

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

ALL-30-14.93
MONROE TWP
ALLEN COUNTY

PROJECT DESCRIPTION

THIS STRUCTURE IS LOCATED OVER U.S. 30 IN ALLEN COUNTY. REPAIR DAMAGED BRIDGE (RESULT OF OVER-HEIGHT VEHICLE HIT) BY REPLACING PORTION OF EXTERIOR BEAM, CROSSFRAMES, PORTION OF DECK, PARAPET, SCUPPERS, AND ROCKER BEARINGS. HEAT STRAIGHTEN INTERIOR BEAM. PAINT REPLACEMENT BEAM, CROSSFRAMES, AND DAMAGED HEAT STRAIGHTENED BEAM.

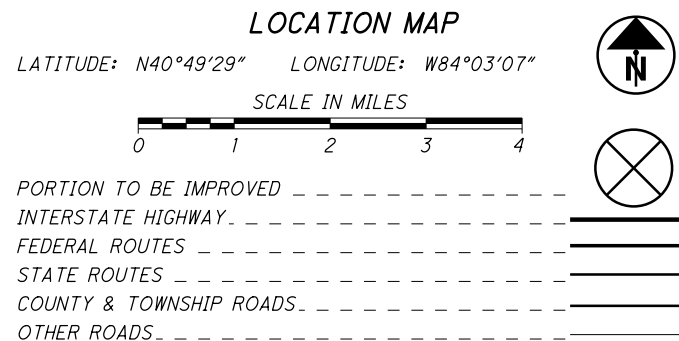
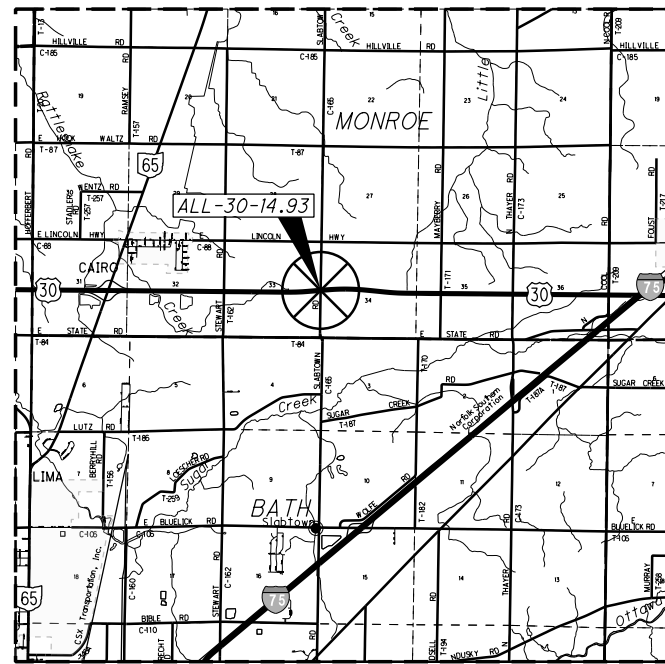
PROJECT EARTH DISTURBED AREA: 0.00 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 0.25 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: N/A (NOI NOT REQUIRED) *

* = MAINTENANCE PROJECT

2019 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT DETOURS WILL BE PROVIDED AS INDICATED ON SHEET 3.



DESIGN DESIGNATION (U.S. 30)

CURRENT ADT (2021)	12000
DESIGN YEAR ADT (2041)	16800
DESIGN HOURLY VOLUME (2041)	1270
DIRECTIONAL DISTRIBUTION	0.51
TRUCKS (24 HOUR B&C)	0.48
DESIGN SPEED	75 MPH
LEGAL SPEED	70 MPH
DESIGN FUNCTIONAL CLASSIFICATION:	
03 OTHER PRINCIPAL ARTERIAL (RURAL)	
NHS PROJECT	YES

DESIGN EXCEPTIONS

NONE REQUIRED

UNDERGROUND UTILITIES
Contact Two Working Days Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non-members must be called directly)

PLAN PREPARED BY:
OHIO DEPARTMENT OF TRANSPORTATION
DISTRICT ONE
LIMA, OHIO

ENGINEERS SEAL:

