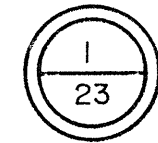


STATE OF OHIO DEPARTMENT OF TRANSPORTATION

PAVEMENT REPAIR

FHWA REGION	STATE	PROJECT	
5	OHIO		



642-91

SHEET 9 & 10 HAVE
BEEN DELETED

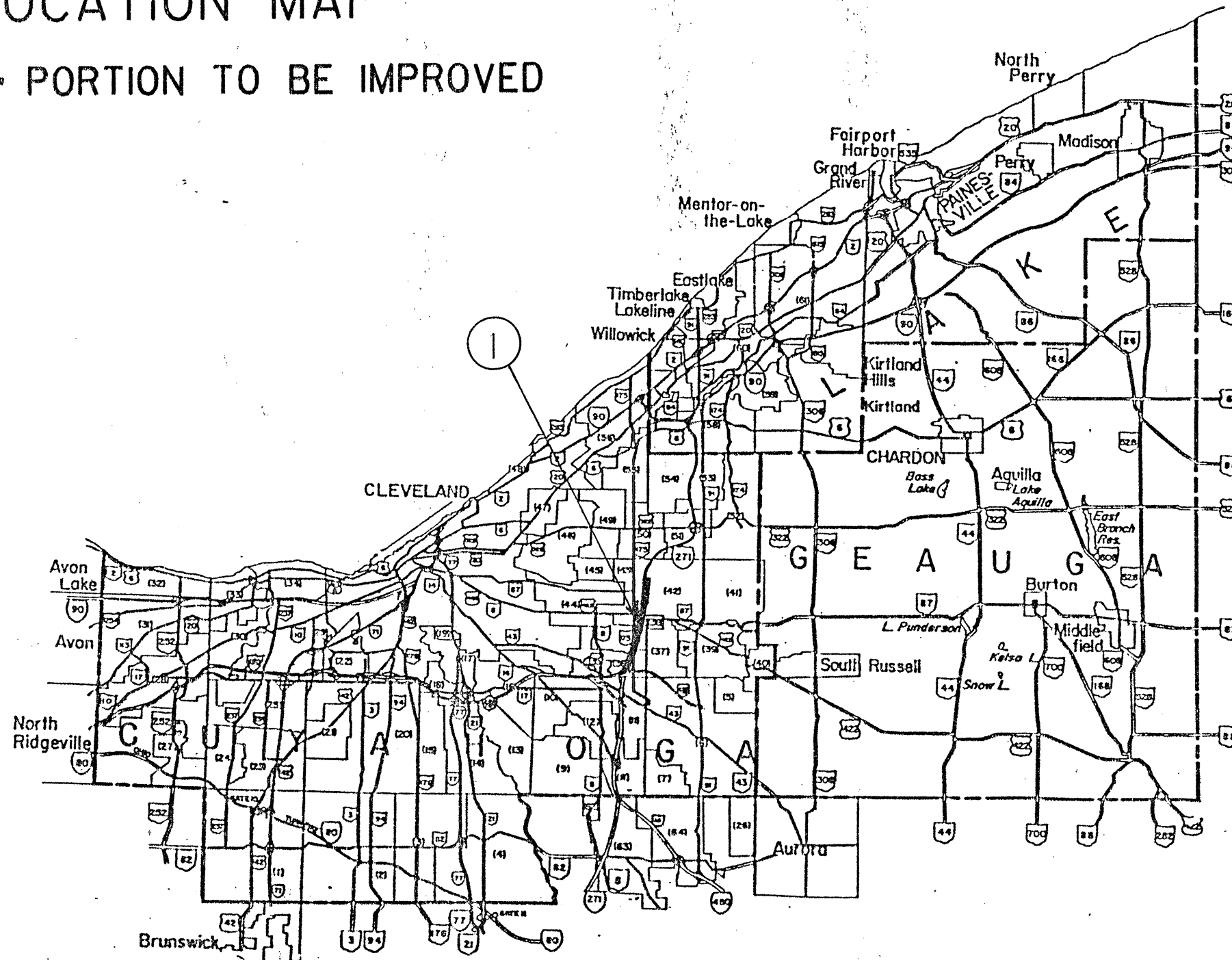
PLAN NO. 65

PART	COUNTY	ROUTE	SECTIONS	PROJECT TERMINI		NET LENGTH MILES	TOWNSHIP	CITY	VILLAGE
				BEGIN	END				
1	CUY	IR-271	(6.04 - 8.18)	6.04	9.92	3.70			

The Standard 1991 Specifications of the State of Ohio, Department of Transportation, including changes and Supplemental Specifications listed in the plans and proposal shall govern these improvements. I hereby approve these plans and declare that the making of these improvements will require the closing of the highways to traffic on Parts No. NONE and that detours will be provided by State forces. The closing to traffic of the highways will not be required on Parts No. 1 and provisions for the maintenance and safety of traffic will be as indicated in the proposal.

LOCATION MAP

— PORTION TO BE IMPROVED



Approved Date 3/20/91

Bryan J. Aoden
District Deputy Director of Transportation

Approved Date 5-2-91

B. D. Kunkelmann
Engineer, Bureau of Bridges and Structural Design

Approved Date 5-13-91

Alexander H. Hynds
Deputy Director, Operations

Approved Date 5-14-91

Jersey W. Dean
Director, Department of Transportation

TABLE OF CONTENTS

TITLE SHEET	1
SCHEMATIC PLAN	2
PAVEMENT DATA	3
TYPICAL SECTIONS	4
GENERAL NOTES	5
PLAN SHEETS	11
TRAFFIC CONTROL	22
GENERAL SUMMARY	23
* SHT. 8 IS DELETED	
" 9 "	
" 10 "	

UNDERGROUND UTILITIES

2 WORKING DAYS

BEFORE YOU DIG

CALL TOLL FREE 800-362-2764

OHIO UTILITIES PROTECTION SERVICE

NON MEMBERS

MUST BE CALLED DIRECTLY

STANDARD DRAWINGS		STANDARD DRAWINGS	
BP-5	10-1-87	TC-72.20	02-26-82
MT-98.12	8-25-89	TC-65.10	2-1-90
MT-98.13	8-25-89	TC-65.11	2-1-90
MT-98.14	8-25-89	TC-65.12	2-1-90
MT-98.15	8-25-89	TC-65.13	2-1-90
MT-99.10	11-14-86		
MT-99.20	04-29-88		
TC-35.10	08-29-84		

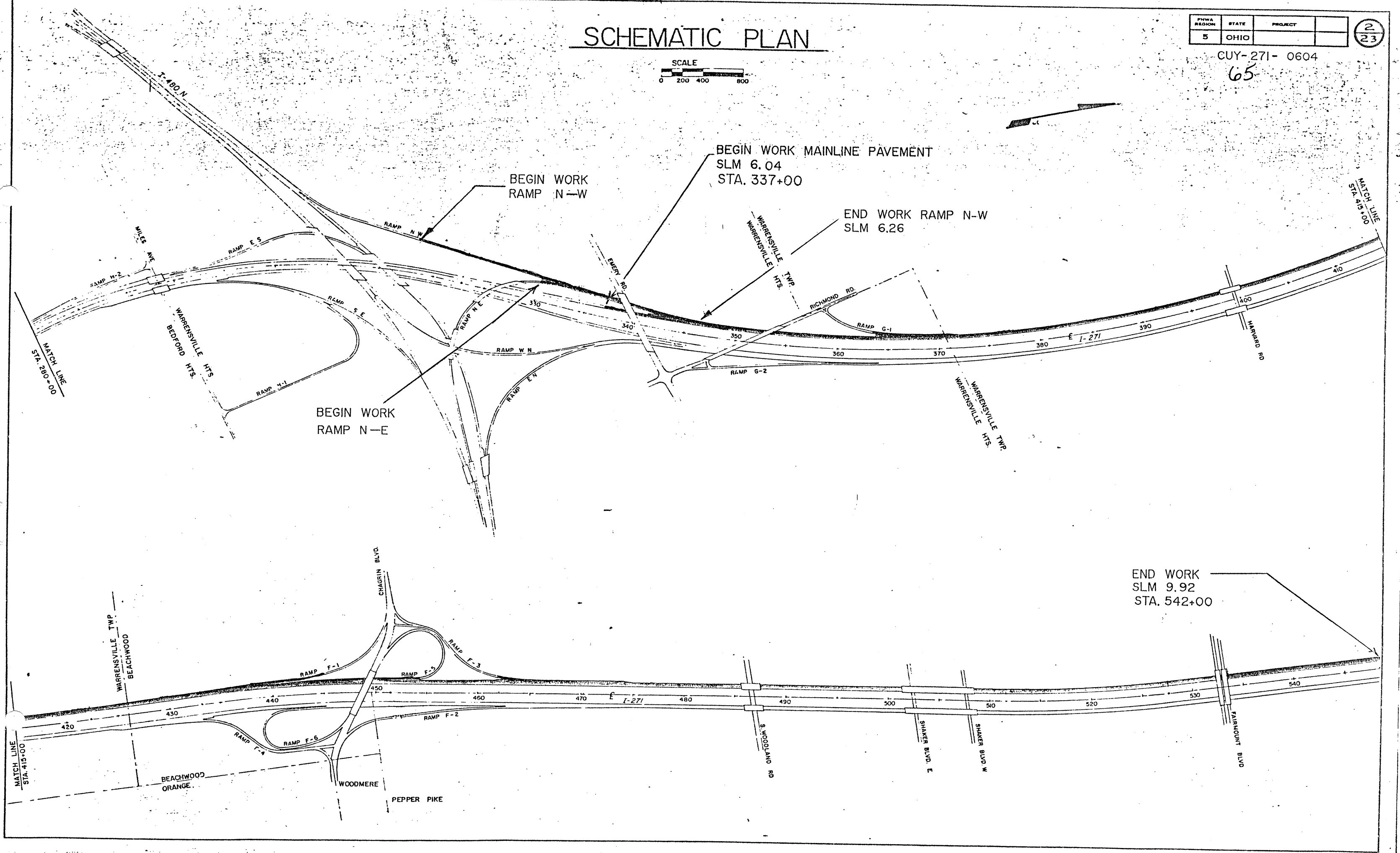
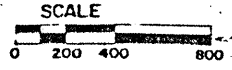
SUPPLEMENTAL SPECIFICATIONS		SUPPLEMENTAL SPECIFICATIONS	
862	12-16-88		
962	1-23-90		

SCHEMATIC PLAN

FHWA REGION	STATE	PROJECT	
5	OHIO		2 23

CUY-271- 0604

65



END WORK
SLM 9.92
STA. 542+00

SCHEMATIC PLAN

ASPHALT CONCRETE

PLAN NO. 65
 CUY - 271 - 0604

+ Approximate
 ++ Differences in pavement areas
 due to area to be Heater Recycled
 and changing pavement widths.

