

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616 - WATER 1 MGAL

MAINTENANCE OF TRAFFIC SCHEME

THE CONTRACTOR SHALL DEVISE A SIMPLE MAINTENANCE OF TRAFFIC SCHEME FOR EACH LOCATION, WHICH SHALL BE STAMPED BY A PROFESSIONAL ENGINEER (SCHEME MAY BE A HAND SKETCH) AND PRESENT IT TO THE DISTRICT WORK ZONE SAFETY ENGINEER AND PROJECT ENGINEER FOR ACCEPTANCE AT LEAST TWO WEEKS PRIOR TO IMPLEMENTATION. IN GENERAL, THE METHODS FOR MAINTAINING TRAFFIC THAT THE CONTRACTOR PROPOSES TO USE FOR CONDUCTING THE WORK IN A SAFE AND EFFICIENT MANNER SHALL BE SUPPORTED BY HAND SKETCHES AS NECESSARY. THE MAINTENANCE OF TRAFFIC SCHEME SHALL BE IN CONFORMANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, LATEST REVISION, THE REFERENCED STANDARD CONSTRUCTION DRAWINGS, THE ATTACHED MAINTENANCE OF TRAFFIC SHEETS, AND THE SPECIFICATIONS. THE CONTRACTOR SHALL NOT COMMENCE WORK UNTIL THE MAINTENANCE OF TRAFFIC SCHEME HAS BEEN ACCEPTED.

IF DURING THE PROJECT, THE ENGINEER DETERMINES THAT THE APPROVED MAINTENANCE OF TRAFFIC PLAN IS NOT PERFORMING AS DESIRED, THE WORK SHALL BE SUSPENDED UNTIL THE PROBLEM IS RESOLVED TO THE SATISFACTION OF THE ENGINEER AND THE MAINTENANCE OF TRAFFIC PLAN IS REVISED ACCORDINGLY. ANY COSTS OR DELAYS INCURRED AS A RESULT OF THE FAILURE OF THE CONTRACTOR TO ADJUST THE MAINTENANCE OF TRAFFIC SCHEME TO THE SATISFACTION OF THE ENGINEER SHALL BE THE FULL RESPONSIBILITY OF THE CONTRACTOR. DURING NON-WORKING HOURS, ALL LANES SHALL BE IN FULL OPERATION WITH ALL TRAFFIC CONTROL SIGNS, EXCEPT OW-124 (ROAD CONSTRUCTION AHEAD) SIGNS, REMOVED OR COVERED AND ALL CHANNELIZING DEVICES REMOVED FROM THE PAVEMENT SURFACES. CHANNELIZING DEVICES MAY BE STORED OR DEPLOYED TEMPORARILY ADJACENT TO THE SHOULDER TO MINIMIZE THE NIGHTLY TRAFFIC CONTROL SET-UP TIME. PAYMENT FOR ALL THE ITEMS REQUIRED TO MAINTAIN TRAFFIC IN ACCORDANCE WITH THESE REQUIREMENTS IS INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

EXTRA ADVANCE WARNING SIGNS

AN EXTRA ADVANCE WARNING SIGN GROUP CONSISTS OF TWO W20-1 (ROAD WORK AHEAD) SIGNS, TWO W20-5 (RIGHT/LEFT LANE CLOSED AHEAD) SIGNS WITH W16-3aP DISTANCE PLATES, AND TWO W3-H4b (WATCH FOR STOPPED TRAFFIC) SIGNS AND REQUIRED WARNING LIGHTS.

THE CONTRACTOR SHALL PROVIDE, ERECT, MAINTAIN AND REMOVE EXTRA ADVANCE WARNING SIGN GROUPS AS SHOWN ON TRAFFIC SCD MT-95.50 AT THE FOLLOWING DISTANCES IN ADVANCE OF THE LANE TAPERS WITH THE APPROPRIATE W16-3aP DISTANCE PLATES:

- 1) LOCATION 4: CUY-71-1640 (IR 71 OVER NORFOLK SOUTHERN RAILWAY) LANE TAPER STATION 922+50±, PHASE 1 (IR 71 NORTHBOUND); PROVIDE SIGN GROUPS AT 2 MILES, 3 MILES, AND 4 MILES.
- 2) LOCATION 4: CUY-71-1640 (IR 71 OVER NORFOLK SOUTHERN RAILWAY) LANE TAPER STATION 958+47±, PHASE 1 (IR 71 SOUTHBOUND); PROVIDE SIGN GROUPS AT 2 MILES, 3 MILES, AND 4 MILES.
- 3) LOCATION 4: CUY-71-1640 (IR 71 OVER NORFOLK SOUTHERN RAILWAY) LANE TAPER STATION 912+14.35±, PHASE 2 (IR 71 NORTHBOUND); PROVIDE SIGN GROUPS AT 2 MILES, 3 MILES, AND 4 MILES.
- 4) LOCATION 4: CUY-71-1640 (IR 71 OVER NORFOLK SOUTHERN RAILWAY) LANE TAPER STATION 955+30±, PHASE 2 (IR 71 SOUTHBOUND); PROVIDE SIGN GROUPS AT 2 MILES, 3 MILES, AND 4 MILES.
- 5) LOCATION 4: CUY-71-1640 (IR 71 OVER NORFOLK SOUTHERN RAILWAY) LANE TAPER STATION 942+89.35±, PHASE 3 (IR 71 NORTHBOUND); PROVIDE SIGN GROUPS AT 2 MILES, 3 MILES, AND 4 MILES.
- 6) LOCATION 4: CUY-71-1640 (IR 71 OVER NORFOLK SOUTHERN RAILWAY) LANE TAPER STATION 952+50±, PHASE 3 (IR 71 SOUTHBOUND); PROVIDE SIGN GROUPS AT 2 MILES, 3 MILES, AND 4 MILES.

THE CONTRACTOR SHALL HAVE AN ADDITIONAL EXTRA ADVANCE WARNING SIGN GROUP (6 SIGNS AND 2 DISTANCE PLATES) AVAILABLE FOR USE WHEN DIRECTED BY THE ENGINEER. THE DISTANCE PLATES FOR THIS GROUP SHALL BE ABLE TO BE MODIFIED IN THE FIELD TO SHOW APPROPRIATE WHOLE MILES TO THE LANE TAPER.)

PAYMENT FOR PROVIDING, ERECTING, MAINTAINING AND REMOVING EXTRA ADVANCE WARNING SIGN GROUPS SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 - MAINTAINING TRAFFIC.

ITEM 614 - WORK ZONE PAVEMENT MARKING, MISC.: REMOVABLE, NON-REFLECTIVE PREFORMED BLACKOUT TAPE

THIS ITEM SHALL CONFORM TO CMS 614.11 WORK ZONE PAVEMENT MARKING REQUIREMENTS WITH THE EXCEPTION THAT THE MARKING MATERIAL SHALL BE BLACKOUT TAPE CONFORMING TO SUPPLEMENTAL SPECIFICATION 987. THE BLACKOUT TAPE SHALL BE MANUFACTURED BY A SUPPLIER ON ODOT'S QUALIFIED PRODUCTS LIST FOR THIS ITEM. PAYMENT FOR THIS ITEM SHALL BE MADE AT THE UNIT PRICE BID PER LINEAR FOOT FOR ITEM 614 - WORK ZONE PAVEMENT MARKING, MISC.: REMOVABLE, NON-REFLECTIVE PREFORMED BLACKOUT TAPE AND SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT AND INCIDENTALS NECESSARY TO FURNISH, INSTALL, MAINTAIN, AND REMOVE THIS ITEM.

THE CONTRACTOR SHALL INSTALL A SINGLE CONTINUOUS PIECE OF BLACKOUT TAPE TO COVER THE EXISTING LONGITUDINAL AND TRANSVERSE PAVEMENT MARKING AS INDICATED IN THE PLANS.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED TO BE USED AS DIRECTED BY THE ENGINEER TO COVER UP PORTIONS OF THE EXISTING SHARED LANE MARKING WHERE THE EXISTING SHARED LANE MARKING IS IN CONFLICT WITH THE MOT MARKING.

LOCATION 2: CUY-42-1457 (US 42 [PEARL ROAD] OVER NS RAILWAY/CSX RAILWAY/BIG CREEK)
ITEM 614 - WORK ZONE PAVEMENT MARKING MISC.: REMOVABLE, NON-REFLECTIVE PREFORMED BLACKOUT TAPE 200 FT

ITEM 621 - RAISED PAVEMENT MARKERS REMOVED, AS PER PLAN

THIS NOTE APPLIES TO THE FOLLOWING LOCATION:

LOCATION 4: CUY-071-1640 (IR 71 OVER NORFOLK SOUTHERN RAILWAY)

THIS ITEM SHALL CONSIST OF REMOVING THE EXISTING RAISED PAVEMENT MARKER REFLECTORS FROM THE EXISTING RAISED PAVEMENT MARKERS PRIOR TO MAINTENANCE OF TRAFFIC PHASE 1.

ALL LABOR, MATERIALS, EQUIPMENT AND INCIDENTALS REQUIRED TO REMOVE THE EXISTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE INCLUDED WITH ITEM 621 - RAISED PAVEMENT MARKER REMOVED, AS PER PLAN.

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH CMS 614.03.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF CMS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

LOCATION 2: CUY-42-1457 (US 42 [PEARL ROAD] OVER NS RAILWAY/CSX RAILWAY/BIG CREEK)
ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN (ASSUMING 4 PCMS SIGNS FOR 1 MONTH) 4 SNMT

LOCATION 4: CUY-71-1640 (IR 71 OVER NORFOLK SOUTHERN RAILWAY)
ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN (ASSUMING 4 PCMS SIGNS FOR 1 MONTH) 6 SNMT

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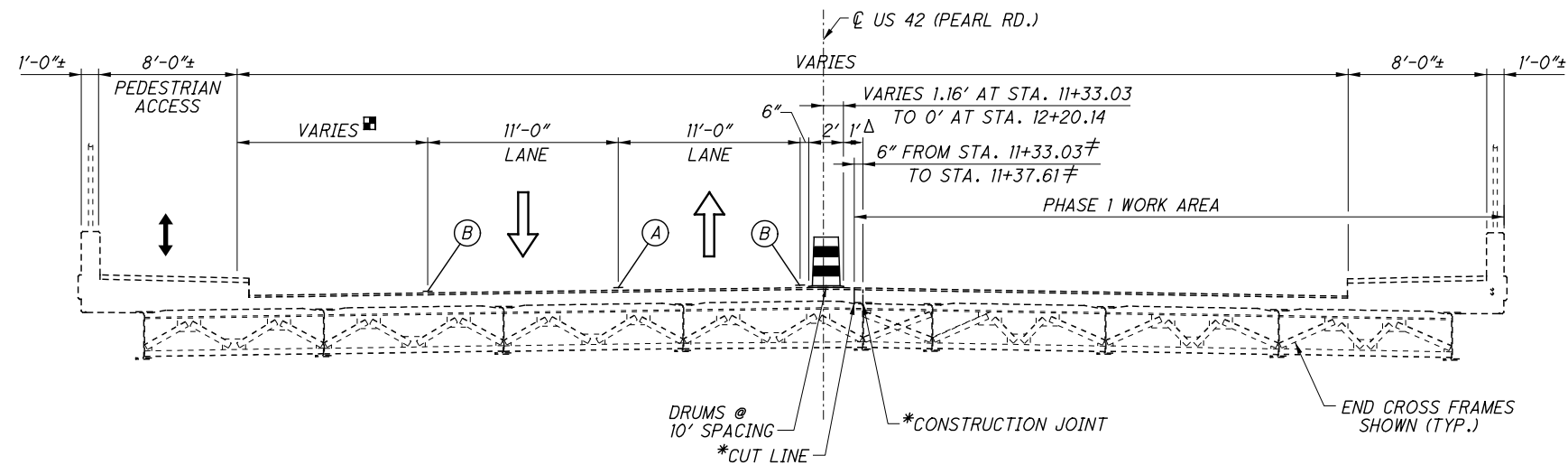
STATION		SIDE	614							642						
			WORK ZONE CENTER LINE, CLASS I, 740.06, TYPE I MILE	WORK ZONE EDGE LINE, CLASS I, 4", 740.06, TYPE I MILE	WORK ZONE CHANNELIZING LINE, CLASS I, 8", 740.06, TYPE I FT	WORK ZONE DOTTED LINE, CLASS I, 4", 740.06, TYPE I FT	WORK ZONE STOP LINE, CLASS I, 740.06, TYPE I FT	WORK ZONE CROSSWALK LINE, CLASS I, 740.06, TYPE I FT	WORK ZONE ARROW, CLASS I, 740.06, TYPE I EACH	WORK ZONE PAVEMENT MARKING, MISC.: REMOVABLE, NON-REFLECTIVE PREFORMED BLACKOUT TAPE FT	EDGE LINE, 4", TYPE I MILE	LANE LINE, 4", TYPE I MILE	CENTER LINE, TYPE I MILE	STOP LINE, TYPE I FT	CROSSWALK LINE, TYPE I FT	SHARED LANE MARKING, TYPE I EACH
FROM	TO															
LOCATION 2: PRE-PHASE 1																
10+15.11	10+57.66	LT												88.00		
28+65.93	29+05.54	LT												71.00		
31+44.25	31+70.48	RT												50.00		
31+80.18	32+00.17	LT/RT												116.00		
LOCATION 2: PHASE 1 BLACKOUT TAPE																
11+32.00	28+65.35	LT							2178.35							
WORK ZONE PAVEMENT MARKING																
9+98.37	10+14.62	LT/RT						244.00								
10+11.27	11+32.00	LT				121.00										
11+32.00		LT					11.00									
11+34.00	28+76.76	LT	1742.76	3485.52												
28+76.76	29+17.00	LT				84.00										
29+17.00	30+82.00	LT			165.00											
29+40.00	30+60.00	LT						6								
29+60.00		RT						2								
LOCATION 2: PHASE 2 BLACKOUT TAPE																
11+05.35	29+03.19	RT							2247.84							
WORK ZONE PAVEMENT MARKING																
8+65.00	9+85.20	RT						6								
8+88.50	9+93.50	RT			105.00											
9+98.37	10+14.62	LT/RT					244.00									
10+11.65	11+35.00	RT				126.00										
11+35.00		RT					11.00									
11+37.00	28+77.00	RT	1740.00	3480.00												
28+77.00	29+17.00	RT/LT				87.00										
29+17.00	30+82.00	LT			165.00											
29+40.00	30+60.00	LT						6								
LOCATION 2: POST PHASE 2																
PERMANENT PAVEMENT MARKING																
10+01.46	10+11.51	LT/RT												124.00		
11+32.00	28+77.00	LT/RT								3490.00	3490.00	1745.00	36.00		18	
TOTALS			3482.76	6965.52	435.00	418.00	22.00	488.00	20	4426.19						
CONVERT TO MILES			0.66	1.32												
TOTALS CARRIED TO GENERAL SUMMARY			0.66	1.32	435	418	22	488	20	4427						

