

CONVENTIONAL SIGNS

| | | | |
|-----------------------------|--------|------------------------|--------|
| COUNTY LINE | ----- | HEDGE | ===== |
| TOWNSHIP LINE | ----- | DRAINPIPE (OLD) | ----- |
| SECTION LINE | ----- | DRAINPIPE (NEW) | ----- |
| CORPORATION LINE | ----- | GUARD RAIL (OLD) | o o o |
| PROPERTY LINE | ----- | GUARD RAIL (NEW) | o o o |
| FENCE LINE | ----- | TREES+STUMPS REMOVED | ⊗ ⊗ |
| CENTER LINE | ----- | Limited Access | LIA |
| POLE LINE | ----- | EXISTING RIGHT OF WAY | EX R/W |
| RAILROAD | ----- | TEMPORARY RIGHT OF WAY | T-T |
| Limited Access Right of Way | LIA RW | Construction Line | CHL |

~LINE DATA~
F-1108(2) U.S. 68

Begin Project Sta. 677+06.30
Clark-Champaign County Line Sta. 842+06.90 Back
Sta. 0+00 Ahead
End Project Sta. 10+71.48 Back
Net Length of Project= 17,512.08 Lin. Ft. or 3.316 Miles
Add for work:
Sta. 652+88 to Sta. 653+93.44 Back = 105.44 Lin. Ft.
Sta. 677+05.60 Ahead to Sta. 677+06.30 = 6.70 Lin. Ft.
Sta. 10+71.48 Back =
Sta. 10+00.95 Ahead to Sta. 11+75 = 108.05 Lin. Ft.
S.R. 72 Sta. 667+05.34 to Sta. 678+40.53 = 1135.19 Lin. Ft.
River Road Approach Sta. 678+40.53 to Sta. 684+82.35 Back = 641.82 Lin. Ft.
Sta. 84+82.35 Ahead to Sta. 92+09.04 Back = 726.69 Lin. Ft.
Sta. 7+09.40 Ahead to Sta. 6+00 = 109.40 Lin. Ft.
Tremont City Road Sta. 2+50 to Sta. 25+90 = 2340.00 Lin. Ft.
County Line Road Sta. 6+50 to Sta. 30+40 = 2390.00 Lin. Ft.
Net Length of Work = 25,069.37 Lin. Ft. or 4.747 Miles

Begin Project S-78(11) S.R. 72 Sta. 659+21.55
End Project Sta. 667+05.34
Net Length of Project= 783.79 Lin. Ft. or 0.148 Miles
Add for Work
S.R. 334 (See sk. 307) = 1952.55 Lin. Ft.
Existing U.S. 68 (See sk. 180) = 4265.99 Lin. Ft.
Net Length of Work = 7002.33 Lin. Ft. or 1.326 Miles
Total Net Length of Project ~F-1108(2) + S-78(11) = 18,295.87 Lin. Ft. or 3.465 Miles
Total Net Length of Work ~F-1108(2) + S-78(11) = 32,071.70 Lin. Ft. or 6.074 Miles

~ INDEX OF SHEETS ~

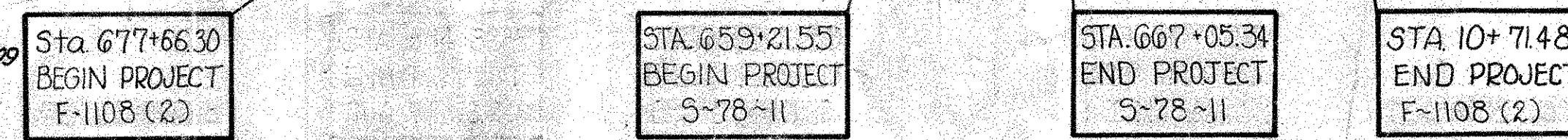
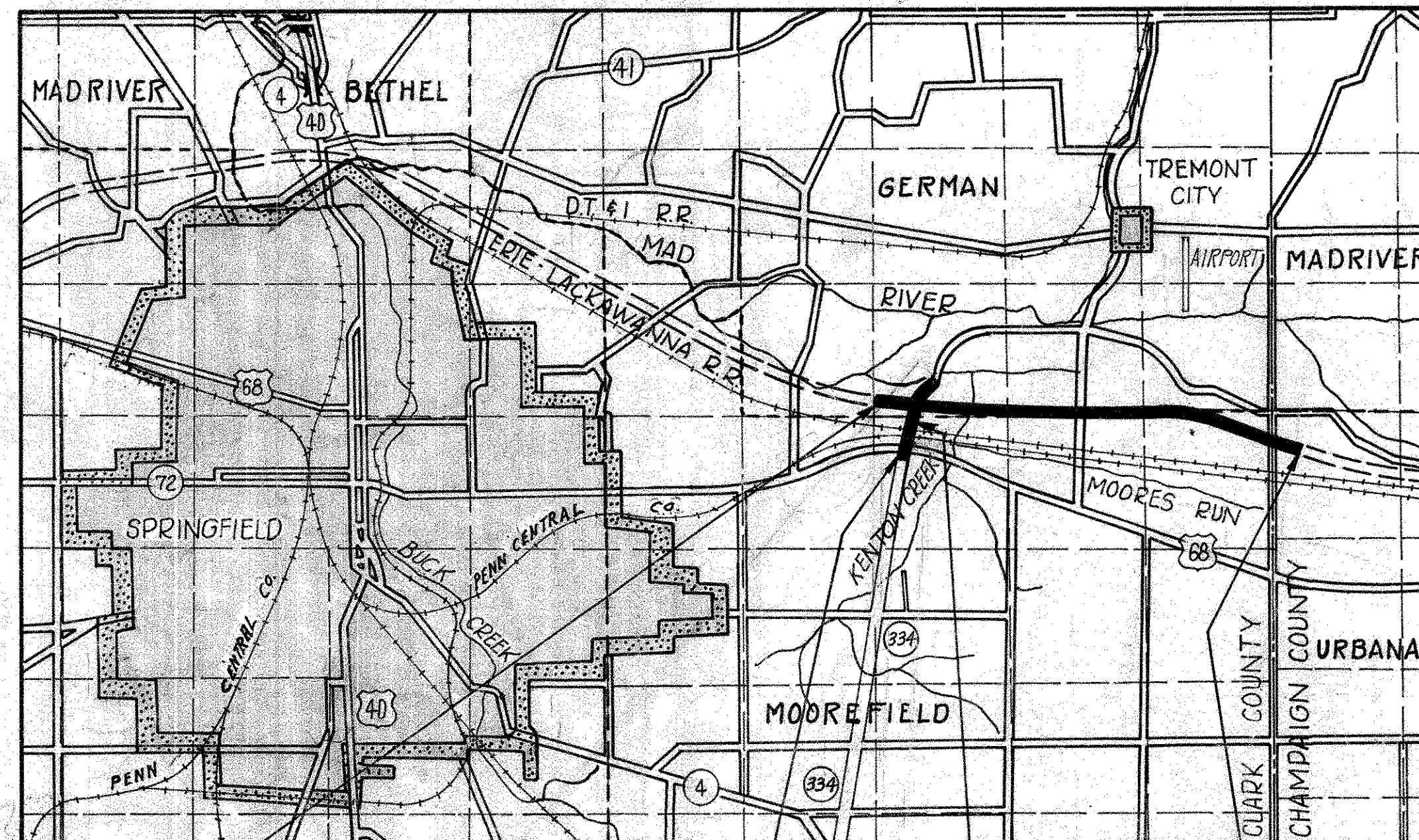
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* Str. No. CLA-72-1320 = CLA-72-2184 (METRIC)

* Str. No. CLA-68-1239 = CLA-68-1992 (METRIC)

STATE OF OHIO
DEPARTMENT OF HIGHWAYS
CLA-72-13.20 (SEE *)
CLA-68-12.39
CHP-68-0.00

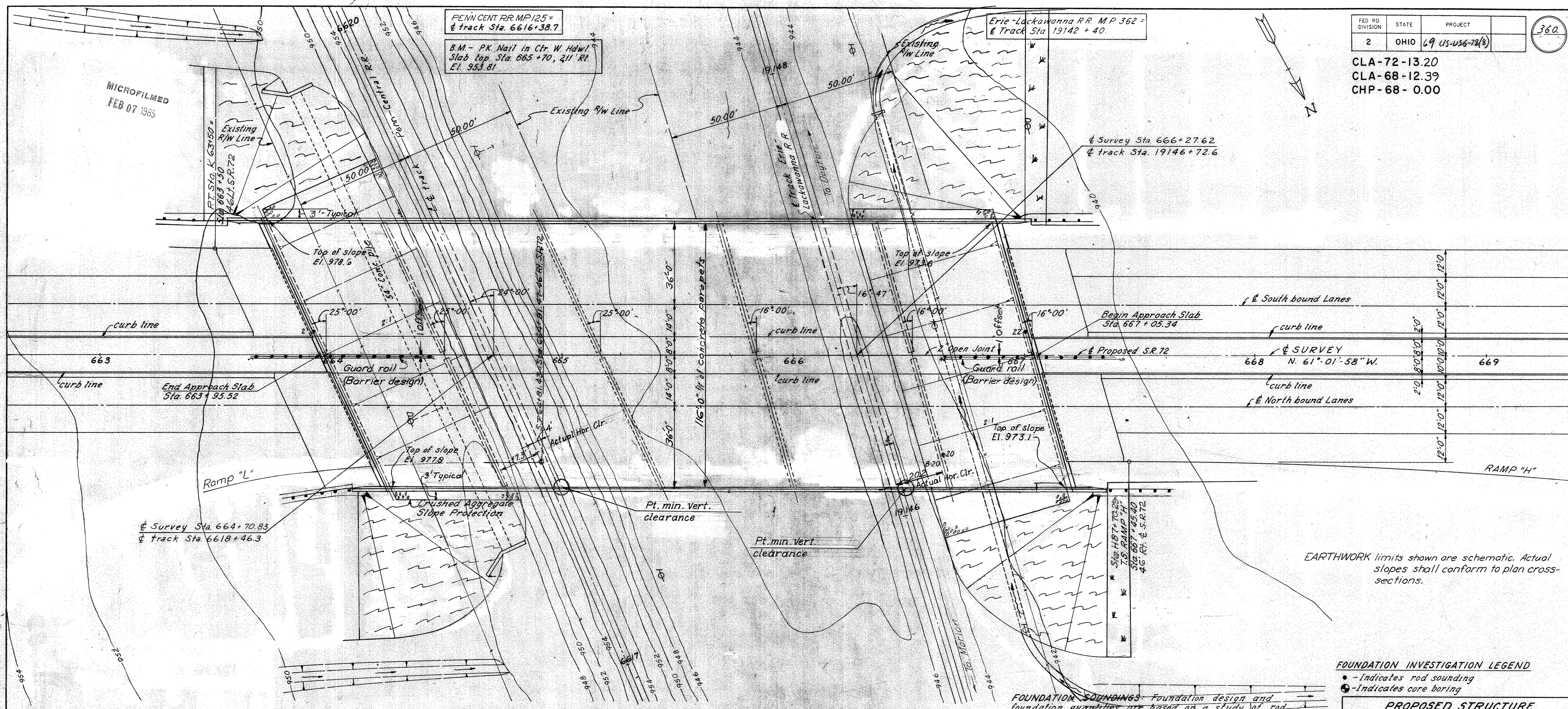
GRADE SEPARATION WITH THE PENN CENTRAL CO. AND
ERIE LACKAWANNA RAILROAD.
CLARK COUNTY
MOOREFIELD AND GERMAN TOWNSHIPS
CHAMPAIGN COUNTY
URBANA TOWNSHIP



LOCATION MAP

| STANDARD | DRAWINGS |
|---------------|----------------|
| HL-1 11-1-65 | HL-3 11-1-65 |
| HL-4 1-1-66 | HL-2 11-1-65 |
| BP-1 6-1-65 | CB-2 2-2-65 |
| BP-2 12-1-68 | CB-6 6-1-65 |
| BP-3 12-1-68 | CB-8 6-6-68 |
| BP-4 12-1-68 | F-2 3-10-69 |
| BP-5 8-1-65 | F-3 3-10-69 |
| BP-6 6-1-65 | FACI-1 9-15-67 |
| BP-7 1-1-66 | GR-1 1-1-67 |
| GR-2B 2-15-68 | GR-2A 1-1-67 |
| | GR-2 6-1-65 |
| | FACI-2 6-1-65 |
| | I-2A 6-6-69 |
| | I-2 6-6-69 |
| | MC-8 12-1-67 |
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CLA-72-13.20
 CLA-68-12.39
 CHP-68-0.00



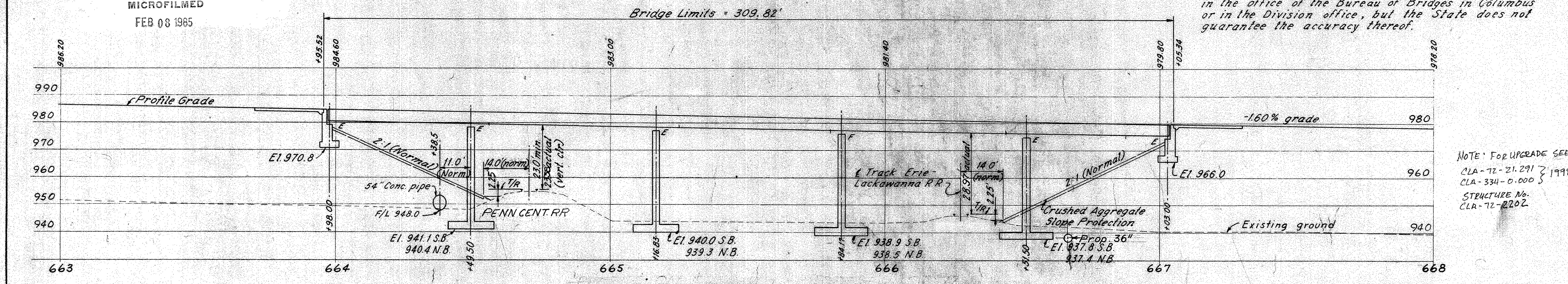
EARTHWORK limits shown are schematic. Actual slopes shall conform to plan cross-sections.

FOUNDATION SOUNDINGS: Foundation design and foundation quantities are based on a study of rod soundings and soil sampling soundings made at the site. This sounding information may be inspected in the office of the Bureau of Bridges in Columbus or in the Division office, but the State does not guarantee the accuracy thereof.

FOUNDATION INVESTIGATION LEGEND
 • - Indicates rod sounding
 ⊙ - Indicates core boring

PROPOSED STRUCTURE
TYPE: Continuous steel beams with reinforced concrete deck and substructure.
SPANS: Variable - 51'-6"; 3 @ 67'-4"; 51'-6" on & Survey.
ROADWAY: 116" of conc. parapets w/ 16" raised median.
LOADING: HS 20-44
WEARING SURFACE: 1" Monolithic concrete.
APPROACH SLAB: AS-1-67 (25'-0" long, modified)
SKIEW: Variable - 25° RF and 16° RF
ALIGNMENT: Tangent
AVERAGE DAILY TRAFFIC: 10,155 (1978)

MICROFILMED
 FEB 03 1985



NOTE: FOR UPGRADE SEE
 CLA-72-21.291 7/1978
 CLA-334-0.000 S
 STRUCTURE No.
 CLA-72-2202

STATE OF OHIO
 DEPARTMENT OF HIGHWAYS
 DIVISION OF DESIGN AND CONSTRUCTION
 BUREAU OF BRIDGES

(SEE NOTE) **SITE PLAN**
 BRIDGE NO. CLA-72-1329 OVER
 PENN CENT. & ERIE-LACKAWANNA R.R.
 CLARK COUNTY S.R. 72
 STA. 663+95.52 TO STA. 667+05.34

| | | | | | | | | | | | | | |
|----------|--------|-------|-----|--------|----|---------|-----|----------|--------|------|--------|---------|--|
| DESIGNED | D.H.T. | DRAWN | Tim | TRACED | UL | CHECKED | RAK | REVIEWED | P.E.S. | DATE | 6/5/68 | REVISED | |
|----------|--------|-------|-----|--------|----|---------|-----|----------|--------|------|--------|---------|--|

B.D.H. B.D.H. N.J.B. BFG 5-27-69

CLA-72-13.20
 CLA-68-12.39
 CHP-68-D.00

| | | | |
|-------------------|-------|---------|--|
| FED. RD. DIVISION | STATE | PROJECT | |
| 2 | OHIO | | |

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GENERAL NOTES

REFERENCE shall be made to Standard Drawings BR-1-67 sheet 1 of 3 (dated 2-1-68), RB-1-55 (revised 2-2-59), SD-1-65 sheets 1, 2 and 3 (dated 11-8-65), and to Supplemental Specifications 808, 811 and 825, all dated 1-1-69

DESIGN SPECIFICATIONS: This structure conforms to "Standard Specifications for Highway Bridges" adopted by the American Association of State Highway Officials, 1965, including the Ohio "Supplement" to these specifications.

DESIGN DATA:
 Design Loading - H520-44
 Concrete Class C - unit stress 1200 p.s.i. for superstructure
 unit stress 1333 p.s.i. for substructure
 Structural Steel - ASTM A36 - unit stress 20,000 p.s.i.
 Reinforcing Steel - ASTM A615, A616 and A617
 unit stress 20,000 p.s.i.

PROCEDURE: The embankment shall be constructed to the level of the subgrade for a minimum distance of 200 feet back of the abutments. Excavation shall then be made for the abutments and for Piers 1 and 4. Compaction requirements as per Section 203.12 shall be increased by 4% for each of the dry weight categories.

FOUNDATION BEARING PRESSURE: Footings are designed for a maximum bearing pressure of 1.5 tons per sq. ft. for the abutments and 3.0 tons per sq. ft. for the piers.

CONSTRUCTION CLEARANCE of 25 feet for the Erie-Lackawanna R.R. and 20 feet for the Penn Central R.R. vertically above the top of the railroad rails and 8 feet horizontally from the center of the tracks shall be maintained at all times.

RAILROAD AERIAL LINES will be relocated by the Railroads. The Contractor shall use all precautions necessary to see that the lines are not disturbed during the construction stage and shall cooperate with the Railroads in the relocation of these lines. The cost of the relocation shall be included in the railroad force account work.

WELDED ATTACHMENTS: No attachments shall be made by field welding to the top flange or flange plate of the continuous beams within a distance of 7'-0" on either side of the interior supports. Welding for attachments to the flanges at other parts of the span shall be kept at least 2" from the edge of flange.

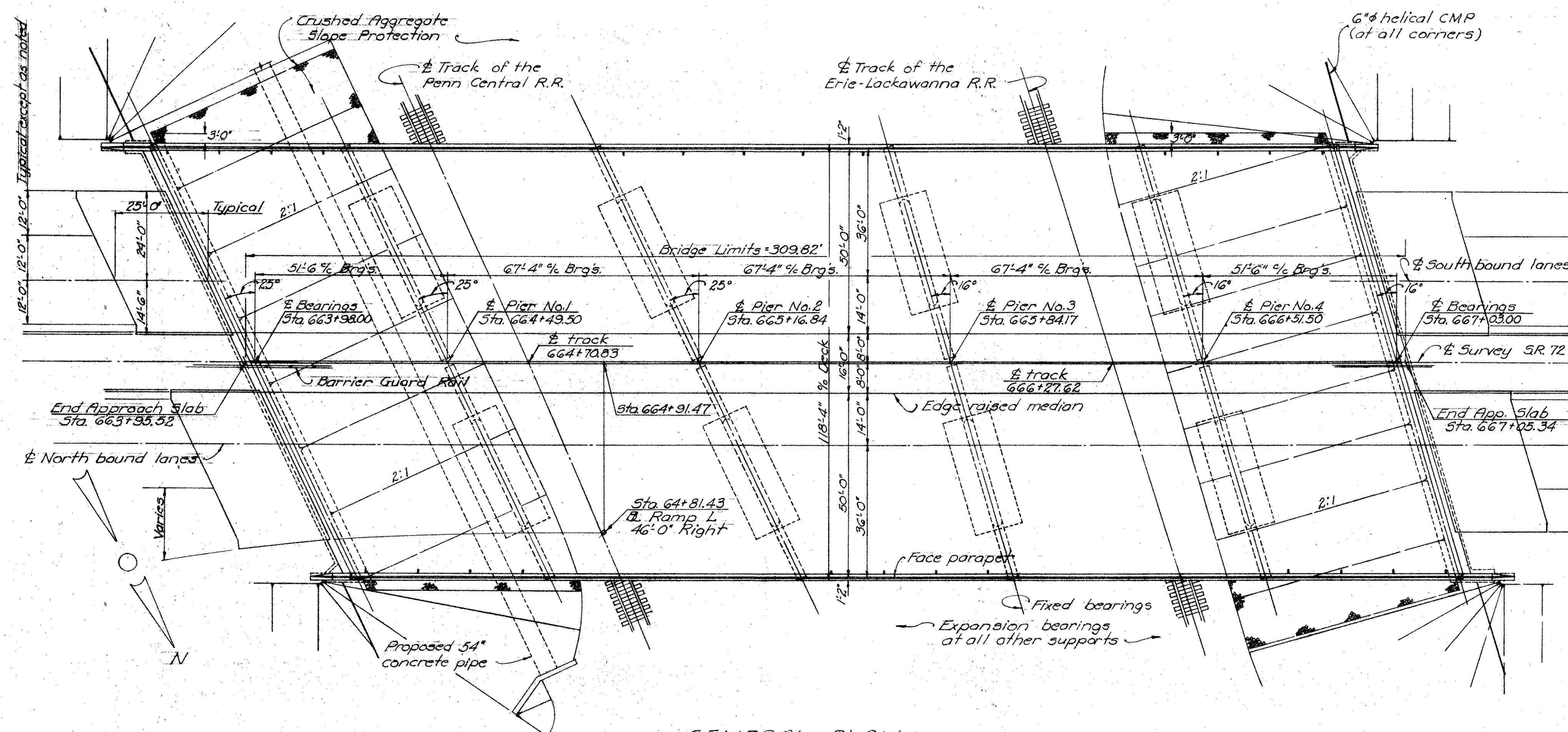
END DAM PAINTING: Portions of end dams which will be in contact with steel or with concrete shall not be painted. All other portions shall be cleaned and painted in accordance with 514.

SCUPPERS: The 1/2" x 1/2" bar shown in Section E-E on Standard Drawing SD-1-65, sheet 3, shall be omitted. The scupper and support angles only shall be galvanized.

