOR BOTH OVERHEAD AND UNDERGROUND USE. THE JACKET SHALL WITHSTAND EXPOSURE TO SUNLIGHT AND ATMOSPHERIC TEMPERATURES AND STRESSES REASONABLY EXPECTED IN NORMAL INSTALLATIONS.

PAYMENT FOR ITEM 809 PREEMPTION DETECTOR CABLE SHALL BE MADE AT THE CONTRACT UNIT PRICE PER FOOT FOR THE CABLE FURNISHED, IN PLACE, ALL CONNECTIONS MADE AND WIRING COMPLETED, TESTED AND ACCEPTED.

809 PREEMPT PHASE SELECTOR

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING PREEMPT PHASE SELECTORS INCLUDING WIRING INTERFACE PANELS IN THE LOCAL CONTROLLER CABINET AND ALL OTHER ACCESSORIES THAT ARE NECESSARY TO MAKE THE PREEMPT PHASE SELECTORS COMPLETELY FUNCTIONAL AND OPERATIONAL AS SHOWN IN THE PLANS. THIS ITEM SHALL INCLUDE THE EXTRA CABINET SPACE NECESSARY TO BE LOCATED IN THE LOCAL CONTROLLER CABINETS WHERE INDICATED IN THE PLANS.

THE PHASE SELECTORS SHALL CONSIST OF A MODULE OR MODULES THAT WILL PROVIDE THE NECESSARY INPUTS TO THE CONTROLLER. PHASE SELECTORS SHALL BE SUPPLIED WITH SUFFICIENT QUANTITIES OF CHANNELS TO PROVIDE PREEMPTION FOR ALL APPROACHES TO THE INTERSECTION SEPARATELY. POWER SHALL BE OBTAINED FROM THE PHASE SELECTOR OR PHASE SELECTOR POWER SUPPLY AND NOT FROM THE LOCAL CONTROLLER TIMER.

THE PHASE SELECTORS SHALL HAVE FRONT PANEL INDICATORS FOR ACTIVE PREEMPT CHANNEL STATUS. IT SHALL HAVE TEST SWITCHES TO ACTIVATE ALL PREEMPT CHANNELS.

FURNISH PREEMPT PHASE SELECTORS WITH 60-MONTH WARRANTIES OR FOR THE MANUFACTURER'S STANDARD WARRANTY WHICHEVER IS GREATER. ENSURE THAT THE WARRANTY PERIOD BEGINS ON THE DATE OF SHIPMENT TO THE PROJECT. ENSURE THAT EACH UNIT HAS A PERMANENT LABEL OR STAMP INDICATING THE DATE OF SHIPMENT.

PAYMENT FOR ITEM 809 PREEMPT PHASE SELECTOR SHALL BE MADE AT THE CONTRACT UNIT PRICE FOR EACH PHASE SELECTOR IN PLACE, COMPLETELY INSTALLED IN THE LOCAL CONTROLLER SHOWN IN THE PLANS, WIRED, TESTED AND ACCEPTED.

809 PREEMPT CONFIRMATION LIGHT, LED

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING PREEMPT CONFIRMATION LIGHTS INCLUDING HARDWARE AND ALL OTHER ACCESSORIES THAT ARE NECESSARY TO MAKE THE PREEMPT CONFIRMATION LIGHT COMPLETELY FUNCTIONAL AND OPERATIONAL AS SHOWN IN THE PLANS.

A CONFIRMATION LIGHT SHALL BE SUPPLIED FOR EACH INTERSECTION TO INDICATE THAT THE EMERGENCY VEHICLE HAS ACHIEVED CONTROL OF THE TRAFFIC SIGNAL.

THE CONFIRMATION LIGHT SHALL BE A WEATHER TIGHT LIGHTING FIXTURE. IT SHALL BE SUPPLIED WITH A CLEAR GLOBE, LED LAMP AND MOUNTING HARDWARE TO ATTACH TO THE TRAFFIC SIGNAL MAST ARM. THE CONFIRMATION LIGHT SHALL BE POWERED BY A LOAD SWITCH IN THE TRAFFIC SIGNAL CONTROLLER. SIGNAL CABLE CONFORMING TO 732.19 SHALL BE USED FOR CONFIRMATION LIGHTS. A MINIMUM OF 4-CONDUCTOR CABLE SHALL BE USED WITH THE GREEN WIRE SERVING AS THE SAFETY GROUND CONDUCTOR. PAYMENT FOR ITEM 809 PREEMPT CONFIRMATION LIGHT, LED SHALL BE MADE AT THE CONTRACT UNIT PRICE FOR EACH LIGHT IN PLACE, COMPLETELY INSTALLED IN THE LOCATION SHOWN IN THE PLANS, WIRED, TESTED AND ACCEPTED.

809 PREEMPTION RECEIVING UNIT

RECEIVING UNITS SHALL CONSIST OF A LIGHTWEIGHT, WEATHER-PROOF AND DIRECTIONAL ASSEMBLY. EACH RECEIVING UNIT SHALL BE 360 DEGREE ADJUSTABLE. THE RECEIVING UNIT SHALL BE CAPABLE OF SENDING THE PROPER ELECTRICAL SIGNAL TO THE TRAFFIC SIGNAL CONTROLLER VIA THE PREEMPTION DETECTOR CABLE. RECEIVING UNITS SHALL BE SUPPLIED WITH MAST ARM MOUNTING HARDWARE AS SHOWN IN THE PLANS.

FURNISH PREEMPTION RECEIVING UNITS WITH 60-MONTH WARRANTIES OR FOR THE MANUFACTURER'S STANDARD WARRANTY WHICHEVER IS GREATER. ENSURE THAT THE WARRANTY PERIOD BEGINS ON THE DATE OF SHIPMENT TO THE PROJECT. ENSURE THAT EACH UNIT HAS A PERMANENT LABEL OR STAMP INDICATING THE DATE OF SHIPMENT.

PAYMENT FOR ITEM 809 PREEMPTION RECEIVING UNIT SHALL BE AT THE CONTRACT UNIT FOR EACH RECEIVING UNIT IN PLACE, COMPLETELY INSTALLED AT THE LOCATION SHOWN IN THE PLANS, WIRED, TESTED AND ACCEPTED.

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\Signals\Sheets\09270_CN002.dgn_Sheet 7/15/2022 3:00:38 PM troyer

CALCULATED
JDC
CHECKED
WDB

TRAFFIC SIGNAL GENERAL NOTES

LAK-US-20-24.99
PART 2

578
697

GROUNDING AND BONDING

THE REQUIREMENTS OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS (C&MS) AND THE TC SERIES OF STANDARD CONSTRUCTION DRAWINGS ARE MODIFIED AS FOLLOWS:

1. ALL METALLIC PARTS CONTAINING ELECTRICAL CONDUCTORS SHALL BE PERMANENTLY JOINED TO FORM AN EFFECTIVE GROUND FAULT CURRENT PATH BACK TO THE GROUNDED CONDUCTOR IN THE POWER SERVICE DISCONNECT SWITCH.
 - A. PROVIDE AN EQUIPMENT GROUNDING CONDUCTOR IN METALLIC CONDUITS (725.04) IN ADDITION TO THE CONDUCTORS SPECIFIED AND BOND THE CONDUIT TO THIS GROUNDING CONDUCTOR.
 - B. WHEN AN EQUIPMENT GROUNDING CONDUCTOR IS REQUIRED IN PLASTIC CONDUIT (725.05), THE INSTALLATION SHALL INCLUDE A SEPARATE EQUIPMENT GROUNDING CONDUCTOR IN ADDITION TO THE CONDUCTORS SPECIFIED.
 - C. METALLIC CONDUIT CARRYING THE LOOP WIRES FROM IN THE PAVEMENT TO THE PULL BOX SPLICE LOCATION WILL ONLY BE BONDED AT THE PULL BOX END, AND WILL NOT CONTAIN AN EQUIPMENT GROUNDING CONDUCTOR.
 - D. IF MULTIPLE CONDUIT RUNS BEGIN AND END AT THE SAME POINTS, ONLY ONE EQUIPMENT GROUNDING CONDUCTOR IS REQUIRED.
 - E. IF AN EQUIPMENT GROUNDING CONDUCTOR IS NEEDED IN CONDUIT BETWEEN SIGNALIZED INTERSECTIONS FOR UNDERGROUND INTERCONNECT CABLE, THE GROUNDING SYSTEM FOR EACH SIGNALIZED INTERSECTION WILL BE SEPARATED ABOUT MIDWAY BETWEEN THE INTERSECTIONS.
 - F. THE MESSENGER WIRE AT SIGNALIZED INTERSECTIONS WILL BE USED AS THE CONDUCTIVE PATH FROM CORNER TO CORNER IF CONDUIT IS NOT PROVIDED UNDER THE ROADWAY. WHEN CONDUIT CONNECTS THE CORNERS OF AN INTERSECTION, AN EQUIPMENT GROUNDING CONDUCTOR SHALL BE USED IN THE CONDUIT.
2. CONDUITS.
 - A. THE 725.04 CONDUIT SHALL HAVE GROUNDING BUSHINGS INSTALLED AT ALL TERMINATION POINTS. THE BUSHING MATERIAL SHALL BE COMPATIBLE WITH GALVANIZED STEEL CONDUIT AND THE GROUNDING LUG MATERIAL SHALL BE COMPATIBLE FOR USE WITH COPPER WIRE. THREADED OR COMPRESSION TYPE BUSHINGS MAY BE USED.
 - B. THE 725.05 CONDUIT SHALL HAVE THE INSIDE AND OUTSIDE DIAMETERS OF THE CONDUIT DEBURRED AT ALL TERMINATION POINTS.
 - C. BOTH ENDS OF METALLIC CONDUIT SHALL BE BONDED TO THE EQUIPMENT GROUNDING CONDUCTOR.
 - D. METALLIC CONDUIT MAY BE BONDED TO METALLIC BOXES THROUGH THE USE OF CONDUIT FITTINGS UL APPROVED FOR THIS TYPE OF CONNECTION, WITH THE BOX BONDED TO THE EQUIPMENT GROUNDING CONDUCTOR.

3. WIRE FOR GROUNDING AND BONDING.
 - A. USE INSULATED, COPPER WIRE FOR THE EQUIPMENT GROUNDING CONDUCTOR. BONDING JUMPERS IN BOXES AND ENCLOSURES MAY BE BARE OR INSULATED COPPER WIRE. WIRE SIZE SHALL BE AS FOLLOWS:
 - I. USE 4 AWG BETWEEN THE POWER SERVICE AND SUPPORTS, POLES, PEDESTALS, CONTROLLER OR FLASHER CABINETS.
 - II. USE A MINIMUM 8 AWG BETWEEN LOOP DETECTOR PULL BOXES AND THE FIRST CONDUIT THAT REQUIRES A LARGER SIZE AS SPECIFIED IN 3.A.I ABOVE.
 - III. USE A MINIMUM 8 AWG BETWEEN THE "PREPARE TO STOP WHEN FLASHING" INSTALLATION (INCLUDING SUPPORT) AND THE FIRST CONDUIT THAT REQUIRES A LARGER SIZE AS SPECIFIED IN 3.A.I ABOVE.
 - IV. THE INSULATION SHALL BE GREEN OR GREEN WITH YELLOW STRIPE(S). FOR 4 AWG OR LARGER, INSULATION MAY ALSO BE BLACK WITH GREEN TAPE/LABELS INSTALLED AT ALL ACCESS POINTS.
 - B. IN A HIGHWAY LIGHTING SYSTEM, THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE THE SAME WIRE SIZE AS THE DUCT CABLE OR DISTRIBUTION CABLE CIRCUIT CONDUCTORS, WITH THE MINIMUM CONDUCTOR SIZE OF 4 AWG. BONDING JUMPERS WILL BE MINIMUM SIZE 4 AWG.

4. GROUND ROD.
 - A. A 3/4-INCH SCHEDULE 40 PVC CONDUIT WILL BE USED IN FOUNDATIONS AND CONCRETE WALLS FOR THE GROUNDING CONDUCTOR (GROUND WIRE) RACEWAY TO THE GROUND ROD. SHOULD METALLIC CONDUIT BE USED, BOTH ENDS OF THE CONDUIT SHALL BE BONDED TO THE GROUNDING CONDUCTOR.
 - B. THE TYPICAL GROUNDING CONDUCTOR (GROUND WIRE) SHALL BE 4 AWG INSULATED, COPPER.

5. THE GREEN CONDUCTOR IN SIGNAL CABLES (CONDUCTOR #4) SHALL NOT BE USED TO SUPPLY POWER TO A SIGNAL INDICATION. IT WILL BE CONNECTED TO THE SIGNAL BODY AS AN EQUIPMENT GROUND IN ALUMINUM HEADS AND IT WILL BE UNUSED IN PLASTIC HEADS. UNUSED CONDUCTORS SHALL BE GROUNDED IN THE CABINET. TYPICAL USE OF CONDUCTORS IS AS FOLLOWS:

COND. NO.	COLOR	VEHICLE SIGNAL	PEDESTRIAN SIGNAL
1	BLACK	GREEN BALL	#1 WALK
2	WHITE	AC NEUTRAL	AC NEUTRAL
3	RED	RED BALL	#1 DW/FDW
4	GREEN	EQUIPMENT GROUND	EQUIPMENT GROUND
5	ORANGE	YELLOW BALL	#2 DW/FDW
6	BLUE	GREEN ARROW	#2 WALK
7	WHITE/BLACK	STRIPE YELLOW ARROW	NOT USED

6. POWER SERVICE AND DISCONNECT SWITCH.
 - A. AT THE POWER SERVICE LOCATION, THE GROUNDING CONDUCTOR (GROUND WIRE) FROM THE DISCONNECT SWITCH NEUTRAL (AC-) BAR TO THE GROUND ROD SHALL BE A CONTINUOUS, UNSPLICED CONDUCTOR. IF SPLICED, IT SHALL BE AN EXOTHERMIC WELD BUTT SPLICE.
 - B. THE SERVICE NEUTRAL (AC-) SHALL ONLY BE CONNECTED TO GROUND AT THE PRIMARY POWER SERVICE DISCONNECT SWITCH.
 - I. NEMA CONTROLLER CABINETS: IF A POWER SERVICE DISCONNECT SWITCH IS LOCATED BEFORE THE CONTROLLER CABINET, THE NEUTRAL (AC-) AND THE GROUNDING BARS IN THE CONTROLLER CABINET SHALL NOT BE CONNECTED TOGETHER AS SHOWN IN NEMA TS-2, FIGURE 5-4.
 - II. IF SECONDARY DISCONNECT SWITCHES ARE CONNECTED AFTER THE PRIMARY DISCONNECT SWITCH, THE NEUTRAL (AC-) SHALL ONLY BE GROUNDED AT THE PRIMARY SWITCH. EQUIPMENT GROUNDING CONDUCTORS SHALL BE BROUGHT TO THE PRIMARY SWITCH, BUT SHALL BE GROUNDED AT BOTH SECONDARY AND PRIMARY SWITCHES.
7. PAYMENT - ALL MATERIALS AND WORK REQUIRED TO COMPLETE THE EFFECTIVE GROUND FAULT CURRENT PATH SYSTEM ARE INCIDENTAL TO THE CONDUCTORS INSTALLED BY CONTRACT.

632 SIGNALIZATION MISC.: UNLASH AND RELASH MESSENGER WIRE

THE CONTRACTOR SHALL REMOVE EXISTING MESSENGER WIRE LASHING RODS AND REINSTALL THEM AS NECESSARY FOR THE INSTALLATION OF ANY NEW CABLES ON THE EXISTING INTERSECTION SIGNAL SPANS. THE CABLES SHALL ENTER THE EXISTING STRAIN POLE THROUGH THE POLE CABLE ENTRANCE FITTING AND USE THE EXISTING CONDUIT SYSTEM TO GET TO THE CONTROLLER CABINET. THE NEW CABLES SHALL BE SUPPORTED BY A NEW CABLE SUPPORT ASSEMBLY AT THE TOP OF THE STRAIN POLE.

THE NEW SIGNAL CABLES SHALL BE BID BY SEPARATE BID ITEMS.

PAYMENT FOR ITEM 632 "SIGNALIZATION MISC.: UNLASH AND RELASH MESSENGER WIRE" SHALL BE MADE AT THE CONTRACT UNIT PRICE PER FOOT AND SHALL INCLUDE ALL LABOR, MATERIALS, CABLE SUPPORT ASSEMBLIES AND EQUIPMENT TO INSTALL NEW CABLES ON EXISTING SIGNAL SPAN WIRE INSTALLATIONS.

633 UNINTERRUPTIBLE POWER SUPPLY (UPS), 1000 WATT, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF C&MS 633 AND 733, POLE ATTACHMENT HARDWARE WILL BE INCLUDED FOR POLE-MOUNTED CABINETS, AND A CABINET RISER (8-INCH MINIMUM) AND ANCHOR BOLTS WILL BE PROVIDED FOR BASE-MOUNTED CABINETS. BEFORE PERFORMING THE WORK, THE CONTRACTOR, THE DISTRICT TRAFFIC ENGINEER AND THE PROJECT ENGINEER WILL PERFORM A SITE INSPECTION TO ESTABLISH THE LOCATION OF THE UPS CABINET AND FOUNDATION.

THE UPS CABINET SHALL INCLUDE A GENERATOR POWER PANEL WITH A HEAVY-DUTY POWER RELAY VERSUS THE LINE VOLTAGE GENERATOR SWITCH. THE GENERATOR INLET SHALL BE A RECESSED PANEL WITH A DOOR THAT IS FLUSH WITH THE EXTERNAL SIDE OF THE UPS CABINET. IT SHALL INCLUDE A RECESSED PLUG, AUTOMATIC TRANSFER SWITCH AND A DOOR THAT SECURELY CLOSES OVER THE POWER CORD.

THE CABINET SHALL HAVE A DOOR STOP MECHANISM AND THERMOSTATICALLY CONTROLLED FAN.

THE CABINET SHALL INCLUDE A BATTERY BALANCING DEVICE THAT REGULATES THE BATTERIES AND OPTIMIZES PERFORMANCE.

AFTER FOUR (4) HOURS OF BATTERY RUNTIME, THE SYSTEM SHALL BE PROGRAMMED TO SWITCH THE INTERSECTION FROM FULL OPERATION TO CONTROLLER AUTOMATIC FLASH OPERATION THROUGH THE MONITOR. THE CONTROLLER SHALL BE PROGRAMMED SO THAT FLASH OPERATION SHALL BEGIN ONCE THE INTERSECTION RUNS MINOR STREET GREEN (TYP. PH. 4 & 8), ALL-RED CLEARANCE, AND THEN FLASH OPERATION.

THE UPS OUTPUT NOTIFICATIONS FOR ON BATTERY, BATTERY 2-HOUR TIMER, AND LOW BATTERY SHALL BE WIRED INTO THE TRAFFIC SIGNAL CABINET BACK PANEL OR THROUGH THE CONTROLLER WITH A C11 TO PROVIDE SPECIAL STATUS ALARMS FOR EACH OUTPUT INTO THE SIGNAL CONTROLLER.

THIS ITEM SHALL INCLUDE A RED LED STATUS INDICATOR LAMP TO ALLOW MAINTENANCE PERSONNEL AND LAW ENFORCEMENT TO QUICKLY ASSESS WHETHER A TRAFFIC SIGNAL CABINET IS BEING POWERED BY A UPS. THE LED HOUSING SHALL BE NEMA 4X, IP65 OR IP66, RATED FOR OUTDOOR USE AND BE TAMPER-/SHATTER RESISTANT. IT SHALL BE A DOMED ENCLOSURE CONTAINING A RED LENS WITH LED THAT IS VISIBLE FROM 100 FOOT MINIMUM. THE ENCLOSURE AND LED MODULE SHOULD BE PLACED ON THE SIDE OF THE UPS CABINET FACING TOWARDS THE MAINLINE ROADWAY AND SEALED FROM WATER INTRUSION. IT SHOULD BE WIRED USING MINIMUM 20GA STRANDED, INSULATED HOOKUP WIRE TO THE STATUS RELAY OUTPUTS OF THE UPS. THE WIRES SHALL BE TERMINATED BY LUGS AT THE DISPLAY END AND PERMANENTLY LABELED "BACKUP POWER STATUS DISPLAY," WITH WIRE POLARITY INDICATED. THE RED LED SHALL ONLY ILLUMINATE TO INDICATE THE CABINET IS OPERATING UNDER UPS BACKUP POWER (THE "BACKUP" OPERATING CONDITION). THIS ITEM INCLUDES PROGRAMMING THE UPS STATUS RELAY OUTPUTS TO PRODUCE THE LAMP STATUS DISPLAYS. THESE STATUS DISPLAYS WILL BE SOLID 100% DUTY CYCLE (NOT FLASHING) DISPLAYS. THE OPERATING VOLTAGE OF THE LED LAMP SHALL BE 120V AC UNLESS OTHERWISE INDICATED.

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\Signals\Sheets\09270_CN003.dgn_Sheet 7/15/2022 3:00:39 PM Troyer

CALCULATED
JDC
CHECKED
WDB

TRAFFIC SIGNAL GENERAL NOTES

LAK-US-20-24.99
PART 2

579
697

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\Signals\Sheets\09270_CN004.dgn Sheet 7/15/2022 3:00:40 PM Troyer

809 ADVANCE RADAR DETECTION, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF FURNISHING AND INSTALLING A WAVETRONIX SMARTSENSOR ADVANCE DETECTION UNIT (MODEL SS-200E). THE DETECTION UNIT SHALL INCLUDE THE FOLLOWING:

1. POWER SHALL BE PROVIDED FROM THE TRAFFIC CABINET.
2. ALL REQUIRED INPUTS CARDS SHALL BE INCLUDED IN THE TRAFFIC CABINET AND SHALL BE COMPATIBLE WITH CALTRANS, NEMA TS1 AND NEMA TS2 DETECTOR RACKS. THE CARDS SHALL PROVIDE TRUE PRESENCE DETECTOR CALLS OR CONTACT CLOSURE TO THE TRAFFIC CONTROLLER.
3. THE UNIT SHALL BE MOUNTED DIRECTLY TO A POLE OR MAST ARM, AS RECOMMENDED BY THE MANUFACTURER. CABLE(S) SHALL BE PROVIDED AS REQUIRED AND RECOMMENDED BY THE MANUFACTURER.
4. SURGE PROTECTION DEVICES, AS RECOMMENDED BY THE MANUFACTURER SHALL BE INCLUDED BOTH AT THE POLE WHERE THE UNIT IS LOCATED TO PROTECT THE UNIT AND IN THE TRAFFIC CABINET TO PROTECT THE CABINET ELECTRONICS.
5. THE MANUFACTURER'S REPRESENTATIVE SHALL BE ON SITE DURING INSTALLATION AND TESTING AND SHALL PROVIDE ONSITE TRAINING ON THE SETUP, OPERATION AND MAINTENANCE OF THE UNIT.
6. A SERIAL TO ETHERNET COMMUNICATIONS MODULE AND ETHERNET CABLE (MINIMUM 7 FEET).
7. THE POWER SUPPLY AND COMMUNICATION MODULES SHALL BE SECURED TO A SINGLE PANEL THAT CAN BE MOUNTED INTERIOR TO THE TRAFFIC CABINET. THE PANEL SHALL INCLUDE MODULAR-PLUG STYLE CONNECTIONS FOR UP TO FOUR (4) SENSOR CABLES. ADDITIONAL SENSORS MAY BE HARD-WIRED TO THE COMMUNICATION MODULES, AS NECESSARY.
8. THE CONTRACTOR SHALL INSTALL THE RADAR DETECTION PRIOR TO MILLING/DISABLING EXISTING LOOPS.
9. THE INSTALLATION SHALL INCLUDE ALL CONTROLLER PROGRAMMING FOR COMPLETE INSTALLATION, WHICH INCLUDES MODIFICATIONS FOR REMOVAL OF EXISTING DETECTION. PAYMENT FOR ITEM 809 ADVANCE RADAR DETECTION, AS PER PLAN SHALL BE MADE AT THE CONTRACT UNIT PRICE FOR EACH UNIT, COMPLETE AND IN PLACE INCLUDING ALL REQUIRED CABINET HARDWARE, MOUNTING BRACKETS, CABLES, CONDUIT, CONNECTIONS TESTED AND ACCEPTED, AND ANY OTHER NECESSARY HARDWARE TO ESTABLISH A FULLY FUNCTIONAL DETECTION SYSTEM.

PAYMENT FOR ITEM 809 ADVANCE RADAR DETECTION, AS PER PLAN SHALL BE MADE AT THE CONTRACT UNIT PRICE FOR EACH UNIT, COMPLETE AND IN PLACE INCLUDING ALL REQUIRED CABINET HARDWARE, MOUNTING BRACKETS, CABLES, CONDUIT, CONNECTIONS TESTED AND ACCEPTED, AND ANY OTHER NECESSARY HARDWARE TO ESTABLISH A FULLY FUNCTIONAL DETECTION SYSTEM.

809 STOP-LINE RADAR DETECTION, AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF FURNISHING AND INSTALLING A WAVETRONIX SMARTSENSOR MATRIX DETECTION UNIT. THE DETECTION UNIT SHALL INCLUDE THE FOLLOWING:

1. POWER SHALL BE PROVIDED FROM THE TRAFFIC CABINET.
2. ALL REQUIRED INPUTS CARDS SHALL BE INCLUDED IN THE TRAFFIC CABINET AND SHALL BE COMPATIBLE WITH CALTRANS, NEMA TS1 AND NEMA TS2 DETECTOR RACKS. THE CARDS SHALL PROVIDE TRUE PRESENCE DETECTOR CALLS OR CONTACT CLOSURE TO THE TRAFFIC CONTROLLER.
3. THE UNIT SHALL BE MOUNTED DIRECTLY TO A POLE OR MAST ARM, AS RECOMMENDED BY THE MANUFACTURER. CABLE(S) SHALL BE PROVIDED AS REQUIRED AND RECOMMENDED BY THE MANUFACTURER.
4. SURGE PROTECTION DEVICES, AS RECOMMENDED BY THE MANUFACTURER SHALL BE INCLUDED BOTH AT THE POLE WHERE THE UNIT IS LOCATED TO PROTECT THE UNIT AND IN THE TRAFFIC CABINET TO PROTECT THE CABINET ELECTRONICS.
5. THE MANUFACTURER'S REPRESENTATIVE SHALL BE ON SITE DURING INSTALLATION AND TESTING AND SHALL PROVIDE ONSITE TRAINING ON THE SETUP, OPERATION AND MAINTENANCE OF THE UNIT.
6. A SERIAL TO ETHERNET COMMUNICATIONS MODULE AND ETHERNET CABLE (MINIMUM 7 FEET).
7. THE POWER SUPPLY AND COMMUNICATION MODULES SHALL BE SECURED TO A SINGLE PANEL THAT CAN BE MOUNTED INTERIOR TO THE TRAFFIC CABINET. THE PANEL SHALL INCLUDE MODULAR-PLUG STYLE CONNECTIONS FOR UP TO FOUR (4) SENSOR CABLES. ADDITIONAL SENSORS MAY BE HARD-WIRED TO THE COMMUNICATION MODULES, AS NECESSARY.
8. THE CONTRACTOR SHALL INSTALL THE RADAR DETECTION PRIOR TO MILLING/DISABLING EXISTING LOOPS.
9. THE INSTALLATION SHALL INCLUDE ALL CONTROLLER PROGRAMMING FOR COMPLETE INSTALLATION, WHICH INCLUDES MODIFICATIONS FOR REMOVAL OF EXISTING DETECTION AND ASSOCIATED PULL BOXES.

PAYMENT FOR ITEM 809 STOP-LINE RADAR DETECTION, AS PER PLAN SHALL BE MADE AT THE CONTRACT UNIT PRICE FOR EACH UNIT, COMPLETE AND IN PLACE INCLUDING ALL REQUIRED CABINET HARDWARE, MOUNTING BRACKETS, CABLES, CONDUIT AND CONNECTIONS TESTED AND ACCEPTED.

632 REMOVAL OF TRAFFIC SIGNAL ITEM (PEDESTRIAN SIGNAL SYSTEM)

PEDESTRIAN SIGNAL SYSTEMS, INCLUDING SIGNAL HEADS, PEDESTALS, TRANSFORMER BASES, PUSH BUTTON ASSEMBLIES, ETC., SHALL BE REMOVED IN ACCORDANCE WITH C&MS 632.26 AND AS INDICATED ON THE PLANS. REMOVED ITEMS SHALL BE REUSED AS PART OF A NEW INSTALLATION ON THE PROJECT OR STORED ON THE PROJECT FOR SALVAGE BY (NAME OF AGENCY RECEIVING STORED ITEMS) IN ACCORDANCE WITH THE LISTING GIVEN HEREIN.

ITEMS TO BE STORED:

- PEDESTRIAN SIGNAL HEADS
- PEDESTRIAN PUSHBUTTON ASSEMBLIES
- PEDESTRIAN PEDESTAL
- PEDESTRIAN TRANSFORMER BASE

REMOVED ITEMS SHALL BE DELIVERED TO THE NEAREST ODOT FACILITY WHOSE ADDRESS IS LISTED BELOW:

ODOT DISTRICT 12 SIGNAL SHOP, ATTN: LEONARD PAIGE
CONTACT PHONE NUMBER 216-584-2302

25609 EMERY ROAD
WARRENSVILLE, OHIO 44128

IN THE EVENT THE ITEMS STORED ON THE PROJECT FOR SALVAGE BY THE LOCAL AGENCY ARE NOT REMOVED, THE CONTRACTOR SHALL, WHEN DIRECTED BY THE ENGINEER IN WRITING, REMOVE AND DISPOSE OF THE ITEMS AT NO ADDITIONAL COST TO THE PROJECT.

CALCULATED
JDC
CHECKED
WDB

TRAFFIC SIGNAL GENERAL NOTES

LAK-US-20-24.99
PART 2

580
697



0 20 40
HORIZONTAL
SCALE IN FEET

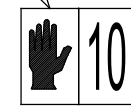
CALCULATED JDC CHECKED WDB

TRAFFIC SIGNAL PLAN
GREEN ROAD

LAK-US-20-24.99
PART 2

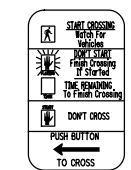
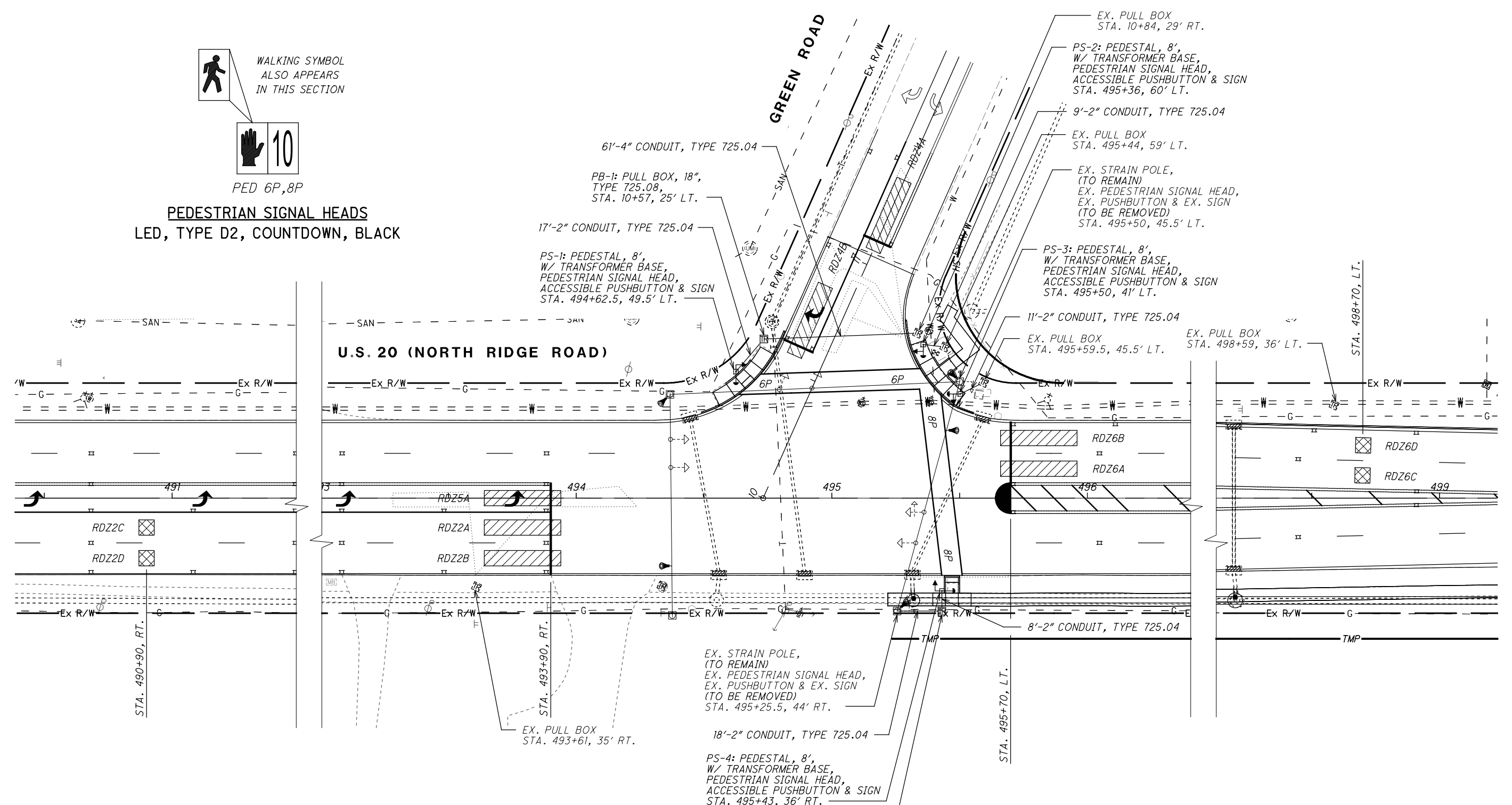
581
697

WALKING SYMBOL
ALSO APPEARS
IN THIS SECTION



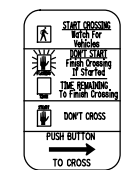
PED 6P,8P

PEDESTRIAN SIGNAL HEADS
LED, TYPE D2, COUNTDOWN, BLACK



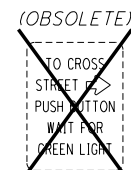
R10-3e-9
9"x15"

1 - LEFT ARROW



R10-3e-9
9"x15"

3 - RIGHT ARROW



(OBSOLETE)
TO CROSS
STREET
PUSH
BUTTON
WAIT FOR
GREEN LIGHT

R10-3a-9
9"x12"

2 - RIGHT ARROW

PEDESTRIAN SIGNS

LEGEND:

- PROPOSED OVERHEAD POWER SERVICE
- PROPOSED CONDUIT
- PROPOSED PULL BOX
- PROPOSED CONTROLLER
- PROPOSED SIGNAL MAST ARM
- PROPOSED PEDESTAL
- 3-SECTION VEHICULAR SIGNAL HEAD
- PEDESTRIAN SIGNAL HEAD
- ACCESSIBLE PEDESTRIAN PUSHBUTTON
- PREEMPT CONFIRMATION LIGHT
- PREEMPT RECEIVING UNIT
- STOP LINE RADAR DETECTION UNIT
- ADVANCE RADAR DETECTION UNIT
- MAST ARM MOUNTED SIGNS
- ▨ DETECTION ZONE
- ▨ DETECTION COUNT

H:\2018\80743\000T\ProjectData\09220_LAK_20_EAST\Design\Signals\09220_CP001.dgn Sheet 7/15/2022 3:00:43 PM troyer

DETECTION ZONE	MOVEMENT	PULSE OR PRESENCE	ASSOCIATED PHASE	DELAY PROGRAMMED IN CONTROLLER (SEC.)	EXTENSION PROGRAMMED IN CONTROLLER (SEC.)	DETECTOR NUMBER	PURPOSE	DETECTION ZONE LENGTH (FT)
RDZ2A	EB	PRESENCE	2	-	-	DS2	STOP-LINE	30
RDZ2B	EB	PRESENCE	2	-	-	DS2	STOP-LINE	30
RDZ2C	EB	PULSE	2	-	-	DD2	ADVANCED DETECTION	6
RDZ2D	EB	PULSE	2	-	-	DD2	ADVANCED DETECTION	6
RDZ4A	SB LT	PRESENCE	4	-	-	DS4	STOP-LINE	30
RDZ4B	SB RT	PRESENCE	4	12	-	DS4	STOP-LINE	30
RDZ5A	EB LT	PRESENCE	5	-	-	DS5	STOP-LINE	30
RDZ6A	WB	PRESENCE	6	-	-	DS6	STOP-LINE	30
RDZ6B	WB	PRESENCE	6	-	-	DS6	STOP-LINE	30
RDZ6C	WB	PULSE	6	-	-	DD6	ADVANCED DETECTION	6
RDZ6D	WB	PULSE	6	-	-	DD6	ADVANCED DETECTION	6
-	-	-	-	-	-	-	-	-
-	-	-	-	-	-	-	-	-

NOTE: ADVANCED DILEMMA ZONE SPEED THRESHOLD: 30 MPH
PURPOSE: STOP-LINE OR ADVANCED DETECTION

RADAR DETECTION CHART

ITEM	TOTAL	UNIT	DESCRIPTION
625	63	FT	CONDUIT, 2", 725.04
625	61	FT	CONDUIT, 4", 725.04
625	124	FT	TRENCH
625	2	EACH	PULL BOX, 725.08, 18"
625	4	EACH	GROUND ROD
630	3.75	SF	SIGN, FLAT SHEET
630	2	EACH	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL
632	4	EACH	PEDESTAL FOUNDATION
632	4	EACH	PEDESTAL, 8', TRANSFORMER BASE
632	4	EACH	REMOVAL OF MISCELLANEOUS TRAFFIC SIGNAL ITEM (PEDESTRIAN SIGNAL SYSTEM)
632	4	EACH	PEDESTRIAN SIGNAL HEAD (LED), TYPE D2, COUNTDOWN, AS PER PLAN
632	4	EACH	COVERING OF PEDESTRIAN SIGNAL HEAD
632	4	EACH	ACCESSIBLE PEDESTRIAN PUSHBUTTON
632	472	FT	SIGNAL CABLE, 5 CONDUCTOR, NO. 14 AWG
632	452	FT	LOOP DETECTOR LEAD-IN CABLE
632	93	FT	SIGNALIZATION, MISC.: UNLASH AND RELASH MESSENGER WIRE
809	2	EACH	ADVANCE RADAR DETECTION, AS PER PLAN
809	3	EACH	STOP LINE RADAR DETECTION, AS PER PLAN

NOTES:

- ALL MOVEMENTS SHALL BE ACTUATED. THE PRIMARY THRU MOVEMENT SHOULD HAVE MIN RECALL ACTIVE TO REST IN GREEN.
- FOR PROTECTED/PERMISSIVE PHASES, IMPLEMENT CALL OMITTS TO AVOID YELLOW BALL TRAP
- ENABLE ϕ 1, 3 & ϕ 5, 7 DETECTOR SWITCHING TO ALLOW ϕ 1 & ϕ 5 TO EXTEND ϕ 2 & ϕ 6 OR ϕ 3 & ϕ 7 TO EXTEND ϕ 4 & ϕ 8, RESPECTIVELY, WHEN ALLOCATED GREEN TIME FOR LEFT TURN PHASES ARE EXHAUSTED.
- COUNTDOWN PEDESTRIAN SIGNALS SHALL GO TO ZERO ON YELLOW PER OMUTCD FIGURE 4E-2.
- RADAR DETECTION UNITS FOR DILEMMA ZONE DETECTION SHALL PLACE A CONSTANT CALL TO THE CONTROLLER WHEN VEHICLE TRAVEL TIMES TO THE STOP BAR ARE BETWEEN 2.5 AND 6 SECONDS. SPEED TRIGGER SHALL BE SET FOR VEHICLES TRAVELING 35 MPH AND GREATER.
- RADAR SHALL HAVE QUEUE DETECTION CONFIGURED AND A ZONE PLACED AT 100-200 FEET FROM STOP BAR FOR SLOW MOVING VEHICLE EXTENSIONS. SPEED TRIGGER SHALL BE SET AT 1-35 MPH.
- ALL DETECTOR DELAYS SHALL BE PLACED IN THE CONTROLLER.
- FOR ANY ENTRY TO FLASHING OPERATION, PROGRAMMING SHALL RUN MINOR STREET GREEN (TYP. ϕ 4 & ϕ 8), ALL-RED CLEARANCE, AND THEN FLASHING OPERATION.

















PULL BOX #	STATION	SIDE	OFFSET	SIZE (IN.)
PB-1	10+57	LT	25	18
PB-2	495+43	RT	44'	18

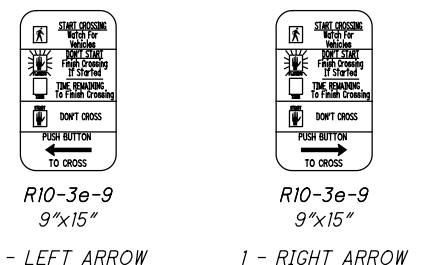
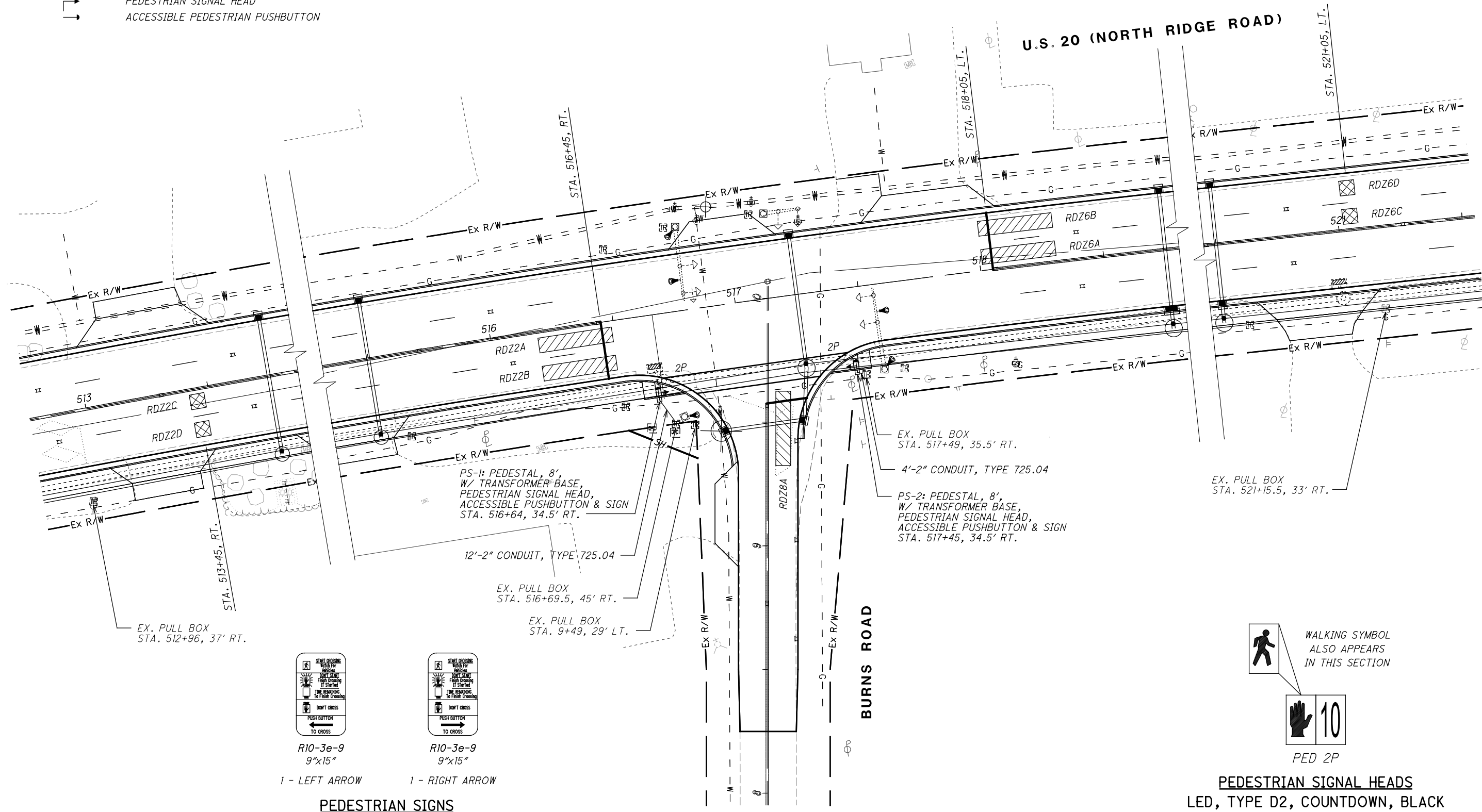
PULLBOX TABLE

SUPPORT NO.	STATION	OFFSET	POLE HEIGHT	PEDESTRIAN SIGNAL	PEDESTRIAN PUSHBUTTON	HANDHOLE
			FT	DEG	DEG	DEG
PS-1	494+62.5	49.5' LT	8'	0	180	180
PS-2	495+36	60' LT	8'	180	180	180
PS-3	495+50	41' LT	8'	270	270	180
PS-4	495+43	36' RT.	8'	90	270	180

PEDESTAL TABLE

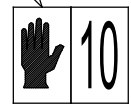
LEGEND:

- | | | | |
|-----------------------------------------------------------------------------------|----------------------------------|-----------------------------------------------------------------------------------|--------------------------------|
|  | PROPOSED OVERHEAD POWER SERVICE |  | PREEMPT CONFIRMATION LIGHT |
|  | PROPOSED CONDUIT |  | PREEMPT RECEIVING UNIT |
|  | PROPOSED PULL BOX |  | STOP LINE RADAR DETECTION UNIT |
|  | PROPOSED CONTROLLER |  | ADVANCE RADAR DETECTION UNIT |
|  | PROPOSED SIGNAL MAST ARM |  | MAST ARM MOUNTED SIGNS |
|  | PROPOSED PEDESTAL |  | DETECTION ZONE |
|  | 3-SECTION VEHICULAR SIGNAL HEAD |  | DETECTION COUNT |
|  | PEDESTRIAN SIGNAL HEAD | | |
|  | ACCESSIBLE PEDESTRIAN PUSHBUTTON | | |



PEDESTRIAN SIGNS

 WALKING SYMBOL
 ALSO APPEARS
 IN THIS SECTION



PEDESTRIAN SIGNAL HEADS
 LED, TYPE D2, COUNTDOWN, BLACK

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Signals\Sheets\09270_CP002.dgn Sheet 7/15/2022 3:00:46 PM Troyer

DETECTION ZONE	MOVEMENT	PULSE OR PRESENCE	ASSOCIATED PHASE	DELAY PROGRAMMED IN CONTROLLER (SEC.)	EXTENSION PROGRAMMED IN CONTROLLER (SEC.)	DETECTOR NUMBER	PURPOSE	DETECTION ZONE LENGTH (FT)
RDZ2A	EB	PRESENCE	2	-	-	DS2	STOP-LINE	30
RDZ2B	EB	PRESENCE	2	-	-	DS2	STOP-LINE	30
RDZ2C	EB	PULSE	2	-	-	DD2	ADVANCED DETECTION	6
RDZ2D	EB	PULSE	2	-	-	DD2	ADVANCED DETECTION	6
RDZ6A	WB	PRESENCE	6	-	-	DS6	STOP-LINE	30
RDZ6B	WB	PRESENCE	6	-	-	DS6	STOP-LINE	30
RDZ6C	WB	PULSE	6	-	-	DD6	ADVANCED DETECTION	6
RDZ6D	WB	PULSE	6	-	-	DD6	ADVANCED DETECTION	6
RDZ8A	NB RT	PRESENCE	8	12	-	DS8	STOP-LINE	30
-	-	-	-	-	-	-	-	-
-	-	-	-	-	-	-	-	-

NOTE: ADVANCED DILEMMA ZONE SPEED THRESHOLD: 30 MPH
PURPOSE: STOP-LINE OR ADVANCED DETECTION

RADAR DETECTION CHART

ITEM	TOTAL	UNIT	DESCRIPTION
625	16	FT	CONDUIT, 2", 725.04
625	16	FT	TRENCH
625	2	EACH	GROUND ROD
630	1.88	SF	SIGN, FLAT SHEET
630	2	EACH	SURFACE PREPARATION, NEW SUPPORT SECTION
630	2	EACH	COATING, ORGANIC ZINC PRIME COAT, SUPPORT SECTION
630	2	EACH	COATING, EPOXY INTERMEDIATE COAT, SUPPORT SECTION
630	2	EACH	COATING, URETHANE TOP COAT, SUPPORT SECTION
632	2	EACH	PEDESTAL FOUNDATION
632	2	EACH	PEDESTAL, 8', TRANSFORMER BASE
632	2	EACH	PEDESTRIAN SIGNAL HEAD (LED), TYPE D2, COUNTDOWN, AS PER PLAN
632	2	EACH	COVERING OF PEDESTRIAN SIGNAL HEAD
632	2	EACH	ACCESSIBLE PEDESTRIAN PUSHBUTTON
632	175	FT	SIGNAL CABLE, 5 CONDUCTOR, NO. 14 AWG
632	165	FT	LOOP DETECTOR LEAD-IN CABLE
809	2	EACH	ADVANCE RADAR DETECTION, AS PER PLAN
809	3	EACH	STOP LINE RADAR DETECTION, AS PER PLAN

NOTES:

- ALL MOVEMENTS SHALL BE ACTUATED. THE PRIMARY THRU MOVEMENT SHOULD HAVE MIN RECALL ACTIVE TO REST IN GREEN.
- FOR PROTECTED/PERMISSIVE PHASES, IMPLEMENT CALL OMITTS TO AVOID YELLOW BALL TRAP
- ENABLE ϕ 1, 3 & ϕ 5, 7 DETECTOR SWITCHING TO ALLOW ϕ 1 & ϕ 5 TO EXTEND ϕ 2 & ϕ 6 OR ϕ 3 & ϕ 7 TO EXTEND ϕ 4 & ϕ 8, RESPECTIVELY, WHEN ALLOCATED GREEN TIME FOR LEFT TURN PHASES ARE EXHAUSTED.
- COUNTDOWN PEDESTRIAN SIGNALS SHALL GO TO ZERO ON YELLOW PER OMTUCD FIGURE 4E-2.
- RADAR DETECTION UNITS FOR DILEMMA ZONE DETECTION SHALL PLACE A CONSTANT CALL TO THE CONTROLLER WHEN VEHICLE TRAVEL TIMES TO THE STOP BAR ARE BETWEEN 2.5 AND 6 SECONDS. SPEED TRIGGER SHALL BE SET FOR VEHICLES TRAVELING 35 MPH AND GREATER.
- RADAR SHALL HAVE QUEUE DETECTION CONFIGURED AND A ZONE PLACED AT 100-200 FEET FROM STOP BAR FOR SLOW MOVING VEHICLE EXTENSIONS. SPEED TRIGGER SHALL BE SET AT 1-35 MPH.
- ALL DETECTOR DELAYS SHALL BE PLACED IN THE CONTROLLER.
- FOR ANY ENTRY TO FLASHING OPERATION, PROGRAMMING SHALL RUN MINOR STREET GREEN (TYP. ϕ 4 & ϕ 8), ALL-RED CLEARANCE, AND THEN FLASHING OPERATION.

SUPPORT NO.	STATION	OFFSET	POLE HEIGHT	PEDESTRIAN SIGNAL	PEDESTRIAN PUSHBUTTON	HANDHOLE
			FT	DEG	DEG	DEG
PS-1	516+64	34.5' RT	8'	180	180	180
PS-2	517+45	34.5' RT	8'	180	180	180

PEDESTAL TABLE



**TRAFFIC SIGNAL DETAILS
BURNS ROAD**



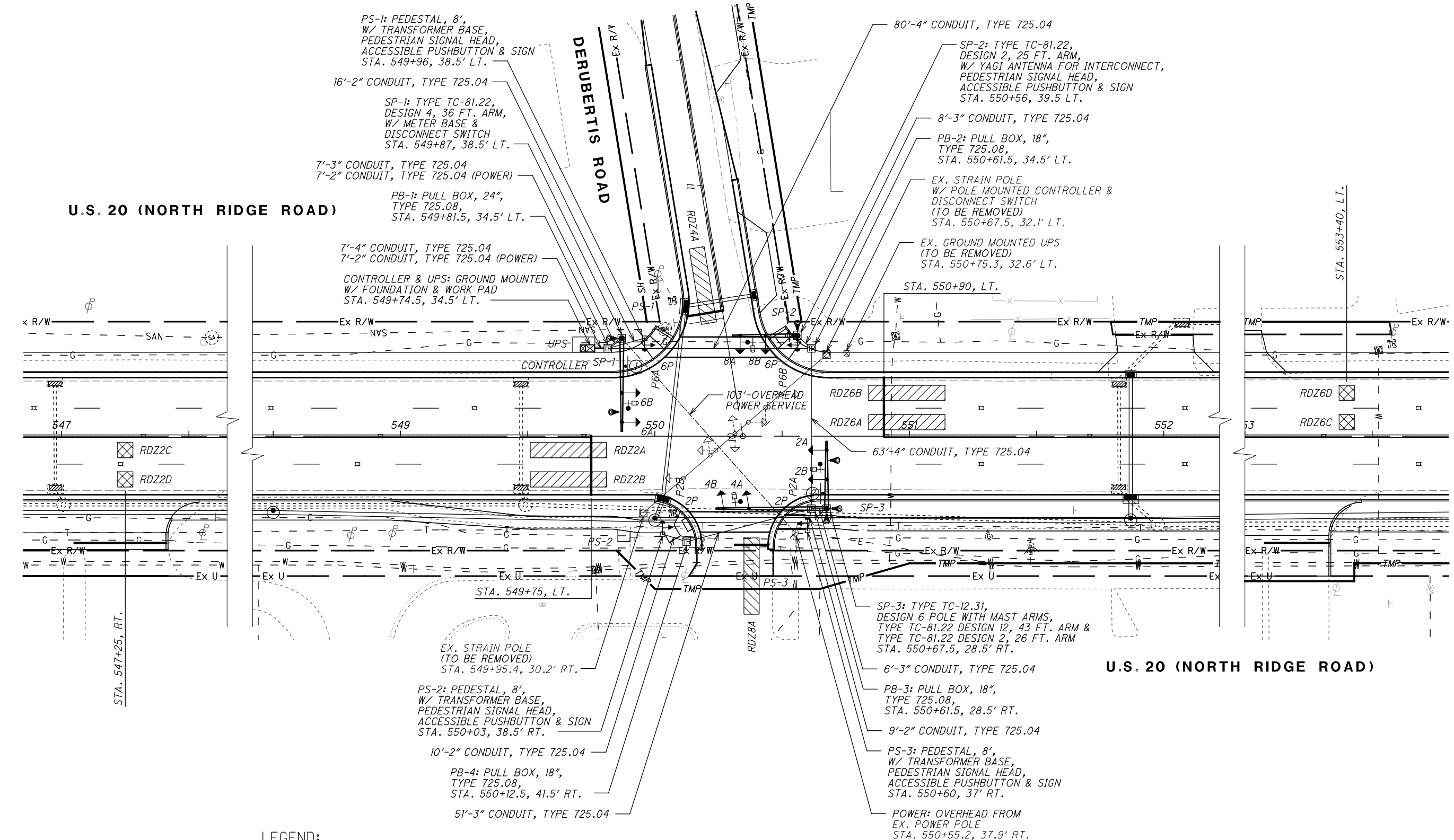
CALCULATED JDC CHECKED WDB

TRAFFIC SIGNAL PLAN
DERUBERTIS ROAD

LAK-US-20-24.99
PART 2

585
697

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\Signals\09270_CP003.dgn Sheet 7/15/2022 3:00:48 PM Troyer



LEGEND:

- | | | | |
|--|----------------------------------|--|--------------------------------|
| | PROPOSED OVERHEAD POWER SERVICE | | PREEMPT CONFIRMATION LIGHT |
| | PROPOSED CONDUIT | | PREEMPT RECEIVING UNIT |
| | PROPOSED PULL BOX | | STOP LINE RADAR DETECTION UNIT |
| | PROPOSED CONTROLLER | | ADVANCE RADAR DETECTION UNIT |
| | PROPOSED SIGNAL MAST ARM | | MAST ARM MOUNTED SIGNS |
| | PROPOSED PEDESTAL | | DETECTION ZONE |
| | 3-SECTION VEHICULAR SIGNAL HEAD | | DETECTION COUNT |
| | PEDESTRIAN SIGNAL HEAD | | |
| | ACCESSIBLE PEDESTRIAN PUSHBUTTON | | |

SIGNAL TIMING CHART

INTERSECTION: U.S. 20 (N. RIDGE RD.) AT DERUBERTIS DR.									
MAINTAINING AGENCY: ODOT									
START UP		DUAL ENTRY: -		PHASES: 2+6					
START IN: ALL-RED FLASH		REST IN RED:		RING 1 -		RING 2 -			
TIME FOR: FLASH, ALL RED (SEC.): 9, 6		OVERLAP		A	B	C	D		
FIRST PHASE(S): 2+6		PHASES		-	-	-	-		
COLOR DISPLAYED: GREEN									
INTERVAL OR FEATURE		CONTROLLER MOVEMENT NO.							
INTERSECTION MOVEMENT (PHASE)		1	2	3	4	5	6	7	8
DIRECTION		-	EB	-	SB	-	WB	-	NB
MINIMUM GREEN (INITIAL) (SEC.)		-	25	-	10	-	25	-	10
ADDED INITIAL *(SEC./ACTUATION)		-	5	-	3	-	5	-	3
MAXIMUM INITIAL *(SEC.)		-	-	-	-	-	-	-	-
PASSAGE TIME (PRESET GAP) (SEC.)		-	-	-	-	-	-	-	-
TIME BEFORE REDUCTION *(SEC.)		-	-	-	-	-	-	-	-
MINIMUM GAP *(SEC.)		-	-	-	-	-	-	-	-
TIME TO REDUCE *(SEC.)		-	-	-	-	-	-	-	-
MAXIMUM GREEN I (SEC.)		-	35	-	20	-	35	-	20
MAXIMUM GREEN II (SEC.)		-	35	-	20	-	35	-	20
YELLOW CHANGE (SEC.)		-	3.6	-	3.6	-	3.6	-	3.6
ALL RED CLEARANCE (SEC.)		-	1.9	-	1.9	-	1.9	-	1.9
DELAYED GREEN (LPI) # (SEC.)		-	5	-	-	-	5	-	-
FLASHING YELLOW ARROW DELAY^ (SEC.)		-	-	-	-	-	-	-	-
WALK (SEC.)		-	8	-	-	-	8	-	-
PEDESTRIAN CLEARANCE (SEC.)		-	7	-	-	-	9	-	-
RECALL	MAXIMUM (ON/OFF)	-	NO	-	NO	-	NO	-	NO
	MINIMUM (ON/OFF)	-	YES	-	NO	-	YES	-	NO
	PEDESTRIAN (ON/OFF)	-	NO	-	NO	-	NO	-	NO
MEMORY (ON/OFF)		-	NO	-	NO	-	NO	-	NO

***VOLUME DENSITY CONTROLS**

FOR CROSSINGS WITH PEDESTRIAN PUSHBUTTONS, LPI'S (LEADING PEDESTRIAN INTERVALS) MAY BE IMPLEMENTED (3-6 SEC.) IN ACCORDANCE WITH LPI DURATION TIME PER THE ODOT SIGNAL CALCULATIONS - CLEARANCE INTERVALS SPREADSHEET

^ WHEN IMPLEMENTING FYA, A MINIMUM 3 SEC. DELAY SHALL BE PROGRAMMED PER FYA PHASE.

