

SCHEMATIC PLAN
MIA-75

DESIGN AGENCY	
DISTRICT 7 ENGINEERING	
DESIGNER	
REB	
REVIEWER	
BJA 08-22-24	
PROJECT ID	
120846	
SUBSET	TOTAL
1	3
SHEET	TOTAL
P.05	20

GENERAL REQUIREMENTS

IT IS THE INTENTION TO PERFORM THE REQUIRED WORK WITHIN THESE PLANS WITH THE LEAST INCONVENIENCE TO AND THE MAXIMUM SAFETY OF THE CONTRACTOR AND THE TRAVELING PUBLIC. THE REQUIREMENTS FOR MAINTAINING TRAFFIC AS SPECIFIED IN THE "OHIO MANUAL OF UNIFORMED TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (CURRENT EDITION, LATEST REVISION), PERTINENT PROVISIONS OF THE "OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS" (INCLUDING SUPPLEMENTAL SPECIFICATIONS), AND APPLICABLE STANDARD CONSTRUCTION DRAWINGS SHALL APPLY TO THIS PROJECT IN ADDITION TO THE FOLLOWING NOTES AND DETAILS. ANY VARIANCES FROM THE INTENT OF THE MAINTENANCE OF TRAFFIC PLANS SHALL BE APPROVED IN ADVANCE IN WRITING BY THE ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO D7 PERMITS AND PIO
RAMP & ROAD CLOSURES	>= 2 WKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HRS & < 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HRS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES AND RESTRICTIONS	>= 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WKS	2 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ITEM 614. MAINTAINING TRAFFIC:

THE FOLLOWING WORK SHALL BE PERFORMED:

IN CLARK COUNTY ALONG I.R. 70, REMOVE AND REPLACE MEDIAN WALL INLET SECTIONS AND VARIOUS MEDIAN WALL SECTIONS AS MARKED IN THESE PLANS.

IN CLARK COUNTY ALONG I.R. 70 PATCH AND REPAIR THE EXISTING MEDIAN WALL AT VARIOUS LOCATIONS FROM S.L.M. 0.00 TO S.L.M. 6.27 AS MARKED IN THESE PLANS.

IN MIAMI COUNTY ALONG I.R. 75, REMOVE AND REPLACE MEDIAN WALL INLET SECTIONS AND VARIOUS MEDIAN WALL SECTIONS AS MARKED IN THESE PLANS.

IN MIAMI COUNTY ALONG I.R. 75 PATCH AND REPAIR THE EXISTING MEDIAN WALL AT VARIOUS LOCATIONS FROM S.L.M. 0.00 TO S.L.M. 13.21 AS MARKED IN THESE PLANS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING SAFE AND EFFECTIVE TRAFFIC CONTROL 24 HOURS A DAY FOR THE DURATION OF THIS PROJECT/CONTRACT. THIS INCLUDES PROVIDING, PLACING, MAINTAINING, AND SUBSEQUENTLY REMOVING ALL NECESSARY TRAFFIC CONTROL MEASURES FOR ALL PROPOSED CONSTRUCTION OPERATIONS.

ALL RAMPS SHALL REMAIN OPEN DURING THE PHASE TO REPLACE OR PATCH MEDIAN WALLS.

THE CONTRACTOR SHALL CONTACT THE ODOT DISTRICT 7 ROADWAY SERVICE OFFICE (937) 497-6891 A MINIMUM OF 14 CALENDAR DAYS PRIOR TO INTENDED LANE RESTRICTIONS TO ALLOW SUFFICIENT TIME FOR THE NECESSARY COORDINATION.

BEFORE WORK BEGINS, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER THE NAME(S) AND TELEPHONE NUMBER(S) OF PERSON(S) WHO CAN BE CONTACTED 24 HOURS A DAY BY THE OHIO DEPARTMENT OF TRANSPORTATION, THE HIGHWAY PATROL, AND ALL OTHER INTERESTED POLICE AGENCIES.

THIS PERSON(S) SHALL BE RESPONSIBLE FOR REPAIRING AND OR REPLACING ALL TRAFFIC CONTROL DEVICES NEEDED TO MAINTAIN SAFETY FOR THE DURATION OF THIS PROJECT. THIS PERSON(S) SHALL HAVE AVAILABLE ALL MATERIALS, EQUIPMENT, AND INCIDENTALS NECESSARY TO PERFORM THE REQUIRED REPAIRS WITHIN A REASONABLE PERIOD OF TIME.

THE CONTRACTOR SHALL PROVIDE, ERECT, AND MAINTAIN SIGNS (IN PROPER POSITION, CLEAN AND LEGIBLE, AND IN GOOD WORKING CONDITION) AND REMOVE ALL LIGHTS, SIGNS, CONES, DRUMS, AND ANY OTHER TRAFFIC CONTROL DEVICES NECESSARY FOR THE MAINTENANCE OF TRAFFIC ACCORDING TO THESE PLAN NOTES AND DETAILS.

THE CONTRACTOR SHALL FURNISH AND INSTALL ADVANCE WARNING "ROAD WORK AHEAD" (W20-1) SIGNS AND "END ROAD WORK" (G20-2) SIGNS ON BOTH IR70 AND IR75 AND RAMPS WITHIN PROJECT LIMITS. THE SIGNS SHALL BE A DUAL INSTALLATIONS AND THE ACTUAL LOCATION SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.

VEHICLES AND OTHER EQUIPMENT SHALL NOT BE PERMITTED TO STOP OR TO BE PARKED ALONG THE ROADWAY EXCEPT WITHIN DESIGNATED WORK AREAS AND SHALL NOT ENTER OR LEAVE WORK AREAS IN A MANNER WHICH WILL BE HAZARDOUS TO, OR INTERFERE WITH THE NORMAL FLOW OF TRAFFIC. PERSONAL VEHICLES WILL NOT BE PERMITTED TO PARK WITHIN THE RIGHT-OF-WAY EXCEPT WITHIN SPECIFIC AREAS DESIGNATED BY THE ENGINEER.

TRAFFIC SHALL BE MAINTAINED IN A UNIFORM PATTERN THROUGHOUT THE ENTIRE LENGTH OF THE PROJECT AND SHALL NOT BE SUBJECTED TO CONSTANT LANE SHIFTS. LIMITS FOR ACTIVE WORK ZONES SHALL BE ONE MILE.

THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY INTENDED CHANGES TO ANY EXISTING OR WORK ZONE TRAFFIC CONTROL DEVICES AND SHALL OBTAIN THE ENGINEERS APPROVAL PRIOR TO MAKING THE CHANGES. THE CONTRACTOR SHALL ALSO NOTIFY THE ENGINEER FORTY-EIGHT (48) HOURS IN ADVANCE OF ANY INTENDED LANE CLOSURES, OR LANE SHIFTS.