**MAINTENANCE OF TRAFFIC SUBSUMMARY
LOCATION 2: CUY-042-1457**

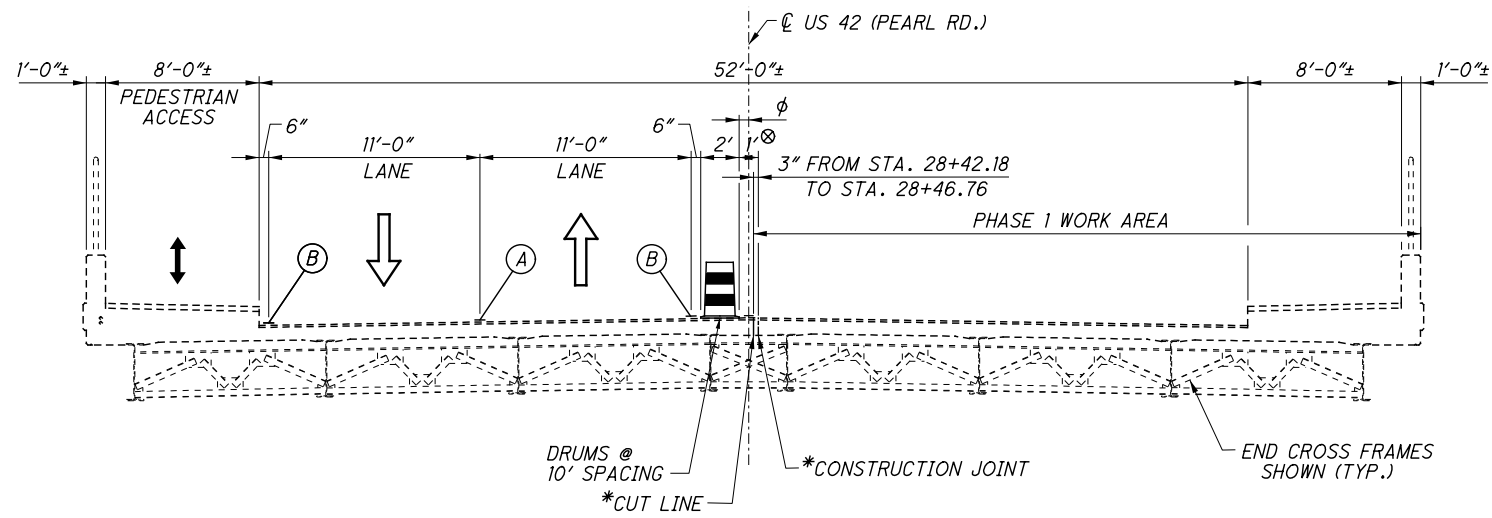
**CUY-071-16.40/ VAR REPAIR
PID NO. 111603**

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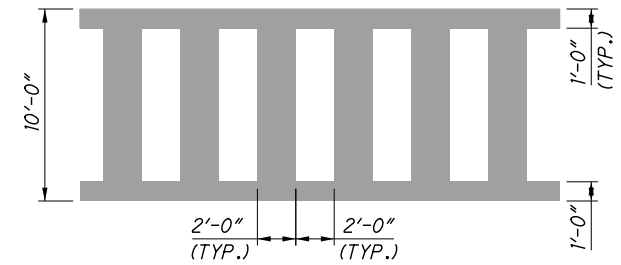
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MAINTENANCE OF TRAFFIC TYPICAL SECTION A-A - PHASE 1
BRIDGE NO. CUY-042-1457 (US 42 [PEARL RD.] OVER NORFOLK SOUTHERN RAILWAY)
 STA. 11+32.83 TO STA. 12+20.14



MAINTENANCE OF TRAFFIC TYPICAL SECTION B-B - PHASE 1
BRIDGE NO. CUY-042-1457 (US 42 [PEARL RD.] OVER NORFOLK SOUTHERN RAILWAY)
 STA. 12+20.14 TO STA. 28+46.76



WORK ZONE CROSSWALK MARKING DETAIL
 (LONGITUDINAL AND TRANSVERSE LINES)



FINAL CROSSWALK MARKING DETAIL
 (LONGITUDINAL LINES ONLY)

LEGEND

- ➔ DIRECTION OF TRAVEL
- * AT WORK AREAS ONLY
- ≠ STATIONING AT CUT LINE
- △ VARIES 1.18' AT STA. 11+32.83 TO 1.05' AT STA. 11+37.44
- VARIES 10.31' AT STA. 11+32.83 TO 0.5' AT STA. 12+20.14
- φ VARIES 0' AT STA. 12+20.14 TO 1' AT STA. 12+70.39
 1' FROM STA. 12+70.39 TO STA. 28+46.76
- ⊗ 1' FROM STA. 28+42.18 TO STA. 28+46.76

MOT PAVEMENT MARKING LEGEND

- (A) ITEM 614 - WORK ZONE CENTER LINE (DOUBLE, SOLID)
- (B) ITEM 614 - WORK ZONE EDGE LINE (WHITE)

NOTES:

1. DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATIONAL PURPOSES ONLY.

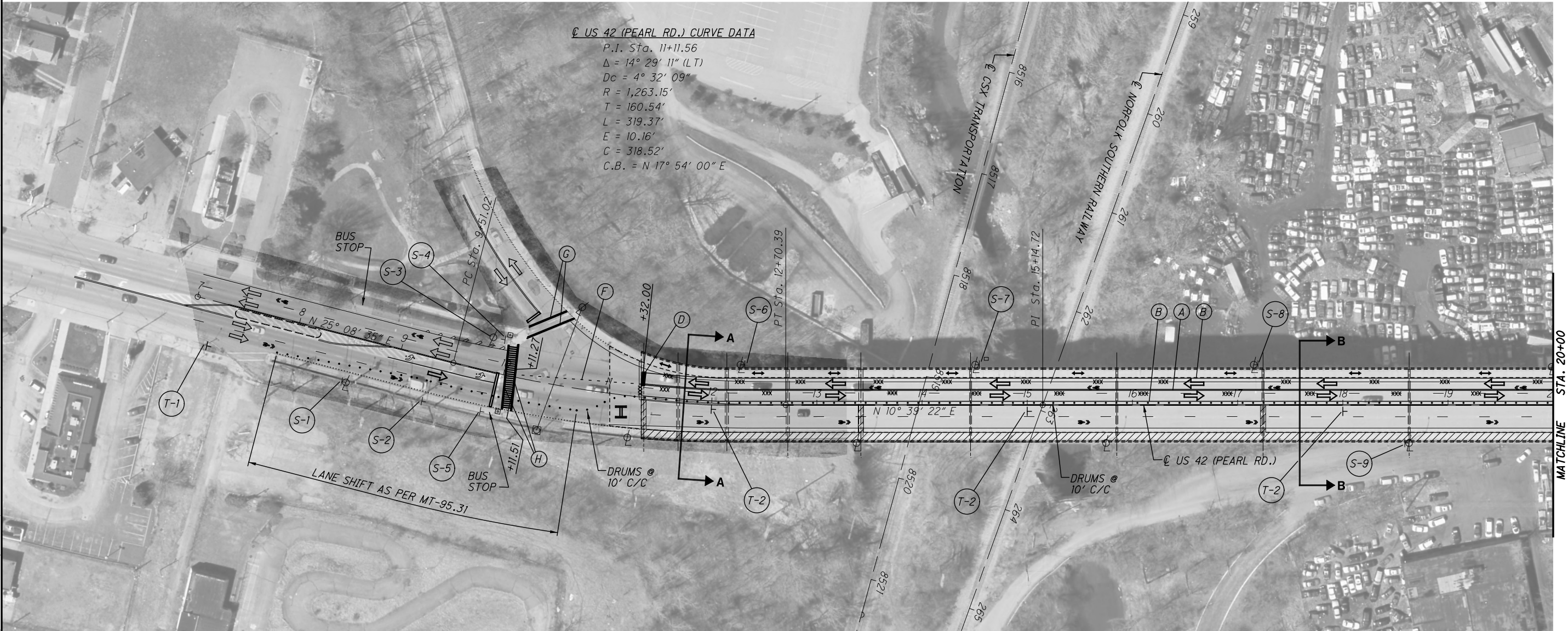
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<div style="border: 1px solid black; padding: 2px; width: 50px; margin: 0 auto;">SHARE THE ROAD</div> <p>W16-1P-18 STA. 7+20.00, 43± RT. (GROUND MOUNTED) (TEMPORARY SIGN TO BE ERECTED PRIOR TO PHASE 1 AND TO BE REMOVED AFTER PHASE 2 CONSTRUCTION)</p> <div style="border: 1px solid black; border-radius: 50%; width: 20px; height: 20px; display: flex; align-items: center; justify-content: center; margin: 0 auto;">T-1</div>	<div style="border: 1px solid black; padding: 2px; width: 50px; margin: 0 auto;">SHARE THE ROAD</div> <p>W16-1P-18 STA. 12+00.00 STA. 15+00.00 STA. 18+00.00 (TEMPORARY SIGN SUPPORT)</p> <div style="border: 1px solid black; border-radius: 50%; width: 20px; height: 20px; display: flex; align-items: center; justify-content: center; margin: 0 auto;">T-2</div>	<div style="border: 1px solid black; padding: 2px; width: 50px; margin: 0 auto;">COVER IN PHASE 1 BUS STOP ZOO</div> <p>STA. 8+56.8±, 43.7± RT. (LIGHT POLE MOUNTED) (DO NOT DISTURB)</p> <div style="border: 1px solid black; border-radius: 50%; width: 20px; height: 20px; display: flex; align-items: center; justify-content: center; margin: 0 auto;">S-1</div>	<div style="border: 1px solid black; padding: 2px; width: 50px; margin: 0 auto;">MAY USE FULL LANE BICYCLE</div> <p>STA. 9+35.2±, 42.6± RT. (POLE MOUNTED) (DO NOT DISTURB)</p> <div style="border: 1px solid black; border-radius: 50%; width: 20px; height: 20px; display: flex; align-items: center; justify-content: center; margin: 0 auto;">S-2</div>	<div style="border: 1px solid black; padding: 2px; width: 50px; margin: 0 auto;">Wildlife Pearl</div> <p>STA. 9+82.6±, 32.6± LT. (LIGHT POLE MOUNTED) (DO NOT DISTURB)</p> <div style="border: 1px solid black; border-radius: 50%; width: 20px; height: 20px; display: flex; align-items: center; justify-content: center; margin: 0 auto;">S-3</div>	<div style="border: 1px solid black; padding: 2px; width: 50px; margin: 0 auto;">MAY USE FULL LANE BICYCLE BUS STOP</div> <p>STA. 9+94.3±, 32.8± LT. (GROUND MOUNTED) (DO NOT DISTURB)</p> <div style="border: 1px solid black; border-radius: 50%; width: 20px; height: 20px; display: flex; align-items: center; justify-content: center; margin: 0 auto;">S-4</div>	<div style="border: 1px solid black; padding: 2px; width: 50px; margin: 0 auto;">RTA</div> <p>STA. 9+86.2±, 38.8± RT. (GROUND MOUNTED) (REMOVE AND REERECT)≠</p> <div style="border: 1px solid black; border-radius: 50%; width: 20px; height: 20px; display: flex; align-items: center; justify-content: center; margin: 0 auto;">S-5</div>	<div style="border: 1px solid black; padding: 2px; width: 50px; margin: 0 auto;">SPEED LIMIT 25</div> <p>STA. 12+26.2±, 37.6± LT. (LIGHT POLE MOUNTED) (DO NOT DISTURB)</p> <div style="border: 1px solid black; border-radius: 50%; width: 20px; height: 20px; display: flex; align-items: center; justify-content: center; margin: 0 auto;">S-6</div>	<div style="border: 1px solid black; padding: 2px; width: 50px; margin: 0 auto;">ZOO</div> <p>STA. 14+50.5±, 38.3± LT. (LIGHT POLE MOUNTED) (DO NOT DISTURB)</p> <div style="border: 1px solid black; border-radius: 50%; width: 20px; height: 20px; display: flex; align-items: center; justify-content: center; margin: 0 auto;">S-7</div>	<div style="border: 1px solid black; padding: 2px; width: 50px; margin: 0 auto;">MAY USE FULL LANE BICYCLE</div> <p>STA. 17+15.3±, 38.3± LT. (LIGHT POLE MOUNTED) (DO NOT DISTURB)</p> <div style="border: 1px solid black; border-radius: 50%; width: 20px; height: 20px; display: flex; align-items: center; justify-content: center; margin: 0 auto;">S-8</div>	<div style="border: 1px solid black; padding: 2px; width: 50px; margin: 0 auto;">SPEED LIMIT 25</div> <p>STA. 18+62.9±, 36.1± RT. (LIGHT POLE MOUNTED) (DO NOT DISTURB)</p> <div style="border: 1px solid black; border-radius: 50%; width: 20px; height: 20px; display: flex; align-items: center; justify-content: center; margin: 0 auto;">S-9</div>
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SIGNING LEGEND

US 42 (PEARL RD.) CURVE DATA

P.I. Sta. 11+11.56
 $\Delta = 14^\circ 29' 11''$ (LT)
 $D_c = 4^\circ 32' 09''$
 $R = 1,263.15'$
 $T = 160.54'$
 $L = 319.37'$
 $E = 10.16'$
 $C = 318.52'$
 $C.B. = N 17^\circ 54' 00'' E$



MOT PAVEMENT MARKING LEGEND

- (A) ITEM 614 - WORK ZONE CENTER LINE (DOUBLE, SOLID)
- (B) ITEM 614 - WORK ZONE EDGE LINE (WHITE)
- (C) ITEM 614 - WORK ZONE CHANNELIZING LINE
- (D) ITEM 614 - WORK ZONE STOP LINE
- (E) ITEM 614 - WORK ZONE ARROW
- (F) ITEM 614 - WORK ZONE DOTTED LINES
- (G) ITEM 642 - CROSSWALK (FINAL MARKING TO BE PERFORMED PRIOR TO PHASE 1 CONSTRUCTION)
- (H) ITEM 614 - WORK ZONE CROSSWALK LINE (FINAL MARKING LAYOUT TO MATCH EXISTING CROSSWALK WITH LONGITUDINAL LINES ONLY)

LEGEND

- WORK AREA
- DRUMS
- TYPE III BARRICADE
- REMOVABLE BLACKOUT TAPE
- TRAFFIC DIRECTION OF TRAVEL
- PEDESTRIAN DIRECTION OF TRAVEL
- \neq THE RTA SIGN AND BUS STOP SHALL BE TEMPORARILY MOVED DURING CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE WITH GCRTA PER THE MAINTENANCE OF TRAFFIC NOTES.