FIELD WIRING HOOK-UP CHART

SIGNAL HEAD	INDICATION	FIELD TERMINAL	FLASH	SIGNAL HEAD	INDICATION	FIELD TERMINAL	FLASH
2A (EB)	R	Ⓣ2 R	Y	6A (WB)	R	Ⓣ6 R	Y
	Y	Ⓣ2 Y			Y	Ⓣ6 Y	
	G	Ⓣ2 G			G	Ⓣ6 G	
2B (EB)	R	Ⓣ2 R	Y	6B (WB)	R	Ⓣ6 R	Y
	Y	Ⓣ2 Y			Y	Ⓣ6 Y	
	G	Ⓣ2 G			G	Ⓣ6 G	
4A (SB)	R	Ⓣ4 R	R	8A (NB)	R	Ⓣ8 R	R
	Y	Ⓣ4 Y			Y	Ⓣ8 Y	
	G	Ⓣ4 G			G	Ⓣ8 G	
4B (SB)	R	Ⓣ4 R	R	8B (NB)	R	Ⓣ8 R	R
	Y	Ⓣ4 Y			Y	Ⓣ8 Y	
	G	Ⓣ4 G			G	Ⓣ8 G	
PEDESTRIAN MOVEMENTS				OVERLAPS			
* PED 2P	WALK	Ⓣ2 PED / LS 9G	OUT	-	-	-	-
	DONT WALK	Ⓣ2 PED / LS 9R		-	-	-	-
* PED 6P	WALK	Ⓣ6 PED / LS 11G	OUT	LS = LOAD SWITCH			
	DONT WALK	Ⓣ6 PED / LS 11R					

* ONLY UPON PEDESTRIAN PUSH BUTTON ACTUATION.

LEGEND:

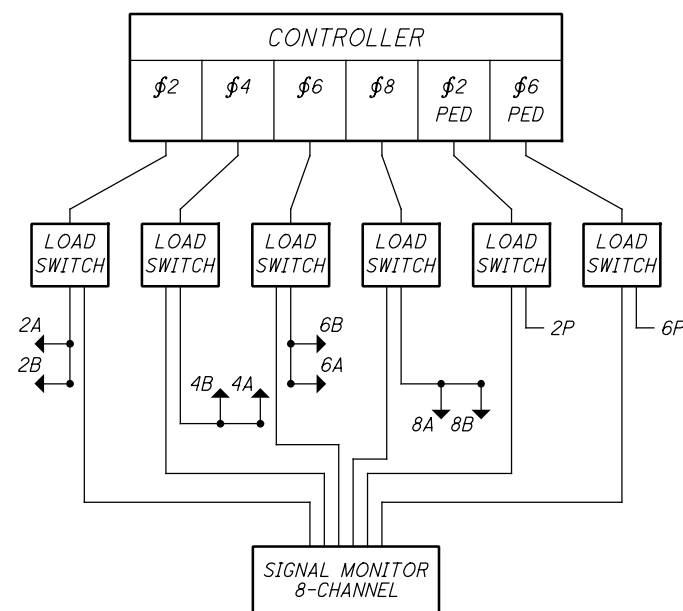
- PEDESTRIAN PHASE
- VEHICLE:
- PROTECTED PHASE
- PERMITTED PHASE

PREEMPT CHANNELS

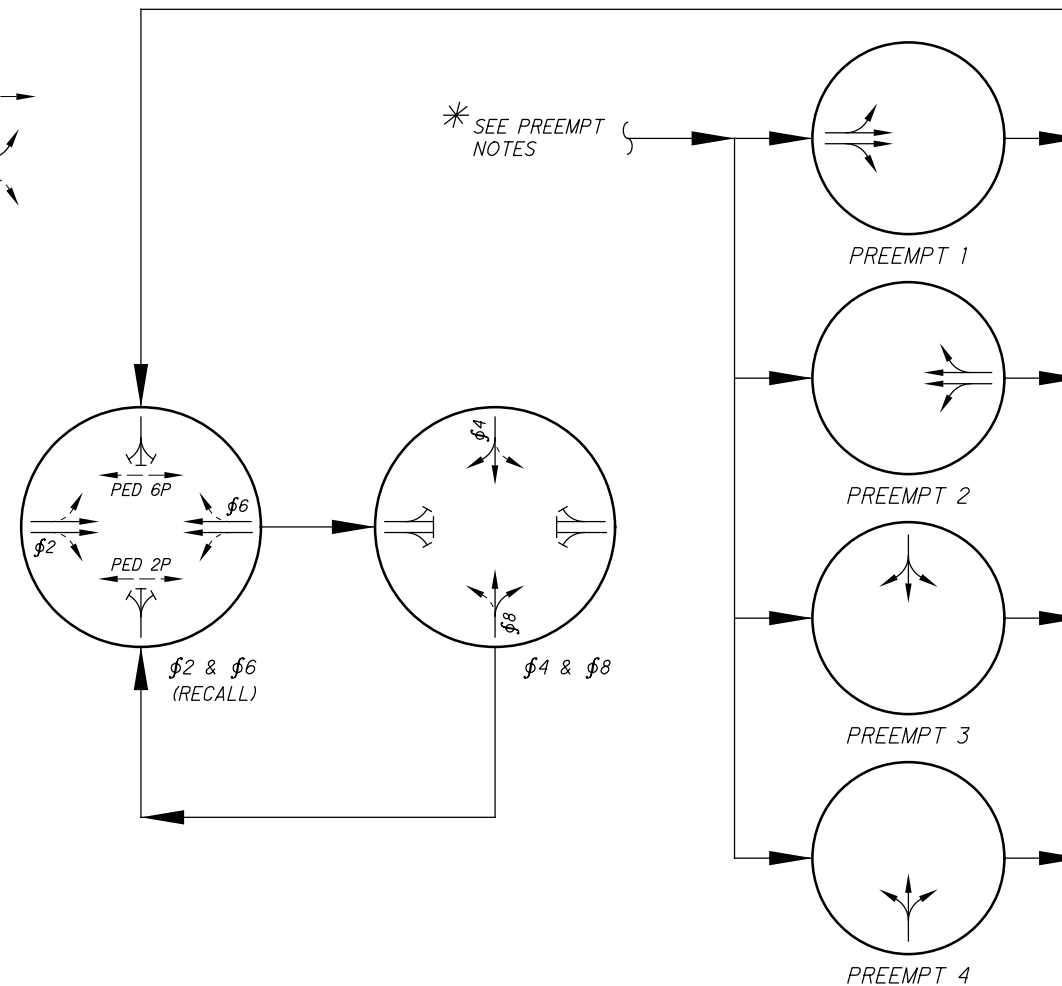
- CHANNEL 1 = Ⓣ2 (EASTBOUND ONLY)
- CHANNEL 2 = Ⓣ6 (WESTBOUND ONLY)
- CHANNEL 3 = Ⓣ4 (SOUTHBOUND ONLY)
- CHANNEL 4 = Ⓣ8 (NORTHBOUND ONLY)

*** PREEMPT NOTES**

1. ACTIVE WALK INDICATIONS SHALL IMMEDIATELY GO TO "DON'T WALK" UPON RECEIVING PREEMPTION SIGNAL.
2. IF PHASE ACTIVE CONFLICTS WITH PREEMPT PHASE CALLED, IT SHALL IMMEDIATELY TIME ITS YELLOW AND ALL RED CLEARANCES.
3. IF ACTIVE PHASE = THE PREEMPT PHASE, THEN THE PHASE SHALL HOLD FOR THE DURATION OF THE PREEMPT SIGNAL.
4. AFTER RELEASE FROM PREEMPT, YELLOW AND ALL RED CLEARANCE SHALL BE DISPLAYED AND RETURN PHASE SHALL BE Ⓣ2 & Ⓣ6
5. IF PREEMPT PHASE = RETURN PHASE Ⓣ2 & Ⓣ6 THEN YELLOW AND ALL RED CLEARANCE AFTER PREEMPT SHALL NOT BE DISPLAYED.



APPROACH MONITORING HOOK-UP DIAGRAM



PHASING DIAGRAM

H:\2018\180743\ODOT\ProjectData\09270_LAK_20_EAST\Design\Signals\Sheets\09270_CD003A.dgn Sheet 7/15/2022 3:00:50 PM troyer



TRAFFIC SIGNAL DETAILS
DERUBERTIS ROAD

LAK-US-20-24.99
PART 2

587
697



RADAR DETECTION CHART

DETECTION ZONE	MOVEMENT	PULSE OR PRESENCE	ASSOCIATED PHASE	DELAY PROGRAMMED IN CONTROLLER (SEC.)	EXTENSION PROGRAMMED IN CONTROLLER (SEC.)	DETECTOR NUMBER	PURPOSE	DETECTION ZONE LENGTH (FT)
RDZ2A	EB	PRESENCE	2	-	-	DS2	STOP-LINE	30
RDZ2B	EB	PRESENCE	2	-	-	DS2	STOP-LINE	30
RDZ2C	EB	PULSE	2	-	-	DD2	ADVANCED DETECTION	6
RDZ2D	EB	PULSE	2	-	-	DD2	ADVANCED DETECTION	6
RDZ4A	SB	PRESENCE	4	12	-	DS4	STOP-LINE	30
RDZ6A	WB	PRESENCE	6	-	-	DS6	STOP-LINE	30
RDZ6B	WB	PRESENCE	6	-	-	DS6	STOP-LINE	30
RDZ6C	WB	PULSE	6	-	-	DD6	ADVANCED DETECTION	6
RDZ6D	WB	PULSE	6	-	-	DD6	ADVANCED DETECTION	6
RDZ8A	NB	PRESENCE	8	12	-	DS8	STOP-LINE	30
-	-	-	-	-	-	-	-	-
-	-	-	-	-	-	-	-	-

NOTE: ADVANCED DILEMMA ZONE SPEED THRESHOLD: 30 MPH
PURPOSE: STOP-LINE OR ADVANCED DETECTION

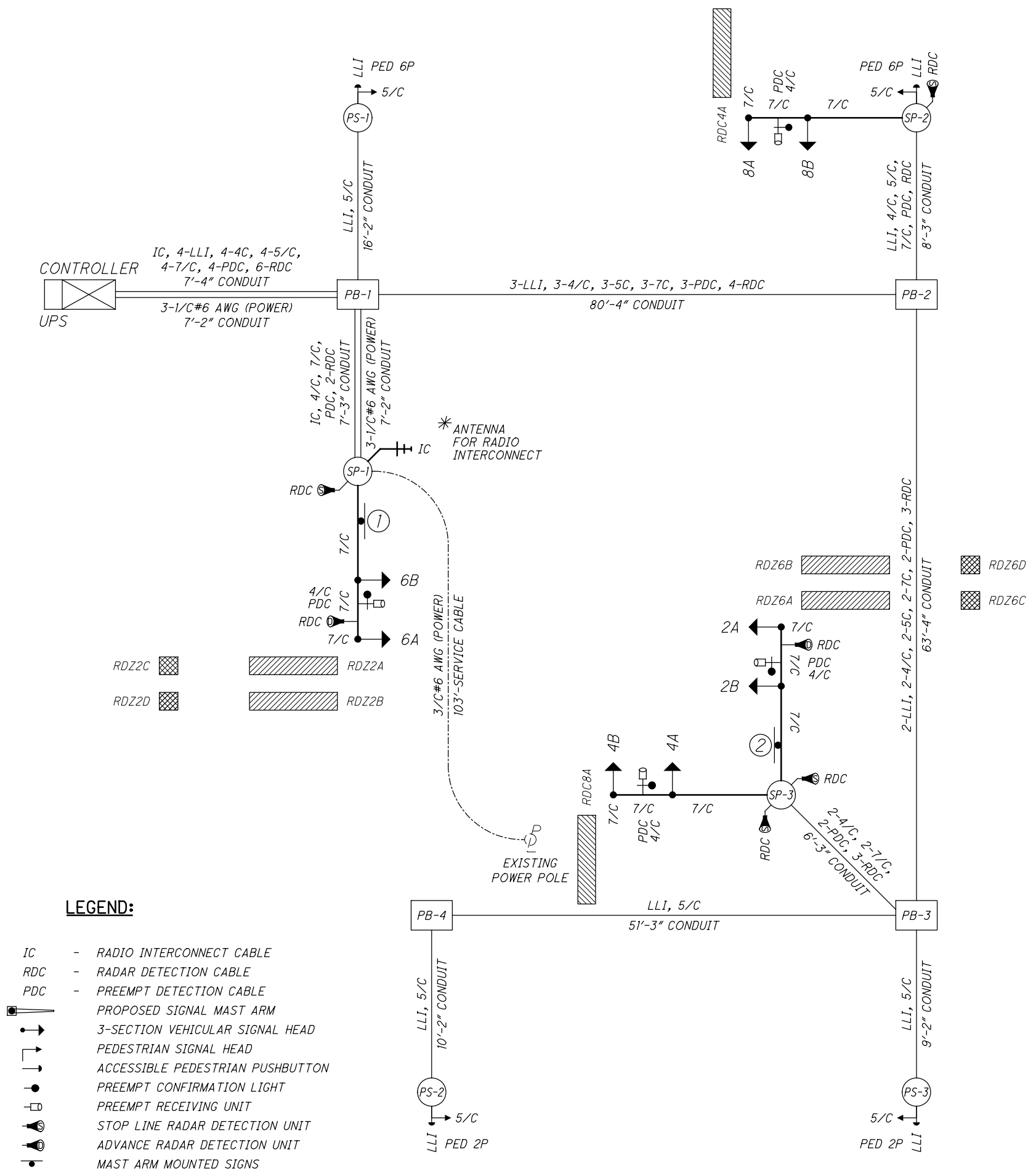
NOTES:

- ALL MOVEMENTS SHALL BE ACTUATED. THE PRIMARY THRU MOVEMENT SHOULD HAVE MIN RECALL ACTIVE TO REST IN GREEN.
- FOR PROTECTED/PERMISSIVE PHASES, IMPLEMENT CALL OMITTS TO AVOID YELLOW BALL TRAP
- ENABLE $\phi 1$, 3 & $\phi 5$, 7 DETECTOR SWITCHING TO ALLOW $\phi 1$ & $\phi 5$ TO EXTEND $\phi 2$ & $\phi 6$ OR $\phi 3$ & $\phi 7$ TO EXTEND $\phi 4$ & $\phi 8$, RESPECTIVELY, WHEN ALLOCATED GREEN TIME FOR LEFT TURN PHASES ARE EXHAUSTED.
- COUNTDOWN PEDESTRIAN SIGNALS SHALL GO TO ZERO ON YELLOW PER ODOTCD FIGURE 4E-2.
- RADAR DETECTION UNITS FOR DILEMMA ZONE DETECTION SHALL PLACE A CONSTANT CALL TO THE CONTROLLER WHEN VEHICLE TRAVEL TIMES TO THE STOP BAR ARE BETWEEN 2.5 AND 6 SECONDS. SPEED TRIGGER SHALL BE SET FOR VEHICLES TRAVELING 35 MPH AND GREATER.
- RADAR SHALL HAVE QUEUE DETECTION CONFIGURED AND A ZONE PLACED AT 100-200 FEET FROM STOP BAR FOR SLOW MOVING VEHICLE EXTENSIONS. SPEED TRIGGER SHALL BE SET AT 1-35 MPH.
- ALL DETECTOR DELAYS SHALL BE PLACED IN THE CONTROLLER.
- FOR ANY ENTRY TO FLASHING OPERATION, PROGRAMMING SHALL RUN MINOR STREET GREEN (TYP. $\phi 4$ & $\phi 8$), ALL-RED CLEARANCE, AND THEN FLASHING OPERATION.

PULLBOX TABLE

PULL BOX #	STATION	SIDE	OFFSET	SIZE (IN.)
PB-1	549+81.5	LT	34.5'	24
PB-2	550+61.5	LT	34.5'	18
PB-3	550+61.5	RT	28.5'	18
PB-4	550+12.5	RT	41.5'	18
-	-	-	-	-
-	-	-	-	-

WIRING DIAGRAM



H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\Signals\Sheets\09270_CS001.dgn Sheet 7/15/2022 3:00:51PM troyer

SHEET NO.	LOCATION	625	625	625	625	625	625	625	625	630	630	630	630	630	632	632	632	632	632	632	633	633	633	633	809
		CONDUIT, 2", 725.04	CONDUIT, 3", 725.04	CONDUIT, 4", 725.04	TRENCH	PULL BOX, 725.08, 18"	PULL BOX, 725.08, 24"	GROUND ROD	PULL BOX REMOVED	SIGN, FLAT SHEET	SURFACE PREPARATION, NEW SUPPORT SECTION	COATING, ORGANIC ZINC PRIME COAT, SUPPORT SECTION	COATING, EPOXY INTERMEDIATE COAT, SUPPORT SECTION	COATING, URETHANE TOP COAT, SUPPORT SECTION	SIGNAL SUPPORT FOUNDATION	PEDESTAL FOUNDATION	SIGNAL SUPPORT, TYPE TC-12.31 DESIGN 6 POLE, WITH MAST ARMS TC-81.22 DESIGN 12 AND DESIGN 2	SIGNAL SUPPORT, TYPE TC-81.22, DESIGN 2	SIGNAL SUPPORT, TYPE TC-81.22, DESIGN 4	PEDESTAL, 8', TRANSFORMER BASE	REMOVAL OF TRAFFIC SIGNAL INSTALLATION	CABINET, TYPE 332L, AS PER PLAN	CABINET FOUNDATION	CONTROLLER WORK PAD	UNINTERRUPTIBLE POWER SUPPLY (UPS), 1000 WATT, AS PER PLAN
		FT	FT	FT	FT	EACH	EACH	EACH	EACH	SF	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH
585	DERUBERTIS ROAD & U.S. 20 INTERSECTION																								
	CONTROLLER																								
585	CONTR. 549+74.5 LT							1														1	1	1	1
	PULL BOXES																								
585	PB-1 549+81.5 LT							1																	
585	PB-2 550+61.5 LT					1																			
585	PB-3 550+61.5 RT					1																			
585	PB-4 550+12.5 RT					1																			
	CONDUITS																								
585	CONTR. TO PB-1 LT			7	7																				
585	UPS TO PB-1 LT POWER	7																							
585	PB-1 TO SP-1 LT POWER	7	7		7																				
585	PB-1 TO PS-1 LT	16			16																				
585	PB-1 TO PB-2 LT			80	80																				
585	PB-2 TO SP-2 LT		8		8																				
585	PB-2 TO PB-3 LT&RT			63	63																				
585	PB-3 TO SP-3 RT		6		6																				
585	PB-3 TO PS-3 RT	9			9																				
585	PB-3 TO PB-4 RT		51		51																				
585	PB-4 TO PS-2 RT	10			10																				
	SIGNAL SUPPORTS																								
585	SP-1 549+87 LT							1			2	2	2	2	1										
585	SP-2 550+56 LT							1	0.94		2	2	2	2	1			1							
585	SP-3 550+67.5 RT							1			3	3	3	3	1			1							
	PEDESTALS																								
585	PS-1 549+96 LT							1	0.94		1	1	1	1							1				
585	PS-2 550+03 RT							1	0.94		1	1	1	1							1				
585	PS-3 550+60 RT							1	0.94		1	1	1	1							1				
	EX. PULL BOXES																								
585	EX PB-1 545+98.2 RT										1														
585	EX PB-2 550+07.1 RT										1														
585	EX PB-3 550+06.9 LT										1														
585	EX PB-4 553+57.8 LT										1														
SUBTOTALS CARRIED TO GENERAL SUMMARY		49	72	150	257	3	1	7	4	3.75	10	10	10	10	3	3	1	1	1	3	1	1	1	1	1

<p>TRAFFIC SIGNAL SUBSUMMARY</p>	<p>LAK-US-20-24.99</p> <p>PART 2</p>	<p>CALCULATED JDC CHECKED WDB</p>
<p>589 697</p>		

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\Signals\Sheets\09270_CS002.dgn Sheet 7/15/2022 3:00:52 PM Troyer

SHEET NO.	LOCATION	632	632	632	632	632	632	632	632	632	632	632	632	632	809	809	809	809	809	809	809	815
		VEHICULAR SIGNAL HEAD, (LED), 3-SECTION, 12" LENS, 1-WAY, POLYCARBONATE (BLACK)	PEDESTRIAN SIGNAL HEAD (LED), TYPE D2, COUNTDOWN, AS PER PLAN	COVERING OF VEHICULAR SIGNAL HEAD	COVERING OF PEDESTRIAN SIGNAL HEAD	ACCESSIBLE PEDESTRIAN PUSHBUTTON	SIGNAL CABLE, 4 CONDUCTOR, NO. 14 AWG	SIGNAL CABLE, 5 CONDUCTOR, NO. 14 AWG	SIGNAL CABLE, 7 CONDUCTOR, NO. 14 AWG	LOOP DETECTOR LEAD-IN CABLE	POWER CABLE, 1 CONDUCTOR, NO. 6 AWG	SERVICE CABLE, 3 CONDUCTOR, NO. 6 AWG	POWER SERVICE	CONDUIT RISER, 1 1/2" DIAMETER	ADVANCE RADAR DETECTION, AS PER PLAN	STOP LINE RADAR DETECTION, AS PER PLAN	EMERGENCY VEHICLE PREEMPTION	PREEMPT RECEIVING UNIT	PREEMPT DETECTOR CABLE	PREEMPT PHASE SELECTOR	PREEMPT CONFIRMATION LIGHT	SPREAD SPECTRUM RADIO
		EACH	EACH	EACH	EACH	EACH	FT	FT	FT	FT	FT	FT	EACH	EACH	EACH	EACH	EACH	EACH	FT	EACH	EACH	EACH
585	DERUBERTIS ROAD & U.S. 20 INTERSECTION																					
	POWER																					
585	EX. PP 550+55.2 RT																					
585	EX. PP TO MB&DS									90	113	1	1									
	CONTROLLER																					
585	CONTR. 549+74.5 LT															1				1		
	SIGNAL SUPPORTS																					
585	SP-1 549+87 LT																					
585	MB&DS TO CONTR. POWER												120									
585	6A&6B TO CONTR.	2		2				99										1	82		1	
585	P1&P2 TO CONTR. PREEMPT						82															
585	D1&D2 TO CONTR. RADAR													1	1							1
585	RADIO TO CONTR.																					
585	SP-2 550+56 LT																					
585	8A&8B TO CONTR.	2		2					175													
585	PED 6P TO CONTR.		1		1	1		125		120								1	160		1	
585	P1&P2 TO CONTR. PREEMPT						160															
585	D2 TO CONTR. RADAR														1							
585	SP-3 550+67.5 RT																					
585	MAST ARM A																					
585	4A&4B TO CONTR.	2		2				259														
585	P1&P2 TO CONTR. PREEMPT						244											1	244		1	
585	D2 TO CONTR. RADAR															1						
585	MAST ARM B																					
585	2A&2B TO CONTR.	2		2				241														
585	P1&P2 TO CONTR. PREEMPT						224											1	224		1	
585	D1&D2 TO CONTR. RADAR													1	1							
	PEDESTALS																					
585	PS-1 549++96 LT																					
585	PED 6P TO CONTR.		1		1	1		48		43												
585	PS-2 550+03 RT																					
585	PED 2P TO CONTR.		1		1	1		251		246												
585	PS-3 550+60 RT																					
585	PED 2P TO CONTR.		1		1	1		194		189												
SUBTOTALS CARRIED TO GENERAL SUMMARY		8	4	8	4	4	710	618	774	598	210	113	1	1	2	4	1	4	710	1	4	1

TRAFFIC SIGNAL SUBSUMMARY

LAK-US-20-24.99 PART 2

CALCULATED
JDC
CHECKED
WDB

590
697



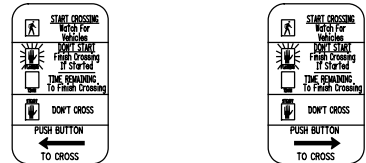
0 10 20 30 40
HORIZONTAL SCALE IN FEET

CALCULATED JDC CHECKED WDB

TRAFFIC SIGNAL PLAN
ACCESS DRIVEWAY

LAK-US-20-24.99
PART 2

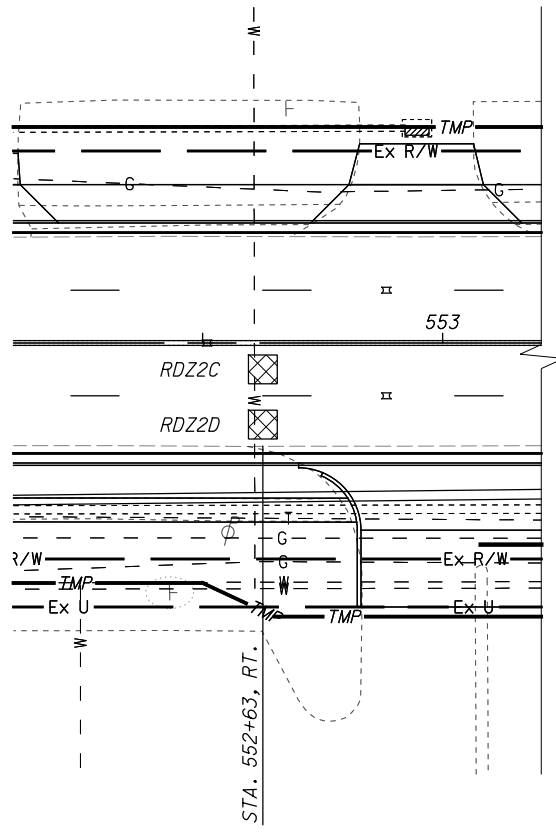
591
697



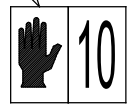
R10-3e-9 9"x15"
5 - LEFT ARROW
3 - RIGHT ARROW

PEDESTRIAN SIGNS

U.S. 20 (NORTH RIDGE ROAD)

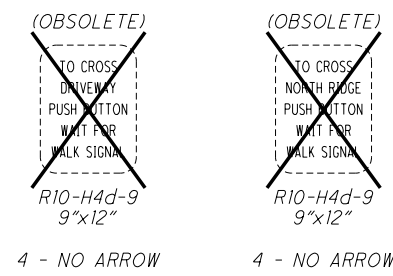
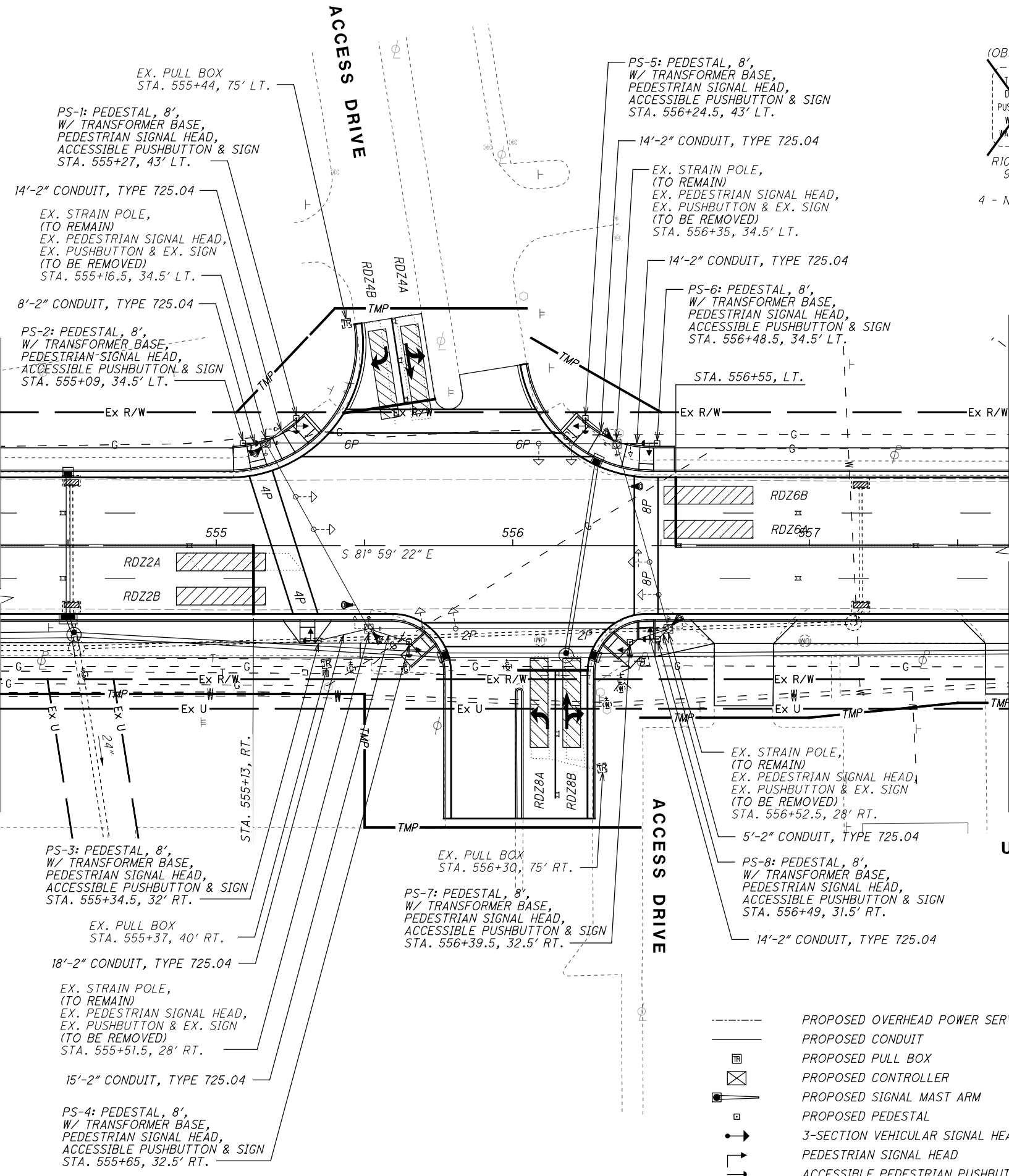


WALKING SYMBOL
ALSO APPEARS
IN THIS SECTION



PED 2P,4P,6P,8P

PEDESTRIAN SIGNAL HEADS
LED, TYPE D2, COUNTDOWN, BLACK



4 - NO ARROW
4 - NO ARROW

U.S. 20 (NORTH RIDGE ROAD)

LEGEND:

- PROPOSED OVERHEAD POWER SERVICE
- - - PROPOSED CONDUIT
- ▣ PROPOSED PULL BOX
- ⊠ PROPOSED CONTROLLER
- ⊞ PROPOSED SIGNAL MAST ARM
- PROPOSED PEDESTAL
- ⊞ 3-SECTION VEHICULAR SIGNAL HEAD
- ⊞ PEDESTRIAN SIGNAL HEAD
- ⊞ ACCESSIBLE PEDESTRIAN PUSHBUTTON
- PREEMPT CONFIRMATION LIGHT
- ⊞ PREEMPT RECEIVING UNIT
- ⊞ STOP LINE RADAR DETECTION UNIT
- ⊞ ADVANCE RADAR DETECTION UNIT
- ⊞ MAST ARM MOUNTED SIGNS
- ▨ DETECTION ZONE
- ▨ DETECTION COUNT

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\Signals\09270_CP004.dgn Sheet 7/15/2022 3:00:54 PM Troyer

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\Signals\Sheets\09270_CD004.dgn Sheet 7/15/2022 3:00:55 PM troyer

DETECTION ZONE	MOVEMENT	PULSE OR PRESENCE	ASSOCIATED PHASE	DELAY PROGRAMMED IN CONTROLLER (SEC.)	EXTENSION PROGRAMMED IN CONTROLLER (SEC.)	DETECTOR NUMBER	PURPOSE	DETECTION ZONE LENGTH (FT)
RDZ2A	EB	PRESENCE	2	-	-	DS2	STOP-LINE	30
RDZ2B	EB	PRESENCE	2	-	-	DS2	STOP-LINE	30
RDZ2C	EB	PULSE	2	-	-	DD2	ADVANCED DETECTION	6
RDZ2D	EB	PULSE	2	-	-	DD2	ADVANCED DETECTION	6
RDZ4A	SB	PRESENCE	4	-	-	DS4	STOP-LINE	30
RDZ4B	SB RT	PRESENCE	4	-	-	DS4	STOP-LINE	30
RDZ6A	WB	PRESENCE	6	-	-	DS6	STOP-LINE	30
RDZ6B	WB	PRESENCE	6	-	-	DS6	STOP-LINE	30
RDZ6C	WB	PULSE	6	-	-	DD6	ADVANCED DETECTION	6
RDZ6D	WB	PULSE	6	-	-	DD6	ADVANCED DETECTION	6
RDZ8A	NB LT	PRESENCE	8	-	-	DS8	STOP-LINE	30
RDZ8B	NB	PRESENCE	8	12	-	DS8	STOP-LINE	30
-	-	-	-	-	-	-	-	-

NOTE: ADVANCED DILEMMA ZONE SPEED THRESHOLD: 30 MPH
PURPOSE: STOP-LINE OR ADVANCED DETECTION

RADAR DETECTION CHART

ITEM	TOTAL	UNIT	DESCRIPTION
625	102	FT	CONDUIT, 2", 725.04
625	102	FT	TRENCH
625	8	EACH	GROUND ROD
630	7.5	SF	SIGN, FLAT SHEET
630	8	EACH	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL
632	8	EACH	PEDESTAL FOUNDATION
632	8	EACH	PEDESTAL, 8', TRANSFORMER BASE
632	16	EACH	REMOVAL OF MISCELLANEOUS TRAFFIC SIGNAL ITEM (PEDESTRIAN SIGNAL SYSTEM)
632	8	EACH	PEDESTRIAN SIGNAL HEAD (LED), TYPE D2, COUNTDOWN, AS PER PLAN
632	8	EACH	COVERING OF PEDESTRIAN SIGNAL HEAD
632	8	EACH	ACCESSIBLE PEDESTRIAN PUSHBUTTON
632	1,656	FT	SIGNAL CABLE, 5 CONDUCTOR, NO. 14 AWG
632	1,616	FT	LOOP DETECTOR LEAD-IN CABLE
632	293	FT	SIGNALIZATION, MISC.: UNLASH AND RELASH MESSSENGER WIRE
809	2	EACH	ADVANCE RADAR DETECTION, AS PER PLAN
809	4	EACH	STOP LINE RADAR DETECTION, AS PER PLAN

SUPPORT NO.	STATION	OFFSET	POLE HEIGHT	PEDESTRIAN SIGNAL	PEDESTRIAN PUSHBUTTON	HANDHOLE
			FT	DEG	DEG	DEG
PS-1	555+27	43' LT	8'	180	180	180
PS-2	555+09	34.5' LT	8'	90	90	180
PS-3	555+34.5	32' RT	8'	90	90	180
PS-4	555+65	32.5' RT	8'	0	0	180
PS-5	556+24.5	43' LT	8'	180	180	180
PS-6	556+48.5	34.5' LT	8'	270	270	180
PS-7	556+39.5	32.5' RT	8'	0	0	180
PS-8	556+49	31.5' RT	8'	90	90	180

PEDESTAL TABLE

NOTES:

- ALL MOVEMENTS SHALL BE ACTUATED. THE PRIMARY THRU MOVEMENT SHOULD HAVE MIN RECALL ACTIVE TO REST IN GREEN.
- FOR PROTECTED/PERMISSIVE PHASES, IMPLEMENT CALL OMITTS TO AVOID YELLOW BALL TRAP
- ENABLE ϕ 1, 3 & ϕ 5, 7 DETECTOR SWITCHING TO ALLOW ϕ 1 & ϕ 5 TO EXTEND ϕ 2 & ϕ 6 OR ϕ 3 & ϕ 7 TO EXTEND ϕ 4 & ϕ 8, RESPECTIVELY, WHEN ALLOCATED GREEN TIME FOR LEFT TURN PHASES ARE EXHAUSTED.
- COUNTDOWN PEDESTRIAN SIGNALS SHALL GO TO ZERO ON YELLOW PER OMUTCD FIGURE 4E-2.
- RADAR DETECTION UNITS FOR DILEMMA ZONE DETECTION SHALL PLACE A CONSTANT CALL TO THE CONTROLLER WHEN VEHICLE TRAVEL TIMES TO THE STOP BAR ARE BETWEEN 2.5 AND 6 SECONDS. SPEED TRIGGER SHALL BE SET FOR VEHICLES TRAVELING 35 MPH AND GREATER.
- RADAR SHALL HAVE QUEUE DETECTION CONFIGURED AND A ZONE PLACED AT 100-200 FEET FROM STOP BAR FOR SLOW MOVING VEHICLE EXTENSIONS. SPEED TRIGGER SHALL BE SET AT 1-35 MPH.
- ALL DETECTOR DELAYS SHALL BE PLACED IN THE CONTROLLER.
- FOR ANY ENTRY TO FLASHING OPERATION, PROGRAMMING SHALL RUN MINOR STREET GREEN (TYP. ϕ 4 & ϕ 8), ALL-RED CLEARANCE, AND THEN FLASHING OPERATION.



**TRAFFIC SIGNAL DETAILS
ACCESS DRIVEWAY**

H:\2018\80743\0D01\ProjectData\09270_LAK_20_EAST\Design\Signals\09270_CP005.dgn Sheet 7/15/2022 3:00:57 PM Troyer

LEGEND:

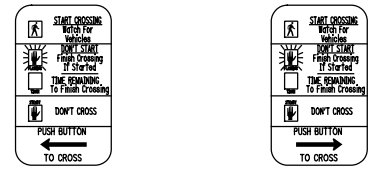
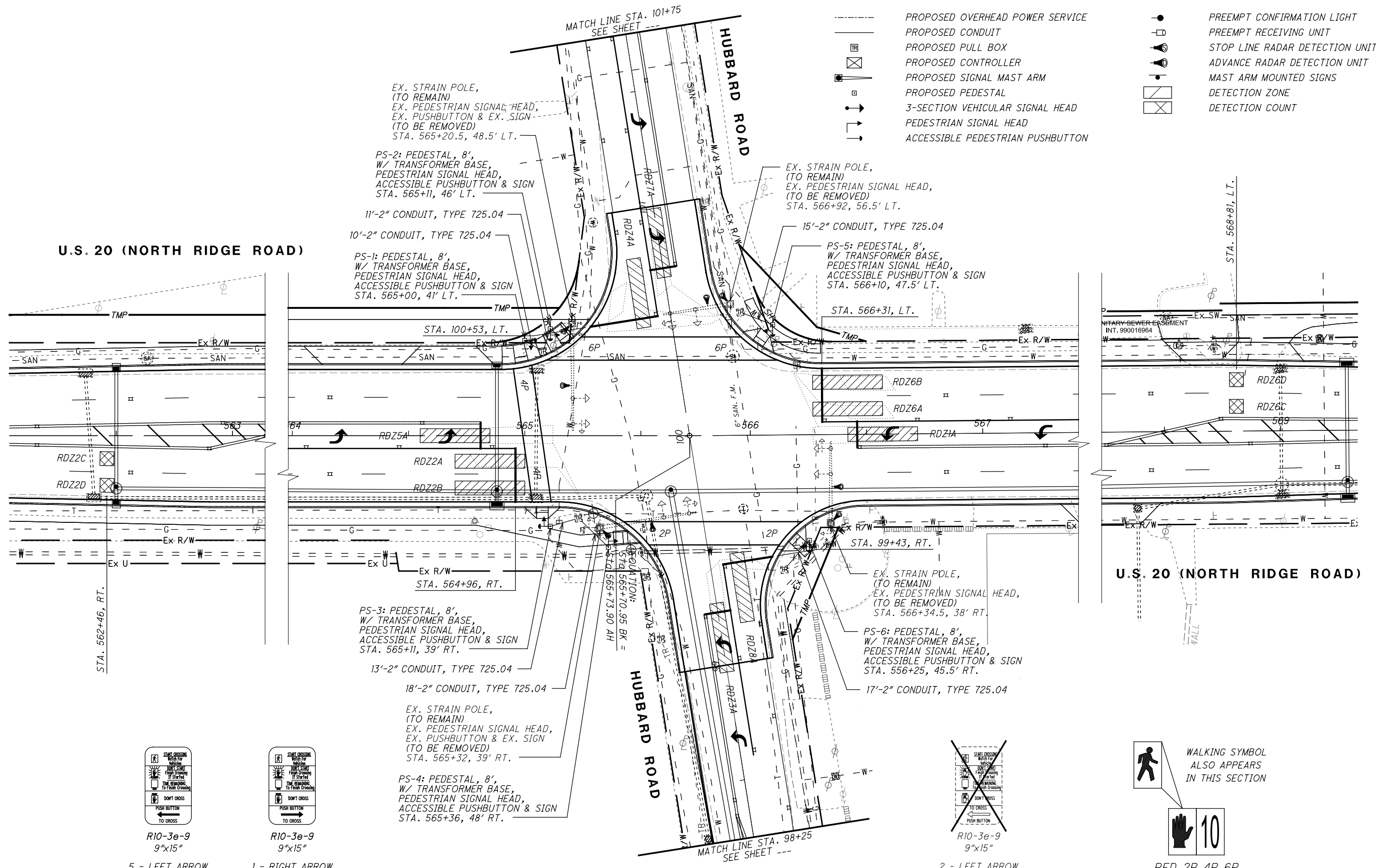
- PROPOSED OVERHEAD POWER SERVICE
- PROPOSED CONDUIT
- PROPOSED PULL BOX
- PROPOSED CONTROLLER
- PROPOSED SIGNAL MAST ARM
- PROPOSED PEDESTAL
- 3-SECTION VEHICULAR SIGNAL HEAD
- PEDESTRIAN SIGNAL HEAD
- ACCESSIBLE PEDESTRIAN PUSHBUTTON
- PREEMPT CONFIRMATION LIGHT
- PREEMPT RECEIVING UNIT
- STOP LINE RADAR DETECTION UNIT
- ADVANCE RADAR DETECTION UNIT
- MAST ARM MOUNTED SIGNS
- DETECTION ZONE
- DETECTION COUNT

CALCULATED JDC CHECKED WDB

0 20 40 HORIZONTAL SCALE IN FEET

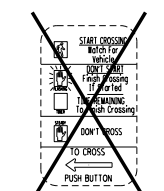
TRAFFIC SIGNAL PLAN
HUBBARD ROAD

LAK-US-20-24.99
PART 2

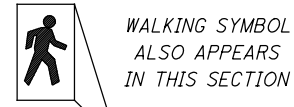


R10-3e-9 9"x15"
5 - LEFT ARROW 1 - RIGHT ARROW

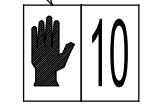
PEDESTRIAN SIGNS



R10-3e-9 9"x15"
2 - LEFT ARROW

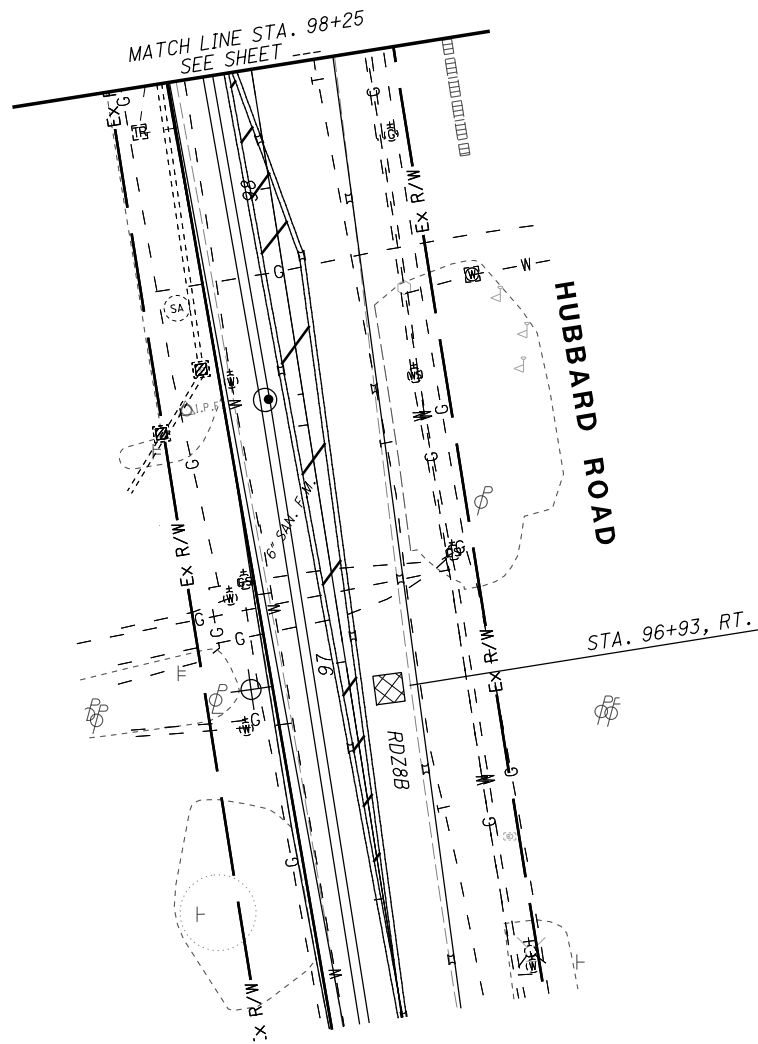
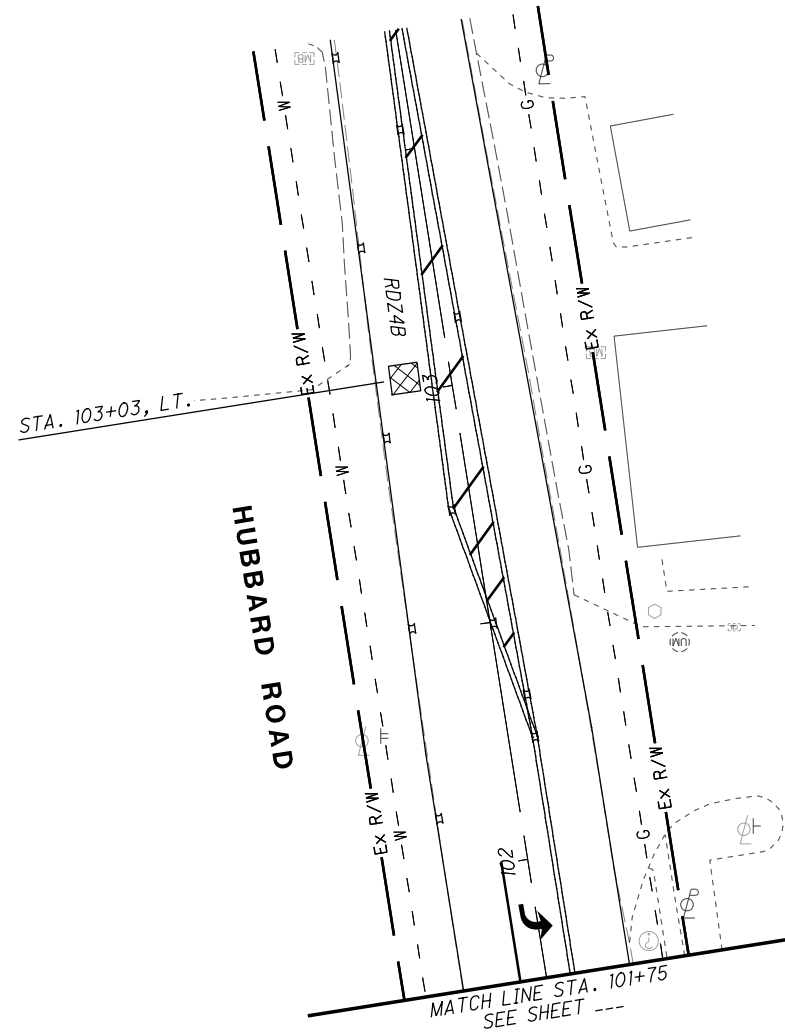


WALKING SYMBOL
ALSO APPEARS
IN THIS SECTION



PED 2P,4P,6P

PEDESTRIAN SIGNAL HEADS
LED, TYPE D2, COUNTDOWN, BLACK



- LEGEND:**
- PROPOSED OVERHEAD POWER SERVICE
 - ===== PROPOSED CONDUIT
 - ☒ PROPOSED PULL BOX
 - ☒ PROPOSED CONTROLLER
 - ☒ PROPOSED SIGNAL MAST ARM
 - ☐ PROPOSED PEDESTAL
 - ↕↕ 3-SECTION VEHICULAR SIGNAL HEAD
 - ↕↕ PEDESTRIAN SIGNAL HEAD
 - ↕↕ ACCESSIBLE PEDESTRIAN PUSHBUTTON
 - PREEMPT CONFIRMATION LIGHT
 - ⊕ PREEMPT RECEIVING UNIT
 - ⊕ STOP LINE RADAR DETECTION UNIT
 - ⊕ ADVANCE RADAR DETECTION UNIT
 - ⊕ MAST ARM MOUNTED SIGNS
 - ▨ DETECTION ZONE
 - ▨ DETECTION COUNT

CALCULATED JDC
 CHECKED WDB

0 20 40
 HORIZONTAL SCALE IN FEET

**TRAFFIC SIGNAL PLAN
 HUBBARD ROAD**

**LAK-US-20-24.99
 PART 2**

DETECTION ZONE	MOVEMENT	PULSE OR PRESENCE	ASSOCIATED PHASE	DELAY PROGRAMMED IN CONTROLLER (SEC.)	EXTENSION PROGRAMMED IN CONTROLLER (SEC.)	DETECTOR NUMBER	PURPOSE	DETECTION ZONE LENGTH (FT)
RDZ1A	WB LT	PRESENCE	1	-	-	DS1	STOP-LINE	30
RDZ2A	EB	PRESENCE	2	-	-	DS2	STOP-LINE	30
RDZ2B	EB	PRESENCE	2	-	-	DS2	STOP-LINE	30
RDZ2C	EB	PULSE	2	-	-	DD2	ADVANCED DETECTION	6
RDZ2D	EB	PULSE	2	-	-	DD2	ADVANCED DETECTION	6
RDZ3A	NB LT	PRESENCE	3	-	-	DS3	STOP-LINE	30
RDZ4A	SB	PRESENCE	4	12	-	DS4	STOP-LINE	30
RDZ4B	SB	PULSE	4	-	-	DD4	ADVANCED DETECTION	6
RDZ5A	EB LT	PRESENCE	5	-	-	DS5	STOP-LINE	30
RDZ6A	WB	PRESENCE	6	-	-	DS6	STOP-LINE	30
RDZ6B	WB	PRESENCE	6	-	-	DS6	STOP-LINE	30
RDZ6C	WB	PULSE	6	-	-	DD6	ADVANCED DETECTION	6
RDZ6D	WB	PULSE	6	-	-	DD6	ADVANCED DETECTION	6
RDZ7A	SB LT	PRESENCE	7	-	-	DS7	STOP-LINE	30
RDZ8A	NB	PRESENCE	8	12	-	DS8	STOP-LINE	30
RDZ8B	NB	PULSE	8	-	-	DD8	ADVANCED DETECTION	6
-	-	-	-	-	-	-	-	-

NOTE: ADVANCED DILEMMA ZONE SPEED THRESHOLD: 30 MPH
PURPOSE: STOP-LINE OR ADVANCED DETECTION

RADAR DETECTION CHART

ITEM	TOTAL	UNIT	DESCRIPTION
625	84	FT	CONDUIT, 2", 725.04
625	84	FT	TRENCH
625	6	EACH	GROUND ROD
630	5.63	SF	SIGN, FLAT SHEET
630	2	EACH	REMOVAL OF POLE MOUNTED SIGN AND DISPOSAL
630	6	EACH	SURFACE PREPARATION, NEW SUPPORT SECTION
630	6	EACH	COATING, ORGANIC ZINC PRIME COAT, SUPPORT SECTION
630	6	EACH	COATING, EPOXY INTERMEDIATE COAT, SUPPORT SECTION
630	6	EACH	COATING, URETHANE TOP COAT, SUPPORT SECTION
632	6	EACH	PEDESTAL FOUNDATION
632	6	EACH	PEDESTAL, 8', TRANSFORMER BASE
632	8	EACH	REMOVAL OF MISCELLANEOUS TRAFFIC SIGNAL ITEM (PEDESTRIAN SIGNAL SYSTEM)
632	6	EACH	PEDESTRIAN SIGNAL HEAD (LED), TYPE D2, COUNTDOWN, AS PER PLAN
632	6	EACH	COVERING OF PEDESTRIAN SIGNAL HEAD
632	6	EACH	ACCESSIBLE PEDESTRIAN PUSHBUTTON
632	738	FT	SIGNAL CABLE, 5 CONDUCTOR, NO. 14 AWG
632	708	FT	LOOP DETECTOR LEAD-IN CABLE
809	6	EACH	ADVANCE RADAR DETECTION, AS PER PLAN
809	10	EACH	STOP LINE RADAR DETECTION, AS PER PLAN

NOTES:

- ALL MOVEMENTS SHALL BE ACTUATED. THE PRIMARY THRU MOVEMENT SHOULD HAVE MIN RECALL ACTIVE TO REST IN GREEN.
- FOR PROTECTED/PERMISSIVE PHASES, IMPLEMENT CALL OMITTS TO AVOID YELLOW BALL TRAP
- ENABLE ϕ 1, 3 & ϕ 5, 7 DETECTOR SWITCHING TO ALLOW ϕ 1 & ϕ 5 TO EXTEND ϕ 2 & ϕ 6 OR ϕ 3 & ϕ 7 TO EXTEND ϕ 4 & ϕ 8, RESPECTIVELY, WHEN ALLOCATED GREEN TIME FOR LEFT TURN PHASES ARE EXHAUSTED.
- COUNTDOWN PEDESTRIAN SIGNALS SHALL GO TO ZERO ON YELLOW PER OMUTCD FIGURE 4E-2.
- RADAR DETECTION UNITS FOR DILEMMA ZONE DETECTION SHALL PLACE A CONSTANT CALL TO THE CONTROLLER WHEN VEHICLE TRAVEL TIMES TO THE STOP BAR ARE BETWEEN 2.5 AND 6 SECONDS. SPEED TRIGGER SHALL BE SET FOR VEHICLES TRAVELING 35 MPH AND GREATER.
- RADAR SHALL HAVE QUEUE DETECTION CONFIGURED AND A ZONE PLACED AT 100-200 FEET FROM STOP BAR FOR SLOW MOVING VEHICLE EXTENSIONS. SPEED TRIGGER SHALL BE SET AT 1-35 MPH.
- ALL DETECTOR DELAYS SHALL BE PLACED IN THE CONTROLLER.
- FOR ANY ENTRY TO FLASHING OPERATION, PROGRAMMING SHALL RUN MINOR STREET GREEN (TYP. ϕ 4 & ϕ 8), ALL-RED CLEARANCE, AND THEN FLASHING OPERATION.

SUPPORT NO.	STATION	OFFSET	POLE HEIGHT	PEDESTRIAN SIGNAL	PEDESTRIAN PUSHBUTTON	HANDHOLE
			FT	DEG	DEG	
PS-1	565+00	41' LT	8'	90	90	180
PS-2	565+11	46' LT	8'	180	180	180
PS-3	565+11	39' RT	8'	90	90	180
PS-4	565+36	48' RT	8'	180	180	180
PS-5	566+10	47.5' LT	8'	180	180	180
PS-6	556+25	45.5' RT	8'	0	0	180

PEDESTAL TABLE



**TRAFFIC SIGNAL DETAILS
HUBBARD ROAD**

NOTES:

Overhead sign supports can be separated into major sections such as end frames, trusses, vertical poles and cantilever arms. For the implementation of this work item it will be beneficial to refer to the major sections of the overhead sign supports rather than the whole support. More specific instructions and flexibility can be given based upon the unit of measure and payment per major support section.

The protective coating of overhead sign support sections shall be a four part process to include surface preparation followed by a three coat paint system. This three coat system shall consist of an organic zinc prime coat, an epoxy intermediate coat and a urethane finish coat, with each coat being a different color. The purpose of this coating is to provide protection for new (unweathered) and older (weathered) galvanized steel support sections from corrosive elements in the atmosphere. Coating and surface preparation of new galvanized support sections should be done by the manufacturer.

In the field, the Contractor shall take all necessary precautions to comply with pollution laws, rules or regulations of Federal, State or Local agencies. The coating materials specified for the work can be hazardous to the health of the applicator if not applied as per manufacturer's instruction. The Contractor shall follow the data sheet and the label on the paint containers. These precautions shall include the use of respirators and eye and skin protection as specified. The Contractor shall also insure that his painting operations and locations will not endanger or adversely affect the public in general.

The proposed cleaning and coating operations shall be performed only when the ambient temperature is 50°F or above. Paint shall not be applied during rain, fog or mist, or when the steel surface temperature is less than 5°F above the dew point. Paint shall not be applied to wet or damp surfaces or on frosted or ice-coated surfaces. Paint shall not be applied when the relative humidity is greater than 85%. All steel surfaces of trusses and end frames including the welded areas, ballast enclosure mounting bracket and base plates are to be cleaned and coated. Before each coating is applied, it shall be mixed with an approved power mechanical mixer to a uniform consistency which shall be maintained during its application. Each coat shall be applied in a workman-like manner as a continuous film of uniform thickness which is free of holidays, pores, runs or sags. All coats shall be applied by brush or roller. Thinning of paint is strictly prohibited. Paint not capable of being applied as specified shall not be used. The coating shall penetrate all joints and connections. The Engineer shall be notified 24 hours prior to any cleaning or coating operations so that inspection services can be provided.

COATING SYSTEM

The coating system shall be a three coat paint system conforming to CMS 708.02. Supply the primer, intermediate and finish coats from the same manufacturer.

SURFACE PREPARATION, EXISTING SUPPORT SECTIONS

Existing, weathered galvanized support sections should have their surface preparation as well as their protective coating done under conditions of temperature and humidity within the same range as specified by the manufacturer of the organic zinc prime coat material

to be used immediately after this cleaning operation. The support sections shall be prepared for coating by SSPC-SP1 (solvent cleaning) followed by SSPC-SP10 (near white blast cleaning). Before the prepared surface degrades from the prescribed standards, the prime coat shall be applied. In every case, the surface shall be coated with organic zinc prime coat on the same day as the surface preparation. Careful handling and storage will be required to prevent any scraping, marring, or other damage to the prepared surface. Payment shall include all labor, equipment, handling, transportation costs and materials necessary to accomplish this item of work per major support section.

Basis of payment will be as follows:

Item 630 Surface Preparation, Existing Support Section at contract bid price per each major support section.

SURFACE PREPARATION, NEW SUPPORT SECTIONS

New, unweathered galvanized support sections should have their surface preparation as well as their protective coating done at the manufacturer of the support sections.

The support sections shall be prepared for coating by SSPC-SP1 (solvent cleaning) (Do not use alkaline cleaners) followed by SSPC-SP7 (100% brush-off blast of surface). Before the prepared surface degrades from the prescribed standards, the prime coat shall be applied. In every case, the surface shall be coated with the organic zinc prime coat on the same day of surface preparation. Careful handling and storage will be required to prevent any scraping, marring, or other damage to the prepared surface. Payment shall include all labor, equipment, handling, transportation costs and materials necessary to accomplish this item of work per major support section.

Basis of payment will be as follows:

Item 630 Surface Preparation, New Support Section at contract bid price per each major support section.

COATING, ORGANIC ZINC PRIME COAT, SUPPORT SECTIONS

This item shall consist of the application of one coat of an organic zinc primer to support sections. The total dry film thickness of this coat shall be between 1.5 to 2.0 mils. If more than one pass is necessary to obtain the required thickness, that cost shall be borne by the Contractor. The color of this coat shall be noticeably different from the base material and other proposed coats. This coat shall, in all cases, be applied over surfaces that were prepared earlier that same day. The thinning of the material is strictly prohibited. Material not capable of being applied as specified shall not be used.

When the average dry film thickness of this coat over the entire support section is less than the specified 1.5 to 2.0 mils but is at least 1.25 mils, the contract bid price for this item shall be reduced in direct proportion to the percent deficiency of coating up to 16 2/3%. If the deficiency of coating is more than 16 2/3% (i.e. the average dry film thickness is less than 1.25 mils) the work for this item shall be considered unsatisfactory and shall be recoated at the full expense of the Contractor, including all labor, equipment, and material.

