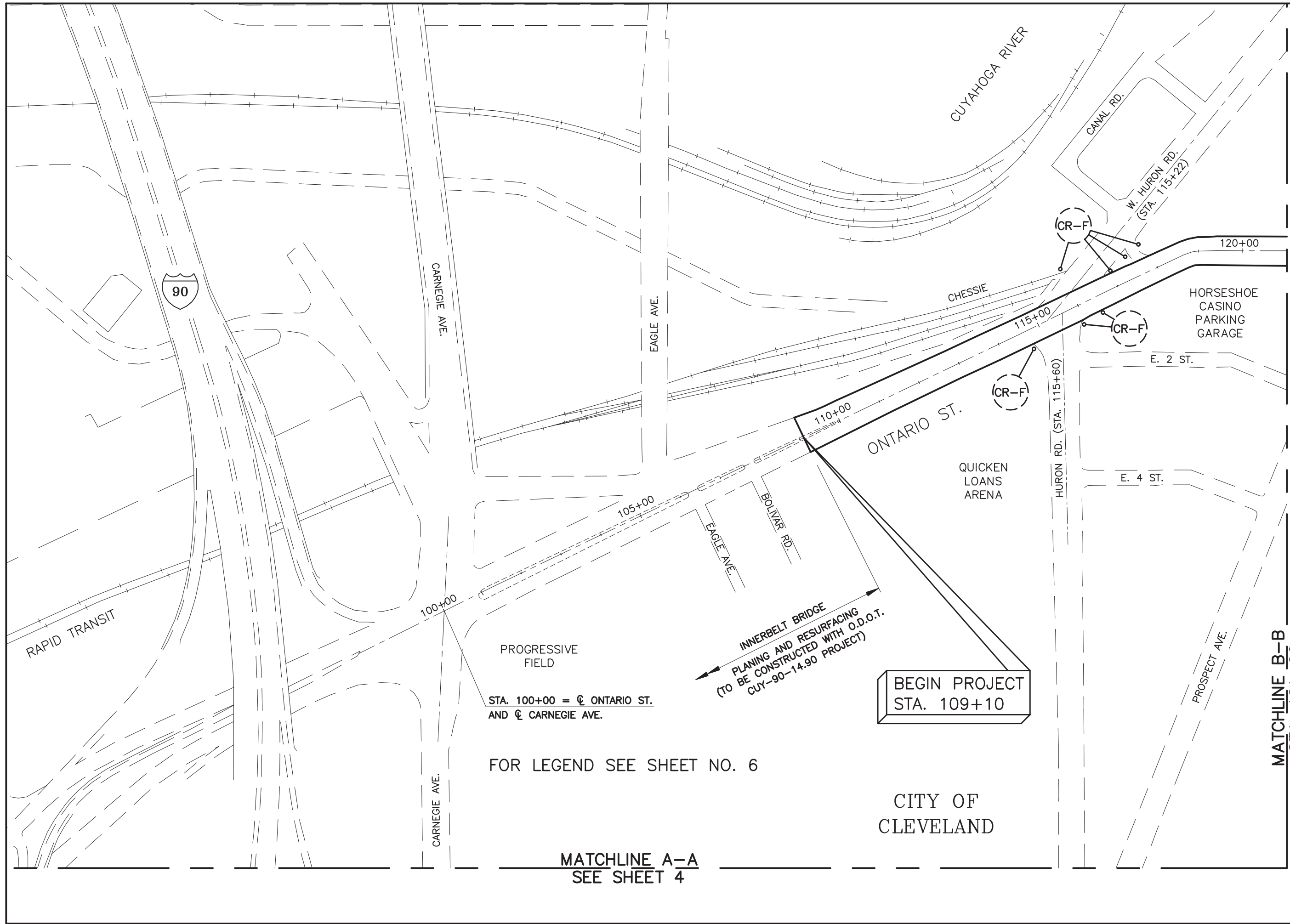
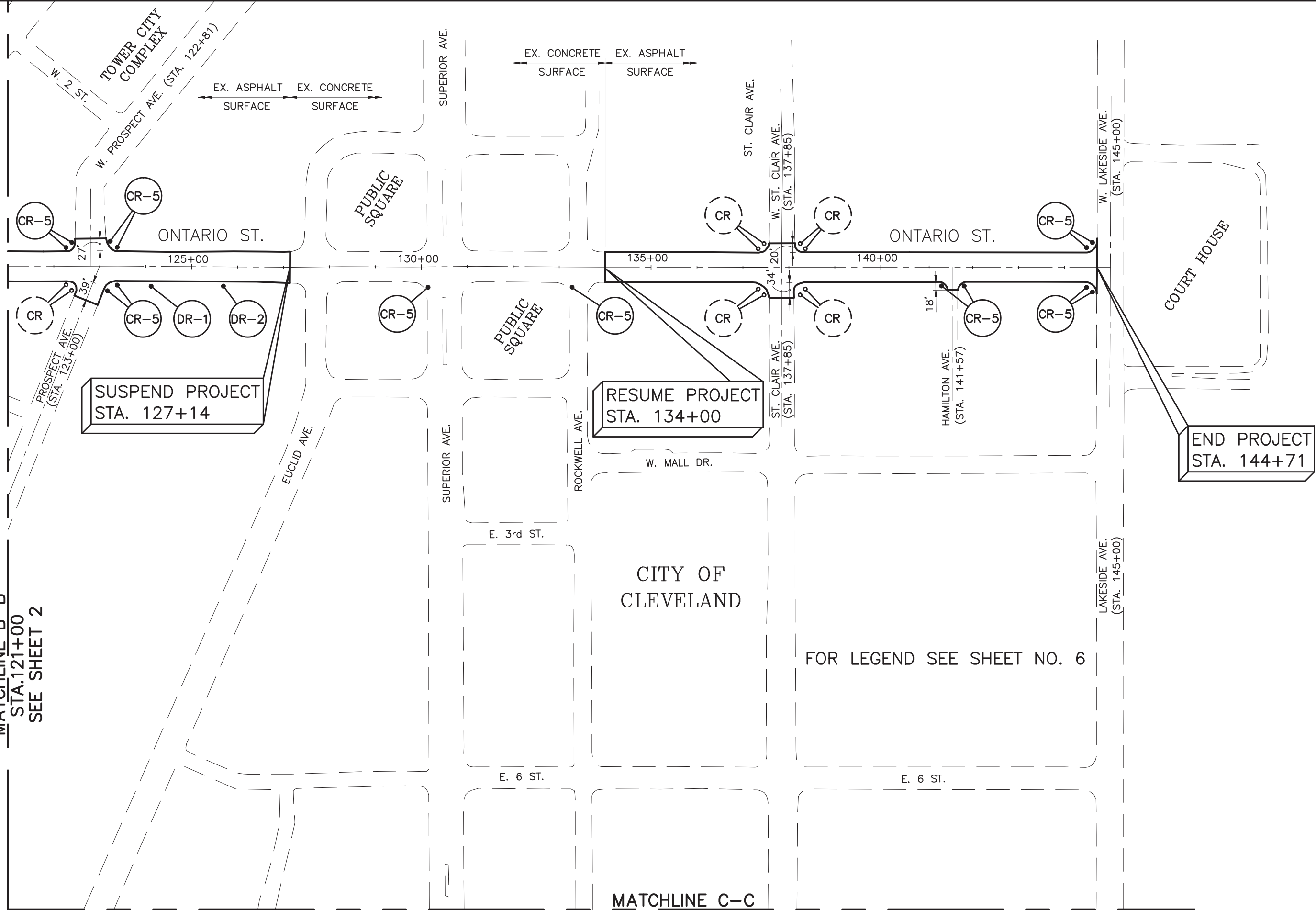


DRAWING NAME: C:\Oper's by Year\Oper 2013\Group-2 (Ontario and East 9)\Ontario\02-06Schematic1.dwg
PLOT DATE: 10-18-12



MATCHLINE B-B
STA.121+00
SEE SHEET 2



DRAWING NAME: G:\Oper's by Year\Oper 2013\Group-2 (Ontario,East 9) Schematic\East 9th 1.dwg
PLOT DATE: 10-18-12

FOR LEGEND SEE SHEET NO. 6.

CITY OF
CLEVELAND

BEGIN PROJECT
STA. 170+32



CENTRAL AVE.

WEBSTER AVE.

GARDNER AVE.

CARNEGIE AVE.
(STA. 169+96)

BRONSON CT.

ERIE
STREET
CEMETERY

E. 14 ST.

MATCHLINE A-A
SEE SHEET 2

170+00

175+00

180+00

185+00

CR-5

CR-5

CR-5

CR-5

CR-5

CR-5

CR-3

CR-3

CR-5

CR-5

CR-5

EX. BRICK EX. ASPHALT
SURFACE

SUMNER AVE.
(STA. 173+35)

(DRIVEWAY)
EX. BRICK EX. ASPHALT
SURFACE

ERIE CT.
(STA. 177+16)

BOLIVAR RD.
(STA. 179+46)

BOLIVAR RD.
(STA. 179+53)

HURON RD.

EUCLID AVE.

HURON RD.

PROSPECT AVE.
(STA. 184+52)

PROSPECT AVE.
(STA. 184+42)

MATCHLINE D-D
STA. 186+00
SEE SHEET 5

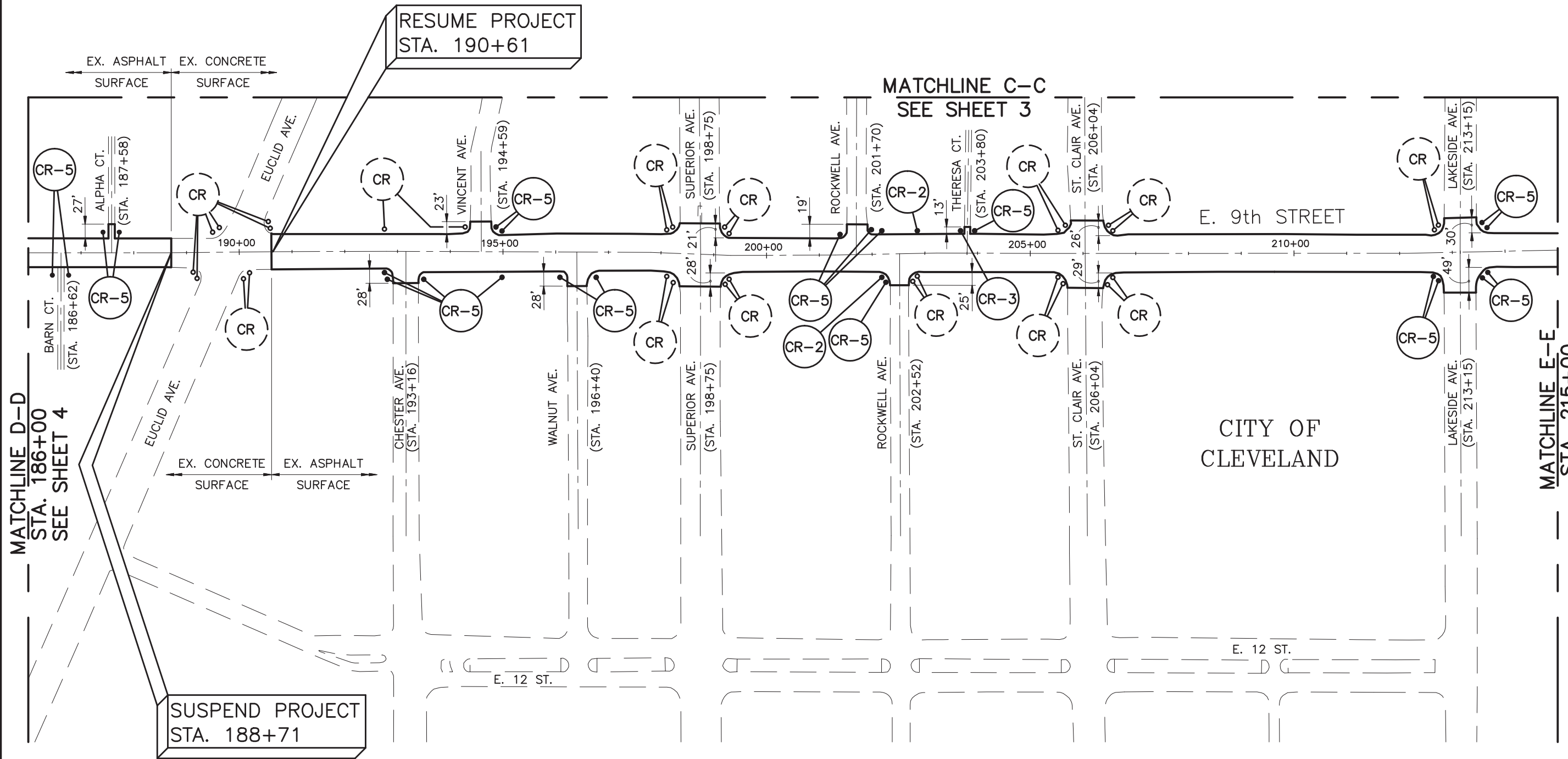
2013 OPERATIONS GROUP 2

SCHEMATIC PLAN: EAST 9th STREET

4
36



DRAWING NAME: C:\Users\by Year\Oper 2013\Group-2 (Ontario,East 9) Schematic\East 9th 2.dwg
PLOT DATE: 10-18-12



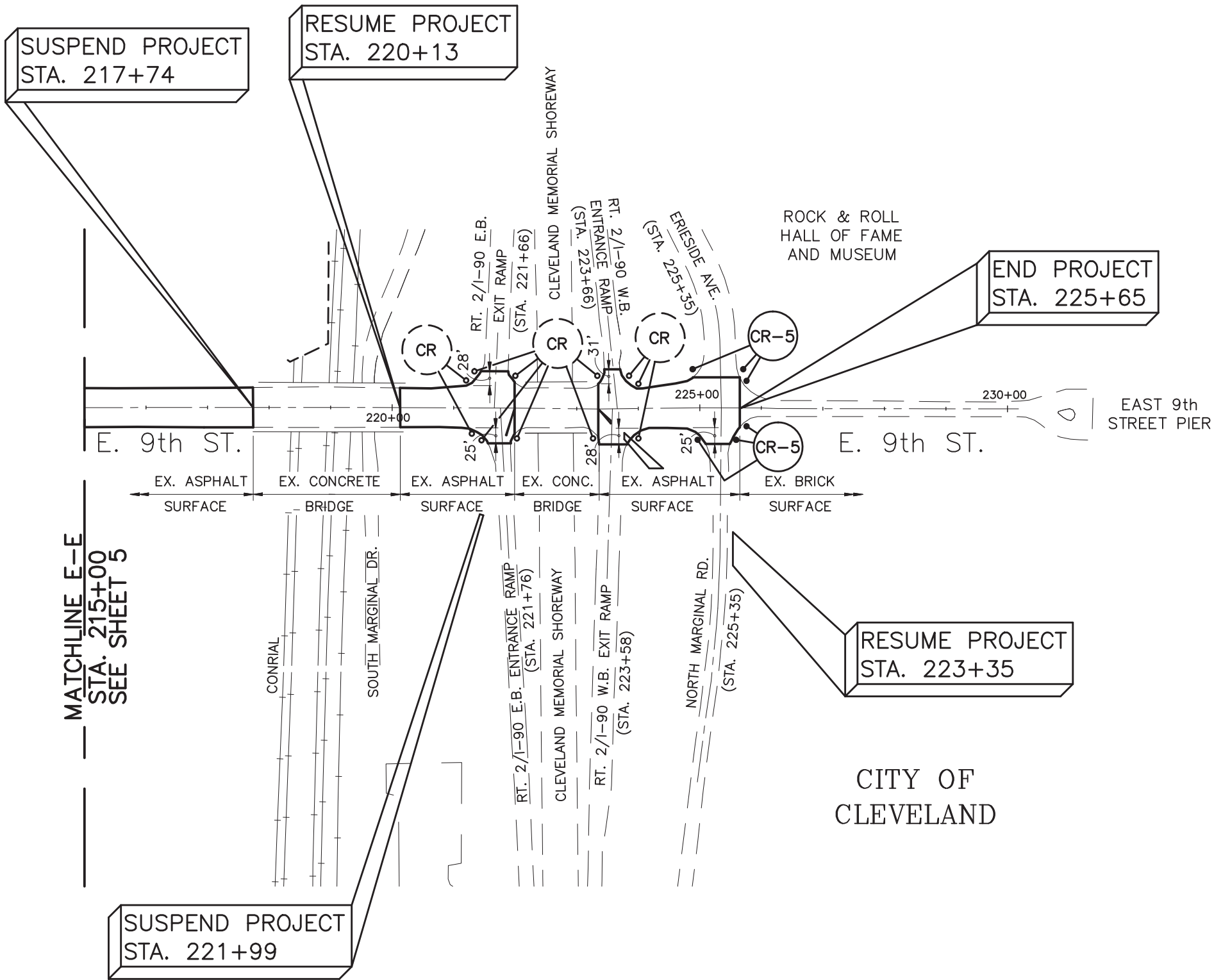
FOR LEGEND SEE SHEET NO. 6



SCHEMATIC PLAN: EAST 9th STREET

2013 OPERATIONS GROUP 2

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LEGEND

☉ OF ROAD	---
EXISTING ROAD	---
RESURFACING PORTIONS OF ROAD	---
EXISTING ADA COMPLIANT CITY OF CLEVELAND CURB RAMPS (TO REMAIN UNDISTURBED)	(CR)
ITEM 608 - CURB RAMP, MISC.: CITY OF CLEVELAND CR-2 CURB RAMP, AS PER PLAN	(CR-2)
ITEM 608 - CURB RAMP, MISC.: CITY OF CLEVELAND CR-3 CURB RAMP, AS PER PLAN	(CR-3)
ITEM 608 - CURB RAMP, MISC.: CITY OF CLEVELAND CR-5 CURB RAMP, AS PER PLAN	(CR-5)
FUTURE ADA COMPLIANT CITY OF CLEVELAND CURB RAMPS (TO BE CONSTRUCTED WITH O.D.O.T. PID 88276 PROJECT)	(CR-F)
EXISTING 72' X 17' CONCRETE DRIVE APRON (TO BE REMOVED - SEE NOTE ON SHEET 15)	(DR-1)
EXISTING 57' X 5' CONCRETE DRIVE APRON (TO BE REMOVED - SEE NOTE ON SHEET 15)	(DR-2)

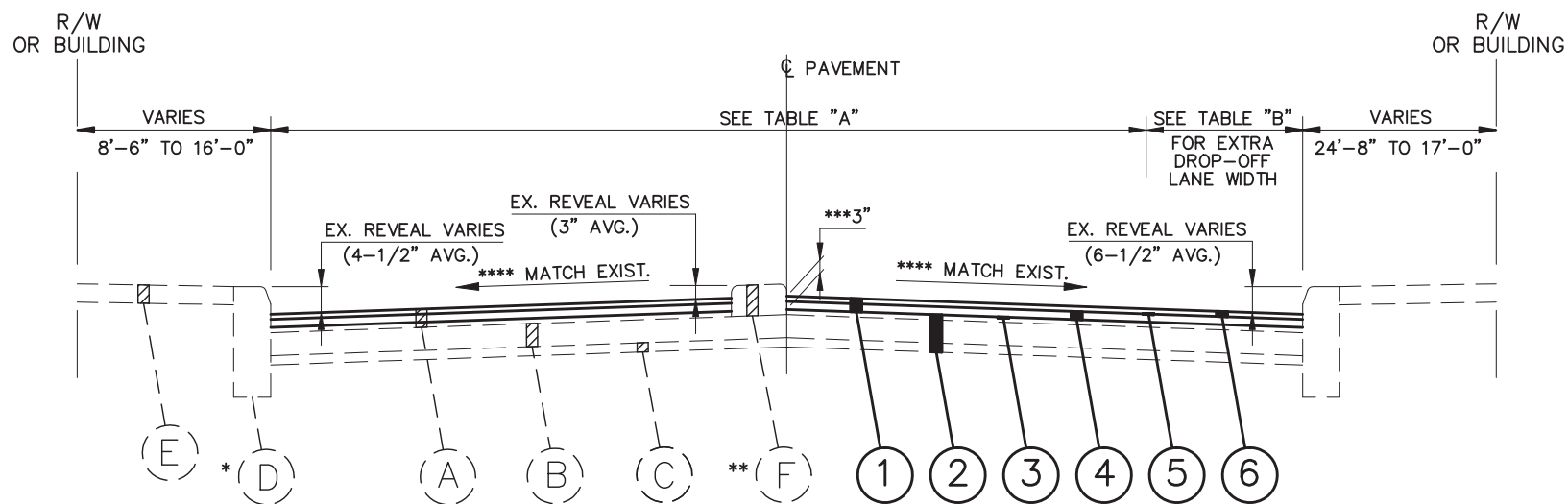


SCHEMATIC PLAN: EAST 9th STREET

2013 OPERATIONS GROUP 2



DRAWING NAME: 01-Operations by Year 1 (Oper 2013) Group 2 - Ontario and East 9th 07-Operations-Typical.dwg
PLOT DATE: 11-7-12



TYPICAL SECTION

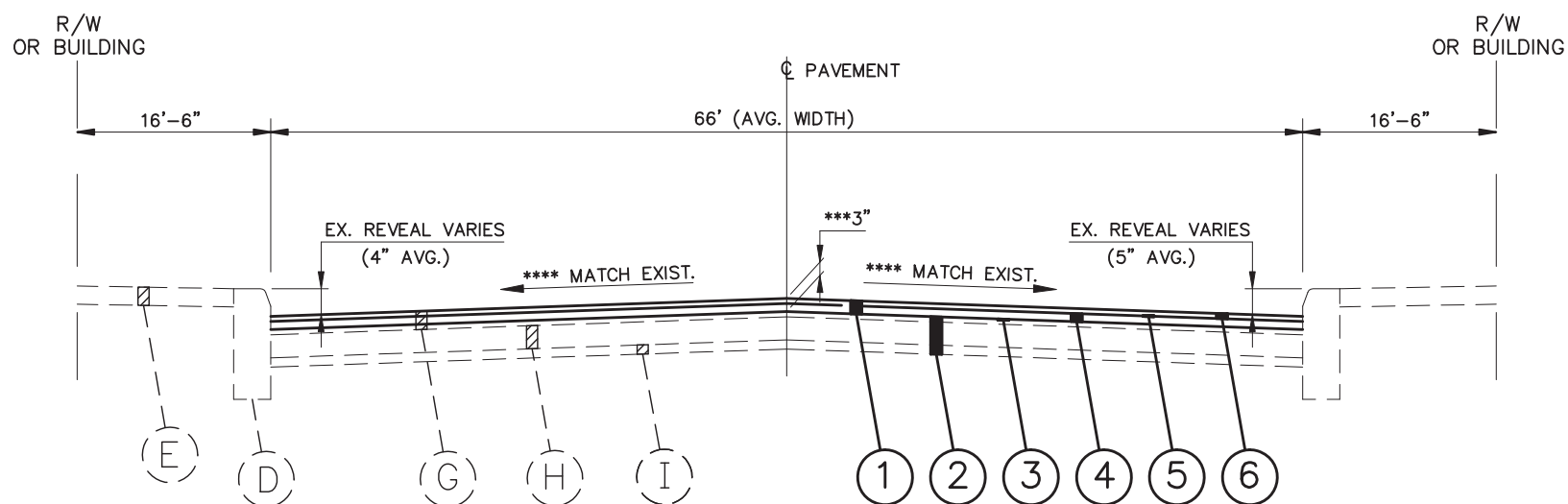
NO SCALE
STA. 109+10 TO STA. 122+81

TABLE "A" EXISTING AVERAGE WIDTHS

STA. 109+10 TO STA. 110+30	105'-6" TO 91'-10"
STA. 110+30 TO STA. 110+46	91'-10" TO 91'-0"
STA. 110+46 TO STA. 113+28	91'-0" TO 86'-0"
STA. 113+28 TO STA. 115+60	86'-0" TO 82'-6"
STA. 115+60 TO STA. 116+80	82'-6"
STA. 116+80 TO STA. 117+49	82'-6" TO 80'-0"
STA. 117+49 TO STA. 118+10	80'-0" TO 77'-9"
STA. 118+10 TO STA. 119+57	77'-9" TO 67'-10"
STA. 119+57 TO STA. 120+40	67'-10" TO 66'-0"
STA. 120+40 TO STA. 122+81	66'-0"

TABLE "B" EXISTING AVERAGE WIDTHS

STA. 109+10 TO STA. 109+89	0'-0" TO 12'-6"
STA. 109+89 TO STA. 112+38	12'-6"
STA. 112+38 TO STA. 113+28	12'-6" TO 0'-0"
STA. 113+28 TO STA. 122+81	0'-0"



TYPICAL SECTION

NO SCALE
STA. 122+81 TO STA. 127+14 (SUSPEND PROJECT)
STA. 134+00 (RESUME PROJECT) TO STA. 144+71

LEGEND

- + (1) ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (3" TYP.)
- ++ (2) PAVEMENT REPAIR
- (3) ITEM 407 - TACK COAT, TRACKLESS TACK, SURFACE COURSE, AS PER PLAN
- (4) ITEM 442 - 1-3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, 19mm, TYPE A (448), AS PER PLAN
- (5) ITEM 407 - TACK COAT, TRACKLESS TACK, INTERMEDIATE COURSE, AS PER PLAN
- (6) ITEM 442 - 1-1/4" ASPHALT CONCRETE SURFACE COURSE, 9.5mm, TYPE A (448), AS PER PLAN
- (A) EXISTING ASPHALT (3" TO 6"; 4" AVG.)
- (B) EXISTING CONCRETE (9" TO 16"; 10" AVG.)
- (C) EXISTING LIMESTONE SUBBASE (5"; PRESENT AT STA. 114+00, 34' RT. PAVEMENT CORE ONLY)
- * (D) EXISTING NON-INTEGRAL GRANITE CURB (6" TO 10" WIDE).
- (E) EXISTING CONCRETE WALK
- ** (F) EXISTING CONCRETE TRAFFIC DIVIDER
- (G) EXISTING ASPHALT (2" TO 9"; 5" AVG.)
- (H) EXISTING SANDSTONE (5" TO 7"; 6" AVG.)
-OR-
EXISTING CONCRETE (5" TO 9"; 7" AVG.)
-OR-
EXISTING GRANITE (UNKNOWN THICKNESS; PRESENT AT STA. 124+00, 1' RT. PAVEMENT CORE ONLY)
- (I) EXISTING SAND SUBBASE (0" TO 3"; 1" AVG.)

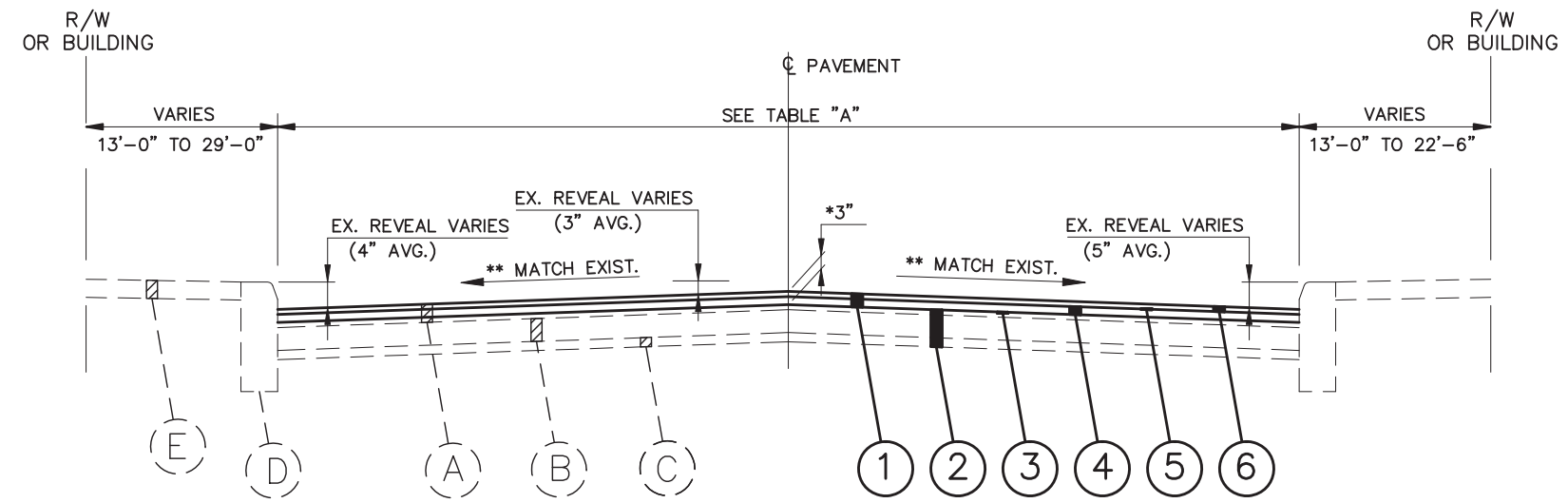
NOTES

- + OR TO THE TOP OF THE EXISTING RIGID BASE WHEN LESS THAN 3" OF EXISTING ASPHALT.
- ++ SEE "PAVEMENT REPAIR" PLAN NOTE (SHEET 19) AND "PAVEMENT REPAIR DETAILS" (SHEET 27) FOR ASSOCIATED PAY ITEMS AND ESTIMATED QUANTITIES TO BE USED AS DIRECTED BY THE ENGINEER.
- * FROM STA. 109+10 TO STA. 115+60, THIS IS EXISTING NON-INTEGRAL CONCRETE CURB (6" WIDE).
- ** EXISTS FROM STA. 109+10 TO STA. 110+46 (EXISTING WIDTH VARIES FROM 7'-3" TO 4'-6").
- *** PAVEMENT PLANING REMOVAL - DEPTH DATUM BEGINS 3" BELOW THE EXISTING SURFACE AT THE PAVEMENT CENTERLINE.
- **** IT IS INTENDED THAT THE FINISHED PLANED SURFACE CROSS SLOPE BE ESTABLISHED TO MATCH THE EXISTING NORMAL PAVEMENT CROSS SLOPE AND TO THE EXTENT FEASIBLE, CORRECT LONGITUDINAL IRREGULARITIES.