UNLESS OTHERWISE NOTED IN THESE PLANS, THE STANDARD CHANNELIZING DEVICE FOR CLOSING ANY LANE TO TRAFFIC SHALL BE PROPERLY WEIGHTED AND REFLECTORIZED PLASTIC DRUMS AND OR GRABBER CONES LOCATED AND SPACED ACCORDING WITH APPLICABLE STANDARD DRAWINGS OR PLAN NOTES AND DETAILS.

THE CONTRACTOR AND THE PROJECT ENGINEER SHALL COOPERATE TO KEEP THE DISTRICT 7 TRAFFIC ENGINEER INFORMED OF THE CURRENT STATUS OF ALL LANE CLOSURES AND/OR LANE SHIFTS IN EFFECT AT ANY GIVEN TIME DURING THE CONSTRUCTION OF THIS PROJECT.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	FOURTH OF JULY
NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING

TROY STRAWBERRY FESTIVAL - JUNE 7-8, 2025

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$150 FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA WIDE.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC, THEREFORE ALL LANES OF TRAFFIC SHALL BE OPENED BEFORE LANE CLOSURE RESTRICTIONS APPLY.

LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

THE CONTRACTOR SHALL TAKE CARE NOT TO SET MERGE TAPERS WITHIN THE SAME AREA OF ENTRANCE RAMP MERGING. THE PROPOSED MERGE TAPER SHALL BE COMPLETE PRIOR TO THE BEGINNING OF THE GORE AREA OF THE MERGING RAMP.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORMED TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE UNIT BID PRICE PER LUMP SUM FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS OTHERWISE SEPARATELY ITEMIZED IN THE PLAN. (FOR QUANTITIES SEE SHEETS 15 & 16.)

DESIGN AGENCY



DISTRICT 7 ENGINEERING

DESIGNER

REB

REVIEWER

NKH 09-09-24

PROJECT ID

120846

SUBSET TOTAL

1 4

SHEET TOTAL

P.12 20

PERMITTED LANE CLOSURE SCHEDULE (PLCS)

LANE CLOSURE(S) SHALL CONFORM TO THE PLCS. PUBLISHED PLCS INFORMATION CAN BE FOUND ON THE ODOT WEBSITE AT:

HTTPS://WWW.TRANSPORTATION.OHIO.GOV/WPS/PORAL/GOV/ODOT/WORKING/DATA-TOOLS/RESOURCES/PERMITTED-LANE-CLOSURE

THE MONTHLY PUBLISHED SCHEDULES REQUIRED TO BE USED, FOR EACH PLCS SEGMENT WITHIN THE PROJECT AREA, ARE THOSE THAT COMPRISE THE CONSECUTIVE 12-MONTH PERIOD BEGINNING 15 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE AND ENDING 4 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE. THESE SAME 12 MONTHS APPLY FOR THE LIFE OF THE PROJECT AND SHALL BE APPLIED TO EACH RESPECTIVE MONTH OF CONSTRUCTION (MONTH OF LANE CLOSURE(S) SHALL MATCH MONTH OF PLCS USED). LANE CLOSURE(S) IN PLACE FOR MULTIPLE MONTHS SHALL ALWAYS COMPLY WITH THE CURRENT RESPECTIVE MONTH.

(FOR EXAMPLE: IF THE SALE DATE FOR THE PROJECT WAS MARCH OF 2021, THE MONTHLY PUBLISHED SCHEDULES FOR EACH APPLICABLE PLCS SEGMENT WOULD BE DECEMBER 2019 TO NOVEMBER 2020. IF THIS WAS A THREE-YEAR PROJECT, YEAR THREE WOULD STILL BE USING THE DECEMBER 2019 TO NOVEMBER 2020 MONTHLY SCHEDULES. IF THE PROJECT DESIRED TO CLOSE TWO LANES IN JUNE 2021, REFERENCE WOULD BE MADE TO THE JUNE 2020 SCHEDULE(S) FOR THE RESPECTIVE PLCS SEGMENT(S). IF THE SAME TWO LANES WERE DESIRED TO BE CLOSED AGAIN IN JULY 2021, REFERENCE WOULD BE MADE TO THE JULY 2020 SCHEDULE(S) FOR THE RESPECTIVE PLCS SEGMENT(S).)

MORE RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE AT THE DISCRETION OF THE ENGINEER IN ORDER TO COMPLY WITH THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

LESS RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE SUBJECT TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)) AND SHALL NOT BE IMPLEMENTED UNTIL, AND UNLESS, APPROVED BY THE PROPER ODOT AUTHORITY.

ALLOWABLE LANE CLOSURE HOURS FOR FACILITIES NOT COVERED BY THE PLCS, IF ANY, SHALL BE AS SPECIFIED ELSEWHERE IN THE PLANS.

LANE VALUE CONTRACT TABLE	
DESCRIPTION OF ROUTE	DISINCENTIVE
CLA-70 SLM 0.00 to CLA-70 SLM 6.27	\$150.00/MIN/LANE
MIA-75 SLM 0.00 to MIA-75 SLM 13.31	\$150.00/MIN/LANE

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

EXISTING MAINTENANCE OF TRAFFIC (MOT) IN PLACE

IN CLARK COUNTY ON IR70 BETWEEN SLM 0.870 AND 0.970 THERE IS AN EXISTING MOT IN PLACE PROTECTING A SECTION OF DAMAGED MEDIAN WALL. THIS WILL REMAIN UNTIL THE CONTRACTOR HAS COORDINATED WITH THE COUNTY MANAGER TO HAVE IT REMOVED. THE CONTRACTOR SHALL THEN PLACE A PROPOSED MOT IN ACCORDANCE WITH THESE PLANS AND CURRENT STANDARDS. QUANTITIES HAVE BEEN PLACED IN THE GENERAL SUMMARY FOR THIS MOT.

ITEM 614. LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE ODOT INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE ODOT, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE ODOT, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP). IN GENERAL, LEOS SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.

IF NECESSITATED BY THE MOT PLANS INCLUDED IN CONTRACT DOCUMENTS, WHEN CONSTRUCTION VEHICLES ARE ENTERING/EXITING THE ZONE DIRECTLY FROM/INTO AN OPEN LANE OF TRAFFIC. IF A LANE HAS BEEN CLOSED TO PROVIDE AN ACCELERATION/DECELERATION LANE FOR THE VEHICLE, THE LEO WILL NOT BE REQUIRED.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE (FOR HOURS SEE SHEETS 14 & 15.)

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

ITEM 614. PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE (OFFICE OF MATERIALS MANAGEMENT WEB PAGE). THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 650 FEET AND 475 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE.

EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS shall be delineated in accordance with C&MS 614.03.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET(S) OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TUNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE. THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE TO THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 3 SIGN MONTH

DESIGN AGENCY



DISTRICT 7 ENGINEERING

DESIGNER

REB

REVIEWER

NKH 09-09-24

PROJECT ID

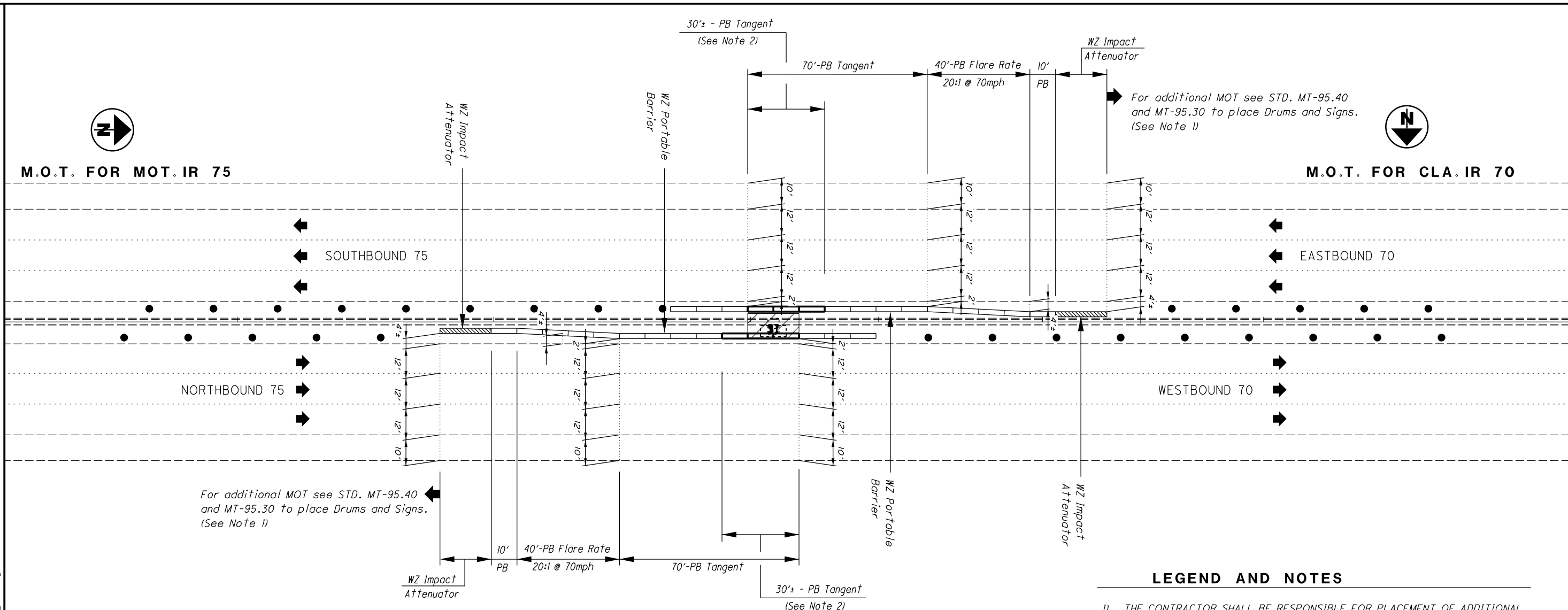
120846

SUBJECT TOTAL

2 4

SHEET TOTAL

P.13 20



NOTE: LOCATION #3, AND LOCATION 9A, HAS MOT ALREADY IN PLACE FROM A SAFETY CONTRACT, THEREFORE THE CONTRACTOR SHALL WORK WITH (ODOT) CLARK COUNTY TO HAVE IT REMOVED UNDER THE CURRENT SAFETY CONTRACT, AND THE CONTRACTOR SHALL INSTALL THE PROPOSED MOT AS SHOWN IN PLANS.

MEDIAN BARRIER WORK LOCATIONS USING MT-95.30 & MT-95.45

- CLA. CO.
- # 1 - SLM. 0.146-0.246 (REMOVAL & PATCHING)
 - # 2 - SLM. 0.646-0.746 (REMOVAL & PATCHING)
 - # 3 - SLM. 0.870-0.970 (REMOVAL & PATCHING)(SEE NOTE)
 - # 4 - SLM. 1.516-1.629 (PATCHING)
 - # 5 - SLM. 1.714-1.837 (PATCHING)
 - # 6 - SLM. 2.826-2.926 (PATCHING)
 - # 7 - SLM. 5.746-5.846 (REMOVAL & PATCHING)
 - # 8 - SLM. 6.166-6.266 (REMOVAL & PATCHING)
- MIA. CO.
- # 9 - SLM. 0.220-0.320 (PATCHING)
 - # 9A - SLM. 0.530-0.625 (REMOVAL & PATCHING)
 - # 10 - SLM. 1.840-1.940 (REMOVAL & PATCHING)
 - # 11 - SLM. 2.380-2.480 (PATCHING)
 - # 12 - SLM. 2.880-2.980 (REMOVAL & PATCHING)
 - # 13 - SLM. 4.130-4.230 (PATCHING)
 - # 14 - SLM. 4.780-4.880 (REMOVAL & PATCHING)
 - # 15 - SLM. 6.080-6.155 (PATCHING)
 - # 16 - SLM. 9.905-10.644 (PATCHING)

LEGEND AND NOTES

- 1). THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACEMENT OF ADDITIONAL MAINTENANCE OF TRAFFIC NOT SHOWN WITH THIS DETAIL, FOR THE PLACEMENT OF DRUMS AND SIGNING TO BE USED DURING PERMITTED LANE CLOSURE HOURS FOR THIS PROJECT. ALL DRUMS, SIGNS AND ANY OTHER M.O.T. RELATED ITEMS SHALL BE REMOVED OR SET ASIDE DURING THE HOURS LANE CLOSURES ARE PROHIBITED.
- 2). THE CONTRACTOR SHALL BE ALLOWED TO MOVE 30'± OF PORTABLE BARRIER LOCATED AT THE BARRIER INLET OR BARRIER WALL REMOVAL LOCATIONS, DURING ACTIVE WORK PERIODS AND WHEN THE ADJOINING LANE IS CLOSED. THE CONTRACTOR SHALL GAIN APPROVAL OF THE TEMPORARY PLACEMENT LOCATION FOR THESE SECTIONS OF PORTABLE BARRIER FROM THE ENGINEER. AT THE END OF EACH ACTIVE WORK PERIOD THESE SECTIONS OF PORTABLE BARRIER SHALL BE PUT BACK IN PLACE, AS SHOWN. ALL MOVING AND RESETTING OF PORTABLE BARRIER, SHALL BE INCLUDED IN THE COST OF THE INITIAL BARRIER PLACEMENT.
- 3). FOR ADVANCED SIGNING ON BOTH NB/SB AND EB/WB SIDES SEE STANDARD DRAWING MT-102.10.
- 4). THE EXAMPLE SHOWN FOR MOT APPLIES TO ANY DIRECTION OF TRAVEL FOR MEDIAN BARRIER WALL REPAIRS WHERE WALL SECTIONS OR INLET SECTIONS WILL BE REMOVED. EACH LOCATION SHALL BE IN ACCORDANCE WITH STANDARD CONSTRUCTION DRAWINGS MT-95.30, MT-95.45, AND CURRENT C&S. FOR CALCULATIONS AN AVERAGE OF 15 DAYS FOR REMOVAL/REPLACE PER LOCATION AND 5 DAYS FOR PATCHING PER LOCATION HAS BEEN USED FOR LEO HOURS.