For new support sections the prime coat should be done at the manufacturer of the support sections. Verification by the manufacturer of the coating material for the prime coat will be required. Careful handling and storage will be required to prevent any scraping, marring, or other surface damage to the prime coat.

Payment shall include all labor, equipment, handling costs, and materials necessary to accomplish this item of work. This prime coat shall be manufactured by the same company supplying the intermediate and top coats. A properly calibrated dry film thickness instrument will be used to check the coating.

Basis of payment will be as follows:

Item 630 Coating, Organic Zinc Prime Coat, Support Section at contract bid price per each major support section.

COATING, EPOXY INTERMEDIATE COAT, SUPPORT SECTIONS

This item shall consist of the application of one coat of epoxy to support sections. The total dry film thickness of this coat shall not be less than 6.0 mils. If more than one pass is necessary to obtain the required thickness, that cost shall be borne by the Contractor. Thinning of the material is strictly prohibited. Material not capable of being applied as specified shall not be used.

When the average dry film thickness of this coat over the entire support section is less than the specified 6.0 mils, but is at least 5.0 mils, the contract price for this item shall be reduced in direct proportion to the percent deficiency of coating up to 16 2/3%. If the deficiency of coating is more than 16 2/3% (i.e. the average dry film thickness is less than 5.0 mils), the work for this item shall be considered unsatisfactory and shall be recoated at the full expense of the Contractor, including all labor, equipment, and material.

At least 24 hours, but no more than three days, shall elapse after the application of the organic zinc prime coat and before the application of the epoxy intermediate coat. Surfaces shall, in all cases, be clean before the intermediate coat is applied.

For new support sections, this intermediate coat should be done at the manufacturer of the support sections. Verification by the manufacturer for the intermediate coat will be required. Careful handling and storage will be required to prevent any scraping, marring or other surface damage to the intermediate coat.

Payment shall include all labor, equipment, handling cost and material necessary to accomplish this item of work. This intermediate coat shall be manufactured by the same company supplying the prime and top coats. A properly calibrated dry film thickness instrument will be used to check the coating.

Basis of payment will be as follows:

Item 630 Coating, Epoxy Intermediate Coat, Support Section at contract bid price per major support section.

COATING, URETHANE FINISH COAT, SUPPORT SECTIONS

This item shall consist of the application of one coat of urethane to support sections. The total dry film thickness of this coat shall not be less than 1.5 mils. If more than one pass is necessary to obtain the required thickness, that cost shall be borne by the Contractor. Thinning of the material is strictly prohibited. Material not capable of being applied as specified shall not be used. The color of this coat shall be medium grey.

When the average dry film thickness of this coat over the entire support section is less than the specified 1.5 mils, but is at least 1.0 mils, the contract price for this item shall be reduced in direct proportion to the percent deficiency of coating up to 33 1/3%. If the deficiency of coating is more than 33 1/3% (i.e. the average dry film thickness is less than 1.0 mils), the work for this item shall be considered unsatisfactory and shall be recoated at the full expense of the Contractor, including all labor, equipment, and material.

At least 24 hours, but no more than three days, shall elapse after the application of the epoxy intermediate coat and before the application of the urethane finish coat. Surfaces shall, in all cases, be clean before the finish coat is applied.

For new support sections, this finish coat should be done by the manufacturer of the support sections. Verification by the manufacturer for the finish coat will be required. Careful handling and storage will be required to prevent any scraping, marring or other surface damage to the finish coat.

Payment shall include all labor, equipment, handling cost and materials necessary to accomplish this item of work. This finish coat shall be manufactured by the same company supplying the prime and intermediate coats. A properly calibrated, dry film thickness instrument will be used to check the coating.

Basis of payment will be as follows:

Item 630 Coating, Urethane Finish Coat, Support Section at contract bid price per each major support section.

DESIGNED XXX	REVIEWED XXX	OFFICE OF ROADWAY ENGINEERING
REVISION DATE 7 / 18 / 2014		
PLAN INSERT SHEET		
PROTECTIVE COATING OF OVERHEAD SIGN SUPPORT SECTIONS		
PIS 202310		
THIS DRAWING REPLACES PIS 202310 DATED 01-21-2011.		
1 / 1		
596 697		

MATERIAL SPECIFICATIONS FOR BBS GENERATOR POWER PANEL EQUIPMENT

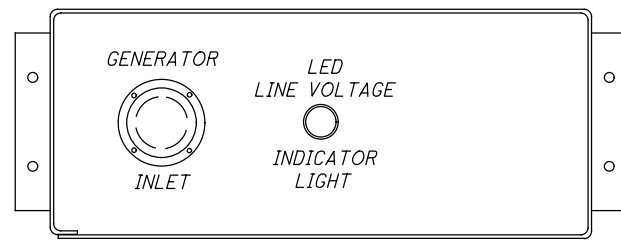
GENERATOR INLET - The inlet shall be 30 amp, 125/250V, locking, four (4) wire grounding and meet the NEMA configuration number L14-30-P 30A 125/250V specification. The inlet shall be a Hubbell catalog #2715.

LINE VOLTAGE GENERATOR SWITCH - The switch shall be 30 amp, 125/250V AC, two (2) pole, three (3) position (On, Off, On). The switch shall be a Hubbell catalog #1388.

LINE VOLTAGE INDICATOR LIGHT - The indicator light shall be 125V AC light emitting diode with a red lens.

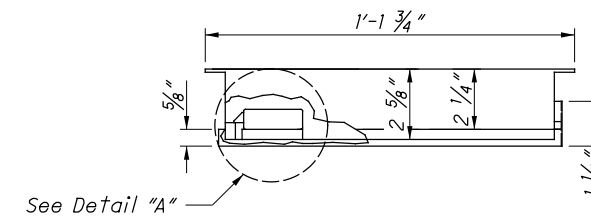
LINE VOLTAGE CIRCUIT BREAKER - The circuit breaker shall be single pole single throw and a minimum of 30 amps. The amperage shall be increased to accommodate greater loads, if necessary. The gauge of the power cable shall be of proper size per N.E.C.

EXTERNAL LINE VOLTAGE INDICATOR LIGHT - The indicator light shall be a 1" waterproof NEMA 4X or IP66 LED lamp with a green lens.

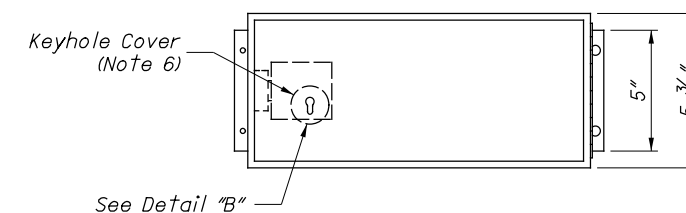


FRONT VIEW OF GENERATOR POWER PANEL

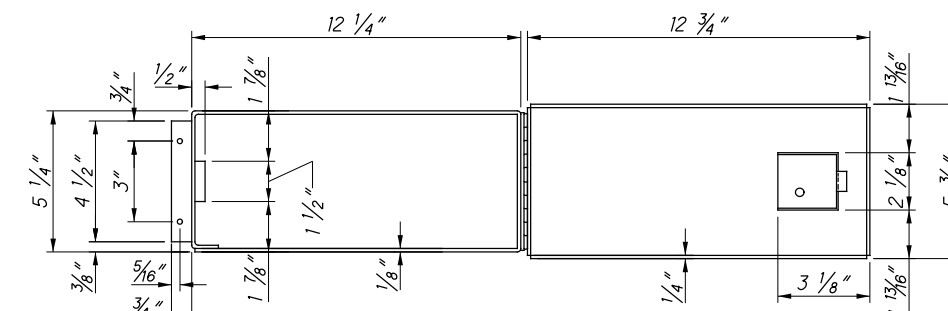
GENERATOR POWER PANEL ENCLOSURE



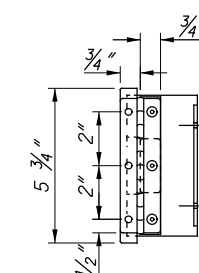
TOP VIEW



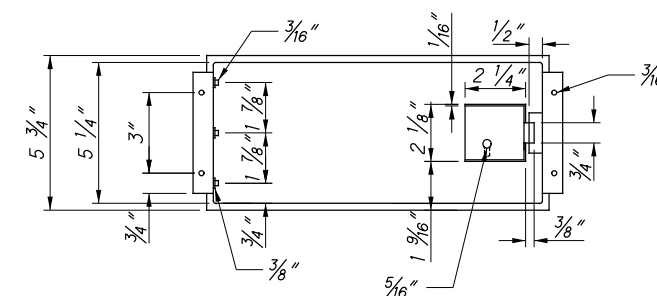
FRONT VIEW CLOSED DOOR



FRONT VIEW OPEN DOOR



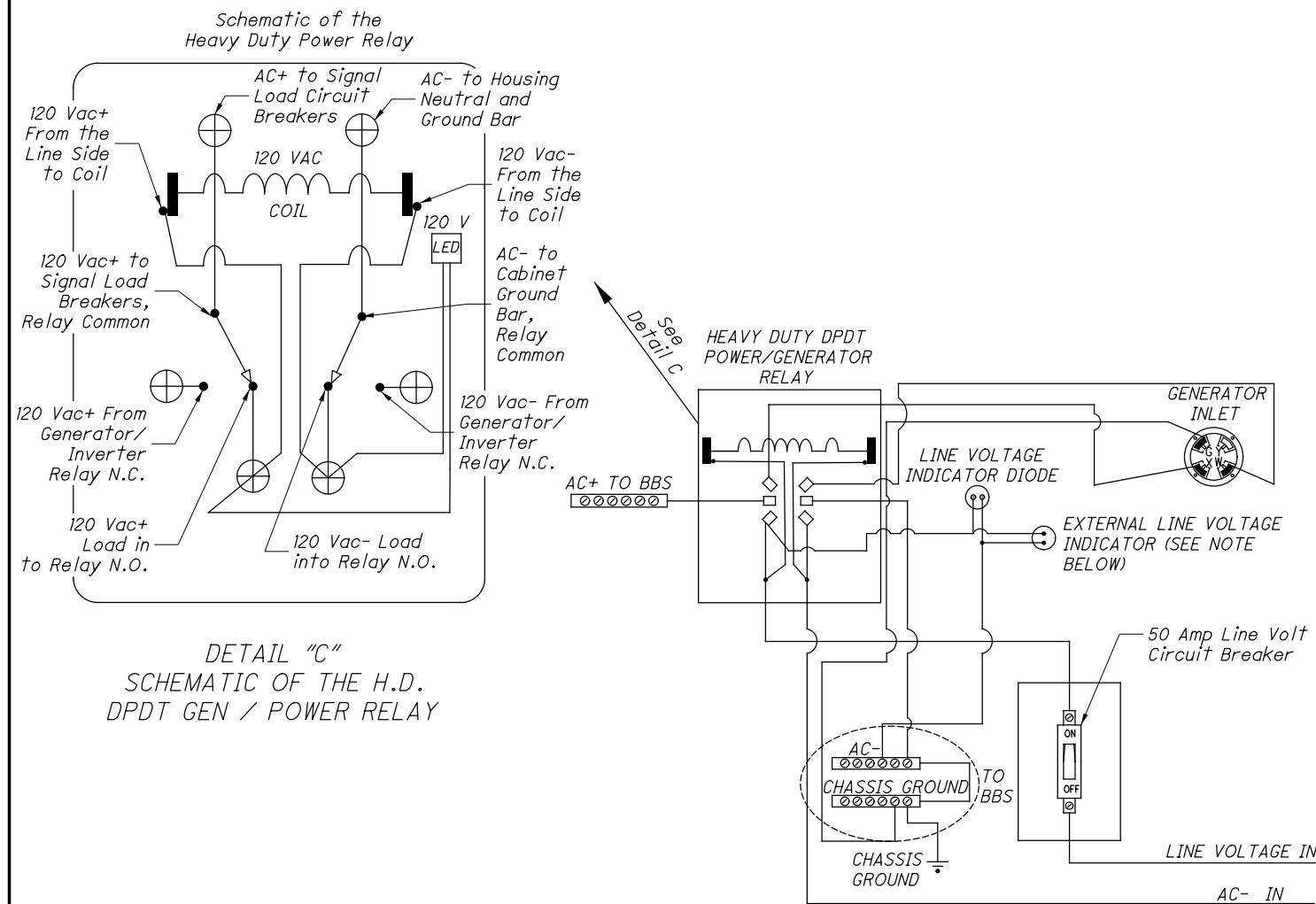
RIGHT SIDE VIEW CLOSED DOOR



BACK VIEW CLOSED DOOR

NOTES:

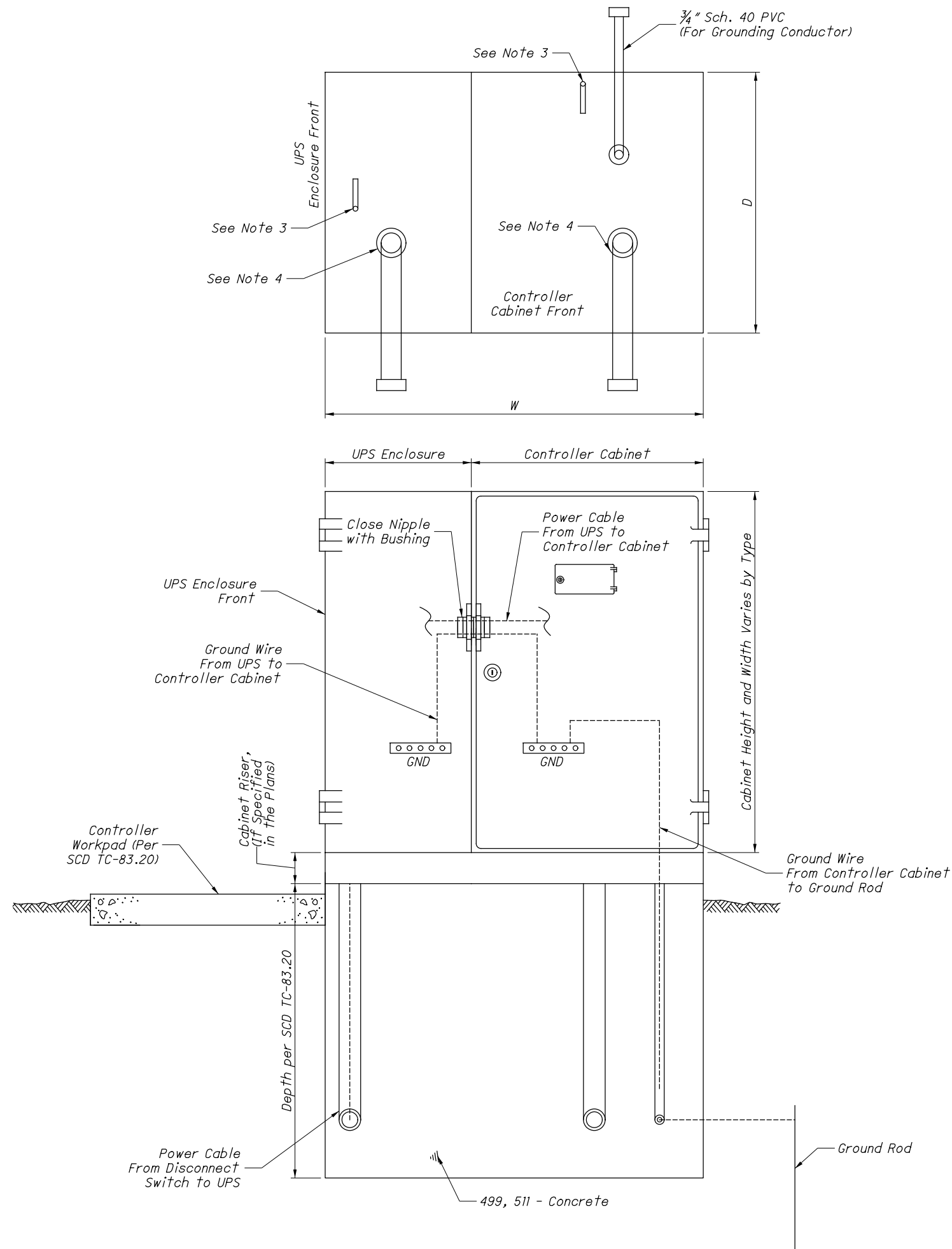
1. The enclosure shall be constructed of 1/8" thick aluminum.
2. The lock shall be the standard police door type, keyed with the standard flasher door skeleton key.
3. The door shall be sealed with a foam rubber gasket to prevent moisture from entering the enclosure.
4. The enclosure shall be mounted onto the outside of the controller cabinet with non-accessible bolts and sealed with a high quality silicon caulk at all surfaces touching the cabinet.
5. The hinge shall be of stainless steel or equivalent corrosive-resistant material.
6. Keyhole shall be covered with a movable circular aluminum or brass cover with top pivot pin.



DETAIL "C"
SCHEMATIC OF THE H.D.
DPDT GEN / POWER RELAY

ELECTRICAL HOOKUP DETAIL FOR THE BBS GENERATOR POWER PANEL

NOTE: EXTERNAL LINE VOLTAGE INDICATOR LIGHT required when called for in the plans.
EXTERNAL LINE VOLTAGE INDICATOR LIGHT shall be located on the enclosure exterior for visibility from the adjacent roadway when all cabinet, and generator panel doors are closed.



NOTES:

1. The Uninterruptible Power Supply (UPS) enclosure shall be mounted flush up against the traffic signal cabinet and sealed with silicone. The Contractor shall be responsible for providing the necessary power cable between the UPS unit and signal cabinet.
2. The UPS should be placed on the opposite side of the pull box on a 332/336 cabinet (per Standard Construction Drawing (SCD) TC-83.20). The UPS placement for a NEMA cabinet varies, placement should provide adequate access with respect to slope, guardrail spacing, etc.
3. The size, number, and location of anchor bolts shall be in accordance with the manufacturer's recommendations.
4. The size, number, and orientation of conduit ells shall be as shown in the plan, except that a 3/4" schedule 40 PVC shall be installed in each foundation.
5. 1/2" preformed joint filler as per CMS 705.03 shall be used between foundations and adjacent paved areas.
6. See SCD TC-83.20 for further details.

TYPE	W (IN.)	D (IN.)	FOUNDATION CONCRETE (CU. YD.)
TS-1	60	24	1.23
TS-2	70	36	2.16
2070/170	50	36	1.54

THIS DRAWING REPLACES PIS 208320 DATED 04-20-2012.

SEPAC AND ASC/3 INPUT FILE INFORMATION FOR THE 332 CABINET

UPPER INPUT FILE (FILE=I)

C U P P A N E N R E L	PHASE	1	2	2	2	3	4	4	4	1	MANUAL CONTROL ADV.	2	6	FLASH	
	DEFAULT FUNCTION	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	SPARE	PED	PED	SENSE	
	SEPAC DETECTOR NO.	VEH 1	VEH 3	VEH 5	VEH 7	VEH 9	VEH 11	VEH 13	VEH 15	VEH 17		PED 2	PED 6		
	ASC/3 DETECTOR NO.	VEH 1	VEH 2	VEH 3	VEH 4	VEH 5	VEH 6	VEH 7	VEH 8	VEH 9		PED 2	PED 6		
	C1 PIN NUMBER	56	39	63	47	58	41	65	49	60		80	67	68	81
FIELD TERMINALS	1-D,E	2-D,E	3-D,E	4-D,E	5-D,E	6-D,E	7-D,E	8-D,E	9-D,E	10-D,E	11-D,E	12-D,E	13-D,E	14-D,E	
SLOT NUMBER		1	2	3	4	5	6	7	8	9	10	11	12	13	14
C L O W N E R L	PHASE	1	2	2	2	3	4	4	4	3	ADV.	4	8	STOP	
	DEFAULT FUNCTION	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	SPARE	ENABLE	PED	PED	TIME
	SEPAC DETECTOR NO.	VEH 1	VEH 4	VEH 6	VEH 7	VEH 9	VEH 12	VEH 14	VEH 15	VEH 18			PED 4	PED 8	
	ASC/3 DETECTOR NO.	VEH 1	VEH 10	VEH 11	VEH 4	VEH 5	VEH 14	VEH 15	VEH 8	VEH 13			PED 4	PED 8	
	C1 PIN NUMBER	56	43	76	47	58	45	78	49	62		53	69	70	82
FIELD TERMINALS	1-J,K	2-J,K	3-J,K	4-J,K	5-J,K	6-J,K	7-J,K	8-J,K	9-J,K	10-J,K	11-J,K	12-J,K	13-J,K	14-J,K	

LOWER INPUT FILE (FILE=J)

C U P P A N E N R E L	PHASE	5	6	6	6	7	8	8	8	5	SPARE	SPARE	EV - A	EV - B	RR - 1
	DEFAULT FUNCTION	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	SPARE	SPARE			
	SEPAC DETECTOR NO.	VEH 19	VEH 21	VEH 23	VEH 25	VEH 29	VEH 31	VEH 33	VEH 35	VEH 37					
	ASC/3 DETECTOR NO.	VEH 17	VEH 18	VEH 19	VEH 20	VEH 21	VEH 22	VEH 23	VEH 24	VEH 25					
	C1 PIN NUMBER	55	40	64	48	57	42	66	50	59		54	71	72	51
FIELD TERMINALS	1-D,E	2-D,E	3-D,E	4-D,E	5-D,E	6-D,E	7-D,E	8-D,E	9-D,E	10-D,E	11-D,E	12-D,E	13-D,E	14-D,E	
SLOT NUMBER		1	2	3	4	5	6	7	8	9	10	11	12	13	14
C L O W N E R L	PHASE	5	6	6	6	7	8	8	8	7	SPARE	SPARE	EV - C	EV - D	RR - 2
	DEFAULT FUNCTION	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	SPARE	SPARE			
	SEPAC DETECTOR NO.	VEH 19	VEH 22	VEH 24	VEH 25	VEH 29	VEH 32	VEH 34	VEH 35	VEH 38					
	ASC/3 DETECTOR NO.	VEH 17	VEH 26	VEH 27	VEH 20	VEH 21	VEH 30	VEH 31	VEH 24	VEH 29					
	C1 PIN NUMBER	55	44	77	48	57	46	79	50	61		75	73	74	52
FIELD TERMINALS	1-J,K	2-J,K	3-J,K	4-J,K	5-J,K	6-J,K	7-J,K	8-J,K	9-J,K	10-J,K	11-J,K	12-J,K	13-J,K	14-J,K	

SEPAC AND ASC/3 INPUT FILE INFORMATION FOR THE 336 CABINET

C U P P A N E N R E L	PHASE	1	2	3	4	5	6	7	8			2	6	FLASH	
	DEFAULT FUNCTION	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	RR - 1	EV - A	EV - B	PED	PED	SENSE
	SEPAC DETECTOR NO.	VEH 1	VEH 3	VEH 9	VEH 11	VEH 19	VEH 21	VEH 29	VEH 31				PED 2	PED 6	
	ASC/3 DETECTOR NO.	VEH 1	VEH 2	VEH 5	VEH 6	VEH 17	VEH 18	VEH 21	VEH 22				PED 2	PED 6	
	C1 PIN NUMBER	56	39	58	41	55	40	57	42	51	71	72	67	68	81
FIELD TERMINALS	1-D,E	2-D,E	3-D,E	4-D,E	5-D,E	6-D,E	7-D,E	8-D,E	9-D,E	10-D,E	11-D,E	12-D,E	13-D,E	14-D,E	
SLOT NUMBER		1	2	3	4	5	6	7	8	9	10	11	12	13	14
C L O W N E N R E L	PHASE	2	2	4	4	6	6	8	8			4	8	STOP	
	DEFAULT FUNCTION	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	EXT-CALL	RR - 2	EV - C	EV - D	PED	PED	TIME
	SEPAC DETECTOR NO.	VEH 7	VEH 4	VEH 15	VEH 12	VEH 25	VEH 22	VEH 35	VEH 32				PED 4	PED 8	
	ASC/3 DETECTOR NO.	VEH 4	VEH 10	VEH 8	VEH 14	VEH 20	VEH 26	VEH 24	VEH 30				PED 4	PED 8	
	C1 PIN NUMBER	47	43	49	45	48	44	50	46	52	73	74	69	70	82
FIELD TERMINALS	1-J,K	2-J,K	3-J,K	4-J,K	5-J,K	6-J,K	7-J,K	8-J,K	9-J,K	10-J,K	11-J,K	12-J,K	13-J,K	14-J,K	

H:\2018\180743\ODOT\ProjectData\109270_LAK_20_EAST\Design\Signals\Sheets\109270_CN008_0203324_07-15-2016.dgn Sheet 7/15/2022 3:01:02 PM troyer

THIS DRAWING REPLACES PIS 203324 DATED 10-18-2013.

DESIGNED XXX	REVIEWED XXX	OFFICE OF ROADWAY ENGINEERING
PLAN INSERT SHEET		
SEPAC AND ASC / 3 INPUT FILE INFORMATION FOR 332 AND 336 CABINETS		
PIS 203324 PART 2		
1 / 1		
599 697		



50
100
200
HORIZONTAL
SCALE IN FEET

PID NO.
109270

R/W DESIGNER
PL
R/W REVIEWER
TMM

CENTERLINE PLAT

**LAK-US-20-24.99
PART 2**

2 / 98

601
697

LAK-20-24.99

PART OF LOTS 5 & 6
TRACT 4
MADISON TOWNSHIP

MONUMENT TABLE			
of U.S. ROUTE 20			PINS TO BE RESET
STATION	OFFSET	DESCRIPTION	
412+88.04	35.99 RT	IRON PIN FOUND	1
415+24.70	32.98 LT	IRON PIPE FOUND	1
415+59.65	35.74 RT	IRON PIN FOUND	1
424+56.23	30.06 RT	IRON PIPE FOUND	1
429+26.44	44.11 LT	IRON PIPE FOUND	1
TOTAL PINS TO BE RE-SET			5

MONUMENT TABLE							
of U.S. ROUTE 20		PROJECT COORDINATES SEE SURVEY CERTIFICATION		MONUMENTS TO BE SET DURING CONSTRUCTION		R/W MON. EXPECTED TO BE DISTURBED	
STATION	OFFSET	NORTH (Y)	EAST (X)	MON. ASSY.	REF. MON.	R/W MON.	DESCRIPTION
405+83.06	℄	777382.696	2349139.427				MONUMENT BOX FND
412+94.74	℄	777604.923	2349815.519	1			MONUMENT ASSEMBLY BOX
425+00.00	℄	777994.591	2350956.050				MONUMENT BOX FOUND
431+47.75	℄	778204.283	2351568.628	1			MONUMENT BOX FND & SET
TOTAL CARRIED TO GENERAL SUMMARY SHEET				2			

NOTE: THE EXISTING R/W WIDTH AND LOCATION WERE DETERMINED USING THE FOLLOWING PLANS:
 CLEVELAND-BUFFALO ROAD SEC. Q (1917)
 CLEVELAND-BUFFALO ROAD S.H. 2 SEC. Q (PT.)
 U.S.R. 20 24.98-28.01 (1940)
 LAKE COUNTY S.H. 2 SEC. Q (PT.) PROPERTY PLAN
 LAK-20-18.72 (PID 20343) VARIOUS INTERSECTIONS (2003)
 LAK-20.25.52 (PID93601) (2014)

BASIS FOR BEARINGS:

ALL BEARINGS SHOWN ARE FOR PROJECT USE ONLY AND ARE BASED ON OHIO STATE PLANE (3401-NORTH ZONE) COORDINATE SYSTEM NAD83 (2011)

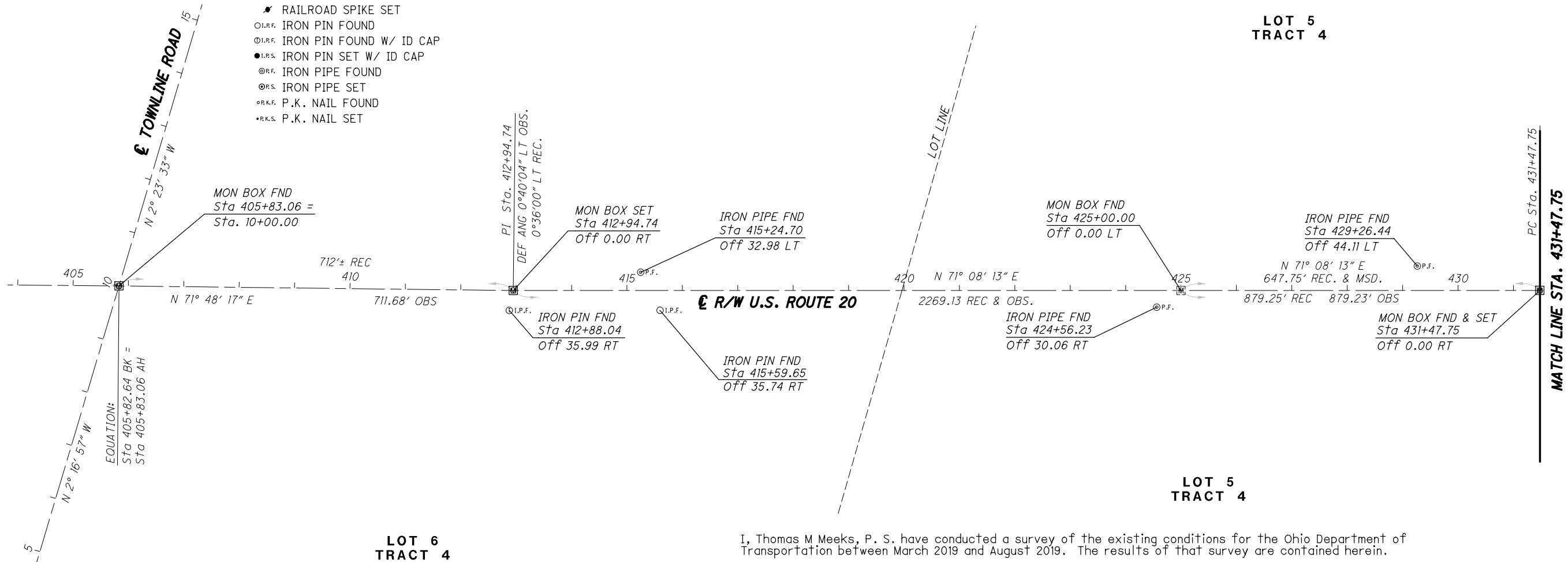
MONUMENT LEGEND

- ☐ EXISTING R/W MONUMENT BOX
- ▣ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- ⊙ P.K.F. P.K. NAIL FOUND
- ⊙ P.K.S. P.K. NAIL SET

**LOT 6
TRACT 4**

**LOT 5
TRACT 4**

**LOT 5
TRACT 4**



SETTING OF ALL MONUMENTS SHALL BE PERFORMED BY A SURVEYOR REGISTERED IN THE STATE OF OHIO. THE MONUMENT ASSEMBLIES AND REFERENCE MONUMENTS WILL BE INSTALLED BY THE CONTRACTOR AT THE TIME OF CONSTRUCTION. THE IRON PIN AND CAP (WHEN REQUIRED) ARE TO BE INSTALLED BY THE CONTRACTOR'S SURVEYOR.

CHANGES OR ALTERATIONS TO THE LOCATION OF ANY MONUMENTS SHOWN IN THIS TABLE, REQUIRE PRIOR APPROVAL FROM THE DISTRICT REAL ESTATE ADMINISTRATOR OF THE OHIO DEPARTMENT OF TRANSPORTATION. IN THE EVENT THAT CHANGES OR ALTERATIONS ARE APPROVED, A REVISED CENTERLINE PLAT WITH THE NEW LOCATIONS SHALL BE RECORDED IN THE APPLICABLE COUNTY RECORDS AND THE OHIO DEPARTMENT OF TRANSPORTATION. SPECIFICATIONS FOR MONUMENT ASSEMBLIES, REFERENCE MONUMENTS AND RIGHT OF WAY MONUMENTS ARE SHOWN ON STANDARD CONSTRUCTION DRAWING RM-1.1.

I, Thomas M Meeks, P. S. have conducted a survey of the existing conditions for the Ohio Department of Transportation between March 2019 and August 2019. The results of that survey are contained herein.

The horizontal coordinates expressed herein are based on the Ohio State Plane Coordinates System North Zone on NAD 83 2011 datum. The Project Coordinates (US Survey Feet) are relative to State Plane Grid Coordinates (Meters or US Survey Feet) by a Project Adjustment Factor of 1.00000885.

As a part of this project I have reestablished the locations of the existing property lines and the existing centerline of Right of Way for property takes contained herein. As a part of this project I have established the proposed property lines, calculated the Gross Take, present roadway occupied (PRO), Net Take and Net Residue; as well as prepared the legal descriptions necessary to acquire the parcels as shown herein.

As a part of this work I have set right of way monuments at the property corners, property line intersection, points along the right of way and/or angle points on the right of way, Section Corners and other points as shown herein.

All of my work contained herein was conducted in accordance with Ohio Administrative Code 4733-37 commonly known as "Minimum Standards for Boundary Surveys in the State of Ohio" unless noted.

The words I and my as used herein are to mean either myself or someone working under my direct supervision.

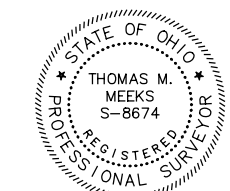
Thomas M. Meeks, Professional Land Surveyor 8674

Date:

H:\2018\180743\0DOT\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RCO01.dgn Sheet 7/15/2022 3:01:03 PM troyer

RECEIVED _____, 20____
 RECORDED _____, 20____
 BOOK _____ PAGE _____
 COUNTY RECORDER

SURVEYORS SEAL



H:\2018\180743\0010\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RC002.dgn Sheet 7/15/2022 3:01:04 PM troyer

MONUMENT TABLE						
of U.S. ROUTE 20		PROJECT COORDINATES SEE SURVEY CERTIFICATION		MONUMENTS TO BE SET DURING CONSTRUCTION		R/W MON. EXPECTED TO BE DISTURBED
STATION	OFFSET	NORTH (Y)	EAST (X)	MON. ASSY.	REF. MON.	R/W MON.
431+47.75	℄	778204.012	2351569.013	1		
433+79.23	1.77' RT	778279.031	2351788.026			
433+79.82	℄	778280.899	2351787.980	1		
439+79.73	℄	778496.712	2352347.673	1		
444+00.06	℄	778656.477	2352736.451			
444+20.01	℄	778664.060	2352754.904	1		
452+70.03	℄	779000.448	2353535.484	1		
TOTAL CARRIED TO GENERAL SUMMARY SHEET				5		

LAK-20-24.99

PART OF LOTS 3, 4 & 5 TRACT 4 MADISON TOWNSHIP

MONUMENT TABLE			
of U.S. ROUTE 20			PINS TO BE RESET
STATION	OFFSET	DESCRIPTION	
437+28.08	41.35 LT	IRON PIN FOUND	1
440+34.12	40.27 RT	IRON PIN FOUND	1
440+83.48	42.61 RT	IRON PIN FOUND	1
443+20.51	30.42 RT	IRON PIN FOUND	1
444+20.50	30.44 RT	IRON PIPE FOUND	1
455+08.21	30.06 RT	IRON PIPE FOUND	1
439+66.54	44.73 LT	IRON PIPE FOUND	1
441+34.40	44.65 LT	IRON PIPE FOUND	1
443+35.01	44.61 LT	IRON PIPE FOUND	1
447+34.37	41.64 LT	IRON PIPE FOUND	1
447+52.13	42.52 LT	IRON PIPE FOUND	1
TOTAL PINS TO BE RE-SET			11

N

HORIZONTAL SCALE IN FEET

0 50 100 200

PID NO. **109270**

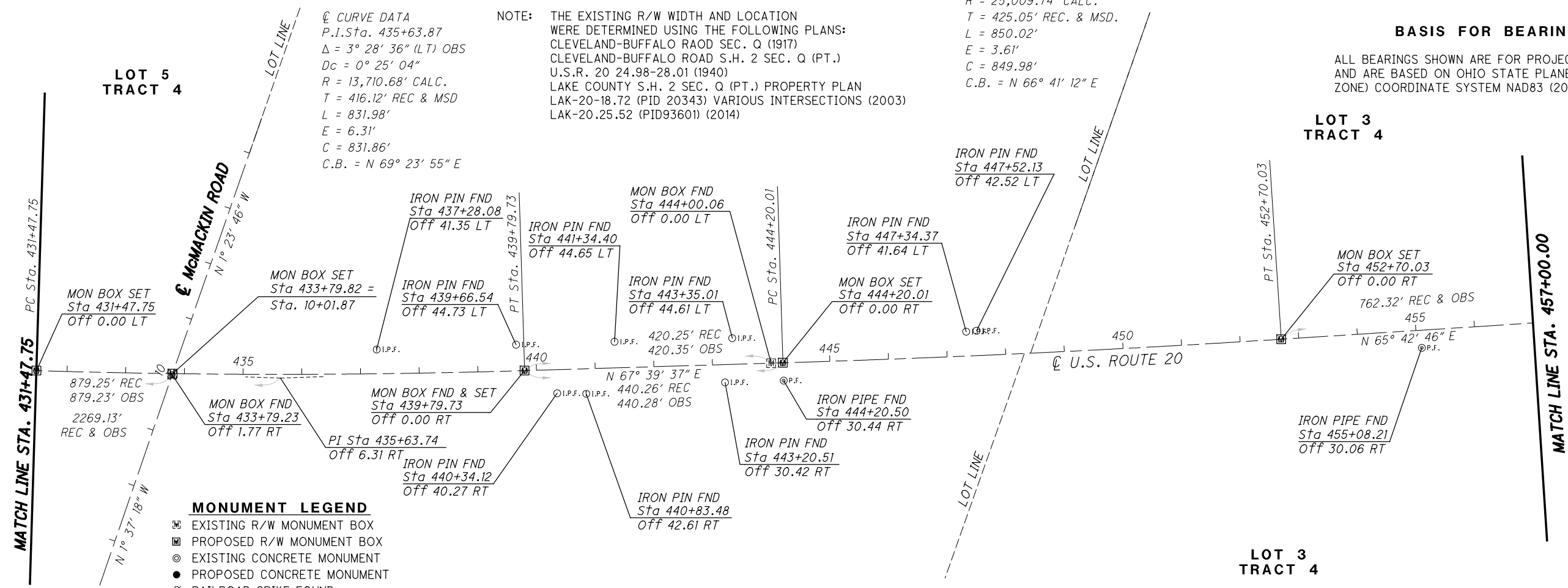
R/W DESIGNER PSJ
R/W REVIEWER TMM

CENTERLINE PLAT

LAK-US-20-24.99 PART 2

3/98

602
697



℄ CURVE DATA
P.I. Sta. 435+63.87
 $\Delta = 3^\circ 28' 36''$ (LT) OBS
 $Dc = 0^\circ 25' 04''$
 $R = 13,710.68'$ CALC.
 $T = 416.12'$ REC & MSD
 $L = 831.98'$
 $E = 6.31'$
 $C = 831.86'$
C.B. = N 69° 23' 55" E

℄ CURVE DATA
P.I. Sta. 448+45.06
 $\Delta = 1^\circ 56' 50''$ (LT) OBS $1^\circ 59'$ REC.
 $Dc = 0^\circ 13' 45''$
 $R = 25,009.74'$ CALC.
 $T = 425.05'$ REC. & MSD.
 $L = 850.02'$
 $E = 3.61'$
 $C = 849.98'$
C.B. = N 66° 41' 12" E

NOTE: THE EXISTING R/W WIDTH AND LOCATION WERE DETERMINED USING THE FOLLOWING PLANS:
CLEVELAND-BUFFALO ROAD SEC. Q (1917)
CLEVELAND-BUFFALO ROAD S.H. 2 SEC. Q (PT.) U.S.R. 20 24.98-28.01 (1940)
LAKE COUNTY S.H. 2 SEC. Q (PT.) PROPERTY PLAN LAK-20-18.72 (PID 20343) VARIOUS INTERSECTIONS (2003)
LAK-20.25.52 (PID93601) (2014)

BASIS FOR BEARINGS.
ALL BEARINGS SHOWN ARE FOR PROJECT USE ONLY AND ARE BASED ON OHIO STATE PLANE (3401-NORTH ZONE) COORDINATE SYSTEM NAD83 (2011)

- MONUMENT LEGEND**
- ☐ EXISTING R/W MONUMENT BOX
 - ▣ PROPOSED R/W MONUMENT BOX
 - ⊙ EXISTING CONCRETE MONUMENT
 - PROPOSED CONCRETE MONUMENT
 - ⌵ RAILROAD SPIKE FOUND
 - ⌵ RAILROAD SPIKE SET
 - ⊙ I.P.F. IRON PIN FOUND
 - ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
 - I.P.S. IRON PIN SET W/ ID CAP
 - ⊙ R.F. IRON PIPE FOUND
 - ⊙ R.S. IRON PIPE SET
 - ⊙ R.K.F. P.K. NAIL FOUND
 - ⊙ R.K.S. P.K. NAIL SET

SETTING OF ALL MONUMENTS SHALL BE PERFORMED BY A SURVEYOR REGISTERED IN THE STATE OF OHIO. THE MONUMENT ASSEMBLIES AND REFERENCE MONUMENTS WILL BE INSTALLED BY THE CONTRACTOR AT THE TIME OF CONSTRUCTION. THE IRON PIN AND CAP (WHEN REQUIRED) ARE TO BE INSTALLED BY THE CONTRACTOR'S SURVEYOR.

CHANGES OR ALTERATIONS TO THE LOCATION OF ANY MONUMENTS SHOWN IN THIS TABLE, REQUIRE PRIOR APPROVAL FROM THE DISTRICT REAL ESTATE ADMINISTRATOR OF THE OHIO DEPARTMENT OF TRANSPORTATION. IN THE EVENT THAT CHANGES OR ALTERATIONS ARE APPROVED, A REVISED CENTERLINE PLAT WITH THE NEW LOCATIONS SHALL BE RECORDED IN THE APPLICABLE COUNTY RECORDS AND THE OHIO DEPARTMENT OF TRANSPORTATION. SPECIFICATIONS FOR MONUMENT ASSEMBLIES, REFERENCE MONUMENTS AND RIGHT OF WAY MONUMENTS ARE SHOWN ON STANDARD CONSTRUCTION DRAWING RM-1.1.

I, Thomas M Meeks, P. S. have conducted a survey of the existing conditions for the Ohio Department of Transportation between March 2019 and August 2019. The results of that survey are contained herein.

The horizontal coordinates expressed herein are based on the Ohio State Plane Coordinates System North Zone on NAD 83 2011 datum. The Project Coordinates (US Survey Feet) are relative to State Plane Grid Coordinates (Meters or US Survey Feet) by a Project Adjustment Factor of 1.00000885.

As a part of this project I have reestablished the locations of the existing property lines and the existing centerline of Right of Way for property takes contained herein. As a part of this project I have established the proposed property lines, calculated the Gross Take, present roadway occupied (PRO), Net Take and Net Residue; as well as prepared the legal descriptions necessary to acquire the parcels as shown herein.

As a part of this work I have set right of way monuments at the property corners, property line intersection, points along the right of way and/or angle points on the right of way, Section Corners and other points as shown herein.

All of my work contained herein was conducted in accordance with Ohio Administrative Code 4733-37 commonly known as "Minimum Standards for Boundary Surveys in the State of Ohio" unless noted.

The words I and my as used herein are to mean either myself or someone working under my direct supervision.

Thomas M. Meeks, Professional Land Surveyor 8674

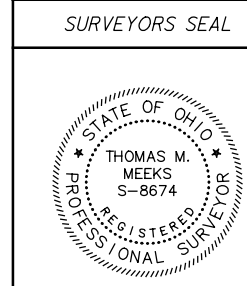
Date:

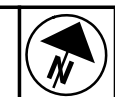
RECEIVED _____, 20____

RECORDED _____, 20____

BOOK _____ PAGE _____

COUNTY RECORDER





PID NO. 109270

R/W DESIGNER PSL TMM
R/W REVIEWER TMM

CENTERLINE PLAT

LAK-US-20-24.99 PART 2

4/98

603
697

LAK-20-24.99

PART OF LOTS 2, 3 & 12 TRACT 4 MADISON TOWNSHIP

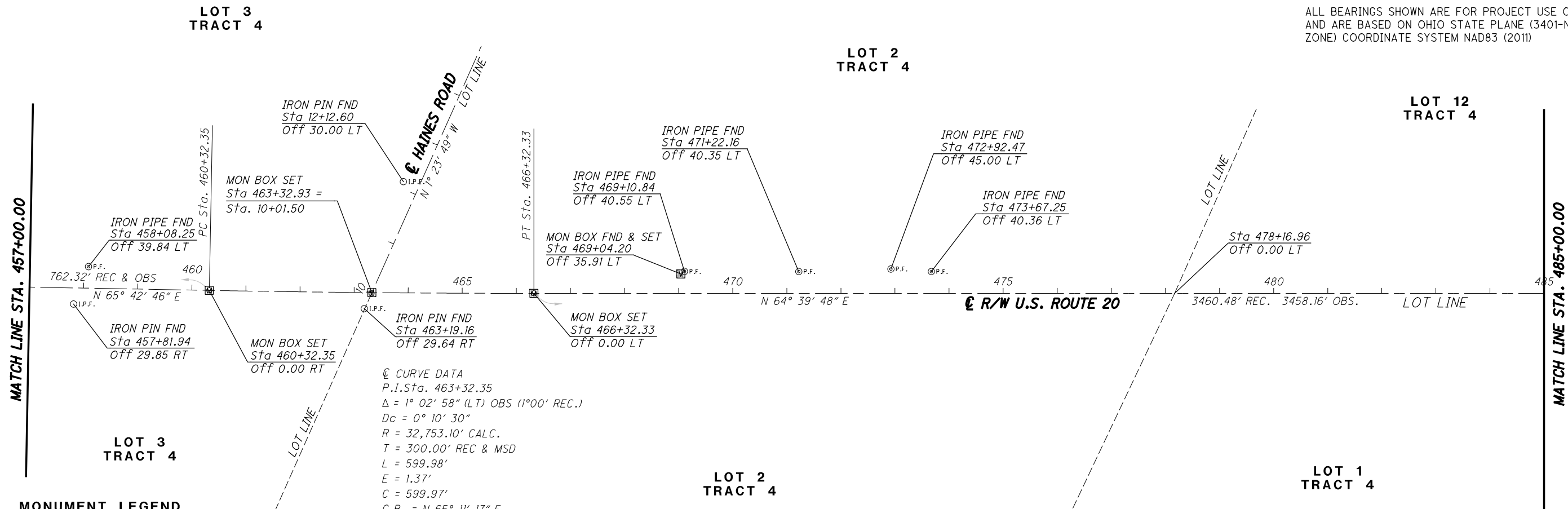
MONUMENT TABLE							
☒ of U.S. ROUTE 20		PROJECT COORDINATES SEE SURVEY CERTIFICATION		MONUMENTS TO BE SET DURING CONSTRUCTION		R/W MON. EXPECTED TO BE DISTURBED	
STATION	OFFSET	NORTH (Y)	EAST (X)	MON. ASSY.	REF. MON.	R/W MON.	DESCRIPTION
460+32.35	☒	779313.997	2354230.335	1			MONUMENT BOX ASSEMBLY
463+32.93	☒	779438.886	2354503.747	1			MONUMENT BOX ASSEMBLY
466+32.33	☒	779565.770	2354774.927	1			MONUMENT BOX ASSEMBLY
469+04.20	35.91' LT	779714.570	2355005.274	1			MONUMENT FOUND & SET
TOTAL CARRIED TO GENERAL SUMMARY SHEET				4			

MONUMENT TABLE			
☒ of U.S. ROUTE 20			PINS TO BE RESET
STATION	OFFSET	DESCRIPTION	
457+81.94	29.85 RT	IRON PIN FOUND	1
458+08.25	39.84 LT	IRON PIPE FOUND	1
463+19.16	29.64 RT	IRON PIN FOUND	1
463+89.65	205.59 LT	IRON PIN FOUND	1
469+10.84	40.55 LT	IRON PIPE FOUND	1
471+22.16	40.35 LT	IRON PIPE FOUND	1
472+92.47	45.00 LT	IRON PIPE FOUND	1
473+67.25	40.36 LT	IRON PIPE FOUND	1
TOTAL PINS TO BE RE-SET			8

NOTE: THE EXISTING R/W WIDTH AND LOCATION WERE DETERMINED USING THE FOLLOWING PLANS:
 CLEVELAND-BUFFALO ROAD SEC. Q (1917)
 CLEVELAND-BUFFALO ROAD S.H. 2 SEC. Q (PT.) U.S.R. 20 24.98-28.01 (1940)
 LAKE COUNTY S.H. 2 SEC. Q (PT.) PROPERTY PLAN LAK-20-18.72 (PID 20343) VARIOUS INTERSECTIONS (2003)
 LAK-20.25.52 (PID93601) (2014)

BASIS FOR BEARINGS:

ALL BEARINGS SHOWN ARE FOR PROJECT USE ONLY AND ARE BASED ON OHIO STATE PLANE (3401-NORTH ZONE) COORDINATE SYSTEM NAD83 (2011)



I, Thomas M Meeks, P. S. have conducted a survey of the existing conditions for the Ohio Department of Transportation between March 2019 and August 2019. The results of that survey are contained herein.

The horizontal coordinates expressed herein are based on the Ohio State Plane Coordinates System North Zone on NAD 83 2011 datum. The Project Coordinates (US Survey Feet) are relative to State Plane Grid Coordinates (Meters or US Survey Feet) by a Project Adjustment Factor of 1.0000885.

As a part of this project I have reestablished the locations of the existing property lines and the existing centerline of Right of Way for property takes contained herein. As a part of this project I have established the proposed property lines, calculated the Gross Take, present roadway occupied (PRO), Net Take and Net Residue; as well as prepared the legal descriptions necessary to acquire the parcels as shown herein.

As a part of this work I have set right of way monuments at the property corners, property line intersection, points along the right of way and/or angle points on the right of way, Section Corners and other points as shown herein.

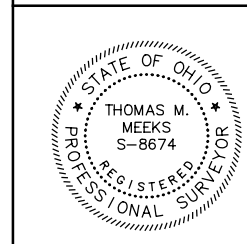
All of my work contained herein was conducted in accordance with Ohio Administrative Code 4733-37 commonly known as "Minimum Standards for Boundary Surveys in the State of Ohio" unless noted.

The words I and my as used herein are to mean either myself or someone working under my direct supervision.

Thomas M. Meeks, Professional Land Surveyor 8674

Date:

SURVEYORS SEAL



H:\2018\180743\0DOT\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RCO03.dgn Sheet 7/15/2022 3:01:05 PM troyer

RECEIVED _____, 20____
 RECORDED _____, 20____
 BOOK _____ PAGE _____
 COUNTY RECORDER

MONUMENT TABLE							
C of U.S. ROUTE 20		PROJECT COORDINATES SEE SURVEY CERTIFICATION		MONUMENTS TO BE SET DURING CONSTRUCTION		R/W MON. EXPECTED TO BE DISTURBED	
STATION	OFFSET	NORTH (Y)	EAST (X)	MON. ASSY.	REF. MON.	R/W MON.	DESCRIPTION
494+73.13	C	780781.454	2357342.472	1			MONUMENT BOX FND & SET
500+90.49	C	781045.644	2357900.446	1			MONUMENT BOX ASSEMBLY
TOTAL CARRIED TO GENERAL SUMMARY SHEET				2			

LAK-20-24.99

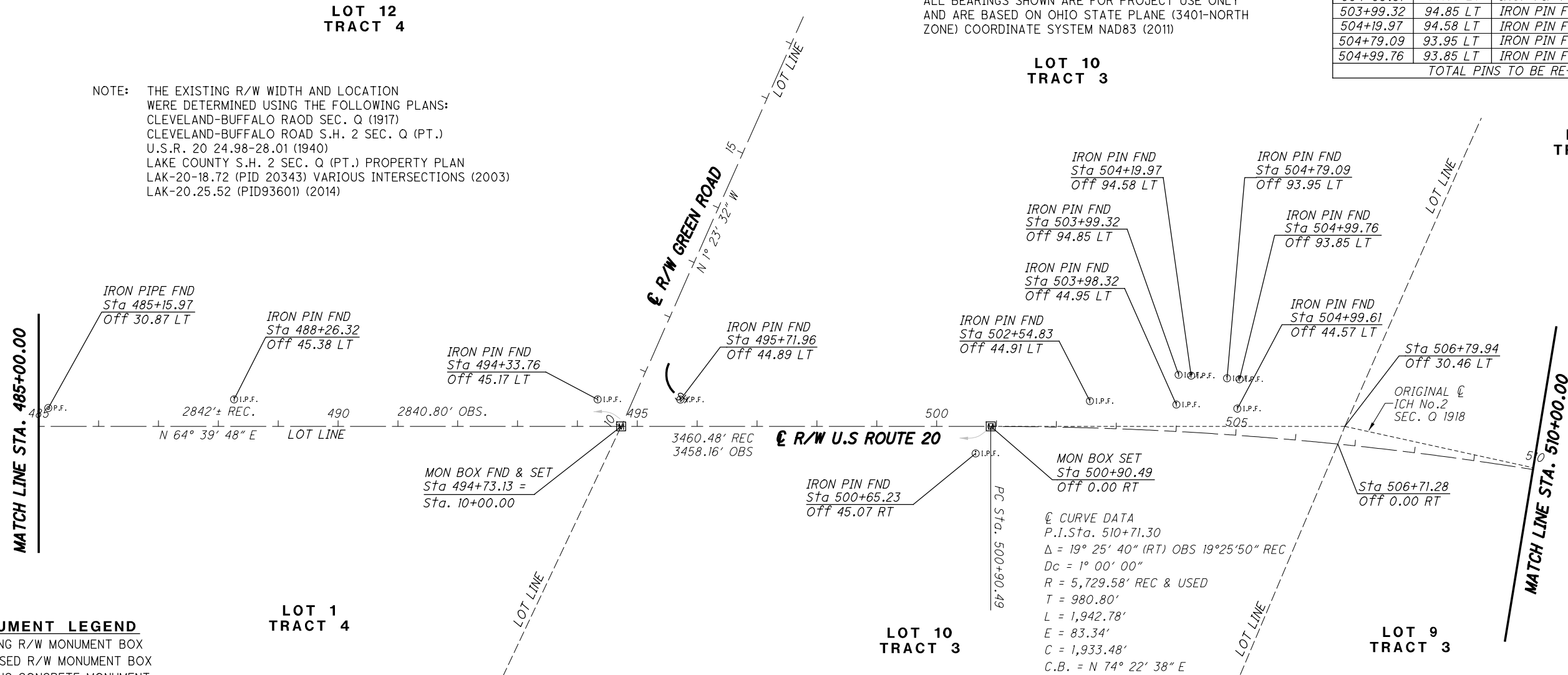
PART OF LOTS 1 & 12
TRACT 4
PART OF LOTS 10 & 9
TRACT 3

MONUMENT TABLE			
C of U.S. ROUTE 20			PINS TO BE RESET
STATION	OFFSET	DESCRIPTION	
485+15.97	30.87 LT	IRON PIPE FOUND	1
488+26.32	45.38 LT	IRON PIN FOUND	1
494+33.76	45.17 LT	IRON PIN FOUND	1
495+71.96	44.89 LT	IRON PIN FOUND	1
500+65.23	45.07 RT	IRON PIN FOUND	1
502+54.83	44.91 LT	IRON PIN FOUND	1
503+98.32	44.95 LT	IRON PIN FOUND	1
504+99.61	44.57 LT	IRON PIN FOUND	1
503+99.32	94.85 LT	IRON PIN FOUND	1
504+19.97	94.58 LT	IRON PIN FOUND	1
504+79.09	93.95 LT	IRON PIN FOUND	1
504+99.76	93.85 LT	IRON PIN FOUND	1
TOTAL PINS TO BE RE-SET			12

BASIS FOR BEARINGS:

ALL BEARINGS SHOWN ARE FOR PROJECT USE ONLY AND ARE BASED ON OHIO STATE PLANE (3401-NORTH ZONE) COORDINATE SYSTEM NAD83 (2011)

NOTE: THE EXISTING R/W WIDTH AND LOCATION WERE DETERMINED USING THE FOLLOWING PLANS:
CLEVELAND-BUFFALO ROAD SEC. Q (1917)
CLEVELAND-BUFFALO ROAD S.H. 2 SEC. Q (PT.) U.S.R. 20 24.98-28.01 (1940)
LAKE COUNTY S.H. 2 SEC. Q (PT.) PROPERTY PLAN LAK-20-18.72 (PID 20343) VARIOUS INTERSECTIONS (2003)
LAK-20.25.52 (PID93601) (2014)



MONUMENT LEGEND

- ☐ EXISTING R/W MONUMENT BOX
- ▣ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ✕ RAILROAD SPIKE FOUND
- ✎ RAILROAD SPIKE SET
- I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- R.K.F. P.K. NAIL FOUND
- R.K.S. P.K. NAIL SET

SETTING OF ALL MONUMENTS SHALL BE PERFORMED BY A SURVEYOR REGISTERED IN THE STATE OF OHIO. THE MONUMENT ASSEMBLIES AND REFERENCE MONUMENTS WILL BE INSTALLED BY THE CONTRACTOR AT THE TIME OF CONSTRUCTION. THE IRON PIN AND CAP (WHEN REQUIRED) ARE TO BE INSTALLED BY THE CONTRACTOR'S SURVEYOR.

CHANGES OR ALTERATIONS TO THE LOCATION OF ANY MONUMENTS SHOWN IN THIS TABLE, REQUIRE PRIOR APPROVAL FROM THE DISTRICT REAL ESTATE ADMINISTRATOR OF THE OHIO DEPARTMENT OF TRANSPORTATION. IN THE EVENT THAT CHANGES OR ALTERATIONS ARE APPROVED, A REVISED CENTERLINE PLAT WITH THE NEW LOCATIONS SHALL BE RECORDED IN THE APPLICABLE COUNTY RECORDS AND THE OHIO DEPARTMENT OF TRANSPORTATION. SPECIFICATIONS FOR MONUMENT ASSEMBLIES, REFERENCE MONUMENTS AND RIGHT OF WAY MONUMENTS ARE SHOWN ON STANDARD CONSTRUCTION DRAWING RM-1.1.

I, Thomas M Meeks, P. S. have conducted a survey of the existing conditions for the Ohio Department of Transportation between March 2019 and August 2019. The results of that survey are contained herein.

The horizontal coordinates expressed herein are based on the Ohio State Plane Coordinates System North Zone on NAD 83 2011 datum. The Project Coordinates (US Survey Feet) are relative to State Plane Grid Coordinates (Meters or US Survey Feet) by a Project Adjustment Factor of 1.0000885.

As a part of this project I have reestablished the locations of the existing property lines and the existing centerline of Right of Way for property takes contained herein. As a part of this project I have established the proposed property lines, calculated the Gross Take, present roadway occupied (PRO), Net Take and Net Residue; as well as prepared the legal descriptions necessary to acquire the parcels as shown herein.

As a part of this work I have set right of way monuments at the property corners, property line intersection, points along the right of way and/or angle points on the right of way, Section Corners and other points as shown herein.

All of my work contained herein was conducted in accordance with Ohio Administrative Code 4733-37 commonly known as "Minimum Standards for Boundary Surveys in the State of Ohio" unless noted.

The words I and my as used herein are to mean either myself or someone working under my direct supervision.