* Harvard Rd., S. Woodland and Shaker Blvd. Bridge Lengths Deleted

** RPM is an abbreviation for Raised Pavement Marker

PAVEMENT DATA

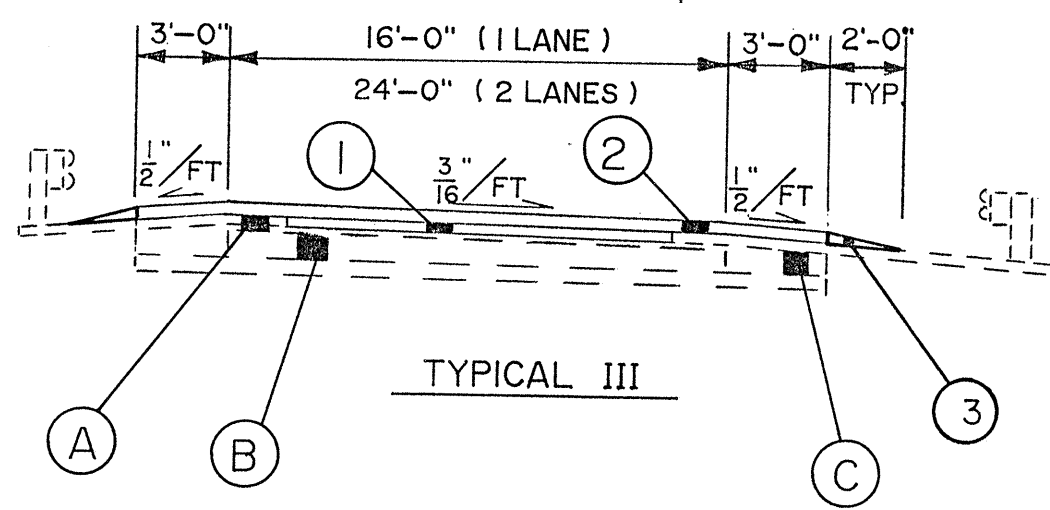
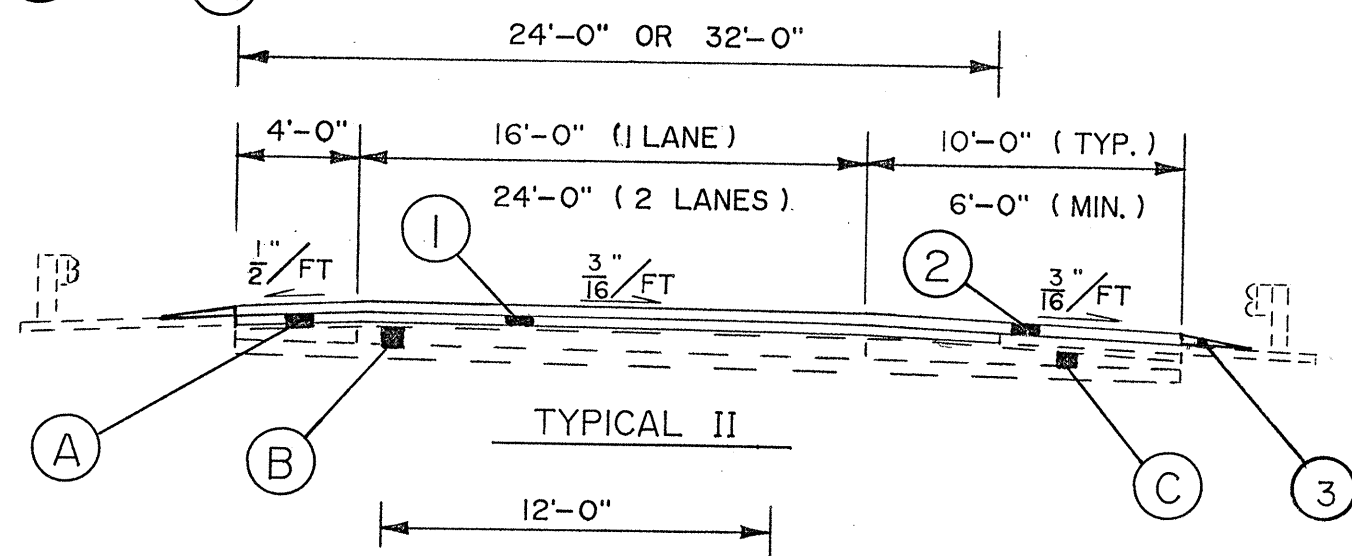
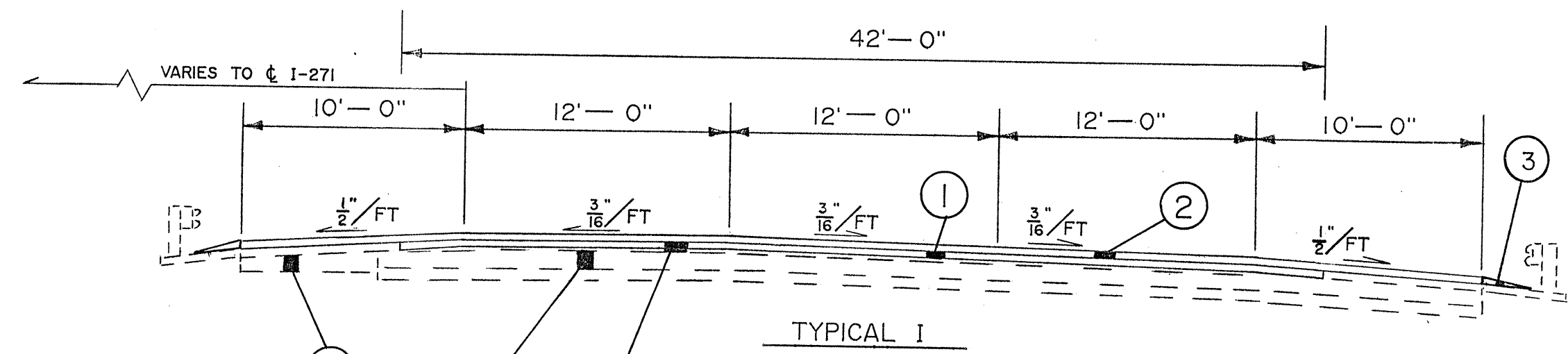
P A P T	ROUTE	LOG POINT TO LOG POINT	LENGTH		MP +MS FEET	T Y P	E X I S T	PAVEMENT AREA SQ. YD.	446		SPECIAL ASPH. CONC. HEATER RECYCLING SQ. YD.	SPECIAL ASPHALT REJUVENATING AGENT GAL.	202 WEARING COURSE REMOVED SQ. YD.	202 **RPM REMOVED FOR STORAGE EACH	621 REMOVAL OF PAVEMENT MARKINGS LIN. FT.
			MI	LIN. FT.					THICK. INCHES	CU. YD.					
1	I-271 SB	6.04-9.92	3.70*	19536*	VHP	I	848	129,684	1.25	4468	++ 108,780	10,878	2237	598	87226
		Ramp N-W	(0.55)	(2877)	VHP	II	848	++ 10,587	1.25	368	++ 10,482	1049	183	45	5093
		Ramp N-E	(0.01)	(75)	VHP	III	848	++ 275	1.25	10	++ 53	5	183		
		Ramp G-1	(0.01)	(50)	24 ±	III	848	++ 136	1.25	5	++ 600	60	136		
		Ramp F-1	(0.04)	(234)	24 ±	III	848	++ 650	1.25	23	++ 253	25	139		
		Ramp F-5	(0.04)	(203)	24 ±	III	848	++ 563	1.25	20	++ 200	20	139		
		Ramp F-3	(0.01)	(50)	24 ±	III	848	++ 147	1.25	5	++ 607	61	147		420
		EXTRA AREA						7050	1.25	245	6050	605			
	TOTALS PART I		3.70*					148,092		5144	127,025	12,703	3164	643	97,739

FHWA REGION	STATE	PROJECT	
5	OHIO		

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CUYAHOGA COUNTY
CUY - 271 - 0604

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EXISTING

- (A) 3" ASPHALT CONCRETE
- (B) REINFORCED PORTLAND CEMENT CONCRETE
- (C) AGGREGATE BASE

PROPOSED

- (1) ITEM SPECIAL — ASPHALT CONCRETE PAVEMENT SURFACE HEATER RECYCLING
- (2) ITEM 446 — ASPHALT CONCRETE SURFACE COURSE, TYPE I, AC-20
- (3) ITEM 617 - COMPACTED AGGREGATE, TYPE A

TYPICAL SECTIONS

GENERAL NOTES

PLAN NO.

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GENERAL :

It is the intent of this project to repair and/or eliminate various pavement surface irregularities including deteriorated joints, potholes, cracks, and raveling areas of the asphalt concrete surface course and to restore the riding quality of the Interstate highway. The repair of the existing surface course will be accomplished through the implementation of an asphalt pavement surface heater recycling operation with an asphalt concrete rejuvenating agent. The recycled pavement will then be resurfaced with 1.25 inches of asphalt concrete.

The areas specified for recycling on the ramps are approximate. The plan sheets shall be used as a guide but the exact work limits on the ramps shall be determined by the Project Engineer. The twelve foot swath through the middle of the ramp(s) is intended to correct the most severely deteriorated areas.

The extra areas provided for in this plan shall be used as directed by the Engineer in locations that may have deteriorated since the preparation of this plan.

The recycling operation shall either begin or suspend work at the areas designated for butt joints, particularly prior to and after bridge decks with concrete wearing surfaces. In other words, areas designated for wearing course removal shall not be recycled.

SCHEDULE OF OPERATIONS (GENERAL) :

Asphalt concrete pavement recycling and the paving operations shall be scheduled and coordinated so that the recycling crews are no more than two days ahead of the paving operations. In other words, the recycled pavement shall not be left open and exposed to traffic for more than two days before paving operations take place.

All temporary pavement markings must be complete and in place both after pavement recycling and after paving operations, and prior to opening any lanes to traffic.

EXISTING PAVEMENT :

Existing pavement thicknesses and areas have been taken from the records and are believed to represent the existing pavement, but the state does not guarantee the accuracy of the same.

For further information in regard to the existing pavement, the contractor shall refer to the previous construction plans. These plans may be reviewed at the Ohio Department of Transportation, District 12 Office, 5500 Transportation Blvd., Garfield Hts., Ohio 44125: (216) 581-2100.

CONTINGENCY QUANTITIES :

The contractor shall not order materials or perform work for plan items set up to be used "as directed by the Engineer" unless authorized by the Engineer.

ITEM 202 - RAISED PAVEMENT MARKERS REMOVED FOR STORAGE, as per plan :

Raised pavement markers shall be removed in a manner that prevents damage to the castings. Removed markers shall be collected, stored and then delivered to the ODOT Warrensville Yard - 25609 Emery Rd., Warrensville Hts., Oh 44128 (SR-175 @ intersection of I-271 & Emery Rd.), by the contractor, as directed by the Engineer. The Project Engineer shall give the Maintenance Department (581-2100) 24 hr. notice prior to any deliveries and the Project Engineer shall be responsible for furnishing all necessary transfer documentation to the Yard. An estimated quantity of 643 markers have been carried over to the General Summary. All costs associated with the removal, storage and delivery of these markers shall be included in the unit price bid for ITEM 202 - RAISED PAVEMENT MARKERS REMOVED FOR STORAGE, as per plan.

ITEM 621 - REMOVAL OF PAVEMENT MARKINGS :

Immediately prior to the heater recycling operations the contractor shall remove the existing affected thermoplastic lane, edge, channelizing and/or transverse lines, as directed by the Engineer, at the work locations specified in these plans. The method of removal shall be approved by the Engineer prior to any removal operations actually taking place. At no time shall any pavement be opened to traffic without the existing and/or the temporary replacement markings complete and in place. The unit of measure for this item of work shall be linear feet (including the gaps associated with lane lines). All labor, equipment and materials required for the removal of these pavement markings shall be included in the unit price bid for ITEM 621 - REMOVAL OF PAVEMENT MARKINGS.