PAVEMENT CORE AT STA. 117+00, 2' LT. INDICATES 5" OF SANDSTONE W/MORTAR JOINTS ON 2" SAND SUBBASE BETWEEN (A) & (B) AND PAVEMENT CORE AT STA. 127+00, 28' RT. INDICATES 7" OF SANDSTONE W/MORTAR JOINTS ON 1" SAND SUBBASE BETWEEN (C) & (H).



DRAWING NAME: 01-Operations by Year 1 Oper 2013 Group-2 (Ontario and East 9th) 08-E9th-Typical.dwg
PLOT DATE: 11-7-12

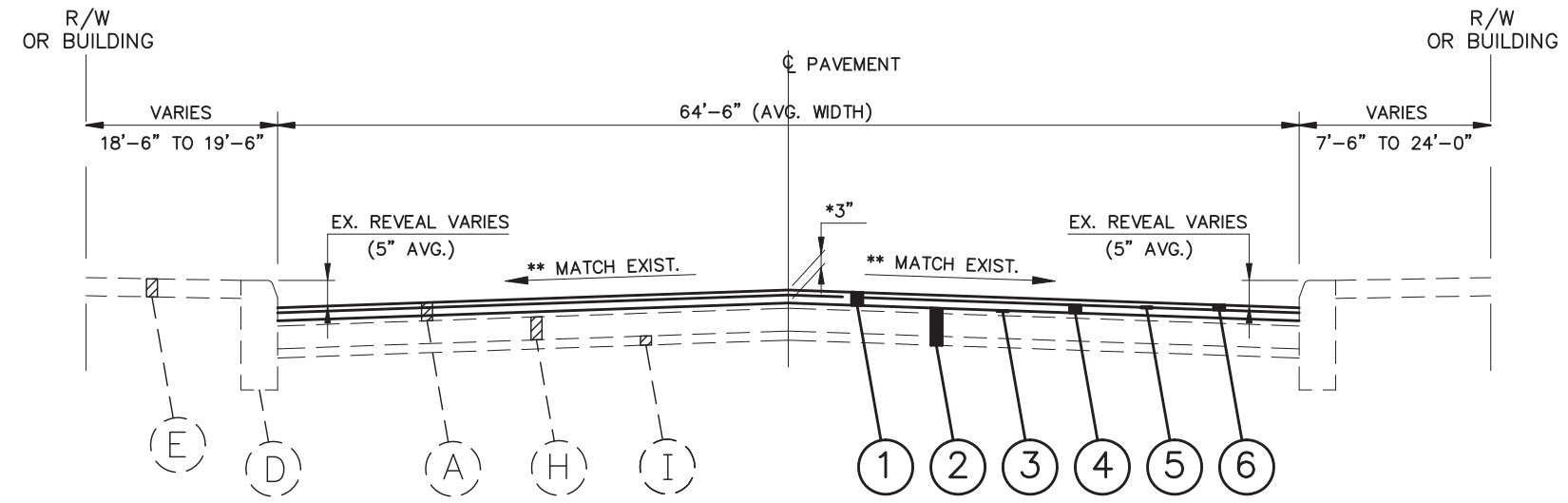


TYPICAL SECTION

NO SCALE

STA. 170+32 TO STA. 188+71 (SUSPEND PROJECT)
STA. 190+61 (RESUME PROJECT) TO STA. 213+15

TABLE "A" EXISTING AVERAGE WIDTHS	
STA. 170+32 TO STA. 184+42	71'-6"
STA. 184+42 TO STA. 188+71	58'-0"
STA. 190+61 TO STA. 193+16	65'-0"
STA. 193+16 TO STA. 198+75	72'-6"
STA. 198+75 TO STA. 201+70	63'-6"
STA. 201+70 TO STA. 213+15	72'-0"



TYPICAL SECTION

NO SCALE

STA. 213+15 TO STA. 217+74 (SUSPEND PROJECT)
STA. 220+13 (RESUME PROJECT) TO STA. 221+99 (SUSPEND PROJECT)
STA. 223+35 (RESUME PROJECT) TO STA. 225+65

LEGEND

- + (1) ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (3" TYP.)
- ++ (2) PAVEMENT REPAIR
- (3) ITEM 407 - TACK COAT, TRACKLESS TACK, SURFACE COURSE, AS PER PLAN
- (4) ITEM 442 - 1-3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, 19mm, TYPE A (448), AS PER PLAN
- (5) ITEM 407 - TACK COAT, TRACKLESS TACK, INTERMEDIATE COURSE, AS PER PLAN
- (6) ITEM 442 - 1-1/4" ASPHALT CONCRETE SURFACE COURSE, 9.5mm, TYPE A (448), AS PER PLAN
- (A) EXISTING ASPHALT (1" TO 11"; 4" AVG.)
- (B) { EXISTING CONCRETE (6" TO 13"; 10" AVG.)
OR
EXISTING SANDSTONE OR BRICK (3" TO 6"; 5" AVG.)
- (C) { EXISTING GRANULAR, 304 OR SAND SUBBASE (0" TO 17"; 4" AVG.)
-OR-
CEMENT STABILIZED RECYCLED CONCRETE BASE (5" TO 9"; 7" AVG.)
- (D) EXISTING NON-INTEGRAL GRANITE CURB (6" TO 10" WIDE)
- (E) EXISTING CONCRETE WALK
- (H) { EXISTING BRICK (4") ON LEVELING MORTAR (1") ON CONCRETE (7" TO 9"; 8" AVG.)
-OR-
EXISTING SANDSTONE (4"; PRESENT AT STA. 225+53, 1' RT. PAVEMENT CORE ONLY)
- (I) { EXISTING SAND, 304, OR GRAVEL SUBBASE (0" TO 10"; 3" AVG.)
-OR-
EXISTING CEMENT STABILIZED RECYCLED CONCRETE BASE (12" TO 14"; 13" AVG.)

NOTES

- + OR TO THE TOP OF THE EXISTING RIGID BASE WHEN LESS THAN 3" OF EXISTING ASPHALT.
- ++ SEE "PAVEMENT REPAIR" PLAN NOTE (SHEET 19) AND "PAVEMENT REPAIR DETAILS" (SHEET 27) FOR ASSOCIATED PAY ITEMS AND ESTIMATED QUANTITIES TO BE USED AS DIRECTED BY THE ENGINEER.
- * PAVEMENT PLANING REMOVAL - DEPTH DATUM BEGINS 3" BELOW THE EXISTING SURFACE AT THE PAVEMENT CENTERLINE.
- ** IT IS INTENDED THAT THE FINISHED PLANED SURFACE CROSS SLOPE BE ESTABLISHED TO MATCH THE EXISTING NORMAL PAVEMENT CROSS SLOPE AND TO THE EXTENT FEASIBLE, CORRECT LONGITUDINAL IRREGULARITIES.
- PAVEMENT CORE AT STA. 203+80, 41' LT. INDICATES 8" OF ASPHALT RAP BASE INSTEAD OF (B).

TYPICAL SECTION: EAST 9th STREET

2013 OPERATIONS GROUP 2



DRAWING NAME: G:\Oper's by Year\Oper 2013\Group-2\OntarioEast 9\09-TYP-QUANTITIES-ONTARIO-1.dwg
PLOT DATE: 10-24-12

PROPOSED PAVEMENT DATA						ITEMIZED QUANTITIES										CALCULATED CAG CHECKED BSD	TYPICAL SECTION QUANTITY TABLE: ONTARIO STREET
STATION TO STATION	SIDE	PAVEMENT LENGTH (FT.)	AVG. PVMT. WIDTH (FT.)	PAVEMENT AREA (SQ. FT.)	PAVEMENT AREA (SQ. YD.)		254	254		407	407		442	442			
							PAVEMENT PLANING, ASPHALT CONCRETE	PAVEMENT PLANING, PORTLAND CEMENT CONCRETE, AS PER PLAN		TACK COAT, TRACKLESS TACK, SURFACE COURSE, AS PER PLAN (0.08 GAL./SQ.YD.)	TACK COAT, TRACKLESS TACK, INTERMEDIATE COURSE, AS PER PLAN (0.04 GAL./SQ.YD.)		ASPHALT CONCRETE SURFACE COURSE, 9.5mm, TYPE A (448), AS PER PLAN (1-1/4")	ASPHALT CONCRETE INTERMEDIATE COURSE, 19mm, TYPE A (448), AS PER PLAN (1-3/4")			
							(SQ. YD.)	(SQ. YD.)		(GALLON)	(GALLON)		(CU. YD.)	(CU. YD.)			
109+10 TO 110+30	LT. & RT.	120	98.67	11,840.40	1,315.60		1,315.60	-		105.25	52.62		45.68	63.95			
110+30 TO 110+46	LT. & RT.	16	91.42	1,462.72	162.52		162.52	-		13.00	6.50		5.64	7.90			
110+46 TO 113+28	LT. & RT.	282	88.50	24,957.00	2,773.00		2,773.00	-		221.84	110.92		96.28	134.80			
113+28 TO 115+60	LT. & RT.	232	84.25	19,546.00	2,171.78		2,171.78	-		173.74	86.87		75.41	105.57			
115+60 TO 116+80	LT. & RT.	120	82.50	9,900.00	1,100.00		1,100.00	-		88.00	44.00		38.19	53.47			
116+80 TO 117+49	LT. & RT.	69	81.25	5,606.25	622.92		622.92	-		49.83	24.92		21.63	30.28			
117+49 TO 118+10	LT. & RT.	61	78.88	4,811.68	534.63		534.63	-		42.77	21.38		18.56	25.99			
118+10 TO 119+57	LT. & RT.	147	72.79	10,700.13	1,188.90		1,188.90	-		95.11	47.56		41.28	57.79			
119+57 TO 120+40	LT. & RT.	83	66.92	5,554.36	617.15		617.15	-		49.37	24.69		21.43	30.00			
120+40 TO 122+81	LT. & RT.	241	66.00	15,906.00	1,767.33		1,767.33	-		141.39	70.69		61.36	85.91			
122+81 TO 127+14	LT. & RT.	433	66.00	28,578.00	3,175.33		3,175.33	-		254.03	127.01		110.25	154.36			
134+00 TO 144+71	LT. & RT.	1,071	66.00	70,686.00	7,854.00		7,854.00	-		628.32	314.16		272.71	381.79			
BUS PAD AREAS TO REMAIN:																	
-	-	-	-	2,131.29	236.81		(-236.81)	-		(-18.94)	(-9.47)		(-8.22)	(-11.51)			
MISC. CONCRETE SURFACE AREAS NOT ASSOCIATED WITH BUS PADS:																	
-	-	-	-	1,876.54	208.50		(-208.50)	208.50		-	-		-	-			
EXTRA AREAS (DROP-OFF LANE)																	
109+10 TO 109+89	RT.	79	6.25	493.75	54.86		54.86	-		4.39	2.19		1.90	2.67			
109+89 TO 112+38	RT.	249	12.50	3,112.50	345.83		345.83	-		27.67	13.83		12.01	16.81			
112+38 TO 113+28	RT.	90	6.25	562.50	62.50		62.50	-		5.00	2.50		2.17	3.04			
EXTRA AREAS (END PROJECT RETURNS)																	
LAKESIDE AVE.	RT.	-	-	195.83	21.76		21.76	-		1.74	0.87		0.76	1.06			
WEST LAKESIDE AVE.	LT.	-	-	195.83	21.76		21.76	-		1.74	0.87		0.76	1.06			
TOTALS:							23,344.56	208.50		1,884.25	942.11		817.80	1,144.94			
TOTALS CARRIED TO THE GENERAL SUMMARY (SHEETS 24 - 25)							23,345	209		1,885	943		818	1,145			

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DRAWING NAME: G:\Open's by Year\Open 2013\Group-2\OntarioEast 9\12-TYP-QUANTITIES-EAST 9th-2.dwg
PLOT DATE: 10-24-12

PROPOSED PAVEMENT DATA						ITEMIZED QUANTITIES										CALCULATED CAG	CHECKED BSD
STATION TO STATION	SIDE	PAVEMENT LENGTH (FT.)	AVG. PVMT. WIDTH (FT.)	PAVEMENT AREA (SQ. FT.)	PAVEMENT AREA (SQ. YD.)		254	254		407	407		448	448			
							PAVEMENT PLANING, ASPHALT CONCRETE	PAVEMENT PLANING, PORTLAND CEMENT CONCRETE, AS PER PLAN		TACK COAT, TRACKLESS TACK, SURFACE COURSE, AS PER PLAN (0.08 GAL./SQ.YD.)	TACK COAT, TRACKLESS TACK, INTERMEDIATE COURSE, AS PER PLAN (0.04 GAL./SQ.YD.)		ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 64-22, AS PER PLAN (1-1/4")	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG 64-22, AS PER PLAN (1-3/4")			
							(SQ. YD.)	(SQ. YD.)		(GALLON)	(GALLON)		(CU. YD.)	(CU. YD.)			
ADDING EXTRA AREAS FOR INTERSECTIONS:																	
CARNEGIE AVE.	RT.	-	-	196.17	21.80		21.80	-		1.74	0.87		0.76	1.06			
CARNEGIE AVE.	LT.	-	-	196.17	21.80		21.80	-		1.74	0.87		0.76	1.06			
SUMNER AVE.	RT.	-	-	1,575.33	175.04		175.04	-		14.00	7.00		6.08	8.51			
ERIE CT.	RT.	-	-	868.67	96.52		96.52	-		7.72	3.86		3.35	4.69			
BOLIVAR RD.	RT.	-	-	697.00	77.44		77.44	-		6.20	3.10		2.69	3.76			
BOLIVAR RD.	LT.	-	-	1,575.67	175.07		175.07	-		14.01	7.00		6.08	8.51			
PROSPECT AVE.	RT.	-	-	2,950.79	327.86		327.86	-		26.23	13.11		11.38	15.94			
PROSPECT AVE.	LT.	-	-	2,721.92	302.44		302.44	-		24.20	12.10		10.50	14.70			
ALPHA CT.	LT.	-	-	585.00	65.00		65.00	-		5.20	2.60		2.26	3.16			
CHESTER AVE.	RT.	-	-	1,616.94	179.66		179.66	-		14.37	7.19		6.24	8.73			
VINCENT AVE.	LT.	-	-	1,078.25	119.80		119.80	-		9.58	4.79		4.16	5.82			
WALNUT AVE.	RT.	-	-	1,169.46	129.94		129.94	-		10.40	5.20		4.51	6.32			
SUPERIOR AVE.	RT.	-	-	2,612.50	290.28		290.28	-		23.22	11.61		10.08	14.11			
SUPERIOR AVE.	LT.	-	-	2,509.54	278.84		278.84	-		22.31	11.15		9.68	13.55			
ROCKWELL AVE.	LT.	-	-	931.00	103.44		103.44	-		8.28	4.14		3.59	5.03			
ROCKWELL AVE.	RT.	-	-	1,250.00	138.89		138.89	-		11.11	5.56		4.82	6.75			
THERESA CT.	LT.	-	-	236.17	26.24		26.24	-		2.10	1.05		0.91	1.28			
ST. CLAIR AVE.	RT.	-	-	2,361.08	262.34		262.34	-		20.99	10.49		9.11	12.75			
ST. CLAIR AVE.	LT.	-	-	2,162.33	240.26		240.26	-		19.22	9.61		8.34	11.68			
LAKESIDE AVE.	RT.	-	-	3,450.42	383.38		383.38	-		30.67	15.34		13.31	18.64			
LAKESIDE AVE.	LT.	-	-	2,330.00	258.89		258.89	-		20.71	10.36		8.99	12.58			
RT. 2/I-90 EAST BOUND ENTRANCE RAMP	RT.	-	-	1,191.67	132.41		132.41	-		10.59	5.30		4.60	6.44			
RT. 2/I-90 EAST BOUND EXIT RAMP	LT.	-	-	1,530.67	170.07		170.07	-		13.60	6.80		5.91	8.27			
RT. 2/I-90 WEST BOUND EXIT RAMP	RT.	-	-	2,184.00	242.67		242.67	-		19.41	9.71		8.43	11.80			
RT. 2/I-90 WEST BOUND ENTRANCE RAMP	LT.	-	-	1,343.33	149.26		149.26	-		11.94	5.97		5.18	7.26			
NORTH MARGINAL RD.	RT.	-	-	848.33	94.26		94.26	-		7.54	3.77		3.27	4.58			
BUS PAD AREAS TO REMAIN:																	
-	-	-	-	83.97	9.33		(-9.33)	-		(-0.75)	(-0.37)		(-0.32)	(-0.45)			
MISC. CONCRETE SURFACE AREAS NOT ASSOCIATED WITH BUS PADS:																	
-	-	-	-	1,371.15	152.35		(-152.35)	152.35		-	-		-	-			
TOTALS:							4,301.92	152.35		356.33	178.18		154.67	216.53			
TOTALS CARRIED TO THE GENERAL SUMMARY (SHEETS 24 - 25)							4,302	153		357	179		155	217			

TYPICAL SECTION QUANTITY TABLE: EAST 9th STREET

2013 OPERATIONS GROUP 2

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DRAWING NAME: G:\Oper's by Year\Oper 2013\Group-2\Ontario,East 9\15-GENOTE1.dwg
PLOT DATE: 11-7-12

GENERAL

SCOPE OF WORK

THE WORK WITHIN THE LIMITS OF THIS 0.54 MILE (ONTARIO) AND 0.94 MILE (EAST 9th) 2013 OPERATIONS RESURFACING IMPROVEMENT (GROUP 2) INCLUDES THE PLANING OR REMOVAL OF THE EXISTING ASPHALT CONCRETE WEARING COURSE, THE REPAIR OF THE DETERIORATED RIGID (GRANITE/ CONCRETE/ BRICK/ SANDSTONE) BASE; ADJUSTMENT, RECONSTRUCTION OR REPLACEMENT AS NEEDED OF THE VARIOUS CASTINGS/STRUCTURES; THE CONSTRUCTION OF A UNIFORM THREE INCH (3”) ASPHALT CONCRETE OVERLAY; THE INSTALLATION OF ADA COMPLIANT CITY OF CLEVELAND CURB RAMPS & STREETSCAPING ITEMS (CURB, WALK, BRICK PAVERS, TREE PIT COLLARS & GRATES) AND OTHER RELATED ITEMS AS SHOWN ON THE PLANS OR STIPULATED IN THE SPECIFICATIONS, PLAN NOTES, PROPOSAL NOTES OR ELSEWHERE IN THE BID PACKAGE FOR ONTARIO STREET (FROM 900 FT. NORTH OF CARNEGIE AVENUE TO LAKESIDE AVENUE) AND EAST 9TH STREET (FROM CARNEGIE AVENUE TO NORTH MARGINAL ROAD) IN THE CITY OF CLEVELAND, OHIO.

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

OHIO DEPARTMENT OF TRANSPORTATION– DISTRICT 12
5500 TRANSPORTATION BOULEVARD
GARFIELD HEIGHTS, OHIO 44125–5396
ATTN: DAVID LASTOVKA, P.E.
(INNERBELT BRIDGE PROJECT)
PHONE: 216–584–2115
FAX: 216–584–2279
E–mail: dave.lastovka@dot.state.oh.us

OHIO DEPARTMENT OF TRANSPORTATION– DISTRICT 12
5500 TRANSPORTATION BOULEVARD
GARFIELD HEIGHTS, OHIO 44125–5396
ATTN: KEITH HAMILTON, P.E.
(ONTARIO/HURON INTERSECTION PROJECT)
PHONE: 216–584–2127
FAX: 216–584–2279
E–mail: keith.hamilton@dot.state.oh.us

OHIO DEPARTMENT OF TRANSPORTATION– DISTRICT 12
5500 TRANSPORTATION BOULEVARD
GARFIELD HEIGHTS, OHIO 44125–5396
ATTN: GARY BENESH, P.E.
PHONE: 216–581–2100
FAX: 216–584–2274
E–mail: gary.benesh@dot.state.oh.us

KA
1468 WEST 9th STREET, SUITE #600
CLEVELAND, OHIO 44113
ATTN: JOHN P. KAMINSKI
(CASINO PEDESTRIAN BRIDGE PROJECT)
PHONE: 216–830–1506
FAX: 216–781–6566
E–mail: jkaminski@kainc.com

DOMINION EAST OHIO
320 SPRINGSIDE DRIVE, SUITE #320
AKRON, OHIO 44333
ATTN: MARY J. LONG
PHONE: 330–664–2409
FAX: 888–504–0126
E–mail: mary.j.long@dom.com

WINDSTREAM
(PAETEC, McLEOD U.S.A., CAVALIER & ELANTIC TELECOM)
1111 SUPERIOR AVENUE EAST, SUITE #500
CLEVELAND, OHIO 44114
ATTN: RODGER MADDERN
PHONE: 440–274–0209
FAX: 440–260–0113

VERIZON BUSINESS (MCI)
OSP NATIONAL SUPPORT/INVESTIGATIONS
(BUSINESS ID #3651–2012)
2400 NORTH GLENNVILLE
RICHARDSON, TEXAS 75082
ATTN: BETH SEUBERT
PHONE: 972–729–6016

G4S TECHNOLOGY LLC
4 WALKER WAY, SUITE #1
ALBANY, NEW YORK 12205
ATTN: DRAGAN KORDICH
PHONE: 518–869–5053
FAX: 518–869–3839
E–mail: dragan.kordich@usa.g4s.com

CLEVELAND THERMAL ENERGY CORPORATION
1921 HAMILTON AVENUE
CLEVELAND, OHIO 44114
ATTN: SCOTT TEMPLETON
PHONE: 216–241–4192
FAX: 216–241–6486

CITY OF CLEVELAND
DIVISION OF WATER
1201 LAKESIDE AVENUE, 6th FLOOR
CLEVELAND, OHIO 44114
ATTN: GUY SINGER, CONSULTING ENGINEER
PHONE: 216–664–2444 EXT. 5555
FAX: 216–664–2838
E–mail: guy_singer@clevelandwater.com

CITY OF CLEVELAND
DIVISION OF TRAFFIC ENGINEERING
601 LAKESIDE AVENUE, ROOM 25
CLEVELAND, OHIO 44114
ATTN: ROBERT MAVEC
PHONE: 216–664–3197
FAX: 216–664–3167
E–mail: rrmavec@city.cleveland.oh.us

CITY OF CLEVELAND
DIVISION OF CLEVELAND PUBLIC POWER (MELP/CCP)
1300 LAKESIDE AVENUE, ROOM 152
CLEVELAND, OHIO 44114
ATTN: CHRISTOPHER M. HIRZEL, P.E.,P.S.
PHONE: 216–664–3922 EXT. 115
FAX: 216–664–2972
E–mail: chirzel@cpp.org

CITY OF CLEVELAND
DEPARTMENT OF PUBLIC WORKS
500 LAKESIDE AVE.
CLEVELAND, OHIO 44114
ATTN: MICHAEL E. COX, DIRECTOR
PHONE: 216–664–2485
FAX: 216–664–4086
E–mail: mcox@city.cleveland.oh.us

CITY OF CLEVELAND
DIVISION OF WATER POLLUTION CONTROL
12302 KIRBY AVENUE
CLEVELAND, OHIO 44108
ATTN: ELIE RAMY
PHONE: 216–664–2756
FAX: 216–664–3477
E–mail: eramy@clevelandwpc.com

CITY OF CLEVELAND
DEPARTMENT OF PUBLIC SAFETY
601 LAKESIDE AVENUE, ROOM 230
CLEVELAND, OHIO 44114
ATTN: MARTIN L. FLASK, DIRECTOR
PHONE: 216–664–2200
FAX: 216–664–3734
E–mail: mflask@city.cleveland.oh.us

CITY OF CLEVELAND
DEPARTMENT OF PUBLIC UTILITIES
CARL B. STOKES BUILDING
1201 LAKESIDE AVENUE
CLEVELAND, OHIO 44114
ATTN: BARRY A. WITHERS, DIRECTOR
PHONE: 216–664–2444
FAX: 216–664–3454
E–mail: bwithers@city.cleveland.oh.us

CITY OF CLEVELAND
MAYOR’S OFFICE OF CAPITAL PROJECTS
601 LAKESIDE AVENUE, ROOM 113
CLEVELAND, OHIO 44114
ATTN: JOMARIE WASIK, DIRECTOR
PHONE: 216–664–2231
FAX: 216–664–2198
E–mail: jwasik@city.cleveland.oh.us

XO COMMUNICATIONS (NEXTLINK)
815 SUPERIOR AVENUE E
SUITE #200
CLEVELAND, OHIO 44114
ATTN: DALE FERGUSON,
IMPLEMENTATION ENGINEER
PHONE: 216–619–3492
FAX: 216–619–3684

TIME WARNER CABLE
8150 DOW CIRCLE
STRONGSVILLE, OHIO 44136
ATTN: PAUL SILVESTRO,
CONSTRUCTION COORDINATOR
PHONE: 216–575–8016 EXT. 5034
FAX: 440–826–2940
E–mail: paul.silvestro@twcable.com

AT&T OHIO
13630 LORAIN AVENUE–2nd FLOOR
CLEVELAND, OHIO 44111
ATTN: JAMES JANIS, MANAGER
PHONE: 216–476–6142
FAX: 216–476–6016
E–mail: pj8191@att.com

NORTHEAST OHIO REGIONAL
SEWER DISTRICT
3900 EUCLID AVENUE
CLEVELAND, OHIO 44115–2504
ATTN: MARY MACIEJOWSKI
PERMIT PROGRAM MANAGER
PHONE: 216–881–6600
FAX: 216–881–2738

FIRST ENERGY
6896 MILLER ROAD,
SUITE #110
BRECKSVILLE, OHIO 44141
ATTN: MARK ROBINSON
PHONE: 440–717–6845
FAX: 440–546–8780
E–mail: robinsonme@firstenergycorp.com

CUYAHOGA COUNTY DEPARTMENT
OF INFORMATION TECHNOLOGY
THE STERLING BUILDING
1255 EUCLID AVENUE, 4th FLOOR
CLEVELAND, OHIO 44115
ATTN: PAUL TARABA, ADMINISTRATOR
OF COMMUNICATION/INFRASTRUCTURE
PHONE: 216–443–8015
FAX: 216–443–7363

LEVEL 3 COMMUNICATIONS, INC.
NETWORK INFRASTRUCTURE SERVICES
1025 ELDORADO BOULEVARD
33A–524
BROOMFIELD, COLORADO 80021
ATTN: MATT PRINK
PHONE: 720–888–2639
E–mail: matthew.prink@level3.com

HLG CONSULTING (AT&T CORPORATION)
5980–G WILCOX PLACE
DUBLIN, OHIO 43016
ATTN: ED HUGHES
PHONE: 614–760–8320
FAX: 614–760–8323
E–mail: ehughes@hlengineering.com

AT&T LOCAL NETWORK SERVICES (TCG)
508 WOOSTER CIRCLE
SAGAMORE HILLS, OHIO 44067
ATTN: BOB PLUTO
PHONE: 216–533–6331
E–mail: rjpluto1948@windstream.net

GREATER CLEVELAND REGIONAL
TRANSIT AUTHORITY
1240 WEST 6th STREET
CLEVELAND, OHIO 44113–1331
ATTN: MIKE SCHIPPER
PHONE: 216–566–5084
FAX: 216–431–6209

FOREST CITY ENTERPRISES
TERMINAL TOWER
50 PUBLIC SQUARE, SUITE #1515
CLEVELAND, OHIO 44113
ATTN: DON BECK,
OPERATIONS DIRECTOR CLEVELAND
PHONE: 216–416–3905
FAX: 216–905–9338
E–mail: donbeck@forestcity.net

ZAYO FIBER SOLUTIONS
(AMERICAN FIBER SYSTEMS)
305 E. WIGGIN STREET
GAMBIER, OHIO 43022
ATTN: SCOTT HEINLEN, OSP MANAGER OHIO
PHONE: 740–501–6921
E–mail: scott.heinlen@zayo.com

CALL OHIO OIL & GAS PRODUCERS
UNDERGROUND PROTECTION SERVICE (OGPUPS)
TOLL FREE NO. 1–800–925–0988

CALL OHIO UTILITIES PROTECTION SERVICE
TWO (2) WORKING DAYS BEFORE YOU DIG
TOLL FREE NO. 1–800–362–2764
(NON–MEMBERS MUST BE CALLED DIRECTLY)

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT IS ANTICIPATED TO ONLY INVOLVE CASTING ADJUSTMENTS OF ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

LOCAL LAWS, ORDINANCES AND REGULATIONS

IN ACCORDANCE WITH SECTION 107.01 OF THE GENERAL PROVISIONS, THE CONTRACTOR SHALL STAY FULLY INFORMED OF ALL LOCAL LAWS, ORDINANCES, REGULATIONS, ORDERS AND DECREES THAT AFFECT THE WORK. IT IS THE CONTRACTOR’S RESPONSIBILITY TO OBSERVE AND COMPLY WITH ALL SUCH LAWS, ORDINANCES, REGULATIONS, ORDERS AND DECREES AT NO ADDITIONAL COST TO THE PROJECT/COUNTY.

CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED ”AS DIRECTED BY THE ENGINEER” UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL SUBSIDIARY AGREEMENT GOVERNING COMPLETION OF THIS PROJECT.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION, AND OPERATION (INCLUDING ERECTION, MAINTENANCE AND REMOVAL) OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS, EXCEPT WHERE OTHERWISE NOTED, SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

EXISTING TYPICAL SECTIONS

EXISTING TYPICAL SECTIONS HAVE BEEN DEVELOPED FROM SITE MEASUREMENTS, PAVEMENT CORES AND RECORD PLANS AND ARE BELIEVED TO REPRESENT THE WIDTH AND COMPOSITION OF THE EXISTING PAVEMENT, BUT THE COUNTY DOES NOT GUARANTEE THE ACCURACY OF SAME. A COPY OF THE PAVEMENT CORE REPORT IS INCLUDED UNDER SECTION 500 OF THE ”CUYAHOGA COUNTY ENGINEER SPECIFICATION BOOKLET” CONTAINED IN THE BID PACKAGE.

ITEM 619 – FIELD OFFICE, TYPE B, AS PER PLAN

THE FIELD OFFICE SHALL, IN ADDITION TO THE ITEMS LISTED IN ITEM 619, BE FURNISHED WITH TWO CELLULAR TELEPHONES. IN ADDITION, IF THE COPYING MACHINE PROVIDED PER TABLE 619.02–1 DOES NOT HAVE PRINTER CAPABILITY, PROVIDE AN APPROVED PRINTER.

THE CONTRACTOR’S INSURANCE SHALL INCLUDE A RIDER FOR COVERAGE OVER ANY CUYAHOGA COUNTY PROPERTY INSIDE THE FIELD OFFICE WHICH MAY BECOME DAMAGED OR STOLEN.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED IN THE GENERAL SUMMARY FOR THIS ITEM:

ITEM 619 – FIELD OFFICE, TYPE B, AS PER PLAN 6 MONTH

GENERAL (CONT.)

ADJUSTMENTS IN CONTRACT TIME

TIME EXTENSIONS WILL ONLY BE CONSIDERED WHEN CONTROLLING ITEMS OF WORK ON THE APPROVED PROGRESS SCHEDULE ARE AFFECTED DUE TO NO FAULT OF THE CONTRACTOR. WHEN ADDITIONAL WORK IS REQUIRED, TIME EXTENSIONS WILL ONLY BE GRANTED FOR CONTROLLING ITEMS ON THE PROGRESS SCHEDULE (SEE SECTION 108.06 OF THE CUYAHOGA COUNTY ENGINEER’S GENERAL PROVISIONS).

COOPERATION BETWEEN CONTRACTORS

IT IS ANTICIPATED THAT OTHER CONTRACTORS WILL BE WORKING ON THE ODOT INNERBELT BRIDGE (INCLUDING THE INTERSECTION OF EAST 9th AND CARNEGIE), THE ODOT ONTARIO/HURON INTERSECTION SIGNAL IMPROVEMENT, THE CLEVELAND DIVISION OF WATER POLLUTION CONTROL SEWER RELINING (ONTARIO STREET), THE NEORS D SEWER INSPECTION & EVALUATION (EAST 9th FROM CARNEGIE TO SUPERIOR), CLEVELAND DIVISION OF WATER WATERMAIN RELINING (EAST 9th STREET) AND THE CASINO PEDESTRIAN BRIDGE (ONTARIO/PROSPECT INTERSECTION) PROJECTS WHILE THIS PROJECT IS BEING CONSTRUCTED. THE CONTRACTOR SHALL COOPERATE AND COORDINATE HIS OPERATIONS, INCLUDING PROVISIONS FOR THE MAINTENANCE OF TRAFFIC, WITH THE CONTRACTORS OF THESE PROJECTS THAT MAY BE IN FORCE DURING THE LIFE OF THIS CONTRACT. THE CONTRACTOR’S ATTENTION IS SPECIFICALLY DRAWN TO SECTION 105.08 OF THE CUYAHOGA COUNTY ENGINEER’S GENERAL PROVISIONS. NO WAIVER OF ANY OF THE PROVISIONS OF SECTION 105.08 IS INTENDED.

PROJECT PROGRESS MEETINGS

PROGRESS MEETINGS WILL BE HELD EVERY FOUR (4) WEEKS AT THE PROJECT OFFICE OR OTHER LOCATION DESIGNATED BY THE AREA CONSTRUCTION ENGINEER, AND WILL BE ATTENDED BY COUNTY AND CONTRACTOR DECISION–MAKING PERSONNEL.

THE PURPOSE OF THESE MEETINGS IS TO DISCUSS CRITICAL OPERATIONS AND POTENTIAL PROBLEMS. ALSO, THE CONTRACTOR WILL CONFIRM THE NUMBER AND DURATION OF WORK SHIFTS, NUMBER OF WORK CREWS, AND SPECIFIC PORTIONS OF THE WORK TO BE PERFORMED DURING THE FOLLOWING WEEKS.

THESE MEETINGS CAN ONLY BE WAIVED BY THE AREA CONSTRUCTION ENGINEER.

DEFINITIONS AND TERMS

WHEREVER THERE APPEARS, IN THE STANDARD SPECIFICATIONS (ODOT ITEMIZED CMS), SUPPLEMENTAL SPECIFICATIONS OR PROPOSAL NOTES, THE TERM "THE STATE", "DIRECTOR OF TRANSPORTATION" "DEPARTMENT", "DISTRICT CONSTRUCTION ENGINEER (DCE)", "ENGINEER", OR ANY OTHER TERM DESIGNATING ANY REPRESENTATIVE OR EMPLOYEE OF THE STATE OR ITS DEPARTMENT OF TRANSPORTATION, SUCH TERM SHALL, FOR THE PURPOSE OF THIS CONTRACT, BE CONSIDERED AND TAKEN AS MEANING AND DESIGNATING THE RESPECTIVE OFFICER OR EMPLOYEE OF CUYAHOGA COUNTY WHOSE DUTY OR FUNCTION IT IS TO DEAL WITH THE SUBJECT MATTER IN CONNECTION WITH WHICH SUCH TERM IS USED AND SPECIFICALLY:

THE STATE	– MEANS CUYAHOGA COUNTY
DEPARTMENT	– MEANS THE CUYAHOGA COUNTY DEPARTMENT OF PUBLIC WORKS.
DIRECTOR	– MEANS THE CUYAHOGA COUNTY DIRECTOR OF PUBLIC WORKS, HIS/HER DEPUTIES, OR ANY ENGINEER DESIGNATED AS THE DIRECTOR’S REPRESENTATIVE.
DISTRICT CONSTRUCTION ENGINEER (DCE) OR ENGINEER	– MEANS THE AREA CONSTRUCTION ENGINEER, OR THE DULY AUTHORIZED AGENT OR REPRESENTATIVE OF THE CUYAHOGA COUNTY DIRECTOR OF PUBLIC WORKS, ACTING WITHIN THE SCOPE OF HIS/HER AUTHORITY FOR THE PURPOSES OF CONSTRUCTION ENGINEERING AND ADMINISTRATION OF THE CONTRACT.
LABORATORY	– MEANS ANY LABORATORY DESIGNATED BY THE COUNTY.

GENERAL PROVISIONS

ALL REFERENCES TO THE GENERAL PROVISIONS (SECTION 100) OF THE OHIO DEPARTMENT OF TRANSPORTATION’S CONSTRUCTION AND MATERIAL SPECIFICATIONS (ODOT CMS) THAT ARE CITED IN THE ITEMIZED SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS, PROPOSAL NOTES, PLANS OR ELSEWHERE IN THE CONTRACT DOCUMENTS

SHALL, FOR THE PURPOSE OF THIS CONTRACT BE CONSIDERED AND TAKEN AS MEANING THE CUYAHOGA COUNTY ENGINEER’S GENERAL PROVISIONS AS CONTAINED IN THE "CUYAHOGA COUNTY ENGINEER SPECIFICATION BOOKLET". THE SUBSECTION NUMBERS AND TOPICS USED IN THE COUNTY’S GENERAL PROVISIONS CORRESPOND TO THOSE CONTAINED IN THE ODOT CMS.

STATIONING AND LOCATIONS

STATIONING AND LOCATIONS INDICATED ON THESE PLANS ARE APPROXIMATE. ALL LOCATIONS AND ITEMS CALLED OUT BY STATION ARE SUBJECT TO ADJUSTMENT IN THE FIELD AS DIRECTED BY THE ENGINEER.

CONSTRUCTION NOISE

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, DO NOT OPERATE AT ANY TIME ANY DEVICE IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT.

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER AND CALCIUM CHLORIDE FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616 – WATER	<u>40</u> M GAL.
ITEM 616 – CALCIUM CHLORIDE	<u>4</u> TON

WORK ON MORE THAN ONE ROAD

THIS PROJECT INCLUDES WORK ON MORE THAN ONE (1) ROAD. THE CONTRACTOR MAY NOT WORK ON TWO (2) ROADS SIMULTANEOUSLY (SEE CONSTRUCTION SEQUENCE NOTE ON SHEET 22). THE FOLLOWING REQUIREMENTS APPLY TO THIS CONTRACT:

- THE PROGRESS SCHEDULE REQUIRED IN 108.03.A SHALL ALSO SHOW THE CONTRACTOR’S PLAN TO CARRY OUT THE WORK REQUIRED ON EACH ROAD INDIVIDUALLY AND GIVE START AND COMPLETION DATES FOR EACH ROAD.
- THE REQUIREMENTS OF 108.03 SHALL APPLY SPECIFICALLY TO EACH ROAD.
- THE REQUIREMENTS OF 108.04 SHALL APPLY SPECIFICALLY TO EACH ROAD. IN DETERMINING WHETHER THE CONTRACTOR HAS COMPLIED WITH THE REQUIREMENT TO ASSURE THE LEAST INTERFERENCE WITH TRAFFIC, THE ENGINEER SHALL ADDITIONALLY TAKE INTO CONSIDERATION SUCH FACTORS AS:
 - THE LENGTH AND DURATION OF LANE CLOSURES.
 - THE RIDEABILITY OF ANY PLANED SURFACES WITH PARTICULAR ATTENTION GIVEN TO CASTING HEIGHTS AND DRIVE ACCESS CURB HEIGHTS.
 - ACCESS TO DRIVEWAYS.
 - UNNECESSARY PERIODS OF INACTIVITY BY THE CONTRACTOR.
 - WHETHER THE FIRST ROAD IS ENTIRELY COMPLETE PRIOR TO BEGINNING WORK ON THE SECOND ROAD.
- THE REQUIREMENTS OF 108.05 SHALL APPLY SPECIFICALLY TO EACH ROAD. THE CONTRACTOR SHALL AT ALL TIMES EMPLOY SUFFICIENT LABOR AND EQUIPMENT FOR PROSECUTING THE SEVERAL CLASSES OF WORK TO FULL COMPLETION IN THE MANNER AND TIME REQUIRED BY THESE SPECIFICATIONS.
- LIQUIDATED DAMAGES, BASED ON THE TOTAL CONTRACT AS SPECIFIED IN 108.07, SHALL APPLY FOR EACH CALENDAR DAY THAT A SPECIFIED ROAD REMAINS UNCOMPLETED AFTER THE ROAD’S SPECIFIC COMPLETION DATE, AS DETERMINED BY THE CONTRACTOR’S ORIGINALLY APPROVED PROGRESS SCHEDULE. DUE ACCOUNT WILL BE TAKEN OF ANY ADJUSTMENT OF THE SPECIFIC ROAD’S COMPLETION DATE GRANTED UNDER 108.06

ITEM 614 – MAINTAINING TRAFFIC, AS PER PLAN –AND–
ITEM 624 – MOBILIZATION, AS PER PLAN

PAYMENT FOR LUMP SUM ITEMS

IN CONJUNCTION WITH SECTION 109.09 OF THE SPECIFICATIONS, THE ABOVE REFERENCED "LUMP SUM" ITEMS SHALL BE PAYABLE TO THE CONTRACTOR AS SPECIFIED BELOW:

- THE BASIS OF PAYMENT FOR EACH OF THE ABOVE LUMP SUM ITEMS WILL BE THE CALCULATED RATIO OF THE LENGTH OF WORK COMPLETED ON EACH ROAD COMPARED TO THE TOTAL LENGTH OF ALL ROADS IN THE CONTRACT.
- THE CONTRACTOR WILL ONLY RECEIVE THE CALCULATED DOLLAR AMOUNT FOR EACH ROAD THAT IS RESURFACED. IF ANY ROADS ARE ADDED OR DELETED, THE LUMP SUM’S AMOUNT WILL EITHER BE INCREASED OR DECREASED ACCORDING TO THE METHOD DESCRIBED IN NO. 1 ABOVE.
- FOR ITEM 624 – MOBILIZATION, AS PER PLAN, THE CONTRACTOR WILL RECEIVE THE PROPORTIONAL DOLLAR AMOUNT SPECIFIED IN SECTION 624.02 OF THE SPECIFICATIONS FOR EACH OF THE ROADS, AS SPECIFIED IN THIS CONTRACT, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

ROADWAY

ITEM 202 – REMOVAL MISC.: EXISTING TROLLEY RAILS REMOVED

ALONG WITH THE REQUIREMENTS OF ITEM 202, THE FOLLOWING SHALL ALSO APPLY TO THIS ITEM:

PER THE PAVEMENT CORES THERE IS AN INDICATION THAT WITHIN THE EXISTING SUBBASE OF EAST 9th STREET, ABANDONED TROLLEY/STREET CAR RAILS MAY BE ENCOUNTERED AND MAY REQUIRE REMOVAL PRIOR TO OVERLAYING.

PAYMENT AT THE UNIT PRICE BID PER FOOT SHALL BE FULL COMPENSATION FOR ALL LABOR, MATERIALS AND EQUIPMENT NECESSARY TO REMOVE AND DISPOSE OF ANY EXISTING RAILS AND/OR TRACK BASE TO THE SATISFACTION OF THE ENGINEER. ANY RESTORATION EARTHWORK TO BACKFILL THE CAVITY CREATED BY REMOVAL SHALL BE CONSIDERED INCIDENTAL TO THIS ITEM AND NO ADDITIONAL PAYMENT SHALL BE MADE.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO COMPLETE THIS WORK. THE QUANTITY IS TO BE USED AS DIRECTED BY THE ENGINEER.

ITEM 202 – REMOVAL MISC.: EXISTING TROLLEY RAILS REMOVED	<u>500</u> FEET
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ITEM 604 – MONUMENT BOX ADJUSTED TO GRADE, AS PER PLAN
ITEM 604 – CUYAHOGA COUNTY MONUMENT BOX
ITEM 604 – CUYAHOGA COUNTY CENTERLINE MONUMENT BOX ASSEMBLY, TYPE 1

WHERE, AS DETERMINED BY THE ENGINEER, EXISTING MONUMENT BOXES IN THE PAVEMENT ARE SUITABLE FOR REUSE, THEY SHALL REMAIN UNDISTURBED IN PLACE OR, IF/WHERE REQUIRED, THEY SHALL SIMPLY BE ADJUSTED TO GRADE.

EXISTING MONUMENT BOXES THAT ARE BOTH SUITABLE FOR REUSE AND CONFORM TO THE CUYAHOGA COUNTY ENGINEER’S CONSTRUCTION DRAWING MB–1C, MAY BE ADJUSTED USING "ADJUSTING RINGS " AS DETAILED ON THE MB–1C DRAWING. EXISTING MONUMENT BOXES THAT ARE DETERMINED BY THE ENGINEER TO BE UNSUITABLE FOR REUSE SHALL BE REMOVED AND REPLACED WITH A CUYAHOGA COUNTY MONUMENT BOX AS DETAILED ON THE CUYAHOGA COUNTY ENGINEER’S CONSTRUCTION DRAWING MB–1C, SET TO GRADE.

CARE AND/OR PROTECTIVE MEASURES SHALL BE EMPLOYED BY THE CONTRACTOR TO AVOID DAMAGE OR DISRUPTION OF EXISTING MONUMENTS DURING THE MONUMENT BOX ADJUSTMENT OR REMOVAL AND REPLACEMENT OPERATIONS AND ALL OTHER OPERATIONS IN THE PROXIMITY OF SAID MONUMENTS. THOSE MONUMENTS THAT ARE DISPLACED OR DAMAGED DUE TO THE CONTRACTOR’S NEGLIGENCE SHALL BE REMOVED AND REPLACED WITH A TYPE 1 CUYAHOGA COUNTY CENTERLINE MONUMENT BOX ASSEMBLY UNDER THE SUPERVISION OF A LICENSED SURVEYOR; ALL AT THE CONTRACTOR’S EXPENSE. WHERE NO EXISTING MONUMENTS ARE FOUND WITHIN SAID MONUMENT BOXES OR WHERE MONUMENTS ARE DISPLACED OR DAMAGED AT NO FAULT OF THE CONTRACTOR, THEY SHALL BE PLACED OR REPLACED IN ACCORDANCE WITH "ITEM 604 – CUYAHOGA COUNTY CENTERLINE MONUMENT BOX ASSEMBLY, TYPE 1", AS SPECIFIED BELOW; ALL AT THE DIRECTION OF THE ENGINEER.

ROADWAY (CONT.)

WHERE DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL REMOVE THE EXISTING MONUMENT ASSEMBLY IN ITS ENTIRETY AND SHALL INSTALL, EXCEPTING THE "MONUMENT PIN," THE ENTIRE MONUMENT ASSEMBLY, INCLUDING THE BOX AS DETAILED ON THE CUYAHOGA COUNTY ENGINEER'S CONSTRUCTION DRAWING MB-1C. INSTALLATION SHALL BE SUCH THAT WHEN THE CUYAHOGA COUNTY DEPARTMENT OF PUBLIC WORKS' SURVEYOR SUBSEQUENTLY PLACES THE "MONUMENT PIN," IT CAN BE CENTERED IN THE BOX. PAYMENT FOR THE ABOVE WORK SHALL BE MADE UNDER ITEM 604 - CUYAHOGA COUNTY CENTERLINE MONUMENT BOX ASSEMBLY, TYPE 1.

EXISTING CONCRETE (BLOCK-OUT) PAVEMENT REMOVED SHALL BE REPLACED USING MODERATE-SETTING CONCRETE (CLASS MS) OR, IF APPROVED/DIRECTED BY THE ENGINEER, FAST-SETTING CONCRETE (CLASS FS) PER 499.05 OF THE SPECIFICATIONS. ALL WORK SHALL BE PERFORMED AT THE DIRECTION OF THE ENGINEER. PAYMENT FOR THE ABOVE SHALL BE MADE AT THE PER EACH CONTRACT UNIT PRICE BID FOR "ITEM 604 - MONUMENT BOX ADJUSTED TO GRADE, AS PER PLAN, "ITEM 604 - CUYAHOGA COUNTY MONUMENT BOX" OR "ITEM 604 - CUYAHOGA COUNTY CENTERLINE MONUMENT BOX ASSEMBLY, TYPE 1"

MONUMENTS SHALL BE LEFT UNDISTURBED UNTIL THE CUYAHOGA COUNTY DEPARTMENT OF PUBLIC WORKS' SURVEY OFFICE HAS CHECKED/VERIFIED THE REFERENCES PROVIDED BY THE CONTRACTOR'S SURVEYOR. IN ALL CASES THE CONTRACTOR SHALL CONTACT THE CUYAHOGA COUNTY DEPARTMENT OF PUBLIC WORKS' SURVEY OFFICE AT (216) 348-3846 NO LESS THAN TWO (2) WEEKS PRIOR TO THE DISTURBANCE OF ANY EXISTING MONUMENTS.

THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 604 - MONUMENT BOX ADJUSTED TO GRADE, AS PER PLAN	<u>16</u>	EACH
ITEM 604 - CUYAHOGA COUNTY MONUMENT BOX	<u>2</u>	EACH
ITEM 604 - CUYAHOGA COUNTY CENTERLINE MONUMENT BOX ASSEMBLY, TYPE 1	<u>2</u>	EACH

ITEM 623 - CONSTRUCTION LAYOUT STAKES. AS PER PLAN

623.02 GENERAL

- 1.ALL HORIZONTAL CONTROL AND, IF NECESSARY, VERTICAL CONTROL REQUIRED FOR THE COMPLETE LAYOUT AND PERFORMANCE OF THE WORK UNDER THIS CONTRACT SHALL BE DONE BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE AND MAY BE PERIODICALLY REVIEWED BY THE ENGINEER. ANY INSPECTION OR REVIEWING OF THE CONTRACTOR'S LAYOUT BY THE ENGINEER AND THE ACCEPTANCE OF ALL OR ANY PART OF IT SHALL NOT RELIEVE THE CONTRACTOR OF HIS/HER RESPONSIBILITY TO PRODUCE THE PROPER WORK DIMENSIONS, GRADES AND ELEVATIONS. THE REQUIREMENTS OF SECTION 107.10 OF THE GENERAL PROVISIONS ALSO APPLY.
- 2.THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND, IF APPLICABLE, ALL GRADES, LINES AND LEVELS AS INDICATED ON THE DRAWINGS AND SPECIFICATIONS, AND HE/SHE SHALL REPORT ANY ERRORS OR INCONSISTENCIES IN THE ABOVE TO THE ENGINEER BEFORE COMMENCING WORK OR ORDERING ANY MATERIAL.
- 3.THE CONTRACTOR SHALL MARK (PAINT) OR STAKE THE PROJECT STATION NUMBERS AT THE INTERVALS SPECIFIED IN 623.02 AND AS DIRECTED OR APPROVED BY THE ENGINEER BEFORE COMMENCING THE WORK.
- 4.THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PRESERVATION OF ALL STAKES AND MARKS AND SHALL REPLACE THEM AT HIS/HER EXPENSE IF THEY ARE DAMAGED, LOST, DISPLACED OR REMOVED. THE CONTRACTOR SHALL USE COMPETENT PERSONNEL AND SUITABLE EQUIPMENT FOR THE LAYOUT WORK REQUIRED AND SHALL PROVIDE THAT IT IS DONE UNDER THE SUPERVISION OF A REGISTERED SURVEYOR.

623.03 BASIS OF PAYMENT

PAYMENT WILL BE MADE UNDER:

ITEM 623 - CONSTRUCTION LAYOUT STAKES, AS PER PLAN	(LUMP SUM)
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ITEM SPECIAL - MONUMENT REFERENCING

THIS ITEM SHALL CONSIST OF LOCATING AND REFERENCING ALL EXISTING PAVEMENT (ROADWAY AND/OR CENTERLINE) MONUMENTS WITHIN THE LIMITS OF THE PROPOSED RESURFACING WORK FOR THE PURPOSES OF RESETTING/ REESTABLISHING THEIR EXACT LOCATION IN THE EVENT THEY ARE DISPLACED OR DAMAGED DURING CONSTRUCTION. ALL WORK SHALL BE PERFORMED UNDER THE SUPERVISION OF A LICENSED SURVEYOR PROVIDED BY THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO THE START OF ANY PROXIMATE GRADING, PAVING OR OTHER SUCH OPERATIONS.

A THOROUGH SEARCH SHALL BE MADE TO LOCATE THE MONUMENTS WITHIN THE LIMITS OF THE PROPOSED RESURFACING WORK. UPON LOCATING THE EXISTING MONUMENTS, THE MONUMENT PINS/POINTS SHALL BE CAREFULLY EXPOSED AND REFERENCED TO NO LESS THAN THREE (3) PERMANENT OBJECTS. ANY EXISTING MONUMENTS THAT MIGHT REQUIRE REPLACEMENT SHALL BE NOTED. THE REFERENCES ALONG WITH ALL NOTATIONS SHALL BE DELIVERED TO THE CUYAHOGA COUNTY DEPARTMENT OF PUBLIC WORKS' SURVEY OFFICE IMMEDIATELY UPON COMPLETION. ADJUSTMENTS OR REPLACEMENTS OF MONUMENT BOXES OR ASSEMBLIES MAY NOT COMMENCE UNTIL THE REFERENCES HAVE BEEN CHECKED/ VERIFIED AND APPROVED BY THE CUYAHOGA COUNTY DEPARTMENT OF PUBLIC WORKS' SURVEY OFFICE.

THE PER EACH UNIT PRICE PAYMENT FOR THIS WORK SHALL INCLUDE LOCATING THE MONUMENTS, CAREFULLY CLEANING OUT THE MONUMENT BOXES AND/OR EXPOSING THE MONUMENT PINS/POINTS, REFERENCING THE MONUMENTS AND ALL RELATED NOTATION.

THE FOLLOWING ESTIMATED QUANTITY IS PROVIDED IN THE GENERAL SUMMARY FOR ALL MONUMENTS KNOWN TO EXIST OR INITIALLY FOUND/ENCOUNTERED WITHIN THE PROPOSED WORK LIMITS:

ITEM SPECIAL - MONUMENT REFERENCING	<u>20</u>	EACH
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ONTARIO STREET ABANDONED DRIVE APRONS

ON THE EAST SIDE OF ONTARIO STREET BETWEEN PROSPECT AVENUE AND THE SUSPEND PROJECT STA. 127+14 LOCATION (SEE SHEET 3), THERE ARE TWO (2) EXISTING ABANDONED COMMERCIAL DRIVE APRONS THAT ARE TO BE REMOVED AND REPLACED WITH GRANITE CURB AND SIDEWALK.

THE CONTRACTOR SHALL REMOVE AND DISPOSE OF THE DESIGNATED PORTIONS OF EXISTING DRIVE APRONS PER 202.05 OR 203.04(E), AS APPLICABLE, AND BY METHODS THAT WILL NOT DISTURB, DAMAGE OR UNDERMINE THE ADJACENT PAVEMENT, CURB, WALK OR THE PORTIONS OF EXISTING DRIVE APRONS INTENDED TO REMAIN IN PLACE. ANY ADJACENT FACILITIES DAMAGED AS A RESULT OF THE CONTRACTOR'S NEGLIGENCE, AS DETERMINED BY THE ENGINEER, AND WHICH ARE NOT OTHERWISE DESIGNATED FOR REPLACEMENT, SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE, ALL AT THE DIRECTION OF THE ENGINEER.

EXCEPT AS OTHERWISE DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL INSTALL ITEM 609 - CURB, MISC.: GRANITE CURB, AS PER PLAN ACROSS THE REMOVED DRIVE APRON PER THE "CURB TO BE USED ON STRAIGHT SECTION" DETAIL ON THE CITY OF CLEVELAND STANDARD CONSTRUCTION DRAWING CD-1. ITEM 608 - 6" CONCRETE WALK, AS PER PLAN SHALL BE INSTALLED PER THE "EXISTING WALK REMOVAL/REPLACEMENT" GENERAL NOTE ON THIS SHEET.

THE FOLLOWING ESTIMATED QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR PERFORMING THE ABOVE DESCRIBED APRON REMOVAL, CURB AND WALK INSTALLATION:

ITEM 202 - PAVEMENT REMOVED	<u>168</u>	SQ. YD.
ITEM 608 - 6" CONCRETE WALK, AS PER PLAN	<u>1,509</u>	SQ. FT.
ITEM 609 - CURB MISC.: GRANITE CURB, AS PER PLAN	<u>129</u>	FT.

EXISTING CONCRETE PLANTERS/GARBAGE CANS

THE CONTRACTOR SHALL CONTACT MICHAEL E. COX OF THE CLEVELAND PUBLIC WORKS DEPARTMENT AT (216) 664-2485 TWO (2) WEEKS PRIOR TO COMMENCING WORK ON THE PROJECT. THE CITY OF CLEVELAND PUBLIC WORKS DEPARTMENT SHALL COORDINATE THE REMOVAL AND REINSTALLATION OF ANY CONCRETE PLANTERS/GARBAGE CANS ALONG THE PROJECT THAT ARE IN THE WAY OF ANY PROPOSED CONSTRUCTION.

