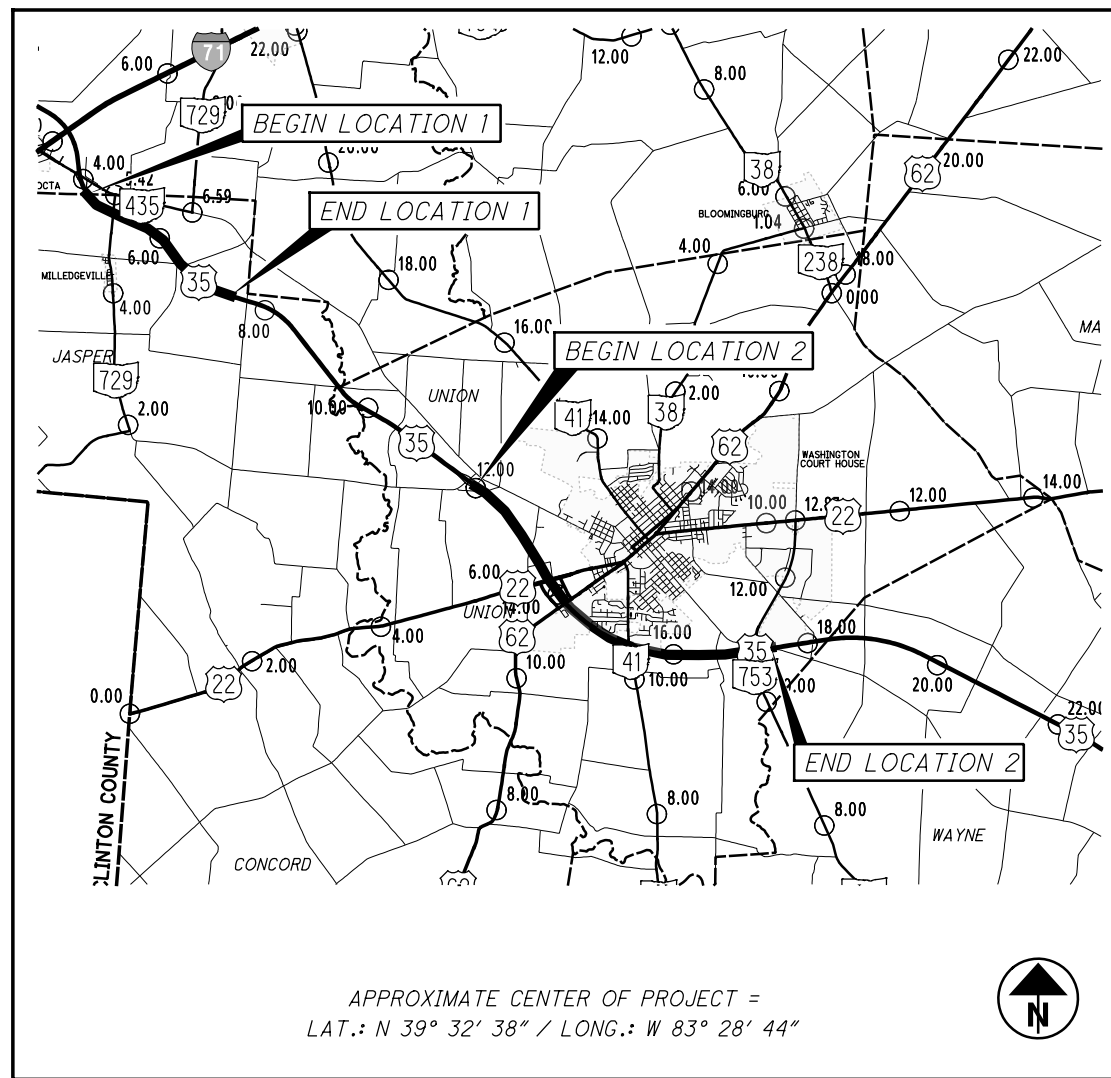


STATE OF OHIO  
DEPARTMENT OF TRANSPORTATION  
**FAY-35-4.52**  
CITY OF WASHINGTON COURTHOUSE  
JASPER AND UNION TOWNSHIPS  
FAYETTE COUNTY



APPROXIMATE CENTER OF PROJECT =  
LAT.: N 39° 32' 38" / LONG.: W 83° 28' 44"

INDEX OF SHEETS

TITLE	1
LOCATION MAP	2
TYPICAL SECTION	3
TYPICAL DETAILS	4 - 5
GENERAL NOTES	6 - 7
MAINTENANCE OF TRAFFIC NOTES	8 - 11
MAINTENANCE OF TRAFFIC SUBSUMMARY	12
MAINTENANCE OF TRAFFIC PLANS	13 - 23, 13A
GENERAL SUMMARY	24 - 25
PAVEMENT SUBSUMMARY	26
PAVEMENT REPAIR SUBSUMMARY	27
INTERCHANGE DETAILS	28 - 31
GUARDRAIL DETAILS	32 - 33
GUARDRAIL SUBSUMMARY	34
GUARDRAIL PLAN	35 - 49
TRAFFIC CONTROL SUBSUMMARY	50 - 51
STRUCTURES 20' AND OVER	52 - 63

PROJECT DESCRIPTION

PAVEMENT RESURFACING ON US-35 IN FAYETTE COUNTY.  
THIS PROJECT INCLUDES GUARDRAIL UPGRADES AND BRIDGE WORK AT VARIOUS LOCATIONS WITHIN THE PROJECT LIMITS AS DETAILED IN THE PLANS.

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2019 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS PROJECT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE OF TRAFFIC AND SAFETY WILL BE AS SET FORTH ON PLANS AND ESTIMATES.

DESIGN DESIGNATION:

SEE LOCATION MAP SHEET 2

EARTH DISTURBED AREA:

PROJECT EARTH DISTURBED AREA N/A\*  
EST. CONTRACTOR EARTH DISTURBED AREA N/A\*  
NOTICE OF INTENT EARTH DISTURBED AREA N/A\*

\* MAINTENANCE PROJECT

**UNDERGROUND UTILITIES**  
Contact Two Working Days  
Before You Dig

**OHIO811.org**  
Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764  
(Non-members must be called directly)



ENGINEERS SEAL:

STATE OF OHIO  
REGISTERED PROFESSIONAL ENGINEER  
ROBERT A. MCNEILL  
E-78025  
SIGNED: *[Signature]*  
DATE: 10.18.19

STANDARD CONSTRUCTION DRAWINGS								SUPPLEMENTAL SPECIFICATIONS	
MT 95.30	07/21/17	TC 61.10	01/17/14	BP 3.1	07/18/14	MGS-1.1	01/19/18	800	07/19/19
MT 95.40	01/20/17	TC 64.10	01/20/17	BP 9.1	01/18/19	MGS-2.1	01/19/18	821	04/20/12
MT 95.50	07/21/17	TC 65.10	01/17/14			MGS-4.3	01/18/13	832	10/19/18
MT 98.10	01/20/17	TC 65.11	07/21/17			MGS-5.2	07/15/16	846	04/17/15
MT 98.11	01/20/17	TC 72.20	07/20/18			MGS-5.3	07/15/16	848	01/20/17
MT 98.20	04/19/19					MGS-6.1	01/19/18	872	07/19/19
MT 98.22	01/20/17					MGS-6.2	01/19/18	875	01/18/19
MT 98.28	01/20/17								
MT 99.20	07/20/18								
MT 99.30	01/19/18								
MT 101.90	07/21/17								
MT 104.10	10/16/15								
MT 105.10	07/19/13								
								<b>SPECIAL PROVISIONS</b>	

APPROVED *[Signature]*  
DATE 10/18/19  
DISTRICT DEPUTY DIRECTOR  
OHIO DEPT. OF TRANSPORTATION

APPROVED \_\_\_\_\_  
DATE \_\_\_\_\_  
DIRECTOR, DEPARTMENT OF TRANSPORTATION

FEDERAL PROJECT NO. E190(143)  
PID NO. 107802  
CONSTRUCTION PROJECT NO. \_\_\_\_\_  
RAILROAD INVOLVEMENT NONE  
FAY-35-4.52  
1/63

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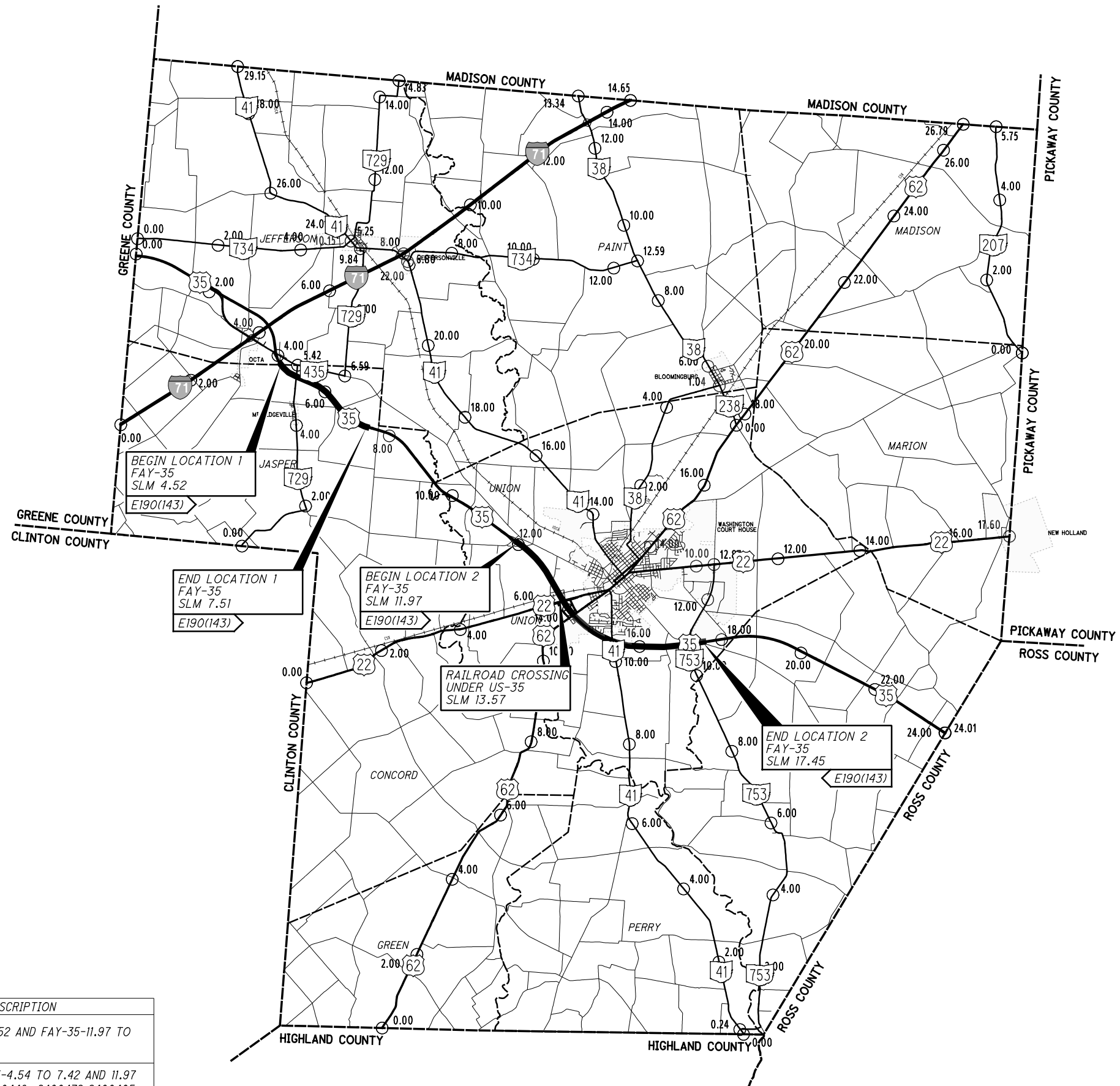


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 CHECKED XXX  
 HORIZONTAL SCALE IN MILES

**DESIGN DESIGNATION**

**FAY-35**  
 (4.52-7.51)  
 CURRENT ADT (2016) ..... 11863  
 DESIGN YEAR ADT (2032) ..... 14000  
 TRUCKS (24 HOUR B&C) ..... 3677  
 DIRECTIONAL DISTRIBUTION ..... 50%  
 DESIGN SPEED ..... 70  
 LEGAL SPEED ..... 70  
 NHS ROUTE ..... YES  
 FEDERAL TRUCK ROUTE ..... YES  
 DESIGN FUNCTIONAL CLASSIFICATION:  
 02 FREEWAYS AND EXPRESSWAYS - (RURAL)

**FAY-35**  
 (11.97-17.45)  
 CURRENT ADT (2016) ..... 14335  
 DESIGN YEAR ADT (2032) ..... 17000  
 TRUCKS (24 HOUR B&C) ..... 4587  
 DIRECTIONAL DISTRIBUTION ..... 50%  
 DESIGN SPEED ..... 70  
 LEGAL SPEED ..... 70  
 NHS ROUTE ..... YES  
 FEDERAL TRUCK ROUTE ..... YES  
 DESIGN FUNCTIONAL CLASSIFICATION:  
 02 FREEWAYS AND EXPRESSWAYS - (RURAL 11.97-13.24/  
 URBAN 13.24-17.45)

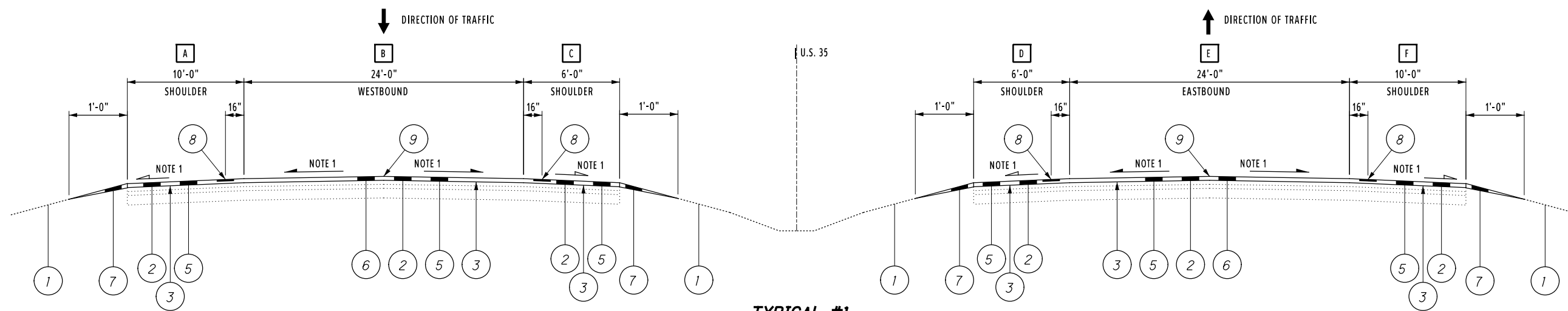


CATEGORY	GROUP	RESPONSIBLE ENTITY	PLAN SPLIT CODE	DESCRIPTION
NHS	PAVEMENT	STATE	01/NHS/PV	FAY-35-4.54 TO 7.52 AND FAY-35-11.97 TO 17.39
NHS	BRIDGE	STATE	02/NHS/BR	BRIDGES ON FAY-35-4.54 TO 7.42 AND 11.97 TO 17.39 SFNs: 2400448, 2400472, 2400405, 2400413, 2400502, 2400499, 2400561, 2400553, 2400596, 2400618, 2400642, 2400650

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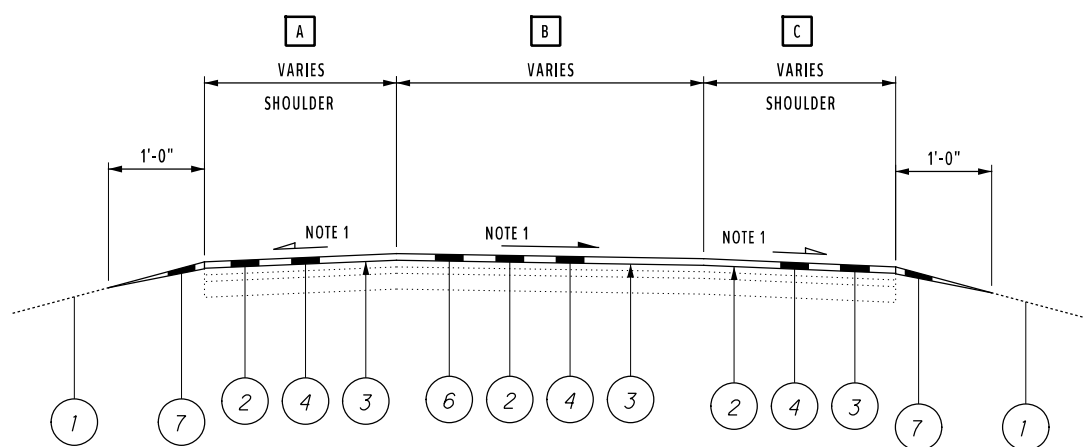
**LOCATION MAP**

**FAY-35-4.52**



**TYPICAL #1**

LOC 1    FAY 35    4.52    TO    7.51    =    2.99  
 LOC 1    FAY 35    11.97    TO    17.45    =    5.48

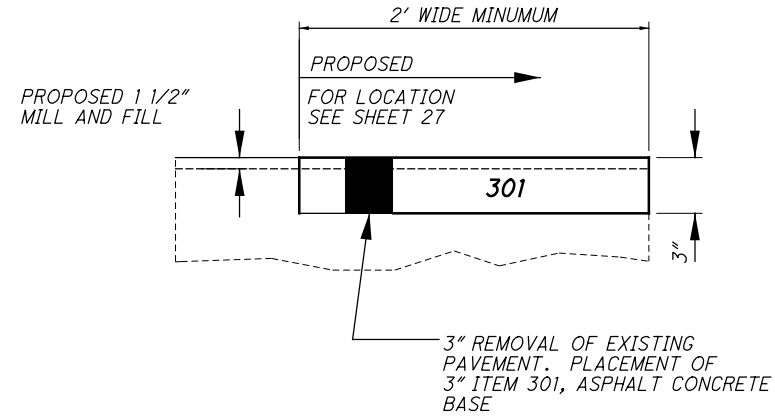


**TYPICAL #2**

ALL RAMPS

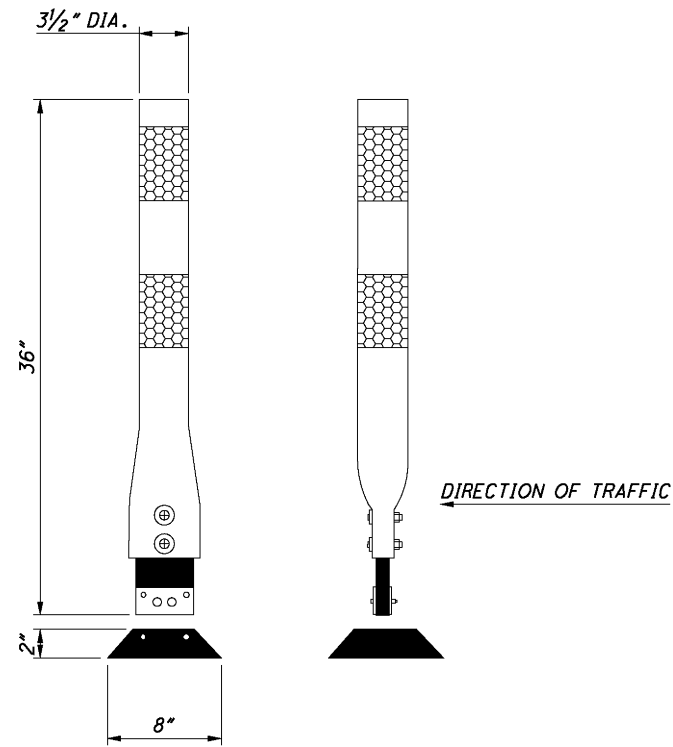
LEGEND	
1	ITEM 209 - LINEAR GRADING
2	ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE
3	ITEM 407 - NON-TRACKING TACK COAT (0.085 GAL/SQ YD)
4	ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (446), PG76-22M
5	ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (447)
6	ITEM 442 - ANTI-SEGREGATION EQUIPMENT
7	ITEM 617 - COMPACTED AGGREGATE (AVERAGE 2" x 1')
8	ITEM 618 - EDGE LINE, RUMBLE STRIP (ASPHALT CONCRETE)
9	ITEM 875 - LONGITUDINAL JOINT ADHESIVE (INCIDENTAL TO ITEM 442 (5) )

NOTE 1:  
PAVEMENT CROSS SLOPES TO BE MAINTAINED.



**PARTIAL DEPTH PAVEMENT REPAIR DETAIL (ASPHALT CONCRETE BASE)**

SEE GENERAL NOTES FOR MORE INFORMATION REGARDING ITEM 251-PARTIAL DEPTH PAVEMENT REPAIR, (ASPHALT CONCRETE BASE) AS PER PLAN.



**DELINEATOR, MISC.:  
REBOUNDABLE TUBULAR PYLON**

MARKER SHALL CONSIST OF A 3.5 INCH PLASTIC TUBE ATTACHED TO A HIGH IMPACT BLACK PLASTIC BASE BY A HEAVY DUTY RUBBER HINGE. TUBE COLOR SHALL BE UNIFORM THROUGHOUT THE PLASTIC MATERIAL. TWO 6 INCH BANDS OF REFLECTIVE SHEETING CONFORMING TO T30.101 SHALL BE INSTALLED WITHIN THE TOP 18 INCHES OF THE POST. THE BASE SHALL BE MOUNTED USING A TWO PART EPOXY SUPPLIED BY THE MANUFACTURER. IN ADDITION TO THE ADHESIVE, THE BASE SHALL ALSO BE MECHANICALLY FASTENED TO THE CONCRETE ISLAND IN A MANNER APPROVED BY THE ENGINEER. THE ASSEMBLY SHALL BE ORIENTED SO THE HINGE IS PERPENDICULAR TO THE DIRECTION OF TRAFFIC. THE COLOR OF THE POST SHALL BE NOMINALLY THE SAME (WHITE). REBOUNDABLE TUBULAR PYLON SHALL BE PLACED 3 FEET FROM THE FACE OF THE CURB AND EACH SHALL BE SPACED BETWEEN 5 FEET AND 8<sup>3</sup>/<sub>32</sub>. QUANTITIES OF THIS ITEM CAN BE FOUND ON SHEET 25/63 OF THIS PLAN.

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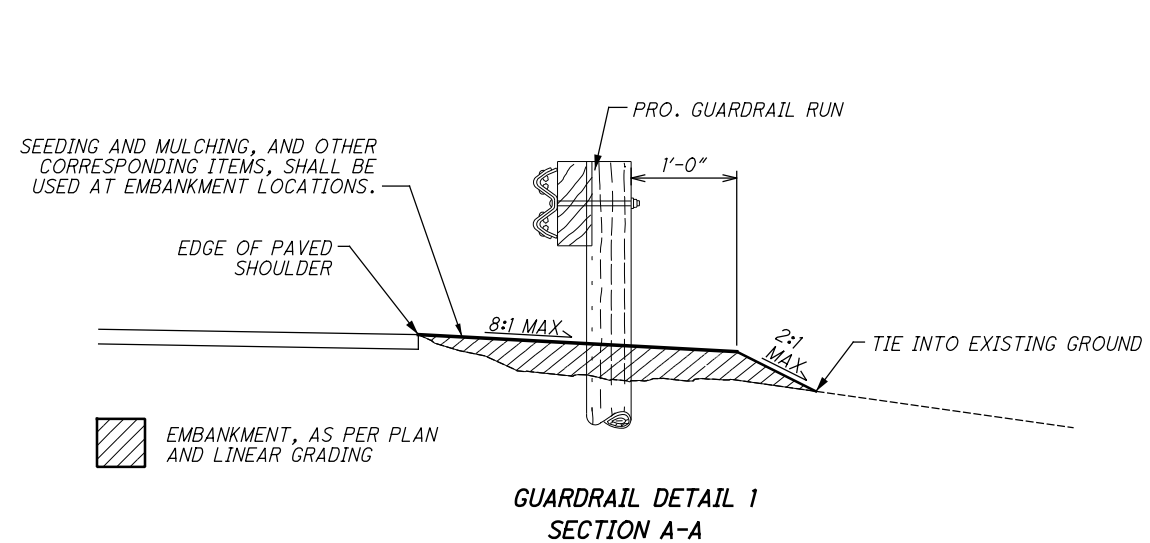
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**TYPICAL DETAILS**

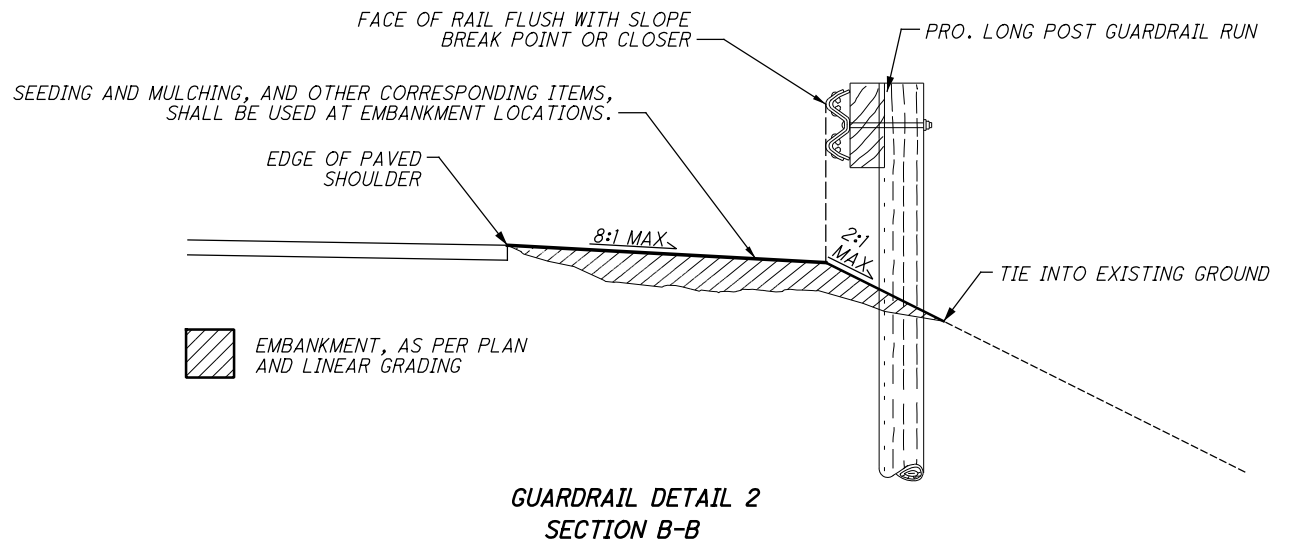
**FAY - 35 - 4.52**

4  
63



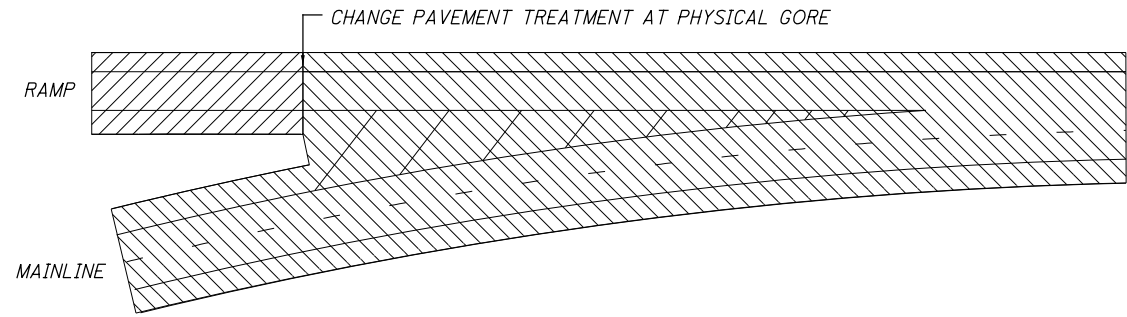


**GUARDRAIL DETAIL 1  
SECTION A-A**



**GUARDRAIL DETAIL 2  
SECTION B-B**

SEE STANDARD DRAWING MGS-1.1  
FOR MORE DETAILS AND DIMENSIONS



**PAVING AT RAMPS DETAIL**

- PAVE PER TYPICAL SECTION 1 (ALL MAINLINE)
- PAVE PER TYPICAL SECTION 2 (ALL RAMPS)

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**GENERAL:**

THE CONTRACTOR SHALL SUBMIT IN WRITING A SCHEDULE OF OPERATIONS TO THE ENGINEER (SEE 108.02) AND RECEIVE APPROVAL IN WRITING BEFORE WORK IS STARTED ON THIS PROJECT. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

**NOTIFICATION OF CONSTRUCTION INITIATION:**

AT LEAST FOURTEEN DAYS PRIOR TO STARTING INITIAL CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL ADVISE THE DISTRICT OFFICE OF COMMUNICATIONS VIA EMAIL AT d06.pio@dot.ohio.gov, THE DISTRICT WORK ZONE TRAFFIC MANAGER VIA EMAIL AT d06.mot@dot.ohio.gov AND THE CENTRAL OFFICE SPECIAL HAUL PERMITS SECTION BY FAX AT (614)728-4099 OF THE ANTICIPATED START DATE OF ANY CONSTRUCTION ACTIVITIES INCLUDING BUT NOT LIMITED TO THE PLACING OF WORK ZONE SIGNS. THE NOTIFICATION SHALL ALSO INCLUDE THE PROJECT NUMBER, PID, NAME AND PHONE NUMBER OF THE CONTRACTOR, A POINT OF CONTACT AND THE ANTICIPATED IMPACT ON TRAFFIC. THE CONTRACTOR WILL IMMEDIATELY INFORM THE DISTRICT OFFICE OF COMMUNICATIONS AND THE DISTRICT WORK ZONE TRAFFIC MANAGER OF ANY AND ALL DELAYS AND/OR CHANGES REGARDING THE CONSTRUCTION INITIATION DATE.

**CONTRACTORS EQUIPMENT - OPERATION AND STORAGE:**

THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL. EQUIPMENT SHALL HAVE AT LEAST ONE AMBER FLASHING LIGHT. WHEN PARKED ALONG THE HIGHWAY, THE EQUIPMENT SHALL BE LOCATED EITHER A MINIMUM OF THIRTY FEET FROM THE EDGE OF PAVEMENT OR SIX FEET BEHIND GUARDRAIL WITH A MINIMUM OF 125 FEET OF GUARDRAIL PRECEDING THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT AN APPROVED CONTRACTOR'S STORAGE AREA.

**CONTINGENCY QUANTITIES:**

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

**WORK LIMITS:**

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

**UTILITIES:**

THE ODOT CONTRACTOR IS REQUIRED TO CONTACT OUPS A MINIMUM OF 48 HOURS EXCLUDING WEEKENDS AND HOLIDAYS TO PERMIT ALL UNDERGROUND UTILITIES AN OPPORTUNITY TO MARK THEIR LINES. IT IS ALSO THE ODOT CONTRACTOR'S RESPONSIBILITY TO CONTACT ALL NON-MEMBERS OF OUPS DIRECTLY A MINIMUM OF 48 HOURS' NOTICE EXCLUDING WEEKENDS AND HOLIDAYS TO PROVIDE THEM WITH THE SAME OPPORTUNITY.

IT IS ODOT'S EXPECTATION THAT ALL GUARD RAIL POSTS WILL BE INSTALLED IN THE SAME LOCATIONS AND THERE WILL BE NO DISRUPTION TO UNDERGROUND UTILITIES. IF THERE IS A UTILITY MARKING WITHIN THE TOLERANCE ZONE OF A UTILITY LOCATE FROM THE PROPOSED GUARDRAIL PLACEMENT IT IS THE ODOT CONTRACTORS RESPONSIBILITY TO DIRECTLY CONTACT THE IMPACTED UTILITY AND WORK WITH THEM TO FIND A SOLUTION THAT DOES NOT CHANGE THE GUARDRAIL PLACEMENT OR DAMAGE THE EXISTING UTILITY. NO UTILITY RELOCATION WILL BE REIMBURSED NOR WILL DELAY CLAIMS BE PERMISSIBLE BASED ON ON LACK OF COORDINATION BETWEEN THE ODOT CONTRACTOR AND THE IMPACTED UTILITY.

**ALIGNMENT AND PROFILE:**

THE WORK PROPOSED BY THIS PROJECT IS FOR TREATMENT ON THE EXISTING PAVEMENT. THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED, AND THE PROFILE OF THE PROPOSED SURFACE WILL BE SIMILAR TO THAT OF THE EXISTING PAVEMENT.

**REMOVAL ITEMS:**

UNLESS OTHERWISE INSTRUCTED, GUARDRAIL, POSTS, ASPHALT, CONCRETE, AND MISCELLANEOUS HARDWARE DESIGNATED FOR REMOVAL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF. PAYMENT FOR THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE REMOVED ITEM.

**ITEM 203 - EMBANKMENT, AS PER PLAN:**

QUANTITIES FOR ITEM 203 - EMBANKMENT HAVE BEEN PROVIDED THROUGHOUT THIS PLAN TO BUILD UP FORE-SLOPES AND ENSURE PROPER GRADING FOR THE PROPOSED ANCHOR ASSEMBLIES AND LONG SPAN GUARDRAIL. THIS ITEM OF WORK INCLUDES ANY CLEARING AND GRUBBING NECESSARY TO PLACE THE EMBANKMENT AT THE LOCATIONS SPECIFIED OR DIRECTED. THE CONTRACTOR SHALL BE PREPARED TO USE EMBANKMENT AT THE LOCATIONS SPECIFIED IN THE PLANS AND ANY OTHER AREAS "AS DIRECTED BY THE ENGINEER".

**GUARDRAIL POST HOLES:**

ALL HOLES REMAINING AFTER REMOVAL OF GUARDRAIL POSTS SHALL BE FILLED WITH GRANULAR MATERIAL, EXCESS MATERIAL RESULTING FROM GUARDRAIL RECONSTRUCTION, OR EXCESS MATERIAL FROM BERM RESHAPING. FILL MATERIAL CONTAINING SOD SHALL NOT BE USED. ALL FILL MATERIAL SHALL BE APPROVED BY THE ENGINEER. MATERIAL PLACED IN HOLES SHALL BE THOROUGHLY COMPACTED AND LEVELED OFF AS DIRECTED BY THE ENGINEER. PAYMENT FOR THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE APPLICABLE GUARDRAIL ITEM.

**CONNECTION BETWEEN EXISTING AND PROPOSED GUARDRAIL:**

WHEN IT IS NECESSARY TO SPLICE PROPOSED GUARDRAIL TO EXISTING GUARDRAIL, ONLY THE EXISTING GUARDRAIL SHALL BE CUT, DRILLED, OR PUNCHED. THE CONNECTION SHALL BE MADE USING A W-BEAM, BEAM SPLICE AS SHOWN IN AASHTO M 180-12, EXCEPT THE BEAM WASHERS ARE NOT TO BE USED. PAYMENT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE RESPECTIVE GUARDRAIL ITEMS.

