

**ITEM 614, MAINTAINING TRAFFIC:**

CLA-40-1975R: MAINTAIN ONE THRU LANE AND THE SLIP RAMP TRAFFIC AT ALL TIMES. OUTSIDE AND INSIDE LANE CLOSURE ARE NOT PERMITTED AT THE SAME TIME.

WHEN STAGING ON THE OUTSIDE DECK EDGE, MAINTAIN TRAFFIC USING STD. DWG. MT-95.30 AND MT-98.10. CLOSE THE OUTSIDE LANE OF US-40EB WEST OF THE INTERSECTION. ONLY THE ACCELERATION LANE IS TO BE CLOSED ON THE BRIDGE FOR WORK AREA. THE OUTSIDE LANE SHALL BE USED AS AN ADD LANE FOR THE SLIP RAMP. WHEN STAGING ON THE INSIDE DECK EDGE, MAINTAIN TRAFFIC USING STD. DWG. MT-95.30. PAINTING EQUIPMENT SHALL BE PROTECTED BY CLOSING THE INSIDE SHOULDER USING STD. DWG. MT-95.45.

CLA-70-1873: MAINTAIN TRAFFIC AS SHOWN ON SHEETS 3-15.

MOT-675-0055L: MAINTAIN TRAFFIC USING STD. DWG. MT-95.30

**ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)**

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS      FOURTH OF JULY  
NEW YEARS      LABOR DAY  
MEMORIAL DAY    THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY    TIME ALL LANES  
OR EVENT            MUST BE OPEN TO TRAFFIC

SUNDAY    12:00N FRIDAY THROUGH 6:00AM MONDAY  
MONDAY    12:00N FRIDAY THROUGH 6:00AM TUESDAY  
TUESDAY    12:00N MONDAY THROUGH 6:00AM WEDNESDAY  
WEDNESDAY 12:00N TUESDAY THROUGH 6:00AM THURSDAY  
THURSDAY    12:00N WEDNESDAY THROUGH 6:00AM FRIDAY  
THURSDAY    (THANKSGIVING ONLY)  
                  6:00AM WEDNESDAY THROUGH 6:00AM MONDAY  
FRIDAY      12:00N THURSDAY THROUGH 6:00AM MONDAY  
SATURDAY    12:00N FRIDAY THROUGH 6:00AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT DESCRIBED IN CMS 108.07 FOR EACH DAY THE LANE RESTRICTION ARE VIOLATED.

**PERMITTED LANE CLOSURE**

LANE CLOSURES SHALL ONLY BE IMPLEMENTED AT THE TIMES LISTED ON THE OHIO DEPARTMENT OF TRANSPORTATION'S PERMITTED LANE CLOSURE WEB SITE WHICH IS LOCATED AT:

HTTP://PLCM.DOT.STATE.OH.US

EXCEPTION TO THE WEBSITE: SINGLE LANE CLOSURES FOR THE CLA-70-1873 WORK CAN ONLY TAKE PLACE 9:00PM TO 6:30AM M-F AND 9:00PM TO 8:00AM S-S. NO TWO LANE CLOSURES ARE PERMITTED.

THE PERMITTED CLOSURE TIMES LISTED ON THE WEBSITE, FOURTEEN (14) CALENDAR DAYS PRIOR TO THE BID LETTING DATE, SHALL BE IN EFFECT FOR THIS PROJECT.

NO WORK WITHIN ACTIVE TRAVEL LANES OR WHICH WILL SLOW TRAFFIC IS PERMITTED AT ANY OTHER TIMES.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED PER THE LANE VALUE CONTRACT TABLE BELOW AND PN 127.

LANE VALUE CONTRACT TABLE	
DESCRIPTION OF ROUTE	DISINCENTIVE
I.R. 70 (CLA-70-1873)	\$100/MIN/LANE

**NOTIFICATION OF TRAFFIC RESTRICTIONS**

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO D7 PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HRS & < 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HRS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES AND RESTRICTIONS	>= 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS AND SIGN SUPPORTS, AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES OF THE TYPE AND LOCATION AS SHOWN IN THE STD. DWGS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

**FLOODLIGHTING**

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

**TRENCH FOR WIDENING**

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

**DUST CONTROL**

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616, WATER 1 M. GAL.