= CONTRACTOR WORK AREA FOR BARRIER REMOVAL.

MT-95.30 & MT-95.45 (FOR CALCULATIONS SEE NOTE 4.)

ITEM	DESCRIPTION	UNIT	QUANTITY					CALC'S	
614	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	HOUR	80						(8x9)x15 = 1080 (8x8)x5 = 320
614	WORK ZONE IMPACT ATTENUATOR	EACH		2					(2x9) = 18
614	BARRIER REFLECTORS	EACH			6				(2x3)x9 = 54
614	OBJECT MARKERS	EACH				6			(2x3)x9 = 54
614	MAINTAINING TRAFFIC	LS					LS		
622	WZ PORTABLE BARRIER UNANCHORED	FOOT					300		(2x150)x9 = 2700
624	MOBILIZATION	LS					LS		
SUB TOTALS			1400	18	54	54	LS	2700	LS
TOTALS CARRIED TO GENERAL SUMMARY			1400	18	54	54	LS	2700	LS

DESIGN AGENCY

DISTRICT 7 ENGINEERING
 DESIGNER

REB

REVIEWER

NKH 09-09-24

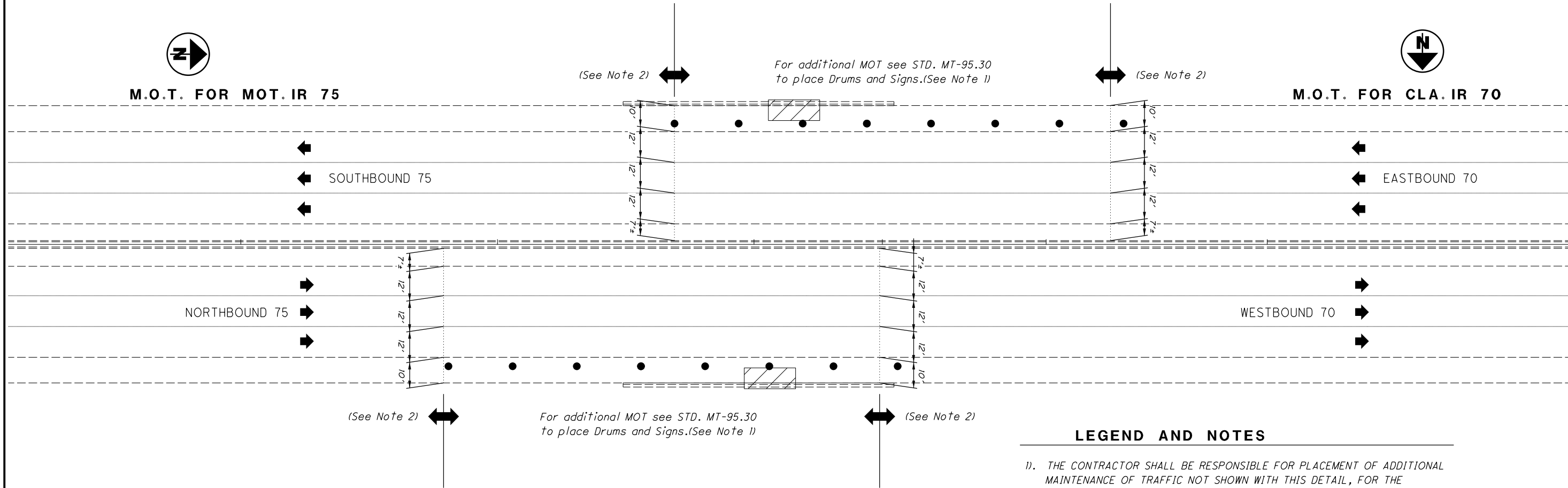
PROJECT ID

120846

SUBSET	TOTAL
3	4
SHEET	TOTAL
P.14	20


M.O.T. FOR MOT. IR 75


M.O.T. FOR CLA. IR 70



**MOT - RIGHT SIDE BARRIER LOCATIONS
 USING MT-95.30 & MT-95.45**

- CLA. CO.
- # 4 - SLM. 1.516-1.629 (EB & WB)
- MIA. CO.
- # 16 - SLM. 13.110-13.210 (NB) (L&R) ON 4 LANE HWY.

LEGEND AND NOTES

- 1). THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACEMENT OF ADDITIONAL MAINTENANCE OF TRAFFIC NOT SHOWN WITH THIS DETAIL, FOR THE PLACEMENT OF DRUMS AND SIGNING TO BE USED DURING PERMITTED LANE CLOSURE HOURS FOR THIS PROJECT. ALL DRUMS, SIGNS AND ANY OTHER M.O.T. RELATED ITEMS SHALL BE REMOVED OR SET ASIDE DURING THE HOURS LANE CLOSURES ARE PROHIBITED.
- 2). FOR ADVANCED SIGNING ON BOTH NB/SB AND EB/WB SIDES SEE STANDARD DRAWING MT-102.10.
- 3). THE EXAMPLE SHOWN FOR MOT APPLIES TO ANY DIRECTION OF TRAVEL FOR RIGHT SIDE BARRIER REPAIRS. EACH LOCATION SHALL BE IN ACCORDANCE WITH STANDARD CONSTRUCTION DRAWING MT-95.30, AND THE CURRENT 2023 C&MS. FOR CALCULATIONS AN AVERAGE OF 5 DAYS PER LOCATION HAS BEEN USED.

 = CONTRACTOR WORK AREA

MT-95.30 & MT-95.45 (FOR CALCULATIONS SEE NOTE 4.)