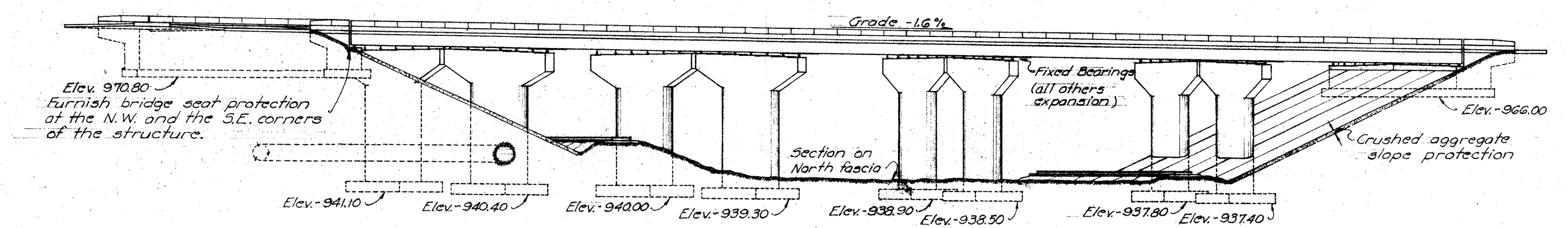
NON-STRESS-carrying welds are indicated thus $\overline{N} \swarrow \searrow \frac{1}{4}$

RAILROAD RIGHT-OF-WAY FENCE will be removed and replaced by railroad forces.

JOINT SEALER: Item 828 joint sealer including bond breaker, shown in Section A-A of Standard Drawing SD-1-65, Sheet No. 1, shall be omitted.



GENERAL PLAN



ELEVATION

MICROFILMED
 FEB 08 1965

ESTIMATED QUANTITIES

| Item | Total | Unit | Description | Super. | Piers | Abut's | Gen'l. | As Built | Item | Total | Unit | Description | Super. | Piers | Abut's | Gen'l. | As Built |
|------|--------|----------|--|--------|--------|--------|--------|----------|------|-------|----------|--|--------|-------|--------|--------|----------|
| 503 | Lump | sum | Cofferdams, cribs and sheeting | | | | Lump | | 518 | 239 | Lin. ft. | 6" perforated, helical corrugated metal pipe including specials 707.01 | | | 239 | | |
| 503 | 2120 | Cu. yd. | Unclassified excavation | | 1430 | 690 | | | 518 | 135 | Lin. ft. | 6" non-perforated, helical corrugated metal pipe, 707.01 | | | 135 | | |
| 509 | 467569 | Lb. | Reinforcing steel | 325432 | 120073 | 22064 | | | 601 | 2020 | Sq. yd. | Crushed aggregate slope protection | | | | | 2020 |
| 511 | 1120 | Cu. yd. | Class C concrete, superstructure | 1120 | | | | | 518 | 164 | Cu. yd. | Porous backfill | | | | | |
| 511 | 960 | Cu. yd. | Class C concrete, piers above footings | | 960 | | | | 808 | 1120 | Units | Water-reducing, set-retarding admixture | 1120 | | | | |
| 511 | 346 | Cu. yd. | Class C concrete, pier footings | | 346 | | | | 825 | 4266 | Sq. yd. | Concrete surface treatment | 4170 | | 96 | | |
| 512 | 24 | Lin. ft. | Premolded sealing strip | | | 24 | | | | | | | | | | | |
| 513 | 834500 | Lb. | Structural steel | 834500 | | | | | | | | | | | | | |
| 514 | 834500 | Lb. | Field painting of structural steel | 834500 | | | | | | | | | | | | | |
| 516 | 74 | Sq. ft. | 1" preformed expansion joint filler | | | 74 | | | | | | | | | | | |
| 517 | 309.64 | Lin. ft. | Barrier guard rail (double faced, deep beam rails with steel post and bolts) | | 309.64 | | | | | | | | | | | | |
| 518 | 25 | Each | Scuppers including supports (galvanized) | | 25 | | | | | | | | | | | | |

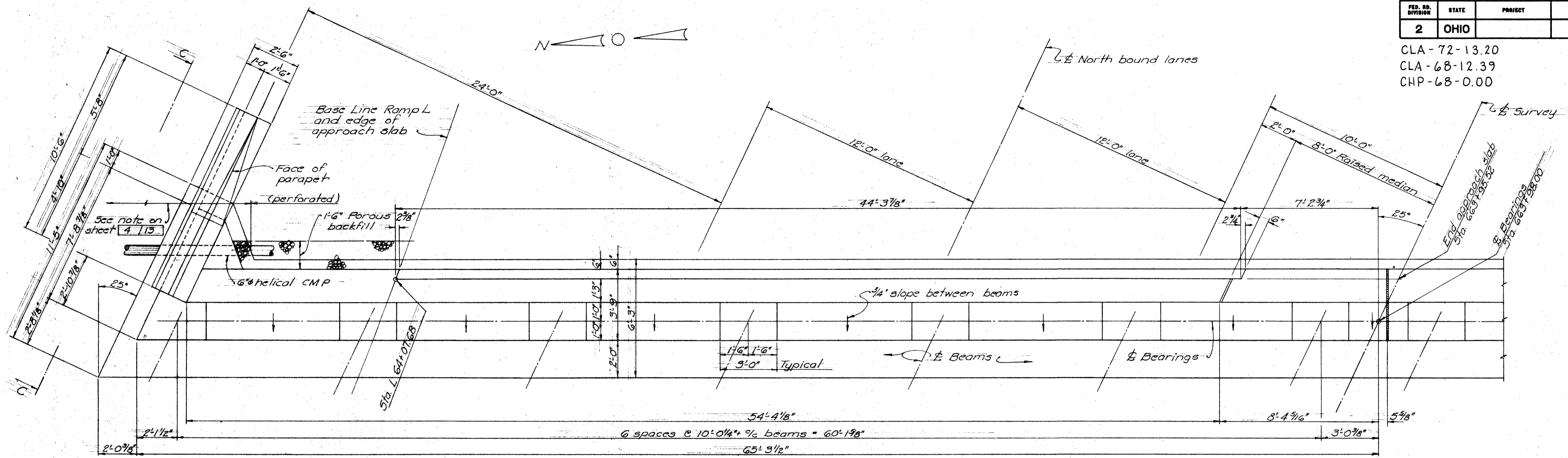
STATE OF OHIO
 DEPARTMENT OF HIGHWAYS
 DIVISION OF DESIGN AND CONSTRUCTION
 BUREAU OF BRIDGES

2/13

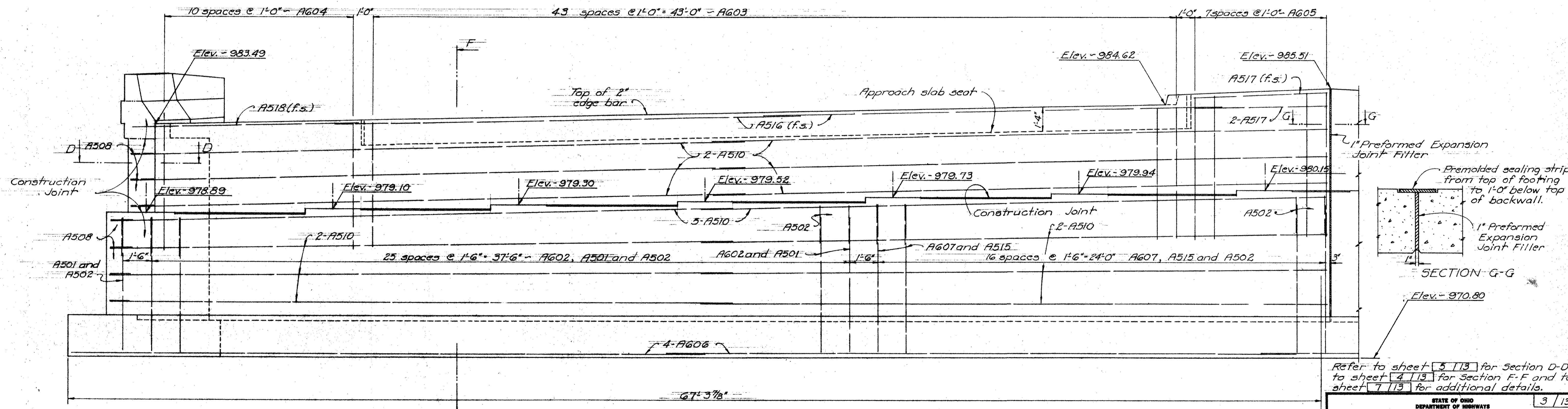
GENERAL PLAN & ELEVATION
 NOTES & QUANTITIES
 BRIDGE NO. CLA-72-1329 over
 PENN CENTRAL and the
 ERIE-LACKAWANNA R. R.

| | | | | | | |
|----------|-------|--------|---------|----------|---------|---------|
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| MPB | MPB | | WCK | BFG | 5-27-69 | |

CLA-72-13.20
 CLA-68-12.39
 CHP-68-0.00



PART PLAN



ELEVATION

Refer to sheet 5113 for Section D-D, to sheet 4113 for Section F-F and to sheet 7113 for additional details.

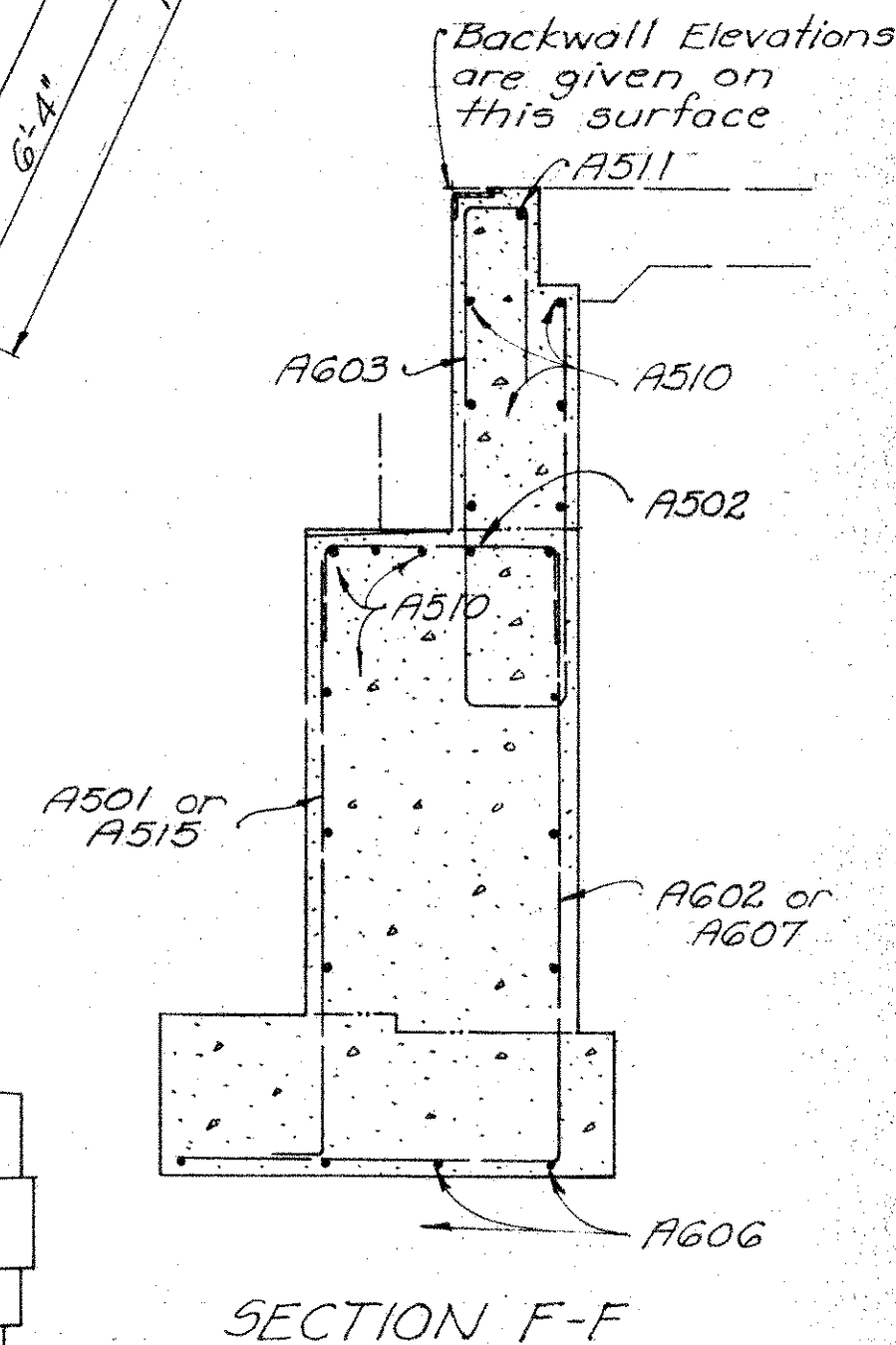
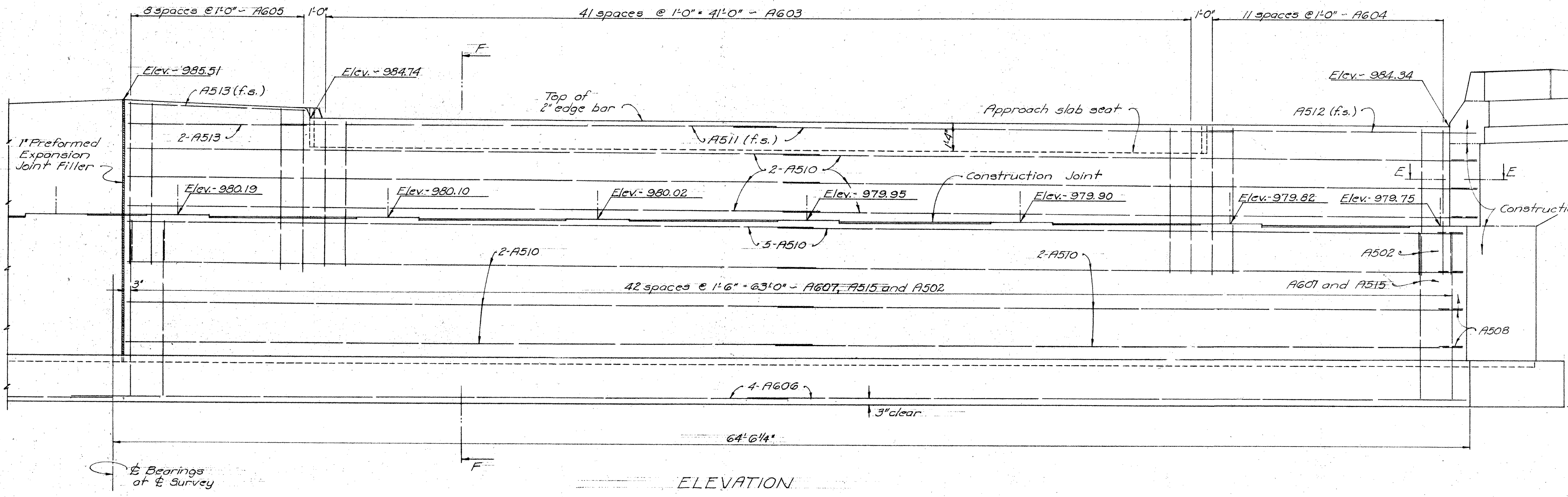
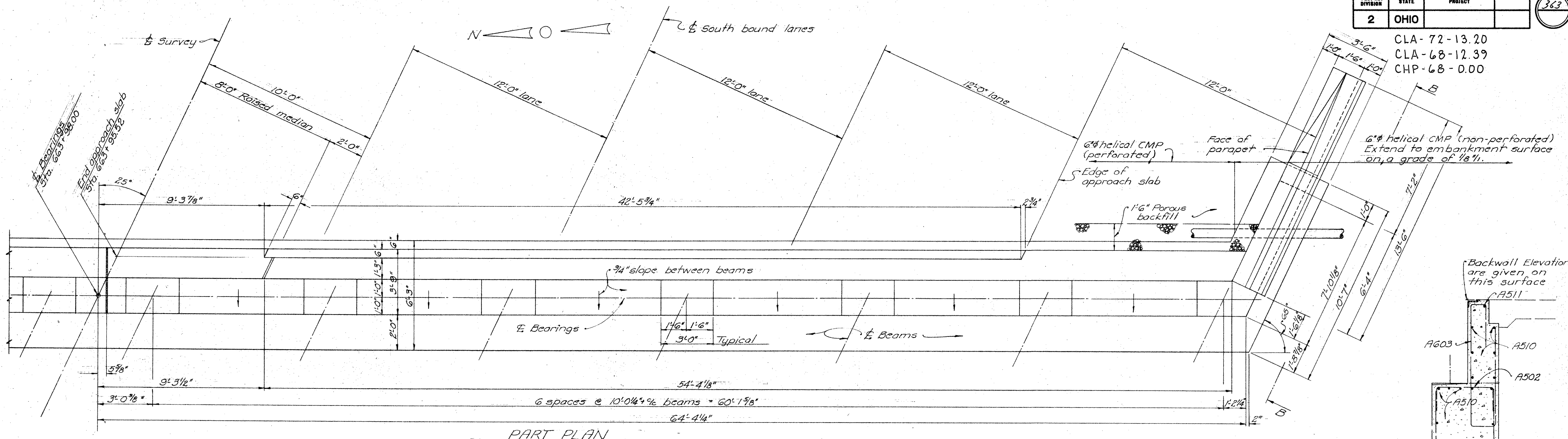
STATE OF OHIO
 DEPARTMENT OF HIGHWAYS
 DIVISION OF DESIGN AND CONSTRUCTION
 BUREAU OF BRIDGES

REAR ABUTMENT DETAILS
 BRIDGE NO. CLA-72-1323 over
 PENN CENTRAL and the
 ERIE-LACKAWANNA R.R.

| | | | | | | |
|----------|-------|--------|---------|----------|---------|---------|
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| MPB | MPB | | WCK | BFG | 5-27-69 | |

MICROFILMED
 FEB 08 1985

CLA-72-13.20
 CLA-68-12.39
 CHP-68-0.00

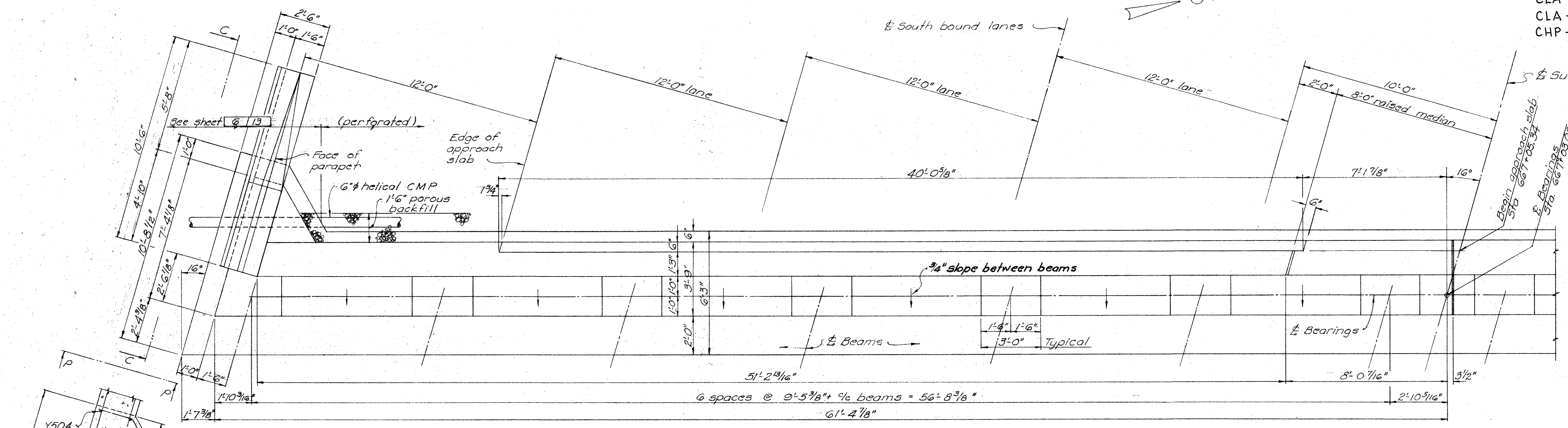
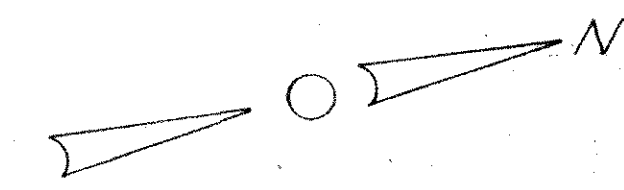


Refer to sheet 6/13 for Section E-E and to sheet 7/13 for additional notes and details.