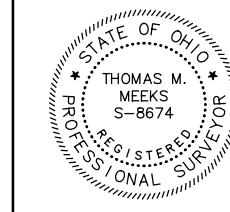
Thomas M. Meeks, Professional Land Surveyor 8674

Date:

H:\2018\180743\0DOT\ProjectData\09270\LAK_20_EAST\Design\RW\Sheets\09270_RC004.dgn Sheet 7/15/2022 3:01:06 PM Troyer

RECEIVED _____, 20____
RECORDED _____, 20____
BOOK _____ PAGE _____
COUNTY RECORDER

SURVEYORS SEAL





PID NO. **109270**

R/W DESIGNER PSL
R/W REVIEWER TMM

CENTERLINE PLAT

LAK-US-20-24.99 PART 2

5 / 98

604
697

LAK-20-24.99

PART OF LOTS 9, 7 & 6
TRACT 3
MADISON TOWNSHIP

MONUMENT TABLE			
℄ of U.S. ROUTE 20			PINS TO BE RESET
STATION	OFFSET	DESCRIPTION	
511+08.02	45.11 LT	IRON PIN FOUND	1
516+38.03	47.29 LT	IRON PIN FOUND	1
520+47.31	45.30 RT	IRON PIN FOUND	1
522+03.16	44.95 LT	IRON PIN FOUND	1
524+11.08	45.29 RT	IRON PIN FOUND	1
527+21.95	44.68 LT	IRON PIPE FOUND	1
528+72.13	44.67 LT	IRON PIN FOUND	1
530+71.27	49.61 LT	IRON PIN FOUND	1
535+96.32	50.26 RT	IRON PIN FOUND	1
TOTAL PINS TO BE RE-SET			9

MONUMENT TABLE							
℄ of U.S. ROUTE 20		PROJECT COORDINATES SEE SURVEY CERTIFICATION		MONUMENTS TO BE SET DURING CONSTRUCTION		R/W MON. EXPECTED TO BE DISTURBED	
STATION	OFFSET	NORTH (Y)	EAST (X)	MON. ASSY.	REF. MON.	R/W MON.	DESCRIPTION
517+13.47	℄	781524.556	2359445.485	1			MONUMENT BOX ASSEMBLY
517+31.34	7.96' LT	781535.225	2359461.899	1			MONUMENT BOX FOUND & SET
520+33.27	℄	781566.336	2359762.500	1			MONUMENT BOX ASSEMBLY
529+59.97	℄	781661.737	2360684.280	1			MONUMENT BOX ASSEMBLY
TOTAL CARRIED TO GENERAL SUMMARY SHEET				4			

SETTING OF ALL MONUMENTS SHALL BE PERFORMED BY A SURVEYOR REGISTERED IN THE STATE OF OHIO. THE MONUMENT ASSEMBLIES AND REFERENCE MONUMENTS WILL BE INSTALLED BY THE CONTRACTOR AT THE TIME OF CONSTRUCTION. THE IRON PIN AND CAP (WHEN REQUIRED) ARE TO BE INSTALLED BY THE CONTRACTOR'S SURVEYOR.

CHANGES OR ALTERATIONS TO THE LOCATION OF ANY MONUMENTS SHOWN IN THIS TABLE, REQUIRE PRIOR APPROVAL FROM THE DISTRICT REAL ESTATE ADMINISTRATOR OF THE OHIO DEPARTMENT OF TRANSPORTATION. IN THE EVENT THAT CHANGES OR ALTERATIONS ARE APPROVED, A REVISED CENTERLINE PLAT WITH THE NEW LOCATIONS SHALL BE RECORDED IN THE APPLICABLE COUNTY RECORDS AND THE OHIO DEPARTMENT OF TRANSPORTATION. SPECIFICATIONS FOR MONUMENT ASSEMBLIES, REFERENCE MONUMENTS AND RIGHT OF WAY MONUMENTS ARE SHOWN ON STANDARD CONSTRUCTION DRAWING RM-1.1.

NOTE: THE EXISTING R/W WIDTH AND LOCATION WERE DETERMINED USING THE FOLLOWING PLANS:
CLEVELAND-BUFFALO ROAD SEC. Q (1917)
CLEVELAND-BUFFALO ROAD S.H. 2 SEC. Q (PT.) U.S.R. 20 24.98-28.01 (1940)
LAKE COUNTY S.H. 2 SEC. Q (PT.) PROPERTY PLAN LAK-20-18.72 (PID 20343) VARIOUS INTERSECTIONS (2003)
LAK-20.25.52 (PID93601) (2014)

BASIS FOR BEARINGS:

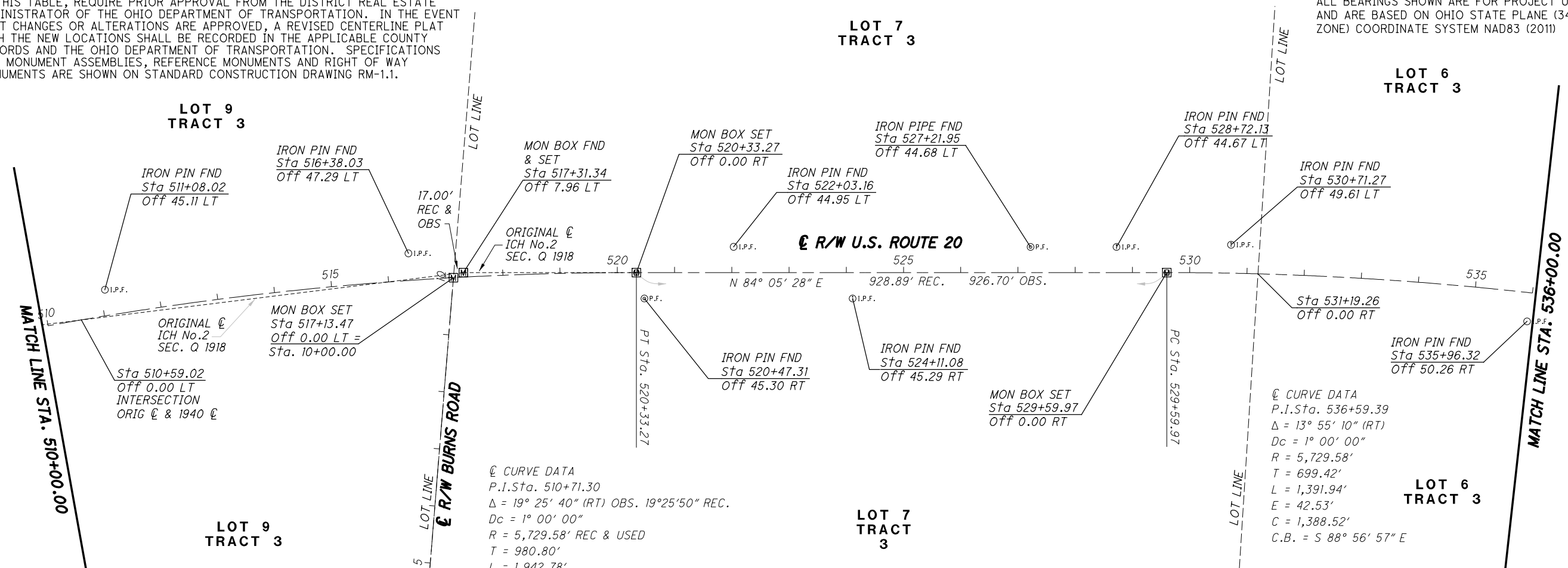
ALL BEARINGS SHOWN ARE FOR PROJECT USE ONLY AND ARE BASED ON OHIO STATE PLANE (3401-NORTH ZONE) COORDINATE SYSTEM NAD83 (2011)

N

10
HORIZONTAL
SCALE IN FEET

PID NO. **109270**

R/W DESIGNER PSL
R/W REVIEWER TMM



**LOT 9
TRACT 3**

**LOT 7
TRACT 3**

**LOT 6
TRACT 3**

℄ R/W U.S. ROUTE 20

**LOT 9
TRACT 3**

**LOT 7
TRACT 3**

**LOT 6
TRACT 3**

MONUMENT LEGEND

- ☐ EXISTING R/W MONUMENT BOX
- ▣ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- ⊙ I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.F. IRON PIPE SET
- ⊙ P.K.F. P.K. NAIL FOUND
- P.K.S. P.K. NAIL SET

℄ CURVE DATA
P.I. Sta. 510+71.30
Δ = 19° 25' 40" (RT) OBS. 19°25'50" REC.
Dc = 1° 00' 00"
R = 5,729.58' REC & USED
T = 980.80'
L = 1,942.78'
E = 83.34'
C = 1,933.48'
C.B. = N 74° 22' 38" E

I, Thomas M. Meeks, P. S. have conducted a survey of the existing conditions for the Ohio Department of Transportation between March 2019 and August 2019. The results of that survey are contained herein.

The horizontal coordinates expressed herein are based on the Ohio State Plane Coordinates System North Zone on NAD 83 2011 datum. The Project Coordinates (US Survey Feet) are relative to State Plane Grid Coordinates (Meters or US Survey Feet) by a Project Adjustment Factor of 1.00000885.

As a part of this project I have reestablished the locations of the existing property lines and the existing centerline of Right of Way for property takes contained herein. As a part of this project I have established the proposed property lines, calculated the Gross Take, present roadway occupied (PRO), Net Take and Net Residue; as well as prepared the legal descriptions necessary to acquire the parcels as shown herein.

As a part of this work I have set right of way monuments at the property corners, property line intersection, points along the right of way and/or angle points on the right of way, Section Corners and other points as shown herein.

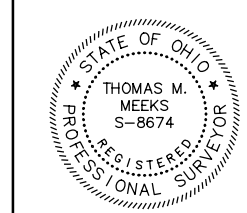
All of my work contained herein was conducted in accordance with Ohio Administrative Code 4733-37 commonly known as "Minimum Standards for Boundary Surveys in the State of Ohio" unless noted.

The words I and my as used herein are to mean either myself or someone working under my direct supervision.

Thomas M. Meeks, Professional Land Surveyor 8674

Date:

SURVEYORS SEAL



H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RC005.dgn Sheet 7/15/2022 3:01:07 PM troyer

RECEIVED _____, 20____

RECORDED _____, 20____

BOOK _____ PAGE _____

COUNTY RECORDER

CENTERLINE PLAT

LAK-US-20-24.99
PART 2

6/98

605
697

LAK-20-24.99

PART OF LOT 1, TRACT 3 PART OF COWLES LOT, TRACT 2 MADISON TOWNSHIP

MONUMENT TABLE							
℄ of U.S. ROUTE 20		PROJECT COORDINATES SEE SURVEY CERTIFICATION		MONUMENTS TO BE SET DURING CONSTRUCTION		R/W MON. EXPECTED TO BE DISTURBED	
STATION	OFFSET	NORTH (Y)	EAST (X)	MON. ASSY.	REF. MON.	R/W MON.	DESCRIPTION
564+78.48	36.70 RT	781303.586	2364173.266			1	NGS MONUMENT FND & RESET
565+73.90	℄	781327.038	2364269.949	1			MONUMENT BOX FND & SET
TOTAL CARRIED TO GENERAL SUMMARY SHEET				1		1	

MONUMENT TABLE			
℄ of U.S. ROUTE 20			PINS TO BE RESET
STATION	OFFSET	DESCRIPTION	
560+93.66	45.06 RT	IRON PIN FOUND	1
561+29.30	40.12 LT	IRON PIN FOUND	1
TOTAL PINS TO BE RE-SET			2

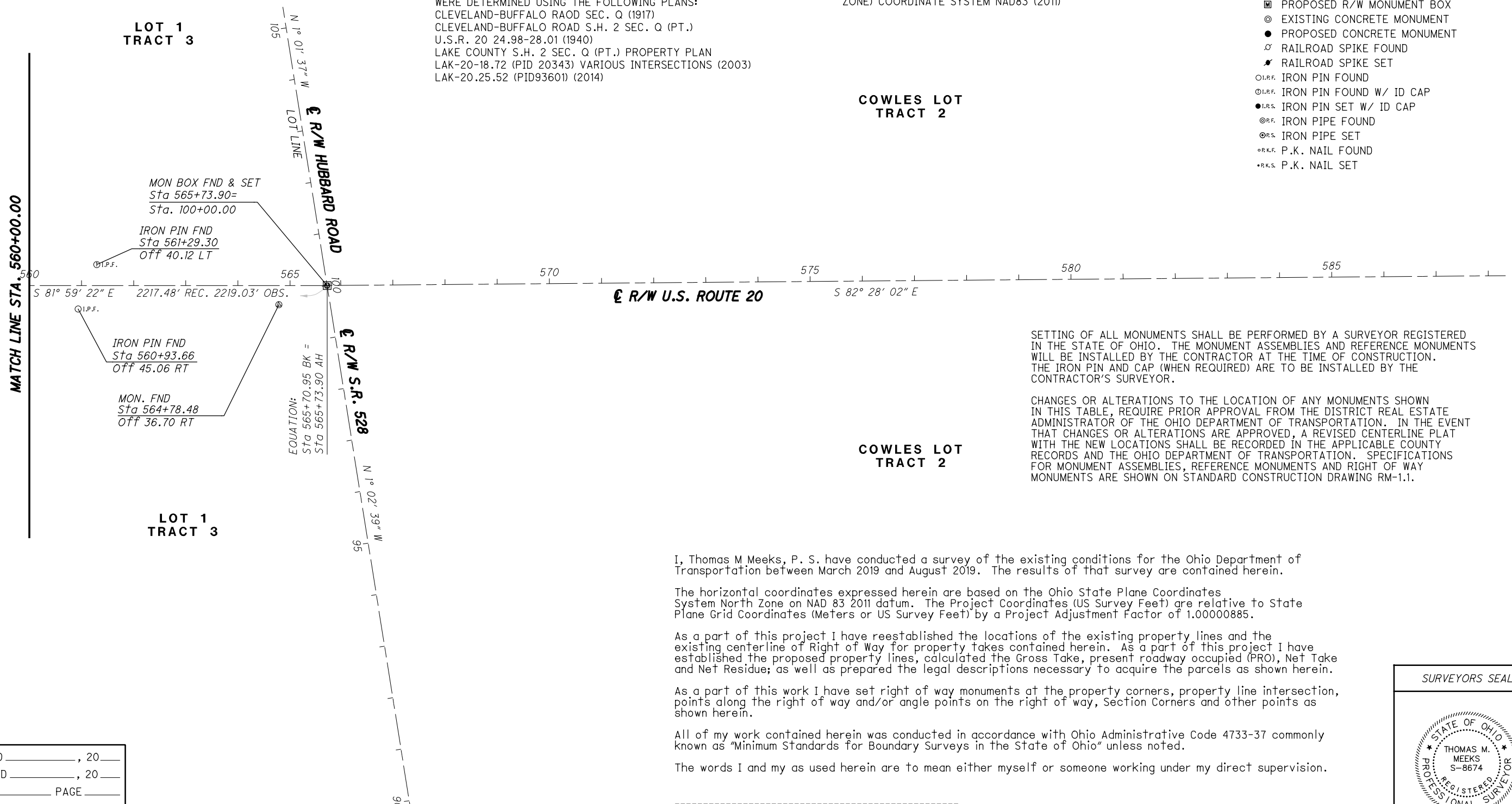
BASIS FOR BEARINGS:

ALL BEARINGS SHOWN ARE FOR PROJECT USE ONLY AND ARE BASED ON OHIO STATE PLANE (3401-NORTH ZONE) COORDINATE SYSTEM NAD83 (2011)

NOTE: THE EXISTING R/W WIDTH AND LOCATION WERE DETERMINED USING THE FOLLOWING PLANS:
CLEVELAND-BUFFALO ROAD SEC. Q (1917)
CLEVELAND-BUFFALO ROAD S.H. 2 SEC. Q (PT.) U.S.R. 20 24.98-28.01 (1940)
LAKE COUNTY S.H. 2 SEC. Q (PT.) PROPERTY PLAN LAK-20-18.72 (PID 20343) VARIOUS INTERSECTIONS (2003)
LAK-20.25.52 (PID93601) (2014)

MONUMENT LEGEND

- ☒ EXISTING R/W MONUMENT BOX
- ☑ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- ⊙ R.K.F. P.K. NAIL FOUND
- R.K.S. P.K. NAIL SET



SETTING OF ALL MONUMENTS SHALL BE PERFORMED BY A SURVEYOR REGISTERED IN THE STATE OF OHIO. THE MONUMENT ASSEMBLIES AND REFERENCE MONUMENTS WILL BE INSTALLED BY THE CONTRACTOR AT THE TIME OF CONSTRUCTION. THE IRON PIN AND CAP (WHEN REQUIRED) ARE TO BE INSTALLED BY THE CONTRACTOR'S SURVEYOR.

CHANGES OR ALTERATIONS TO THE LOCATION OF ANY MONUMENTS SHOWN IN THIS TABLE, REQUIRE PRIOR APPROVAL FROM THE DISTRICT REAL ESTATE ADMINISTRATOR OF THE OHIO DEPARTMENT OF TRANSPORTATION. IN THE EVENT THAT CHANGES OR ALTERATIONS ARE APPROVED, A REVISED CENTERLINE PLAT WITH THE NEW LOCATIONS SHALL BE RECORDED IN THE APPLICABLE COUNTY RECORDS AND THE OHIO DEPARTMENT OF TRANSPORTATION. SPECIFICATIONS FOR MONUMENT ASSEMBLIES, REFERENCE MONUMENTS AND RIGHT OF WAY MONUMENTS ARE SHOWN ON STANDARD CONSTRUCTION DRAWING RM-1.1.

I, Thomas M Meeks, P. S. have conducted a survey of the existing conditions for the Ohio Department of Transportation between March 2019 and August 2019. The results of that survey are contained herein.

The horizontal coordinates expressed herein are based on the Ohio State Plane Coordinates System North Zone on NAD 83 2011 datum. The Project Coordinates (US Survey Feet) are relative to State Plane Grid Coordinates (Meters or US Survey Feet) by a Project Adjustment Factor of 1.00000885.

As a part of this project I have reestablished the locations of the existing property lines and the existing centerline of Right of Way for property takes contained herein. As a part of this project I have established the proposed property lines, calculated the Gross Take, present roadway occupied (PRO), Net Take and Net Residue; as well as prepared the legal descriptions necessary to acquire the parcels as shown herein.

As a part of this work I have set right of way monuments at the property corners, property line intersection, points along the right of way and/or angle points on the right of way, Section Corners and other points as shown herein.

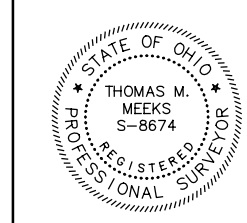
All of my work contained herein was conducted in accordance with Ohio Administrative Code 4733-37 commonly known as "Minimum Standards for Boundary Surveys in the State of Ohio" unless noted.

The words I and my as used herein are to mean either myself or someone working under my direct supervision.

Thomas M. Meeks, Professional Land Surveyor 8674

Date:

SURVEYORS SEAL



H:\2018\180743\0D01\Project\Data\09270_LAK_20_EAST\Design\RW\Sheets\09270_RCO07.dgn Sheet 7/15/2022 3:01:09 PM Troyer

RECEIVED _____, 20____
RECORDED _____, 20____
BOOK _____ PAGE _____
COUNTY RECORDER



PID NO. **190720**

R/W DESIGNER PSL
R/W REVIEWER TMM

CENTERLINE PLAT

LAK-US-20-24.99 PART 2

8 / 98

607
697

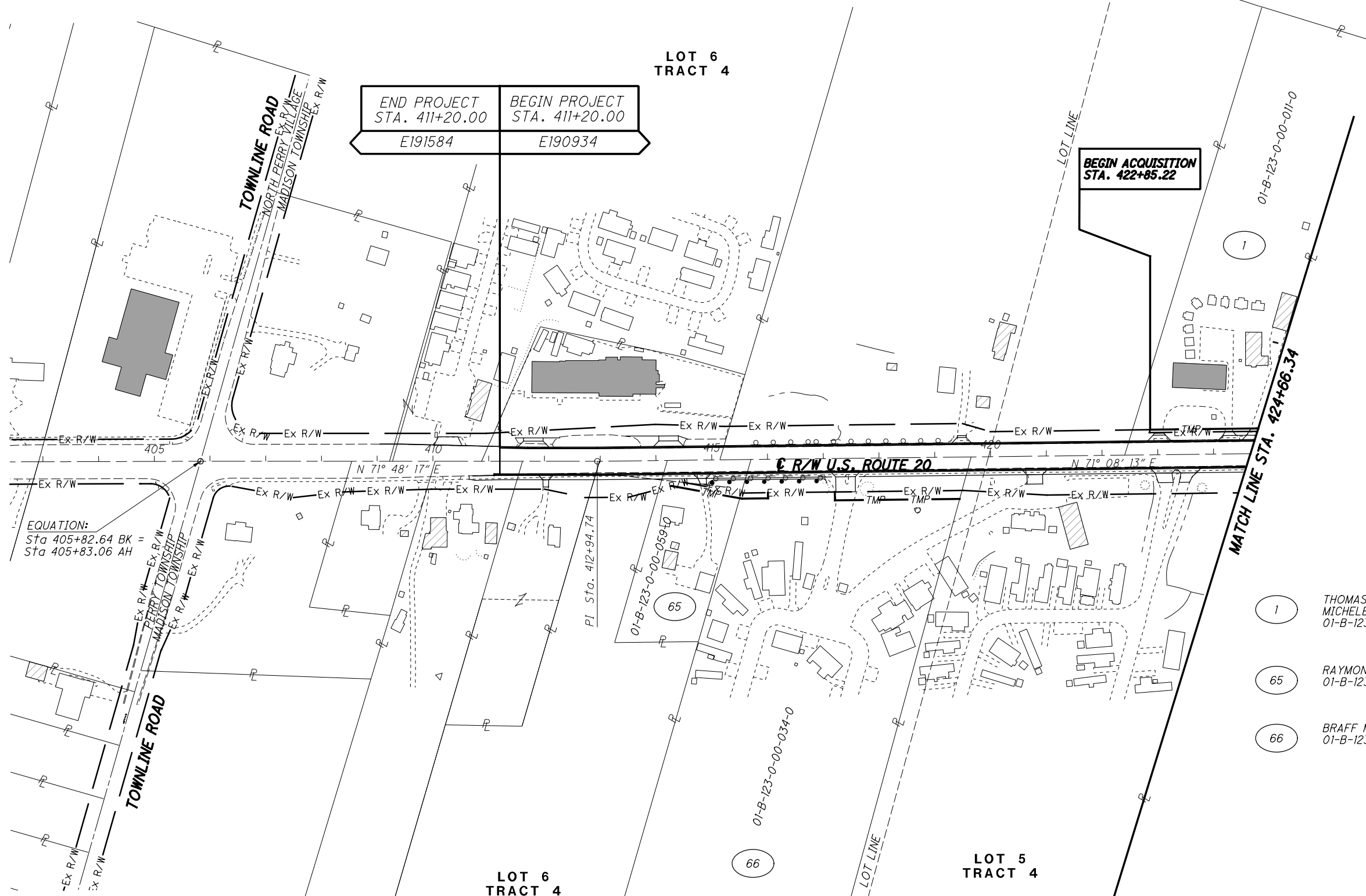
PART OF LOT 6 & 5
TRACT 4
MADISON TOWNSHIP
LAKE COUNTY

LOT 5
TRACT 4

LOT 6
TRACT 4

END PROJECT STA. 411+20.00 E191584	BEGIN PROJECT STA. 411+20.00 E190934
------------------------------------------	--------------------------------------------

BEGIN ACQUISITION
STA. 422+85.22



EQUATION:
Sta 405+82.64 BK =
Sta 405+83.06 AH

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

- 1 THOMAS J HOWELL & MICHELE L HOWELL
01-B-123-0-00-011-0
- 65 RAYMOND B. PHILLIPP
01-B-123-0-00-059-0
- 66 BRAFF MOBILE HOME PARK CO
01-B-123-0-00-034-0

CNK	7/14/22	ADDED PARCELS 65 AND 66

REV. BY	DATE	DESCRIPTION

DATE COMPLETED



PID NO.
109270

R/W DESIGNER PSL
R/W REVIEWER TMM

PROPERTY MAP

LAK-US-20-24.99
PART 2

9/98

608
697

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RM002.dgn Sheet 7/15/2022 3:01:11PM troyer

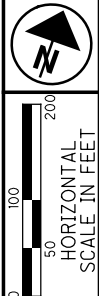
2 DONALD J MAH, TRUSTEE OF THE DONALD J MAH REVOCABLE LIVING TRUST, JACQUELINE M WRIGHT THERESE MACKEY & VICKI GURLEY 01-B-122-0-00-018-0

3 MUSIC EXPRESS BROADCASTING 01-B-122-0-00-061-0

4 RODNEY WILEY 01-B-122-0-00-026-0

5 FIVE GUYS DEVELOPMENT LLC 01-B-122-0-00-019-0

PART OF LOT 5 & 4 TRACT 4 MADISON TOWNSHIP LAKE COUNTY



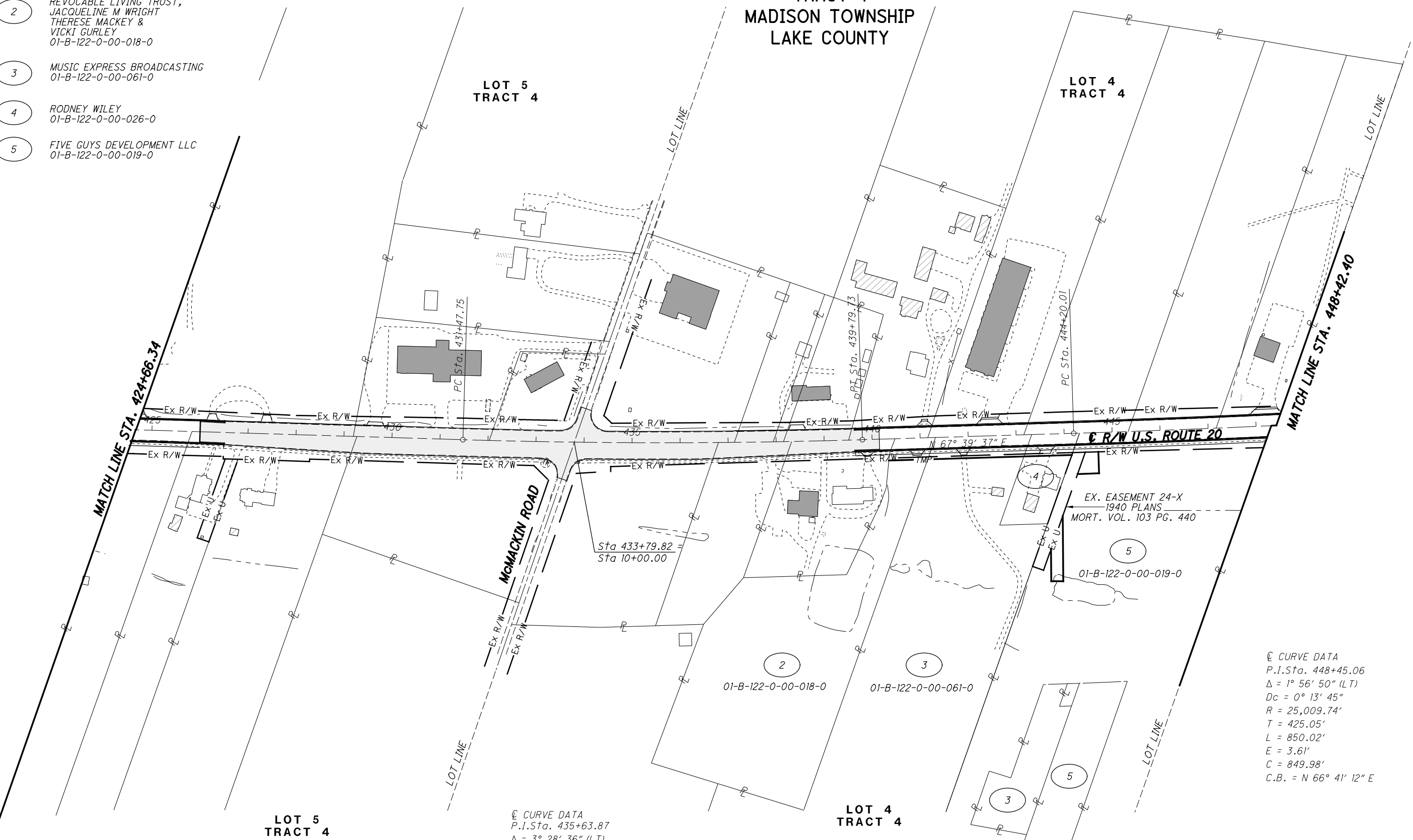
PID NO. 109270
R/W DESIGNER PSL
R/W REVIEWER TMM

PROPERTY MAP

LAK-US-20-24.99 PART 2

10/98

609
697



STRUCTURE KEY

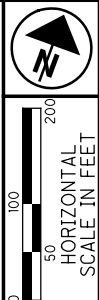
- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

⊙ CURVE DATA
 P.I. Sta. 435+63.87
 $\Delta = 3^\circ 28' 36''$ (LT)
 $D_c = 0^\circ 25' 04''$
 $R = 13,710.68'$
 $T = 416.12'$
 $L = 831.98'$
 $E = 6.31'$
 $C = 831.86'$
 $C.B. = N 69^\circ 23' 55'' E$

⊙ CURVE DATA
 P.I. Sta. 448+45.06
 $\Delta = 1^\circ 56' 50''$ (LT)
 $D_c = 0^\circ 13' 45''$
 $R = 25,009.74'$
 $T = 425.05'$
 $L = 850.02'$
 $E = 3.61'$
 $C = 849.98'$
 $C.B. = N 66^\circ 41' 12'' E$

REV. BY	DATE	DESCRIPTION

PART OF LOT NO 2 & 3
TRACT 4
MADISON TOWNSHIP
LAKE COUNTY



PID NO.
109270

R/W DESIGNER
PSL

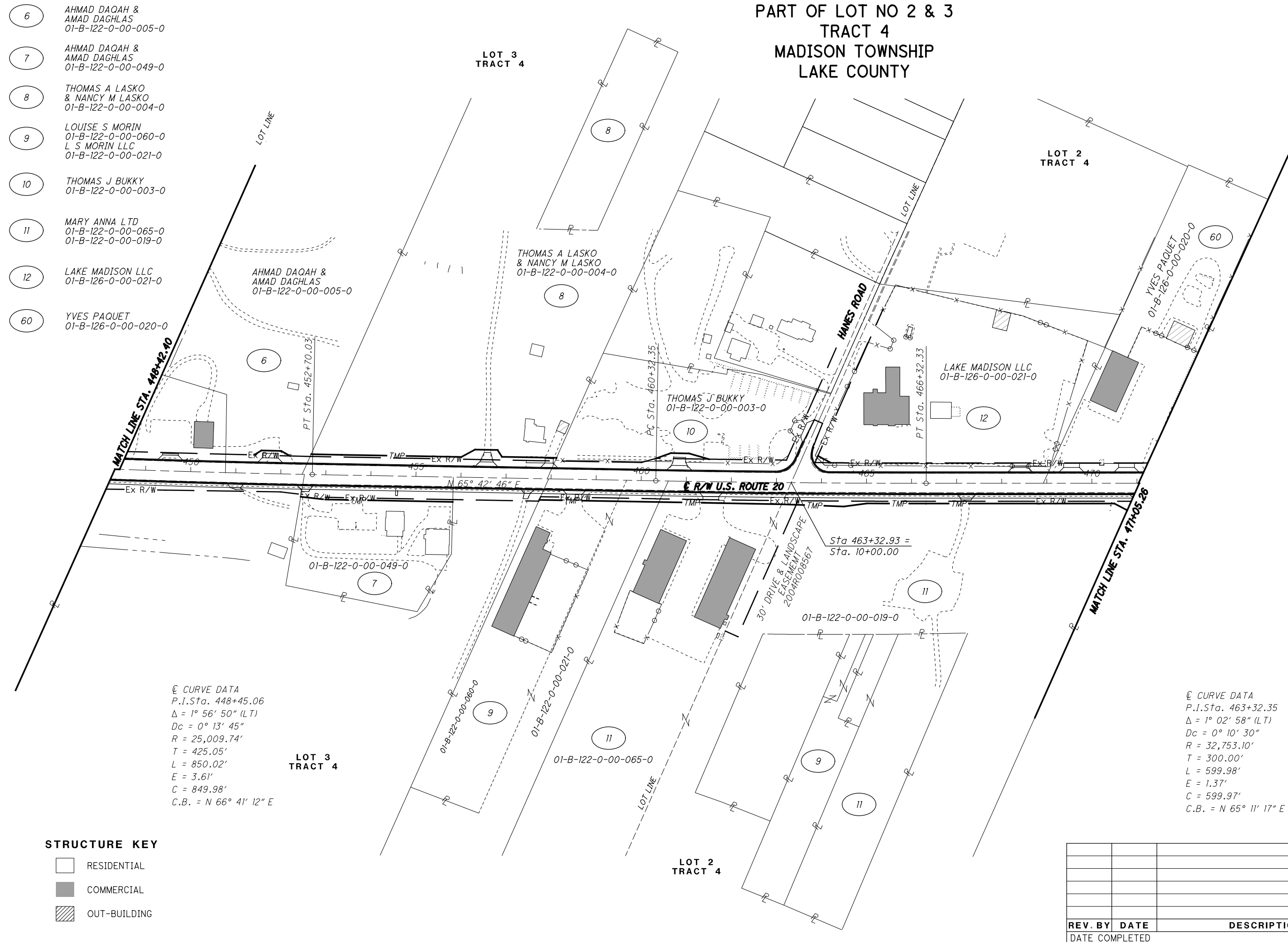
R/W REVIEWER
TMM

PROPERTY MAP

LAK-US-20-24.99
PART 2

11 / 98

610
697



- 6 AHMAD DAQAH & AMAD DAGHLAS
01-B-122-0-00-005-0
- 7 AHMAD DAQAH & AMAD DAGHLAS
01-B-122-0-00-049-0
- 8 THOMAS A LASKO & NANCY M LASKO
01-B-122-0-00-004-0
- 9 LOUISE S MORIN
01-B-122-0-00-060-0
L S MORIN LLC
01-B-122-0-00-021-0
- 10 THOMAS J BUKKY
01-B-122-0-00-003-0
- 11 MARY ANNA LTD
01-B-122-0-00-065-0
01-B-122-0-00-019-0
- 12 LAKE MADISON LLC
01-B-126-0-00-021-0
- 60 YVES PAQUET
01-B-126-0-00-020-0

⊙ CURVE DATA
P.I.Sta. 448+45.06
Δ = 1° 56' 50" (LT)
Dc = 0° 13' 45"
R = 25,009.74'
T = 425.05'
L = 850.02'
E = 3.61'
C = 849.98'
C.B. = N 66° 41' 12" E

⊙ CURVE DATA
P.I.Sta. 463+32.35
Δ = 1° 02' 58" (LT)
Dc = 0° 10' 30"
R = 32,753.10'
T = 300.00'
L = 599.98'
E = 1.37'
C = 599.97'
C.B. = N 65° 11' 17" E

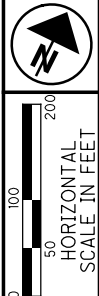
STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

REV. BY	DATE	DESCRIPTION

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RW003.dgn Sheet 7/15/2022 3:01:13 PM troyer

PART OF LOTS 9 & 10
TRACT 3
MADISON TOWNSHIP
LAKE COUNTY



PID NO.
109270

R/W DESIGNER
PSL

R/W REVIEWER
TMM

PROPERTY MAP

LAK-US-20-24.99
PART 2

13/98

612
697

- 21 JOSEPH R GREEN JR, TRUSTEE
01-B-111-0-00-003-0
- 22 DAVID A RAPP
01-B-111-0-00-005-0
- 23 MIKE ZISIS &
KATINA ZISIS
01-B-111-0-00-028-0
- 24 WAL MART REAL ESTATE
BUSINESS TRUST
01-B-112-0-00-019-0
- 25 RVE REAL ESTATE LTD
& DAKE WGC LLC
01-B-112-0-00-023-0
- 26 PATRICIA A HOFFMAN &
WILLIAM E HOFFMAN
01-B-112-0-00-006-0
- 27 CHRISTOPHER R SAMMET
01-B-111-0-00-007-0
- 59 JOSEPH F WELCH
01-B-111-0-00-006-0
- 63 CRAFT BROTHERS INC.
01-B-111-0-00-008-0

Sta 494+73.13 =
Sta. 10+00.00

64 RVE REAL ESTATE LTD (65%)
AND DAKE WGC LLC (35%)
01-B-112-0-00-020-0

RVE REAL ESTATE LTD (65%)
AND DAKE WGC LLC (35%)
01-B-112-0-00-020-0

ACCESS ROAD
EASEMENT
INST. 2008R003365

EX. 5' WATERLINE
EASEMENT
DOC. #2006R013060

EX. 10' TELEPHONE
& ELECTRIC EASEMENT
DOC. #2006R013060

Ⓢ CURVE DATA
P.I. Sta. 510+71.30
Δ = 19° 25' 40" (RT)
Dc = 1° 00' 00"
R = 5,729.58'
T = 980.80'
L = 1,942.78'
E = 83.34'
C = 1,933.48'
C.B. = N 74° 22' 38" E

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

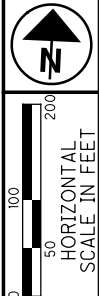
REV. BY	DATE	DESCRIPTION

H:\2018\80743\Project\Data\09270_LAK_20_EAST\Design\RW\Sheets\09270_RM005.dgn Sheet 7/15/2022 3:01:16 PM troyer

PART OF LOTS 6 & 7
TRACT 3
MADISON TOWNSHIP
LAKE COUNTY

⊘ CURVE DATA
P.I. Sta. 536+59.39
Δ = 13° 55' 10" (RT)
Dc = 1° 00' 00"
R = 5,729.58'
T = 699.42'
L = 1,391.94'
E = 42.53'
C = 1,388.52'
C.B. = S 88° 56' 57" E

⊘ CURVE DATA
P.I. Sta. 510+71.30
Δ = 19° 25' 40" (RT)
Dc = 1° 00' 00"
R = 5,729.58'
T = 980.80'
L = 1,942.78'
E = 83.34'
C = 1,933.48'
C.B. = N 74° 22' 38" E



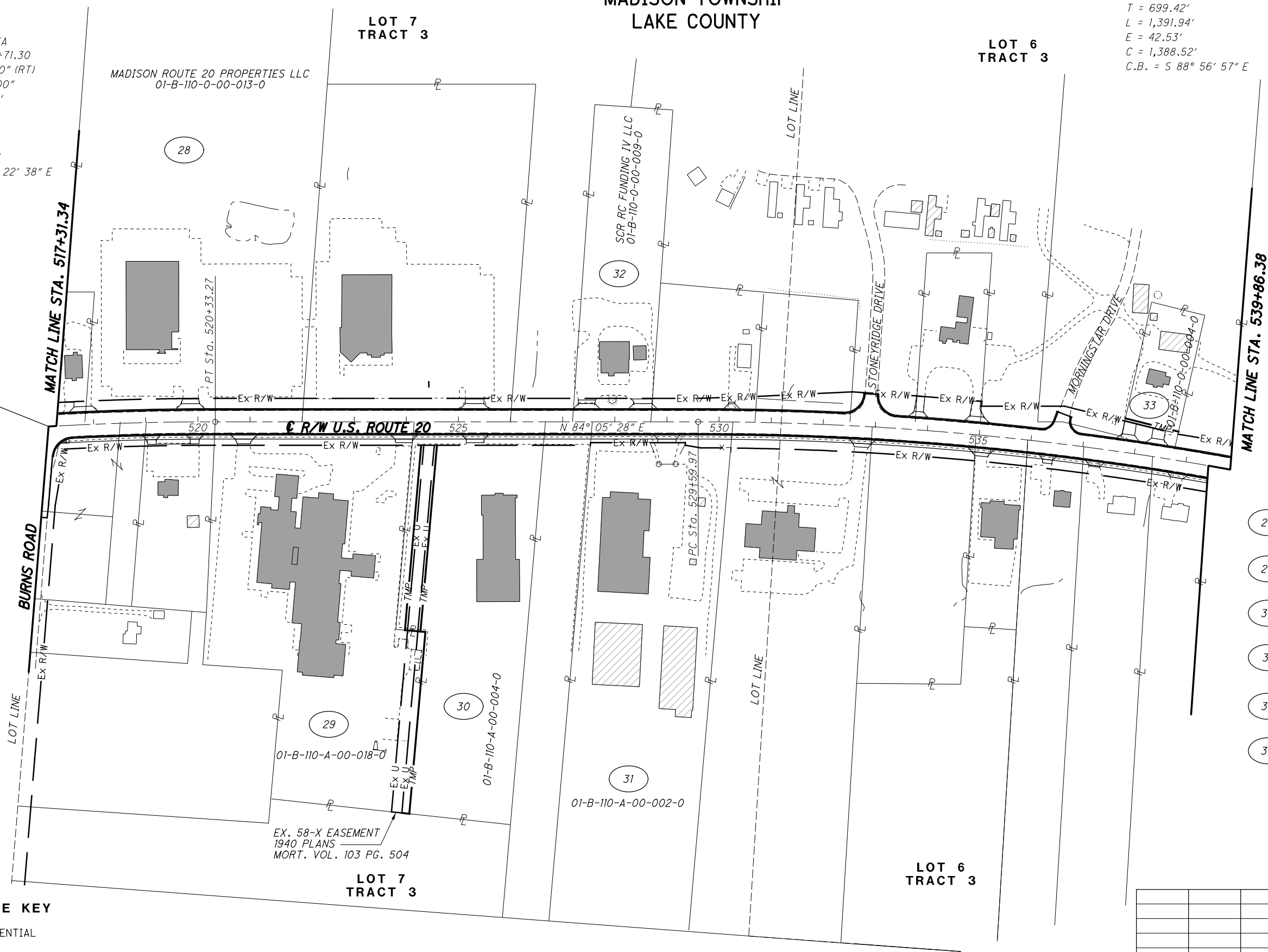
PID NO. **109270**
R/W DESIGNER PSL
R/W REVIEWER TMM

PROPERTY MAP

LAK-US-20-24.99
PART 2

14/98

613
697



- 28 MADISON ROUTE 20 PROPERTIES LLC
01-B-110-0-00-013-0
- 29 LAKE HOSPITAL SYSTEMS INC
01-B-110-A-00-018-0
- 30 MADISON ROUTE 20 PROPERTIES LLC
01-B-110-A-00-004-0
- 31 CARTER JONES LUMBER CO
01-B-110-A-00-002-0
- 32 SCF RC FUNDING IV LLC
01-B-110-0-00-009-0
- 33 JAMES PLACE PROPERTIES INC
01-B-110-0-00-004-0

STRUCTURE KEY

	RESIDENTIAL
	COMMERCIAL
	OUT-BUILDING

REV. BY	DATE	DESCRIPTION

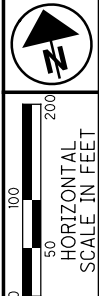
H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RM006.dgn Sheet 7/15/2022 3:04:17 PM troyer

Sta 517+13.47 =
Sta. 10+00.00

EX. 58-X EASEMENT
1940 PLANS
MORT. VOL. 103 PG. 504

PARAT OF LOTS 1, 2 & 6
TRACT 3
MADISON TOWNSHIP
LAKE COUNTY

⊘ CURVE DATA
P.I. Sta. 536+59.39
Δ = 13° 55' 10" (RT)
Dc = 1° 00' 00"
R = 5,729.58'
T = 699.42'
L = 1,391.94'
E = 42.53'
C = 1,388.52'
C.B. = S 88° 56' 57" E



PID NO.
109270

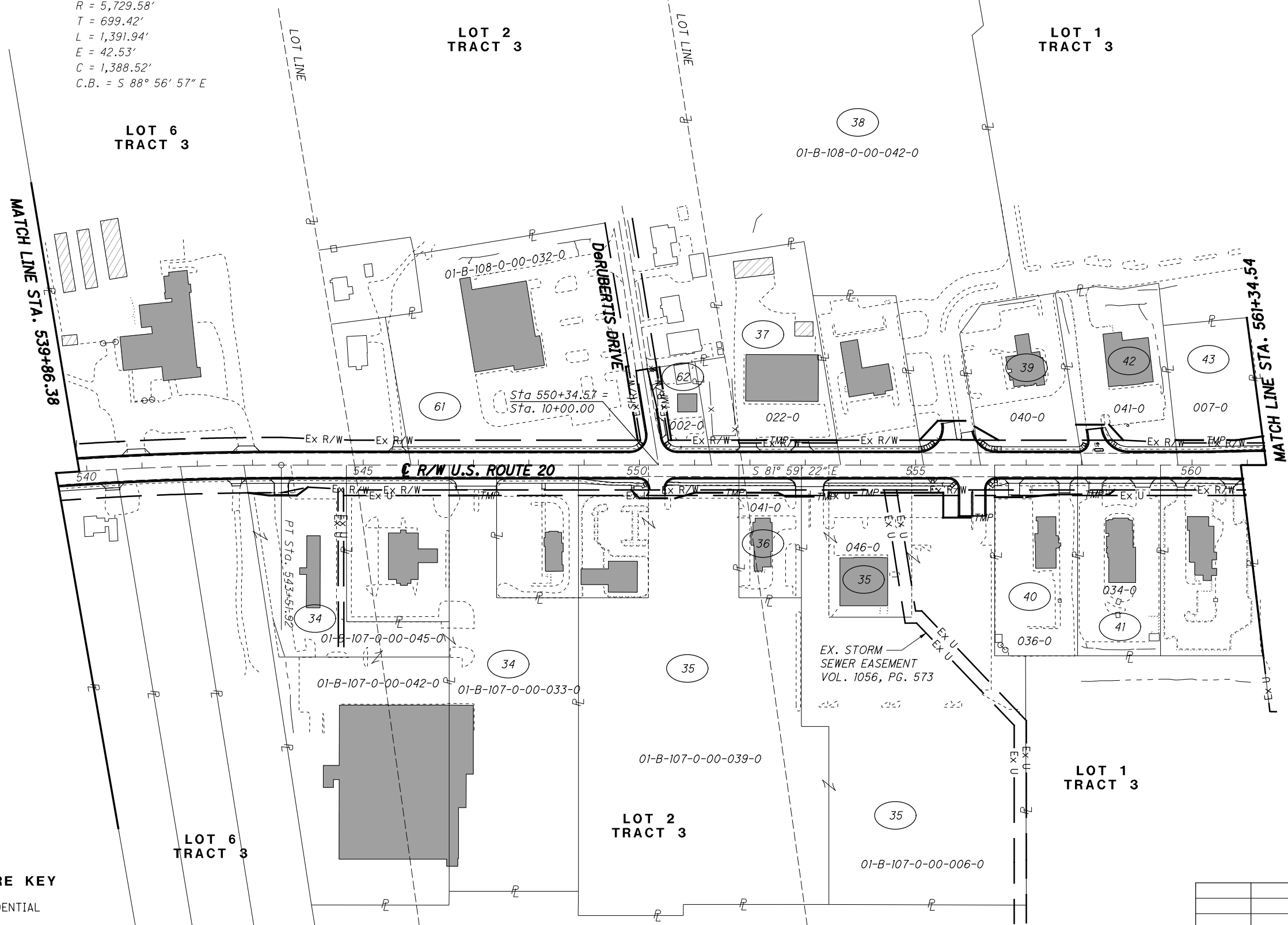
R/W DESIGNER PSL
R/W REVIEWER TMM

PROPERTY MAP

**LAK-US-20-24.99
PART 2**

15 / 98

614
697



- 34 CASTLE CITY ASSOCIATES
01-B-107-0-00-045-0
01-B-107-0-00-042-0
01-B-107-0-00-033-0
- 35 PASQUALE DESANTIS AND CARMELA DESANTIS AS TRUSTEES OF THE DESANTIS FAMILY TRUST DATED AUGUST 12, 2003
01-B-107-0-00-039-0
01-B-107-0-00-040-0
01-B-107-0-00-006-0
01-B-107-0-00-046-0
- 36 THOMAS 22 LIMITED
01-B-107-0-00-041-0
- 37 RAKES FLOORING INC
01-B-108-0-00-022-0
- 38 CLINIC CARE INC
01-B-108-0-00-042-0
- 39 ANDOVER BANK
01-B-108-0-00-040-0
- 40 ARBY'S PROPERTIES LLC
01-B-107-0-00-036-0
- 41 McDONALDS REAL ESTATE COMPANY
01-B-107-0-00-034-0
- 42 BALOGH REAL ESTATE PARTNERSHIP
01-B-108-0-00-041-0
- 43 NIHARI MADISON LLC
01-B-108-A-00-007-0
- 61 ALDI INC.
01-B-108-0-00-032-0
- 62 LORRAINE PETTIT
01-B-108-B-00-002-0

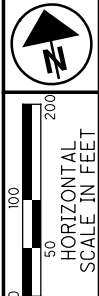
STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

REV. BY	DATE	DESCRIPTION

H:\2018\180743\000\Project\Data\09270_LAK_20_EAST\Design\RW\Sheets\09270_RM007.dgn Sheet 7/15/2022 3:01:19 PM troyer

PART OF LOT NO. 1, TRACT 3
PART OF THE COWLES LOT, TRACT 2
MADISON TOWNSHIP
LAKE COUNTY



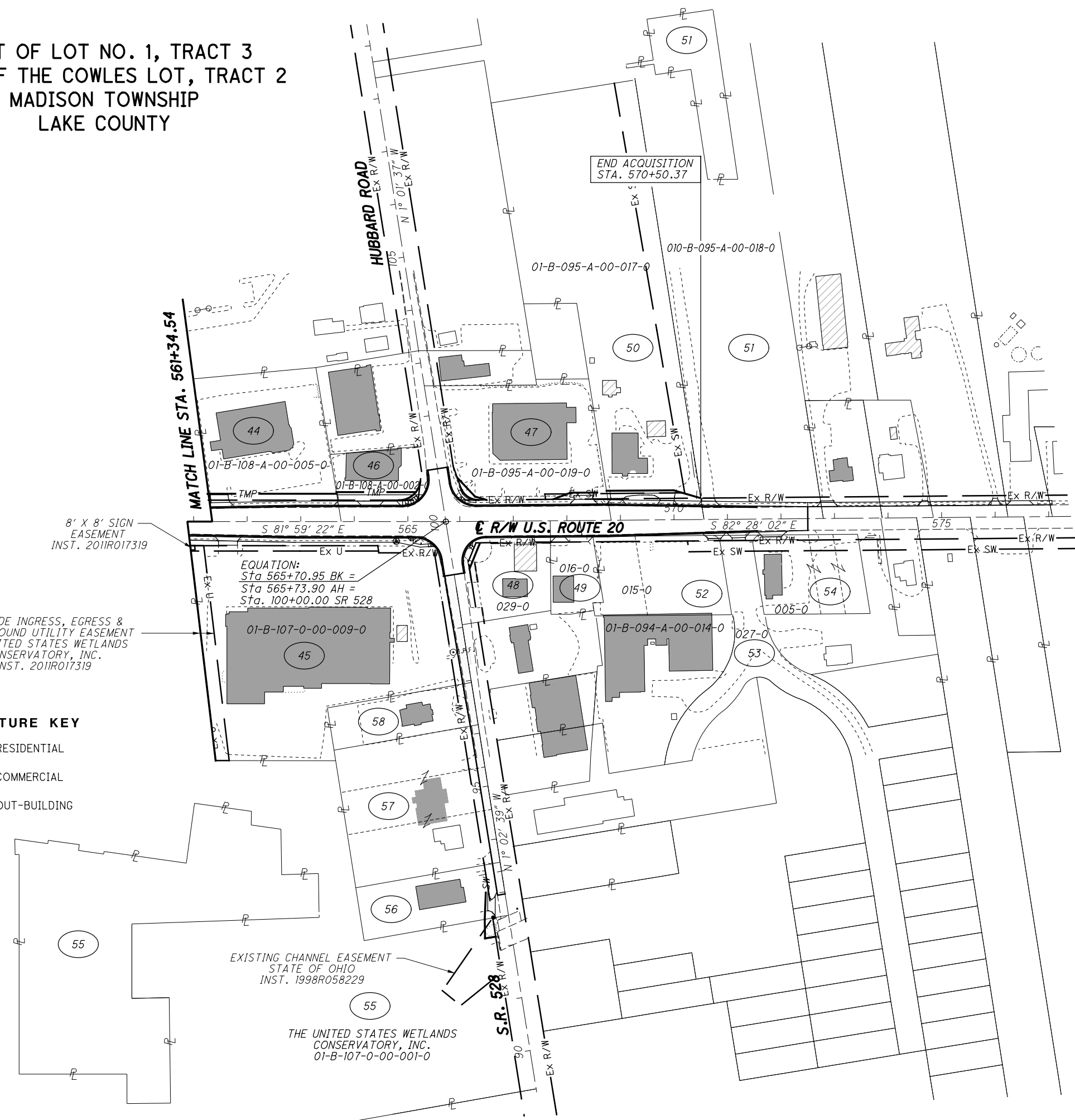
PID NO. **109270**
R/W DESIGNER PSL
R/W REVIEWER TMM

PROPERTY MAP

LAK-US-20-24.99
PART 2

16/98
615
697

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RM008.dgn Sheet 7/15/2022 3:01:20 PM Troyer



STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

- 44 REALTY INCOME CORPORATION
01-B-108-A-00-005-0
- 45 SAMUEL RUBINFELD, TRUSTEE OF THE SAMUEL RUBINFELD LIVING TRUST DATED SEPTEMBER 5, 2001 - 50% PAIGE R BECK, TRUSTEE OF THE PAIGE RUBINFELD BECK LIVING TRUST DATED JUNE 10, 2010 - 25% AND TAMMY OLSON aka TAMMY ROCHELLE MCKENZIE, TRUSTEE OF THE TAMMY ROCHELLE MCKENZIE LIVING TRUST - 25%
01-B-107-0-00-009-0
- 46 HLPV LLC
01-B-108-A-00-002-0
- 47 GLENN L GNAGY, TRUSTEE OF THE MAIN GNAGY FAMILY TRUST, U/A DATED 10/18/2002
01-B-095-A-00-019-0
- 48 WORLD PARTNERS LLC
01-B-094-A-00-029-0
- 49 ADWAY LLC
01-B-094-A-00-016-0
- 50 TATES REAL ESTATE INVESTMENTS LLC
01-B-095-A-00-017-0
- 51 NORMAN B NICHOLS AND RUTH ANN NICHOLS
010-B-095-A-00-018-0
- 52 GEO BOB KEN INC
01-B-094-A-00-014-0
01-B-094-A-00-015-0
- 53 ELEANOR GERAK
01-B-094-A-00-027-0
- 54 EKG MANAGEMENT LLC
01-B-094-A-00-005-0
01-B-094-0-00-004-0
01-B-094-0-00-003-0
- 55 THE UNITED STATES WETLANDS CONSERVATORY, INC.
01-B-107-0-00-001-0
- 56 CLS OHIO SYSTEM, LLC
01-B-888-0-00-600-0
- 57 HDGV LIMITED
01-B-107-0-00-010-0
01-B-107-0-00-029-0
01-B-107-0-00-030-0
- 58 ROBERT M POTTI & CAROL J POTTI
01-B-107-0-00-031-0

REV. BY	DATE	DESCRIPTION

TOTAL NUMBER OF :

47 OWNERSHIPS 0 TOTAL TAKES
 58 PARCELS 0 OWNERSHIPS W/ STRUCTURES INVOLVED

NET RESIDUE = RECORD AREA - TOTAL PRO - NET TAKE

GRANTEE :

ALL RIGHT OF WAY ACQUIRED IN THE NAME OF
 THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION
 UNLESS OTHERWISE SHOWN.

(c) = CALCULATED AREA

ALL AREAS IN ACRES

PARCEL NO.	OWNER	SHEET NO.	OWNERS RECORD		AUDITOR'S PARCEL	RECORD AREA	TOTAL P.R.O.	GROSS TAKE	P.R.O. IN TAKE	NET TAKE	STRUC-TURE	NET RESIDUE		TYPE FUND	REMARKS	AS ACQUIRED		
			BOOK	PAGE								LEFT	RIGHT			BOOK	PAGE	
1-T	THOMAS J HOWELL AND MICHELE L HOWELL	27-28	1999R016666		01-B-123-0-00-011-0	3.943	0.1557	0.022		0.022					↑	DRIVEWAY CONSTRUCTION & GRADING		
			2010R023642															
2-T	DONALD J MAH, TRUSTEE OF THE DONALD J MAH REVOCABLE LIVING TRUST, JAQUELINE M WRIGHT, THERESE MACKEY AND VICKI GURLEY	35-36	2015R011371		01-B-122-0-00-018-0	3.902	0.058	0.009		0.009						GRADING		
3-T	MUSIC EXPRESS BROADCASTING CORPORATION OF NORTHEAST OHIO	35-36	1044	1229	01-B-122-0-00-061-0	9.862	0.140	0.023		0.023						DRIVEWAY CONSTRUCTION & GRADING		
4-T	RODNEY WILEY	37-38	2013R024205		01-B-122-0-00-026-0	0.450	0.092 (c)	0.011		0.011	YES					DRIVEWAY CONSTRUCTION & GRADING *PRIVATE WALK ACCESS TO MAILBOX		
5-SH	FIVE GUYS DEVELOPMENT LLC	37-38	2005R056322		01-B-122-0-00-019-0	18.000	0.389	0.037	0	0.037						WATER QUALITY STRUCTURE DRAINAGE OUTLET		
5-S		37-38						0.058	0	0.058								
					01-B-122-0-00-020-0	39.760	0.640	0	0	0				100% STATE				
6-T	AHMAD DAQAH AND AMAD DAGHLAS	39-40	2014R015624		01-B-122-0-00-005-0	17.047	0.160	0.015		0.015						DRIVEWAY CONSTRUCTION & GRADING		
7-T	AHMAD DAQAH AND AMAD DAGHLAS	41-42	2016R032679		01-B-122-0-00-049-0	1.730	0.176	0.039		0.039						DRIVEWAY CONSTRUCTION & GRADING *KRISHACK SIGN ENCROACHES		
8-T	THOMAS A LASKO AND NANCY M LASKO	41-42	2004R031885		01-B-122-0-00-004-0	21.100	0.340	0.096		0.096						DRIVEWAY CONSTRUCTION & GRADING *ASPHALT PARKING AREA ENCROACHES		
9-T	LOUISE S MORIN	43-44	800	265	01-B-122-0-00-060-0	2.900	0.121	0.028		0.028						DRIVEWAY CONSTRUCTION & GRADING		
9A-T	LS MORIN, LLC	43-44	2003R005330		01-B-122-0-00-021-0	26.820	0.050	0.015		0.015						GRADING		
10-T	THOMAS J BUKKY	43-44	913	1210	01-B-122-0-00-003-0	3.050	0.450	0.022		0.022						DRIVEWAY CONSTRUCTION & GRADING *274 LF SPLIT RAIL FENCE ENCROACHES *37 LF CONC. BARRIER ENCROACHES		

NOTE: ALL TEMPORARY PARCELS TO BE OF 36 MONTH DURATION.

NOTE: UNDER NO CIRCUMSTANCES ARE TEMPORARY EASEMENTS TO BE USED FOR STORAGE OF MATERIAL OR EQUIPMENT BY THE CONTRACTOR UNLESS NOTED OTHERWISE.

TYPES OF TITLE LEGEND:

SH = STANDARD HIGHWAY EASEMENT
 T = TEMPORARY EASEMENT
 S = SEWER EASEMENT

+ DENOTES REMOVAL ITEMS
 SEE CORRESPONDING RIGHT OF WAY PLAN SHEET FOR DESCRIPTION

* DENOTES RIGHT OF WAY ENCROACHMENT

REV. BY	DATE	DESCRIPTION
FIELD REVIEW BY	DATE:	
OWNERSHIP VERIFIED BY	DATE:	
DATE COMPLETED		

FEDERAL PROJECT NO. E190934
 PID NO. 109270
 STATE JOB NO. 527708
 R/W DESIGNER PSL
 R/W REVIEWER TMM
SUMMARY OF ADDITIONAL RIGHT OF WAY
LAK-US-20-24.99 PART 2
 17/98
 616
 697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RS001.dgn_Sheet 7/5/2022 3:01:22 PM troyer

TOTAL NUMBER OF :
 47 OWNERSHIPS 0 TOTAL TAKES
 58 PARCELS 0 OWNERSHIPS W/ STRUCTURES INVOLVED

NET RESIDUE = RECORD AREA - TOTAL PRO - NET TAKE

GRANTEE:
 ALL RIGHT OF WAY ACQUIRED IN THE NAME OF
 THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION
 UNLESS OTHERWISE SHOWN.