**ITEM 614, WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS (UNIDIRECTIONAL)**

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NONGATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

**ITEM 611 - CATCH BASIN, MISC.: TEMPORARY BYPASS**

THE CONTRACTOR SHALL TEMPORARY BYPASS THE ODOT 2-3 CATCH BASIN LOCATED AT STA. 992+65; 19' LT. AS DETAILED ON SHEET [12/30]. PRIOR TO BYPASSING THE CATCH BASIN THE CONTRACTOR SHALL SUBMIT A BYPASS PLAN FOR ACCEPTANCE. UPON COMPLETION OF THE PROJECT, THE CATCH BASIN SHALL BE RESTORED TO PRE CONSTRUCTION CONDITIONS TO THE APPROVAL OF THE ENGINEER. ALL LABOR, MATERIALS AND EQUIPMENT NEEDED TO PERFORM THIS ITEM OF WORK SHALL BE INCLUDED IN ITEM 611 - CATCH BASIN, MISC.: TEMPORARY BYPASS ..... 1 EACH

**DELINEATION OF TEMPORARY AND PERMANENT GUARDRAIL**

BARRIER REFLECTORS SHALL BE INSTALLED ON ALL TEMPORARY GUARDRAIL USED FOR TRAFFIC CONTROL; AND, ON ALL PERMANENT GUARDRAIL LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE. BARRIER REFLECTORS SHALL CONFORM TO C&MS 626 AND THE SPACING SHALL BE APPROXIMATELY 50 FEET.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

ITEM 614, BARRIER REFLECTOR, TYPE 2, ONE-WAY... 20 EACH

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE ABOVE ITEM(S).

CLA-40-1975R ITEMIZED TEMPORARY TRAFFIC CONTROL ITEMS

THE FOLLOWING ESTIMATED QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY:

ITEM 614 WORK ZONE IMPACT ATTENUATOR (UNIDIRECTIONAL).....1 EACH  
ITEM 614 BARRIER REFLECTOR, TYPE 1, ONE WAY..... 20 EACH  
ITEM 614 OBJECT MARKER, ONE WAY..... 20 EACH  
ITEM 622 PORTABLE BARRIER, UNANCHORED..... 800 FT

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MAINTENANCE OF TRAFFIC GENERAL NOTES (1 OF 2)

D07-BH-FY21

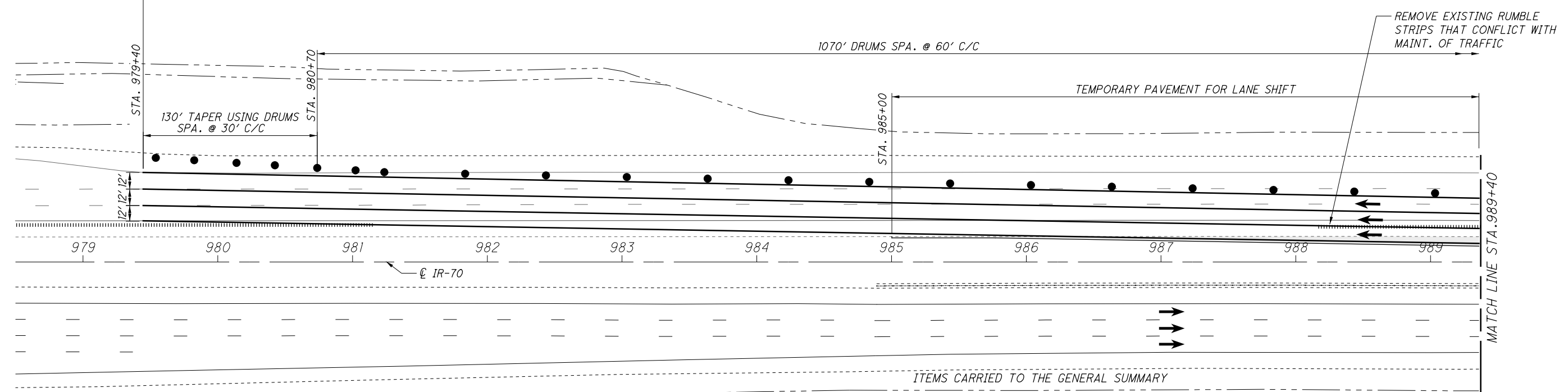


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**MAINTENANCE OF TRAFFIC (SHEET 1 OF 2)**  
**CLA-70-1873 OVER IR-70**