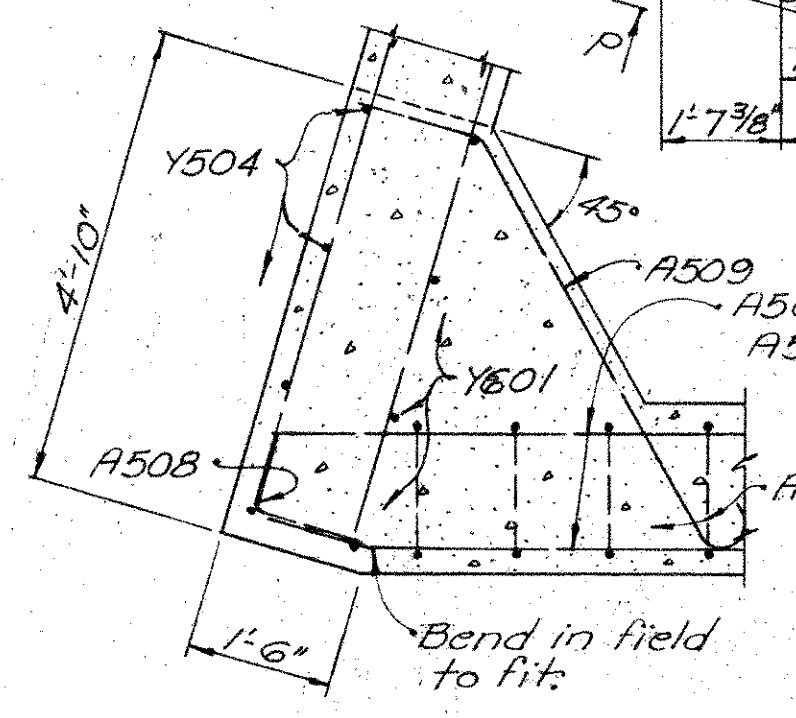
MICROFILMED
 FEB 08 1965

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| STATE OF OHIO DEPARTMENT OF HIGHWAYS DIVISION OF DESIGN AND CONSTRUCTION BUREAU OF BRIDGES | | | | | | | 4/13 |
| REAR ABUTMENT DETAILS | | | | | | | |
| BRIDGE NO. CLA-72-1329 over PENN CENTRAL and the ERIE-LACKAWANNA R. R. | | | | | | | |
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED | |
| MPB | MPB | | WCK | BFG | 5-27-69 | | |

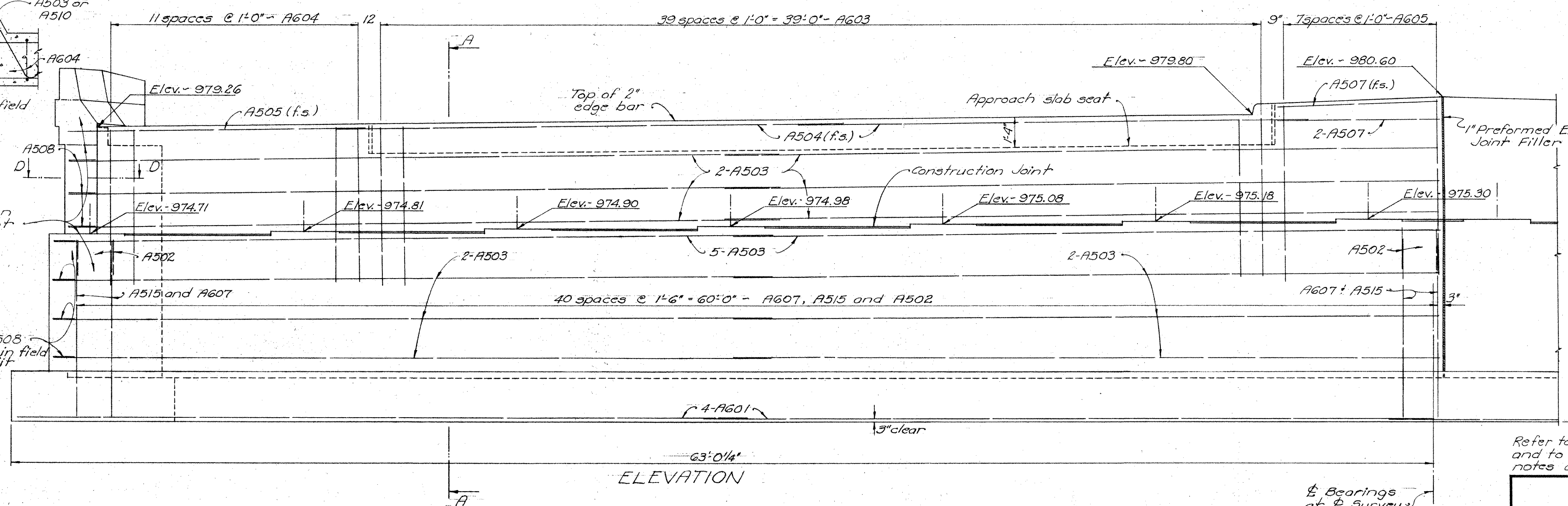
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 CLA-68-12.39
 CHP-68-D.00



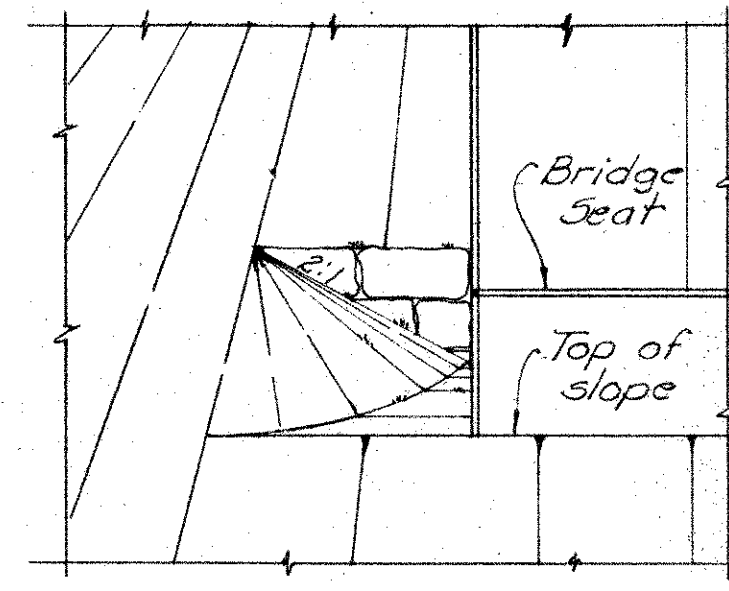
PART PLAN



SECTION D-D



ELEVATION



VIEW P-P
 Bridge seat protection
 Refer to sheet 7/13

Refer to sheet 6/13 for Section A-A and to sheet 7/13 for additional notes and details.

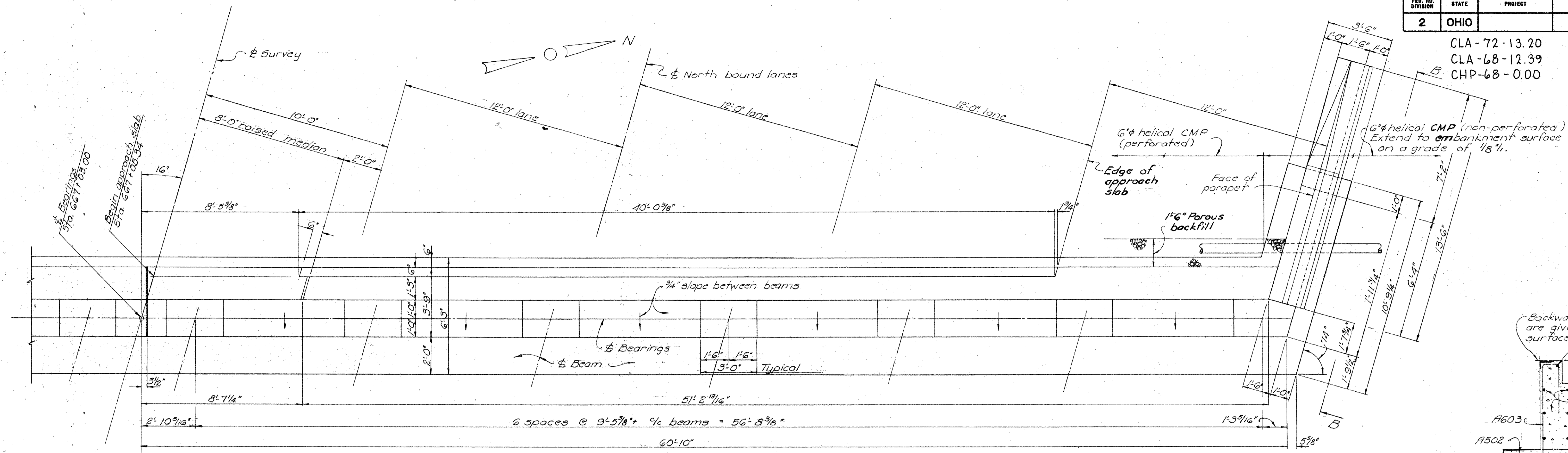
FORWARD ABUTMENT DETAILS
 BRIDGE NO. CLA-72-1329 over
 PENN CENTRAL and the
 ERIE - LACKAWANNA R.R.

| | | | | | | |
|----------|-------|--------|---------|----------|---------|---------|
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| MPB | MPB | | WCK | BFG | 5-27-69 | |

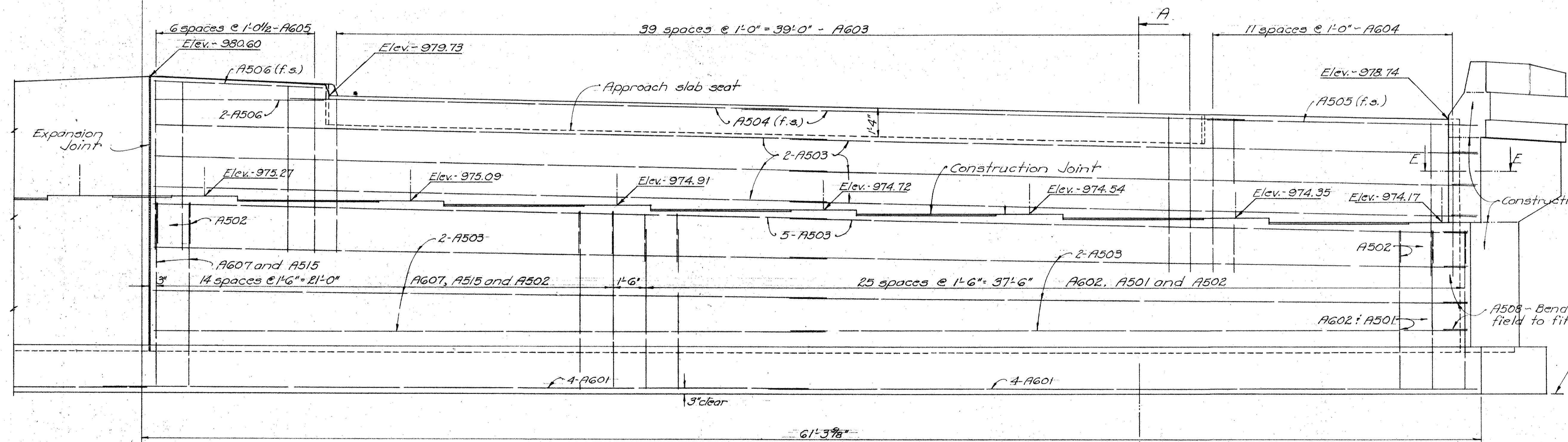
MICROFILMED
 FEB 08 1985

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| FED. RD. DIVISION | STATE | PROJECT | |
| 2 | OHIO | | 365 |

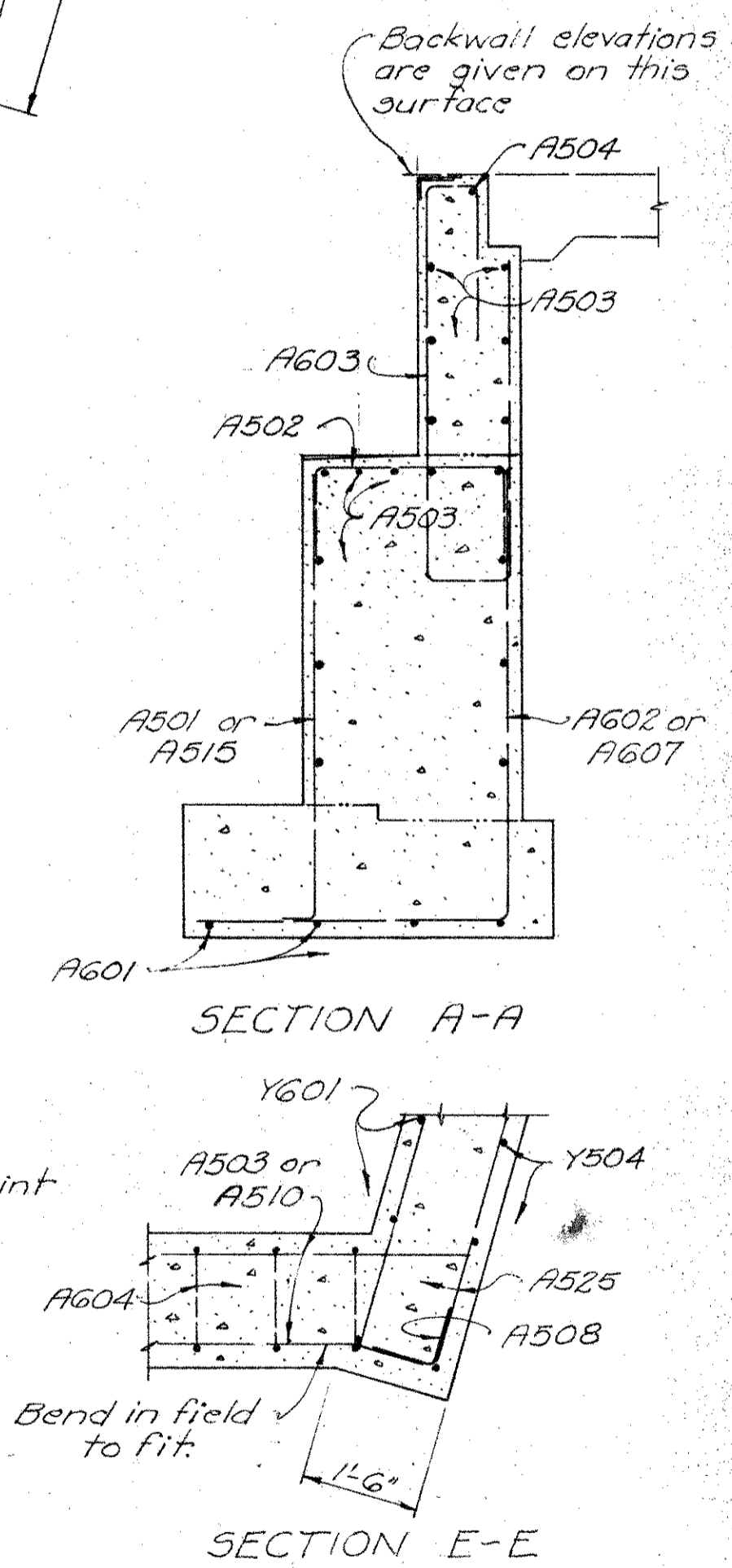
CLA-72-13.20
 CLA-68-12.39
 CHP-68-0.00



PART PLAN



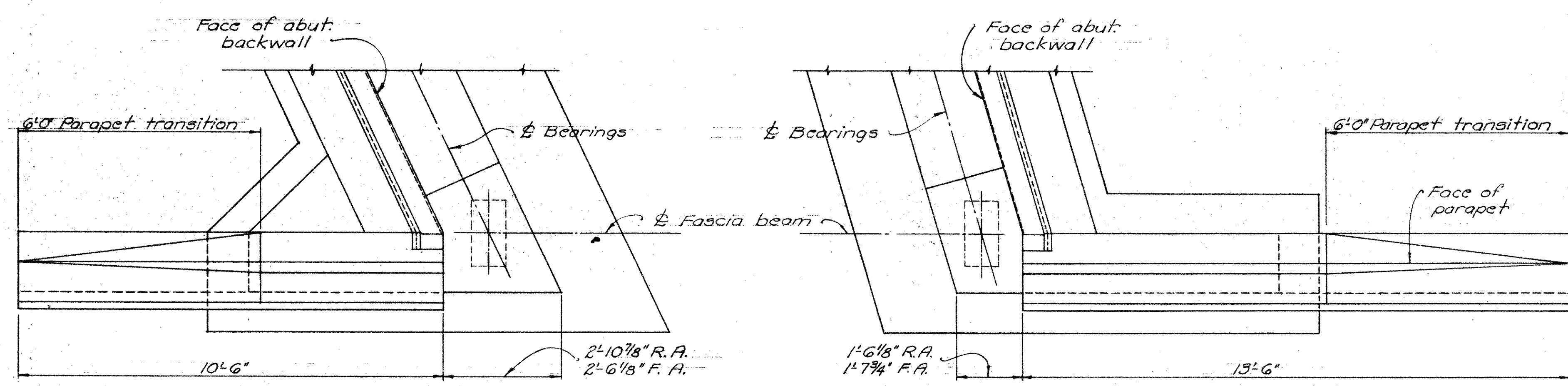
PART ELEVATION



Refer to sheet 7113 for additional notes and details.

| | | | | | | |
|---|-------|--------|---------|----------|---------|---------|
| STATE OF OHIO DEPARTMENT OF HIGHWAYS DIVISION OF DESIGN AND CONSTRUCTION BUREAU OF BRIDGES | | | | | | 6/13 |
| FORWARD ABUTMENT DETAILS BRIDGE NO. CLA-72-1329 over PENN CENTRAL and the ERIE LACKAWANNA R.R. | | | | | | |
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| MPB | MPB | | WCK | BFG | 5-27-69 | |

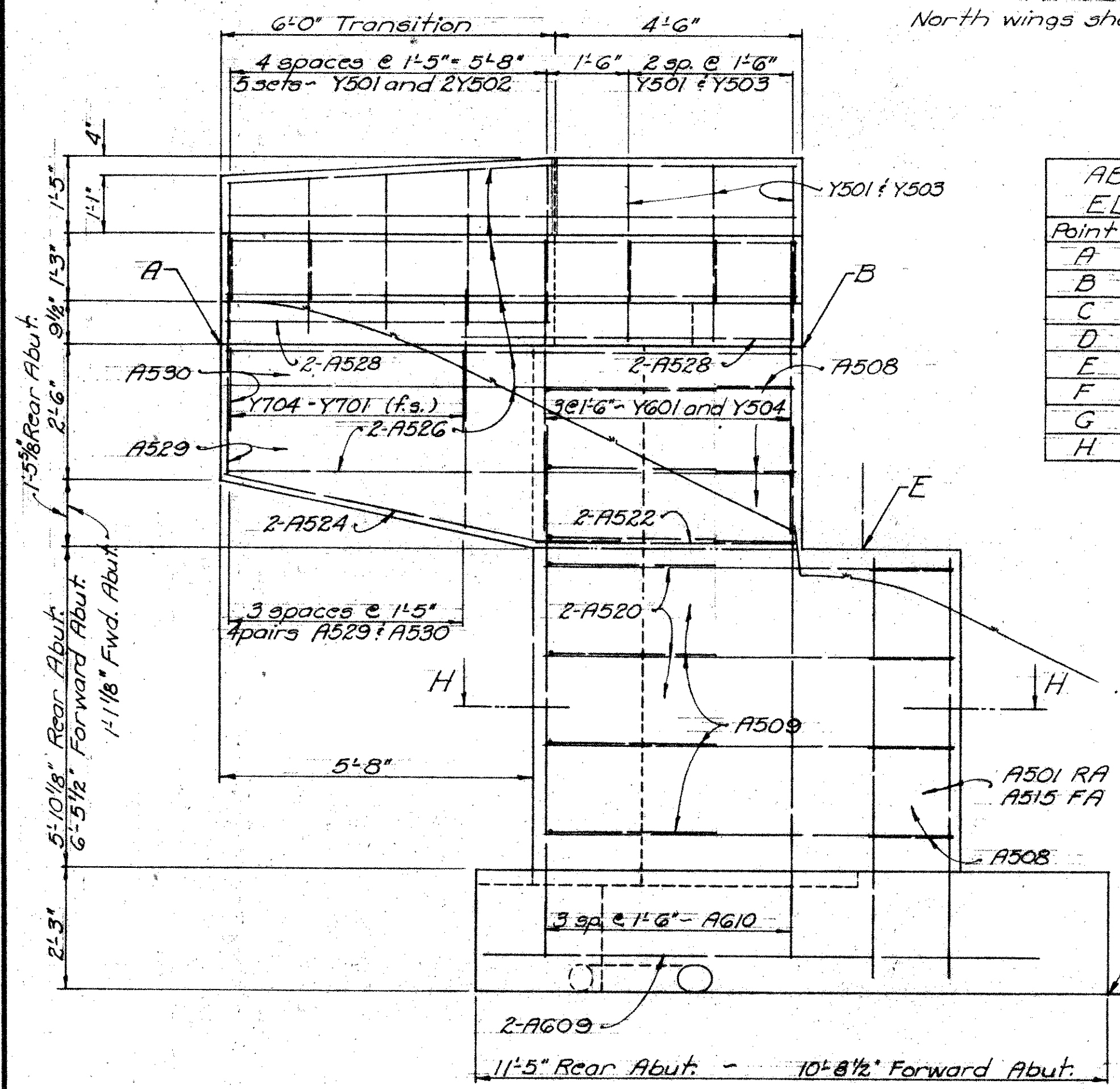
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 CLA-68-12.39
 CHP-68-0.00



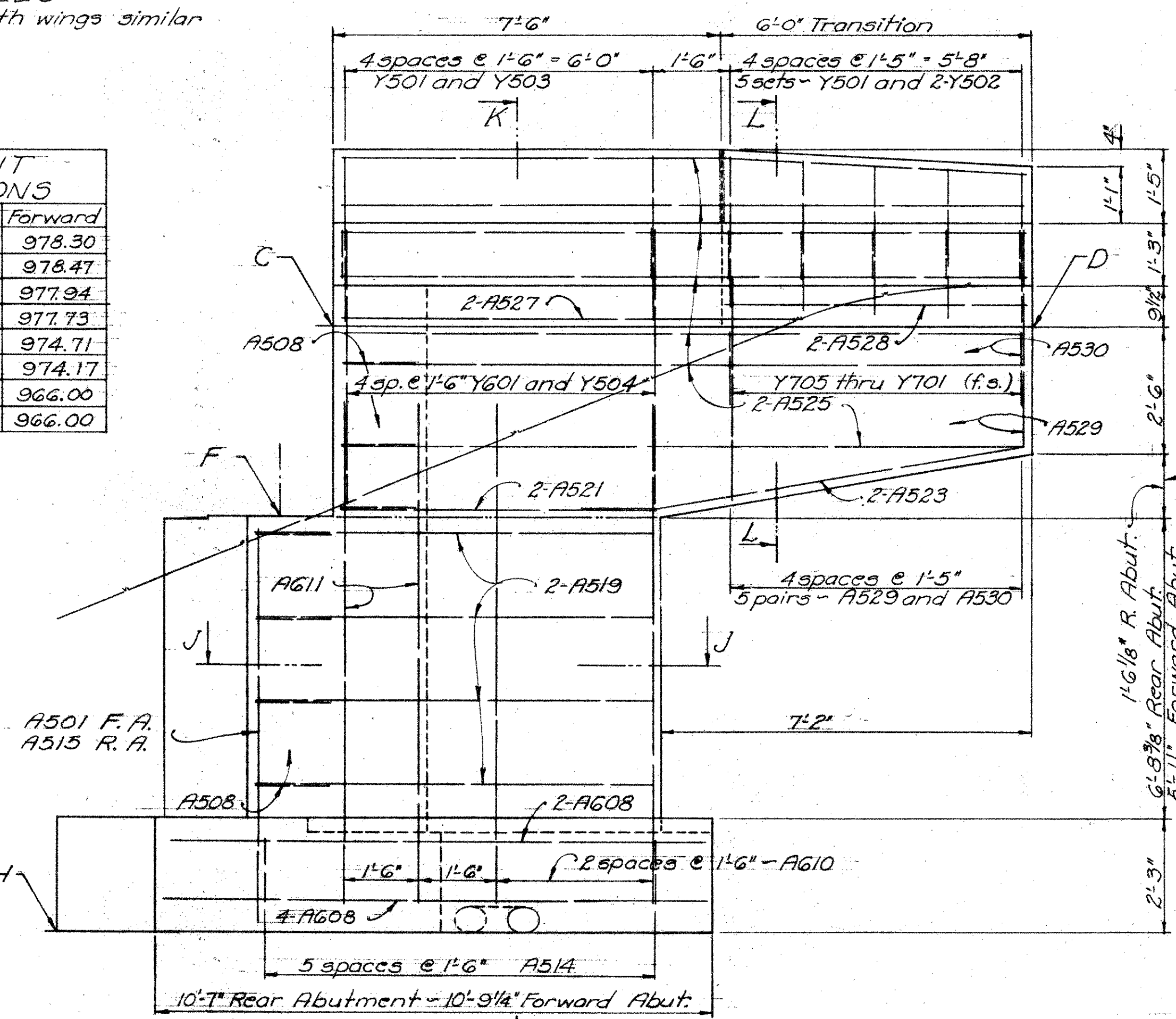
PLAN
 Right Rear and Left Forward

PLAN
 Left Rear and Right Forward

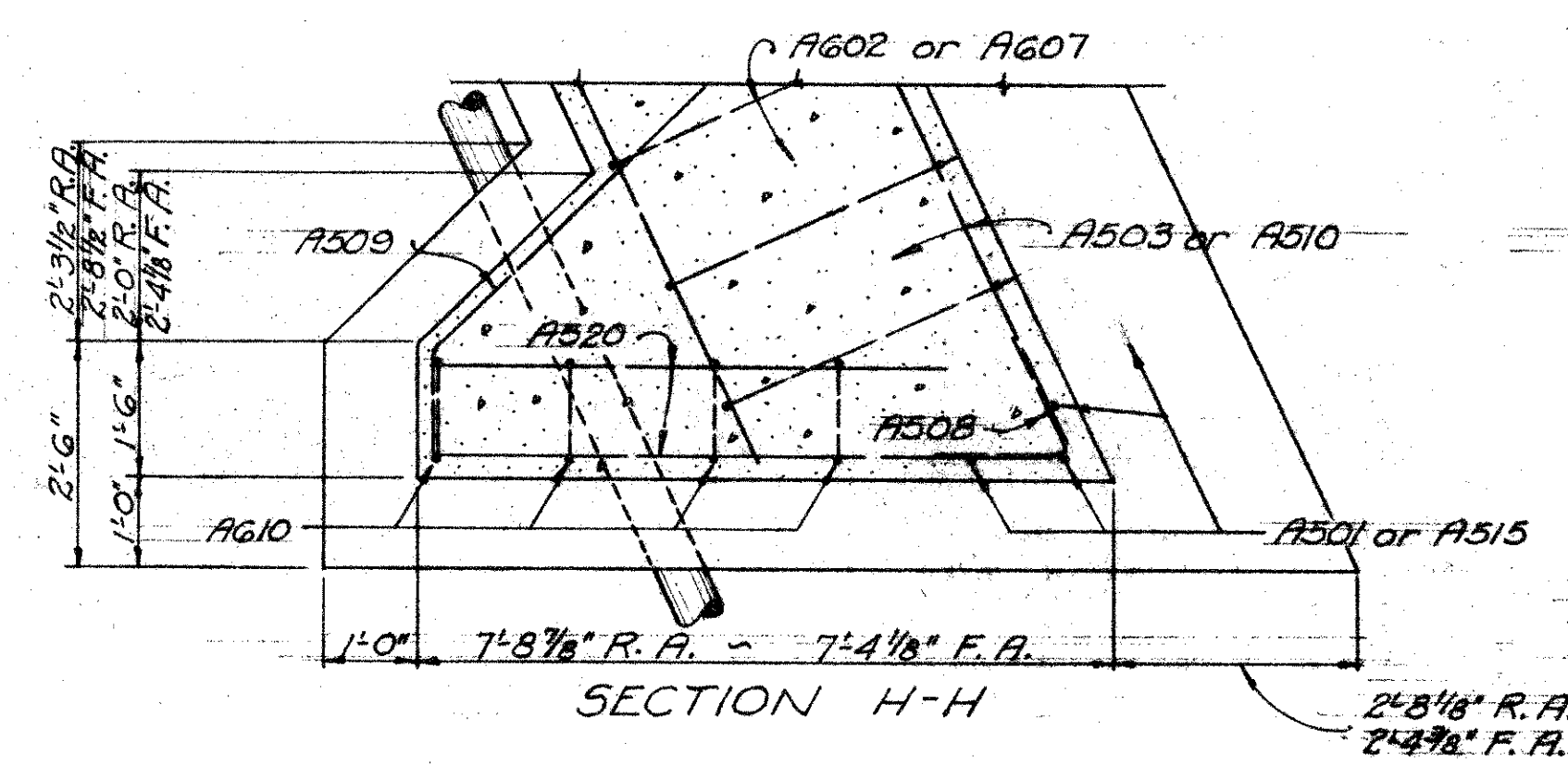
WING WALLS
 North wings shown, South wings similar