(c) = CALCULATED AREA

ALL AREAS IN ACRES

PARCEL NO.	OWNER	SHEET NO.	OWNERS RECORD		AUDITOR'S PARCEL	RECORD AREA	TOTAL P.R.O.	GROSS TAKE	P.R.O. IN TAKE	NET TAKE	STRUC-TURE	NET RESIDUE		TYPE FUND	REMARKS	AS ACQUIRED	
			BOOK	PAGE								LEFT	RIGHT			BOOK	PAGE
11-T	MARY ANNA LTD	43-48	2002R020347		01-B-122-0-00-065-0	2.958	0.258	0.046		0.046					DRIVEWAY CONSTRUCTION & GRADING		
			838	1237	01-B-121-0-00-019-0	54.892	0.709 (c)	0.115		0.115							
					TOTAL			0.161		0.161							
12-T	LAKE MADISON LLC	45-46	2001R039307		01-B-126-0-00-021-0	5.150	0.690	0.006		0.006					GRADING * 517 LF CHAIN LINK FENCE ENCROACHES		
13-T	THOMAS J BUKKY	47-50	2005R007937		01-B-121-0-00-009-0	2.000	0.070	0.032		0.032					DRIVEWAY CONSTRUCTION & GRADING		
			2011R019181		01-B-121-0-00-008-0	24.620	0.080	0.017		0.017							
			2011R019181		01-B-121-0-00-017-0	0.930	0.170	0.027		0.027							
			2001R057416		01-B-121-0-00-007-0	3.000	0.1507	0.035		0.035							
					TOTAL	30.55	0.4707	0.111		0.111							
14-T	KELSEY J REYNOLDS	49-50 91-92	2014R021438		01-B-126-0-00-042-0	0.810	0.055	0.033		0.033					DRAINAGE OUTLET CONSTRUCTION 6" TREE TO BE REMOVED		
15-T	SM BECKER PROPERTIES LLC	49-50 91-92	2016R029575		01-B-126-0-00-013-0	5.320	0.160	0.291		0.291					DRAINAGE OUTLET CONSTRUCTION OVERLAPS EX. DRAINAGE EASEMENT BY 7546 SF 3", 2-12" 20" TREES AND 20" STUMP TO BE REMOVED		
16-T1	A & L NURSERIES, LLC	51-52	2021R044597		01-B-121-0-00-005-0	3.091	0.140	0.007		0.007					DRIVEWAY CONSTRUCTION & GRADING		
16-T2		51-52			01-B-121-0-00-003-0	94.050	0.510	0.009		0.009					* 888 LF CHAIN LINK FENCE ENCROACHES		
17-T	FISKE ESTATES I, LLC	51-52	2022R003890		01-B-126-0-00-011-0	1.669	0.140	0.091		0.091					DRIVEWAY CONSTRUCTION & GRADING		
18-T	J R PACHECO, LLC	53-54	2021R040069		01-B-126-0-00-009-0	2.187	0.1529	0.040		0.040					GRADING		
19	GYPSY PEDDLER	53-54	2016R020391		01-B-126-0-00-007-0	3.155									NO RW REQUIRED * FULL ASPHALT FRONTAGE		
20-T	DAVID FISHER AND ROSE FISHER	53-54	2011R007814		01-B-126-0-00-035-0	8.200	0.140	0.016		0.016					DRIVEWAY CONSTRUCTION & GRADING ONE (1) FORMED SILVER MAPLE TREE TO BE REMOVED		

100% STATE

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RS002.dgn Sheet 7/15/2022 3:01:23 PM troyer

NOTE: ALL TEMPORARY PARCELS TO BE OF 36 MONTH DURATION.
 NOTE: UNDER NO CIRCUMSTANCES ARE TEMPORARY EASEMENTS TO BE USED FOR STORAGE OF MATERIAL OR EQUIPMENT BY THE CONTRACTOR UNLESS NOTED OTHERWISE.

TYPES OF TITLE LEGEND:
 SH = STANDARD HIGHWAY EASEMENT
 T = TEMPORARY EASEMENT
 S = SEWER EASEMENT

+ DENOTES REMOVAL ITEMS SEE CORRESPONDING RIGHT OF WAY PLAN SHEET FOR DESCRIPTION
 * DENOTES RIGHT OF WAY ENCROACHMENT

TMM	3/16/22	REVISED OWNERSHIP PARCEL 18
REV. BY	DATE	DESCRIPTION
FIELD REVIEW BY	DATE:	
OWNERSHIP VERIFIED BY	DATE:	
DATE COMPLETED		

FEDERAL PROJECT NO. E190934
 PID NO. 109270
 STATE JOB NO. 527708
 R/W DESIGNER PSL
 R/W REVIEWER TMM
SUMMARY OF ADDITIONAL RIGHT OF WAY
 LAK-US-20-24.99 PART 2
 18/98
 617
 697

TOTAL NUMBER OF :

47 OWNERSHIPS 0 TOTAL TAKES
 58 PARCELS 0 OWNERSHIPS W/ STRUCTURES INVOLVED

NET RESIDUE = RECORD AREA - TOTAL PRO - NET TAKE

GRANTEE :

ALL RIGHT OF WAY ACQUIRED IN THE NAME OF
 THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION
 UNLESS OTHERWISE SHOWN.

ALL AREAS IN ACRES

(c) = CALCULATED AREA

PARCEL NO.	OWNER	SHEET NO.	OWNERS RECORD		AUDITOR'S PARCEL	RECORD AREA	TOTAL P.R.O.	GROSS TAKE	P.R.O. IN TAKE	NET TAKE	STRUC-TURE	NET RESIDUE		TYPE FUND	REMARKS	AS ACQUIRED	
			BOOK	PAGE								LEFT	RIGHT			BOOK	PAGE
21-T	JOSEPH R GREEN, JR, SUCCESSOR TRUSTEE OF THE JOSEPH GREEN TRUST DATED JULY 13, 2000	59-60	2018R023285		01-B-111-0-00-003-0	10.683	0.556	0.117		0.117					GRADING		
			2013R036801														
22-T1 22-T2	DAVID A RAPP	61-62 61-62	2017R013288		01-B-111-0-00-005-0	15.526	0.2356	0.011		0.011					DRIVEWAY CONSTRUCTION & GRADING		
			2017R013287					0.038		0.038					GRADING		
			2017R013286														
23-T	MIKE ZISIS AND KATINA ZISIS	61-62	2001R030582		01-B-111-0-00-028-0	1.110	0.138 (c)	0.034		0.034					DRIVEWAY CONSTRUCTION & GRADING 8 PARKING SPACES ALONG FRONT OF BUILDING WILL BE IMPACTED BY TEMPORARY TAKE DUE TO REDUCED INGRESS EGRESS AREA		
24-T	WAL-MART REAL ESTATE BUSINESS TRUST A DELAWARE STATUTORY TRUST	61-62	2008R021972		01-B-112-0-00-019-0	35.020	0.9718	0.137		0.137					DRIVEWAY CONSTRUCTION & GRADING OVERLAPS EX. TELEPHONE & ELEC. EASEMENT BY 439 SF & EX. WATERLINE EASEMENT BY 239 SF OVERLAPS EX. ACCESS EASEMENT BY 2208 SF		
25-T	RVE REAL ESTATE, LTD, AN OHIO LIMITED LIABILITY COMPANY (65%) AND DAKE WGC, LLC, AN OHIO LIMITED LIABILITY COMPANY (35%)	63-64	2019R025099		01-B-112-0-00-023-0	2.102	0.1251	0.030		0.030			100% STATE		GRADING		
26	WILLIAM E HOFFMAN AND PATRICIA A HOFFMAN	63-64	216	972	01-B-112-0-00-006-0	2.890									NO RW REQUIRED		
27-T	CHRISTOPHER R SAMMET	63-64	2016R013700		01-B-111-0-00-007-0	0.970	0.120	0.015		0.015					DRIVEWAY CONSTRUCTION & GRADING * ROCK LANDSCAPING ENCROACHES		
28	MADISON ROUTE 20 PROPERTIES LLC	67-68 69-72	2011R015592		01-B-110-0-00-013-0	15.960									NO RW REQUIRED		
			2019R012321		01-B-110-0-00-011-0	5.950											
29-T	LAKE HOSPITAL SYSTEMS, INC	93-94	1997R040280		01-B-110-A-00-018-0	5.783	0.269	0.116		0.116	YES				STORM SEWER CONSTRUCTION & GRADING HELIPAD COORDINATION WITH HOSPITAL FENCE, DUMPSTER AND SHED TO BE REMOVED CONTRACTOR TO BE LIMITED TO 2 WEEK TIME FRAME FOR CONSTRUCTION OF OUTLET		
30-T	MADISON ROUTE 20 PROPERTIES LLC	69-70 93-94	2019R012321		01-B-110-A-00-004-0	3.946	0.170	0.246		0.246					STORM SEWER CONSTRUCTION & GRADING OVERLAPS EX. DRAINAGE EASEMENT BY 4522 SF * 6 LIGHT POLES ENCROACH - TAKE APPROX. 54 PARKING SPACE AFFECTED CONTRACTOR TO BE LIMITED TO 2 WEEK TIME FRAME FOR CONSTRUCTION OF OUTLET PIPE		
			2003R044050														

NOTE: ALL TEMPORARY PARCELS TO BE OF 36 MONTH DURATION.

NOTE: UNDER NO CIRCUMSTANCES ARE TEMPORARY EASEMENTS TO BE USED FOR STORAGE OF MATERIAL OR EQUIPMENT BY THE CONTRACTOR UNLESS NOTED OTHERWISE.

TYPES OF TITLE LEGEND:

SH = STANDARD HIGHWAY EASEMENT
 T = TEMPORARY EASEMENT
 S = SEWER EASEMENT

+ DENOTES REMOVAL ITEMS
 SEE CORRESPONDING RIGHT OF WAY PLAN SHEET FOR DESCRIPTION

* DENOTES RIGHT OF WAY ENCROACHMENT

PSL	10/15/21	REVISED PARCEL 21
REV. BY	DATE	DESCRIPTION
FIELD REVIEW BY	DATE:	
OWNERSHIP VERIFIED BY	DATE:	
DATE COMPLETED		

FEDERAL PROJECT NO. E190934
 PTD NO. 109270
 STATE JOB NO. 527708
 R/W DESIGNER PSL
 R/W REVIEWER TMM
SUMMARY OF ADDITIONAL RIGHT OF WAY
LAK-US-20-24.99 PART 2
 19/98
 618
 697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RS003.dgn Sheet 7/15/2022 3:01:24 PM troyer

TOTAL NUMBER OF :
 47 OWNERSHIPS 0 TOTAL TAKES
 58 PARCELS 0 OWNERSHIPS W/ STRUCTURES INVOLVED

NET RESIDUE = RECORD AREA - TOTAL PRO - NET TAKE

GRANTEE :

ALL RIGHT OF WAY ACQUIRED IN THE NAME OF
 THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION
 UNLESS OTHERWISE SHOWN.

(c) = CALCULATED AREA

ALL AREAS IN ACRES

PARCEL NO.	OWNER	SHEET NO.	OWNERS RECORD		AUDITOR'S PARCEL	RECORD AREA	TOTAL P.R.O.	GROSS TAKE	P.R.O. IN TAKE	NET TAKE	STRUC-TURE	NET RESIDUE		TYPE FUND	REMARKS	AS ACQUIRED	
			BOOK	PAGE								LEFT	RIGHT			BOOK	PAGE
31	THE CARTER JONES LUMBER CO	71-72	545	753	01-B-110-A-00-002-0	6.041									NO RW REQUIRED * 174 L.F. CHAIN LINK FENCE ENCROACHES		
32	SCF RC FUNDING IV LLC, A DELAWARE LIMITED LIABILITY COMPANY	71-72	2017R028730		01-B-110-0-00-009-0	2.080									NO RW REQUIRED * REFRESH DENTAL SIGN & FLOWER BED ENCROACHES		
33-T	JAMES PLACE PROPERTIES INC., A CORPORATION	75-76	655	746	01-B-110-0-00-004-0	0.630	0.070	0.013		0.013					DRIVEWAY CONSTRUCTION & GRADING		
34-T1	CASTLE CITY ASSOCIATES,	77-78	1999R008901		01-B-107-0-00-045-0	1.370	0.069	0.012		0.012					DRIVEWAY CONSTRUCTION & GRADING		
34-T2	A PENNSYLVANIA LIMITED PARTNERSHIP	77-78	1999R008902		01-B-107-0-00-033-0	3.183	0.0888	0.020		0.020					DRIVEWAY CONSTRUCTION & GRADING		
			1998R013662		01-B-107-0-00-042-0	3.087	0.096 (c)								34T-2 OVERLAPS EX. W/L EASEMENT BY 860 S.F.		
35-T1	PASQUALE DESANTIS AND	79-80	2008R003146		01-B-107-0-00-040-0	0.620	0.087	0.008		0.008					DRIVEWAY CONSTRUCTION & GRADING		
35-T2	CARMELA DESANTIS AS TRUSTEES OF THEDESANTIS FAMILY TRUST DATED 12-Aug-2003	81-82			01-B-107-0-00-039-0	6.500	0.1701	0.034		0.034					DRIVEWAY CONSTRUCTION & GRADING		
					01-B-107-0-00-006-0	6.053	0.1377	0.127		0.127					35-T1 OVERLAPS EX. WATERLINE EASEMENT BY 1455 SF		
			2014R013123		01-B-107-0-00-046-0	0.947	0.155	0.017		0.017					35-T2 OVERLAPS EX. UTILITY EASEMENT BY 111 SF & EX. WATERLINE EASEMENT BY 2434 SF		
36-T	THOMAS 22 LIMITED	81-82	2008R030234		01-B-107-0-00-041-0	0.700	0.130	0.019		0.019					DRIVEWAY CONSTRUCTION & GRADING 36-T OVERLAPS EX. WATERLINE EASEMENT BY 753 SF		
37-T	RAKES FLOORING, INC., AN OHIO CORPORATION	81-82	362	1185	01-B-108-0-00-022-0	1.630	0.163 (c)	0.020		0.020					DRIVEWAY CONSTRUCTION & GRADING		
38-T	CLINIC CARE, INC., AN OHIO LIMITED LIABILITY COMPANY	81-82	2007R004800		01-B-108-0-00-042-0	8.618	0.0709	0.067		0.067					DRIVEWAY CONSTRUCTION & GRADING		
39-T	THE ANDOVER BANK	83-84	2010R012430		01-B-108-0-00-040-0	1.234	0.127	0.011		0.011					CURB RAMP CONSTRUCTION AND GRADING		
40-T	ARBY'S PROPERTIES LLC	83-84	2016R001981		01-B-107-0-00-036-0	1.190	0.100	0.037		0.037					DRIVEWAY CONSTRUCTION & GRADING 40-T OVERLAPS EX. WATERLINE EASEMENT BY 1393 SF		

NOTE: ALL TEMPORARY PARCELS TO BE OF 36 MONTH DURATION.

NOTE: UNDER NO CIRCUMSTANCES ARE TEMPORARY EASEMENTS TO BE USED FOR STORAGE OF MATERIAL OR EQUIPMENT BY THE CONTRACTOR UNLESS NOTED OTHERWISE.

TYPES OF TITLE LEGEND:

SH = STANDARD HIGHWAY EASEMENT
 T = TEMPORARY EASEMENT
 S = SEWER EASEMENT

+ DENOTES REMOVAL ITEMS SEE CORRESPONDING RIGHT OF WAY PLAN SHEET FOR DESCRIPTION

* DENOTES RIGHT OF WAY ENCROACHMENT

REV. BY	DATE	DESCRIPTION
FIELD REVIEW BY	DATE:	
OWNERSHIP VERIFIED BY	DATE:	
DATE COMPLETED		

FEDERAL PROJECT NO. E190934
 PID NO. 109270
 STATE JOB NO. 527708
 R/W DESIGNER PSL
 R/W REVIEWER TMM
SUMMARY OF ADDITIONAL RIGHT OF WAY
 LAK-US-20-24.99 PART 2
 20/98
 619
 697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RS004.dgn Sheet 7/15/2022 3:01:25 PM troyer

TOTAL NUMBER OF :

47 OWNERSHIPS 0 TOTAL TAKES
 58 PARCELS 0 OWNERSHIPS W/ STRUCTURES INVOLVED

NET RESIDUE = RECORD AREA - TOTAL PRO - NET TAKE

GRANTEE :

ALL RIGHT OF WAY ACQUIRED IN THE NAME OF
 THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION
 UNLESS OTHERWISE SHOWN.

ALL AREAS IN ACRES

(c) = CALCULATED AREA

PARCEL NO.	OWNER	SHEET NO.	OWNERS RECORD		AUDITOR'S PARCEL	RECORD AREA	TOTAL P.R.O.	GROSS TAKE	P.R.O. IN TAKE	NET TAKE	STRUC-TURE	NET RESIDUE		TYPE FUND	REMARKS	AS ACQUIRED	
			BOOK	PAGE								LEFT	RIGHT			BOOK	PAGE
51	NORMAN B NICHOLS AND RUTH ANN NICHOLS	89-90	2017R016383		01-B-095-A-00-018-0	9.126									NO RW REQUIRED		
52	GEO BOB KEN INC	87-88	1999R026872		01-B-094-A-00-015-0	0.810									NO RW REQUIRED		
			586	323	01-B-094-A-00-014-0	1.210											
			TOTAL			2.020											
53	ELEANOR GERAK	89-90	2003R043800		01-B-094-A-00-027-0	1.180									NO RW REQUIRED		
54	EKG MANAGEMENT, A LIMITED LIABILITY CORPORATION	89-90	2003R043799		01-B-094-A-00-005-0	0.350									NO RW REQUIRED		
					01-B-094-A-00-004-0	0.150											
					01-B-094-A-00-003-0	0.150											
55-T	THE UNITED STATES WETLANDS CONSERVATORY, INC., AN OHIO CORPORATION	95-96	2006R051920		01-B-107-0-00-001-0	56.420	0.226	0.014		0.014					STORM SEWER CONSTRUCTION & GRADING OVERLAP EX. CHANNEL EASEMENT BY 492 S.F.		
			1046	337													
56-S	CSL OHIO SYSTEM, LLC	95-96	2015R032004		01-B-888-0-00-600-0	0.630	(c)	0.069		0.013					STORM SEWER EASEMENT		
57	HDTV LIMITED, AN OHIO LIMITED LIABILITY COMPANY	95-96	2018R032162		01-B-107-0-00-010-0	0.630									NO RW REQUIRED		
					01-B-107-0-00-029-0	0.500	(c)										
					01-B-107-0-00-030-0	0.500											
			TOTAL			1.630											
58	ROBERT M POTTI AND CAROL J POTTI	97-98	2005R008008		01-B-107-0-00-031-0	0.510									NO RW REQUIRED		
59-T	JOSEPH F WELCH	61-62	1998R057711		01-B-111-0-00-006-0	0.440	0.060	0.023		0.023					DRIVEWAY CONSTRUCTION & GRADING		
			891	776													
60	YVES PAQUET	47-48	260 944		01-B-126-0-00-020-0	3.230									NO RW REQUIRED *PAQUET AUTO SALES SIGN ENCROACHES FULL ASPHALT FRONTAGE-PARKING IMPACTS		
61-SH	ALDI INC.	79-80	2016R005292		01-B-108-0-00-032-0	3.817	0.4374	0.015		0.015			3.3646		GRADING		
			2015R018112														
62-T	LORRAINE PETTIT	79-80	2018R015156		01-B-108-B-00-002-0	0.230	0	0.015		0.015					GRADING		
			2016R023978														
63-SH	CRAFT BROTHERS INC.	67-68	2004R057833		01-B-111-0-00-008-0	13.050	1.200	0.005		0.005			11.845				
64-SH	RVE REAL ESTATE LTD & DAKE WGC LLC	56-60	2019R025099		01-B-112-0-00-020-0	4.8686	0.7903	0.011		0.011			4.0673				
65-T	RAYMOND B PHILLIPP	23-24	2018R014706		01-B-123-0-00-059-0	1.110	0.168	0.026		0.026					EMBANKMENT GRADING		
66-T1	BRAFF MOBILE HOME PARK CO	25-26	868 832		01-B-123-0-00-034-0	16.390	0.544	0.011		0.011					EMBANKMENT GRADING		
66-T2		25-26						0.078		0.078					EMBANKMENT GRADING		

100% STATE

PROJECT NO. 190934
 PTD NO. 109270
 STATE JOB NO. 527708
 R/W DESIGNER PSL
 R/W REVIEWER TMM
SUMMARY OF ADDITIONAL RIGHT OF WAY
LAK-US-20-24.99 PART 2

NOTE: YD ALL TEMPORARY PARCELS TO BE OF 36 MONTH DURATION.
 NOTE: UNDER NO CIRCUMSTANCES ARE TEMPORARY EASEMENTS TO BE USED FOR STORAGE OF MATERIAL OR EQUIPMENT BY THE CONTRACTOR UNLESS NOTED OTHERWISE.

TYPES OF TITLE LEGEND:
 SH = STANDARD HIGHWAY EASEMENT
 T = TEMPORARY EASEMENT
 S = SEWER EASEMENT

+ DENOTES REMOVAL ITEMS SEE CORRESPONDING RIGHT OF WAY PLAN SHEET FOR DESCRIPTION
 * DENOTES RIGHT OF WAY ENCROACHMENT

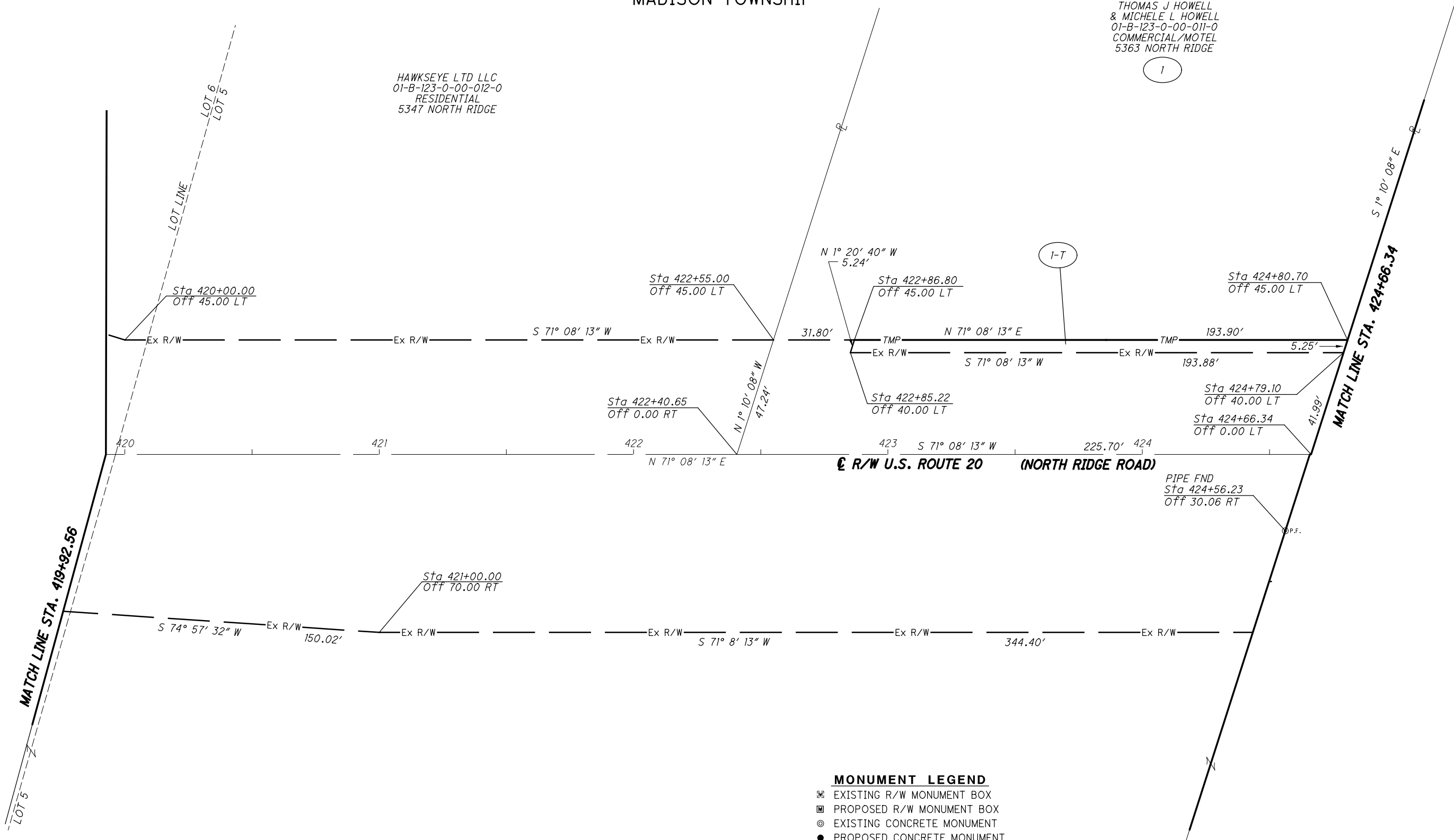
PSL	9/1/21	REVISED PARCEL 61-T TO 61-SH
TMM	1/18/22	ADDED PARCEL 64-SH
TMM	3/16/22	REVISED PARCLE 55-T TO 55-S
CNK	7/15/22	ADDED PARCELS 65-T, 66-T1, AND 66-T2
REV. BY	DATE	DESCRIPTION
FIELD REVIEW BY	DATE:	
OWNERSHIP VERIFIED BY	DATE:	
DATE COMPLETED		

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RS006.dgn Sheet 7/15/2022 3:01:26 PM Troyer

PART OF LOT NOS. 5 & 6
TRACT 4
MADISON TOWNSHIP

HAWKSEYE LTD LLC
01-B-123-0-00-012-0
RESIDENTIAL
5347 NORTH RIDGE

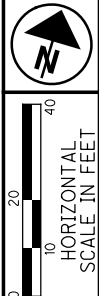
THOMAS J HOWELL
& MICHELE L HOWELL
01-B-123-0-00-011-0
COMMERCIAL/MOTEL
5363 NORTH RIDGE



ROBERT N BRAFF, SUCCESSOR TRUSTEE
OF BETTE G BRAFF
AUGUST 19, 1986
01-B-123-0-00-035-0
COMMERCIAL/MOBILE HOME PARK
5300 NORTH RIDGE

- MONUMENT LEGEND**
- ☐ EXISTING R/W MONUMENT BOX
 - ▣ PROPOSED R/W MONUMENT BOX
 - ⊙ EXISTING CONCRETE MONUMENT
 - PROPOSED CONCRETE MONUMENT
 - ⚡ RAILROAD SPIKE FOUND
 - ⚡ RAILROAD SPIKE SET
 - I.P.F. IRON PIN FOUND
 - ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
 - I.P.S. IRON PIN SET W/ ID CAP
 - ⊙ P.F. IRON PIPE FOUND
 - ⊙ P.S. IRON PIPE SET
 - ⊙ P.K.F. P.K. NAIL FOUND
 - P.K.S. P.K. NAIL SET

REV. BY	DATE	DESCRIPTION



PID NO. **109270**
R/W DESIGNER PSL
R/W REVIEWER TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 419+92.56 TO STA. 424+66.34

LAK-US-20-24.99
PART 2

28/98
627
697

H:\2018\80743\000T\Projec+Data\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB003.dgn Sheet 7/15/2022 3:01:35 PM troyer

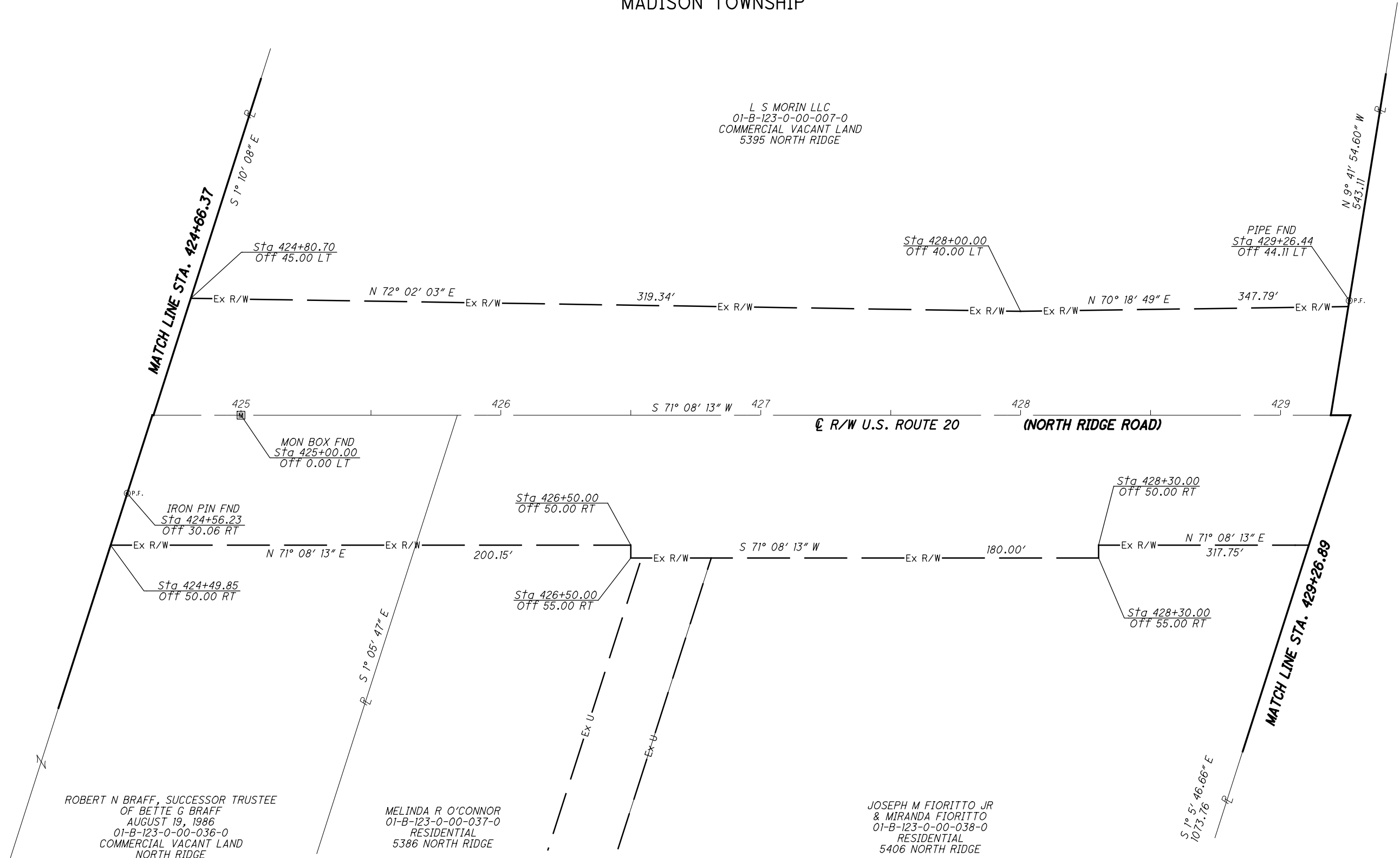
PART OF LOT NO. 5
TOWNSHIP 4
MADISON TOWNSHIP

L S MORIN LLC
01-B-123-0-00-007-0
COMMERCIAL VACANT LAND
5395 NORTH RIDGE

ROBERT N BRAFF, SUCCESSOR TRUSTEE
OF BETTE G BRAFF
AUGUST 19, 1986
01-B-123-0-00-036-0
COMMERCIAL VACANT LAND
NORTH RIDGE

MELINDA R O'CONNOR
01-B-123-0-00-037-0
RESIDENTIAL
5386 NORTH RIDGE

JOSEPH M FIORITTO JR
& MIRANDA FIORITTO
01-B-123-0-00-038-0
RESIDENTIAL
5406 NORTH RIDGE



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 424+66.37 TO STA. 429+26.89

LAK-US-20-24.99
PART 2

30/98

629
697

REV. BY	DATE	DESCRIPTION

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB004.dgn Sheet 7/15/2022 3:01:37 PM troyer

PART OF LOT NO. 5
TRACT 4
MADISON TOWNSHIP



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

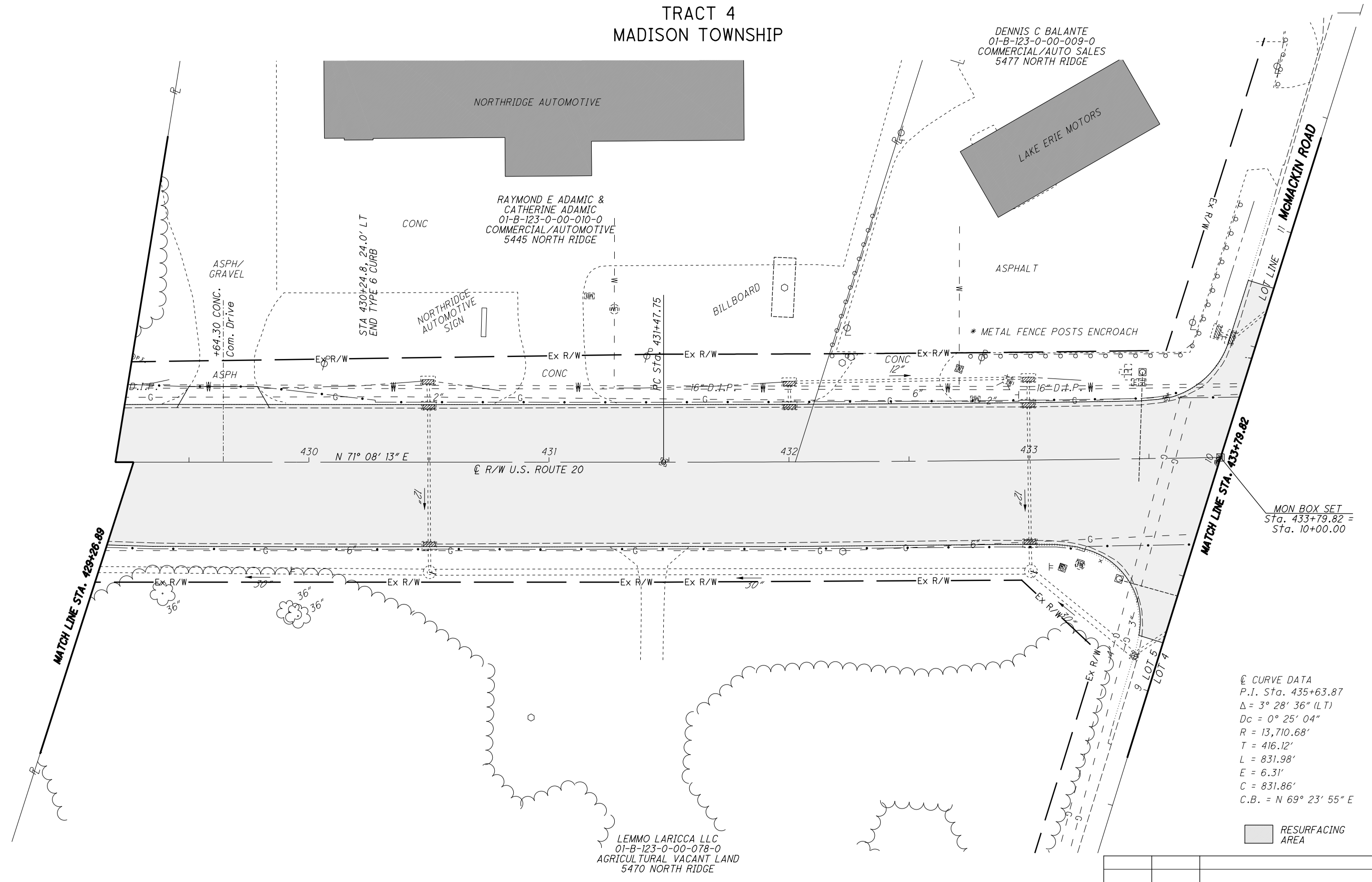
RIGHT OF WAY TOPO SHEET
STA. 429+26.89 TO STA. 433+79.82

LAK-US-20-24.99
PART 2

31/98

630
697

H:\2018\80743\0001\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT005.dgn Sheet 7/15/2022 3:01:39 PM troyer



DENNIS C BALANTE
01-B-123-0-00-009-0
COMMERCIAL/AUTO SALES
5477 NORTH RIDGE

NORTHDRIDGE AUTOMOTIVE

LAKE ERIE MOTORS

RAYMOND E ADAMIC &
CATHERINE ADAMIC
01-B-123-0-00-010-0
COMMERCIAL/AUTOMOTIVE
5445 NORTH RIDGE

LEMMO LARICCA LLC
01-B-123-0-00-078-0
AGRICULTURAL VACANT LAND
5470 NORTH RIDGE

☐ RESURFACING AREA

☐ CURVE DATA
P.I. Sta. 435+63.87
 $\Delta = 3^\circ 28' 36''$ (LT)
 $Dc = 0^\circ 25' 04''$
 $R = 13,710.68'$
 $T = 416.12'$
 $L = 831.98'$
 $E = 6.31'$
 $C = 831.86'$
 $C.B. = N 69^\circ 23' 55'' E$

REV. BY	DATE	DESCRIPTION

PART OF LOT NO. 5
TRACT 4
MADISON TOWNSHIP

RAYMOND E ADAMIC &
CATHERINE ADAMIC
01-B-123-0-00-010-0
COMMERCIAL/AUTOMOTIVE
5445 NORTH RIDGE

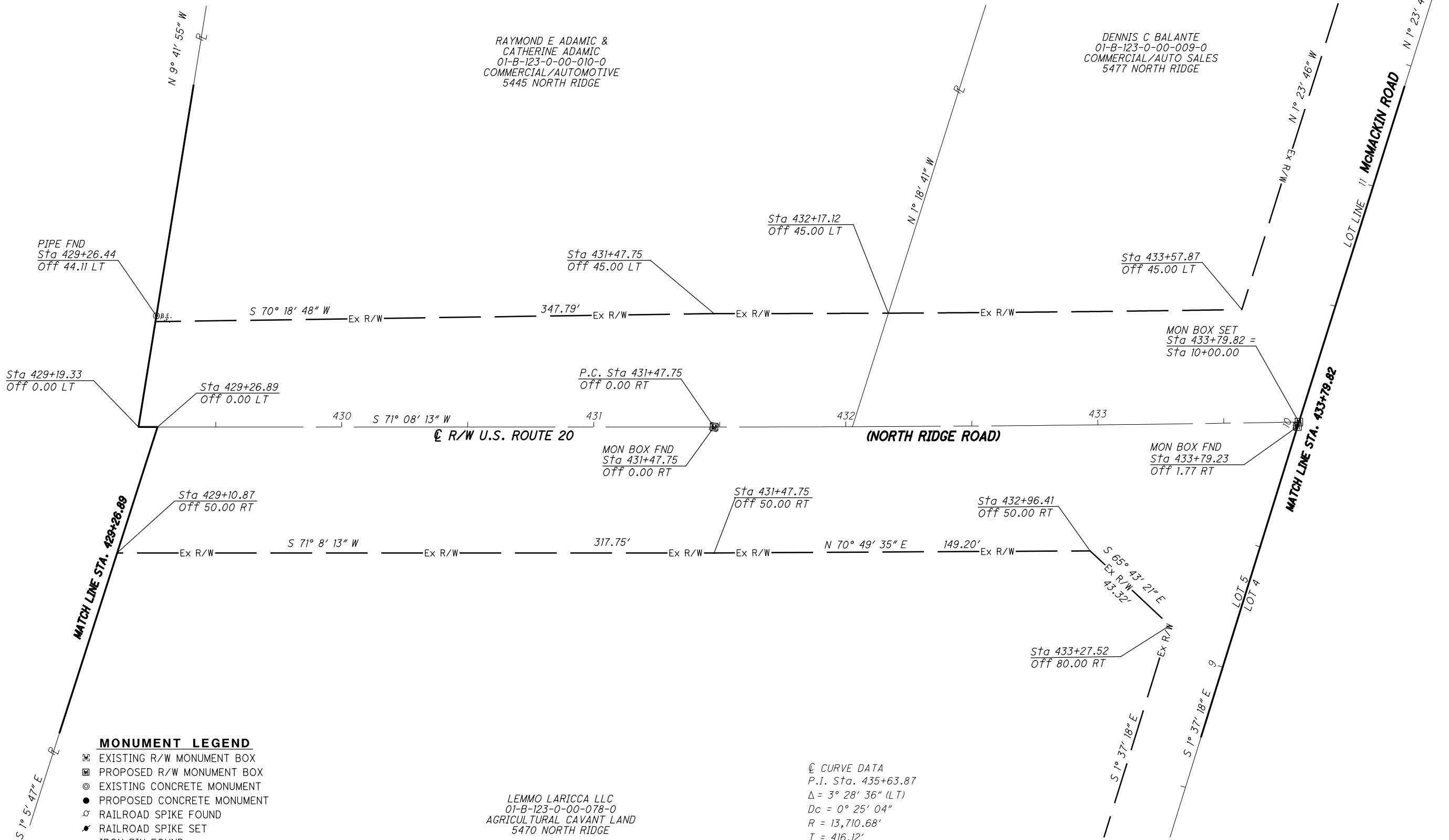
DENNIS C BALANTE
01-B-123-0-00-009-0
COMMERCIAL/AUTO SALES
5477 NORTH RIDGE

LEMMO LARICCA LLC
01-B-123-0-00-078-0
AGRICULTURAL CAVANT LAND
5470 NORTH RIDGE

☉ CURVE DATA
P.I. Sta. 435+63.87
 $\Delta = 3^\circ 28' 36''$ (LT)
 $Dc = 0^\circ 25' 04''$
 $R = 13,710.68'$
 $T = 416.12'$
 $L = 831.98'$
 $E = 6.31'$
 $C = 831.86'$
C.B. = N 69° 23' 55" E

MONUMENT LEGEND

- ☐ EXISTING R/W MONUMENT BOX
- ▣ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- I.P.F. IRON PIN FOUND
- I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- ⊙ P.K.F. P.K. NAIL FOUND
- P.K.S. P.K. NAIL SET



REV. BY	DATE	DESCRIPTION

N

10
HORIZONTAL
SCALE IN FEET

PID NO. **109270**

R/W DESIGNER PSL R/W REVIEWER TMM

RIGHT OF WAY BOUNDARY SHEET

STA. 429+26.89 TO STA. 433+79.82

LAK-US-20-24.99 PART 2

32 / 98

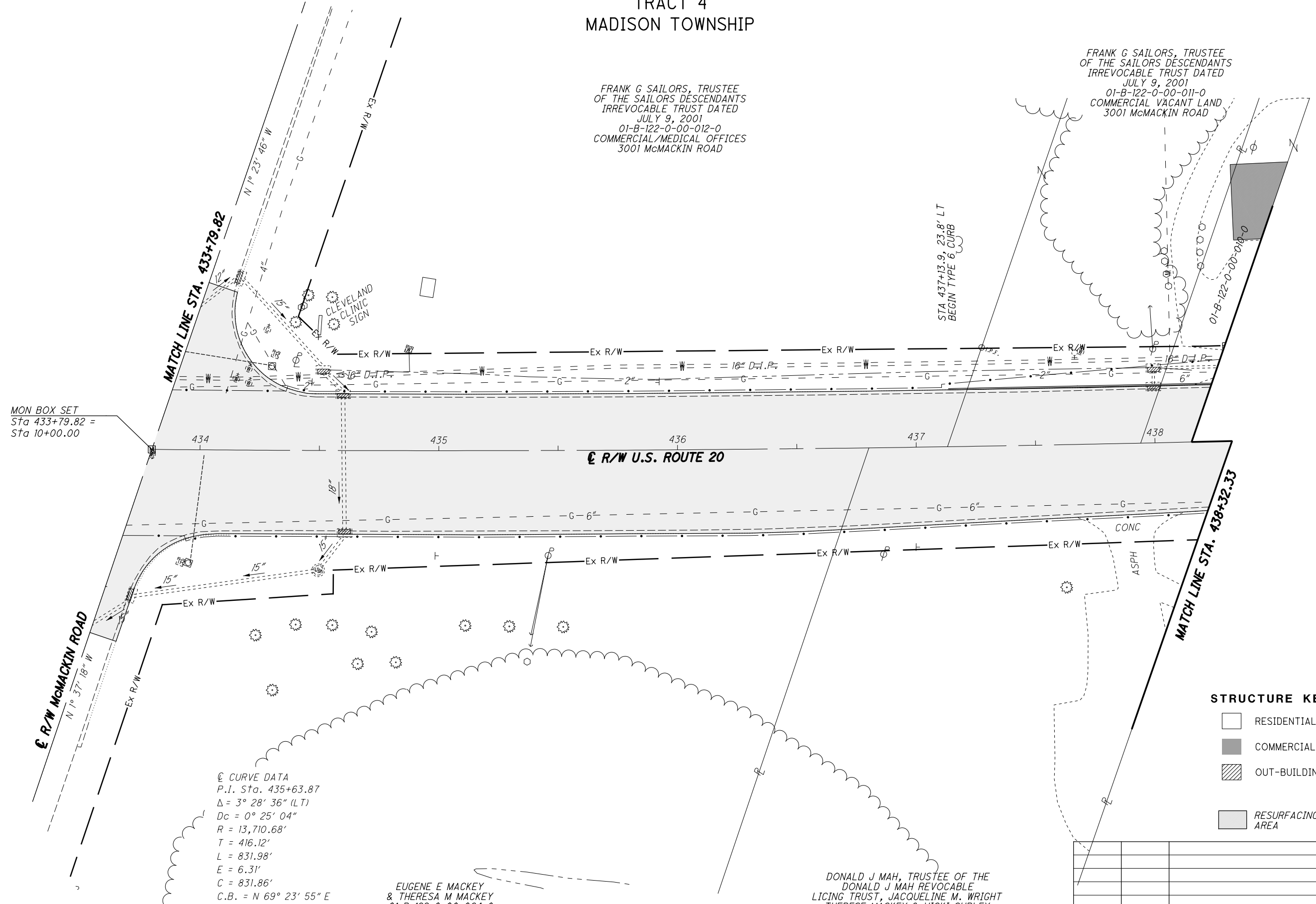
631
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB005.dgn Sheet 7/15/2022 3:01:40 PM Troyer

PART OF LOT NO. 4
TRACT 4
MADISON TOWNSHIP

FRANK G SAILORS, TRUSTEE
OF THE SAILORS DESCENDANTS
IRREVOCABLE TRUST DATED
JULY 9, 2001
01-B-122-0-00-012-0
COMMERCIAL/MEDICAL OFFICES
3001 McMACKIN ROAD

FRANK G SAILORS, TRUSTEE
OF THE SAILORS DESCENDANTS
IRREVOCABLE TRUST DATED
JULY 9, 2001
01-B-122-0-00-011-0
COMMERCIAL VACANT LAND
3001 McMACKIN ROAD



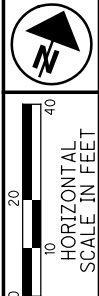
⊕ CURVE DATA
P.I. Sta. 435+63.87
 $\Delta = 3^\circ 28' 36''$ (LT)
 $D_c = 0^\circ 25' 04''$
 $R = 13,710.68'$
 $T = 416.12'$
 $L = 831.98'$
 $E = 6.31'$
 $C = 831.86'$
C.B. = $N 69^\circ 23' 55'' E$

EUGENE E MACKEY
& THERESA M MACKEY
01-B-122-0-00-064-0
RESIDENTIAL VACANT LAND
McMACKIN ROAD

DONALD J MAH, TRUSTEE OF THE
DONALD J MAH REVOCABLE
LICING TRUST, JACQUELINE M. WRIGHT
THERESE MACKEY & VICKI GURLEY
01-B-122-0-00-069-0
AGRICULTURAL VACANT LAND
NORTH RIDGE

- STRUCTURE KEY**
- RESIDENTIAL
 - COMMERCIAL
 - OUT-BUILDING
 - RESURFACING AREA

REV. BY	DATE	DESCRIPTION



R/W DESIGNER
PSL
R/W REVIEWER
TMM

PID NO.
109270

RIGHT OF WAY TOPO SHEET
STA. 433+79.82 TO STA. 438+32.33

LAK-US-20-24.99
PART 2

33/98

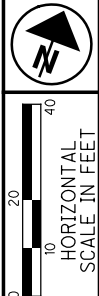
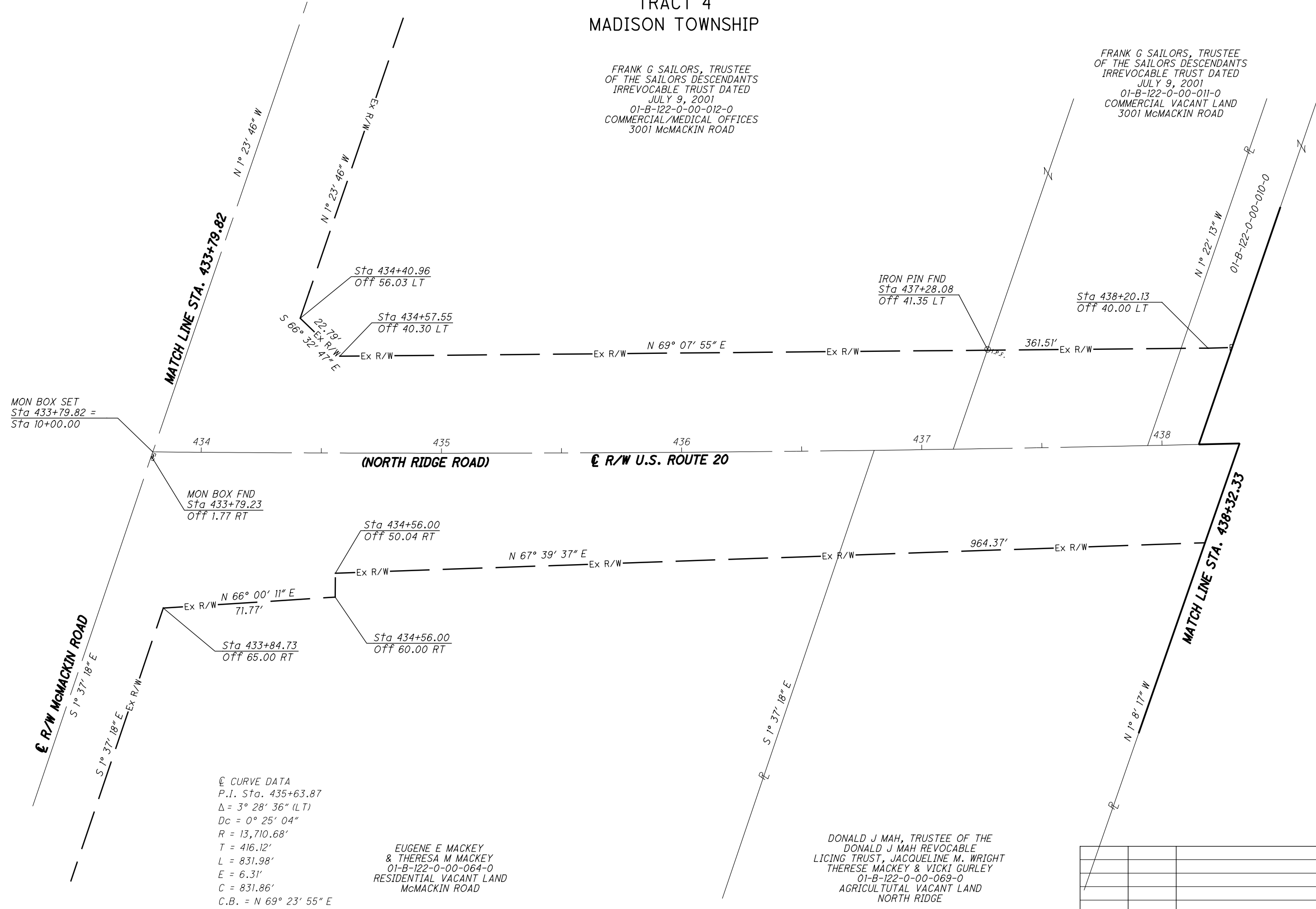
632
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT006.dgn Sheet 7/15/2022 3:01:41PM Troyer

PART OF LOT NO. 4
TRACT 4
MADISON TOWNSHIP

FRANK G SAILORS, TRUSTEE
OF THE SAILORS DESCENDANTS
IRREVOCABLE TRUST DATED
JULY 9, 2001
01-B-122-0-00-012-0
COMMERCIAL/MEDICAL OFFICES
3001 McMACKIN ROAD

FRANK G SAILORS, TRUSTEE
OF THE SAILORS DESCENDANTS
IRREVOCABLE TRUST DATED
JULY 9, 2001
01-B-122-0-00-011-0
COMMERCIAL VACANT LAND
3001 McMACKIN ROAD



PID NO. **109270**
R/W DESIGNER: PSL
R/W REVIEWER: TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 433+79.82 TO STA. 438+32.33

LAK-US-20-24.99
PART 2

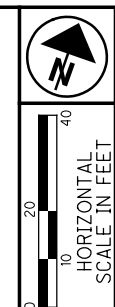
34/98

633
697

REV. BY	DATE	DESCRIPTION

H:\2018\180743\0D0T\Projec+Data\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB006.dgn Sheet 7/15/2022 3:01:42 PM troyer

PART OF LOT NO. 4
TRACT NO. 4
MADISON TOWNSHIP

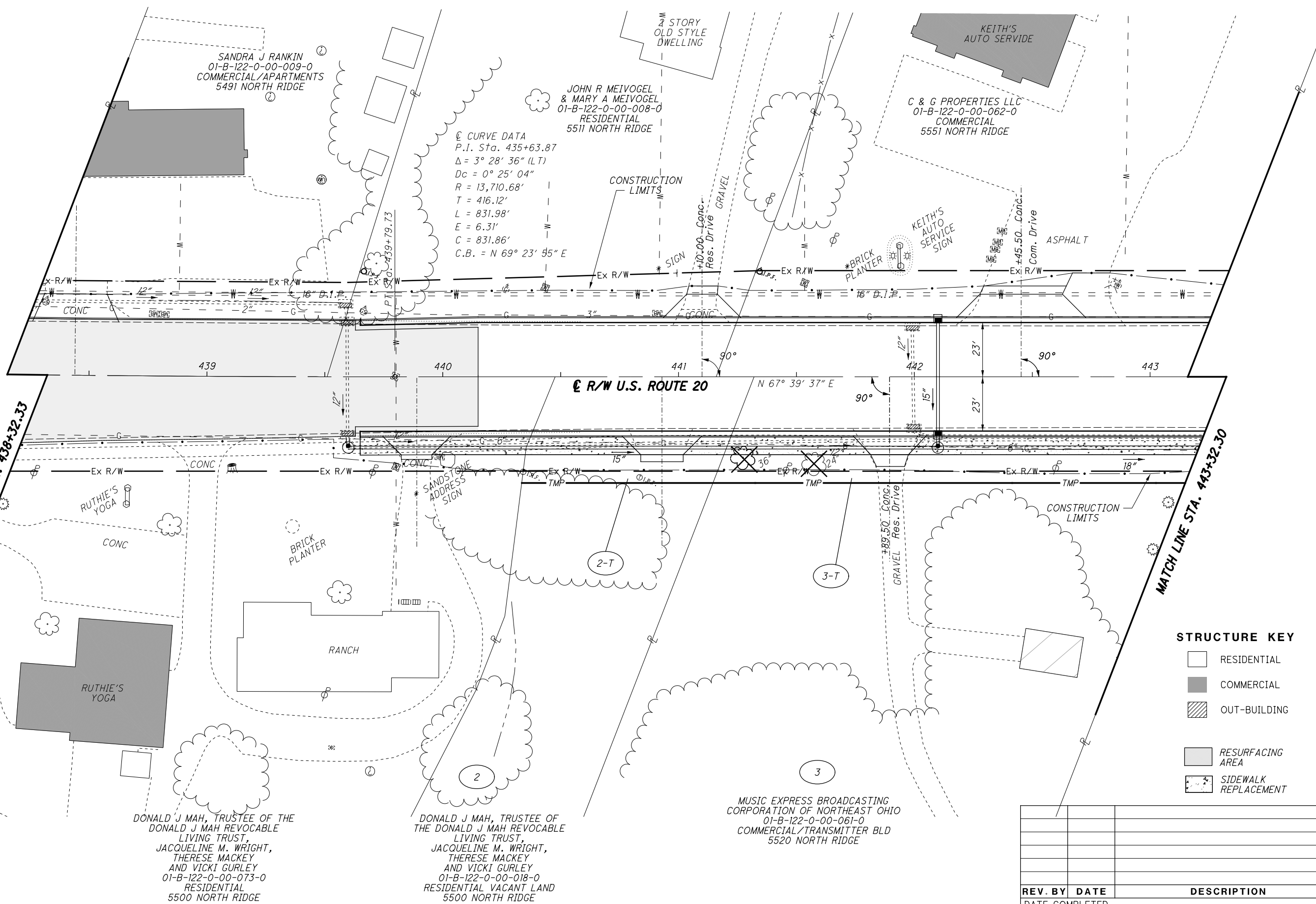


PID NO. **109270**
R/W DESIGNER PSL
R/W REVIEWER TMM

RIGHT OF WAY TOPO SHEET
STA. 438+32.33 TO STA. 443+32.30

LAK-US-20-24.99
PART 2

35/98
634
697



H:\2018\80743\0D01\Project\Data\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT007.dgn Sheet 7/15/2022 3:01:44 PM troyer

REV. BY	DATE	DESCRIPTION

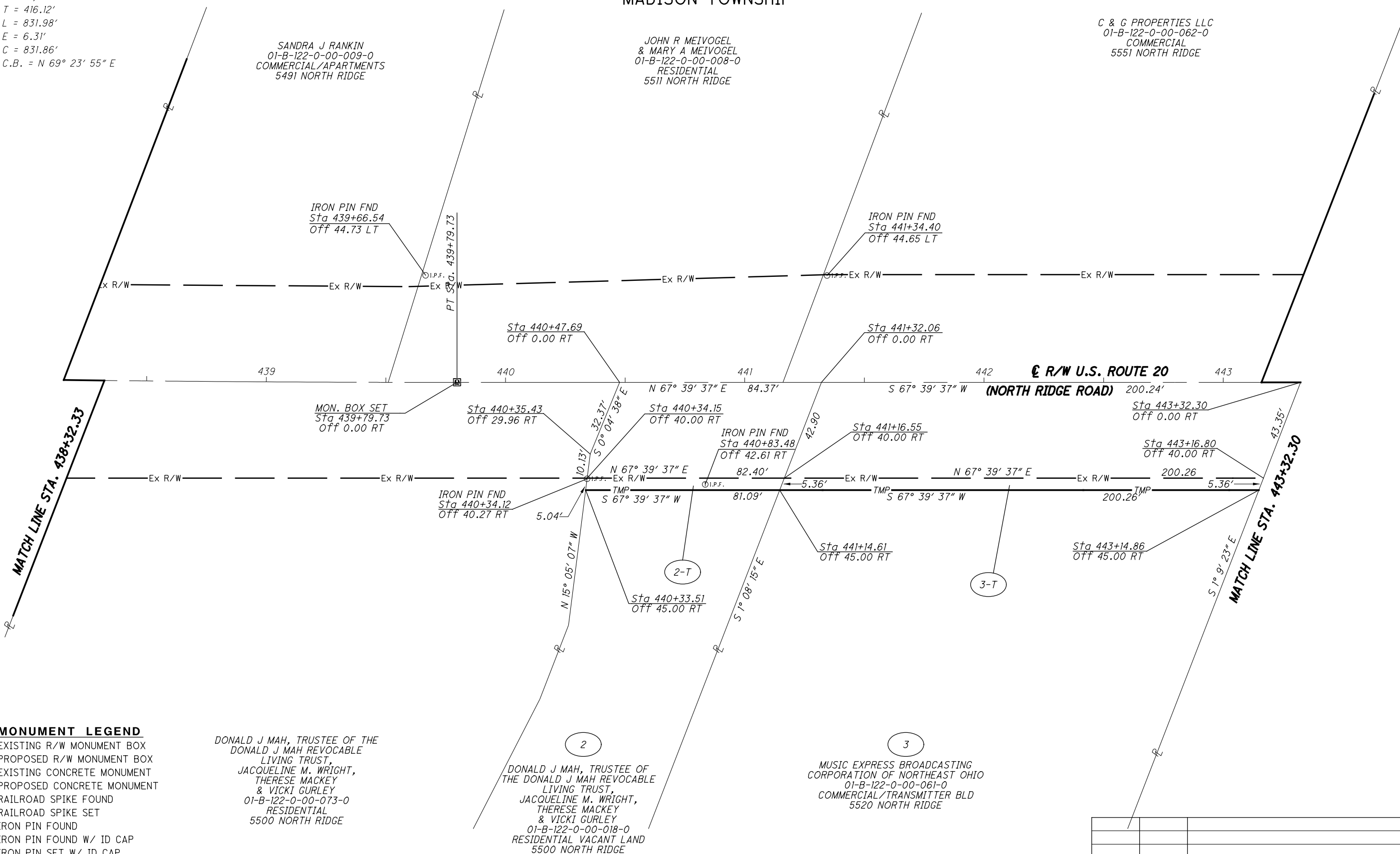
@ CURVE DATA
 P.I. Sta. 435+63.87
 $\Delta = 3^\circ 28' 36''$ (LT)
 $Dc = 0^\circ 25' 04''$
 $R = 13,710.68'$
 $T = 416.12'$
 $L = 831.98'$
 $E = 6.31'$
 $C = 831.86'$
 $C.B. = N 69^\circ 23' 55'' E$