**CURVED RAIL ELEMENTS:**

ALL RADII OF CURVED RAIL ARE ESTIMATED AND ACTUAL RADII OF PROPOSED RAIL SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR PRIOR TO ORDERING. LENGTH OF CURVED RAIL ELEMENTS, WHERE CALLED FOR IN A RUN, SHALL BE INCLUDED IN THE TOTAL LENGTH OF RUN SHOWN IN THE GUARDRAIL COLUMN AND THE CURVED RAIL ELEMENT TOTAL ARE INCLUDED WITH THE GUARDRAIL TOTALS ON THE GENERAL SUMMARY SHEET. LOCATIONS OF ANY CURVED RAIL ARE IDENTIFIED IN THE PLAN SHEETS.

**ITEM 202 - GUARDRAIL REMOVED, AS PER PLAN:**

**ITEM 202 - ANCHOR ASSEMBLY REMOVED, TYPE B, AS PER PLAN:**

**ITEM 202 - BRIDGE TERMINAL ASSEMBLY REMOVED, TYPE 1, AS PER PLAN:**

IN ADDITION TO THE REQUIREMENTS OF ITEM 202, REMOVAL OF SPECIFIED GUARDRAIL ITEMS SHALL INCLUDE BUT NOT BE LIMITED TO ANY ATTACHED POSTS, SIGNS AND DELINEATORS (NOT OTHERWISE SPECIFIED). THIS REMOVAL WILL INCLUDE ALL POSTS, ANCHORS AND HARDWARE UNDER GROUND.

THE CONTRACTOR SHALL REMOVE ALL CONCRETE FOUNDATIONS COMPLETELY AT ALL LOCATIONS.

EXISTING TYPE T ANCHOR ASSEMBLIES (NOT IDENTIFIED IN THE PLANS) ENCOUNTERED WITHIN REMOVAL LIMITS SHALL BE REMOVED UNDER THE QUANTITIES PROVIDED FOR ITEM 202 - GUARDRAIL REMOVED, AS PER PLAN, AND BE PAID FOR BY "FEET".

ALL HOLES AND VOIDS REMAINING AFTER REMOVAL OF GUARDRAIL POSTS AND FOUNDATIONS SHALL BE FILLED WITH GRANULAR MATERIAL CONFORMING TO CMS 203.02R. FILL MATERIAL CONTAINING SOD SHALL NOT BE USED. ALL FILL MATERIAL SHALL BE APPROVED BY THE ENGINEER. MATERIAL PLACED IN HOLES SHALL BE THOROUGHLY COMPACTED AND LEVELED OFF AS DIRECTED BY THE ENGINEER. PAYMENT FOR THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE APPLICABLE GUARDRAIL REMOVAL ITEM.

NO HAZARD SHALL BE LEFT UNPROTECTED EXCEPT FOR THE ACTUAL TIME NECESSARY TO REMOVE THE EXISTING GUARDRAIL, PREPARE THE SITE, AND INSTALL NEW GUARDRAIL IN A CONTINUOUS OPERATION. GUARDRAIL DESIGNATED FOR REMOVAL SHALL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF.

**ITEM 606 - GUARDRAIL, MISC.: ALTERNATIVE GUARDRAIL PLACEMENT:**

THIS ITEM SHALL BE USED WHEN THE CONTRACTOR IS REQUIRED TO USE AN ALTERNATE METHOD TO SET POSTS TO PREVENT DAMAGE TO AN UNDERGROUND OBSTACLE, SUCH AS A UTILITY. THE USE OF THIS ITEM WILL BE AS DEEMED NECESSARY BY THE ENGINEER. THIS ITEM SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIAL NEEDED TO SET AND BACKFILL POSTS WHILE MEETING THE REQUIREMENTS OF THE APPLICABLE GUARDRAIL ITEM BEING PERFORMED. APPLICABLE GUARDRAIL ITEMS INCLUDE BUT ARE NOT LIMITED TO SETTING POSTS (AND SLEEVES) FOR TYPE 5, TYPE MGS, ANCHOR ASSEMBLIES, AND BRIDGE TERMINAL ASSEMBLIES. PAYMENT SHALL BE AT THE UNIT BID PRICE OF EACH AND SHALL BE PAID FOR IN ADDITION TO THE APPLICABLE GUARDRAIL PLACEMENT ITEM LISTED ABOVE.

LOC	COUNTY	ROUTE	BEG SLM	END SLM	QUANTITY	UNIT
1	FAY	35	4.52	7.51	50	FT
2	FAY	35	11.97	17.45	150	FT
TOTAL					200	FT

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN PROVIDED:  
ITEM 606 - GUARDRAIL, MISC.: ALTERNATIVE GUARDRAIL PLACEMENT:  
= 200 FT

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GENERAL NOTES

FAY-35-4.52

**GRADING AND EROSION CONTROL:**

AREAS DISTURBED BY GUARDRAIL ACTIVITIES AND AREAS WHERE EMBANKMENT HAVE BEEN PLACED SHALL BE REPAIRED WITH THE FOLLOWING QUANTITIES, AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN PROVIDED:

ITEM	QUANTITY	UNIT	DESCRIPTION
659	622	CU YD	TOPSOIL
659	5,550	SQ YD	SEEDING AND MULCHING
659	278	SQ YD	REPAIR SEEDING AND MULCHING
659	278	SQ YD	INTER-SEEDING
659	0.75	TON	COMMERCIAL FERTILIZER
659	1.15	ACRE	LIME
659	30	M.GAL	WATER

**MANHOLES AND OTHER CASTING STRUCTURES:**

THE CASTING TOPS OF MANHOLES, VALVE BOXES, AND OTHER STRUCTURES OWNED BY PUBLIC SERVICE CORPORATIONS MAY BE ADJUSTED TO GRADE BY THEIR RESPECTIVE OWNERS OR GIVE AUTHORIZATION TO ODOT TO ADJUST AS PART OF THIS CONTRACT. THIS WORK NEEDS TO BE COMPLETED PRIOR TO THE CONSTRUCTION OF THE SURFACE COURSE. THE CONTRACTOR SHALL NOTIFY SUCH PUBLIC SERVICE CORPORATIONS A MINIMUM OF 7 CALENDAR DAYS IN ADVANCE OF WORK OPERATIONS SO THAT WORK MAY BE PROPERLY SCHEDULED.

THE CASTING TOPS OF MANHOLES, VALVE BOXES, AND OTHER STRUCTURES REQUIRING ADJUSTMENT THAT ARE OWNED BY PRIVATE UTILITIES NEED TO BE ADJUSTED TO GRADE BY THEIR RESPECTIVE OWNERS. THE ODOT CONTRACTOR SHALL NOTIFY THE PRIVATE OWNER A MINIMUM OF 7 CALENDAR DAYS IN ADVANCE OF WORK OPERATIONS SO THE WORK MAY BE PROPERLY SCHEDULED.

IF ADJUSTMENTS HAVE NOT BEEN COMPLETED 14 CALENDAR DAYS AFTER NOTIFICATION, THE ODOT CONTRACTOR WILL NOTIFY THE ODOT PROJECT ENGINEER AND PROVIDE SPECIFIC STATION LOCATIONS AND OWNER INFORMATION. THE ODOT PROJECT ENGINEER WILL WORK WITH THE DISTRICT UTILITY COORDINATOR TO ISSUE AND OBSTRUCTION REMOVAL NOTICE WITHIN 5 DAYS OF RECEIPT WHICH WILL INFORM THE PRIVATE UTILITY TO ADJUST THE STRUCTURES AS NECESSARY OR ODOT WILL AUTHORIZE THE ODOT CONTRACTOR TO ADJUST AS NEEDED AND BILL THE OWNER OF THE FACILITY FOR THE ADJUSTMENT TO THE STRUCTURE.

SHOULD THE CONTRACTOR FAIL TO NOTIFY PUBLIC SERVICE CORPORATIONS OR PRIVATE UTILITIES OF EXISTING MANHOLES, VALVE BOXES, AND OTHER STRUCTURES THAT REQUIRE ADJUSTMENTS TO GRADE, AND COVER THESE WITH THE PROPOSED ASPHALT TREATMENT, THE CONTRACTOR WILL BE REQUIRED TO UNCOVER THE MANHOLES, VALVE BOXES, AND OTHER STRUCTURES AT THEIR OWN EXPENSE SO THAT THE NECESSARY ADJUSTMENTS CAN BE MADE. THE METHOD OF REMOVAL AND REPAIR OF THE ASPHALT SHALL MEET ALL REQUIREMENTS OF THE ODOT ENGINEER AND SHALL BE AT THE CONTRACTORS EXPENSE.

THESE ITEMS PROVIDED BELOW ARE CONTINGENCY QUANTITIES TO BE USED AS DIRECTED BY THE PROJECT ENGINEER AT VARIOUS LOCATIONS. THESE ITEMS SHALL INCLUDE THE COST OF ALL MATERIAL, LABOR, EQUIPMENT, AND HARDWARE NECESSARY TO ADJUST CASTINGS TO GRADE TO THE PROPOSED ASPHALT ELEVATION AS DIRECTED. THE FOLLOWING QUANTITIES HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 611 - CATCH BASIN ADJUSTED TO GRADE = 1 EACH

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN:**

ALL AREAS TO BE REPAIRED SHALL BE LOCATED BY THE ENGINEER AND THE WORK PERFORMED PRIOR TO THE PLACEMENT OF THE SURFACE COURSE. THE REPAIR AREAS SHALL BE OF VARYING LENGTH AND HAVE A MINIMUM WIDTH OF 2 FEET AND SHALL CONSIST OF REMOVING 3" OF PAVEMENT AND PLACING 3" OF ITEM 301 - ASPHALT CONCRETE BASE, PG64-22. PARTIAL DEPTH PAVEMENT REPAIRS SHALL BE STARTED AND COMPLETED IN THE SAME WORKING DAY.

IN ADDITION TO THE SPECIFIC LOCATIONS GIVEN ON SHEET 12/30, THE FOLLOWING CONTINGENCY QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN = 270 SY

**ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE**

THE CONTRACTOR SHALL BE TOTALLY RESPONSIBLE FOR ANY AND ALL DAMAGE TO THE CONTRACTORS EQUIPMENT THAT MAY RESULT FROM THE PLANING OPERATION, INCLUDING DAMAGE CAUSED BY CASTINGS AND LOOP DETECTORS. THE DEPTH OF PLANING CLOSE TO THE CASTINGS SHALL BE AS DIRECTED; TO ACHIEVE A SMOOTH RIDING FINISHED PAVEMENT. GREAT CARE SHALL BE TAKEN TO PREVENT THE REMOVAL OF THE EXISTING PAVEMENT CROSS SLOPE (CROWN) DURING THE PLANING OPERATIONS.

THE CONTRACTOR SHALL LIMIT THE PLANING OPERATION TO ONE LANE AT A TIME AS TO ENSURE THAT THE PROPOSED SURFACE COURSE IS BUTTING UP TO EITHER PROPOSED OR EXISTING ASPHALT.

ALL PLANED PAVEMENT SHALL BE PLANED TO A DEPTH OF 1 1/2 INCHES AND RESURFACED WITH 1 1/2 INCHES OF THE ASPHALT CONCRETE SURFACE COURSE WITHIN THE SAME WORK PERIOD. FAILURE TO MEET THIS REQUIREMENT WILL SUBJECT THE CONTRACTOR TO A DISINCENTIVE OF \$900/DAY FOR EACH DAY THE PLANED SURFACE IS NOT RESURFACED.

**ANTI-SEGREGATION EQUIPMENT:**

PROVIDE ANTI-SEGREGATION EQUIPMENT FOR ALL COURSES OF UNIFORM THICKNESS IN ACCORDANCE WITH CMS 401.12.

**ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A, (446), AS PER PLAN:**

ITEM 442 SHALL REQUIRE A PG76-22M BINDER WHEN PLACED ON RAMPS.

**ITEM 617 - WATER:**

THIS ITEM IS A CONTINGENCY QUANTITY AND SHALL BE USED WHERE AND AS DIRECTED BY THE ENGINEER.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.  
ITEM 617 - WATER = 2 MGAL

**ITEM 644 - PAVEMENT MARKING:**

WITH THE EXCEPTION OF THE PROPOSED PASSING ZONES, IT IS THE INTENT OF THE PROPOSED PAVEMENT MARKINGS TO BE THE SAME AS EXISTING. ANY DEVIATION FROM EXISTING WILL BE IDENTIFIED WITHIN THIS PLAN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE AND SHAPE OF THESE EXISTING PAVEMENT MARKINGS BEFORE THE SURFACE PREP OBLITERATES THEM. ANY PAVEMENT MARKING WHICH IS PLACED AT THE WRONG LOCATION SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.:

LOC	COUNTY	ROUTE	EDGE LINE	LANE LINE
			WIDTH	WIDTH
1	FAY	35	6"	6"
2	FAY	35	6"	6"

**ITEM 632 - SIGNALIZATION, MISC.; RWIS SENSOR, VX21-2:**

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NEW AND COMPLETE RWIS SENSOR MEETING THE REQUIREMENTS AS DESCRIBED BELOW.

THE PROPOSED SENSOR UNIT SHALL BE M.H. CORBIN, INC., TYPE VX21 (SAME AS EXISTING). INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS AND MANUFACTURER'S INSTALLATION GUIDELINES. THE REMOVAL AND DISPOSAL OF THE EXISTING SENSOR SHALL BE INCIDENTAL TO THE COST OF THIS ITEM.

THE CONTRACTOR WILL CONTACT THE SENSOR MANUFACTURER'S REPRESENTATIVE, WHO WILL BE PRESENT WHILE THE PROPOSED SENSOR IS BEING INSTALLED.

SENSOR MANUFACTURER'S REPRESENTATIVE  
M.H. CORBIN, INC.  
9042 HERITAGE DRIVE  
PLAIN CITY, OH 43064  
PHONE: 614-592-7430

THE EXISTING SENSOR WILL BE REMOVED WITH THE PLANING OPERATION. THE SENSOR IS LOCATED ON US-35 AT THE FOLLOWING APPROXIMATE LOCATIONS:  
**FAY-35-14.19, AT US-62, EAST BOUND DRIVING LANE**

THE PROPOSED CANISTER WILL BE INSTALLED AFTER THE FINAL SURFACE COURSE USING THE PROPER CANISTER INSTALLATION TOOLS PER THE MANUFACTURERS' INSTRUCTIONS AND MANUFACTURER'S INSTALLATION GUIDELINES. IT IS THE INTENT FOR THE PROPOSED SENSOR TO BE PLACED AT THE SAME LOCATION AS THE EXISTING UNLESS OTHERWISE SPECIFIED BY THE MANUFACTURER'S REPRESENTATIVE.

THE ODOT DISTRICT 6 HIGHWAY MANAGEMENT [DAN WISE, /40-833-8023] SHALL BE NOTIFIED WHEN THE SENSOR IS REMOVED FROM THE PAVEMENT AND WHEN THE PROPOSED INSTALLATION IS COMPLETE. THE DISTRICT WILL MONITOR THE SENSORS PERFORMANCE FOR A MINIMUM OF FIVE WORKING DAYS TO VERIFY PROPER OPERATION. IF THE SENSORS DO NOT PERFORM PROPERLY WITHIN THIS TEST PERIOD, THE CONTRACTOR SHALL VERIFY THAT THE INSTALLATION IS CORRECT. IF A SENSOR FAILS AFTER IT IS REMOVED FROM THE PAVEMENT, THE CONTRACTOR SHALL REPLACE THE FAILED SENSOR WITH A NEW WIRELESS PAVEMENT/TRAFFIC SENSOR, MODEL VX21-2.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID OF ITEM 632 - RWIS SENSOR, VX21-2, EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO REMOVE AND INSTALL A COMPLETE AND FUNCTIONING RWIS SENSOR, VX21-2.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND CARRIED TO THE GENERAL SUMMARY:

ITEM 632- SIGNALIZATION. MISC.; RWIS SENSOR, VX21-2 = 1 EACH

**ITEM 623 - CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN:**

THE ITEM SHALL CONSIST OF STATION USING 3 FT LATH STAKES OR PAINT MARKINGS. THE STAKES OR PAINT MARKINGS SHALL BE SPACED EVERY 200' FOR THE ENTIRE LENGTH. PLACEMENT OF THE STAKES OR PAINT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY DAMAGED, MISSING STAKES, OR PAINT MARKINGS. PAINT MARKINGS SHALL BE PLACED ON CURBS AND USED IN AREAS WERE THE PLACEMENT OF STAKES IS NOT POSSIBLE AND APPROVED BY THE PROJECT ENGINEER. THIS ITEM HAS ALSO BEEN PROVIDED FOR THE CONTRACTOR TO MONITOR AND VERIFY ALL OVERHEAD CLEARANCE HEIGHT AT OVERHEAD STRUCTURES HAVE BEEN MAINTAINED.

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GENERAL NOTES

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**GENERAL:**

ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CURRENT EDITION). COPIES ARE AVAILABLE FROM:

THE OHIO DEPARTMENT OF TRANSPORTATION  
BUREAU OF TRAFFIC,  
1980 WEST BROAD STREET  
COLUMBUS, OHIO 43223.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

ALL PERMANENT TRAFFIC CONTROLS NOT IN CONFLICT WITH THE TEMPORARY TRAFFIC CONTROLS SHALL BE MAINTAINED THROUGHOUT THIS PROJECT BY THE CONTRACTOR. PERMANENT TRAFFIC CONTROLS MAY BE TEMPORARILY RELOCATED BY THE ENGINEER. THE CONTRACTOR SHALL ASSUME ALL LIABILITY FOR MISSING, DAMAGED, AND PROPERLY PLACED SIGNS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

**WORK SITE LIGHTING**

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR, AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

**MAINTENANCE OF TRAFFIC FOR MARKING PAVEMENT REPAIRS**

PROVIDE LANE CLOSURES AS PER THE MAINTENANCE OF TRAFFIC NOTES IN THESE PLANS A MINIMUM OF 24 HOURS PRIOR TO PERFORMING PAVEMENT REPAIRS TO ALLOW THE ENGINEER TO IDENTIFY AND MARK THE AREAS OF THE PAVEMENT IN NEED OF REPAIRS.

PAYMENT FOR ALL LABOR, EQUIPMENT, LAW ENFORCEMENT OFFICERS AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

**RIGHT OF WAY PERMITS:**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE RIGHT OF WAY USE PERMITS TO INSTALL MAINTENANCE OF TRAFFIC SIGNING.

**LANES OPEN DURING HOLIDAYS AND SPECIAL EVENTS:**

NO WORK SHALL BE PERFORMED AND THE SAME NUMBER OF LANES AS WERE AVAILABLE AT THE START OF THE PROJECT SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

HOLIDAYS

CHRISTMAS                      FOURTH OF JULY  
NEW YEARS                      LABOR DAY  
MEMORIAL DAY                  THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00 NOON FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00 NOON FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00 NOON MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00 NOON TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00 NOON WEDNESDAY THROUGH 6:00 AM FRIDAY
THANKSGIVING	5:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00 NOON THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00 NOON FRIDAY THROUGH 6:00 AM MONDAY

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA WIDE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN ACCORDANCE WITH AMOUNTS SPECIFIED IN CMS TABLE 108.07-1.

**USE OF STANDARD DRAWINGS:**

FOR THE PURPOSE OF THIS PROJECT, "MOVING OPERATION" SHALL BE LIMITED TO PAVEMENT MARKING STRIPING. IT MAY BE NECESSARY TO EXTEND THE ADVANCE WARNING AND BUFFER ZONES BEYOND THE MINIMUM DISTANCES SHOWN ON THE STANDARD DRAWINGS. THIS MAY BE DUE TO HORIZONTAL ALIGNMENT, VERTICAL ALIGNMENT, RAMP LOCATIONS, OR OTHER SIGHT OBSTRUCTIONS. LOCATIONS OF THE TAPER ZONES MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER, BUT TAPER LENGTHS MUST MEET THE MINIMUM STANDARDS. TAPERS SHOULD BE PLACED IN TANGENT SECTIONS WHENEVER POSSIBLE. ADDITIONAL YIELD SIGNS MAY BE REQUIRED FOR RAMP WITHIN 1,000 FEET OF A WORK ZONE. PAYMENT SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 MAINTAINING TRAFFIC.

FOR ANY MULTILANE HIGHWAY, DEVICE SPACING SHALL BE A MAXIMUM OF 40' (FEET) CENTER ON CENTER IN THE TAPERS AND 80' (FEET) CENTER ON CENTER IN THE TANGENT SECTIONS.

**TRUCK MOUNTED ATTENUATOR (TMA)**

WHEN WORKING IN A CLOSED LANE OR SHOULDER ON A MULTILANE HIGHWAY WITHOUT TEMPORARY OR PERMANENT TRAFFIC BARRIERS SEPARATING THE WORK AREA FROM THE TRAVELED LANES, A TRUCK MOUNTED ATTENUATOR (TMA) SHALL BE PROVIDED TO PROTECT EACH WORK AREA IN ACCORDANCE WITH STANDARD DRAWINGS MT-95.30, MT-95.31, MT-95.32 OR OMUTCD TYPICAL APPLICATION (TA) 4 AND TA-6. THE TMA SHALL BE PLACED IN SUCH A WAY TO ADEQUATELY PROTECT THE WORKERS INSIDE THE WORK ZONE. THE TMA IS NOT INTENDED TO BE USED AS OR SUBSTITUTED FOR THE FLASHING ARROW PANEL AT THE BEGINNING OF THE MERGE TAPER. THE TMA SHALL MEET NCHRP REPORT 350 OR MASH TL-3 COMPLIANT AND SHOULD BE OPERATED AND LOCATED PER MANUFACTURER RECOMMENDATIONS AND AASHTO'S ROADSIDE DESIGN GUIDE. THE COST FOR PROVIDING THE TMA SHALL INCLUDE ALL MATERIAL, LABOR, EQUIPMENT, AND HARDWARE REPLACEMENT AND IS TO BE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

**USE OF WEIGHTED CHANNELIZER:**

THE WEIGHTED CHANNELIZER MAY BE USED IN ACCORDANCE WITH THIS SECTION. THE WEIGHTED CHANNELIZER SHALL BE PREDOMINANTLY ORANGE IN COLOR AND SHALL BE MADE OF LIGHTWEIGHT, FLEXIBLE, AND DEFORMABLE MATERIAL. THEY SHALL BE AT LEAST 42 INCHES IN HEIGHT WITH A WEIGHTED BASE. THEY MAY HAVE A HANDLE OR LIFTING DEVICE WHICH EXTENDS ABOVE THE 42" MINIMUM HEIGHT.

THE MARKINGS ON THE WEIGHTED CHANNELIZER SHALL BE HORIZONTAL, CIRCUMFERENTIAL, ALTERNATING ORANGE AND WHITE RETROREFLECTIVE STRIPES 6 INCHES WIDE. EACH WEIGHTED CHANNELIZER SHALL HAVE

A MINIMUM OF TWO ORANGE AND TWO WHITE STRIPES. ANY NON-RETROREFLECTIVE SPACES BETWEEN THE HORIZONTAL ORANGE AND WHITE STRIPES SHALL NOT EXCEED 2 INCHES WIDE. THE WEIGHTED CHANNELIZER SHALL HAVE A 4-INCH MINIMUM WIDTH, REGARDLESS OF ORIENTATION.

USE OF WEIGHTED CHANNELIZERS ON FREEWAYS AND MULTILANE HIGHWAYS SHALL BE LIMITED TO SHORT-TERM OPERATION FOR EITHER DAY OR NIGHT. UPON COMPLETION OF WORK, THE WEIGHTED CHANNELIZERS SHALL BE REMOVED. THE WEIGHTED CHANNELIZERS MAY AGAIN BE PLACED ON THE HIGHWAY WHEN THE WORK IS TO RESUME ON THE FOLLOWING DAY OR NIGHT. ANY LANE CLOSURE USING CHANNELIZATION DEVICES, EXPECTED TO REMAIN FOR MORE THAN TWELVE HOURS, SHALL REQUIRE THE USE OF DRUMS OR BARRIERS.

WHEN USED AT NIGHT, WEIGHTED CHANNELIZERS SHALL ONLY BE PLACED IN THE TANGENT AREA. THE TANGENT AREA IS DEFINED AS THE AREA AFTER THE TRANSITION TAPER WHERE THE WORK TAKES PLACE. DRUMS SHALL BE USED IN THE TRANSITION TAPERS FOR NIGHT OPERATIONS. MAXIMUM SPACING OF THE WEIGHTED CHANNELIZER SHALL BE 40 FEET AT NIGHT.

STEPS SHOULD BE TAKEN TO ENSURE THAT THE WEIGHTED CHANNELIZERS WILL NOT BE BLOWN OVER OR DISPLACED BY WIND OR MOVING TRAFFIC. BALLASTS SHOULD NOT PRESENT A HAZARD IF THE WEIGHTED CHANNELIZERS ARE INADVERTENTLY STRUCK, NOR SHOULD THEY AFFECT THE VISIBILITY OF THE WEIGHTED CHANNELIZERS. ALL BALLASTS USED SHOULD BE IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

**NOTIFICATION OF CONSTRUCTION INITIATION:**

AT LEAST FOURTEEN DAYS PRIOR TO ANY CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL ADVISE THE DISTRICT OFFICE OF COMMUNICATIONS VIA EMAIL AT [d06.pio@dot.ohio.gov](mailto:d06.pio@dot.ohio.gov) AND THE DISTRICT WORK ZONE TRAFFIC MANAGER VIA EMAIL AT [d06.mot@dot.ohio.gov](mailto:d06.mot@dot.ohio.gov) OF THE ANTICIPATED START DATE OF ANY CONSTRUCTION ACTIVITIES, INCLUDING BUT NOT LIMITED TO THE PLACING OF WORK ZONE SIGNS. THE NOTIFICATION SHALL ALSO INCLUDE THE PROJECT NUMBER, PID, NAME AND NUMBER OF THE CONTRACTOR, A POINT OF CONTACT AND THE ANTICIPATED IMPACT ON TRAFFIC. THE CONTRACTOR WILL IMMEDIATELY INFORM THE DISTRICT OFFICE OF COMMUNICATIONS AND THE DISTRICT WORK ZONE TRAFFIC MANAGER OF ANY DELAYS AND/OR CHANGES REGARDING THE CONSTRUCTION INITIATION DATE.

**PUBLIC OUTREACH AND NOTIFICATION:**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE DISTRICT 6 PUBLIC INFORMATION OFFICE VIA EMAIL AT [d06.pio@dot.ohio.gov](mailto:d06.pio@dot.ohio.gov) TO COORDINATE EFFORTS TO NOTIFY ADJACENT RESIDENTS AND BUSINESSES OF THE UPCOMING PAVING PROJECT. ADVANCE NOTIFICATION SHALL OCCUR NO LATER THAN FOURTEEN (14) DAYS PRIOR TO A NEW LOCATION. ALL NOTIFICATIONS SHALL BE MADE UTILIZING THE TEMPLATE PROVIDED BY THE DISTRICT 6 PUBLIC INFORMATION OFFICE.

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MAINTENANCE OF TRAFFIC NOTES

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**PERMITTED LANE CLOSURES:**

THE EXISTING NUMBER OF LANES IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES EXCEPT DURING PERIODS OF WORK AT WHICH TIME LANES MAY BE CLOSED IN ACCORDANCE WITH THE LANE VALUE CONTRACT TABLE FOR EACH LOCATION UNLESS OTHERWISE SHOWN IN THE PLANS.

ATLEAST ONE LANE OF TRAFFIC SHALL BE MAINTAINED (EACH DIRECTION) AT ALL TIMES.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITME 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

**LANE VALUE CONTRACT TABLE**

THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE AS DESIGNATED IN THE LANE VALUE CONTRACT TABLE FOR EACH UNIT OF TIME A LANE/SHOULDER/RAMP IS CLOSED BY THE CONTRACTOR'S ACTION WHILE NOT OTHERWISE PERMITTED BY THE UNAUTHORIZED LANE USE TABLE BELOW.

LANE VALUE CONTRACT TABLE					
Section (SLM)	Existing Number of Lanes per Direction	Lane closures are NOT permitted:			
		Lane Reduction	Mon to Fri	Sat	Sun
<b>FAY-35</b>					
FAY-435 (4.37) to SR 739 (17.23)	2	2 to 1	No Restriction	No Restriction	No Restriction
Shoulder closures are permitted any time except 5AM-9AM & 3PM-6PM Monday-Friday					

RAMP CLOSURE RESTRICTIONS					
US ROUTE 35 IN FAYETTE COUNTY					
Secondary Route: State Route 435			SLM along 35: 4.37		
Ramp Designation	Movement	No Closures Allowed		Detour Routes	
		Mon-Fri	Sat-Sun	Primary Detour Route	Secondary Detour Route
C	SR 435 to US-35 EB	5AM-10PM	6AM-10PM	SR-435 W to US-35 W to SR-72 S to US-35 E	Old US-35e to Palmer Rd W to US-35
D	US-35 WB to SR 435	5AM-10PM	6AM-10PM	US-35 W to SR-72 S to US-35E to SR-435	
SECONDARY ROUTE: PALMER RD (CR-11)			SLM ALONG 35: 11.86		
Ramp Designation	Movement	No Closures Allowed		Detour Routes	
		Mon-Fri	Sat-Sun	Primary Detour Route	Secondary Detour Route
A	US-35 WB to Palmer Rd (CR-11)	6AM-8PM	No Restriction	US-35 W to SR-435 W to US-35 E to Palmer Rd (Ramp AA)	
AA	US-35 EB to Palmer Rd (CR-11)	6AM-9AM & 3PM-6PM	No Restriction	US-35 E to US-22 E to US-35 W to Palmer Rd (Ramp A)	
B	Palmer Rd (CR-11) to US-35 EB	6AM-8PM	No Restriction	US-35 W to SR-435 W to US-35 E	
BB	Palmer Rd (CR-11) to US-35 WB	6AM-9AM & 3PM-6PM	No Restriction	US-35 E to US-22 E to US-35 W	
SECONDARY ROUTE: US-22/ SR-3/US-62			SLM ALONG 35: 13.79		
Ramp Designation	Movement	No Closures Allowed		Detour Routes	
		Mon-Fri	Sat-Sun	Primary Detour Route	Secondary Detour Route
A	US-22/ SR-3/US-62 to US-35 WB	6AM-7PM	8AM-8PM	US-35 E to SR-41 N to US-35 W	
B	US-35 WB to US-22/ SR-3/US-62	6AM-7PM	8AM-8PM	US-35 W to Palmer Rd W to US-35 E to Ramp C	
C	US-35 EB to US-22/ SR-3/US-62	6AM-7PM	8AM-8PM	US-35 E to SR-41 N to US-35 W to Ramp B	
D	US-22/ SR-3/US-62 to US-35 EB	6AM-7PM	8AM-8PM	US-35 W to Palmer Rd W to US-35 E	
SECONDARY ROUTE: STATE ROUTE 41			SLM ALONG 35: 15.37		
Ramp Designation	Movement	No Closures Allowed		Detour Routes	
		Mon-Fri	Sat-Sun	Primary Detour Route	Secondary Detour Route
A	SR-41 to US-35 WB	6AM-9AM & 3PM-6PM	8AM-8PM	US-35 E to SR-753 to US-35 W	
B	US-35 WB to SR-41	6AM-9AM & 3PM-6PM	8AM-8PM	US-35 W to US-22/SR-3/US-62 to US-35 E to SR-41 (Ramp C)	
C	US-35 EB to SR-41	6AM-9AM & 3PM-6PM	8AM-8PM	US-35 E to SR-753 to US-35 W to SR-41 (Ramp B)	
D	SR-41 to US-35 EB	6AM-9AM & 3PM-6PM	8AM-8PM	US-35 W to US-22/SR-3/US-62 to US-35 E	
SECONDARY ROUTE: STATE ROUTE 753			SLM ALONG 35: 17.23		
Ramp Designation	Movement	No Closures Allowed		Detour Routes	
		Mon-Fri	Sat-Sun	Primary Detour Route	Secondary Detour Route
A	SR-753 to US-35 WB	6AM-10PM	8AM-8PM	US-35 E to SR-138 N to US-35 W	
B	US-35 WB to SR-753	6AM-9AM & 3PM-6PM	8AM-8PM	US-35 W to SR-41 S to US-35 E to SR-753 (Ramp C)	
C	US-35 EB to SR-753	6AM-10PM	8AM-8PM	US-35 E to SR-138 N to US-35 W to SR-753 (Ramp B)	
D	SR-753 to US-35 EB	6AM-9AM & 3PM-6PM	8AM-8PM	US-35 W to SR-41 S to US-35 E	

**SHORT DURATION RAMP CLOSURES**

FOR THE PURPOSE OF PERFORMING THE REQUIRED WORK OR WHEN REQUIRED BY THE INTERSTATE ENTRANCE RAMP CLOSURE NOTE, RAMPS MAY BE CLOSED FOR SHORT DURATIONS AND DETOURED IN ACCORDANCE WITH THE RAMP CLOSURE TABLE IF APPROVED BY THE ENGINEER. RAMP CLOSURES ARE SUBJECT TO DISINCENTIVES.

FOR ALL SERVICE RAMP CLOSURES LASTING MORE THAN 12 HOURS BUT LESS THAN 60 HOURS AND/OR, FOR ALL SYSTEM RAMP CLOSURES LASTING MORE THAN 12 HOURS BUT LESS THAN 24 HOURS

THE CONTRACTOR SHALL PROVIDE THE FOLLOWING:

- A MINIMUM OF TWO PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) PLACED, AS DIRECTED BY THE ENGINEER, TO WARN DRIVERS OF THE CLOSURE AND TO PROVIDE THE DESIGNATED DETOUR ROUTE.
- POSITIVE GUIDANCE ALONG THE DETOUR ROUTE WITH DETOUR SIGNS (M4-9 SERIES) IN ACCORDANCE WITH THE DETOUR SIGNS NOTE.

FOR ALL RAMP CLOSURES LASTING LESS THAN 12 HOURS, THE CONTRACTOR SHALL PROVIDE THE FOLLOWING:

- A MINIMUM OF TWO PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) PLACED, AS DIRECTED BY THE ENGINEER, TO WARN DRIVERS OF THE CLOSURE AND TO PROVIDE THE DESIGNATED DETOUR ROUTE.

WHEN CLOSING ENTRANCE RAMPS, CORRESPONDING LEAD-IN LANES AND TURN LANES SHALL ALSO BE CLOSED.

IF A DESIGNATED DETOUR ROUTE IS NOT PROVIDED IN THE PLANS, TRAFFIC SHALL BE DIRECTED TO THE NEXT INTERCHANGE, IF AVAILABLE, TO TURN AROUND. IF THE USE OF THE NEXT INTERCHANGE IS NOT POSSIBLE, AN ALTERNATIVE DETOUR ROUTE SHALL BE PROVIDED BY THE ENGINEER.