ITEM	DESCRIPTION	UNIT	QUANTITY							CALC'S
614	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	HOUR	24							(3x8x5) = 120
614										
614										
614	WORK ZONE IMPACT ATTENUATOR	EACH		0						
614	OBJECT MARKERS	EACH		0						
614	MAINTAINING TRAFFIC	LS				LS				
622	WZ PORTABLE BARRIER UNANCHORED	FOOT					0			
624	MOBILIZATION	LS						LS		
626	BARRIER REFLECTORS	EACH							0	
SUB TOTALS			120	0	0	LS	0	LS	0	
TOTALS CARRIED TO GENERAL SUMMARY			120	0	0	LS	0	LS	0	

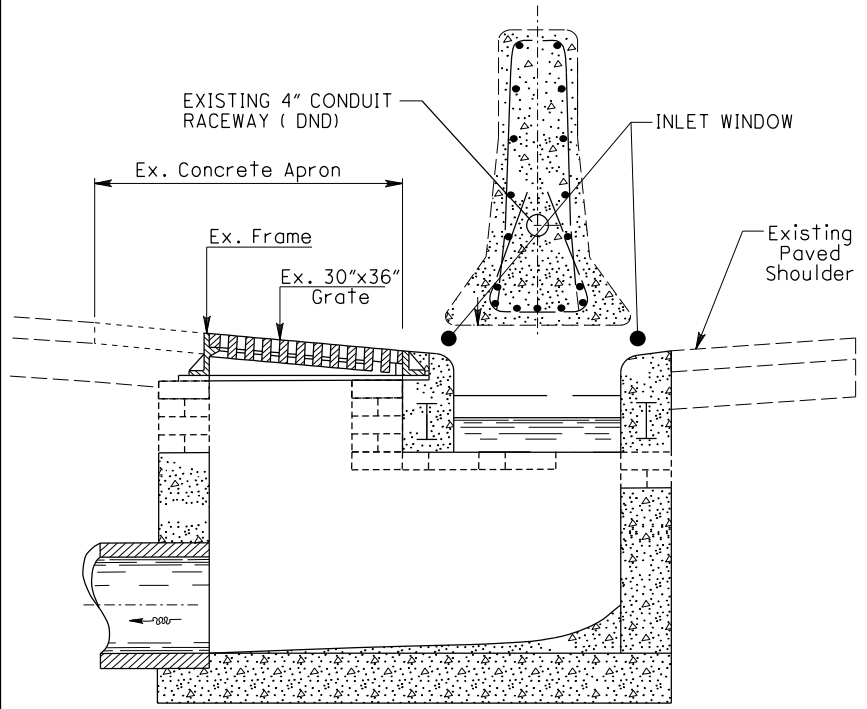
DESIGN AGENCY

 DESIGNER
REB
 REVIEWER
NKH 09-09-24
 PROJECT ID
120846
 SUBSET TOTAL
 4 4
 P.15 20

SHEET NUM.											PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.
			13		14			15	17	19	01/IMS/21	EXT	TOTAL				
															ROADWAY		
					2,700						2,700	622	41100	2,700	FT	PORTABLE BARRIER, UNANCHORED	
									230		230	622	90000	230	FT	BARRIER, MISC.:PORTION OF CONCRETE BARRIER REBUILT	10
															DRAINAGE		
									5		5	611	98011	5	EACH	CONCRETE BARRIER (TYPE D) INLET, AS PER PLAN	10
															MISCELLANEOUS STRUCTURE (BARRIER WALL)		
										527	527	509	10000	527	LB	EPOXY COATED STEEL REINFORCEMENT	
										149	149	510	10000	149	EACH	DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT	
									36.4		36.4	512	10050	36.4	SY	SEALING OF CONCRETE SURFACES (NON-EPOXY)	
									328		328	519	11101	328	SF	PATCHING CONCRETE STRUCTURE, AS PER PLAN	11
															MAINTENANCE OF TRAFFIC		
					1,400			120			1,520	614	11110	1,520	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	13
					18						18	614	12380	18	EACH	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (UNIDIRECTIONAL)	
					54						54	614	13310	54	EACH	BARRIER REFLECTOR, TYPE 1, ONE-WAY	
					54						54	614	13350	54	EACH	OBJECT MARKER, ONE WAY	
			3								3	614	18601	3	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	13
															INCIDENTALS		
												614	11000	LS	MAINTAINING TRAFFIC		
					LS			LS			LS	624	10000	LS	MOBILIZATION		

GENERAL SUMMARY

DESIGN AGENCY	
DISTRICT 7 ENGINEERING	
DESIGNER	REB
REVIEWER	REB
PROJECT ID	08-22-24
	120846
SHEET	TOTAL
P.16	20

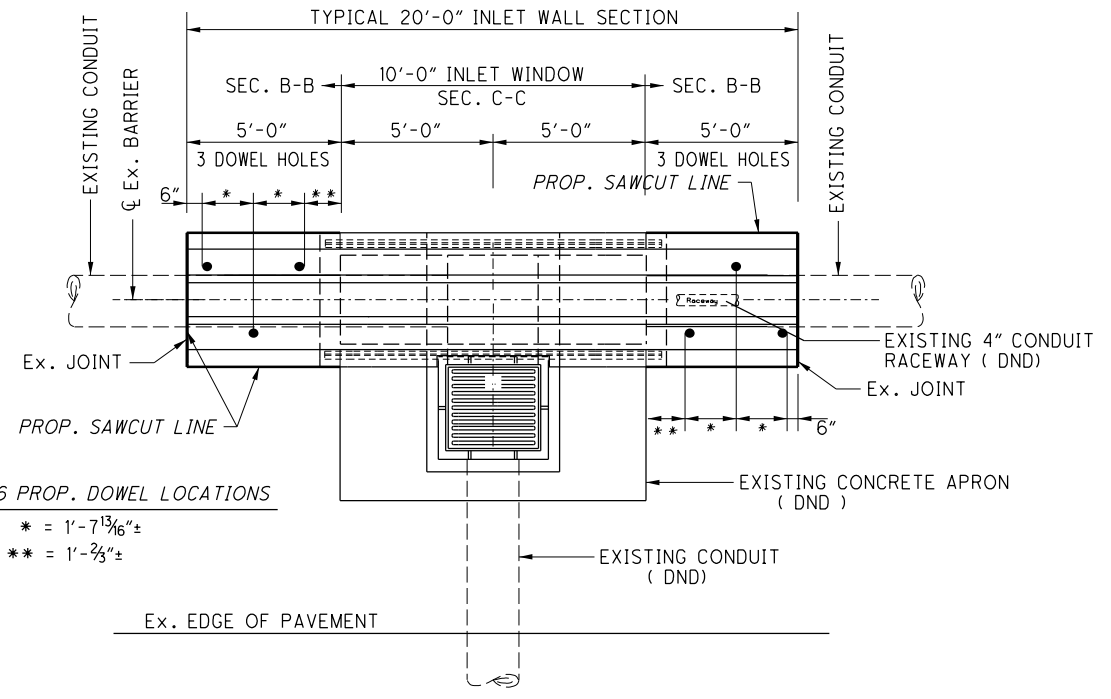


INLET SEC. C-C (SEE BELOW)

(PROP. WORK SEE SHT. 20 SEC. B-B AND C-C)