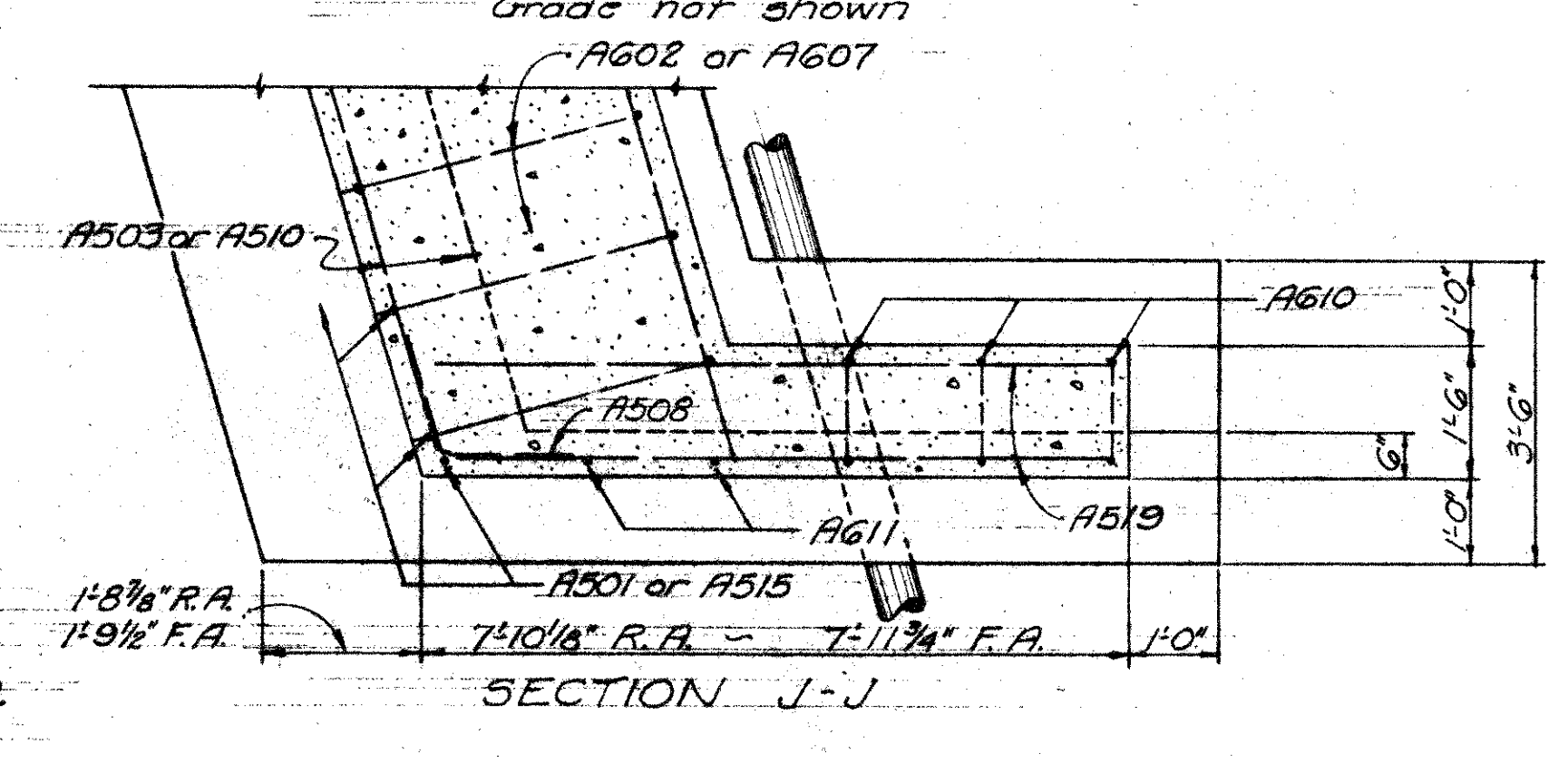
VIEW C-C
 Grade not shown



VIEW B-B
 Grade not shown

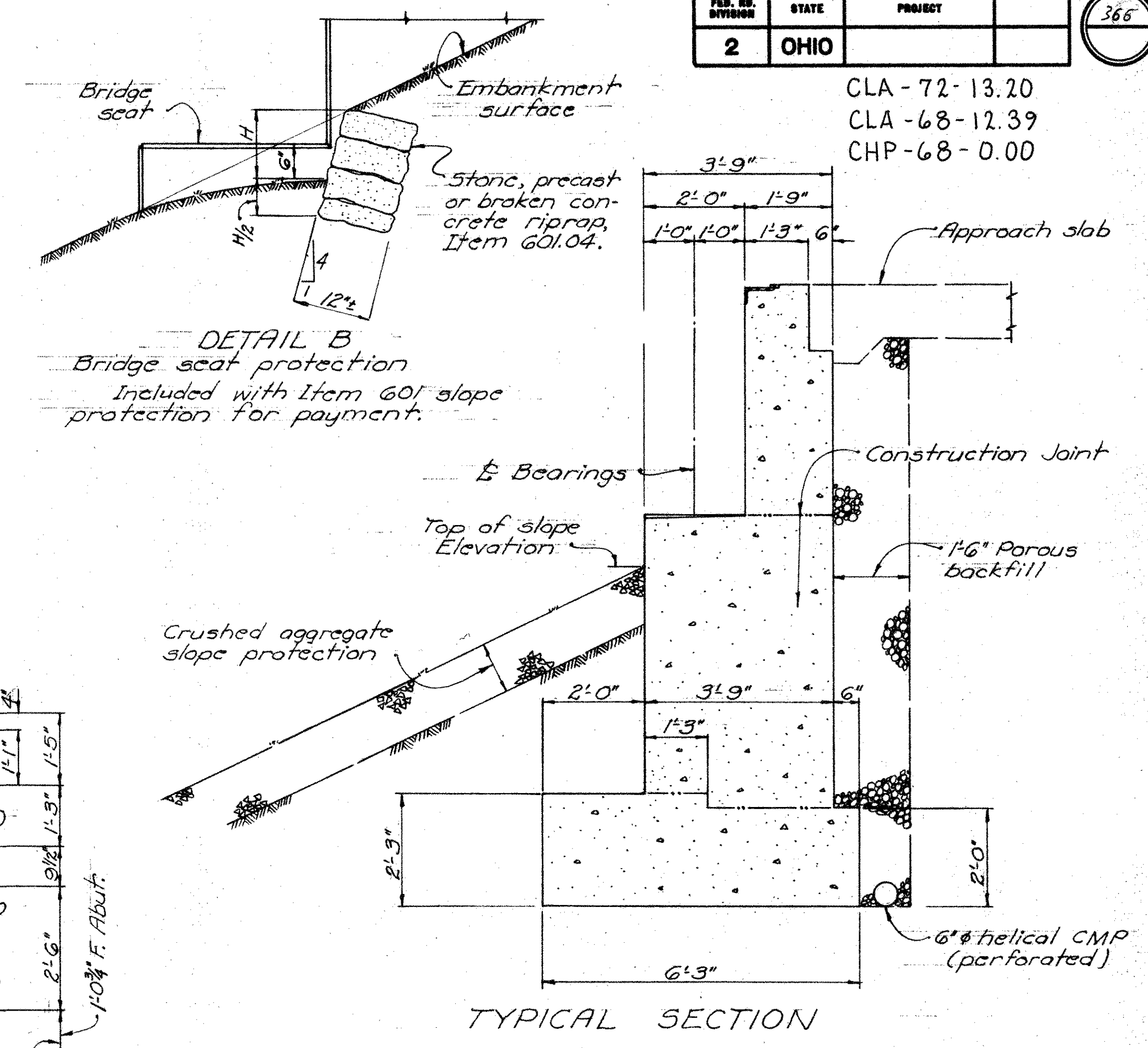


SECTION H-H



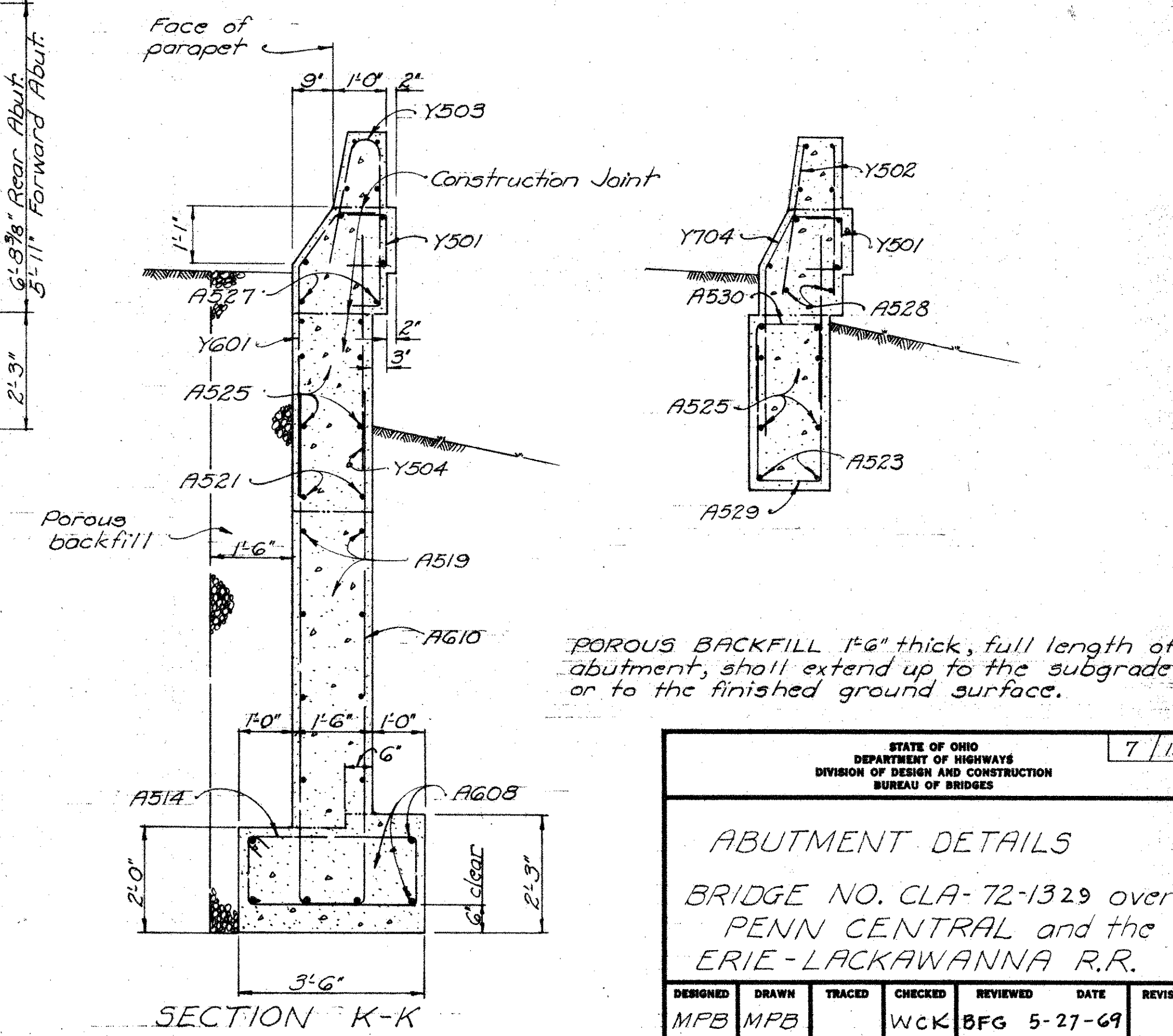
SECTION J-J

| ABUTMENT ELEVATIONS | | |
|---------------------|--------|---------|
| Point | Rear | Forward |
| A | 982.86 | 978.30 |
| B | 982.69 | 978.47 |
| C | 983.55 | 977.94 |
| D | 983.76 | 977.73 |
| E | 978.89 | 974.71 |
| F | 979.75 | 974.17 |
| G | 970.80 | 966.00 |
| H | 970.80 | 966.00 |



DETAIL B
 Bridge seat protection
 Included with Item 601 slope
 protection for payment.

TYPICAL SECTION



SECTION K-K

POROUS BACKFILL 1'-6" thick, full length of abutment, shall extend up to the subgrade or to the finished ground surface.

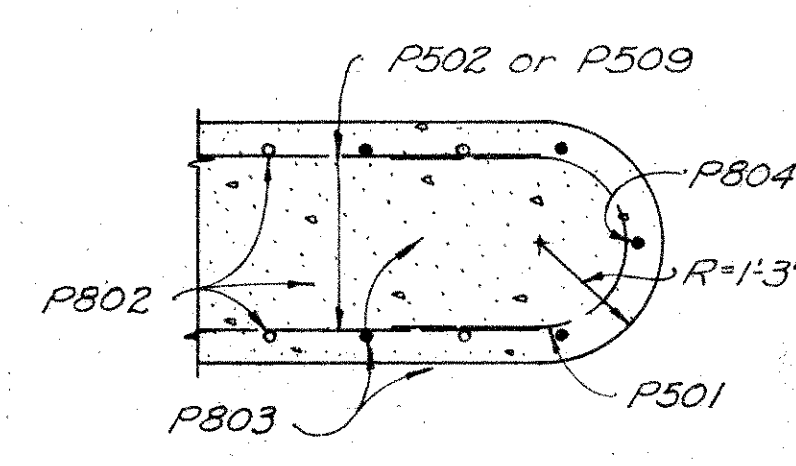
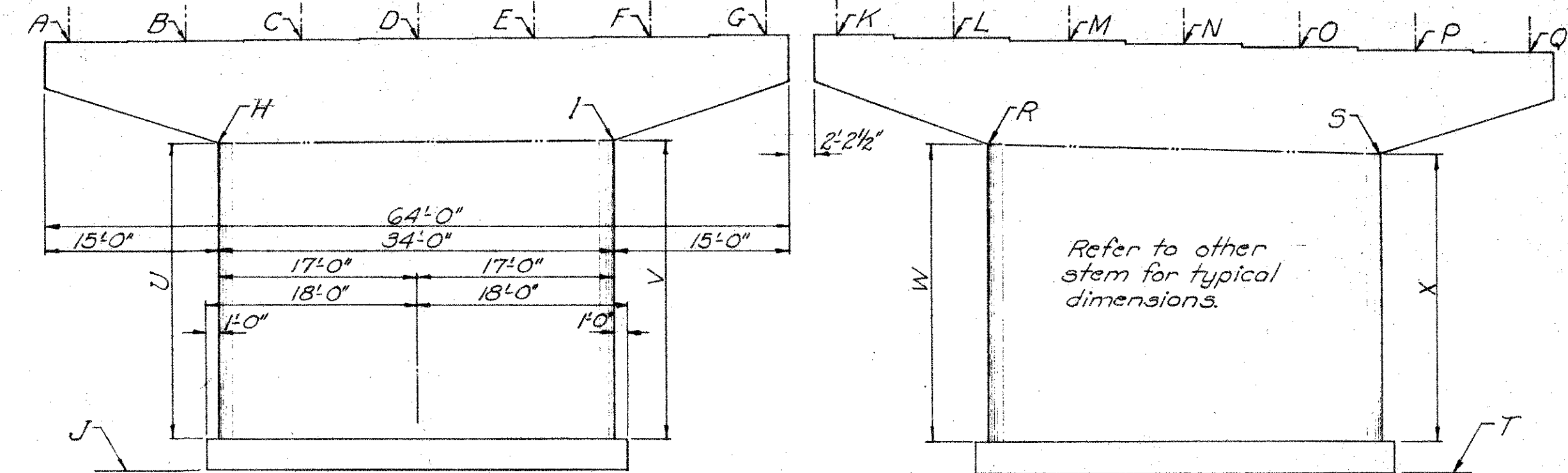
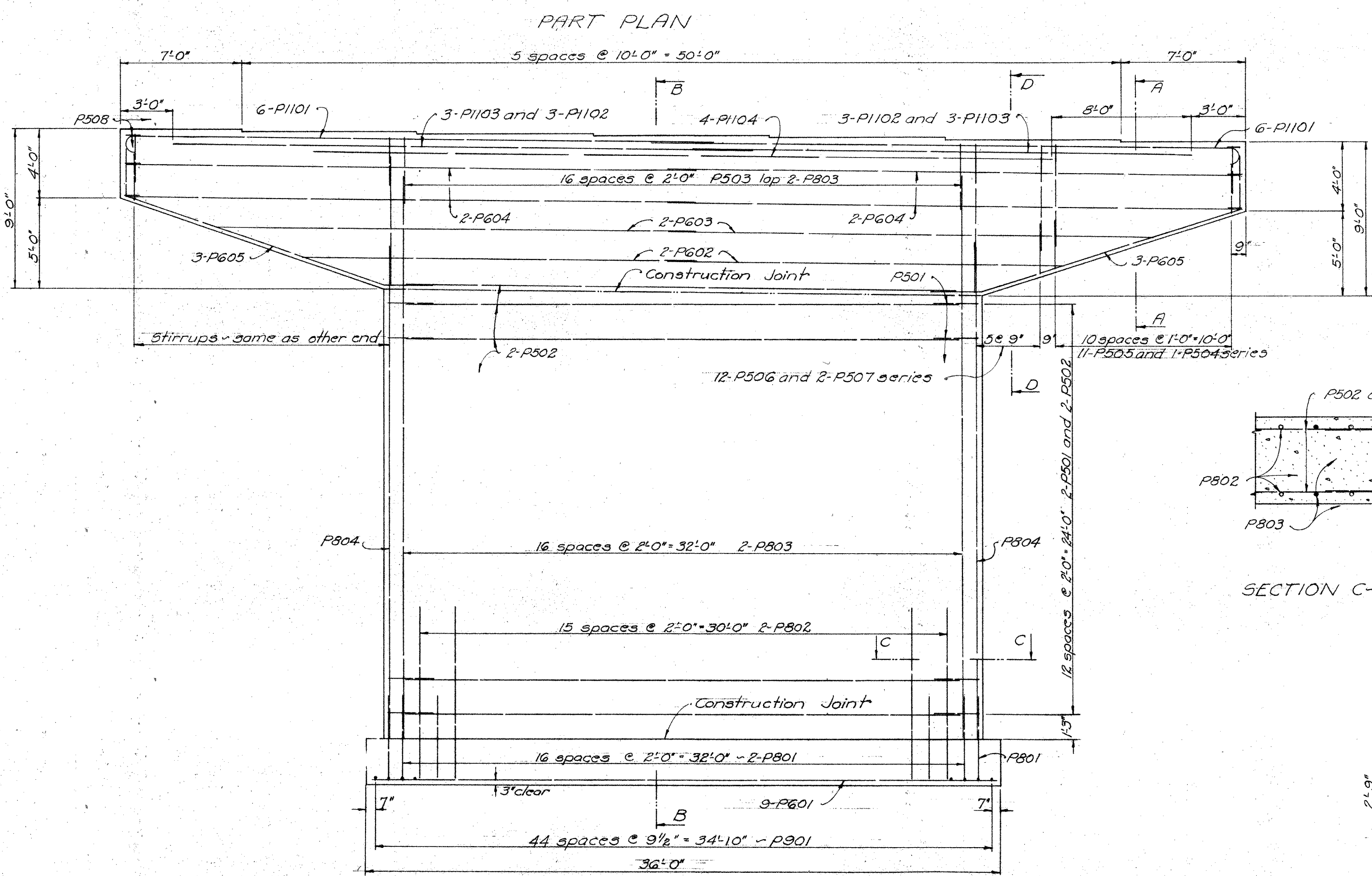
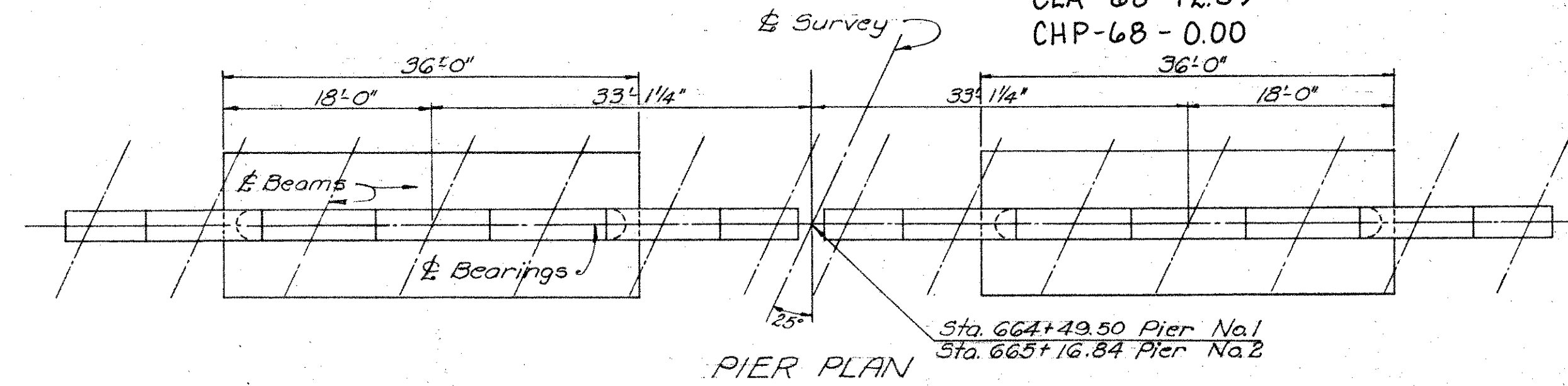
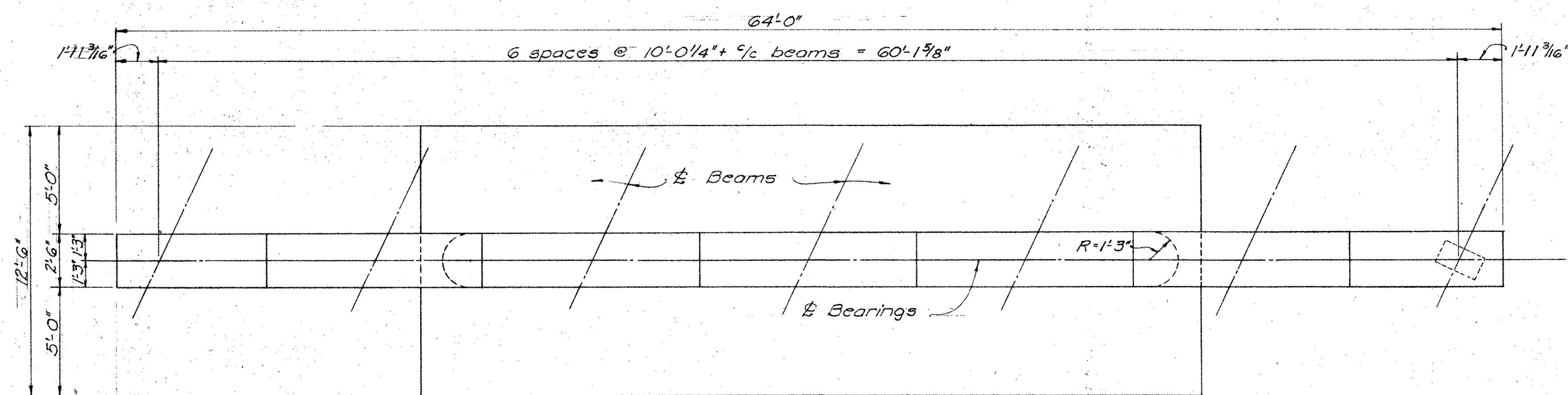
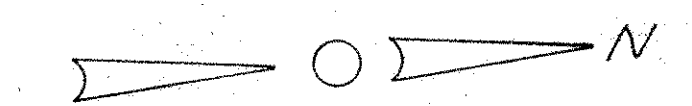
STATE OF OHIO
 DEPARTMENT OF HIGHWAYS
 DIVISION OF DESIGN AND CONSTRUCTION
 BUREAU OF BRIDGES