SERVICE RAMP: INTERCHANGE RAMPS BETWEEN FREEWAYS (OR EXPRESSWAYS) AND NON-FREEWAYS (OR NONEXPRESSWAYS). THESE RAMPS PROVIDE ACCESS (CONNECTIONS) BETWEEN FREEWAYS/EXPRESSWAYS AND OTHER PRINCIPAL/MINOR ARTERIALS, COLLECTORS OR LOCAL ROADS.

SYSTEM RAMP: INTERCHANGE RAMPS (OR CONNECTORS) BETWEEN FREEWAYS (OR EXPRESSWAYS) AND FREEWAYS (OR EXPRESSWAYS).



**ITEM 614 - DETOUR SIGNING**

SIZE AND PLACEMENT OF DETOUR SIGNS (M4-9) SHOULD FOLLOW THE REQUIREMENTS OF THE OMUTCD SECTION 6F.03, SECTION 2A.11 AND TABLE 6F.01.

DETOUR SIGNING SHALL PROVIDE DRIVERS ADEQUATE TIME TO CLEARLY READ THE SIGNS AND MAKE THE PROPER DECISIONS AT EACH REQUIRED TURNING MOVEMENT. THE DESIGNATED DETOUR ROUTE SHALL BE SIGNED IN ACCORDANCE WITH THE REQUIREMENTS BELOW:

- APPROXIMATELY 1500 FEET PRIOR TO TIP OF THE PAINTED GORE AT AN INTERCHANGE WHEN EXITING A HIGH SPEED (45 MPH OR HIGHER) FACILITY.
- AT OR NEAR THE EXISTING SIGN IN THE GORE OF AN INTERCHANGE RAMP.
- AT OR NEAR THE FIRST EXISTING LANE ASSIGNMENT SIGN ON AN INTERCHANGE EXIT RAMP.
- AT OR NEAR THE EXISTING LANE ASSIGNMENT SIGN OR EXISTING ROUTE MARKER AT THE END OF AN EXIT RAMP.
- APPROXIMATELY 500 FEET PRIOR TO A REQUIRED TURN AT AN INTERSECTION NOT CONTROLLED BY A STOP SIGN (FOR 45 MPH OR HIGHER ONLY).
- AT OR NEAR THE EXISTING LANE ASSIGNMENT SIGN OR EXISTING ROUTE MARKER AT AN INTERSECTION.
- EVERY TWO MILES ALONG A TANGENT SECTION BETWEEN TURNING MOVEMENTS OUTSIDE A CITY.
- EVERY TWO BLOCKS ALONG A TANGENT SECTION BETWEEN TURNING MOVEMENTS WITHIN A CITY.
- AT ANY OTHER INTERSECTION OR DECISION POINT WHERE THE DETOUR ROUTE IS CONTRARY TO THE NORMAL, EXPECTED TURNING MANEUVER OR OTHERWISE UNCLEAR.

DETOUR SIGNS SHALL BE PLACED, WHEN POSSIBLE, NEXT TO BUT NOT BLOCKING EXISTING ROUTE MARKERS OR LANE ASSIGNMENT SIGNS. DETOUR SIGNS SHALL NOT OBSCURE OR BE OBSCURED BY OTHER EXISTING OR TEMPORARY SIGNS.

DETOUR SIGNS SHALL BE ERECTED AND/OR UNCOVERED PRIOR TO THE ROAD OR RAMP BEING CLOSED TO TRAFFIC BUT NO EARLIER THAN FOUR HOURS PRIOR TO THE CLOSURE. DETOUR SIGNS SHALL BE COVERED AND/OR REMOVED NO LATER THAN FOUR HOURS FOLLOWING THE ROAD OR RAMP RE-OPENING TO TRAFFIC.

PAYMENT FOR ACCEPTED QUANTITIES WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, PROPER SIGN PLACEMENT AND SIZING, TIMELY ERECTING AND/OR UNCOVERING OF SIGNS, MAINTAINING SIGNS, AND TIMELY COVERING AND/OR REMOVING SIGNS AND SUPPORTS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.  
 ITEM 614 - DETOUR SIGNING = LUMP SUM

**NOTIFICATION OF TRAFFIC RESTRICTIONS:**

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE BUT IS NOT LIMITED TO ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

Notification Time Frame Table			
Item	Duration of Closure	Notification due to District 6 Communications Office	Sign Displayed to Public
Ramp & Road Closures	>= 2 weeks	21 calendar days prior to closure	14 calendar days prior to closure
	> 12 hours & < 2 weeks	14 calendar days prior to closure	7 calendar days prior to closure
	< 12 hours	4 business days prior to closure	2 business days prior to closure
Lane Closures & Restrictions	> = 2 weeks	14 calendar days prior to closure	
	< 2 weeks	5 business days prior to closure	
Start of Construction & Traffic Pattern Changes	N/A	14 calendar days prior to implementation	

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME FRAME TABLE.

**DRUM REQUIREMENTS:**

IN ADDITION TO THE REQUIREMENTS OF THE PLANS, SPECIFICATION AND PROPOSAL, DRUMS FURNISHED BY THE CONTRACTOR SHALL BE NEW AND UNUSED AT THE TIME OF ARRIVAL ON THE PROJECT. ANY DRUMS BROUGHT ON THE PROJECT, WHICH HAVE PREVIOUSLY BEEN USED ELSEWHERE, WILL NOT BE ACCEPTED.

PAYMENT FOR DRUMS SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED.

**BRIDGE WORK**

PRIOR TO GENERAL SURFACING OF US-35, THE CONTRACTOR SHALL COMPLETE BRIDGE WORK SHOWN ON SHEET 61/63 AT SLM 71.21 (STRUCTURES #2400642 & #2400650). THIS SHALL AVOID ADDITIONAL DAMAGE TO THE PAVEMENT DUE TO THE VARIOUS REQUIRED PHASING WORK, AS SHOWN ON SHEETS 13-23, FROM DAMAGING FRESHLY PAVED ASPHALT.

**ITEM 614 - WORK ZONE MARKING SIGN, AS PER PLAN:**

THE FOLLOWING ESTIMATED QUANTITIES FOR "NO EDGE LINE" SIGNS (R4-1-18) HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT THE LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE MARKING SIGNS PER THE REQUIREMENTS OF MT-101.90, 614.04 AND 614.11.

LOC	ROUTE	R4-1-18
		NO EDGE LINES
1	FAY-35	8
2	FAY-35	12
SUBTOTAL		20

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614 - WORK ZONE MARKING SIGN, AS PER PLAN = 20 EACH

**ITEM 614 - WORK ZONE LANE LINE, CLASS III, 642 PAINT:**

WORK ZONE LANE LINE SHALL BE PLACED TO REFLECT THE PROPOSED LANE LINE AS DETERMINED FROM THE PROPOSED MARKINGS WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND TYPE OF WORK ZONE MARKINGS NEEDED MEETING THE REQUIREMENTS OF ITEM 614 BEFORE THE REMOVAL OR RESURFACING OBLITERATES THE EXISTING.

LOC	ROUTE	BEGIN	END	QUANTITY	UNIT
1	FAY-35	4.52	7.51	2.99	MI
2	FAY-35	11.97	17.45	5.48	MI
				8.47	MI

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614 - WORK ZONE LANE LINE, CLASS III, 642 PAINT = 8.47 X 2 (EB & WB DIRECTIONS) = 16.94 MI

WORK ZONE PAVEMENT MARKINGS ARE NOT TO BE SUBSTITUTED FOR PERMANENT PAVEMENT MARKINGS.

**ITEM 614 - WORK ZONE STOP LINE, CLASS III, 642 PAINT:**

WORK ZONE STOP LINE SHALL BE PLACED TO REFLECT THE EXISTING STOP LINE AS DETERMINED FROM THE EXISTING MARKINGS WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND TYPE OF WORK ZONE MARKINGS NEEDED MEETING THE REQUIREMENTS OF ITEM 614 BEFORE THE REMOVAL OR RESURFACING OBLITERATES THE EXISTING.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

- RAMP C (SR-435 TO US-35 EB) = 40 FT
- RAMP B (US-35 WB TO SR-3, US-22, AND US-62) = 55 FT
- RAMP C (US-35 EB TO SR-3, US-22, AND US-62) = 45 FT
- RAMP B (US-35 WB TO SR-41) = 45 FT
- RAMP C (US-35 EB TO SR-41) = 45 FT
- RAMP B (US-35 WB TO SR-753) = 30 FT
- RAMP C (US-35 EB TO SR-753) = 45 FT

TOTAL = 305 FT

**DELINEATION OF PORTABLE AND PERMANENT BARRIER:**

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL AND ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70. WHEN THE PB CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

INCREASED BARRIER DELINEATION, AS SPECIFIED HEREIN, SHALL BE INSTALLED ON ALL PB AND CONCRETE PERMANENT BARRIER LOCATED WITHIN 5 FEET OF THE EDGE OF THE TRAVELED LANE.

THE INCREASED BARRIER DELINEATION SHALL CONSIST OF THE TRIPLE STACKING OF WORK ZONE BARRIER REFLECTORS.

TRIPLE-STACKED BARRIER REFLECTORS SHALL CONSIST OF ALIGNING THREE BARRIER REFLECTORS VERTICALLY, AT LOCATIONS WHERE A SINGLE BARRIER REFLECTOR WOULD BE OTHERWISE ATTACHED. THERE SHALL BE NO OPEN SPACE BETWEEN THE ADJACENT BARRIER REFLECTORS. THE TRIPLE-STACKED BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THEY SHALL BE SPACED AND ALIGNED PER TRAFFIC SCD MT-101.70.

IN ADDITION TO THE QUANTITIES PROVIDED ON SHEET 12 THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND SHALL BE USED AT THE ENGINEERS DISCRETION, THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614, BARRIER REFLECTOR, TYPE 1 (ONE-WAY) = 75 EACH  
ITEM 614, OBJECT MARKER, 1-WAY = 25 EACH

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE ABOVE ITEMS.

**WRECKER AND EMERGENCY VEHICLE ACCESS:**

THE CONTRACTOR SHALL MAKE PROVISIONS TO ASSIST IN THE ACCESS OF WRECKERS AND EMERGENCY VEHICLES THROUGHOUT THE WORK ZONE. THIS MAY INCLUDE, BUT NOT LIMITED TO, PROVIDING FLAGGERS OR REMOVING SECTIONS OF BARRIER TO ALLOW EMERGENCY VEHICLES AND WRECKER TO MOVE THROUGH PORTIONS OF THE WORK ZONE TO REACH ACCIDENTS AND/OR BREAKDOWNS. THE INTENT IS TO MINIMIZE EXTENDED DELAYS TO THE TRAVELLING PUBLIC AND TO PROVIDE QUICKER RESPONSE TIMES FOR WRECKERS AND EMERGENCY VEHICLES. ALL AGENTS AND EMPLOYEES OF THE CONTRACTOR SHALL BE MADE AWARE OF THIS PROVISION BEFORE WORK BEGINS.

ALL ACTIVITIES ASSOCIATED WITH ACCOMMODATING WRECKER SERVICES AND EMERGENCY VEHICLE ACCESS THROUGHOUT THE WORK ZONE SHALL BE COORDINATED BY THE WORKSITE TRAFFIC SUPERVISOR AND THE ENGINEER.

PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

**RAISED PAVEMENT MARKER REFLECTORS:**

THE CONTRACTOR SHALL REMOVE EXISTING RAISED PAVEMENT MARKER REFLECTORS WHEN THE REFLECTORS CONFLICT WITH TRAFFIC PATTERNS WITHIN THE WORK ZONE. ONCE CONFLICTING MARKINGS ARE REMOVED ALL REMOVED REFLECTORS SHALL BE REINSTALLED.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

**WORK ZONE IMPACT ATTENUATOR:**

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARD'S WEB PAGE FOR ROADWAY STANDARDS APPROVED PRODUCTS.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT. THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

**ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS**

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

- DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.
- DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

**ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONT.)**

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT.

THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 100 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

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MAINTENANCE OF TRAFFIC NOTES

FAY-35-4.52

SEE SHEET	SLM	REFERENCE NUMBER	PHASE	LOCATION		614	614	614	614	614		614	614	622			
				FROM	TO	WORKZONE IMPACT ATTENUATOR	BARRIER REFLECTOR, TYPE I, ONE-WAY	OBJECT MARKER, ONE-WAY	WORK ZONE LANE LINE, CLASS III, 4", 642 PAINT	WORK ZONE EDGE LINE, CLASS III, 642 PAINT		WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT	WORK ZONE DOTTED LINE, CLASS III, 6", 642 PAINT	PORTABLE BARRIER, 32"			
										EACH	EACH				EACH	MILES	MILE
									WHITE	YELLOW							
13-14	FAY-35-17.22 EASTBOUND	WZEL-1	PHASE 1	54+98	64+58					0.18	0.18						
		WZEL-2		64+58	82+13				0.33	0.33							
		WZEL-3		82+13	89+05				0.13	0.13							
		WZEL-4		89+05	102+08				0.25	0.25							
		WZDL-1		54+98	64+58							960					
		WZDL-2		82+13	88+38							554					
		WZCH-1		88+38	89+73						273						
		PB-1		97+08	102+08	1	30	9					470				
15-17	FAY-35-17.22 EASTBOUND	WZEL-5	PHASE 2	54+98	64+58					0.18							
		WZEL-6		64+58	91+45				0.51								
		WZEL-7		91+45	95+65				0.08								
		WZEL-8		91+45	97+06				0.11								
		WZEL-9		95+65	102+08					0.12							
		WZEL-10		97+06	102+08				0.10								
		WZEL-11		102+08	107+68				0.11								
		WZDL-3		54+98	64+58							960					
PB-2			1	30	9					470							
18-20	FAY-35-17.22 WESTBOUND	WZEL-1	PHASE 1	99+90	111+87				0.23								
		WZEL-2		111+87	119+69				0.15								
		WZEL-3		119+69	120+69				0.02								
		WZEL-4		120+69	130+49				0.19								
		WZDL-1		113+72	119+69							597					
		WZDL-2		120+69	130+49							980					
		WZCH-1		111+87	113+72						371						
		PB-1		99+90	105+10	1	30	10					490				
21-23	FAY-35-17.22 WESTBOUND	WZEL-5	PHASE 2	95+70	110+18				0.19								
		WZEL-6		99+90	110+18				0.11								
		WZEL-7		110+18	129+78				0.19								
		WZEL-8		129+78	139+38				0.04								
		WZDL-3		129+78	139+38							823					
		PB-2		99+90	105+10	1	30	10					490				
CONTINGENCY								1.18	0.66								
<b>TOTALS CARRIED TO GENERAL SUMMARY</b>								4	120	38	1.18	4.77	2.65	2.12	644	4874	1920

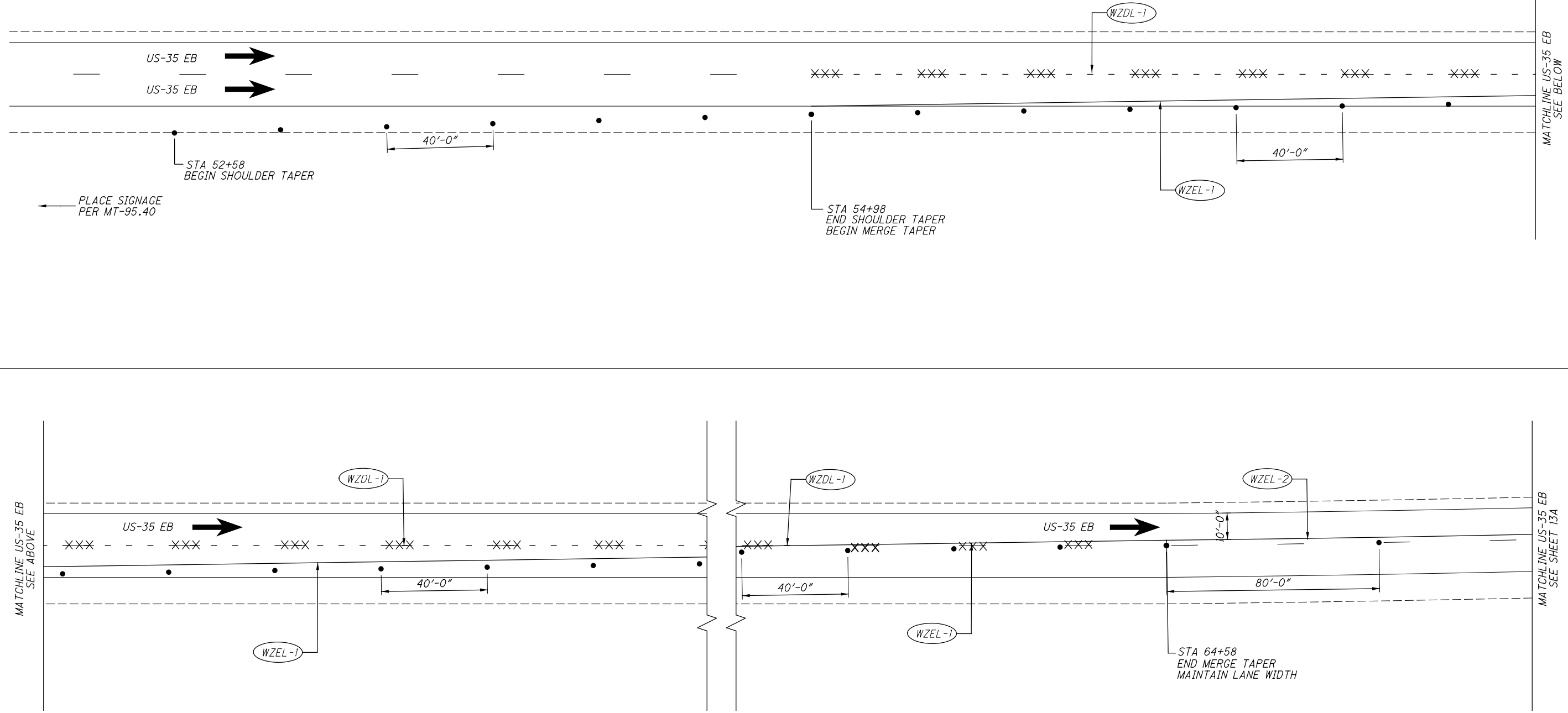
NOTE:  
 A CONTINGENCY QUANTITY HAS BEEN INCLUDED AND SHALL BE USED AT THE DIRECTION OF THE ENGINEER TO BRING THE TRAVEL LANE CONFIGURATION BACK INTO ORIGINAL ALIGNMENT.

**MAINTENANCE OF TRAFFIC SUBSUMMARY**

**FAY - 35 - 4.52**

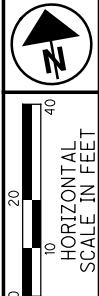


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**NOTE:**  
 1. ALL STA SHOWN ON SHEETS 13-14 ARE FOR DISTANCE REPRESENTATION ONLY.  
 2. 100+00 DESIGNATES BEGINNING OF BRIDGE WORK.  
 3. DESIGN SPEED 70MPH.  
 4. QUANTITIES SHOWN ON SHEET 12  
 5. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.

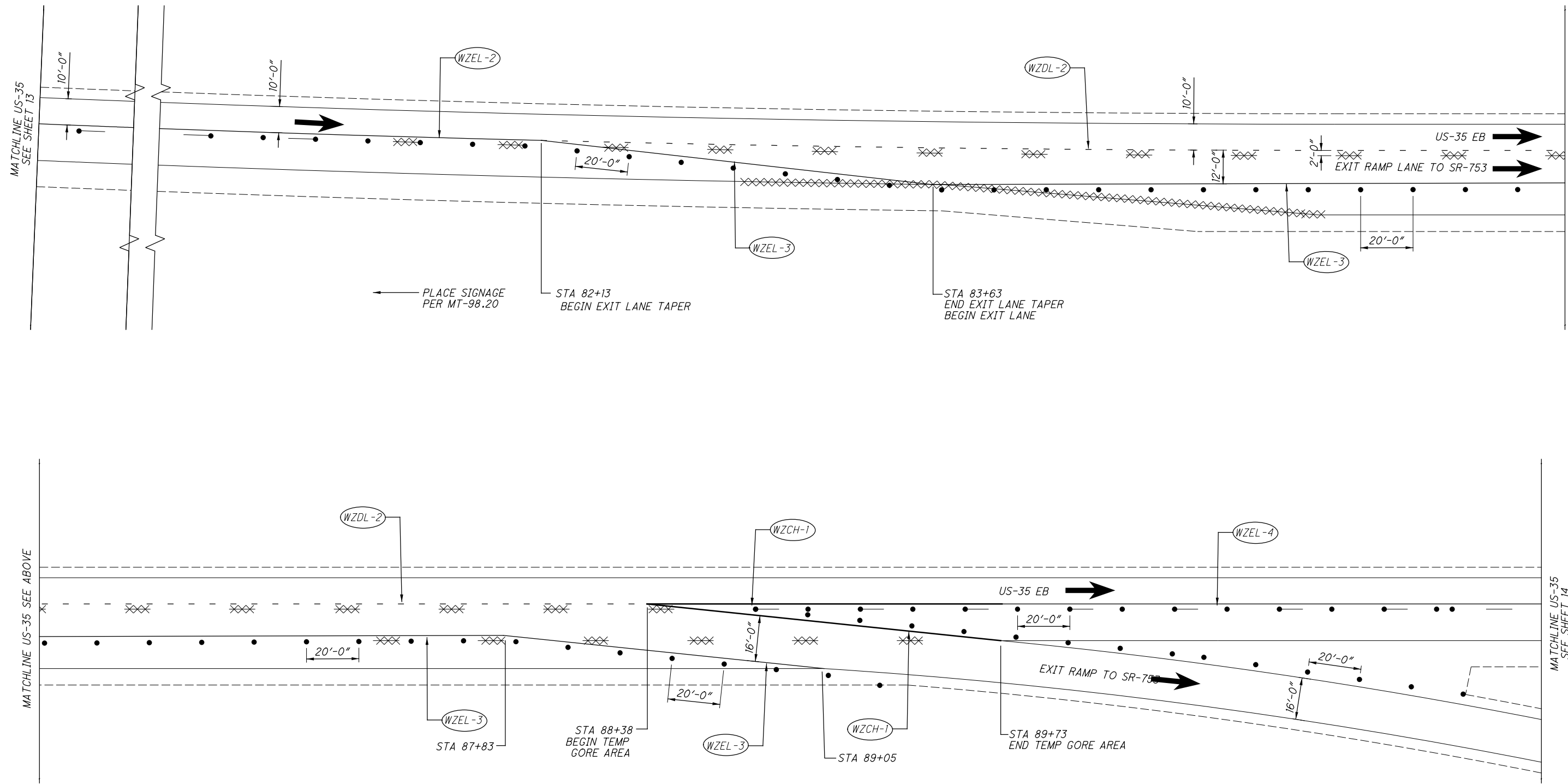
LEGEND	
	= WORK ZONE
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED



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**MAINTENANCE OF TRAFFIC PLAN VIEWS  
 FAY-35-17.22 - EASTBOUND PHASE 1**

**FAY-35-4.52**



**NOTE:**  
 1. ALL STA SHOWN ON SHEETS 13-14 ARE FOR DISTANCE REPRESENTATION ONLY.  
 2. 100+00 DESIGNATES BEGINNING OF BRIDGE WORK.  
 3. DESIGN SPEED 70MPH.  
 4. QUANTITIES SHOWN ON SHEET 12.  
 5. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.

LEGEND	
	= WORK ZONE
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

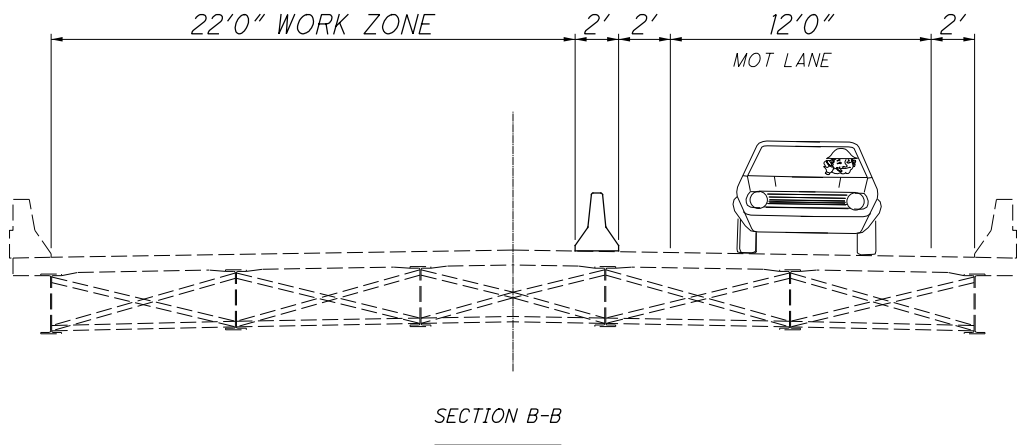
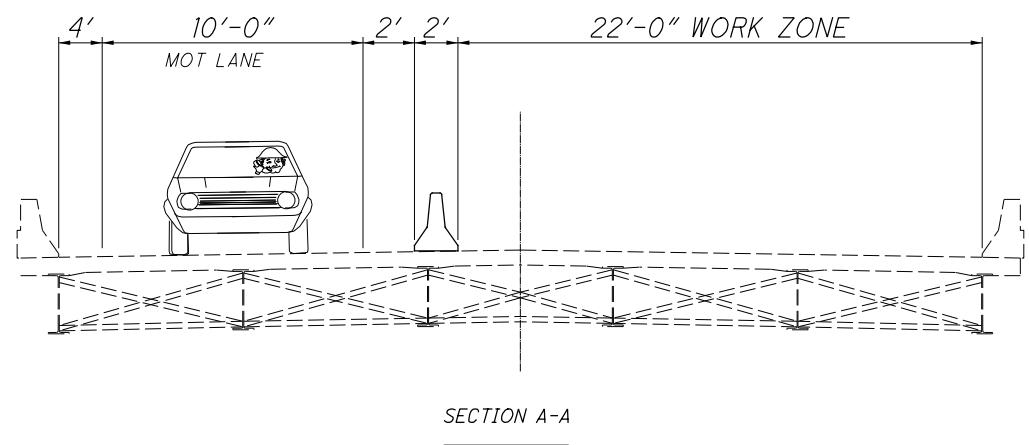
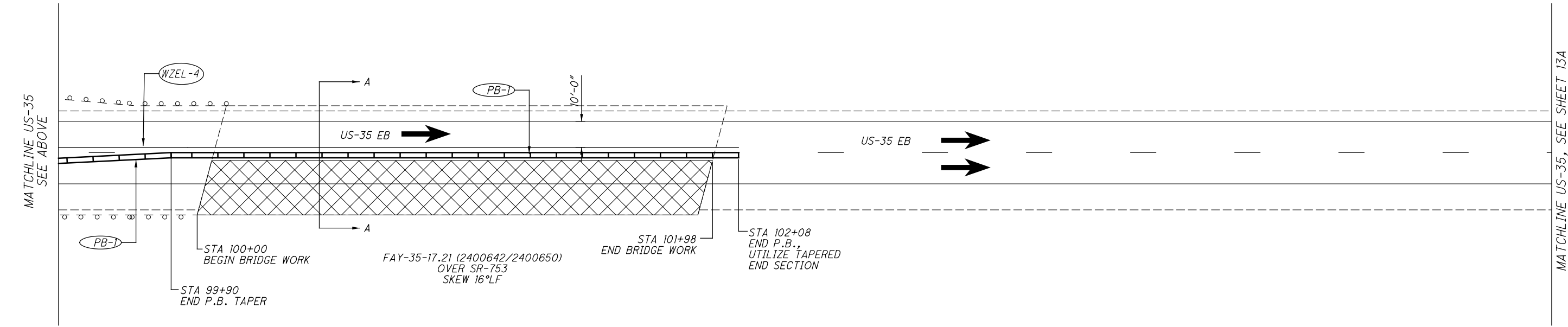
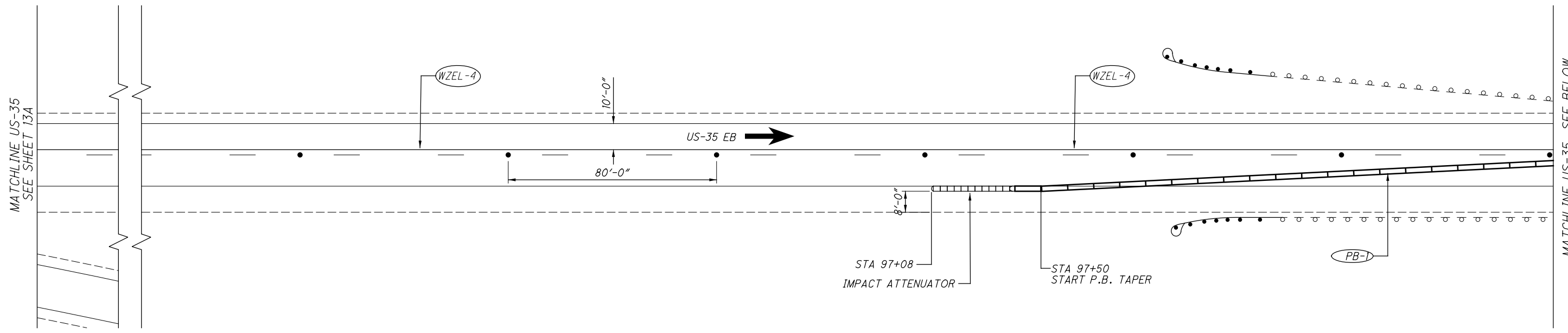
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**MAINTENANCE OF TRAFFIC PLAN VIEWS  
 FAY-35-17.22 EASTBOUND PHASE 1**

**FAY-35-4.52**

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LEGEND	
	= WORK ZONE
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

- NOTE:
1. ALL STA SHOWN ON SHEETS 13-14 ARE FOR DISTANCE REPRESENTATION ONLY.
  2. 100+00 DESIGNATES BEGINNING OF BRIDGE WORK.
  3. DESIGN SPEED 70MPH.
  4. QUANTITIES SHOWN ON SHEET 12.
  5. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.

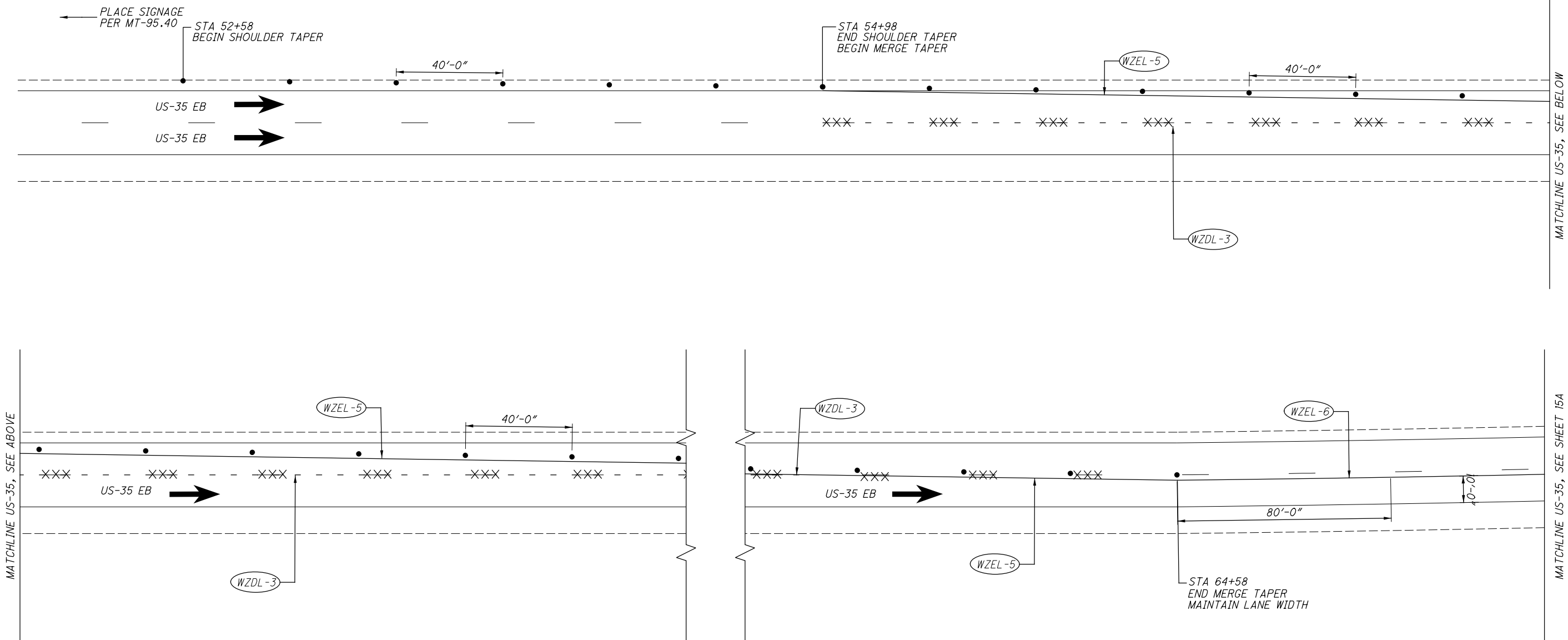
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**MAINTENANCE OF TRAFFIC PLAN VIEWS  
FAY-35-17.22 EASTBOUND PHASE 1**

**FAY-35-4.52**

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LEGEND	
	= WORK ZONE
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

**NOTE:**

- ALL STA SHOWN ON SHEETS 15-17 ARE FOR DISTANCE REPRESENTATION ONLY.
- 100+00 DESIGNATES BEGINNING OF BRIDGE WORK.
- DESIGN SPEED 70MPH.
- QUANTITIES SHOWN ON SHEET 12.
- ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.