ESTIMATED QUANTITIES

REF. #	SHEET #	CO./RTE.	SLM. TO SLM.		SIDE	TREATMENT	SEALING (512) SY.	PATCHING (519) SF.	INLET TYPE D (611) EA.	BARRIER (622) FT.	COMMENTS
LOC.-1	3	CLA.-70	0.146	0.246	EB/WB	A & C	1.3	6		20	MINOR PATCHING THRU LIMITS/REPLACE FIRST SECTION BEYOND STRUCTURE CLA-70-0.16.
LOC.-2	3	CLA.-70	0.646	0.746	EB/WB	A & C	1.2	5	1	10	MINOR PATCHING THRU LIMITS / REPLACE SECTION PRIOR TO AND OVER INLET.
LOC.-3	3	CLA.-70	0.870	0.970	EB/WB	C	.5		1	20	REPLACE INLET AND SECTION AFTER IT.
LOC.-4	3	CLA.-70	1.516	1.629	EB/WB	A	.3	3			MINOR PATCHING THRU LIMITS .
LOC.-5	3	CLA.-70	1.714	1.837	EB/WB	A	6	55			MINOR PATCHING THRU LIMITS. (TOP OF WALL)
LOC.-6	4	CLA.-70	2.826	2.926	EB/WB	A & B	1.5	8			MINOR/MAJOR PATCHING THRU LIMITS / TOP OF WALL
LOC.-7	4	CLA.-70	5.746	5.846	EB/WB	A & B & C	.5	24		90	MINOR/MAJOR PATCHING THRU LIMITS, AND COMPLETE REPLACEMENT.
LOC.-8	4	CLA.-70	6.166	6.266	EB/WB	A & B & C	1.5	12		50	MINOR PATCHING THRU LIMITS, AND COMPLETE REPLACEMENT.
LOC.-9	5	MIA.-75	0.220	0.320	NB/SB	A & B	4.8	43			MINOR PATCHING THRU LIMITS, WITH ONE MAJOR PATCHING AREA.
LOC.-9A	5	MIA.-75	0.530	0.625	NB/SB	A & C	1.2	5		20	MINOR PATCHING THRU LIMITS, WITH ONE MAJOR PATCHING AREA.
LOC.-10	5	MIA.-75	1.840	1.940	NB/SB	A & C	2	20		10	MINOR PATCHING THRU LIMITS, AND COMPLETE REPLACEMENT.
LOC.-11	5	MIA.-75	2.380	2.480	NB/SB	A & B	5.5	50			MINOR PATCHING THRU LIMITS, WITH ONE MAJOR PATCHING AREA.
LOC.-12	5	MIA.-75	2.880	2.980	NB/SB	A & C	1.2	13	2		MINOR PATCHING THRU LIMITS/REPLACE TWO INLET SECTIONS. (WALL ONLY)
LOC.-13	6	MIA.-75	4.130	4.230	NB/SB	A & B & C	3.4	31		10	MINOR PATCHING THRU LIMITS, WITH COMPLETE REPLACEMENT
LOC.-14	6	MIA.-75	4.780	4.880	NB/SB	A & C	1.1	10	1		MINOR PATCHING THRU LIMITS/REPLACE ONE INLET SECTIONS. (WALL ONLY)
LOC.-15	6	MIA.-75	6.080	6.155	SB	A	.4	4			MINOR PATCHING THRU LIMITS. (AT BASE OF WALL)
LOC.-16	6	MIA.-75	9.905	10.644	SB	A & B	2	19			MINOR PATCHING THRU LIMITS, WITH TWO MAJOR PATCHING AREAS AT OVERHEAD SIGNS.
LOC.-17	7	MIA.-75	13.110	13.210	NB	A	2	20			MINOR PATCHING THRU LIMITS, (MIA-75-1315R) BOTH PARAPETS
TOTALS CARRIED TO GENERAL SUMMARY							36.4	328	5	230	



20' INLET SECTION OVERHEAD VIEW (TYP.)

**TYPE D NEW JERSEY WALL SHOWN (MIA/CLA CO.)
 SINGLE SLOPE WALL NOT SHOWN (CLA CO.)**

TREATMENT LEGEND

TREATMENT "A"

TREATMENT "A" AREAS INCLUDE THOSE WHERE PARTIAL DEPTH PATCHING ALONE IS A SUFFICIENT REPAIR. THIS INCLUDES FULL DEPTH REPAIRS LIMITED TO ONLY THE TOP 6" OF THE BARRIERS.

- 1). SET MOT PER THESE PLANS AND THE ODOT TEM.
- 2). REPAIR VISIBLY UNSOUND AND DAMAGED AREAS NOTED IN THE PLANS PER C&MS 519.
- 3). SEAL PATCH REPAIRED CONCRETE SURFACES PER ITEM 512.

TREATMENT "B"

TREATMENT "B" AREAS INCLUDE PARTIAL FULL DEPTH REPLACEMENT OF BARRIERS.

- 1). SET MOT PER THESE PLANS AND THE ODOT TEM.
- 2). REMOVE VISIBLY UNSOUND AND DAMAGED AREAS NOTED IN THE PLANS PER C&MS 202. REMOVED AREAS SHALL BE SIMPLE RECTANGULAR SHAPES.
- 3). REPLACE PORTIONS OF MEDIAN BARRIER PER C&MS 622.
- 4). REPLACE PORTIONS OF REINFORCED BRIDGE PARAPETS PER C&MS 517.
- 5). SEAL PATCH REPAIRED CONCRETE SURFACES PER ITEM 512.

TREATMENT "C"

TREATMENT "C" AREAS INCLUDE REMOVE & REPLACEMENT OF CONCRETE BARRIERS AND INLETS. THE EXISTING APRONS AND INLETS BASINS ARE TO REMAIN. SEE LIMITS ON SHEET 19.

