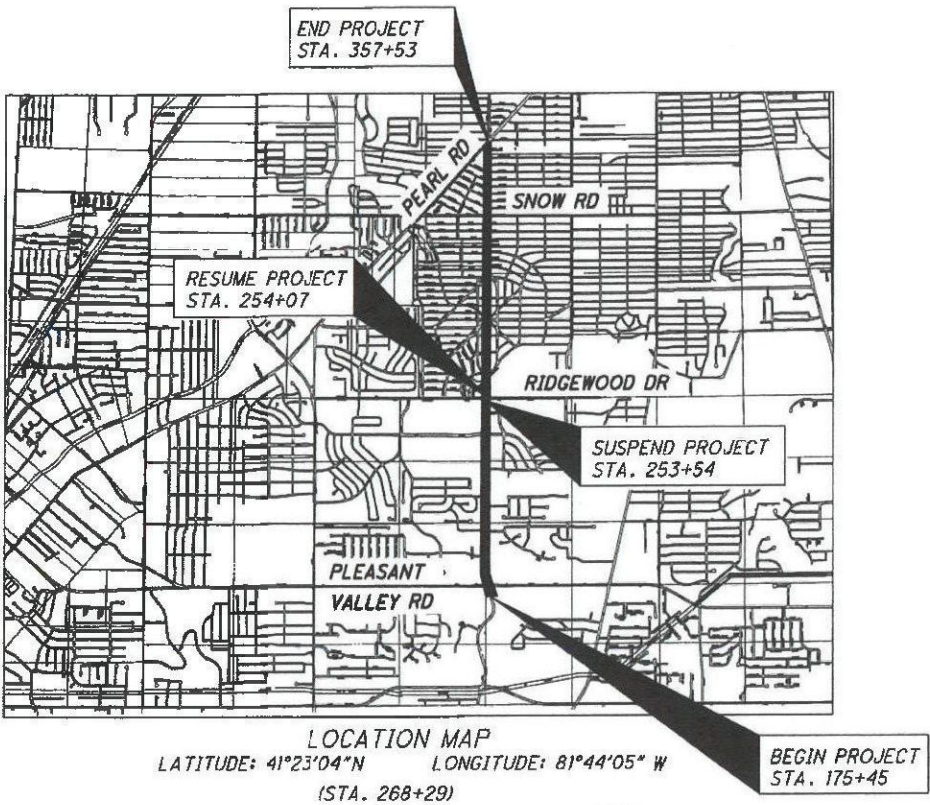


STATE OF OHIO  
 DEPARTMENT OF TRANSPORTATION  
**CUY-SR 003 06.22**  
**CITY OF PARMA**



**PROJECT DESCRIPTION**  
 CONCRETE REPAIRS OF APPROXIMATELY 1.5 MILES OF RIDGE ROAD FROM 300' SOUTH OF PLEASANT VALLEY ROAD TO RIDGEWOOD DRIVE. RESURFACING OF APPROXIMATELY 2.0 MILES OF RIDGE ROAD FROM RIDGEWOOD DRIVE TO PEARL ROAD. WORK WILL INCLUDE MILLING OF EXISTING ASPHALT AND REPLACEMENT, ALONG WITH FULL AND PARTIAL DEPTH REPAIRS, UTILITY ADJUSTMENTS, AND RECONSTRUCTING CURB RAMPS AND LANDINGS.

**EARTH DISTURBED AREAS**  
 PROJECT EARTH DISTURBED AREA: N/A\*  
 ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A\*  
 NOTICE OF INTENT EARTH DISTURBED AREA: N/A\*

\*MAINTENANCE PROJECT

**2019 SPECIFICATIONS**  
 THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT

**CUYAHOGA COUNTY**

**DESIGN DESIGNATION**

CURRENT ADT (2019)	23551
DESIGN YEAR ADT (2039)	29315
DESIGN HOURLY VOLUME (2039)	2540
DIRECTIONAL DISTRIBUTION	50%
TRUCKS (24 HOUR B&C)	---
DESIGN SPEED	25-35 MPH
LEGAL SPEED	25-35 MPH
DESIGN FUNCTIONAL CLASSIFICATION:	
URBAN OTHER PRINCIPAL ARTERIAL	
NHS PROJECT	NHS PRIMARY ROUTE

**INDEX OF SHEETS**

TITLE SHEET	1
SCHEMATIC PLAN	2-3
TYPICAL SECTIONS	4-6
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PLAN SHEETS	53-71
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PAVEMENT MARKING PLANS	73-93

**DESIGN EXCEPTIONS**

NONE REQUIRED

**UNDERGROUND UTILITIES**

CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

Call Before You Dig  
1-800-382-2764

(Non-members must be called directly)

1-800-925-0988

PLANS PREPARED BY:

614 West Superior Ave • Suite 1000 • Cleveland, OH 44113  
(216) 771-1090

ENGINEERS SEAL:



SIGNED:   
 DATE: 8/30/19

STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS				
CITY	ODOT							
	BP-1.1	7-28-00	TC-71.10	1-19-18	MT-105.10	7-19-13	801	10/18/19
	BP-2.1	7-17-15			MT-110.10	7-19-13	821	4/20/2012
	BP-2.2	7-18-08	MT-95.30	4-19-19			832	10/18/2018
			MT-95.31	4-19-19			875	1/18/19
	BP-2.5	7-19-13	MT-95.32	4-19-19			921	4/20/2012
			MT-95.50	7-21-17				
	BP-3.1	10-18-19	MT-95.60	4-19-19				
	BP-3.1	1-18-19	MT-95.61	4-19-19				
	BP-7.1	7-20-18						
			MT-99.20	4-19-19				
	RM-1.1	7-18-14						SPECIAL PROVISIONS
			MT-101.90	7-21-17				
	DM-1.1	7-21-17						
	DM-1.2	1-18-13	MT-102.20	4-19-19				

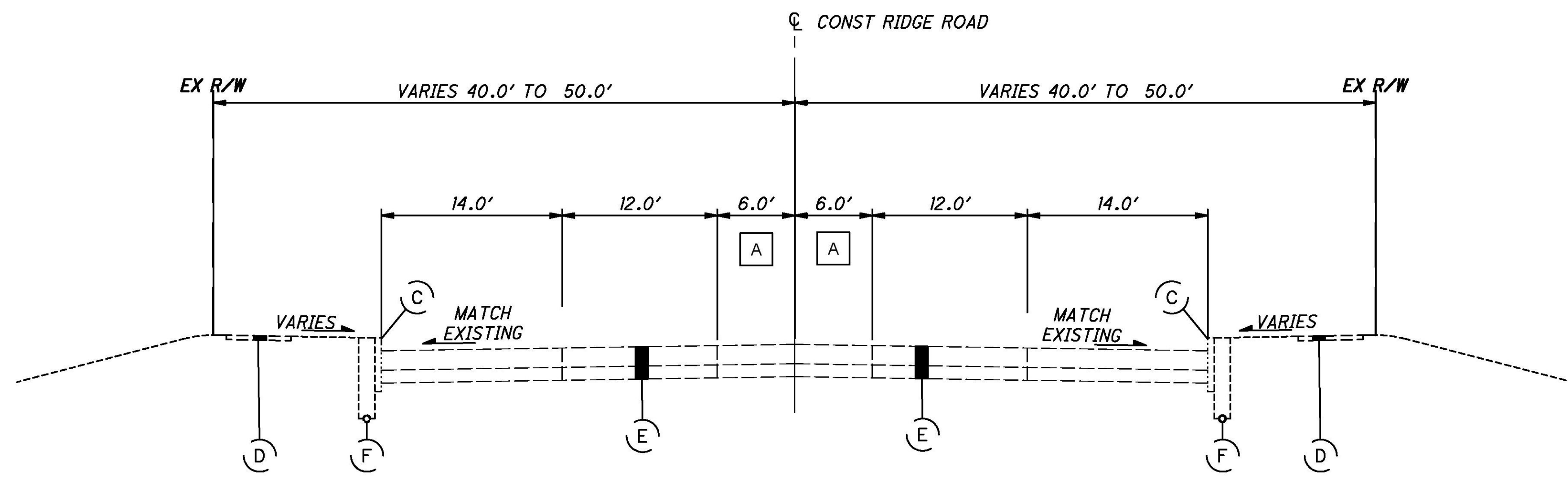
I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR MAINTENANCE OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES

APPROVED:   
 DATE: 9/4/19 DISTRICT DEPUTY DIRECTOR, DEPARTMENT OF TRANSPORTATION

APPROVED: \_\_\_\_\_  
 DATE: \_\_\_\_\_ DIRECTOR, DEPARTMENT OF TRANSPORTATION

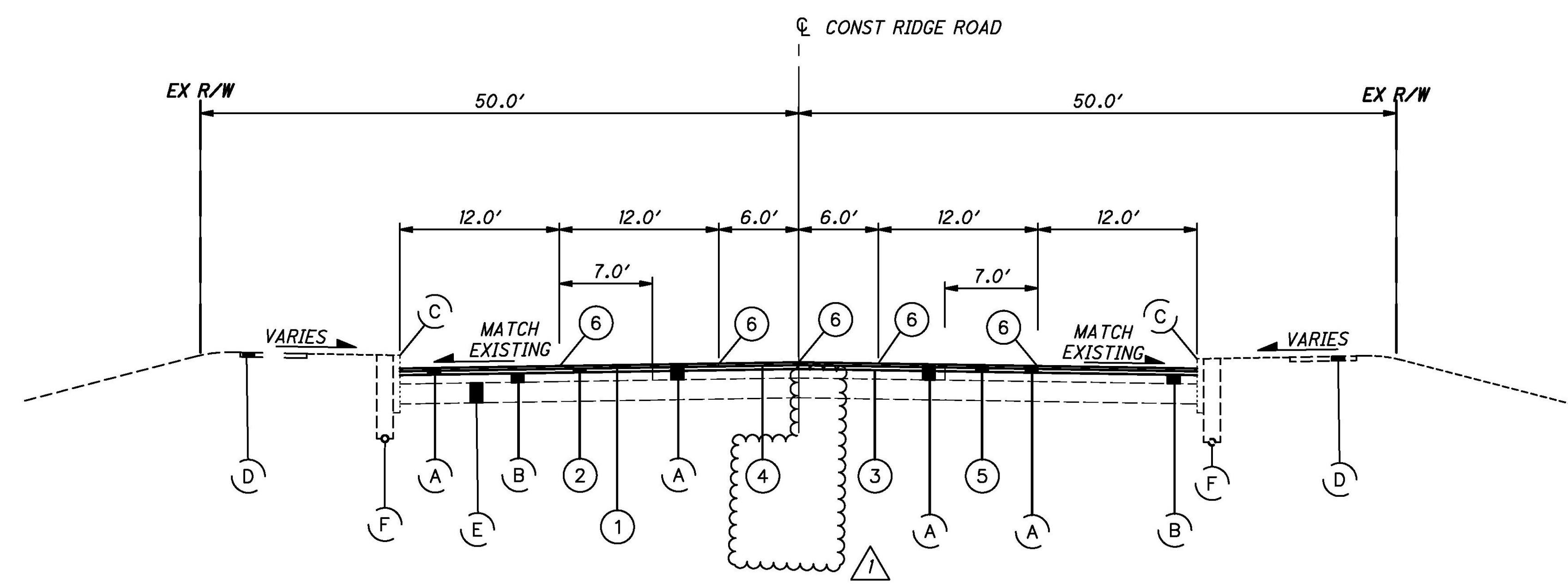
FEDERAL PROJECT NO.	<b>E (190) 256</b>
PID NO.	<b>105907</b>
CONSTRUCTION PROJECT NO.	
RAILROAD INVOLVEMENT	<b>NONE</b>
CUY-SR 003 06.22	
1	93

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TYPICAL SECTION - RIDGE ROAD  
STA. 175+45 TO STA. 253+54

- A VARIES FROM 6.0' AT STA. 183+42 TO 0.0' AT STA. 185+42  
0.0' FROM STA. 185+42 TO STA. 222+23  
VARIES FROM 0.0' AT STA. 222+23 TO 6.0' AT STA. 225+00  
6.0' FROM STA. 225+00 TO STA. 253+54



TYPICAL SECTION - RIDGE ROAD  
STA. 254+07 TO STA. 357+53

PROPOSED LEGEND

- ① ITEM 441 - ASPHALT CONCRETE SURFACE COURSE TYPE 1, PG70-22M, (T=1.25"), AS PER PLAN
- ② ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448), PG 64-22, (T=1.75")
- ③ ITEM 407 - NON-TRACKING TACK COAT
- ④ ITEM 407 - NON-TRACKING TACK COAT (FOR INTERMEDIATE COURSE)
- ⑤ ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (3"MIN)
- ⑥ ITEM 875 - LONGITUDINAL JOINT ADHESIVE
- ⑦ ITEM 605 - 6" BASE PIPE UNDERDRAIN WITH GEOTEXTILE FABRIC
- ⑧ ITEM 609 - CURB, TYPE 6
- ⑨ ITEM 609 - CURB, TYPE 6, AS PER PLAN
- ⑩ ITEM 609 - CURB, TYPE 2-A, AS PER PLAN
- ⑪ ITEM 659 - SEEDING AND MULCHING, CLASS 1
- ⑫ ITEM 608 - 4" CONCRETE WALK

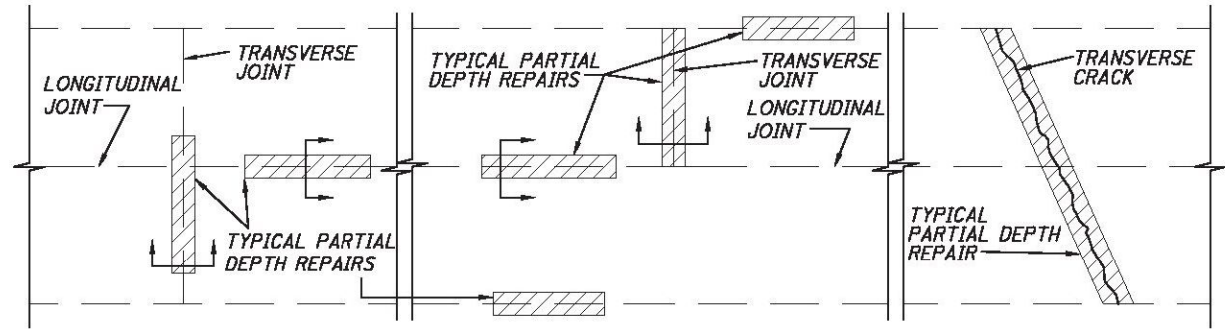
EXISTING LEGEND

- (A) - EXISTING ASPHALT CONCRETE, MIN=2"± TO MAX. 11.25"±
- (B) - EXISTING BRICK BASE 4"
- (C) - EXISTING CONCRETE OR SANDSTONE CURB
- (D) - EXISTING 4"± CONCRETE WALK
- (E) - EXISTING CONCRETE, MIN 0"± TO MAX. 10"±
- (F) - EXISTING UNCLASSIFIED PIPE UNDERDRAIN

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/13/2020

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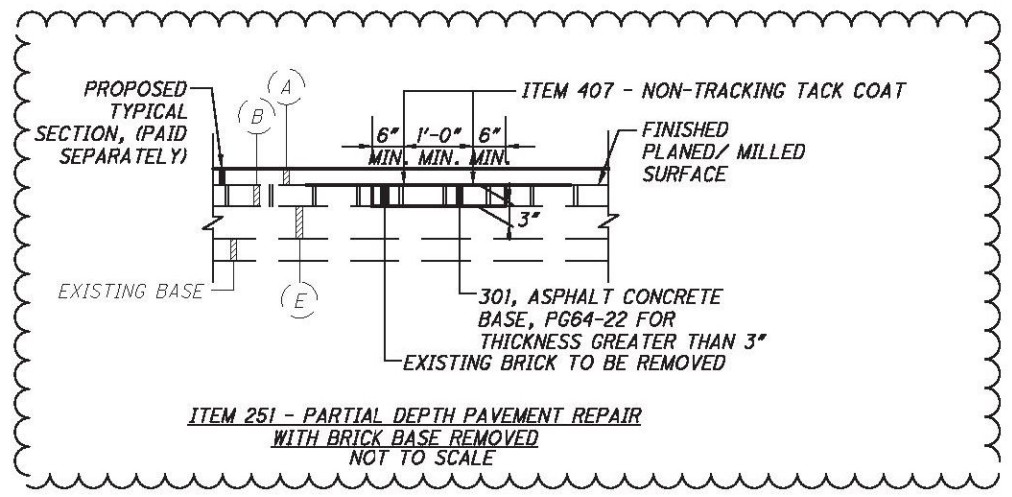
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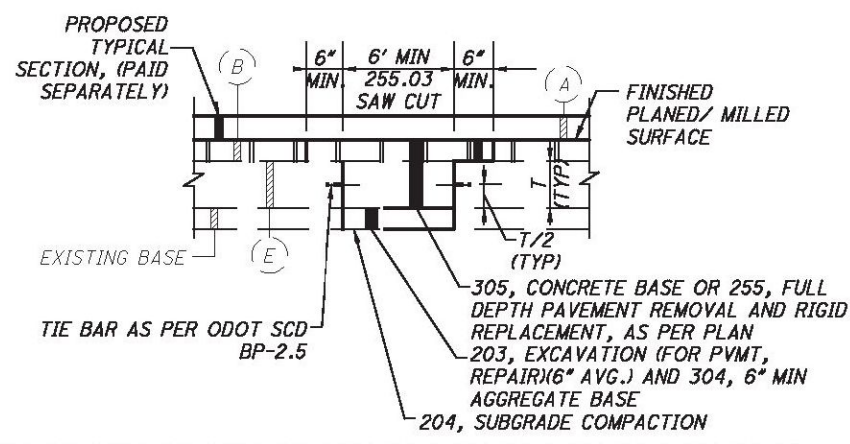
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR DETAIL PLAN NOT TO SCALE

NOTES:  
ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448), PG 64-22 (T=1.75" TYPICAL, 0" MIN) TO PROVIDE 4" TO 6" OF CURB REVEAL. MAINTAIN A 1.75" DEPTH WHERE POSSIBLE THROUGH ADDITIONAL MILLING AT THE CURB FACE. WHERE IT IS NOT POSSIBLE TO ACHIEVE THE CURB REVEAL OR A MINIMUM PAVEMENT SLOPE OF 0.016 WITH A 1.75" DEPTH, USE BP-3.1 AND A MINIMUM DEPTH OF INTERMEDIATE COURSE TO ACHIEVE.

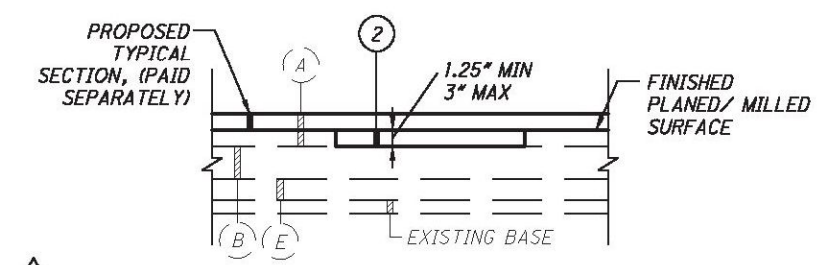
SAW CUT INCLUDED WITH PAVEMENT REMOVAL AND REPLACEMENT.



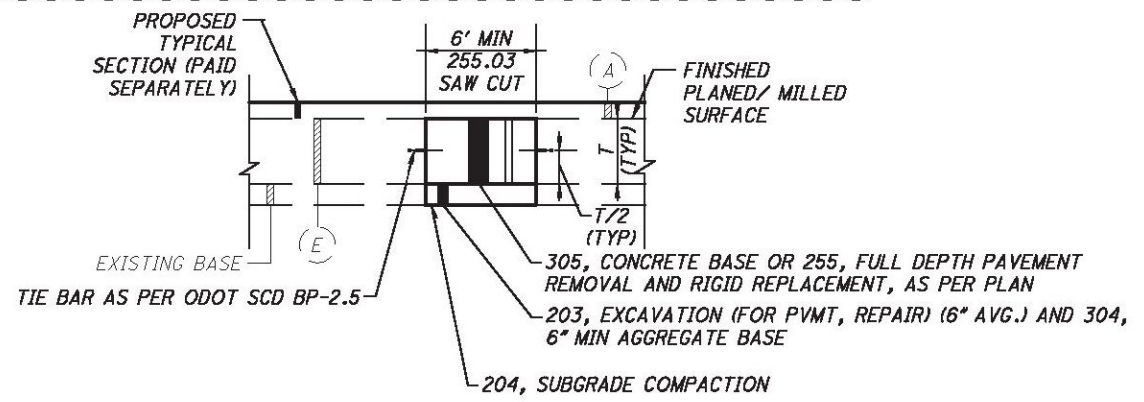
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR WITH BRICK BASE REMOVED NOT TO SCALE



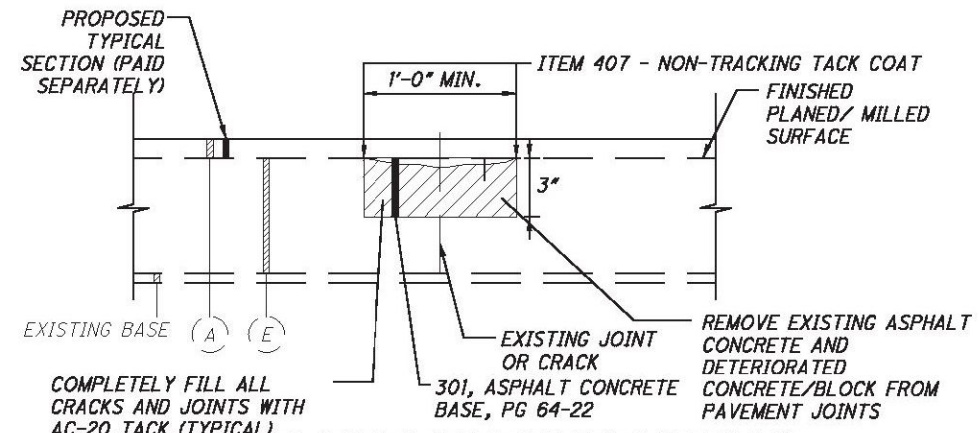
ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN ASPHALT CONCRETE WITH BRICK BASE NOT TO SCALE



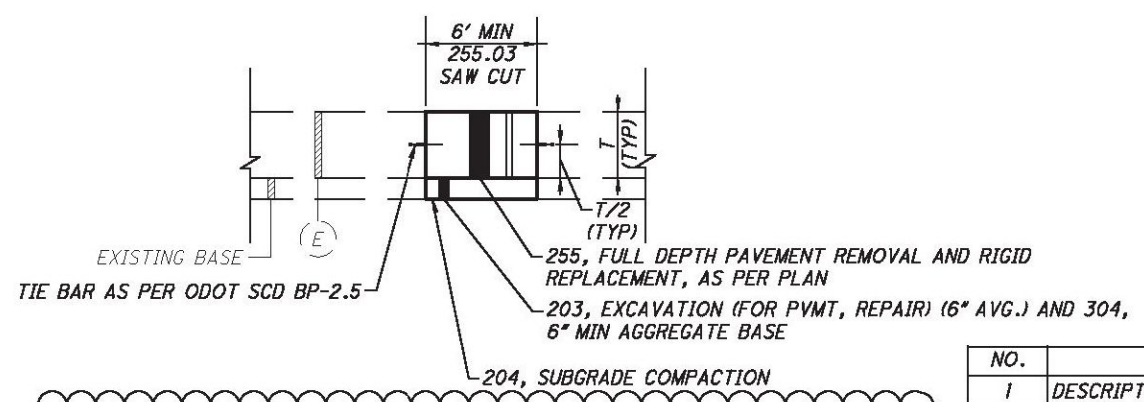
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR WITH BRICK BASE REMAINING NOT TO SCALE



ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN ASPHALT CONCRETE WITH CONCRETE BASE NOT TO SCALE



ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR WITH CONCRETE BASE NOT TO SCALE



ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN CONCRETE PAVEMENT NOT TO SCALE

DOWEL TABLE	
PAVEMENT THICKNESS	DIAMETER OF DOWELS
8" OR LESS	1"
9"	1 1/8"
10"	1 1/4"
OVER 10"	AS DIRECTED
DOWELS SHALL BE SPACED EVERY 30"	

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020

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PAVEMENT REPAIR DETAILS

CUY-SR 003 06.22

GENERAL

PROJECT DESCRIPTION

THIS PROJECT INVOLVES FULL DEPTH CONCRETE REPAIRS FROM PLEASANT VALLEY RD TO RIDGEWOOD DR, PLANING OF THE EXISTING ASPHALT OVERLAY AND PLACING A 1.25" ASPHALT CONCRETE SURFACE COURSE ON A 1.75" INTERMEDIATE ASPHALT CONCRETE COURSE AND PARTIAL AND FULL DEPTH REPAIRS FROM RIDGEWOOD DR TO PEARL RD.

THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED. THE PROFILE OF THE PROPOSED SURFACE WILL BE SIMILAR TO THAT OF THE EXISTING PAVEMENT.

RIGHT OF WAY

ALL WORK IS TO BE PERFORMED WITHIN THE EXISTING RIGHT OF WAY.

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

CITY OF CLEVELAND
DEPT. OF PUBLIC UTILITIES
DEPT. OF WATER
1201 LAKESIDE AVENUE
CLEVELAND, OHIO 44114
PHONE: (216) 664-2444
FAX: (216) 664-2378

CITY OF PARMA
6611 RIDGE RD
PARMA, OHIO 44129
PHONE: (440) 885-8000
FAX: (440) 885-8172

COLUMBIA GAS OF OHIO
7080 FRY ROAD
MIDDLEBURG HEIGHTS, OHIO 44130
PHONE: (440) 891-2428
FAX: (440) 826-3485

WINDSTREAM OHIO
929 MARTHA'S WAY
HIAWATHA, IA 52233
PHONE: (800) 289-1901

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER, OR ADJACENT TO, THE WORK AREA.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

EXISTING TYPICAL SECTIONS

EXISTING TYPICAL SECTIONS HAVE BEEN TAKEN FROM THE RECORDS AND ARE BELIEVED TO REPRESENT THE EXISTING PAVEMENT, BUT THE STATE DOES NOT GUARANTEE THE ACCURACY OF THE SECTION.

FOR FURTHER INFORMATION IN REGARD TO THE EXISTING TYPICAL SECTIONS, THE CONTRACTOR SHALL REFER TO THE PREVIOUS CONSTRUCTION PLANS.

THESE PLANS MAY BE REVIEWED AT:
OHIO DEPARTMENT OF TRANSPORTATION
DISTRICT 12 OFFICE
5500 TRANSPORTATION BOULEVARD
GARFIELD HEIGHTS, OHIO 44125

PLAN SHEET STATIONING

THE ROADWAY WAS NOT SURVEYED PRIOR TO THE PREPARATION OF THESE PLANS. PREVIOUS CONSTRUCTION PLAN STATIONING WAS USED TO PREPARE SHEETS AND CALCULATE ESTIMATED PAVEMENT AREA QUANTITIES AND PAVEMENT MARKING QUANTITIES.

ALL DIMENSIONS AND LOCATIONS OF JOINT LINES SHOWN ON THE PLAN SHEETS ARE APPROXIMATE. FINAL QUANTITIES TO BE DETERMINED IN THE FIELD BY THE ENGINEER.

CONSTRUCTION NOTIFICATION

THE CONTRACTOR WILL ADVISE THE PROJECT ENGINEER A MINIMUM OF FOURTEEN (14) DAYS PRIOR TO THE START OF CONSTRUCTION ACTIVITIES, LANE CLOSURE, AND ROAD CLOSURES. THE PROJECT ENGINEER WILL FORWARD THE INFORMATION TO THE STATE AND/OR CITY AND ANY OTHER LOCAL OFFICIALS RESPONSIBLE FOR PUBLIC NOTIFICATION.

CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK LISTED IN THE GENERAL SUMMARY FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED AT THE ENGINEER'S DISCRETION SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING THIS PROJECT.

CONSTRUCTION NOISE

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, WORK HOURS WILL BE RESTRICTED TO 7:00 AM TO 7:00 PM.

THE CONTRACTOR SHALL APPLY FOR A NOISE VARIANCE TO PERFORM NIGHT TIME PAVING. IN THE EVENT A NOISE VARIANCE IS NOT GRANTED BY THE DIRECTOR OF SAFETY, WORK SHALL COMMENCE DURING DAYTIME HOURS AT NO ADDITIONAL COST. ANY ADDITIONAL MAINTENANCE OF TRAFFIC COST WILL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 - MAINTAINING TRAFFIC, NO SEPARATE PAYMENT WILL BE MADE.

DO NOT OPERATE AT ANY TIME ANY DEVICE IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT. THE NOISE LEVEL RESULTING FROM CONSTRUCTION SHALL BE WITHIN THE LIMITS SPECIFIED IN OSHA REGULATIONS AND ALL LOCAL ORDINANCES.

ROADWAY

CLEARING AND GRUBBING

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

PROOF ROLLING

THE FOLLOWING QUANTITY IS PROVIDED IN THE GENERAL SUMMARY TO ADDRESS LOCATIONS REQUIRING PROOF ROLLING. SEE PLAN SHEET NO. 51 FOR ADDITIONAL INFORMATION.

ITEM 204 - PROOF ROLLING. . . . .9.0 HR

MONUMENT BOXES

THE CONTRACTOR AND FIELD ENGINEER SHALL FIELD CHECK ALL MONUMENT BOXES FOR ADJUSTMENT TO GRADE. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

ITEM 623 - MONUMENT BOX ADJUSTED TO GRADE. . . . .30 EACH

PAVEMENT

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO THE FOLLOWING ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. (PREVIOUS CONSTRUCTION PLANS, PROJECT NO. 13993 AND CUY-3-6.24, SHOWING THE ORIGINAL ALIGNMENT AND PROFILE, ARE AVAILABLE FOR INSPECTION AT THE ODOT DISTRICT 12 OFFICE). PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY WITH A UNIFORM THICKNESS OF 1.25 INCHES SURFACE COURSE AND 1.75 INCHES OF INTERMEDIATE COURSE (AS SHOWN ON THE TYPICAL SECTIONS).

CONTRACTION AND/OR EXPANSION

ALTHOUGH SPECIFIC LOCATIONS OF CERTAIN CONTRACTION AND EXPANSION JOINTS HAVE BEEN DETAILED ON THIS PLAN, NO WAIVER OF THE SPECIFICATIONS IS INTENDED. IN ALL CASES, THE PROVISION OF EXPANSION JOINTS AT ALL MAJOR STRUCTURES INCLUDING THE MAXIMUM SPACING BETWEEN CONTRACTION JOINTS IS IN ACCORDANCE WITH STANDARD CONSTRUCTION DRAWING BP-2.2 AND THE SPECIFICATIONS.

ASPHALT CONCRETE SURFACE COURSE SEALING REQUIREMENTS

IN ADDITION TO THE GUTTER SEALING REQUIREMENTS SPECIFIED ON SCD BP-3.1, IN 401.15 AND 401.17, AFTER COMPLETION OF THE SURFACE COURSE, THE CONTRACTOR SHALL SEAL, WITH A CERTIFIED PG BINDER, THE FOLLOWING LOCATIONS:

- ALL CASTINGS INCLUDING BUT NOT LIMITED TO MONUMENTS, MANHOLES, WATER VALVES, CATCH BASINS, CURB INLETS.
BUTT JOINTS AND FEATHER JOINTS INCLUDING BRIDGE APPROACHES.
FORWARD JOINT FOR DRIVEWAY ASPHALT AND TRAILING JOINT WHEN BUTTING TO EXISTING ASPHALT DRIVE.
PERIMETER OF ALL PAVEMENT REPAIRS OR OTHER ASPHALT INLAYS WHEN PAVEMENT REPAIRS/INLAYS ARE NOT OVERLAID WITH AN ASPHALT CONCRETE SURFACE COURSE.
ALL COLD TRANSVERSE CONSTRUCTION JOINTS PER 401.17.
ALL COLD LONGITUDINAL JOINTS BETWEEN PAVED SHOULDERS AND GUARDRAIL ASPHALT.

THE MATERIAL USED SHALL BE CERTIFIED 705.04 PG BINDER. THE WIDTH OF THE SEALER

SHALL BE 2 INCHES.

ANY ADDITIONAL COSTS ASSOCIATED WITH THE WORK IDENTIFIED IN THIS NOTE SHALL BE INCLUDED IN THE APPROPRIATE ASPHALT CONCRETE SURFACE COURSE ITEM OF WORK.

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN

THIS ITEM CONSISTS OF FULL DEPTH PAVEMENT REPLACEMENT PER ODOT CMS ITEM 255, WITH THE FOLLOWING EXCEPTIONS: WORK FOR THIS ITEM SHALL INCLUDE ITEM 255 - SAW CUTTING. WORK FOR THIS ITEM SHALL EXCLUDE ITEM 203 - EXCAVATION OF 6" AVE BASE FOR PAVT REPAIR, ITEM 204 - SUBGRADE COMPACTION, ITEM 304 - 6" MIN AGREGATE BASE AND REMOVAL AND INSTALLATION OF WEARING COURSE, WHERE REQUIRED. THESE ITEMS ARE QUANTIFIED SEPARATELY ON SHEET 51, AS DETAILED ON SHEET 5.

ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG 70-22M, AS PER PLAN

THE COARSE VIRGIN AGGREGATE FOR THIS ITEM SHALL BE A BLEND OF 60% MIN. AIR COOLED BLAST FURNACE SLAG (ACBFS) OR TRAP ROCK FROM ONTARIO WITH LIMESTONE COMPRISING THE REMAINING PERCENTAGE.

ITEM 609 - CURB, TYPE 2-A, AS PER PLAN

THIS ITEM SHALL INCLUDE THE INSTALLATION OF THE CURB AND 1'0" WIDTH OF CONCRETE PAVEMENT, MEASURED FROM THE FACE OF THE CURB. THE CONCRETE PAVEMENT COMPOSITION SHALL MATCH THAT SHOWN ON THE "FULL DEPTH CONCRETE PAVEMENT REPAIR" DETAIL SHOWN ON SHEET 5. THE COST OF THE INSTALLATION OF THE CURB AND CONCRETE PAVEMENT IS INCLUDED IN THE CONTRACT BID PRICE FOR ITEM 609 - CURB TYPE 2-A, AS PER PLAN

ITEM 609 - CURB, TYPE 6, AS PER PLAN

THE CONTRACTOR SHALL INSTALL THE CURB, AT THE DIRECTION OF THE ENGINEER, TO ACHIEVE A DESIRED CURB REVEAL OF 6 INCHES, BUT NO LESS THAN A 4 INCH REVEAL. THE INTENT OF THE CURB INSTALLATION IS TO OBTAIN GRADING BEHIND THE CURB THAT GENERALLY SLOPES TOWARDS THE PAVEMENT. FOR EACH LOCATION OF INSTALLATION, THE CONTRACTOR SHALL DETERMINE THE MAXIMUM CURB HEIGHT POSSIBLE, BETWEEN 4 AND 6 INCHES, AND OBTAIN APPROVAL OF THE ENGINEER PRIOR TO THE CURB INSTALLATION. THE COST OF CURB LAYOUT AND INSTALLATION IS INCLUDED IN THE CONTRACT BID PRICE FOR ITEM 609 - CURB TYPE 6, AS PER PLAN.

ITEM 875 - LONGITUDINAL JOINT ADHESIVE

SEE SUPPLEMENTAL SPECIFICATION 875 - HOT APPLIED ASPHALTIC JOINT ADHESIVE MATERIAL AND CONSTRUCTION REQUIREMENTS.

ALTERNATE BID ITEM - SINGLE CHIP SEAL INTERLAYER

AT THE DIRECTION OF THE ENGINEER, A CHIP SEAL INTERLAYER MAY BE INCORPORATED AS PART OF THE RESURFACING PAVEMENT BUILDUP. IF SO DIRECTED BY THE ENGINEER, THE FOLLOWING ITEMS SHALL BE NON-PERFORMED.

ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448). PG 64-22 (T=1.75")

IN LIEU OF THE NON-PERFORMED ITEMS ABOVE, THE ITEMS AND QUANTITIES BELOW HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER TO INCORPORATE A CHIP SEAL INTERLAYER INTO THE RESURFACING PAVEMENT BUILDUP.

ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448), PG 64-22 (T=1.25") . . . . .2,396 CY
ITEM 422 - SINGLE CHIP SEAL, TYPE A. . . . .68,977 SY

PLAN LOCATION OF THE ALTERNATE BID ITEMS SHALL MATCH THE INTERMEDIATE COURSE AS SHOWN IN THE TYPICAL SECTIONS, SHEET 4. ITEM 407 NON-TRACKING TACK COAT SHALL BE APPLIED TO FINISH CHIP SEAL SURFACE PRIOR TO PLACEMENT OF INTERMEDIATE COURSE.

ITEM 422 - SINGLE CHIP SEAL, TYPE A SHALL BE CONSTRUCTED AS PER ODOT CMS ITEM 422. WHEN USED AS AN INTERLAYER, EXCLUDE APPLICATION OF FOG SEAL.

Table with 4 columns: NO., DESCRIPTION, REV. BY, DATE. Row 1: 1, DESCRIPTION REVISIONS, SMM, 1/13/2020.

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DESIGNED SMM CHECKED CSR

GENERAL NOTES

CUY-SR 003 06.22

DRIVEWAY APRON AND CURB REPLACEMENT

THE ENGINEER MIGHT DESIGNATE ADDITIONAL DRIVEWAY APRONS FOR REMOVAL AT LOCATIONS WHERE EXISTING CURB TO BE REMOVED AND REPLACED ADJACENT TO EXISTING PAVEMENT.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER FOR RESIDENTIAL AND COMMERCIAL DRIVEWAY APRON REPLACEMENTS.

- 202, PAVEMENT REMOVED . . . . . 270 SY
- 204, SUBGRADE COMPACTION . . . . . 270 SY
- 304, AGGREGATE BASE . . . . . 49 CY
- 452, 6" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS . . . . . 241 SY
- 452, 8" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS . . . . . 29 SY

DRAINAGE

EXISTING SUBSURFACE DRAINAGE

PROVIDE UNOBSTRUCTED OUTLETS FOR ALL EXISTING UNDERDRAINS OR AGGREGATE DRAINS ENCOUNTERED DURING CONSTRUCTION.

PROVIDE AN OUTLET PER STANDARD CONSTRUCTION DRAWING DM-1.1 FOR ALL UNDERDRAINS THAT OUTLET TO A SLOPE. UNDERDRAINS THAT CAN BE CONNECTED TO THE NEW OR EXISTING UNDERDRAINS AT THE END OF THE PROJECT LIMITS AS WELL AS ALL NECESSARY BENDS OR BRANCHES REQUIRED FOR CONNECTION ARE INCLUDED IN THE BASIS OF PAYMENT FOR UNCLASSIFIED PIPE UNDERDRAINS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE WORK NOTED ABOVE:

- 605, 6" BASE PIPE UNDERDRAINS WITH GEOTEXTILE FABRIC. . . . . 10,866 FT

REVIEW OF DRAINAGE FACILITIES

BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE BY THE CITY, REPRESENTATIVES OF THE CITY AND THE CONTRACTOR, ALONG WITH LOCAL REPRESENTATIVES, SHALL MAKE AN INSPECTION OF ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCE SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING BY THE CITY.

ALL NEW CONDUITS, INLETS, CATCH BASINS, AND MANHOLES CONSTRUCTED AS PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEAN CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE CITY.

ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE MENTIONED PARTIES SHALL BE MAINTAINED AND LEFT IN A CONDITION REASONABLY COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. ANY CHANGE IN THE CONDITION RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEMS.

SEEDING AND MULCHING

THE FOLLOWING QUANTITIES ARE PROVIDED TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDED AREAS:

- 659, SOIL ANALYSIS TEST. . . . . .1 EACH
- 659, SEEDING AND MULCHING. . . . . 1,286 SQYD
- 659, REPAIR SEEDING AND MULCHING . . . . . 64.5 SQYD
- 659, INTER-SEEDING. . . . . 64.5 SQYD
- 659, COMMERCIAL FERTILIZER. . . . . 0.02 TON
- 659, LIME. . . . . 0.27 ACRES
- 659, WATER. . . . . 8.0 MGAL

SEEDING AND MULCHING SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE RIGHT-OF-WAY LINES, AND WITHIN THE CONSTRUCTION LIMITS FOR AREAS OUTSIDE THE RIGHT-OF-WAY LINES COVERED BY WORK AGREEMENT OR SLOPE EASEMENT. QUANTITY CALCULATES FOR SEEDING AND MULCHING ARE BASED ON THESE LIMITS.

CASTINGS ADJUSTED TO GRADE

ITEM 611 - MANHOLE, CATCH BASIN, OR INLET RECONSTRUCTED TO GRADE

THE CONTRACTOR AND FIELD ENGINEER SHALL FIELD CHECK ALL EXISTING CATCH BASINS, INLETS AND MANHOLES LOCATED WITHIN THE LIMITS OF THE PROJECT. ANY CATCH BASIN,

INLET OR MANHOLE FOUND THAT EXHIBITS SUBSTANTIAL DETERIORATION AND REQUIRES MORE WORK THAN IS SPECIFIED UNDER CASTINGS ADJUSTED TO GRADE, SHALL BE RECONSTRUCTED TO GRADE AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

- ITEM 611 - CATCH BASIN RECONSTRUCTED TO GRADE. . . . . .5 EACH
- ITEM 611 - INLET RECONSTRUCTED TO GRADE . . . . . 2 EACH
- ITEM 611 - MANHOLE RECONSTRUCTED TO GRADE. . . . . .5 EACH

THE TIME BETWEEN ADJUSTING THE CASTINGS AND RESURFACING SHALL BE KEPT TO AN ABSOLUTE MINIMUM. NO ADJUSTING RINGS SHALL BE PERMITTED.

ITEM SPECIAL - MISCELLANEOUS METAL

EXISTING CASTINGS MAY PROVE TO BE UNSUITABLE FOR REUSE, AS DETERMINED BY THE ENGINEER. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE THE CASTINGS OF THE REQUIRED TYPE, SIZE AND STRENGTH (HEAVY) FOR THE PARTICULAR STRUCTURE IN QUESTION. ALL MATERIALS SHALL MEET ITEM 611 OF THE SPECIFICATIONS AND SHALL HAVE THE PRIOR APPROVAL OF THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

- SPECIAL, MISCELLANEOUS METAL. . . . . 8500 LBS

THE CONTRACTOR IS CAUTIONED TO USE EXTREME CARE IN THE REMOVAL, STORAGE AND REPLACEMENT OF ALL EXISTING CASTINGS. CASTINGS DAMAGED BY THE NEGLIGENCE OF THE CONTRACTOR, AS DETERMINED BY THE ENGINEER, SHALL BE REPLACED WITH THE PROPER NEW CASTINGS AT THE EXPENSE OF THE CONTRACTOR.

TRAFFIC CONTROL

ITEM 632 - DETECTOR LOOP, AS PER PLAN

AN ESTIMATED QUANTITY OF ITEM 632, DETECTOR LOOP, AS PER PLAN HAS BEEN PROVIDED FOR THE PURPOSE OF REPLACING DAMAGED DETECTOR LOOPS AND/OR UPGRADING DETECTOR LOOPS TO IMPROVE MOTORCYCLE DETECTION. IT IS IMPERATIVE THAT THE REPLACEMENT OF DETECTOR LOOPS BE INSTALLED AND FULLY FUNCTIONAL IN THE SHORTEST POSSIBLE TIME. THE CONTRACTOR SHALL HAVE REPLACEMENT DETECTOR LOOPS INSTALLED AND FULLY FUNCTIONAL WITHIN 7 CALENDAR DAYS OF DESTRUCTION OF THE EXISTING DETECTOR LOOPS.

THE CONTRACTOR SHALL NOTIFY THE CITY OF PARMA SERVICE DEPARTMENT (440-885-8191) AND DAN GOOD OF THE PARMA PUBLIC SAFETY DEPARTMENT (OFFICE 216-849-6408 CELL 440-476-1230) 5 WORKING DAYS IN ADVANCE OF ANY PLANING OPERATIONS OR PAVEMENT REPAIR WORK THAT WILL DAMAGE THE DETECTOR LOOP INSTALLATIONS. THE CONTRACTOR SHALL SCHEDULE/COORDINATE TEMPORARY SIGNAL TIMING MODIFICATIONS FOR THE PERIOD WHEN THE DETECTOR LOOPS ARE OUT OF OPERATION. THE CONTRACTOR SHALL THEN RE-NOTIFY THE CITY OF PARMA WITHIN 2 WORKING DAYS AFTER THE DAMAGED DETECTOR LOOPS ARE REPLACED.

FAILURE TO COMPLY WITH THE ABOVE STATE REQUIREMENTS WILL RESULT IN THE ASSESSMENT OF LIQUIDATED DAMAGES ACCORDING TO SECTION 108.07 OF THE CMS FOR EACH CALENDAR DAY BEYOND THE SPECIFIED LIMIT.

THE NEW LOOP DETECTORS SHALL BE AT THE LOCATION OF THE CURRENT LOOPS AND APPROVED BY THE ENGINEER IN WRITING PRIOR TO INSTALLATION. THE DETECTOR LOOPS SHALL NOT BE CUT INTO THE SURFACE COURSE. RUN THE WIRE CONTINUOUSLY AROUND THE LOOP PERIMETER AND THROUGH THE SLOT LEADING TO THE PAVEMENT EDGE AND BY UNDERGROUND CONDUIT TO A ROADSIDE PULL BOX FOR SLACK AND SPLICE. THE CONTRACTOR MAY ONLY SPLICE THE LOOP DETECTOR LEAD-IN CABLE IN A PULLBOX.