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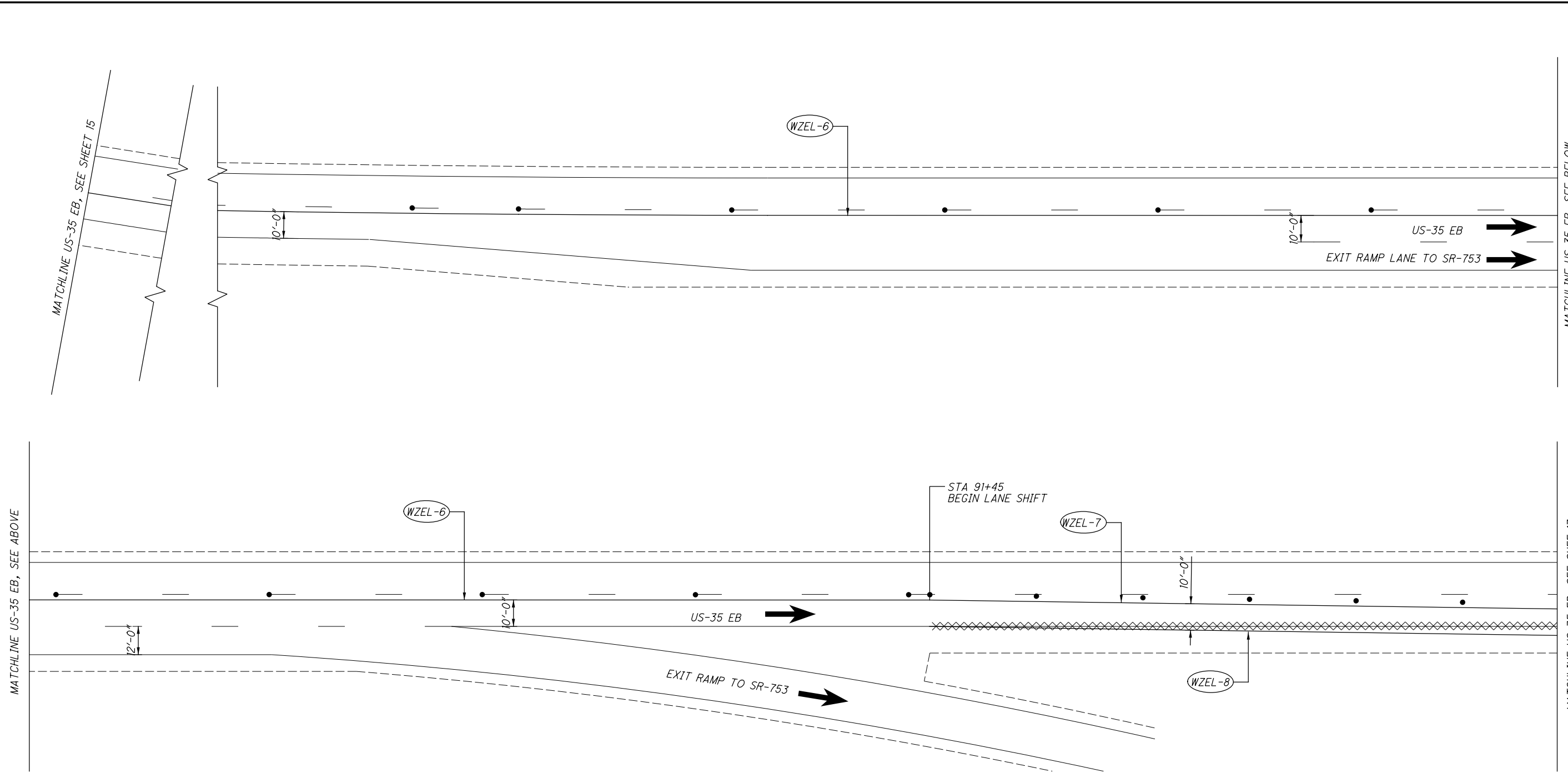
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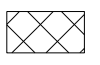


MAINTENANCE OF TRAFFIC PLAN VIEWS  
 FAY-35-17.22 EASTBOUND PHASE 2

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LEGEND	
	= WORK ZONE
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

NOTE:  
 1. ALL STA SHOWN ON SHEETS 15-17 ARE FOR DISTANCE REPRESENTATION ONLY.  
 2. 100+00 DESIGNATES BEGINNING OF BRIDGE WORK.  
 3. DESIGN SPEED 70MPH.  
 4. QUANTITIES SHOWN ON SHEET 12.  
 5. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.

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HORIZONTAL SCALE IN FEET



**MAINTENANCE OF TRAFFIC PLAN VIEWS  
 FAY-35-17.22 EASTBOUND PHASE 2**

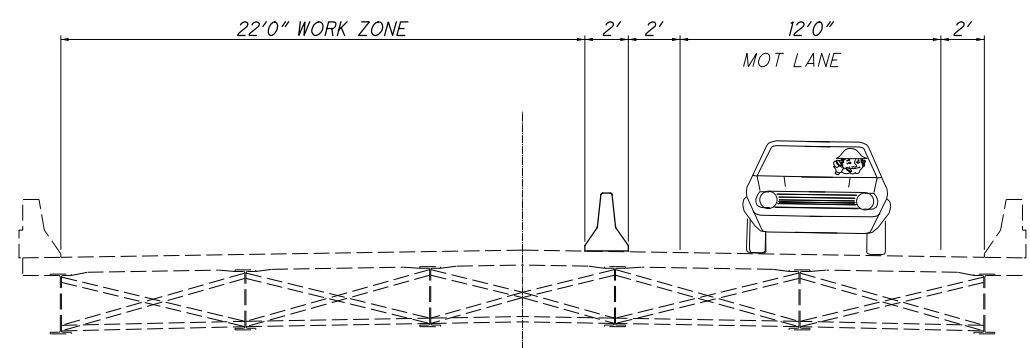
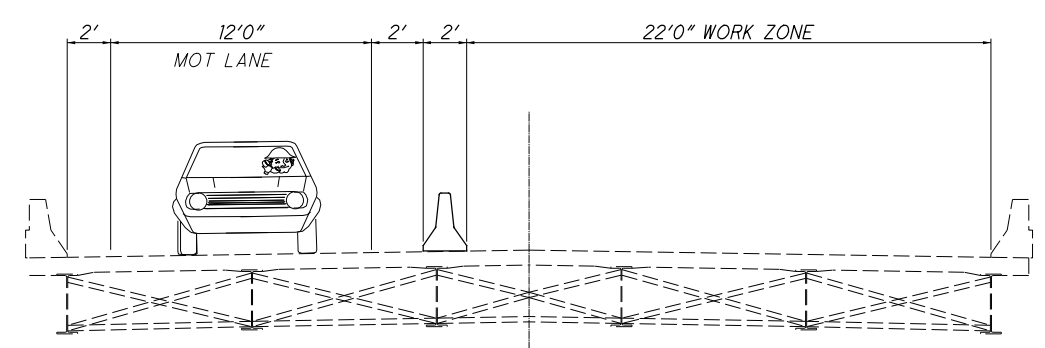
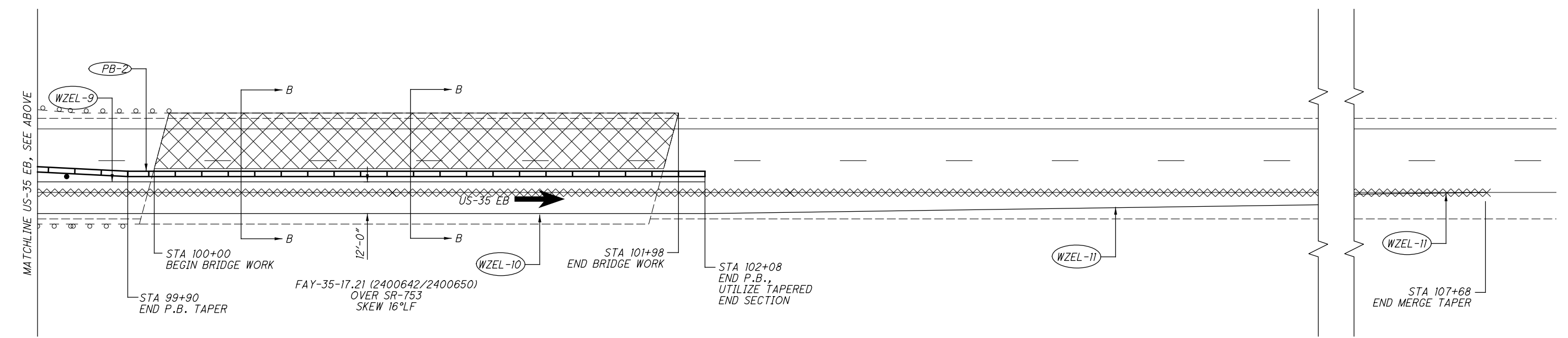
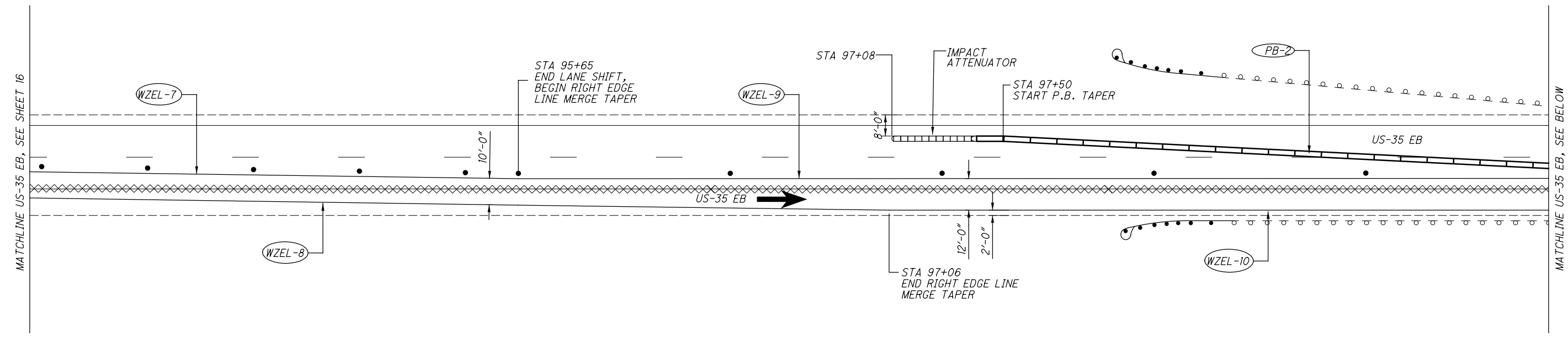
**FAY-35-4.52**



CALCULATED  
RAM  
CHECKED  
VHP

**MAINTENANCE OF TRAFFIC PLAN VIEWS  
FAY-35-17.22 EASTBOUND PHASE 2**

**FAY-35-4.52**

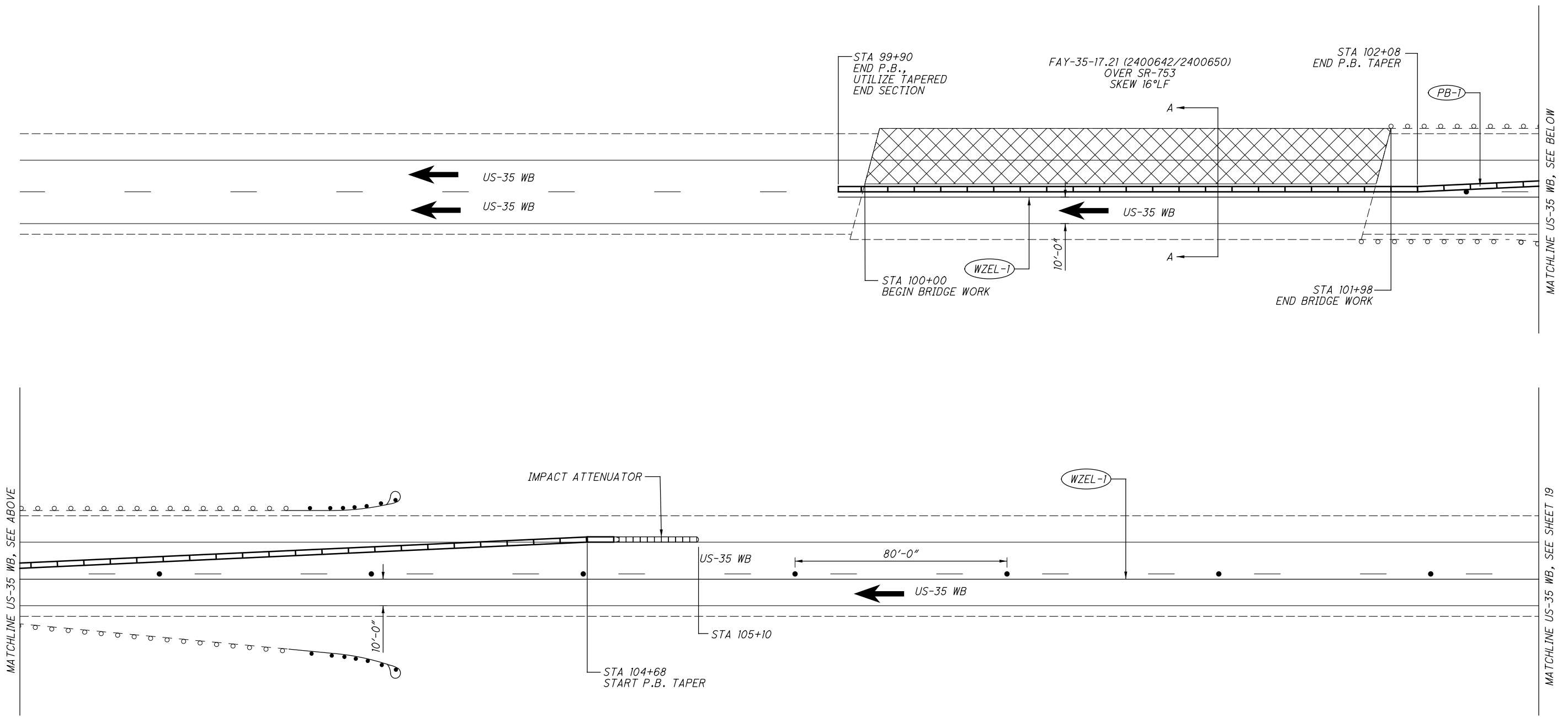


LEGEND	
	= WORK ZONE
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

- NOTE:
1. ALL STA SHOWN ON SHEETS 15-17 ARE FOR DISTANCE REPRESENTATION ONLY.
  2. 100+00 DESIGNATES BEGINNING OF BRIDGE WORK.
  3. DESIGN SPEED 70MPH.
  4. QUANTITIES SHOWN ON SHEET 12.
  5. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.

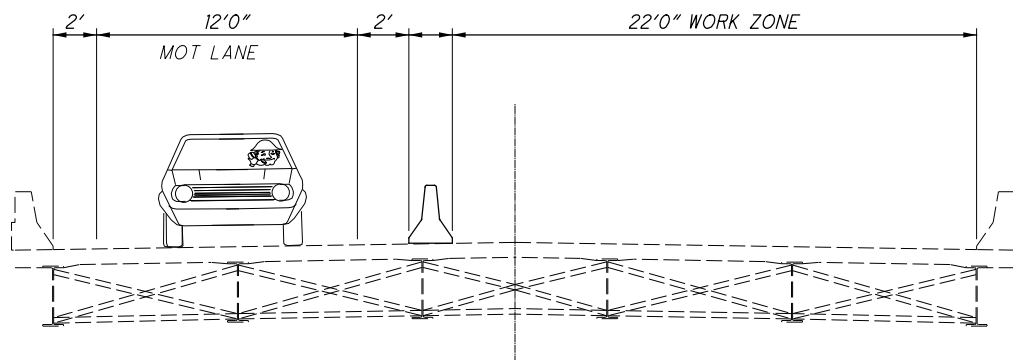
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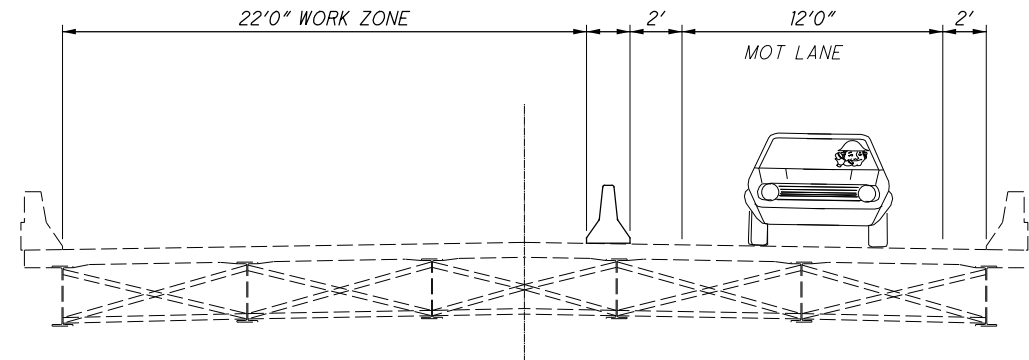


MATCHLINE US-35 WB, SEE ABOVE

MATCHLINE US-35 WB, SEE BELOW



SECTION A-A



SECTION B-B

LEGEND	
	= WORK ZONE
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

- NOTE:
1. ALL STA SHOWN ON SHEETS 18-20 ARE FOR DISTANCE REPRESENTATION ONLY.
  2. 100+00 DESIGNATES BEGINNING OF BRIDGE WORK.
  3. DESIGN SPEED 70MPH.
  4. QUANTITIES SHOWN ON SHEET 12.
  5. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.

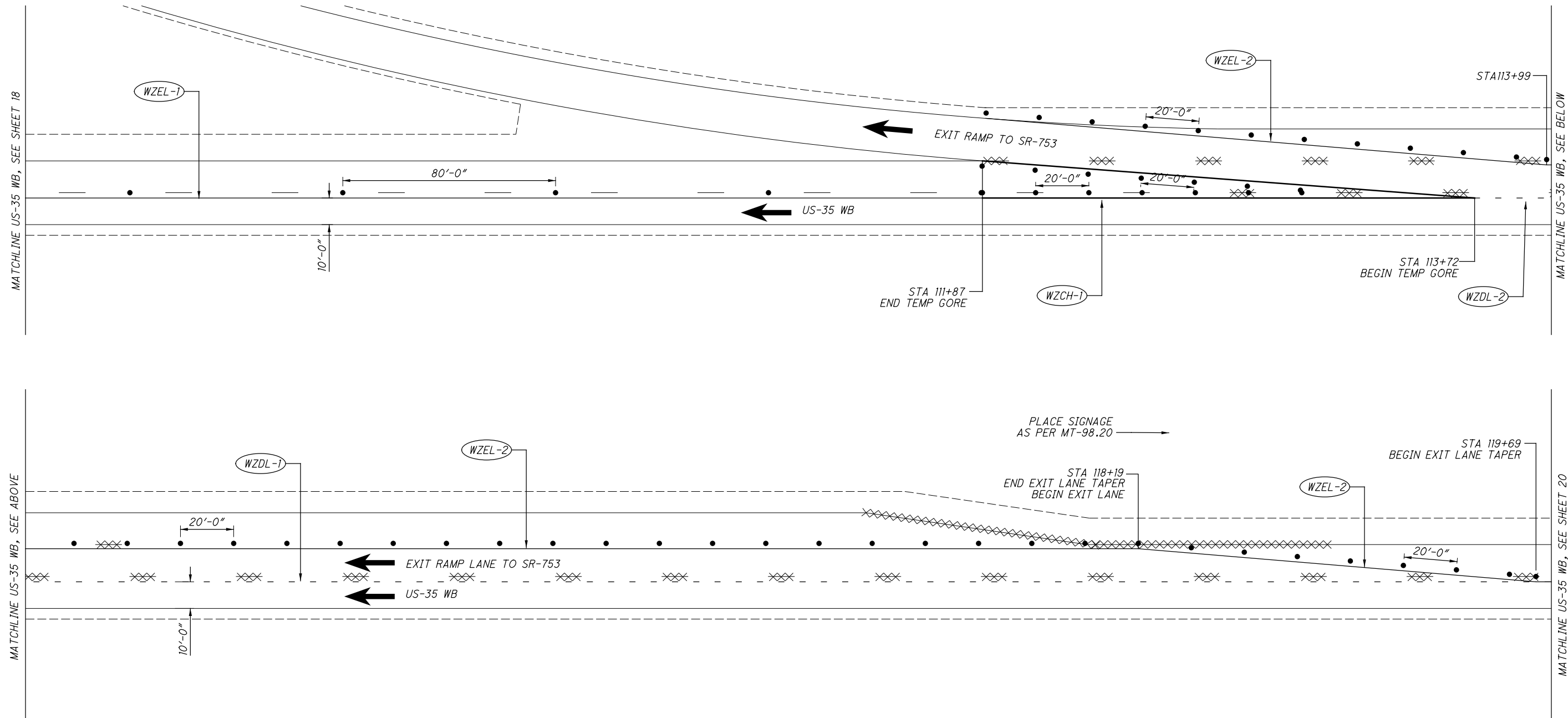


CALCULATED  
RAM  
CHECKED  
VHP

**MAINTENANCE OF TRAFFIC PLAN VIEWS  
FAY-35-17.22 WESTBOUND PHASE 1**

**FAY-35-4.52**

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LEGEND	
	= WORK ZONE
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

NOTE:  
 1. ALL STA SHOWN ON SHEETS 18-20 ARE FOR DISTANCE REPRESENTATION ONLY.  
 2. 100+00 DESIGNATES BEGINNING OF BRIDGE WORK.  
 3. DESIGN SPEED 70MPH.  
 4. QUANTITIES SHOWN ON SHEET 12.  
 5. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.

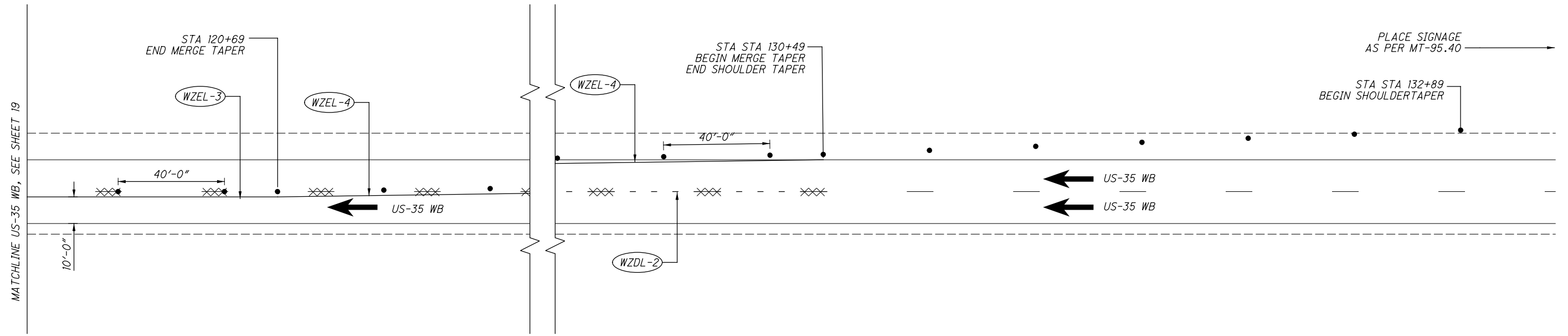
CALCULATED  
RAM  
CHECKED  
VHP

0 20 40  
HORIZONTAL SCALE IN FEET

**MAINTENANCE OF TRAFFIC PLAN VIEWS  
 FAY-35-17.22 WESTBOUND PHASE 1**

**FAY-35-4.52**





CALCULATED  
RAM  
CHECKED  
VHP

0 20 40  
HORIZONTAL  
SCALE IN FEET

**MAINTENANCE OF TRAFFIC PLAN VIEWS  
FAY-35-17.22 WESTBOUND PHASE 1**

LEGEND	
	= WORK ZONE
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

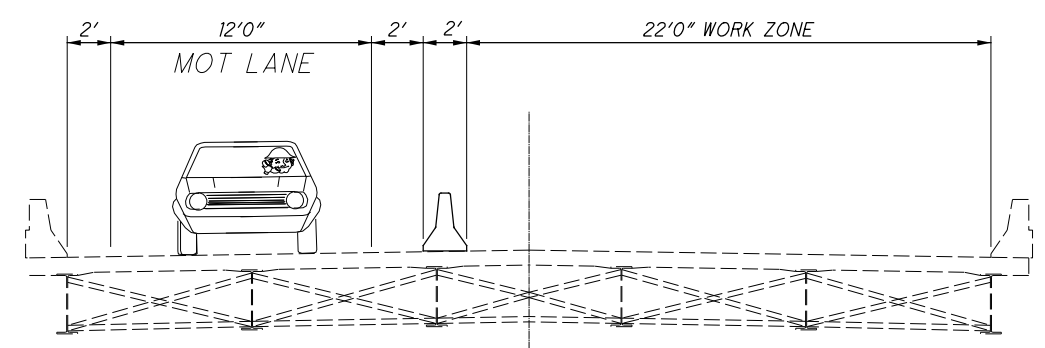
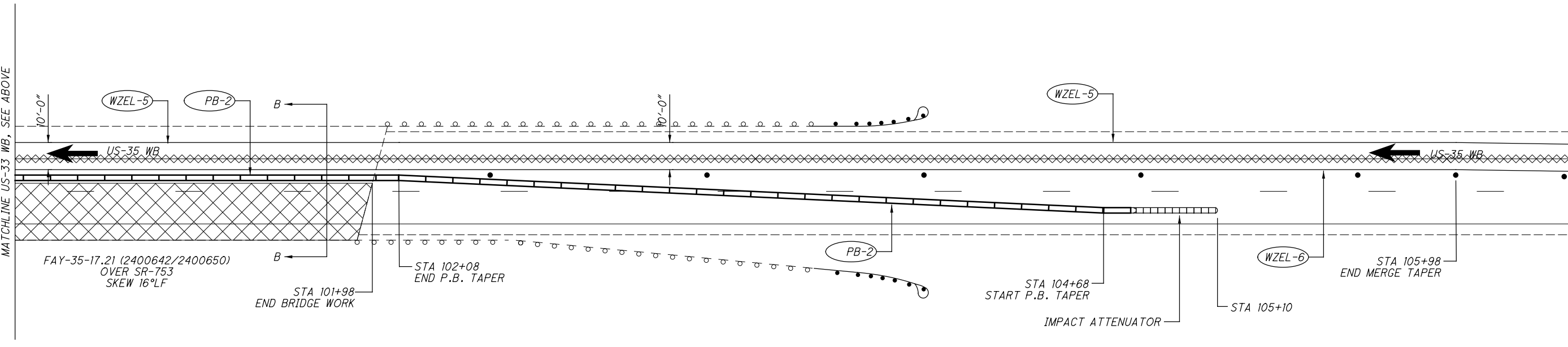
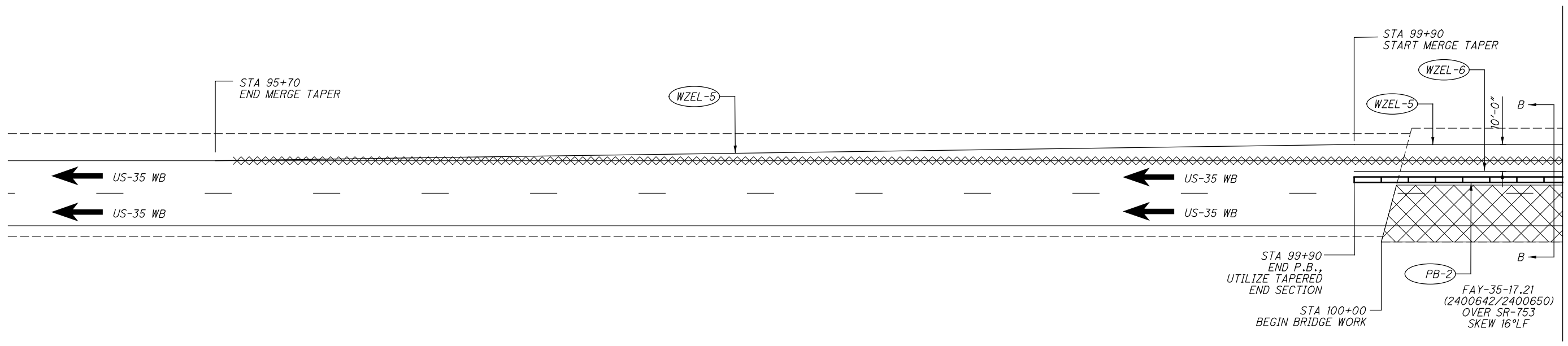
- NOTE:**
1. ALL STA SHOWN ON SHEETS 18-20 ARE FOR DISTANCE REPRESENTATION ONLY.
  2. 100+00 DESIGNATES BEGINNING OF BRIDGE WORK.
  3. DESIGN SPEED 70MPH.
  4. QUANTITIES SHOWN ON SHEET 12.
  5. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.



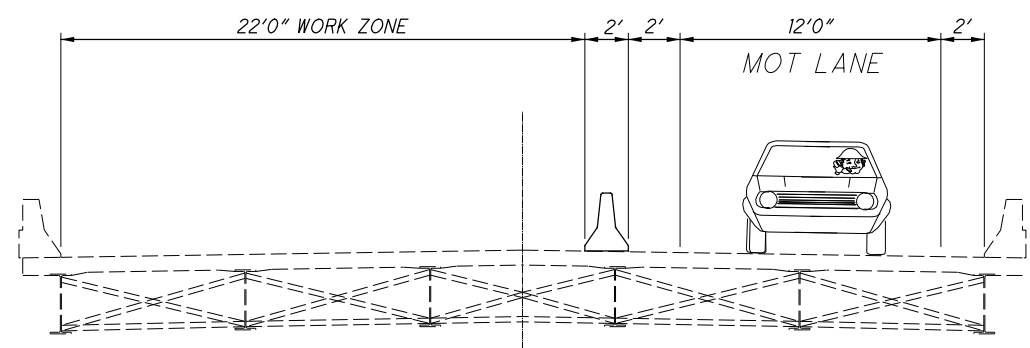
CALCULATED  
RAM  
CHECKED  
VHP

**MAINTENANCE OF TRAFFIC PLAN VIEWS  
FAY-35-17.22 WESTBOUND PHASE 2**

**FAY-35-4.52**



SECTION A-A



SECTION B-B

LEGEND	
	= WORK ZONE
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

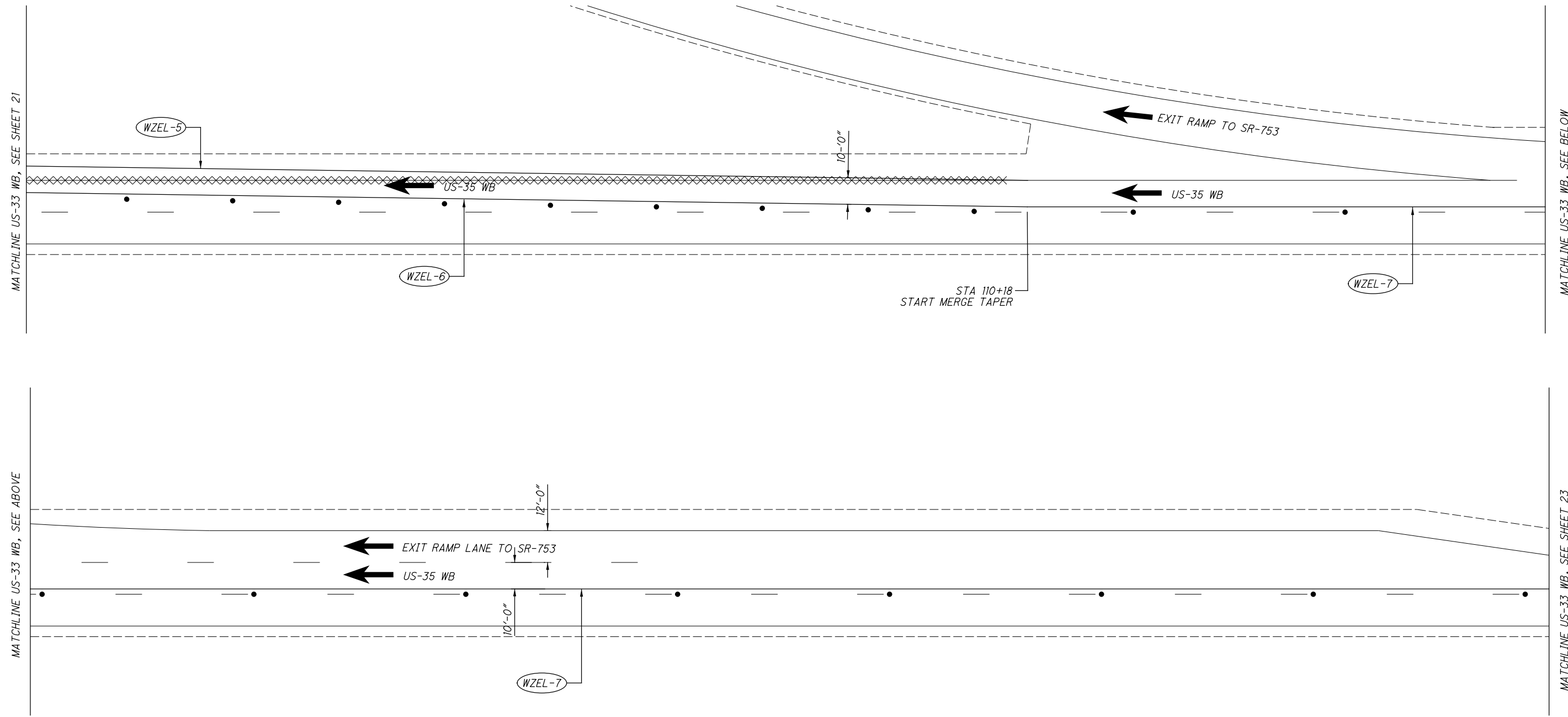
- NOTE:
1. ALL STA SHOWN ON SHEETS 21-23 ARE FOR DISTANCE REPRESENTATION ONLY.
  2. 100+00 DESIGNATES BEGINNING OF BRIDGE WORK.
  3. DESIGN SPEED 70MPH.
  4. QUANTITIES SHOWN ON SHEET 12.
  5. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.