SIGNED: *Mark A. Limbaugh*
DATE: 2/12/21

INDEX OF SHEETS:

TITLE SHEET	1
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STRUCTURE OVER 20 FOOT SPAN	6-14A

STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
GSD-1-19	1/18/19			800 1/15/21	ASBESTOS
BR-1-67	2/1/69			832 10/19/18	REPORT
RB-1-55	7/19/13			849 1/18/13	1/11/21
SD-1-65	11/8/65				
MT-95.30	7/19/19				
MT-95.45	1/17/20				
MT-95.50	7/21/17				
MT-95.60	4/19/19				
MT-101.60	1/17/20				
MT-105.10	1/17/20				

APPROVED *Christopher A. Hughes*
DATE 02/12/2021 DISTRICT DEPUTY DIRECTOR

APPROVED _____
DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION

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FEDERAL PROJECT NO. **NON-FEDERAL**
CONSTRUCTION PROJECT NO. **114446**
RAILROAD INVOLVEMENT **NONE**
ALL-30-14.93
1/14

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ITEM 614, MAINTAINING TRAFFIC

A MINIMUM OF ONE (1) ELEVEN (11) FOOT LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES ON U.S.R. 30 EXCEPT FOR ONE PERMITTED NIGHT TIME, SHORT-TERM CLOSURE OF WESTBOUND U.S.R. 30 AS STIPULATED PER STANDARD CONSTRUCTION DRAWING MT-99.60 FOR INSTALLATION OF THE PROPOSED FASCIA BEAM OVER WESTBOUND U.S.R. 30.

THE SHORT-TERM CLOSURE SHALL FALL WITHIN THE HOURS NOTED IN SCD MT-99.60 AND THE DURATION SHALL NOT EXCEED THE DURATION NOTED IN SCD MT-99.60. SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127) AND THE LANE VALUE CONTRACT TABLE ON SHEET 4.

SLABTOWN ROAD IS CLOSED TO TRAFFIC. THE SLABTOWN ROAD BRIDGE REPAIR WORK IS CRITICAL WORK FOR THE REOPENING OF SLABTOWN ROAD TO TRAFFIC. THE CONTRACTOR WILL BE ASSESSED A DISINCENTIVE DAILY PER THE FLEXIBLE START WINDOW CONTRACT (PN 129) AND THE WINDOW CONTRACT TABLE ON SHEET 4.

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48 X 30 INCH ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AS DETAILED IN SCD MT-101.60 AT THE VARIOUS EXISTING SIGN LOCATIONS DURING PERIODS IN WHICH SLABTOWN ROAD REMAINS CLOSED TO TRAFFIC. PER THE DIRECTION OF THE PROJECT ENGINEER, THE CONTRACTOR SHALL REMOVE THE EXISTING ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AND RETURN THEM TO THE DEPARTMENT OR COORDINATE THE EXISTING REMOVALS WITH THE DEPARTMENT'S FORCES.

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS AND SIGN SUPPORTS, AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AND TYPE III BARRICADES OF THE TYPE AND LOCATION AS ALREADY BEEN ESTABLISHED ALONG SLABTOWN ROAD. PER THE DIRECTION OF THE PROJECT ENGINEER, THE CONTRACTOR SHALL REMOVE THE EXISTING ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AND RETURN THEM TO THE DEPARTMENT OR COORDINATE THE EXISTING REMOVALS WITH THE DEPARTMENT FORCES.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS ON U.S.R. 30 SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATED WITH THE WORK IN PROGRESS.