NOTES:

1. DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATIONAL PURPOSES ONLY.
2. SEE MT-95.31 AND MT-110.10 FOR ADDITIONAL DETAILS.
3. THE CONTRACTOR SHALL COVER MESSAGES THAT ARE IN CONFLICT WITH THE MOT PHASING PLANS ON ALL GUIDE SIGNS. THE CONTRACTOR SHALL COVER GROUND MOUNTED SIGNS THAT ARE IN CONFLICT WITH THE MOT PHASING.
4. FOR SECTION A-A AND SECTION B-B SEE SHEET 21.
5. FOR CROSSWALK MARKING DETAILS SEE SHEET 21.
6. THE CONTRACTOR SHALL FURNISH, ERECT, MAINTAIN, AND REMOVE THE SHARE THE ROAD SIGNS (W16-1P-18). THIS WORK SHALL BE INCLUDED WITH ITEM 614 - MAINTAINING TRAFFIC FOR PAYMENT.

CALCULATED ALP
CHECKED TJF

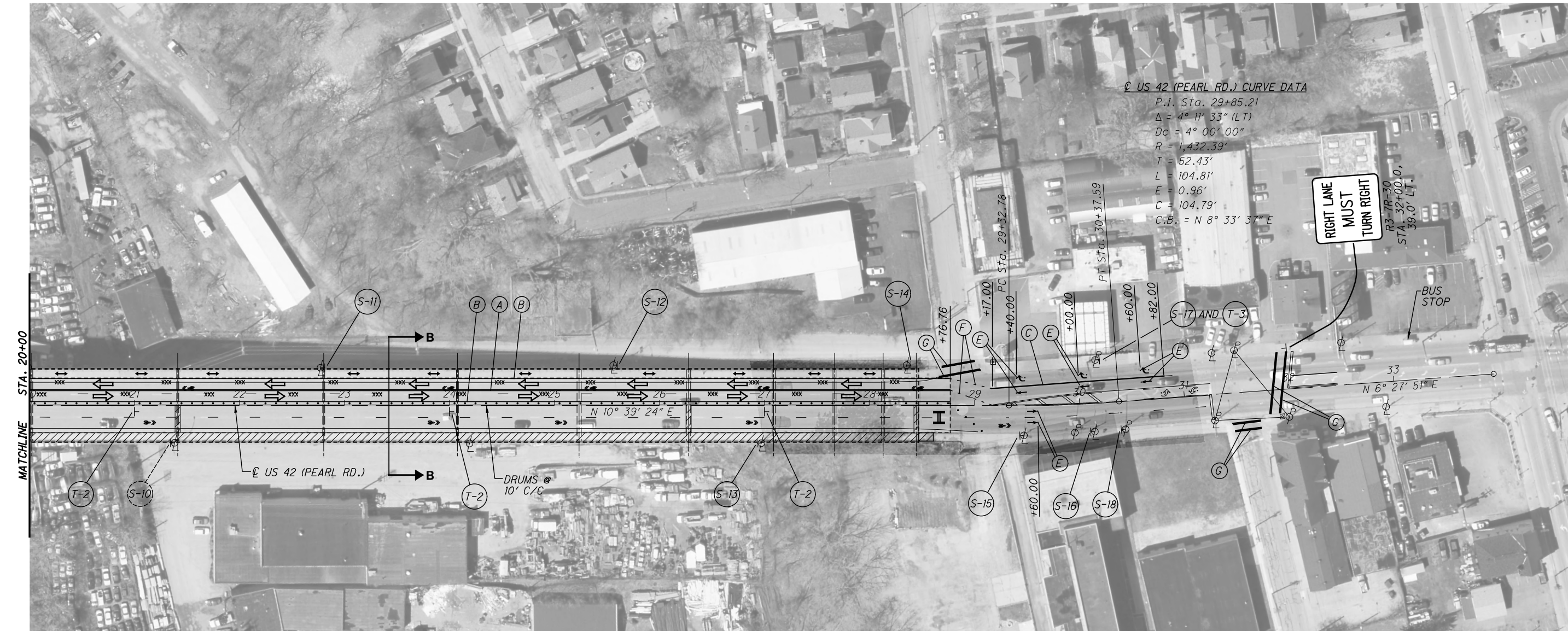
MAINTENANCE OF TRAFFIC PLAN
LOCATION 2: CUY-042-1457 - PHASE 1

CUY-071-16.40/ VAR REPAIR
PID NO. 111603

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SHARE THE ROAD
W16-1P-18
STA. 21+00.00
STA. 24+00.00
STA. 27+00.00
(TEMPORARY SIGN SUPPORT)
T-2

SHARE THE ROAD
W16-1P-18
STA. 7+20.00,
43'± RT.
(GROUND MOUNTED)
TO BE ERECTED PRIOR
TO PHASE 1 AND TO BE
REMOVED AFTER PHASE 2
CONSTRUCTION
T-3

MAY USE FULL LANE
W16-1P-18
STA. 21+38.2±,
35.6'± RT.
(LIGHT POLE MOUNTED)
(REMOVE AND REERECT)
S-10

SPEED LIMIT 25
W16-1P-18
STA. 22+77.1±,
35.9'± LT.
(LIGHT POLE MOUNTED)
(DO NOT DISTURB)
S-11

MAY USE FULL LANE
W16-1P-18
STA. 25+55.9±,
38.3'± LT.
(LIGHT POLE MOUNTED)
(DO NOT DISTURB)
S-12

MAY USE FULL LANE
W16-1P-18
STA. 26+97.0±,
36.0'± RT.
(LIGHT POLE MOUNTED)
(DO NOT DISTURB)
S-13

SPEED LIMIT 25
W16-1P-18
STA. 28+35.3±,
38.3'± LT.
(LIGHT POLE MOUNTED)
(DO NOT DISTURB)
S-14

SPEED LIMIT 25
W16-1P-18
STA. 29+46.7±,
28.7'± RT.
(LIGHT POLE MOUNTED)
(DO NOT DISTURB)
S-15

EMERGENCY SNOW STREET NO PARKING WHEN SNOW EXCEEDS 2 IN
W16-1P-18
STA. 30+13.1±,
27.2'± RT.
(LIGHT POLE MOUNTED)
(DO NOT DISTURB)
S-16

MAY USE FULL LANE
W16-1P-18
STA. 30+17.2±,
40.8'± LT.
(POLE MOUNTED)
(DO NOT DISTURB)
S-17

NO STOPPING BUS FULL STOP
W16-1P-18
STA. 30+42.0±,
26.9'± RT.
(POLE MOUNTED)
(DO NOT DISTURB)
S-18

SIGNING LEGEND

- MOT PAVEMENT MARKING LEGEND**
- (A) ITEM 614 - WORK ZONE CENTER LINE (DOUBLE, SOLID)
 - (B) ITEM 614 - WORK ZONE EDGE LINE (WHITE)
 - (C) ITEM 614 - WORK ZONE CHANNELIZING LINE
 - (D) ITEM 614 - WORK ZONE STOP LINE
 - (E) ITEM 614 - WORK ZONE ARROW
 - (F) ITEM 614 - WORK ZONE DOTTED LINES
 - (G) ITEM 642 - CROSSWALK (FINAL MARKING TO BE PERFORMED PRIOR TO PHASE 1 CONSTRUCTION)

- LEGEND**
- WORK AREA
 - DRUMS
 - TYPE III BARRICADE
 - REMOVABLE BLACKOUT TAPE
 - TRAFFIC DIRECTION OF TRAVEL
 - PEDESTRIAN DIRECTION OF TRAVEL
 - THE REMOVAL AND REERECTION OF THE EXISTING LIGHT POLE AND SIGN SHALL BE INCLUDED WITH ITEM 625 - REMOVE AND REERECT EXISTING LIGHT POLE, AS PER PLAN FOR PAYMENT.

- NOTES:**
1. DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATIONAL PURPOSES ONLY.
 2. SEE MT-95.31 AND MT-110.10 FOR ADDITIONAL DETAILS.
 3. THE CONTRACTOR SHALL COVER MESSAGES THAT ARE IN CONFLICT WITH THE MOT PHASING PLANS ON ALL GUIDE SIGNS. THE CONTRACTOR SHALL COVER GROUND MOUNTED SIGNS THAT ARE IN CONFLICT WITH THE MOT PHASING.
 4. FOR SECTION B-B SEE SHEET 21.
 5. FOR CROSSWALK MARKING DETAILS SEE SHEET 21.
 6. THE CONTRACTOR SHALL FURNISH, ERECT, MAINTAIN, AND REMOVE THE SHARE THE ROAD SIGNS (W16-1P-18). THIS WORK SHALL BE INCLUDED WITH ITEM 614 - MAINTAINING TRAFFIC FOR PAYMENT.



MAINTENANCE OF TRAFFIC PLAN
LOCATION 2: CUY-042-1457 - PHASE 1

CUY-071-16.40/ VAR REPAIR
PID NO. 111603

- SHARE THE ROAD

W16-1P-18
STA. 7+20.00,
43± RT.
(GROUND MOUNTED)
(TEMPORARY SIGN TO BE
ERECTED PRIOR
TO PHASE 1 AND TO BE
REMOVED AFTER PHASE 2
CONSTRUCTION)

(T-1)
- SHARE THE ROAD

W16-1P-18
STA. 13+00.00
STA. 16+00.00
STA. 19+00.00
(TEMPORARY SIGN
SUPPORT)

(T-2)
- ZOO
BUS STOP

COVER IN
PHASE 2

STA. 8+56.8±,
43.7± RT.
(LIGHT POLE MOUNTED)
(DO NOT DISTURB)

(S-1)
- MAY USE FULL LANE

STA. 9+35.2±,
42.6± RT.
(POLE MOUNTED)
(DO NOT DISTURB)

(S-2)
- Wildlife w/ Pearl

STA. 9+82.6±,
32.6± LT.
(LIGHT POLE MOUNTED)
(DO NOT DISTURB)

(S-3)
- MAY USE FULL LANE

STA. 9+94.3±,
32.8± LT.
(GROUND MOUNTED)
(DO NOT DISTURB)

(S-4)
- RTA

STA. 9+86.2±,
38.8± RT.
(GROUND MOUNTED)
(REMOVE AND
REERECT AT
STA. 6+20.00 RT.
DURING PHASE 1) ≠

(S-5)
- SPEED LIMIT 25

STA. 12+26.2±,
37.6± LT.
(LIGHT POLE MOUNTED)
(DO NOT DISTURB)

(S-6)
- ZOO

STA. 14+50.5±,
38.3± LT.
(LIGHT POLE MOUNTED)
(DO NOT DISTURB)

(S-7)
- MAY USE FULL LANE

STA. 17+15.3±,
38.3± LT.
(LIGHT POLE MOUNTED)
(DO NOT DISTURB)

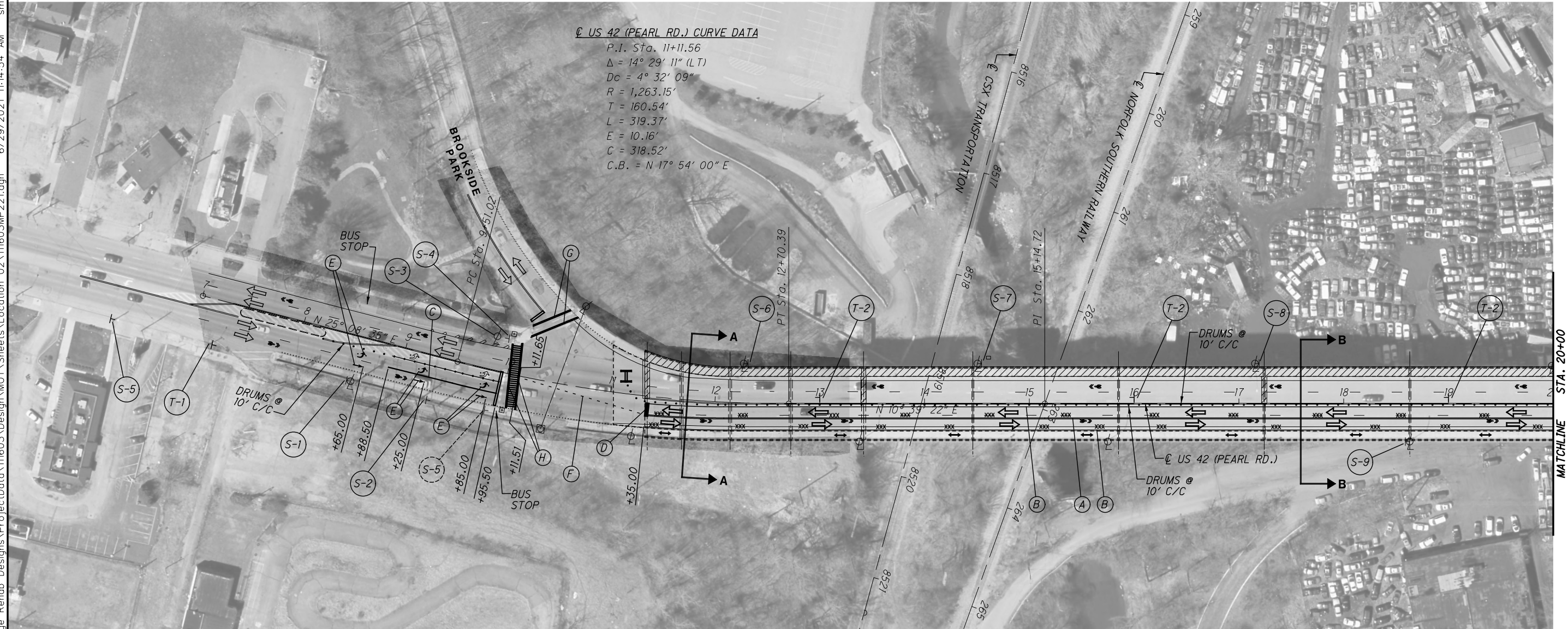
(S-8)
- SPEED LIMIT 25

STA. 18+62.9±,
36.1± RT.
(LIGHT POLE MOUNTED)
(DO NOT DISTURB)