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MATCHLINE US-33 WB, SEE ABOVE

MATCHLINE US-33 WB, SEE BELOW

MATCHLINE US-33 WB, SEE SHEET 22



CALCULATED  
RAM  
CHECKED  
VHP

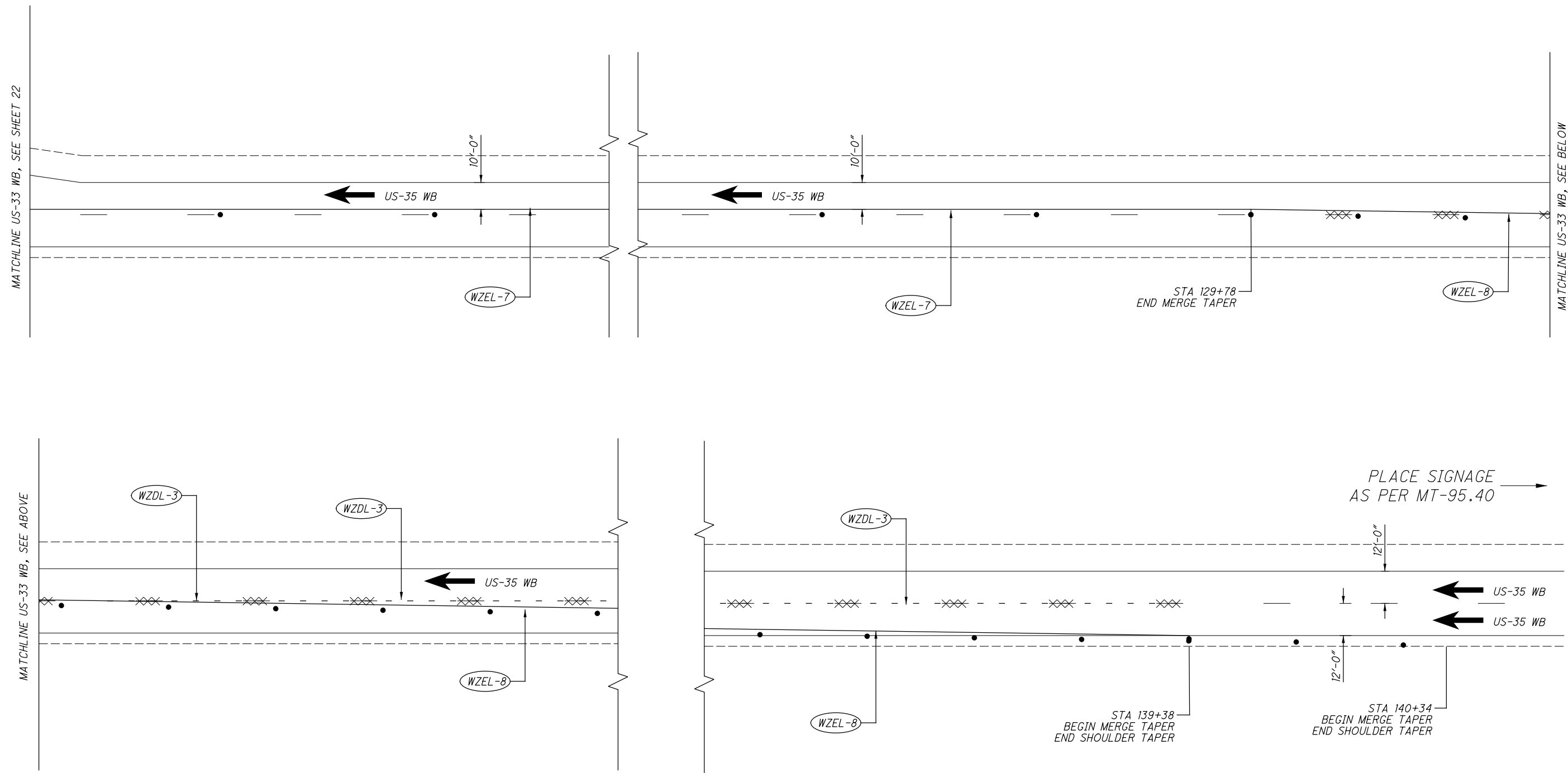
0 20 40  
HORIZONTAL  
SCALE IN FEET

**MAINTENANCE OF TRAFFIC PLAN VIEWS  
FAY -35 -17.22 WESTBOUND PHASE 2**

LEGEND	
	= WORK ZONE
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

- NOTE:**
1. ALL STA SHOWN ON SHEETS 21-23 ARE FOR DISTANCE REPRESENTATION ONLY.
  2. 100+00 DESIGNATES BEGINNING OF BRIDGE WORK.
  3. DESIGN SPEED TOMPH.
  4. QUANTITIES SHOWN ON SHEET 12.
  5. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.

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**NOTE:**  
 1. ALL STA SHOWN ON SHEETS 21-23 ARE FOR DISTANCE REPRESENTATION ONLY.  
 2. 100+00 DESIGNATES BEGINNING OF BRIDGE WORK.  
 3. DESIGN SPEED 70MPH.  
 4. QUANTITIES SHOWN ON SHEET 12.  
 5. ALL CONFLICTING RAISED PAVEMENT MARKER REFLECTORS SHALL BE REMOVED.

LEGEND	
	= WORK ZONE
	= PORTABLE BARRIER
	= PAVEMENT MARKING REMOVED

CALCULATED  
RAM

CHECKED  
VHP

0 20 40  
HORIZONTAL  
SCALE IN FEET

**MAINTENANCE OF TRAFFIC PLAN VIEWS  
 FAY-35-17.22 WESTBOUND PHASE 2**

**FAY-35-4.52**

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SHEET NUMBER										FUNDING		ITEM	ITEM EXT	TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
6-7	8-11	12	26	27	34	50	51	01/NHS/PV	02/NHS/BR								
<b>ROADWAY</b>																	
					10300.0			10300.0		202	38001	10300.0	FT	GUARDRAIL REMOVED, AS PER PLAN			
					22			22		202	42001	22	EACH	ANCHOR ASSEMBLY REMOVED, TYPE A, AS PER PLAN			
					26			26		202	47001	26	EACH	BRIDGE TERMINAL ASSEMBLY REMOVED, AS PER PLAN			
					1115			1115		203	20001	1115	CY	EMBANKMENT, AS PER PLAN			
					97.5			97.5		209	60200	97.5	STA	LINEAR GRADING			
			39.66		39.66			39.66		209	60500	39.66	MILE	LINEAR GRADING			
					9150.0			9150.0		606	15050	9150.0	FT	GUARDRAIL, TYPE MGS			
					22			22		606	26050	22	EACH	ANCHOR ASSEMBLY, MGS TYPE B			
					4			4		606	26550	4	EACH	ANCHOR ASSEMBLY, MGS TYPE T			
					26			26		606	35000	26	EACH	BRIDGE TERMINAL ASSEMBLY, TYPE 1			
					7			7		606	35100	7	EACH	BRIDGE TERMINAL ASSEMBLY, TYPE 2			
200					200			200		606	98000	200	FT	GUARDRAIL, MISC.: ALTERNATIVE GUARDRAIL PLACEMENT			
					6			6		606	60012	6	EACH	IMPACT ATTENUATOR, BI-DIRECTIONAL			
					505			505		622	10101	505	FT	CONCRETE BARRIER, SINGLE SLOPE, TYPE B1, AS PER PLAN			
<b>EROSION CONTROL</b>																	
622					622			622		659	00300	622	CY	TOPSOIL			
5550					5550			5550		659	10000	5550	SY	SEEDING AND MULCHING			
278					278			278		659	14000	278	SY	REPAIR SEEDING AND MULCHING			
278					278			278		659	15000	278	SY	INTER-SEEDING			
0.75					0.75			0.75		659	20000	0.75	TON	COMMERCIAL FERTILIZER			
1.15					1.15			1.15		659	31000	1.15	ACRE	LIME			
30					30			30		659	35000	30	MGAL	WATER			
					1000			1000		832	30000	1000	EACH	EROSION CONTROL			
<b>DRAINAGE</b>																	
1					1			1		611	98631	1	EACH	CATCH BASIN ADJUSTED TO GRADE, AS PER PLAN			
<b>PAVEMENT</b>																	
270					15230			15500		251	01041	15500	SY	PARTIAL DEPTH PAVEMENT REPAIR, (ASPHALT CONCRETE BASE), AS PER PLAN			
				464616				464616		254	01000	464616	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1.5" DEPTH			
				39492				39492		407	20000	39492	GAL	NON-TRACKING TACK COAT			
					11253			11253		442	00100	11253	CY	ANTI-SEGREGATION EQUIPMENT			
					1755			1755		442	10001	1755	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (446), A.P.P., PG76-22M			
					17604			17604		442	10300	17604	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (447)			
					1360			1360		617	10100	1360	CY	COMPACTED AGGREGATE			
2					2			2		617	25000	2	MGAL	WATER			
					33.88			33.88		618	40600	33.88	MILE	RUMBLE STRIP, SHOULDER (ASPHALT CONCRETE)			

CALCULATED	XXX
CHECKED	XXX

**GENERAL SUMMARY**

**FAY - 35 - 4.52**

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SHEET NUMBER										FUNDING		ITEM	ITEM EXT	TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
6-7	8-11	12	26	27	34	50	51	01/NHS/PV	02/NHS/BR								
																<b>TRAFFIC SURVEILLANCE</b>	
	1									1		632	90400	1	EACH	SIGNALIZATION, MISC.: RWIS SENSOR, VX21-2	
																<b>TRAFFIC CONTROL</b>	
						82				82		620	70000	82	EACH	DELINEATOR, MISC.: REBOUNDABLE TUBULAR PYLON	4
							1119			1119		621	00100	1119	EACH	RPM	7
							1119			1119		621	54000	1119	EACH	RAISED PAVEMENT MARKER REMOVED	
						245				245		626	00110	245	EACH	BARRIER REFLECTOR, TYPE 2, ONE DIRECTION	
						195				195		626	00114	195	EACH	BARRIER REFLECTOR, TYPE 4, ONE DIRECTION	
						39.58				39.58		644	00104	39.58	MILE	EDGE LINE, 6"	
						16.90				16.90		644	00204	16.90	MILE	LANE LINE, 6"	
						7015				7015		644	00404	7015	FT	CHANNELIZING LINE	
						305				305		644	00500	305	FT	STOP LINE	
						7870				7870		644	01510	7870	FT	DOTTED LINE, 6"	
						0.08				0.08		646	10010	0.08	MILE	EDGE LINE, 6"	
						0.04				0.04		646	10110	0.04	MILE	LANE LINE, 6"	
																<b>STRUCTURES OVER 20' SPAN</b>	
																SEE STRUCTURE ESTIMATED QUANTITIES	54-55
																<b>MAINTENANCE OF TRAFFIC</b>	
		100								100		614	11110	100	hour	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	9
			4								4	614	12350	4	EACH	WORK ZONE IMPACT ATTENUATOR	
		LS								LS		614	12420	LS		DETOUR SIGNING	9
		20								20		614	12461	20	EACH	WORK ZONE MARKING SIGN, AS PER PLAN	9
		75	120								195	614	13310	195	EACH	BARRIER REFLECTOR, TYPE 1, ONE-WAY	
		25	38								63	614	13350	63	EACH	OBJECT MARKER, ONE-WAY	
		16.94								16.94		614	20560	16.94	MILE	WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT	
			4.77								4.77	614	22360	4.77	MILE	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT	
			644								644.0	614	23690	644	FT	WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT	
			4874								4874	614	24612	4874	FT	WORK ZONE DOTTED LINE, CLASS III, 6", 642 PAINT	
		305								305		614	26610	305	FT	WORK ZONE STOP LINE, CLASS III, 642 PAINT	
			1920								1920	622	41000	1920	EACH	PORTABLE BARRIER, 32"	
																<b>INCIDENTALS</b>	
										LS	LS	614	11000	LS		MAINTAINING TRAFFIC	
										LS	LS	623	10001	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN	7
										LS	LS	624	10000	LS		MOBILIZATION	

CALCULATED XXX CHECKED XXX	<b>GENERAL SUMMARY</b>	<b>FAY - 35 - 4.52</b>
<div style="border: 1px solid black; border-radius: 50%; width: 20px; height: 20px; display: inline-block; margin: 0 auto; text-align: center; line-height: 20px;">             25 63           </div>		

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													DESIGN										REMARKS																				
L O C A T I O N	C O U N T Y	R O U T E	B E G I N I N G	E N D I N G	L E N G T H	L E N G T H	T Y P E	AVG PAVEMENT WIDTH						P V M T A R E A	209	254		407		442		442		617		618																	
								LINEAR GRADING	PAVEMENT PLANING, ASPHALT CONCRETE	NON- TRACKING TACK COAT	ANTI- SEGREGATION EQUIPMENT	ASPHALT CONCRETE, SURFACE COURSE,	ASPHALT CONCRETE, SURFACE COURSE,		COMPACTED AGGREGATE	EDGE LINE, RUMBLE STRIP (ASPHALT CONCRETE)		AVG DEPTH		GAL / SQ YD	AVG DEPTH	AVG DEPTH	12.5MM TYPE A, A.P.P.	AVG DEPTH	12.5MM TYPE A, (447)	AVG DEPTH																	
								MI	FT	A FT	B FT	C FT	D FT		E FT	F FT	SQ YD	MILE	IN	SQ YD	GAL	IN	CU YD	IN	CU YD	IN	CU YD		IN	CU YD	MILE												
1	FAY	35	4.520	7.510	2.990	15787	1	10	24	6	6	24	10	140331	11.96	1.50	140,331	0.085	11,928	1.500	3,508			1.500	5,847	2.00	390		11.96	MAINLINE													
			4.520		0.322	1700	2	4	24	8				6800	0.64	1.50	6,800	0.085	578	1.500	189	1.500	283			2.00	21			RAMP C (SR-435 TO US-35 EB)													
			4.520		0.218	1150	2	4	16	6				3322	0.44	1.50	3,322	0.085	282	1.500	85	1.500	138			2.00	14			RAMP D (US-35 WB TO SR 425)													
EXTRA AREAS																																											
			INTERCHANGES												5300		1.50	5,300	0.085	451	1.500	25			1.500	221						ACCELERATION/DECELERATION LANES											
			CROSSOVERS												535		1.50	535	0.085	45				1.500	22							@ SLM 7.23											
			TOP OF RAMPS																								10																
LOCATION 1 SUBTOTAL														<b>13.04</b>		<b>156288</b>		<b>13284</b>		<b>3807</b>		<b>422</b>		<b>6090</b>		<b>435</b>		<b>11.96</b>															
2	FAY	35	11.970	17.450	5.480	28934	1	10	24	6	6	24	10	257195	21.92	1.50	257,195	0.085	21,862	1.500	6,430			1.500	10,716	2.00	714		21.92	MAINLINE													
			14.100		0.220	1160	2	4	18	4				3351	0.44	1.50	3,351	0.085	285	1.500	97	1.500	140			2.00	14				RAMP A (SR-3, US-22, AND US-62 TO US-35 WB)												
			14.100		0.176	930	2	4	16	4				2480	0.35	1.50	2,480	0.085	211	1.500	69	1.500	103			2.00	11				RAMP B (US-35 WB TO SR-3, US-22, AND US-62)												
			14.100		0.144	760	2	4	16	4				2027	0.29	1.50	2,027	0.085	172	1.500	56	1.500	84			2.00	9				RAMP C (US-35 EB TO SR-3, US-22, AND US-62)												
			14.100		0.322	1700	2	4	16	4				4533	0.64	1.50	4,533	0.085	385	1.500	126	1.500	189			2.00	21				RAMP D (SR-3, US-22, AND US-62 TO US-35 EB)												
			15.370		0.138	730	2	3	16	3				1784	0.28	1.50	1,784	0.085	152	1.500	54	1.500	74			2.00	9				RAMP A (SR-41 TO US-35 WB)												
			15.370		0.218	1150	2	3	16	3				2811	0.44	1.50	2,811	0.085	239	1.500	85	1.500	117			2.00	14				RAMP B (US-35 WB TO SR-41)												
			15.370		0.223	1180	2	3	16	3				2884	0.45	1.50	2,884	0.085	245	1.500	87	1.500	120			2.00	15				RAMP C (US-35 EB TO SR-41)												
			15.370		0.155	820	2	3	16	3				2004	0.31	1.50	2,004	0.085	170	1.500	61	1.500	84			2.00	10				RAMP D (SR-41 TO US-35 EB)												
			17.230		0.205	1080	2	3	16	3				2640	0.41	1.50	2,640	0.085	224	1.500	80	1.500	110			2.00	13				RAMP A (SR-753 TO US-35 WB)												
			17.230		0.159	840	2	3	16	4				2147	0.32	1.50	2,147	0.085	182	1.500	62	1.500	89			2.00	10				RAMP B (US-35 WB TO SR-753)												
			17.230		0.167	880	2	4	16	4				2347	0.33	1.50	2,347	0.085	199	1.500	65	1.500	98			2.00	11				RAMP C (US-35 EB TO SR-753)												
			17.230		0.222	1170	2	3	16	4				2990	0.44	1.50	2,990	0.085	254	1.500	87	1.500	125			2.00	14				RAMP D (SR-753 TO US-35 EB)												
EXTRA AREAS																																											
			INTERCHANGES												18700		1.50	18,700	0.085	1,590	1.500	87			1.500	779							ACCELERATION/DECELERATION LANES										
			CROSSOVERS												435		1.50	435	0.085	37					1.500	18							@ SLM 12.50 AND SLM 16.25										
			TOP OF RAMPS																									60															
LOCATION 2 SUBTOTAL														<b>26.62</b>		<b>308328</b>		<b>26208</b>		<b>7446</b>		<b>1333</b>		<b>11514</b>		<b>925</b>		<b>21.92</b>															
TOTALS CARRIED TO GENERAL SUMMARY														<b>39.66</b>		<b>464616</b>		<b>39492</b>		<b>11253</b>		<b>1755</b>		<b>17604</b>		<b>1360</b>		<b>33.88</b>															

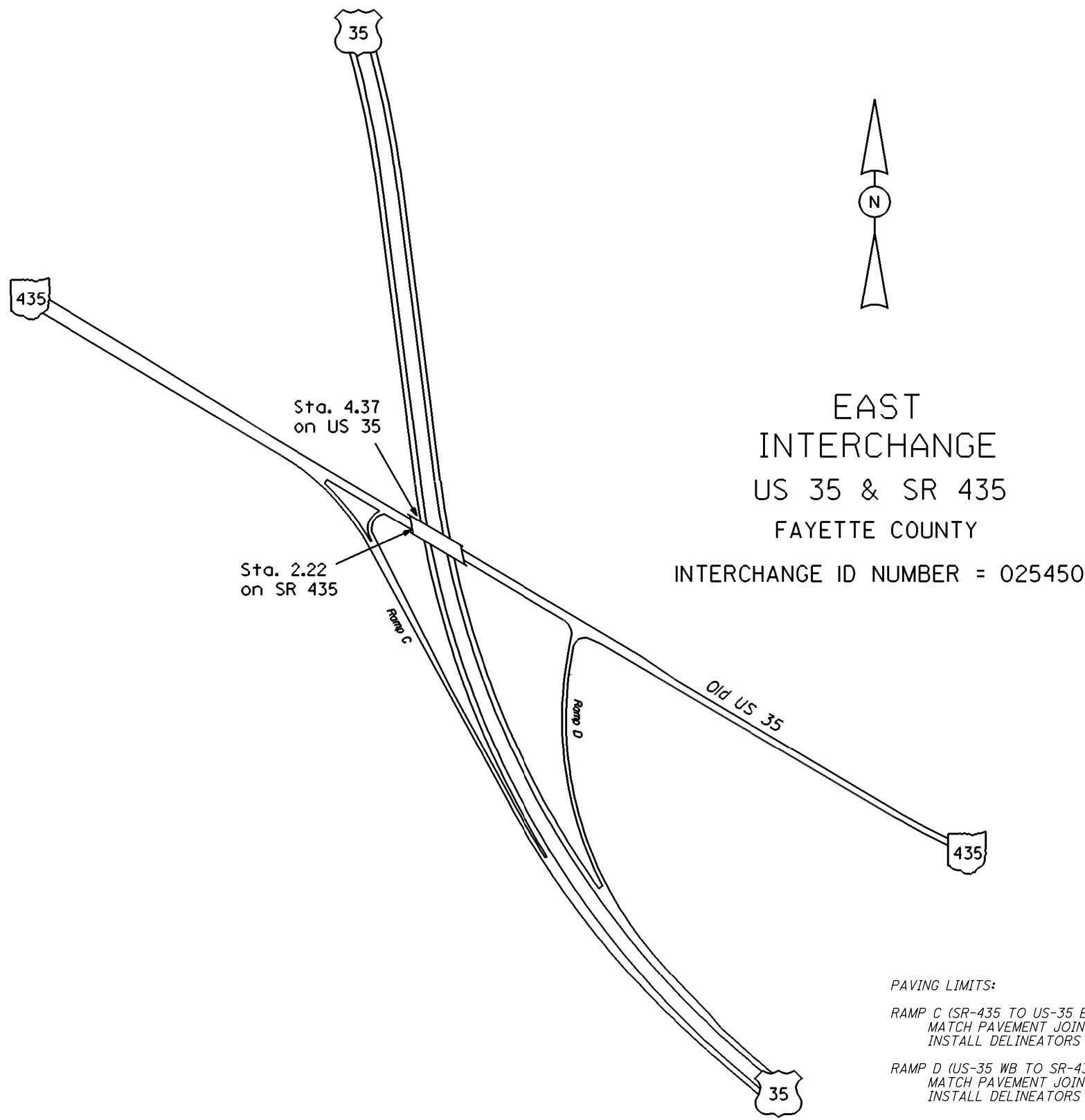
CALCULATED XXX  
 CHECKED XXX  
**PAVEMENT SUBSUMMARY**  
**FAY - 35 - 4.52**  
 26  
 63

LOCATION						DESIGN			REMARKS					
LOCATION	COUNTY	ROUTE	BEGIN SLM	END SLM	DIRECTION	LENGTH	AVG. WIDTH	PAVEMENT AREA	251		253			
									PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN				PAVEMENT REPAIR, AS PER PLAN	
									DEPTH		DEPTH			
FT	FT	SY	IN	SY	IN	CY								
2	FAY	35	12.440	14.970	EB	13358	2	2968.4	3.00	2968.4			LANE 1	
2	FAY	35	12.440	14.970	EB	13358	2	2968.4	3.00	2968.4			LANE 2	
2	FAY	35	17.010		WB	7	50	38.9	3.00	38.9			GORE AT 753 ENTRANCE RAMP	
2	FAY	35	16.650	16.190	WB	2429	2	539.8	3.00	539.8			LANE 1	
2	FAY	35	16.650	16.190	WB	2429	2	539.8	3.00	539.8			LANE 2	
2	FAY	35	16.080	15.500	WB	3062	2	680.4	3.00	680.4			LANE 1	
2	FAY	35	16.080	15.500	WB	3062	2	680.4	3.00	680.4			LANE 2	
2	FAY	35	15.450	15.100	WB	1848	2	410.7	3.00	410.7			LANE 1	
2	FAY	35	15.450	15.100	WB	1848	2	410.7	3.00	410.7			LANE 2	
2	FAY	35	15.170		WB	20	2	4.4	3.00	4.4			GORE	
2	FAY	35	14.970	12.440	WB	13358	2	2968.4	3.00	2968.4			LANE 1	
2	FAY	35	14.970	12.440	WB	13358	2	2968.4	3.00	2968.4			LANE 2	
2	FAY	35	14.130		WB	200	2	44.4	3.00	44.4			LONGITUDINAL JOINT OFF RAMP AT US-62	
2	FAY	35	12.440		WB	10	6	6.7	3.00	6.7			HALF OF LANE 2	
<b>TOTALS CARRIED TO GENERAL SUMMARY</b>										<b>15230.0</b>				

**PAVEMENT REPAIR SUBSUMMARY**



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EAST  
 INTERCHANGE  
 US 35 & SR 435  
 FAYETTE COUNTY  
 INTERCHANGE ID NUMBER = 025450

PAVING LIMITS:  
 RAMP C (SR-435 TO US-35 EB)  
 MATCH PAVEMENT JOINT AT SR-435  
 INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10  
 RAMP D (US-35 WB TO SR-435)  
 MATCH PAVEMENT JOINT AT SR-435  
 INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10

NOTE:  
 QUANTITIES FOR DELINEATORS FOUND IN TRAFFIC CONTROL  
 SUBSUMMARY SHEET 50/63.

CALCULATED	RAM
	CHECKED
	XXX

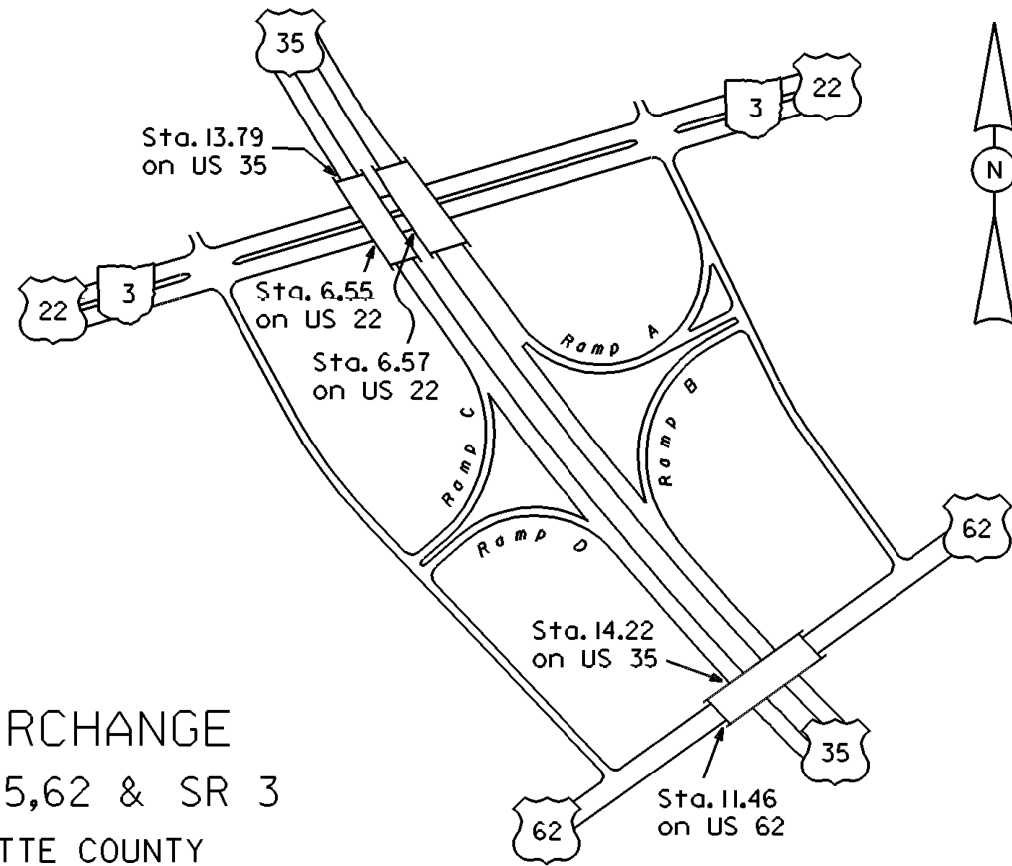
INTERCHANGE DETAIL - US-35 & SR-435

FAY-35-4.52

28
63

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INTERCHANGE  
US 22,35,62 & SR 3  
FAYETTE COUNTY  
INTERCHANGE ID NUMBER = 025400



PAVING LIMITS:

RAMP A (SR-3, US-22, & US-62 TO US-35 WB)  
MATCH PAVEMENT JOINT AT THE DISTRIBUTOR ROAD  
INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10

RAMP B (US-35 WB TO SR-3, US-22, & US-62)  
MATCH PAVEMENT JOINT AT THE DISTRIBUTOR ROAD  
INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10

RAMP C (US-35 EB TO SR-3, US-22, & US-62)  
MATCH PAVEMENT JOINT AT THE DISTRIBUTOR ROAD  
INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10

RAMP D (SR-3, US-22, & US-62 TO US-35 EB)  
MATCH PAVEMENT JOINT AT THE DISTRIBUTOR ROAD  
INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10

NOTE:  
QUANTITIES FOR DELINEATORS FOUND IN TRAFFIC CONTROL  
SUBSUMMARY SHEET 50/63.

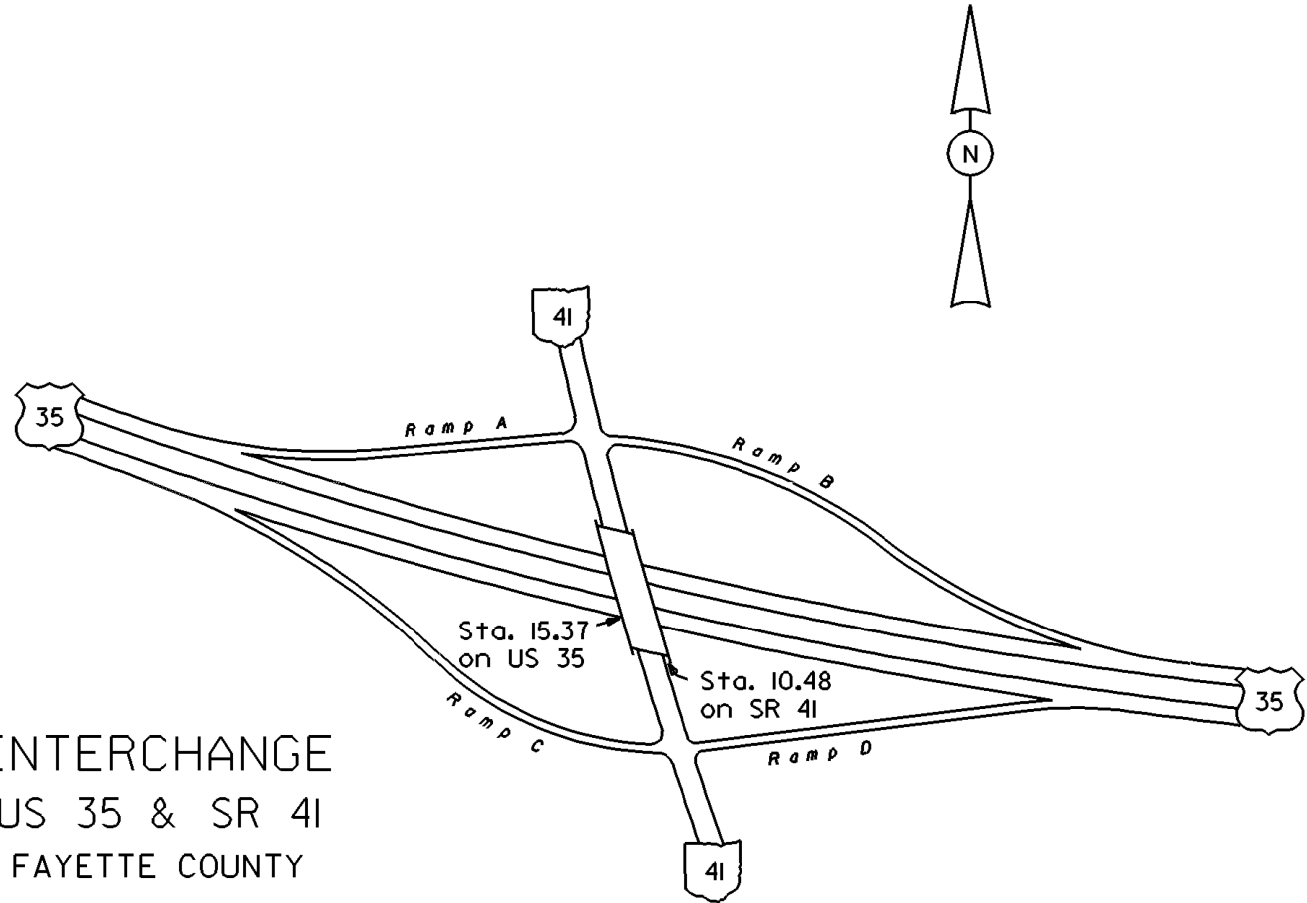
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INTERCHANGE DETAIL - US-35 & US-22, US-62, AND SR-3

FAY-35-4.52

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INTERCHANGE  
US 35 & SR 41  
FAYETTE COUNTY  
INTERCHANGE ID NUMBER = 025600



- PAVING LIMITS:
- RAMP A (SR-41 TO US-35 WB)  
MATCH PAVEMENT JOINT AT SR-41  
INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10
  - RAMP B (US-35 WB TO SR-41)  
MATCH PAVEMENT JOINT AT SR-41  
INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10
  - RAMP C (US-35 EB TO SR-41)  
MATCH PAVEMENT JOINT AT SR-41  
INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10
  - RAMP D (SR-41 TO US-35 EB)  
MATCH PAVEMENT JOINT AT SR-41  
INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10

NOTE:  
QUANTITIES FOR DELINEATORS FOUND IN TRAFFIC CONTROL  
SUBSUMMARY SHEET 50/63.

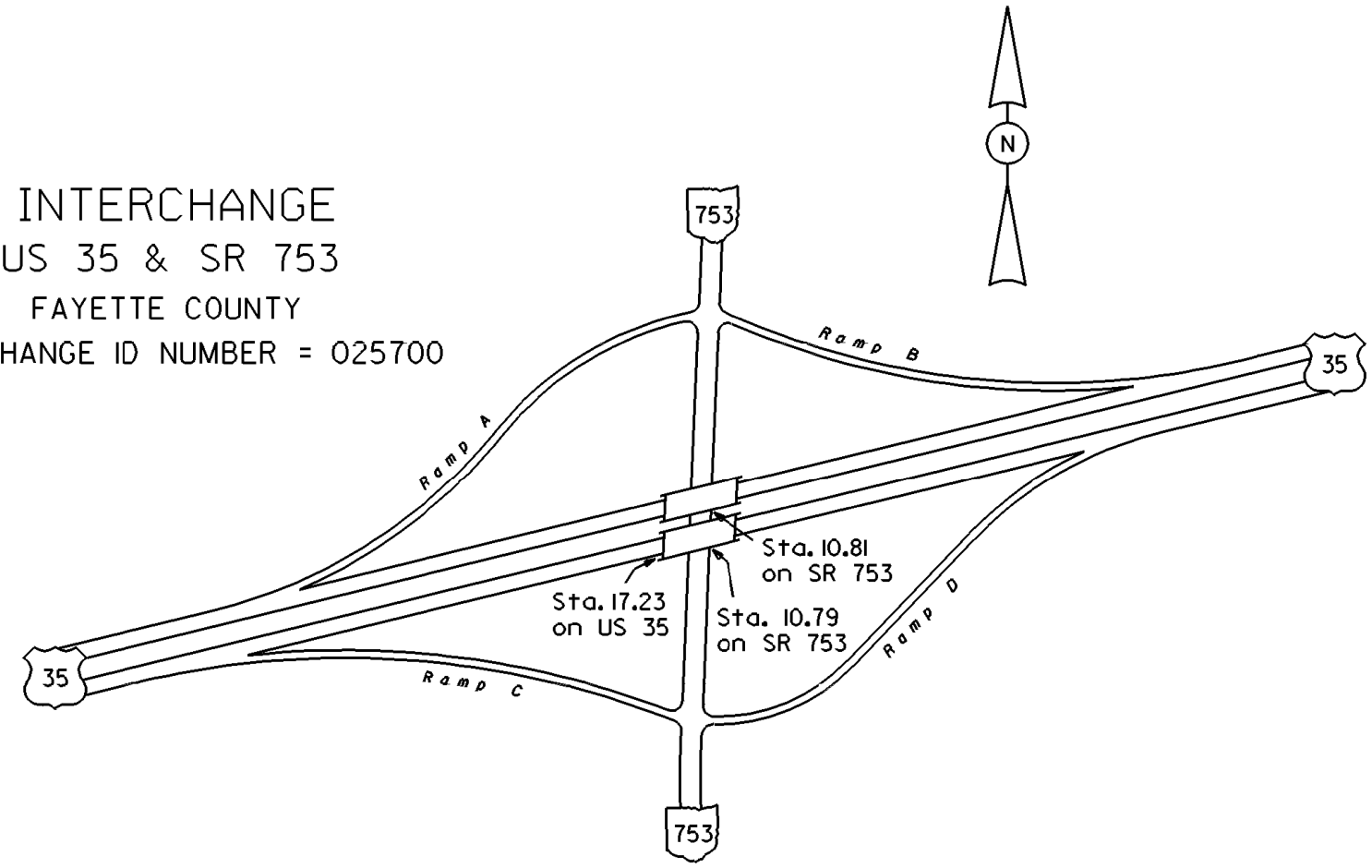
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INTERCHANGE DETAIL - US-35 & SR-41

FAY-35-4.52

30
63

INTERCHANGE  
 US 35 & SR 753  
 FAYETTE COUNTY  
 INTERCHANGE ID NUMBER = 025700

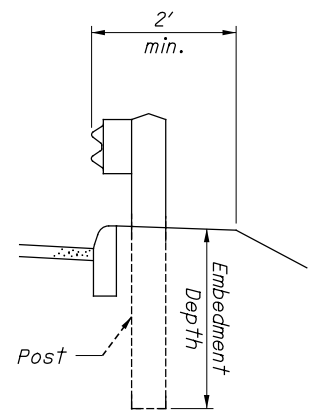


PAVING LIMITS:

- RAMP A (SR-753 TO US-35 WB)  
 MATCH PAVEMENT JOINT AT SR-753  
 INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10
- RAMP B (US-35 WB TO SR-753)  
 MATCH PAVEMENT JOINT AT SR-753  
 INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10
- RAMP C (US-35 EB TO SR-753)  
 MATCH PAVEMENT JOINT AT SR-753  
 INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10
- RAMP D (SR-753 TO US-35 EB)  
 MATCH PAVEMENT JOINT AT SR-753  
 INSTALL DELINEATORS PER STANDARD DRAWING TC-61.10

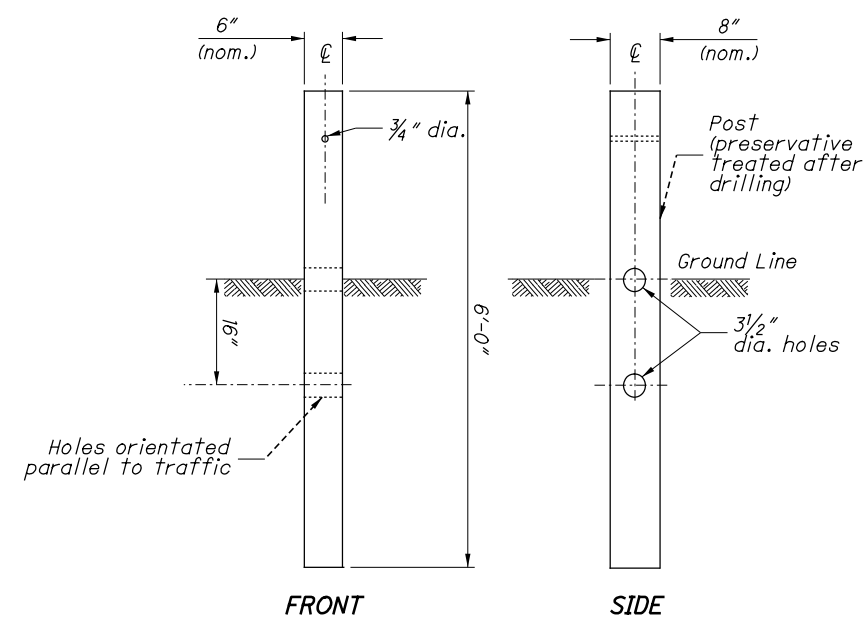
NOTE:  
 QUANTITIES FOR DELINEATORS FOUND IN TRAFFIC CONTROL  
 SUBSUMMARY SHEET 50/63.