7/13

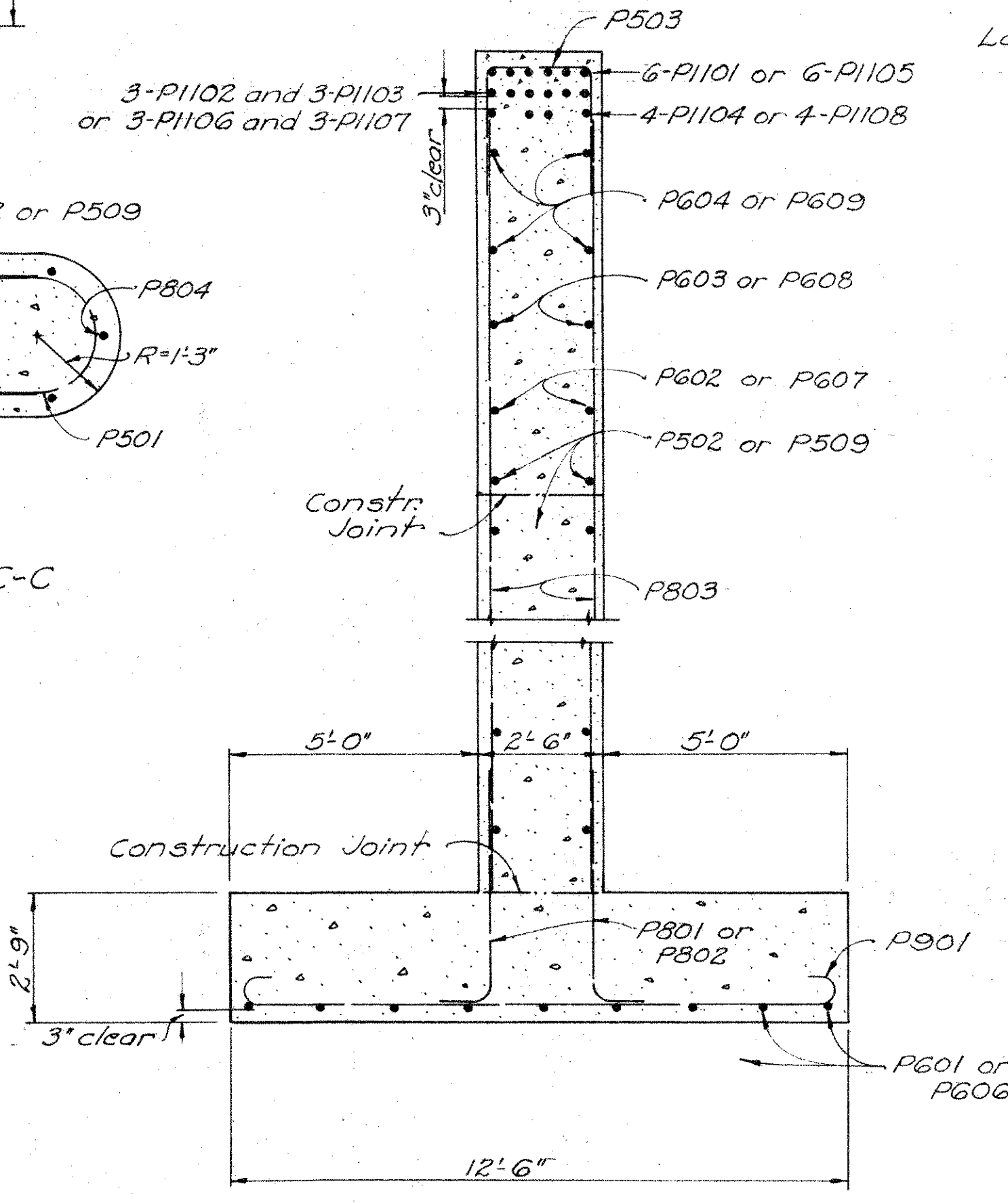
ABUTMENT DETAILS
 BRIDGE NO. CLA-72-1329 over
 PENN CENTRAL and the
 ERIE-LACKAWANNA R.R.

| | | | | | | |
|----------|-------|--------|---------|----------|---------|---------|
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| MPB | MPB | | WCK | BFG | 5-27-69 | |

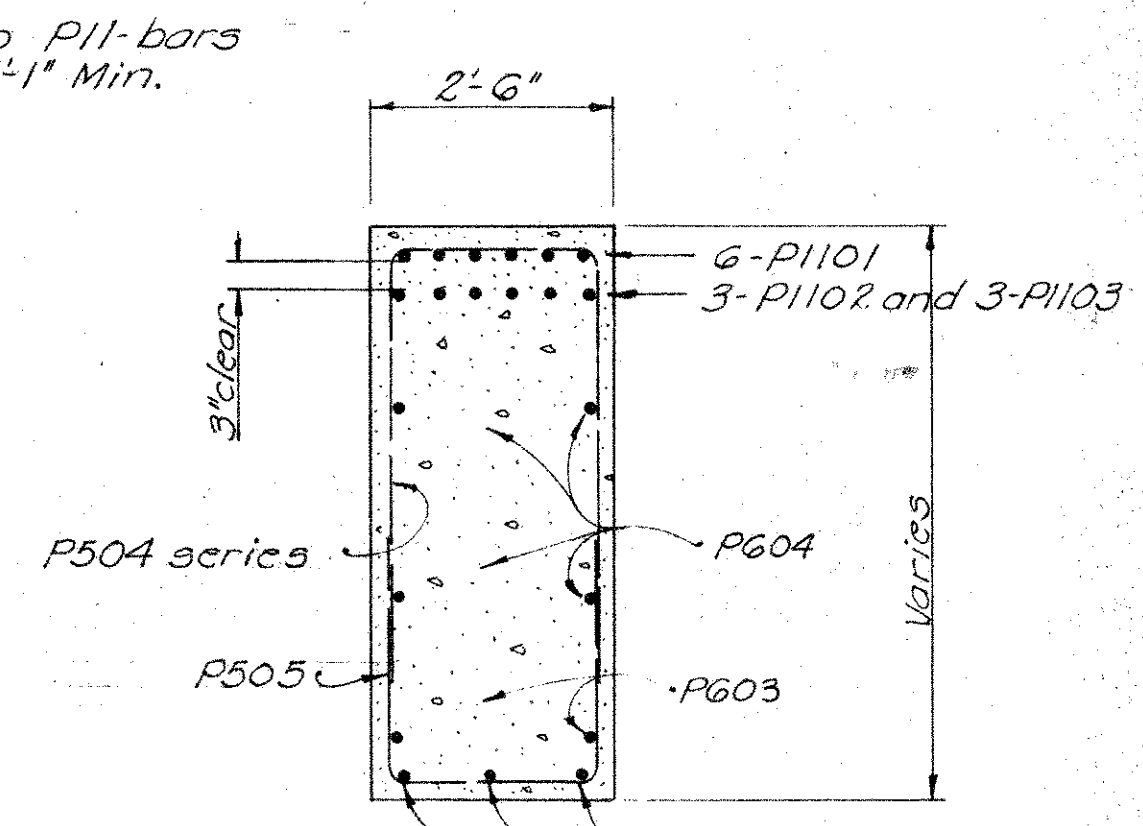
CLA-72-13.20
 CLA-68-12.39
 CHP-68-0.00



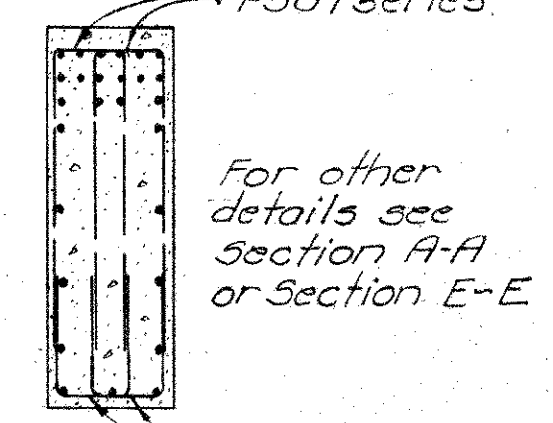
SECTION C-C



SECTION B-B



SECTION A-A



SECTION D-D

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 FEB 08 1985

PART ELEVATION

ELEVATIONS AND DIMENSIONS

| LOCATION | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X |
|------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|------------|-------------|------------|------------|
| PIER NO. 1 | 978.51 | 978.58 | 978.66 | 978.71 | 978.78 | 978.86 | 978.95 | 969.51 | 969.95 | 941.10 | 978.91 | 978.70 | 978.49 | 978.28 | 978.06 | 977.85 | 977.64 | 969.91 | 968.64 | 940.40 | 25'-8" | 26'-1 1/4" | 26'-9 1/8" | 25'-5 7/8" |
| PIER NO. 2 | 977.29 | 977.36 | 977.43 | 977.48 | 977.56 | 977.63 | 977.72 | 968.29 | 968.72 | 940.00 | 977.68 | 977.47 | 977.26 | 977.05 | 976.87 | 976.66 | 976.45 | 968.68 | 967.45 | 939.30 | 25'-6 1/2" | 25'-11 5/8" | 26'-7 1/2" | 25'-4 3/4" |

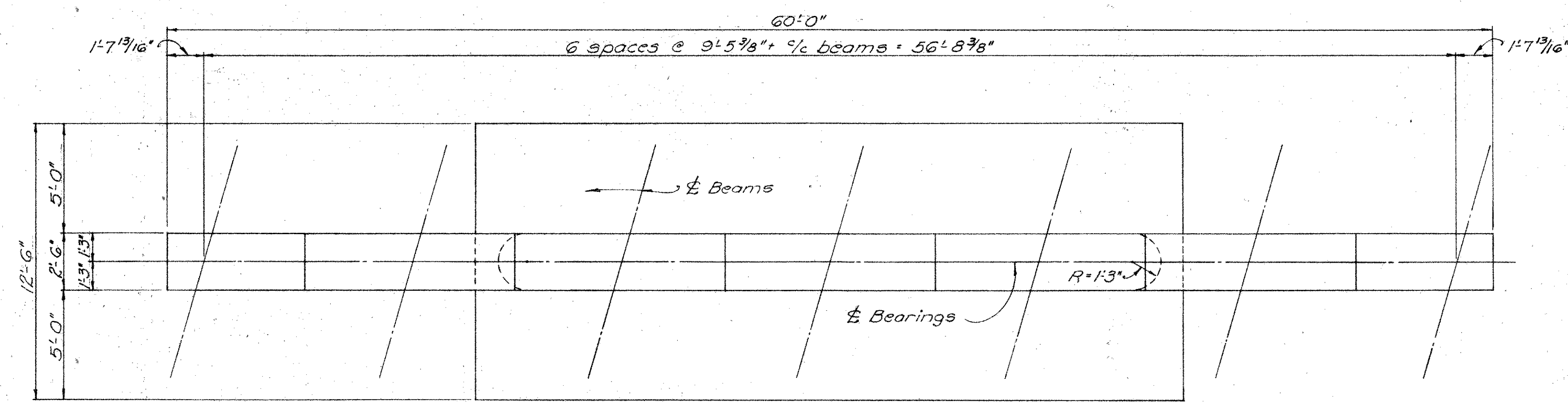
STATE OF OHIO
 DEPARTMENT OF HIGHWAYS
 DIVISION OF DESIGN AND CONSTRUCTION
 BUREAU OF BRIDGES

PIER NO. 1 AND PIER NO. 2
 BRIDGE NO. CLA-72-1329 over
 PENN CENTRAL and the
 ERIE - LACKAWANNA R. R.

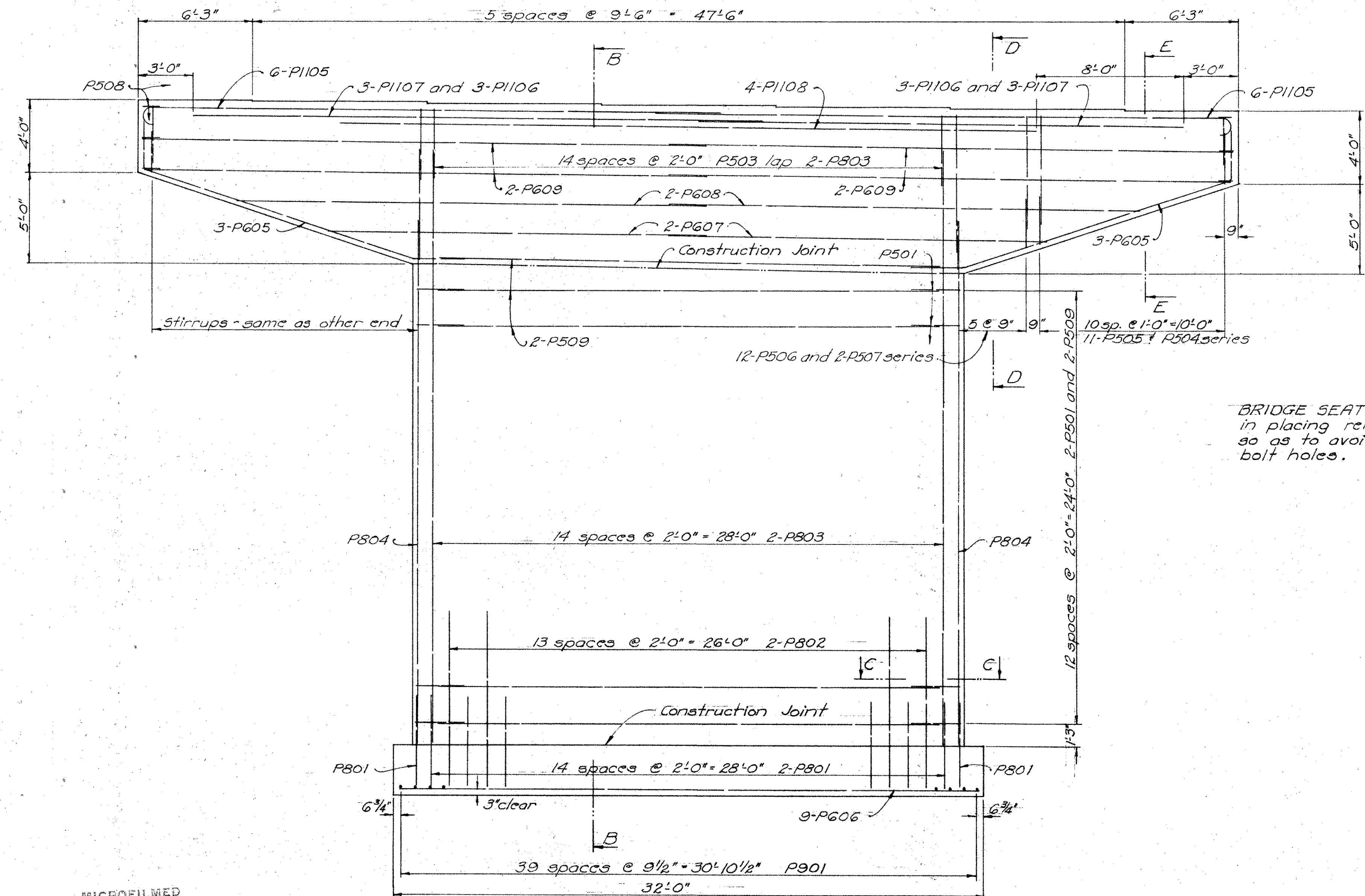
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|----------|-------|--------|---------|----------|---------|---------|
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| MPB | MPB | | WCK | BFG | 5-27-69 | |

CLA-72-13.20
 CLA-68-12.39
 CHP-68-0.00

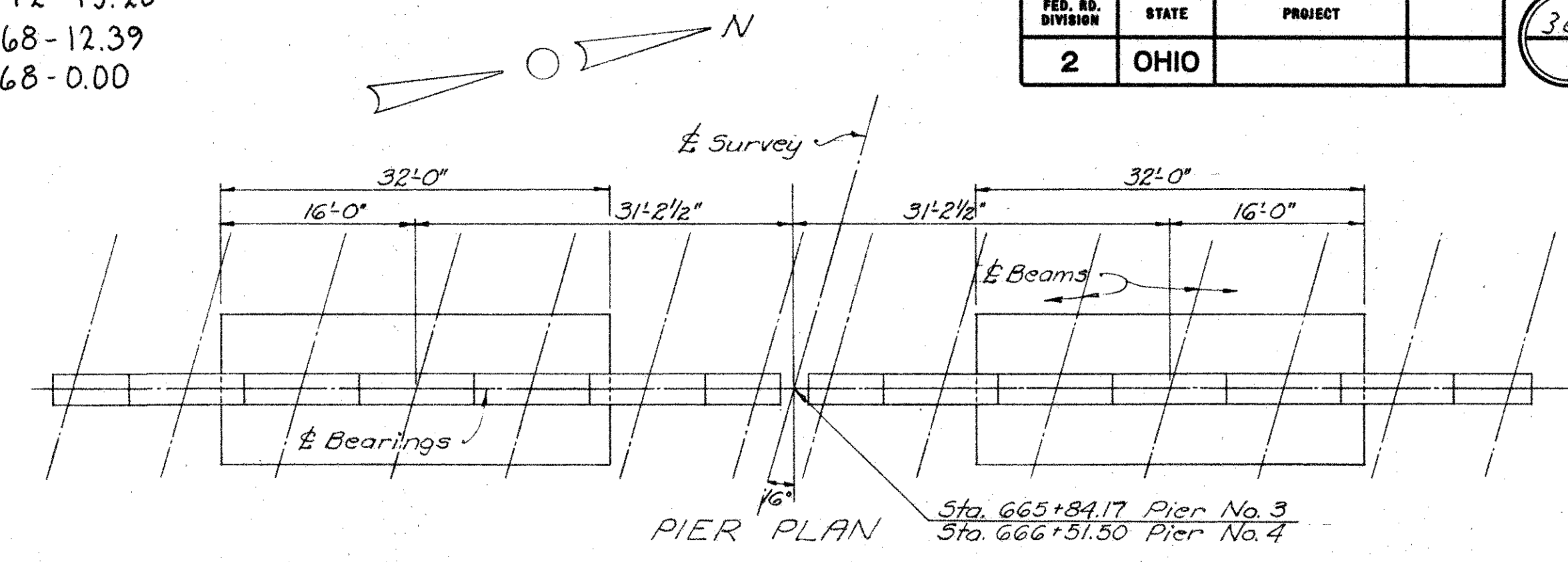
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| FED. RD. DIVISION | STATE | PROJECT | 368 |
| 2 | OHIO | | |