BUTT JOINTS :

Item 202 - Wearing Course Removed shall be used for the construction of all butt joints. All butt joints specified in this plan shall typically be 50 ft. in length. Particular care shall be taken in achieving a smooth and level transition in the construction of all butt joints at both forward and trailing bridge approaches.

ITEM 614 - MAINTAINING TRAFFIC :

Generally, the contractor shall conduct his operations so as to construct the proposed pavement surface recycling and paving with a minimum of hazard, delay and inconvenience to the motorist using the highway affected by the work done under this contract. In addition to the ODOT Construction and Material Specifications, the Ohio Manual of Uniform Traffic Control Devices for Streets and Highways and the plan sheets, the following provisions shall apply :

I. NOTIFICATION

Since functional traffic is a major concern on this project, it is essential that the motoring public be adequately forewarned of future lane and ramp closures and traffic constrictions. The contractor shall submit a work schedule to the Ohio Department of Transportation indicating the locations and dates of each lane/ramp closure at least 3 days prior to the implementation of any such closure.

II. SCHEDULE OF OPERATIONS - DAY/NIGHT TIME WORK :

The Contractor will be required to limit his hours of operation for paving operations and the majority of recycling operations to night

GENERAL NOTES

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time hours. Lane closures for daytime hours will only be permitted as noted below. All nighttime work shall be done between 7:00 PM and 6:30 AM Sunday through Thursday. All traffic control devices will be removed from the pavement prior to the commencement of rush hours.

Limited daytime work will be permitted based upon the following requirements:

- 1) Absolutely no lane closures are permitted between the hours of 6:30 - 9:30 A.M. or between 3:30 - 7:00 P.M. Mon. thru Fri.
- 2) Work on Saturday and Sunday and between 9:30 A.M. and 3:30 P.M. Mon. thru Fri. will be restricted to closing one out of three lanes only. A minimum of two lanes shall be maintained open to traffic during all daytime hours.
- 3) Traffic must be maintained on all ramps during daytime hours.

The Engineer shall have the authority to adjust and limit the hours of operation for any unusual traffic conditions including but not limited to holidays, special events and traffic accidents.

III. NIGHT TIME ILLUMINATION :

The Contractor is responsible for providing adequate night illumination for construction and inspection procedures. The Engineer shall have the authority to have the Contractor provide additional lighting if necessary for proper construction and inspection and/or if it is determined beneficial in promoting safety for the motoring public and the Contractor.

IV. ADDITIONAL RESTRICTIONS :

All through traffic lanes shall be kept open at all times except as noted here and in the maintenance of traffic plans. At least one through lane shall be maintained at all times unless noted previously or below.

Traffic shall be maintained on the ramps. The Project Engineer shall have the authority to adjust traffic control along ramps in order to perform recycling operations and paving operations, and to promote efficient traffic flow. Any anticipated temporary closures shall require prior approvals from the District Operations Engineer. Any closures determined necessary shall be kept to the absolute minimum time period necessary to perform the work. Reopening shall occur immediately after the pavement has cooled sufficiently and any affected pavement markings are restored completely. The Engineer reserves the right to request additional signing in these situations to be supplied by the contractor and included in the Lump Sum bid for ITEM 614 - MAINTAINING TRAFFIC.

The Contractor shall not route traffic on both sides of a lane closure.

Within the length of the closure, provision shall be made to control the traffic entering and exiting on ramps as necessary to prevent wrong movements and to keep off the recycled or resurfaced pavement not ready for traffic.

The Project Engineer has the authority to adjust the length of any lane closures if extreme traffic delays result from the work specified in this plan.

V. SIGN PLACEMENT :

In the event a ramp closure is required, the contractor shall furnish and install an OC-45-48 SPECIAL "EXIT RAMP CLOSED AHEAD" sign and an OC-46-48 "EXIT RAMP CLOSED" sign or an OC-46-48 SPECIAL "ENTRANCE RAMP CLOSED" sign to be placed as directed by the Engineer.

The short term closure of Ramp Lane N-E is anticipated. A temporary detour will be required if this closure is implemented. This detour will consist of directing traffic from I-271 SB to I-480 WB to Miles Ave. and then back to I-480N to US-422 ALT EB. The following detour signs, with their respective locations noted, will be required: an OW-29 SPECIAL (diagonal right) at the gore area between ramps N-E and N-W and at the gore area between the I-480 WB mainline and the exit to Miles Ave. An OW-29L sign will be required at the bottom of the Miles exit and at the entrance ramp from Miles Ave. to go I-271 NB. Exact locations of these signs in order to maximize their effectiveness in the field, will be as directed by the Engineer.

In this plan, ITEM SPECIAL - LAW ENFORCEMENT OFFICER WITH PATROL CAR has been provided to be used as directed by the Engineer to help with the effectiveness of the traffic control and closures, control traffic movements, speed and/or to aid in directing traffic as necessary.

VI. TRAFFIC CONTROL SYSTEMS

A. WHEN REQUIRED

Whenever any part of the traveled surface is being worked upon or is otherwise not suitable for safe and convenient use by vehicles, traffic control devices sufficient to protect such areas to assure the safe and convenient passage of vehicular traffic, shall be installed and maintained. Such traffic control devices and the manner in which they are used shall be consistent with these plans and the OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, hereinafter referred to as the "Manual". The traffic control device system shall constitute the minimum provisions for traffic control for each particular situation. Whenever the Engineer determines it necessary, especially where a grade, curve or merge condition exists, he may direct that additional or alternative devices be used.

B. CONDITIONS

During all parts of this project, signing, barrels, etc. shall be located as indicated in the plans. The number of lanes maintained shall be as indicated in the traffic control notes and sheets.

C. ADVANCE WARNING SIGNS

All advance warning signs for any conditions which restrict traffic shall be erected before any such restriction is put into effect. All such signs shall be covered or removed from the view of traffic whenever they are not applicable.

D. FLASHING ARROW REQUIREMENT

Whenever any part of the traveled surface is closed, the motorists shall be warned and directed by the contractor through the use of one flashing arrow for each lane closed in addition to those provisions set forth in the "Manual" and Std. Dwg. TC-35.10.

E. LAW ENFORCEMENT OFFICER WITH PATROL CAR

The Engineer shall determine the number of officers and cars required for this purpose. The officer(s) shall move their patrol car(s) as necessary to maximize their effect on traffic. The contractor shall make all arrangements regarding scheduling of and payment for the officer(s) and patrol car(s). An estimated quantity of 75 hours has been carried over to the General Summary to be used as directed by the Engineer. Payment for the above shall be included in the unit price bid for ITEM SPECIAL - LAW ENFORCEMENT OFFICER WITH PATROL CAR.

GENERAL NOTES

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G. FAILURE TO COMPLY

If there is any failure to comply with provisions for traffic control set out in these plans and notes, or with the provisions of the "Manual", the highway in the vicinity of the work area shall not be considered in a condition for the safe and convenient use by the traveling public. Any failure to keep the highway in the vicinity of the work area in a safe and convenient use by the traveling public shall be considered a breach of this contract. Work shall be suspended until the contractor complies with the provisions of the aforementioned items.

VII. TRAFFIC CONTROL MATERIAL

A. SIGNS

Sign dimensions and specifications, including letter sizes shall be as provided in the "MANUAL" or in sign design drawings provided by the Department of Transportation. The signs shall be subject to approval of the Engineer prior to the start of the project.

B. SIGN SUPPORTS

Sign supports shall be of sufficient size and height as to support the signs at least 1 foot above the pavement. Supports shall also be adequate in mass and stability to prevent the signs from being blown over by wind or vehicular generated air turbulence.

C. FLASHING ARROWS

The electric flashing arrow(s) shall be Type A, as shown in Standard Construction Drawing TC-35.10. Payment for these shall be included under ITEM 614 - MAINTAINING TRAFFIC.

D. BARRELS

Barrels shall be located as indicated on the traffic control plans and the applicable Standard Drawings. Reflectorized plastic barrels (meeting ODOT Specifications) are required for all nighttime work.

It is imperative that the barrels used for traffic control be in very good condition, clean and free from any material impairing their visibility and effectiveness. Particular attention is made to the condition of the reflectorized tape on the barrels. The Project Engineer reserves the right to have the contractor remove and replace (at no additional cost to the State) any barrels he determines to be in any condition compromising their reflectivity and/or effectiveness. All costs for installing, maintaining and subsequent removal of said barrels shall be included in the lump sum price bid for ITEM 614 - MAINTAINING TRAFFIC.

VIII. TEMPORARY PAVEMENT MARKINGS

Due to the temporary nature of the lane closures associated with the construction specified in this contract, temporary edge lines as noted in the Standard Drawings for lane/ramp closures will not be required.

The temporary pavement markings specified in this plan are intended to be used after both the pavement recycling and resurfacing operations prior to opening the lane(s) and ramp(s) to traffic and prior to the installation of the permanent pavement markings. The temporary markings shall be installed after compaction operations and sufficient

cooling of the pavement. It is imperative that these temporary markings are in place prior to opening any lanes to traffic. PRIOR TO ANY HEATER RECYCLING AND PAVING OPERATIONS, THE CONTRACTOR IS RESPONSIBLE FOR CONDUCTING A FIELD SURVEY OF THE EXISTING MARKINGS. ANY STAKING OR MARKING REQUIRED TO ESTABLISH CONTROL POINTS TO INSURE THAT EXISTING MARKINGS ARE ACCURATELY REPLACED WITH BOTH TEMPORARY AND PERMANENT MARKINGS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. Particular attention is directed to the US-422 interchange area and the Ramp Lane N-W beginning near Richmond Rd. and continuing to go I-480 westbound. All temporary markings shall be replaced exactly as the the existing markings are except that the channelizing line beginning near Richmond Rd. will be shortened on the approach end, by approximately 600 ft. so that it begins at the overhead sign structure. It is also the responsibility of the contractor to insure that these temporary markings are visible and effective. If these markings are absorbed into the recycled or resurfaced pavement or their effectiveness is impaired in any way, it shall be the responsibility of the contractor to reapply or to touch up the markings as directed by the Project Engineer. These temporary markings shall be maintained to the satisfaction of the Engineer until the permanent markings are complete and in place. Any additional labor, equipment and materials required for touch up work or repeat applications shall be performed at no additional cost to the State. All costs associated with the above described work shall be included in the unit price bid for each individual applicable Temporary Pavement Marking Item.

IX. PERMANENT PAVEMENT MARKINGS

Prior to any recycling and paving operations, the contractor is responsible for conducting a field survey of the existing permanent markings. Any staking or marking required to establish control points to insure that existing markings are accurately replaced shall be the responsibility of the contractor. Particular attention is directed to the US-422 interchange area and the Ramp Lane N-W beginning near Richmond Rd. and continuing to go I-480 westbound. All permanent markings shall be replaced exactly as they are except that the channelizing line beginning near Richmond Rd. will be shortened, as directed by the Engineer, on the approach end by approximately 600 ft. so that it begins at the overhead sign structure. All permanent pavement marking locations and layout shall be verified with the Project Engineer prior to the actual installation.