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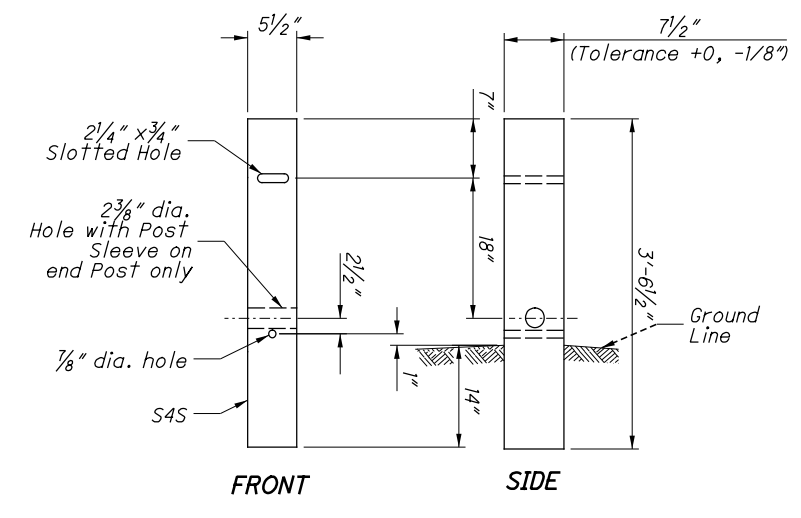


DETAIL A

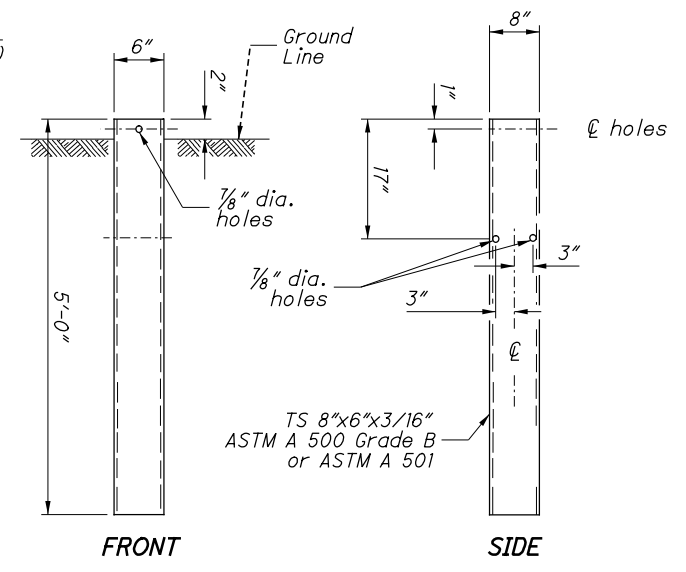
See POST EMBEDMENT DEPTH Note



TYPE 1 BREAKAWAY CRT POST



TYPE 2 BREAKAWAY CRT POST



STEEL GROUND TUBE

NOTES

**GUARDRAIL HEIGHT:** For initial installation, construct the guardrail within  $\pm 1"$  of the standard height,  $h$ , or **29"** to the top of W-Beam rail. (See MEASURING GUARDRAIL HEIGHT Detail.)

When subsequent projects, such as resurfacings, affect the height of existing guardrail, the finished height is to be within  $\pm 2.5"$  of the standard height.

**POST EMBEDMENT DEPTH:** Standard embedment is 3'-5" min. Where less than 2' of graded shoulder width (10:1 or flatter) exists, measured from the face of the guardrail (see DETAIL "A"), use longer posts so that a minimum of 5'-5" embedment depth is provided. Payment for the longer posts will be made at the unit price bid for **ITEM 606 - GUARDRAIL POST, 9', Each.**

**SPECIAL POST MOUNTINGS:** Install posts located over a drainage inlet or structure as shown in the FOOTING ANCHOR Detail, or anchor per the details shown on **SCD GR-2.2.**

Install posts located over a footing with a cover of less than 2'-6" with a footing anchor as detailed here. (A plate, as detailed on SECTION B-B of **SCD GR-2.2,** may be used as an alternative attachment method.) Where the cover is between 2'-6" and 3'-5", the footing anchor may be omitted and the post encased instead with 4" (min.) of concrete.

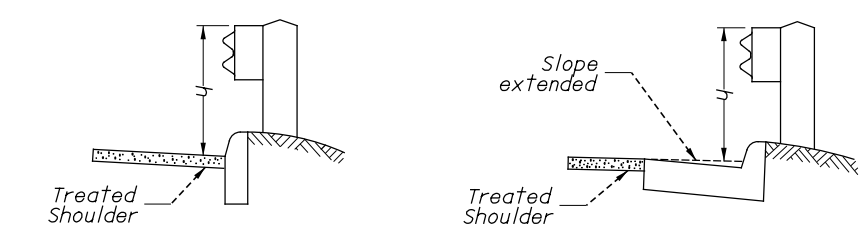
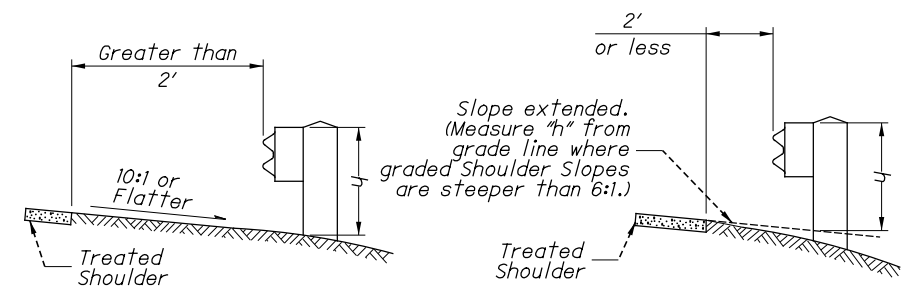
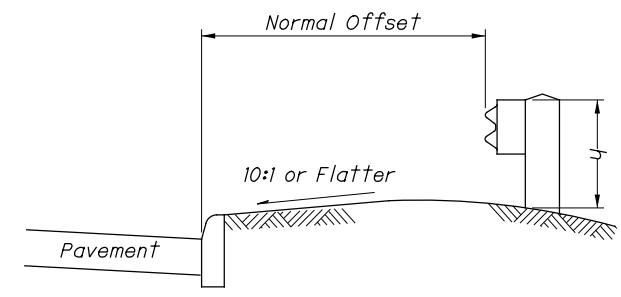
Do not drive posts located over a culvert with less than 4'-3" of cover; instead set in drilled or dug holes. Where the available post embedment depth is less than 3'-5", encase the post with a minimum of 4" concrete.

All costs associated with special post mountings are included in the unit price bid of Item 606 Guardrail of the type specified in the plans.

**ANCHORS:** Holes and grouting shall comply with CMS 510. Use either cement or non-shrink, nonmetallic grout.

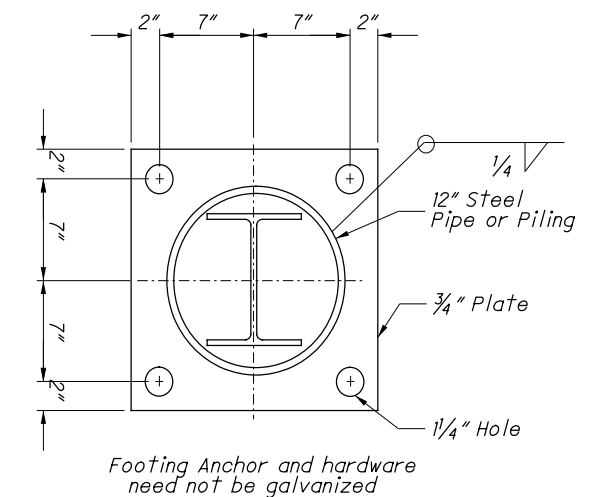
Expansion shield anchors as specified in CMS 712.01 may be substituted except where concrete deterioration has occurred, as determined by the Engineer. Where self-drilling anchors are used, drill the holes with the expansion shield (not by a drill bit) and install the shield flush with the concrete surface.

**PROTECTIVE COATING:** In lieu of the complying with CMS 710.06, coat expansion shields, anchors and concrete insert anchor assemblies embedded in concrete in accordance with ASTM A 153 or be of stainless steel. Any bolts screwed into these devices shall meet CMS 710.06. (See sheet 3 for Concrete Insert Anchor Assembly Detail.)

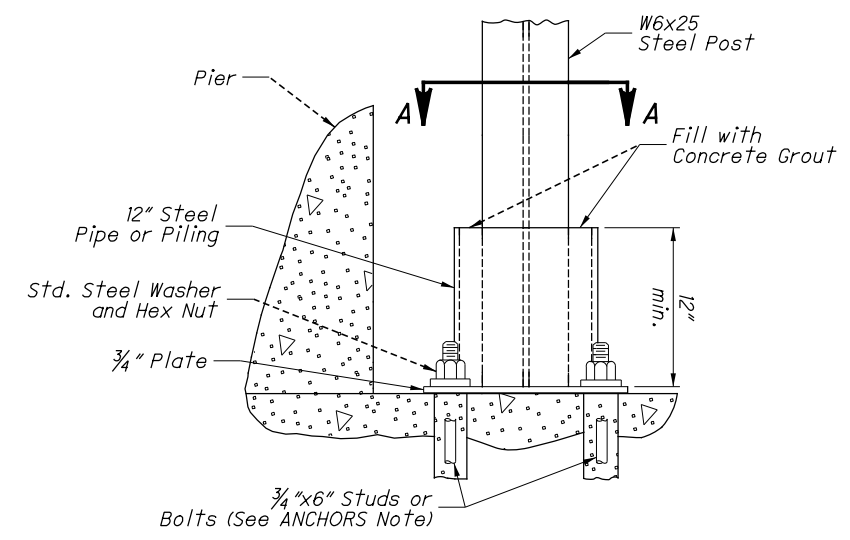


$h$  = Standard Height (See GUARDRAIL HEIGHT Note)

MEASURING GUARDRAIL HEIGHT

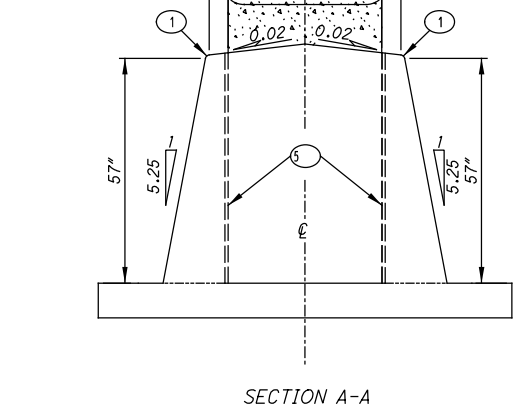
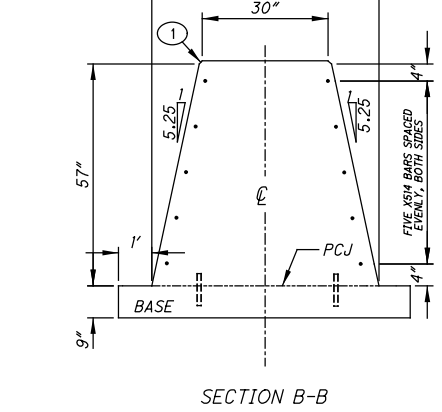
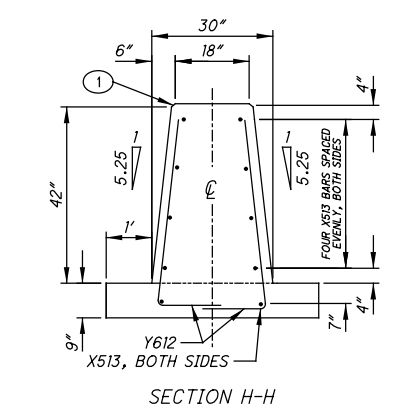
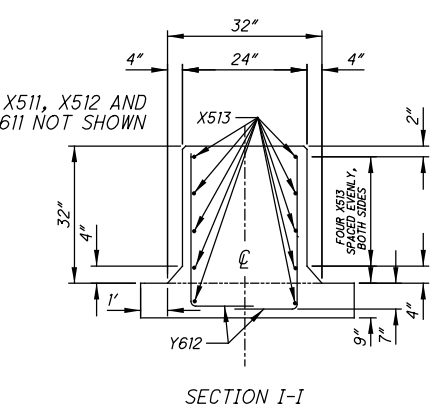
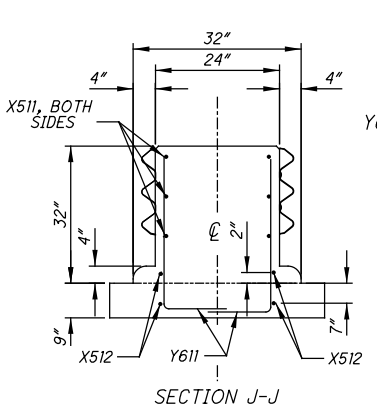
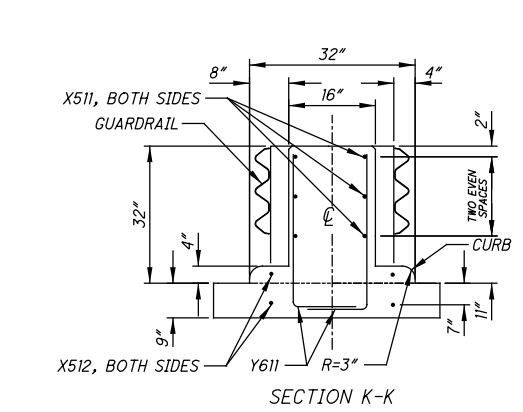
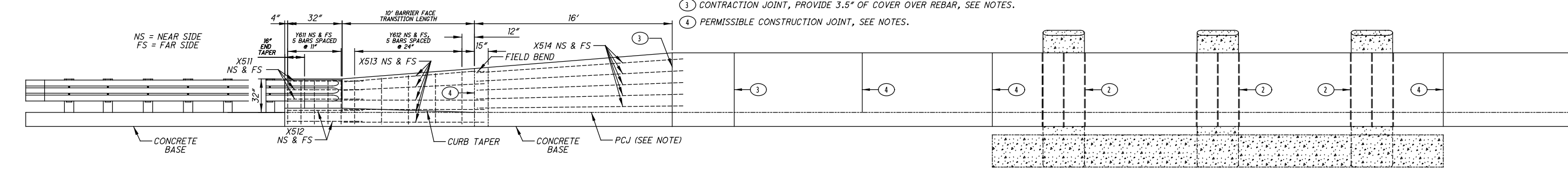
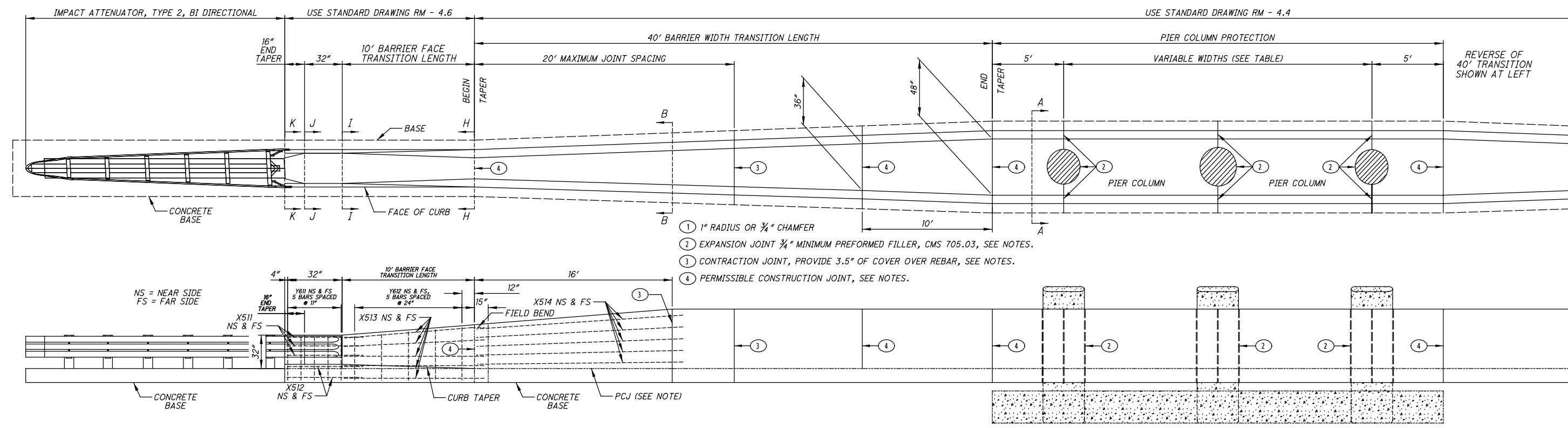


SECTION A-A



ELEVATION FOOTING ANCHOR

See SPECIAL POST MOUNTINGS Note.



**ITEM 622- CONCRETE BARRIER, SINGLE SLOPE, TYPE B1, AS PER PLAN:**  
THE END SECTIONS AS SHOWN ON SCD RM-4.3 ARE TO BE CONSTRUCTED ON BOTH SIDES OF THE PROPOSED SINGLE SLOPE CONCRETE BARRIER, SINGLE SLOPE, TYPE B1, BARRIER TRANSITION AS SHOWN ON SCD RM-4.4. PROVIDE 2" CONCRETE COVER OVER ALL REBAR EXCEPT AS NOTED. FOR BARRIER INFORMATION NOT SHOWN ON THIS DRAWING, SEE CMS 622.

**MEDIAN GRADING**  
THE EXISTING MEDIAN SHALL BE GRADED AS SHOWN IN SCD GR - 6.2. QUANTITIES FOR RE-SEEDING OF THE RE-GRADED MEDIAN AREA SHALL BE PAID FOR UNDER ITEM 659.

**GUARDRAIL**  
INFORMATION ON IMPACT ATTENUATORS IS FOUND IN THE LOCATION AND DESIGN MANUAL, VOLUME 1, SECTION 603.

**BARRIER FACE TRANSITIONS**  
TO PREVENT VEHICLE SNAGGING, SMOOTH TRANSITIONS FROM VERTICAL FACES TO THE SINGLE SLOPE FACES ARE MADE OVER A 10' DISTANCE.

**CONCRETE**  
USE CLASS C CONCRETE. CONSTRUCT TOP AND END EDGES WITH EITHER A 1" RADIUS OR 3/4" CHAMFER, EXCEPT AT LIGHT POLE FOUNDATIONS.

**CONTRACTION JOINTS**  
MAXIMUM ALLOWABLE SPACING OF UNSEALED JOINTS IS 20' THROUGHOUT THE RUN OF THE BARRIER. CONSTRUCT JOINTS BY USING METAL INSERTS INSIDE THE FORMS, PREFORMED FULL WIDTH JOINT FILLER, A GROOVING TOOL, OR BY SAWING. INSERTS, TOOLED, OR SAWED JOINTS WILL HAVE A 3" DEPTH. CONSTRUCT ALL JOINTS FOR THE FULL HEIGHT OF THE BARRIER. SAW AS SOON AS CURING WILL ALLOW TO PREVENT SPALLING. WHEN USED IN CONJUNCTION WITH CONCRETE PAVEMENT, MATCH JOINTS TO THOSE IN THE CONCRETE PAVEMENT BUT NOT EXCEEDING THE MAXIMUM ALLOW-ABLE SPACING.

**PERMISSIBLE CONSTRUCTION JOINTS**  
BARRIER RUNS WITH ABUTTING VERTICAL SURFACES AT EITHER REQUIRED OR PERMISSIBLE CONSTRUCTION JOINTS ARE TO BE DOWELED TO EACH OTHER BY USE OF 3/4" DIA. BY 18" LONG EPOXY COATED DEFORMED DOWEL BARS AS PER CMS 622.02 AND IN THE UNREINFORCED BASE SECTION, BARRIER MAY BE PLACED ON TOP OF THE CONCRETE BASE IF DOWELED AS SHOWN ON IN THE DOWEL BAR PLACEMENT DETAIL ON SCD RM-4.3.

**EXPANSION JOINTS**  
CONSTRUCT 3/4" JOINT WITH CMS 705.03 FILLER AT INDICATED POSITIONS.

**PAYMENT**  
WILL BE MADE AT THE UNIT PRICE BID PER FOOT FOR ITEM 622 - CONCRETE BARRIER, SINGLE SLOPE, TYPE B1, AS PER PLAN AND INCLUDE ALL MATERIALS, LABOR, REINFORCING STEEL, END ANCHORAGES AND OTHER INCIDENTALS NECESSARY TO CONSTRUCT THE BARRIER END SECTIONS AND TAPERED TRANSITIONS.

TYPE B1 BARRIER END SECTION STEEL LIST				
MARK	BAR	SHAPE	NO.	LENGTH
X511	#5	BENT	6	5' - 6"
X512	#5	STR.	4	5' - 6"
X513	#5	STR.	10	11' - 1"
X514	#5	STR.	10	17' - 2"
Y611	#6	BENT	10	4' - 2"
Y612	#6	BENT	2 SERIES OF 5	VARIES 4' - 3" TO 5' - 3"

PIER COLUMN WIDTHS	
FAY-35-13.24	45'
FAY-35-14.19	35'
FAY-35-15.35	65'

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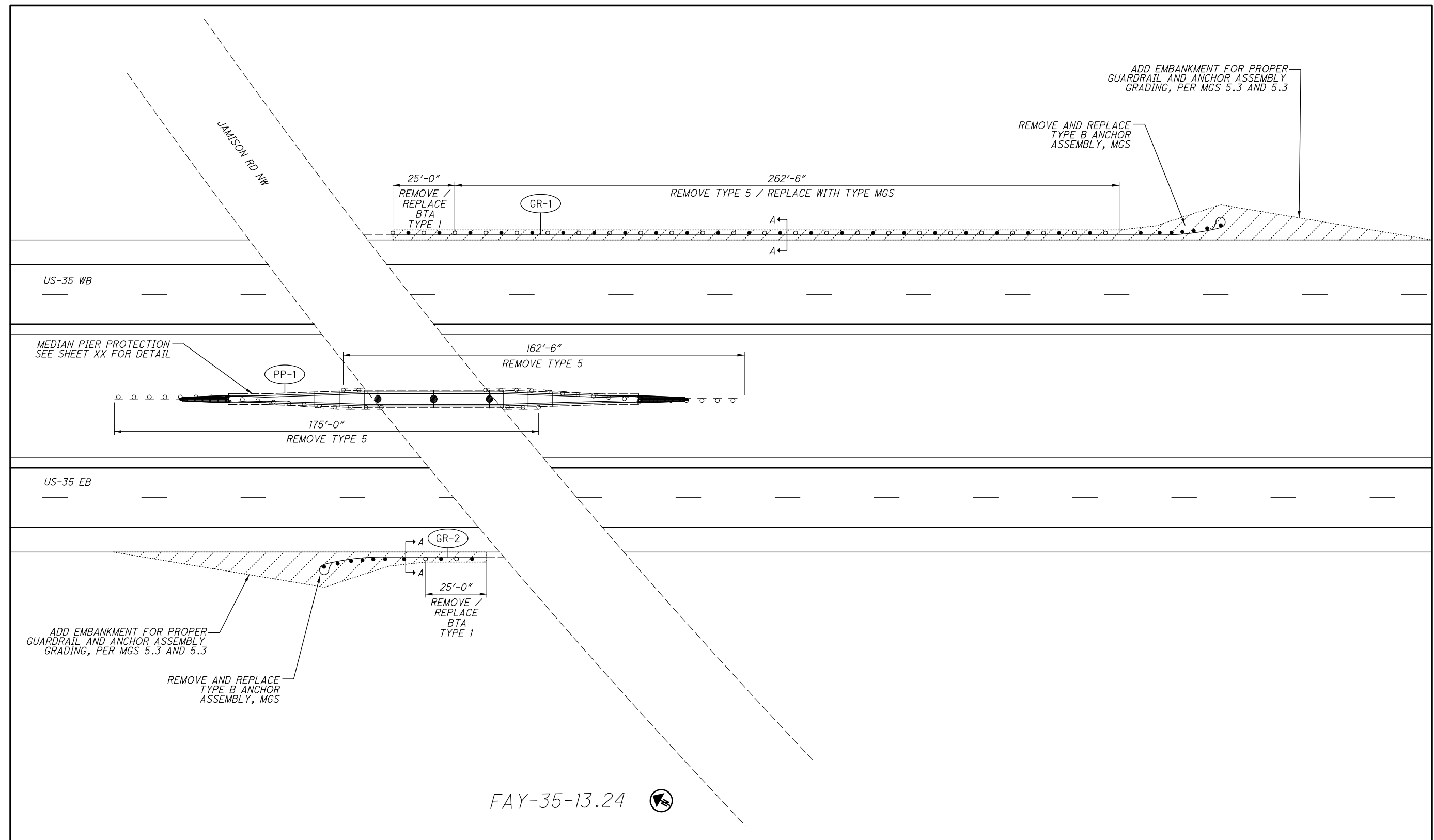
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SEE SHEET	REFERENCE NUMBER	LOCATION			202	202	202	203	209	606	606	606	606	606	622	
		ROUTE	SLM	SIDE	GUARDRAIL REMOVED, AS PER PLAN FT	ANCHOR ASSEMBLY REMOVED, TYPE B, AS PER PLAN EACH	BRIDGE TERMINAL ASSEMBLY REMOVED, TYPE 1, AS PER PLAN EACH	EMBANKMENT, AS PER PLAN CY	LINEAR GRADING STA	GUARDRAIL, TYPE MGS FT	BRIDGE TERMINAL ASSEMBLY, TYPE 1 EACH	ANCHOR ASSEMBLY, MGS, TYPE B EACH	ANCHOR ASSEMBLY, MGS, TYPE T EACH	IMPACT ATTENUATOR, BI-DIRECTIONAL EACH	CONCRETE BARRIER, SINGLE SLOPE, TYPE B1, AS PER PLAN FT	
	22	GR-1	FAY-35	13.24	WB	262.5	1	1	57.5	2.6	262.5	1	1			
	22	GR-2	FAY-35	13.24	EB		1	1	5	0.3		1	1			
	22	PP-1	FAY-35	13.24	MEDIAN	337.5				1.7				2	165	
	23-24	GR-3	FAY-35	13.38 TO 13.86	WB	962.5			192.5	9.6	962.5			1		
	23-24	GR-4	FAY-35	13.38 TO 13.86	EB		1	1	5	0.3	0	1	1			
	24	GR-5	FAY-35	13.38 TO 13.86	MEDIAN	150	1	1	35	1.5	150	1	1			
	25-26	GR-6	FAY-35	13.38 TO 13.86	WB	812.5			162.5	8.1	812.5	1				
	25-26	GR-7	FAY-35	13.38 TO 13.86	EB	887.5			177.5	8.9	887.5	1				
	25	GR-8	FAY-35	13.38 TO 13.86	MEDIAN	150	1	1	35	1.5	150	1	1			
	26	GR-9	FAY-35	13.38 TO 13.86	MEDIAN	150	1	1	35	1.5	150	1	1			
	27	GR-10	FAY-35	13.38 TO 13.86	WB	300	1	1	65	3.0	300	1	1			
	27	GR-11	FAY-35	13.38 TO 13.86	MEDIAN	150	1	1	35	1.5	150	1	1			
	28	GR-12	FAY-35	14.19	WB	300			60	3.0	300			1		
	28	GR-13	FAY-35	14.19	EB		1	1	5	0.3		1	1			
	28	GR-14	FAY-35	14.19	WB		1	1	5	0.3		1	1			
	28	PP-2	FAY-35	14.19	MEDIAN	312.5				1.6				2	155	
	29	GR-15	FAY-35	15.35	WB	25	1		10	0.3	25	0	1			
	29	GR-16	FAY-35	15.35	EB	25	1		10	0.3	25	0	1			
	29	PP-3	FAY-35	15.35	MEDIAN	400				1.9				2	185	
	30	GR-17	FAY-35	16.58 TO 17.00	WB	250			50	2.5	250			1		
	30	GR-18	FAY-35	16.58 TO 17.00	EB	300	1	1	65	3.0	300	1	1			
	30	GR-19	FAY-35	16.58 TO 17.00	MEDIAN	150	1	1	35	1.5	150	1	1			
	31	GR-20	FAY-35	16.58 TO 17.00	WB	300			60	3.0	300	1	0			
	31	GR-21	FAY-35	16.58 TO 17.00	EB	475			95	4.8	475	1	0			
	31	GR-22	FAY-35	16.58 TO 17.00	MEDIAN	150	1	1	35	1.5	150	1	1			
	31	GR-23	FAY-35	16.58 TO 17.00	MEDIAN	150	1	1	35	1.5	150	1	1			
	32-33	GR-24	FAY-35	16.58 TO 17.00	WB	900	1	1	185	9.0	900	1	1			
	32-33	GR-25	FAY-35	16.58 TO 17.00	EB	787.5			157.5	7.9	787.5			1		
	32	GR-26	FAY-35	16.58 TO 17.00	MEDIAN	150	1	1	35	1.5	150	1	1			
	34	GR-27	FAY-35	17.22	MEDIAN	150	1	1	35	1.5	150	1	1			
	34	GR-28	FAY-35	17.22	EB	125	1	1	30	1.3	125	1	1			
	35	GR-29	FAY-35	17.22	WB	125	1	1	30	1.3	125	1	1			
	35	GR-30	FAY-35	17.22	MEDIAN	150	1	1	35	1.5	150	1	1			
	36	GR-31	RAMP US 62/22/3 TO SR 35 E			100										
<b>TOTALS CARRIED TO GENERAL SUMMARY</b>					9487.5	22	24	1777.5	89.6	8337.5	24	22	4	6	505	

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**GUARDRAIL SUBSUMMARY**

**FAY - 35 - 4.52**

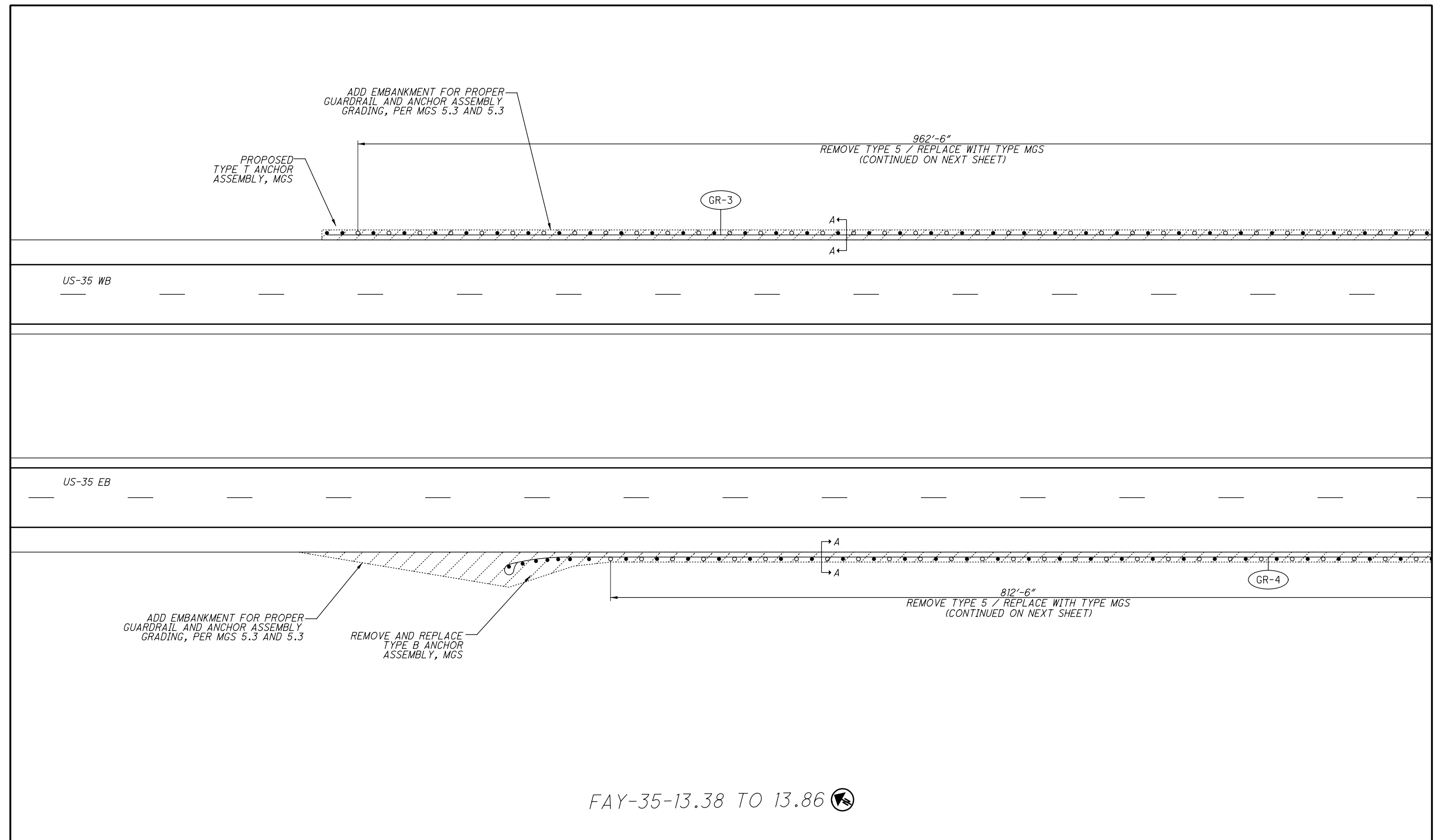


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SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.  
FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