**D07-BH-FY21**

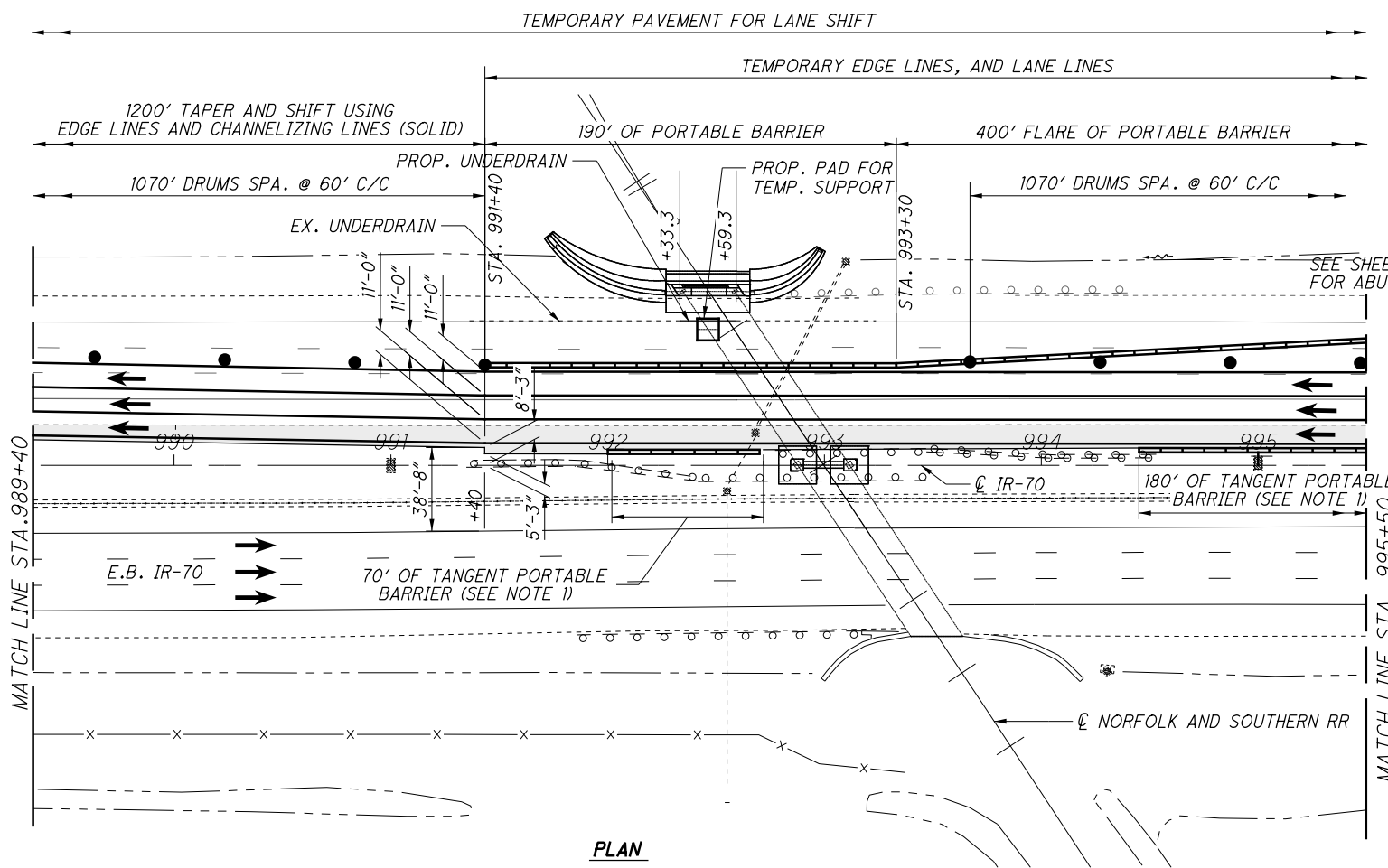
1200' TAPER AND SHIFT USING CHANNELIZING LINES AND LANE LINES (SOLID)



