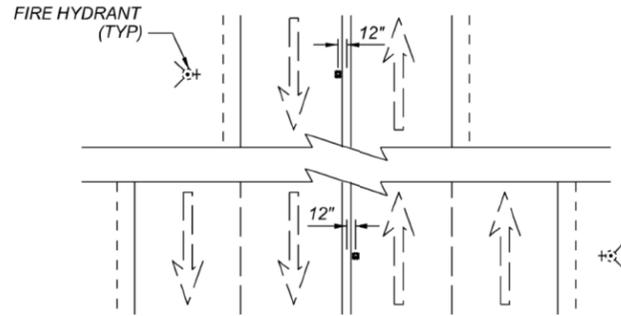


**RPMs FOR FIRE HYDRANTS (TWO- AND FOUR-LANE UNDIVIDED)**

INSTALL ONE BLUE/BLEU RAISED PAVEMENT MARKER ALONG THE CENTERLINE OF THE ROADWAY IN FRONT OF ALL FIRE HYDRANTS WITHIN THE PROJECT LIMITS AS LISTED ON THE RPM SUBSUMMARY. OFFSET THESE RPMs A DISTANCE OF 12 INCHES, MEASURED FROM THE CENTER OF THE CENTERLINE TO THE CENTER OF THE RPM, LATERALLY FROM THE CENTERLINE OF THE ROADWAY, TOWARD THE MARKED FIRE HYDRANT. SEE THE DETAIL BELOW AS INFORMATION. THESE RPMs SHOULD BE ADDED IN ADDITION TO THE STANDARD LONG LINE MARKING PLAN; THEY SHOULD NOT REPLACE ANY YELLOW, RED, OR WHITE MARKINGS.

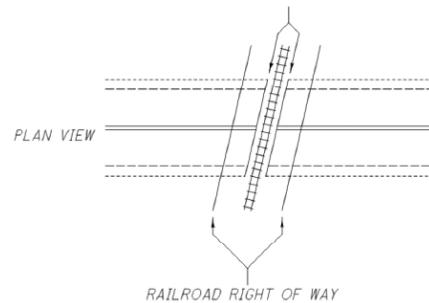


**PAVING AT RAILROAD CROSSING**

WORK THE CROWN OUT OF THE PROPOSED PAVEMENT ON EACH SIDE OF THE RAILROAD CROSSING, BEGINNING 50 FEET FROM THE NEAREST RAIL, BY RAISING THE EDGES OF THE NEW PAVEMENT TO MEET THE PLATFORM ELEVATION.

**DETAIL - PAVING AT RAILROAD CROSSING**

BUTT JOINT/BEGIN AND END RESURFACING



NOTE:

- 1.) DO NOT DISTURB RAILROAD GATES
- 2.) RE-INSTALL PAVEMENT MARKINGS
- 3.) RAILROAD MAY DIRECT ENGINEER ON THE LOCATION OF BUTT JOINTS. OTHERWISE OMIT AND RESUME RESURFACING AT THE EDGE OF THE EXISTING CROSSING SURFACE ON BOTH SIDES OF THE TRACK.

**ITEM 900 - SPECIAL - RAILROAD COORDINATION**

THIS PROJECT INCLUDES WORK WITHIN THE RIGHT OF WAY OF ONE OR MORE RAILWAYS. THE CONTRACTOR SHALL COMPLETE AND SECURE ALL INSURANCE AND DOCUMENTATION REQUIREMENTS WITH EACH AFFECTED RAILROAD ON THE PROJECT. SEE THE PROJECT PROPOSAL FOR ADDITIONAL INFORMATION AND REQUIREMENTS AS PART OF THE RAILROAD AGREEMENT AND/OR RAILROAD CLAUSES INCLUDED WITH THE PROJECT.

