

D01 - CPR FY20
 190601 PID - 98554
 Dist 1 12/12/2019

Contract Proposal Available @
 www.contracts.dot.state.oh.us/home

STATE OF OHIO
 DEPARTMENT OF TRANSPORTATION

D01-CPR-FY20
 CITY OF FINDLAY
 VILLAGE OF VANBUREN
 ANTERIM, CRANE & ALLEN TOWNSHIPS
 HANCOCK & WYANDOT COUNTIES

PROJECT DESCRIPTION

PAVEMENT REPAIR ON 7.06 MILES OF US30,
 0.39 MILES ON SR12, 0.53 MILES ON SR613, &
 0.14 MILES ON CR 99 FOR A TOTAL 8.12 MILES
 IN HANCOCK AND WYANDOT COUNTIES

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: NA# ACRES
 ESTIMATED CONTRACTOR EARTH DISTURBED AREA: NA# ACRES
 NOTICE OF INTENT EARTH DISTURBED AREA: NA# ACRES
 (*MAINTENANCE PROJECT)

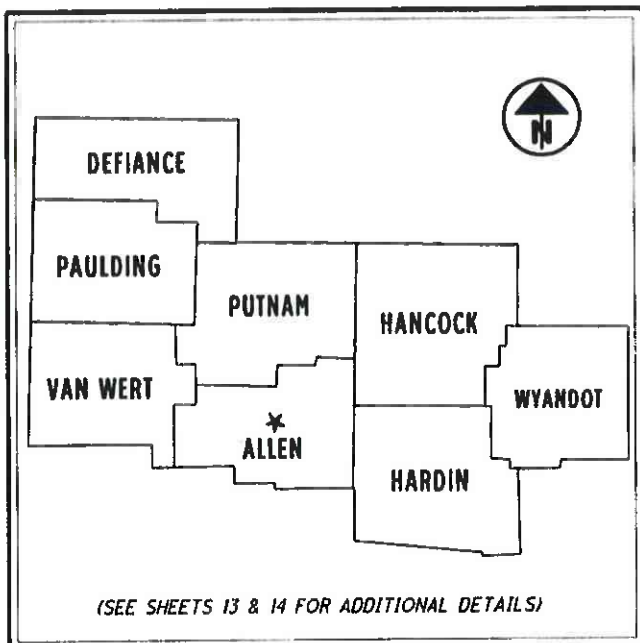
LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR
 THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED
 ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE
 DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF
 SECTION 5511.02 OF THE OHIO REVISED CODE.

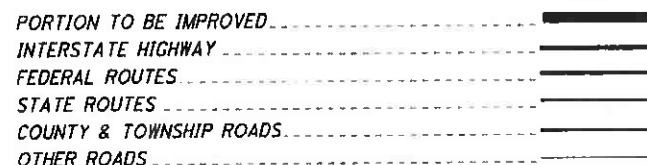
2019 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF
 OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING
 SUPPLEMENTAL SPECIFICATIONS LISTED IN THE
 PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL
 GOVERN THIS IMPROVEMENT.

FEDERAL PROJECT NO. **E161149**
 PID NO. **98554**
 CONSTRUCTION PROJECT NO.
 RAILROAD INVOLVEMENT **NONE**
D01-CPR-FY20
 1/15



LOCATION MAP
 LATITUDE: 40°46'18" LONGITUDE: 84°05'34"



NHS PROJECT YES

DESIGN EXCEPTIONS

NONE

INDEX OF SHEETS:

TITLE SHEET	1
GENERAL NOTES	2
MAINTENANCE OF TRAFFIC	3-10 & 3A
GENERAL SUMMARY	11
SUBSUMMARY	12
PAVEMENT REPAIR DETAILS	13-15

UNDERGROUND UTILITIES
 Contact Two Working Days
 Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
 (Non-members must be called directly)

PLAN PREPARED BY:
 OHIO DEPARTMENT OF
 TRANSPORTATION - DISTRICT 1
 LIMA, OHIO

ENGINEERS SEAL:

 SIGNED: Eric J. Schucheliff
 DATE: September 16, 2019

STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS		SPECIAL PROVISIONS	
BP-1.1	7/28/00	MT-101.70	7/20/18		DM-4.3	1/15/16	800	10/18/19	
BP-2.1	7/17/15	MT-101.90	7/21/17	TC-41.20	10/18/13	DM-4.4	1/15/16	821	4/20/18
BP-2.2	7/18/08			TC-52.10	10/18/13			832	10/19/18
BP-2.5	7/19/13	MT-105.10	7/19/13	TC-52.20	7/20/18			921	4/20/18
								808	1/18/19
								908	10/27/17
MT-95.30	7/19/19	BP-9.1	1/18/19						
MT-95.40	1/20/17			TC-65.10	1/17/14				
MT-98.10	1/20/17	MT-95.31	9/19/19	TC-65.11	7/21/17				
MT-98.11	4/19/19	MT-95.41	7/21/17	TC-71.10	1/19/18				
MT-98.20	4/19/19	MT-95.45	4/19/19	TC-72.20	7/20/18				
		MT-95.50	7/21/17						
MT-98.21	7/18/14	MT-99.70	4/19/19						
MT-98.22	1/20/17	MT-104.10	10/16/15						
MT-98.28	1/20/17								
MT-98.29	7/19/19								
MT-101.60	1/20/17								

APPROVED:
 DATE: 9/16/19 DISTRICT DEPUTY DIRECTOR

APPROVED:
 DATE: 9/23/19 DIRECTOR, DEPARTMENT OF TRANSPORTATION

I:\Projectdata\98554\Design\Roadway\Sheets\98554_01001.dgn Sheet eschecke 9/16/2019 3:26PM

ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS (UNIDIRECTIONAL OR BIDIRECTIONAL)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS. THE APPROVED LIST IS AVAILABLE AT THE "ROADWAY STANDARDS: PROPRIETARY ROADSIDE SAFETY DEVICES" WEB PAGE ON THE OFFICE OF ROADWAY ENGINEERING WEBSITE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

CONTRACTOR'S EQUIPMENT - OPERATION AND STORAGE

IN ADDITION TO THE REQUIREMENTS OF SECTION 614.03(a) OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS THE FOLLOWING SHALL APPLY. THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL. EXTREME CAUTION SHALL BE USED WHERE THE CONTRACTOR'S VEHICLES AND EQUIPMENT MERGE WITH THE TRAFFIC STREAM. THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE EQUIPPED WITH AT LEAST ONE AMBER FLASHING LIGHT. CONSTRUCTION EQUIPMENT MAY BE PARKED IN AREA ALONG THE HIGHWAY, THIRTY FEET (30') FROM THE EDGE OF THE TRAVELED HIGHWAY UNLESS BEHIND GUARDRAIL, WHEN VARIOUS OPERATIONS ARE SCHEDULED TO CONTINUE THE NEXT WORKDAY. EQUIPMENT PARKED BEHIND THE GUARDRAIL SHALL BE AT LEAST 6 FEET FROM THE FACE OF THE GUARDRAIL NO EQUIPMENT SHALL BE PARKED BEHIND A GUARDRAIL ATTENUATOR. ON WEEKENDS OR AT OTHER TIMES OF SUSPENSION OF WORK, EQUIPMENT SHALL BE STORED AT A STORAGE AREA REMOVED FROM THE INTERSTATE RIGHT OF WAY. NO EQUIPMENT SHALL BE PARKED IN THE MEDIAN OF THE HIGHWAY EXCEPT WHEN TRAFFIC IS MAINTAINED ON THE OUTSIDE LANES. ADEQUATE BARRICADES AND LIGHT SHALL BE PLACED ON THE PAVEMENT SIDE OF THE EQUIPMENT TO IDENTIFY THE LIMITS OF THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT THE APPROVED CONTRACTOR'S STORAGE AREA.

PAVEMENT MARKING

PRIOR TO PLACEMENT OF ANY WORK ZONE PAVEMENT MARKINGS, THE CONTRACTOR SHALL COMPLETELY OBLITERATE, AS PER SPEC. 641.10, ALL EXISTING PAVEMENT MARKINGS THAT WOULD CREATE CONFUSION OR CONFLICT WITH THE WORK ZONE PAVEMENT MARKINGS. PAYMENT FOR THIS COMPLETE REMOVAL SHALL BE INCLUDED IN ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENT OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP). IN GENERAL, LEOS SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.