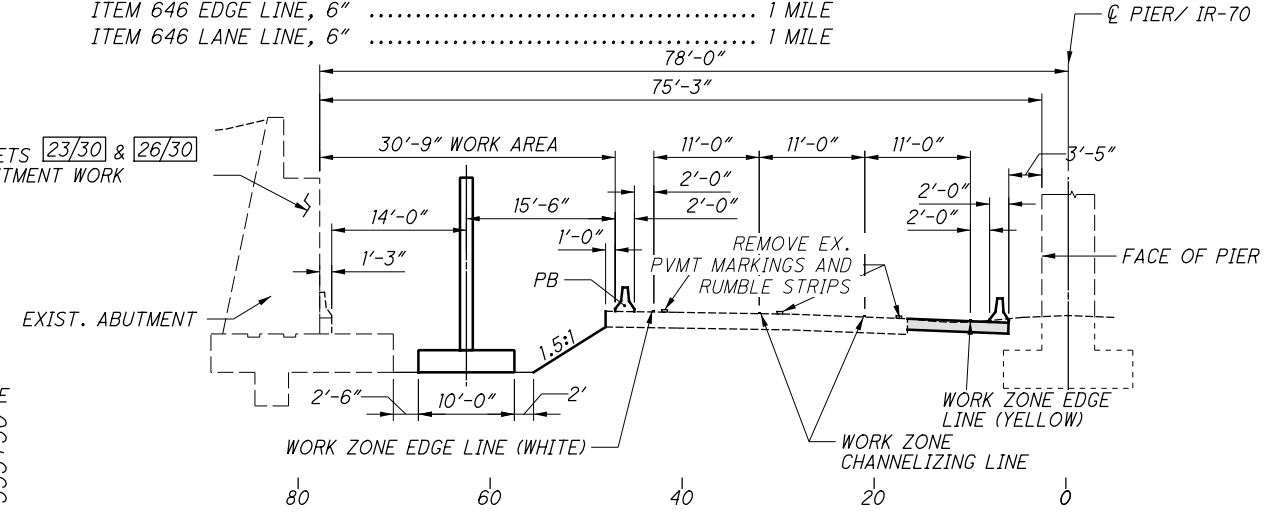
**PLAN**

ITEMS CARRIED TO THE GENERAL SUMMARY

- ITEM 605 6" SHALLOW PIPE UNDERDRAIN ..... 26 FT.
- ITEM 614 WORK ZONE IMPACT ATTENUATOR (UNIDIRECTIONAL)... 2 EACH
- ITEM 614 BARRIER REFLECTOR, TYPE 1, ONE WAY..... 22 EACH
- ITEM 614 OBJECT MARKER, ONE WAY..... 22 EACH
- ITEM 614 WORK ZONE CHANNELIZING LINE, CLASS 1, 8", 740.06, TYPE I..... 7470 FT
- ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A..... 1.44 MILE
- ITEM 618 RUMBLE STRIPS, (ASPHALT CONCRETE), AS PER PLAN ..... 3500 FT.
- ITEM 618 RUMBLE STRIPS, (ASPHALT CONCRETE) ..... 3500 FT.
- ITEM 622 PORTABLE BARRIER, UNANCHORED..... 840 FT.
- ITEM 646 EDGE LINE, 6" ..... 1 MILE
- ITEM 646 LANE LINE, 6" ..... 1 MILE