RAILROAD FLAGGING WILL BE REQUIRED BY THE RAILWAY(S) WHEN ANY PROJECT OPERATIONS ARE BEING COMPLETED WITHIN RAILROAD RIGHTS OF WAY. IN ORDER TO LIMIT THE RISK OF DELAYS IN THE PROJECT SCHEDULE, THE CONTRACTOR SHALL MAKE EFFORTS TO SCHEDULE A RAILROAD FLAGGER WITH THE AFFECTED RAILROAD(S) AS EARLY AS POSSIBLE, PREFERABLY WITHIN TWO WEEKS OF THE AWARD OF THE CONTRACT. THE PROJECT PROPOSAL AND/OR RAILROAD CLAUSES LISTS THE RAILWAY-REQUESTED MINIMUM NOTIFICATION REQUIREMENTS FOR EACH AFFECTED RAILROAD. IF IT IS DETERMINED BY THE ENGINEER THAT THE CONTRACTOR DID NOT NOTIFY THE RAILROAD WITHIN THE REQUIRED TIMEFRAME TO SECURE A RAILROAD FLAGGER, AND THUS INCURS A DELAY IN PROJECT OPERATIONS, ANY DELAY CLAIMS MADE BY THE CONTRACTOR AS A RESULT OF DELAYED COORDINATION WITH SUCH RAILROAD(S) WILL BE DENIED.

IT MAY BE NECESSARY TO COMPLETE WORK WITHIN RAILROAD RIGHTS OF WAY UNDER A SEPARATE PHASE FROM THE REMAINDER OF THE PROJECT PENDING THE AVAILABILITY OF RAILROAD FLAGGING.

PAYMENT FOR ALL REMOBILIZATION OR IMPACTS DUE TO REVISIONS OF SCHEDULES SECONDARY TO DELAYS CAUSED BY THE LACK OF AVAILABILITY OF RAILROAD FLAGGING SERVICES, IN ACCORDANCE WITH THE INFORMATION SHOWN ABOVE, INCLUDING ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THIS REMOBILIZATION OR SCHEDULE REVISION, NOT INCLUDING THE ACTUAL COST OF RAILROAD FLAGGING SERVICES, IS TO BE MADE UNDER THE CONTRACT LUMP SUM BID PRICE FOR ITEM 900 - SPECIAL - RAILROAD COORDINATION, UNLESS SEPARATELY ITEMIZED IN THIS PLAN AND PROPOSAL.

**RAILROAD COORDINATION WITH CSXT**

IF ANY ISSUE OR INCIDENT OCCURS WITHIN CSXT ROW, PLEASE CONTACT THE CSXT PUBLIC SAFETY COORDINATION CENTER AT 800-232-0144.

CSXT ROW LIMITS ARE DEFINED AS:  
 APPROXIMATELY 50' NORTH OF THE CENTERLINE OF THE NORTHERNMOST TRACK  
 APPROXIMATELY 60' SOUTH OF THE CENTERLINE OF THE SOUTHERNMOST TRACK

ROADWAY FLAGGERS MUST BE PRESENT ON EACH SIDE OF THE RAILROAD CROSSING ANYTIME TRAFFIC IS DIRECTED INTO OPPOSING TRAFFIC LANES AT THE RAILROAD CROSSING.

CHANNELIZATION/MOT DEVICES SHALL NOT BE PLACED WITHIN 15' FROM THE CENTERLINE OF THE RAILROAD TRACKS.

**ITEM 611 - CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN**

THIS ITEM SHALL INCLUDE THE REMOVAL OF THE EXISTING CATCH BASIN WALLS DOWN TO THE FLOW LINE, AND THE REPLACEMENT OF SAID CATCH BASIN WALLS WITH 8" THICK CLASS QC1 CONCRETE CAST IN PLACE. A CONSTRUCTION JOINT SHALL BE PLACED 12" BELOW TOP OF GRATE ELEVATION. THE GRATE ELEVATION SHALL BE A MINIMUM 1/2" BELOW THE NORMAL PAVEMENT SLOPE MEASURED AT THE MIDDLE OF THE GRATE(S). IT MAY BE DETERMINED IN THE FIELD THAT A SUMP DEPTH GREATER THAN 1/2" SHALL BE USED. THE FINAL SUMP DEPTH SHALL BE DETERMINED BY THE ENGINEER.