STOP BAR DETECTOR LOOPS SHALL BE 6' X 6' RECTANGULAR DETECTOR LOOPS.

IN ADDITION TO THE REQUIREMENTS OF CMS 632.11, THE CONTRACTOR SHALL PROVIDE A POSITIVE AND EFFECTIVE MEANS FOR REMOVAL OF SOLID RESIDUE RESULTING FROM DRY SAW BLADE CUTTING OF LOOP DETECTOR SLOTS IN THE PAVEMENT. THE RESIDUE SHALL BE REMOVED BY VACUUM OR OTHER EFFECTIVE MEANS, BEFORE IT IS BLOWN BY TRAFFIC ACTION OR WIND.

LOOP DETECTOR WIRE TO LEAD-IN CABLE SPLICES WITHIN EPOXY ENCAPSULATED SPLICE ENCLOSURES SHALL BE JOINED BY AN APPROVED CONNECTOR AND SOLDERED PER CMS 632.23 & 725.15. ALL COSTS ASSOCIATED WITH THE SOLDERED SPLICE CONNECTION AND EPOXY SPLICE KIT SHALL BE INCLUDED WITH THE DETECTOR LOOP.

LOOP DETECTOR WIRE ROUTED THROUGH CONDUIT, PULL BOXES, POLES AND PEDESTALS SHALL BE TWISTED PER CMS 632.23.

FURNISH ALL MATERIALS ACCORDING TO ODOT'S QUALIFIED PRODUCTS LIST (QPL).

PAYMENT FOR ALL OF THE ABOVE SHALL BE INCLUDED IN THE UNIT BID PRICE PER EACH FOR ITEM 632, DETECTOR LOOP, AS PER PLAN. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

- ITEM 632 - DETECTOR LOOP, AS PER PLAN . . . . . 78 EACH

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GENERAL NOTES

CUY-SR 003 06.22

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SHEET NUM.				PART.		ALT	ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	DESIGNED SMM	CHECKED CSR
7	8	51	52	01/NHS/P V	02/NFP/OT	(X)								
											ROADWAY			
			LS				201	11000	LS		CLEARING AND GRUBBING			
				4,104			202	30000	4,104	SF	WALK REMOVED			
				10,866			202	32000	10,866	FT	CURB REMOVED			
			270				202	23000	270	SY	PAVEMENT REMOVED			
				432			203	10000	432	CY	EXCAVATION			
				3,047		1,958	203	10000	3,047	CY	EXCAVATION (FOR PVMT REPAIR)(6" AVG.)			
			270				204	10000	18,539	SY	SUBGRADE COMPACTION			
							204	45000	9	HOUR	PROOF ROLLING			
							608	10000	3,548	SF	4" CONCRETE WALK			
							608	52000	556	SF	CURB RAMP			
							608	53020	36	SF	DETECTABLE WARNING			
							623	39500	32	EACH	MONUMENT BOX ADJUSTED TO GRADE			
											EROSION CONTROL			
				1			659	00100	1	EACH	SOIL ANALYSIS TEST			
				1,286			659	00500	1,286	SY	SEEDING AND MULCHING, CLASS 1			
				64.5			659	14000	64.5	SY	REPAIR SEEDING AND MULCHING			
				64.5			659	15000	64.5	SY	INTER-SEEDING			
				0.02			659	20000	0.02	TON	COMMERCIAL FERTILIZER			
				0.27			659	31000	0.27	ACRE	LIME			
				8			659	35000	8	MGAL	WATER			
											DRAINAGE			
							605	14020	10,866	FT	6" BASE PIPE UNDERDRAINS WITH GEOTEXTILE FABRIC			
							611	98630	9	EACH	CATCH BASIN ADJUSTED TO GRADE			
				5			611	98634	5	EACH	CATCH BASIN RECONSTRUCTED TO GRADE			
				2			611	99154	2	EACH	INLET RECONSTRUCTED TO GRADE			
							611	99654	11	EACH	MANHOLE ADJUSTED TO GRADE			
				5			611	99660	5	EACH	MANHOLE RECONSTRUCTED TO GRADE			
				8,500			SPECIAL	61199820	8,500	LB	MISCELLANEOUS METAL			8
											PAVEMENT			
				1,234			251	01000	1,234	SY	PARTIAL DEPTH PAVEMENT REPAIR (441)			
				68,977			254	01000	68,977	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 3" MIN			
				18,269			255	10011	18,269	SY	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC1, AS PER PLAN			7
				103			301	46000	103	CY	ASPHALT CONCRETE BASE, PG64-22			
			49	3,047		1,958	304	20000	3,096	CY	AGGREGATE BASE			
				11,040			407	20000	11,040	GAL	NON-TRACKING TACK COAT			
							441	10101	2,401	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), AS PER PLAN , PG70-22M			7
							441	50300	3,354	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448), PG64-22	△		
			241				452	10050	241	SY	6" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS			
			29				452	12050	29	SY	8" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS			
							609	14001	61	FT	CURB, TYPE 2-A, AS PER PLAN			7
							609	26000	7,017	FT	CURB, TYPE 6			
							609	26001	3,788	FT	CURB, TYPE 6, AS PER PLAN			7
							875	10000	10,346	LB	LONGITUDINAL JOINT ADHESIVE	△		
											PAVEMENT ALTERNATES			
							68,977	422	10000	68,977	SY	SINGLE CHIP SEAL, TYPE A		7
							2,396	441	50200	2,396	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448), PG64-22		7
											WATER WORK			
							638	10800	75.0	EACH	VALVE BOX ADJUSTED TO GRADE			

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/13/2020

GENERAL SUMMARY CUY-SR 003 06.22

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REF NO.	SHEET NO.	STATION TO STATION		441	441	605	608	608	608	609	609	609	611	611	623	638	875
			TO	CY	CY	FT	SF	SF	SF	FT	FT	FT	EACH	EACH	EACH	EACH	LB
	53	175+45.00	185+00.00														
	54	185+00.00	195+00.00			10				10							
	55	195+00.00	205+00.00														
	56	205+00.00	215+00.00											2		1	
	57	215+00.00	225+00.00						24					1			
	58	225+00.00	235+00.00			30				30						2	
	59	235+00.00	245+00.00			21				21							
	60	245+00.00	255+00.00	22	31	73								1		1	
	61	255+00.00	265+00.00											3		5	93
	62	265+00.00	275+00.00	232	324	425							1			10	1000
	63	275+00.00	285+00.00	232	324	1924										5	1000
	64	285+00.00	295+00.00	232	324	1852		284					3			6	1000
	65	295+00.00	305+00.00	232	324	1775							2			12	1000
	66	305+00.00	315+00.00	232	324	1924	1099									4	1000
	67	315+00.00	325+00.00	232	324	1293		39	12							4	1000
	68	325+00.00	335+00.00	232	324	389		1348	79							6	1000
	69	335+00.00	345+00.00	232	324	245		406					1			7	1000
	70	345+00.00	355+00.00	232	324	441		97	93				1	1	1	6	1000
	71	355+00.00	357+53.00	232	324	464		598	61				1	2	1	5	1000
				59	83									1		1	253
<b>TOTALS CARRIED TO GENERAL SUMMARY</b>				2401	3354	10866	3548	556	36	61	7017	3788	9	11	2	75	10346

ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), AS PER PLAN PG10-22M (T=1.25")  
 ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448) PG64-22 (T=1.75")



NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/13/2020

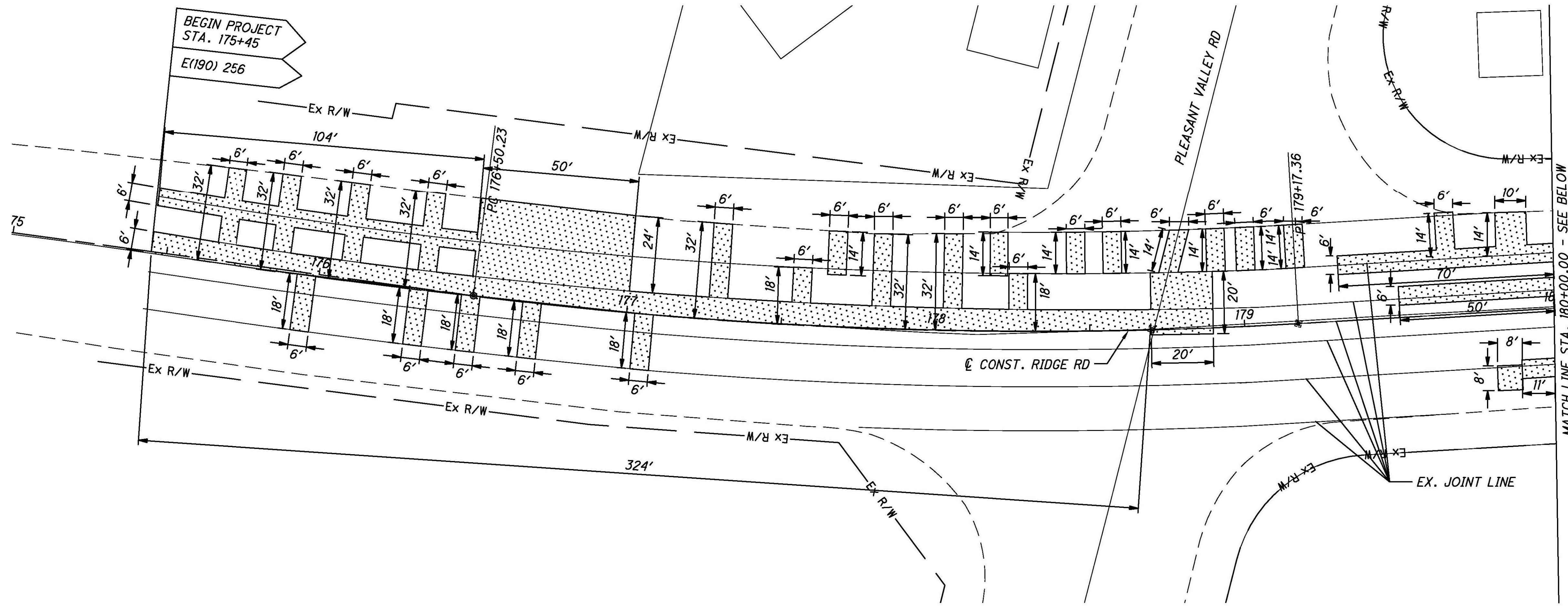
ROADWAY SUBSUMMARY

CUY-SR 003 06.22

DESIGNED SMM CHECKED GB

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93

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**LEGEND**

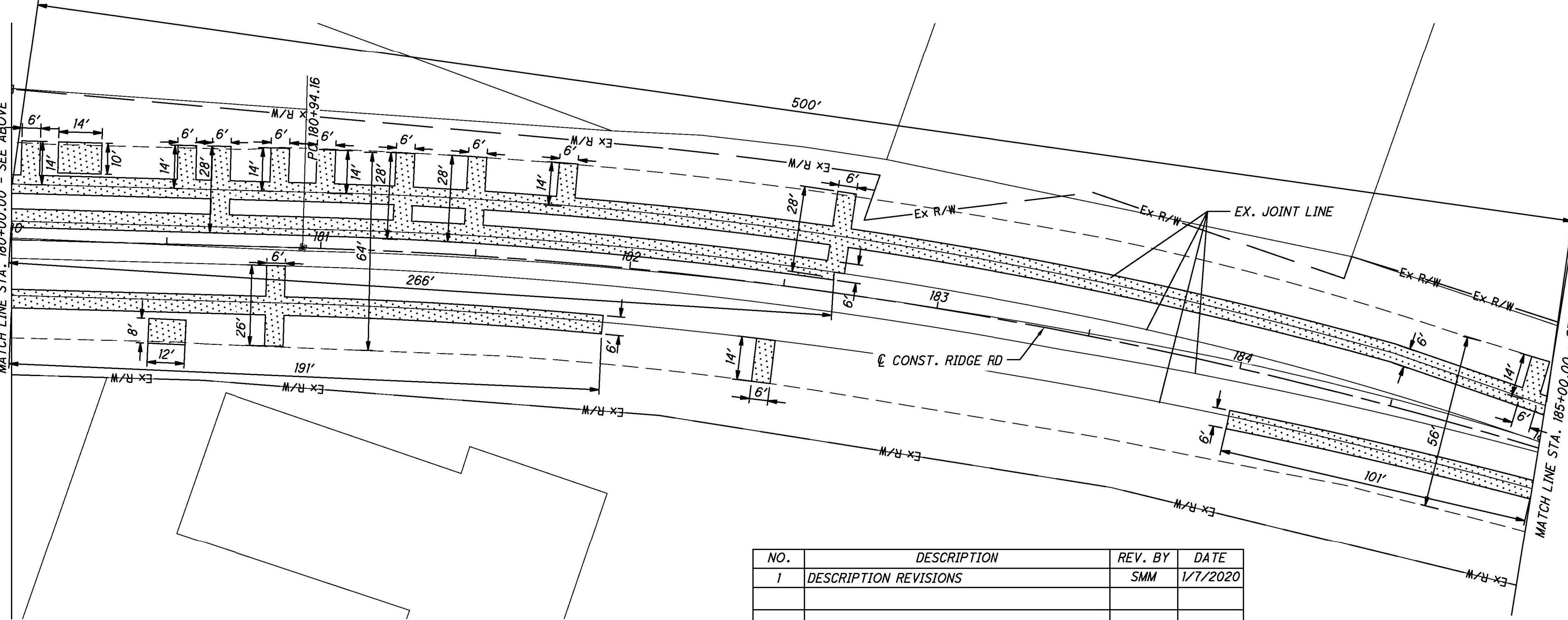
- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

4" CONCRETE WALK

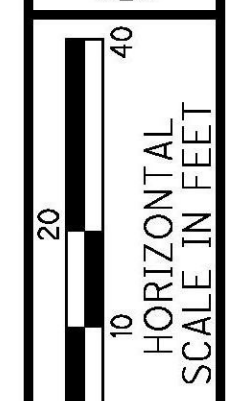
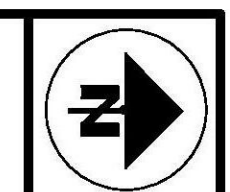


NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020

RIDGE RD PLAN/PLAN  
 STA. 175+00 TO STA. 185+00  
 CUY-SR 003 06.22  
 53  
 93



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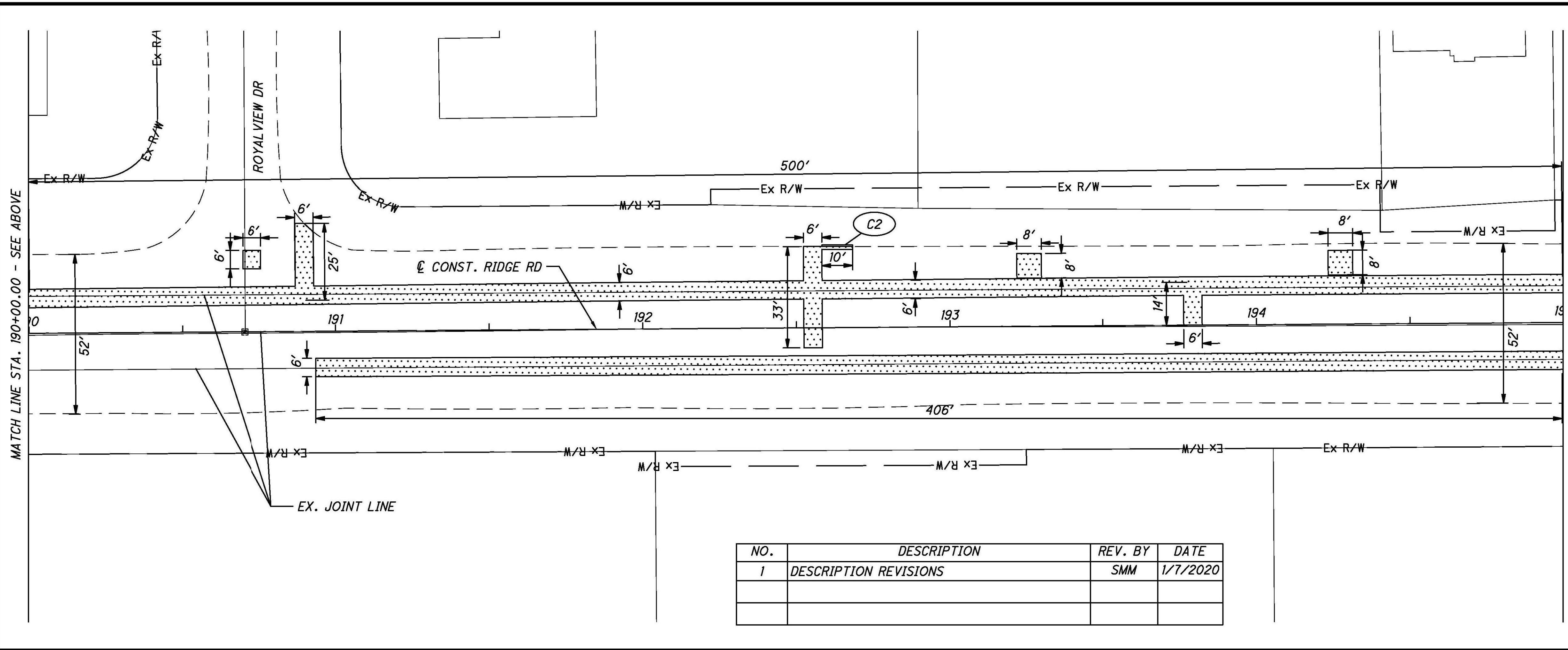
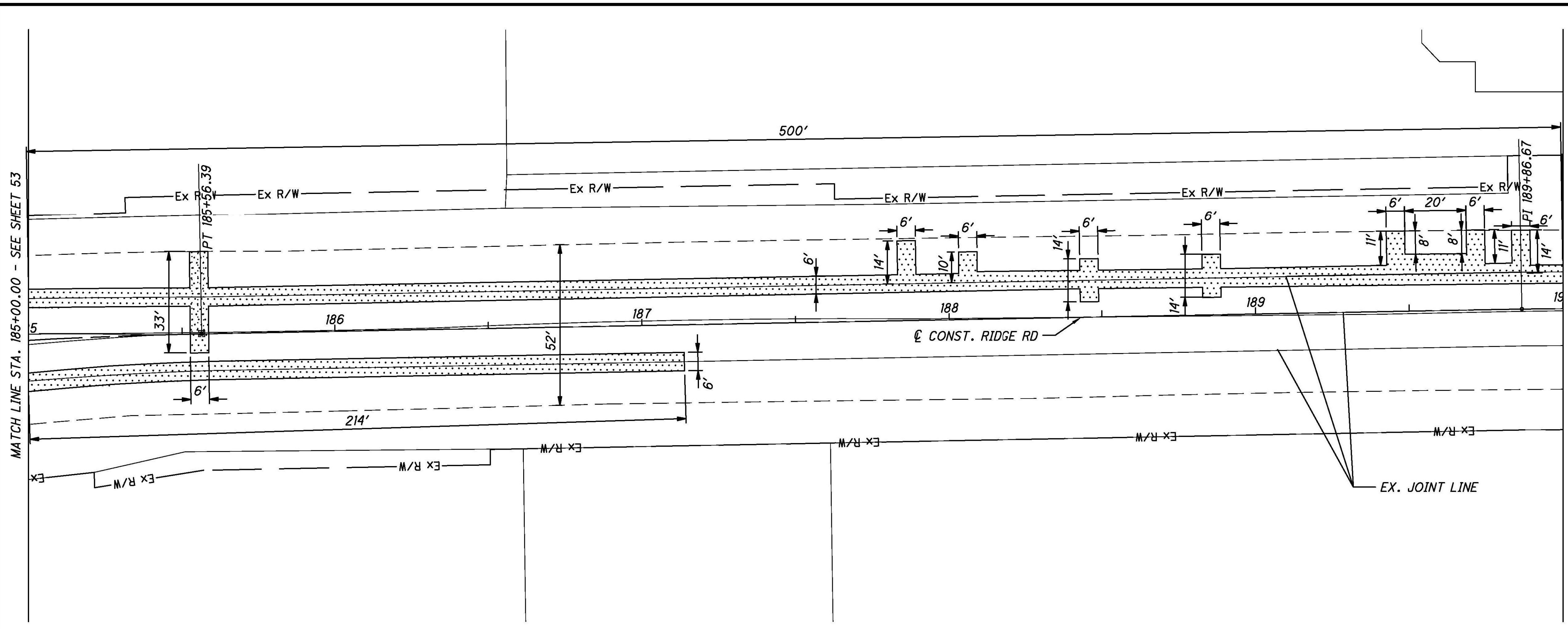


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RIDGE RD PLAN/PLAN  
STA. 185+00 TO STA. 195+00

CUY-SR 003 06.22

54  
93



- LEGEND**
- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
  - MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
  - (CR-#) RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
  - (C) PROPOSED CURB, TYPE 6
  - (C1) PROPOSED CURB, TYPE 6, AS PER PLAN
  - (C2) PROPOSED CURB, TYPE 2-A, AS PER PLAN
  - (M) EXISTING MONUMENT TO BE ADJUSTED TO GRADE
  - (TD) PROPOSED TRUNCATED DOMES
  - (V) EXISTING VALVE TO BE ADJUSTED TO GRADE
  - [Hatched Box] ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)
  - [Dotted Box] ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)
  - [Diagonal Lines Box] ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR
  - [Stippled Box] 4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020