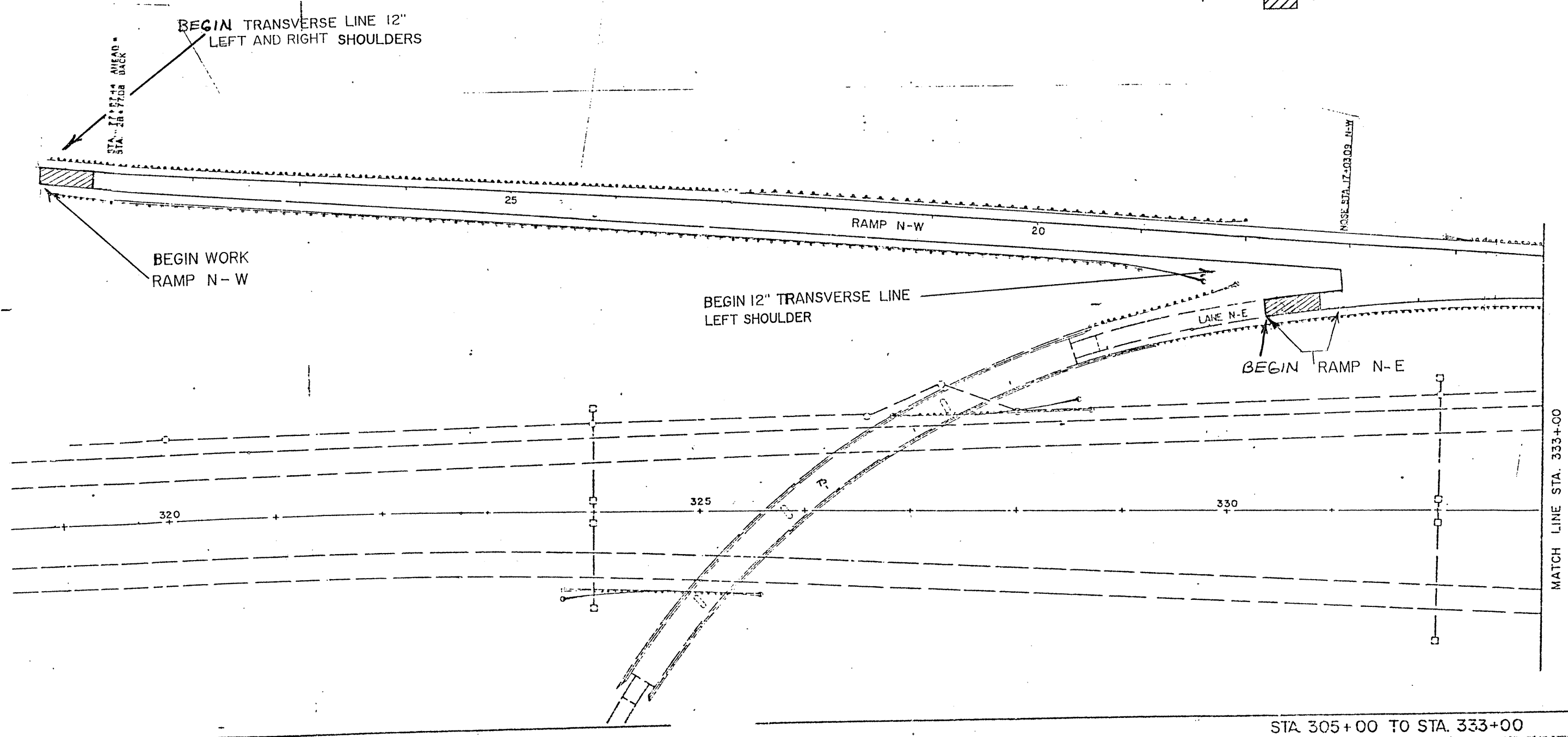
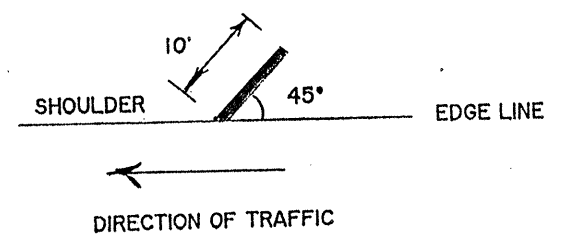
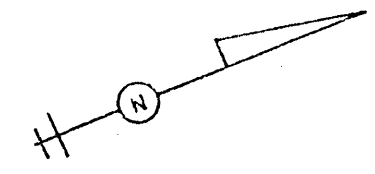
The 12 inch Transverse Line is intended to be used to replace the existing shoulder transverse lines providing demarcation between the shoulders and Ramps G-1 and Lane N-W. These lines shall be installed in 10 ft. lengths at a 45 degree angle from the edge line in 100 ft. spacings. See plan sheets for details.

FEDERAL REGION	STATE	PROJECT
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CUYAHOGA COUNTY
CUY - 271 - 0604

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STA 305+00 TO STA. 333+00

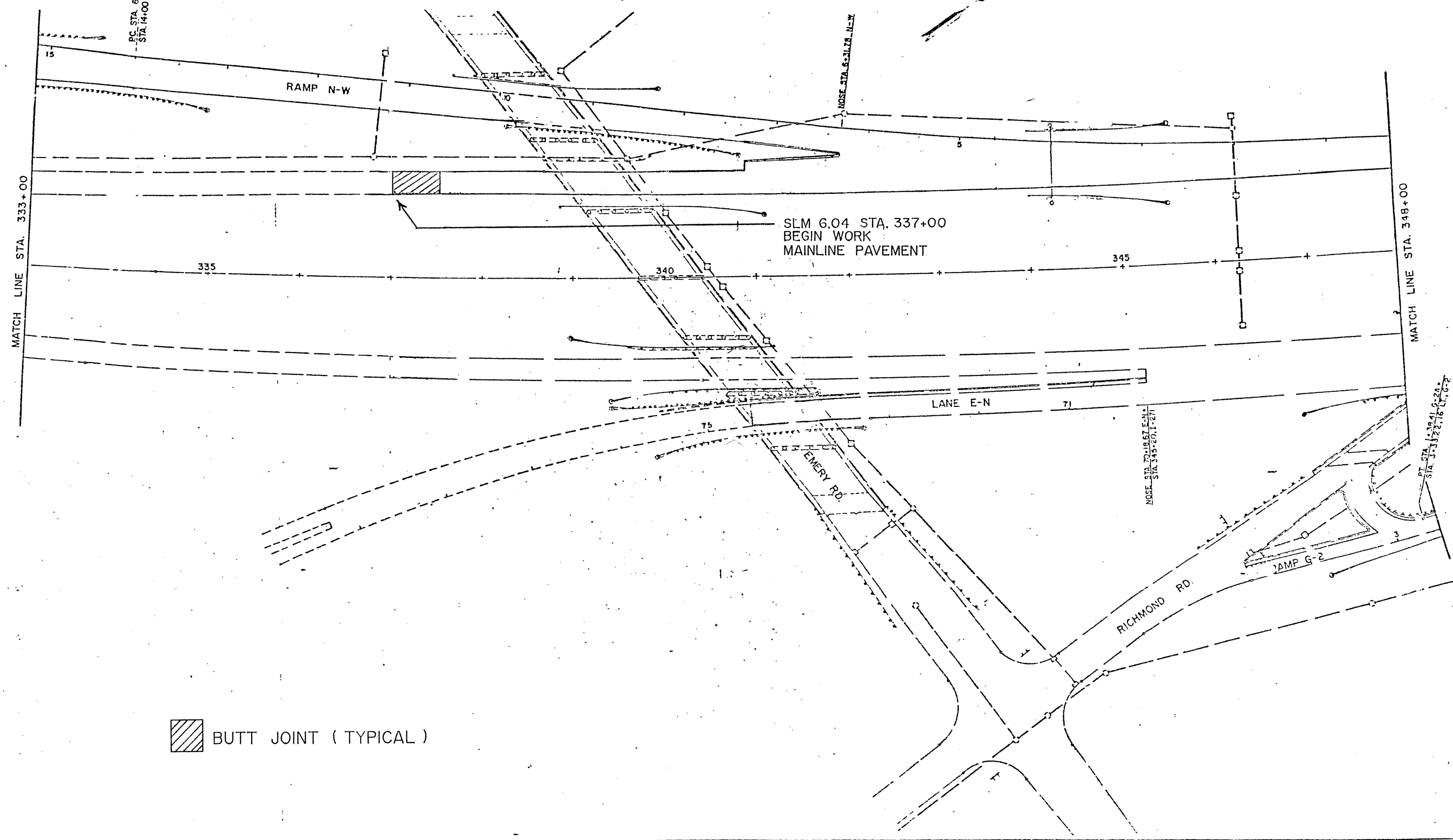
MATCH LINE STA. 333+00

AREA REGION	STATE	PROJECT
5	OHIO	

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CUYAHOGA COUNTY

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 BUTT JOINT (TYPICAL)

STA. 333+00 TO STA. 348+00

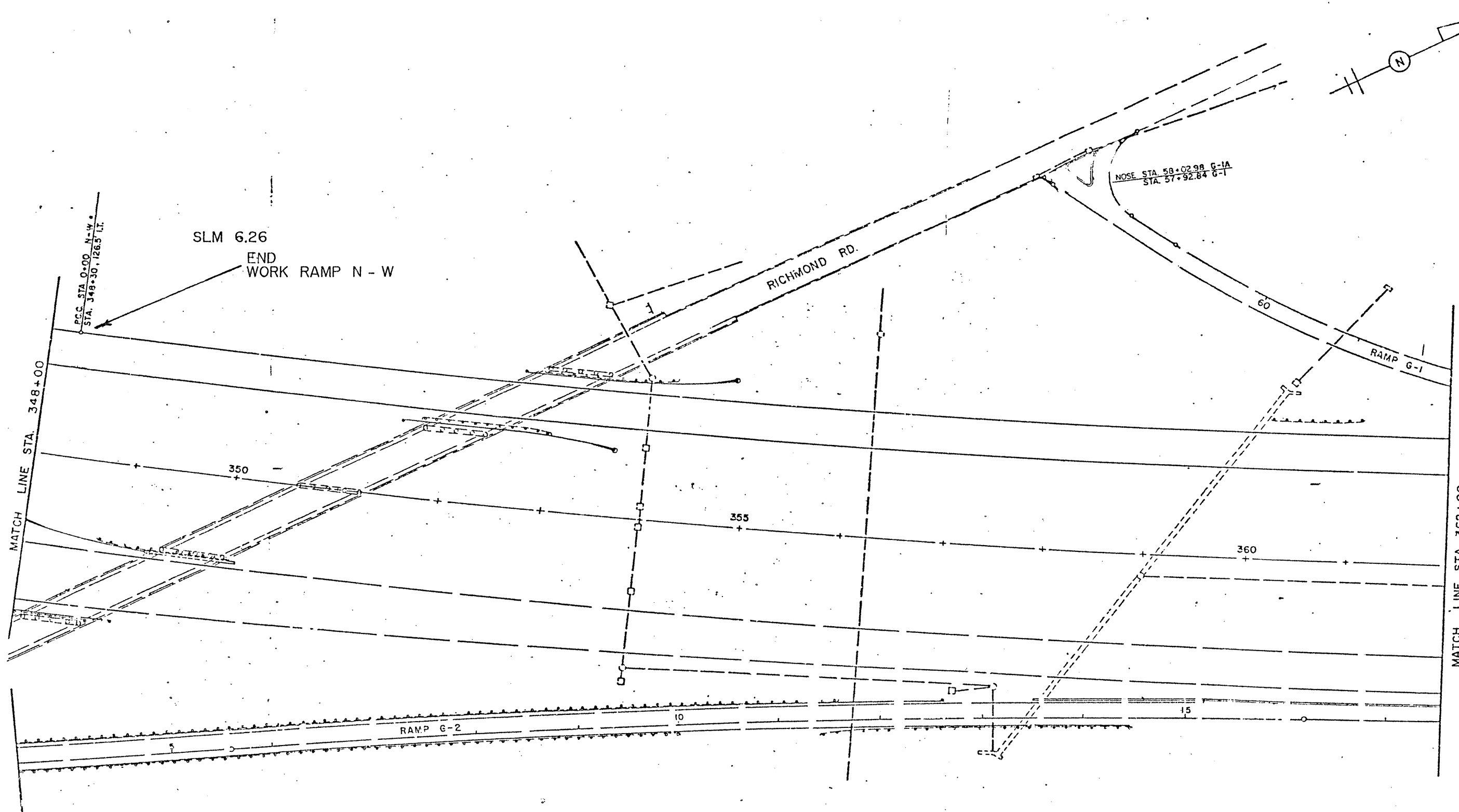
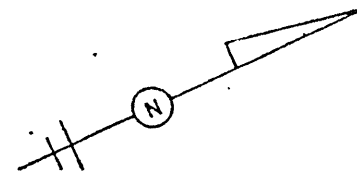
TRWA REGION	STATE	PROJECT
5	OHIO	