PART OF LOT NO. 4
 TRACT NO. 4
 MADISON TOWNSHIP

C & G PROPERTIES LLC
 01-B-122-0-00-062-0
 COMMERCIAL
 5551 NORTH RIDGE

SANDRA J RANKIN
 01-B-122-0-00-009-0
 COMMERCIAL/APARTMENTS
 5491 NORTH RIDGE

JOHN R MEIVOGEL
 & MARY A MEIVOGEL
 01-B-122-0-00-008-0
 RESIDENTIAL
 5511 NORTH RIDGE



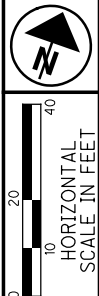
MONUMENT LEGEND

- ☒ EXISTING R/W MONUMENT BOX
- ☑ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- ⊙ I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- ⊙ P.K.F. P.K. NAIL FOUND
- P.K.S. P.K. NAIL SET

DONALD J MAH, TRUSTEE OF THE
 DONALD J MAH REVOCABLE
 LIVING TRUST,
 JACQUELINE M. WRIGHT,
 THERESE MACKEY
 & VICKI GURLEY
 01-B-122-0-00-073-0
 RESIDENTIAL
 5500 NORTH RIDGE

DONALD J MAH, TRUSTEE OF
 THE DONALD J MAH REVOCABLE
 LIVING TRUST,
 JACQUELINE M. WRIGHT,
 THERESE MACKEY
 & VICKI GURLEY
 01-B-122-0-00-018-0
 RESIDENTIAL VACANT LAND
 5500 NORTH RIDGE

MUSIC EXPRESS BROADCASTING
 CORPORATION OF NORTHEAST OHIO
 01-B-122-0-00-061-0
 COMMERCIAL/TRANSMITTER BLD
 5520 NORTH RIDGE



PID NO.
109270

R/W DESIGNER
 PSL
 R/W REVIEWER
 TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 438+32.33 TO STA. 443+32.30

LAK-US-20-24.99
PART 2

36/98

635
 697

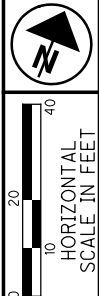
REV. BY	DATE	DESCRIPTION

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB007.dgn Sheet 7/15/2022 3:01:45 PM troyer

PART OF LOT NO. 4
TRACT NO. 4
MADISON TOWNSHIP

C & G PROPERTIES LLC
01-B-122-0-00-007-0
COMMERCIAL VACANT LAND
NORTH RIDGE

EDWIN IRVIN &
MARTHA IRVIN
01-B-122-0-00-074-0
COMMERCIAL VACANT LAND
NORTH RIDGE



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

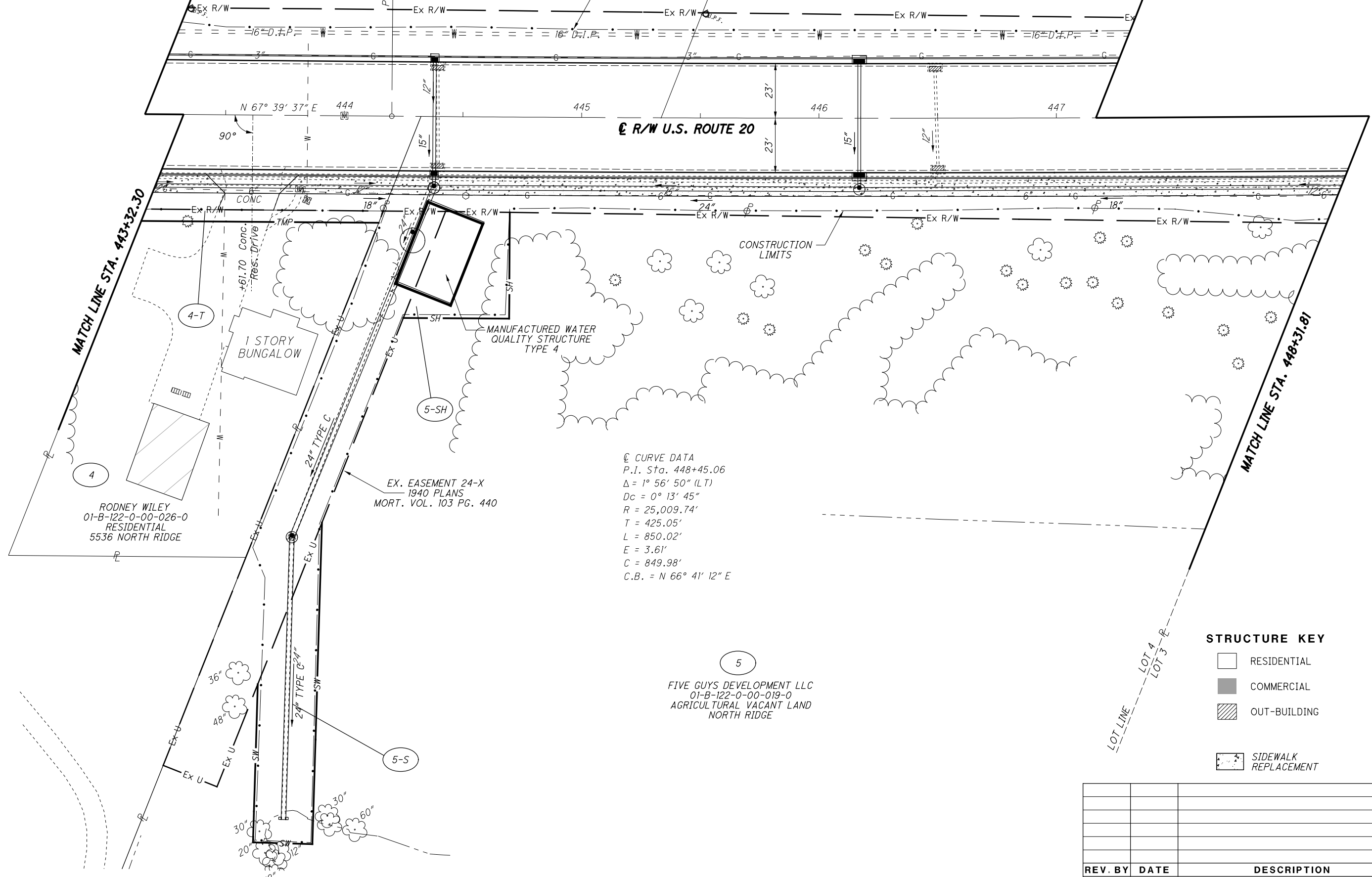
RIGHT OF WAY TOPO SHEET
STA. 443+32.30 TO STA. 448+31.81

LAK-US-20-24.99
PART 2

37/98

636
697

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RTO08.dgn Sheet 7/15/2022 3:01:47 PM troyer



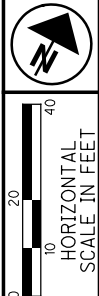
Ⓢ CURVE DATA
P.I. Sta. 448+45.06
 $\Delta = 1^\circ 56' 50''$ (LT)
 $Dc = 0^\circ 13' 45''$
 $R = 25,009.74'$
 $T = 425.05'$
 $L = 850.02'$
 $E = 3.61'$
 $C = 849.98'$
 $C.B. = N 66^\circ 41' 12'' E$

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING
- SIDEWALK REPLACEMENT

REV. BY	DATE	DESCRIPTION

PART OF LOT NO. 3
TRACT NO. 4
MADISON TOWNSHIP



PID NO. **109270**
R/W DESIGNER PSL
R/W REVIEWER TMM

RIGHT OF WAY TOPO SHEET
STA. 452+47.50 TO STA. 457+94.65

LAK-US-20-24.99
PART 2

41/98

640
697

THOMAS A LASKO &
NANCY M LASKO
01-B-122-0-00-004-0
AGRICULTURAL/FARM
5673 NORTH RIDGE

8

8-T

*KRISHACK
INSURANCE
AGENCY SIGN

2 STORY
OLD STYLE
DWELLING

1 STORY
RANCH
#5676

AHMAD DAQAH &
AHMAD DAGHLAS
01-B-122-0-00-049-0
MIXED COMMERCIAL/RESIDENTIAL
5662 NORTH RIDGE

7

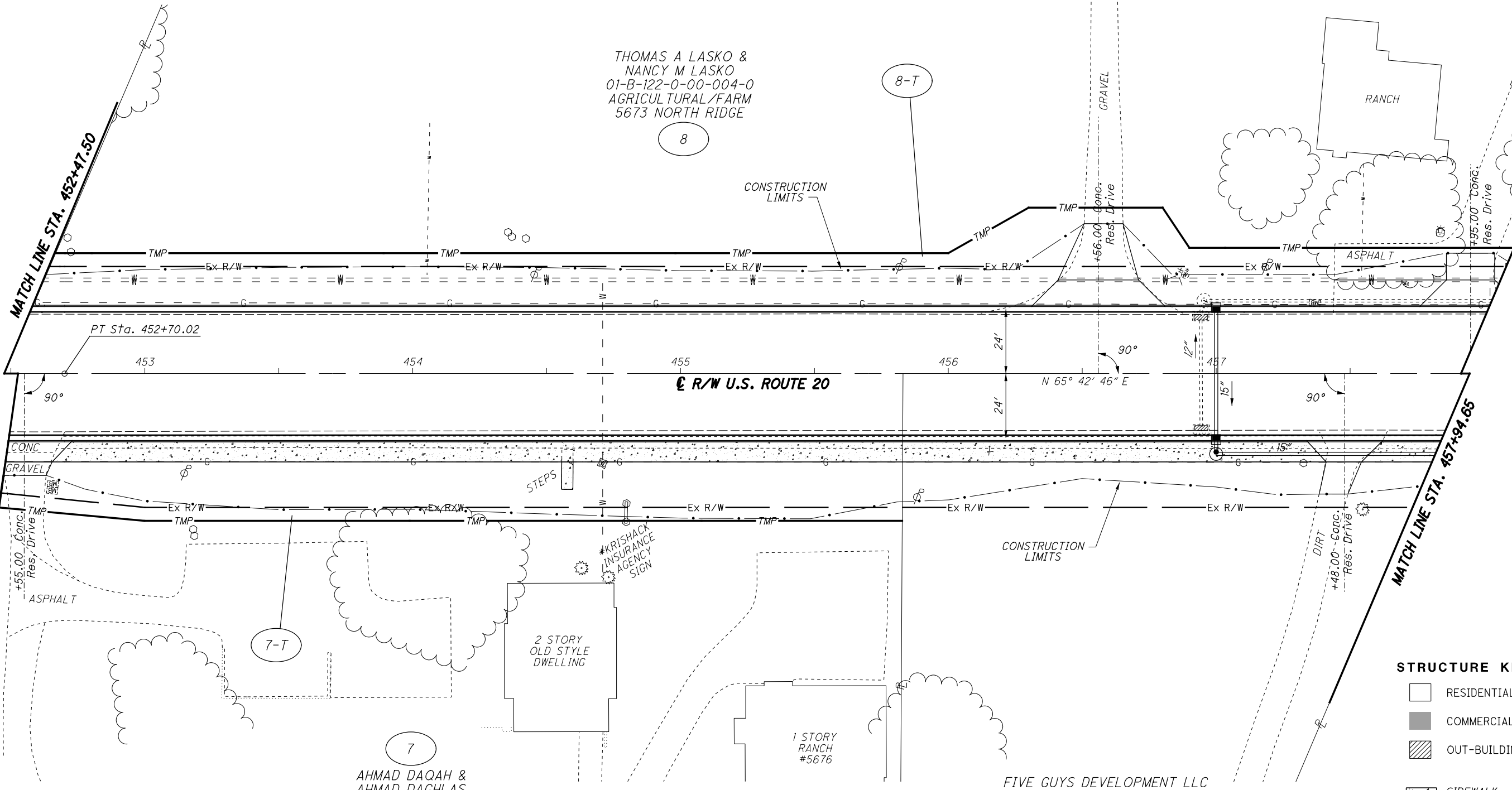
7-T

FIVE GUYS DEVELOPMENT LLC
01-B-122-0-00-020-0
AGRICULTURAL VACANT LAND
NORTH RIDGE

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING
- SIDEWALK REPLACEMENT

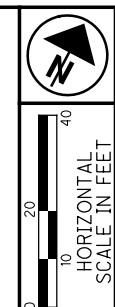
REV. BY	DATE	DESCRIPTION



H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT010.dgn Sheet 7/15/2022 3:01:52 PM troyer

@ CURVE DATA
 P.I. Sta. 448+45.06
 $\Delta = 1^\circ 56' 50''$ (LT)
 $\Delta c = 0^\circ 13' 45''$
 $R = 25,009.74'$
 $T = 425.05'$
 $L = 850.02'$
 $E = 3.61'$
 $C = 849.98'$
 $C.B. = N 66^\circ 41' 12'' E$

PART OF LOT NO. 3
 TRACT NO. 4
 MADISON TOWNSHIP



PID NO.
109270

R/W DESIGNER PSL
 R/W REVIEWER TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 452+47.50 TO STA. 457+94.65

LAK-US-20-24.99
PART 2

42/98

641
 697

THOMAS A LASKO &
 NANCY M LASKO
 01-B-122-0-00-004-0
 AGRICULTURAL/FARM
 5673 NORTH RIDGE

8

8-T

AHMAD DAQAH &
 AHMAD DAGHLAS
 01-B-122-0-00-049-0
 MIXED COMMERCIAL/RESIDENTIAL
 5662 NORTH RIDGE

7-T

7

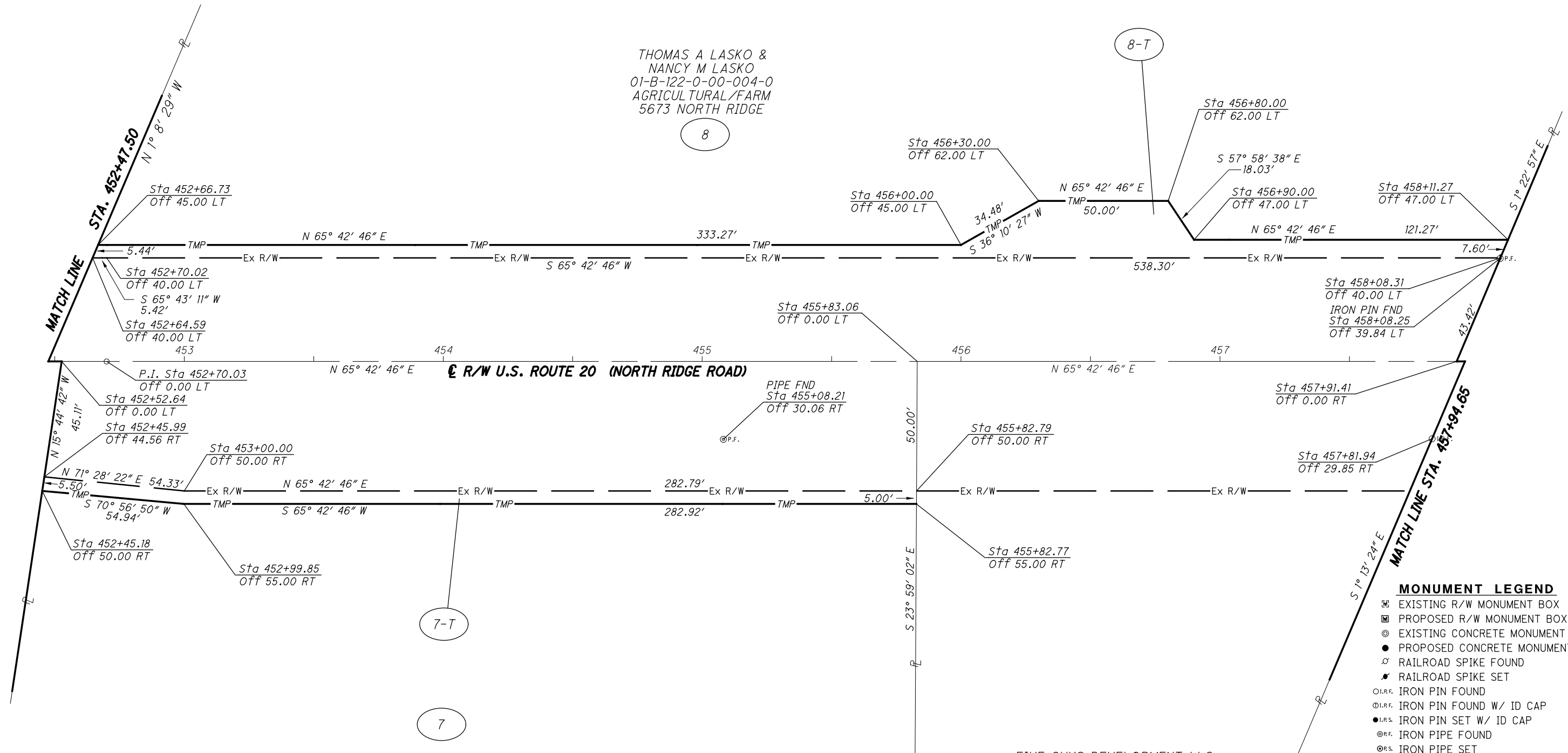
FIVE GUYS DEVELOPMENT LLC
 01-B-122-0-00-020-0
 AGRICULTURAL VACANT LAND
 NORTH RIDGE

MONUMENT LEGEND

- EXISTING R/W MONUMENT BOX
- PROPOSED R/W MONUMENT BOX
- EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- RAILROAD SPIKE FOUND
- RAILROAD SPIKE SET
- IRON PIN FOUND
- IRON PIN FOUND W/ ID CAP
- IRON PIN SET W/ ID CAP
- IRON PIPE FOUND
- IRON PIPE SET
- P.K. NAIL FOUND
- P.K. NAIL SET

REV. BY	DATE	DESCRIPTION

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB010.dgn Sheet 7/15/2022 3:01:53 PM troyer

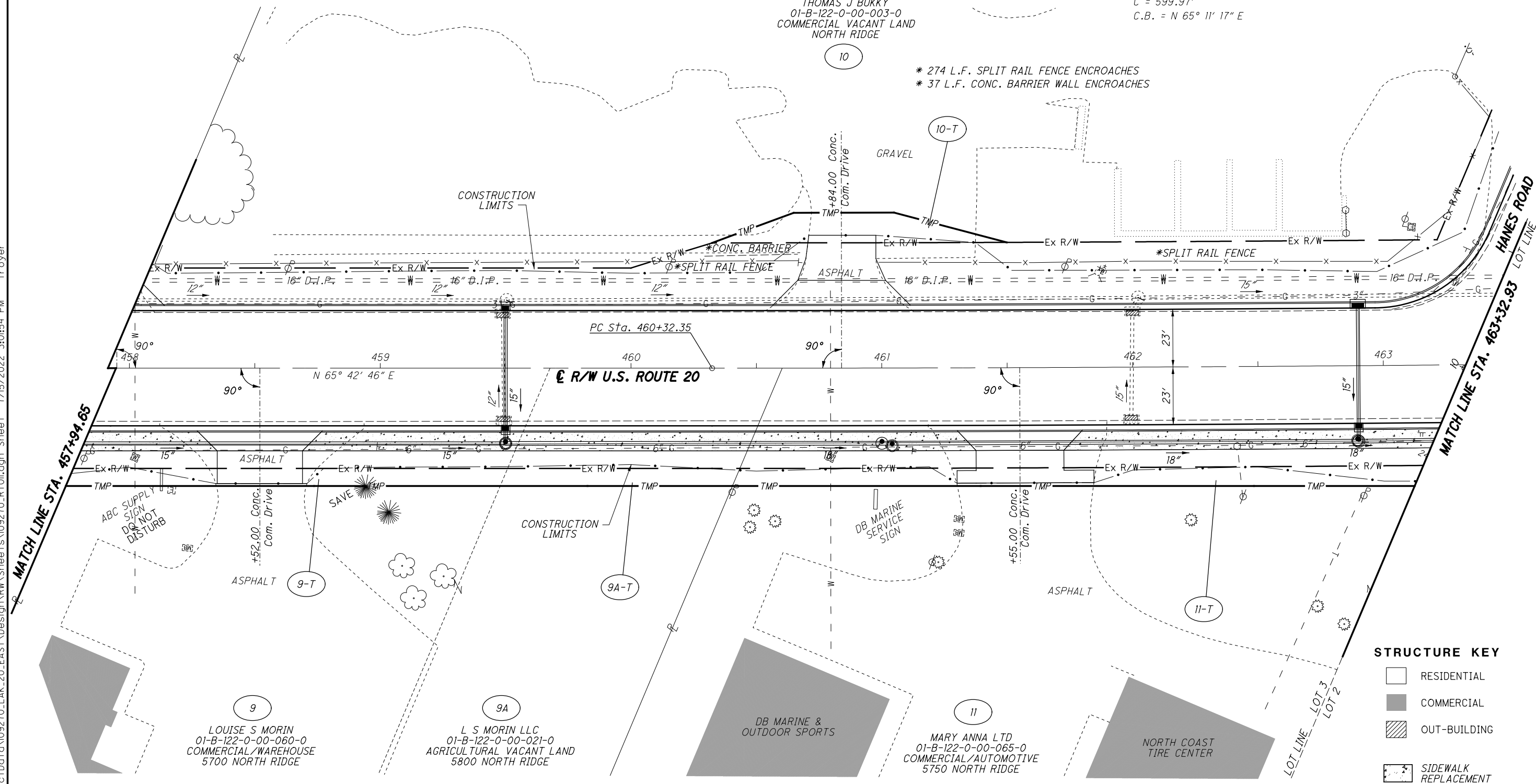


PART OF LOT NO. 3
TRACT NO. 4
MADISON TOWNSHIP

⊕ CURVE DATA
P.I. Sta. 463+32.35
Δ = 1° 02' 58" (LT)
Dc = 0° 10' 30"
R = 32,753.10'
T = 300.00'
L = 599.98'
E = 1.37'
C = 599.97'
C.B. = N 65° 11' 17" E

THOMAS J BUKKY
01-B-122-0-00-003-0
COMMERCIAL VACANT LAND
NORTH RIDGE

* 274 L.F. SPLIT RAIL FENCE ENCROACHES
* 37 L.F. CONC. BARRIER WALL ENCROACHES



STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING
- SIDEWALK REPLACEMENT

9
LOUISE S MORIN
01-B-122-0-00-060-0
COMMERCIAL/WAREHOUSE
5700 NORTH RIDGE

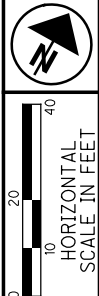
9A
L S MORIN LLC
01-B-122-0-00-021-0
AGRICULTURAL VACANT LAND
5800 NORTH RIDGE

DB MARINE &
OUTDOOR SPORTS

11
MARY ANNA LTD
01-B-122-0-00-065-0
COMMERCIAL/AUTOMOTIVE
5750 NORTH RIDGE

NORTH COAST
TIRE CENTER

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RTOil.dgn Sheet 7/15/2022 3:01:54 PM Troyer



PID NO.
PSL
TMM
109270

RIGHT OF WAY TOPO SHEET
STA. 457+94.65 TO STA. 463+32.93

LAK-US-20-24.99
PART 2

43/98

642
697

REV. BY	DATE	DESCRIPTION



PID NO. **109270**

R/W DESIGNER PSL
R/W REVIEWER TMM

RIGHT OF WAY TOPO SHEET
STA. 463+32.93 TO STA. 468+00.00

LAK-US-20-24.99
PART 2

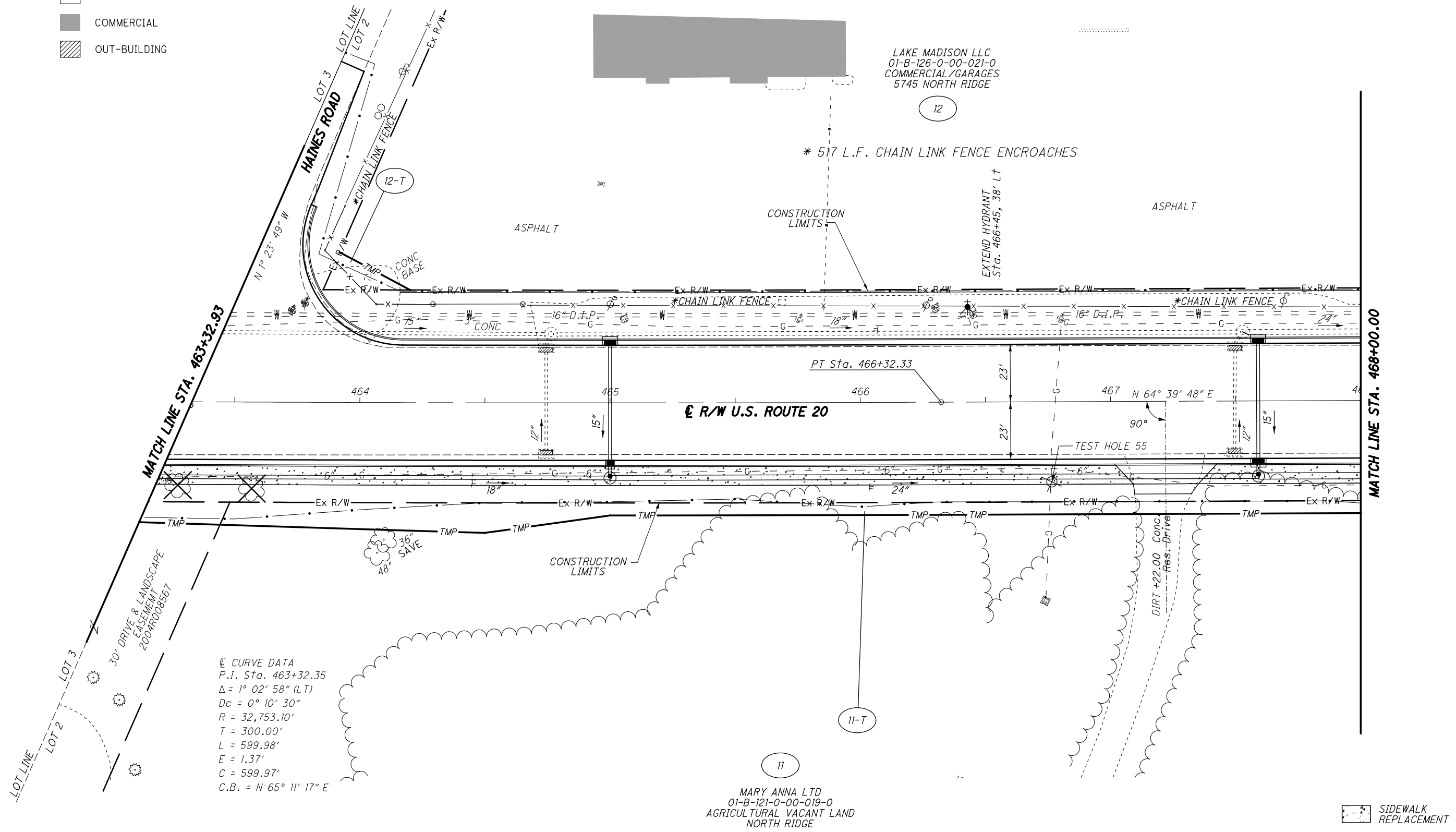
45/98

644
697

PART OF LOT NO. 2 TRACT NO. 4 MADISON TOWNSHIP

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING



REV. BY	DATE	DESCRIPTION

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT012.dgn Sheet 7/15/2022 3:04:57 PM Troyer

MONUMENT LEGEND

- ☒ EXISTING R/W MONUMENT BOX
- ☒ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- ⊙ P.K.F. P.K. NAIL FOUND
- ⊙ P.K.S. P.K. NAIL SET

PART OF LOT NO. 2
TRACT NO. 4
MADISON TOWNSHIP

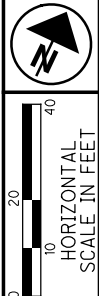
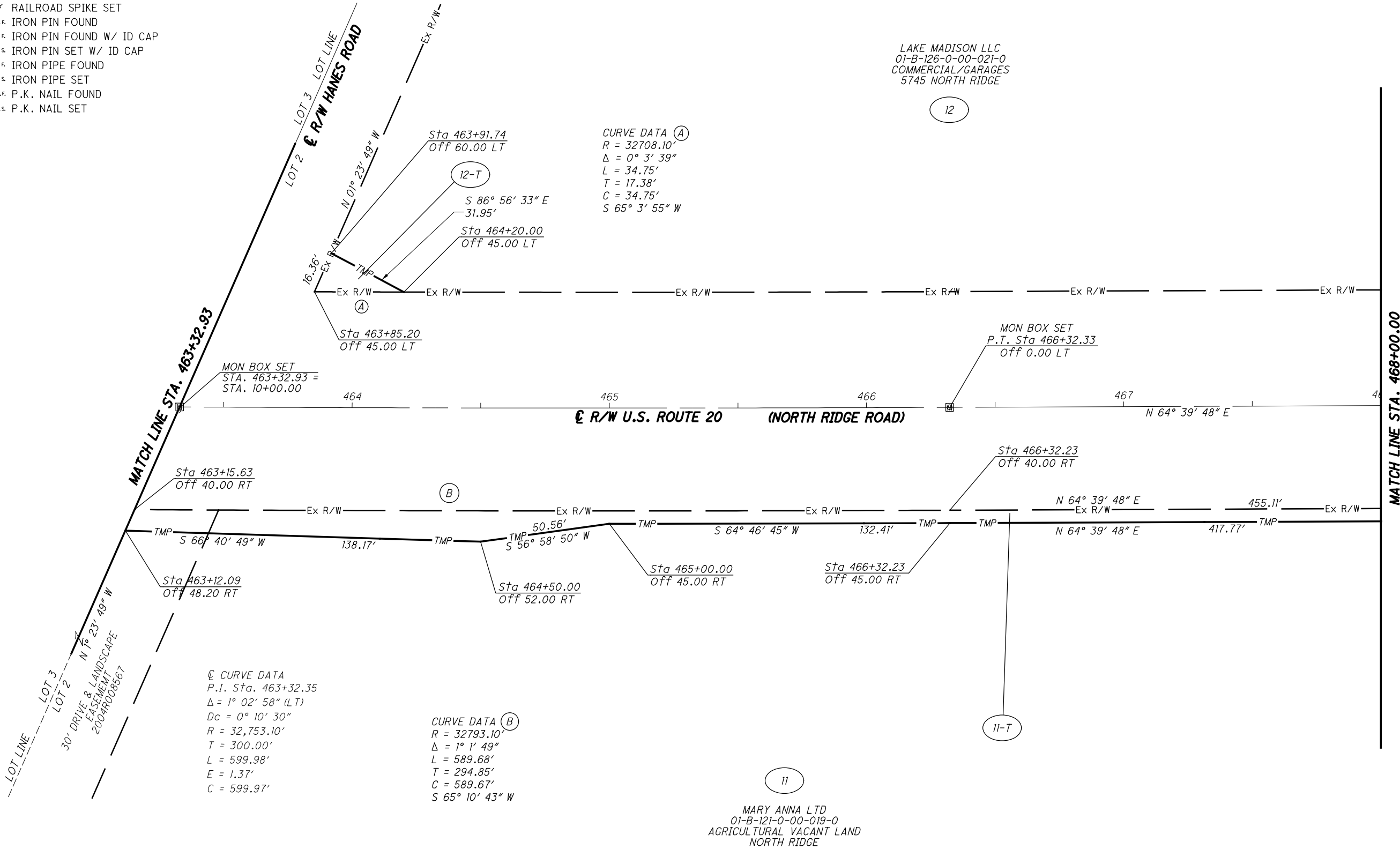
LAKE MADISON LLC
01-B-126-0-00-021-0
COMMERCIAL/GARAGES
5745 NORTH RIDGE

CURVE DATA (A)
R = 32708.10'
Δ = 0° 3' 39"
L = 34.75'
T = 17.38'
C = 34.75'
S 65° 3' 55" W

⊙ CURVE DATA
P.I. Sta. 463+32.35
Δ = 1° 02' 58" (LT)
Dc = 0° 10' 30"
R = 32,753.10'
T = 300.00'
L = 599.98'
E = 1.37'
C = 599.97'

CURVE DATA (B)
R = 32793.10'
Δ = 1° 1' 49"
L = 589.68'
T = 294.85'
C = 589.67'
S 65° 10' 43" W

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB012.dgn Sheet 7/15/2022 3:01:58 PM troyer



PID NO. **109270**

R/W DESIGNER: PSL
R/W REVIEWER: TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 463+32.93 TO STA. 468+00.00

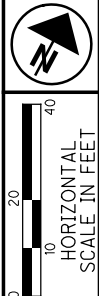
LAK-US-20-24.99
PART 2

46/98

645
697

REV. BY	DATE	DESCRIPTION

PART OF LOT NO. 2
TRACT NO. 4
MADISON TOWNSHIP

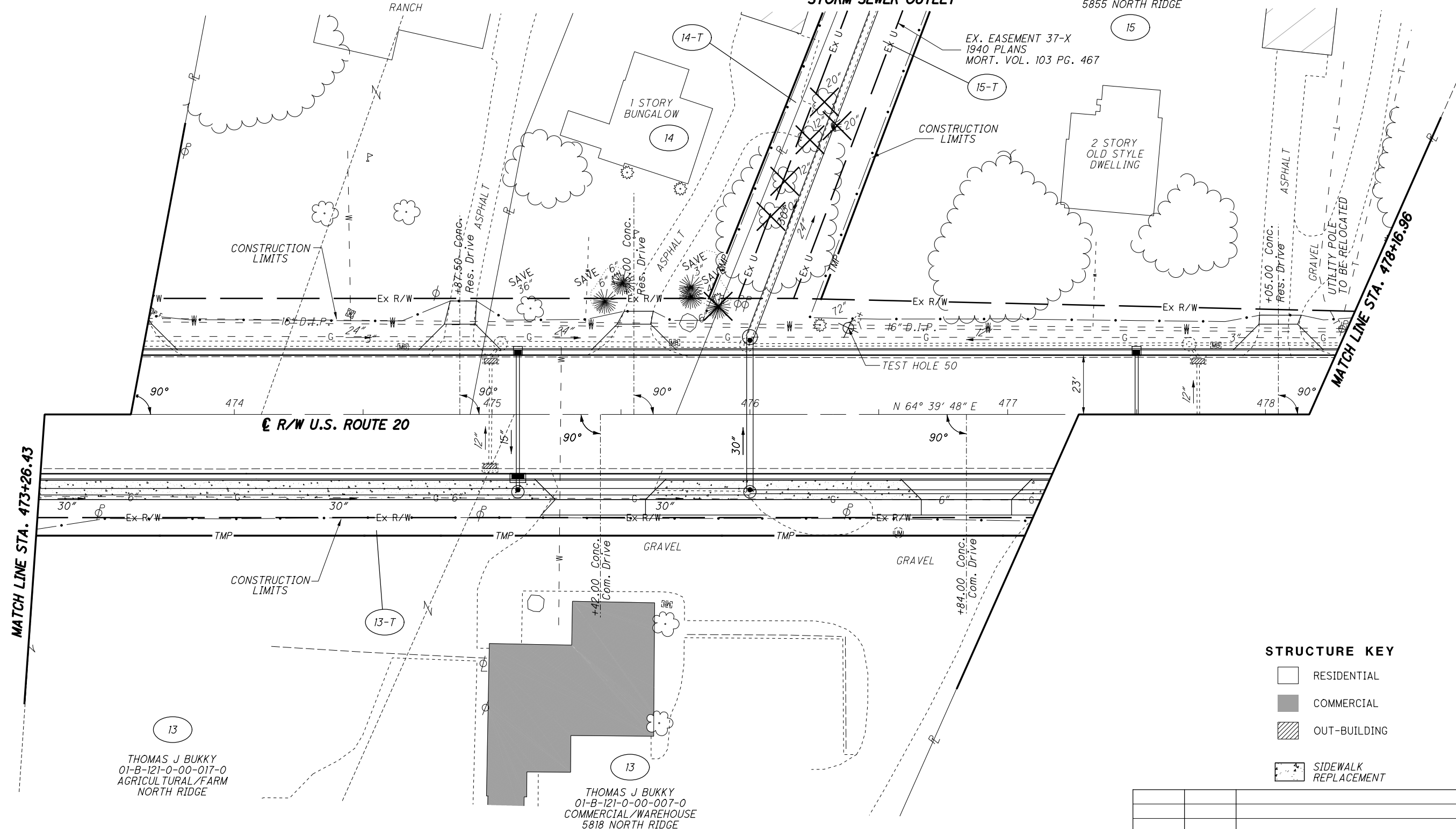


ROBERT GRUBBS &
LEANNE R GRUBBS
01-B-126-0-00-018-0
RESIDENTIAL
5829 NORTH RIDGE

ROBERT GRUBBS &
LEANNE R GRUBBS
VACANT RESIDENTIAL LAND
01-B-126-0-00-016-0
NORTH RIDGE

KELSEY J REYNOLDS
01-B-126-0-00-042-0
MIXED RESIDENTIAL/COMMERCIAL
5843 NORTH RIDGE

SM BECKER PROPERTIES LLC
01-B-126-0-00-013-0
RESIDENTIAL
5855 NORTH RIDGE



STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING
- SIDEWALK REPLACEMENT

13
THOMAS J BUKKY
01-B-121-0-00-017-0
AGRICULTURAL/FARM
NORTH RIDGE

13
THOMAS J BUKKY
01-B-121-0-00-007-0
COMMERCIAL/WAREHOUSE
5818 NORTH RIDGE

PID NO.
109270

R/W DESIGNER
PSL

R/W REVIEWER
TMM

RIGHT OF WAY TOPO SHEET
STA. 473+26.43 TO STA. 478+16.96

LAK-US-20-24.99
PART 2

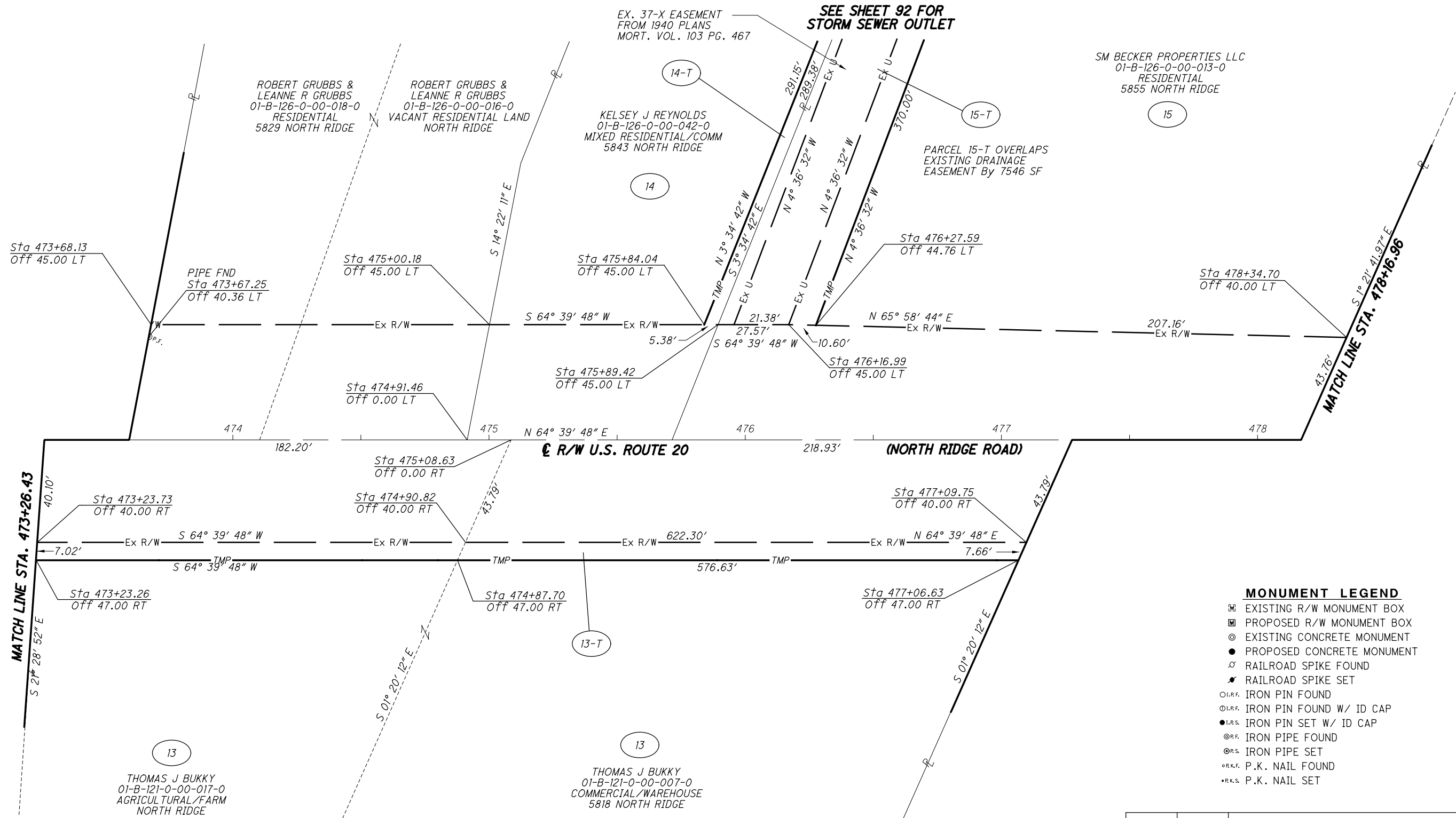
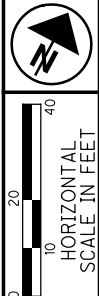
49/98

648
697

REV. BY	DATE	DESCRIPTION

H:\2018\180743\001\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT014.dgn Sheet 7/15/2022 3:02:02 PM troyer

PART OF LOT NO. 2
TRACT NO. 4
MADISON TOWNSHIP



MONUMENT LEGEND

- ☐ EXISTING R/W MONUMENT BOX
- ▣ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⊗ RAILROAD SPIKE FOUND
- ⊛ RAILROAD SPIKE SET
- I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.S. IRON PIPE FOUND
- ⊛ I.P.S. IRON PIPE SET
- ⊙ P.K.F. P.K. NAIL FOUND
- ⊛ P.K.S. P.K. NAIL SET

REV. BY	DATE	DESCRIPTION

PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 473+26.43 TO STA. 478+16.96

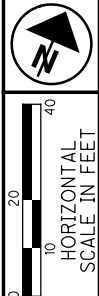
LAK-US-20-24.99
PART 2

50/98

649
697

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB014.dgn Sheet 7/15/2022 3:02:03 PM troyer

PART OF LOT NO. 12, 1 & 2
TRACT NO. 4
MADISON TOWNSHIP



PID NO. **109270**
R/W DESIGNER PSL
R/W REVIEWER TMM

RIGHT OF WAY TOPO SHEET
STA. 478+16.96 TO STA. 482+00.00

LAK-US-20-24.99
PART 2

51/98

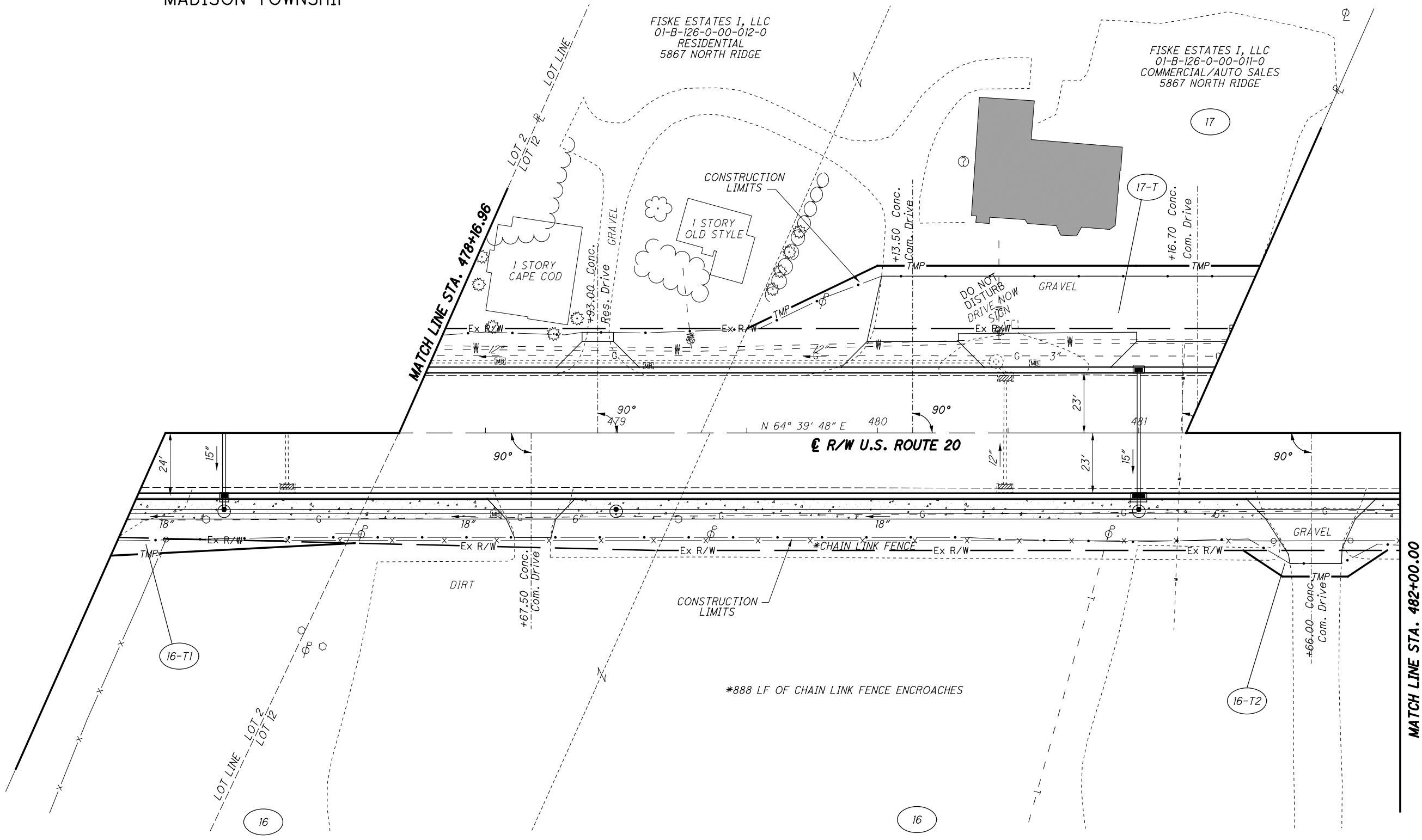
650
697

FISKE ESTATES I, LLC
01-B-126-0-00-012-0
RESIDENTIAL
5867 NORTH RIDGE

FISKE ESTATES I, LLC
01-B-126-0-00-011-0
COMMERCIAL/AUTO SALES
5867 NORTH RIDGE

A & L NURSERIES, LLC
01-B-121-0-00-005-0
RESIDENTIAL
5830 NORTH RIDGE

A & L NURSERIES, LLC
01-B-121-0-00-003-0
AGRICULTURAL/FARM
5946 NORTH RIDGE



- STRUCTURE KEY**
- RESIDENTIAL
 - COMMERCIAL
 - OUT-BUILDING
 - SIDEWALK REPLACEMENT

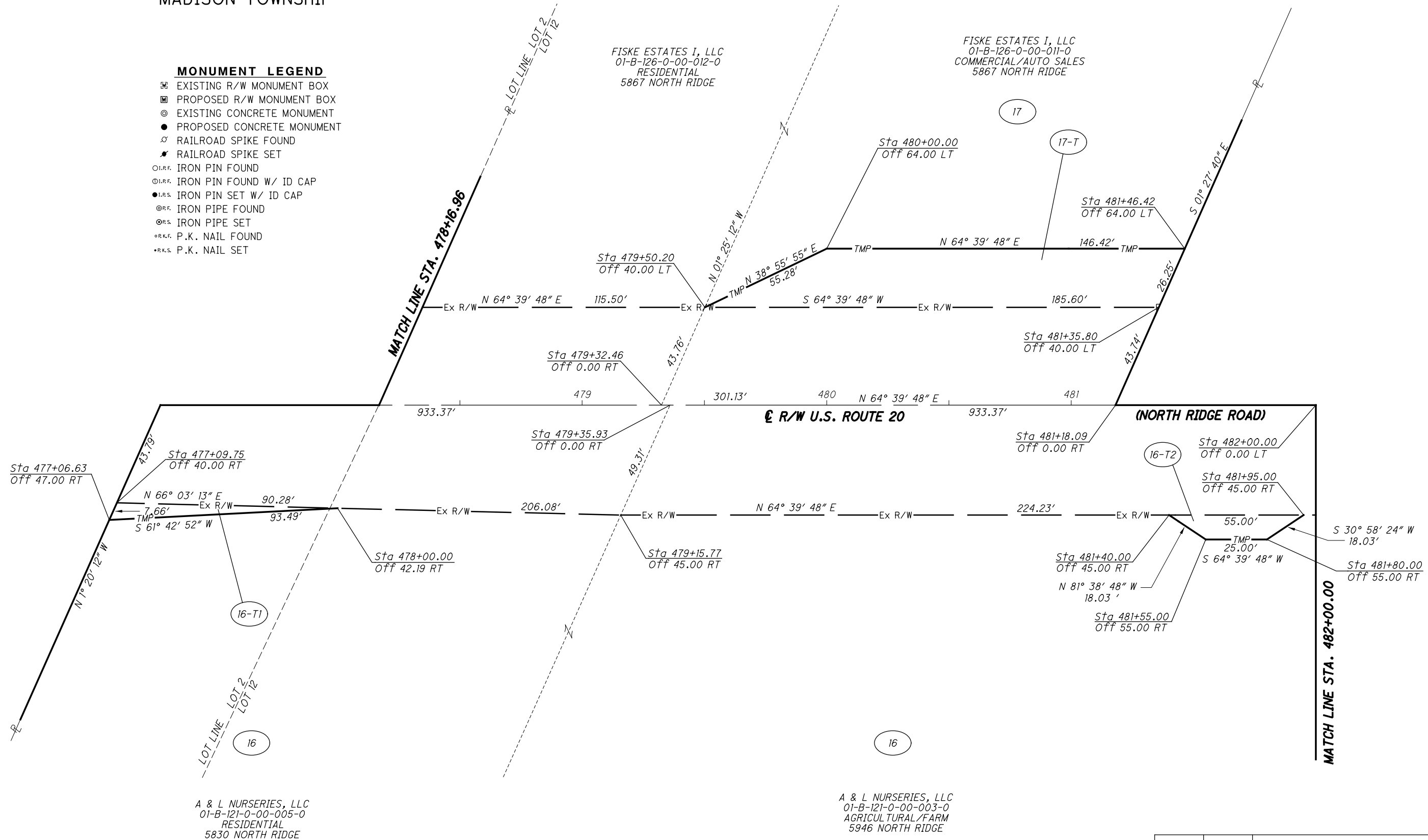
REV. BY	DATE	DESCRIPTION

H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT015.dgn Sheet 7/15/2022 3:02:04 PM troyer

PART OF LOT NO. 12, 1 & 2
TRACT NO. 4
MADISON TOWNSHIP

MONUMENT LEGEND

- ☒ EXISTING R/W MONUMENT BOX
- ☒ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- I.P.F. IRON PIN FOUND
- I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- I.P.F. IRON PIPE FOUND
- I.P.S. IRON PIPE SET
- ⊙ P.K.F. P.K. NAIL FOUND
- ⊙ P.K.S. P.K. NAIL SET



FISKE ESTATES I, LLC
01-B-126-0-00-012-0
RESIDENTIAL
5867 NORTH RIDGE

FISKE ESTATES I, LLC
01-B-126-0-00-011-0
COMMERCIAL/AUTO SALES
5867 NORTH RIDGE

A & L NURSERIES, LLC
01-B-121-0-00-005-0
RESIDENTIAL
5830 NORTH RIDGE

A & L NURSERIES, LLC
01-B-121-0-00-003-0
AGRICULTURAL/FARM
5946 NORTH RIDGE



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 478+16.96 TO STA. 482+00.00

LAK-US-20-24.99
PART 2

52/98

651
697

REV. BY	DATE	DESCRIPTION

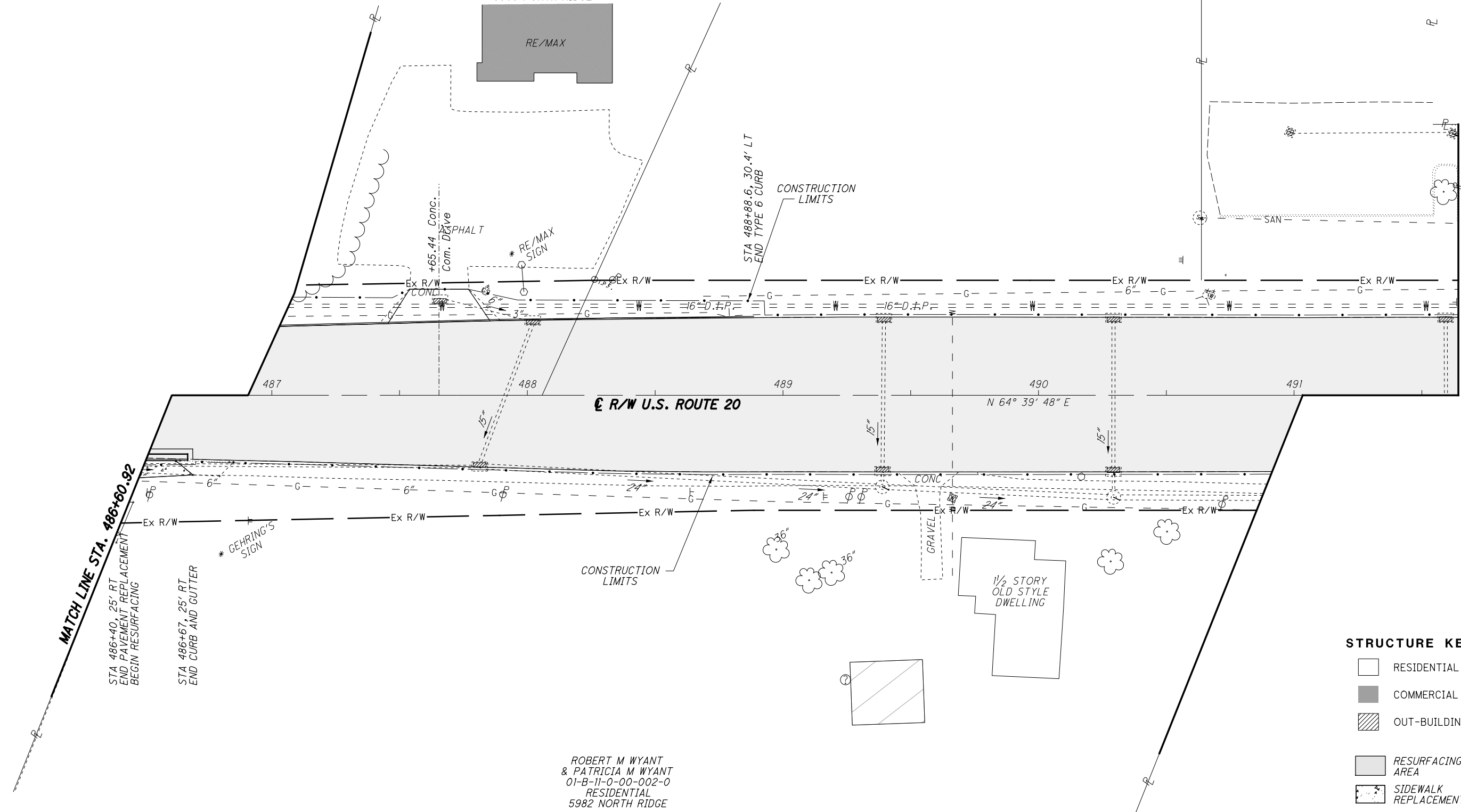
H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB015.dgn Sheet 7/15/2022 3:02:05 PM troyer

PART OF LOT NO. 12 & 1
TRACT NO. 4
MADISON TOWNSHIP

N R PROPERTY HOLDINGS LLC
SHAUN SILL
& ELIZABETH SILL
01-B-126-0-00-006-0
COMMERCIAL
5965 NORTH RIDGE

GREEN MADISON LLC
01-B-126-0-00-005-0
RESIDENTIAL/VACANT LAND
GREEN ROAD

GRHH CSOK LLC ET AL
01-B-126-0-00-059-0
RESIDENTIAL LAND
NORTH RIDGE

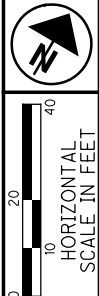


ROBERT M WYANT
& PATRICIA M WYANT
01-B-11-0-00-002-0
RESIDENTIAL
5982 NORTH RIDGE

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING
- RESURFACING AREA
- SIDEWALK REPLACEMENT

REV. BY	DATE	DESCRIPTION



PID NO. 109270
R/W DESIGNER PSL
R/W REVIEWER TMM

RIGHT OF WAY TOPO SHEET
STA. 486+60.92 TO STA. 491+64.27

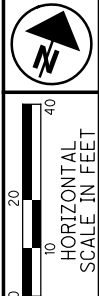
LAK-US-20-24.99
PART 2

55/98

654
697

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT017.dgn Sheet 7/15/2022 3:02:10 PM troyer