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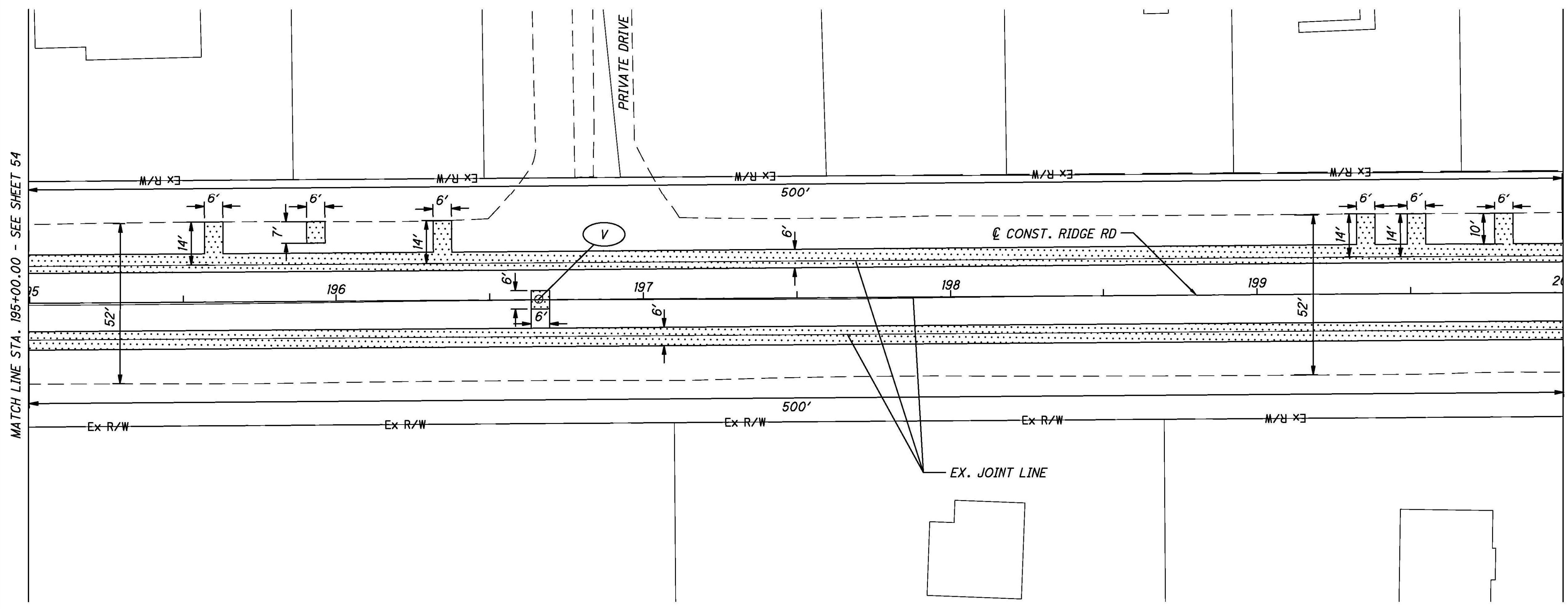
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RIDGE RD PLAN/PLAN  
STA. 195+00 TO STA. 205+00

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55  
93

MATCH LINE STA. 195+00.00 - SEE SHEET 54

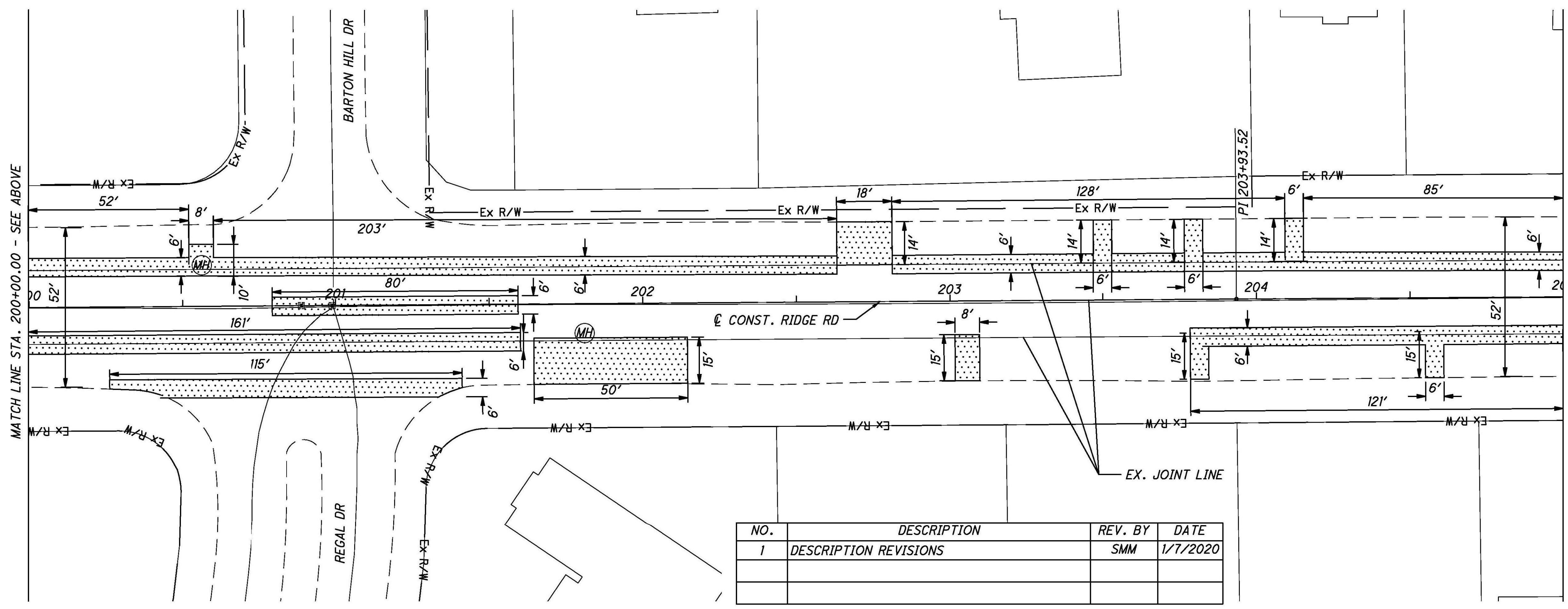
MATCH LINE STA. 200+00.00 - SEE BELOW



STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
			FT	EACH	EACH
196+66	CL				1
200+56	LT	13		1	
201+81	RT	10		1	
<b>SHEET TOTALS</b>			0	2	0

MATCH LINE STA. 200+00.00 - SEE ABOVE

MATCH LINE STA. 205+00.00 - SEE SHEET 56

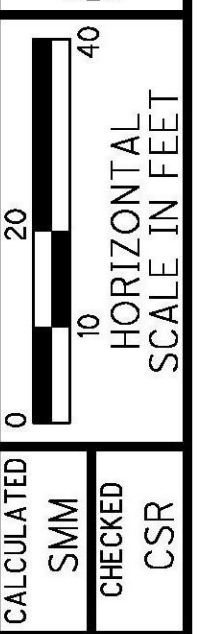


**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE

- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)
- ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR
- 4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020

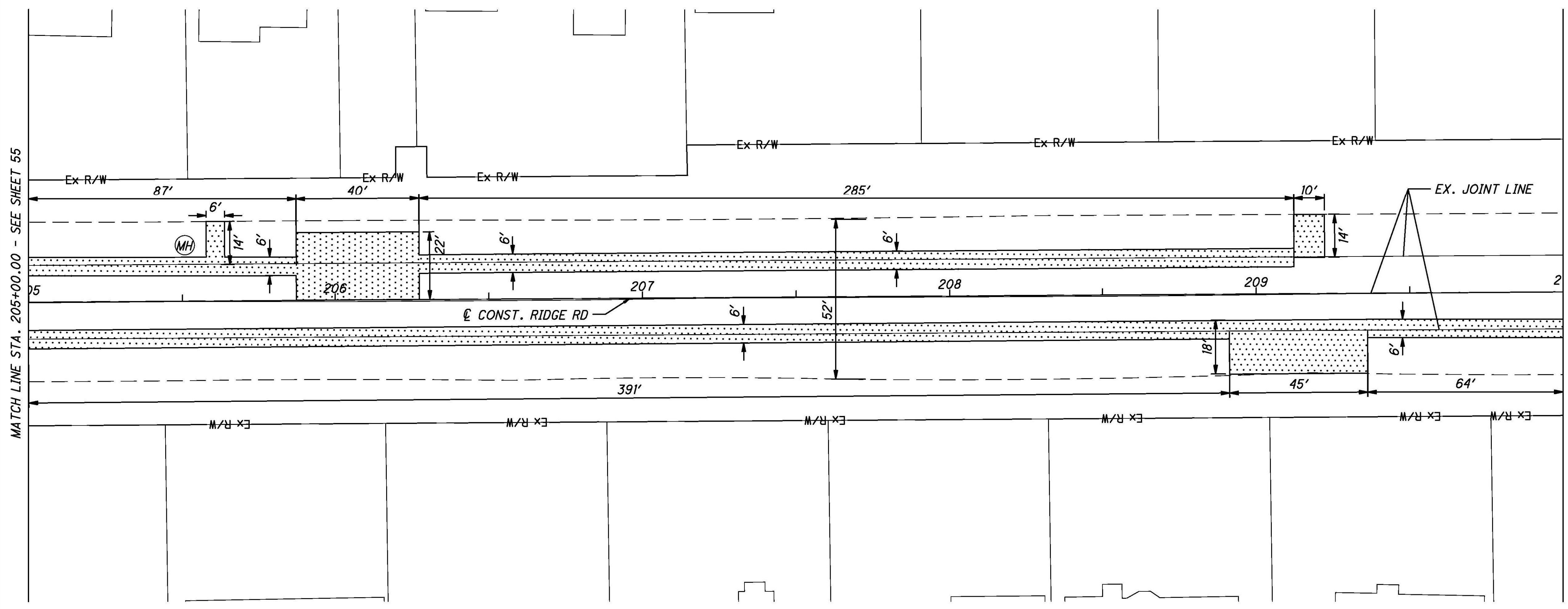


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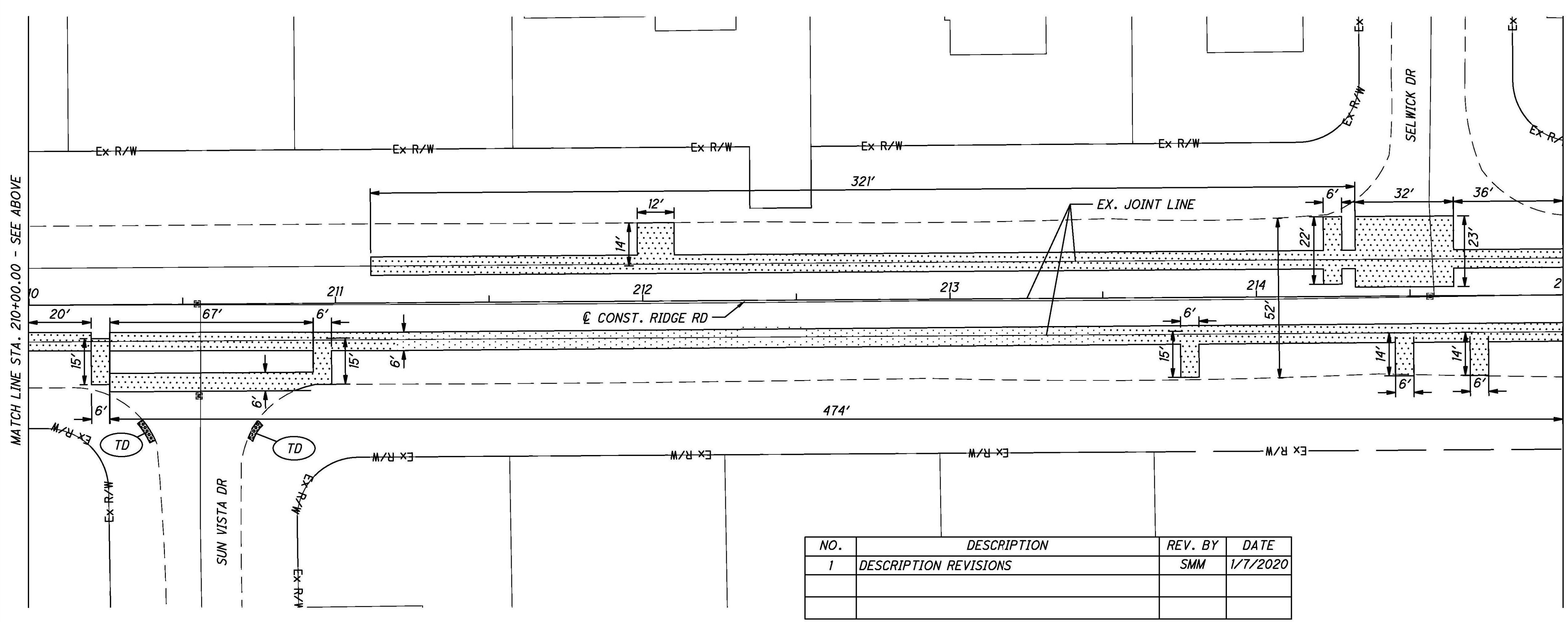
RIDGE RD PLAN/PLAN  
STA. 205+00 TO STA. 215+00

CUY-SR 003 06.22

56  
93



STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
			FT	EACH	EACH
205+51	LT	18		1	
<b>SHEET TOTALS</b>			0	1	0



**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- (CR-#) RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- (C) PROPOSED CURB, TYPE 6
- (C1) PROPOSED CURB, TYPE 6, AS PER PLAN
- (C2) PROPOSED CURB, TYPE 2-A, AS PER PLAN
- (M) EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- (TD) PROPOSED TRUNCATED DOMES
- (V) EXISTING VALVE TO BE ADJUSTED TO GRADE

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020

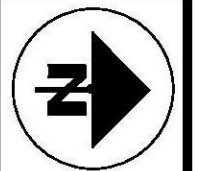
MATCH LINE STA. 205+00.00 - SEE SHEET 55

MATCH LINE STA. 210+00.00 - SEE BELOW

MATCH LINE STA. 210+00.00 - SEE ABOVE

MATCH LINE STA. 215+00.00 - SEE SHEET 57

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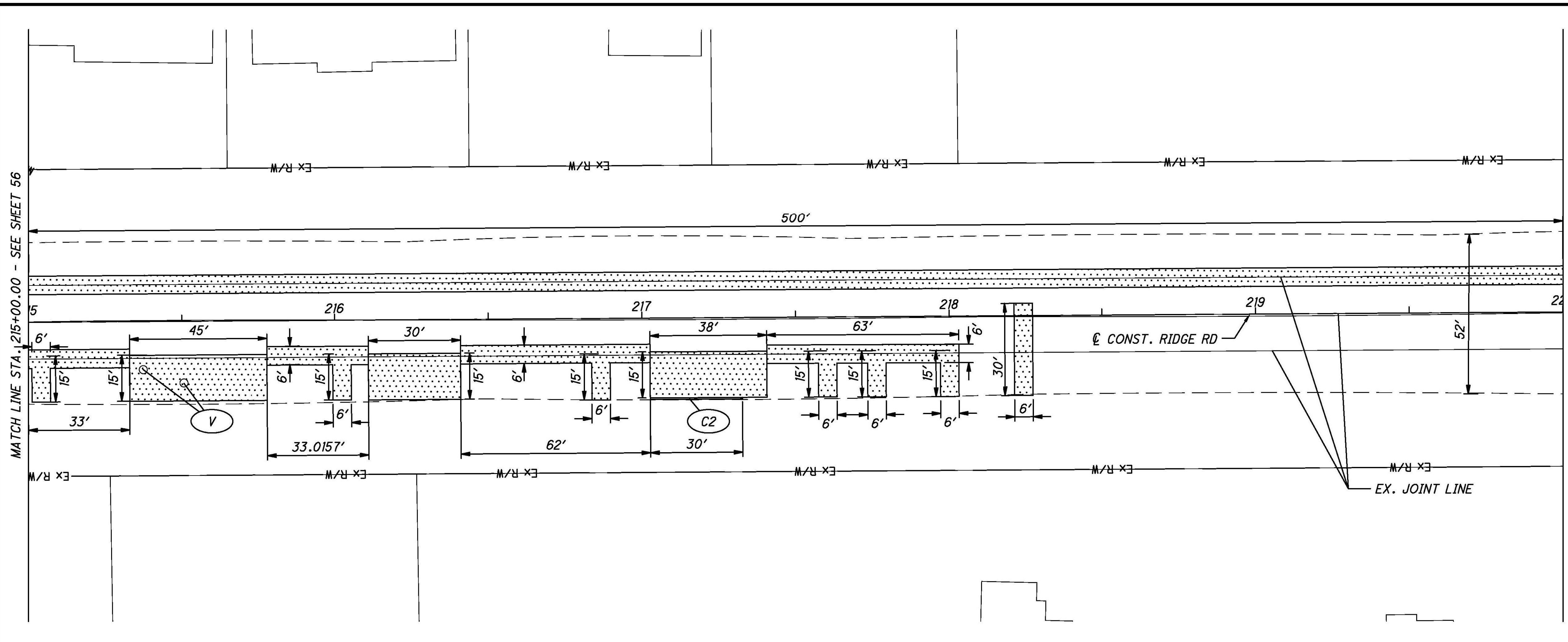
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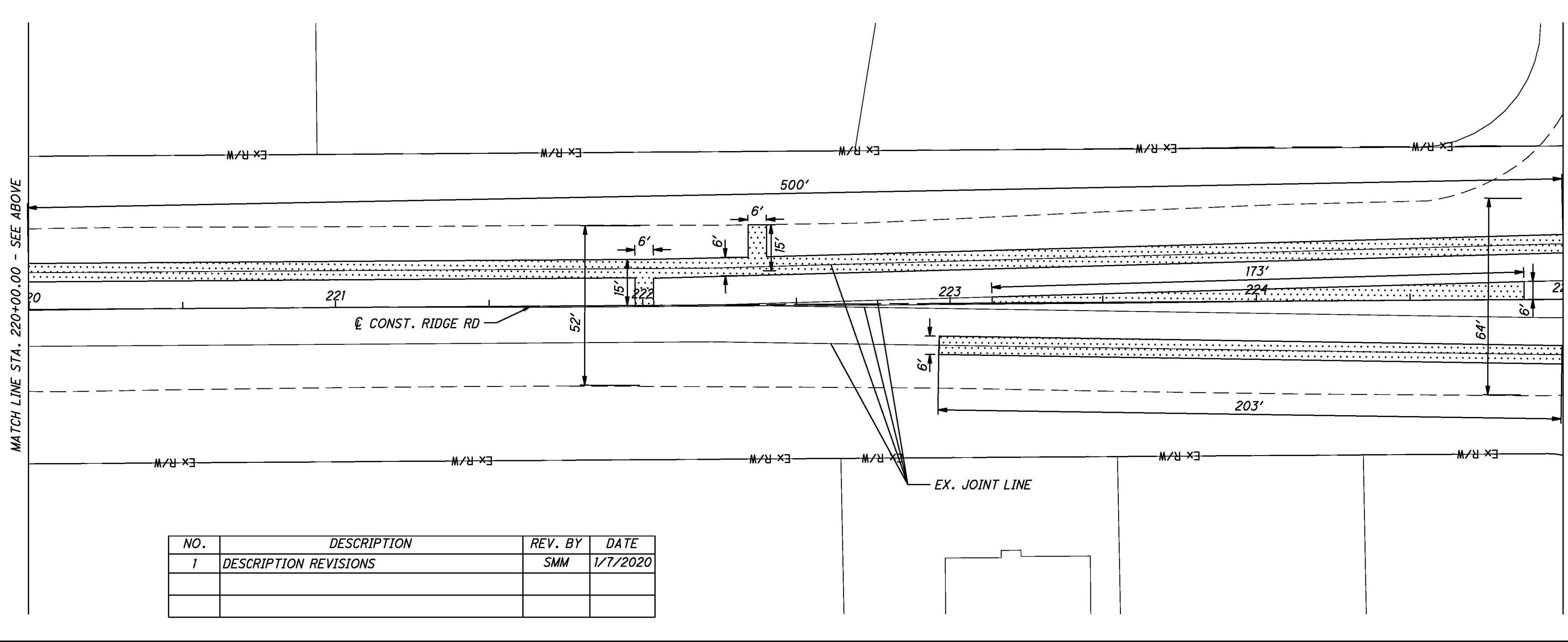
RIDGE RD PLAN/PLAN  
STA. 215+00 TO STA. 225+00

CUY-SR 003 06.22

57  
93



STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
			FT	EACH	EACH
215+37	RT	16			1
215+51	RT	20			1
<b>SHEET TOTALS</b>			0	0	2



**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- (CR-#) RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- (C) PROPOSED CURB, TYPE 6
- (C1) PROPOSED CURB, TYPE 6, AS PER PLAN
- (C2) PROPOSED CURB, TYPE 2-A, AS PER PLAN
- (M) EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- (TD) PROPOSED TRUNCATED DOMES
- (V) EXISTING VALVE TO BE ADJUSTED TO GRADE

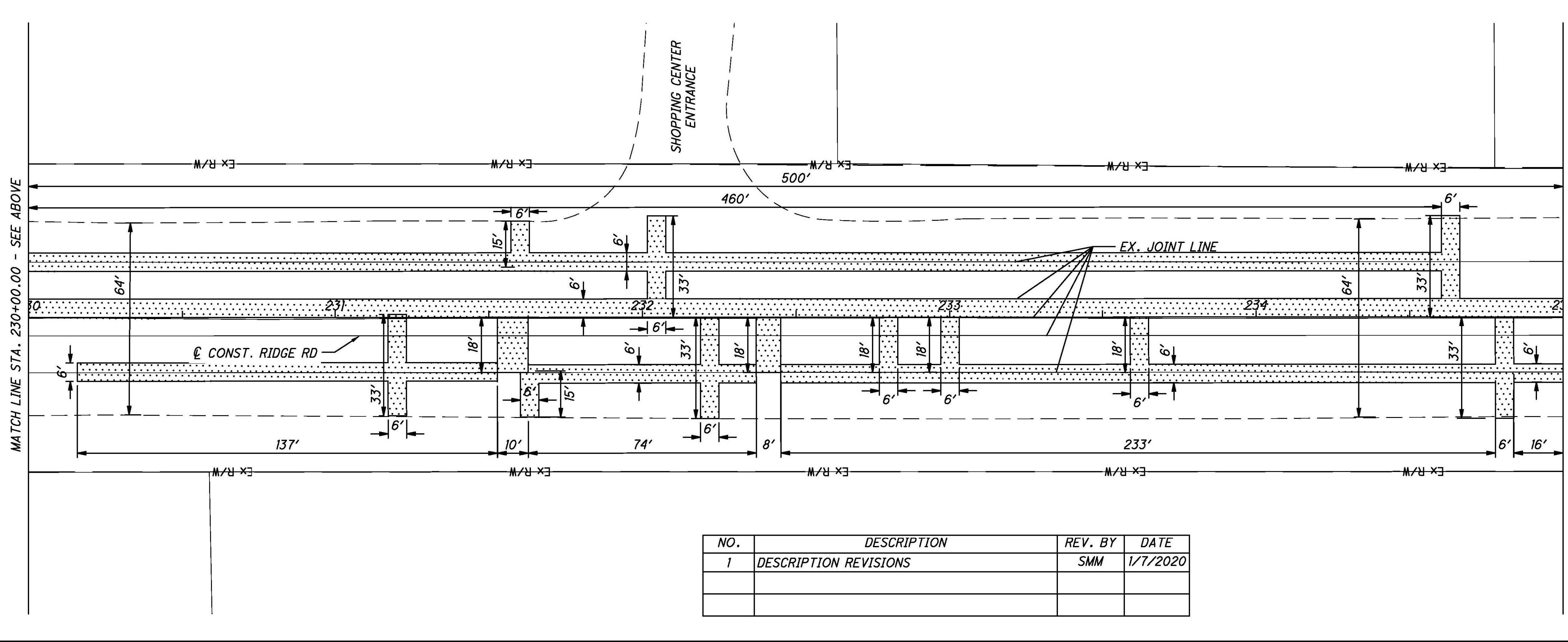
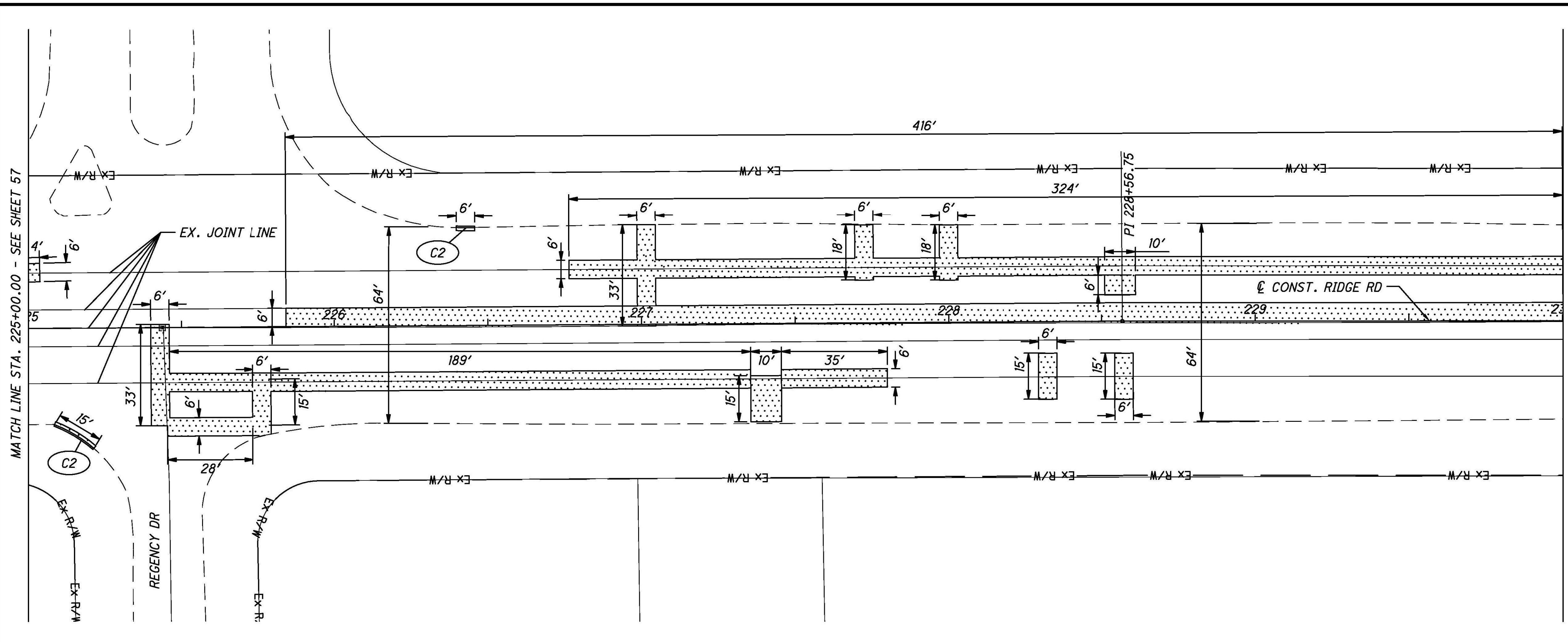
ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020