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23

CUYAHOGA COUNTY

CUY - 271 - 0604

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STA. 348+00 TO STA. 362+00

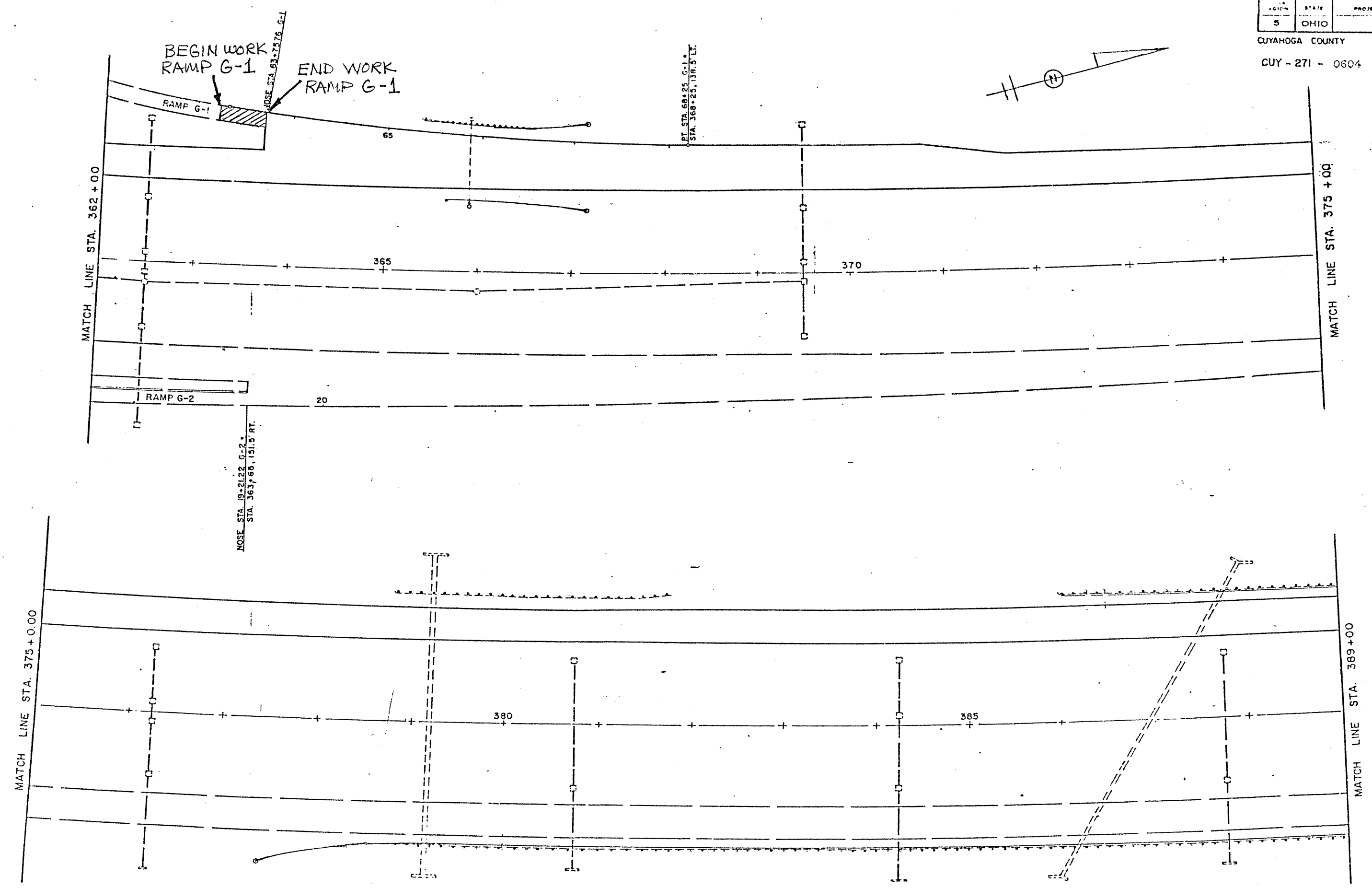
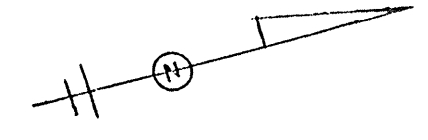
SECTION	STATE	PROJECT
5	OHIO	

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CUYAHOGA COUNTY

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STA. 362+00 TO STA. 389+00

END ITEM 847-TRANSVERSE LINE, 12", 947.02
I-271 SB RIGHT SHOULDER

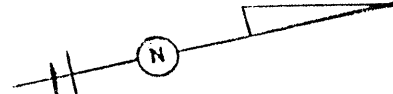
FEDERAL REGION	STATE	PROJECT
5	OHIO	

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CUYAHOGA COUNTY

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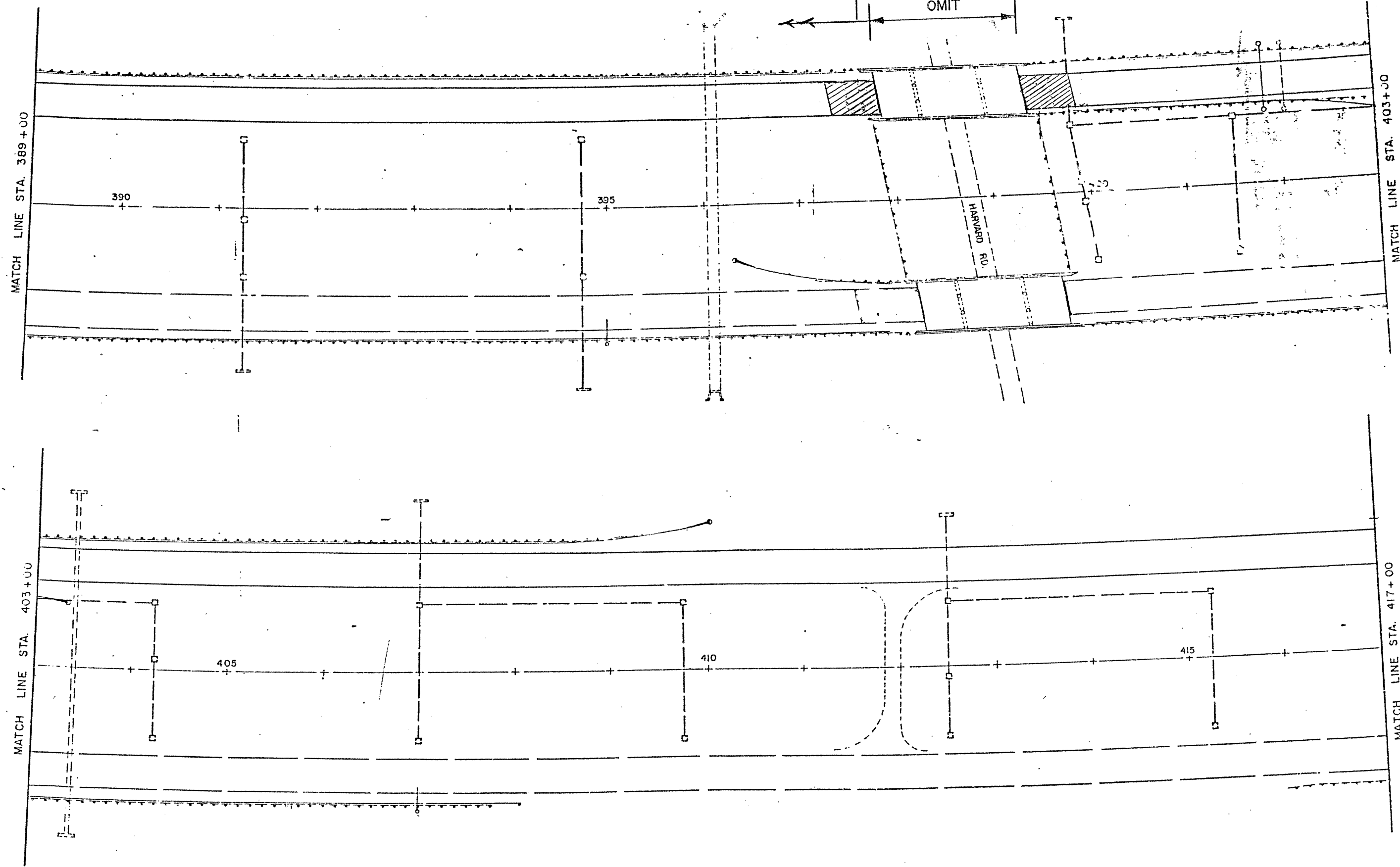
OMIT