PART PLAN

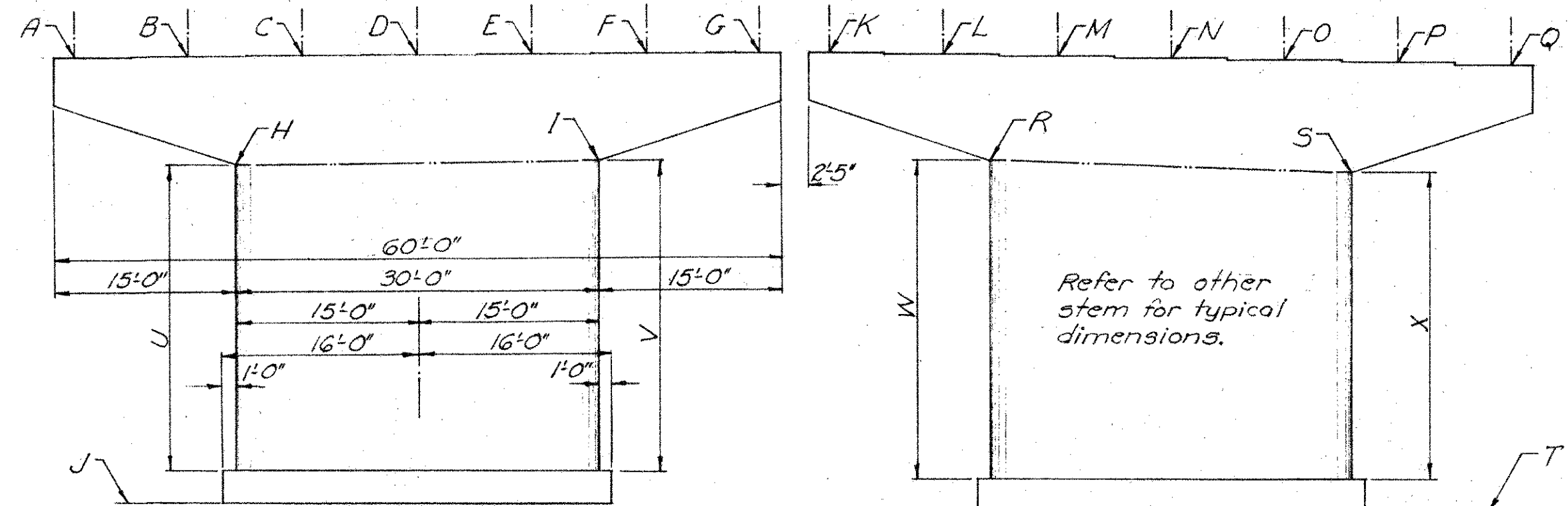


PART ELEVATION

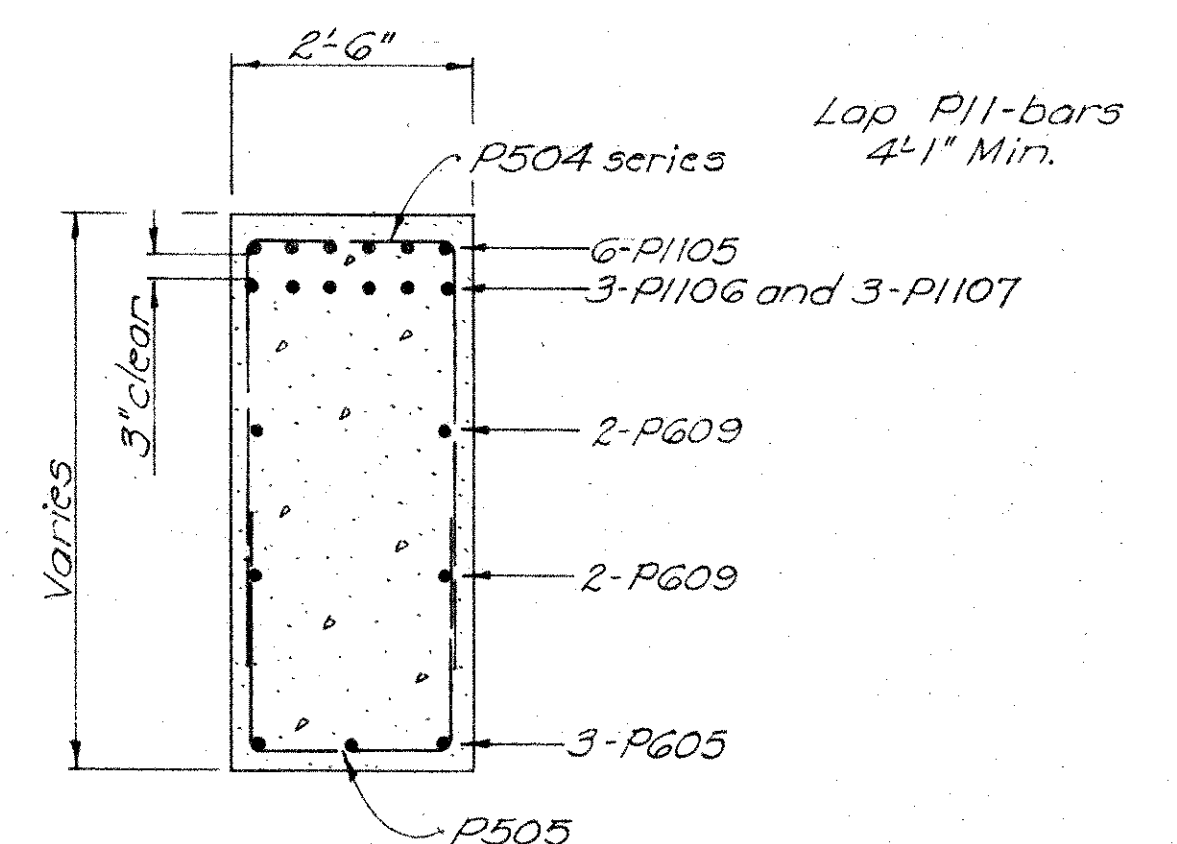


PIER PLAN

Sta. 665+84.17 Pier No. 3
 Sta. 666+51.50 Pier No. 4



PIER ELEVATION



SECTION E-E

BRIDGE SEAT REINFORCING: Special care shall be taken in placing reinforcing steel in the Pier No. 3 bridge seat so as to avoid interference with the drilling of anchor bolt holes.

For Sections B-B, C-C and D-D, refer to sheet 8/13.

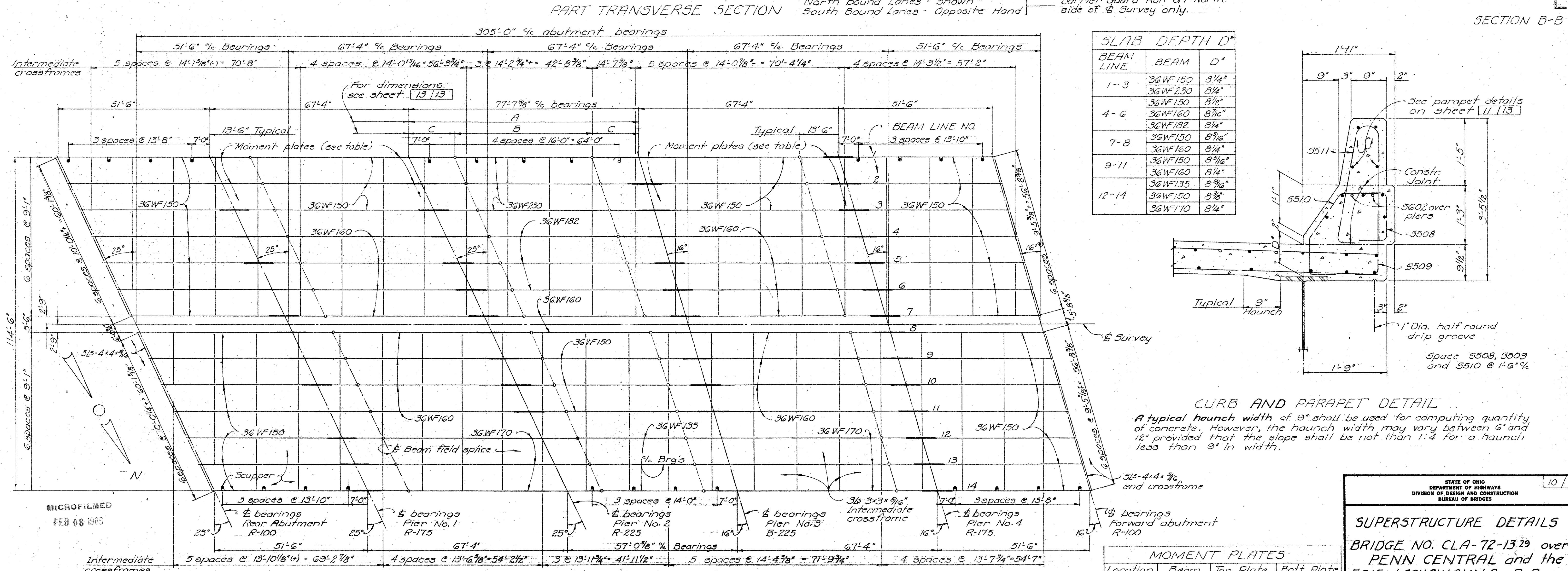
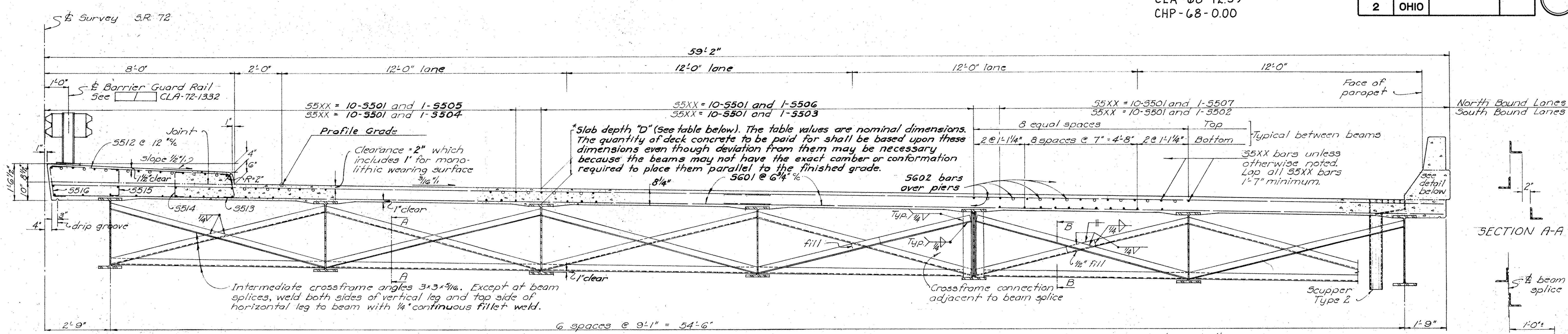
MICROFILMED
 FEB 08 1965

| LOCATION | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X |
|------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|------------|-------------|------------|------------|
| PIER NO. 3 | 976.04 | 976.14 | 976.24 | 976.32 | 976.42 | 976.52 | 976.63 | 967.04 | 967.63 | 938.90 | 976.61 | 976.42 | 976.24 | 976.06 | 975.91 | 975.72 | 975.54 | 967.61 | 966.54 | 938.50 | 25'-4 5/8" | 25'-11 3/4" | 26'-4 3/8" | 25'-3 1/2" |
| PIER NO. 4 | 975.12 | 975.21 | 975.31 | 975.39 | 975.49 | 975.59 | 975.71 | 966.12 | 966.71 | 937.80 | 975.68 | 975.50 | 975.32 | 975.13 | 974.94 | 974.76 | 974.58 | 966.68 | 965.58 | 937.40 | 25'-6 7/8" | 26'-1 7/8" | 26'-6 3/8" | 25'-5 1/8" |

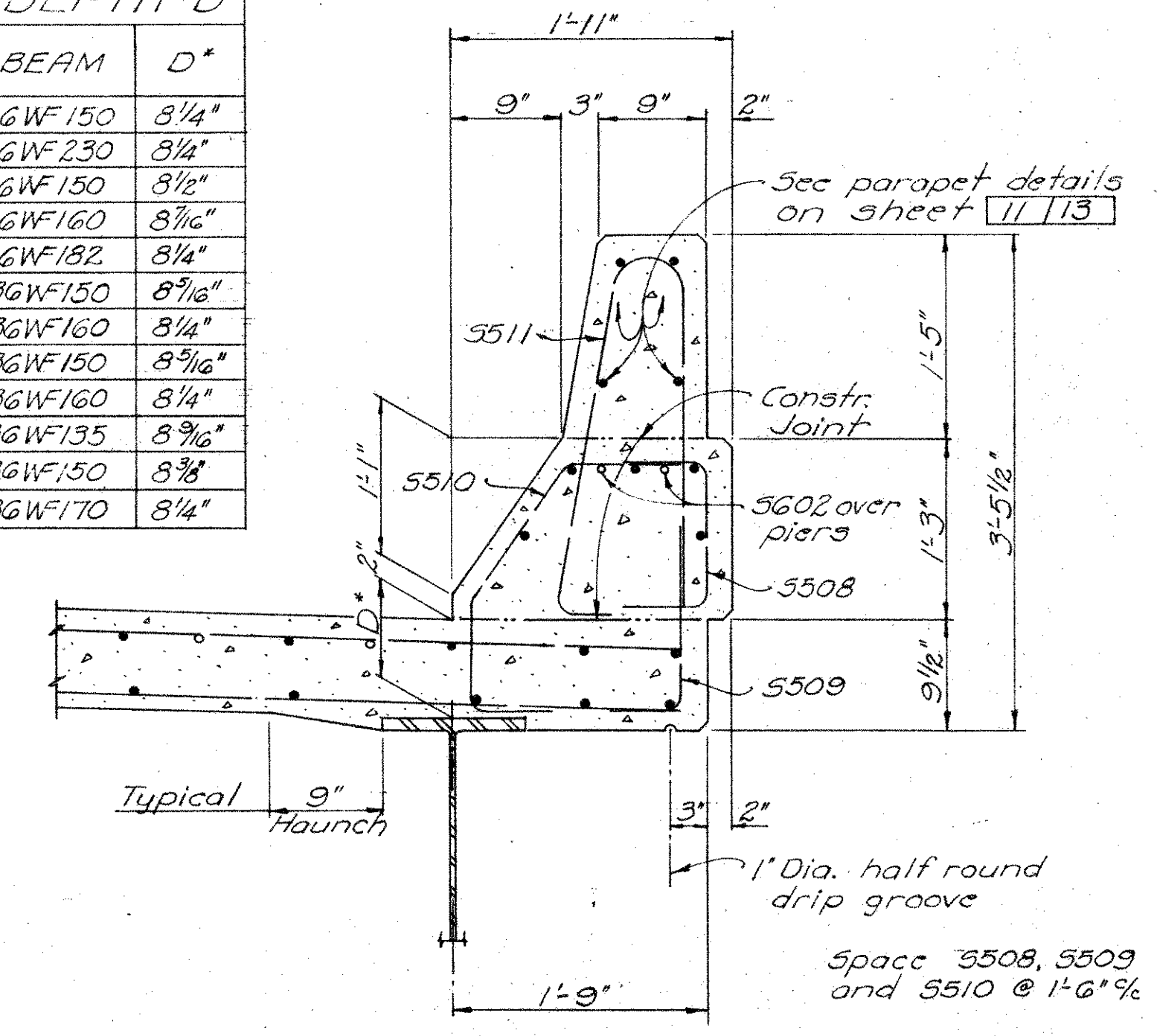
STATE OF OHIO
 DEPARTMENT OF HIGHWAYS
 DIVISION OF DESIGN AND CONSTRUCTION
 BUREAU OF BRIDGES

PIER NO. 3 AND PIER NO. 4
 BRIDGE NO. CLA-72-1329 over
 PENN CENTRAL and the
 ERIE - LACKAWANNA R. R.

| | | | | | | |
|----------|-------|--------|---------|----------|---------|---------|
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| MPB | MPB | | WCK | BFG | 5-27-69 | |



| BEAM LINE | BEAM | D* |
|-----------|---------|---------|
| 1-3 | 36WF150 | 8 1/4" |
| | 36WF230 | 8 1/4" |
| 4-6 | 36WF150 | 8 1/2" |
| | 36WF160 | 8 1/4" |
| 7-8 | 36WF150 | 8 1/4" |
| | 36WF160 | 8 1/4" |
| 9-11 | 36WF150 | 8 1/4" |
| | 36WF160 | 8 1/4" |
| 12-14 | 36WF135 | 8 9/16" |
| | 36WF150 | 8 3/4" |
| | 36WF170 | 8 1/4" |



| Location | Beam | Top Plate | Bot. Plate |
|------------|---------|-------------------------|-------------------------|
| Pier 1 & 4 | 36WF150 | 10 1/2" x 1/2" x 9'-0" | 13 1/2" x 1/2" x 9'-0" |
| Pier 2 & 3 | 36WF150 | 10 1/2" x 1/2" x 13'-0" | 13 1/2" x 1/2" x 13'-0" |
| | 36WF160 | 10 1/2" x 1/2" x 13'-0" | 13 1/2" x 1/2" x 13'-0" |

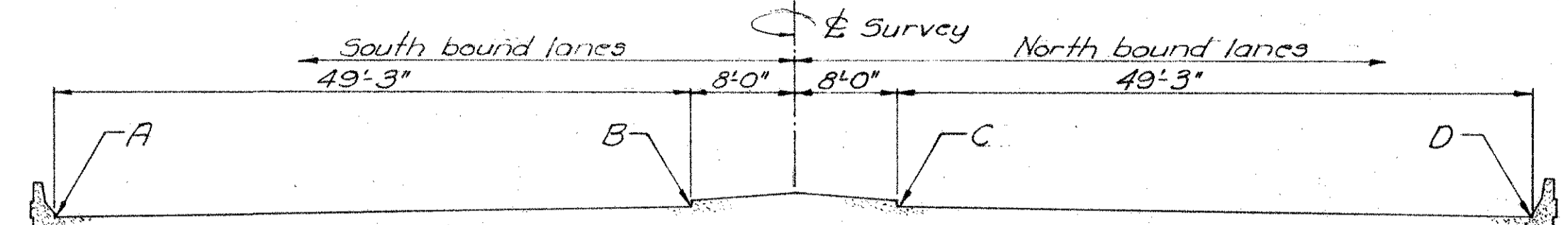
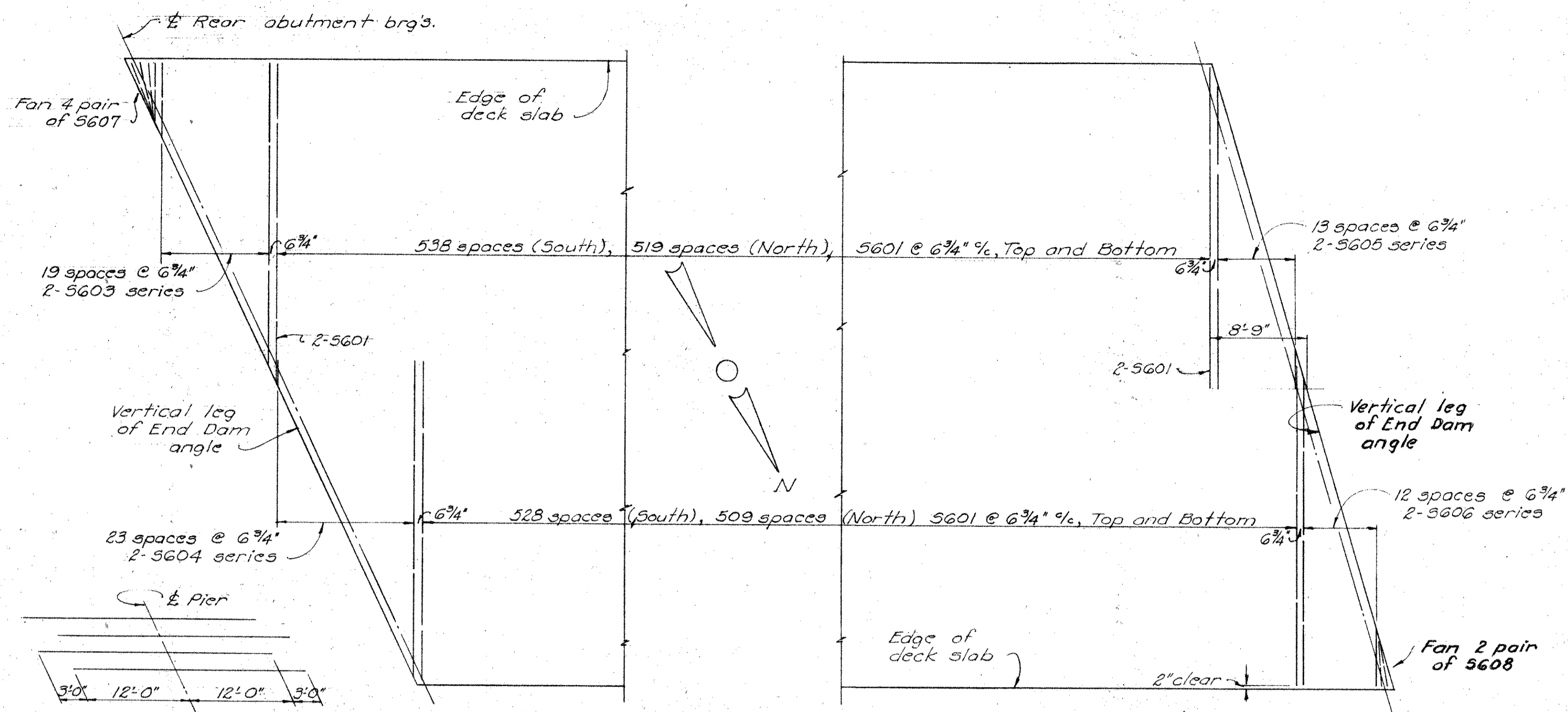
STATE OF OHIO
DEPARTMENT OF HIGHWAYS
DIVISION OF DESIGN AND CONSTRUCTION
BUREAU OF BRIDGES

10/13

SUPERSTRUCTURE DETAILS
BRIDGE NO. CLA-72-13.29 over
PENN CENTRAL and the
ERIE-LACKAWANNA R. R.

| | | | | | | |
|----------|-------|--------|---------|----------|---------|---------|
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| MPB | MPB | | WCK | BFG | 5-27-69 | |