(S-9)

SIGNING LEGEND

℄ US 42 (PEARL RD.) CURVE DATA
P.I. Sta. 11+11.56
Δ = 14° 29' 11" (LT)
Dc = 4° 32' 09"
R = 1,263.15'
T = 160.54'
L = 319.37'
E = 10.16'
C = 318.52'
C.B. = N 17° 54' 00" E



- MOT PAVEMENT MARKING LEGEND**
- (A) ITEM 614 - WORK ZONE CENTER LINE (DOUBLE, SOLID)
 - (B) ITEM 614 - WORK ZONE EDGE LINE (WHITE)
 - (C) ITEM 614 - WORK ZONE CHANNELIZING LINE
 - (D) ITEM 614 - WORK ZONE STOP LINE
 - (E) ITEM 614 - WORK ZONE ARROW
 - (F) ITEM 614 - WORK ZONE DOTTED LINES
 - (G) ITEM 642 - CROSSWALK (FINAL MARKING TO BE PERFORMED PRIOR TO PHASE 1 CONSTRUCTION)
 - (H) ITEM 614 - WORK ZONE CROSSWALK LINE (FINAL MARKING LAYOUT TO MATCH EXISTING CROSSWALK WITH LONGITUDINAL LINES ONLY)

- LEGEND**
- WORK AREA
 - DRUMS
 - TYPE III BARRICADE
 - REMOVABLE BLACKOUT TAPE
 - TRAFFIC DIRECTION OF TRAVEL
 - PEDESTRIAN DIRECTION OF TRAVEL
 - ≠ THE RTA SIGN AND BUS STOP SHALL BE TEMPORARILY MOVED DURING CONSTRUCTION. THE CONTRACTOR SHALL MOVE THE SIGN TO THE ORIGINAL LOCATION UPON COMPLETION OF THE PROJECT. THIS WORK SHALL BE INCLUDED WITH ITEM 630 - REMOVAL OF GROUND MOUNTED SIGN AND REERECTION, AS PER PLAN FOR PAYMENT.

- NOTES:**
1. DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATIONAL PURPOSES ONLY.
 2. SEE MT-95.31 AND MT-110.10 FOR ADDITIONAL DETAILS.
 3. THE CONTRACTOR SHALL COVER MESSAGES THAT ARE IN CONFLICT WITH THE MOT PHASING PLANS ON ALL GUIDE SIGNS. THE CONTRACTOR SHALL COVER GROUND MOUNTED SIGNS THAT ARE IN CONFLICT WITH THE MOT PHASING.
 4. FOR SECTION A-A AND SECTION B-B SEE SHEET 24.
 5. FOR CROSSWALK MARKING DETAILS SEE SHEET 21.
 6. THE CONTRACTOR SHALL FURNISH, ERECT, MAINTAIN, AND REMOVE THE SHARE THE ROAD SIGNS (W16-1P-18). THIS WORK SHALL BE INCLUDED WITH ITEM 614 - MAINTAINING TRAFFIC FOR PAYMENT.

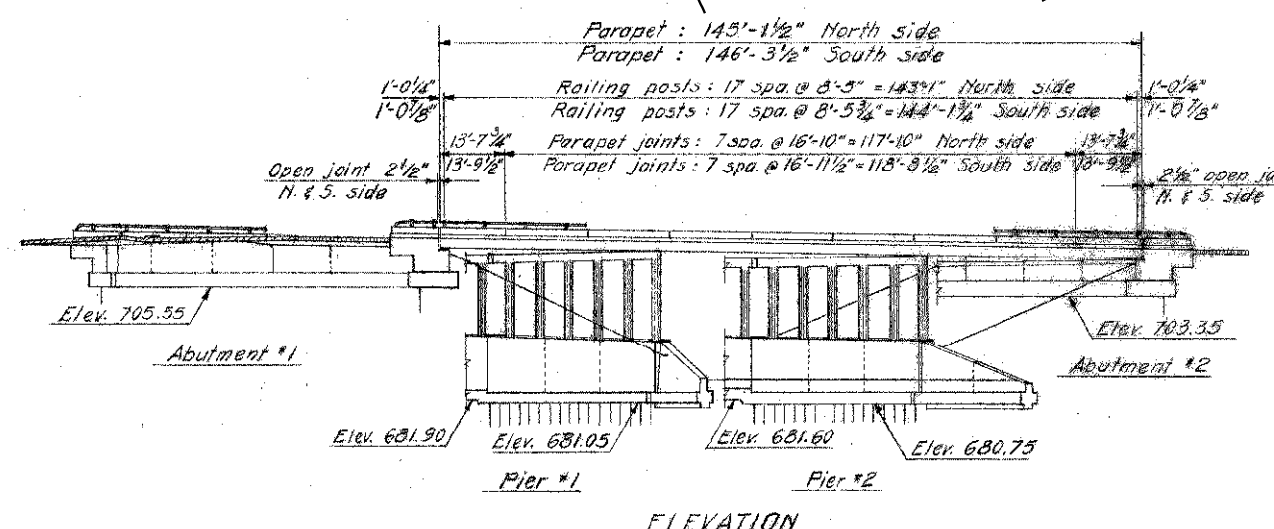
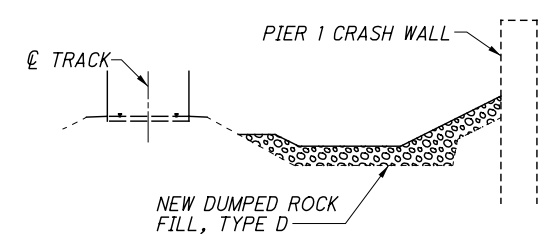
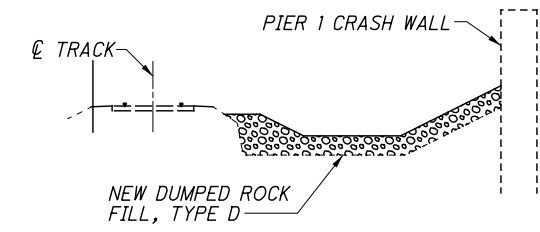
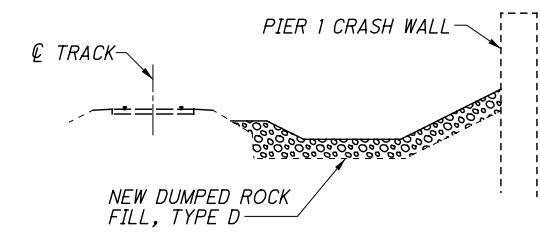
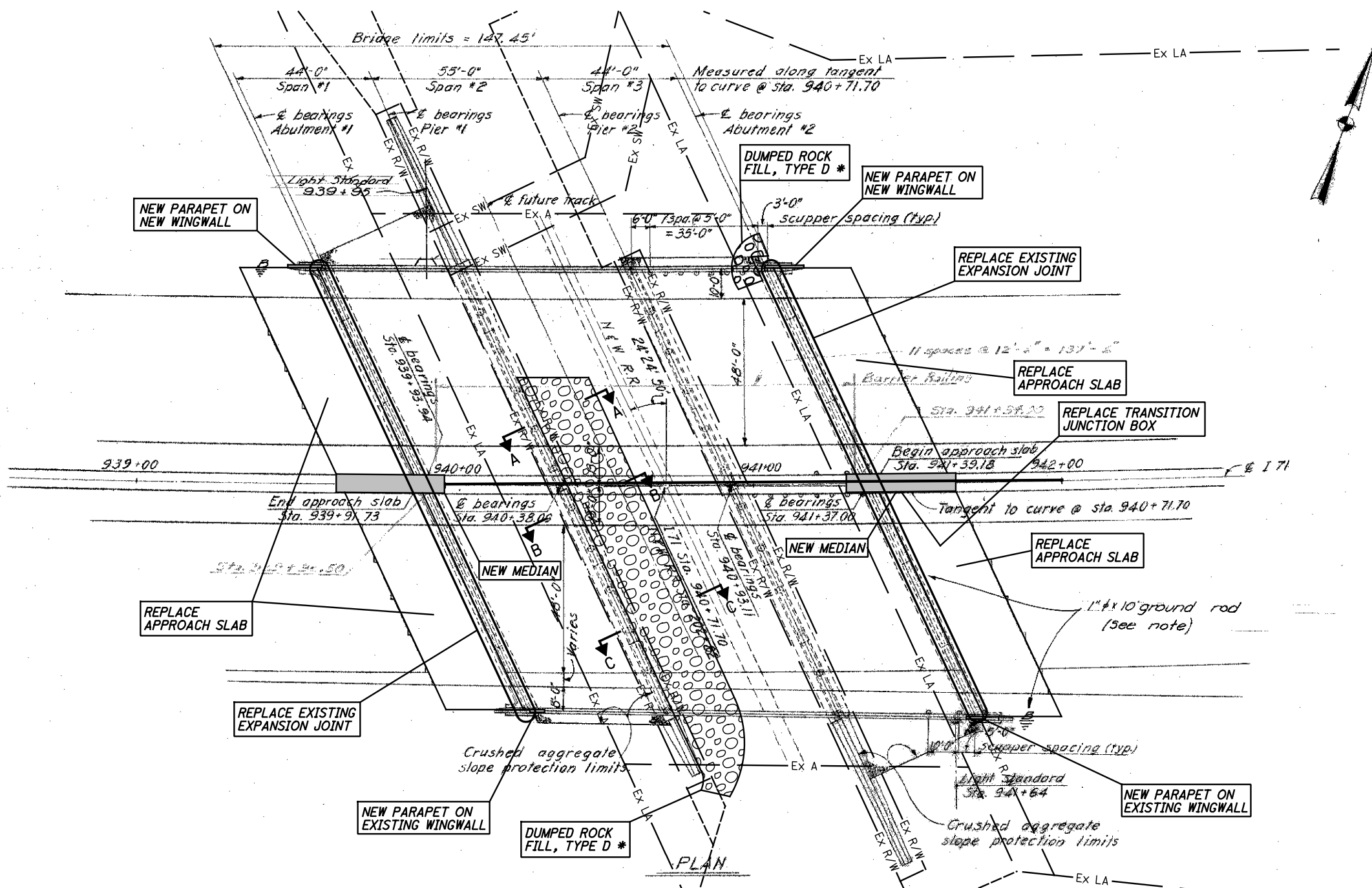
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SHEET NUMBER										ITEM	ITEM EXT.	PARTICIPATION		GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
			5-8	9-36	40-44	46-50	51-90	91-123									
TRAFFIC CONTROL																	
				357						621	00300			357	EACH	RPM REFLECTOR	
				357						621	54001			357	EACH	RAISED PAVEMENT MARKER REMOVED, AS PER PLAN	13
					8					626	00102			8	EACH	BARRIER REFLECTOR, TYPE 1, ONE WAY	
					4					626	00110			4	EACH	BARRIER REFLECTOR, TYPE 2, ONE WAY	
				0.66						642	00100			0.66	MILE	EDGE LINE, 4", TYPE 1	
				3.82						642	00104			3.82	MILE	EDGE LINE, 6", TYPE 1	
				0.66						642	00200			0.66	MILE	LANE LINE, 4", TYPE 1	
				3.92						642	00204			3.92	MILE	LANE LINE, 6", TYPE 1	
				0.33						642	00300			0.33	MILE	CENTER LINE, TYPE 1	
				2966						642	00404			2966	FT	CHANNELIZING LINE, 12", TYPE 1	
				36						642	00500			36	FT	STOP LINE, TYPE 1	
				449						642	00600			449	FT	CROSSWALK LINE, TYPE 1	
				161						642	00700			161	FT	TRANSVERSE/DIAGONAL LINE, TYPE 1	
				5320						642	01510			5320	FT	DOTTED LINE, 6", TYPE 1	
				18						642	19000			18	EACH	SHARED LANE MARKING, TYPE 1	
STRUCTURE REPAIR (CUY-008-0127, SFN 1801201 - LOCATION 1)																	
					LS					202	11202			LS		PORIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN	
					462					513	21599			462	LB	STRUCTURAL STEEL FOR REHABILITATION	
					LS					514	00100			LS		SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL	
					LS					514	00200			LS		FIELD PAINTING OF EXISTING STRUCTURAL STEEL, PRIME COAT	
					LS					514	00300			LS		FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT	
					LS					514	00401			LS		FIELD PAINTING STRUCTURAL STEEL, FINISH COAT, AS PER PLAN	47
					2622					SPECIAL	51900100			2622	SF	COMPOSITE FIBER WRAP SYSTEM	47
STRUCTURE REPAIR (CUY-042-1457, SFN 1803271 - LOCATION 2)																	
					LS					202	11203			LS		PORIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN	53
					500					SPECIAL	20270110			500	FT	PIPE CLEANOUT, 24" AND UNDER	53
					400					202	75267			400	FT	VANDAL PROTECTION FENCE REMOVED AND RESET, AS PER PLAN	53
					9948					509	10000			9948	LB	EPOXY COATED REINFORCING STEEL	
					37					511	34410			37	CY	CLASS QC2 CONCRETE, SUPERSTRUCTURE	
					36					511	34417			36	CY	CLASS QC SCC CONCRETE, SUPERSTRUCTURE, AS PER PLAN (WITH STEEL FIBERS)	53
					9838					511	34423			9838	SF	CLASS QC2 CONCRETE, SIDEWALK WEARING SURFACE, AS PER PLAN	53
					1441					512	10101			1441	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE), AS PER PLAN	53
					1398					512	74001			1398	SY	REMOVAL OF EXISTING COATING FROM CONCRETE SURFACE, AS PER PLAN	53
					140					516	01301			140	FT	ELASTOMERIC STRIP SEAL WITHOUT STEEL EXTRUSIONS, AS PER PLAN (SEAL REPLACEMENT PIERS 4 & 14 JOINTS)	53
					149					516	11210			149	FT	STRUCTURAL EXPANSION JOINT INCLUDING ELASTOMERIC STRIP SEAL	
					140					SPECIAL	51612400			140	FT	MODULAR EXPANSION JOINT (SEAL REPLACEMENT PIERS 7 & 10 JOINTS)	53
					200					518	51101			200	FT	8" PIPE DOWNSPOUT, INCLUDING SPECIALS, AS PER PLAN	54
					2					518	51300			2	EACH	DOWNSPOUT MODIFICATION, 8"	54
					1800					518	62100			1800	FT	STRUCTURE DRAINAGE, MISC.: FLUSHING AND CLEANING OF THE DRAINAGE SYSTEM INCLUDING SCUPPERS AND DOWNSPOUTS	54
					413					519	11101			413	SF	PATCHING CONCRETE STRUCTURE, AS PER PLAN	54
					8					519	12300			8	SY	PATCHING CONCRETE BRIDGE DECK - TYPE B	54
					17792					SPECIAL	53000600			17792	SF	STRUCTURES: TIMBER SUBDECK	54