EXISTING PARKING METERS

EXISTING PARKING METERS LOCATED WITHIN THE PROJECT LIMITS ARE TO BE COVERED. THE CONTRACTOR SHALL CONTACT REGINALD MATTHEWS (MANAGER OF PARKING) AT 216-664-2748 AT LEAST ONE (1) WEEK PRIOR TO CONSTRUCTION BEGINNING TO COORDINATE.

EXISTING WALK REMOVAL/REPLACEMENT

AN ESTIMATED QUANTITY OF WALK REMOVAL AND REPLACEMENT HAS BEEN PROVIDED HEREIN IN ORDER TO BRING ALL CURB RAMPS INTO COMPLIANCE WITH THE AMERICAN WITH DISABILITIES ACT (ADA) REQUIREMENTS. SEE THE "ITEM 608 - CURB RAMP, MISC.: CITY OF CLEVELAND (CR-2, CR-3, CR-5) CURB RAMP, AS PER PLAN" NOTE ON SHEET 16. WALK REMOVAL AND REPLACEMENT IS ALSO INVOLVED WITH THE REMOVAL AND RESETTING OF THE EXISTING GRANITE CURB (SEE "ITEM SPECIAL - GRANITE CURB REMOVED AND RESET" GENERAL NOTE ON SHEET 34).

WALK REMOVAL/REPLACEMENT LIMITS SHALL TYPICALLY EXTEND TO THE NEAREST EXISTING WALK JOINT/DIVISION LINE UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE EXACT LOCATIONS AND LIMITS OF EXISTING WALK REMOVAL/REPLACEMENT SHALL BE ESTABLISHED AND DESIGNATED BY THE ENGINEER. AEROSOL SPRAY PAINT FOR OUTLINING SHALL BE PROVIDED BY THE CONTRACTOR.

EXISTING WALK SHALL BE REMOVED AND DISPOSED OF PER 202.05 AND IN A MANNER THAT WILL NOT DISTURB, DAMAGE OR UNDERMINE ADJACENT WALK OR DRIVES INTENDED TO REMAIN IN PLACE. ANY ADJACENT FACILITIES DAMAGED AS A RESULT OF THE CONTRACTOR'S NEGLIGENCE, AS DETERMINED BY THE ENGINEER, AND WHICH ARE NOT OTHERWISE DESIGNATED FOR REPLACEMENT, SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE; ALL AT THE DIRECTION OF THE ENGINEER.

REPLACEMENT WALK SHALL BE CONSTRUCTED IN ACCORDANCE WITH "ITEM 608 - 6" CONCRETE WALK, AS PER PLAN" (SEE PLAN NOTE ON SHEET 16).

GENERAL NOTES

2013 OPERATIONS GROUP 2



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ROADWAY (CONT.)

WHERE REQUIRED TO FACILITATE CURB RAMP CONSTRUCTION THROUGH EXISTING CURB, THE FOLLOWING METHOD SHALL BE EMPLOYED IN LIEU OF CONSTRUCTING THE CURB INTEGRALLY AS PART OF THE CURB RAMP (CLEVELAND STANDARD DRAWING CR-1):

EXISTING NON-INTEGRAL CURB SHALL BE COMPLETELY REMOVED (ITEM 202-CURB REMOVED) AND REPLACED (ITEM 609 – CURB, TYPE 6, AS PER PLAN OR ITEM 609 – CURB, MISC.: GRANITE CURB, AS PER PLAN) TO THE LIMITS SPECIFIED BY THE ENGINEER. NON-INTEGRAL CURB SHALL BE REMOVED AND DISPOSED OF PER ITEM 202.05 IN A MANNER AND BY THE METHODS THAT WILL NOT DISTURB, DAMAGE OR UNDERMINE THE ADJACENT PAVEMENT INTENDED TO REMAIN IN PLACE.

PROVIDE 1/2 INCH THICK EXPANSION JOINT MATERIAL (705.03) BETWEEN THE NEWLY INSTALLED CURB AND CURB RAMP WALK PER 608.03. CURB RAMP LOCATIONS AS SHOWN ON THE SCHEMATIC PLAN ARE SUBJECT TO ADJUSTMENT TO MINIMIZE CONFLICTS WITH EXISTING CATCH BASINS, MANHOLES, UTILITY POLES, HYDRANTS OR OTHER SUCH APPURTENANCES. ALSO SEE THE "ITEM 608-CURB RAMP, MISC.: CITY OF CLEVELAND (CR-2, CR-3, CR-5) CURB RAMP, AS PER PLAN" NOTE.

PAYMENT FOR THE ACTUALLY COMPLETED AND ACCEPTED QUANTITIES OF THE ABOVE WORK SHALL BE MADE AT THE CONTRACT UNIT PRICE BID FOR:

ITEM 202 – WALK REMOVED	SQ. FT.
ITEM 202 – CURB REMOVED	FT.
ITEM 608 – 6” CONCRETE WALK, AS PER PLAN	SQ. FT.
ITEM 608 – CURB RAMP, MISC.: CITY OF CLEVELAND (CR-2, CR-3, CR-5) CURB RAMP, AS PER PLAN	EACH
ITEM 609 – CURB, TYPE 6, AS PER PLAN	FT.
ITEM 609 – CURB, MISC.: GRANITE CURB, AS PER PLAN	FT.

IN CONJUNCTION WITH THE CURB RAMP ITEMS, THE FOLLOWING ESTIMATED REMOVAL AND REPLACEMENT ITEMS/QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 202 – WALK REMOVED	<u>9,828</u>	SQ. FT.
ITEM 202 – CURB REMOVED	<u>1,068</u>	FT.
ITEM 608 – 6” CONCRETE WALK, AS PER PLAN	<u>9,828</u>	SQ. FT.
ITEM 609 – CURB, TYPE 6, AS PER PLAN	<u>60</u>	FT.
ITEM 609 – CURB, MISC.: GRANITE CURB, AS PER PLAN	<u>1,068</u>	FT.

THE LIMITS OF ADDITIONAL CONTINGENCY WALK REMOVAL/REPLACEMENT REQUIRED TO FACILITATE ANY OTHER ADJACENT WORK NOT SHOWN OR SPECIFIED IN THE PLANS SHALL BE DESIGNATED BY THE ENGINEER. THIS WORK SHALL INCLUDE THE COST OF ANY SAW CUTS AND PRE-MOLDED JOINT FILLER REQUIRED BY THE ENGINEER TO COMPLETE THIS WORK.

THE FOLLOWING CONTINGENCY QUANITIES ARE CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER AND ARE IN ADDITION TO THE PLAN QUANTITIES TABULATED ABOVE:

ITEM 202 – WALK REMOVED	<u>180</u>	SQ. FT.
ITEM 608 – 6” CONCRETE WALK, AS PER PLAN	<u>180</u>	SQ. FT.

ITEM 608 – 6” CONCRETE WALK, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF 608, THE FOLLOWING SHALL APPLY:

THE "CLASS C" CONCRETE USED FOR SIDEWALK (608) SHALL CONFORM TO THE "MODIFICATIONS TO ITEM 499 CONCRETE – GENERAL" AS CONTAINED IN THE "SECTION 400 PROPOSAL NOTES" OF THE "CUYAHOGA COUNTY ENGINEER SPECIFICATION BOOKLET" AS WELL AS THE SPECIFICATIONS IN THE "CONCRETE DESIGN MIX (CITY OF CLEVELAND)" GENERAL NOTE ON SHEET 19.

ALL CONCRETE WALK SHALL BE A MINIMUM OF 6” THICK AND HAVE A 2” COMPACTED SCREENINGS BED WHICH MEETS THE REQUIREMENTS OF 703.10 (LIMITED TO CRUSHED STONE). THE COST FOR FURNISHING AND PLACING THE 2” COMPACTED SCREENINGS BED SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE BID FOR ITEM 608 – 6” CONCRETE WALK, AS PER PLAN.

PER 608.03(C), IT IS REQUIRED THAT 1/2 INCH THICK EXPANSION JOINT MATERIAL (705.03) BE INSTALLED BETWEEN THE WALK AND THE BACK OF CURB OR ANY OTHER FIXED OBJECT. IN ADDITION TO THE LOCATIONS SPECIFIED UNDER 608.03(C), TRANSVERSE EXPANSION JOINTS SHALL BE CONSTRUCTED AT INTERVALS OF NOT MORE THAN 25 TO 30 FEET UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE EXPANSION JOINT FILLER (705.03) SHALL BE PLACED AT THE TRANSVERSE EXPANSION JOINTS FOR THE FULL DEPTH/WIDTH OF THE CONCRETE WALK AND SHALL BE TRULY NORMAL TO GRADE. THE TOP 1/2 INCH OF THE EXPANSION JOINT PLACED BETWEEN THE WALK AND BACK OF CURB SHALL BE SEALED WITH 705.04 JOINT SEALER.

UNLESS OTHERWISE REQUIRED BY MUNICIPAL STANDARDS/ORDINANCES, THE FINAL SURFACE SHALL BE TEXTURED BY USE OF AN ACCEPTED BROOM SO AS TO PRODUCE A UNIFORM, GRITTY, TRANSVERSE TEXTURE, AFTER WHICH, THE CONTRACTOR IS REQUIRED TO RETRACE THE PREVIOUSLY FORMED JOINTS AND OUTSIDE EDGES OF THE WALK.

FINAL FINISH SHALL BE IN ACCORDANCE WITH THE APPLICABLE MUNICIPAL STANDARDS/ORDINANCES.

THE COST OF THE MATERIAL AND LABOR ASSOCIATED WITH THE ABOVE WORK IS INCLUDED IN THE CONTRACT UNIT PRICE BID FOR ITEM 608 – 6” CONCRETE WALK, AS PER PLAN.

ITEM 608 – CURB RAMP, MISC.: CITY OF CLEVELAND (CR-2, CR-3, CR-5) CURB RAMP, AS PER PLAN

ALL APPLICABLE PROVISIONS OF ITEM 608 SHALL APPLY EXCEPT AS MODIFIED HEREIN.

608.01 DESCRIPTION

THIS WORK SHALL CONSIST OF CONSTRUCTING AMERICANS WITH DISABILITIES ACT (ADA) COMPLIANT CURB RAMPS IN ACCORDANCE WITH THE CITY OF CLEVELAND STANDARD CONSTRUCTION DRAWING CR 1 (EXCEPT AS MODIFIED HEREIN) AT THE LOCATIONS SHOWN ON THE SCHEMATIC PLAN OR AT LOCATIONS DETERMINED BY THE ENGINEER.

608.02 MATERIALS

THE "CLASS C" CONCRETE USED FOR SIDEWALK (608) SHALL CONFORM TO THE "MODIFICATIONS TO ITEM 499 CONCRETE – GENERAL" AS CONTAINED IN THE "SECTION 400 PROPOSAL NOTES" OF THE "CUYAHOGA COUNTY ENGINEER SPECIFICATION BOOKLET" AS WELL AS THE SPECIFICATIONS IN THE "CONCRETE DESIGN MIX (CITY OF CLEVELAND)" GENERAL NOTE ON SHEET 19.

TRUNCATED DOME TILES

TRUNCATED DOME TILES TINTED RED AS MANUFACTURED BY ENGINEERED PLASTICS INC., TRUNCATED DOME TACTILE SYSTEMS AS MANUFACTURED BY ADA SOLUTIONS INC. OR APPROVED EQUAL SHALL BE INSTALLED AS DIRECTED BY THE ENGINEER.

608.07 CURB RAMPS

THE ENGINEER WILL DETERMINE THE EXACT LOCATIONS AND APPROPRIATE CURB RAMP AND/OR TRUNCATED DOME DESIGNS TO BE CONSTRUCTED. EXISTING CURB RAMPS THAT ARE NOT AFFECTED BY THE CONSTRUCTION AND ARE CURRENTLY ADA COMPLIANT NEED NOT BE REPLACED. AT THE COMPLETION OF THE PROJECT ALL CURB RAMPS WITHIN THE PROJECT LIMITS SHALL BE ADA COMPLIANT (EXCEPT POSSIBLY AT THE ONTARIO/HURON INTERSECTION, WHERE THE CURB RAMPS WILL BE CONSTRUCTED WITH THE ODOT SIGNAL IMPROVEMENT PID 88276 PROJECT).

THE ENGINEER SHALL CAREFULLY CONSIDER POTENTIAL IMPACTS TO UTILITIES AND ADJACENT PROPERTY OWNERS WHEN DETERMINING THE EXACT CURB RAMP LOCATIONS AND DESIGNS.

THICKNESS OF THE CURB RAMP WALK AND CONCRETE BASE BENEATH THE TRUNCATED DOMES SHALL BE A MINIMUM OF EIGHT (8) INCHES. IN ADDITION, A TWO (2) INCH COMPACTED SCREENINGS BED, AS SPECIFIED UNDER "ITEM 608 – 6” CONCRETE WALK, AS PER PLAN", SHALL BE FURNISHED AND PLACED BENEATH ALL CURB RAMP AREAS.

608.09 BASIS OF PAYMENT

PAYMENT WILL BE MADE UNDER ITEM 608 – CURB RAMP, MISC.: CITY OF CLEVELAND (CR-2, CR-3, CR-5) CURB RAMP, AS PER PLAN (EACH).

THE PER EACH PAYMENT FOR CURB RAMPS SHALL ALSO INCLUDE THICKENING THE CONCRETE WALK TO EIGHT (8) INCHES AND THE DETECTABLE WARNINGS (TRUNCATED DOME TILES).

PAYMENT FOR FURNISHING AND PLACING THE TWO (2) INCH COMPACTED SCREENINGS BED IS INCLUDED IN THE CONTRACT UNIT PRICE BID FOR ITEM 608 – 6” CONCRETE WALK, AS PER PLAN WHICH IS MEASURED THROUGH ALL CURB RAMP AREAS.

THE FOLLOWING ITEMS/ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS WORK:

ITEM 608 – CURB RAMP, MISC.: CITY OF CLEVELAND CR-2 CURB RAMP, AS PER PLAN	<u>1</u>	EACH
ITEM 608 – CURB RAMP, MISC.: CITY OF CLEVELAND CR-3 CURB RAMP, AS PER PLAN	<u>3</u>	EACH
ITEM 608 – CURB RAMP, MISC.: CITY OF CLEVELAND CR-5 CURB RAMP, AS PER PLAN	<u>64</u>	EACH

ITEM 203 – EMBANKMENT, AS PER PLAN

USE OF SLAG, IN ANY FORM, IS NOT PERMITTED.

EROSION CONTROL

ITEM 832 – EROSION CONTROL

THE FOLLOWING ESTIMATED QUANTITY IS PROVIDED FOR TEMPORARY SEDIMENT AND EROSION CONTROL (TSEC) IN ACCORDANCE WITH THE APPLICABLE REQUIREMENTS OF ODOT SUPPLEMENTAL SPECIFICATION 832:

ITEM 832 – EROSION CONTROL	<u>4,000</u>	EACH
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DRAINAGE

REVIEW OF DRAINAGE FACILITIES

BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE BY THE COUNTY, REPRESENTATIVES OF THE COUNTY AND THE CONTRACTOR, ALONG WITH LOCAL REPRESENTATIVES, SHALL MAKE AN INSPECTION OF ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCES SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING BY THE COUNTY.

ALL NEW CONDUITS, INLETS, CATCH BASINS, AND MANHOLES CONSTRUCTED AS A PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEAN CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE COUNTY.

ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE-MENTIONED PARTIES SHALL BE MAINTAINED AND LEFT IN A CONDITION REASONABLY COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. ANY CHANGE IN THE CONDITION RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 603 CONDUIT ITEMS.

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GENERAL NOTES

2013 OPERATIONS GROUP 2

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DRAINAGE (CONT.)

ITEM SPECIAL – MISCELLANEOUS METAL

THE ENGINEER SHALL INSPECT THE EXISTING CASTINGS ON ALL STRUCTURES WITHIN THE WORK LIMITS, EXCEPTING CASTINGS ON STRUCTURES OWNED BY PRIVATE COMPANIES AND CASTINGS ON STRUCTURES TO BE REMOVED AND/OR REPLACED. THOSE CASTINGS THAT THE ENGINEER DETERMINES ARE UNSUITABLE SHALL BE REPLACED. TO PROVIDE FOR THIS CONTINGENCY, AN ESTIMATED QUANTITY OF ITEM SPECIAL – MISCELLANEOUS METAL HAS BEEN INCLUDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER. IT SHALL BE THE CONTRACTOR’S RESPONSIBILITY TO PROVIDE THE CASTINGS OF THE REQUIRED TYPE, SIZE AND STRENGTH (HEAVY DUTY) FOR THE PARTICULAR STRUCTURE IN QUESTION. ALL CASTING MATERIALS SHALL CONFORM TO ITEM 604 OR ITEM 638, IF APPLICABLE, AND HAVE THE PRIOR APPROVAL OF THE ENGINEER.

WHERE IT IS NECESSARY TO REPLACE UNSUITABLE STORM/SANITARY SEWER MANHOLE CASTINGS (FRAMES AND COVERS), THEY SHALL BE REPLACED WITH CITY OF CLEVELAND STANDARD FRAMES AND COVERS PER STANDARD CONSTRUCTION DRAWINGS A–503, A–605 AND A–695.

WHERE THE ENGINEER DETERMINES CATCH BASIN GRATES ENCOUNTERED WITHIN THE WORK ARE NON–BICYCLE/PEDESTRIAN SAFE, THEY SHALL BE REMOVED AND REPLACED WITH THE APPROPRIATE BICYCLE/PEDESTRIAN SAFE GRATES AS AVAILABLE AND RECOMMENDED BY THE VARIOUS CASTING MANUFACTURERS AND APPROVED BY THE ENGINEER.

THE CONTRACTOR IS CAUTIONED TO USE EXTREME CARE IN THE REMOVAL, CLEANING, STORAGE AND RESETTING OF ALL EXISTING CASTINGS. CASTINGS DAMAGED BY THE NEGLIGENCE OF THE CONTRACTOR, AS DETERMINED BY THE ENGINEER, SHALL BE REPLACED WITH THE PROPER NEW CASTINGS AT THE CONTRACTOR’S EXPENSE.

EXCEPT WHERE/IF SPECIFICALLY REQUIRED AND SO SPECIFIED ELSEWHERE IN THE PLANS, THE CONTRACTOR SHALL NOT ORDER ANY OF THE ABOVE CASTINGS UNTIL DIRECTED BY THE ENGINEER, AND IN THE EVENT NO REPLACEMENT CASTINGS ARE REQUIRED, THE ITEM SHALL BE NONPERFORMED.

ALL COST ASSOCIATED WITH FURNISHING ACCEPTABLE NEW CASTING MATERIALS SHALL BE INCLUDED UNDER THIS ITEM. IN ADDITION, THE COST FOR SIMPLY INSTALLING NEW GRATES/COVERS ONLY SHALL BE CONSIDERED INCIDENTAL TO THIS ITEM. ALL COST ASSOCIATED WITH SETTING NEW CASTINGS (FRAMES AND COVERS/GRATES) TO GRADE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE PERTINENT STRUCTURE (604) ”ADJUSTED TO GRADE, AS PER PLAN” OR ”RECONSTRUCTED TO GRADE, AS PER PLAN”.

THE CONTRACTOR IS ADVISED THAT THE REPLACEMENT OF SOME EXISTING CASTINGS MAY BE SPECIAL ORDER/NON–STANDARD ITEMS AND MAY NEED ADDITIONAL TIME FOR DELIVERY. NO ADDITIONAL COSTS WILL BE PROVIDED FOR DELAYS IN RECEIVING THESE CASTINGS. THE OVER–SIZED NORTHEAST OHIO REGIONAL SEWER DISTRICT (NEORS) AND RECTANGULAR CLEVELAND PUBLIC POWER (CPP) CASTINGS HAVE BEEN ITEMIZED SEPARATELY FOR REPLACEMENT IF AUTHORIZED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

ITEM SPECIAL – MISCELLANEOUS METAL	<u>230,000</u> POUND
ITEM SPECIAL – MISCELLANEOUS METAL (NEORS)	<u>3,000</u> POUND
ITEM SPECIAL – MISCELLANEOUS METAL (CPP)	<u>2,000</u> POUND

EXISTING TEST TEES

STORM WATER DRAINAGE AND/OR ACTIVE SANITARY SEWER CONNECTION TEST TEES ENCOUNTERED WITHIN THE CONSTRUCTION LIMITS SHALL BE ADJUSTED TO GRADE, RECONSTRUCTED TO GRADE, ABANDONED WITH NEW TEST TEES INSTALLED AT ALTERNATE LOCATIONS, OR SIMPLY REPLACED, AS REQUIRED AT THE LOCATION SHOWN IN THE PLANS AND/OR DIRECTED BY THE ENGINEER; ALL IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF ITEM 604 AND THE DETAILS ON THE CUYAHOGA COUNTY ENGINEER’S CONSTRUCTION DRAWING MD–6C. WHEN IT BECOMES NECESSARY TO PLACE OR REPLACE TEST TEES WITHIN PAVED AREAS SUCH AS DRIVEWAYS, SIDEWALKS, ETC. (RELOCATE OUTSIDE PAVED AREAS WHERE POSSIBLE), THEY SHALL BE SET TO FINISHED PAVED SURFACE GRADE AS DETAILED ON THE CUYAHOGA COUNTY ENGINEER’S CONSTRUCTION DRAWING MD–6C AND/OR AS OTHERWISE SPECIFIED BY THE ENGINEER.

WHEN ADJUSTING/RECONSTRUCTING TEST TEES TO GRADE, NEW PIPE MATERIALS SHALL, TO THE EXTENT FEASIBLE, MATCH EXISTING IN–KIND MATERIAL (PVC FOR PVC; DUCTILE IRON FOR DUCTILE IRON; ETC.). MATERIAL SPECIFICATIONS ARE INDICATED ON MD–6. SEWER JOINTS SHALL CONFORM TO ASTM C–425 FOR CLAY PIPE, ASTM D–3212 FOR PLASTIC PIPE, AWWA C–111 FOR CAST IRON PIPE, AWWA C–111 FOR DUCTILE IRON PIPE. JOINTS FOR PVC PIPE SHALL BE ELASTOMERIC O–RING. SOLVENT CEMENT JOINTS FOR PIPES SIX (6) INCHES OR UNDER IS ACCEPTABLE. IF THE JOINT IS OF THE SOLVENT CEMENT TYPE, IT SHALL BE INSTALLED PER ASTM D–2235 AND THE MANUFACTURER’S RECOMMENDATIONS. ADDITIONALLY, ALL EXPOSED ENDS OF THE ABS COMPOSITE PIPE SHALL BE FULLY SEALED WITH SOLVENT CEMENT. ELASTOMERIC QUALITIES OF THE JOINT GASKETS OR O–RINGS SHALL MEET ASTM F–477. SOLVENT

CEMENT FOR PVC PIPING AND FITTINGS SHALL CONFORM TO ASTM D–2564. WELDED JOINTS SHOULD BE AIR TESTED 24 HOURS AFTER INSTALLATION.

WHEN SIMPLY REPLACING TEST TEES IN THE SAME LOCATION, THE COST FOR REMOVAL AND DISPOSAL OF THE EXISTING TEST TEE SHALL BE INCLUDED IN THE PAYMENT FOR THE NEW TEST TEE.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 202 – ABANDON MISC.: TEST TEE	<u>5</u> EACH
ITEM 604 – DRAINAGE STRUCTURE, MISC.: TEST TEE ADJUSTED TO GRADE	<u>5</u> EACH
ITEM 604 – DRAINAGE STRUCTURE, MISC.: TEST TEE RECONSTRUCTED TO GRADE	<u>5</u> EACH
ITEM 604 – DRAINAGE STRUCTURE, MISC.: TEST TEE, AS PER PLAN	<u>5</u> EACH

NONE OF THE ABOVE MATERIALS SHALL BE ORDERED BY THE CONTRACTOR UNTIL AUTHORIZED BY THE ENGINEER.

STRUCTURES ADJUSTED TO GRADE, RECONSTRUCTED TO GRADE OR REPLACED

ALL ADJUSTMENT, RECONSTRUCTION OR REPLACED WORK, EXCEPT FOR THOSE STRUCTURES OWNED BY PRIVATE COMPANIES, SHALL BE PERFORMED BY THE CONTRACTOR. WHERE APPLICABLE, THE TIME BETWEEN RESETTING THE CASTINGS AND RESURFACING SHALL BE KEPT TO AN ABSOLUTE MINIMUM.

CASTINGS SHALL BE ADJUSTED OR RECONSTRUCTED TO GRADE IN ACCORDANCE WITH 604.03 USING CLAY BRICKS (704.01), CLASS C CONCRETE AND/OR CONCRETE MORTAR, OR PER 638.18 AND AS SPECIFIED BELOW FOR SERVICE BOX AND VALVE BOX ADJUSTMENTS. NO GRADE RINGS OR METAL ADJUSTING RINGS (CASTINGS) ARE PERMITTED. MAXIMUM CONCRETE MORTAR THICKNESS IS 1–1/2”.

THE CONTRACTOR SHALL RESET EXISTING VALVE BOXES OR EXISTING CURB SHUT–OFF VALVE BOXES TO ESTABLISHED GRADE BY RAISING OR LOWERING THE EXISTING CASTINGS OR BY EITHER ADDING, DELETING OR CUTTING THE APPROPRIATE VALVE BOX STEM SECTIONS. IN RAISING OF THE CASTINGS, NO INSERTS WILL BE PERMITTED. ANY VALVE BOXES OR CURB SHUT–OFF VALVE BOXES FOUND TO BE DAMAGED OR UNSUITABLE FOR REUSE SHALL BE REPLACED BY THE CONTRACTOR AND PAID FOR UNDER ITEM SPECIAL – MISCELLANEOUS METAL. THE CONTRACTOR SHALL PERFORM ALL WORK NECESSARY AS REQUIRED OR AS ORDERED TO COMPLETE THE ITEM.

ADJUST TO GRADE, RECONSTRUCT TO GRADE OR TOTAL REPLACEMENT WORK SHALL, WHERE REQUIRED, INCLUDE THE REMOVAL AND REPLACEMENT OF ANY EXISTING CONCRETE BLOCKOUT CURB AND/OR PAVEMENT USING MODERATE – SETTING CONCRETE (CLASS MS) OR, IF APPROVED/DIRECTED BY THE ENGINEER, FAST– SETTING CONCRETE (CLASS FS) PER 499.05 OF THE SPECIFICATIONS. TO FACILITATE REMOVAL, THE BLOCKOUT PAVEMENTS SHALL BE SAWED FULL DEPTH (PER 255.03) ALONG THE LIMITS OF THEIR REMOVAL UNLESS OTHERWISE DESIGNATED/DIRECTED BY THE ENGINEER. UNLESS OTHERWISE APPROVED/DIRECTED BY THE ENGINEER, BLOCKOUT PAVEMENT, REINFORCING STEEL, JOINT MATERIAL AND LOAD TRANSFER DEVICES SHALL BE REPLACED/INSTALLED IN ACCORDANCE WITH THE CITY OF CLEVELAND CONSTRUCTION DRAWING CONC–1, CB–1 OR OTHER APPROPRIATE CATCH BASIN DRAWING(S). UNLESS OTHERWISE DIRECTED/APPROVED BY THE ENGINEER, REPLACE EXCAVATED SUBBASE AND SUBGRADE MATERIALS BELOW THE PROPOSED BLOCKOUT PAVEMENT WITH ”CLEVELAND LSM” (SEE ”BEDDING, BACKFILL AND JOINTS FOR ITEM 604 – STRUCTURES, AS PER PLAN” NOTE). REMOVALS/REPLACEMENTS SHALL BE CONSIDERED INCIDENTAL TO THE PER EACH BID ITEMS.

EXISTING PULL BOXES SHALL BE ADJUSTED TO GRADE IN ACCORDANCE WITH 604.03 USING CLAY BRICKS (704.01), CLASS ”C” CONCRETE AND/OR CONCRETE MORTAR (4” MINIMUM THICKNESS).

EXISTING WATER METER MANHOLES (OUTSIDE OF THE PAVEMENT WITH LIGHT DUTY/NON–TRAFFIC BEARING CASTINGS) THAT NEED TO BE ADJUSTED/ RECONSTRUCTED TO GRADE HAVE BEEN ITEMIZED SEPARATELY.

ALL EXISTING CASTINGS FOR STRUCTURES TO BE ADJUSTED OR RECONSTRUCTED TO GRADE SHALL BE FIELD CHECKED AT THE TIME OF CONSTRUCTION AND MARKED SUITABLE FOR SALVAGE AND REUSE OR REPLACED AS DIRECTED BY THE ENGINEER. UNLESS OTHERWISE INDICATED ON THE PLAN, REPLACEMENT CASTINGS ARE PAID UNDER ITEM SPECIAL – MISCELLANEOUS METAL.