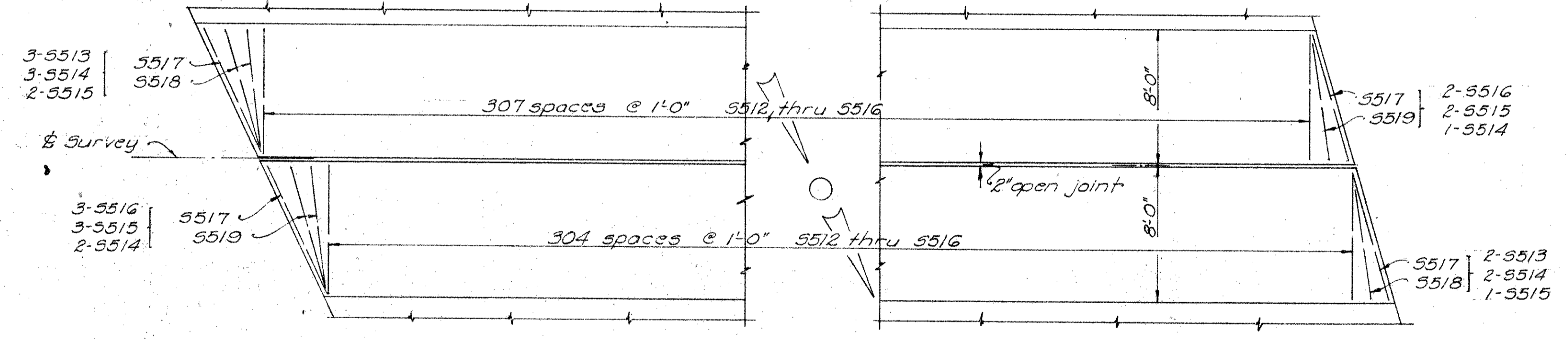
MICROFILMED
FEB 08 1985



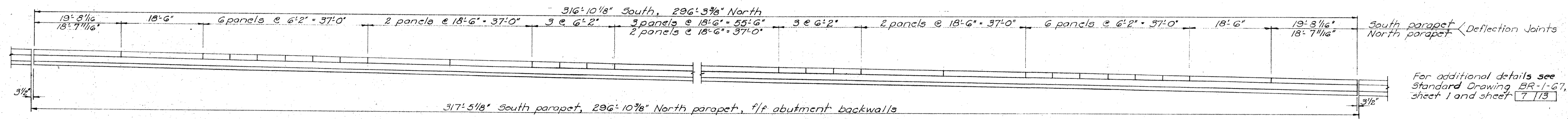
| LOCATION | SOUTH BOUND LANES | | | | NORTH BOUND LANES | | | |
|-----------|-------------------|--------|-----------|--------|-------------------|--------|-----------|--------|
| | A | | B | | C | | D | |
| | Station | Elev. | Station | Elev. | Station | Elev. | Station | Elev. |
| 1/2 Brg's | 663+71.31 | 984.32 | 663+94.27 | 984.72 | 664+01.73 | 984.60 | 664+24.70 | 983.47 |
| 0.25 pt. | 663+84.18 | 984.14 | 664+07.15 | 984.54 | 664+14.61 | 984.41 | 664+37.57 | 983.27 |
| 0.50 pt. | 663+97.06 | 983.93 | 664+20.02 | 984.33 | 664+27.48 | 984.21 | 664+50.45 | 983.07 |
| 0.75 pt. | 664+09.93 | 983.71 | 664+32.90 | 984.11 | 664+40.36 | 984.00 | 664+63.32 | 982.85 |
| 1/2 Brg's | 664+22.81 | 983.50 | 664+45.77 | 983.90 | 664+53.23 | 983.78 | 664+76.20 | 982.64 |
| Splice | 664+36.30 | 983.29 | 664+59.27 | 983.69 | 664+66.73 | 983.57 | 664+89.70 | 982.44 |
| 0.5 pt. | 664+56.47 | 982.98 | 664+79.44 | 983.39 | 664+86.90 | 983.27 | 665+09.87 | 982.14 |
| 0.75 pt. | 664+73.31 | 982.70 | 664+96.27 | 983.11 | 665+03.73 | 982.99 | 665+26.70 | 981.86 |
| 1/2 Brg's | 664+90.14 | 982.42 | 665+13.11 | 982.82 | 665+20.57 | 982.70 | 665+43.53 | 981.57 |
| Splice | 665+05.64 | 982.19 | 665+28.68 | 982.61 | 665+33.78 | 982.50 | 665+55.03 | 981.38 |
| 0.5 pt. | 665+28.95 | 981.84 | 665+47.49 | 982.29 | 665+53.52 | 982.20 | 665+72.06 | 981.12 |
| Splice | 665+52.25 | 981.45 | 665+68.09 | 981.95 | 665+73.25 | 981.87 | 665+89.09 | 980.84 |
| 1/2 Brg's | 665+67.75 | 981.18 | 665+81.88 | 981.72 | 665+86.46 | 981.65 | 666+00.59 | 980.65 |
| 0.25 pt. | 665+84.59 | 980.92 | 665+98.71 | 981.47 | 666+03.29 | 981.40 | 666+17.42 | 980.41 |
| 0.50 pt. | 666+01.42 | 980.66 | 666+15.54 | 981.21 | 666+20.13 | 981.14 | 666+34.26 | 980.15 |
| Splice | 666+21.59 | 980.33 | 666+35.71 | 980.87 | 666+40.29 | 980.80 | 666+54.42 | 979.80 |
| 1/2 Brg's | 666+35.09 | 980.10 | 666+49.21 | 980.64 | 666+53.80 | 980.57 | 666+67.92 | 979.58 |
| 0.25 pt. | 666+47.96 | 979.91 | 666+62.08 | 980.45 | 666+66.68 | 980.37 | 666+80.79 | 979.37 |
| 0.50 pt. | 666+60.84 | 979.71 | 666+74.96 | 980.25 | 666+79.55 | 980.18 | 666+93.67 | 979.17 |
| 0.75 pt. | 666+73.71 | 979.50 | 666+87.83 | 980.05 | 666+92.43 | 979.96 | 667+06.54 | 978.97 |
| 1/2 Brg's | 666+86.59 | 979.28 | 667+00.71 | 979.82 | 667+05.30 | 979.75 | 667+19.42 | 978.75 |

*Elevations are given for the surface of the deck prior to the placing of deck concrete.

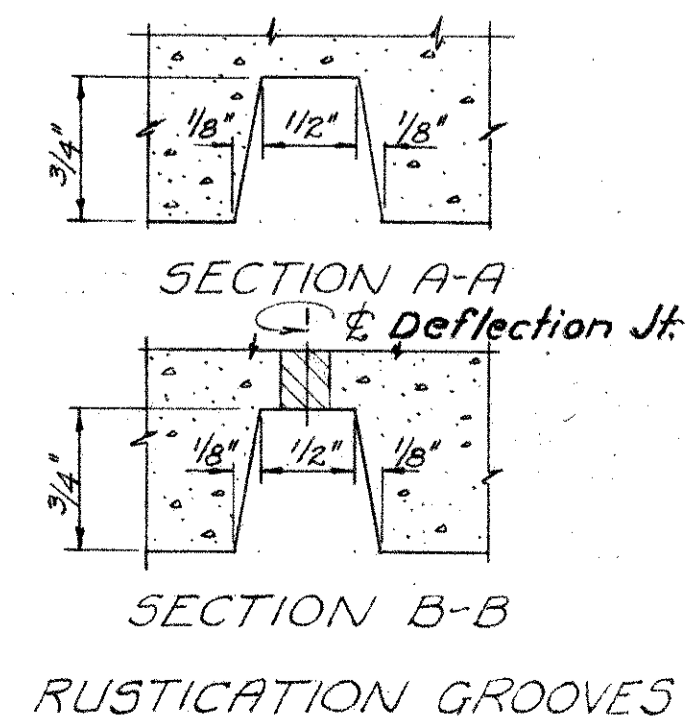
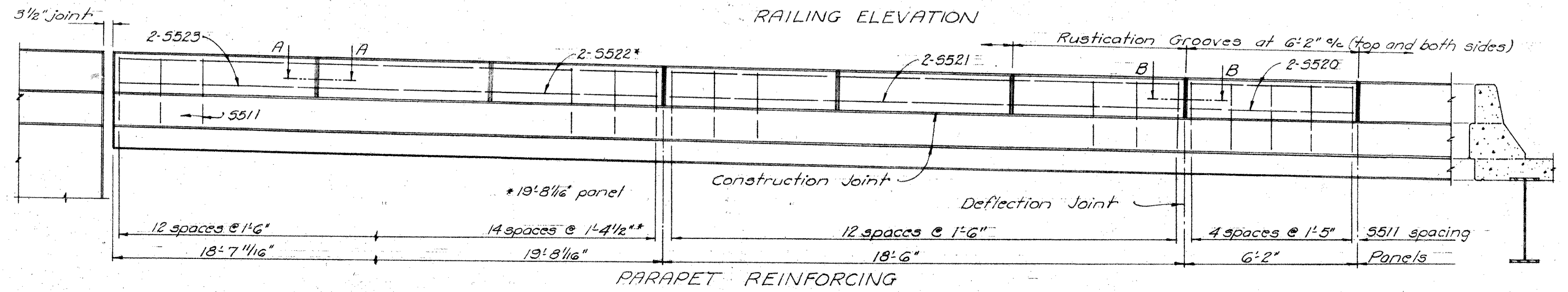
STAGGER OF 5602 BARS OVER PIERS
 DECK SLAB PLAN
 North Bound or South Bound Lanes



PART PLAN - RAISED MEDIAN



For additional details see Standard Drawing BR-1-67, sheet 1 and sheet 7/13



SECTION A-A
 SECTION B-B
 RUSTICATION GROOVES

STATE OF OHIO
 DEPARTMENT OF HIGHWAYS
 DIVISION OF DESIGN AND CONSTRUCTION
 BUREAU OF BRIDGES

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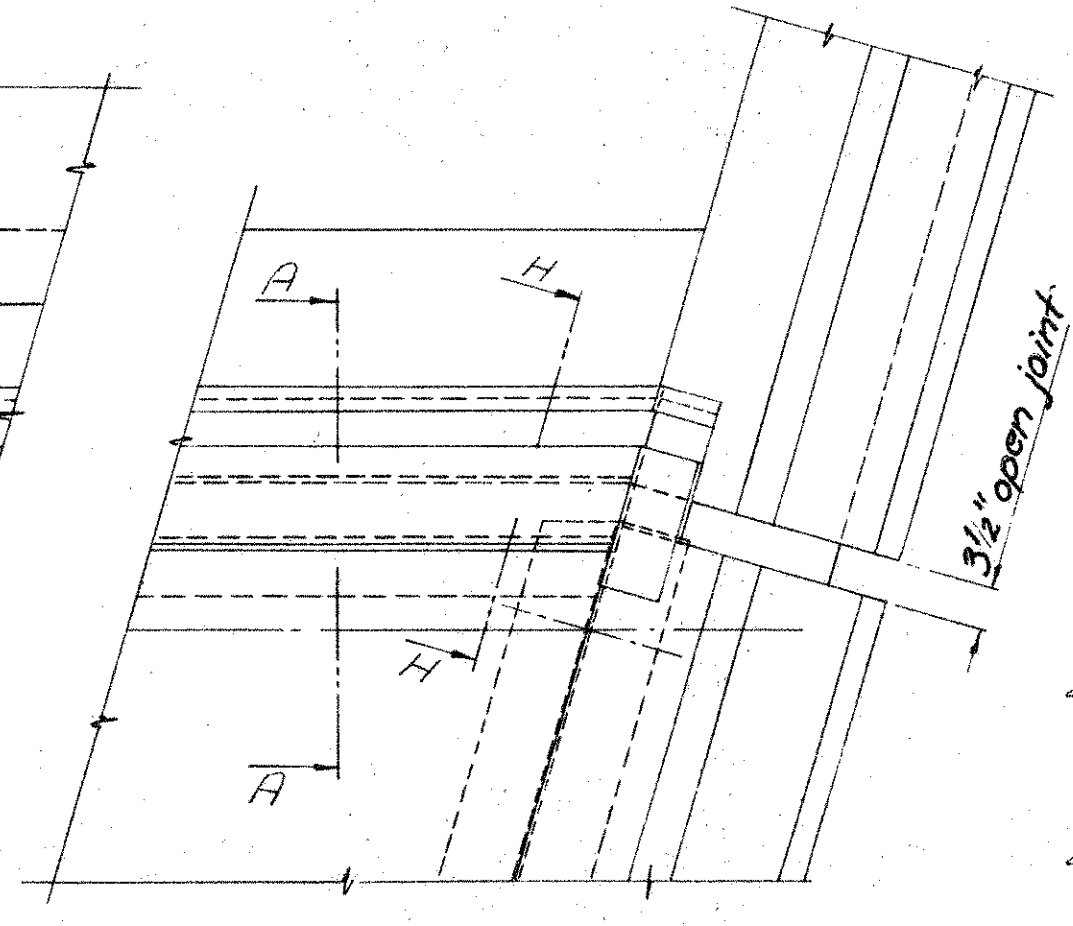
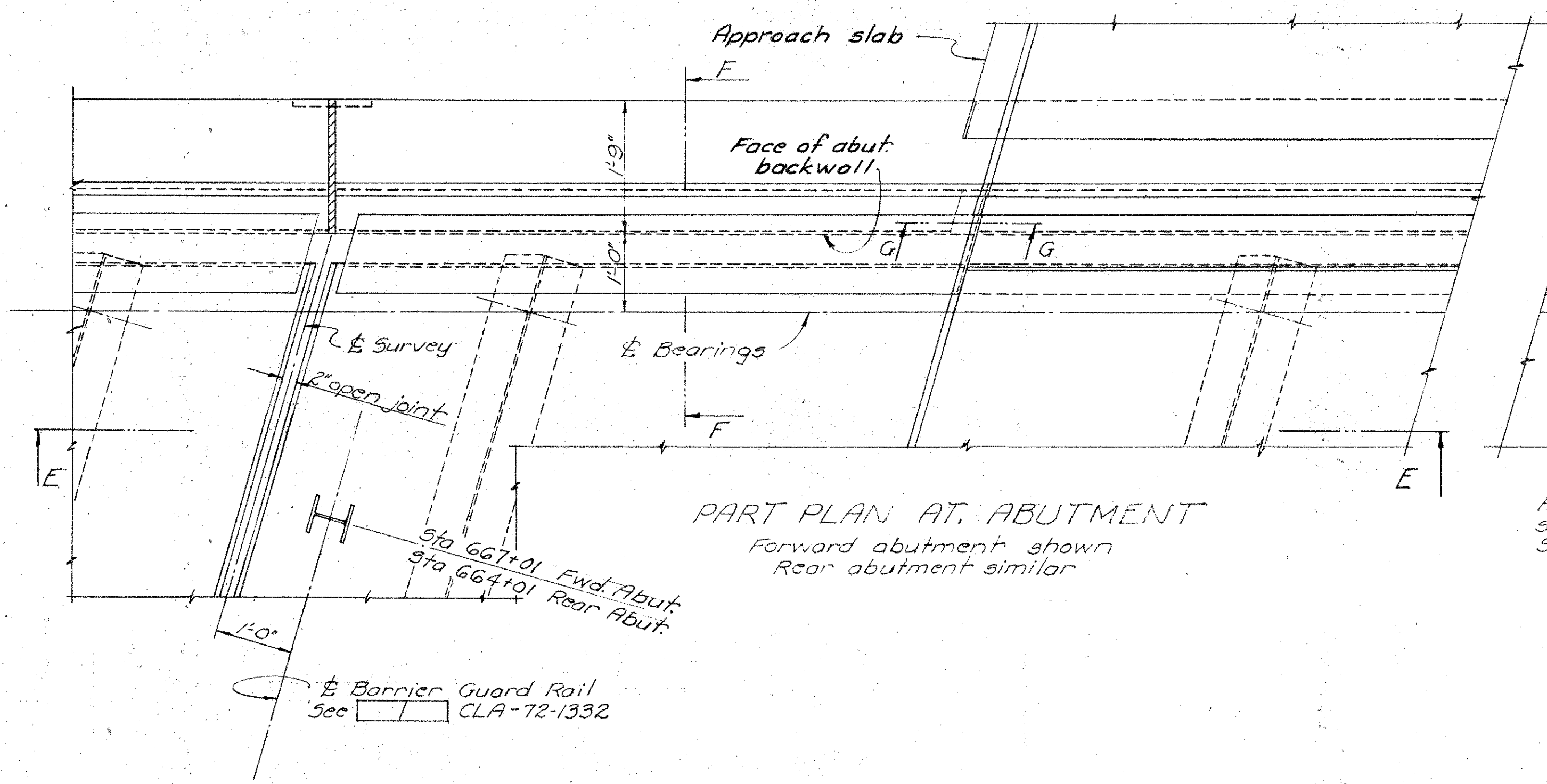
SUPERSTRUCTURE DETAILS
 BRIDGE NO. CLA-72-13.20 over
 PENN CENTRAL and the
 ERIE-LACKAWANNA R.R.

| | | | | | | |
|----------|-------|--------|---------|----------|---------|---------|
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| MPB | MPB | | WCK | BFG | 5-27-69 | |

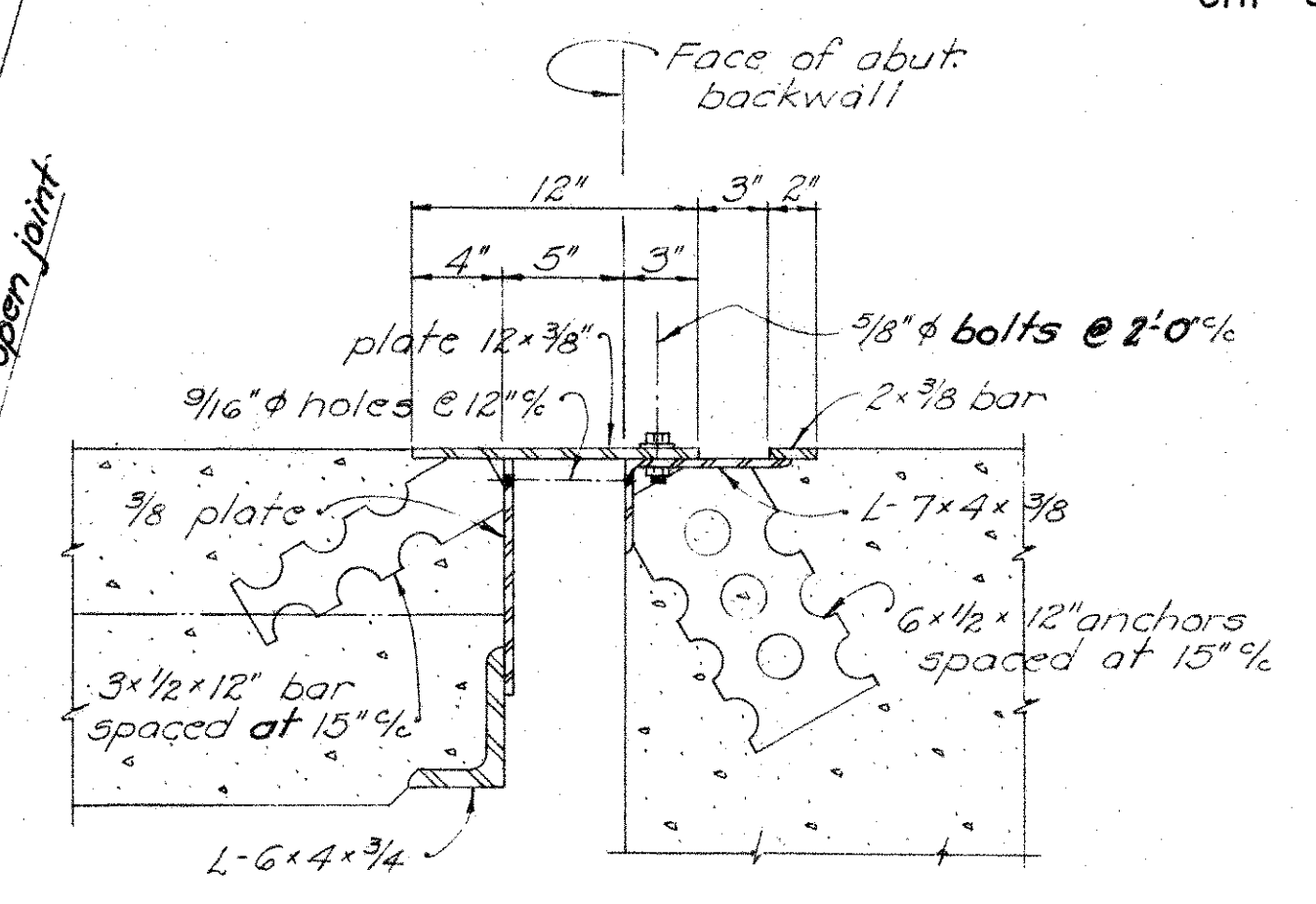
MICROFILMED
 FEB 08 1965

CLA-72-13.20
 CLA-68-12.39
 CHP-68-0.00

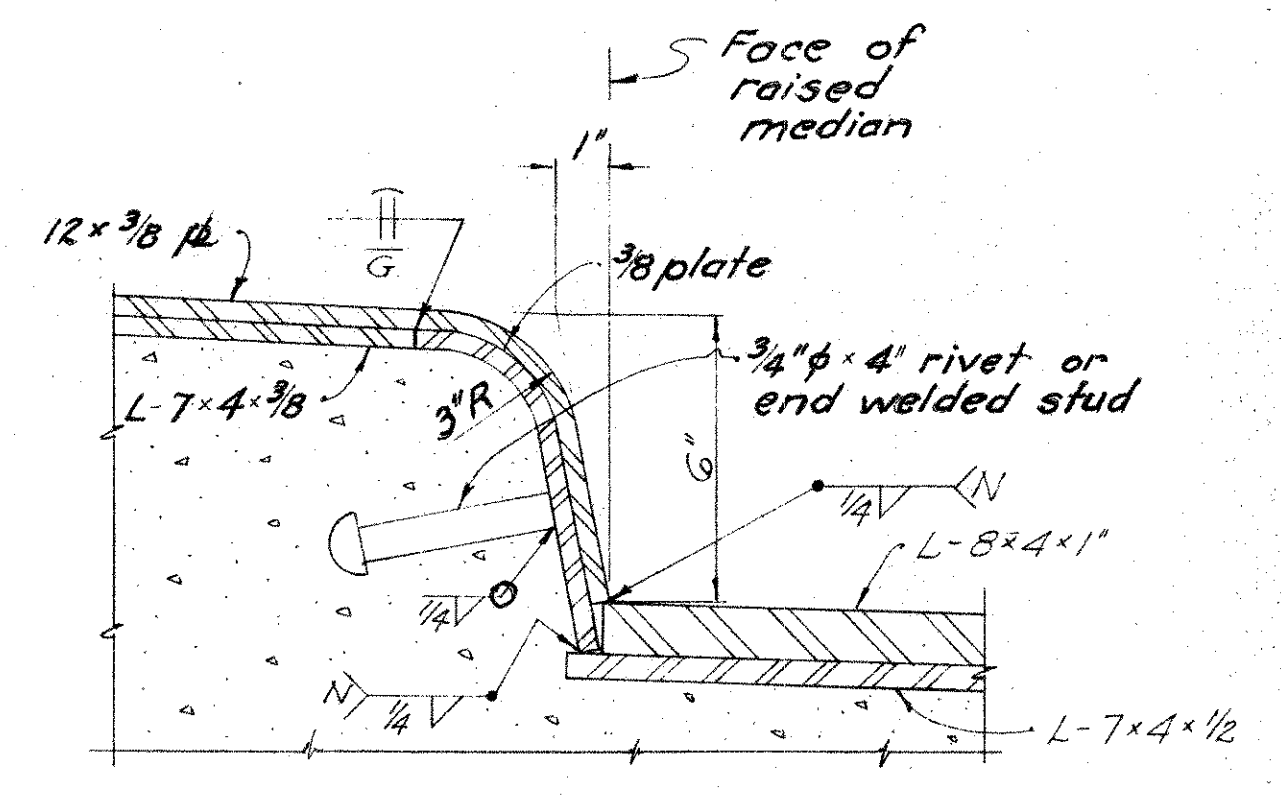
| | | | |
|-------------------|-------|---------|-----|
| FED. RD. DIVISION | STATE | PROJECT | |
| 2 | OHIO | | 371 |



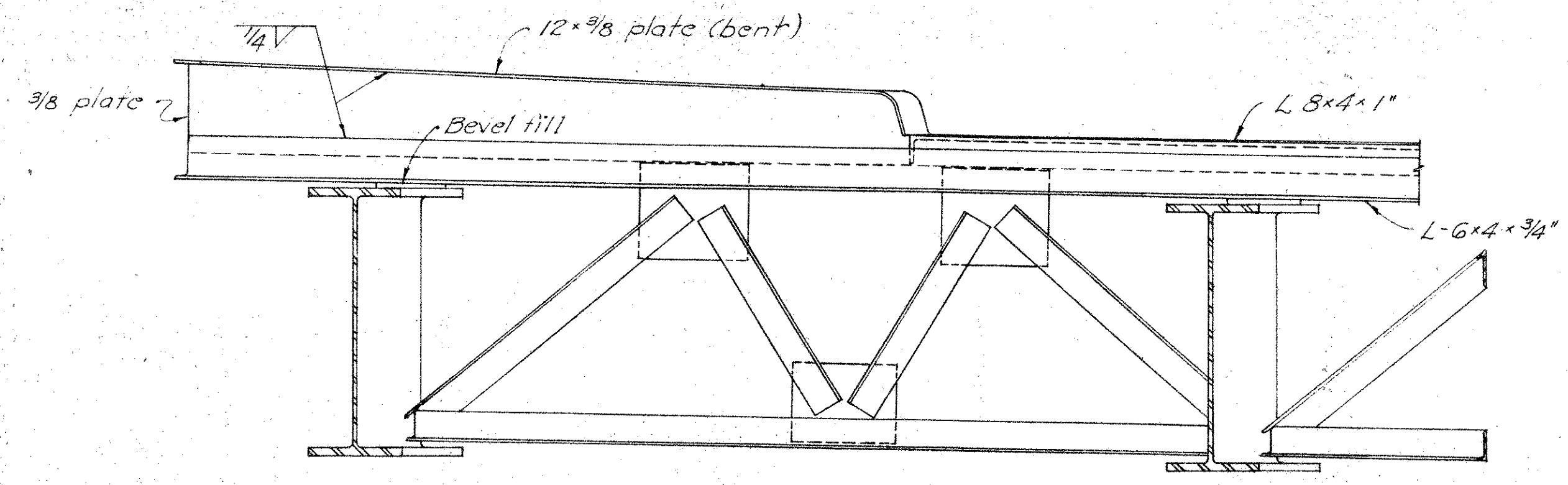
For SECTION A-A
 see Standard Dwg.
 5D-1-65, sheet 1.