GENERAL SUMMARY	CALCULATED
	TJF CHECKED ALP

CUY-071-16.40/VAR REPAIR
PID NO. 111603

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LEGEND

* A QUANTITY OF 280 C.Y. OF DUMPED ROCK FILL HAS BEEN INCLUDED IN THE ESTIMATED QUANTITIES FOR ITEM 601 - DUMPED ROCK FILL, TYPE D, TO BE PLACED AT THE DIRECTION OF THE ENGINEER.

NOTES:

1. DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.
2. PERFORM ONLY THE WORK AS INDICATED IN THE STRUCTURE DATA SHEET, FRAMED TEXT, AND/OR DESCRIBED IN THE GENERAL NOTES.
3. FOR ESTIMATED QUANTITIES SEE SHEET 3/33.

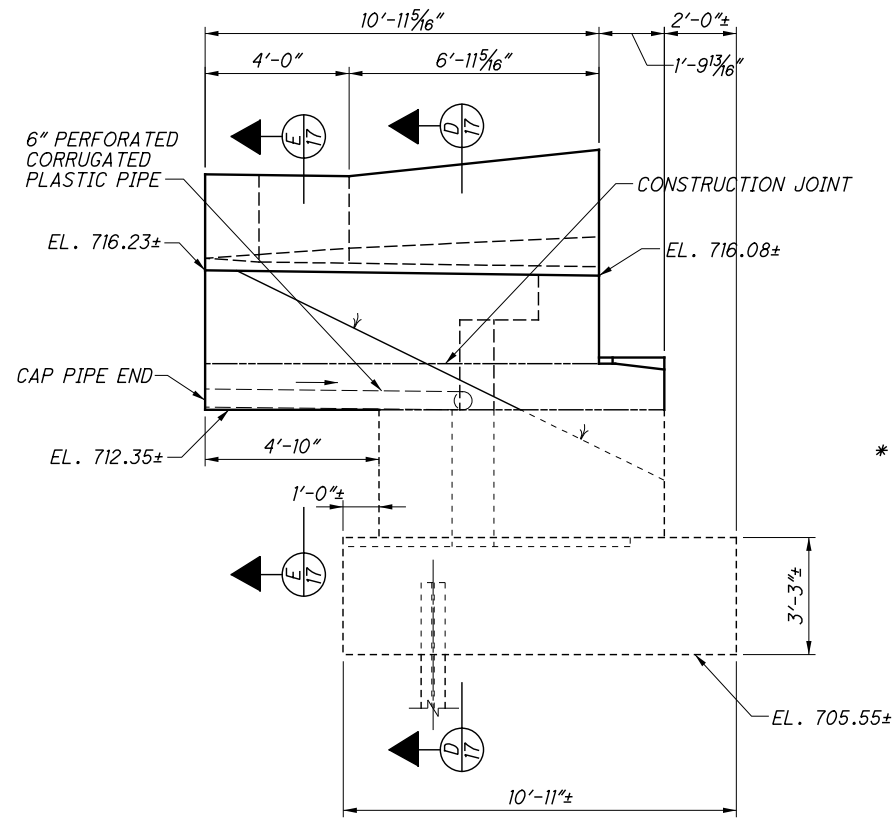
ESTIMATED QUANTITIES

CALCULATED DLR DATED 07/21
 CHECKED dht DATED 07/21

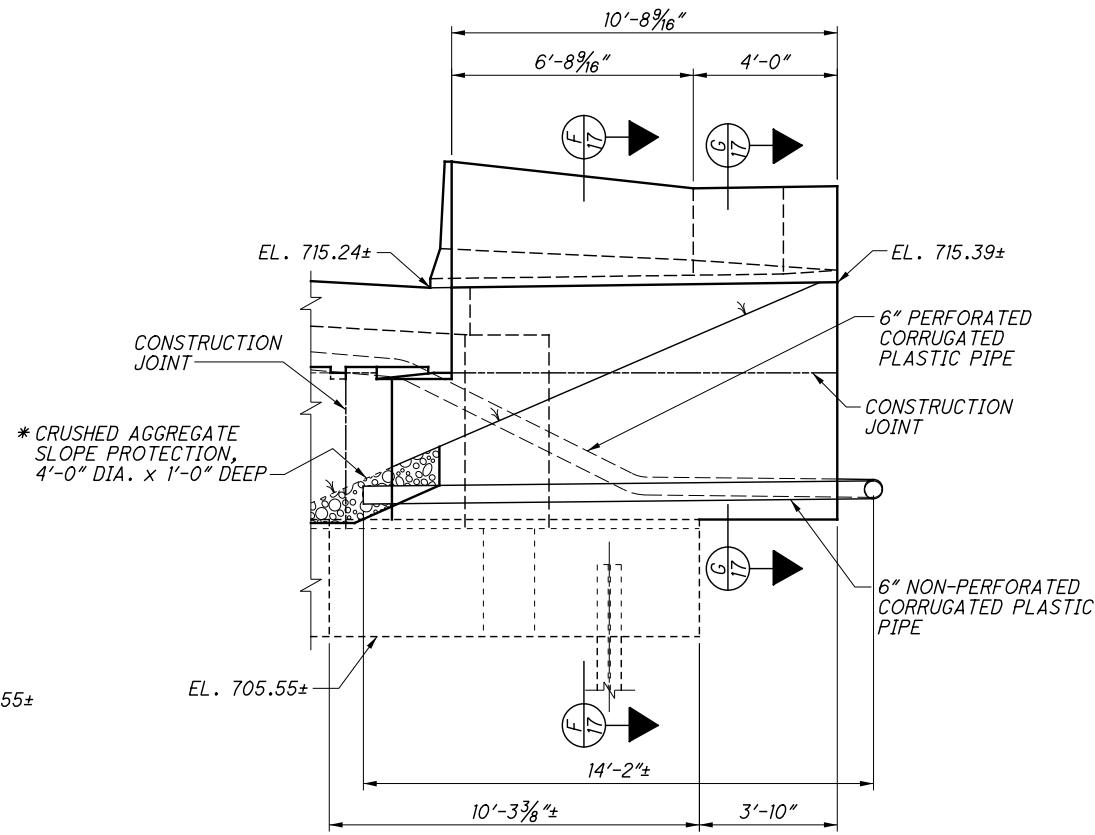
ITEM	ITEM EXT.	TOTAL	UNIT	DESCRIPTION	SUPER.	ABUTS.	GEN'L	REF. SHEET
202	11203	LS		PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN			LS	2/33
202	22900	806	SY	APPROACH SLAB REMOVED			806	
202	23500	392	SY	WEARING COURSE REMOVED			392	
503	21300	LS		UNCLASSIFIED EXCAVATION			LS	
509	10000	21,137	LB	EPOXY COATED REINFORCING STEEL	8831	12,306		
510	10000	1152	EACH	DOWEL HOLES WITH NON-SHRINK, NON-METALLIC GROUT	836	316		
511	34410	9	CY	CLASS QC2 CONCRETE, SUPERSTRUCTURE	3	6		
511	34417	67	CY	CLASS QC SCC CONCRETE, SUPERSTRUCTURE, AS PER PLAN (WITH STEEL FIBERS)	25	42		2/33
511	45710	15	CY	CLASS QC1 CONCRETE, ABUTMENT		15		
512	10101	577	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE), AS PER PLAN	459	60	58	2/33
512	74001	436	SY	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES, AS PER PLAN	433	3		2/33
516	11210	319	FT	STRUCTURAL EXPANSION JOINT INCLUDING ELASTOMERIC STRIP SEAL	319			
516	13200	21	SF	1/2" PREFORMED EXPANSION JOINT FILLER	21			
516	13600	348	SF	1" PREFORMED EXPANSION JOINT FILLER	348			
516	13900	19	SF	2" PREFORMED EXPANSION JOINT FILLER	19			
516	31010	41	FT	2" DEEP JOINT SEALER	41			
518	21200	40	CY	POROUS BACKFILL WITH GEOTEXTILE FABRIC		40		
518	40000	345	FT	6" PERFORATED CORRUGATED PLASTIC PIPE		345		
518	40010	39	FT	6" NON-PERFORATED CORRUGATED PLASTIC PIPE, INCLUDING SPECIALS		39		
526	25011	800	SY	REINFORCED CONCRETE APPROACH SLABS WITH QC/QA (T=15"), AS PER PLAN			800	2/33
601	28000	280	CY	DUMPED ROCK FILL, TYPE D			280	

3 / 33 93 123	ESTIMATED QUANTITIES - LOCATION 4 BRIDGE NO. CUY-71-1640 IR 71 OVER NORFOLK SOUTHERN RAILWAY	CUY-071-16.40 VAR REPAIR PID No. 111603	RICHLAND ENGINEERING LIMITED 29 NORTH PARK STREET MANSFIELD, OHIO 44902
DESIGNED BLN	DRAWN JLS	REVIEWED DATE	STRUCTURE FILE NUMBER 1805223
CHECKED dht	REVISED		

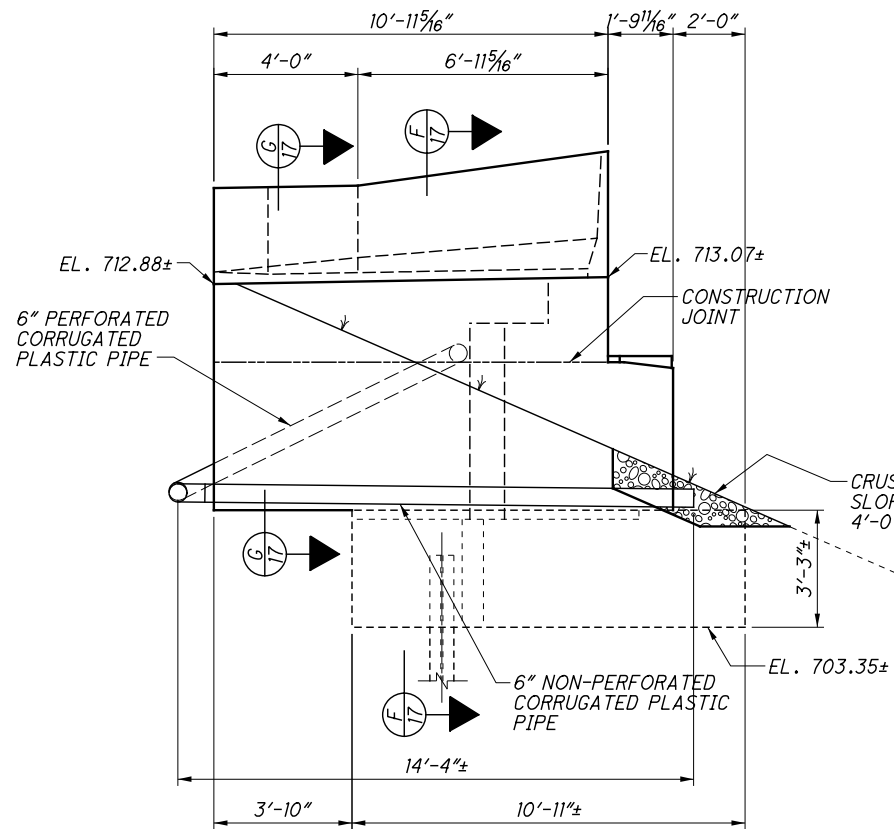
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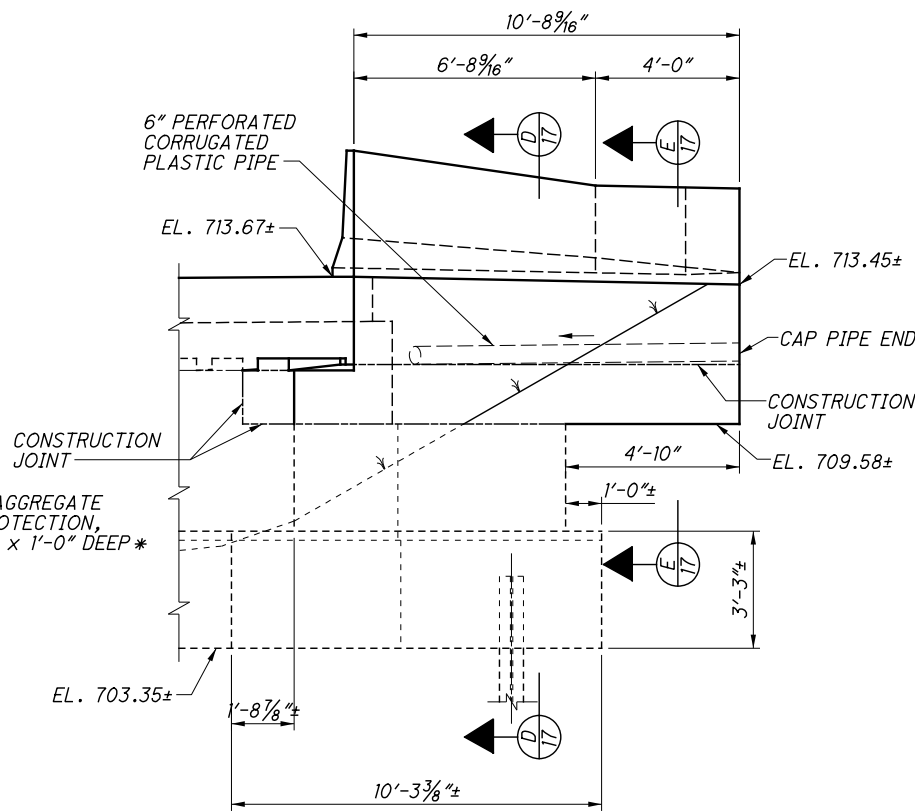
REAR ABUTMENT - RIGHT WINGWALL