THE ENGINEER WILL DETERMINE THE ITEM 604 WORK REQUIRED BASED ON THE GUIDELINES STIPULATED BELOW:

- ”ADJUST TO GRADE, AS PER PLAN” SHALL INCLUDE ALL WORK SPECIFIED IN 604.03 (B) AS NECESSARY TO RAISE THE EXISTING/NEW CASTING NO MORE THAN ONE (1) FOOT FROM ITS EXISTING ELEVATION OR TO LOWER THE EXISTING/NEW CASTING NO MORE THAN SIX (6) INCHES FROM ITS EXISTING ELEVATION. **IN ADDITION, THIS ITEM WILL INCLUDE ANY SUPPORTING WALL REPAIR WORK NECESSARY, UP TO SIX (6) INCHES BELOW THE TOP OF THE EXISTING SUPPORTING WALL.**

PAYMENT SHALL BE MADE UNDER THE APPROPRIATE STRUCTURE (ITEM 604) ADJUSTED TO GRADE, AS PER PLAN.

- ”RECONSTRUCT TO GRADE, AS PER PLAN” SHALL INCLUDE ALL WORK SPECIFIED IN 604.03(A) AND SHALL ALSO INCLUDE EXISTING/NEW CASTINGS RAISED MORE THAN ONE (1) FOOT FROM THEIR EXISTING ELEVATION OR LOWER THAN SIX (6) INCHES FROM THEIR EXISTING ELEVATION.

THE WORK LIMIT SHALL BE SPECIFIED BY THE ENGINEER AND PAYMENT SHALL BE MADE UNDER THE APPROPRIATE STRUCTURE (ITEM 604) RECONSTRUCTED TO GRADE, AS PER PLAN.

”RECONSTRUCT TO GRADE, AS PER PLAN” WORK SHALL NOT BE PERFORMED UNLESS SPECIFICALLY INDICATED IN THE PLANS OR OTHERWISE ORDERED BY THE ENGINEER. ANY SUCH WORK MADE NECESSARY DUE TO THE CONTRACTOR’S

NEGLIGENT OPERATIONS, AS DETERMINED BY THE ENGINEER, SHALL BE DONE AT THE CONTRACTOR’S EXPENSE.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 604 – CATCH BASIN ADJUSTED TO GRADE, AS PER PLAN	<u>50</u> EACH
ITEM 604 – CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN	<u>50</u> EACH
ITEM 604 – MANHOLE ADJUSTED TO GRADE, AS PER PLAN	<u>195</u> EACH
ITEM 604 – WATER METER MANHOLE ADJUSTED TO GRADE, AS PER PLAN	<u>1</u> EACH
ITEM 604 – MANHOLE RECONSTRUCTED TO GRADE, AS PER PLAN	<u>65</u> EACH
ITEM 604 – PULL BOX, 12” X 18” (CPP), ADJUSTED TO GRADE, AS PER PLAN	<u>4</u> EACH
ITEM 604 – PULL BOX, 13” X 24” (CLEVELAND TRAFFIC), ADJUSTED TO GRADE, AS PER PLAN	<u>10</u> EACH
ITEM 638 – VALVE BOX ADJUSTED TO GRADE, AS PER PLAN	<u>126</u> EACH
ITEM 638 – SERVICE BOX ADJUSTED TO GRADE, AS PER PLAN	<u>126</u> EACH

THE ENGINEER MAY DETERMINE THAT CERTAIN STRUCTURES SHALL BE REPLACED RATHER THAN BEING ADJUSTED OR RECONSTRUCTED. HIS DETERMINATION MAY BE BASED ON EITHER OR BOTH OF THESE JUDGMENTS:

- THE CONDITION OF THE STRUCTURE.
- THE COMPARATIVE CONTRACT PRICES OF THE ADJUST TO GRADE, RECONSTRUCT TO GRADE AND REPLACEMENT WORK ITEMS.

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DRAINAGE (CONT.)

WHERE TOTAL REPLACEMENT WORK IS DETERMINED TO BE PRUDENT BY THE ENGINEER, THE EXISTING STRUCTURE SHALL BE COMPLETELY REMOVED AND REPLACED IN KIND WITH A NEW STRUCTURE OF THE TYPE SPECIFIED BY THE ENGINEER AND IN ACCORDANCE WITH ITEM 604. NEW STORM OR SANITARY SEWER MANHOLE CASTINGS SHALL CONFORM WITH CITY OF CLEVELAND STANDARD FRAMES AND COVERS PER STANDARD CONSTRUCTION DRAWINGS A-503, A-605 AND 695.

THE FOLLOWING ESTIMATED QUANTITIES ARE PROVIDED FOR THIS CONTINGENCY:

ITEM 202 – CATCH BASIN REMOVED	___3___	EACH
ITEM 202 – MANHOLE REMOVED	___5___	EACH
ITEM 604 – MANHOLE, NO. 1, AS PER PLAN	___5___	EACH
ITEM 604 – CATCH BASIN, CITY OF CLEVELAND NO. 1, AS PER PLAN	___3___	EACH

NONE OF THE ABOVE MATERIALS SHALL BE ORDERED BY THE CONTRACTOR UNTIL AUTHORIZED BY THE ENGINEER.

THE COST OF REPLACING EXISTING CONDUIT CONNECTIONS, IF REQUIRED, SHALL BE PAID FOR BY SIZE AND TYPE SPECIFIED AND/OR DESIGNATED BY THE ENGINEER. DISTURBED CONNECTIONS SHALL BE REPLACED AT ONE PERCENT (1%) MINIMUM SLOPES. ALL NEW CATCH BASIN CONNECTIONS SHALL HAVE THREE (3) FEET OF MINIMUM COVER AND TIE DIRECTLY TO THE NEAREST STORM MANHOLE. AN ESTIMATED QUANTITY OF THE FOLLOWING IS PROVIDED IN THE GENERAL SUMMARY FOR THIS WORK:

ITEM 202 – PIPE REMOVED, 24” AND UNDER	___112___	FT.
ITEM 603 – 12” CONDUIT, TYPE B, 706.08 (ES), AS PER PLAN	___64___	FT.
ITEM 603 – 12” CONDUIT, TYPE C, 706.08 (ES), AS PER PLAN	___24___	FT.
ITEM 603 – 4” CONDUIT, TYPE F FOR UNDERDRAIN OUTLETS, AS PER PLAN	___24___	FT.

NONE OF THE ABOVE MATERIALS SHALL BE ORDERED BY THE CONTRACTOR UNTIL AUTHORIZED BY THE ENGINEER.

BEDDING, BACKFILL AND JOINTS FOR ITEM 603 – CONDUITS, AS PER PLAN AND ITEM 604 – STRUCTURES, AS PER PLAN

OUTSIDE PAVEMENT LIMITS

STRUCTURAL BACKFILL MATERIAL (703.11) FURNISHED FOR BEDDING AND BACKFILL OPERATIONS SHALL BE LIMITED TO CRUSHED CARBONATE STONE. GRANULAR EMBANKMENT MATERIAL (203.02.R) FURNISHED FOR NON-STRUCTURAL BACKFILL OPERATIONS SHALL BE LIMITED TO NATURAL GRANULAR MATERIAL (203.02.H) AND RECYCLED PORTLAND CEMENT CONCRETE (RPCC). IN ADDITION TO THE REQUIREMENTS OF 703.16, THE USE OF RPCC IS SUBJECT TO THE RESTRICTIONS OF 203.03 (B) AND 203.03 (E). BEDDING SHALL BE PLACED AND COMPACTED PER 603.06 AND 603.11. THE METHOD OF BACKFILLING SHALL BE PER 603.10 AND 603.11.

WITHIN PAVEMENT LIMITS

FOR ALL STRUCTURAL BACKFILL WITHIN PAVEMENT LIMITS THE MATERIAL MENTIONED ABOVE SHALL BE REPLACED WITH A FLOWABLE FILL OF CLEVELAND LSM (LOW STRENGTH MORTAR). THE LSM SHALL MEET THE FOLLOWING SPECIFICATIONS:

MATERIAL MUST COME FROM A PLANT WITH A CURRENT CERTIFICATE OF COMPLIANCE DEMONSTRATING THE ABILITY OF THE MIX DESIGN TO MEET THE SPECIFIED REQUIREMENTS. CERTIFICATES IN EXCESS OF ONE YEAR WILL NOT BE ACCEPTED. CERTIFICATES MUST CONTAIN THE NAME OF SUPPLIER, DATE, CONTRACT NUMBER AND MIX DESIGN DATA ON EACH DELIVERY TICKET.

ALL MATERIALS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS STATED HEREIN.

- 1.) CEMENT SHALL BE ASTM C-150 TYPE I.
- 2.) THE USE OF FLY ASH IS STRICTLY PROHIBITED
- 3.) FINE AGGREGATE SHALL CONFORM TO ODOT SPEC. 703.03 FINE AGGREGATE FOR MORTAR OR GROUT. THE USE OF SPENT FOUNDRY SAND OR CORE SAND IS STRICTLY PROHIBITED.

AN AIR ENHANCING ADMIXTURE SHALL BE INCORPORATED IN THE MIX THAT WILL HAVE THE EFFECT OF LOWERING THE WATER/CEMENT RATIO BETWEEN 95 AND 105 LBS/CUBIC FOOT. THE AIR ENTRAINED CONTENT OF THE MIX SHALL BE 30% TO ELIMINATE/MINIMIZE THE EXCESSIVE WATER AND SEGREGATION. COMPRESSIVE STRENGTHS WITH A RANGE OF 50 PSI TO 80 PSI AT 28 DAYS WILL BE REQUIRED IF ADDITIONAL EXCAVATION BY MACHINE OR HAND IS REQUIRED.

APPROVED ADMIXTURES:

MASTER BUILDERS	RHEOFILL
AXIM	FLOW AIR
W.R. GRACE	DARAFILL
APPROVED EQUAL	

MIX DESIGN PROPORTIONS

CEMENT (TYPE 1)	50 LBS./CU. YD.
SAND (SSD)	2475 LBS./CU. YD.
WATER	25 GALLONS/CU. YD.
ADMIXTURE (AIR)	3 OZ./CU. YD.

MATERIAL FOR PIPE BEDDING AND PIPE ZONE TO A DEPTH OF 12” (12 INCHES) OVER THE TOP OF PIPE SHALL BE AS SPECIFIED ABOVE FOR PIPES OUTSIDE PAVEMENT LIMITS. THE FLOWABLE FILL SHALL CONTINUE FROM 12” ABOVE THE TOP OF PIPE TO THE TOP OF THE PROPOSED SUBGRADE. ALL BOLTS AND VALVES EXPOSED IN THE TRENCH SHOULD BE WRAPPED WITH POLYETHYLENE MATERIAL CONFORMING TO ODOT 748.07 (8 MIL. THICK). COVER ALL JOINTS IN CLAY PIPE IN THE TRENCH AREA WITH POLYETHYLENE MATERIAL BEFORE POURING FLOWABLE FILL. REPAIR ALL OBSERVED OPENINGS IN ANY PIPE OR MANHOLE IN THE TRENCH AREA PRIOR TO BACKFILLING WITH TECHNIQUES IN ACCORDANCE WITH THE MAINTAINING AGENCIES STANDARD PROCEDURES.

PROVIDE 706.11 JOINTS FOR ALL CONCRETE PIPE (706.02) AND FOR ALL PRECAST MANHOLE OR CATCH BASIN SECTIONS. PROVIDE 706.12 JOINTS FOR CLAY PIPE (706.08ES).

EXISTING UNDERDRAINS

IT IS INTENDED NOT TO DISTURB ANY EXISTING UNDERDRAINS (THAT MAY BE PRESENT ALONG ANY SECTION OF THE PROJECT LENGTH) DURING THE CURB AND PAVEMENT REMOVAL AND REPLACEMENT OPERATIONS. ANY EXISTING UNDERDRAIN DAMAGED DURING THE REMOVAL OF EXISTING CURB AND PAVEMENT DUE TO THE CARELESSNESS OF THE CONTRACTOR SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT HIS OWN EXPENSE.

EXISTING UNDERDRAINS WHICH THE ENGINEER DETERMINES ARE ADVERSELY AFFECTED (THROUGH NO FAULT OF THE CONTRACTOR) BY THE CONSTRUCTION OF THIS IMPROVEMENT OR ARE FOUND TO BE OTHERWISE UNSUITABLE SHALL BE REMOVED AND REPLACED, IN KIND, (INCLUDING ALL NECESSARY BENDS OR BRANCHES), AT THE SAME TRENCH WIDTH AND DEPTH, UNLESS OTHERWISE SPECIFIED BY THE ENGINEER.

GRANULAR BACKFILL MATERIAL SHALL BE LIMITED TO No. 8 LIMESTONE.

THE COST FOR REMOVAL AND DISPOSAL OF EXISTING UNDERDRAIN PIPE/ OUTLET PIPE IS INCLUDED UNDER THESE ITEMS.

THE FOLLOWING ESTIMATED QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 603 – 4” CONDUIT, TYPE F FOR UNDERDRAIN OUTLETS, AS PER PLAN	___96___	FT.
ITEM 605 – 4” BASE PIPE UNDERDRAINS, 706.08(ES), AS PER PLAN	1,800	FT.

NONE OF THE ABOVE MATERIALS SHALL BE ORDERED BY THE CONTRACTOR UNTIL AUTHORIZED BY THE ENGINEER.

WATERWORK

ALL WATERWORK SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND DRAWINGS OF THE CITY OF CLEVELAND, DEPARTMENT OF PUBLIC UTILITIES, DIVISION OF WATER (C.W.D.), 1201 LAKESIDE AVENUE, CLEVELAND, OHIO. THE COST OF ALL LABOR, MATERIAL, EQUIPMENT, TOOLS, EXCAVATION, BEDDING AND BACKFILL FOR THE WATER WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE BID FOR EACH WATER WORK ITEM. THE REPLACEMENT OF PAVEMENT, WALKS, DRIVES, LANDSCAPING AND ALL OTHER WORK EXCEPTING THE ”WATERWORK” ITSELF, SHALL BE AS STIPULATED IN THE CONTRACT SPECIFICATIONS.

ALL PERMITS, FEES AND C.W.D. CHARGES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND THEIR ASSOCIATED COST SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE BID FOR THE PERTINENT WATERWORK ITEMS. THE CLEVELAND DIVISION OF WATER (CWD) CHARGES AND FEES MAY BE OBTAINED FROM THE CWD ”WATER DISTRIBUTION ENGINEERING OFFICE”.

PAVEMENT

ITEMS 301 AND 448, PG64-22, AS PER PLAN

USE OF RECLAIMED ASPHALT CONCRETE PAVEMENT IS NOT PERMITTED FOR SURFACE COURSES.

THE COARSE AGGREGATE SHALL BE CRUSHED CARBONATE STONE (CCS) AND/OR CRUSHED AIR COOLED BLAST FURNACE SLAG (ACBFS).

PRIOR TO PRODUCING THE ASPHALT CONCRETE FOR THIS CONTRACT, SUBMIT A JOB MIX FORMULA (JMF) FOR APPROVAL BY THE ENGINEER. USE A JMF THAT MEETS ALL REQUIREMENTS ESTABLISHED IN THE CONTRACT AND HAS PREVIOUSLY BEEN APPROVED FOR USE ON ODOT WORK. WHERE NO PREVIOUSLY APPROVED (BY ODOT) JMF IS AVAILABLE, DEVELOP A JMF MEETING ALL CRITERIA ESTABLISHED IN THE CONTRACT AND HAVE IT REVIEWED AND APPROVED BY AN INDEPENDENT TESTING LABORATORY PRIOR TO SUBMISSION TO THE ENGINEER. THE INDEPENDENT TESTING LABORATORY SHALL HAVE APPROPRIATELY APPROVED PERSONNEL AND TESTING EQUIPMENT PER ODOT SUPPLEMENT 1041. ALL COST ASSOCIATED WITH THE ABOVE SHALL BE INCLUDED IN THE CUBIC YARD COST OF THE ASPHALT CONCRETE BID ITEMS.

SAMPLING FOR VERIFICATION ACCEPTANCE (403.06.A) SHALL BE PERFORMED IN ACCORDANCE WITH ODOT SUPPLEMENT 1035 FOR ALL ITEM 448 MIXES.

UNLESS OTHERWISE DIRECTED/APPROVED BY THE ENGINEER, CONDUCT DENSITY GAUGE QUALITY CONTROL TESTING ON BOTH THE ITEM 448 ASPHALT CONCRETE SURFACE COURSE AND INTERMEDIATE COURSE MATS ACCORDING TO ODOT SUPPLEMENT 1055 REGARDLESS OF THE NUMBER OF LANES OR LENGTH OF CONTINUOUS PAVING.

ITEM 442 – ASPHALT CONCRETE SURFACE COURSE, 9.5MM, TYPE A (448), AS PER PLAN ITEM 442 – ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE A (448), AS PER PLAN

USE OF RECLAIMED ASPHALT CONCRETE PAVEMENT IS NOT PERMITTED FOR THE SURFACE COURSE.

THE COARSE AGGREGATE FOR THE SURFACE COURSE SHALL BE A BLEND OF FIFTY PERCENT (50%) MINIMUM CRUSHED AIR COOLED BLAST FURNACE SLAG (ACBFS) OR TRAP ROCK FROM ONTARIO WITH THE REMAINING PERCENTAGE COMPRISED OF CRUSHED CARBONATE STONE (CCS). THE COARSE AGGREGATE FOR THE INTERMEDIATE COURSE SHALL BE CRUSHED CARBONATE STONE (CCS) AND/OR CRUSHED AIR COOLED BLAST FURNACE SLAG (ACBFS).

PRIOR TO PRODUCING THE ASPHALT CONCRETE FOR THIS CONTRACT, SUBMIT A JOB MIX FORMULA (JMF) FOR APPROVAL BY THE ENGINEER. USE A JMF THAT MEETS ALL REQUIREMENTS ESTABLISHED IN THE CONTRACT AND HAS PREVIOUSLY BEEN APPROVED FOR USE ON ODOT WORK. WHERE NO PREVIOUSLY APPROVED (BY ODOT) JMF IS AVAILABLE, DEVELOP A JMF MEETING ALL CRITERIA ESTABLISHED IN THE CONTRACT AND HAVE IT REVIEWED AND APPROVED BY AN INDEPENDENT TESTING LABORATORY PRIOR TO SUBMISSION TO THE ENGINEER. THE INDEPENDENT TESTING LABORATORY SHALL HAVE APPROPRIATELY APPROVED PERSONNEL AND TESTING EQUIPMENT PER ODOT SUPPLEMENT 1041. ALL COST ASSOCIATED WITH THE ABOVE SHALL BE INCLUDED IN THE CUBIC YARD COST OF THE ASPHALT CONCRETE BID ITEMS.

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PAVEMENT (CONT.)

SAMPLING FOR VERIFICATION ACCEPTANCE (403.06.A) SHALL BE PERFORMED IN ACCORDANCE WITH ODOT SUPPLEMENT 1035.

UNLESS OTHERWISE DIRECTED/APPROVED BY THE ENGINEER, CONDUCT DENSITY GAUGE QUALITY CONTROL TESTING ON BOTH THE ITEM 442 ASPHALT CONCRETE SURFACE COURSE AND INTERMEDIATE COURSE MATS ACCORDING TO ODOT SUPPLEMENT 1055 REGARDLESS OF THE NUMBER OF LANES OR LENGTH OF CONTINUOUS PAVING.

ITEM 254 – PAVEMENT PLANING, ASPHALT CONCRETE

IN CONJUNCTION WITH THE ITEM 254 PLANING OPERATIONS, A QUANTITY OF THE FOLLOWING HAS BEEN ESTABLISHED FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 254 – PATCHING PLANED SURFACE 3,772 SQ. YD.

PAVEMENT REPAIR

WHEN THE PAVEMENT PLANING OPERATIONS HAVE BEEN ACCEPTABLY COMPLETED, THE ENGINEER WILL DESIGNATE THE LOCATIONS AND LIMITS OF ANY FURTHER PAVEMENT REPAIR REQUIRED AFTER HE/SHE HAS DETERMINED THAT THE REPAIR CANNOT BE MADE UNDER PAVEMENT ITEM 254 – PATCHING PLANED SURFACE. IN AREAS DESIGNATED FOR FURTHER PAVEMENT REPAIR, REMOVE THE REMAINING ASPHALT CONCRETE OVERLAY DOWN TO THE EXISTING RIGID (GRANITE, CONCRETE, SANDSTONE OR BRICK) BASE. THE ENGINEER WILL THEN EXAMINE THE EXISTING RIGID BASE AND DETERMINE THE LIMITS OF PARTIAL DEPTH PAVEMENT REPAIR (ITEM 251) OR FULL DEPTH PAVEMENT REPAIR (ITEM 255) REQUIRED. SEE PAVEMENT REPAIR DETAILS ON SHEET 27.

APPROVED METHODS FOR REMOVAL OF THE REMAINING (AFTER PLANING) ASPHALT CONCRETE OVERLAY AND/OR THE REMOVAL OF THE EXISTING RIGID BASE COURSES SHALL SATISFACTORILY ESTABLISH NEAT VERTICAL FACES ALONG THE ENTIRE PERIMETER OF THE RECTANGULARLY SHAPED REMOVAL AREA IN ORDER TO SUBSEQUENTLY PERMIT THE PROPER PLACEMENT AND COMPACTION OF THE ASPHALT CONCRETE REPLACEMENT MATERIALS.

BEFORE PLACING ANY ASPHALT CONCRETE REPLACEMENT MATERIALS, CLEAN ALL VERTICAL FACES OF THE EXISTING PAVEMENT AND COAT THEM WITH ASPHALT MATERIAL ACCORDING TO 401.14. ALSO APPLY ITEM 407 – TACK COAT (702.13) TO THE SURFACE OF THE REPAIRED/EXISTING RIGID BASE COURSES.

MATERIALS FOR THE REPLACEMENT OF THE ASPHALT CONCRETE LAYER(S) BETWEEN THE FINISHED PLANED SURFACE AND THE REPAIRED/EXISTING RIGID BASE COURSES SHALL BE AS DETAILED ON THE PAVEMENT REPAIR DETAILS (SHEET 27). PLACE AND COMPACT THE REPLACEMENT ASPHALT CONCRETE MATERIALS IN ONE OR MORE LIFTS IN ACCORDANCE WITH 252.04 OF THE SPECIFICATIONS.

REMOVAL OF THE REMAINING (AFTER PLANING) ASPHALT CONCRETE PAVEMENT LAYERS SHALL BE MEASURED AND PAID FOR PER SQUARE YARD IN ACCORDANCE WITH ITEM 202 – WEARING COURSE REMOVED, AS PER PLAN. REMOVAL OF THE EXISTING RIGID BASE COURSES (GRANITE, CONCRETE, SANDSTONE OR BRICK) SHALL BE MEASURED AND PAID FOR PER SQUARE YARD IN ACCORDANCE WITH 255.09 AND 255.10. THE REPLACEMENT OF THE ASPHALT CONCRETE PAVEMENT LAYER(S) BETWEEN THE FINISHED PLANED SURFACE AND THE EXISTING/REPAIRED RIGID BASE COURSES SHALL BE MEASURED AND PAID FOR PER CUBIC YARD IN ACCORDANCE WITH ITEM 301 – ASPHALT CONCRETE BASE, PG64–22, AS PER PLAN; ITEM 448 – ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1 OR 2, PG64–22, AS PER PLAN.

THE FOLLOWING ESTIMATED QUANTITIES TO BE USED AS DIRECTED BY THE ENGINEER ARE CARRIED TO THE GENERAL SUMMARY FOR THIS WORK AND PAYMENT IS INCLUDED IN THE CONTRACT UNIT BID PRICE FOR:

ITEM 202 – WEARING COURSE REMOVED, AS PER PLAN 7,688 SQ. YD.

ITEM 204 – EMBANKMENT, AS PER PLAN 110 CU. YD.

ITEM 204 – GRANULAR EMBANKMENT, AS PER PLAN 110 CU. YD.

ITEM 204 – GEOTEXTILE FABRIC 637 SQ. YD.

ITEM 251 – PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN 6,410 SQ. YD.

ITEM 255 – FULL DEPTH PAVEMENT SAWING 15,866 FT.

ITEM 255 – FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS MS, AS PER PLAN 6,346 SQ. YD.

ITEM 301 – ASPHALT CONCRETE BASE, PG64–22, AS PER PLAN 642 CU. YD.

ITEM 304 – AGGREGATE BASE, AS PER PLAN 456 CU. YD.

ITEM 407 – TACK COAT, 702.13 385 GALLON

ITEM 448 – ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64–22, AS PER PLAN 214 CU. YD.

ITEM 448 – ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG64–22, AS PER PLAN 428 CU. YD.

CONCRETE DESIGN MIX (CITY OF CLEVELAND)

ALL APPLICABLE WORK ITEMS SHALL BE BID USING THE CONCRETE MIX DESIGN SPECIFIED IN THIS SECTION. UNDER THIS SECTION OF THESE SPECIFICATIONS THE CONTRACTOR IS REQUIRED TO SUBMIT A SEPARATE MIX DESIGN FOR EACH COMBINATION OF CEMENT TYPE, AGGREGATE TYPE AND CONCRETE SUPPLIER THEY WILL USE UNDER THIS CONTRACT. EACH MIX SHALL BE DESIGNED IN ACCORDANCE WITH ASTM–C94–94 AND AS HEREIN MODIFIED.

REQUIREMENT	DESCRIPTION
MINIMUM TWENTY EIGHT (28)	4000 PSI FOR 28 DAYS COMPRESSIVE STRENGTH TEST. FOUR CYLINDERS WILL BE TAKEN AND TESTED AS PER ASTM C–39–04. ONE TO BE TESTED AT 7 DAYS AND THE REMAINING THREE WILL BE TESTED AT TWENTY EIGHT DAYS. ACCEPTANCE WILL BE BASED ON THE AVERAGE RESULTS OF THE THREE CYLINDERS.
MINIMUM CEMENT CONTENT	650 LBS. PER CUBIC YARD. THE CEMENT SHALL CONFORM TO ASTM C–150–04 OR C–595–04. IF ANY LIMESTONE IS USED, THEN THE CITY REQUIRES THAT ALL OF THE INFORMATION DESCRIBED IN ASTM C–150–04 BE FURNISHED.
WATER CEMENT RATIO	0.45 MAXIMUM
SLUMP	NOMINAL THREE INCHES (3”) AS PER ASTM C–94–04 (2”–4” ACTUAL). THE USE OF CHEMICAL ADMIXTURES MEETING ASTM C–494, TO INCREASE THE SLUMP TO A MAXIMUM OF 7”, MAY BE USED WITH PRIOR WRITTEN APPROVAL OF THE ENGINEER. IF THIS OPTION IS SELECTED THE ADMIXTURE AND RESULTANT MAXIMUM SLUMP SHALL BE SUBMITTED FOR APPROVAL.
AIR CONTENT	FOUR PERCENT (4%) TO SEVEN AND ONE HALF PERCENT (7.5%) ASTM C–173–04 OR C–231–04.
AGGREGATE SIZE	NO. 57 FOR COARSE AGGREGATE SHALL BE LIMESTONE, GRAVEL OR CRUSHED AIR COOLED BLAST FURNACE SLAG. BOTH COARSE AND FINE AGGREGATE PER ASTM C–33–04.

IF CRUSHED AIR–COOLED BLAST FURNACE SLAG IS USED IT SHALL MEET ALL OF THE REQUIREMENTS OF ODOT 703.01 AND 703.02. COPIES OF ALL TESTS AND CERTIFICATIONS FOR THE CRUSHED AIR–COOLED BLAST FURNACE SLAG, IF USED, SHALL BE SUBMITTED AS PART OF THE CONCRETE MIX DESIGN.

STEEL SLAG AGGREGATE (703.01E) IS NOT PERMITTED FOR USE AS AN AGGREGATE IN CONCRETE.

WHEN HIGH EARLY STRENGTH IS REQUIRED, ASTM C–150–04 TYPE III A CEMENT OR ADMIXTURES IN ACCORDANCE WITH ASTM C–494–04 SHALL BE USED.

THE ENGINEER WILL DETERMINE IF THE CONTRACTOR IS REQUIRED TO FURNISH A SIGNED AFFIDAVIT, IN TRIPLICATE, FROM EACH CONCRETE SUPPLIER TO THE CITY GIVING DRY WEIGHT AND TYPE OF CONCRETE, SATURATED SURFACE–DRY WEIGHT AND THE TYPE OF FINE AND COARSE AGGREGATE, AND QUANTITY, TYPE AND NAME OF EACH ADMIXTURE AND WEIGHT OF WATER PER CUBIC YARD OF CONCRETE. THE CONTRACTOR SHALL ALSO FURNISH TWENTY–EIGHT (28) DAY CYLINDER TESTS (PER TESTING SECTION) AS VERIFICATION THAT THE MATERIALS USED AND THE PROPORTIONS SELECTED WILL PRODUCE CONCRETE OF THE QUANTITY SPECIFIED.

HOT AND COLD WEATHER PROTECTION (BLANKETS, HEATERS, ICE, ETC.) SHALL BE INCLUDED IN THE UNIT PRICE BID.

THE CONTRACTOR IS REQUIRED TO COMPLY WITH ALL THE ABOVE REQUIREMENTS. THE CONTRACTOR SHALL ALSO REQUIRE THAT ALL OF THE SUB–CONTRACTORS PLACING CONCRETE UNDER THIS CONTRACT COMPLY WITH ALL OF THE ABOVE REQUIREMENTS.