**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- (CR-#) RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- (C) PROPOSED CURB, TYPE 6
- (C1) PROPOSED CURB, TYPE 6, AS PER PLAN
- (C2) PROPOSED CURB, TYPE 2-A, AS PER PLAN
- (M) EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- (TD) PROPOSED TRUNCATED DOMES
- (V) EXISTING VALVE TO BE ADJUSTED TO GRADE

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020

RIDGE RD PLAN/PLAN

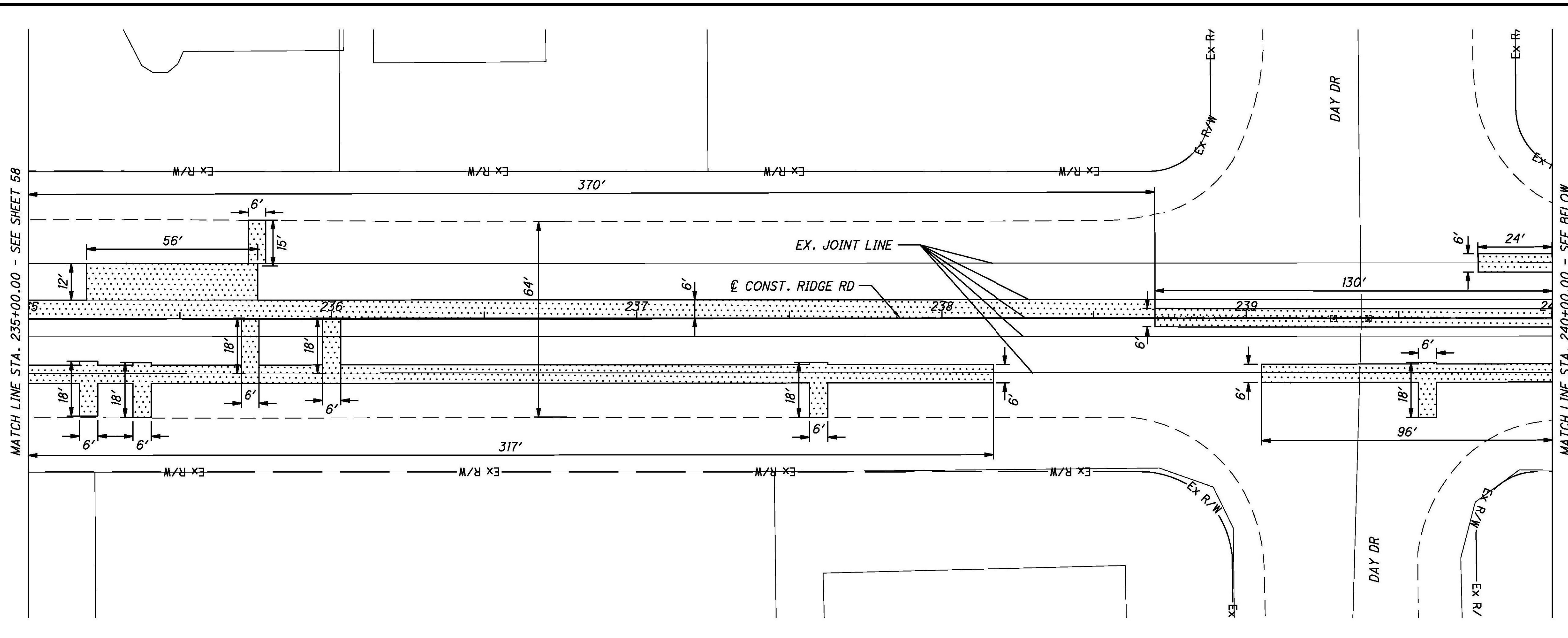
STA. 225+00 TO STA. 235+00

CUY-SR 003 06.22

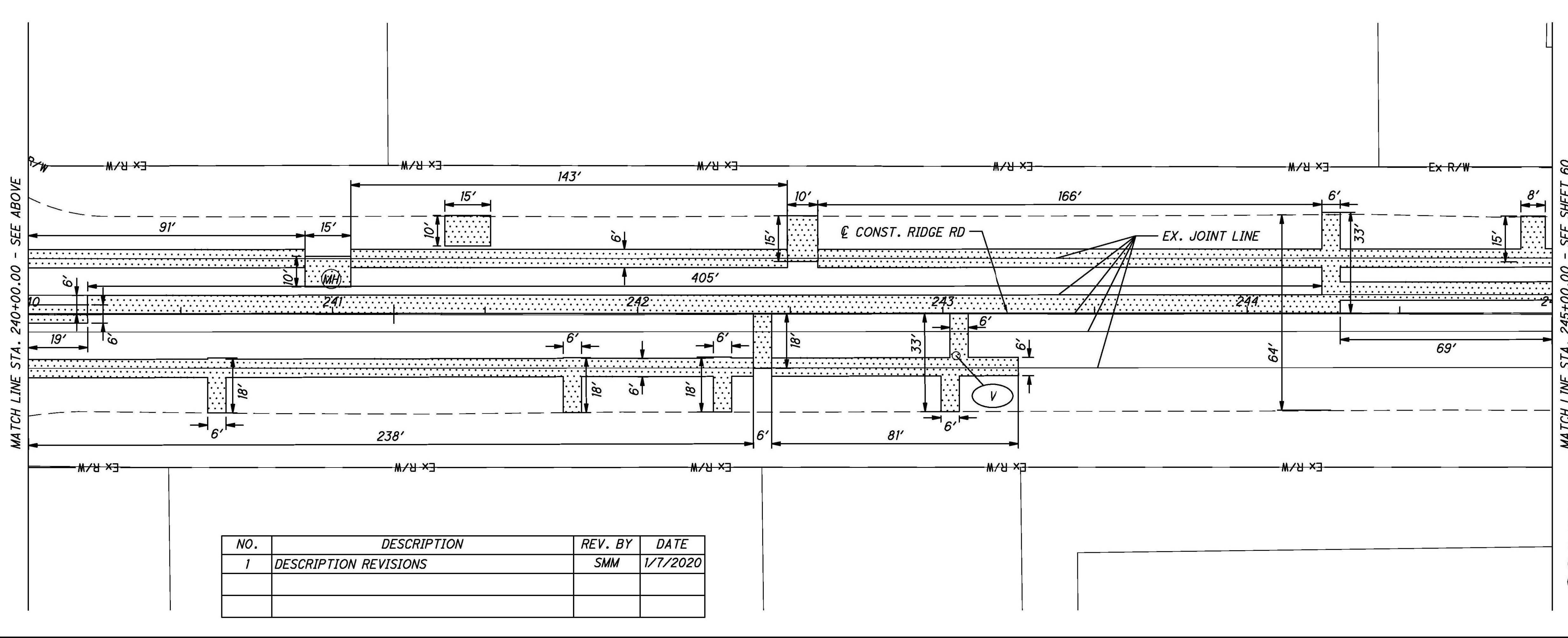
58  
93

0 10 20  
HORIZONTAL SCALE IN FEET

CALCULATED  
SMM  
CHECKED  
CSR



STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
			FT EACH	EACH	EACH
241+00	LT	11		1	
243+04	RT	14			1
<b>SHEET TOTALS</b>			0	1	1



**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- (CR-#) RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- (C) PROPOSED CURB, TYPE 6
- (C1) PROPOSED CURB, TYPE 6, AS PER PLAN
- (C2) PROPOSED CURB, TYPE 2-A, AS PER PLAN
- (M) EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- (TD) PROPOSED TRUNCATED DOMES
- (V) EXISTING VALVE TO BE ADJUSTED TO GRADE

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)

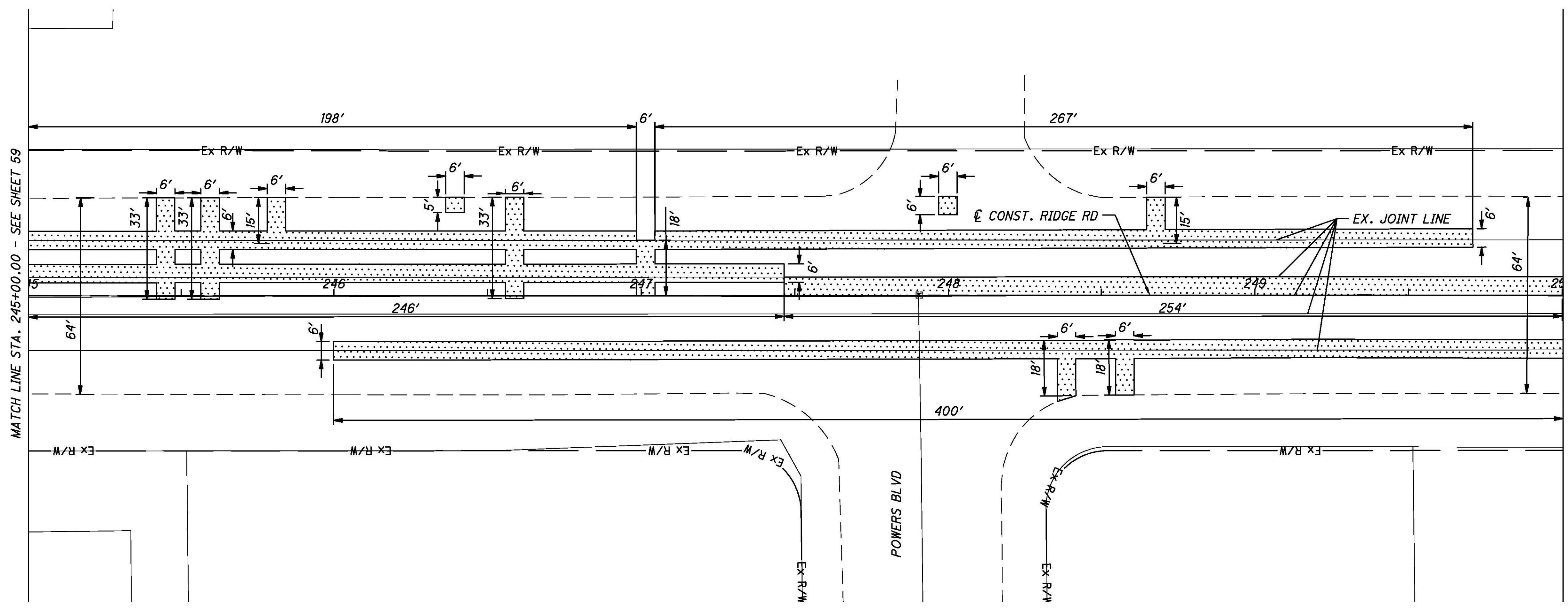
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020

MATCH LINE STA. 245+00.00 - SEE SHEET 59

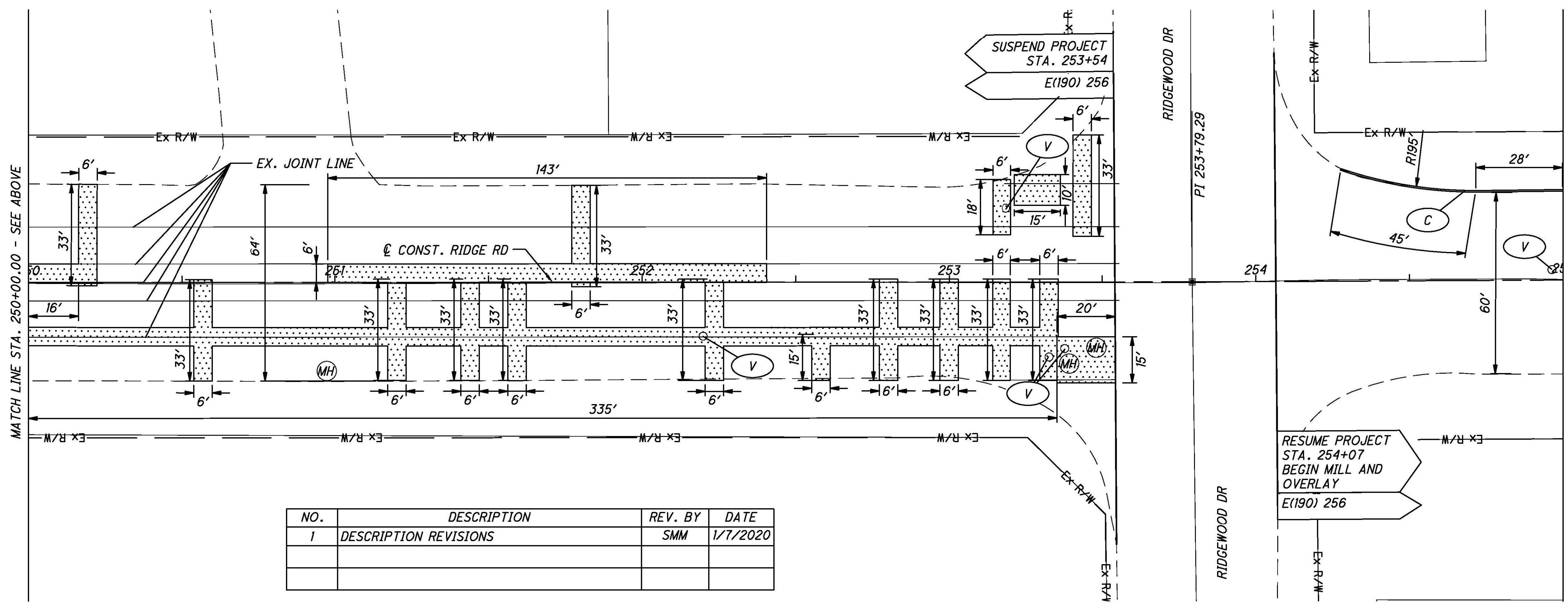
MATCH LINE STA. 250+00.00 - SEE BELOW



STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
			FT	EACH	EACH
250+97	RT	29		1	
252+20	RT	18			1
253+33	RT	24			1
253+38	RT	21			1
253+39	RT	27		1	
253+48	RT	21		1	
253+19	LT	24			1
254+96	LT	4			1
<b>SHEET TOTALS</b>			0	3	5

MATCH LINE STA. 250+00.00 - SEE ABOVE

MATCH LINE STA. 255+00.00 - SEE SHEET 61



NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020

**LEGEND**

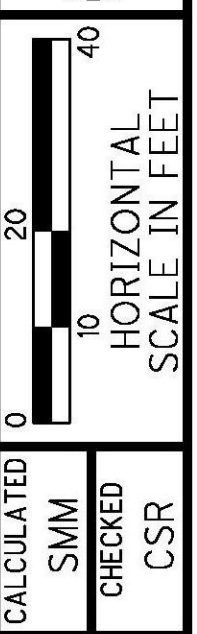
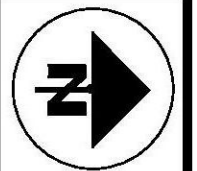
- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

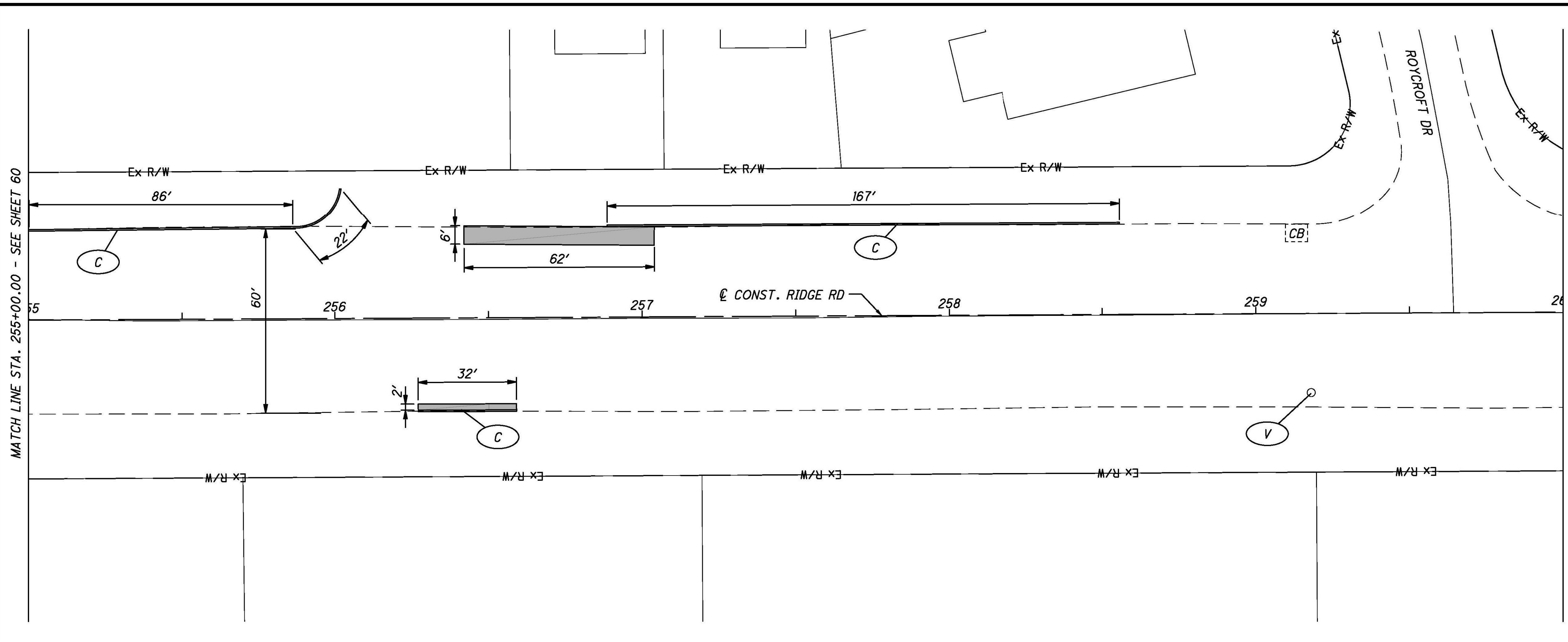
4" CONCRETE WALK



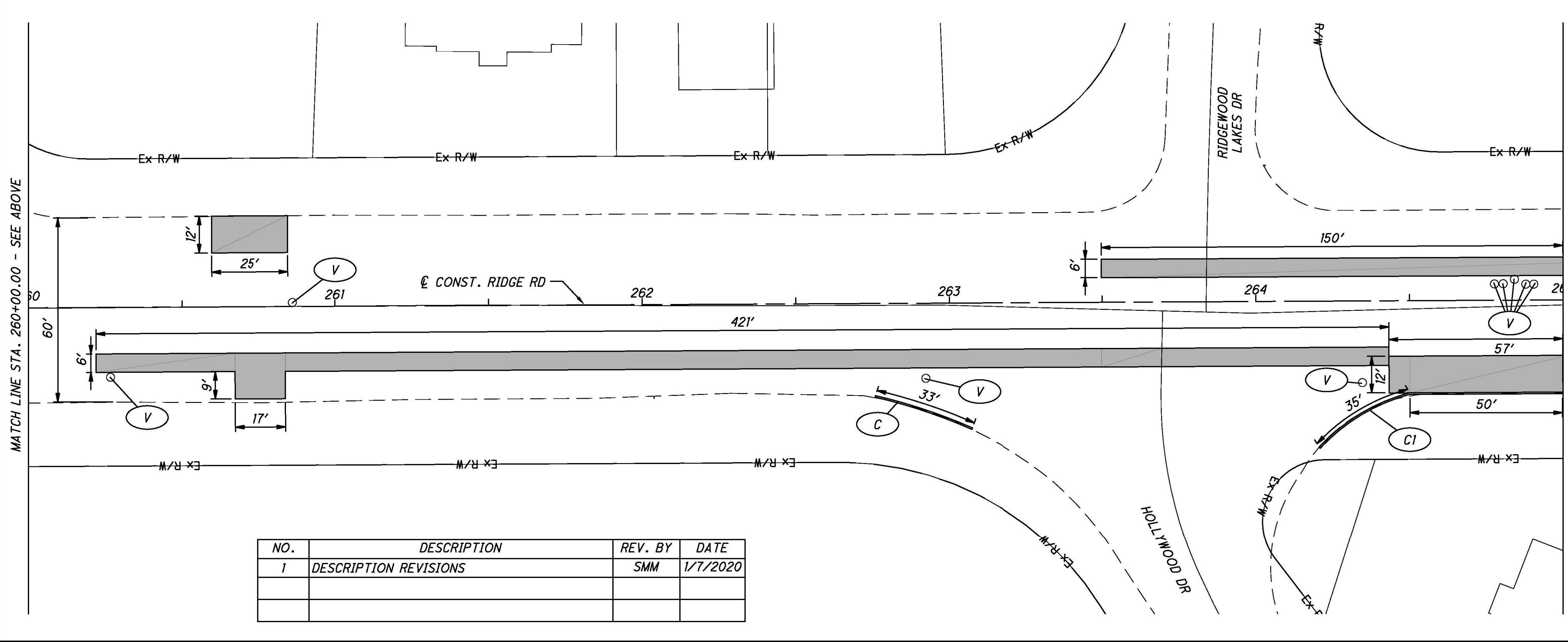
CALCULATED SMM CHECKED CSR

RIDGE RD PLAN/PLAN STA. 255+00 TO STA. 265+00

CUY-SR 003 06.22



STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
		FT	EACH	EACH	EACH
259+13	LT	26	1		
259+18	RT	26			1
260+27	RT	23			1
260+86	LT	1			1
262+92	RT	24			1
264+35	RT	27			1
264+78	LT	5			1
264+80	LT	5			1
264+84	LT	7			1
264+88	LT	5			1
264+90	LT	5			1
<b>SHEET TOTALS</b>			<b>1</b>	<b>0</b>	<b>10</b>