REAR ABUTMENT - LEFT WINGWALL



FORWARD ABUTMENT - LEFT WINGWALL



FORWARD ABUTMENT - RIGHT WINGWALL

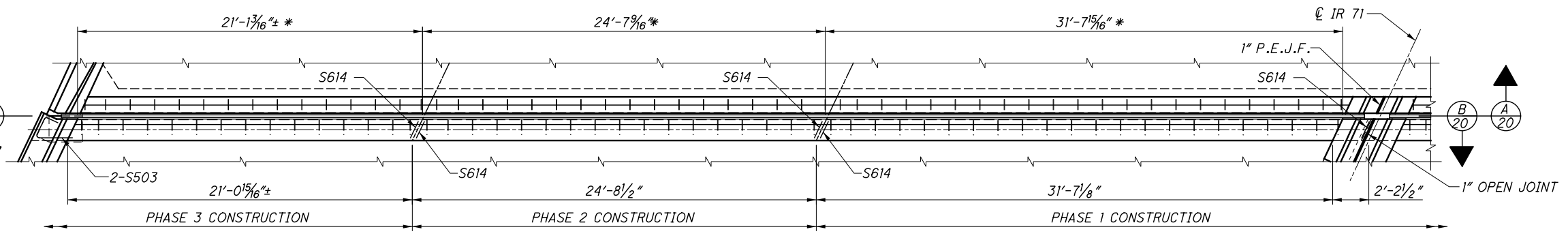
LEGEND

* INCLUDED WITH ITEM 518 - 6" NON-PERFORATED CORRUGATED PLASTIC PIPE, INCLUDING SPECIALS FOR PAYMENT.

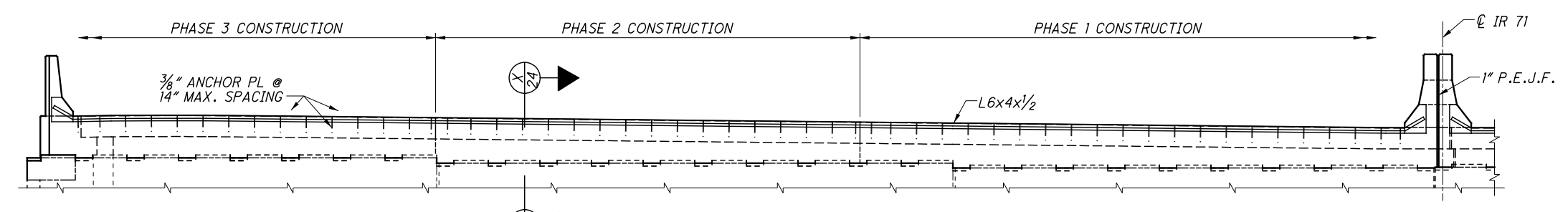
NOTES

ELEVATIONS SHOWN ARE BASED ON ORIGINAL AND REHAB PLANS AND ARE FOR INFORMATIONAL PURPOSES ONLY. CONTRACTOR SHALL MATCH EXISTING.

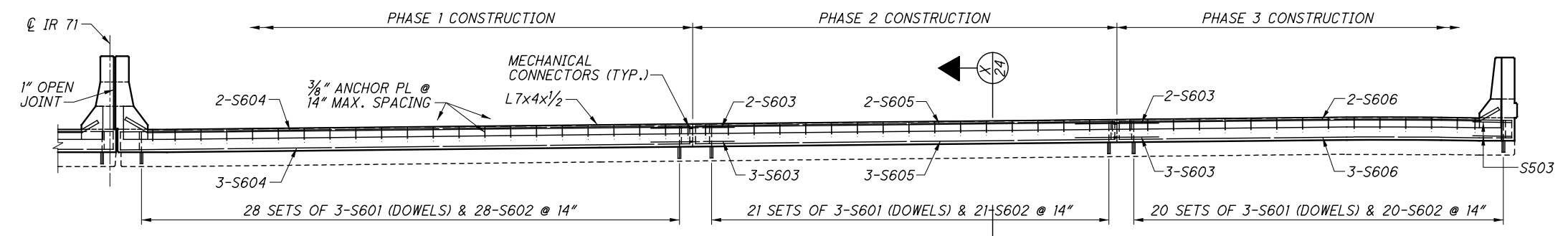
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REAR ABUTMENT JOINT PLAN - RIGHT BRIDGE



SECTION A-A



SECTION B-B

LEGEND

* MEASURED ALONG EDGE OF DECK JOINT

NOTES

NOTATION: P.E.J.F. - PREFORMED EXPANSION JOINT FILLER.

REINFORCING STEEL SPLICE LENGTH SHALL BE 3'-4" FOR HORIZONTAL #6 BARS.

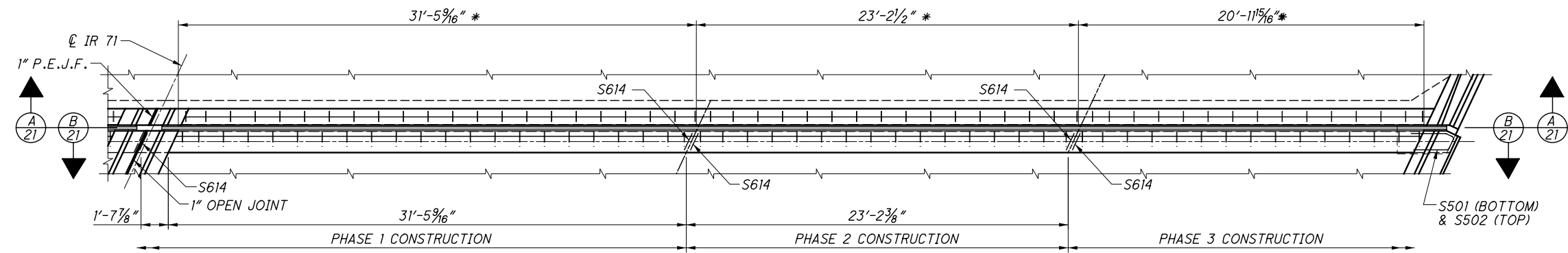
ABUTMENT REINFORCING: SEE SHEET [12/33].

PARAPET & MEDIAN BARRIER DETAILS & REINFORCING: SEE SHEETS [25/33] & [26/33].

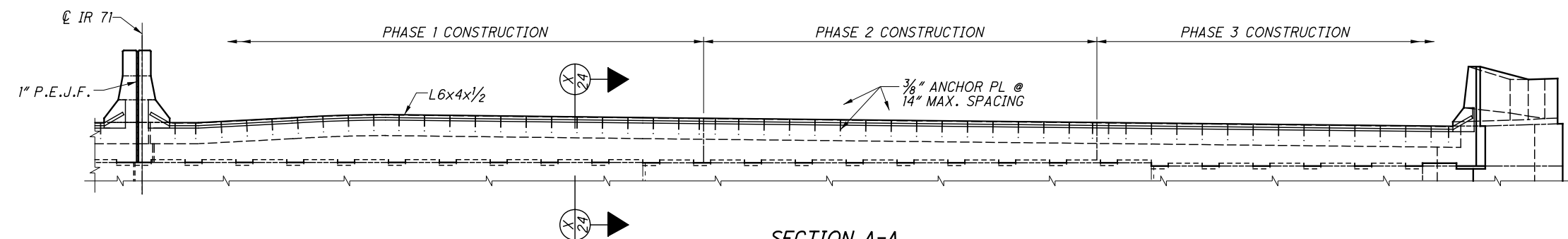
ADDITIONAL NOTES & DETAILS: SEE STANDARD DRAWING EXJ-5-93.

		RICHLAND ENGINEERING LIMITED 29 NORTH PARK STREET MANSFIELD, OHIO 44902
REVIEWED DLR	DATE 06/2021	STRUCTURE FILE NUMBER 1805223
DRAWN JLS	REVISIONS	
DESIGNED BLN	CHECKED dnt	BRIDGE NO. CUY-71-1640 IR 71 OVER NORFOLK SOUTHERN RAILWAY
RIGHT BRIDGE REAR ABUTMENT JOINT - LOCATION 4		
CUY-071-16.40 VAR REPAIR PID No. 111603		
20/33		110 123

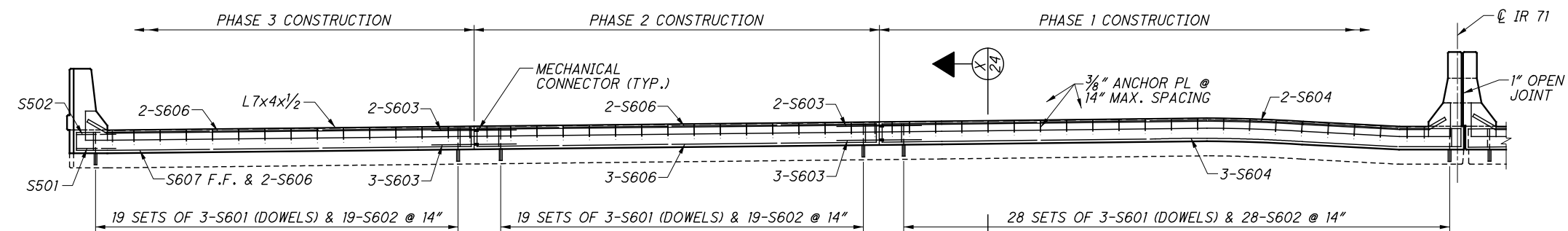
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REAR ABUTMENT JOINT PLAN - LEFT BRIDGE



SECTION A-A



SECTION B-B

LEGEND

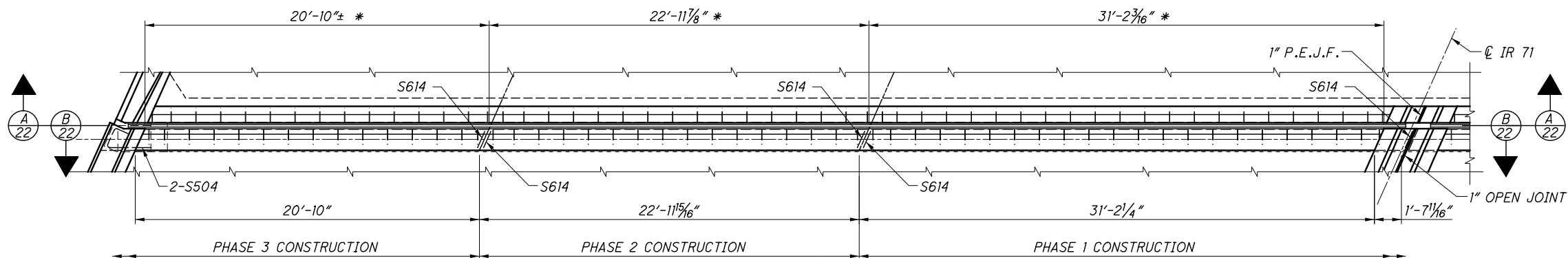
* MEASURED ALONG EDGE OF DECK JOINT

NOTES

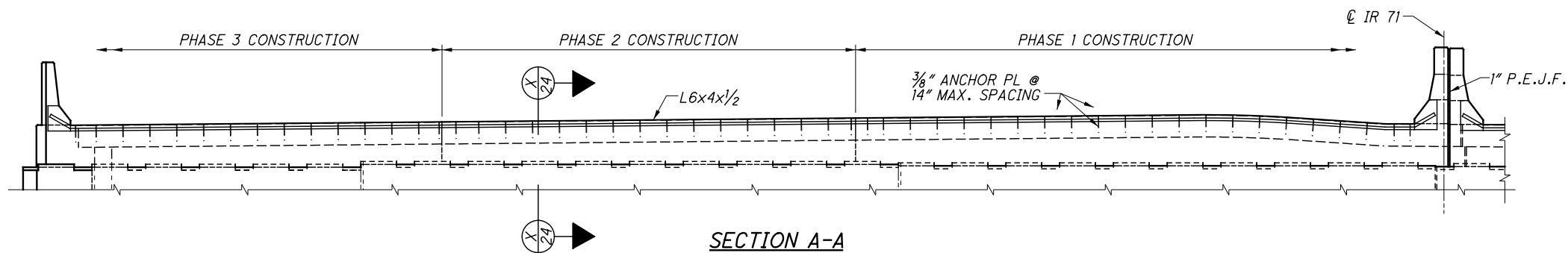
- ABUTMENT REINFORCING:** SEE SHEET [13/33].
- PARAPET & MEDIAN BARRIER DETAILS & REINFORCING:** SEE SHEETS [25/33] & [26/33].
- ADDITIONAL NOTES:** SEE SHEET [20/33].