THE ENGINEER MAY REQUIRE THE CONTRACTOR TO SUPPLY TEST RESULTS FOR MATERIALS USED WITHIN THE PROJECT LIMITS. CURRENT ODOT AND CITY OF CLEVELAND STANDARDS FOR TESTING ASPHALT CONCRETE, EMBANKMENT, ETC. MUST BE FOLLOWED, WITH ALL RESULTS BEING SUBMITTED TO THE ENGINEER.

ITEM 304 – AGGREGATE BASE, AS PER PLAN

MATERIAL FURNISHED FOR THIS ITEM SHALL BE LIMITED TO CRUSHED CARBONATE STONE.

PROFILE AND ALIGNMENT

THE PROPOSED PAVEMENT RESURFACING SHALL FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. THE PROPOSED ASPHALT CONCRETE OVERLAY SHALL HAVE A UNIFORM THICKNESS OF THREE (3) INCHES.

ITEM 251 – PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN

WHEN THIS ITEM IS CALLED FOR ON THE PLANS OR IN THE PROPOSAL, ALL APPLICABLE PROVISIONS OF ITEM 251, AS SET FORTH IN THE CONSTRUCTION AND MATERIAL SPECIFICATIONS, SHALL APPLY EXCEPT AS MODIFIED HEREIN.

251.02 REMOVAL OF EXISTING PAVEMENT.

APPROVED REMOVAL METHODS SHALL SATISFACTORILY ESTABLISH A NEAT VERTICAL FACE ALONG THE ENTIRE PERIMETER OF THE REPAIR AREA IN ORDER TO SUBSEQUENTLY PERMIT THE PROPER PLACEMENT AND COMPACTION OF THE ASPHALT CONCRETE PATCHING MATERIAL. UNLESS OTHERWISE SPECIFIED BY THE ENGINEER, REMOVAL DEPTHS SHALL VARY FROM A ONE AND A HALF (1–1/2) INCH MINIMUM TO A THREE (3) INCH MAXIMUM.

PARTIALLY EMBEDDED STEEL MESH EXPOSED SHALL BE WIRE–BRUSHED OR OTHERWISE CLEANED TO REMOVE ALL LOOSE RUST. LOOSENED OR TOTALLY EXPOSED WIRE MESH REINFORCING SHALL BE CUT AND REMOVED AS REQUIRED WITHOUT DISPLACEMENT OR DISRUPTION TO THE REINFORCEMENT AND/OR PAVEMENT TO REMAIN.

251.05 BASIS OF PAYMENT.

A QUANTITY FOR THE ABOVE WORK HAS BEEN ESTIMATED/CALCULATED IN THE "PAVEMENT REPAIR" GENERAL NOTE AND HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER. PAYMENT FOR THE ABOVE SHALL BE MADE AT THE CONTRACT UNIT PRICE BID PER SQUARE YARD FOR:

ITEM 251 – PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN SQ. YD.

ITEM 255 – FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS MS, AS PER PLAN

WHEN THIS ITEM IS CALLED FOR ON THE PLANS OR IN THE PROPOSAL, ALL APPLICABLE PROVISIONS OF ITEM 255, AS SET FORTH IN THE CONSTRUCTION AND MATERIAL SPECIFICATIONS, SHALL APPLY EXCEPT AS MODIFIED HEREIN AND AS OTHERWISE DETAILED OR SPECIFIED ON THE CUYAHOGA COUNTY ENGINEER’S CONSTRUCTION DRAWING BP–2.5C AND THE CITY OF CLEVELAND’S CONSTRUCTION DRAWING CONC–1.

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PAVEMENT (CONT.)

255.02 MATERIALS

THE "CLASS MS" CONCRETE USED FOR THE RIGID PAVEMENT REPLACEMENT (255) SHALL CONFORM TO THE "MODIFICATIONS TO ITEM 499 CONCRETE – GENERAL" AS CONTAINED IN THE "SECTION 400 PROPOSAL NOTES" OF THE "CUYAHOGA COUNTY ENGINEER SPECIFICATION BOOKLET" AS WELL AS THE SPECIFICATIONS CONTAINED IN THE "CONCRETE DESIGN MIX (CITY OF CLEVELAND)" PLAN NOTE.

255.04 CORRECTION OF DISTURBED SUBBASE AND SUBGRADE

SUITABLE SUBBASE DISTURBED IN AREAS WHERE CONCRETE PAVEMENT IS REMOVED SHALL BE SHAPED AND RECOMPACTED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE COUNTY. UNSUITABLE SUBBASE, AS DETERMINED BY THE ENGINEER, SHALL BE REMOVED AND REPLACED WITH "ITEM 304 – AGGREGATE BASE, AS PER PLAN" TO THE DEPTH OF ADJACENT SUBBASE.

WHERE UNSUITABLE SUBGRADE MATERIAL IS ENCOUNTERED, IT SHALL BE REMOVED TO THE DEPTH DETERMINED BY THE ENGINEER, AND REPLACED IN EIGHT (8) INCH MAXIMUM (LOOSE DEPTH) MECHANICALLY COMPACTED LAYERS. SUITABLE EMBANKMENT MATERIAL (204.02) REQUIRED TO REPLACE THE UNDERCUT SUBGRADE SHALL, TO THE EXTENT POSSIBLE, EXHIBIT THE SAME PHYSICAL PROPERTIES AS THE ADJACENT SOUND SUBGRADE MATERIALS. HOWEVER, USE OF SLAG, IN ANY FORM, IS **NOT** PERMITTED. ALL EXPOSED OR RECONSTRUCTED SUBGRADE SOILS SHALL BE COMPACTED TO THE SATISFACTION OF THE ENGINEER. IN CONJUNCTION WITH THE ABOVE SUBGRADE WORK, AN ESTIMATED QUANTITY OF ITEM 204 – GEOTEXTILE FABRIC IS PROVIDED FOR USE AS DIRECTED BY THE ENGINEER.

REMOVAL AND DISPOSAL OF UNSUITABLE SUBBASE OR SUBGRADE MATERIAL SHALL BE CONSIDERED INCIDENTAL TO ITEM 255 AND NO SEPARATE PAYMENT WILL BE MADE.

255.07 WEARING COURSE REPLACEMENT

MATERIAL FOR ANY ASPHALT CONCRETE OVERLAYS SHALL BE PAID FOR SEPARATELY AND BE AS DETAILED/SPECIFIED ELSEWHERE IN THE PLANS (SEE SHEETS 7, 8 & 27).

255.09 METHOD OF MEASUREMENT

THE REPLACEMENT MATERIAL FOR UNSUITABLE SUBGRADE MATERIAL SHALL BE FURNISHED, PLACED, MEASURED AND PAID FOR IN ACCORDANCE WITH ITEM 204 – EMBANKMENT, AS PER PLAN OR, WHERE DIRECTED BY THE ENGINEER, ITEM 204 – GRANULAR EMBANKMENT, AS PER PLAN. THE REPLACEMENT MATERIAL FOR UNSUITABLE SUBBASE SHALL BE FURNISHED, PLACED, MEASURED AND PAID FOR IN ACCORDANCE WITH ITEM 304 – AGGREGATE BASE, AS PER PLAN.

255.10 BASIS FOR PAYMENT

PAYMENT FOR ITEM 255 – FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS MS, AS PER PLAN IS FULL COMPENSATION FOR FURNISHING ALL MATERIALS AND LABOR PER 255.10 AND AS SPECIFIED HEREIN, INCLUDING BUT NOT LIMITED TO PAVEMENT REMOVAL, SUBBASE/SUBGRADE CORRECTION AND/OR REMOVAL, AS NECESSARY, FURNISHING AND PLACING DOWELS, TIE BARS, MESH AND THE CLASS MS CONCRETE.

QUANTITIES FOR THE ABOVE WORK ARE ESTIMATED/CALCULATED IN THE "PAVEMENT REPAIR" GENERAL NOTE ON SHEET NO. 19 AND CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER. PAYMENT FOR ACTUALLY COMPLETED AND ACCEPTED QUANTITIES SHALL BE MADE AT THE CONTRACT UNIT PRICE BID FOR:

ITEM 255 – FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS MS, AS PER PLAN	SQ. YD.
ITEM 255 – FULL DEPTH PAVEMENT SAWING	FT.
ITEM 204 – EMBANKMENT, AS PER PLAN	CU. YD.
ITEM 204 – GRANULAR EMBANKMENT, AS PER PLAN	CU. YD.
ITEM 204 – GEOTEXTILE FABRIC	SQ. YD.
ITEM 304 – AGGREGATE BASE, AS PER PLAN	CU. YD.

ITEM 407 – TACK COAT. TRACKLESS TACK. INTERMEDIATE COURSE. AS PER PLAN

ITEM 407 – TACK COAT. TRACKLESS TACK. SURFACE COURSE. AS PER PLAN

407.01 DESCRIPTION

THE WORK DESCRIBED UNDER THIS SPECIFICATION IS TO BE PERFORMED PRIOR TO THE PLACEMENT OF THE INTERMEDIATE OR SURFACE COURSE AS REQUIRED FOR PREPARING AND TREATING THE SURFACE WITH AN NTSS–IHM TRACKLESS TACK PRODUCED BY BLACKLIDGE EMULSIONS, INC. ALL REQUIREMENTS OF ODOT 407 TACK COAT SPECIFICATION IN THE CONSTRUCTION AND MATERIAL SPECIFICATION SHALL BE MET EXCEPT AS MODIFIED HEREIN:

407.02 MATERIALS

MATERIALS SHALL CONFORM TO THE FOLLOWING TYPICAL PHYSICAL PROPERTIES:

PARAMETER	TEST METHOD	MIN	MAX.
SAYBOLT FUROL VISCOSITY, SFS @ 25° C	ASTM D88	15	100
STORAGE STABILITY, 24 HRS, %	ASTM D244	–	1
STORAGE STABILITY, 5 DAYS, %	ASTM D244	–	5
RESIDUE BY DISTILLATION, %	ASTM D244	50	–
OIL DISTILLATE, %	ASTM D244	–	1
SIEVE TEST, %	ASTM D244	–	0.3

TEST ON RESIDUE:

PENETRATION @ 25°C	ASTM D5	–	20
SOFTENING POINT RANGE DEG C	ASTM D36	65	–
SOLUBILITY, %	ASTM D2042	97.5	–
ORIGINAL BINDER DSR @ 82°C G*/SIN δ, 10 RAD/SEC	AASHTO T111	1	–

NOTE: SUPPLY CERTIFIED TEST DATA TO THE ENGINEER SHOWING THE MATERIAL SUPPLIED WAS TESTED FOR AND MEETS THE PROPERTIES LISTED ABOVE. PRODUCT SHOULD NOT CONTAIN FILLER SUCH AS CLAY, ETC. KEEP FROM FREEZING.

407.03 EQUIPMENT

SEE MANUFACTURER’S REPRESENTATIVE FOR CORRECT DISTRIBUTOR SETTINGS.

NOTE: THIS PRODUCT (NTSS–IHM) IS NOT COMPATIBLE WITH CATIONIC EMULSIONS (CRS, CQS, CMS, CSS, EC). ALL EQUIPMENT SHOULD BE THOROUGHLY CLEANED IF CATIONIC EMULSION WAS PREVIOUSLY PRESENT. IF PRODUCT IS TO BE STORED FOR AN EXTENDED PERIOD OF TIME, PRIOR TO APPLICATION, THE MATERIAL SHOULD BE AGITATED OR GENTLY CIRCULATED PRIOR TO USE.

407.04 WEATHER LIMITATIONS

DO NOT APPLY THE ASPHALT MATERIAL IF THE SURFACE TEMPERATURE IS BELOW THE MINIMUM PLACEMENT TEMPERATURE FOR THE COURSE TO BE PLACED, AS SPECIFIED IN 401.06. NOTE: SUBJECT TO DAMAGE IF FROZEN. KEEP FROM FREEZING.

407.05 PREPARATION OF SURFACE

ENSURE THAT THE SURFACE IS THOROUGHLY CLEAN AND DRY WHEN THE ASPHALT MATERIAL IS APPLIED. REMOVE MATERIAL CLEANED FROM THE SURFACE AND DISPOSE OF, AS DIRECTED BY THE ENGINEER.

407.06 APPLICATION OF ASPHALT MATERIAL

UNIFORMLY APPLY THE ASPHALT MATERIAL WITH A DISTRIBUTOR PER THE REQUIREMENTS OF 407.06 EXCEPT AS NOTED. DILUTION IS **NOT** PERMITTED.

ALL NOZZLES AND SPRAY PATTERNS SHOULD BE IDENTICAL TO ONE ANOTHER ALONG THE DISTRIBUTOR SPRAY BAR. THE ANGLE OF THE NOZZLE SHOULD BE A 15 TO 30 DEGREE ANGLE TO THE SPRAY BAR AXIS TO MAXIMIZE OVERLAP OR AS RECOMMENDED BY THE NOZZLE MANUFACTURER. CONTACT THE MANUFACTURER’S REPRESENTATIVE FOR REQUIRED SPRAY NOZZLE SIZE AND DISTRIBUTOR AND NOZZLE SETTINGS.

APPLY PRODUCT AT AN AVERAGE RATE OF 0.04 TO 0.08 GALLON PER SQUARE YARD. RECOMMENDED APPLICATION TEMPERATURE IS 160°F TO 180°F. DO NOT EXCEED 180°F.

THE REQUIREMENT TO CONFORM TO 702.13 IS NOT APPLICABLE.

THE ENGINEER AND MANUFACTURER’S REPRESENTATIVE WILL APPROVE THE RATE OF APPLICATION, TEMPERATURE, DISTRIBUTOR SETTINGS AND AREAS TO BE TREATED BEFORE APPLICATION OF TACK COAT. THE ENGINEER WILL DETERMINE THE ACTUAL APPLICATION IN GALLON PER SQUARE YARD BY A CHECK ON THE PROJECT. THE APPLICATION IS CONSIDERED SATISFACTORY WHEN THE ACTUAL RATE IS WITHIN ±10% OF THE REQUIRED RATE AND THE MATERIAL IS APPLIED UNIFORMLY WITH NO VISIBLE EVIDENCE OF STREAKING OR RIDGING.

407.07 METHOD OF MEASUREMENT.

THE ENGINEER WILL VERIFY TRACKLESS TACK COAT BY THE NUMBER OF GALLONS OF UNDILUTED ASPHALT MATERIAL APPLIED.

407.08 BASIS OF PAYMENT.

THE ACCEPTED QUANTITY OF TRACKLESS TACK COAT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID PER GALLON FOR ITEM 407 – TACK COAT, TRACKLESS TACK, INTERMEDIATE COURSE, AS PER PLAN OR ITEM 407 – TACK COAT, TRACKLESS TACK, SURFACE COURSE, AS PER PLAN WHICH PRICE AND PAYMENT SHALL BE FULL COMPENSATION FOR FURNISHING ALL LABOR, MATERIALS, TOOLS, EQUIPMENT AND INCIDENTALS FOR DOING ALL WORK INVOLVED IN FURNISHING AND PLACING THE ASPHALT MATERIAL.

ITEM	UNIT	DESCRIPTION
407	GALLON	TACK COAT, TRACKLESS TACK, INTERMEDIATE COURSE, AS PER PLAN
407	GALLON	TACK COAT, TRACKLESS TACK, SURFACE COURSE, AS PER PLAN

ITEM 254 – PAVEMENT PLANING. PORTLAND CEMENT CONCRETE. AS PER PLAN

THE CONTRACTOR SHALL COORDINATE WITH THE CLEVELAND THERMAL ENERGY CORPORATION PRIOR TO BEGINNING ANY "ITEM 254 – PAVEMENT PLANING, PORTLAND CEMENT CONCRETE, AS PER PLAN" WORK. CLEVELAND THERMAL WILL DESIGNATE IN THE FIELD WHICH MANHOLE CONCRETE BLOCKOUTS ARE THEIR RESPONSIBILITY AND ARE THUS NOT TO BE PLANED BY THE CONTRACTOR.

ALL PROVISIONS OF ITEM 254, AS SET FORTH IN THE CONSTRUCTION AND MATERIAL SPECIFICATIONS, SHALL APPLY EXCEPT AS MODIFIED HEREIN.

254.01 DESCRIPTION.

THE WORK CONSISTS OF PLANING THE EXISTING **CONCRETE** PAVEMENT AND DISPOSING OF THE CUTTINGS.

254.06 METHOD OF MEASUREMENT.

THE ENGINEER WILL MEASURE THE QUANTITY OF **CONCRETE** PAVEMENT PLANING BY THE NUMBER OF SQUARE YARDS.

254.07 BASIS OF PAYMENT.

NO PAYMENT WILL BE MADE FOR REPAIRS DUE TO DAMAGE CAUSED BY PLANING OPERATIONS.

PAYMENT WILL BE MADE AT THE CONTRACT UNIT PRICE BID PER SQUARE YARD FOR THE ACCEPTED QUANTITY OF ITEM 254 – PAVEMENT PLANING, PORTLAND CEMENT CONCRETE, AS PER PLAN.

ITEM 609 – CURB. TYPE 6. AS PER PLAN
ITEM 609 – CURB. MISC.: GRANITE CURB. AS PER PLAN

THE "CLASS C" CONCRETE FOR THE TYPE 6 CURB SHALL CONFORM TO THE "MODIFICATIONS TO ITEM 499 CONCRETE – GENERAL" AS CONTAINED IN THE "SECTION 400 PROPOSAL NOTES" OF THE "CUYAHOGA COUNTY ENGINEER SPECIFICATION BOOKLET" AS WELL AS THE SPECIFICATIONS IN THE "CONCRETE DESIGN MIX (CITY OF CLEVELAND)" GENERAL NOTE ON SHEET 19.

BOTH THE TYPE 6 AND THE GRANITE CURB SHALL BE 10" IN WIDTH AND CONFORM TO THE CITY OF CLEVELAND STANDARD CONSTRUCTION DRAWING CD–1.

ANY CAVATIES BELOW THE PROPOSED TYPE 6 OR GRANITE CURB RESULTING FROM THE EXISTING CURB REMOVAL OPERATIONS SHALL BE BACKFILLED WITH "CLASS C" CONCRETE. THE COST OF THIS IS TO BE INCLUDED IN THE UNIT PRICE BID FOR "ITEM 609 – CURB, TYPE 6, AS PER PLAN" OR "ITEM 609 – CURB, MISC.: GRANITE CURB, AS PER PLAN."

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PLOT DATE: 11/06/12

GENERAL NOTES

2013 OPERATIONS GROUP 2



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PLOT DATE: 11/06/12

PAVEMENT (CONT.)

ANY RESULTING CAVITIES BEHIND THE PROPOSED TYPE 6 CURB OR GRANITE CURB WITHIN THE EXISTING UNDERDRAIN TRENCH SHALL BE BACKFILLED PER 605.03(C) WITH THE TOP 8” DEPTH CONSISTING OF 6” OF ITEM 608 – 6” CONCRETE WALK, AS PER PLAN ON 2” OF COMPACTED SCREENINGS BED (SEE PLAN NOTE). THE COST OF THIS IS TO BE INCLUDED IN THE UNIT PRICE BID FOR ”ITEM 609 – CURB, TYPE 6, AS PER PLAN” OR ”ITEM 609 – CURB, MISC.: GRANITE CURB, AS PER PLAN”.

IN LIEU OF INSTALLING EXPANSION JOINT MATERIAL PER BP-5.1, THOROUGHLY SEAL THE ABUTTING JOINT WHEN CURB IS PLACED ADJACENT TO EXISTING PAVEMENT.

MAINTENANCE OF TRAFFIC

ITEM 614 – MAINTAINING TRAFFIC. AS PER PLAN

RESTRICTIONS:

THE WORK WILL BE PERFORMED SEPARATELY ON BOTH ROADS AS PER THE ”CONSTRUCTION SEQUENCE” NOTE. ONTARIO AND EAST 9th STREETS WILL NOT BE WORKED ON CONCURRENTLY.

THE PAVEMENT PLANING (254) ITEMS AND THE SUBSEQUENT OVERLAY PAVING (407, 442 & 448) ITEMS SHALL BE PERFORMED BETWEEN THE NIGHT TIME HOURS OF 7:00 PM AND 6:00 AM MONDAY THROUGH FRIDAY OR ANYTIME ON SATURDAY & SUNDAY. BY THE END OF EACH NIGHT THAT PAVEMENT PLANING OPERATIONS ARE PERFORMED, ALL REQUIRED TEMPORARY RAMPING (ITEM 614 – ASPHALT CONCRETE FOR MAINTAINING TRAFFIC) SHALL BE ACCEPTABLY INSTALLED IN PLACE PER THE ”TEMPORARY RAMPING OF VERTICAL SURFACES” PLAN NOTE. ALL APPLICABLE WORK ZONE (ITEM 614) OR FINAL (ITEM 644) PAVEMENT MARKINGS SHALL BE ACCEPTABLY INSTALLED AS WELL. THE NIGHT WORK PROVISIONS OF 401.15 SHALL APPLY TO THE ABOVE.

WHEN AN EVENT IS SCHEDULED AT PROGRESSIVE FIELD, QUICKEN LOANS ARENA OR AT CLEVELAND BROWNS STADIUM, ONTARIO STREET FROM STA. 109+10 TO STA. 127+14 AND EAST 9th STREET FROM STA. 170+32 TO STA. 188+71 MUST HAVE NO CONSTRUCTION ACTIVITY FROM TWO (2) HOURS PRIOR TO THE EVENT THROUGH ONE (1) HOUR AFTER THE CONCLUSION OF THE EVENT.

THE CONTRACTOR SHOULD NOTE THAT THERE WILL BE VARIOUS DOWNTOWN ACTIVITIES (INCLUDING MARATHONS, PARADES, FESTIVALS, ETC.) DURING THE LIFE OF THIS CONTRACT. THE CONTRACTOR IS EXPECTED TO COORDINATE/ REARRANGE THEIR WORK ACTIVITIES AS NECESSARY TO ACCOMODATE THESE DOWNTOWN ACTIVITIES.

TWO WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES ON SIDE STREETS.

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES IN ACCORDANCE WITH 108.07 OF THE CUYAHOGA COUNTY ENGINEER’S GENERAL PROVISIONS. THE AMOUNT OF THESE LIQUIDATED DAMAGES WILL BE \$25,000 EACH CALENDAR DAY THAT THE ABOVE STIPULATED REQUIREMENTS ARE NOT BEING MET TO THE SATISFACTION OF THE ENGINEER.

ONTARIO STREET FROM STA. 109+10 (BEGIN PROJECT) TO STA. 127+14 (EUCLID AVE.):

A MINIMUM OF ONE 12 FOOT WIDE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE FINISHED/ ACCEPTED PLANED SURFACE OR THE COMPLETED PAVEMENT AND IN ACCORDANCE WITH SECTION 614 AND THE DETAILS AND NOTES ON STANDARD CONSTRUCTION DRAWINGS MT-95.30, MT-95.31 AND MT-95.32.

ONTARIO STREET FROM STA. 134+00 TO STA. 144+71:

THE CONTRACTOR MAY CHOOSE BETWEEN THE FOLLOWING TWO (2) METHODS OF MAINTAINING TRAFFIC:

(1) A MINIMUM OF ONE 12 FOOT WIDE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE FINISHED/ACCEPTED PLANED SURFACE OR THE COMPLETED PAVEMENT AND IN ACCORDANCE WITH SECTION 614 AND THE DETAILS AND NOTES ON STANDARD CONSTRUCTION DRAWINGS MT-95.31 AND MT-95.32.

(2) THE PORTION OF ONTARIO STREET MAY BE CLOSED TO NORTHBOUND TRAFFIC. NORTHBOUND POSTED DETOUR ROUTES SHALL BE FROM ROCKWELL AVENUE TO EAST 6TH STREET TO LAKESIDE AVENUE. THE CONTRACTOR SHALL FURNISH A DETOUR PLAN (INCLUDING THE ANTICIPATED DURATION) TO THE ENGINEER FOR WRITTEN APPROVAL PRIOR TO IMPLEMENTATION OF THIS PROPOSED NORTHBOUND ROAD CLOSURE. SOUTHBOUND TRAFFIC WOULD BE MAINTAINED.

EAST 9th STREET FROM STA. 170+32 TO STA. 188+71 AND FROM STA. 213+15 TO STA. 225+65:

A MINIMUM OF ONE 12 FOOT WIDE LANE OF TRAFFIC IN ONE DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE FINISHED/ ACCEPTED PLANED SURFACE OR THE COMPLETED PAVEMENT AND IN ACCORDANCE WITH SECTION 614 AND THE DETAILS AND NOTES ON STANDARD CONSTRUCTION DRAWINGS MT-95.31 AND MT-95.32.

EAST 9th STREET FROM STA. 190+61 TO STA. 213+15:

THE CONTRACTOR MAY CHOOSE BETWEEN THE FOLLOWING TWO (2) METHODS OF MAINTAINING TRAFFIC:

(1) A MINIMUM OF ONE 12 FOOT WIDE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE FINISHED/ACCEPTED PLANED SURFACE OR THE COMPLETED PAVEMENT AND IN ACCORDANCE WITH SECTION 614 AND THE DETAILS AND NOTES ON STANDARD CONSTRUCTION DRAWINGS MT-95.31 AND MT-95.32.

(2) THIS PORTION OF EAST 9TH STREET MAY BE CLOSED TO SOUTHBOUND TRAFFIC. THE POSTED DETOUR ROUTE FOR SOUTHBOUND TRAFFIC SHALL BE FROM LAKESIDE AVENUE TO EAST 12th STREET TO EUCLID AVENUE. THE CONTRACTOR SHALL FURNISH A DETOUR PLAN (INCLUDING THE ANTICIPATED DURATION) TO THE ENGINEER FOR WRITTEN APPROVAL PRIOR TO IMPLEMENTATION OF THIS PROPOSED SOUTHBOUND ROAD CLOSURE.

ACCESS FOR ABUTTING PROPERTIES AND ADJACENT STREETS SHALL BE MAINTAINED AT ALL TIMES THROUGH THE USE OF THE EXISTING PAVEMENT, THE FINISHED/ACCEPTED PLANED SURFACE OR THE COMPLETED PAVEMENT BY MAINTAINING ONE MINIMUM 12 FOOT WIDE LANE FOR NORTHBOUND TRAFFIC.

THE LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE KEPT TO A MINIMUM CONSISTENT WITH THE SPECIFICATION REQUIREMENTS FOR PROTECTION OF COMPLETED PAVEMENT COURSES, AND SHALL BE AT THE APPROVAL OF THE ENGINEER.

THE CONTRACTOR SHALL MAINTAIN SAFE AND SATISFACTORY ACCESS TO ABUTTING PROPERTY. THE CONTRACTOR SHALL MAINTAIN ADEQUATE PEDESTRIAN WALKS AT ALL INTERSECTIONS, OR WHEN NECESSARY DETOUR THEM TO THE NEAREST INTERSECTION PER MT-110.10, OR AS OTHERWISE DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL DIVERT TRAFFIC FROM NORMAL CHANNELS BY PLASTIC DRUMS, FLASHING ARROW BOARDS COMPLYING WITH SS 821, TRAFFIC SIGNS, FLAGGERS WHERE NECESSARY AND WORK ZONE PAVEMENT MARKINGS AS SHOWN ON MT-95.30, MT-95.31, MT-95.32, MT-95.50, MT-97.11, MT-99.20 AND MT-101.60, AS APPLICABLE OR AS OTHERWISE DIRECTED BY THE ENGINEER.

ALL CONSTRUCTION TRAFFIC CONTROL DEVICES USED FOR THIS PROJECT SHALL CONFORM TO THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, AND SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR, EXCEPT AS NOTED BELOW.

THE CONTRACTOR SHALL FURNISH AND MAINTAIN ALL NECESSARY SAFEGUARDS, SUCH AS TYPE III BARRICADES, LIGHTING, FLAGGERS, AND SUCH OTHER TRAFFIC CONTROL DEVICES AS PROVIDED IN ITEM 614, MAINTAINING TRAFFIC, SO AS TO AVOID DAMAGE AND/OR INJURY TO VEHICLES AND PERSONS USING THE ROADWAY DURING CONSTRUCTION.

EXISTING TRAFFIC CONTROL DEVICES (SIGNS AND/OR TRAFFIC SIGNALS), LOCATED WITHIN THE WORK AREA, WHICH ARE REQUIRED FOR INTERIM OR PERMANENT TRAFFIC CONTROL, SHALL BE RELOCATED TO POINTS APPROVED BY THE ENGINEER. APPROPRIATE TRAFFIC CONTROL DEVICES SHALL BE MAINTAINED, IN COMPLIANCE WITH THE MANUAL, AT ALL TIMES WHILE TRAFFIC IS MAINTAINED. THE COST OF RELOCATION, IF REQUIRED, SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 – MAINTAINING TRAFFIC, AS PER PLAN.