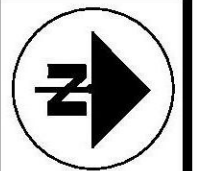
**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)
- ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR
- 4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020



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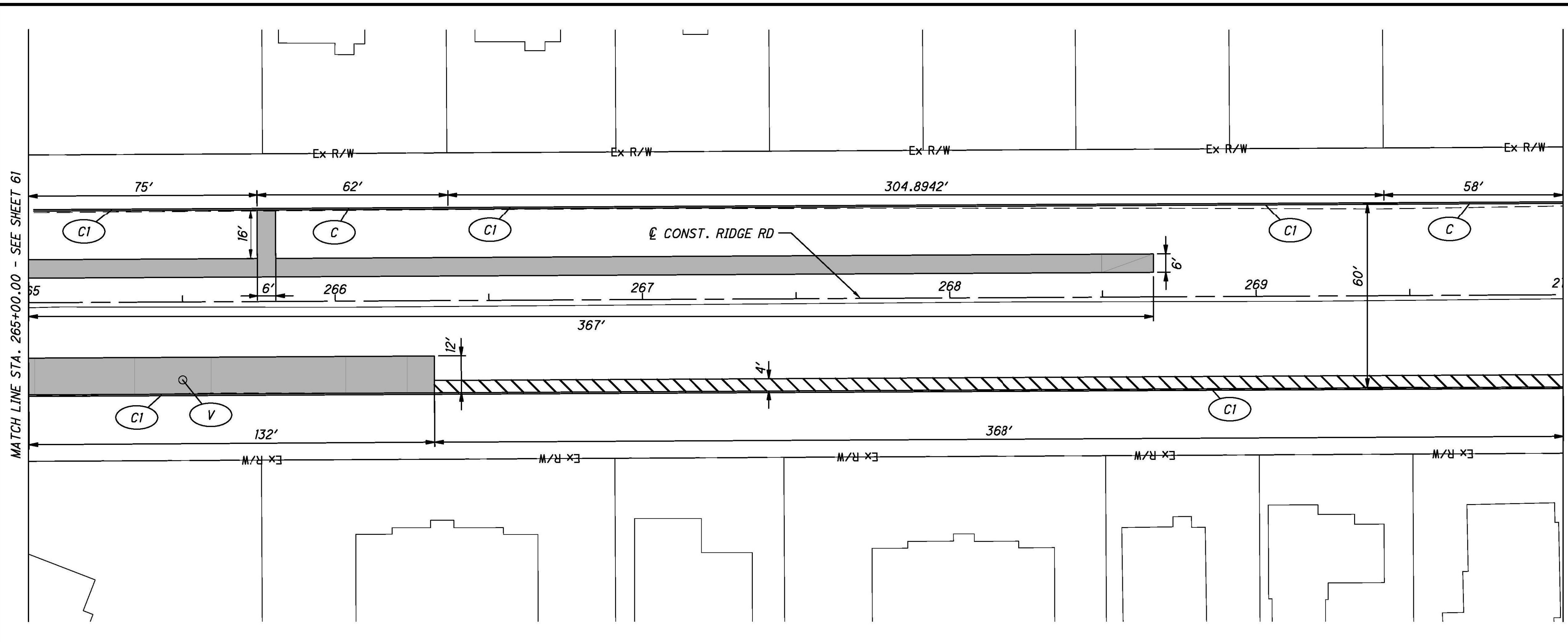


0 20  
10  
HORIZONTAL  
SCALE IN FEET

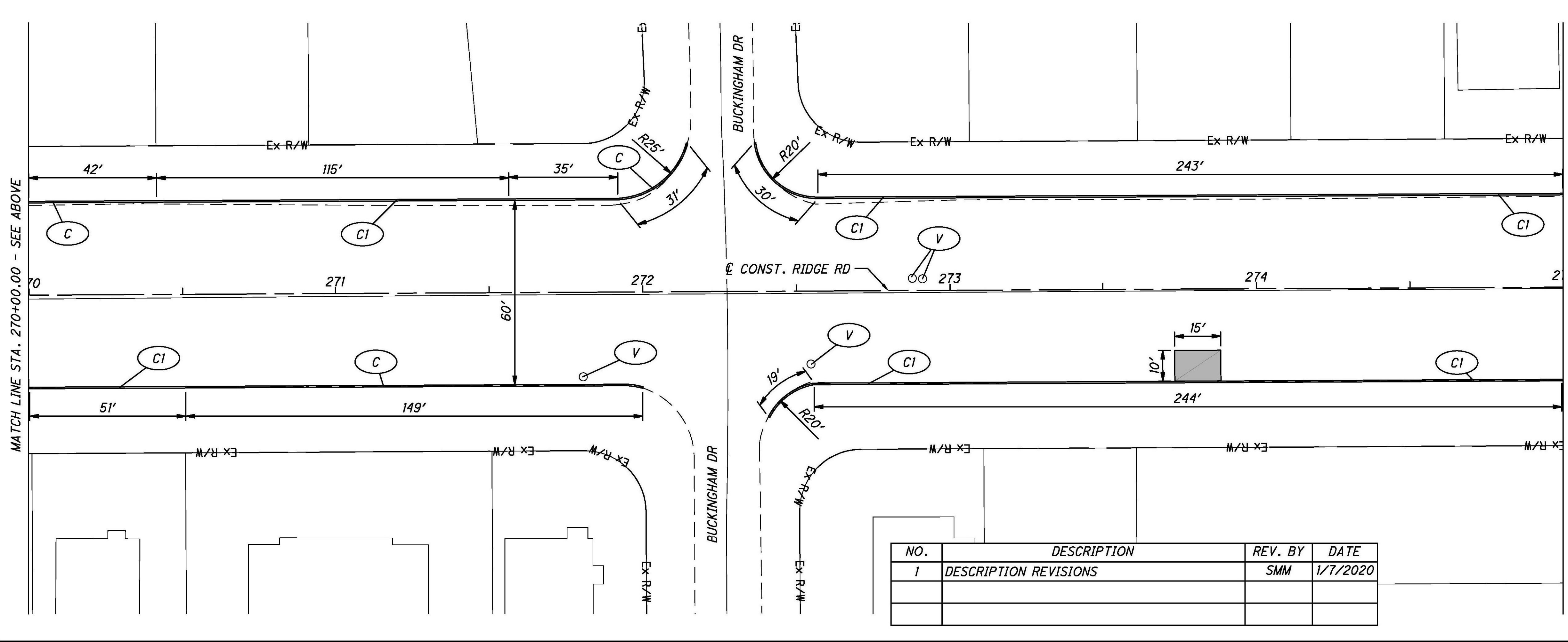
CALCULATED  
SMM  
CHECKED  
CSR

RIDGE RD PLAN/PLAN  
STA. 265+00 TO STA. 275+00

CUY-SR 003 06.22  
62  
93



STATION	SIDE	OFFSET FT	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
SHEET TOTALS			0	0	5
265+50	RT	25			1
271+80	RT	28			1
272+55	RT	24			1
272+88	LT	4			1
272+91	LT	4			1



**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE

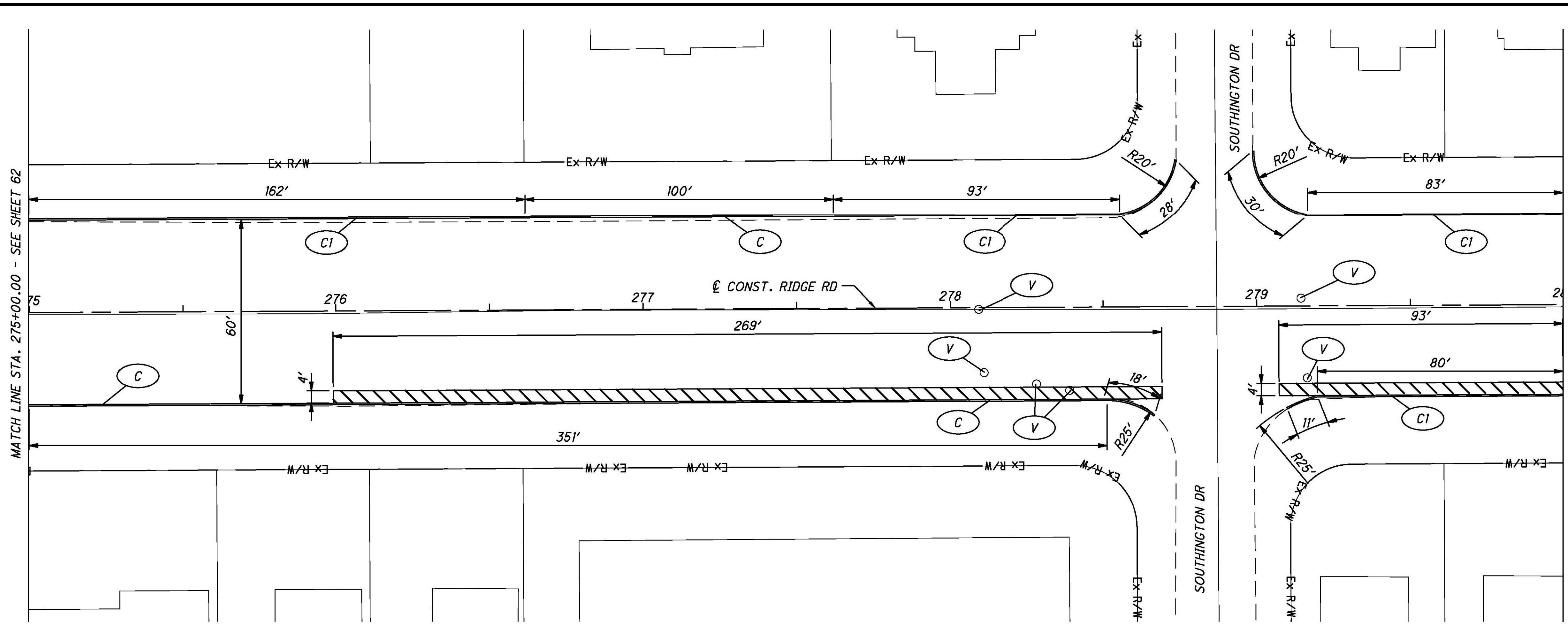
ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)

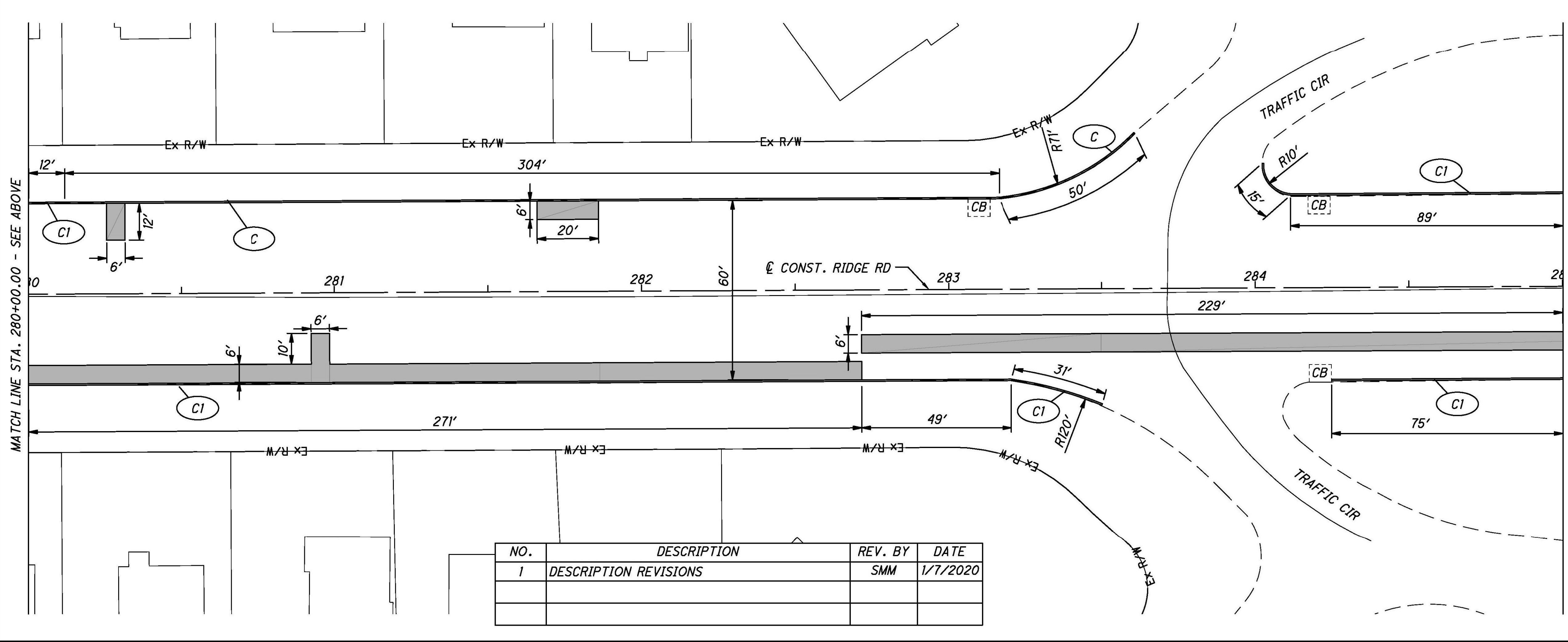
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020



STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
			FT	EACH	EACH
278+09		0			1
278+11	RT	21			1
278+28	RT	25			1
278+39	RT	27			1
279+14	LT	3			1
279+16	RT	23			1
283+10	LT	27	1		
284+21	LT	27	1		
284+21	RT	28	1		
<b>SHEET TOTALS</b>			<b>3</b>	<b>0</b>	<b>6</b>



**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE

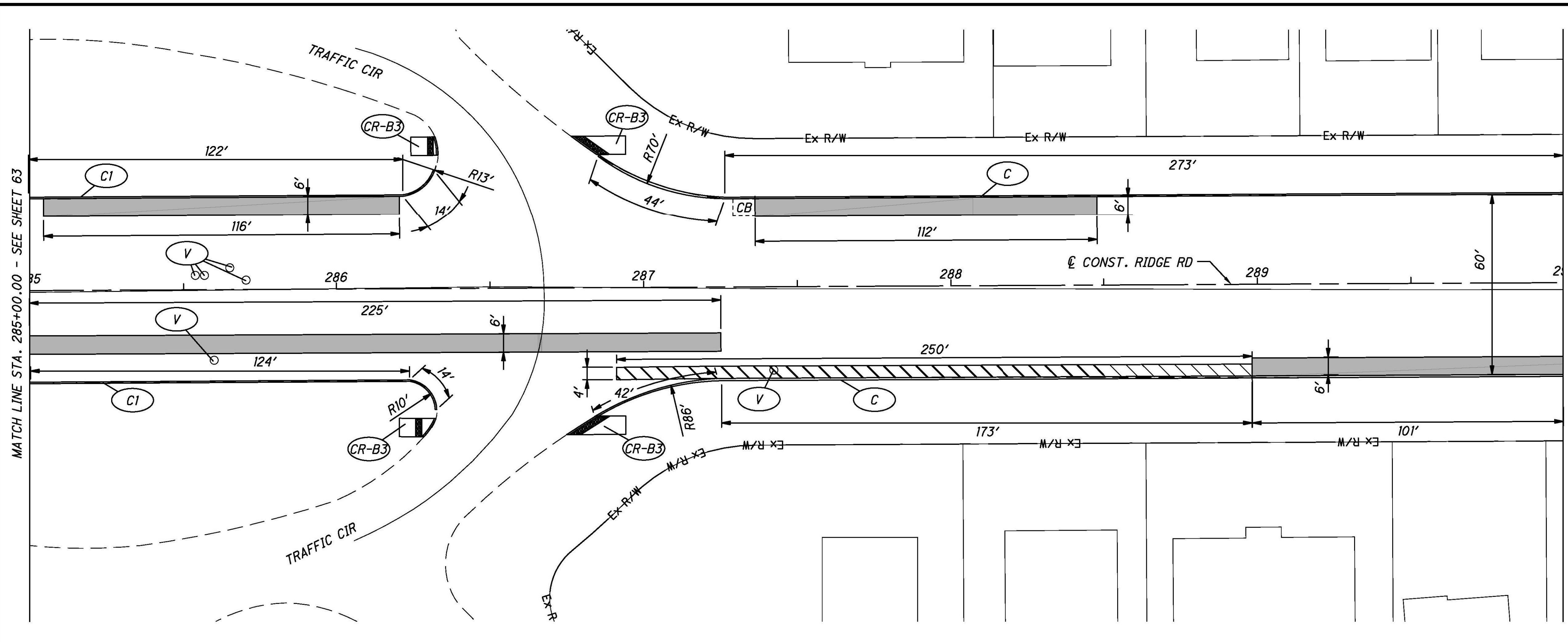
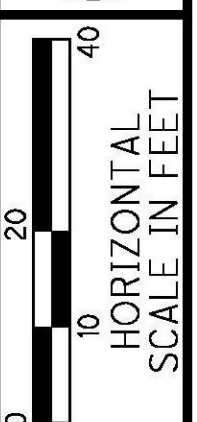
ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)

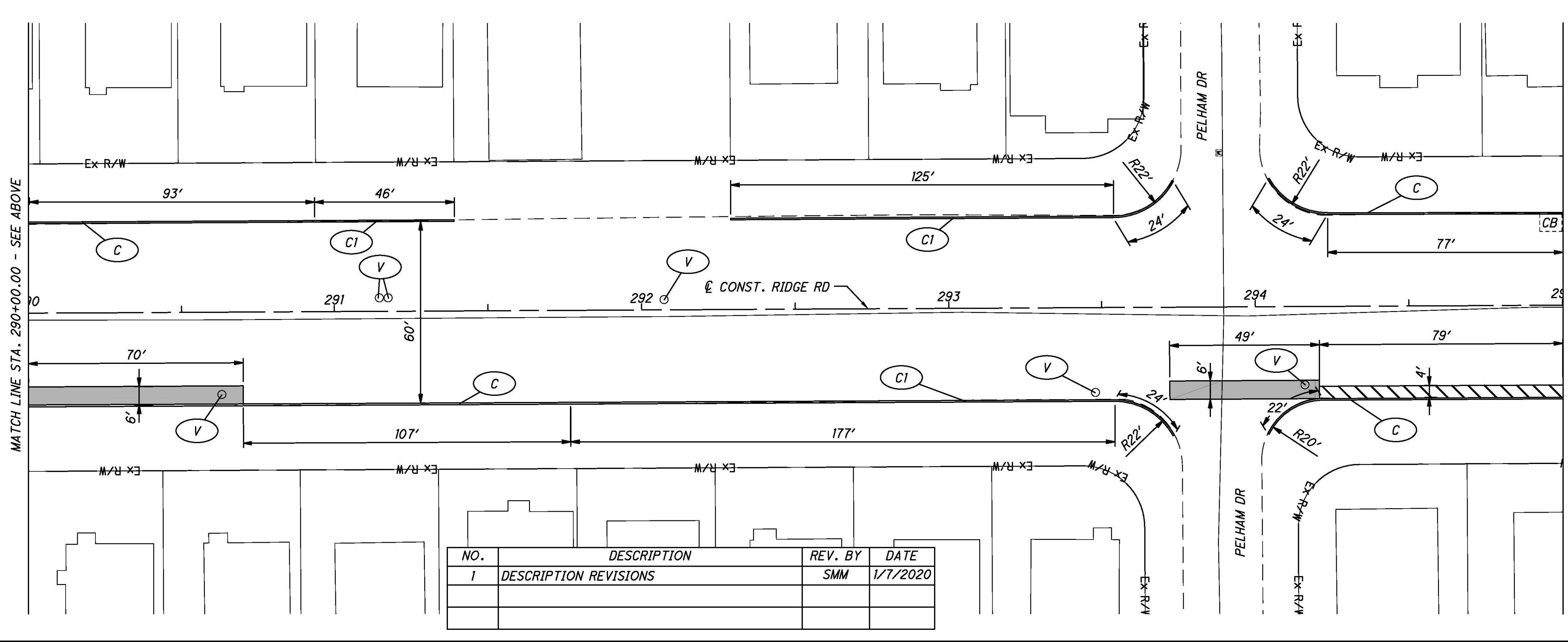
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020



STATION	SIDE	OFFSET	611		
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
		FT	EACH	EACH	EACH
285+54	LT	5			1
285+57	LT	5			1
285+60	RT	23			1
285+65	LT	7			1
285+70	LT	3			1
287+33	LT	26	1		
287+42	RT	27			1
290+63	RT	27			1
291+15	LT	4			1
291+17	LT	4			1
292+07	LT	3			1
293+48	RT	28			1
294+16	RT	26			1
294+97	LT	27	1		
<b>SHEET TOTALS</b>			<b>2</b>	<b>0</b>	<b>12</b>