WHEN CONSTRUCTION VEHICLES ARE ENTERING/EXITING THE ZONE DIRECTLY FROM/INTO AN OPEN LANE OF TRAFFIC. IF A LANE HAS BEEN CLOSED TO PROVIDE AN ACCELERATION/ DECELERATION LANE FOR THE VEHICLE, THE LEO WILL NOT BE REQUIRED.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 300 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONT.)

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

COORDINATION OF CONTRACTORS

SINCE THE MAINTENANCE OF TRAFFIC AND WORK ON THIS PROJECT MAY OVERLAP OTHER PROJECTS, IT IS ESSENTIAL THAT EACH CONTRACTOR CONDUCT THEIR WORK AND COOPERATE WITH EACH OTHER IN SUCH A MANNER AS NOT TO HINDER THE PROGRESS OR COMPLETION OF THE WORK BEING PERFORMED BY THE OTHER CONTRACTOR.

ALTERNATE METHODS

IF THE CONTRACTOR SO ELECTS, THEY MAY SUBMIT ALTERNATE METHODS FOR MAINTENANCE OF TRAFFIC PROVIDED THE INTENT OF THE ABOVE PROVISIONS ARE FOLLOWED AND NO ADDITIONAL INCONVENIENCE TO THE TRAVELING PUBLIC RESULTS THERE FROM. NO ALTERNATE PLAN SHALL BE PLACED INTO EFFECT UNTIL APPROVAL HAS BEEN GRANTED, IN WRITING, BY THE DISTRICT CONSTRUCTION ENGINEER.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN.

ROADSIDE REST AREA CLOSURES

THE REST AREAS SHALL BE MAINTAINED TO TRAFFIC AT ALL TIMES EXCEPT FOR A SINGLE PERIOD NOT TO EXCEED 3 CONSECUTIVE DAYS. REST AREAS SHALL BE OPEN TO TRAFFIC FROM FRIDAY, 6:00 AM TO MONDAY, 6:00 AM. THE CONTRACTOR SHALL PROVIDE MOTORISTS 7 DAYS ADVANCED NOTICE OF THE PLANNED REST AREA CLOSURE THROUGH USE OF A PORTABLE CHANGEABLE MESSAGE SIGN. THE CONTRACTOR SHALL COVER THE EXISTING REST AREA GUIDE SIGNS WITH A CLOSED OVERLAY SIGN AND PROVIDE THE APPROPRIATE TRAFFIC CONTROL TO CLOSE THE REST AREA ENTRANCE AND EXIT RAMPS.

ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN

THE CONTRACTOR SHALL MAINTAIN TRAFFIC AT ALL TIMES AT THE LOCATIONS IN THESE PLANS IN ACCORDANCE WITH THE REQUIREMENTS OF SPEC. 614, THESE MAINTENANCE OF TRAFFIC NOTES AND DETAILS AND THE TRAFFIC CONTROL DETAILS DESCRIBED IN THESE PLANS.

UNLESS OTHERWISE NOTED, A MINIMUM OF ONE LANE OF TRAFFIC SHALL BE MAINTAINED IN EACH DIRECTION AT ALL TIMES ON US23, US30, SR12, CR99 AND SR613. MINIMUM LANE WIDTH SHALL BE 11 FEET. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ORGANIZE HIS WORK IN SUCH A MANNER TO PROVIDE THE MOST SAFETY WITH THE LEAST INCONVENIENCE TO THE TRAVELING PUBLIC. FOR THE RAMPS & IF POSSIBLE, A MINIMUM OF ONE LANE OF TRAFFIC SHALL MAINTAINED. IF NOT POSSIBLE, RAMPS MAY BE CLOSED AND DETOURED AS SPECIFIED.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR DESIGNING AND MAINTAINING SAFE AND EFFECTIVE TRAFFIC CONTROL 24 HOURS A DAY FOR THE DURATION OF THIS PROJECT. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR. THIS CONSISTS OF NOTIFYING THE OHIO STATE PATROL AFTER ENCOUNTERING ANY ACCIDENTS OR DISABLED VEHICLES OR OBJECTS HINDERING THE FLOW OF TRAFFIC.

THE CONTRACTOR SHALL DESIGNATE TO THE ENGINEER, A PERSON RESPONSIBLE FOR MAINTENANCE OF TRAFFIC CONTROL DURING NON-WORK HOURS WHO SHALL BE AVAILABLE WITHIN THIRTY (30) MINUTES AFTER NOTIFICATION.