<p>RICHLAND ENGINEERING LIMITED 29 NORTH PARK STREET MANSFIELD, OHIO 44902</p>	<p>DATE: 06/2021 REVIEWED DLR: STRUCTURE FILE NUMBER 1805223</p>	<p>LEFT BRIDGE REAR ABUTMENT JOINT - LOCATION 4 BRIDGE NO. CUY-71-1640 IR 71 OVER NORFOLK SOUTHERN RAILWAY</p>
<p>DESIGNED: BLN CHECKED: dnt</p>	<p>DRAWN: JLS REVISED:</p>	<p>PID No. 111603</p>
<p>CUY-071-16.40 VAR REPAIR</p>		<p>21 / 33</p>
<p>111</p>		<p>123</p>

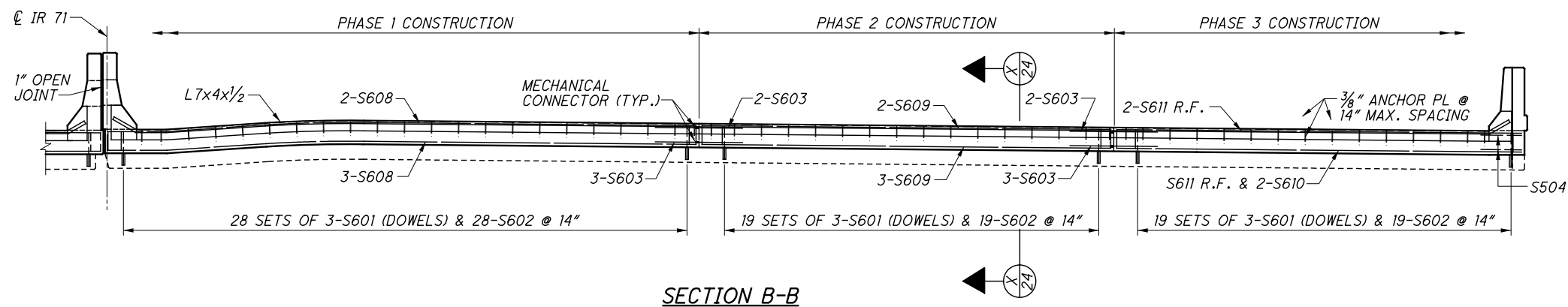
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FORWARD ABUTMENT JOINT PLAN - LEFT BRIDGE



SECTION A-A



SECTION B-B

LEGEND

* MEASURED ALONG EDGE OF DECK JOINT

NOTES

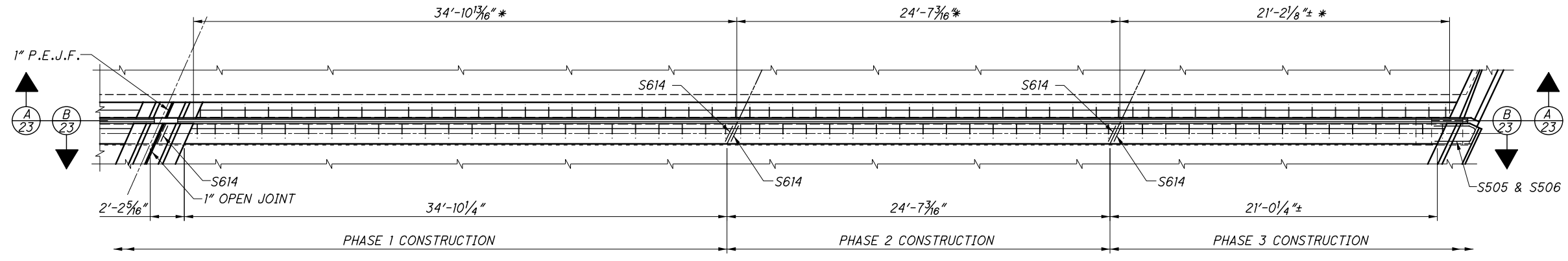
ABUTMENT REINFORCING: SEE SHEET [14/33].

PARAPET & MEDIAN BARRIER DETAILS & REINFORCING: SEE SHEET [25/33] & [26/33].

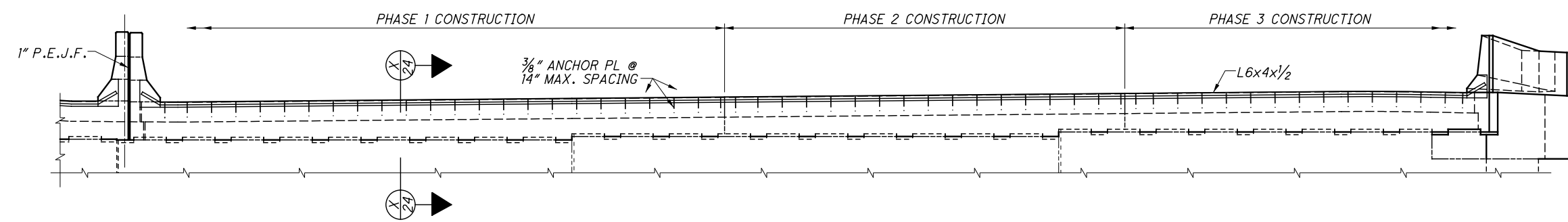
ADDITIONAL NOTES: SEE SHEET [20/33].

RICHLAND ENGINEERING LIMITED 29 NORTH PARK STREET MANSFIELD, OHIO 44902	
DATE 06/2021	STRUCTURE FILE NUMBER 1805223
REVIEWED DLR	CHECKED dnt
DRAWN JLS	REVISIONS
LEFT BRIDGE FORWARD ABUTMENT JOINT - LOCATION 4 BRIDGE NO. CUY-71-1640 IR 71 OVER NORFOLK SOUTHERN RAILWAY	
CUY-071-16.40 VAR REPAIR PID No. 111603	
22 / 33	
112 123	

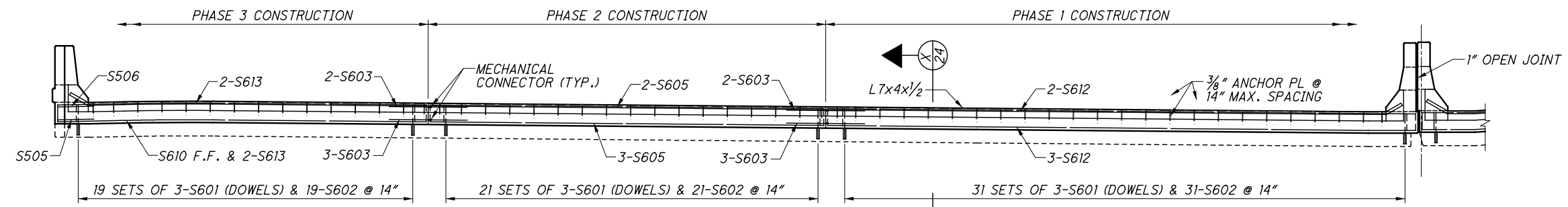
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FORWARD ABUTMENT JOINT PLAN - RIGHT BRIDGE



SECTION A-A



SECTION B-B

LEGEND

* MEASURED ALONG EDGE OF DECK JOINT

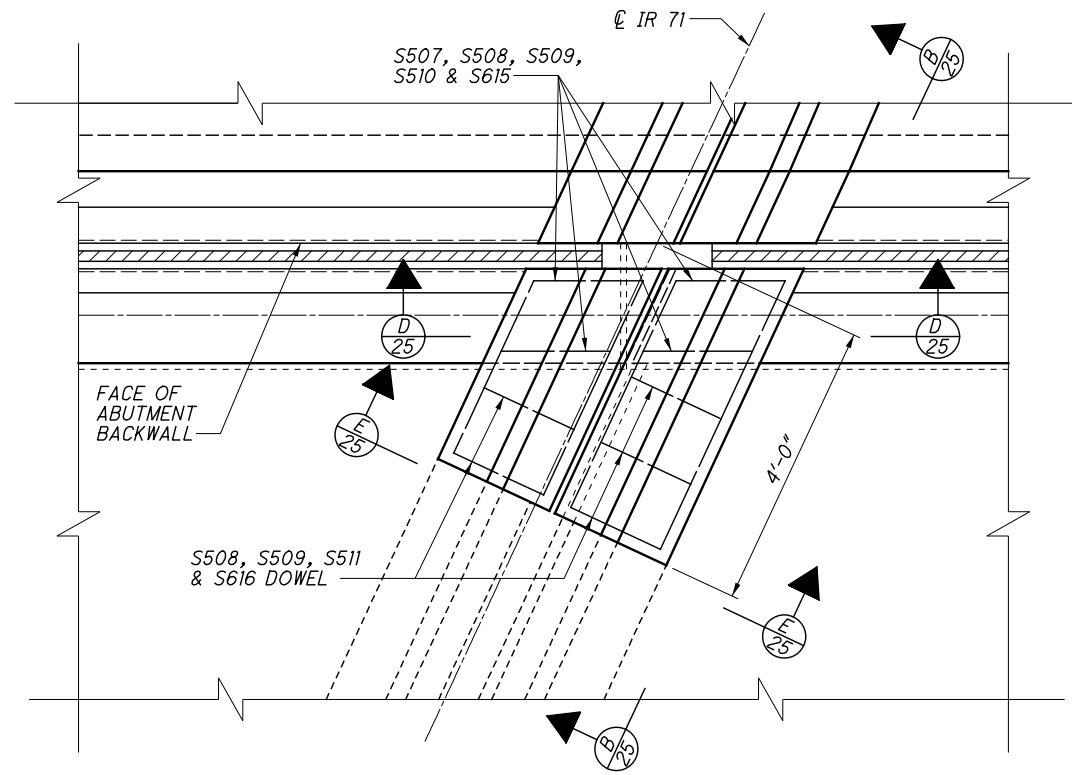
NOTES

ABUTMENT REINFORCING: SEE SHEET [15/33].

PARAPET & MEDIAN BARRIER DETAILS & REINFORCING: SEE SHEET [25/33] & [26/33].

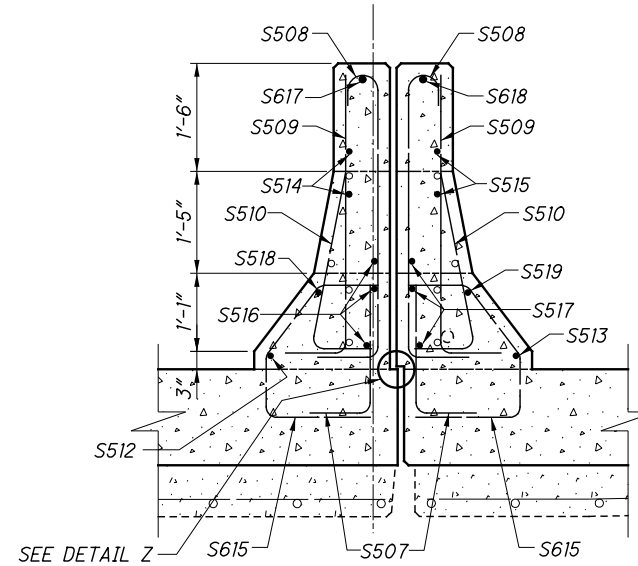
ADDITIONAL NOTES: SEE SHEET [20/33].

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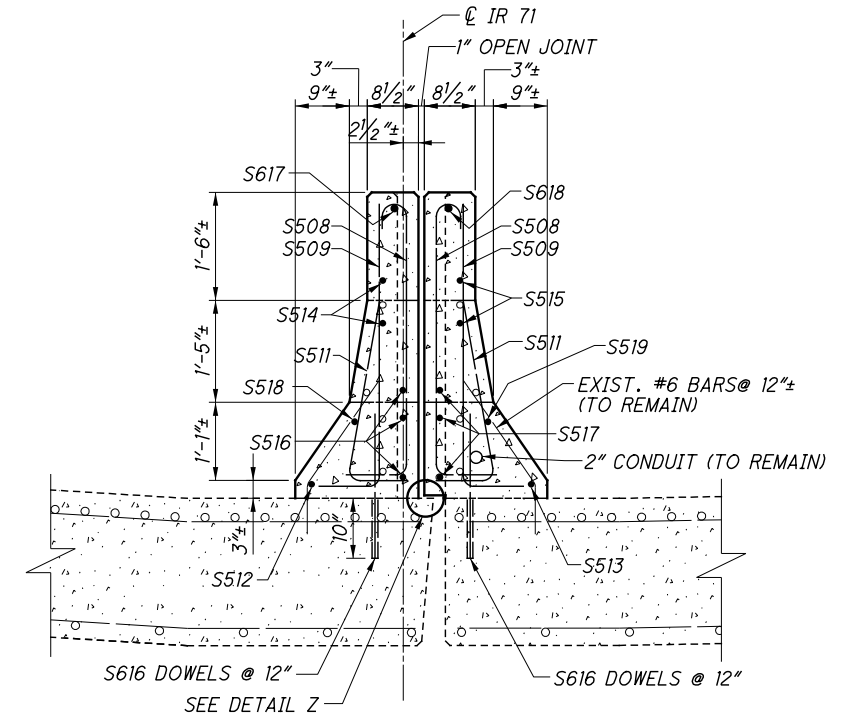
MEDIAN BARRIER PLAN AT ABUTMENT JOINTS