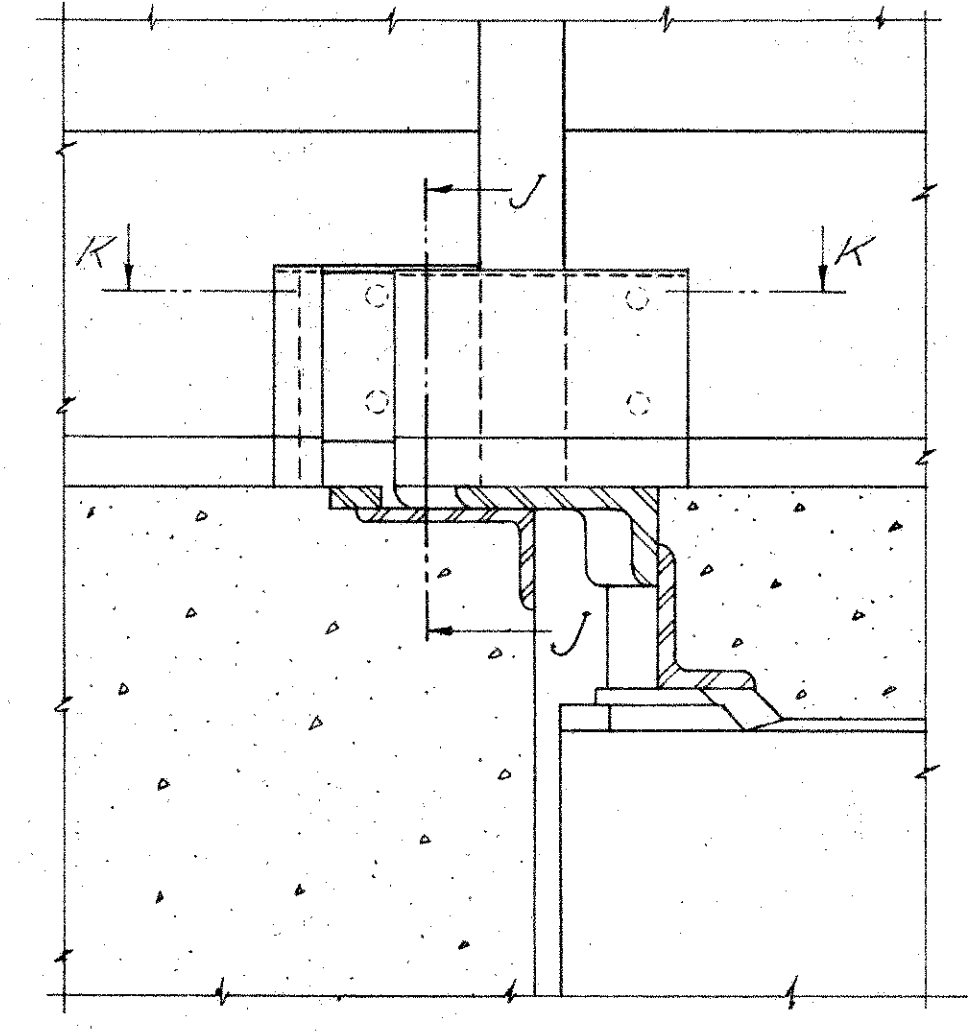
SECTION F-F



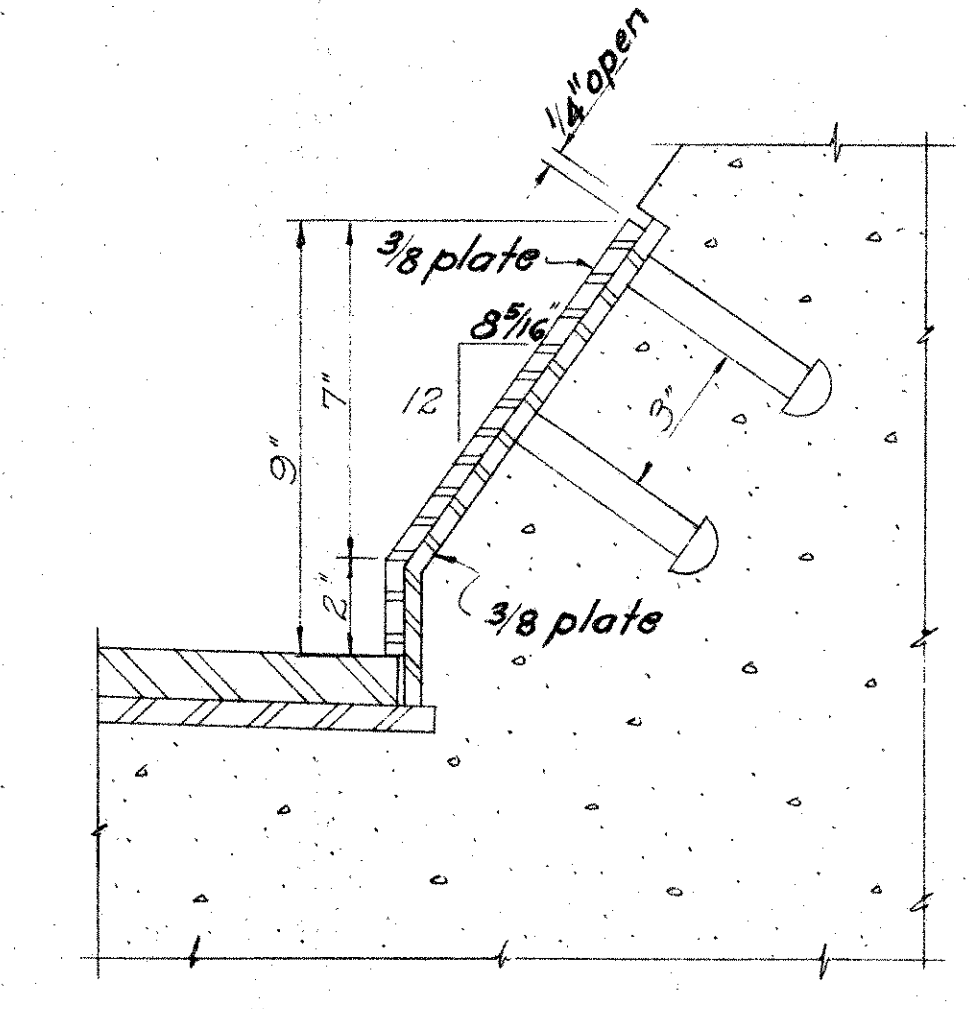
SECTION G-G



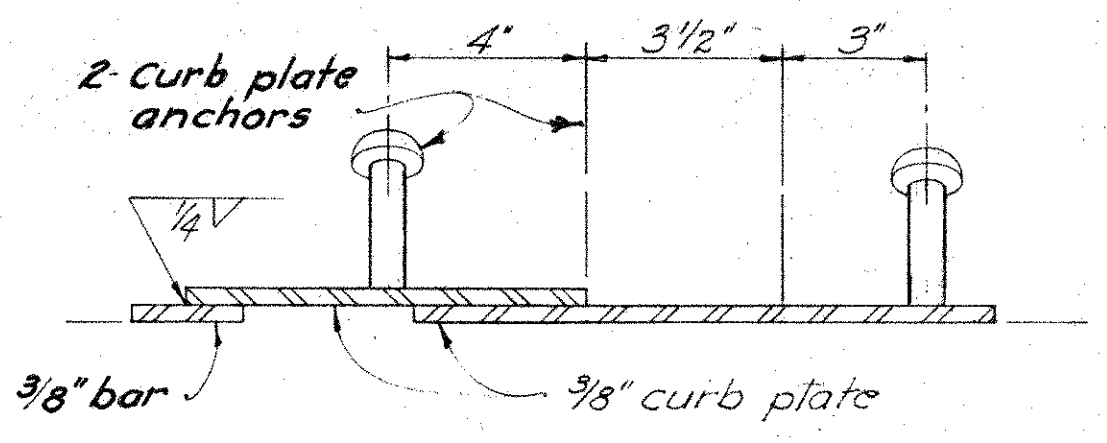
SECTION E-E



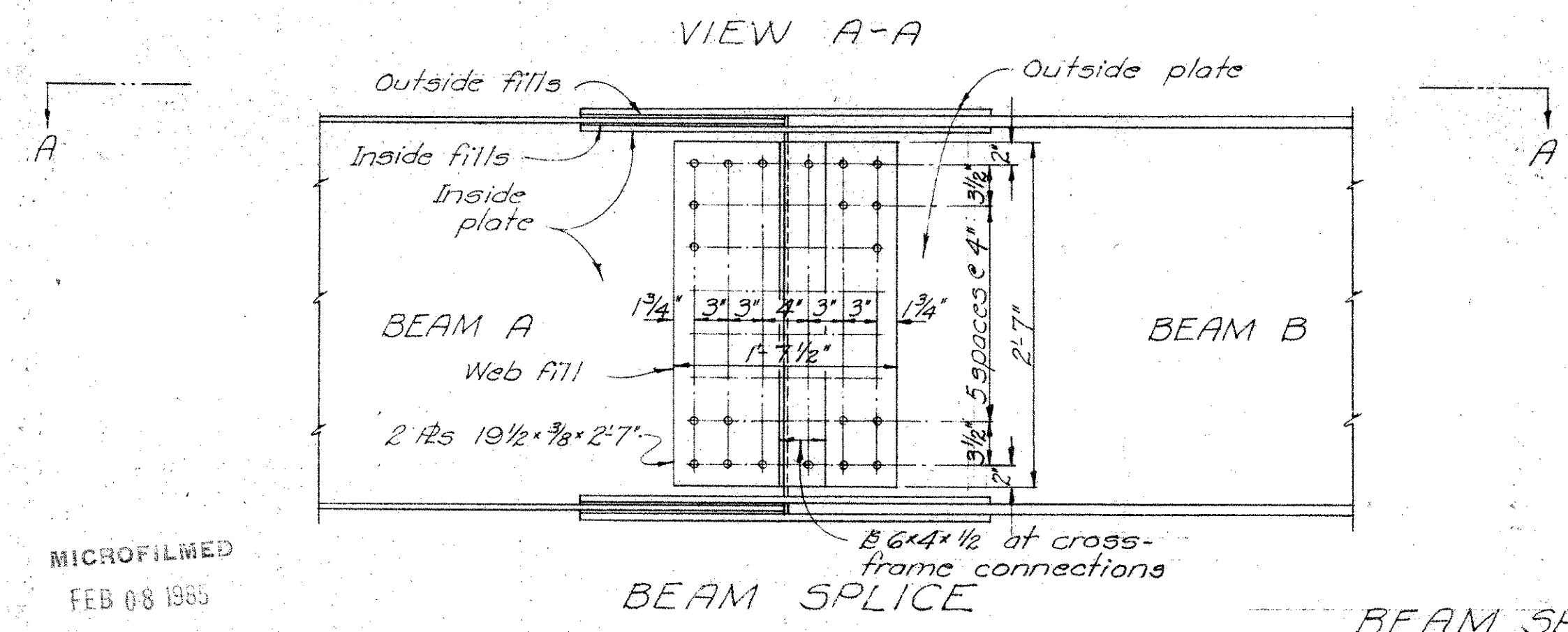
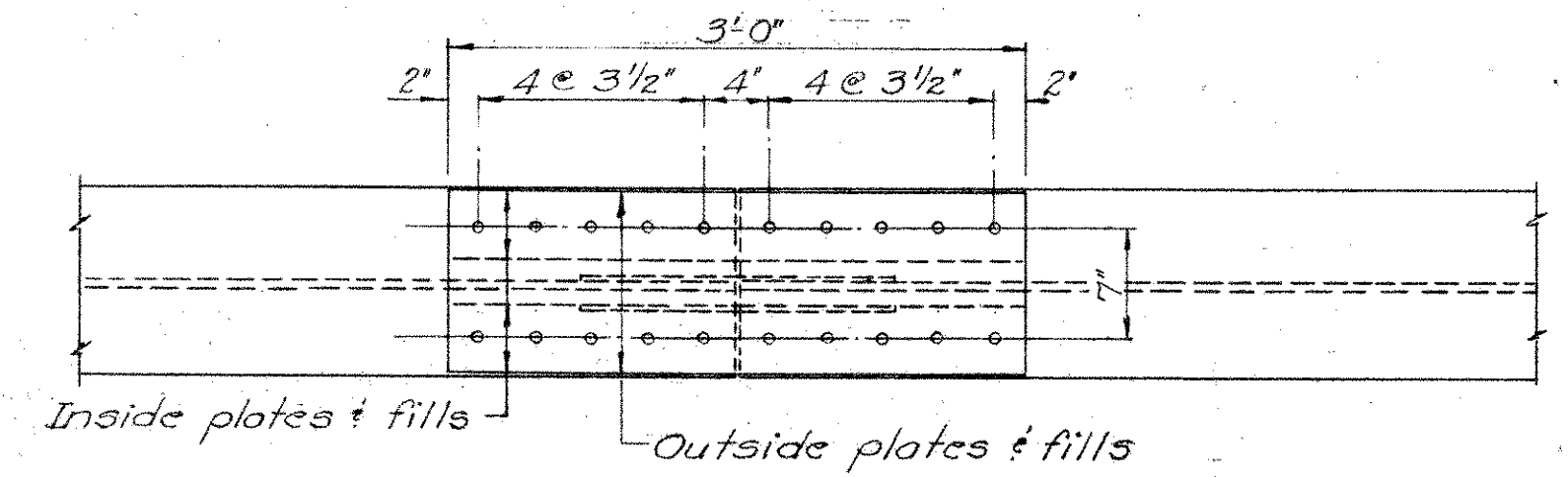
SECTION H-H



For additional details
 refer to Section G-G



SECTION K-K



BEAM SPLICE DETAILS
 See Standard Drawing
 5D-1-65 (sheet 3) for additional details

| BEAM SPLICE DATA | | | | | | |
|------------------|---------|---------------|------------------|------------------|---------------|-----|
| BEAM A | BEAM B | SPLICE PLATES | | FILLS | | |
| | | OUTSIDE | INSIDE | OUTSIDE | INSIDE | WEB |
| 36WF135 | 36WF170 | 11x3/8x3'-0" | 4 1/2x1/2x3'-0" | 11x3/16x1'-6" | - | - |
| 36WF150 | 36WF150 | - | - | - | - | - |
| 36WF150 | 36WF160 | - | - | - | - | - |
| 36WF150 | 36WF170 | 11x3/16x3'-0" | 4 1/2x3/16x3'-0" | 11x1/8x1'-6" | - | - |
| 36WF150 | 36WF230 | - | - | 4 1/2x3/16x1'-6" | 3/4x1/8x2'-7" | - |
| 36WF160 | 36WF160 | 11x1/2x3'-0" | 4 1/2x3/16x3'-0" | - | - | - |
| 36WF160 | 36WF182 | - | - | 11x3/16x1'-6" | - | - |

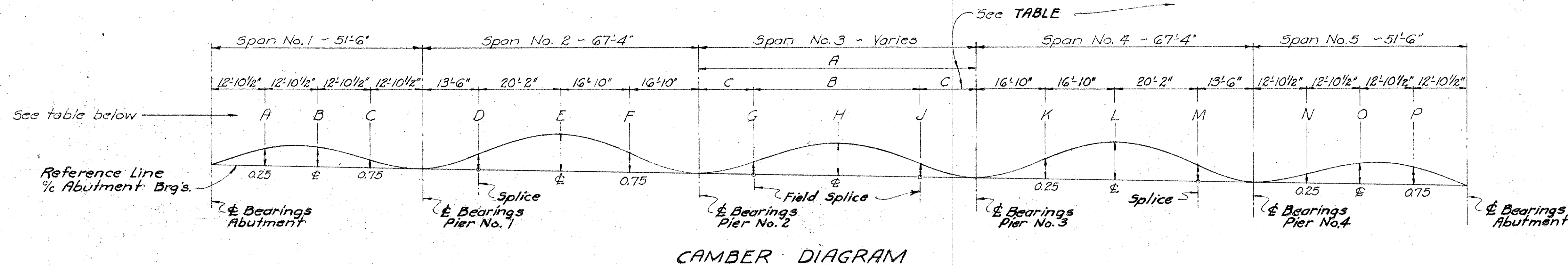
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 FEB 08 1965

STATE OF OHIO
 DEPARTMENT OF HIGHWAYS
 DIVISION OF DESIGN AND CONSTRUCTION
 BUREAU OF BRIDGES

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SUPERSTRUCTURE DETAILS
 BRIDGE NO. CLA-72-1329 over
 PENN CENTRAL and the
 ERIE-LACKAWANNA R.R.

| | | | | | | |
|----------|-------|--------|---------|----------|---------|---------|
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| MPB | MPB | | WCK | BFJ | 5-27-69 | |



CAMBER DIAGRAM

DEFLECTION AND CAMBER

| DESCRIPTION | SPAN NO. 1 | | SPAN NO. 2 | | | | SPAN NO. 3 | | | | | | | | | SPAN NO. 4 | | | SPAN NO. 5 | | | | | | | | |
|---------------------------------------|------------|-------|------------|--------|-------|--------|------------|--------|--------|----------|--------|--------|----------|--------|--------|------------|-------|--------|------------|--------|--------|--------|--------|--------|--------|--------|--------|
| | | | | | | | LINE 1-3 | | | LINE 4-6 | | | LINE 7-8 | | | LINE 9-11 | | | LINE 12-14 | | | | | | | | |
| | A | B | C | D | E | F | G | H | J | G | H | J | G | H | J | G | H | J | G | H | J | K | L | M | N | O | P |
| Deflection due to weight of steel | - | - | - | - | - | - | +1/16" | +1/16" | +1/16" | +1/16" | +1/16" | +1/16" | - | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Deflection due to remaining dead load | +3/16" | +1/4" | +1/16" | +3/16" | +3/8" | +3/16" | +1/4" | +3/16" | +1/4" | +3/16" | +1/16" | +3/16" | +3/8" | +3/16" | +1/8" | +1/4" | +1/8" | +1/16" | +1/8" | +1/16" | +3/16" | +3/8" | +3/16" | +1/16" | +1/4" | +3/16" | |
| Required Camber equals deflection sum | +3/16" | +1/4" | +1/16" | +3/16" | +3/8" | +3/16" | +3/16" | +3/8" | +3/16" | +1/4" | +1/2" | +1/4" | +3/16" | +3/8" | +3/16" | +1/8" | +1/4" | +1/8" | +1/16" | +1/8" | +1/16" | +3/16" | +3/8" | +3/16" | +1/16" | +1/4" | +3/16" |

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BEAM DIMENSIONS
 See sheet 10/13

| BEAM | A | B | C |
|------|--------------|-------------|-------------|
| 1 | 77'-7 3/8" | 46'-7 3/8" | 15'-6" |
| 2 | 75'-11 1/2" | 45'-7 1/16" | 15'-2 3/16" |
| 3 | 74'-4 7/16" | 44'-7 1/16" | 14'-10 7/8" |
| 4 | 72'-8 3/8" | 43'-7 1/2" | 14'-6 7/16" |
| 5 | 71'-1 1/16" | 42'-7 3/16" | 14'-2 3/4" |
| 6 | 69'-5 1/2" | 41'-7 1/2" | 13'-11" |
| 7 | 67'-9 1/2" | 40'-7 1/16" | 13'-7 1/8" |
| 8 | 66'-10 1/16" | 40'-0 7/16" | 13'-4 7/8" |
| 9 | 65'-2 1/2" | 39'-0 7/8" | 13'-1 1/16" |
| 10 | 63'-6 1/16" | 38'-0 7/16" | 12'-9 1/4" |
| 11 | 61'-11 3/8" | 37'-0 1/2" | 12'-5 7/16" |
| 12 | 60'-3 1/16" | 36'-0 7/16" | 12'-1 5/8" |
| 13 | 58'-8 1/4" | 35'-0 7/8" | 11'-9 1/16" |
| 14 | 57'-0 7/8" | 34'-0 7/8" | 11'-6" |

REINFORCING STEEL LIST

| Mark | No. | Length | Weight | Sp | Abut. | Mark | No. | Length | Weight | Sp | Abut. | Mark | No. | Length | Weight | Sp | Abut. | Mark | No. | Length | Weight | Sp | Abut. | | |
|------------------|-----|--------|--------|----|-------|------|-----|--------|--------|----|-------|------|-----|--------|--------|----|-------|------|-----|--------|--------|----|-------|--|--|
| Bending Diagrams | | | | | | | | | | | | | | | | | | | | | | | | | |
| Piers (Cont) | | | | | | | | | | | | | | | | | | | | | | | | | |
| Replacement Bars | | | | | | | | | | | | | | | | | | | | | | | | | |

* Bend in field to fit. Cost of field bending included with 509 for payment.

BAR SIZE is indicated in the bar mark. The first digit where three digits are used, and the first two digits where four are used, indicate the bar size number. For example, A700 is a No. 7 size bar and A1014 is a No. 10 size.