UNLESS PHYSICALLY IMPOSSIBLE, ALL CONSTRUCTION EQUIPMENT SHALL EXIT ALL WORK ZONES FROM THE DOWNSTREAM END OF THE WORK ZONE OR BY INTERCHANGE RAMPS.

UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO DIRECTLY TRANSPORT OR OPERATE ANY EQUIPMENT ACROSS THE OPEN LANES OF TRAFFIC. ALSO THE CONTRACTOR WILL NOT BE PERMITTED TO UTILIZE THE EMERGENCY TURN-AROUNDS AS PART OF THE HAUL ROUTE.

ALL WORK VEHICLES LICENSED TO OPERATE ON THE HIGHWAY, INCLUDING MATERIAL TRUCKS, SHALL BE EQUIPPED WITH A FLASHING, ROTATING OR OSCILLATING AMBER LIGHT VISIBLE TO ALL DIRECTIONS OF TRAFFIC A MINIMUM OF ONE QUARTER MILE IN BRIGHT SUNLIGHT AND SHALL BE OPERATED WITH LIGHTED HEAD AND TAIL LAMPS. THE AMBER LIGHT SHALL BE IN OPERATION AT ALL TIMES WITHIN THE WORK ZONE AND WHILE TRAVELLING TO AND FROM THE WORK ZONE WHENEVER THE VEHICLE SPEED IS BELOW 40 MPH. VEHICLE HAZARD LAMPS DO NOT SATISFY THIS REQUIREMENT. ALL OTHER EQUIPMENT SHALL BE EQUIPPED WITH A FLASHING, ROTATING OR OSCILLATING AMBER LIGHT VISIBLE TO ALL DIRECTIONS OF TRAFFIC A MINIMUM OF ONE QUARTER MILE IN BRIGHT SUNLIGHT. THE AMBER LIGHT SHALL BE IN OPERATION WHILE THE EQUIPMENT IS WITHIN THE WORK ZONE.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL ITEMS, LABOR, EQUIPMENT AND MATERIALS REQUIRED TO MAINTAIN TRAFFIC IN ACCORDANCE WITH THESE REQUIREMENTS AND DEEMED NECESSARY BY THE ENGINEER SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

I:\Project\data\98554\Design\Roadway\Sheets\98554_003_MN.dgn Sheet 17 - JAN-2020 2:47PM eschecke

CALCULATED
XXX
CHECKED
XXX

MAINTENANCE OF TRAFFIC NOTES

D01-CPR-FY20

3
15

ITEM 614, MAINTAINING TRAFFIC (TIME LIMITATION ON RAMP DETOURS AND SR 613)

A MINIMUM OF ONE LANE OF TRAFFIC ON THE RAMP AND ONE LANE OF TRAFFIC IN EACH DIRECTION ON SR 613 SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED THE CONSECUTIVE CALENDAR DAYS AS, NOTED IN THE PLANS, WHEN THROUGH TRAFFIC MAY BE DETOURED AS SHOWN IN THESE PLANS OR AS APPROVED BY THE ENGINEER. A DIS-INCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$5,000 PER DAY FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMITS.

ITEM 614, MAINTAINING TRAFFIC (SIGNS AND BARRICADES)

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS AND SIGN SUPPORTS, AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AND TYPE III BARRICADES OF THE TYPE AND LOCATION AS SHOWN IN THE PLANS AND STANDARD CONSTRUCTION DRAWINGS.

ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMP AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE

ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
RAMP & ROAD	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
ROAD	> 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
CLOSURES	< 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE

ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	4 CALENDAR DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE

START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES N/A 14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

WORK ZONE SPEED ZONES (WZSZS)

THE FOLLOWING WORK ZONE SPEED ZONE (WZSZ) SPEED LIMIT REVISION(S) HAVE BEEN APPROVED FOR USE ON THIS PROJECT WHEN WORK ZONE CONDITIONS AND FACTORS ARE MET AS DESCRIBED BELOW:

WZSZ REVISION NUMBER	COUNTY-ROUTE-SECTION
WZ- (TO BE PROVIDED)	WYA-30 SLM 17.50 TO 24.56 IN BOTH DIRECTIONS

POTENTIAL WZSZ LOCATIONS SHALL HAVE AN ORIGINAL (PRECONSTRUCTION) POSTED SPEED LIMIT OF 55 MPH OR GREATER, A QUALIFYING WORK ZONE CONDITION OF AT LEAST 0.5 MILE IN LENGTH, AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS, AND A WORK ZONE CONDITION IN PLACE THAT REDUCES THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS (I.E., LANE CLOSURE, LANE SHIFT, CROSSOVER, CONTRAFLOW AND/OR SHOULDER CLOSURE). THE LENGTH OF THE WORK ZONE CONDITION IS MEASURED FROM THE BEGINNING OF THE TAPER FOR THE SUBJECT WORK ZONE CONDITION IMPACTING THE TRAVEL LANES AND/OR SHOULDER TO THE END OF THE DOWNSTREAM TAPER, WHERE DRIVERS ARE RETURNED TO TYPICAL ALIGNMENT. AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS IS REQUIRED TO BALANCE THE ADDITIONAL EXPOSURE CREATED BY INSTALLING AND REMOVING WZSZ SIGNING WITH THE TIME NEEDED TO COMPLETE THE WORK.

IF THE WORK ZONE MEETS THESE MINIMUM CRITERIA, IT SHALL BE ANALYZED FURTHER USING TABLE 1 BELOW TO DETERMINE IF AND WHEN IT QUALIFIES FOR A SPEED LIMIT REDUCTION. DEPENDING ON THE ORIGINAL POSTED SPEED LIMIT, THE TYPE OF TEMPORARY TRAFFIC CONTROL USED, AND WHETHER OR NOT WORKERS ARE PRESENT, A WARRANTED WZSZ WILL VARY IN THE APPROVED SPEED LIMIT TO BE POSTED OVER TIME.

C&MS ITEM 614, PARAGRAPH 614.02(B), INDICATES THAT TWO DIRECTIONS OF A DIVIDED HIGHWAY ARE CONSIDERED SEPARATE HIGHWAY SECTIONS. THEREFORE, IF THE WORK ON A MULTI-LANE DIVIDED HIGHWAY IS LIMITED TO ONLY ONE DIRECTION, A SPEED LIMIT REDUCTION IN THE DIRECTION OF THE WORK DOES NOT AUTOMATICALLY CONSTITUTE A SPEED LIMIT REDUCTION IN THE OPPOSITE DIRECTION. EACH DIRECTION SHALL BE ANALYZED INDEPENDENTLY FROM EACH OTHER.

ALL WZSZS FLUCTUATE BETWEEN TWO APPROVED REDUCED SPEED LIMITS OR BETWEEN AN APPROVED REDUCED SPEED LIMIT AND THE ORIGINAL POSTED SPEED LIMIT. ONLY ONE OF TWO SIGNING STRATEGIES SHALL BE USED TO IMPLEMENT A WZSZ.

[WZSZS USING DSL SIGN ASSEMBLIES SHALL BE IN ACCORDANCE WITH THE NOTE, APPROVED LIST, SUPPLEMENTAL SPECIFICATIONS (SS) 808 AND 908, AND TRAFFIC SCD MT-104.10.J

[WZSZS USING TEMPORARY FLATSHEET SPEED LIMIT SIGNS SHALL BE IN ACCORDANCE WITH THIS NOTE AND SCD MT-104.10. ADDITIONALLY PAYMENT MAY BE REMOVED, OR A DISINCENTIVE APPLIED, FOR WZSZS USING TEMPORARY FLATSHEET SPEED LIMIT SIGNS THE SAME AS DESCRIBED IN THE MOST RECENT PUBLICATION OF SS 808 IN REGARDS TO WZSZS USING DSL SIGN ASSEMBLIES (SEE SS 808.06 PARAGRAPHS 4 THROUGH 7, INCLUDING TABLE 1).J

WORK ZONE SPEED ZONES (WZSZS) (CONTINUED)

ONLY ONE WARRANTED SPEED LIMIT APPLIES AT ANY ONE TIME; SPEED LIMIT REDUCTIONS ARE NOT CUMULATIVE. WZSZS SHALL NOT BE USED FOR MOVING/MOBILE ACTIVITIES, AS DEFINED IN OMUTCD PART 6.