IN THE EVENT OF A SIGNIFICANT LATE SNOWFALL AND AT THE DIRECTION OF THE ENGINEER, BOTH LANES OF U.S.R. 30 SHALL BE OPENED TO THRU TRAFFIC UNTIL WEATHER CONDITIONS IMPROVE. UPON NOTIFICATION BY THE ENGINEER, THE CONTRACTOR HAS TWO (2) HOURS TO REMOVE THE ZONE. IF THE ZONE REMAINS IN PLACE BEYOND TWO (2) HOURS, A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$1,125 FOR EACH FIFTEEN (15) MINUTES THAT A LANE OF U.S.R. 30 REMAINS CLOSED TO THRU TRAFFIC.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ERECTING AND MAINTAINING SAFE AND EFFECTIVE TRAFFIC CONTROL 24 HOURS A DAY FOR THE DURATION OF THIS PROJECT. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED,

ITEM 614, MAINTAINING TRAFFIC (CONTINUED)

AND REMOVED BY THE CONTRACTOR. THIS CONSISTS OF NOTIFYING THE OHIO STATE PATROL AFTER ENCOUNTERING ANY ACCIDENTS OR DISABLED VEHICLES OR OBJECTS HINDERING THE FLOW OF TRAFFIC.

THE CONTRACTOR SHALL DESIGNATE TO THE ENGINEER, A PERSON RESPONSIBLE FOR MAINTENANCE OF TRAFFIC CONTROL DURING NON-WORK HOURS WHO SHALL BE AVAILABLE ON SITE WITHIN THIRTY (30) MINUTES AFTER NOTIFICATION.

UNLESS PHYSICALLY IMPOSSIBLE, ALL CONSTRUCTION EQUIPMENT SHALL EXIT ALL WORK ZONES FROM THE DOWNSTREAM END OF THE WORK ZONE OR BY INTERCHANGE RAMPS.

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE		
ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
RAMP &	>=2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
ROAD	> 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
CLOSURES	<= 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONTINUED)

OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 100 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

LANE CLOSURES

THE CONTRACTOR SHALL PROVIDE AN ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN IN ADVANCE WARNING OF ANY LANE CLOSURES ON U.S.R. 30. SEE NOTE ON SHEET 4.

ITEM 614, REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ECT.

AN ESTIMATED QUANTITY OF 1 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

ITEM 614, REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 5 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

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ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PCMS SHALL BE LOCATED 2 MILES BEFORE TAPERS FOR FOR LANE CLOSURES ON BOTH U.S.R. 30 AND 224. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL

TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE, AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN - 6 SIGN MONTH ASSUMING 6 PCMS SIGNS

REMOVAL OF PAVEMENT MARKINGS

THE CONTRACTOR SHALL COVER ALL CONFLICTING PAVEMENT MARKINGS ALONG THE WORK ZONE WITH BLACKOUT TAPE PER C&MS 614.11.B. THE LINE TYPE MAY VARY THROUGHOUT THE ZONE. PAYMENT TO COVER THESE MARKINGS AND ANY OTHER CONFLICTING MARKINGS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

WORK ZONE MARKINGS AND SIGNS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

ITEM 614, WORK ZONE EDGE LINE, CLASS I, 6", 740.06, TYPE 1 - 2.05 MI

ITEM 614, WORK ZONE DOTTED LINE, CLASS I, 6", 740.06, TYPE 1 - 420 FT

WINDOW CONTRACT TABLE

USE THE FOLLOWING TABLE AS REFERRED TO IN THE PROPOSAL:

DESCRIPTION OF CRITICAL WORK	CALENDAR DAYS TO COMPLETE	DISINCENTIVE \$ PER DAY	WORK WINDOW START	WORK WINDOW END
COMPLETE ALL BRIDGE REPAIR WORK TO BRIDGE NO. ALL-30-1493 AND HAVE SLABTOWN ROAD OPEN TO UNRESTRICTED TRAFFIC.	70 DAYS	\$2,000 PER DAY	4/15/2021	8/1/2021
ALL WORK ON PROJECT (INCLUDING WORK LISTED ABOVE)	80 DAYS	PER C&MS 108.07	CONTRACT EXECUTION DATE	PROJECT COMPLETION DATE

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	4 CALENDAR DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