**LEGEND**

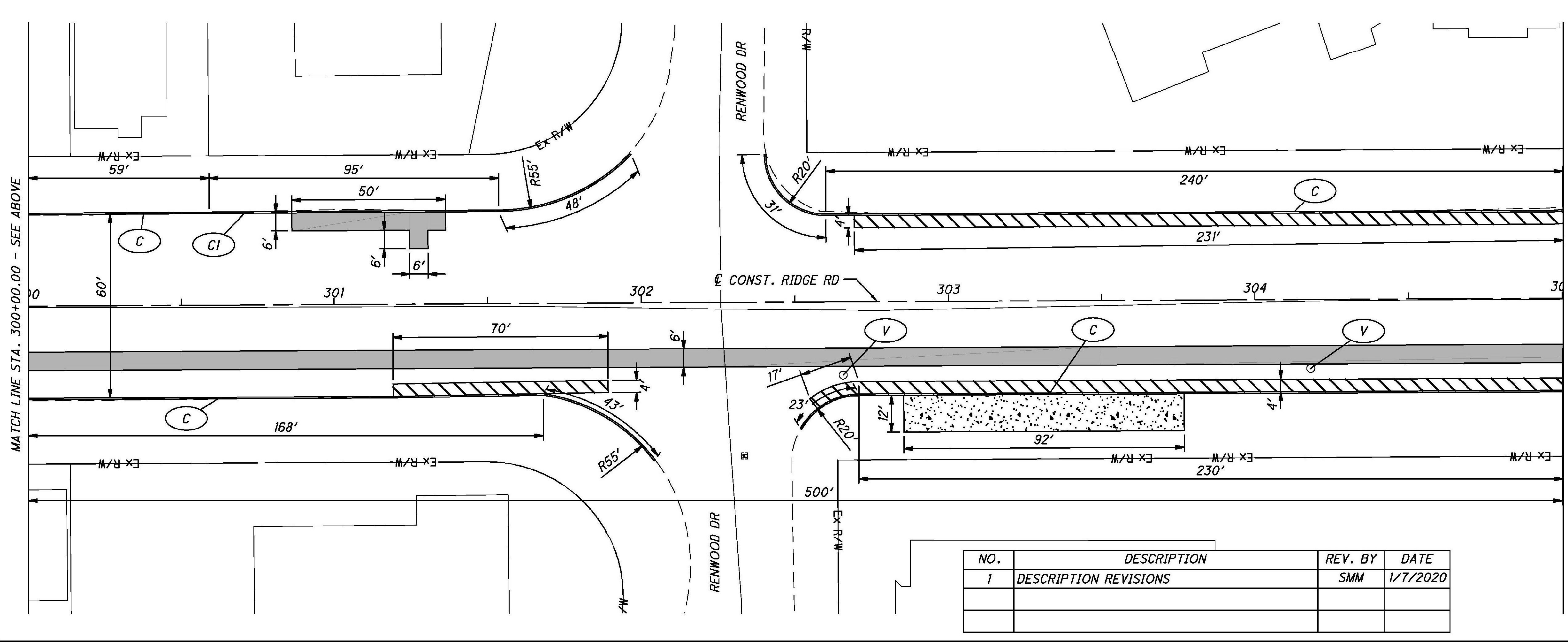
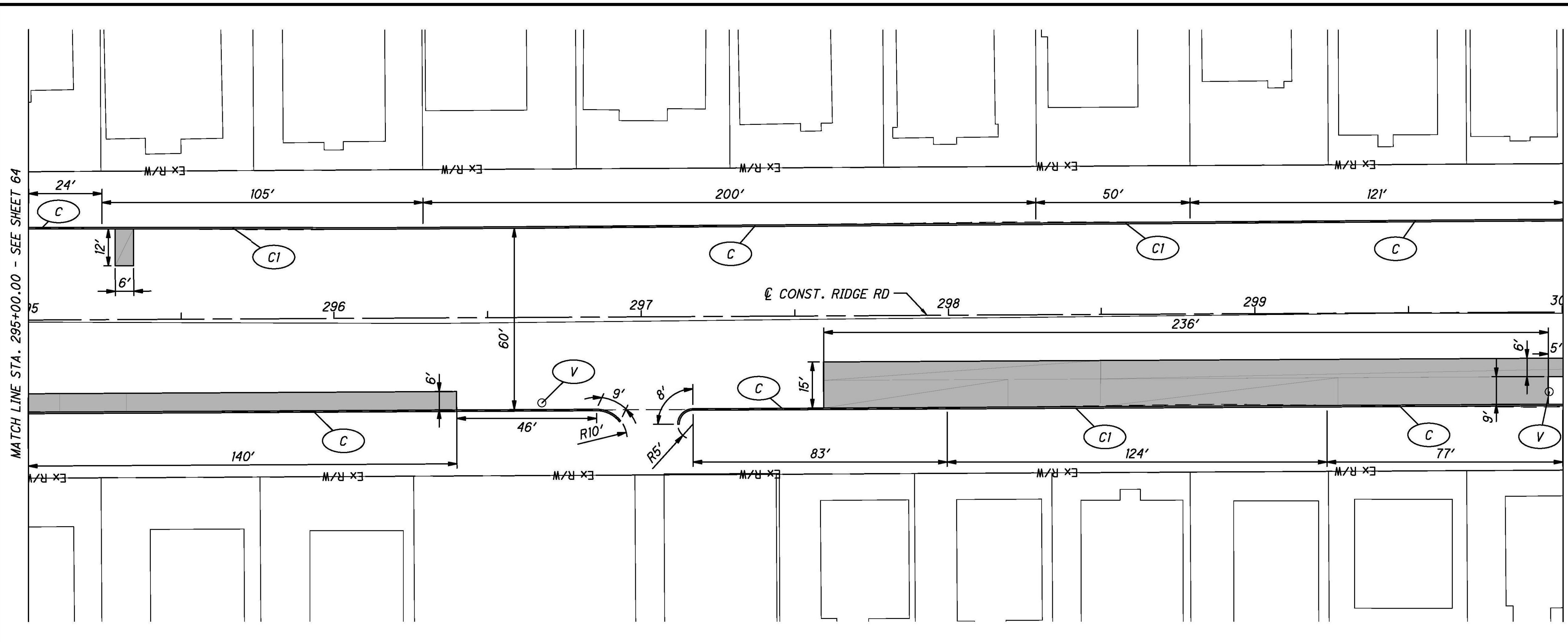
- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)
- ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR
- 4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020

RIDGE RD PLAN/PLAN STA. 285+00 TO STA. 295+00

CUY-SR 003 06.22

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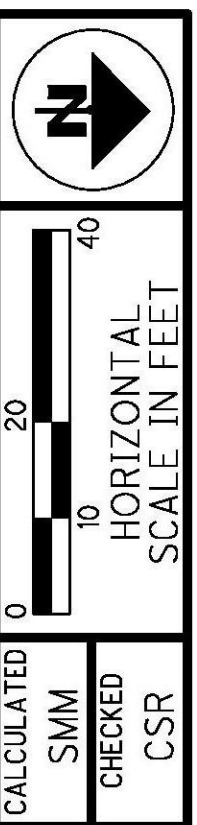


STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
			FT	EACH	EACH
296+68	RT	28			1
299+96	RT	26			1
302+66	RT	24			1
304+18	RT	22			1
<b>SHEET TOTALS</b>			0	0	4

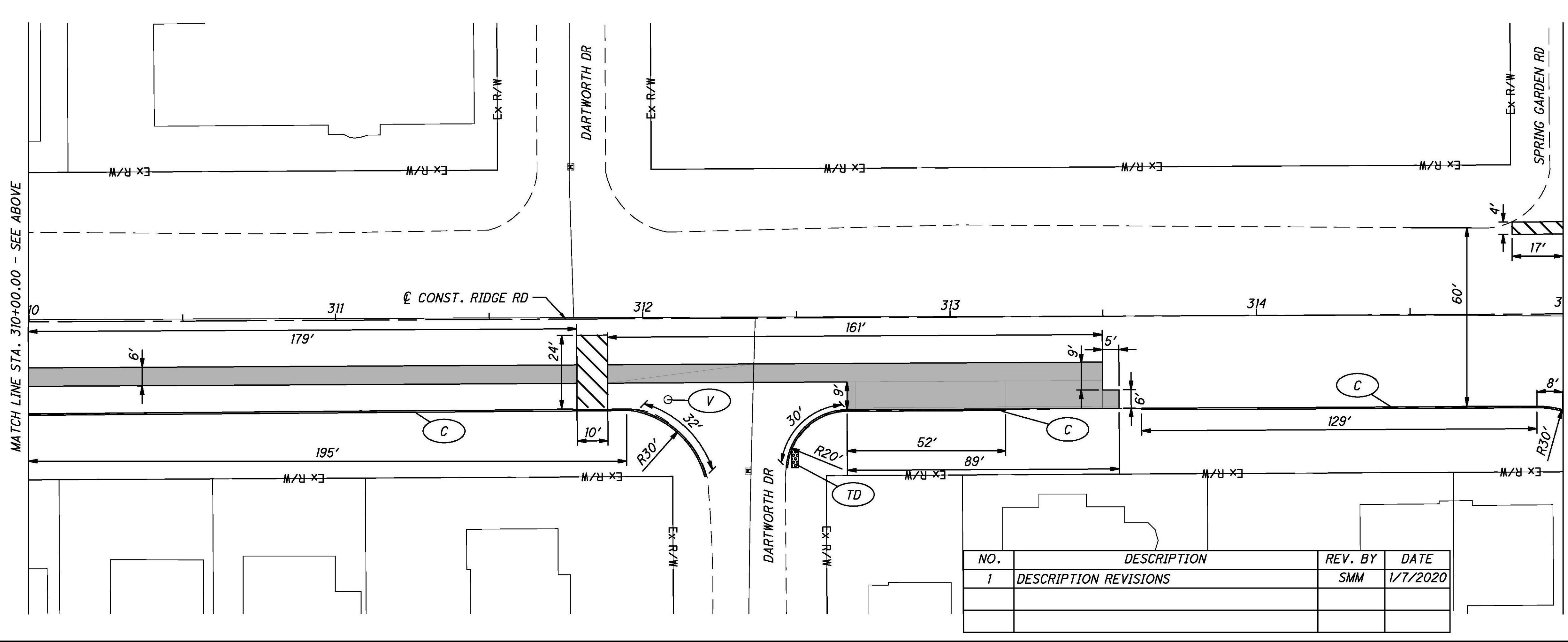
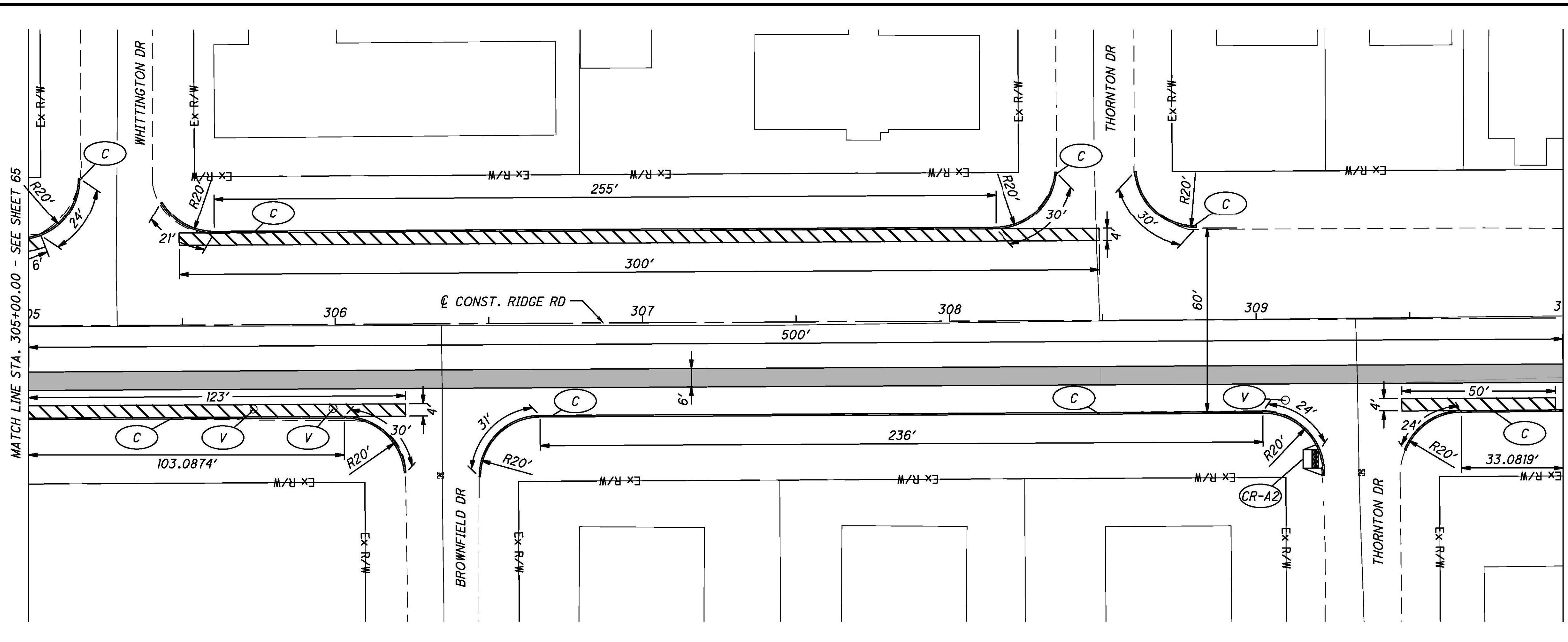
**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)
- ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR
- 4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020



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STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
			FT	EACH	EACH
305+73	RT	28			1
305+99	RT	27			1
309+09	RT	26			1
312+08	RT	26			1
<b>SHEET TOTALS</b>			0	0	4

**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE

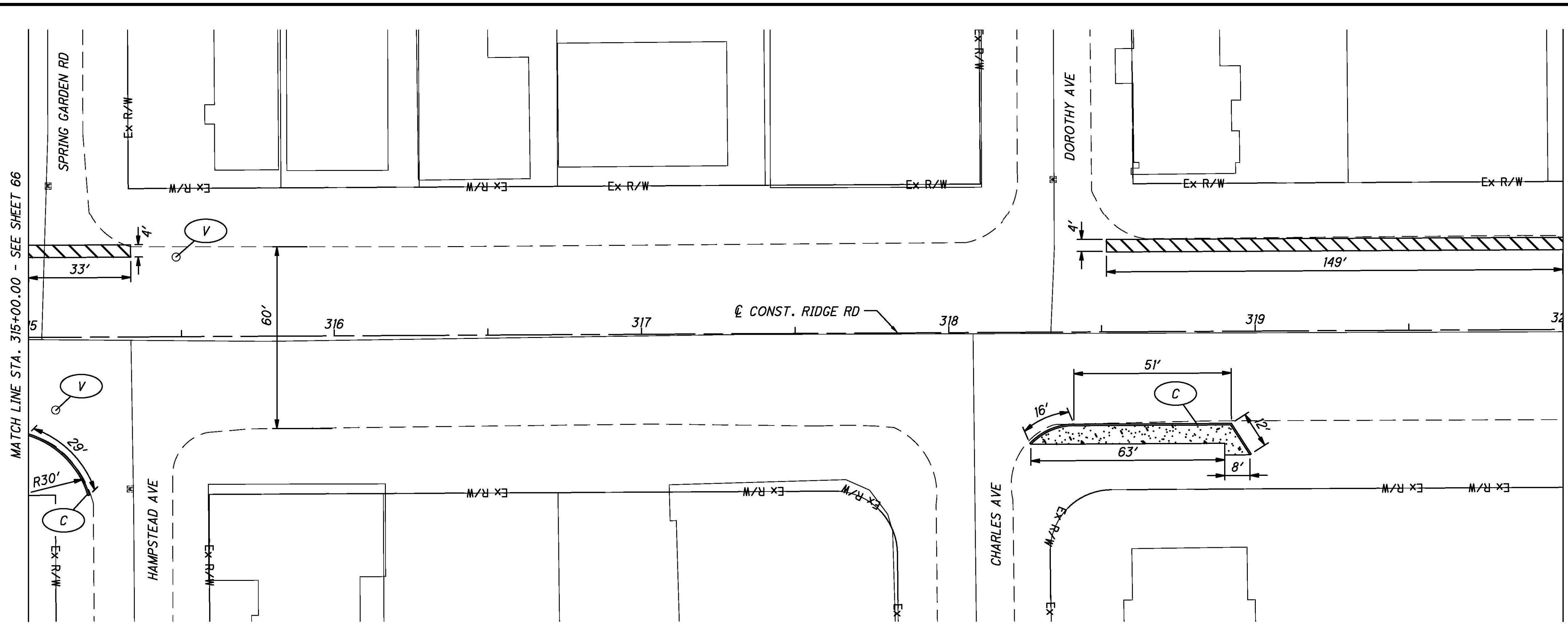
ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)

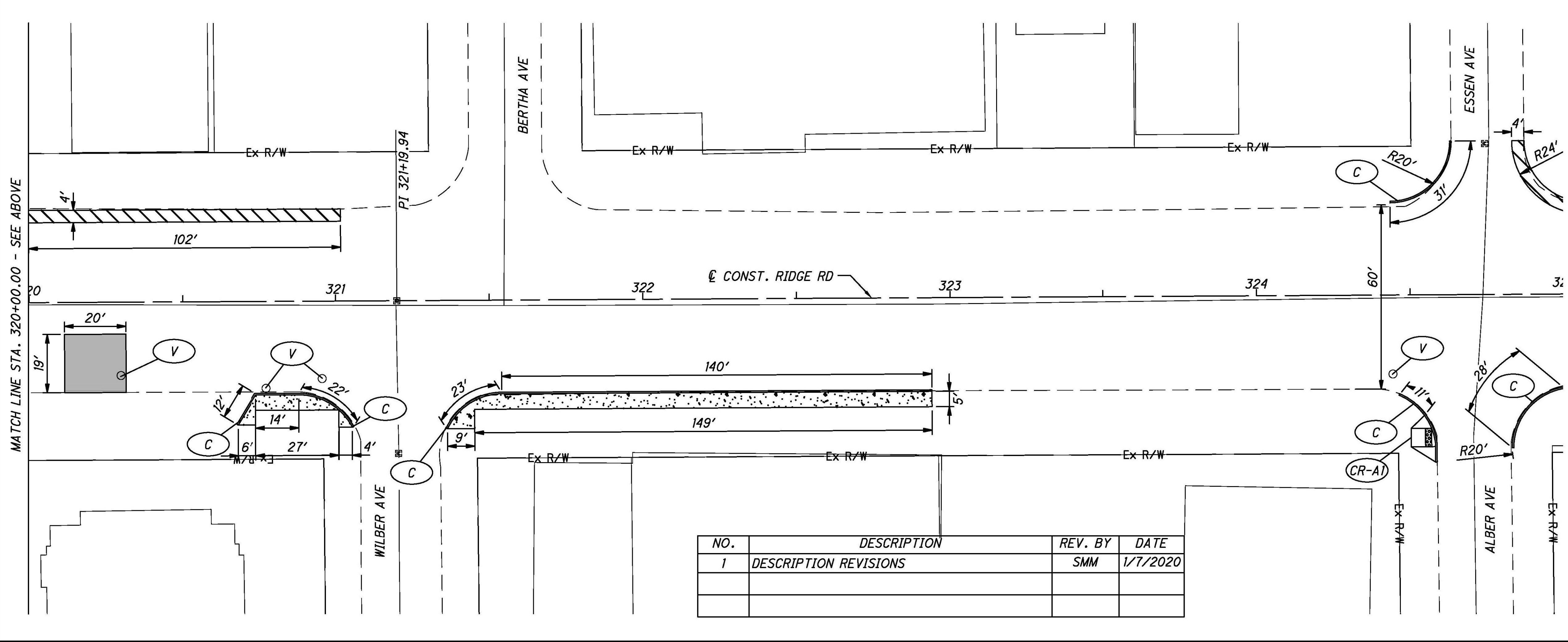
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020



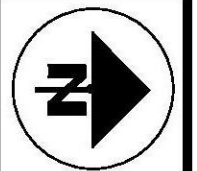
STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
			FT EACH	EACH	EACH
315+09	RT	24			1
315+48	LT	26			1
320+30	RT	24			1
320+77	RT	28			1
320+96	RT	25			1
324+44	RT	26			1
<b>SHEET TOTALS</b>			0	0	6



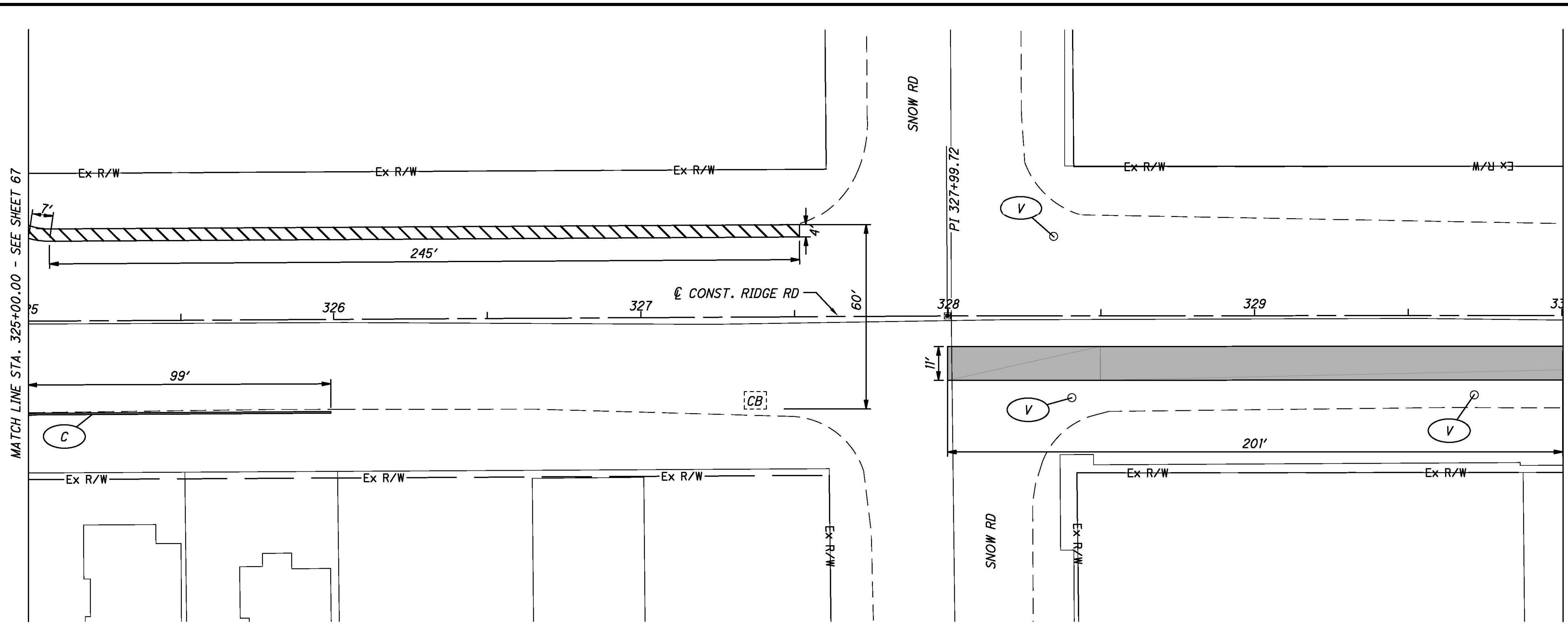
**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)
- ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR
- 4" CONCRETE WALK