WHEN LOOKING UP THE WARRANTED WORK ZONE SPEED LIMITS, ALWAYS USE THE ORIGINAL, PRECONSTRUCTION, POSTED SPEED LIMIT. DO NOT USE A PRIOR OR CURRENT WORK ZONE SPEED LIMIT AS A LOOK UP VALUE IN THE TABLE. POSITIVE PROTECTION IS GENERALLY REGARDED AS PORTABLE BARRIER OR OTHER RIGID BARRIER IN USE ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WITHOUT POSITIVE PROTECTION IS GENERALLY REGARDED AS USING DRUMS, CONES, SHADOW VEHICLE, ETC., ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WORKERS ARE CONSIDERED AS BEING PRESENT WHEN ON-SITE, WORKING WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WHEN THE WORK ZONE CONDITION REDUCING THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS IS REMOVED, THE SPEED LIMIT DISPLAYED SHALL RETURN TO THE ORIGINAL POSTED SPEED LIMIT.

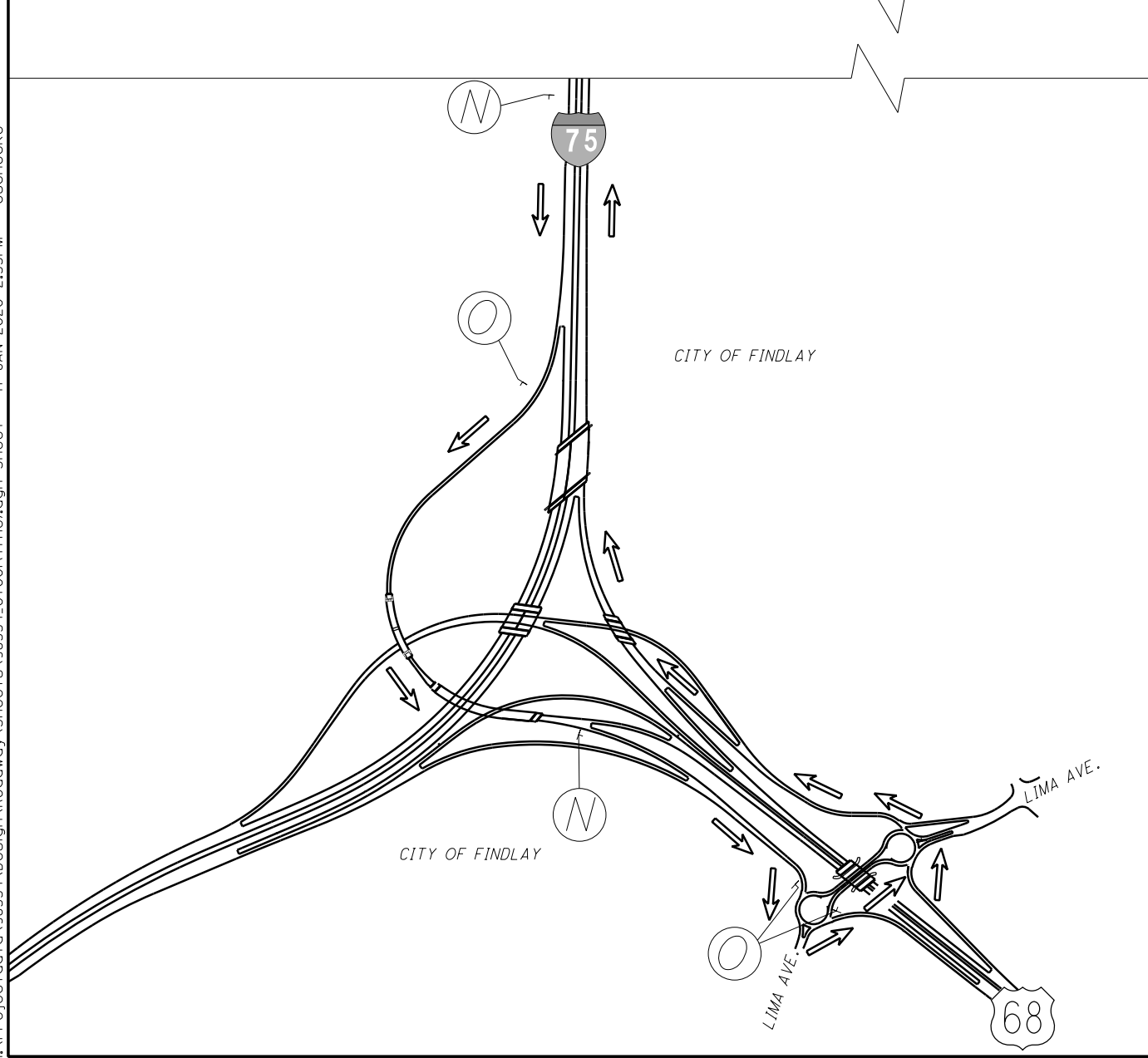
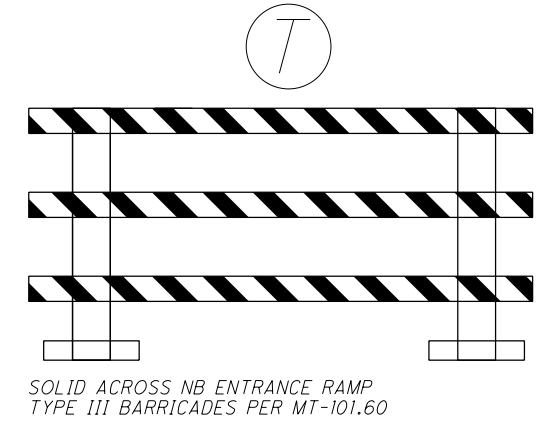
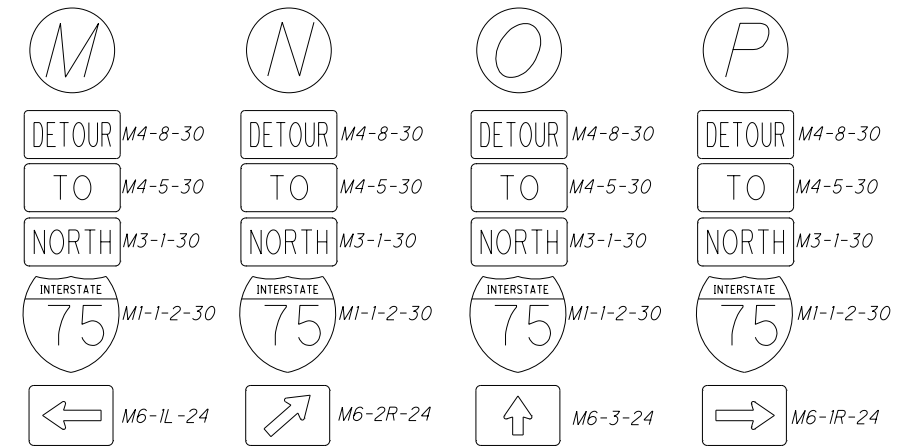
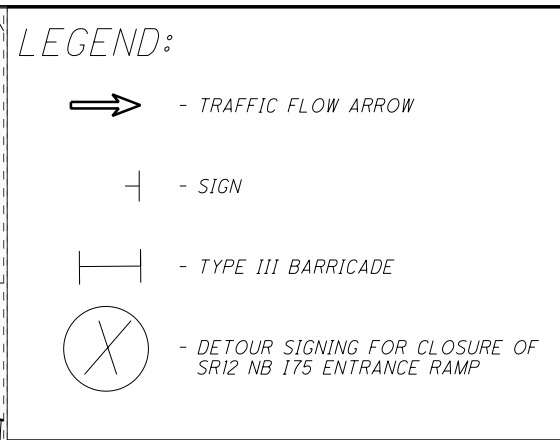
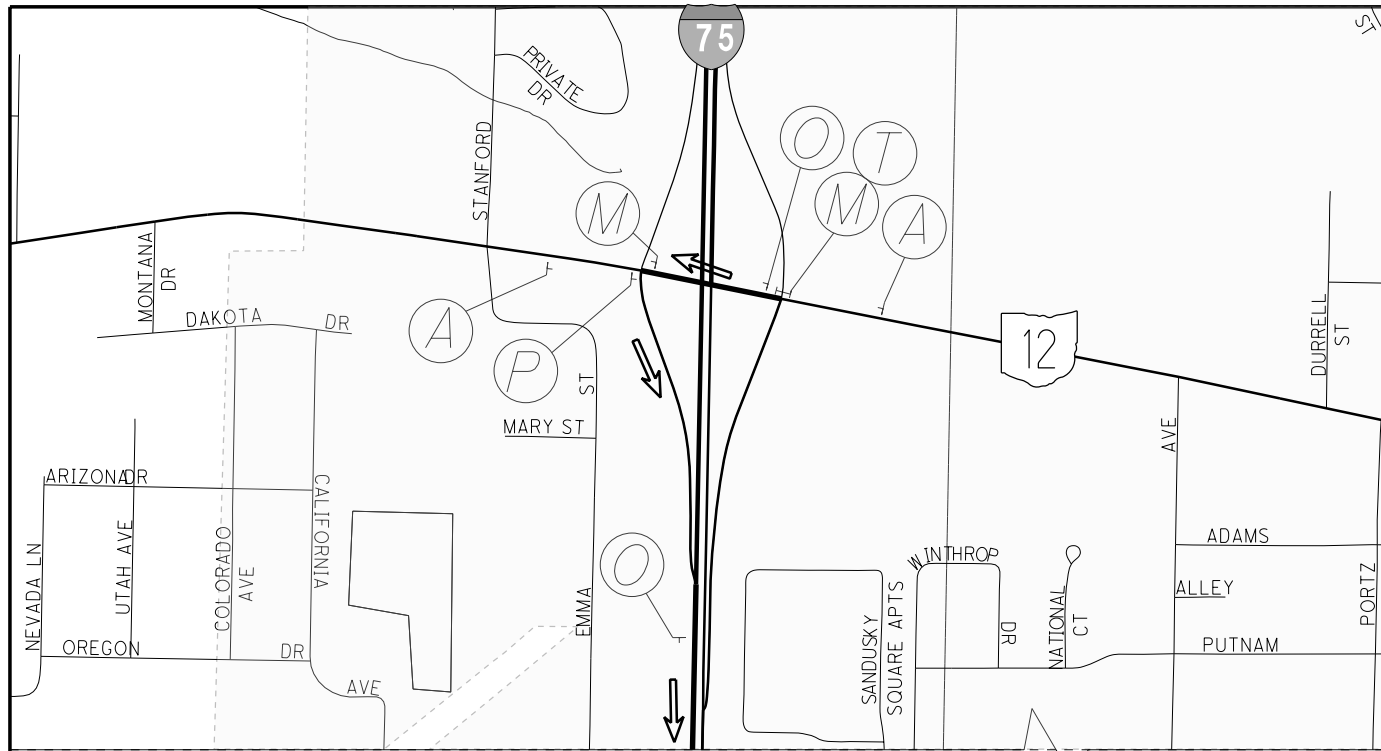
TABLE 1: WARRANTED WORK ZONE SPEED LIMITS (MPH) FOR WORK ZONES ON HIGH-SPEED (55 MPH OR GREATER) MULTI-LANE HIGHWAYS