MATCH LINE STA. 389+00

MATCH LINE STA. 403+00

MATCH LINE STA. 403+00

MATCH LINE STA. 417+00



STA. 389+00 TO STA. 417+00

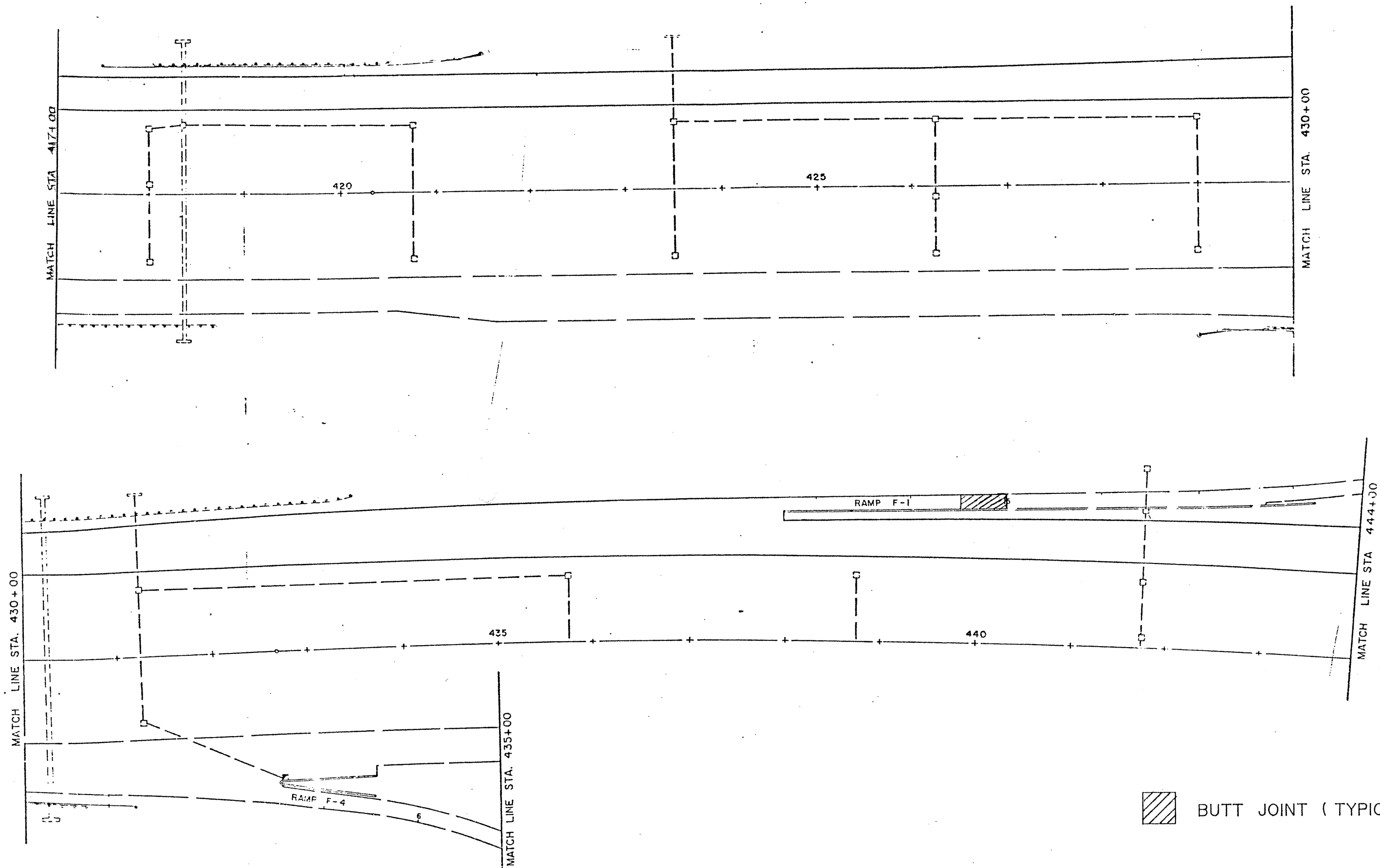
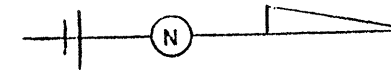
AREA REGION	STATE	PROJECT
5	OHIO	

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CUYAHOGA COUNTY

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 BUTT JOINT (TYPICAL)

STA. 417+00 TO STA. 444+00

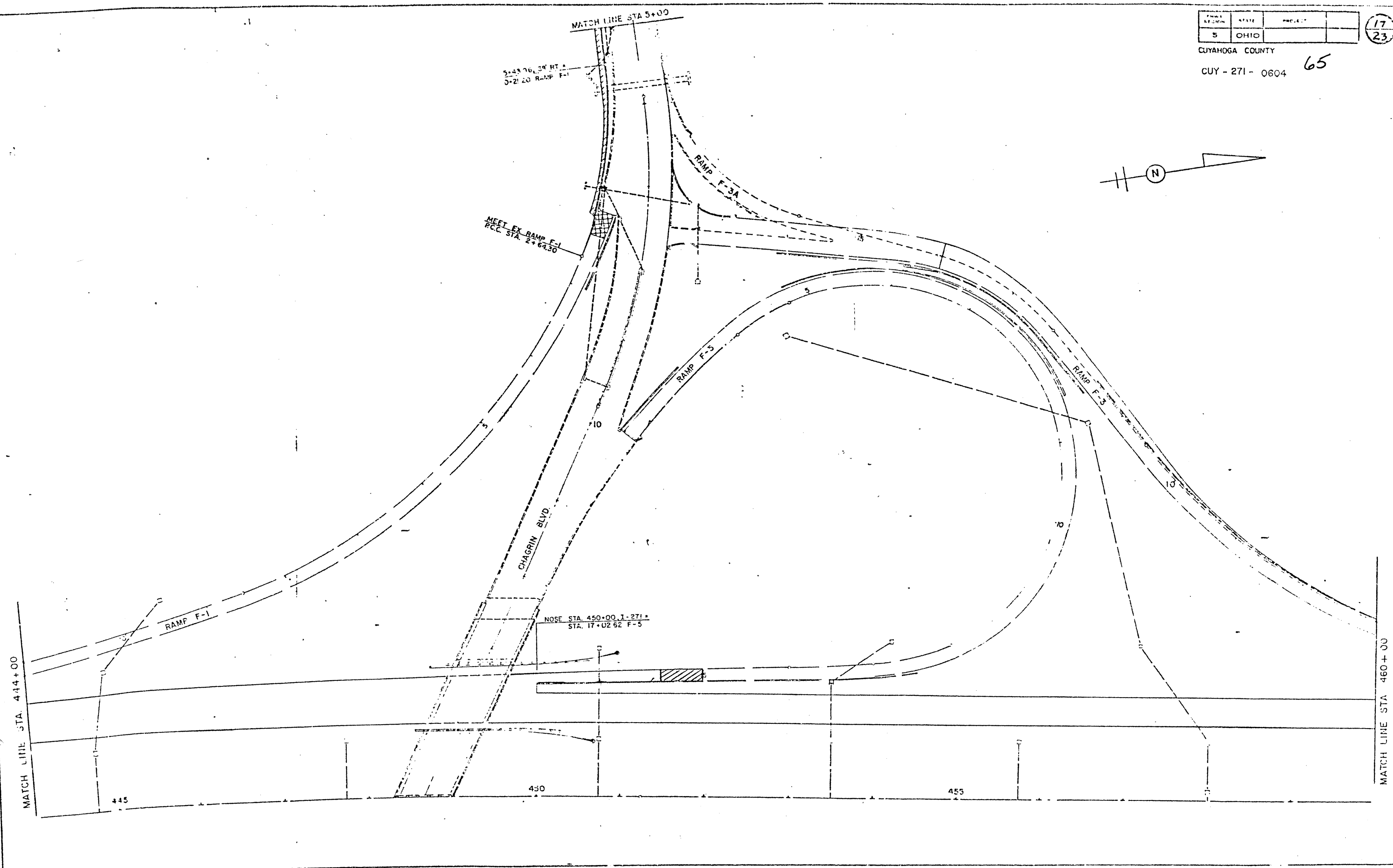
PROJECT NO.	STATE	DATE	
5	OHIO		

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CUYAHOGA COUNTY

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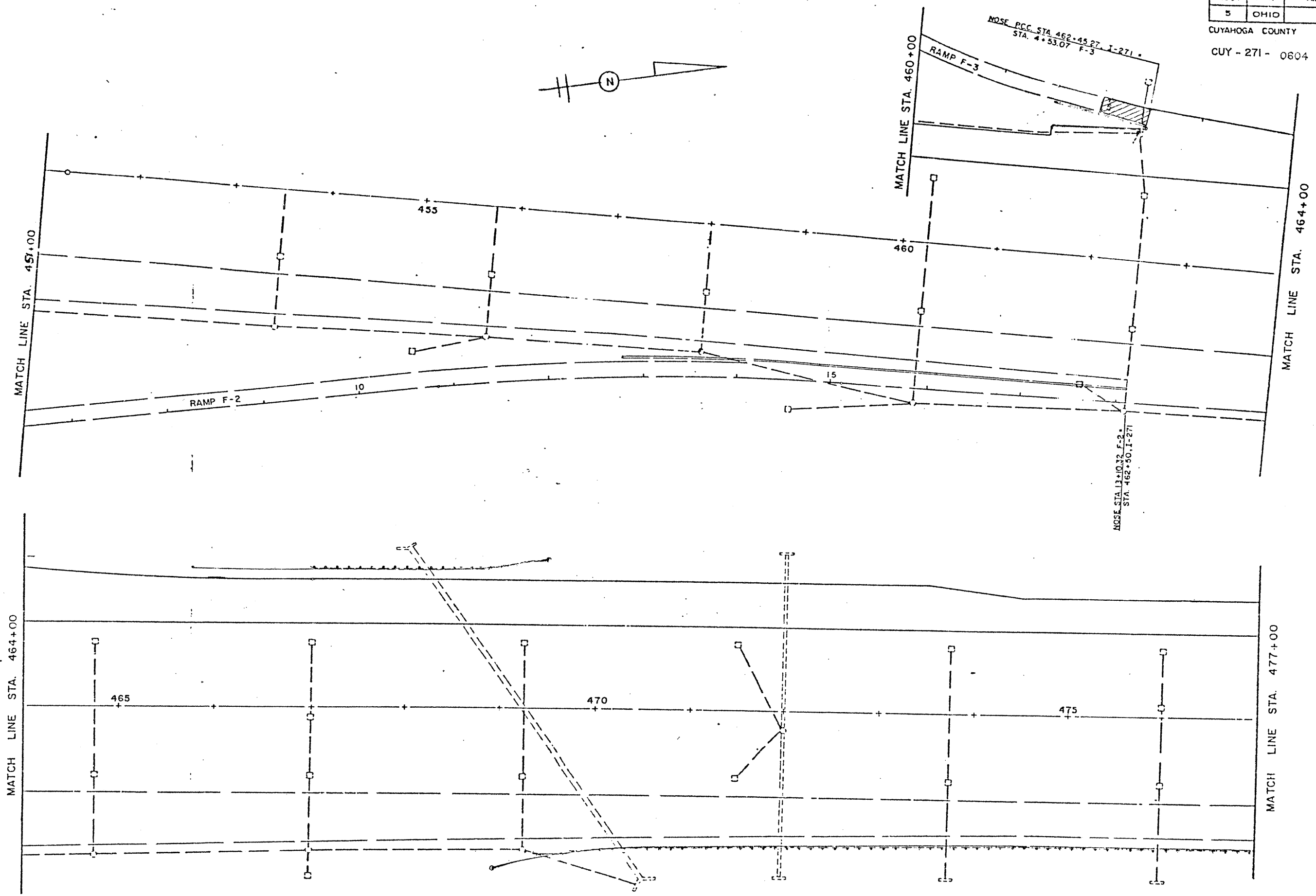
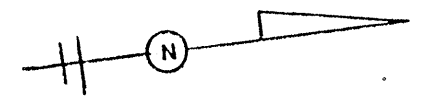
STA. 444 + 00 SB. TO STA. 460 + 00 SB.