**PLAN**



**SECTION A-A**

NOTES AND LEGEND

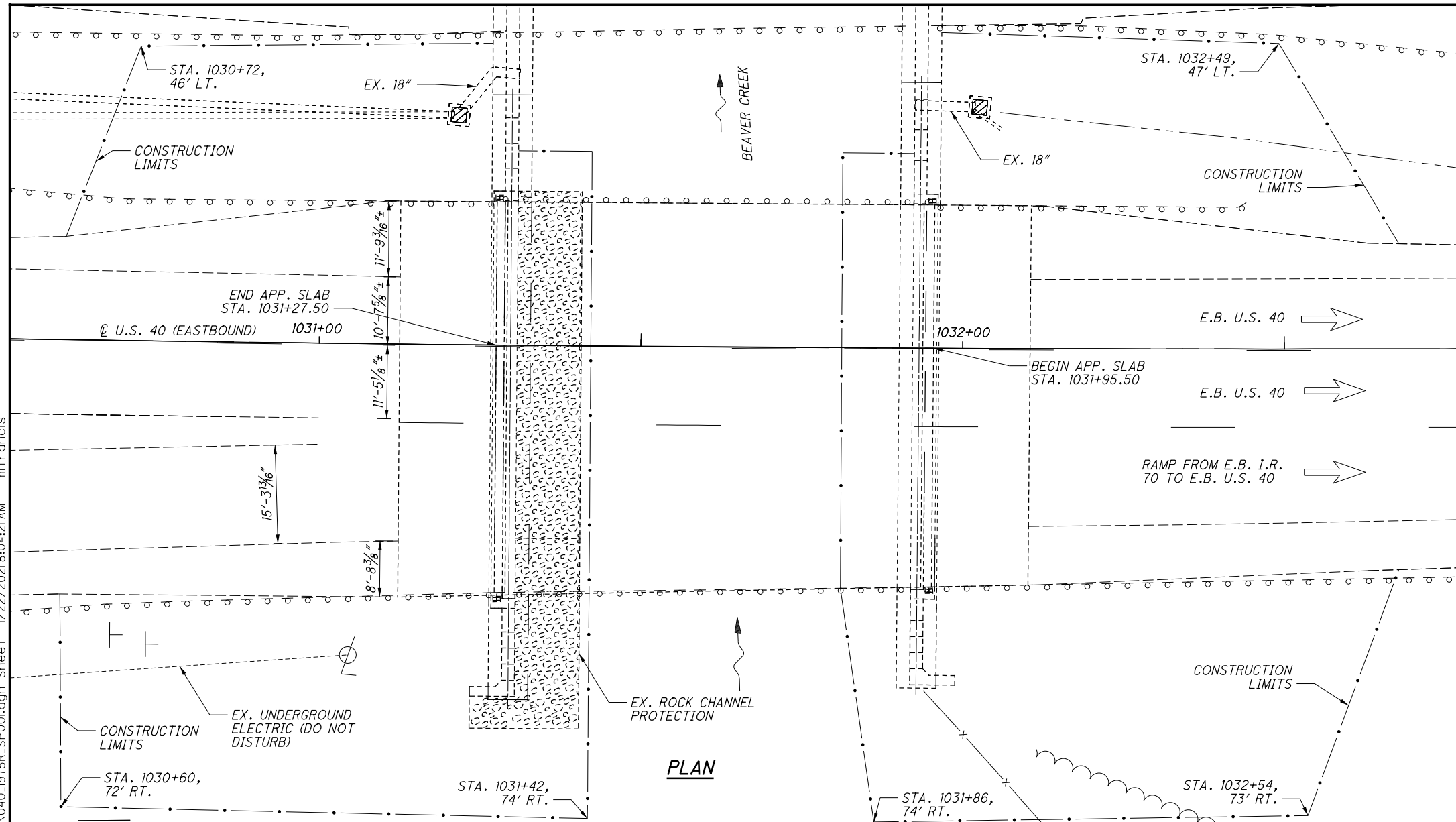
1. WHEN PLACING PORTABLE BARRIER WITH EXISTING GUARDRAIL, OVERLAP TRAILING END SO NO BLUNT END IS FACING TRAFFIC AS SHOWN IN THE PLAN VIEW
2. FOR ADDITIONAL MAINTENANCE OF TRAFFIC NOT SHOWN SEE SDT. DWG. MT-102.10
3. EXISTING NO. 2-3 CATCH BASIN, RECONSTRUCT AND ADJUST TO GRADE
4. FOR FULL DEPTH PAVEMENT REPLACEMENT AREAS SEE SHEET 12/30

- = EXISTING RUMBLE STRIPES IN CONFLICT TO BE REMOVED
- = TEMPORARY PAVEMENT FOR MAINTAINING TRAFFIC

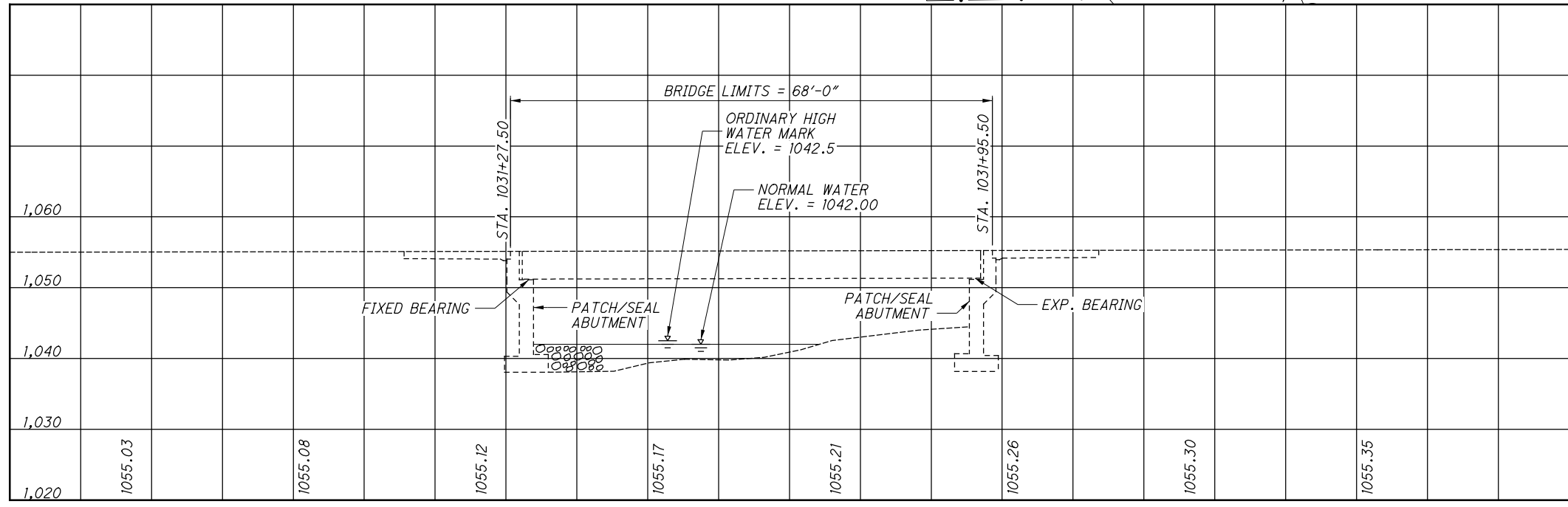
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**PLAN**