ORIGINAL POSTED SPEED LIMIT	WITH POSITIVE PROTECTION		WITHOUT POSITIVE PROTECTION	
	WORKERS PRESENT	WORKERS NOT PRESENT	WORKERS PRESENT	WORKERS NOT PRESENT
70	60	65	55	65
65	55	60	50	60
60	55	60	50	60
55	50	55	45	55

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 808, DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY 20 SIGN MNTH ASSUMING 20 DSL SIGN ASSEMBLIES FOR 1 MONTH

I:\Project\98554\Design\Roadway\Sheets\98554_GTO01(title).dgn Sheet 17-JAN-2020 2:53PM eschecke



NOTES:
DETOUR SIGNING LOCATIONS SHALL BE AS PER OMUTCD

DETOUR: SR12 TRAFFIC TO TAKE NB ENTRANCE RAMP WILL
1. TAKE SB ENTRANCE RAMP
2. TAKE I75 TO LIMA AVE
3. TAKE THE ROUNDABOUT NB

WHEN THE CLOSURE OCCURS, COMPLETE DETOUR SIGNING AS INDICATED ON THIS PLAN AND PER ODOT STANDARD CONSTRUCTION DRAWINGS SHALL BE IMPLEMENTED.

DETOUR SIGNS SHALL BE UNCOVERED AND VISIBLE TO TRAFFIC ONLY WHEN THE RAMP CLOSURE IS IN EFFECT.

CONTRACTOR SHALL COORDINATE THESE DETOURS WITH ANY CONFLICTING DETOUR/SIGNING OR CLOSURE IN THE AREA OR AS DIRECTED BY THE ENGINEER.