THE LENGTH AND DURATION OF LANE CLOSURES AND/OR TRAFFIC RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. THE INTENT IS TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614 – MAINTAINING TRAFFIC, AS PER PLAN, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 615 – ROADS FOR MAINTAINING TRAFFIC. AS PER PLAN

IN ADDITION TO THE APPLICABLE REQUIREMENTS OF ITEM 615, THIS ITEM INCLUDES ANY REQUIRED PREPARATORY WORK, MAINTENANCE AND THE SUBSEQUENT REMOVAL OF THE ITEMS LISTED IN THE ”ESTIMATED QUANTITIES FOR MAINTAINING TRAFFIC” PLAN NOTE. PAYMENT FOR ALL NECESSARY LABOR, EQUIPMENT AND ADDITIONAL MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT BID FOR ITEM 615 – ROADS FOR MAINTAINING TRAFFIC, AS PER PLAN.

ESTIMATED QUANTITIES FOR MAINTAINING TRAFFIC

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC:

410 – TRAFFIC COMPACTED SURFACE, TYPE A OR B, AS PER PLAN	<u>500</u>	CU. YD.
614 – ASPHALT CONCRETE FOR MAINTAINING TRAFFIC	<u>400</u>	CU. YD.
614 – PORTABLE CHANGEABLE MESSAGE SIGN	<u>24</u>	SIGN MONTH
614 – WORK ZONE CENTER LINE, CLASS II	<u>4.20</u>	MILE
614 – WORK ZONE EDGE LINE, CLASS I	<u>5.92</u>	MILE
614 – WORK ZONE CHANNELIZING LINE, CLASS I	<u>5,800</u>	FT.
614 – WORK ZONE STOP LINE, CLASS I	<u>3,400</u>	FT.
614 – WORK ZONE CROSSWALK LINE, CLASS I	<u>6,220</u>	FT.
614 – WORK ZONE LANE LINE, CLASS II	<u>12.20</u>	MILE
614 – WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS I	<u>1,500</u>	FT.
614 – WORK ZONE DOTTED LINE, CLASS I	<u>2,300</u>	FT.
616 – WATER	<u>10</u>	M GAL.
616 – CALCIUM CHLORIDE	<u>1</u>	TON
614 – WORK ZONE ARROW, CLASS I	<u>136</u>	EACH
614 – WORK ZONE ISLAND MARKING, CLASS I	<u>620</u>	SQ. FT.

USE OF SLAG IS NOT PERMITTED FOR ITEM 410 MATERIAL.

FOR ITEM 614 – PORTABLE CHANGEABLE MESSAGE SIGN, IT IS INTENDED THAT A MINIMUM OF FOUR (4) SIGNS BE SUPPLIED FOR APPROXIMATELY SIX (6) MONTHS EACH, TOTALING TWENTY FOUR (24) SIGN MONTHS.

MAINTENANCE OF TRAFFIC (CONT.)

NOTIFICATION

THE CONTRACTOR SHALL NOTIFY IN WRITING THE FOLLOWING AGENCIES AT LEAST ONE WEEK PRIOR TO THE START OF CONSTRUCTION, AND AT LEAST 72 HOURS BEFORE IMPLEMENTING ANY SUBSTANTIAL CHANGE IN TRAFFIC PATTERN OR CLOSING ANY STREET TO TRAFFIC:

THE CUYAHOGA COUNTY DEPARTMENT OF PUBLIC WORKS – PUBLIC INFORMATION OFFICER

THE OHIO DEPARTMENT OF TRANSPORTATION – DISTRICT 12 – PUBLIC INFORMATION OFFICE

THE GREATER CLEVELAND REGIONAL TRANSIT AUTHORITY

THE CUYAHOGA COUNTY BOARD OF MENTAL RETARDATION

THE UNITED STATES POSTAL SERVICE

THE CITY OF CLEVELAND BOARD OF EDUCATION

THE CITY OF CLEVELAND COMMISSIONER OF TRAFFIC ENGINEERING, COMMISSIONER OF ENGINEERING & CONSTRUCTION, COMMISSIONER OF EMERGENCY MEDICAL SERVICES, PUBLIC WORKS DIRECTOR, POLICE CHIEF AND FIRE CHIEF.

CONSTRUCTION TRAFFIC

ALL CONSTRUCTION TRAFFIC SHALL USE ACCEPTABLE TRUCK ROUTES TO ACCESS THE CONSTRUCTION AREA. USE OF LOCAL RESIDENTIAL STREETS IS STRICTLY PROHIBITED UNLESS ALLOWED IN WRITING BY THE LOCAL ENFORCEMENT AUTHORITY.

DETOUR SIGNING

IF THE CONTRACTOR ELECTS TO USE ALLOWABLE ROAD CLOSURES TO COMPLETE THE WORK, ADVANCE TRAFFIC SIGNING AND SUPPORTS, INCLUDING DETOUR SIGNING, CONSTRUCTION WORK ZONE APPROACH SIGNING, BARRICADES AND SIGNS ON BARRICADES SHOWN ON THE PLANS BEYOND THE WORK LIMITS SHALL BE FURNISHED, ERECTED, MAINTAINED, AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR.

FLUORESCENT ORANGE TYPE G SIGN SHEETING SHALL BE USED FOR ALL DETOUR AND CONSTRUCTION WARNING SIGNS. ALL ASSOCIATED COST SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 – MAINTAINING TRAFFIC, AS PER PLAN.

CONSTRUCTION WARNING SIGNS

IMMEDIATELY PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL INSTALL THE CONSTRUCTION WARNING SIGNS SHOWN ON SHEET 28.

AS A MINIMUM, G20–1 SIGNS FACING TRAFFIC ENTERING THE PROJECT, AND G20–2 SIGNS FACING TRAFFIC LEAVING THE PROJECT, SHALL BE PLACED AS SHOWN IN THE MANUAL. ADDITIONAL G20–1 SIGNS SHALL BE PLACED AFTER EACH MAJOR INTERSECTION, IN BOTH DIRECTIONS, AND AFTER EACH SUSPENSION AND RESUMPTION OF WORK.

ADDITIONALLY, A W20–1 SIGN SHALL BE PLACED ON EACH INTERSECTING STREET A MINIMUM OF 200 FEET IN ADVANCE OF THE PROJECT, AND ON THE APPROACHES TO THE PROJECT A MINIMUM OF 500 FEET IN ADVANCE OF THE WORK LIMITS. G20–2 SIGNS SHALL ALSO BE INSTALLED ON EACH MAJOR INTERSECTING STREET, FACING TRAFFIC LEAVING THE PROJECT, A MINIMUM OF 200 FEET FROM THE PROJECT.

THE TRAFFIC CONTROL DEVICES SHOWN ON MT–95.30, MT–95.31, MT–95.32, MT–95.50, MT–97.11, MT–101.60 OR MT–110.10, AS APPLICABLE, SHALL BE IN ADDITION TO THOSE INDICATED ABOVE. IF DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL ALSO INSTALL W8–11 "UNEVEN LANES", W8–7 "LOOSE GRAVEL", AND/OR W21–2 "FRESH OIL/TAR" SIGNS.

FLUORESCENT ORANGE TYPE G SIGN SHEETING SHALL BE USED FOR ALL CONSTRUCTION WARNING SIGNS.

ALL CONSTRUCTION WARNING SIGNS SHALL BE COVERED OR REMOVED WHEN NO LONGER APPLICABLE.

THE COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING ALL CONSTRUCTION WARNING SIGNS SHALL BE INCLUDED IN THE LUMP SUM BID FOR "ITEM 614 – MAINTAINING TRAFFIC, AS PER PLAN".

ITEM 614 – LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP–MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

FOR LANE CLOSURES: DURING INITIAL SET–UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG–TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP). IN GENERAL, LEOS SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.

WHEN CONSTRUCTION VEHICLES ARE ENTERING/EXITING THE ZONE DIRECTLY FROM/INTO AN OPEN LANE OF TRAFFIC. IF A LANE HAS BEEN CLOSED TO PROVIDE AN ACCELERATION/DECELERATION LANE FOR THE VEHICLE, THE LEO WILL NOT BE REQUIRED.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST’S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS’ DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO–WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS WITH PATROL CAR REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614 – LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614 – LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 140 HOUR

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW–UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614 – LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

TEMPORARY RAMPING OF VERTICAL SURFACES

IN ORDER TO PROVIDE FOR LOCAL ACCESS, LONGITUDINAL VERTICAL FACES ABUTTING DRIVES SHALL BE TEMPORARILY RAMPED. TRANSVERSE VERTICAL FACES SHALL BE TEMPORARILY RAMPED A MINIMUM OF TEN (10) FEET IN LENGTH AND TRAFFIC SHALL BE WARNED WITH W8–1 "BUMP" SIGNS IN ADVANCE OF THE RAMPED AREAS.

ALL CASTINGS ENCOUNTERED SHALL BE SET TO GRADE AND PAID FOR UNDER VARIOUS ITEMS DESCRIBED ELSEWHERE IN THE GENERAL NOTES OR SPECIFICATIONS. THE CASTING ELEVATION DIFFERENTIAL SHALL NOT BE GREATER THAN ONE (1) INCH WHEN EXPOSED TO TRAFFIC.

ALL TEMPORARY RAMPING SHALL BE INSTALLED, AT THE DIRECTION OF THE ENGINEER, USING ITEM 614 – ASPHALT CONCRETE FOR MAINTAINING TRAFFIC.

ITEM 614 – REPLACEMENT SIGN

FLAT SHEET SIGNS, USING TYPE G SIGN SHEETING, FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED BUT IN GOOD CONDITION SUBJECT TO THE APPROVAL OF THE ENGINEER.

WHEN ADDITIONAL SIGNS ARE FOUND BY THE ENGINEER TO BE NECESSARY FOR THE SAFE MAINTENANCE OF TRAFFIC, ABOVE AND BEYOND THE SIGNING SHOWN IN THE PLANS AND/OR ON THE STANDARD CONSTRUCTION DRAWINGS, THE CONTRACTOR SHALL FURNISH, ERECT, MAINTAIN AND REMOVE THE ADDITIONAL SIGNS UNDER THIS ITEM, AS DIRECTED BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 10 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

CONSTRUCTION SEQUENCE

THE RECONSTRUCTION OF THIS PROJECT SHALL BE ACCOMPLISHED AS TWO (2) SEPARATE CONSTRUCTION SECTIONS, AS OUTLINED BELOW:

SECTION I – ONTARIO STREET
SECTION II – EAST 9th STREET

THE CONTRACTOR SHALL COMPLETE ALL WORK IN SECTION I BEFORE BEGINNING ANY WORK IN SECTION II SUBJECT TO THE APPROVAL OF THE ENGINEER. NORMAL VEHICULAR TRAFFIC SHALL BE MAINTAINED AT ALL TIMES BEYOND THE WORK LIMITS OF THE SECTION CURRENTLY UNDER CONSTRUCTION.

THE CONTRACTOR SHALL BEGIN WORK IN SECTION I (ONTARIO STREET) FOLLOWED IN TURN BY SECTION II (EAST 9th STREET).

ONTARIO STREET IS TO BE COMPLETED BY JUNE 30, 2013.

IF ONTARIO STREET IS NOT COMPLETED TO THE SATISFACTION OF THE ENGINEER BY JUNE 30, 2013, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES IN ACCORDANCE WITH 108.07 OF THE CUYAHOGA COUNTY ENGINEER’S GENERAL PROVISIONS. THE AMOUNT OF THESE LIQUIDATED DAMAGES WILL BE \$25,000 EACH CALENDAR DAY AFTER JUNE 30, 2013 THAT ONTARIO STREET HAS NOT BEEN COMPLETED.

CONSTRUCTION ADJACENT TO COMMERCIAL DRIVES

ACCESS TO COMMERCIAL PROPERTY SHALL BE MAINTAINED AT ALL TIMES. THE PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS REQUIRED TO MAINTAIN COMMERCIAL ACCESS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR "ITEM 614 – MAINTAINING TRAFFIC, AS PER PLAN"; EXCEPT FOR THE SEPARATELY ESTIMATED ITEMS/QUANTITIES ADDITIONALLY PROVIDED IN THE PLANS FOR MAINTAINING TRAFFIC (ACCESS).

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GENERAL NOTES

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MAINTENANCE OF TRAFFIC (CONT.)

THE CONTRACTOR SHALL PLAN/STAGE ALL WORK TO MAINTAIN SAFE ACCESS TO COMMERCIAL PROPERTY AT ALL TIMES. PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT A PLAN FOR THE APPROVAL OF THE ENGINEER WHICH OUTLINES HIS/HER STRATEGY FOR THE MAINTENANCE OF SAFE ACCESS TO COMMERCIAL PROPERTY. EXCEPT AS NOTED ABOVE, ALL ASSOCIATED COSTS SHALL BE INCLUDED UNDER "ITEM 614 – MAINTAINING TRAFFIC, AS PER PLAN."

WORK ZONE MARKING SIGNS

WORK ZONE MARKING SIGNS (R4–1, R4–2, W8–H12, W8–H12a, AND/OR W8–11) SHOWN ON SHEET 28 SHALL BE INSTALLED AT THE LOCATION SHOWN, AS DESCRIBED IN SECTION 614.04.

THESE SIGNS SHALL BE ERECTED AND VISIBLE TO TRAFFIC ONLY WHEN, AND FOR THE DURATION OF THE PERIOD WHEN THE APPROPRIATE PAVEMENT MARKINGS ARE NOT PRESENT. THEY SHALL BE PROMPTLY COVERED OR REMOVED AFTER THE FINAL PAVEMENT MARKINGS HAVE BEEN INSTALLED.

NO SEPARATE PAYMENT FOR THESE SIGNS WILL BE MADE. THE COST OF FURNISHING, INSTALLING, MAINTAINING, AND REMOVING THESE SIGNS SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614–MAINTAINING TRAFFIC, AS PER PLAN.

TRAFFIC CONTROL

INSTALLATION OF PAVEMENT MARKINGS

THE CONTRACTOR MAY REDUCE THE NUMBER OF THROUGH TRAFFIC LANES BY 50%, AS DIRECTED BY THE ENGINEER, IN ORDER TO REMOVE PAVEMENT MARKINGS, OR TO INSTALL WORK ZONE OR PERMANENT PAVEMENT MARKINGS. HE SHALL LIMIT THE AFOREMENTIONED CLOSURE TO BETWEEN THE NIGHT TIME HOURS OF 7:00 P.M. AND 6:00 A.M., UNLESS OTHERWISE APPROVED BY THE ENGINEER.

ALL WORK ZONE PAVEMENT MARKINGS AND SIGNS REQUIRED FOR A PARTICULAR LANE CLOSURE OR TRAFFIC PATTERN SHALL BE INSTALLED ON A SINGLE WORK NIGHT, AND THE CORRESPONDING TRAFFIC PATTERN SHALL BE IMPLEMENTED IMMEDIATELY. IN ADDITION, THE REQUIREMENTS OF 614.11 (G) SHALL APPLY.

ALTHOUGH PERMANENT PAVEMENT MARKINGS ARE TO BE INSTALLED AT THE END OF CONSTRUCTION, PAVEMENT MARKING PLAN SHEETS HAVE NOT BEEN INCLUDED IN THE CONTRACT PLANS. IN LIEU OF A PAVEMENT MARKING PLAN, THE CONTRACTOR SHALL, PRIOR TO THE START OF CONSTRUCTION, PREPARE AN INVENTORY AND LOG OF ALL EXISTING PAVEMENT MARKINGS, INCLUDING STOP BARS AND CROSS WALKS ON SIDE STREETS, LANE WIDTHS, NO PASSING ZONES AND CHANNELIZATIONS, FOR USE IN RESTORING THE MARKINGS AT THE END OF CONSTRUCTION. HE SHALL DELIVER TWO (2) COPIES OF THE INVENTORY AND LOG TO THE ENGINEER BEFORE BEGINNING ANY PAVEMENT REMOVALS. NOTE THAT THE CONSTRUCTION OF ADA COMPLIANT CURB RAMPS MAY NECESSITATE ADJUSTMENTS OF THE CROSSWALK AND/OR STOP BAR LOCATIONS, ALL AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LAYOUT OF THE VARIOUS FINAL PAVEMENT MARKINGS, INCLUDING LOCATION OF NO PASSING ZONES, CENTER LINES, LANE LINES, AND EDGE LINES ON THE FINAL SURFACE COURSE, IN ACCORDANCE WITH SECTION 641.06.

THE FINAL PAVEMENT MARKINGS SHALL BE PLACED WITHIN TWO (2) WORKING DAYS AFTER THE FINAL SURFACE COURSE IS COMPLETE AND AFTER THE LOOP DETECTORS HAVE BEEN INSTALLED. UNLESS DIRECTED OTHERWISE BY THE ENGINEER, THE FINAL PAVEMENT MARKINGS SHALL BE RESTORED IN THEIR ORIGINAL PATTERNS AND LOCATIONS, EXCEPT AS NOTED ABOVE FOR ADA COMPLIANT CURB RAMP CONSTRUCTION.

THE COST OF LOGGING AND ALL PREMARKING SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS PAVEMENT MARKING ITEMS. NO SEPARATE PAYMENT WILL BE MADE.

THE FOLLOWING QUANTITIES ARE INCLUDED IN THE GENERAL SUMMARY, AS ESTIMATES OF THE TYPE AND QUANTITY OF THE VARIOUS PAVEMENT MARKINGS TO BE INSTALLED UNDER ITEM 644. PAYMENT SHALL BE BASED ON THE MEASUREMENTS OF THE FINAL QUANTITIES INSTALLED:

ITEM 644 – CENTER LINE	2.10	MILE
ITEM 644 – CHANNELIZING LINE, 8"	2,900	FT.
ITEM 644 – STOP LINE	1,700	FT.
ITEM 644 – CROSSWALK LINE	3,110	FT.
ITEM 644 – TRASVERSE/DIAGONAL LINE	750	FT.
ITEM 644 – EDGE LINE, 4"	0.15	MILE
ITEM 644 – LANE LINE, 4"	6.10	MILE
ITEM 644 – ISLAND MARKING	310	SQ. FT.
ITEM 644 – LANE ARROW	68	EACH
ITEM 644 – DOTTED LINE, 4"	1,150	FT.

ITEM 632 – DETECTOR LOOP, AS PER PLAN

ALTHOUGH TRAFFIC SIGNAL DETECTOR LOOPS ARE TO BE INSTALLED AT THE END OF CONSTRUCTION, A PLAN SHOWING THE SIZE AND LOCATION OF THE VARIOUS LOOPS HAS NOT BEEN INCLUDED IN THE CONTRACT PLANS. IN LIEU OF A TRAFFIC SIGNAL PLAN, THE CONTRACTOR SHALL, PRIOR TO THE START OF CONSTRUCTION, PREPARE AN INVENTORY AND LOG OF ANY AND ALL EXISTING DETECTOR LOOPS, FOR USE IN RESTORING THEM AT THE END OF CONSTRUCTION. HE SHALL DELIVER TWO COPIES OF THE INVENTORY AND LOG TO THE ENGINEER BEFORE BEGINNING ANY PAVEMENT REMOVALS

THE ENGINEER WILL COORDINATE THE INSTALLATION OF THE DETECTOR LOOPS WITH THE CITY OF CLEVELAND PUBLIC WORKS DEPARTMENT (216–664–2485).

REPLACE ALL EXISTING STOP LINE INDUCTANCE DETECTOR LOOPS PER THE POWERHEAD CONFIGURATION SHOWN ON TC–82.10. THE WIDTH SHALL BE AS SPECIFIED ON TC–82.10 AND THE LENGTH SHALL BE AS INDICATED BELOW. THE STOP LINE DETECTOR LOOPS SHALL NOT BE WIRED TO ANY OTHER LOOPS AND SHALL HAVE ITS OWN DETECTOR CHANNEL. THE LOCATION OF THESE LOOPS SHALL BE SUCH THAT THE POWERHEAD IS LOCATED AT THE STOP LINE, NOT PAST IT. SIGN R10–22 AND PAVEMENT MARKING FIGURE 9C–7 FROM PAGE 9C–10 IN SECTION 9C.05 OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES SHALL BE INSTALLED TO DENOTE THE POWERHEAD LOCATION.

SYSTEM LOOPS DIMENSIONS SHALL BE 6’ X 6’, CENTERED IN THE LANE AND THE ANGULAR DESIGN (ADD) LOOP AS SHOWN ON TC–82.10.

ALL STOP LINE DETECTION SHALL BE TESTED FOR A BICYCLE TARGET AND ALL DILEMMA ZONES SHALL BE TESTED FOR A MOTORCYCLE TARGET.

THE INSTALLATION OF POURED EPOXY INSULATED SPLICES BETWEEN THE LOOP DETECTOR WIRES AND THE EXISTING LOOP DETECTOR LEAD IN CABLE SHALL BE CONSIDERED AS INCIDENTAL TO THIS ITEM OF WORK. NO SEPARATE PAYMENT FOR THESE SPLICES WILL BE MADE.

THE FOLLOWING ESTIMATED QUANTITY IS CARRIED TO THE GENERAL SUMMARY AS A CONTINGENCY QUANTITY WHICH IS INCLUDED FOR USE ONLY AND IN AMOUNTS AS DIRECTED BY THE ENGINEER. THE PROVISIONS OF SECTION 104.02 WILL APPLY TO THIS ITEM. THE AMOUNT OF THIS ITEM AND THE LOCATIONS WHERE USED SHALL BE RECORDED AS USED, AND PAYMENT WILL BE BASED ON FINAL MEASUREMENTS.

ITEM 632 – DETECTOR LOOP, AS PER PLAN	30	EACH
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GENERAL NOTES

2013 OPERATIONS GROUP 2

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PLOT DATE: 10-22-12

REF. NO.	SHEET NUMBER																				GRAND TOTAL	UNIT	ITEM NO.	ITEM EXTENSION	DESCRIPTION	FOR AS PER PLAN OR ITEM SPECIAL, SEE SHEET NO.
							14	15	16	17	18	19														
1								168													168	SQ. YD.	202	23000	PAVEMENT REMOVED	
2												7,688									7,688	SQ. YD.	202	23501	WEARING COURSE REMOVED, AS PER PLAN	19
3									10,008												10,008	SQ. FT.	202	30000	WALK REMOVED	
4									1,068												1,068	FT.	202	32000	CURB REMOVED	
5												112									112	FT.	202	35100	PIPE REMOVED, 24" AND UNDER	
6												5									5	EACH	202	58000	MANHOLE REMOVED	
7												3									3	EACH	202	58100	CATCH BASIN REMOVED	
8							500														500	FT.	202	98200	REMOVAL MISC.: EXISTING TROLLEY RAILS REMOVED	
9										5											5	EACH	202	98600	ABANDON MISC.: TEST TEE	
10													110								110	CU. YD.	204	20001	EMBANKMENT, AS PER PLAN	20
11													110								110	CU. YD.	204	21001	GRANULAR EMBANKMENT, AS PER PLAN	20
12													637								637	SQ. YD.	204	50000	GEOTEXTILE FABRIC	
13								2													2	EACH	604	38504	CUYAHOGA COUNTY CENTERLINE MONUMENT BOX ASSEMBLY, TYPE 1	
14								2													2	EACH	604	38600	CUYAHOGA COUNTY MONUMENT BOX	
15								16													16	EACH	604	39501	MONUMENT BOX ADJUSTED TO GRADE, AS PER PLAN	14 & 15
16								1,509	10,008												11,517	SQ. FT.	608	13001	6" CONCRETE WALK, AS PER PLAN	16
17									1												1	EACH	608	97200	CURB RAMP, MISC.: CITY OF CLEVELAND, CR-2 CURB RAMP, AS PER PLAN	16
18									3												3	EACH	608	97200	CURB RAMP, MISC.: CITY OF CLEVELAND, CR-3 CURB RAMP, AS PER PLAN	16
19									64												64	EACH	608	97200	CURB RAMP, MISC.: CITY OF CLEVELAND, CR-5 CURB RAMP, AS PER PLAN	16
20								20													20	EACH	SPECIAL	60440500	MONUMENT REFERENCING	15
21									4,000												4,000	EACH	832	30000	EROSION CONTROL	
22												120									120	FT.	603	00411	4" CONDUIT, TYPE F FOR UNDERDRAIN OUTLETS, AS PER PLAN	17 & 18
23												64									64	FT.	603	04401	12" CONDUIT, TYPE B, 706.08 (ES), AS PER PLAN	17 & 18
24												24									24	FT.	603	04601	12" CONDUIT, TYPE C, 706.08 (ES), AS PER PLAN	17 & 18
25												3									3	EACH	604	00301	CATCH BASIN, CITY OF CLEVELAND NO. 1, AS PER PLAN	17 & 18
26												50									50	EACH	604	09001	CATCH BASIN ADJUSTED TO GRADE, AS PER PLAN	17 & 18
27												50									50	EACH	604	09501	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN	17 & 18
28												5									5	EACH	604	30101	MANHOLE, NO. 1, AS PER PLAN	17 & 18
29												195									195	EACH	604	34501	MANHOLE ADJUSTED TO GRADE, AS PER PLAN	17 & 18
30												1									1	EACH	604	34501	WATER METER MANHOLE ADJUSTED TO GRADE, AS PER PLAN	17 & 18
31												4									4	EACH	604	34501	PULL BOX, 12" X 18" (CPP), ADJUSTED TO GRADE, AS PER PLAN	17 & 18
32												10									10	EACH	604	34501	PULL BOX, 13" X 24" (CLEVELAND TRAFFIC), ADJUSTED TO GRADE,	17 & 18
																									AS PER PLAN	
33												65									65	EACH	604	35501	MANHOLE RECONSTRUCTED TO GRADE, AS PER PLAN	17 & 18
34												5									5	EACH	604	98000	DRAINAGE STRUCTURE, MISC.: TEST TEE ADJUSTED TO GRADE	
35												5									5	EACH	604	98000	DRAINAGE STRUCTURE, MISC.: TEST TEE RECONSTRUCTED TO GRADE	
36												5									5	EACH	604	98001	DRAINAGE STRUCTURE, MISC.: TEST TEE, AS PER PLAN	17 & 18
37													1,800								1,800	FT.	605	06001	4" BASE PIPE UNDERDRAINS, 706.08(ES), AS PER PLAN	18
38												126									126	EACH	638	10801	VALVE BOX ADJUSTED TO GRADE, AS PER PLAN	17 & 18
39												126									126	EACH	638	10901	SERVICE BOX ADJUSTED TO GRADE, AS PER PLAN	17 & 18
40												230,000									230,000	POUND	SPECIAL	60450000	MISCELLANEOUS METAL	17
41												3,000									3,000	POUND	SPECIAL	60450000	MISCELLANEOUS METAL (NEORS)	17
42												2,000									2,000	POUND	SPECIAL	60450000	MISCELLANEOUS METAL (CPP)	17

GENERAL SUMMARY

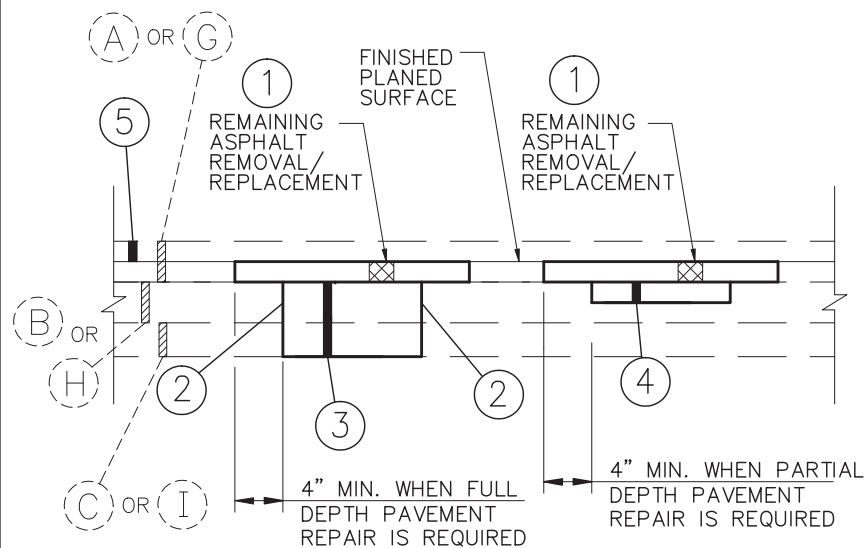
2013 OPERATIONS GROUP 2

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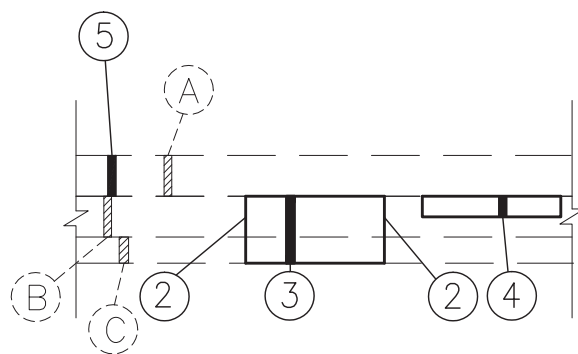
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PLOT DATE: 10-24-12

PAVEMENT REPAIR DETAILS AFTER ITEM 254 –
PAVEMENT PLANING, ASPHALT CONCRETE
(SEE TYPICAL SECTIONS, SHEET NO. 7-8)



EXISTING ASPHALT > 3"

NO SCALE



EXISTING ASPHALT ≤ 3"

NO SCALE

LEGEND

ONTARIO STREET STA. 109+10 TO STA. 122+81

- (A) EX. ASPHALT (3" TO 6"; 4" AVG.)
- (B) EX. CONCRETE (9" TO 16"; 10" AVG.)
- (C) EX. (LIMESTONE) SUBBASE (5"; PRESENT AT STA. 114+00, 34' RT. PAVEMENT CORE ONLY)

ONTARIO STREET STA. 122+81 TO STA. 127+14 &
STA. 134+00 TO STA. 144+71

- (G) EX. ASPHALT (2" TO 9"; 5" AVG.)
- (H) EX. SANDSTONE (5" TO 7"; 6" AVG.)
OR
EX. CONCRETE (5" TO 9"; 7" AVG.)
OR
EX. GRANITE (UNKNOWN THICKNESS; PRESENT AT STA. 124+00, 1' RT. PAVEMENT CORE ONLY)
- (I) EX. SAND SUBBASE (0" TO 3"; 1" AVG.)