**PROFILE ALONG C U.S. 40**



**NOTES**

DESIGN TRAFFIC E.B. U.S. 40:  
 2021 ADT = 3946      2021 ADTT = 119  
 DESIGN TRAFFIC DATA TAKEN FROM TRANSPORTATION DATA MANAGEMENT SYSTEM  
 DIMENSIONS AND DATA ARE TAKEN FROM EXISTING PLANS AND FIELD MEASUREMENT. ALL DIMENSIONS AND ELEVATIONS ARE TO BE CONSIDERED +/- AND FOR REFERENCE ONLY.

**EXISTING R/W LOCATION**

THE EXISTING R/W IS BEYOND THE SHOWN LIMITS OF THE PLAN VIEW.

**HYDRAULIC DATA**

DRAINAGE AREA = 19.9 SQ. MILES  
 Q (50) = 2790 CFS  
 Q (100) = 3190 CFS  
 HYDRAULIC DATA TAKEN FROM STREAMSTATS

**EXISTING STRUCTURE**

TYPE: STEEL BEAM WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE  
 SPANS: 63'-6" C/C BRGS.  
 ROADWAY: VARIES 61.09' TO 59.39' F/F GUARDRAIL  
 LOADING: HS 20-44 CASE 1 AND ALTERNATE MILITARY  
 SKEW: NONE  
 APPROACH SLABS: AS-1-81 (15' LONG)  
 ALIGNMENT: 0° 28' CURVE LEFT (EDGE ON 40:1 TAPER)  
 CROWN: 3/16"/FT  
 STRUCTURAL FILE NUMBER: 1201298  
 DATE BUILT: 1949 REHABILITATED: 1965, 1992, 2010  
 WEARING SURFACE: MONOLITHIC CONCRETE  
 COORDINATES: LATITUDE N 39° 55' 27"  
 LONGITUDE W 83° 41' 05"  
 DISPOSITION: TO BE REHABILITATED