LANE VALUE CONTRACT TABLE

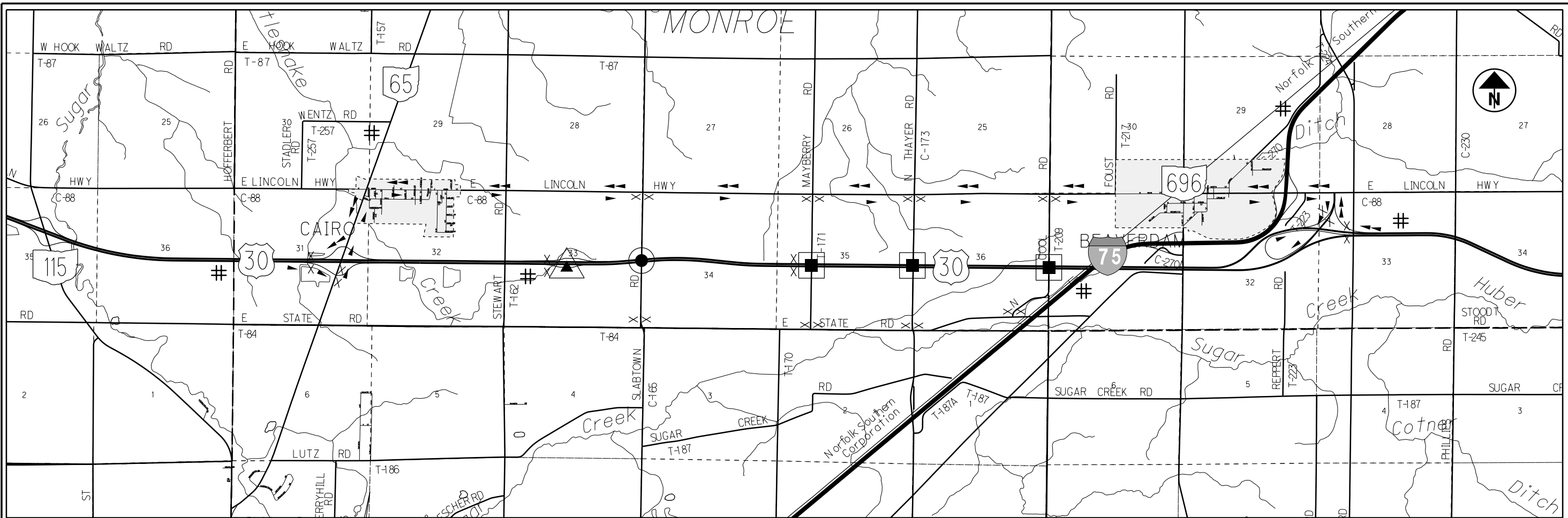
DESCRIPTION OF CRITICAL LANE(S) TO BE MAINTAINED	RESTRICTED TIME PERIOD	TIME UNIT	DISINCENTIVE \$ PER TIME UNIT
MINIMUM OF 1 LANE OF EB & WB ALL-US 30	5 AM - 12 AM PER SCD MT-99.60	EACH FIVE (5) MINUTES	\$375
MINIMUM OF 1 LANE OF EB & WB ALL-US 30	OUTSIDE 15 MINUTE PERMITTED SHORT-TERM CLOSURE PER SCD MT-99.60	EACH FIVE (5) MINUTES	\$375

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

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ITEM 614, MAINTAINING TRAFFIC (FOR REPLACING TEMPORARY BEARINGS WITH PERMANENT BEARINGS)

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION ON U.S.R. 30 AND SLABTOWN ROAD (ONCE OPEN TO TRAFFIC) SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A SINGLE PERIOD NOT TO EXCEED 12 AM THROUGH 5 AM ON A SUNDAY, WHEN THROUGH TRAFFIC MAY BE DETOURED AS SHOWN ON THIS SHEET FOR REPLACING THE TEMPORARY BEARINGS ON BRIDGE NO. ALL-30-1493 WITH PERMANENT BEARINGS. U.S.R. 30 AND SLABTOWN ROAD SHALL NOT BE OPEN TO TRAFFIC DURING THE JACKING OF THE BRIDGE AND WHEN SWITCHING OUT THE TEMPORARY BEARINGS WITH THE PERMANENT BEARINGS. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$700 EACH 15 MINUTES THE ROADWAYS REMAIN CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

TO COORDINATE THE CLOSURE OF EASTBOUND (EB) U.S.R. 30, THE EB U.S.R. 30 REST AREA LOCATED BETWEEN THE U.S.R. 30 INTERCHANGE WITH S.R. 65 AND BRIDGE NO. ALL-30-1493 NEEDS TO BE CLOSED AT 5 PM PRIOR TO THE 12 AM CLOSURE. THE CONTRACTOR AND PROJECT ENGINEER SHALL CONFIRM NO VEHICLES ARE PRESENT IN THE EB U.S.R. 30 REST AREA WHEN THE CLOSURE OF EB U.S.R. 30 STARTS. THE EB U.S.R. 30 REST AREA SHALL REOPEN ONCE EB U.S.R. 30 IS OPEN TO TRAFFIC.