PLANS	STATE	PROJECT
5	OHIO	

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CUYAHOGA COUNTY
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MOSE P.C.C. STA. 462+45.27 I-271
STA. 4+53.07 F-3

MOSE STA. 13+00.32 F-2
STA. 462+50.1 I-271

STA. 451+00 TO STA. 477+00

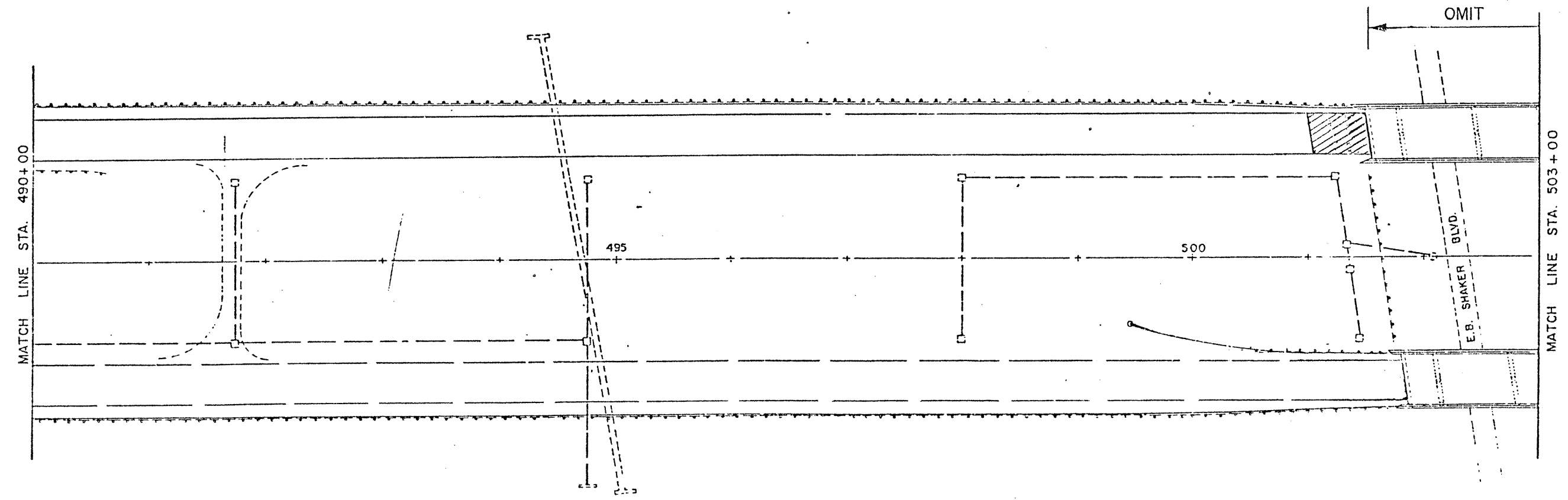
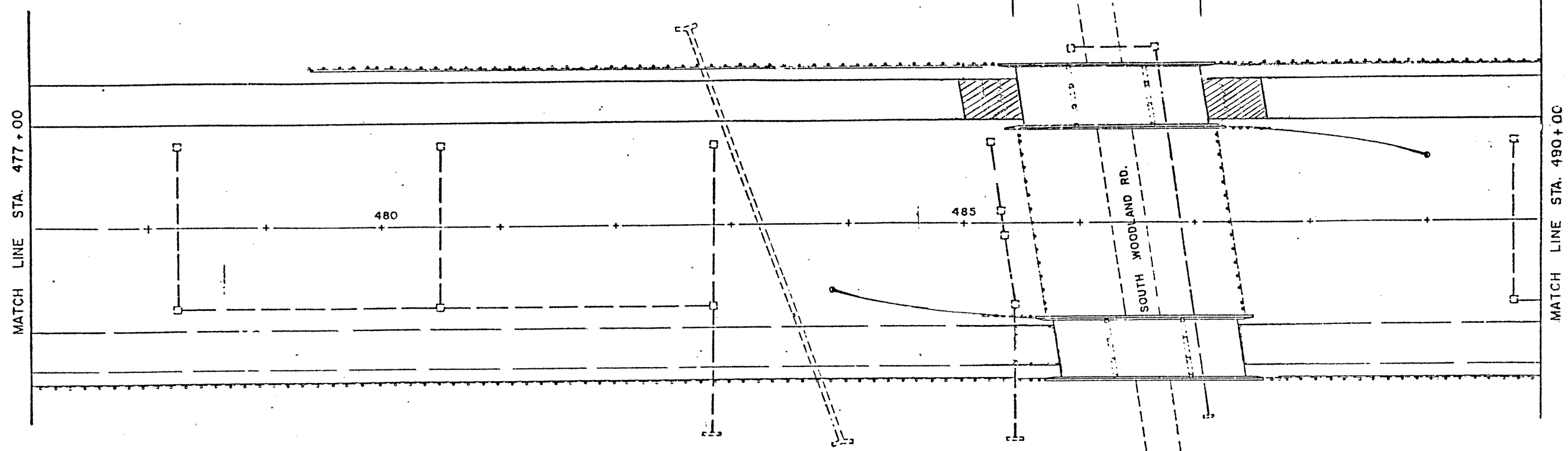
F.W.D.A. REGION	STATE	PROJ.
5	OHIO	

19
23

CUYAHOGA COUNTY

CUY - 271 - 0604

65



STA. 477+00 TO STA. 503+00

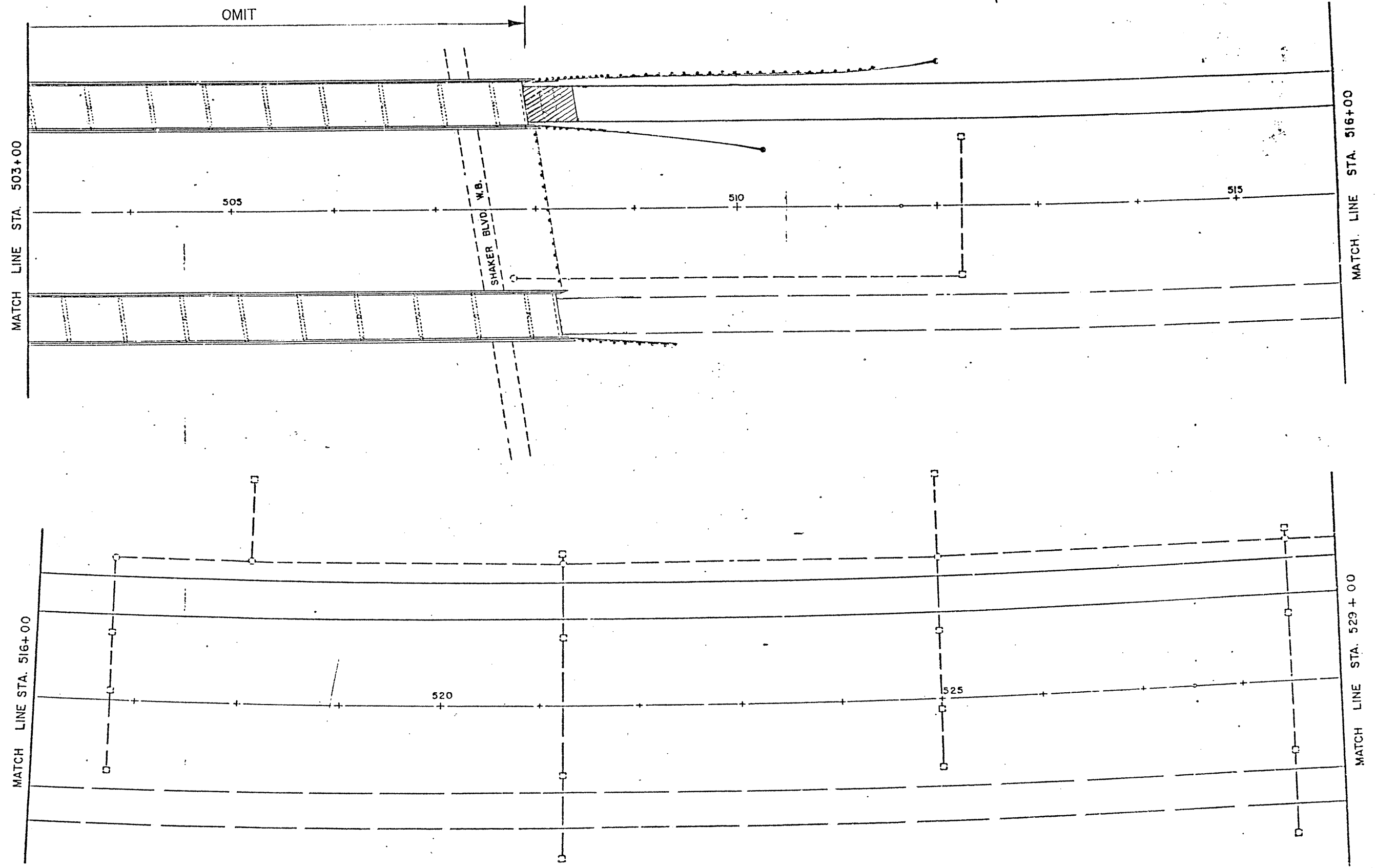
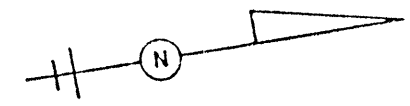
FEDERAL REGION	STATE	PROJECT
5	OHIO	