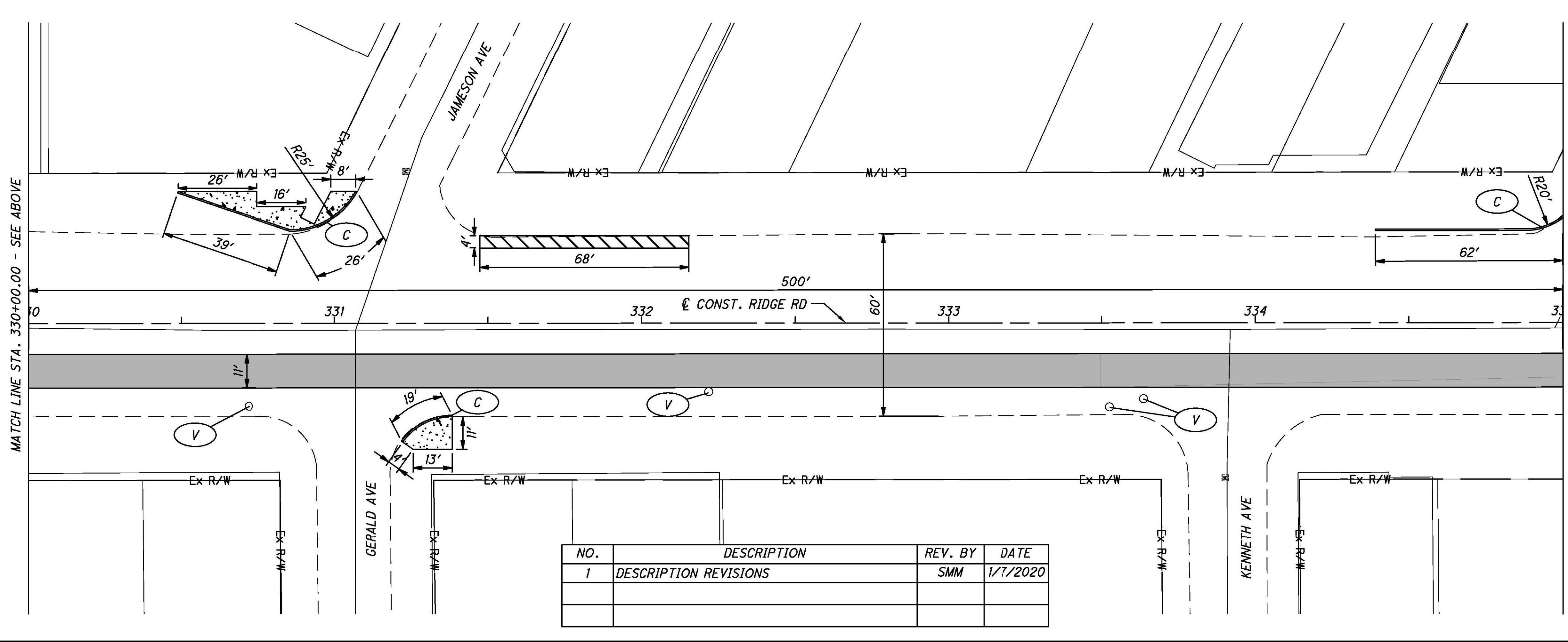
NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020



0 20 40  
HORIZONTAL SCALE IN FEET  
CALCULATED  
SMM  
CHECKED  
CSR



STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
		FT	EACH	EACH	EACH
327+27	RT	27	1		
328+35	LT	26			1
328+41	RT	27			1
329+41	RT	26			1
330+72	RT	27			1
332+22	RT	22			1
333+52	RT	27			1
333+64	RT	25			1
<b>SHEET TOTALS</b>			<b>1</b>	<b>0</b>	<b>7</b>



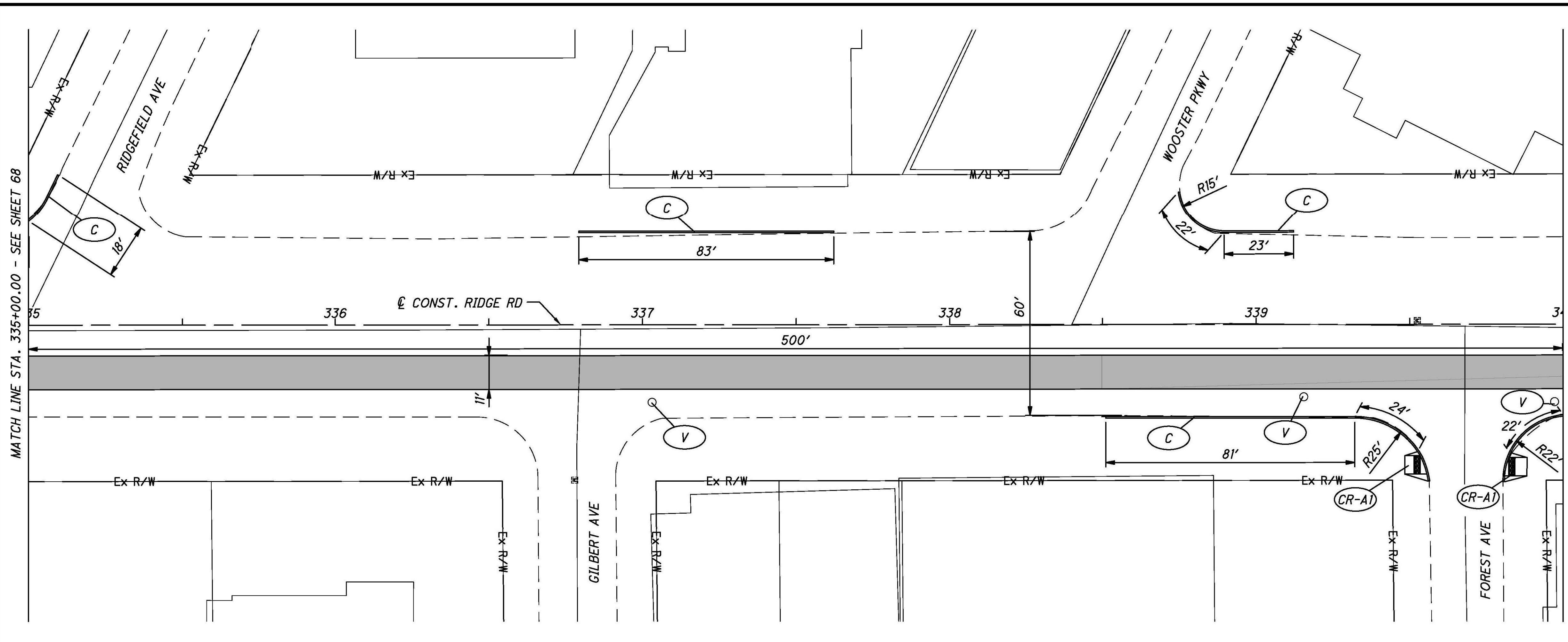
**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)
- ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR
- 4" CONCRETE WALK

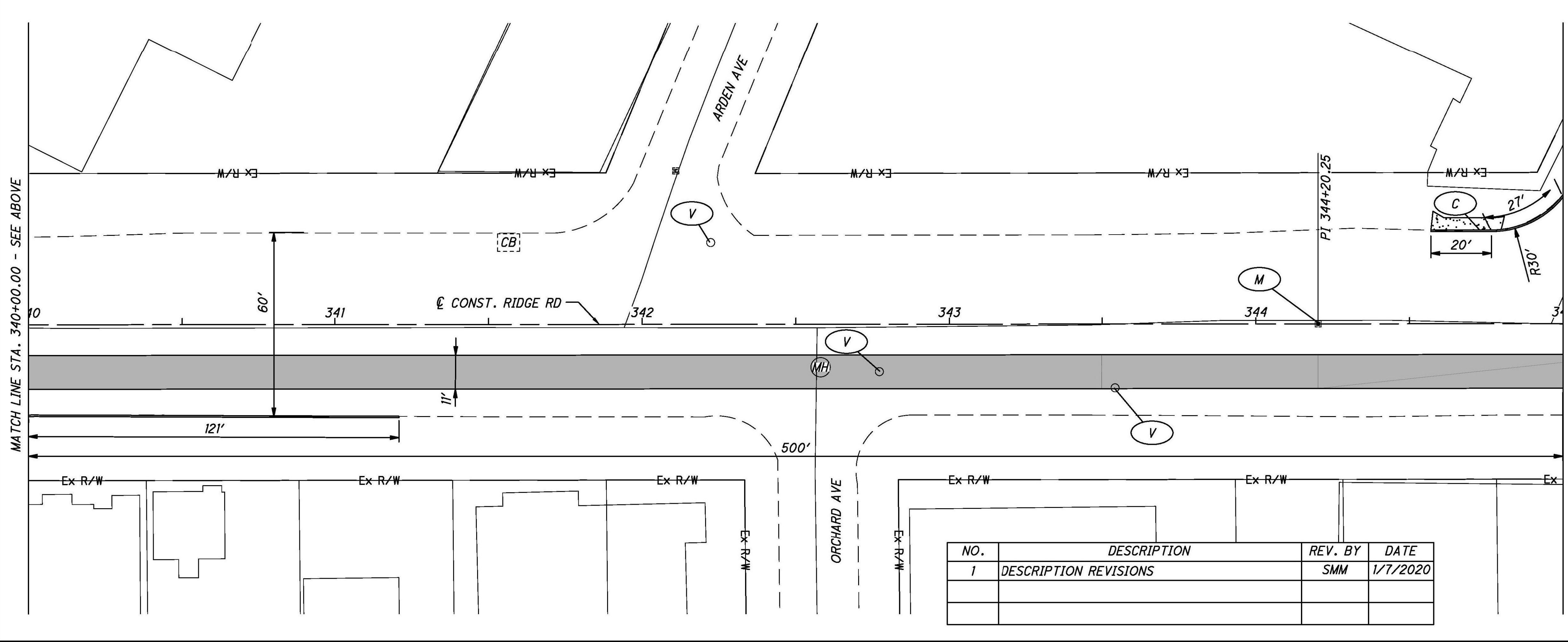
NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020

RIDGE RD PLAN/PLAN  
STA. 325+00 TO STA. 335+00

CUY-SR 003 06.22  
68  
93



STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
			FT	EACH	EACH
337+03	RT	25			1
339+16	RT	24			1
339+97	RT	25			1
341+57	LT	27	1		
342+22	LT	27			1
342+58	RT	14		1	
342+77	RT	15			1
343+54	RT	21			1
<b>SHEET TOTALS</b>			1	1	6



**LEGEND**

- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)

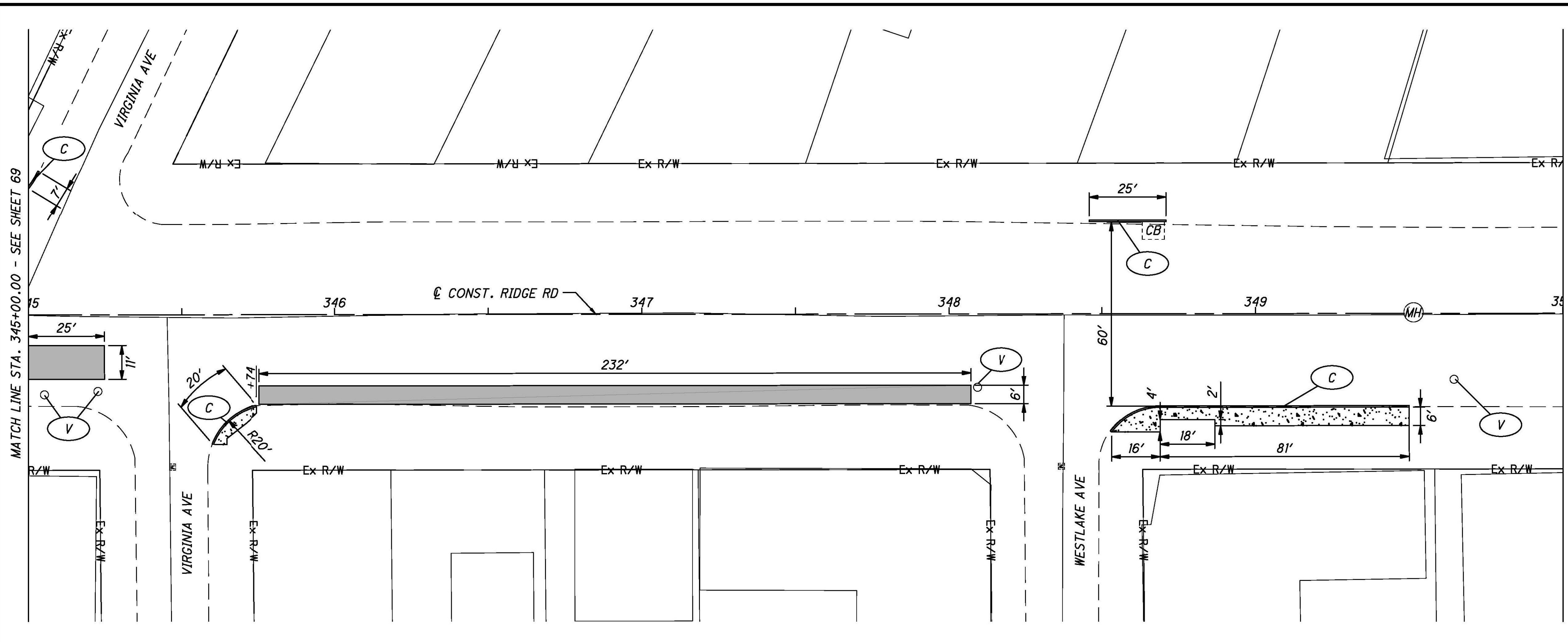
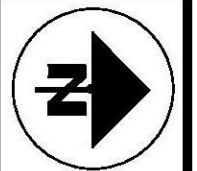
ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

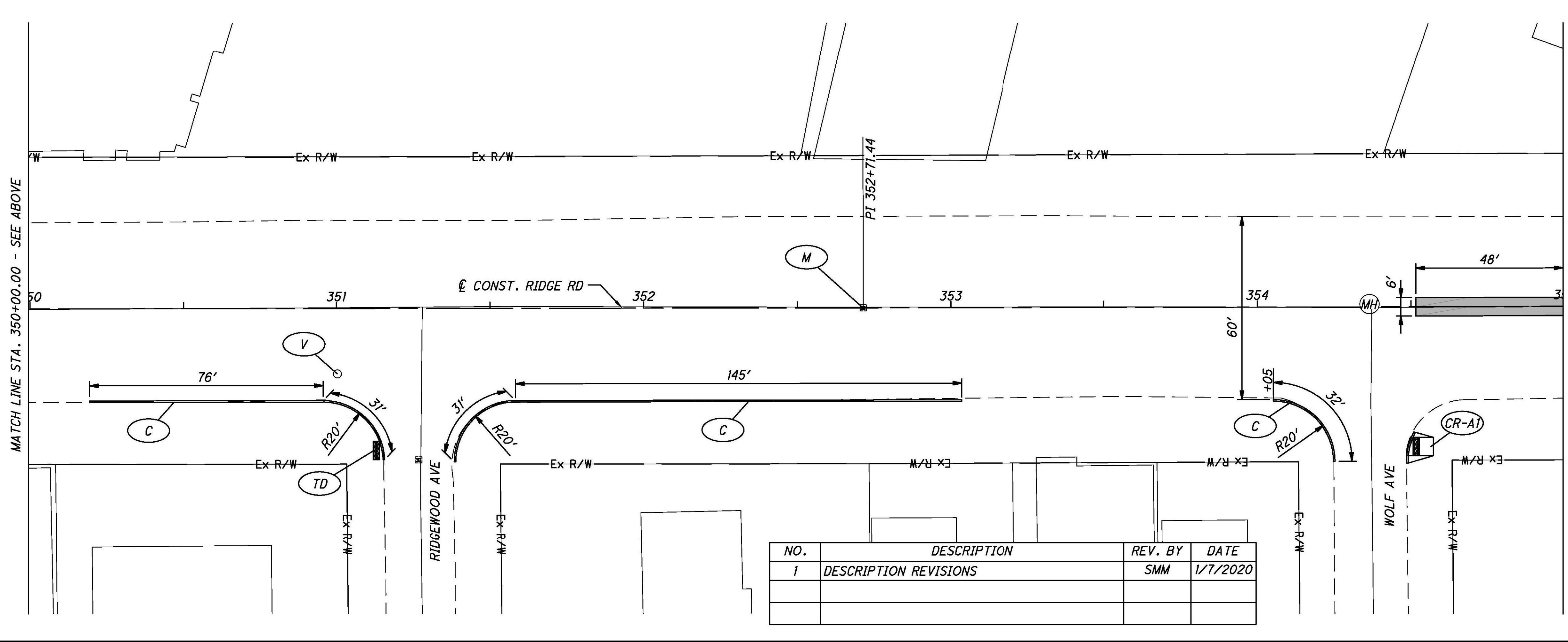
4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020





STATION	SIDE	OFFSET	611	611	638
			CATCH BASIN ADJUSTED TO GRADE	MANHOLE ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE
			FT EACH	EACH	EACH
345+05	RT	26			1
345+23	RT	25			1
348+09	RT	24			1
348+67	LT	27	1		
349+51	CL			1	
349+65	RT	22			1
351+00	RT	21			1
354+37	CL			1	
<b>SHEET TOTALS</b>			1	2	5



**LEGEND**

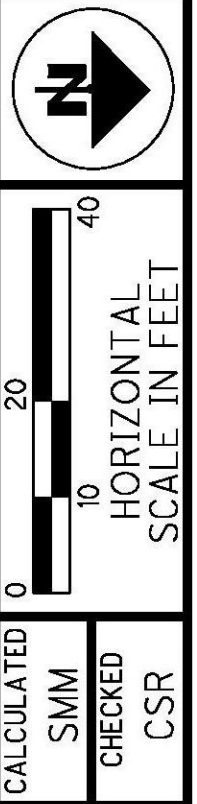
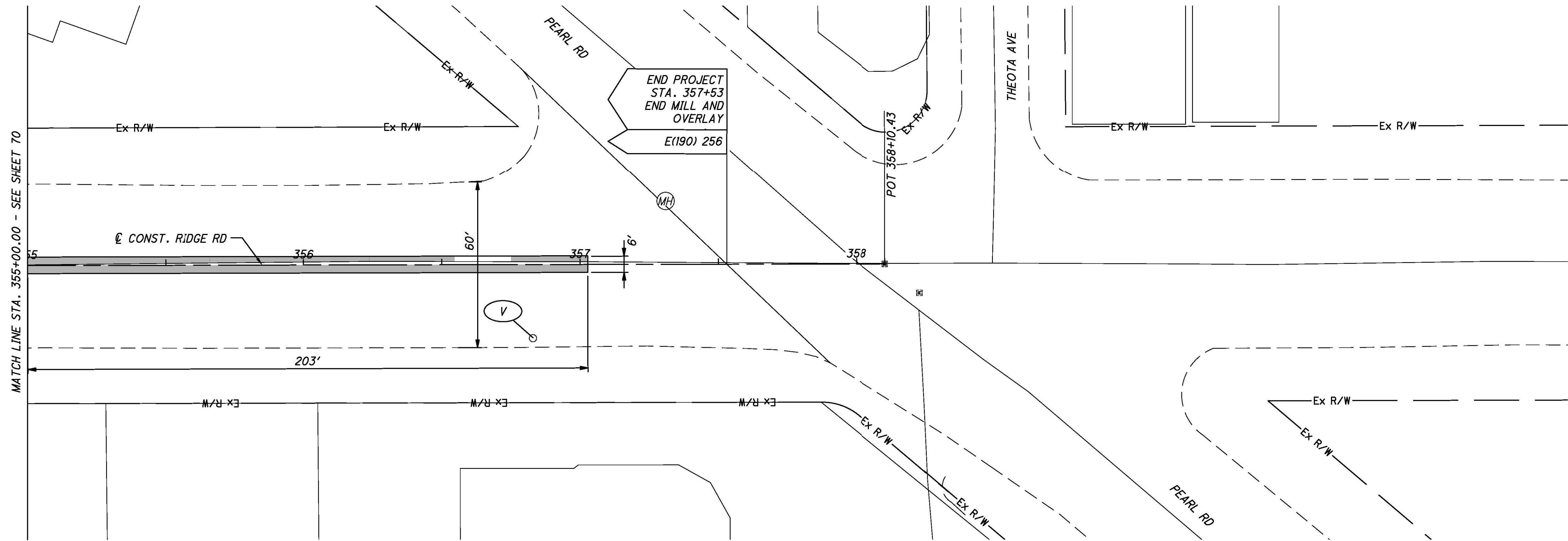
- CB EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
- MH EXISTING MANHOLE TO BE ADJUSTED TO GRADE
- CR-# RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMP, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
- C PROPOSED CURB, TYPE 6
- C1 PROPOSED CURB, TYPE 6, AS PER PLAN
- C2 PROPOSED CURB, TYPE 2-A, AS PER PLAN
- M EXISTING MONUMENT TO BE ADJUSTED TO GRADE
- TD PROPOSED TRUNCATED DOMES
- V EXISTING VALVE TO BE ADJUSTED TO GRADE
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)
- ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)
- ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR
- 4" CONCRETE WALK

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020

MATCH LINE STA. 355+00.00 - SEE SHEET 71

PI 352+71.44

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CALCULATED SMM CHECKED CSR  
 RIDGE RD PLAN/PLAN  
 STA. 355+00 TO STA. 358+10

CUY-SR 003 06.22  
 71  
 93

NO.	DESCRIPTION	REV. BY	DATE
1	DESCRIPTION REVISIONS	SMM	1/7/2020

STATION	SIDE	OFFSET FT	611	611	638
			CATCH BASIN ADJUSTED TO GRADE EACH	MANHOLE ADJUSTED TO GRADE EACH	VALVE BOX ADJUSTED TO GRADE EACH
356+83	RT	27			1
357+31	LT	23		1	
<b>SHEET TOTALS</b>			0	1	1

- LEGEND**
- EXISTING CATCH BASIN TO BE ADJUSTED TO GRADE.
  - EXISTING MANHOLE TO BE ADJUSTED TO GRADE
  - RECONSTRUCT WITH NEW ADA COMPLIANT CURB RAMPS, PER ITEM 608 CURB RAMP, COMPLETE IN PLACE WITH TILE (NUMBER REFLECTS CURB RAMP TYPE PER ODOT STANDARD DRAWINGS; SEE GENERAL NOTES)
  - PROPOSED CURB, TYPE 6
  - PROPOSED CURB, TYPE 6, AS PER PLAN
  - PROPOSED CURB, TYPE 2-A, AS PER PLAN
  - EXISTING MONUMENT TO BE ADJUSTED TO GRADE
  - PROPOSED TRUNCATED DOMES
  - EXISTING VALVE TO BE ADJUSTED TO GRADE
  - ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (ASPHALT CONCRETE)
  - ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (CONCRETE)
  - ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR
  - 4" CONCRETE WALK