THE CLOSURE OF U.S.R. 30 AND THE EB U.S.R. 30 REST AREA SHALL BE INSTALLED BY THE CONTRACTOR PER STANDARD CONSTRUCTION DRAWING (SCD) MT-99.50. UPON THE APPROVAL OF THE ENGINEER, SCD MT-99.60 SHALL BE USED DURING THE SET UP OF THE CLOSURES ALONG U.S.R. 30.

ADDITIONAL CLOSURES OF THROUGH TRAFFIC ON ADJACENT RAMPS AND LOCAL ROADWAYS SHALL BE REQUIRED. THESE ADDITIONAL CLOSURE POINTS ARE NOTED ON THIS SHEET AND SHALL BE PROVIDED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER.

ITEM 614, MAINTAINING TRAFFIC (FOR REPLACING TEMPORARY BEARINGS WITH PERMANENT BEARINGS - CONTINUED)

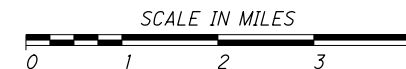
ACCESS TO RESIDENCES AND PROPERTY ALONG THESE LOCAL ROADWAY SHALL BE MAINTAINED BY THE CONTRACTOR DURING THE SUBJECT CLOSURE OF U.S.R. 30 AND SLABTOWN ROAD.

THE DETOUR FOR U.S.R. 30 SHALL BE ESTABLISHED, MAINTAINED AND SUBSEQUENTLY REMOVED BY THE STATE OF OHIO.

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48 X 30 INCH ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AS DETAILED IN SCD MT-101.60 AT THE LOCATIONS NOTED ON THIS SHEET DURING PERIODS IN WHICH THE THE AFFECTED ROADS ARE CLOSED TO TRAFFIC.

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS AND SIGN SUPPORTS, AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AND TYPE III BARRICADES AT LOCATIONS AS NOTED ON THIS SHEET AND REQUIRED BY THESE PLANS AND PERTINENT STANDARD CONSTRUCTION DRAWINGS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS NEEDED TO PROVIDE THE ABOVE NOTED CLOSURES SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.



LEGEND

- ◄ - WESTBOUND (WB) U.S.R. 30 DETOUR ROUTE
- - EASTBOUND (EB) U.S.R. 30 DETOUR ROUTE
- # - PORTABLE CHANGABLE MESSAGES SIGNS
- - PROJECT ALL-30-14.93; PID 114446
- - PROJECT ALL-30-16.19; PID 111375
- ▲ - EB U.S.R. 30 REST AREA CLOSURE
- X - CLOSURE POINTS/BARRICADES (TYPE III)
- - EB LANES OF U.S.R. 30 AT INTERCHANGE WITH S.R. 65
- - EB U.S.R. 30 ENTRANCE RAMP FROM S.R. 65
- - EB ENTRANCE RAMP TO EB REST AREA
- - WB LANES OF U.S.R. 30 AT INTERCHANGE WITH S.R. 696
- - WB U.S.R. 30 ENTRANCE RAMP FROM S.R. 696
- - WB CLOSURE OF U.S.R. 30 AT MAYBERRY RD.
- - CLOSURE OF SLABTOWN RD. AT LINCOLN HIGHWAY (C.R. 88)
- - CLOSURE OF MAYBERRY RD. AT LINCOLN HIGHWAY (C.R. 88)
- - CLOSURE OF THAYER RD. AT LINCOLN HIGHWAY (C.R. 88)
- - CLOSURE OF COOL RD. AT LINCOLN HIGHWAY (C.R. 88)
- - CLOSURE OF SLABTOWN RD. AT STATE RD. (T.R. 84)
- - CLOSURE OF MAYBERRY RD. AT STATE RD. (T.R. 84)
- - CLOSURE OF THAYER RD. AT STATE RD. (T.R. 84)
- - CLOSURE OF COOL RD. AT STATE RD. (T.R. 84)