20
23

CUYAHOGA COUNTY

65

CUY - 271 - 0604



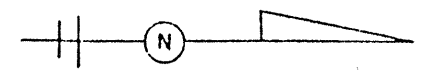
STA. 503+00 TO STA. 529+00

FED. REGION	STATE	PROJECT
5	OHIO	

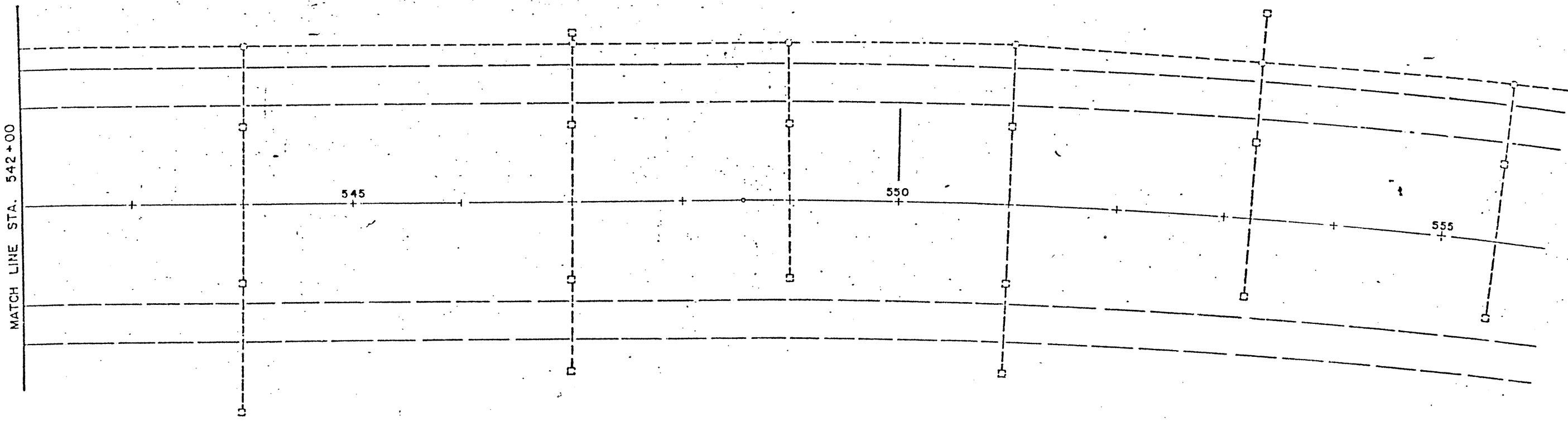
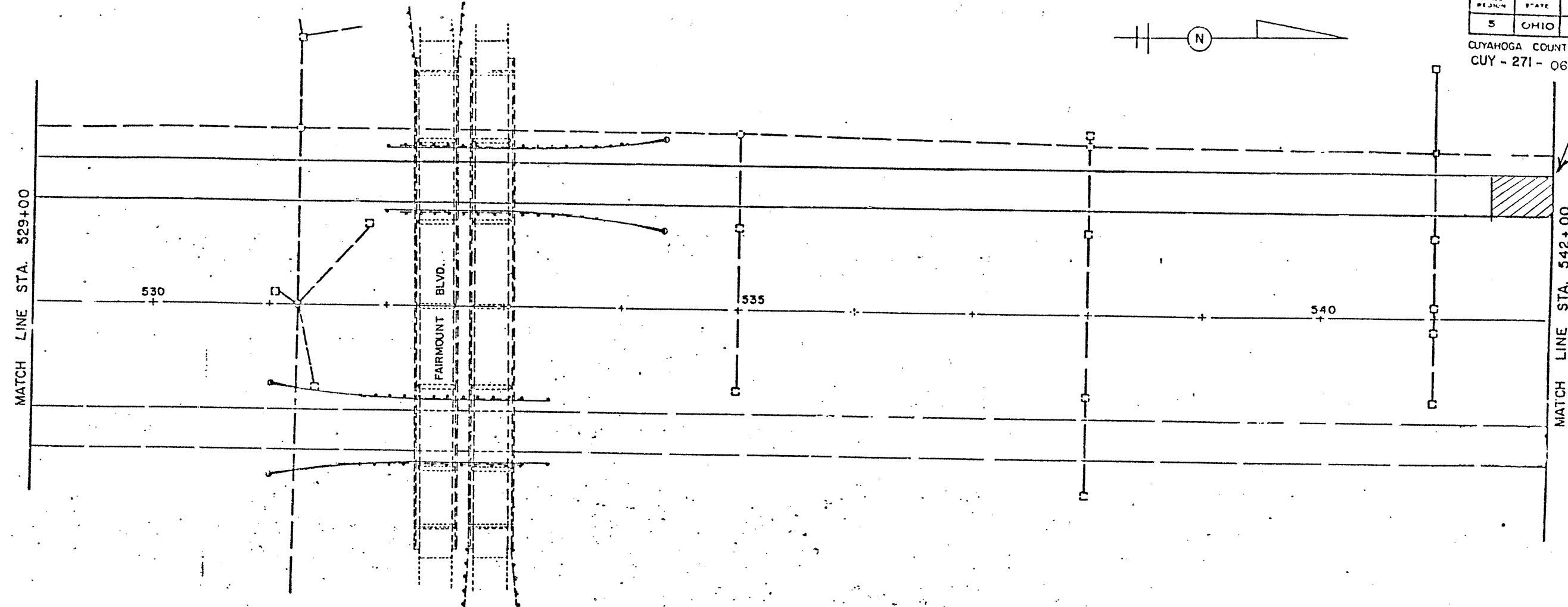
21
23

CUYAHOGA COUNTY
CUY - 271 - 0604

65



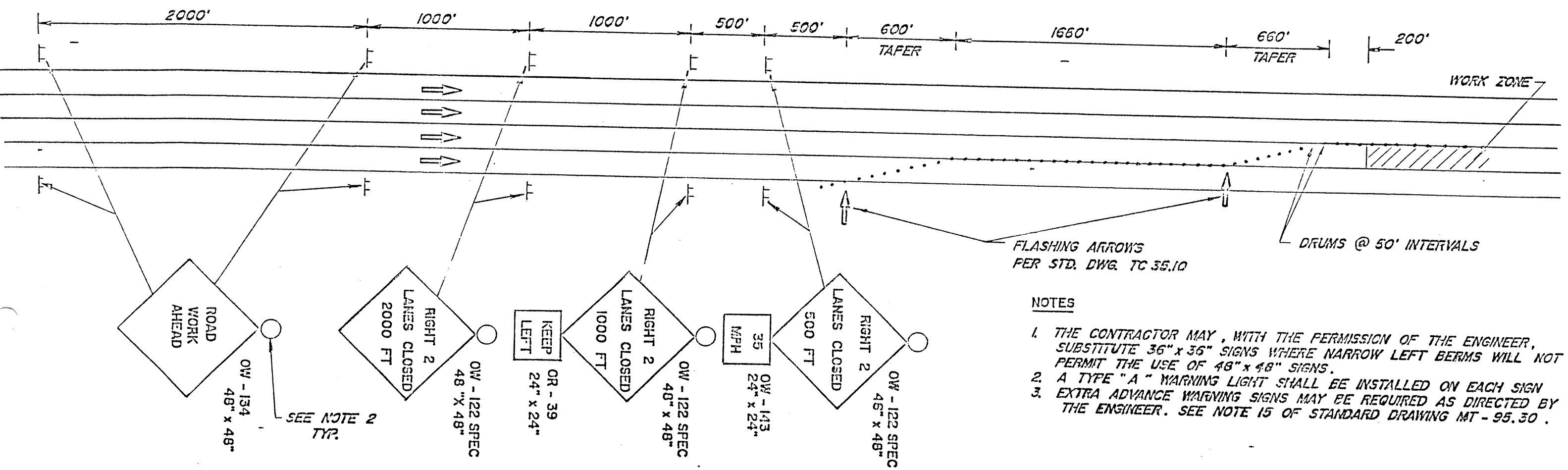
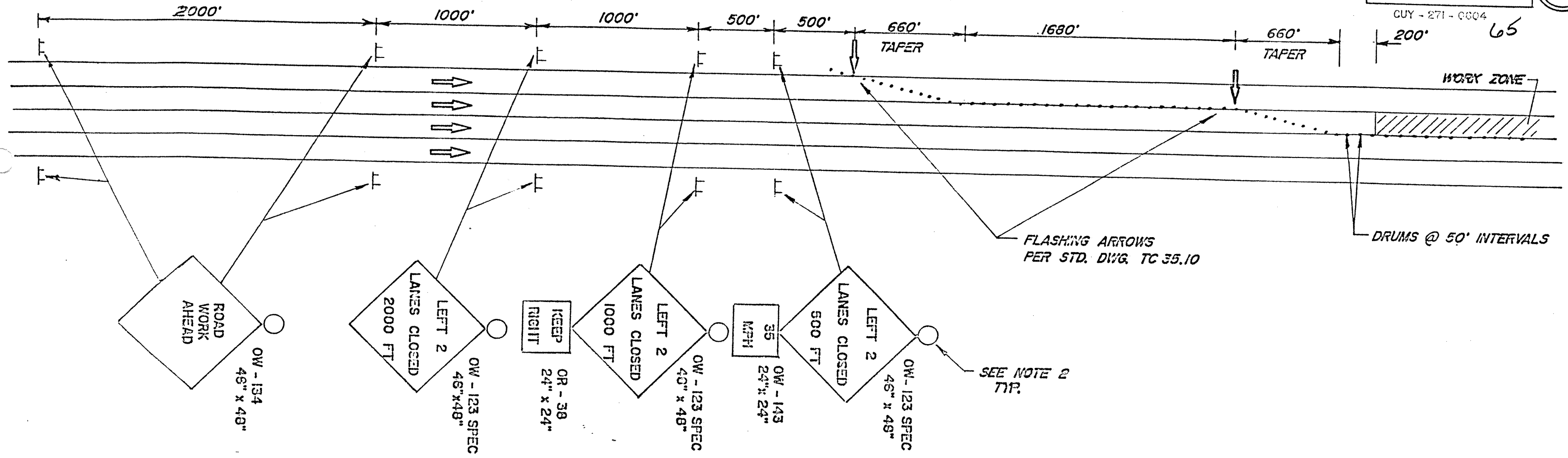
END WORK
STA. 542+00
SLM 9.92



STA. 529+00 TO STA. 555+00

TRAFFIC CONTROL DETAIL

PLAN NO. 22
23
 CUY - 271 - 0004
 65
 200'



NOTES

1. THE CONTRACTOR MAY, WITH THE PERMISSION OF THE ENGINEER, SUBSTITUTE 36" x 36" SIGNS WHERE NARROW LEFT BERMS WILL NOT PERMIT THE USE OF 48" x 48" SIGNS.
2. A TYPE "A" WARNING LIGHT SHALL BE INSTALLED ON EACH SIGN.
3. EXTRA ADVANCE WARNING SIGNS MAY BE REQUIRED AS DIRECTED BY THE ENGINEER. SEE NOTE 15 OF STANDARD DRAWING MT - 95.30.

CLOSING TWO LANES OF A MULTILANE DIVIDED HIGHWAY WITH DRUMS

M & R

GENERAL SUMMARY

PLAN NO. 65
CUY - 271 - 060423
23

ITEM	PART 1	ITEM	ITEM EXTENSION	GRAND TOTAL	UNIT	DESCRIPTION
202	3164	202	23500	3164	SQ. YD.	WEARING COURSE REMOVED
202	643	202	54100	643	EACH	RAISED PAVEMENT MARKERS REMOVED FOR STORAGE
862		862	00100	643	EACH	RAISED PAVEMENT MARKER
SPECIAL	127025	SPECIAL	40465000	127025	SQ. YD.	ASPHALT CONCRETE PAVEMENT SURFACE HEATER RECYCLING
SPECIAL	12703	SPECIAL	40466000	12703	GALLON	ASPHALT REJUVENATING AGENT
446	5144	446	01400	5144	CU. YD.	ASPHALT CONCRETE, SURFACE COURSE, TYPE I, AC-20
614	17.82	614		17.82	MILE	TEMPORARY EDGE LINE, CLASS I, 642 PAINT
614	14.88	614	20100	14.88	MILE	TEMPORARY LANE LINE, CLASS II, 642 PAINT
614	16858	614	23200	16858	LIN. FT.	TEMPORARY CHANNELIZING LINE, CLASS I, 642 PAINT
614	3250	614	25200	3250	LIN. FT.	TEMPORARY TRANSVERSE LINE, CLASS I, 642 PAINT
614	LUMP	614	11000	LUMP	LUMP	MAINTAINING TRAFFIC
617	426	617	10100	426	CU. YD.	COMPACTED AGGREGATE, TYPE A
SPECIAL	75	SPECIAL	61411100	75	HOURL	LAW ENFORCEMENT OFFICER WITH PATROL CAR
621	92739	642	30000	92739	LIN. FT.	REMOVAL OF PAVEMENT MARKINGS
624	LUMP	624	10000	LUMP	LUMP	MOBILIZATION
847	8.91	644	00100	8.91	MILE	EDGE LINE
847	7.44	644	00200	7.44	MILE	LANE LINE
847	8429	644	00400	8429	LIN. FT.	CHANNELIZING LINE
847	1625	644	00700	1625	LIN. FT.	TRANSVERSE LINE
847	880	644	00700	880	LIN. FT.	TRANSVERSE LINE 12"