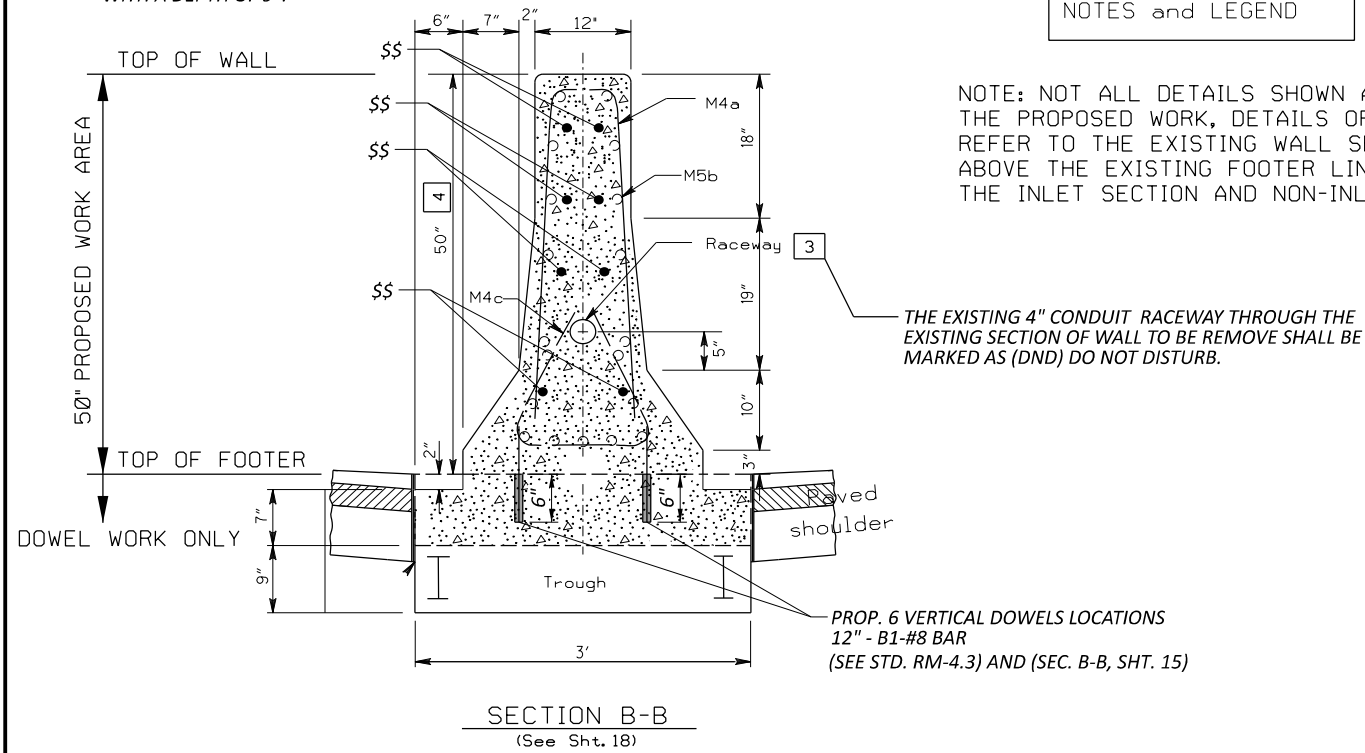
- 1). SET MOT PER THESE PLANS AND THE ODOT TEM.
- 2). REMOVE DAMAGED CONCRETE BARRIER OR INLET NOTED IN THE PLANS PER C&MS 202. SAW CUTS TO BE AT EXISTING CONTROL JOINTS.
- 3). REPLACE PORTIONS OF MEDIAN BARRIER PER C&MS 611.
- 4). SEAL PATCH REPAIRED CONCRETE SURFACES PER ITEM 512.

DESIGN AGENCY

DISTRICT 7 ENGINEERING
 DESIGNER
REB
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\$\$ = 16 PROP. PARALLEL DOWELS - 18" B3 #8 BARS WITH A DEPTH OF 9".

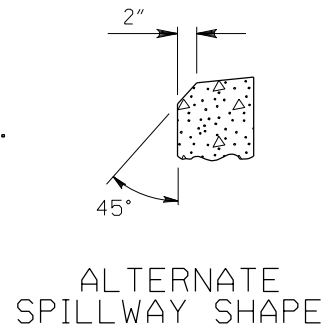
See Sheet 18 for NOTES and LEGEND



NOTE: NOT ALL DETAILS SHOWN APPLY TO THE PROPOSED WORK, DETAILS OF INTEREST REFER TO THE EXISTING WALL SECTIONS ABOVE THE EXISTING FOOTER LINE, AT BOTH THE INLET SECTION AND NON-INLET SECTION.

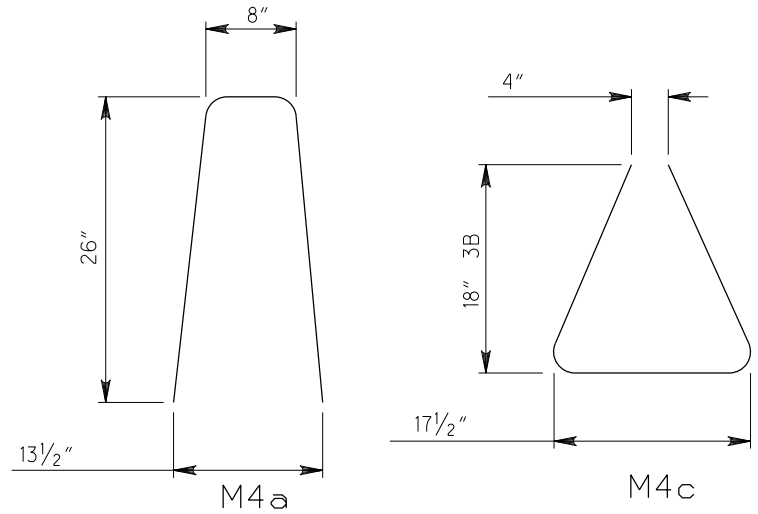
THE EXISTING 4" CONDUIT RACEWAY THROUGH THE EXISTING SECTION OF WALL TO BE REMOVE SHALL BE MARKED AS (DND) DO NOT DISTURB.

PROP. 6 VERTICAL DOWELS LOCATIONS 12" - B1-#8 BAR (SEE STD. RM-4.3) AND (SEC. B-B, SHT. 15)



INLET NO.	W	M4a #4		M5b #5		M4c #4		S6x12.5	
		No.	Length	No.	Length	No.	Length	No.	Length
3	6"	10	5'-0"	13	19'-8"	10	4'-6"	2	11'-0"