PART OF LOT NO. 10
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

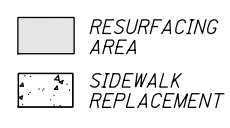
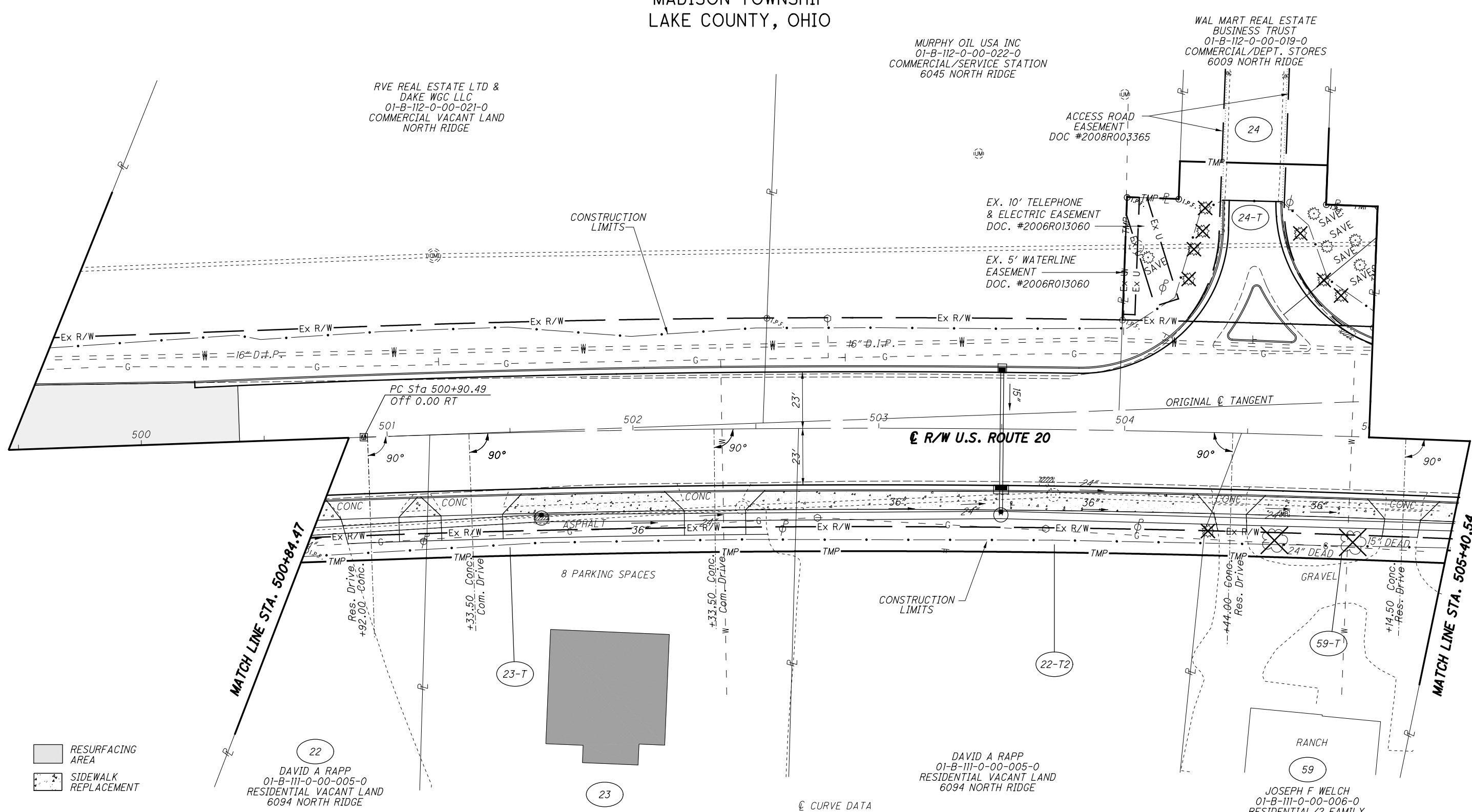
RIGHT OF WAY TOPO SHEET
STA. 500+84.47 TO STA. 505+40.54

LAK-US-20-24.99
PART 2

61/98

660
697

H:\2018\180743\ProjectData\109270_LAK_20_EAST\Design\RW\Sheets\109270_RT020.dgn Sheet 7/15/2022 3:02:33 PM Troyer



STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

Q CURVE DATA
P.I. Sta. 510+71.30
 $\Delta = 19^\circ 25' 40''$ (RT)
 $D_c = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $L = 980.80'$
 $E = 1,942.78'$
 $C = 83.34'$
 $C.B. = N 74^\circ 22' 38'' E$

REV. BY	DATE	DESCRIPTION

PART OF LOT NO. 10
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO

RVE REAL ESTATE LTD
& DAKE WGC LLC
01-B-112-0-00-023-0
COMMERCIAL VACANT LAND
NORTH RIDGE

PATRICIA A HOFFMAN
& WILLIAM E HOFFMAN
01-B-112-0-00-006-0
COMMERCIAL/MFG
6131 NORTH RIDGE

FRENCH REAL ESTATE DEVELOPMENT LLC
01-B-111-0-00-025-0
COMMERCIAL/OFFICE BLDG
6166 NORTH RIDGE

CHRISTOPHER R SAMMET
01-B-111-0-00-007-0
COMMERCIAL/AUTO SALES
6114 NORTH RIDGE



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

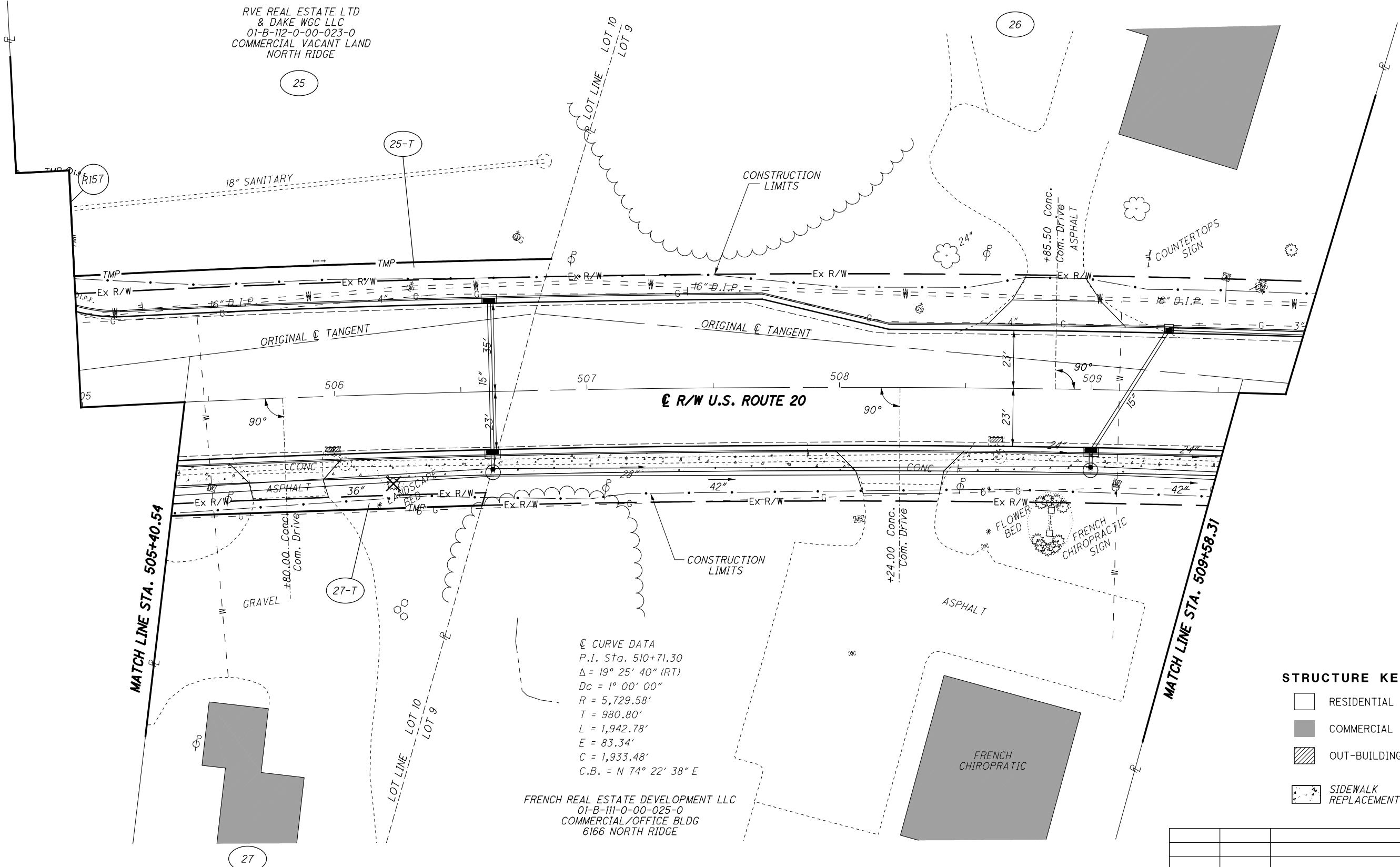
RIGHT OF WAY TOPO SHEET
STA. 505+33.36 TO STA. 509+58.31

LAK-US-20-24.99
PART 2

63/98

662
697

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT02\dgn_Sheet 7/15/2022 3:02:35 PM troyer



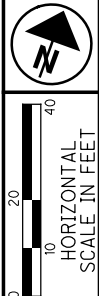
Q CURVE DATA
P.I. Sta. 510+71.30
 $\Delta = 19^\circ 25' 40''$ (RT)
 $Dc = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 980.80'$
 $L = 1,942.78'$
 $E = 83.34'$
 $C = 1,933.48'$
C.B. = N $74^\circ 22' 38''$ E

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING
- SIDEWALK REPLACEMENT

REV. BY	DATE	DESCRIPTION

PART OF LOT NO. 10
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 505+43.47 TO STA. 509+58.31

LAK-US-20-24.99
PART 2

64/98

663
697

CURVE DATA (B)
R = 5781.58'
Δ = 01° 52' 23"
L = 189.02'
T = 94.52'
C = 189.01'
N 69° 41' 25" E

RVE REAL ESTATE LTD
& DAKE WGC LLC
01-B-112-0-00-023-0
COMMERCIAL VACANT LAND
NORTH RIDGE

CURVE DATA (C)
R = 5774.58'
Δ = 01° 51' 03"
L = 186.55'
T = 93.28
C = 186.54'
S 69° 40' 45" W

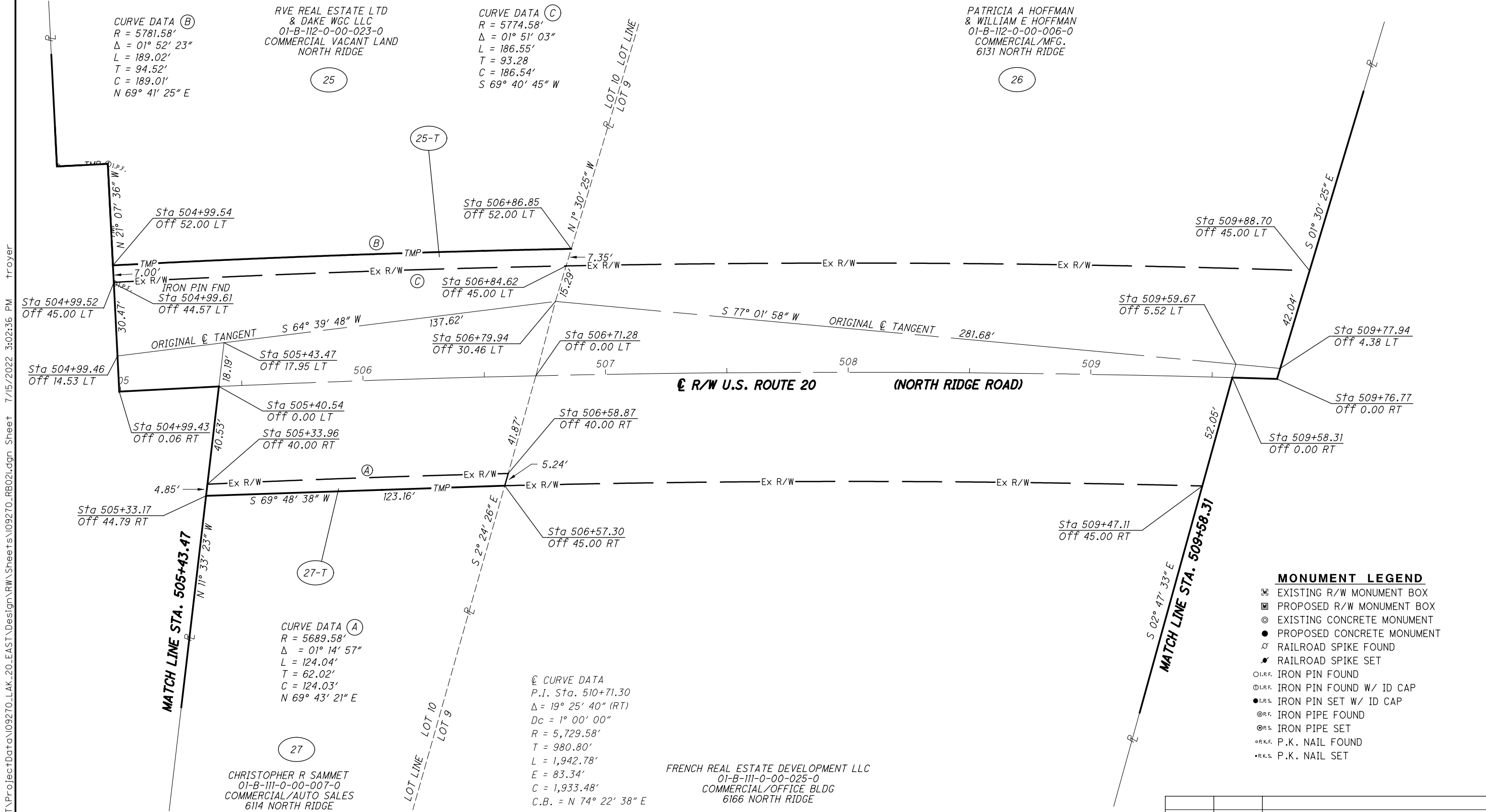
PATRICIA A HOFFMAN
& WILLIAM E HOFFMAN
01-B-112-0-00-006-0
COMMERCIAL/MFG.
6131 NORTH RIDGE

CURVE DATA (A)
R = 5689.58'
Δ = 01° 14' 57"
L = 124.04'
T = 62.02'
C = 124.03'
N 69° 43' 21" E

CHRISTOPHER R SAMMET
01-B-111-0-00-007-0
COMMERCIAL/AUTO SALES
6114 NORTH RIDGE

⊕ CURVE DATA
P.I. Sta. 510+71.30
Δ = 19° 25' 40" (RT)
Dc = 1° 00' 00"
R = 5,729.58'
T = 980.80'
L = 1,942.78'
E = 83.34'
C = 1,933.48'
C.B. = N 74° 22' 38" E

FRENCH REAL ESTATE DEVELOPMENT LLC
01-B-111-0-00-025-0
COMMERCIAL/OFFICE BLDG
6166 NORTH RIDGE



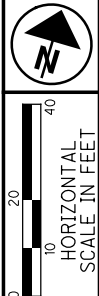
MONUMENT LEGEND

- ▣ EXISTING R/W MONUMENT BOX
- ▤ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- ⊙ I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.S. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- ⊙ P.K.F. P.K. NAIL FOUND
- ⊙ P.K.S. P.K. NAIL SET

REV. BY	DATE	DESCRIPTION

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB02.dgn Sheet 7/15/2022 3:02:36 PM troyer

PART OF LOT NO. 9
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO



PID NO. **109270**
R/W DESIGNER PSL
R/W REVIEWER TMM

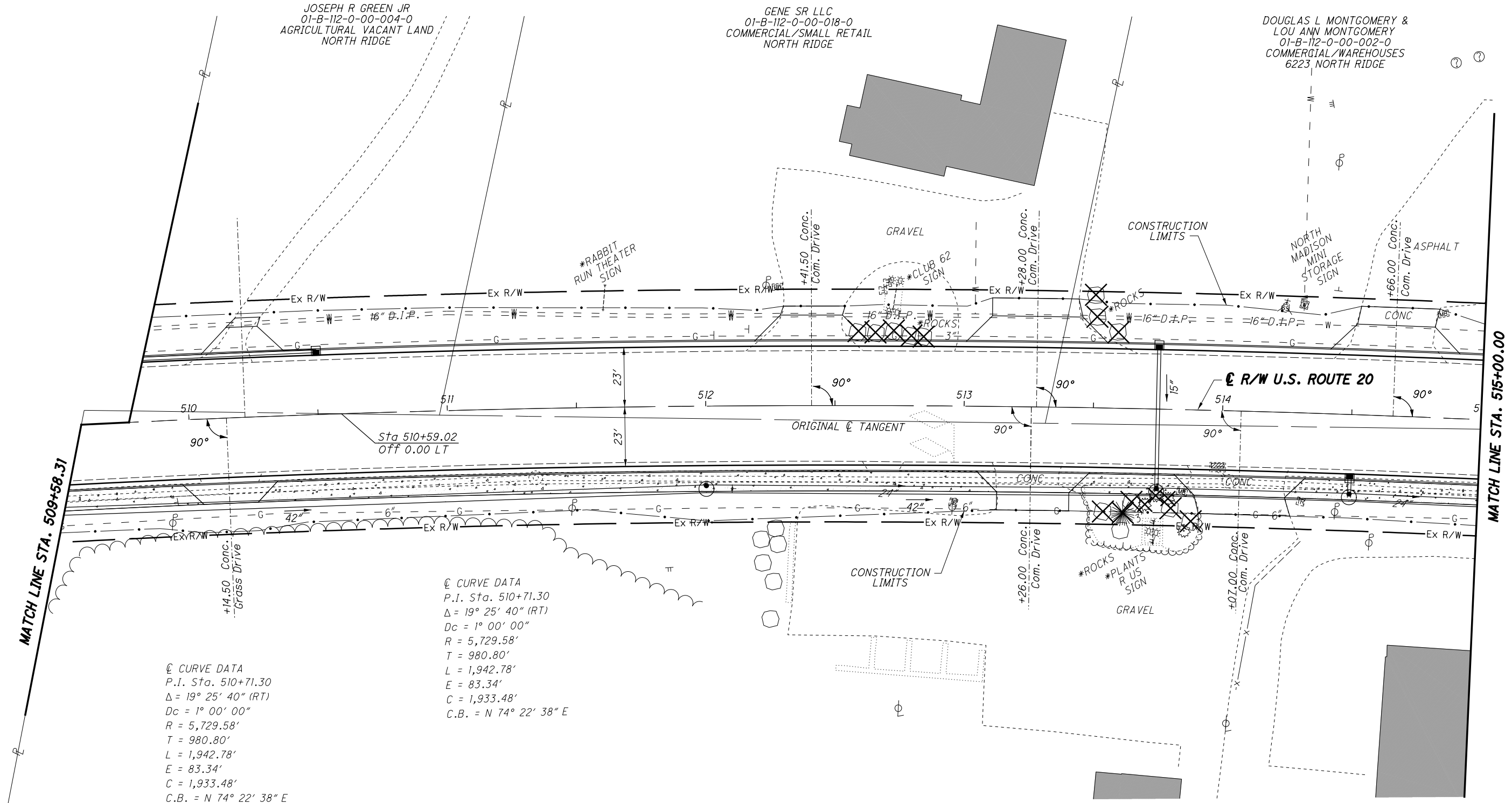
RIGHT OF WAY TOPO SHEET
STA. 509+58.31 TO STA. 515+00.00

LAK-US-20-24.99
PART 2

65/98

664
697

H:\2018\80743\000\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT022.dgn Sheet 7/15/2022 3:02:38 PM Troyer



Q CURVE DATA
P.I. Sta. 510+71.30
Δ = 19° 25' 40" (RT)
Dc = 1° 00' 00"
R = 5,729.58'
T = 980.80'
L = 1,942.78'
E = 83.34'
C = 1,933.48'
C.B. = N 74° 22' 38" E

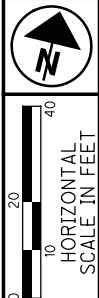
Q CURVE DATA
P.I. Sta. 510+71.30
Δ = 19° 25' 40" (RT)
Dc = 1° 00' 00"
R = 5,729.58'
T = 980.80'
L = 1,942.78'
E = 83.34'
C = 1,933.48'
C.B. = N 74° 22' 38" E

CRAFT BROTHERS INC
01-B-111-0-00-008-0
COMMERCIAL/RETAIL
6214 NORTH RIDGE

STRUCTURE KEY

	RESIDENTIAL
	COMMERCIAL
	OUT-BUILDING
	SIDEWALK REPLACEMENT

REV. BY	DATE	DESCRIPTION



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

RIGHT OF WAY TOPO SHEET
STA. 515+00.00 TO STA. 520+33.27

LAK-US-20-24.99
PART 2

67/98

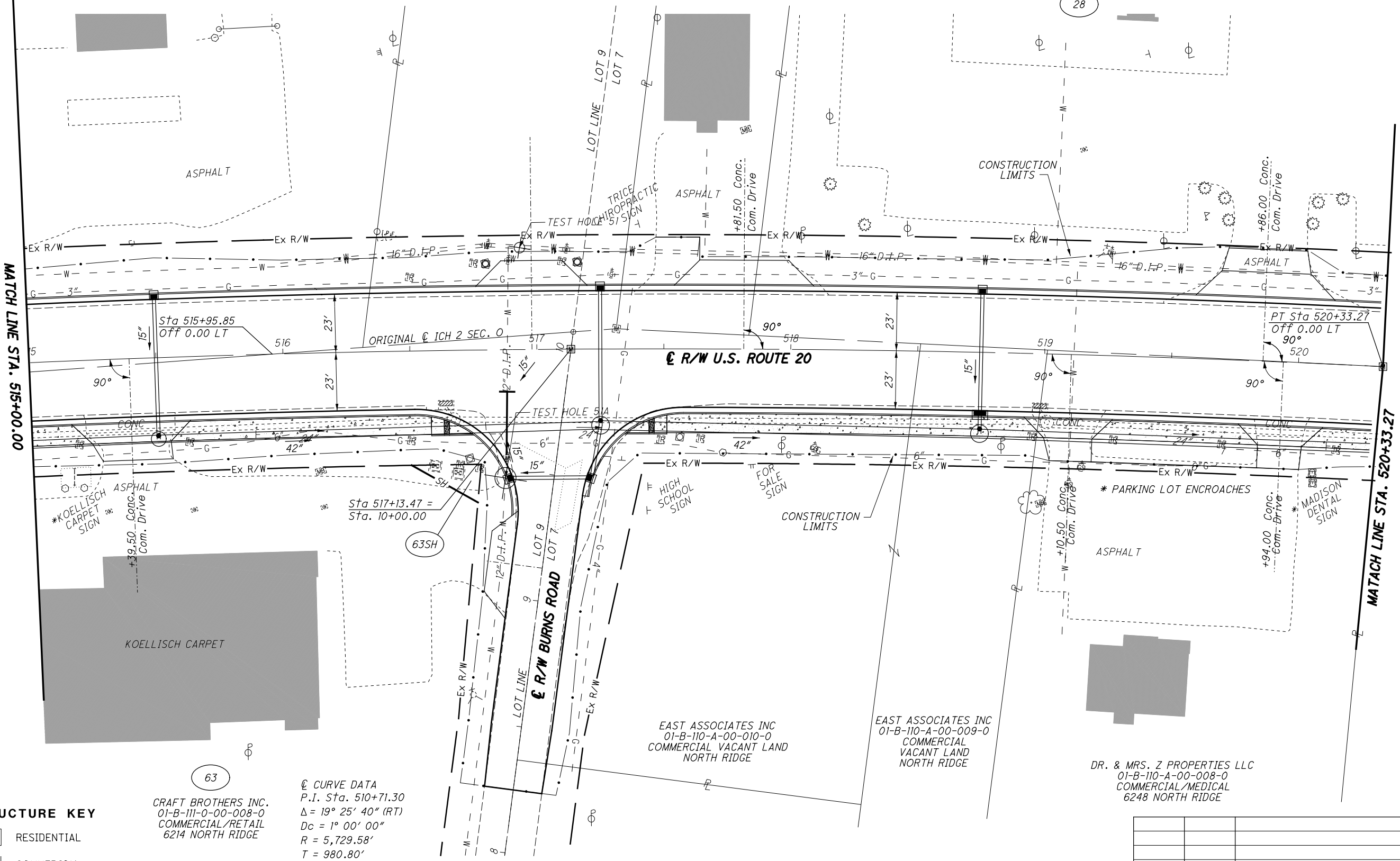
666
697

DOUGLAS L MONTGOMERY
& LOU ANN MONTGOMERY
01-B-112-0-00-002-0
COMMERCIAL/ MINI STORAGE
6223 NORTH RIDGE

RANDY D TRICE
& REBECCA TRICE
01-B-112-0-00-001-0
COMMERCIAL VACANT LAND
NORTH RIDGE

RANDY DEAN TRICE &
REBECCA LYNN TRICE
01-B-110-0-00-016-0
COMMERCIAL OFFICE
6227 NORTH RIDGE

MADISON ROUTE 20 PROPERTIES LLC
01-B-110-0-00-013-0
COMMERCIAL/AUTO SALES
6251 NORTH RIDGE



MATCH LINE STA. 515+00.00

MATCH LINE STA. 520+33.27

STRUCTURE KEY

	RESIDENTIAL
	COMMERCIAL
	OUT-BUILDING

63
CRAFT BROTHERS INC.
01-B-111-0-00-008-0
COMMERCIAL/RETAIL
6214 NORTH RIDGE

② CURVE DATA
P.I. Sta. 510+71.30
Δ = 19° 25' 40" (RT)
Dc = 1° 00' 00"
R = 5,729.58'
T = 980.80'
L = 1,942.78'
E = 83.34'
C = 1,933.48'
C.B. = N 74° 22' 38" E

EAST ASSOCIATES INC
01-B-110-A-00-010-0
COMMERCIAL VACANT LAND
NORTH RIDGE

EAST ASSOCIATES INC
01-B-110-A-00-009-0
COMMERCIAL VACANT LAND
NORTH RIDGE

DR. & MRS. Z PROPERTIES LLC
01-B-110-A-00-008-0
COMMERCIAL/MEDICAL
6248 NORTH RIDGE

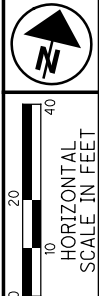
SIDEWALK REPLACEMENT

REV. BY	DATE	DESCRIPTION

H:\2018\180743\000\Proj\Drawings\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT023.dgn Sheet 7/15/2022 3:02:40 PM Troyer

PART OF LOT NO, +
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO

⊕ CURVE DATA U.S 20
P.I. Sta. 510+71.30
Δ = 19° 25' 40" (RT)
Dc = 1° 00' 00"
R = 5,729.58'
T = 980.80'
L = 1,942.78'
E = 83.34'
C = 1,933.48'
C.B. = N 74° 22' 38" E



PID NO. **109270**
R/W DESIGNER PSL
R/W REVIEWER TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 515+00.00 TO STA. 520+33.27

LAK-US-20-24.99
PART 2

68/98
667
697

MADISON ROUTE 20 PROPERTIES LLC
01-B-110-0-00-013-0
COMMERCIAL/AUTO SALES
6251 NORTH RIDGE

DOUGLAS L MONTGOMERY
& LOU ANN MONTGOMERY
01-B-112-0-00-002-0
COMMERCIAL/mini STORAGE
6223 NORTH RIDGE

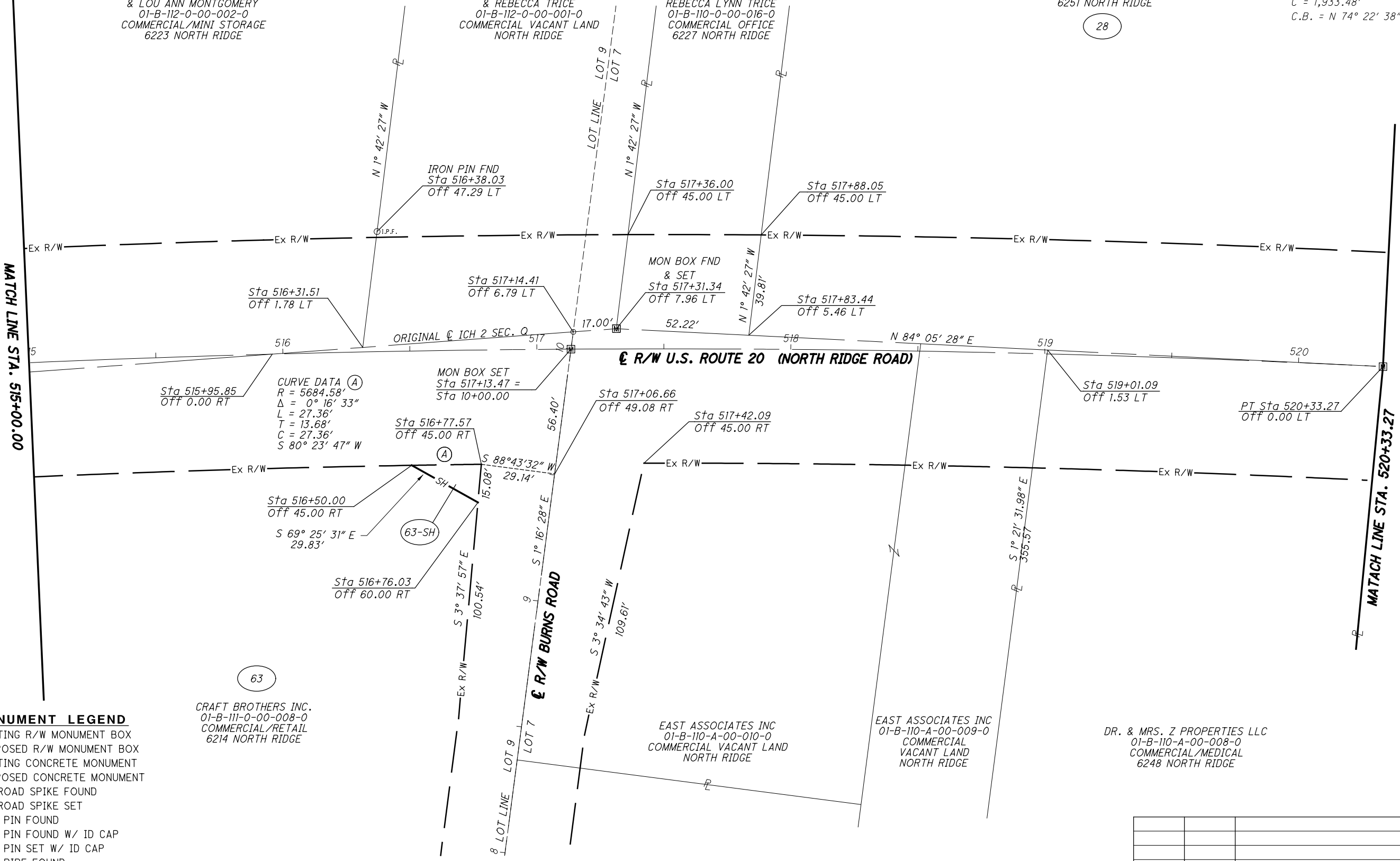
RANDY D TRICE
& REBECCA TRICE
01-B-112-0-00-001-0
COMMERCIAL VACANT LAND
NORTH RIDGE

RANDY DEAN TRICE &
REBECCA LYNN TRICE
01-B-110-0-00-016-0
COMMERCIAL OFFICE
6227 NORTH RIDGE

28

MATCH LINE STA. 515+00.00

MATCH LINE STA. 520+33.27



CURVE DATA (A)
R = 5684.58'
Δ = 0° 16' 33"
L = 27.36'
T = 13.68'
C = 27.36'
S 80° 23' 47" W

63
CRAFT BROTHERS INC.
01-B-111-0-00-008-0
COMMERCIAL/RETAIL
6214 NORTH RIDGE

EAST ASSOCIATES INC
01-B-110-A-00-010-0
COMMERCIAL VACANT LAND
NORTH RIDGE

EAST ASSOCIATES INC
01-B-110-A-00-009-0
COMMERCIAL VACANT LAND
NORTH RIDGE

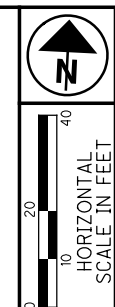
DR. & MRS. Z PROPERTIES LLC
01-B-110-A-00-008-0
COMMERCIAL/MEDICAL
6248 NORTH RIDGE

- MONUMENT LEGEND**
- ☒ EXISTING R/W MONUMENT BOX
 - ☒ PROPOSED R/W MONUMENT BOX
 - ⊙ EXISTING CONCRETE MONUMENT
 - PROPOSED CONCRETE MONUMENT
 - ⚡ RAILROAD SPIKE FOUND
 - ⚡ RAILROAD SPIKE SET
 - ⊙ I.P.F. IRON PIN FOUND
 - ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
 - I.P.S. IRON PIN SET W/ ID CAP
 - ⊙ I.P.F. IRON PIPE FOUND
 - ⊙ I.P.S. IRON PIPE SET
 - ⊙ P.K.F. P.K. NAIL FOUND
 - ⊙ P.K.S. P.K. NAIL SET

REV. BY	DATE	DESCRIPTION

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB023.dgn Sheet 7/15/2022 3:02:41PM troyer

PART OF LOT NO. 7
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO



PID NO. 109270
R/W DESIGNER PSL
R/W REVIEWER TMM

RIGHT OF WAY TOPO SHEET
STA. 525+50.00 TO STA. 531+19.35

LAK-US-20-24.99
PART 2

71/98

670
697

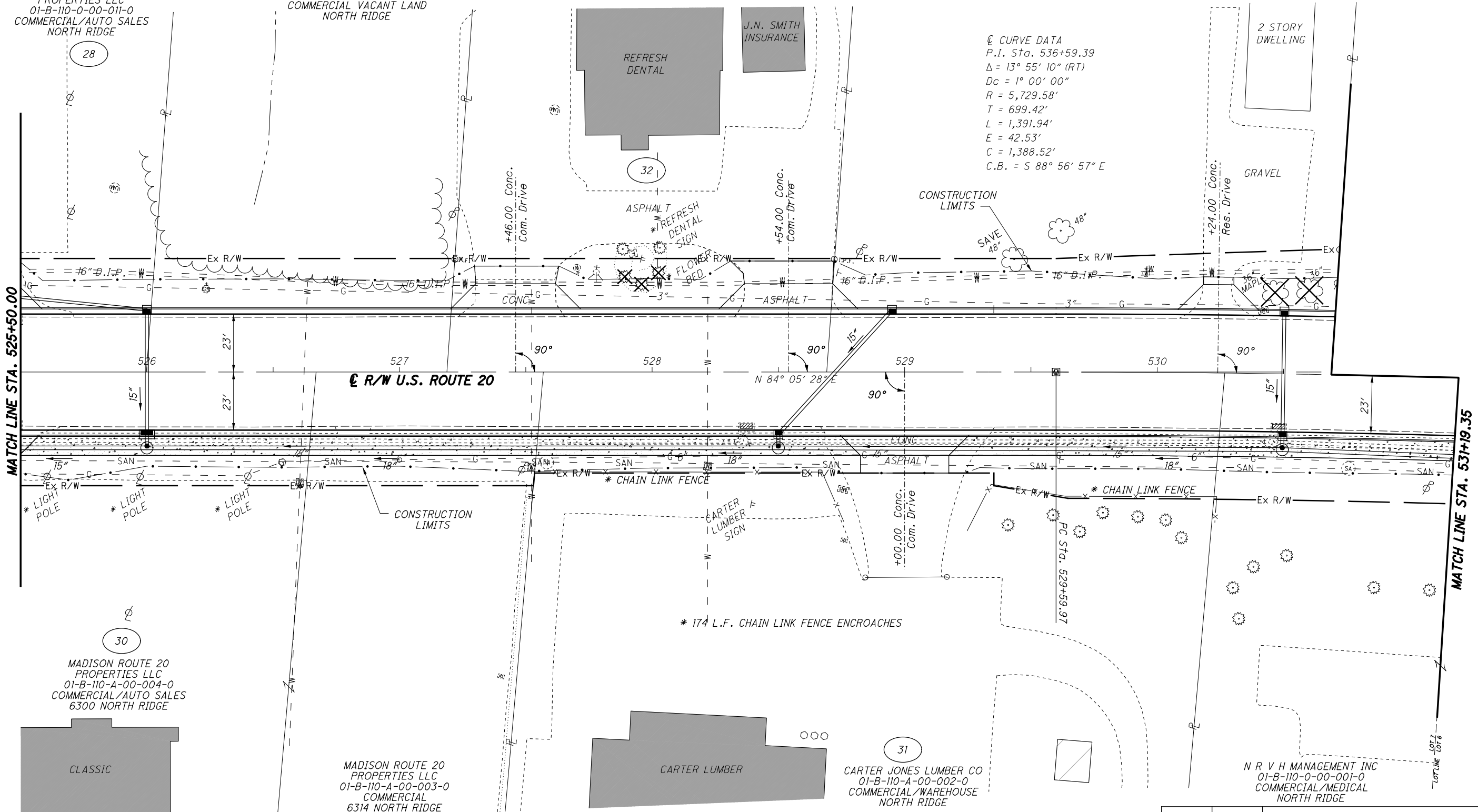
SCF RC FUNDING IV LLC
01-B-110-0-00-009-0
COMMERCIAL/MEDICAL
6325 NORTH RIDGE

JAMES PLACE PROPERTIES ONC
01-B-110-0-00-037-0
MIXED RESIDENTIAL/COMM.
6337 NORTH RIDGE

MADISON ROUTE 20
PROPERTIES LLC
01-B-110-0-00-011-0
COMMERCIAL/AUTO SALES
NORTH RIDGE

JAMES PLACE PROPERTIES INC
01-B-110-0-00-036-0
COMMERCIAL VACANT LAND
NORTH RIDGE

☉ CURVE DATA
P.I. Sta. 536+59.39
 $\Delta = 13^\circ 55' 10''$ (RT)
 $Dc = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 699.42'$
 $L = 1,391.94'$
 $E = 42.53'$
 $C = 1,388.52'$
 $C.B. = S 88^\circ 56' 57'' E$



STRUCTURE KEY

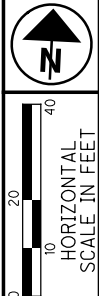
- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

SIDEWALK REPLACEMENT

REV. BY	DATE	DESCRIPTION

H:\2018\80743\000T\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_R1025.dgn Sheet 7/15/2022 3:02:46 PM Troyer

PART OF LOT NO. 7
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO



PID NO.
109270

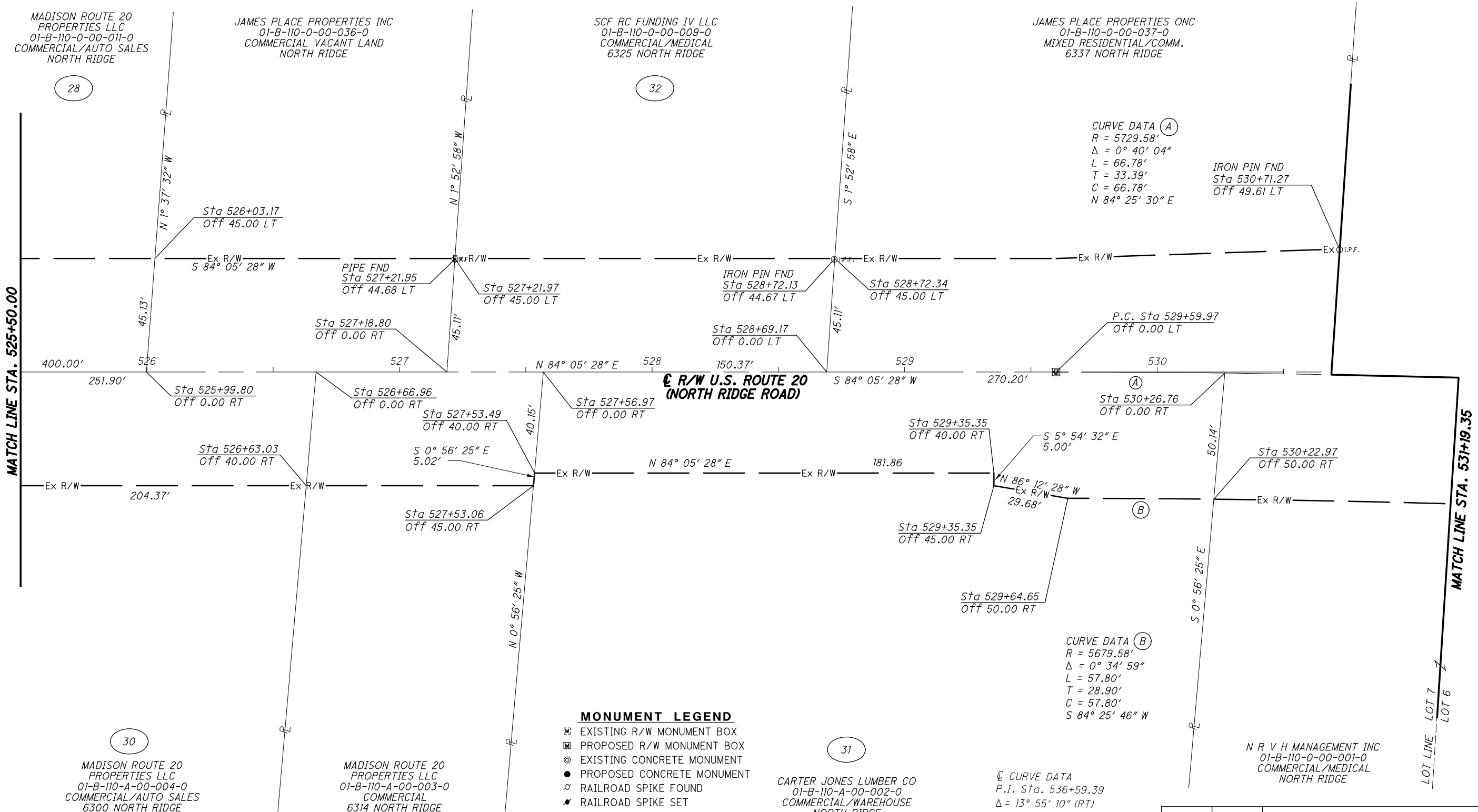
R/W DESIGNER
PSL
R/W REVIEWER
TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 525+50.00 TO STA. 531+19.35

LAK-US-20-24.99
PART 2

72/98

671
697



MONUMENT LEGEND

- ☐ EXISTING R/W MONUMENT BOX
- ▣ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ✱ RAILROAD SPIKE FOUND
- ✱ RAILROAD SPIKE SET
- I.P.F. IRON PIN FOUND
- I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- P.K.F. P.K. NAIL FOUND
- P.K.S. P.K. NAIL SET

CURVE DATA (A)
R = 5729.58'
Δ = 0° 40' 04"
L = 66.78'
T = 33.39'
C = 66.78'
N 84° 25' 30" E

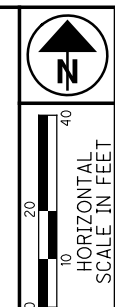
CURVE DATA (B)
R = 5679.58'
Δ = 0° 34' 59"
L = 57.80'
T = 28.90'
C = 57.80'
S 84° 25' 46" W

Ⓢ CURVE DATA
P.I. Sta. 536+59.39
Δ = 13° 55' 10" (RT)
Dc = 1° 00' 00"
R = 5,729.58'
T = 699.42'
L = 1,391.94'
E = 42.53'
C = 1,388.52'
C.B. = S 88° 56' 57" E

REV. BY	DATE	DESCRIPTION

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB025.dgn Sheet 7/15/2022 3:02:47 PM Troyer

PART OF LOT NO. 6
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 531+19.35 TO STA. 535+93.79

LAK-US-20-24.99
PART 2

74/98

673
697

MADISON BOARD OF TRUSTEES CEMETERIES
01-B-110-0-00-007-0
EXEMPT
CEMETARY
NORTH RIDGE

JAMES PLACE PROPERTIES INC
01-B-110-0-00-035-0
COMMERCIAL
MOBILE HOMES
NORTH RIDGE

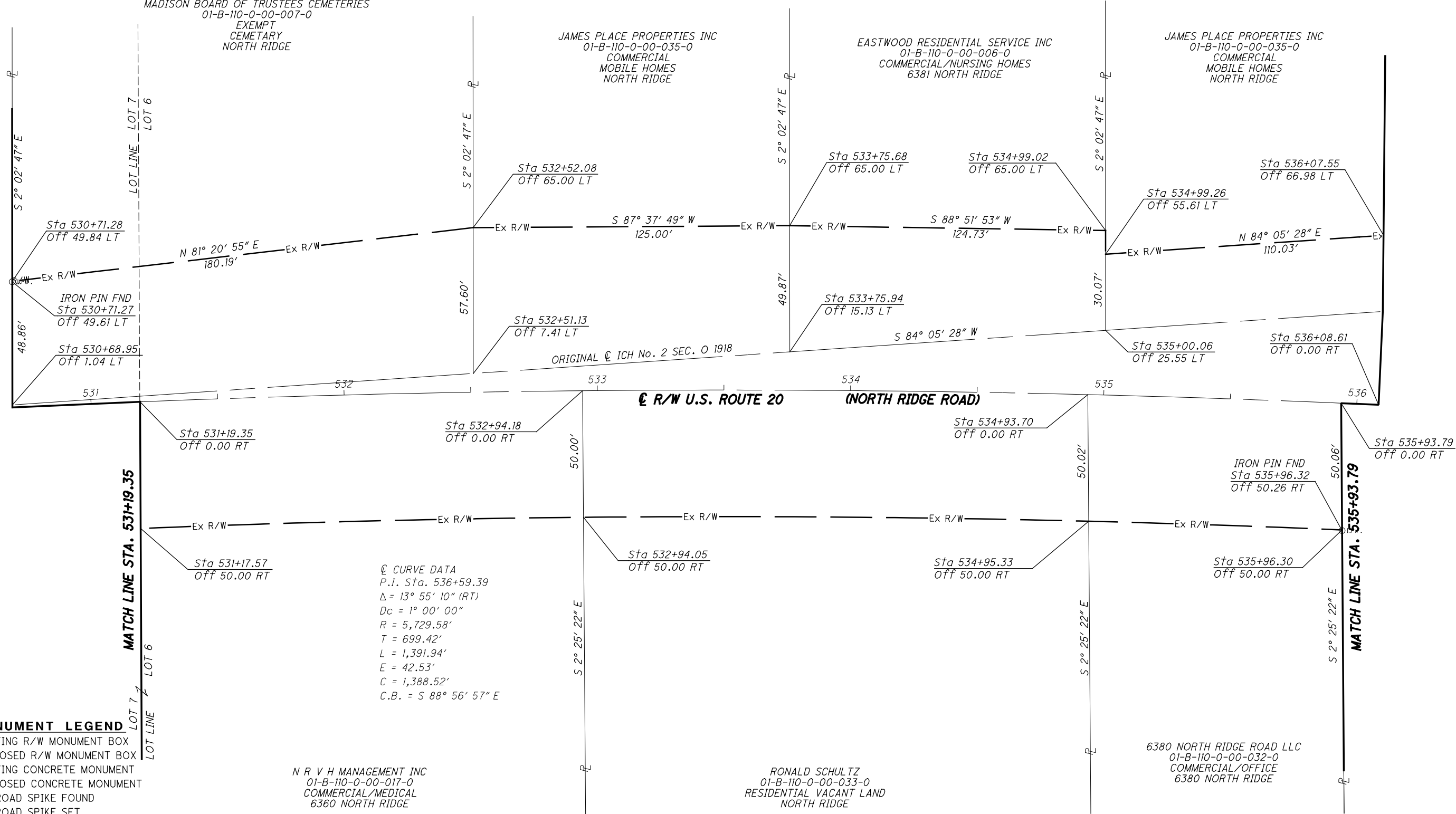
EASTWOOD RESIDENTIAL SERVICE INC
01-B-110-0-00-006-0
COMMERCIAL/NURSING HOMES
6381 NORTH RIDGE

JAMES PLACE PROPERTIES INC
01-B-110-0-00-035-0
COMMERCIAL
MOBILE HOMES
NORTH RIDGE

N R V H MANAGEMENT INC
01-B-110-0-00-017-0
COMMERCIAL/MEDICAL
6360 NORTH RIDGE

RONALD SCHULTZ
01-B-110-0-00-033-0
RESIDENTIAL VACANT LAND
NORTH RIDGE

6380 NORTH RIDGE ROAD LLC
01-B-110-0-00-032-0
COMMERCIAL/OFFICE
6380 NORTH RIDGE



☺ CURVE DATA
P.I. Sta. 536+59.39
 $\Delta = 13^\circ 55' 10''$ (RT)
 $D_c = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 699.42'$
 $L = 1,391.94'$
 $E = 42.53'$
 $C = 1,388.52'$
C.B. = $S 88^\circ 56' 57'' E$

MONUMENT LEGEND

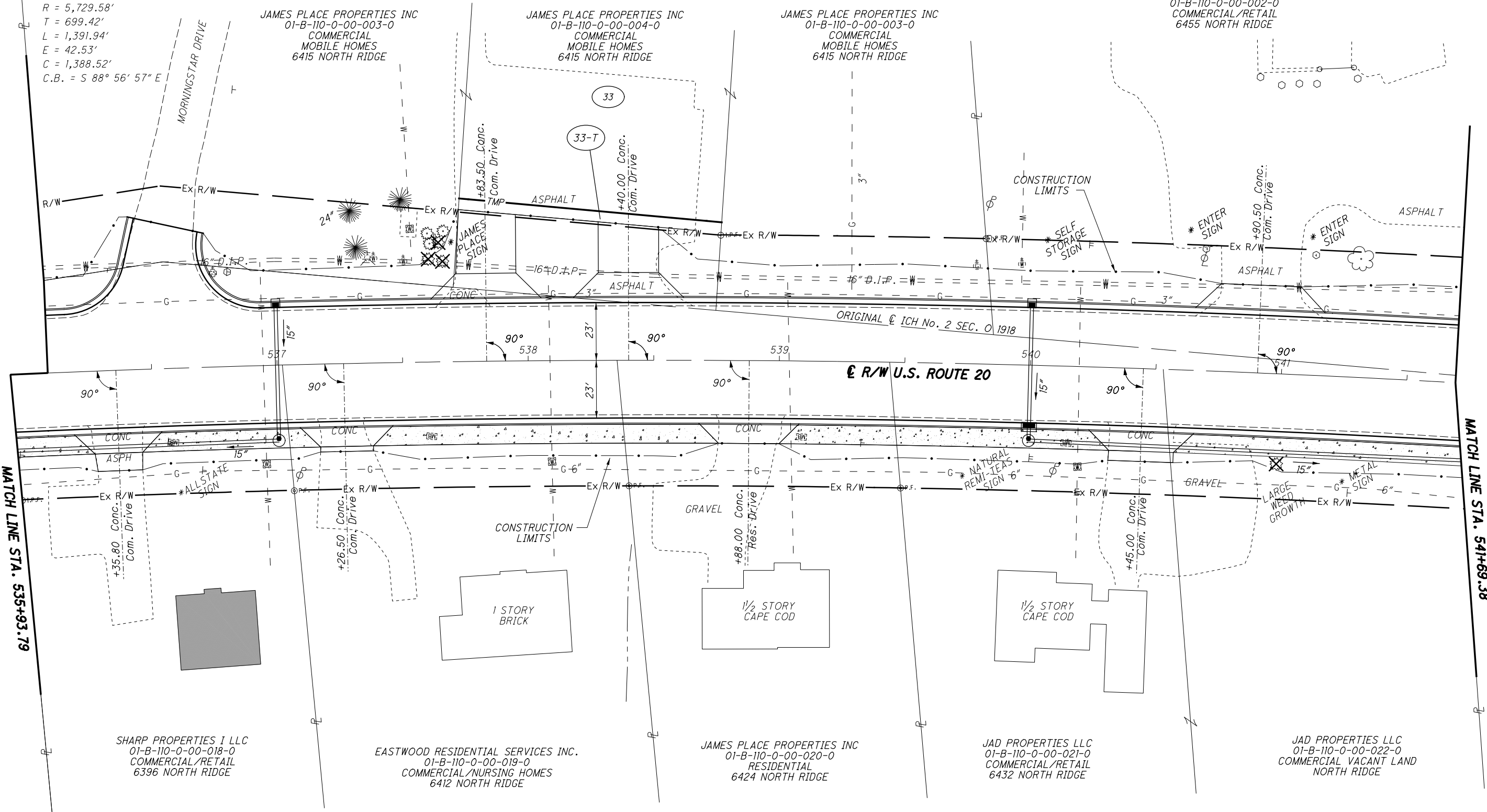
- ☒ EXISTING R/W MONUMENT BOX
- ☒ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- ⊙ I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- ⊙ R.K.F. P.K. NAIL FOUND
- R.K.S. P.K. NAIL SET

REV. BY	DATE	DESCRIPTION

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB026.dgn Sheet 7/15/2022 3:02:49 PM troyer

PART OF LOT NO. 6
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO

⊙ CURVE DATA
P.I. Sta. 536+59.39
Δ = 13° 55' 10" (RT)
Dc = 1° 00' 00"
R = 5,729.58'
T = 699.42'
L = 1,391.94'
E = 42.53'
C = 1,388.52'
C.B. = S 88° 56' 57" E



MUSTANG PROPERTIES LLC
01-B-110-0-00-002-0
COMMERCIAL/RETAIL
6455 NORTH RIDGE

JAMES PLACE PROPERTIES INC
01-B-110-0-00-003-0
COMMERCIAL
MOBILE HOMES
6415 NORTH RIDGE

JAMES PLACE PROPERTIES INC
01-B-110-0-00-004-0
COMMERCIAL
MOBILE HOMES
6415 NORTH RIDGE

JAMES PLACE PROPERTIES INC
01-B-110-0-00-003-0
COMMERCIAL
MOBILE HOMES
6415 NORTH RIDGE

SHARP PROPERTIES I LLC
01-B-110-0-00-018-0
COMMERCIAL/RETAIL
6396 NORTH RIDGE

EASTWOOD RESIDENTIAL SERVICES INC.
01-B-110-0-00-019-0
COMMERCIAL/NURSING HOMES
6412 NORTH RIDGE

JAMES PLACE PROPERTIES INC
01-B-110-0-00-020-0
RESIDENTIAL
6424 NORTH RIDGE

JAD PROPERTIES LLC
01-B-110-0-00-021-0
COMMERCIAL/RETAIL
6432 NORTH RIDGE

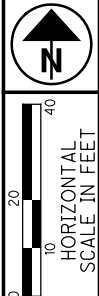
JAD PROPERTIES LLC
01-B-110-0-00-022-0
COMMERCIAL VACANT LAND
NORTH RIDGE

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING



REV. BY	DATE	DESCRIPTION



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

RIGHT OF WAY TOPO SHEET
STA. 535+93.79 TO STA. 541+69.38

LAK-US-20-24.99
PART 2

75/98

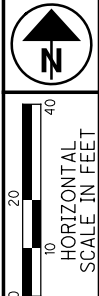
674
697

H:\2018\180743\0D0T\Projec+Data\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT027.dgn Sheet 7/15/2022 3:02:51 PM troyer

MATCH LINE STA. 535+93.79

MATCH LINE STA. 541+69.38

PART OF LOT NO. 6
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

LAK-US-20-24.99
PART 2
RIGHT OF WAY BOUNDARY SHEET
STA. 535+93.79 TO STA. 541+69.38

76/98
675
697

④ CURVE DATA
P.I. Sta. 536+59.39
 $\Delta = 13^\circ 55' 10''$ (RT)
 $D_c = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 699.42'$
 $L = 1,391.94'$
 $E = 42.53'$
 $C = 1,388.52'$
C.B. = $S 88^\circ 56' 57'' E$

JAMES PLACE PROPERTIES INC
01-B-110-0-00-003-0
COMMERCIAL
MOBILE HOMES
6415 NORTH RIDGE

JAMES PLACE PROPERTIES, INC
01-B-110-0-00-004-0
COMMERCIAL
MOBILE HOMES
6415 NORTH RIDGE

JAMES PLACE PROPERTIES INC
01-B-110-0-00-003-0
COMMERCIAL
MOBILE HOMES
6415 NORTH RIDGE

MUSTANG PROPERTIES LLC
01-B-110-0-00-002-0
MUSTANG PROPERTIES LLC
6455 NORTH RIDGE

SHARP PROPERTIES I LLC
01-B-110-0-00-018-0
COMMERCIAL/RETAIL
6396 NORTH RIDGE

EASTWOOD RESIDENTIAL SERVICES INC
01-B-110-0-00-019-0
COMMERCIAL/NURSING HOMES
6412 NORTH RIDGE

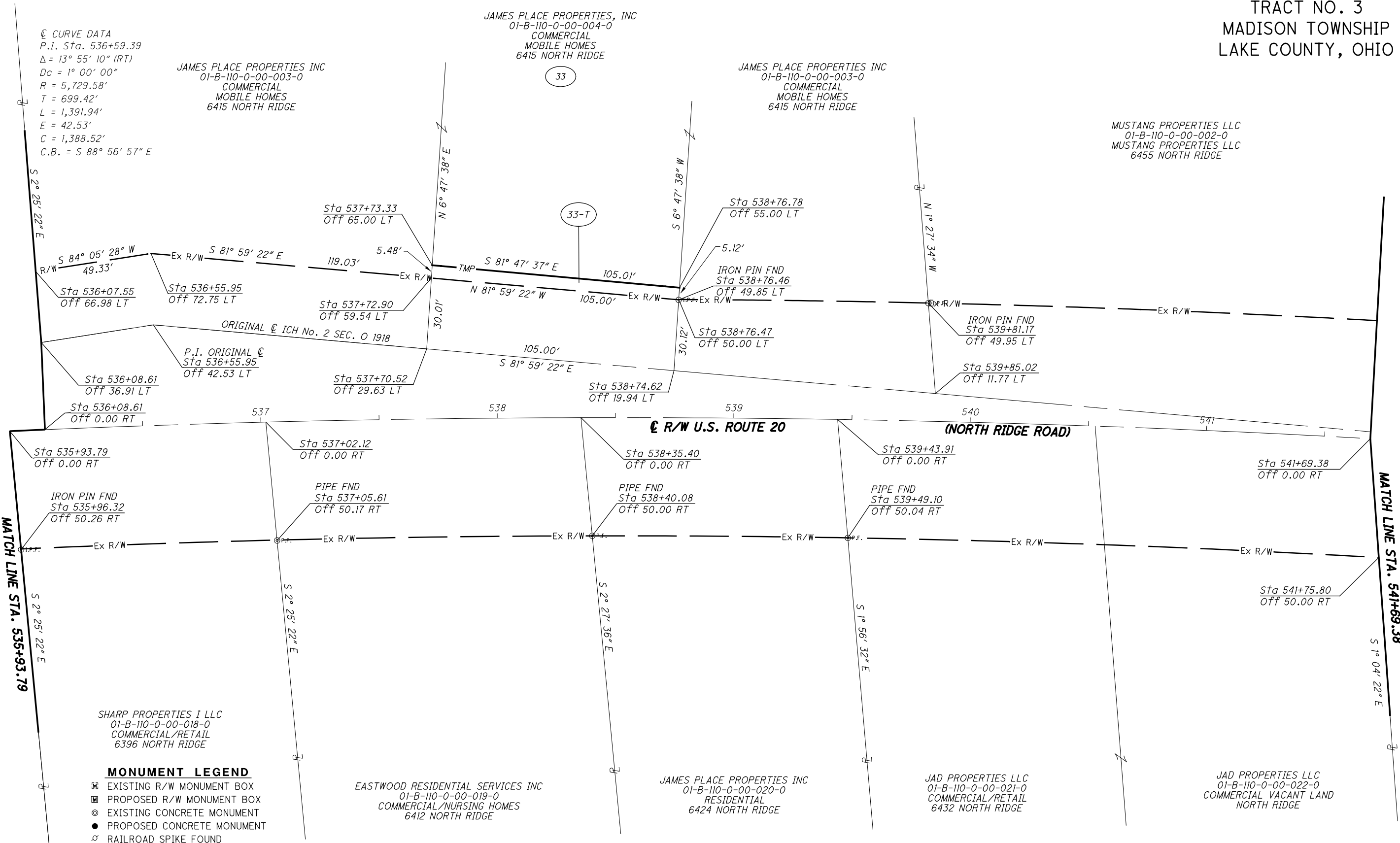
JAMES PLACE PROPERTIES INC
01-B-110-0-00-020-0
RESIDENTIAL
6424 NORTH RIDGE

JAD PROPERTIES LLC
01-B-110-0-00-021-0
COMMERCIAL/RETAIL
6432 NORTH RIDGE

JAD PROPERTIES LLC
01-B-110-0-00-022-0
COMMERCIAL VACANT LAND
NORTH RIDGE

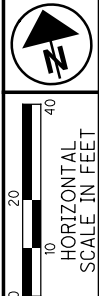
MONUMENT LEGEND

- ☐ EXISTING R/W MONUMENT BOX
- ▣ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- ⊙ I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- ⊙ P.K. P.K. NAIL FOUND
- P.K. P.K. NAIL SET