FORWARD ABUTMENT JOINT - SHOWN
REAR ABUTMENT JOINT - SIMILAR



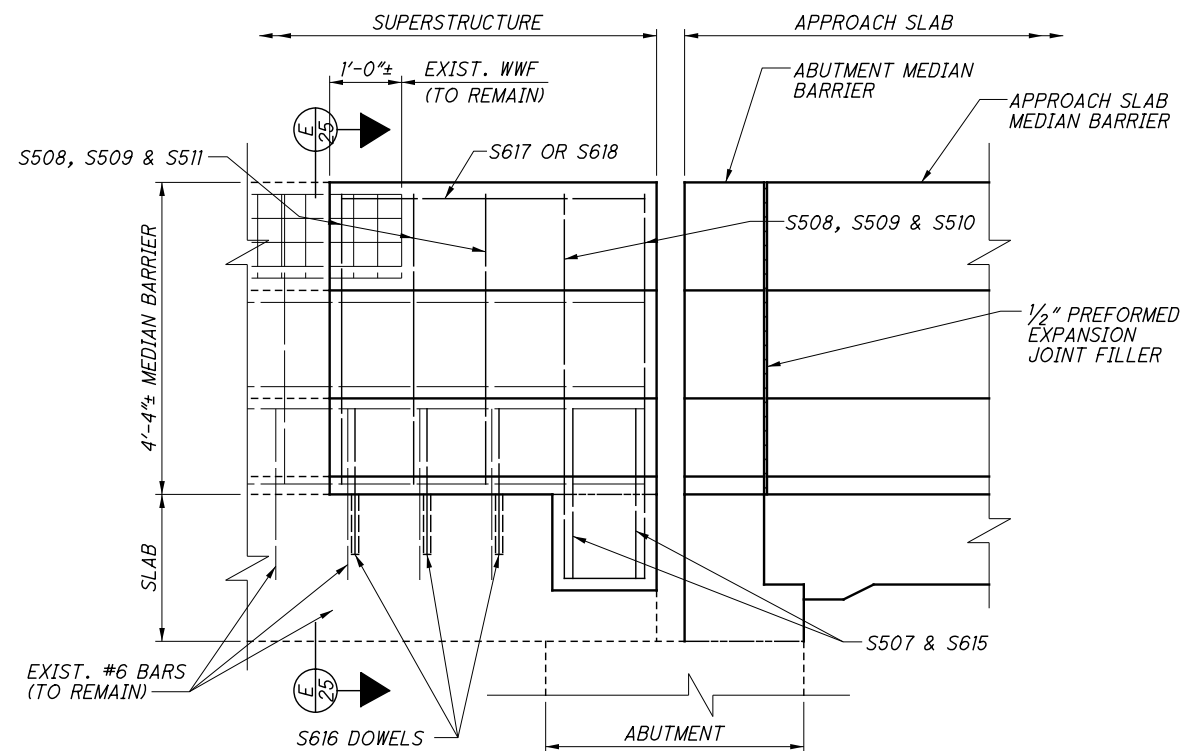
SECTION D-D

EXISTING HORIZONTAL RAILING BARS ARE #5 BARS TO REMAIN. ROTATE S508 BARS AS NECESSARY TO FIT.

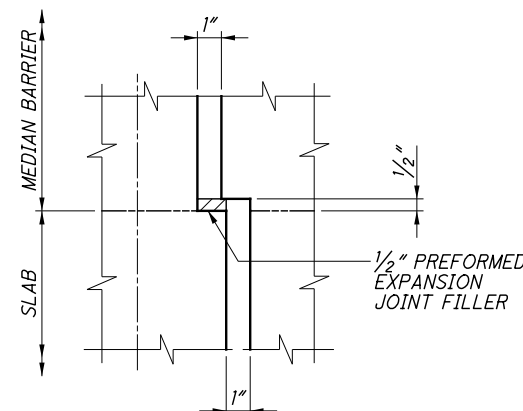


SECTION E-E

EXISTING HORIZONTAL RAILING BARS ARE #5 BARS TO REMAIN. ROTATE S508 BARS AS NECESSARY TO FIT.



SECTION B-B



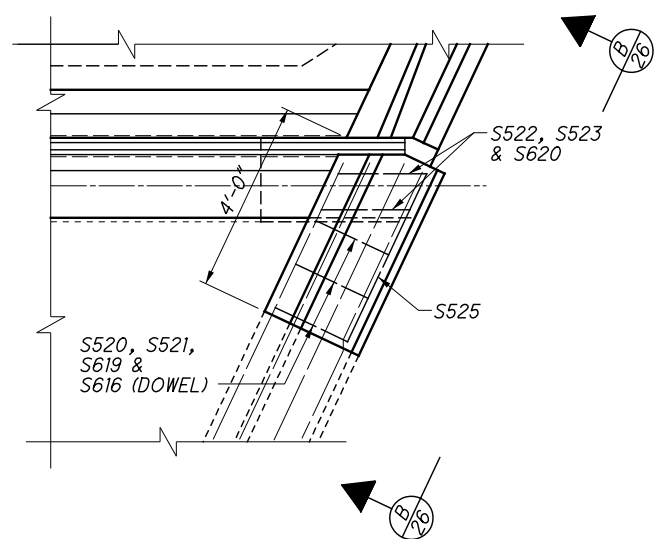
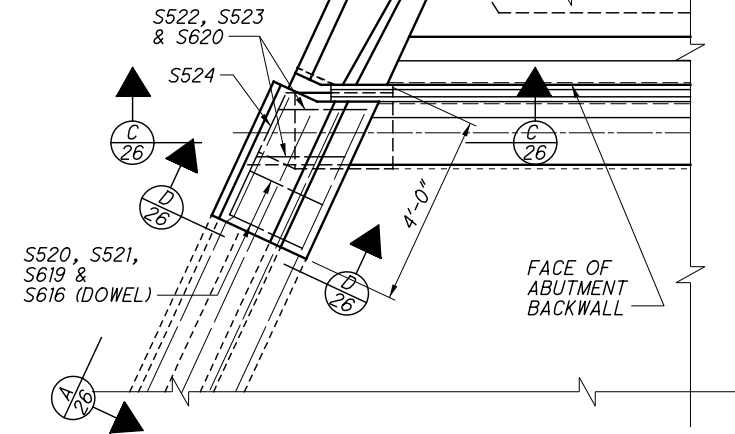
DETAIL Z

NOTES

MEDIAN BARRIER CONCRETE ON THE SUPERSTRUCTURE SHALL BE ITEM 511 - CLASS QC SCC CONCRETE, SUPERSTRUCTURE, AS PER PLAN (WITH STEEL FIBERS).

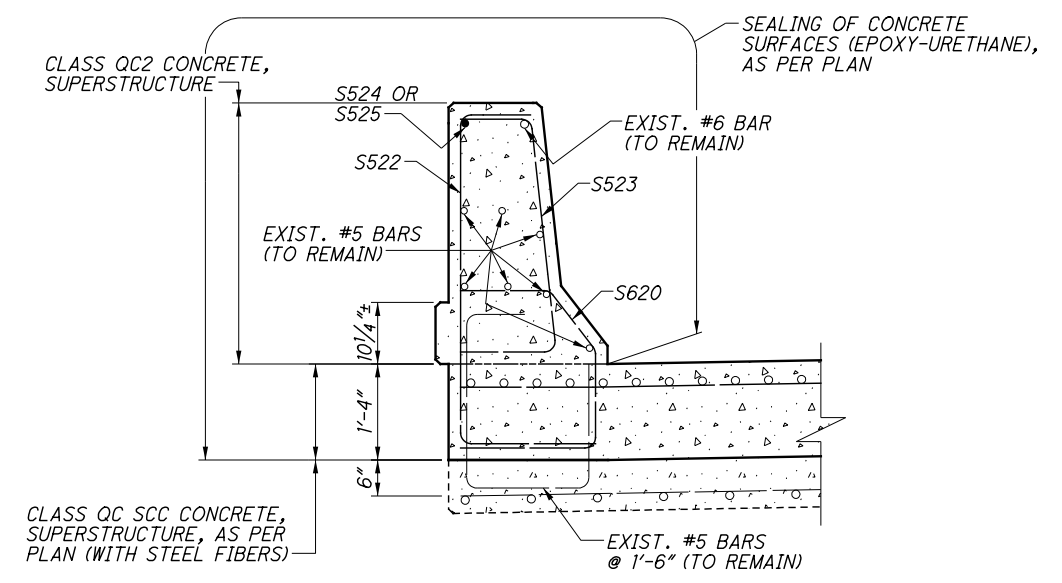
RICHLAND ENGINEERING LIMITED 29 NORTH PARK STREET MANSFIELD, OHIO 44902	
DATE 06/2021	STRUCTURE FILE NUMBER 1805223
REVIEWED DLR	CHECKED dnt
DRAWN JLS	REVISED
MEDIAN BARRIER DETAILS - LOCATION 4 BRIDGE NO. CUY-71-1640 IR 71 OVER NORFOLK SOUTHERN RAILWAY	
CUY-071-16.40 VAR REPAIR PID No. 111603	
25 / 33	
115 123	

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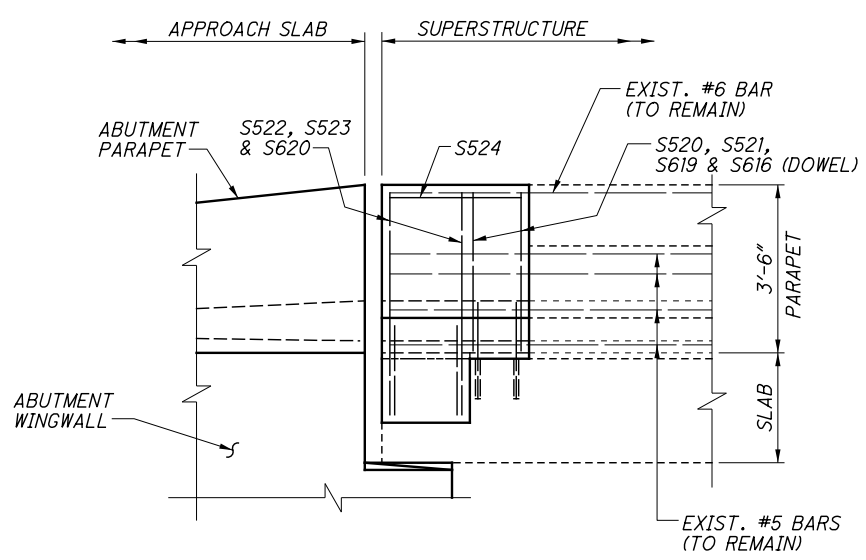


TYPICAL PARAPET AT ABUTMENT JOINTS

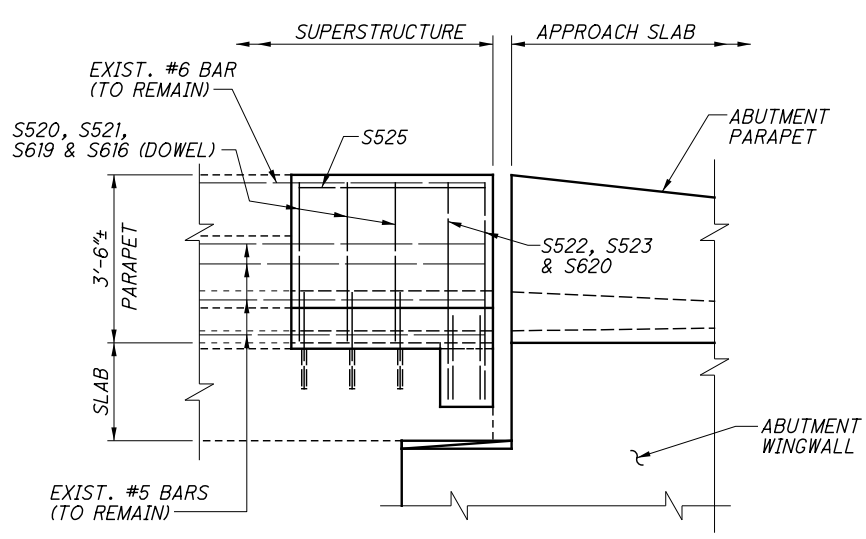
FORWARD ABUTMENT JOINT - SHOWN
REAR ABUTMENT JOINT - SIMILAR



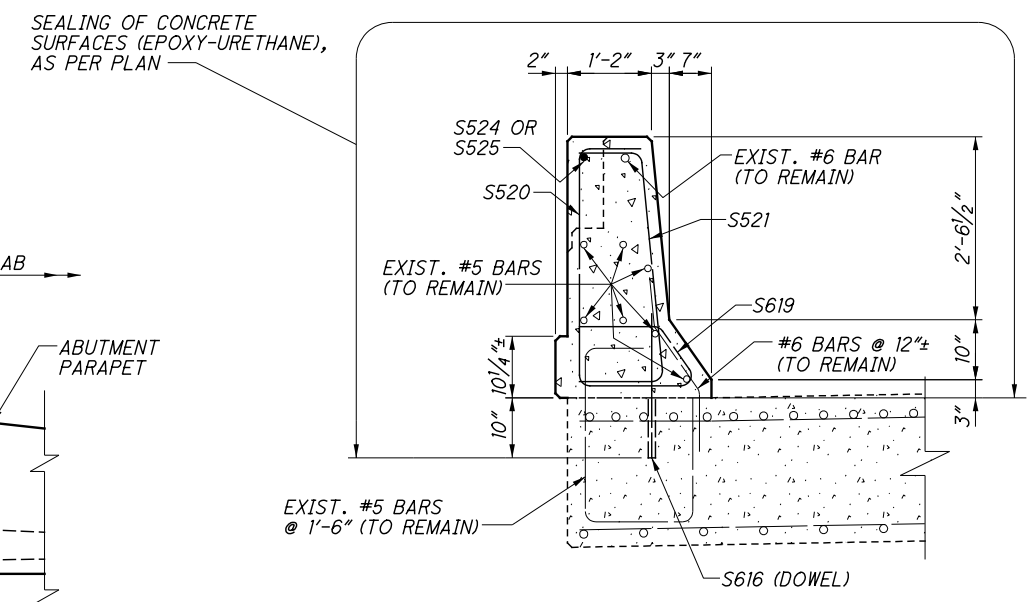
SECTION C-C



VIEW A-A



VIEW B-B



SECTION D-D

RICHLAND ENGINEERING LIMITED 29 NORTH PARK STREET MANSFIELD, OHIO 44902	
DATE 06/2021	STRUCTURE FILE NUMBER 1805223
REVIEWED DLR	STRUCTURE FILE NUMBER 1805223
DRAWN JLS	REVISED
DESIGNED BLN	CHECKED dnt
PARAPET DETAILS - LOCATION 4 BRIDGE NO. CUY-71-1640 IR 71 OVER NORFOLK SOUTHERN RAILWAY	
CUY-071-16.40 VAR REPAIR PID No. 111603	
26 / 33	
116 123	