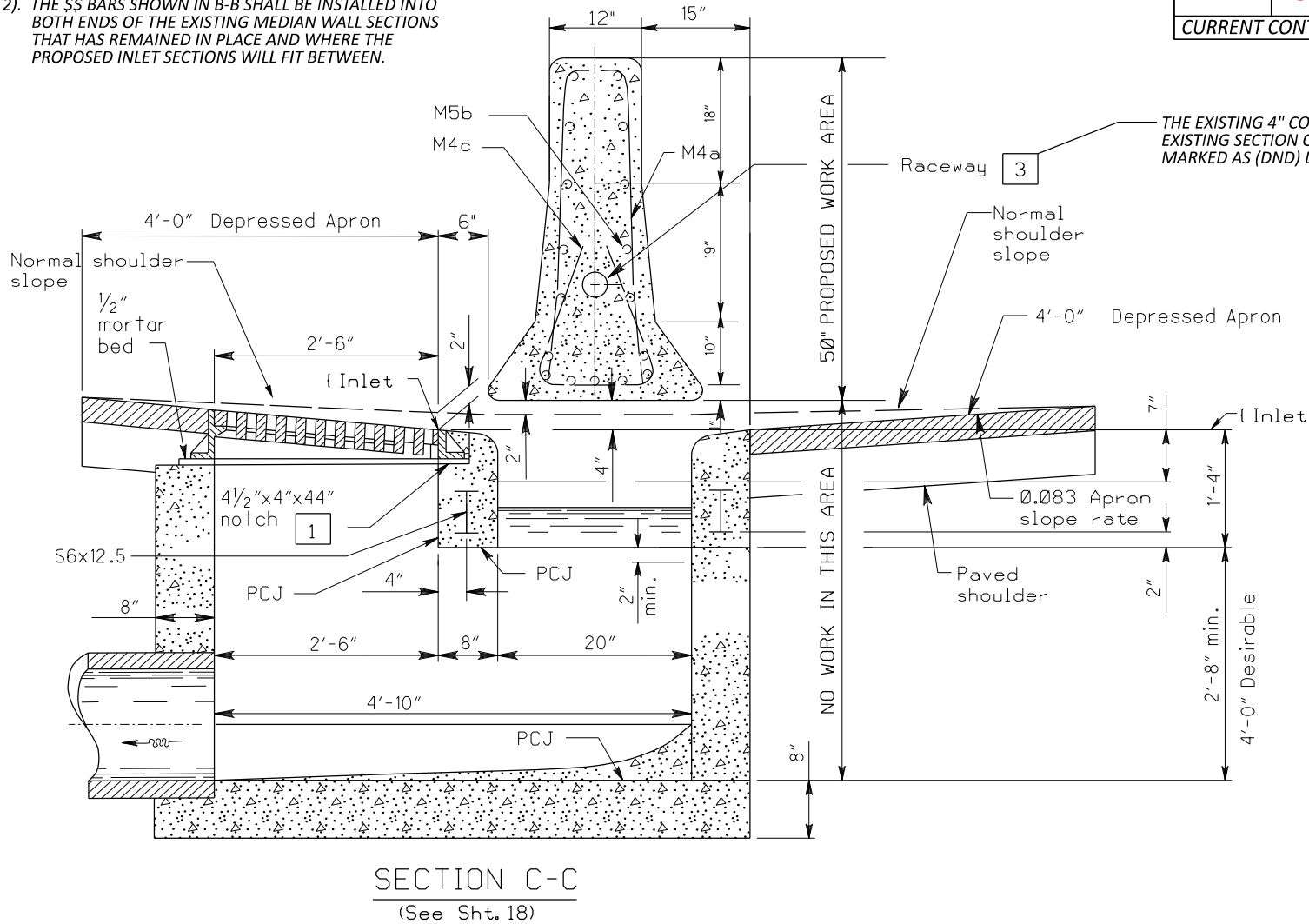
Included for estimating purposes only. The cost of furnishing and placing all reinforcing steel shall be included in Item 611 for payment.



MARK	NUMBER TOTAL	LENGTH	WEIGHT	TYPE	DOWEL HOLES W/NS, NM, GROUT EA.
DOWEL HOLES & REINFORCING STEEL FOR CURRENT CONTRACT					
B1	24	1'-0"	64	STR	24
B2	29	1'-0"	78	STR	29
B3	96	1'-6"	385	STR	96
CURRENT CONTRACT SUB-TOTAL			527		149

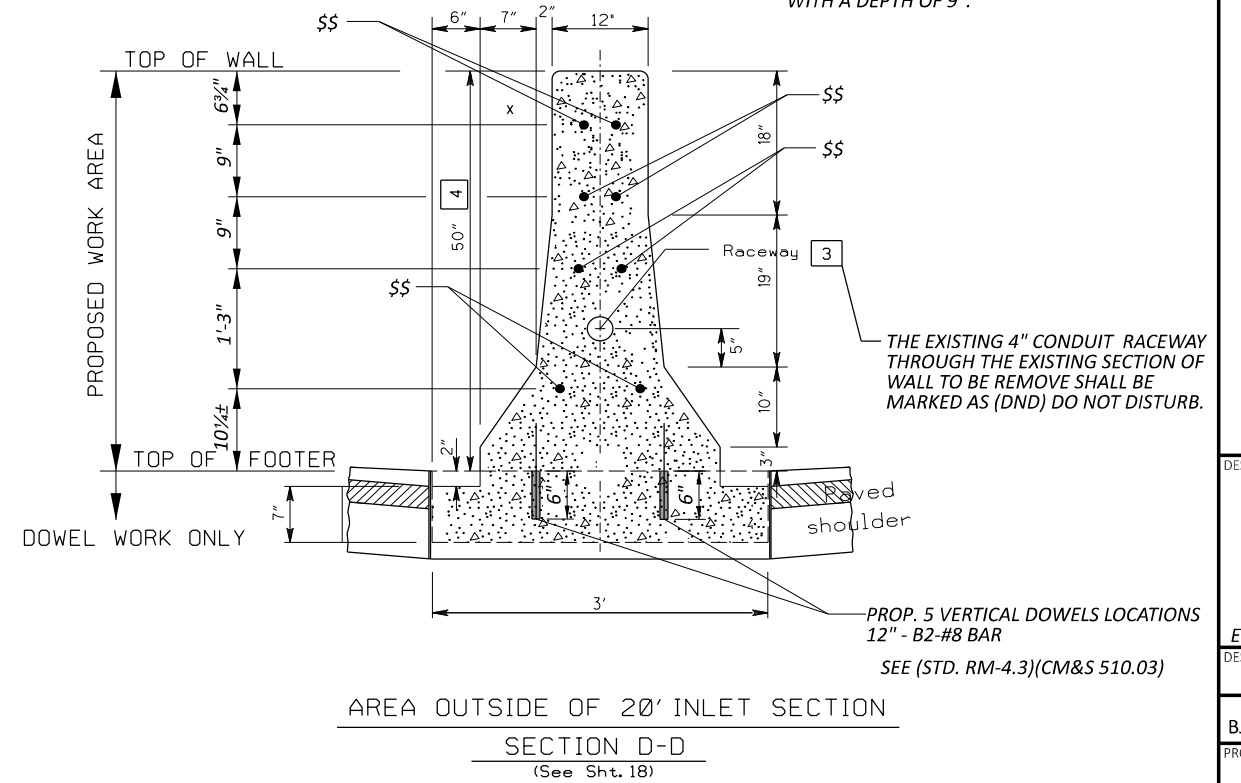
BENDING DIAGRAM

- EXISTING BARS SHOWN IN SECTIONS B-B AND C-C ARE INCLUDED IN THE COST FOR THE INLET SECTION.
- THE \$\$ BARS SHOWN IN B-B SHALL BE INSTALLED INTO BOTH ENDS OF THE EXISTING MEDIAN WALL SECTIONS THAT HAS REMAINED IN PLACE AND WHERE THE PROPOSED INLET SECTIONS WILL FIT BETWEEN.



THE EXISTING 4" CONDUIT RACEWAY THROUGH THE EXISTING SECTION OF WALL TO BE REMOVE SHALL BE MARKED AS (DND) DO NOT DISTURB.

\$\$ = 16 PROP. PARALLEL DOWELS - 18" B3 #8 BARS WITH A DEPTH OF 9".



THE EXISTING 4" CONDUIT RACEWAY THROUGH THE EXISTING SECTION OF WALL TO BE REMOVE SHALL BE MARKED AS (DND) DO NOT DISTURB.

PROP. 5 VERTICAL DOWELS LOCATIONS 12" - B2-#8 BAR (SEE STD. RM-4.3)(CM&S 510.03)

AREA OUTSIDE OF 20' INLET SECTION

SECTION D-D (See Sht. 18)