REV. BY	DATE	DESCRIPTION

H:\2018\180743\000\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270.dgn Sheet 7/15/2022 3:02:52 PM Troyer

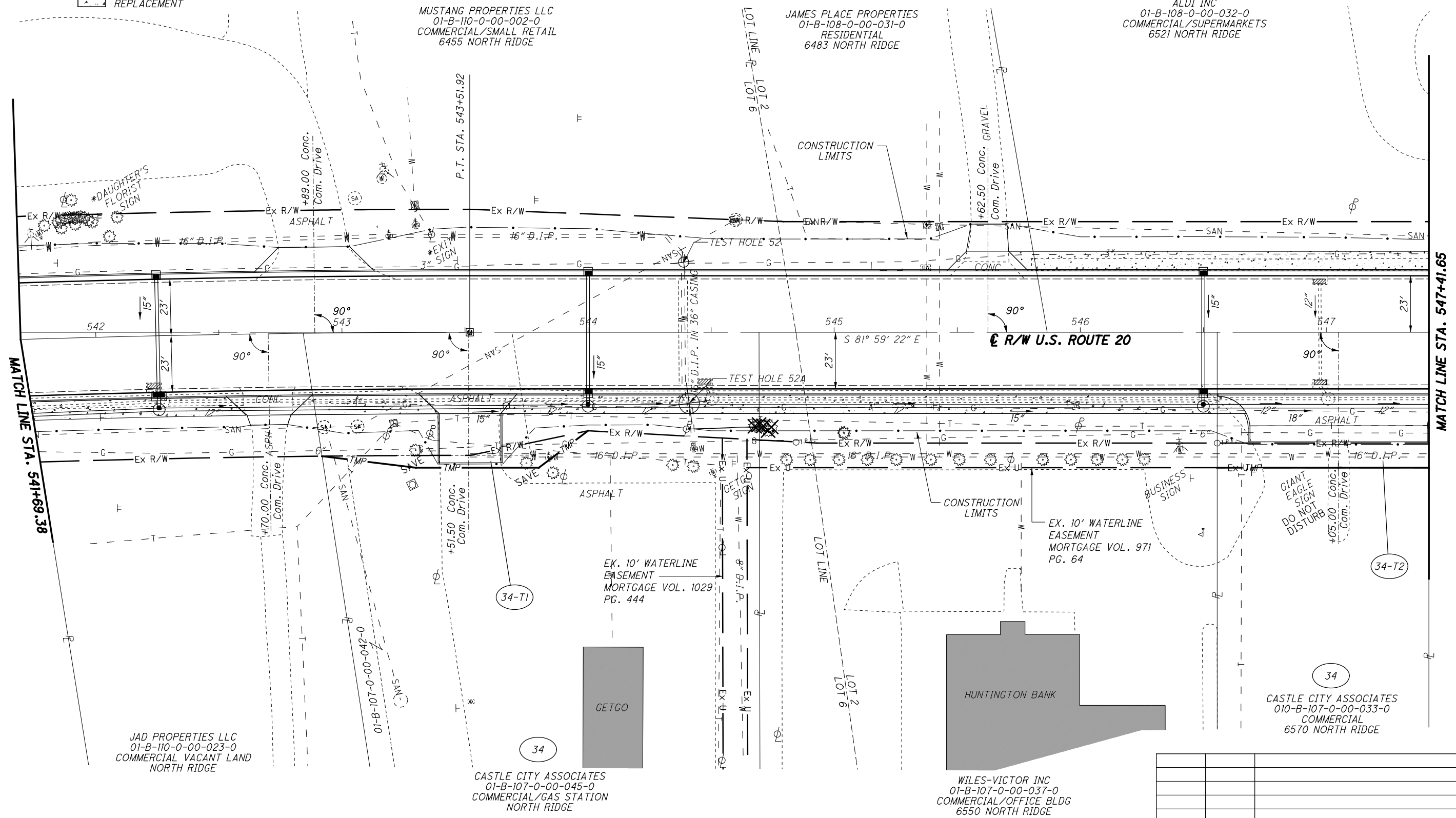


**PART OF LOT NOS. 6 & 2
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO**

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING
- SIDEWALK REPLACEMENT

C CURVE DATA
 P.I. Sta. 536+59.39
 $\Delta = 13^\circ 55' 10''$ (RT)
 $D_c = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 699.42'$
 $L = 1,391.94'$
 $E = 42.53'$
 $C = 1,388.52'$
 $C.B. = S 88^\circ 56' 57'' E$



H:\2018\180743\00101\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT028.dgn Sheet 7/15/2022 3:02:54 PM Troyer

PID NO. **109270**
 R/W DESIGNER PSL
 R/W REVIEWER TMM

RIGHT OF WAY TOPO SHEET
STA. 541+69.38 TO STA. 547+41.65

LAK-US-20-24.99
PART 2

77/98

676
 697

REV. BY	DATE	DESCRIPTION

PART OF LOT NOS. 6 & 2
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO

Q CURVE DATA
P.I. Sta. 536+59.39
 $\Delta = 13^\circ 55' 10''$ (RT)
 $D_c = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 699.42'$
 $L = 1,391.94'$
 $E = 42.53'$
 $C = 1,388.52'$
C.B. = S 88° 56' 57" E

MUSTANG PROPERTIES LLC
01-B-110-0-00-002-0
COMMERCIAL/SMALL RETAIL
6455 NORTH RIDGE

JAMES PLACE PROPERTIES
01-B-108-0-00-031-0
RESIDENTIAL
6483 NORTH RIDGE

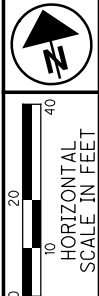
ALDI INC
01-B-108-0-00-032-0
COMMERCIAL/SUPERMARKETS
6521 NORTH RIDGE

CURVE DATA (A)
 $R = 5729.58'$
 $\Delta = 0^\circ 40' 36''$
 $L = 67.68'$
 $T = 33.84'$
 $C = 67.68'$
S 82° 19' 40" E

CURVE DATA (B)
 $R = 5679.58'$
 $\Delta = 0^\circ 36' 23''$
 $L = 60.11'$
 $T = 30.05'$
 $C = 60.11'$
S 82° 17' 30" E

MONUMENT LEGEND

- EXISTING R/W MONUMENT BOX
- PROPOSED R/W MONUMENT BOX
- EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- RAILROAD SPIKE FOUND
- RAILROAD SPIKE SET
- IRON PIN FOUND
- IRON PIN FOUND W/ ID CAP
- IRON PIN SET W/ ID CAP
- IRON PIPE FOUND
- IRON PIPE SET
- P.K. NAIL FOUND
- P.K. NAIL SET



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 541+69.38 TO STA. 547+41.65

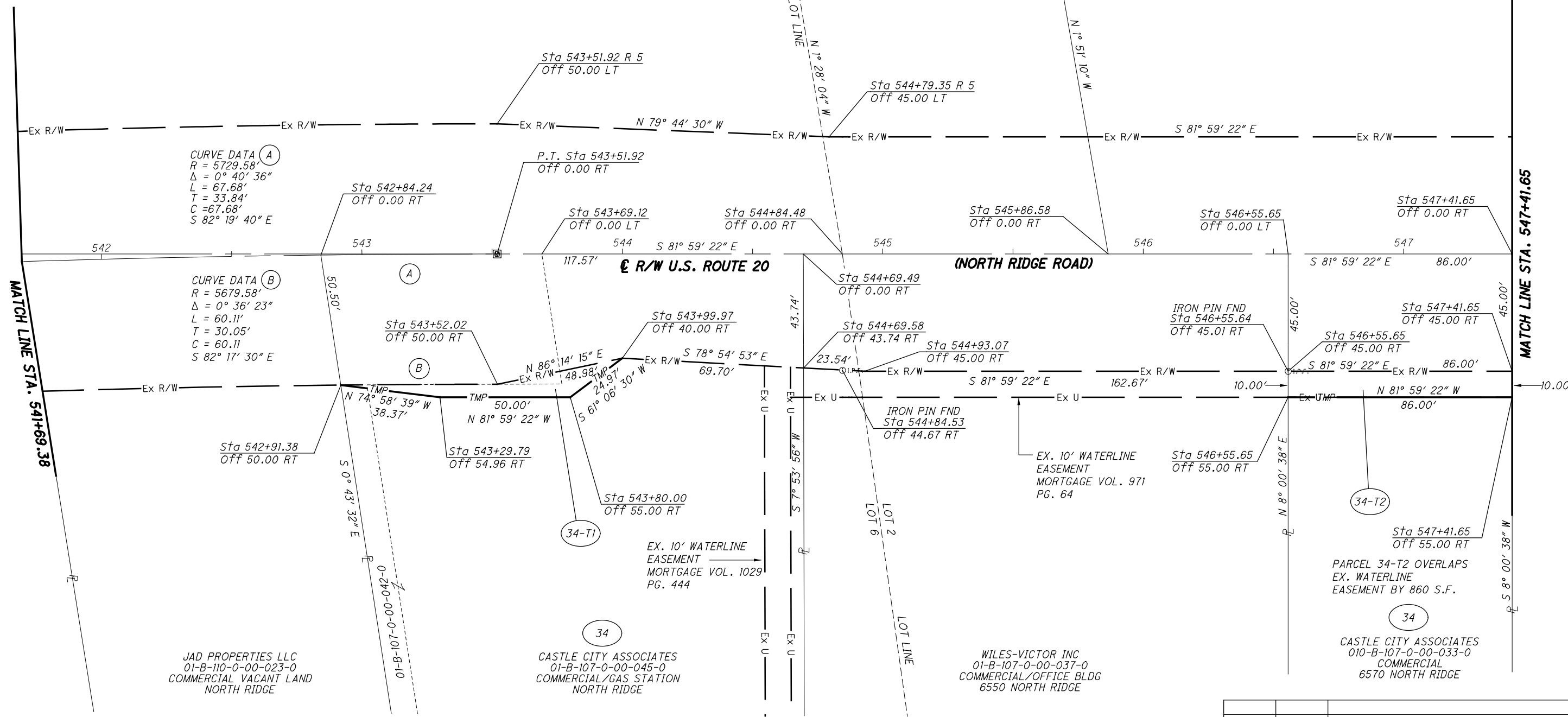
LAK-US-20-24.99
PART 2

78/98

677
697

REV. BY	DATE	DESCRIPTION

H:\2018\180743\0001\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB028.dgn_Sheet 7/15/2022 3:02:55 PM Troyer



JAD PROPERTIES LLC
01-B-110-0-00-023-0
COMMERCIAL VACANT LAND
NORTH RIDGE

CASTLE CITY ASSOCIATES
01-B-107-0-00-045-0
COMMERCIAL/GAS STATION
NORTH RIDGE

WILES-VICTOR INC
01-B-107-0-00-037-0
COMMERCIAL/OFFICE BLDG
6550 NORTH RIDGE

CASTLE CITY ASSOCIATES
010-B-107-0-00-033-0
COMMERCIAL
6570 NORTH RIDGE

EX. 10' WATERLINE
EASEMENT
MORTGAGE VOL. 1029
PG. 444

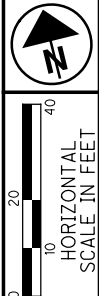
EX. 10' WATERLINE
EASEMENT
MORTGAGE VOL. 971
PG. 64

PARCEL 34-T2 OVERLAPS
EX. WATERLINE
EASEMENT BY 860 S.F.

MATCH LINE STA. 541+69.38

MATCH LINE STA. 547+41.65

PART OF LOT NO. 1
TRACT NO. 3
MADISON TOWSHIP
LAKE COUNTY, OHIO



PID NO. **109270**

R/W DESIGNER PSL
R/W REVIEWER TMM

RIGHT OF WAY TOPO SHEET
STA. 551+80.00 TO STA. 556+43.02

LAK-US-20-24.99
PART 2

81/98

680
697

RAKES FLOORING INC
01-B-108-0-00-022-0
COMMERCIAL/SMALL RETAIL
6559 NORTH RIDGE

CLINIC CARE INC
01-B-108-0-00-042-0
COMMERCIAL VACANT LAND
NORTH RIDGE

HUNTINGTON BANK

FIRST MERIT BANK NA
01-B-108-0-00-038-0
COMMERCIAL/BANKS
6565 NORTH RIDGE

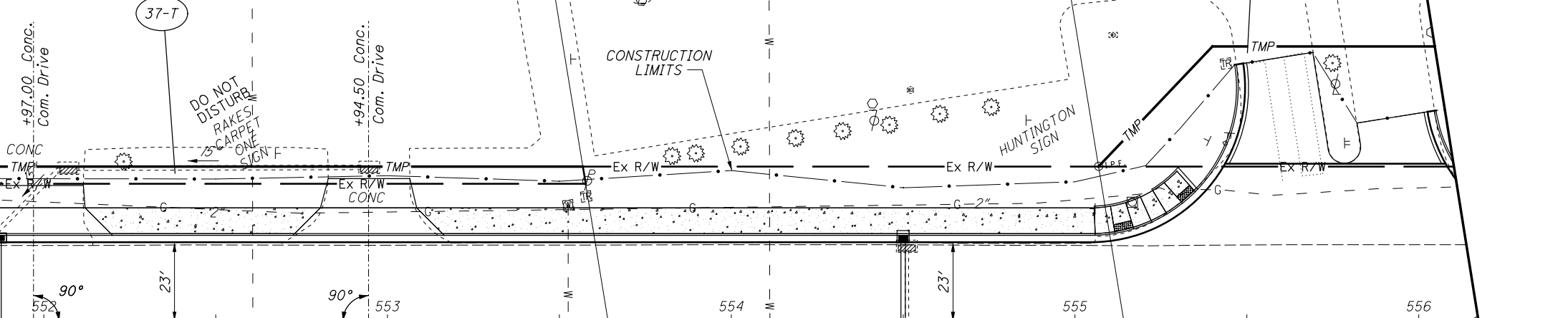
RAKES CARPET ONE

37-T

38

38-T

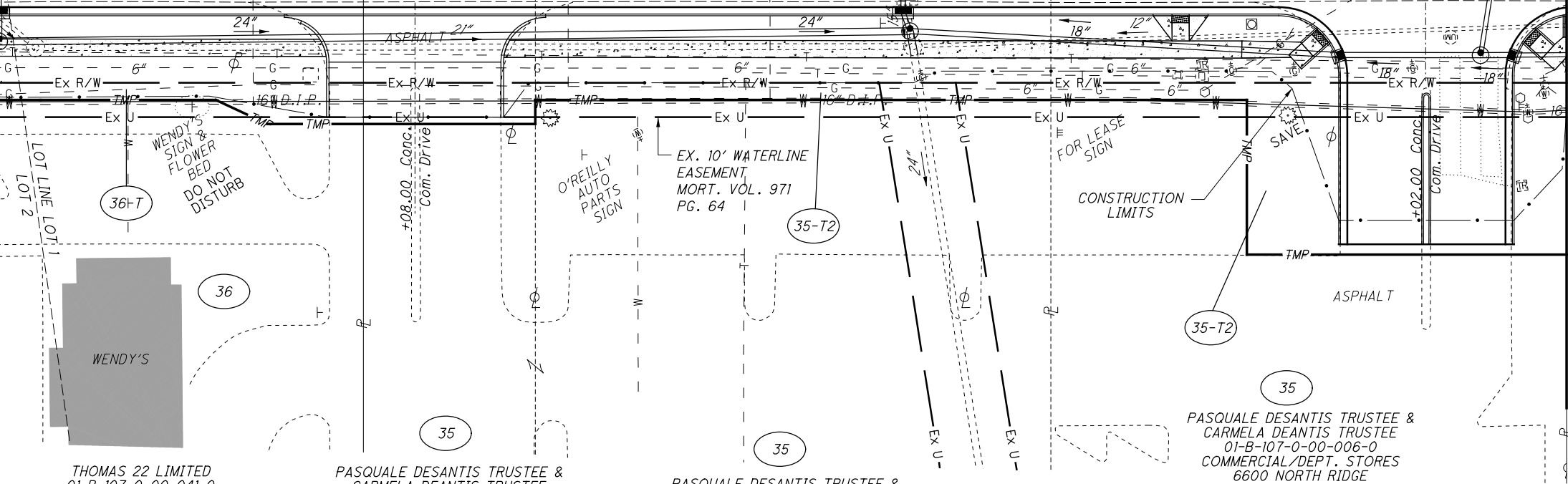
LOT 2
LOT LINE
LOT 1



R/W U.S. ROUTE 20

MATCH LINE STA. 551+80.00

MATCH LINE STA. 556+43.02



THOMAS 22 LIMITED
01-B-107-0-00-041-0
COMMERCIAL
RESTAURANT
6598 NORTH RIDGE

PASQUALE DESANTIS TRUSTEE &
CARMELA DEANTIS TRUSTEE
01-B-107-0-00-006-0
COMMERCIAL/DEPT. STORES
6600 NORTH RIDGE

PASQUALE DESANTIS TRUSTEE &
CARMELA DEANTIS TRUSTEE
01-B-107-0-00-046-0
COMMERCIAL/SMALL RETAIL
6602 NORTH RIDGE

PASQUALE DESANTIS TRUSTEE &
CARMELA DEANTIS TRUSTEE
01-B-107-0-00-006-0
COMMERCIAL/DEPT. STORES
6600 NORTH RIDGE

STRUCTURE KEY

- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

SIDEWALK REPLACEMENT

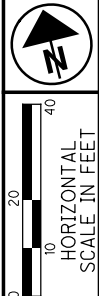
REV. BY	DATE	DESCRIPTION

H:\2018\80743\0D01\ProjecData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT030.dgn Sheet 7/15/2022 3:02:59 PM Troyer

PART OF LOT NO. 1
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO

MONUMENT LEGEND

- ☒ EXISTING R/W MONUMENT BOX
- ☒ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- ⊙ R.K.F. P.K. NAIL FOUND
- ⊙ R.K.S. P.K. NAIL SET



PID NO. 109270

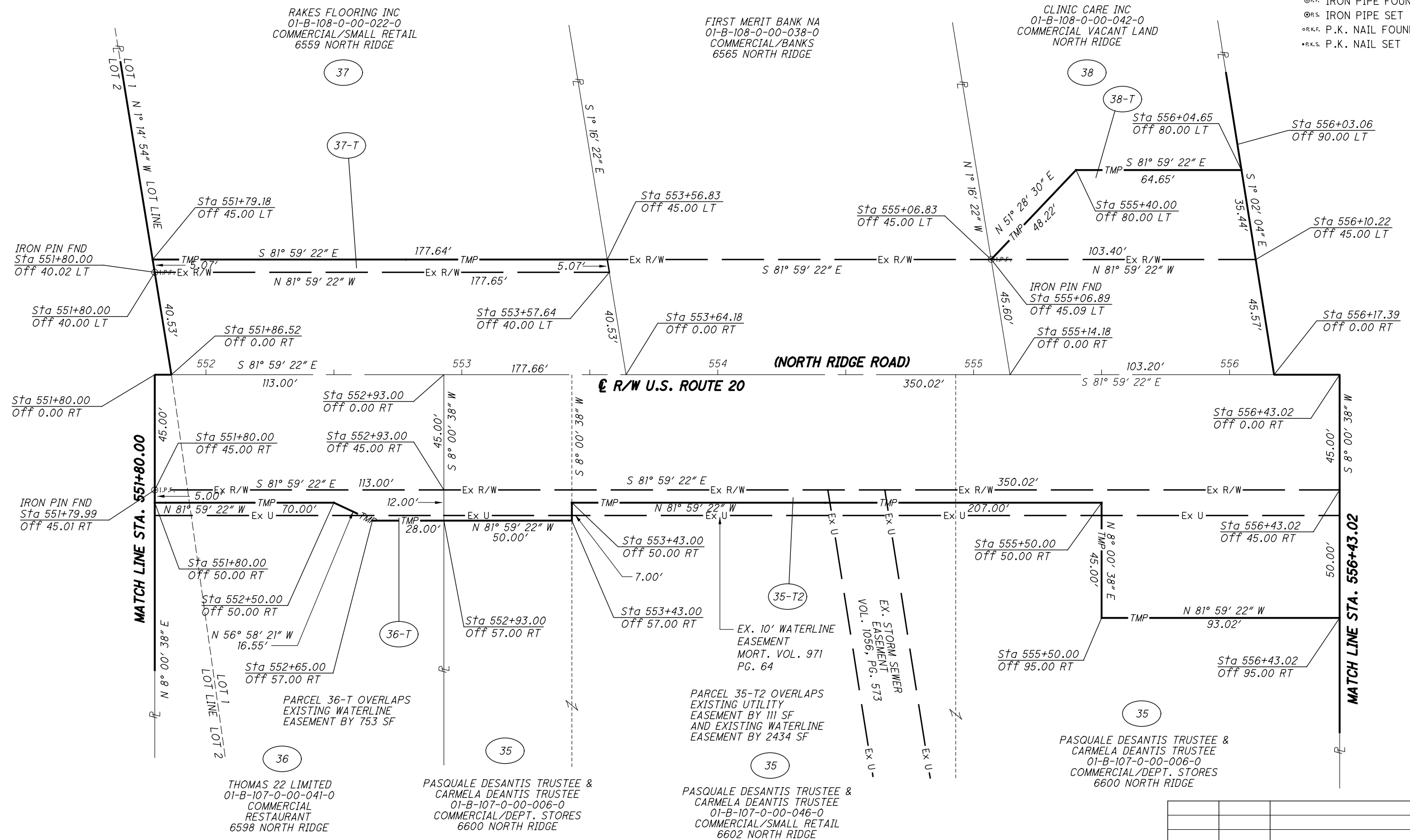
R/W DESIGNER PSL
R/W REVIEWER TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 551+80.00 TO STA. 556+43.02

LAK-US-20-24.99
PART 2

82/98

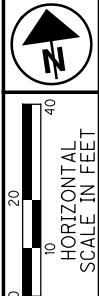
681
697



REV. BY	DATE	DESCRIPTION

H:\2018\180743\0010T\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB030.dgn Sheet 7/15/2022 3:03:00 PM troyer

PART OF LOT NO. 1
TRACT NO.3
MADISON TOWNSHIP
LAKE COUNTY, OHIO

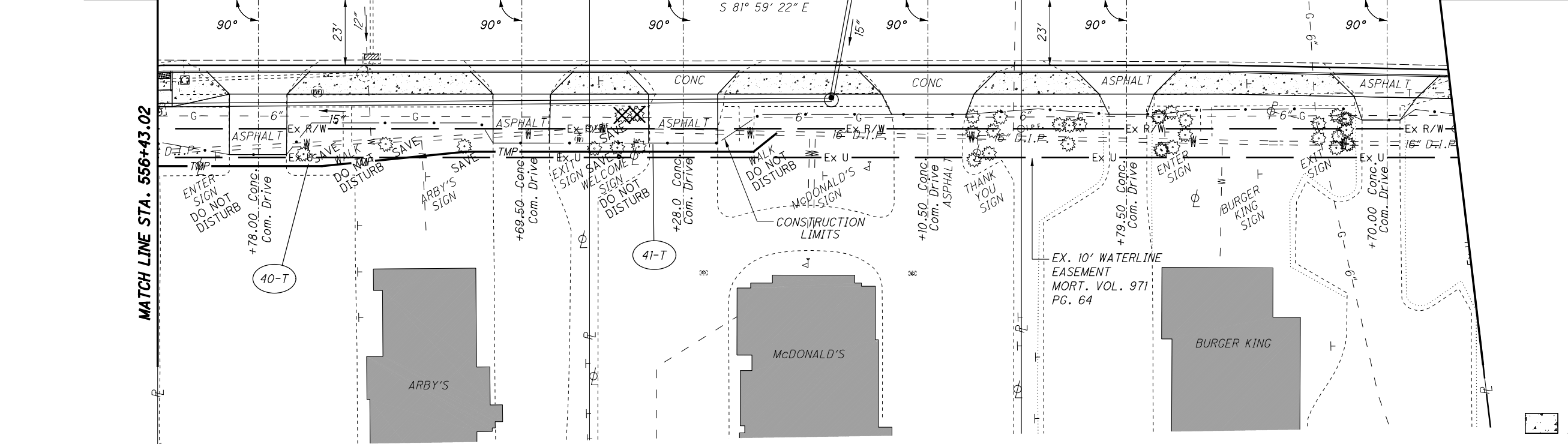
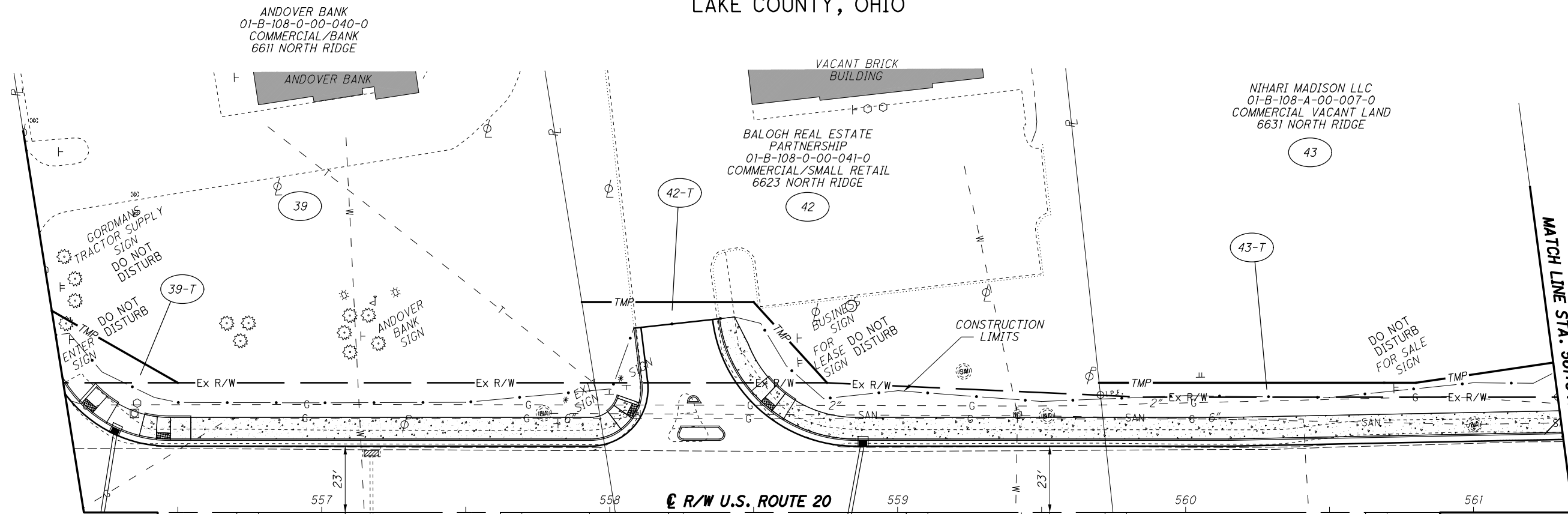


PID NO. **109270**
R/W DESIGNER: PSL
R/W REVIEWER: TMM

RIGHT OF WAY TOPO SHEET
STA. 556+43.02 TO STA. 561+34.54

LAK-US-20-24.99
PART 2

83/98
682
697



STRUCTURE KEY

	RESIDENTIAL
	COMMERCIAL
	OUT-BUILDING

ARBY'S PROPERTIES LLC
01-B-107-0-00-036-0
COMMERCIAL/RESTAURANT
6616 NORTH RIDGE

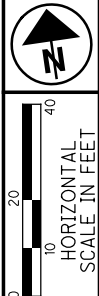
McDONALDS REAL ESTATE COMPANY
01-B-107-0-00-034-0
COMMERCIAL/RESTAURANT
6626 NORTH RIDGE

ARC BKMST41001 LLC
01-B-107-0-00-035-0
COMMERCIAL/RESTAURANT
6634 NORTH RIDGE

REV. BY	DATE	DESCRIPTION

H:\2018\180743\0D0T\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT03.dgn Sheet 7/15/2022 3:03:01PM Troyer

PART OF LOT NO. 1
TRACT NO.3
MADISON TOWNSHIP
LAKE COUNTY, OHIO



PID NO. 109270

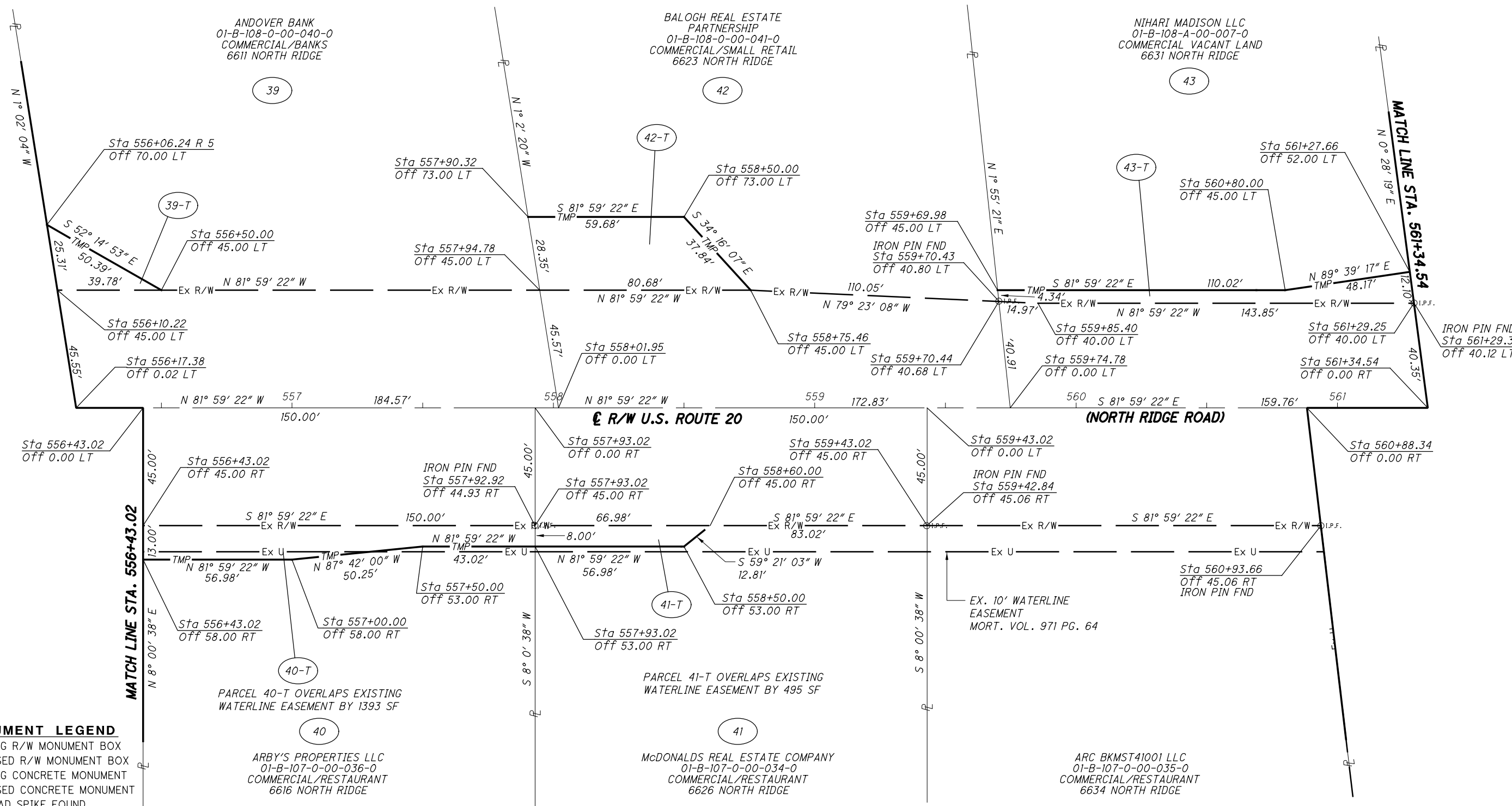
R/W DESIGNER PSL
R/W REVIEWER TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 556+43.02 TO STA. 561+34.54

LAK-US-20-24.99
PART 2

84/98

683
697



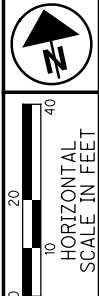
MONUMENT LEGEND

- ☐ EXISTING R/W MONUMENT BOX
- ▣ PROPOSED R/W MONUMENT BOX
- ▣ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⊙ RAILROAD SPIKE FOUND
- ⊙ RAILROAD SPIKE SET
- I.P.F. IRON PIN FOUND
- I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.S. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- ⊙ P.K.F. P.K. NAIL FOUND
- ⊙ P.K.S. P.K. NAIL SET

REV. BY	DATE	DESCRIPTION

H:\2018\180743\0D01\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB03.dgn Sheet 7/15/2022 3:03:02 PM troyer

PART OF LOT NO. 1
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO



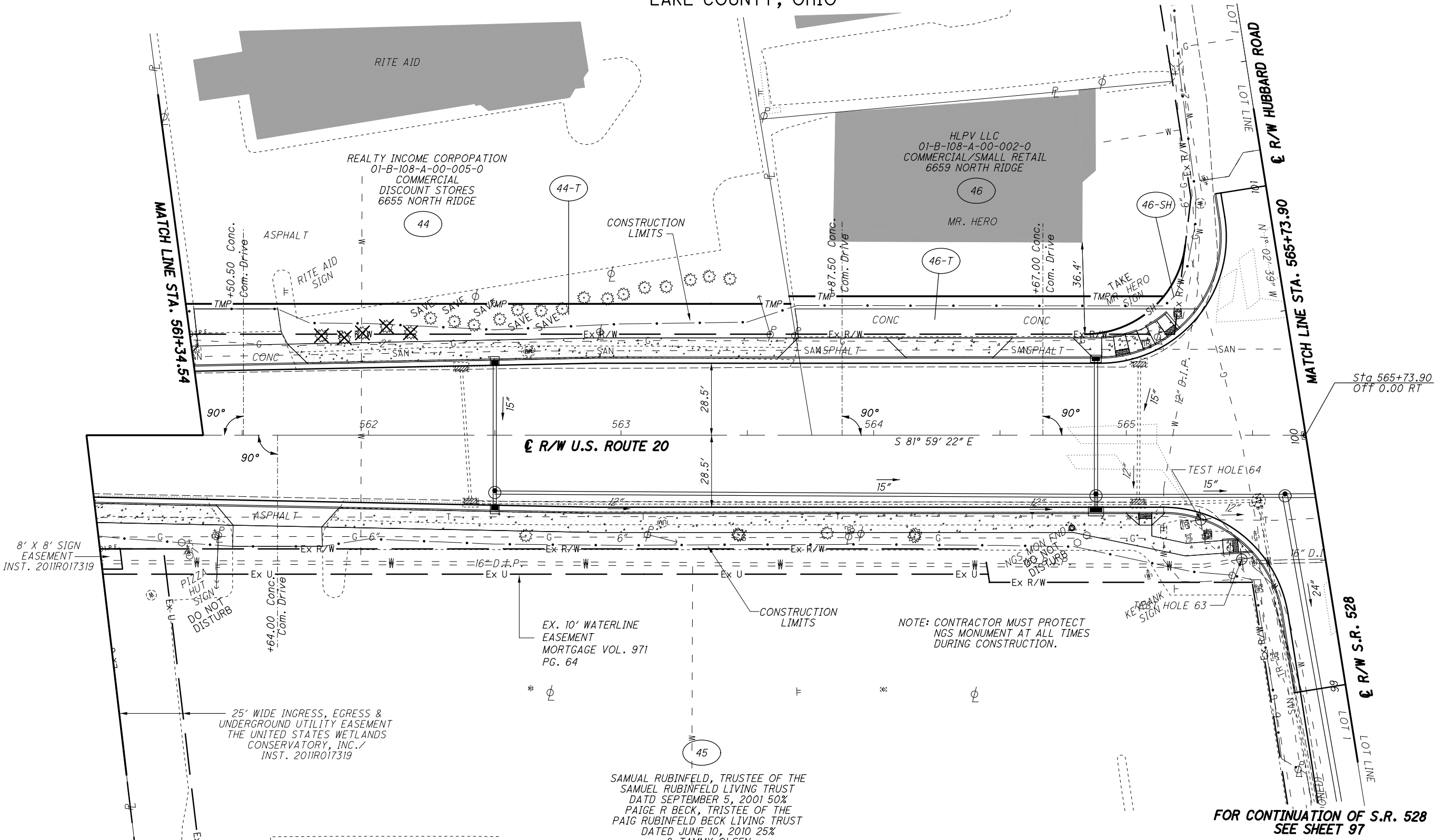
PID NO. **109270**
R/W DESIGNER: PSL
R/W REVIEWER: TMM

RIGHT OF WAY TOPO SHEET
STA. 561+34.54 TO STA. 565+73.90

LAK-US-20-24.99
PART 2

85/98

684
697



NOTE: CONTRACTOR MUST PROTECT NGS MONUMENT AT ALL TIMES DURING CONSTRUCTION.

SAMUAL RUBINFELD, TRUSTEE OF THE SAMUEL RUBINFELD LIVING TRUST DATED SEPTEMBER 5, 2001 50% PAIGE R BECK, TRISTEE OF THE PAIG RUBINFELD BECK LIVING TRUST DATED JUNE 10, 2010 25% & TAMMY OLSEN aka TAMMY ROCHELLE MCKENZIE, TRUSTEE OF THE TAMMY ROCHELLE MCKENZIE LIVING TRUST 25% 01-B-107-0-00-009-0 COMMERCIAL/SHOPPING CENTER 6640 NORTH RIDGE

STRUCTURE KEY

[White Box]	RESIDENTIAL
[Grey Box]	COMMERCIAL
[Hatched Box]	OUT-BUILDING

REV. BY	DATE	DESCRIPTION

FOR CONTINUATION OF S.R. 528
SEE SHEET 97

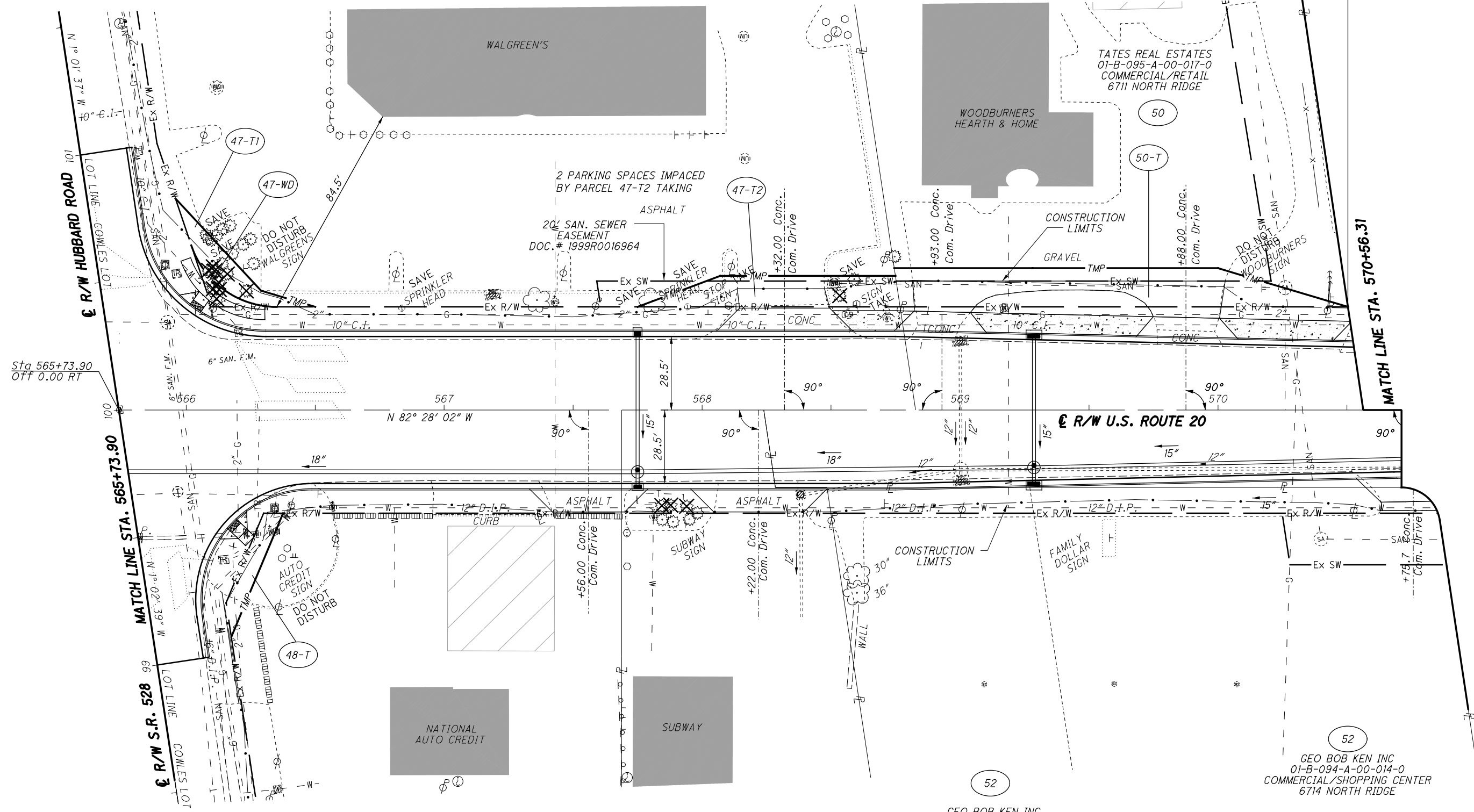
H:\2018\80743\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT032.dgn Sheet 7/15/2022 3:03:04 PM Troyer

PART OF COWLES LOT
TRACT NO. 2
MADISON TOWNSHIP
LAKE COUNTY, OHIO

GLENN L GNAGY, TRUSTEE
01-B-095-A-00-019-0
COMMERCIAL/DISCOUNT STORES
6707 NORTH RIDGE

47

END ACQUISITION
STA. 570+50.37



STRUCTURE KEY

	RESIDENTIAL
	COMMERCIAL
	OUT-BUILDING

48
WORLD PARTNERS LLC
01-B-094-A-00-029-0
COMMERCIAL/AUTOMOTIVE
6686 NORTH RIDGE

49
ADWAY LLC
01-B-094-A-00-016-0
COMMERCIAL/RETAIL
6700 NORTH RIDGE

52
GEO BOB KEN INC
01-B-094-A-00-015-0
COMMERCIAL VACANT LAND
NORTH RIDGE

52
GEO BOB KEN INC
01-B-094-A-00-014-0
COMMERCIAL/SHOPPING CENTER
6714 NORTH RIDGE

REV. BY	DATE	DESCRIPTION

SIDEWALK REPLACEMENT



PID NO.
109270

R/W DESIGNER
PSL
R/W REVIEWER
TMM

RIGHT OF WAY TOPO SHEET
STA. 565+73.90 TO STA. 570+56.31

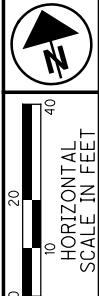
LAK-US-20-24.99
PART 2

87/98

686
697

H:\2018\80743\0D0T\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT033.dgn Sheet 7/15/2022 3:03:07 PM Troyer

PART OF COWLES LOT
TRACT NO. 2
MADISON TOWNSHIP
LAKE COUNTY, OHIO



PID NO.
109270

R/W DESIGNER
PSL

R/W REVIEWER
TMM

RIGHT OF WAY TOPO SHEET
STA. 570+55.47 TO STA. 575+00.00

LAK-US-20-24.99
PART 2

89/98

688
697

NORMAN B NICHOLS
& RUTH ANN NICHOLS
01-B-095-A-00-018-0
COMMERCIAL VACANT LAND
NORTH RIDGE

51

NICHOLS

BENDELEWSKI
MEATS

ELEANOR GERAK
01-B-094-A-00-027-0
COMMERCIAL
VACANT LAND
NORWOOD DRIVE

EKG MANAGEMENT LLC
01-B-094-A-00-005-0
COMMERCIAL/SMALL RETAIL
6720 NORTH RIDGE

STRUCTURE KEY

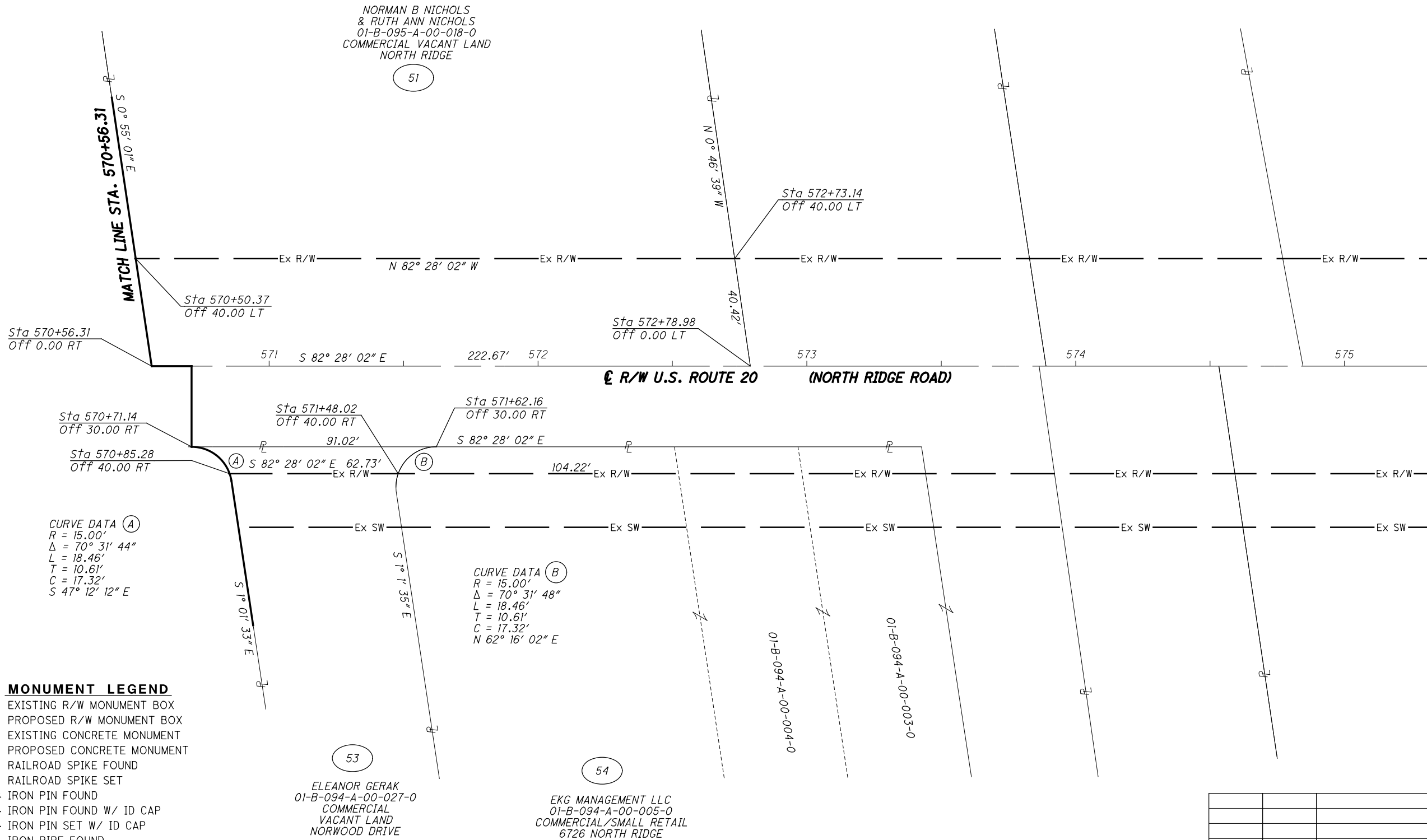
- RESIDENTIAL
- COMMERCIAL
- OUT-BUILDING

SIDEWALK REPLACEMENT

REV. BY	DATE	DESCRIPTION

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RT034.dgn Sheet 7/15/2022 3:03:09 PM Troyer

PART OF COWLES LOT
TRACT NO. 2
MADISON TOWNSHIP
LAKE COUNTY, OHIO



CURVE DATA (A)
 R = 15.00'
 Δ = 70° 31' 44"
 L = 18.46'
 T = 10.61'
 C = 17.32'
 S 47° 12' 12" E

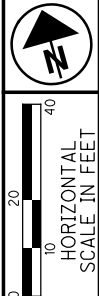
CURVE DATA (B)
 R = 15.00'
 Δ = 70° 31' 48"
 L = 18.46'
 T = 10.61'
 C = 17.32'
 N 62° 16' 02" E

- MONUMENT LEGEND**
- ☐ EXISTING R/W MONUMENT BOX
 - ▣ PROPOSED R/W MONUMENT BOX
 - EXISTING CONCRETE MONUMENT
 - PROPOSED CONCRETE MONUMENT
 - ⚡ RAILROAD SPIKE FOUND
 - ⚡ RAILROAD SPIKE SET
 - I.P.F. IRON PIN FOUND
 - I.P.F. IRON PIN FOUND W/ ID CAP
 - I.P.S. IRON PIN SET W/ ID CAP
 - I.P.F. IRON PIPE FOUND
 - I.P.S. IRON PIPE SET
 - P.K.F. P.K. NAIL FOUND
 - P.K.S. P.K. NAIL SET

53
 ELEANOR GERAK
 01-B-094-A-00-027-0
 COMMERCIAL
 VACANT LAND
 NORWOOD DRIVE

54
 EKG MANAGEMENT LLC
 01-B-094-A-00-005-0
 COMMERCIAL/SMALL RETAIL
 6726 NORTH RIDGE

NORMAN B NICHOLS
 & RUTH ANN NICHOLS
 01-B-095-A-00-018-0
 COMMERCIAL VACANT LAND
 NORTH RIDGE



PID NO.
109270

R/W DESIGNER
 PSL
 R/W REVIEWER
 TMM

RIGHT OF WAY BOUNDARY SHEET
STA. 570+56.31 TO STA. 575+00.00

LAK-US-20-24.99
PART 2

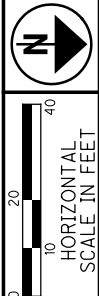
90/98

689
 697

REV. BY	DATE	DESCRIPTION

H:\2018\180743\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB034.dgn Sheet 7/15/2022 3:03:10 PM Troyer

PART OF LOT NO. 2
TRACT NO. 4
MADISON TOWNSHIP
LAKE COUNTY, OHIO



PID NO.
109270

R/W DESIGNER
PSL

R/W REVIEWER
TMM

RIGHT OF WAY BOUNDARY SHEET
STORM OUTLET STA. 475+89.06

LAK-US-20-24.99
PART 2

92/98

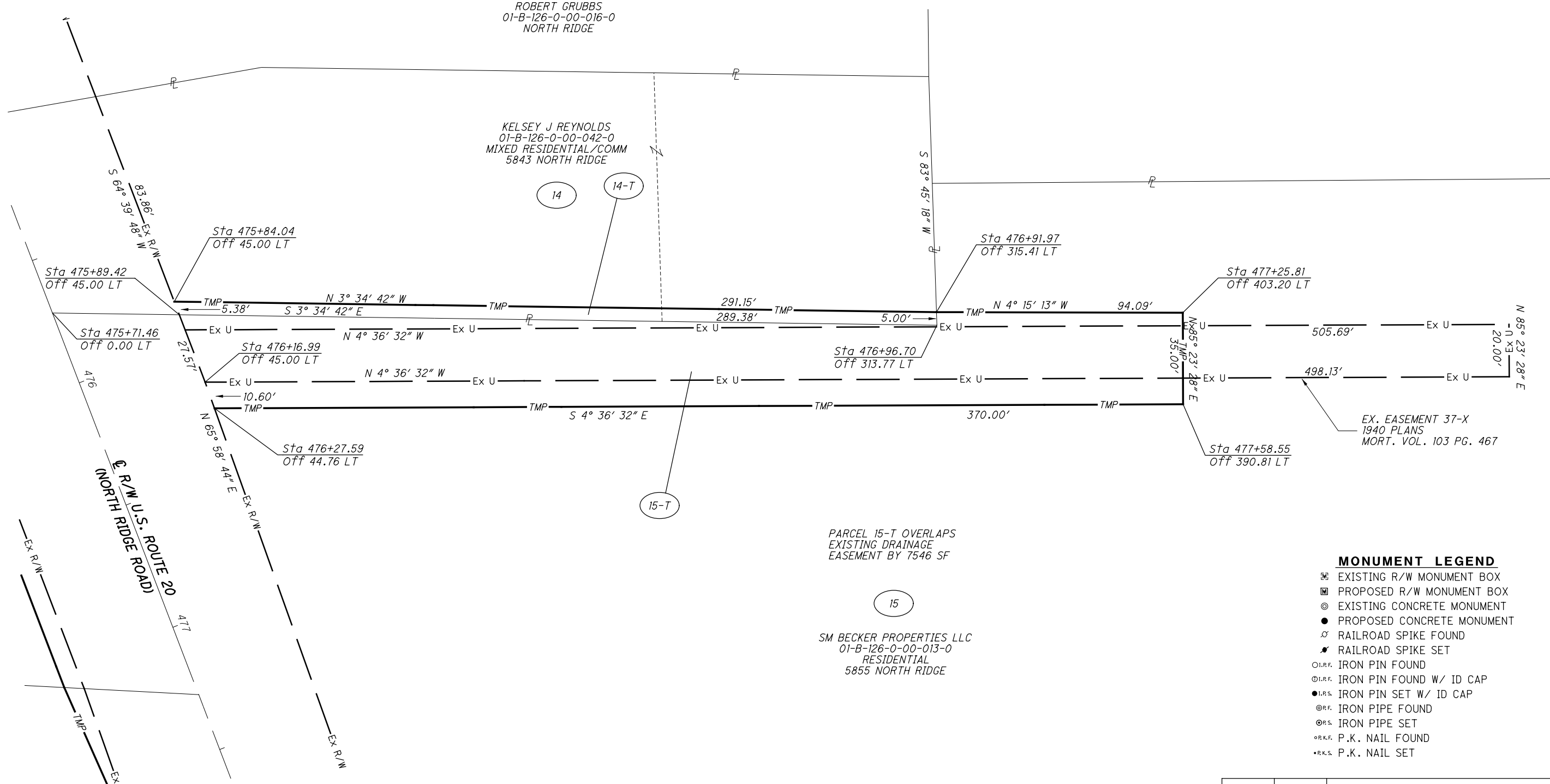
691
697

ROBERT GRUBBS
01-B-126-0-00-016-0
NORTH RIDGE

KELSEY J REYNOLDS
01-B-126-0-00-042-0
MIXED RESIDENTIAL/COMM
5843 NORTH RIDGE

PARCEL 15-T OVERLAPS
EXISTING DRAINAGE
EASEMENT BY 7546 SF

SM BECKER PROPERTIES LLC
01-B-126-0-00-013-0
RESIDENTIAL
5855 NORTH RIDGE



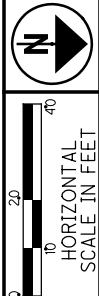
MONUMENT LEGEND

- ☐ EXISTING R/W MONUMENT BOX
- ▣ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- I.R.F. IRON PIN FOUND
- I.R.F. IRON PIN FOUND W/ ID CAP
- I.R.S. IRON PIN SET W/ ID CAP
- ⊙ I.R.F. IRON PIPE FOUND
- ⊙ I.R.S. IRON PIPE SET
- ⊙ R.K.F. P.K. NAIL FOUND
- R.K.S. P.K. NAIL SET

REV. BY	DATE	DESCRIPTION

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB035.dgn Sheet 7/15/2022 3:03:13 PM troyer

PART OF LOT NO. 7
TRACT NO. 3
MADISON TOWNSHIP
LAKE COUNTY, OHIO



PID NO. **109270**
R/W DESIGNER PSL
R/W REVIEWER TMM

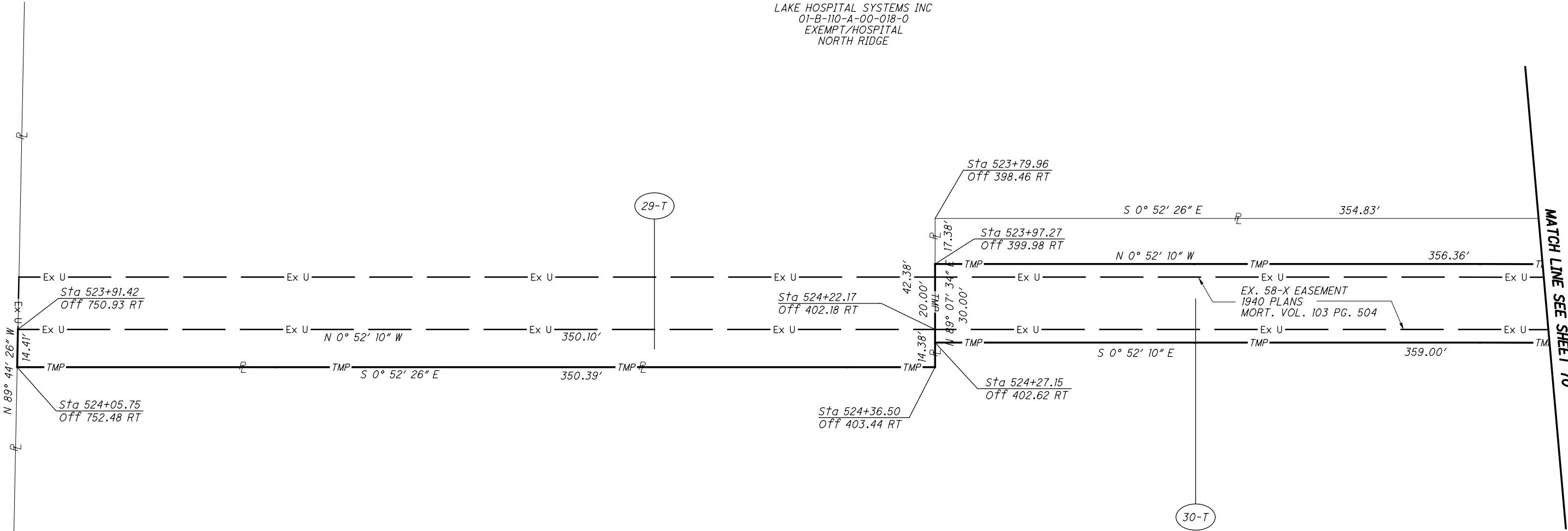
RIGHT OF WAY BOUNDARY SHEET
STORM OUTLET STA. 524+15.06

LAK-US-20-24.99
PART 2

94/98
693
697

29
LAKE HOSPITAL SYSTEMS INC
01-B-110-A-00-018-0
EXEMPT/HOSPITAL
NORTH RIDGE

30
MADISON ROUTE 20
PROPERTIES LLC
01-B-110-A-00-004-0
COMMERCIAL/AUTO SALES
6300 NORTH RIDGE



MONUMENT LEGEND

- ▣ EXISTING R/W MONUMENT BOX
- ▣ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- P.K.F. P.K. NAIL FOUND
- P.K.S. P.K. NAIL SET

REV. BY	DATE	DESCRIPTION

H:\2018\180743\000T\ProjectData\09270_LAK_20_EAST\Design\RW\Sheets\09270_RB036.dgn Sheet 7/15/2022 3:03:15 PM troyer