THE CONCRETE BEARING AREA SHALL BE A MINIMUM OF 2 1/2" IN WIDTH AND SHALL BE SMOOTH AND EVEN FOR ALL PORTIONS OF THE GRATE TO PREVENT THE GRATE FROM ROCKING DURING LIVE LOAD IMPACT. ALL GRATES SHALL BE REUSED. IF THE EXISTING GRATE IS MISSING OR NOT SUITABLE FOR REUSE, A NEW GRATE WILL BE FURNISHED BY THE VILLAGE AT NO COST TO THE CONTRACTOR. FRAMES WILL NOT BE USED.

THE NEW APRON SHALL CONFORM AS NEARLY AS PRACTICABLE TO THE EXISTING DIMENSIONS.

ALL DRAINAGE CONDUITS OUT-LETTING INTO THE CATCH BASIN SHALL BE MAINTAINED WITH A CONDUIT OF THE SAME SIZE WITH A CONCRETE COLLAR POURED COMPLETELY AROUND THE JOINT. ANY VOIDS FOUND AROUND THE CATCH BASIN DURING THE RECONSTRUCTION WILL BE FILLED WITH LOW STRENGTH MORTAR BACKFILL AND SHALL BE INCLUDED IN THIS ITEM.

THIS ITEM SHALL ALSO INCLUDE THE NECESSARY TOPSOIL, SEEDING, AND MULCHING FOR THE ASSOCIATED DISTURBED AREAS. THE CONTRACTOR SHALL ENSURE A DENSITY OF AT LEAST 70% GRASS COVER. REPAIR SEEDING AND MULCHING MAY BE NECESSARY. NO SEPARATE PAYMENT WILL BE MADE FOR REPAIR SEEDING AND MULCHING.

PAYMENT FOR THE CURB AT THE CATCH BASIN SHALL BE INCLUDED IN THIS ITEM.

PAYMENT FOR ITEM 611 - CATCH BASIN RECONSTRUCTED TO GRADE SHALL BE MADE AT THE UNIT PRICE BID PER EACH AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS REQUIRED TO PERFORM THE ITEM OF WORK AS DESCRIBED IN THESE SECTIONS.

APPROXIMATE LOCATIONS OF CATCH BASINS ARE AS FOLLOWS:  
 SLM 25.11 (RT)

ITEM 611 - CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN 1 EACH

**ITEM SPECIAL - MISCELLANEOUS METAL**

EXISTING CASTINGS MAY PROVE TO BE UNSUITABLE FOR REUSE, AS DETERMINED BY THE ENGINEER. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE THE CASTINGS OF THE REQUIRED TYPE, SIZE AND STRENGTH (HEAVY OR LIGHT DUTY) FOR THE PARTICULAR STRUCTURE IN QUESTION. ALL MATERIAL SHALL MEET ITEM 611 OF THE SPECIFICATIONS AND SHALL HAVE THE PRIOR APPROVAL OF THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

SPECIAL - MISCELLANEOUS METAL 500 LB

THE CONTRACTOR IS CAUTIONED TO USED EXTREME CARE IN THE REMOVAL, STORAGE AND REPLACEMENT OF ALL EXISTING CASTINGS. CASTINGS DAMAGED BY THE NEGLIGENCE OF THE CONTRACTOR, AS DETERMINED BY THE ENGINEER, SHALL BE REPLACED WITH THE PROPER NEW CASTINGS AT THE EXPENSE OF THE CONTRACTOR.

DESIGN AGENCY

DISTRICT 3



ENGINEERING TEAM ONE

DESIGNER

JLL

REVIEWER

ACM 11-2025

PROJECT ID

112637

SHEET TOTAL

P.06 24