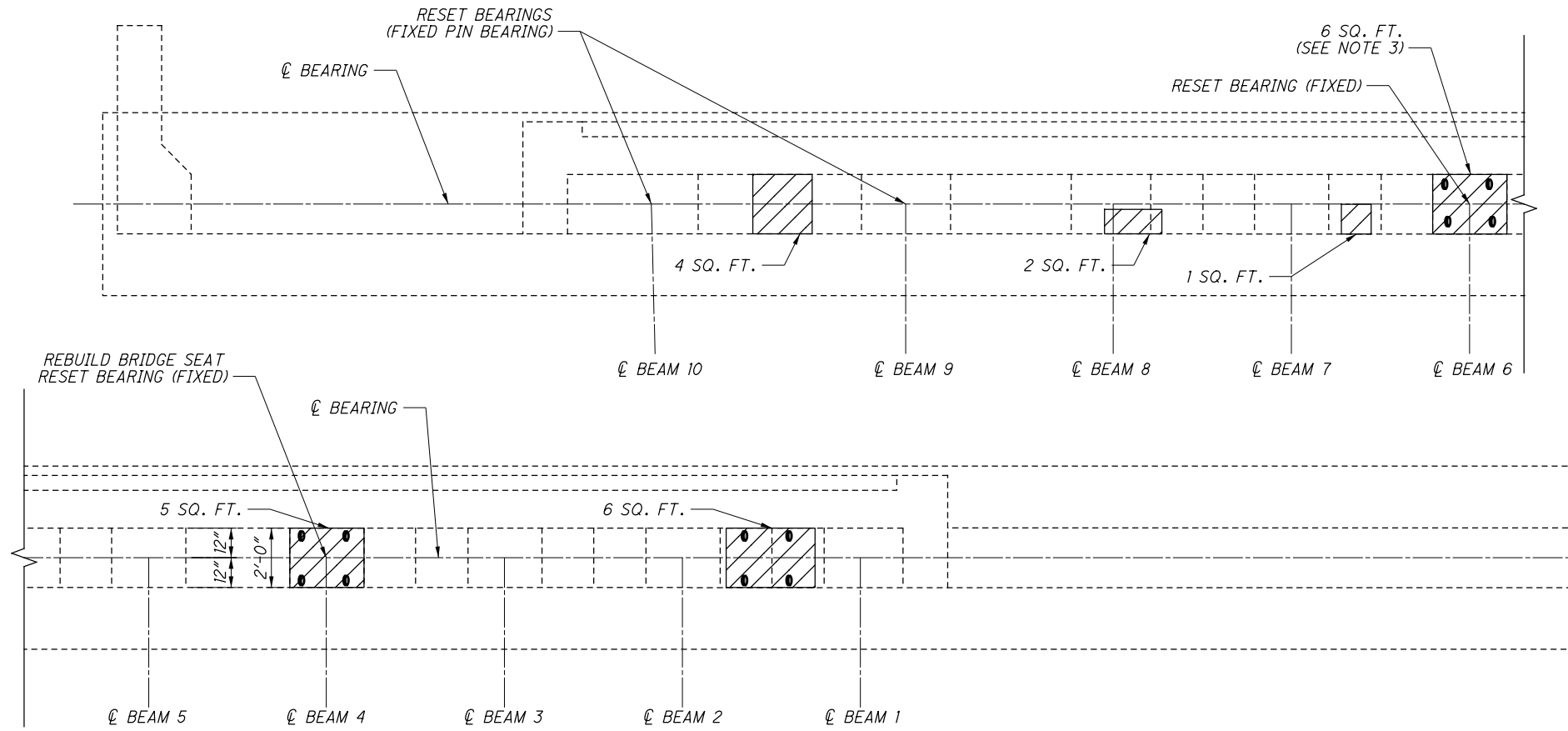
**PROPOSED WORK**

1. PATCH ABUTMENTS AND WINGWALLS USING ANODES.
2. REWELD ENDFRAMES AND BEARING PLATE.
3. PAINT END 5' OF BEAMS, INCLUDING END FRAMES AND BEARINGS.
4. RESET BEAM 4, 6, 9, AND 10 REAR BEARINGS.

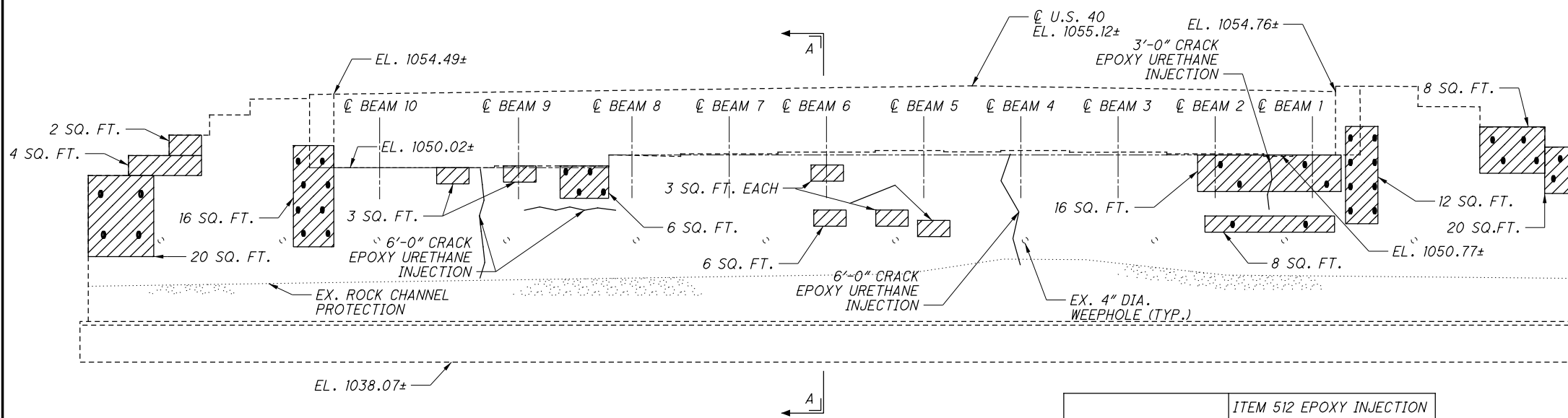
WORK LISTED IS NOT INCLUSIVE. CONTRACTOR WILL SEQUENCE WORK AS NEEDED.