CALCULATED EJS CHECKED MAL
MAINTENANCE OF TRAFFIC - CLOSURE OF EB & WB U.S.R. 30
ALL-30-14.93
 4A
 14

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SHEET NUM.				PART.	ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
2	3	4	7	01/NFP/BR						
									<i>EROSION CONTROL</i>	
1,000				1,000	832	30000	1,000	EACH	EROSION CONTROL	2
									<i>STRUCTURE OVER 20 FOOT SPAN (ALL-30-14.93)</i>	
			LS	LS	202	11201	LS		PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	8
			34,917	34,917	509	10001	34,917	LB	EPOXY COATED REINFORCING STEEL, AS PER PLAN	8
			117	117	511	34444	117	CY	CLASS QC2 CONCRETE, BRIDGE DECK	
			27	27	511	34448	27	CY	CLASS QC2 CONCRETE, BRIDGE DECK (PARAPET)	
			197	197	512	10100	197	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)	
			6	6	512	10300	6	SY	SEALING CONCRETE BRIDGE DECKS WITH HMWM RESIN	
			17,600	17,600	513	10261	17,600	LB	STRUCTURAL STEEL MEMBERS, LEVEL 3, AS PER PLAN	8
			1	1	513	21001	1	EACH	TRIMMING OF BEAM END, AS PER PLAN	9
			131	131	514	00050	131	SF	SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL	
			131	131	514	00056	131	SF	FIELD PAINTING OF EXISTING STRUCTURAL STEEL, PRIME COAT	
			1,234	1,234	514	00060	1,234	SF	FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT	
			1,234	1,234	514	00066	1,234	SF	FIELD PAINTING STRUCTURAL STEEL, FINISH COAT	
			2	2	514	10000	2	EACH	FINAL INSPECTION REPAIR	
			2	2	516	46200	2	EACH	BEARING DEVICE, ROCKER	
			5	5	516	46700	5	EACH	RESET BEARING	
			LS	LS	516	46930	LS		BEARING DEVICE, MISC.:TEMPORARY BEARINGS	14A
			LS	LS	516	47000	LS		JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE	
			4	4	518	12301	4	EACH	SCUPPERS, INCLUDING SUPPORTS, AS PER PLAN	9
			4	4	614	13310	4	EACH	BARRIER REFLECTOR, TYPE 1 (BI-DIRECTIONAL)	
			0.03	0.03	642	00300	0.03	MILE	CENTER LINE, TYPE 1	
			LS	LS	849	10000	LS		DAMAGE ASSESSMENT	
			LS	LS	849	10500	LS		SURFACE PREPARATION	
			7	7	849	10600	7	HOUR	REPAIRING DAMAGED MEMBERS BY GRINDING	
			LS	LS	849	10700	LS		STRAIGHTENING DAMAGED MEMBERS	
									<i>MAINTENANCE OF TRAFFIC</i>	
	100			100	614	11110	100	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
	1			1	614	12500	1	EACH	REPLACEMENT SIGN	
	1			1	614	12600	1	EACH	REPLACEMENT DRUM	
		6		6	614	18601	6	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	4
		2.05		2.05	614	22210	2.05	MILE	WORK ZONE EDGE LINE, CLASS I, 6", 740.06, TYPE I	
		420		420	614	24402	420	FT	WORK ZONE DOTTED LINE, CLASS I, 6", 740.06, TYPE I	
									<i>INCIDENTALS</i>	
				LS	614	11000	LS		MAINTAINING TRAFFIC	3
				3	619	16010	3	MNTH	FIELD OFFICE, TYPE B	
				LS	623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING	
				LS	624	10000	LS		MOBILIZATION	

CALCULATED	MAL	CHECKED	TAB
GENERAL SUMMARY			
ALL - 30 - 14.93			
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