- (1) ITEM 202 – WEARING COURSE REMOVED, AS PER PLAN
- (2) ITEM 255 – FULL DEPTH PAVEMENT SAWING
- (3) ITEM 255 – FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS MS, AS PER PLAN, (SEE GENERAL NOTE ON SHEETS 19 & 20).
- (4) ITEM 251 – PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN (SEE GENERAL NOTE ON SHEET 19).
- (5) ITEM 254 – PAVEMENT PLANING, ASPHALT CONCRETE (3" TYPICAL OR TO THE TOP OF THE EXISTING RIGID BASE WHEN LESS THAN 3" OF EXISTING ASPHALT OVERLAY).



REMOVAL/REPLACEMENT OF REMAINING (AFTER PLANING)
ASPHALT FOR PARTIAL AND FULL DEPTH PAVEMENT REPAIR

REMOVE REMAINING ASPHALT WITH ITEM 202 – WEARING COURSE REMOVED, AS PER PLAN.

FOR REPLACEMENT DEPTHS IN EXCESS OF 3", USE ITEM 301 – ASPHALT CONCRETE BASE, PG64-22, AS PER PLAN MATERIAL.

FOR REPLACEMENT DEPTHS RANGING FROM 1-1/2" TO 3", USE ITEM 448 – INTERMEDIATE COURSE, TYPE 2, PG64-22, AS PER PLAN MATERIAL.

FOR REPLACEMENT DEPTHS RANGING FROM 1/2" TO 1-1/2", USE ITEM 448 – INTERMEDIATE COURSE, TYPE 1, PG64-22, AS PER PLAN MATERIAL.

SIMPLY REPLACE DEPTHS LESS THAN 1/2" WITH THE PROPOSED INTERMEDIATE COURSE OVERLAY.

PLACE AND COMPACT THE ABOVE ASPHALT CONCRETE REPLACEMENT MATERIALS (301 AND 448) IN ONE OR MORE LIFTS IN ACCORDANCE WITH 252.04 OF THE SPECIFICATIONS.

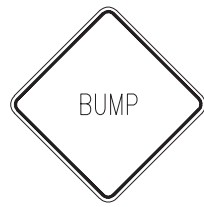
SEE "PAVEMENT REPAIR" GENERAL NOTE ON SHEET 19.

EAST 9TH STREET STA. 107+32 TO STA. 188+71 &
STA. 190+61 TO STA. 213+15

- (A) EX. ASPHALT (1" TO 11"; 4" AVG.)
- (B) EX. CONCRETE (6" TO 13"; 10" AVG.)
OR
EX. SANDSTONE OR BRICK (3" TO 6"; 5" AVG.)
- (C) EX. GRANULAR, 304 OR SAND SUBBASE (0" TO 17"; 4" AVG.)
OR
EX. CEMENT STABILIZED RECYCLED CONCRETE BASE (5" TO 9"; 7" AVG.)

EAST 9TH STREET STA. 213+15 TO STA. 217+74, STA. 220+13 TO
STA. 221+99 AND STA. 223+35 TO STA. 225+65

- (H) EX. BRICK (4") ON LEVELING MORTAR (1") ON CONCRETE (7" TO 9"; 8" AVG.)
OR
EX. SANDSTONE (4"; PRESENT AT STA. 225+53, 1' RT. PAVEMENT CORE ONLY)
- (I) EX. SAND, 304 OR GRAVEL SUBBASE (0" TO 10"; 3" AVG.)
OR
EX. CEMENT STABILIZED RECYCLED CONCRETE BASE (12" TO 14"; 13" AVG.)



W8-1

30"x30"



G20-1

36"x18"



G20-2

36"x18"



W20-1

48"x48"



W8-7

30"x30"



W21-2

30"x30"



W8-H12a

36"x36"



W8-H12

36"x36"



W8-11

36"x36"



R4-1

24"x30"



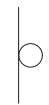
R4-2

24"x30"

MAINTENANCE OF TRAFFIC DETAILS

Sign Codes, Legends & Sizes

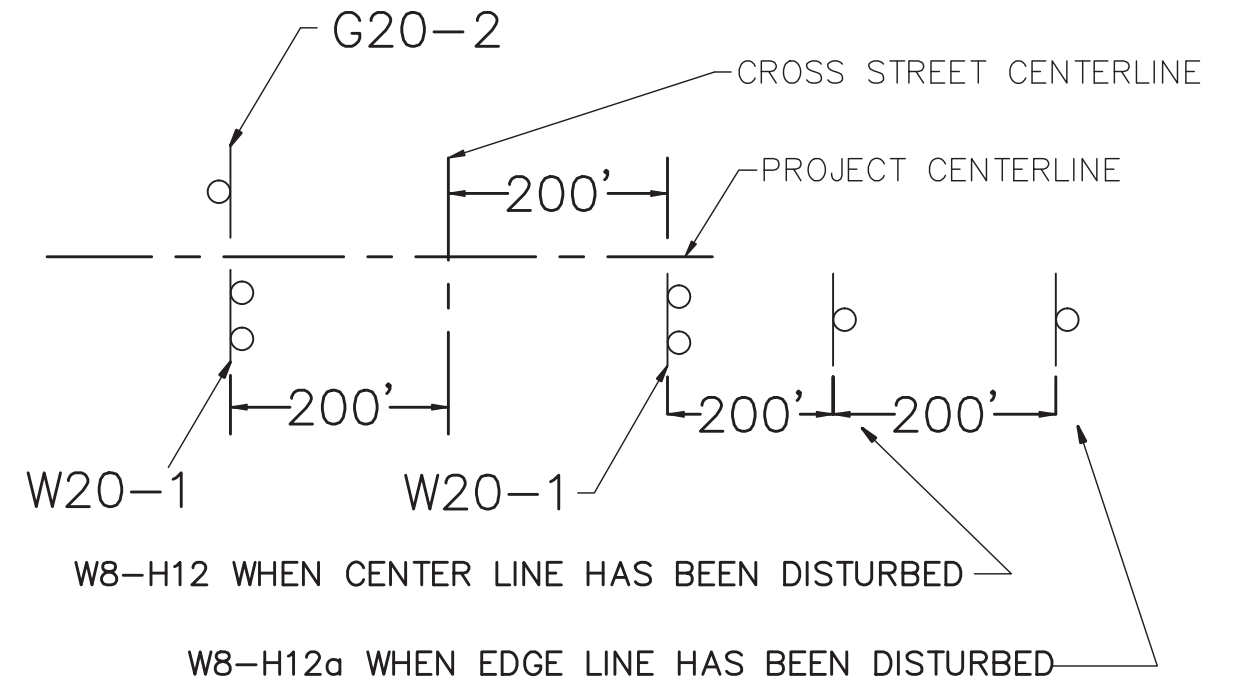
Legend



Sign mounted on 1 Channel Post



Sign Mounted on 2 Channel Posts



Typical Sign Spacing

$$\frac{30}{36}$$

DRAWING NAME: G:\Open's by Year\Open 2013\Group-2\Ontario\East 9\31-CLEVELAND STREETSCAPING EAST 9th-QUANTITY TABLE.dwg
PLOT DATE: 10-26-12

PROPOSED WALK & BRICK PAVER (REPLACED) DATA								ITEMIZED QUANTITIES															CALCULATED		CAG		CHECKED		BSD	
STATION TO STATION	SIDE	WALK LENGTH (FT.)	AVG. WALK WIDTH (FT.)	WALK AREA (SQ. FT.)	BRICK PAVER LENGTH (FT.)	BRICK PAVER WIDTH (FT.)	BRICK PAVER AREA (SQ. FT.)	202	202	604	604	604	604	604	604	604	SPECIAL	SPECIAL	608	608	SPECIAL	SPECIAL								
								CURB REMOVED	WALK REMOVED	MANHOLE ADJUSTED TO GRADE, AS PER PLAN	WATER METER MANHOLE ADJUSTED TO GRADE, AS PER PLAN	WATER METER MANHOLE RECONST. TO GRADE, AS PER PLAN	CPP VAULT ADJUSTED TO GRADE, AS PER PLAN	MONUMENT BOX ADJUSTED TO GRADE, AS PER PLAN	PULL BOX, 12"x 18" (CPP), ADJUSTED TO GRADE, AS PER PLAN	PULL BOX, 13"x24" (CLEVELAND TRAFFIC), ADJUSTED TO GRADE, AS PER PLAN	TREE PIT COLLAR CURB REMOVED AND REPLACED	MISCELLANEOUS METAL TREE PIT GRATE	4" CONCRETE WALK, AS PER PLAN	6" CONCRETE WALK, AS PER PLAN	BRICK PAVER WALK REMOVED AND RESET	REBUILD INTEGRAL CURB								
								(FT.)	(SQ. FT.)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(SQ. FT.)	(SQ. FT.)	(SQ. FT.)	(FT.)								
170+32 TO 173+35	RT.	VARIES	VARIES	2,640	-	-	-	-	2,640	-	-	-	-	-	-	-	-	-	-	2,640	-	-								
										</																				

CLEVELAND STREETSCAPING: EAST 9th STREET QUANTITY TABLE

2013 OPERATIONS GROUP 2

DRAWING NAME: C:\Oper's by Year\Oper 2013\Group-2\Ontario\East 9\33-690\Ontario Stenwalk Curb Details-2.dwg
PLOT DATE: 10-24-12

SOUTH APPROACH
EAST 9TH BRIDGE OVER SR 2

NORTH APPROACH
EAST 9TH BRIDGE OVER SR 2

PROPOSED APPROACH CURB

EPOXY COATED #4 U-HAIRPIN

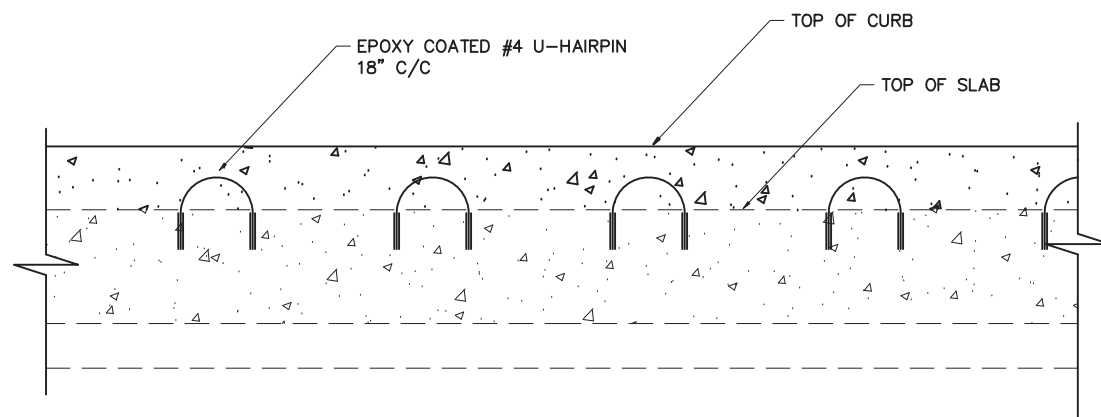
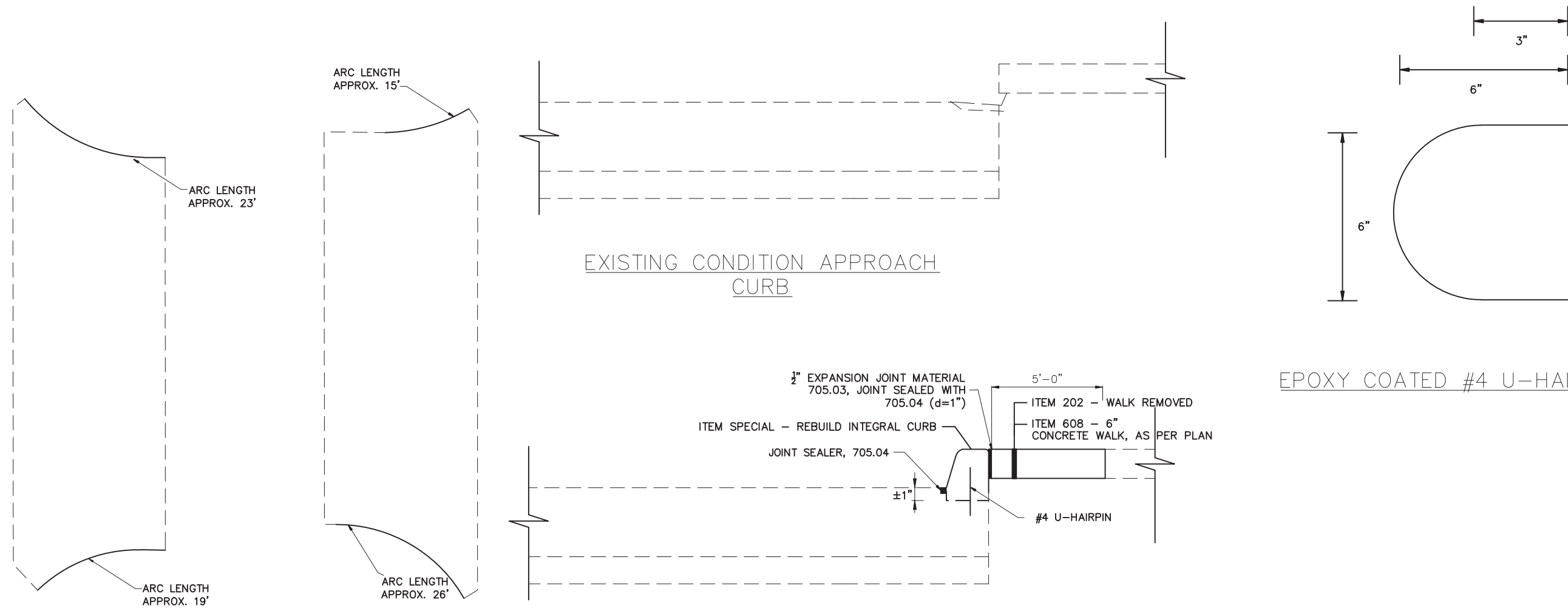
ITEM SPECIAL – REBUILD INTEGRAL CURB

THIS WORK SHALL INCLUDE:

1. THE REMOVAL OF THE EXISTING CURB AT APPROACH SLAB RADIUS RETURN
2. SAWCUT AND PREPARE CURB SURFACE ON SLAB
3. $\frac{5}{8}$ " DRILL HOLE FOR U-HAIRPIN PLACEMENT
4. PLACEMENT OF EPOXY COATED #4 U-HAIRPINS WITH EPOXY GROUT
5. PLACEMENT OF CURB INTEGRAL TO SLAB
6. SEAL JOINT WITH 705.04

BID PRICE SHALL INCLUDE THE FURNISHING OF ALL MATERIALS, LABOR, TOOLS, EQUIPMENT, AND SERVICES FOR AND INCIDENTAL TO THE IMPROVEMENTS PROPOSED (IN ACCORDANCE WITH ODOT CMS ITEMS 202 & 609) AND INCLUDED IN THE BID PRICE OF:

ITEM SPECIAL –REBUILD INTEGRAL CURB – FEET



ELEVATION



DRAWING NAME: G:\Oper's by Year\Oper 2013\Group-2\Ontario-East 9\34-STREETSCAPING GENERAL NOTES.dwg
PLOT DATE: 10-26-12

ITEM SPECIAL – GRANITE CURB REMOVED AND RESET

ALONG ONTARIO AND EAST 9th STREETS, SECTIONS OF THE EXISTING GRANITE CURB SHALL BE REMOVED AND RESET AS DIRECTED BY THE ENGINEER AND IN ACCORDANCE WITH THE DETAILS ON SHEET 32.

SUNKEN OR OUT OF LINE SECTIONS OF GRANITE CURB SHALL BE CAREFULLY REMOVED AND STORED WITHIN THE EXISTING RIGHT OF WAY UNTIL THE EXISTING TRENCH HAS BEEN PREPARED FOR RESETTING. REMOVAL SHALL BE BY METHODS THAT WILL NOT DISTURB, DAMAGE OR UNDERMINE ADJACENT PAVEMENT, UNDERDRAIN OR CURB INTENDED TO REMAIN IN PLACE. ANY ADJACENT FACILITIES DAMAGED AS A RESULT OF THE CONTRACTOR’S NEGLIGENCE, AS DETERMINED BY THE ENGINEER, SHALL BE REPLACED AT THE CONTRACTOR’S EXPENSE. THE COST OF REMOVAL SHALL BE INCLUDED IN THE BID PRICE PER FOOT OF "ITEM SPECIAL – GRANITE CURB REMOVED AND RESET".

THE PERTINENT PORTIONS OF SECTION 609.03 SHALL APPLY TO THE CURB RESETTING, ALL AS DIRECTED BY THE ENGINEER. EXCAVATION AND EARTH FILL MATERIAL SHALL BE PER 203.

ANY CAVATIES BELOW THE PROPOSED GRANITE CURB RESULTING FROM THE EXISTING CURB REMOVAL OPERATIONS SHALL BE BACKFILLED WITH CLASS "C" CONCRETE PER THE CITY OF CLEVELAND STANDARD CONSTRUCTION DRAWING CD-1. THE COST OF THIS IS TO BE INCLUDED IN THE UNIT PRICE BID FOR "ITEM SPECIAL – GRANITE CURB REMOVED AND RESET". THE "CLASS C" CONCRETE USED FOR THIS ITEM SHALL CONFORM TO THE "MODIFICATIONS TO ITEM 499 CONCRETE – GENERAL" AS CONTAINED IN THE "SECTION 400 PROPOSAL NOTES" OF THE "CUYAHOGA COUNTY ENGINEER SPECIFICATION BOOKLET" AS WELL AS THE SPECIFICATIONS IN THE "CONCRETE DESIGN MIX (CITY OF CLEVELAND)" GENERAL NOTE ON SHEET 19.

ANY ADJACENT SIDEWALK DISTUBED DURING THE REMOVAL/RESETTING OF THE EXISTING GRANITE CURB SHALL BE REMOVED AND REPLACED AS PER THE "EXISTING WALK REMOVAL/REPLACEMENT" GENERAL NOTE ON SHEETS 15 & 16.

IT IS INTENDED NOT TO DISTURB ANY EXISTING UNDERDRAINS (THAT MAY BE PRESENT ALONG ANY SECTION OF THE PROJECT LENGTHS) DURING THE CURB REMOVAL AND RESETTING OPERATIONS. ANY EXISTING UNDERDRAIN DAMAGED DURING THE REMOVAL OF EXISTING CURB DUE TO THE CARELESSNESS OF THE CONTRACTOR, AS DETERMINED BY THE ENGINEER, SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT HIS OWN EXPENSE.

EXISTING UNDERDRAINS WHICH THE ENGINEER DETERMINES ARE ADVERSELY AFFECTED (THROUGH NO FAULT OF THE CONTRACTOR) BY THESE OPERATIONS OR ARE FOUND TO BE OTHERWISE UNSUITABLE SHALL BE REMOVED AND REPLACED, IN KIND (INCLUDING ALL NECESSARY BENDS OR BRANCHES), AT THE SAME TRENCH WIDTH AND DEPTH, UNLESS OTHERWISE SPECIFIED BY THE ENGINEER.

ANY RESULTING CAVATIES BEHIND THE PROPOSED TYPE 6 CURB (AND STILL WITHIN THE EXISTING UNDERDRAIN TRENCH WIDTH) SHALL BE BACKFILLED PER 605.03(C) AND THEN HAVE AN 8" DEPTH OF NON-GRANULAR EARTH FILL (703.16.A) TAMPED UP TO THE BOTTOM OF THE 2" COMPACTED SCREENINGS BED ASSOCIATED WITH THE REPLACED CONCRETE WALK (SEE "ITEM 608 – 6" CONCRETE WALK, AS PER PLAN" GENERAL NOTE ON SHEET 16). THE COST OF THIS IS TO BE INCLUDED IN THE UNIT PRICE BID FOR "ITEM SPECIAL – GRANITE CURB REMOVED AND RESET" UNLESS THE EXISTING UNDERDRAIN IS REPLACED.

THE CONTRACTOR SHALL EVENLY RESET THE EXISTING GRANITE CURB AS DIRECTED BY THE ENGINEER TO MATCH THE EXISTING/ABUTTING CURB HEIGHT. RESULTANT CURB REVEAL MAY VARY ACCORDINGLY. THE COST TO RESET THE CURB SHALL BE INCLUDED IN THE UNIT PRICE BID PER FOOT OF "ITEM SPECIAL – GRANITE CURB REMOVED AND RESET".

IN LIEU OF INSTALLING EXPANSION JOINT MATERIAL PER BP-5.1, THOROUGHLY SEAL THE ABUTTING JOINT WHEN CURB IS PLACED ADJACENT TO EXISTING PAVEMENT.

THIS ITEM SHALL ALSO INCLUDE ALL NECESSARY EXCAVATION, BEDDING, BACKFILL, INSTALLING JOINT MATERIAL AND DISPOSAL OF SURPLUS EXCAVATION AND DISCARDED MATERIALS PER ITEM 203.

IN CONJUNCTION WITH THE ABOVE DESCRIBED WORK, QUANTITIES HAVE BEEN TABULATED ON SHEETS 30 & 31.

ITEM SPECIAL – BRICK PAVER WALK REMOVED AND RESET

AS DIRECTED BY THE ENGINEER AND AT THE APPROXIMATE LOCATIONS SHOWN ON SHEETS 35 & 36, THE EXISTING BRICK PAVERS SHALL BE CAREFULLY REMOVED AND RESET PER THE DETAILS AND NOTES ON SHEET 32.

QUANTITIES FOR THIS WORK HAVE BEEN TABULATED ON SHEET 31.

ITEM 604 – CPP VAULT ADJUSTED TO GRADE, AS PER PLAN

AS DIRECTED BY THE ENGINEER, THESE WILL BE ADJUSTED TO GRADE IN ACCORDANCE WITH 604.03, THE "STRUCTURES ADJUSTED TO GRADE, RECONSTRUCTED TO GRADE OR REPLACED" GENERAL NOTE ON SHEET 1Z AND IN ACCORDANCE WITH ANY APPLICABLE CLEVELAND PUBLIC POWER STANDARDS/SPECIFICATIONS.

QUANTITIES FOR THIS WORK HAVE BEEN TABULATED ON SHEETS 30 & 31.

CITY OF CLEVELAND MONUMENT REFERENCING (TO BE DONE BY CITY)

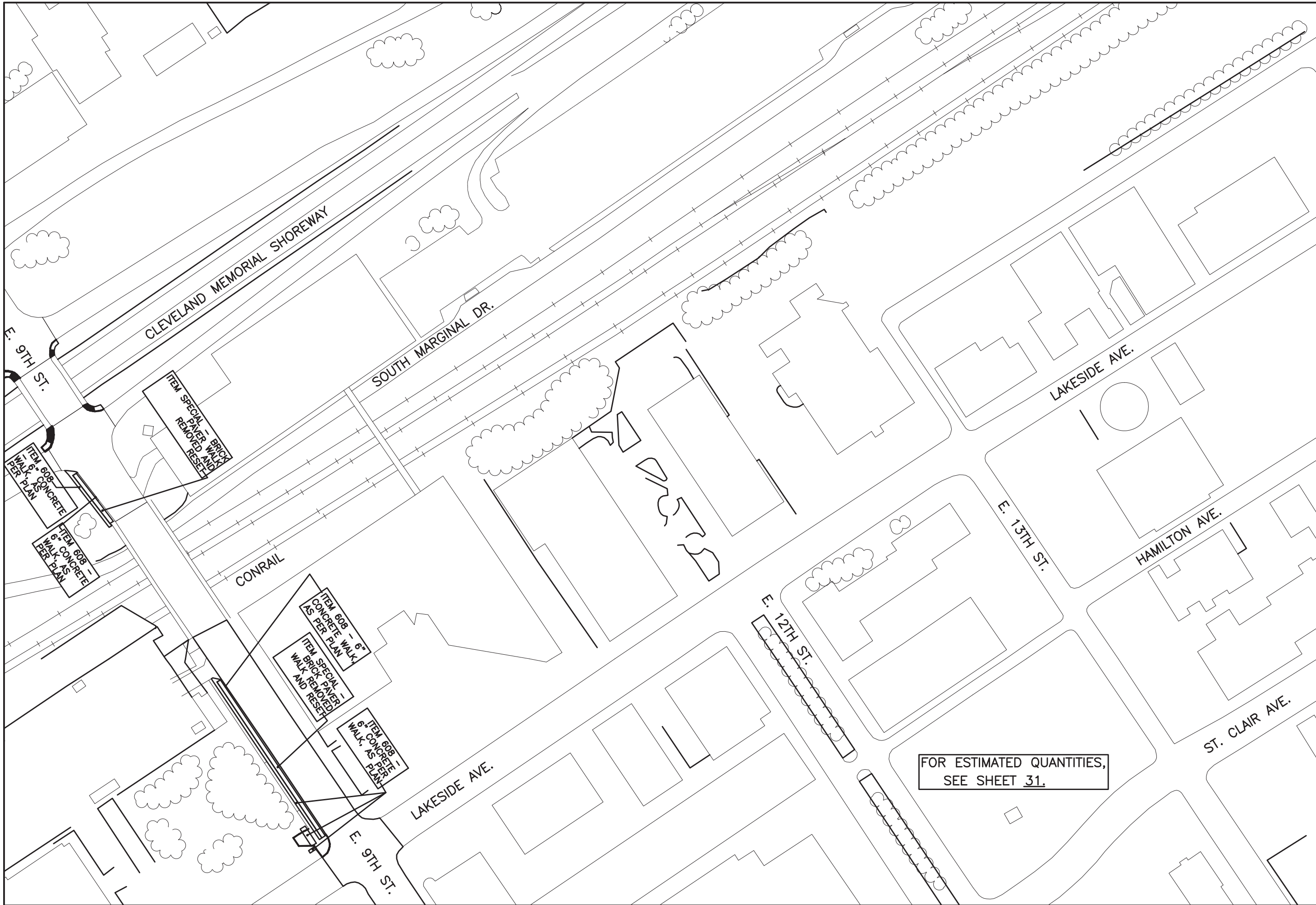
AS A RESULT OF THE CITY OF CLEVELAND STREETSCAPING WORK ITEMS SHOWN ON SHEETS 29 – 36, THE CONTRACTOR WILL BE WORKING BEYOND THE CURB AND PERHAPS AS FAR AS THE EXISTING RIGHT OF WAY LINES. THERE IS A CHANCE THAT THE CONTRACTOR MAY DISTURB EXISTING PRIVATE PROPERTY AND/OR RIGHT OF WAY MONUMENTS THAT MAY EXIST ALONG THE PROJECT LENGTHS.

THE CITY OF CLEVELAND SURVEYOR SHOULD THUS PROVIDE REFERENCING FOR ALL OF THE EXISTING PRIVATE PROPERTY AND RIGHT OF WAY MONUMENTS USING GPS DATA COLLECTION METHODS AND CONTROL COORDINATES. THIS REFERENCING SHOULD BE COMPLETED (IN A TABLE FORMAT) AND GIVEN TO THE ENGINEER PRIOR TO ANY WORK BEING PERFORMED.

PER THE "ITEM SPECIAL – MONUMENT REFERENCING" GENERAL NOTE ON SHEET 15, THE CONTRACTOR WILL PROVIDE THE REFERENCING FOR ALL EXISTING PAVEMENT (ROADWAY AND/OR CENTERLINE) MONUMENTS THAT MAY BE IMPACTED BY THE PROPOSED RESURFACING.

ITEM 608 – 4" CONCRETE WALK, AS PER PLAN

ALL OF THE INFORMATION CONTAINED IN THE "ITEM 608 – 6" CONCRETE WALK, AS PER PLAN" GENERAL NOTE ON SHEET 16 IS APPLICABLE TO THIS ITEM, EXCEPT THAT THE THICKNESS OF THE PROPOSED CONCRETE WALK SHALL BE 4".



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PLOT DATE: 10-18-12