DESIGN AGENCY ODOT DISTRICT 7 ENGINEERING	DATE 10/1/20	REVIEWED MRB	STRUCTURE FILE NUMBER 1201298	DRAWN MEF	REVIS XXX	DESIGNED MEF	CHECKED DHG	CLARK COUNTY STA. 1031+27.50 STA. 1031+95.50	SITE PLAN BRIDGE NO.: CLA-040-1975R E.B. U.S. 40 OVER BEAVER CREEK	D07-BH-FY21 PID No. 110154	1 / 4
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**REAR ABUTMENT PLAN**

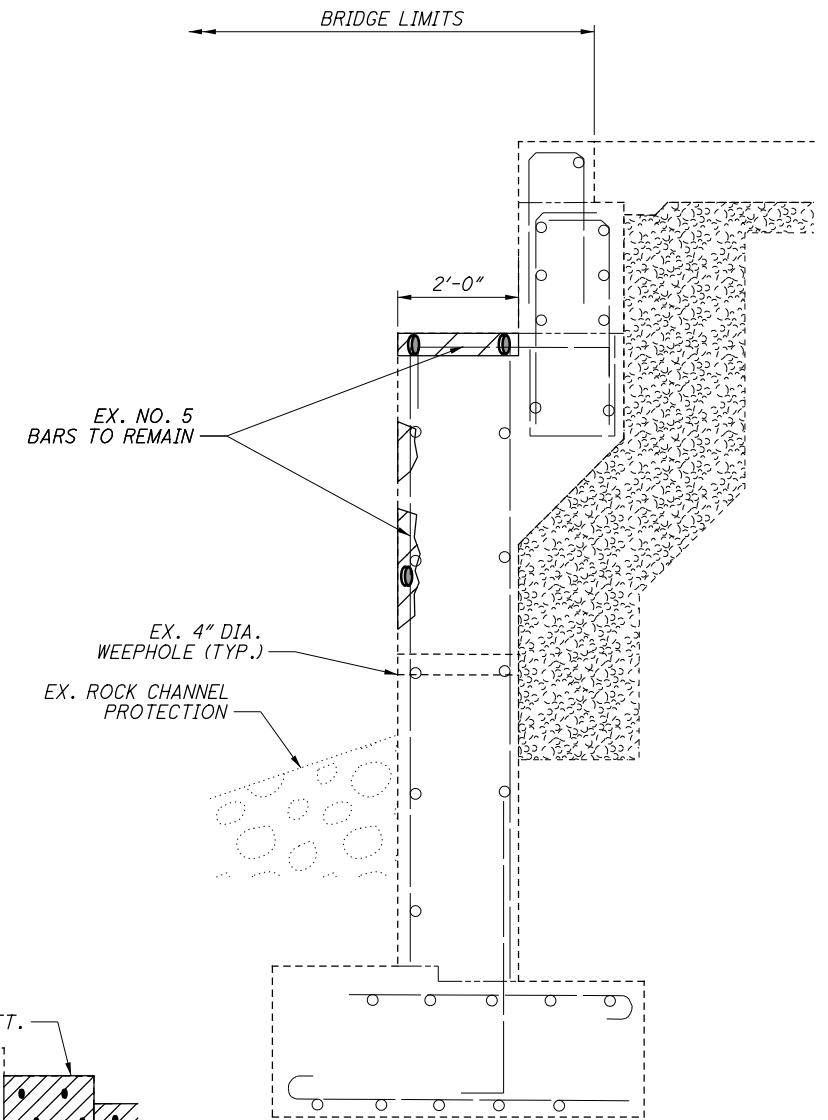


**REAR ABUTMENT ELEVATION**

	ITEM 512 EPOXY INJECTION
REAR ABUTMENT	18 FT.
TOTAL =	18 FT.

	ITEM 844 PATCHING
REAR ABUTMENT	157 SQ. FT.
TOTAL =	157 SQ. FT.

	# OF ANODES	
REAR ABUTMENT	28	
TOTAL =	28	FOR REFERENCE ONLY



**SECTION A-A**

**NOTES & LEGEND**

1. SEAL CONCRETE SURFACES WITH EPOXY URETHANE 6" BEYOND REPAIR LIMITS.
2. CONFIRM THICKNESS OF BEARING PRIOR TO PLACING CONCRETE UNDER BEAMS.
3. ANODES TO BE PLACED IN PATCHES GREATER THAN OR EQUAL TO 5 SQ. FT. PER SS844
4. SEE SHEET 4/4 FOR BEARING DETAILS

 ITEM 844 - CONCRETE PATCHING WITH GALVANIC ANODE PROTECTION, AS PER PLAN