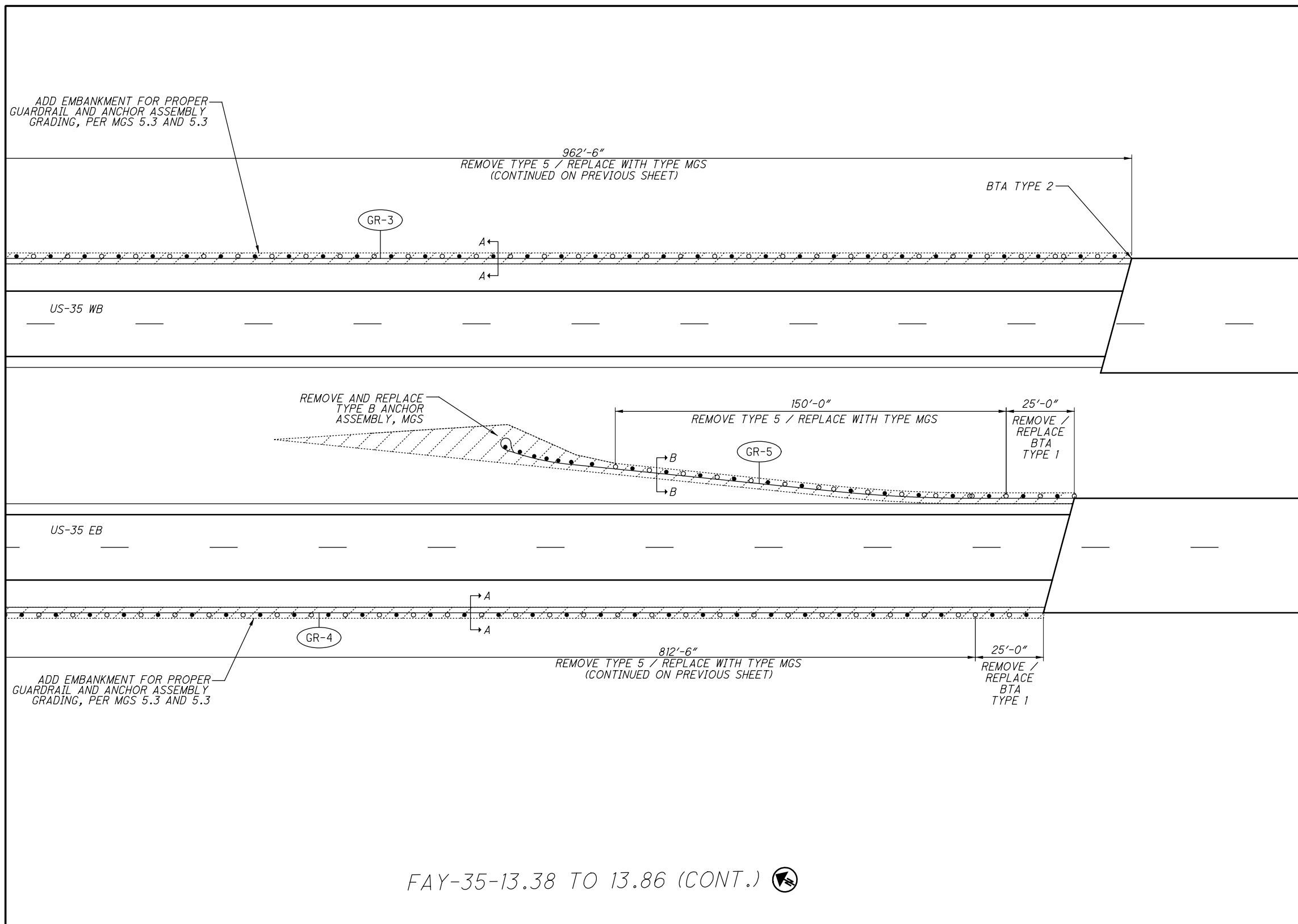




SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.  
FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

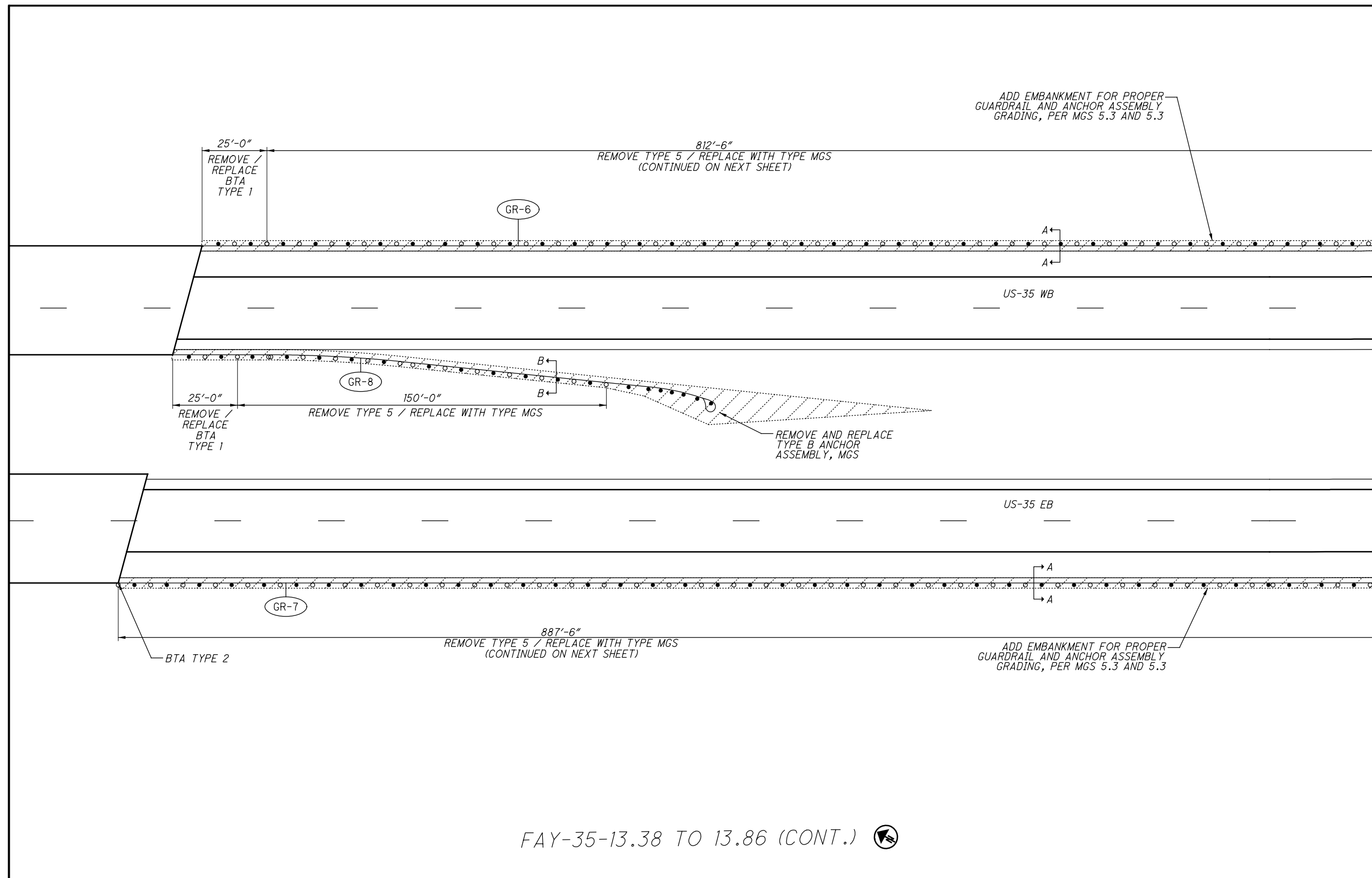
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SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.  
FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

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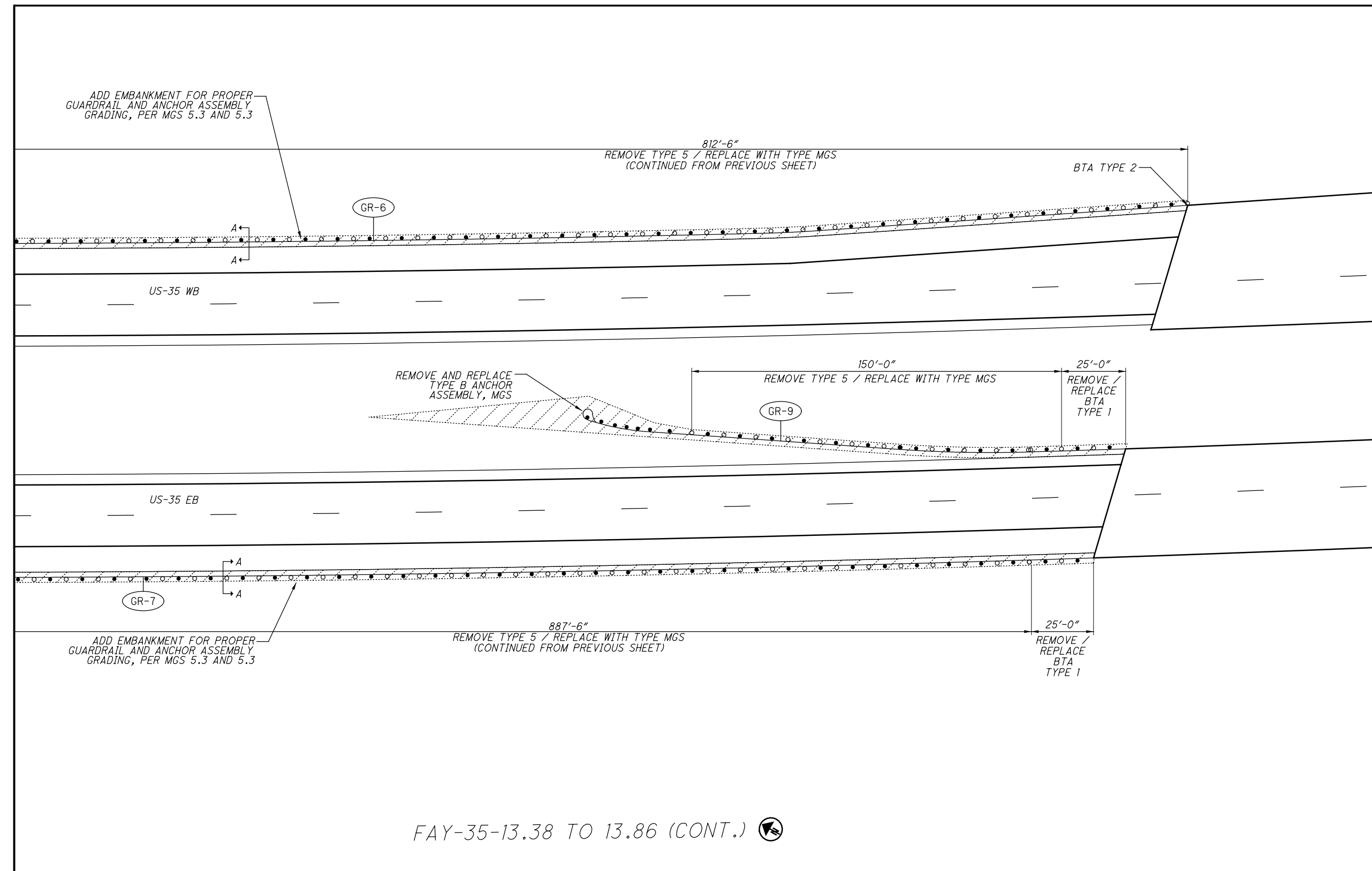
FAY-35-13.38 TO 13.86 (CONT.)

SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.

FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

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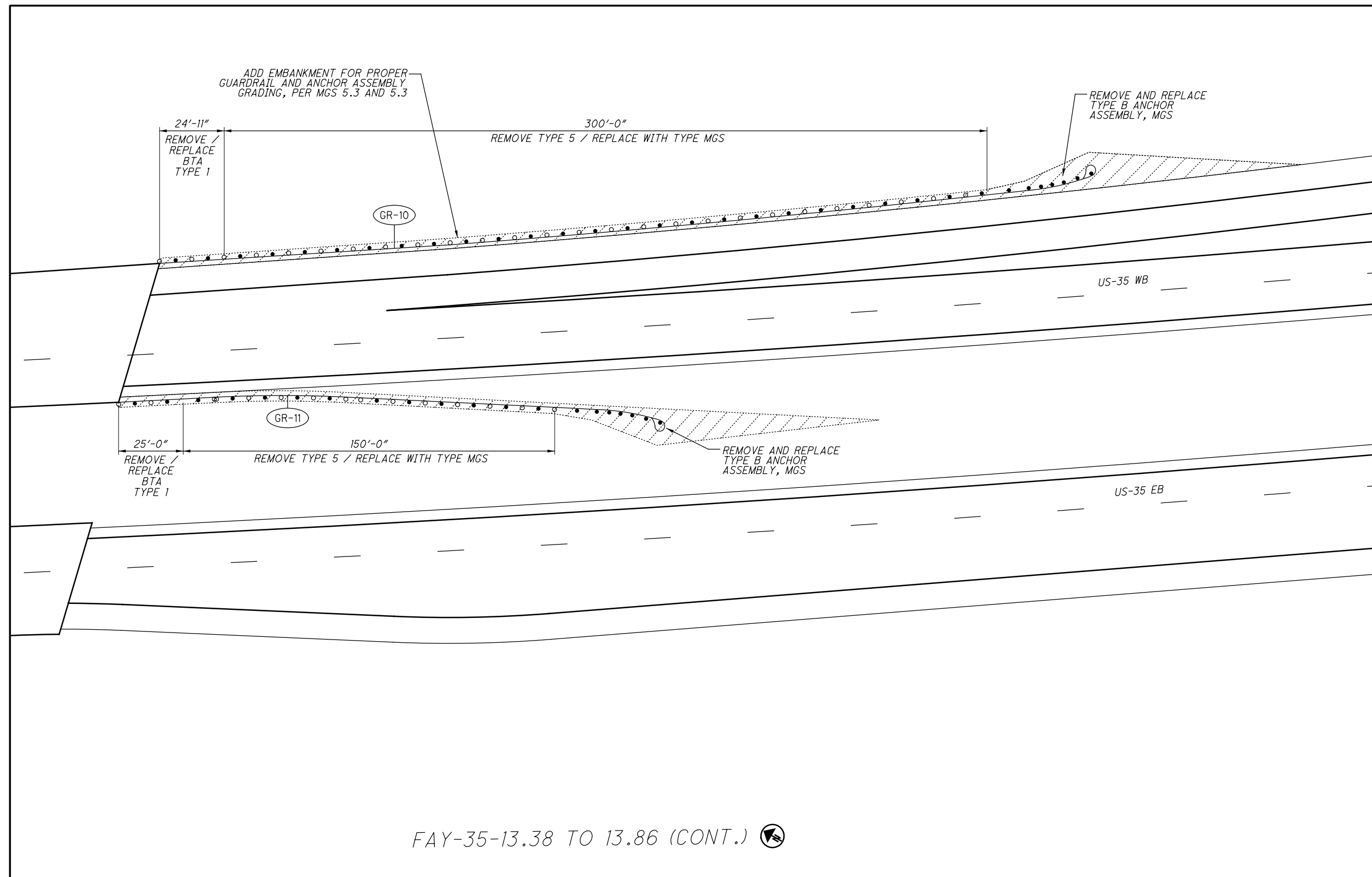



FAY-35-13.38 TO 13.86 (CONT.) ↗

SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.  
FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

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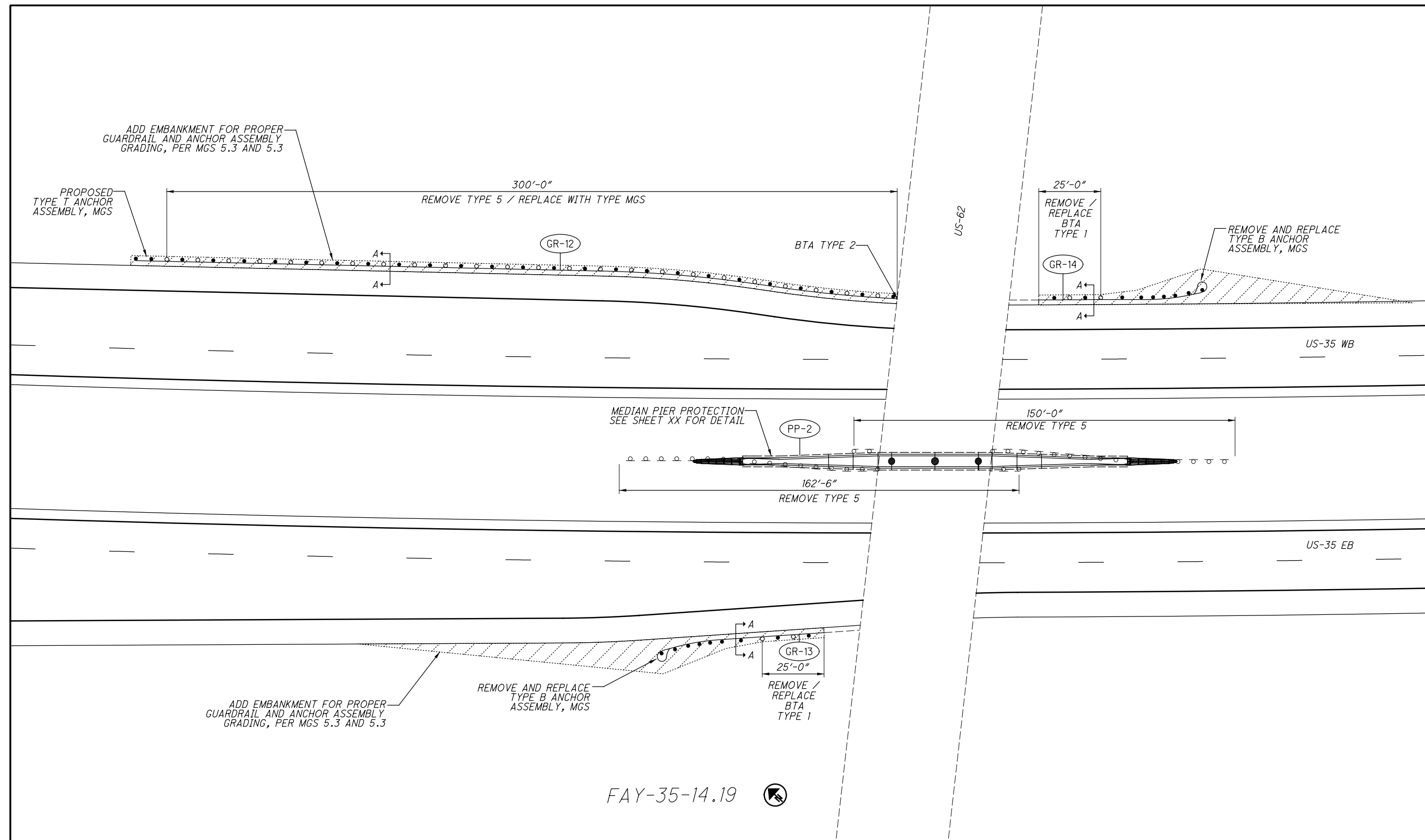
FAY-35-13.38 TO 13.86 (CONT.) 

SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.

FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

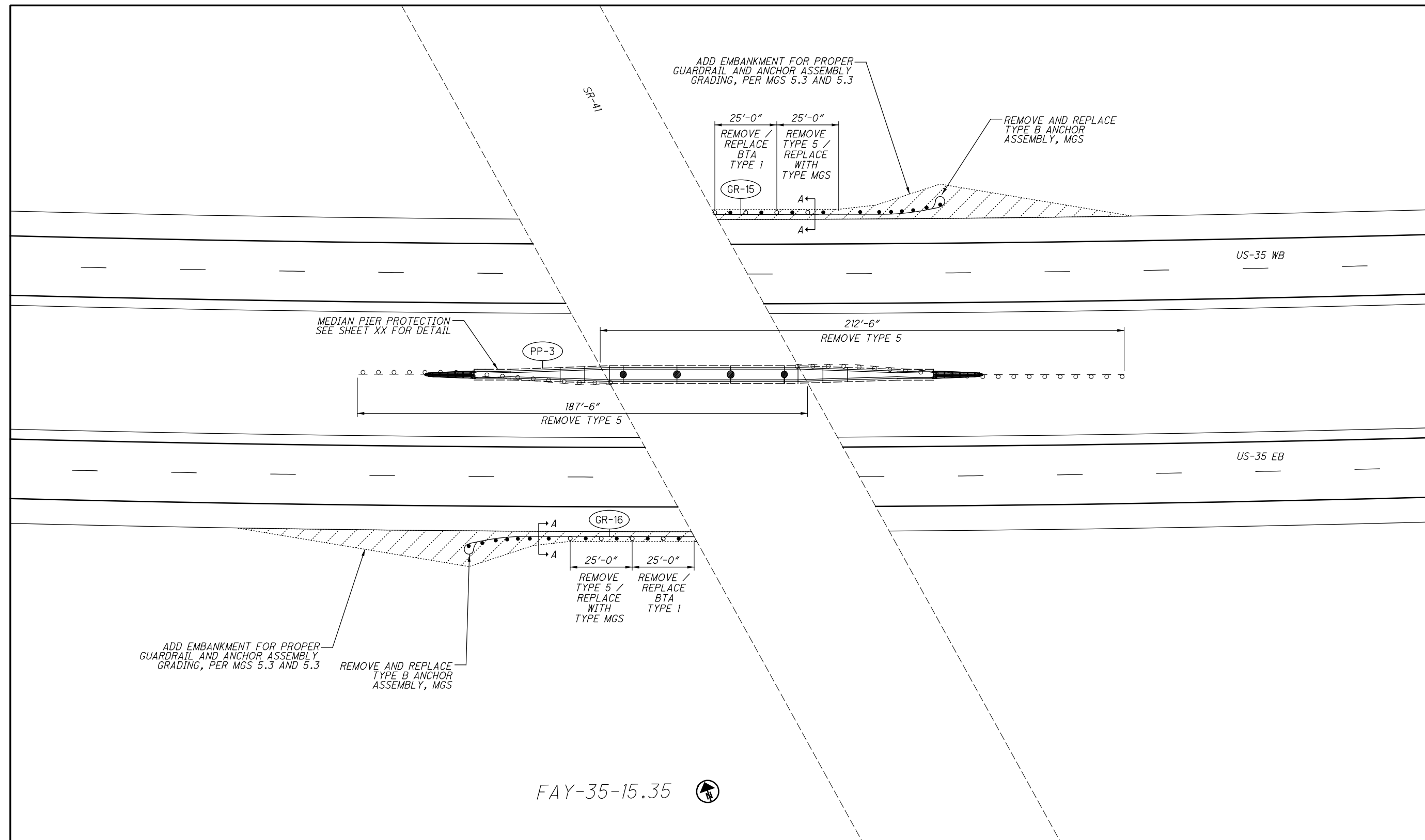
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SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.  
FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

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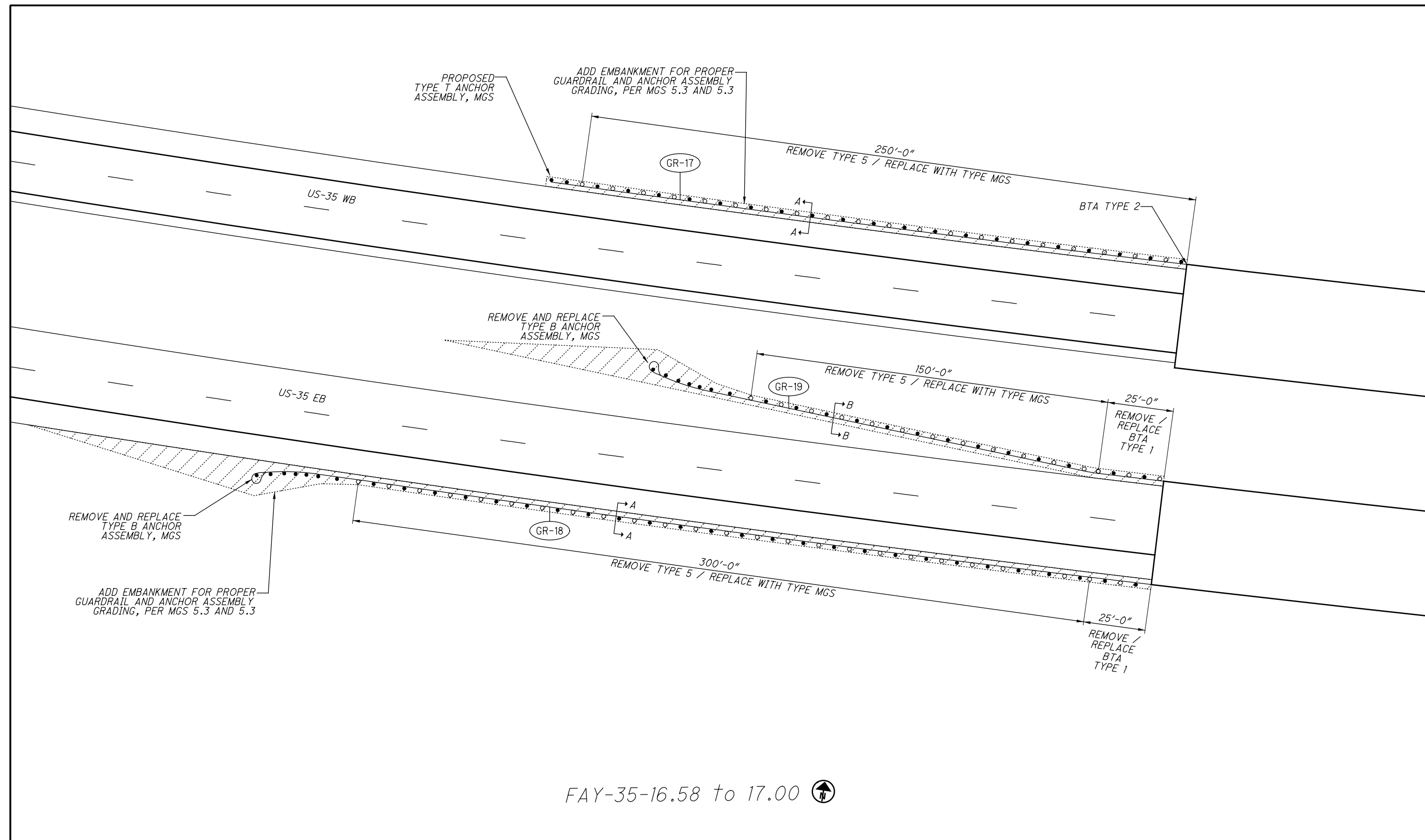
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SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.  
FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

GUARDRAIL PLAN

FAY-35-4.52



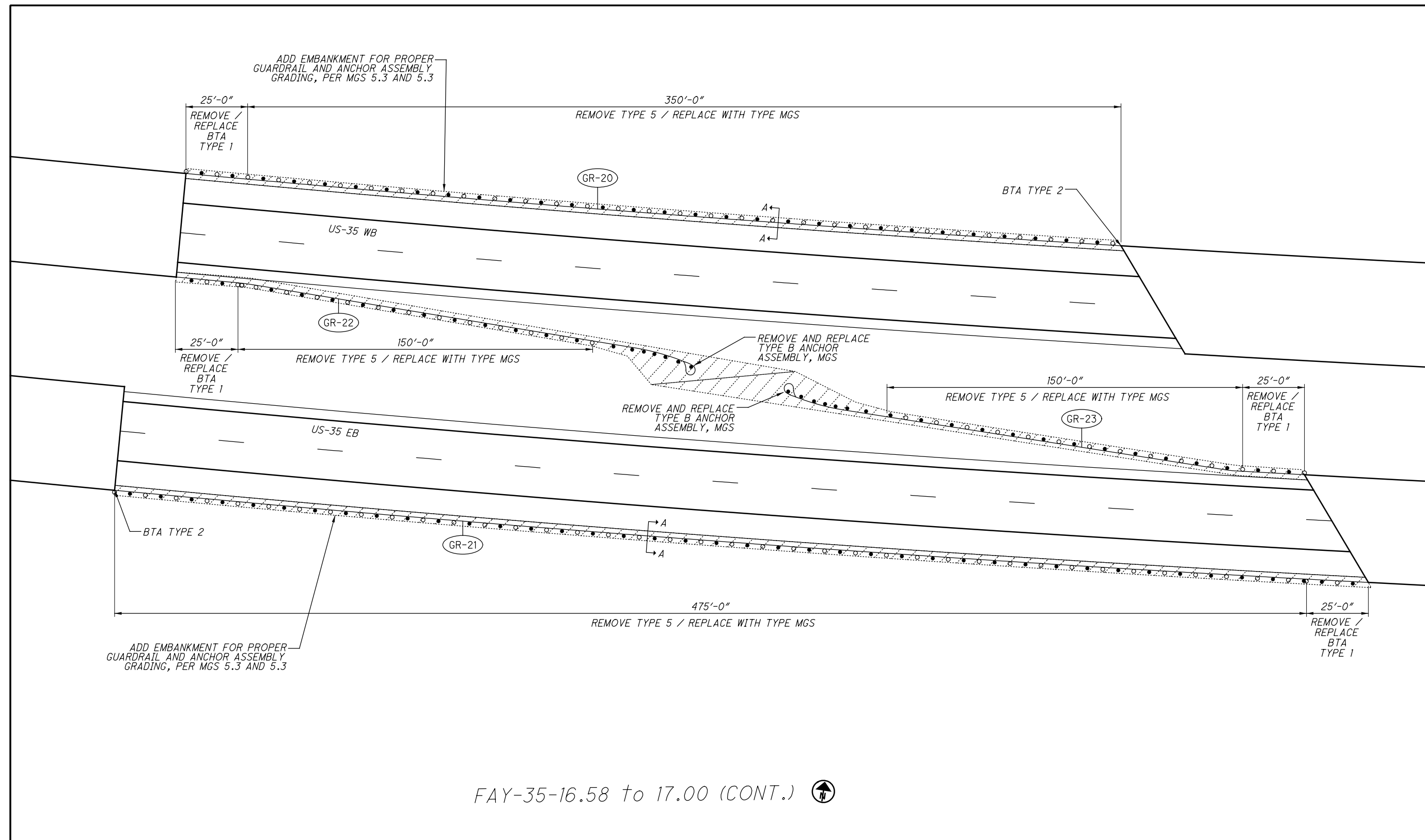
FAY-35-16.58 to 17.00


SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.  
FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

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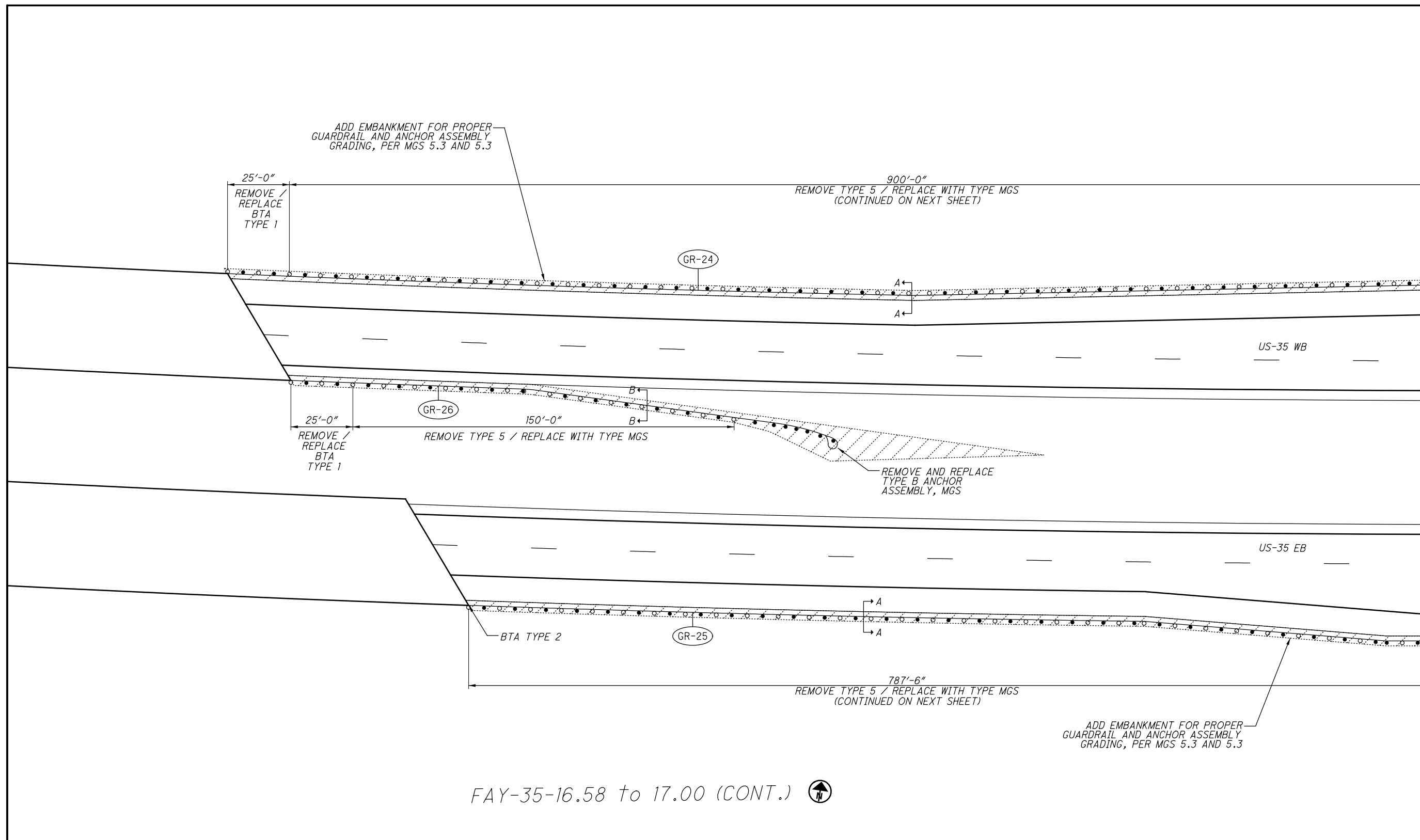


FAY-35-16.58 to 17.00 (CONT.) 

SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.  
FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

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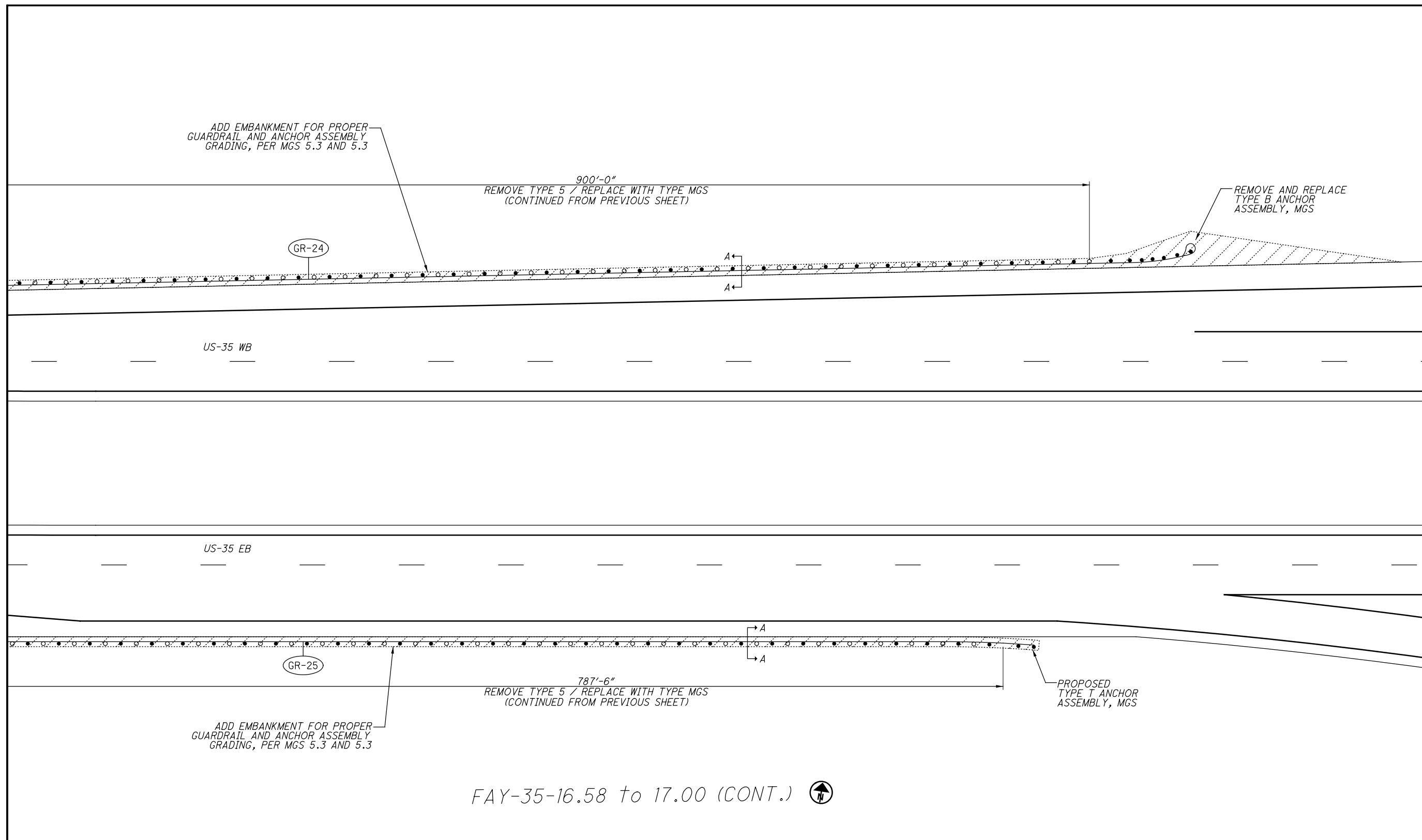


SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.

FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

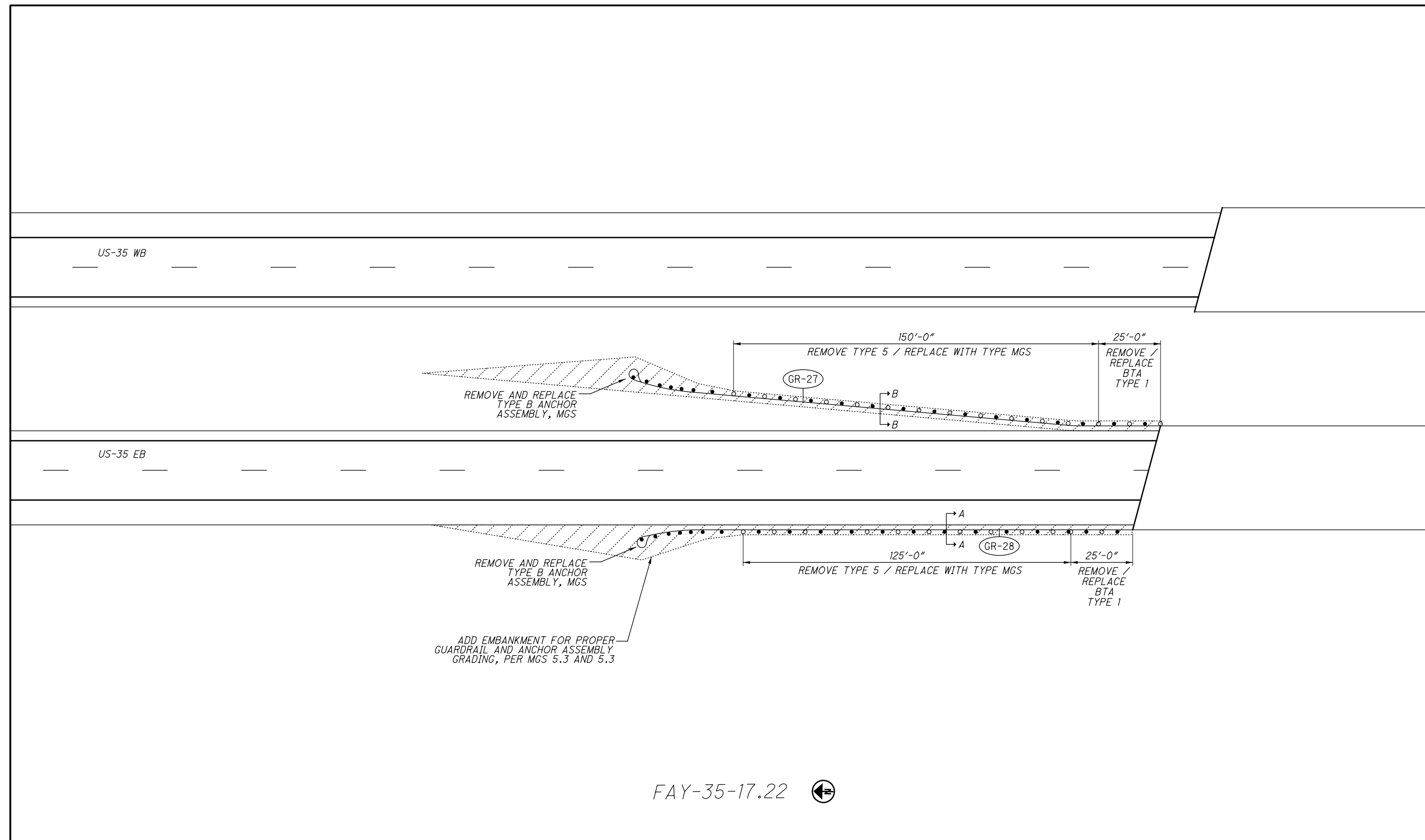
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SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.  
FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

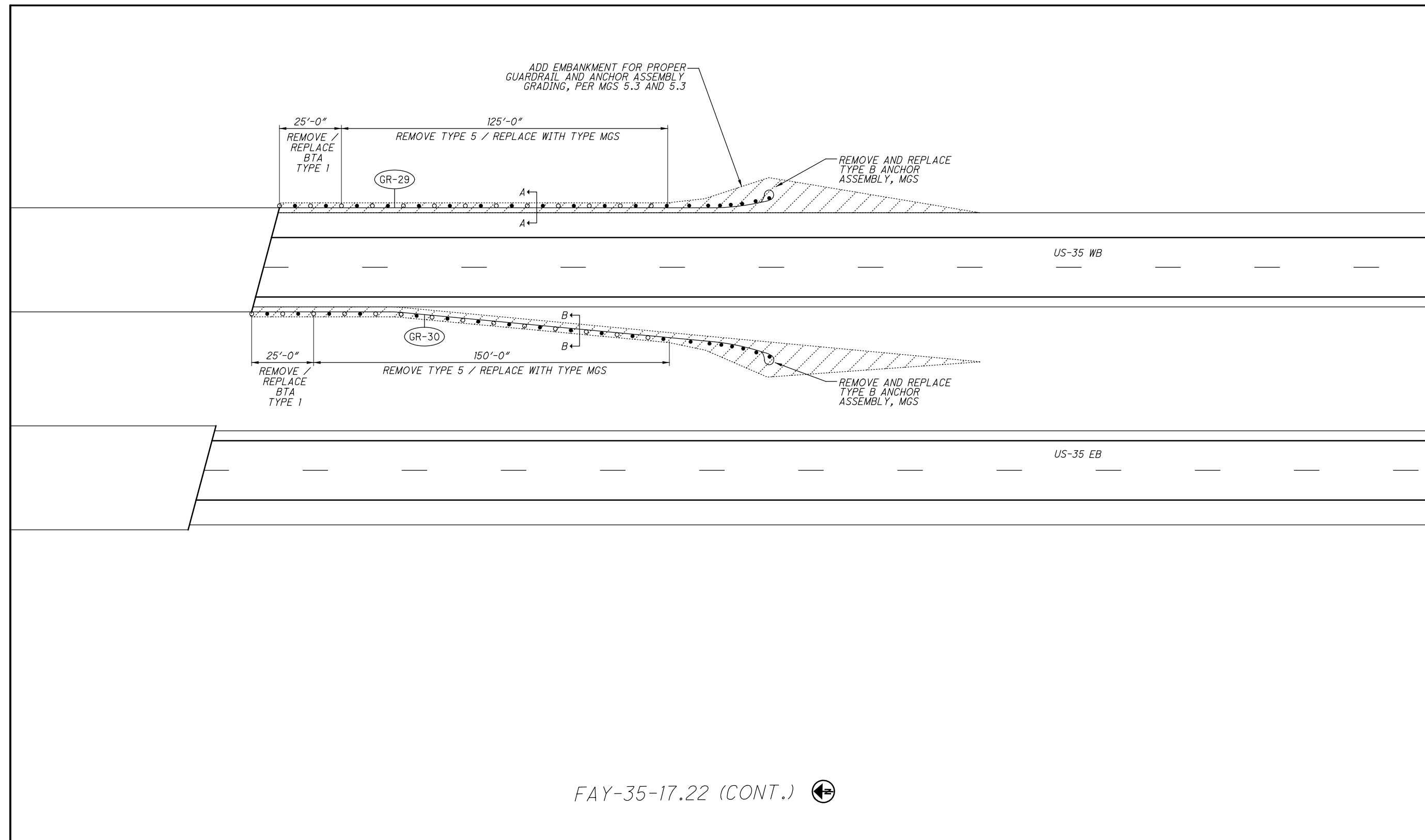
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SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.  
FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

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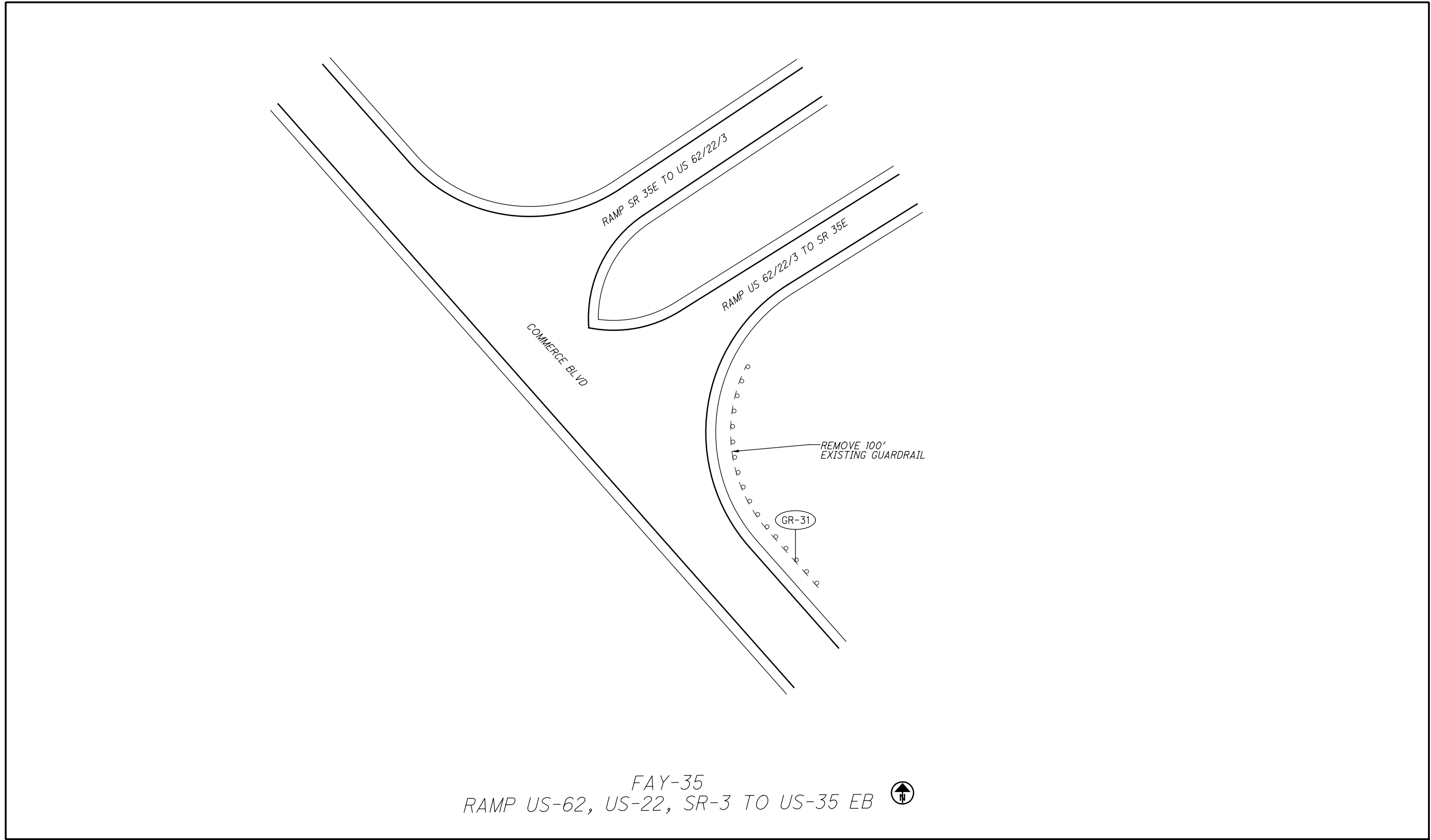
FAY-35-17.22 (CONT.)

SEE TYPICAL DETAILS FOR SECTIONS A-A AND B-B.

PROPOSED GUARDRAIL OFFSETS TO REMAIN THE SAME UNLESS OTHERWISE NOTED IN THE PLAN.  
FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

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FAY-35  
RAMP US-62, US-22, SR-3 TO US-35 EB



FOR QUANTITIES, SEE GUARDRAIL SUBSUMMARY ON SHEET 21.

CALCULATED
RAM
CHECKED
XXX

**GUARDRAIL PLAN**

**FAY-35-4.52**

49
63

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LOCATION							620	644														REMARKS					
L O C A T I O N	C O U N T Y	R O U T E	B E G I N G M	E N D I N G M	L E N G T H	L E N G T H	D E L I N E A T O R, M I S C. R E B O U N D A B L E T U B U L A R P Y L O N	E D G E L I N E, 6"		L A N E L I N E, 6"	C H A N N E L I Z I N G L I N E, 8"	C H A N N E L I Z I N G L I N E, 12"	S T O P L I N E	C R O S S W A L K L I N E	T R A N S V E R S E/ D I A G O N A L L I N E		R A I L R O A D S Y M B O L M A R K I N G	S C H O O L S Y M B O L M A R K I N G	L A N E A R R O W					W O R D O N P A V E M E N T "O N L Y"			
								W	Y						W	Y			LT	LT	THRU		RT		RT		
								FT	MI						EACH	MILE			MILE	MILE	FT		FT		FT	FT	FT
1	FAY	35	4.520	7.510		2.99		5.98	5.98	5.98																MAINLINE	
			4.520		1700		9	0.32	0.32			680													RAMP C (SR-435 TO US-35 EB)		
		AT SR-435	4.520		1150		6	0.22	0.22			630	40												RAMP D (US-35 WB TO SR 425)		
2	FAY	35	11.970	17.450		5.48		10.96	10.96	10.96															MAINLINE		
			14.100		1160		6	0.22	0.22			350													RAMP A (SR-3, US-22, AND US-62 TO US-35 WB)		
		AT SR-3, US-22, AND US-62	14.100		930		5	0.18	0.18			410	55												RAMP B (US-35 WB TO SR-3, US-22, AND US-62)		
			14.100		760		4	0.14	0.14			430	45												RAMP C (US-35 EB TO SR-3, US-22, AND US-62)		
			14.100		1700		9	0.32	0.32			630													RAMP D (SR-3, US-22, AND US-62 TO US-35 EB)		
			15.370		730		4	0.14	0.14			545													RAMP A (SR-41 TO US-35 WB)		
		AT SR-41	15.370		1150		6	0.22	0.22			480	45												RAMP B (US-35 WB TO SR-41)		
			15.370		1180		6	0.22	0.22			500	45												RAMP C (US-35 EB TO SR-41)		
			15.370		820		5	0.16	0.16			560													RAMP D (SR-41 TO US-35 EB)		
			17.230		1080		6	0.20	0.20			550													RAMP A (SR-753 TO US-35 WB)		
		AT SR-753	17.230		840		5	0.16	0.16			350	30												RAMP B (US-35 WB TO SR-753)		
			17.230		880		5	0.17	0.17			370	45												RAMP C (US-35 EB TO SR-753)		
			17.230		1170		6	0.22	0.22			530													RAMP D (SR-753 TO US-35 EB)		
								19.83	19.83																		
TOTAL CARRIED TO GENERAL SUMMARY								82	39.66	16.94		7015	305														

CALCULATED  
RAM  
CHECKED  
XXX

TRAFFIC CONTROL SUBSUMMARY

FAY - 35 - 4.52

DETAIL	STANDARD DRAWING TC-65.10
1	EDGE LINE
2	CHANNELIZING LINE
3	LANE LINE
4	CENTER LINE

DETAIL	STANDARD DRAWING TC-65.11
5	ENTRANCE RAMP
6	EXIT RAMP
7	4 LANE DIVIDED TO 2 LANE TRANSITION
8	4 LANE UNDIVIDED TO 2 LANE TRANSITION
9	MULTILANE DIVIDED HIGHWAY

DETAIL	STANDARD DRAWING TC-65.11
10	APPROACH W/ LEFT TURN LANE
11	STOP APPROACH
12	TWO WAY LEFT TURN LANE
13	ONE LANE BRIDGE
14	HORIZONTAL CURVE

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LOCATION								REFLECTOR TYPE										621		REMARKS
L O C A T I O N	C O U N T Y	R O U T E	B E G I N I N G M	E N D I N G M	L E N G T H F T	S I D E	D I R E C T I O N	ONE WAY				TWO WAY						RAISED PAVEMENT MARKER REMOVED	RPM	
								WHITE		YELLOW	WHITE RED		YELLOW RED	YELLOW YELLOW						
								RIGHT EDGE LINE	LANE LINE	LEFT EDGE LINE	RIGHT EDGE LINE	CHANNELIZING LINE	LANE LINE	LEFT EDGE LINE	CENTER LINE					
								40'	80'	120'	80'	40'	80'	40'	80'	80'	80'			20'
1	FAY	35	4.520	7.510	15787	EB/WB	9			264								264	264	MAINLINE
			4.520		1700		5						17					39	39	RAMP C (SR-435 TO US-35 EB)
			4.520		1150		6						16					31	31	RAMP D (US-35 WB TO SR 425)
2	FAY	35	11.970	17.450	28934	EB/WB	3			484								484	484	MAINLINE
			14.100		1160		5						9					24	24	RAMP A (SR-3, US-22, AND US-62 TO US-35 WB)
			14.100		930		6						11					23	23	RAMP B (US-35 WB TO SR-3, US-22, AND US-62)
			14.100		760		6						11					21	21	RAMP C (US-35 EB TO SR-3, US-22, AND US-62)
			14.100		1700		5						9					31	31	RAMP D (SR-3, US-22, AND US-62 TO US-35 EB)
			15.370		730		5						14					24	24	RAMP A (SR-41 TO US-35 WB)
			15.370		1150		6						12					27	27	RAMP B (US-35 WB TO SR-41)
			15.370		1180		6						13					28	28	RAMP C (US-35 EB TO SR-41)
			15.370		820		5						14					25	25	RAMP D (SR-41 TO US-35 EB)
			17.230		1080		5						14					28	28	RAMP A (SR-753 TO US-35 WB)
			17.230		840		6						9					20	20	RAMP B (US-35 WB TO SR-753)
			17.230		880		6						10					21	21	RAMP C (US-35 EB TO SR-753)
			17.230		1170		5						14					29	29	RAMP D (SR-753 TO US-35 EB)
TOTALS CARRIED TO GENERAL SUMMARY															1,119	1,119				

CALCULATED RAM CHECKED XXX  
**RAISED PAVEMENT MARKER SUBSUMMARY**  
**FAY-35-4.52**  
51  
63



**SUPPLEMENTAL DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS:**

REFER TO THE FOLLOWING SUPPLEMENTAL SPECIFICATIONS

846 DATE/REVISED 4/17/2015  
848 DATE/REVISED 1/20/2017

**DESIGN SPECIFICATIONS:**

THIS STRUCTURE CONFORMS TO THE 8TH EDITION OF THE "LRFD BRIDGE DESIGN SPECIFICATIONS" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 2019 AND THE ODOT BRIDGE DESIGN MANUAL, 2019

**DECK PROTECTION METHOD:**

NEW SDC OVERLAY 1.75" THICKNESS FOR STRUCTURE SFN 2400642

NEW SDC OVERLAY 1.75" THICKNESS FOR STRUCTURE SFN 2400650

**EXISTING STRUCTURE VERIFICATION:**

EXISTING STRUCTURE VERIFICATION: DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND THE PROPOSED WORK, BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05, 105.02, AND 513.04. BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE, HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS WHICH HAVE BEEN VERIFIED IN THE FIELD. ALL REMOVED MATERIALS EXCEPT AS NOTED ELSEWHERE IN THE PLANS SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED BY CONTRACTOR FROM THE JOB SITE.

**ITEM 516 JOINT SEALER, AS PER PLAN:**

UPON COMPLETION OF THE RIGID OVERLAY, THE CONTRACTOR SHALL SAW CUT THE INTERFACE BETWEEN THE APPROACH PAVEMENT AND OVERLAY AN AREA OF 1" WIDE BY THE DEPTH OF THE NEW SDC OVERLAY AND FILL THIS AREA WITH HOT APPLIED JOINT SEALER 705.04.

**DOCUMENTATION OF VARIABLE DEPTH:**

PRIOR TO POURING PROPOSED SDC OVERLAYS, THE PROJECT ENGINEER MUST DOCUMENT THE APPROXIMATE VARIABLE DEPTH LOCATIONS ON THE DECK AND TAKE PICTURES OF THESE LOCATIONS AND OTHER SIGNIFICANT FINDINGS. ADDITIONALLY, DOCUMENT THE AS BUILT OVERLAY THICKNESS AND TOTAL AMOUNT OF VARIABLE DEPTH USED.

**ITEM 848 SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN**

**ITEM 848 SURFACE PREPARATION USING HYDRODEMOLITION, AS PER PLAN**

**ITEM 848 SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN**

**ITEM 848 HAND CHIPPING, AS PER PLAN**

**ITEM 848 TEST SLAB, AS PER PLAN**

THESE ITEMS SHALL BE PERFORMED PER SUPPLEMENTAL SPECIFICATION 848, "BRIDGE DECK REPAIR AND OVERLAY WITH CONCRETE USING HYDRODEMOLITION," ON STRUCTURE FAY-35-17.21 AND ITS APPROACH SLABS WITH THE FOLLOWING REVISIONS:

OVERLAY OF THE APPROACH SLAB IS INCLUDED WITH THIS PROJECT. PAYMENT FOR OVERLAYING THE APPROACH SLAB SHALL BE INCLUDED WITH THE 848 ITEMS ACCORDINGLY. SEE SHEET 49/49 FOR DETAILS.

A) THE THICKNESS OF THE CONCRETE DECK REMOVED, PROPOSED OVERLAY, AND THE DEPTH OF HYDRODEMOLITION OF CONCRETE DECK SHALL BE AS SPECIFIED IN THE PLANS.

B) THE THICKNESS OF CONCRETE REMOVED FROM APPROACH SLABS, AND DEPTH OF HYDRODEMOLITION OF APPROACH SLABS SHALL BE 1/2 INCH.

C) THE THICKNESS OF PROPOSED SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION SHALL BE SPECIFIED IN THE PLANS.

D) CONSTRUCTION JOINTS WILL NOT BE PERMITTED IN THE WHEEL LINE.

E) TWO TEST SLABS WILL BE REQUIRED IN ACCORDANCE WITH SS848 IF A PERIOD OF 30 DAYS OR MORE HAS ELAPSED SINCE THE POURING OF THE TEST SLABS OR ANY OVERLAY OPERATION AS PART OF THIS PROJECT.

F) THE REMOVAL OPERATIONS SHALL NOT BEGIN IF SUSTAINED RAINS (5 HOURS OR MORE WITH BREAKS BETWEEN SHOWERS LESS THAN 1/2 HOURS) ARE PREDICTED WITHIN 48 HOURS OF COMMENCEMENT.

G) THE FINAL SOUNDING MAY TAKE PLACE WITHIN 24 HOURS OF RAIN, AND THE DECK DOES NOT HAVE TO BE COMPLETELY DRY.

H) HAND CHIPPING IS FOR THE PURPOSE OF CHIPPING AREAS WHERE THE HYDRODEMOLITION MACHINE DOES NOT HAVE ACCESS. HAND CHIPPING SHALL ALSO BE USED TO REMOVE AREAS IDENTIFIED AS UNSOUND DURING THE FINAL SOUNDING.

I) THE WET CURE TIME IS REDUCED FROM 72 HOURS TO 24 HOURS OR UNTIL A BEAM BREAK OF 600 PSI IS ACHIEVED, WHICHEVER IS GREATER. AFTER THE 24 HOUR WET CURE, THE FINISHED OVERLAY SURFACE SHALL BE CURED BY SPRAYING A UNIFORM APPLICATION OF THE CURING MATERIAL 705.07, TYPE 1 OR 1D, AS PER CMS 511.14 METHOD B MEMBRANE CURING. THE DECK SURFACE MUST BE DRY PRIOR TO PLACEMENT OF THE CURING MATERIAL.

**ITEM 848 CONTINUED**

J) TRAFFIC WILL NOT BE PERMITTED ON THE FINISHED OVERLAY SURFACE UNTIL AFTER THE COMPLETION OF THE 24 HOUR WET CURE, AND AFTER TWO TEST BEAMS HAVE ATTAINED AN AVERAGE MODULUS OF RUPTURE OF 600 PSI.

K) THE OVERLAY SURFACE EVAPORATION RATE REQUIREMENTS ARE IN EFFECT FROM 9:30 AM TO 11:00 PM. THEY ARE NOT IN EFFECT FROM 11:00 PM TO 9:30 AM.

L) FOR EACH POUR, THE CONTRACTOR SHALL PROVIDE ENOUGH MATERIAL FOR TWO BEAM BREAKS EACH AT 12 HOURS, 24 HOURS, 36 HOURS, AND 48 HOURS. THE DEPARTMENT WILL PERFORM THE BEAM BREAK TESTS AND DOCUMENT THE TIME OF POUR, THE TIME OF THE BEAM BREAK TESTS, AND THE MODULUS OF RUPTURE OF EACH.

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DESIGNED		DRAWN	REVIEWED	DATE	DESIGN AGENCY
XXX	XXX	XXX	KRF	09/27/19	ODOT DEPARTMENT OF TRANSPORTATION DISTRICT 6
CHECKED	REVISED	STRUCTURE FILE NUMBER			
KRF	XXX	XXX			
<b>STRUCTURE NOTES</b>					
BRIDGE NO. FAY-35-17.21 OVER SR 753					
<b>FAY-35-17.21</b>					
<b>PID No. 107802</b>					
1 / 12					
52					
63					

**ITEM 409-SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS**

**1) DESCRIPTION:**

THIS WORK SHALL CONSIST OF CUTTING AND SEALING TRANSVERSE JOINTS IN THE NEW ASPHALT CONCRETE OVERLAY OF BRIDGES. ASPHALT CONCRETE JOINTS SHALL BE CONSTRUCTED DIRECTLY OVER, AND IN LINE WITH, THE EXISTING UNDERLYING TRANSVERSE ABUTMENT AND APPROACH SLAB JOINTS.

**2) MATERIALS:**

THE JOINT SEALANT SHALL MEET THE REQUIREMENTS OF ITEM 705.04, JOINT SEALANTS, HOT-POURED, FOR CONCRETE AND ASPHALT PAVEMENTS. ACCEPTABLE ALTERNATE MATERIALS ARE:

A SILICONE SEALANT MEETING FEDERAL SPECIFICATIONS TT-S-001543A CLASS A (ONE-PART SILICONE SEALANTS) AND TT-S-00230C CLASS A (ONE-COMPONENT SEALANTS), SUCH AS THOSE MANUFACTURED BY GENERAL ELECTRIC, SILICONE PRODUCTS DIVISION, 4015 EXECUTIVE PARK DRIVE, CINCINNATI, OHIO 45242 (513-243-1953) OR DOW CORNING, 400 TECHNE CENTER, SUITE 103, MILFORD, OHIO 45150 (513-831-3586); OR SOF-SEAL, A COLD-APPLIED, LOW-MODULUS, TWO-COMPONENT POLY-MERIC COMPOUND HORIZONTAL SEALANT AS MANUFACTURED BY W.R.MEADOWS, INC., P.O. BOX 543, ELGIN, ILLINOIS 60121 (800-342-5976).

**3) CONSTRUCTION DETAILS:**

A) GENERAL: THE CONTRACTOR SHALL CONDUCT HIS OPERATION SO THAT THE CUTTING, CLEANING AND SEALING OF TRANSVERSE JOINTS IS A CONTINUOUS OPERATION THAT WILL BE PERFORMED AS SOON AS PRACTICAL AFTER THE PAVING, BUT NO LATER THAN FOUR (4) DAYS AFTER PLACEMENT OF THE ASPHALT CONCRETE SURFACE COURSE. TRAFFIC SHALL NOT BE ALLOWED TO KNEAD TOGETHER OR DAMAGE JOINT CUT PRIOR TO SEALING.

B) CUTTING OF TRANSVERSE JOINTS: THE CONTRACTOR SHALL SAW OR ROUT TRANSVERSE JOINTS TO THE DIMENSIONS SHOWN IN THE DETAILS ON THIS SHEET. THE CUT JOINTS SHALL LIE DIRECTLY ABOVE EACH TRANSVERSE JOINT.

THE BLADE OR BLADES SHALL BE OF SUCH SIZE THAT THE FULL WIDTH AND DEPTH OF THE CUT CAN BE MADE WITH ONE PASS. DRY OR WET CUTTING WILL BE ALLOWED. JOINTS SHALL EXTEND THE FULL WIDTH OF THE BRIDGE.

C) CLEANING JOINTS: DRY SAWED JOINTS SHALL BE THOROUGHLY CLEANED WITH A SUFFICIENT AMOUNT OF COMPRESSED AIR TO REMOVE ANY DIRT, DUST, OR DELETERIOUS MATTER. WET SAWED JOINTS SHALL BE WASHED CLEAN OF ALL CUTTINGS BY FLUSHING WITH A JET OF WATER AND WITH OTHER TOOLS AS NECESSARY. AFTER FLUSHING, THE JOINT SHALL BE BLOWN OUT WITH COMPRESSED AIR. WHEN THE SURFACES ARE THOROUGHLY CLEAN AND DRY, AND JUST PRIOR TO PLACING THE JOINT SEALER, COMPRESSED AIR HAVING A PRESSURE OF AT LEAST 90 PSI SHALL BE USED TO BLOW OUT THE JOINT AND REMOVE ALL TRACES OF DUST.

IN THE EVENT FRESHLY CUT JOINTS BECOME CONTAMINATED BEFORE THEY ARE SEALED, THEY SHALL BE RE-CLEANED OF ALL FOREIGN MATERIAL BY HIGH PRESSURE WATER JET.

D) SEALING JOINTS: THE JOINT SHALL BE THOROUGHLY DRY WHEN THE SEALANT IS PLACED. AFTER CLEANING AND DRYING, A BOND-BREAKER MATERIAL SHALL BE APPLIED TO THE BOTTOM OF THE GROOVE.

HOT-POURED JOINT SEALANT MATERIAL SHALL BE HEATED IN A KETTLE OR MELTER CONSTRUCTED AS A DOUBLE BOILER, WITH THE SPACE BETWEEN THE INNER AND OUTER SHELLS FILLED WITH OIL OR OTHER HEAT TRANSFER MEDIUM. POSITIVE TEMPERATURE CONTROL AND MECHANICAL AGITATION SHALL BE PROVIDED. HEATING MUST BE IN STRICT ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION. JOINT SEALER MATERIAL SHALL NEVER BE KEPT HEATED AT THE POURING TEMPERATURE FOR MORE THAN FOUR (4) HOURS AND SHALL NEVER BE REHEATED. SEALER LEFT IN THE APPLICATOR AT THE END OF A DAY'S WORK SHALL NOT BE USED.

HOT-POURED SEALANT SHALL BE APPLIED IMMEDIATELY THROUGH A NOZZLE, WHICH MUST PROJECT INTO THE SAWED JOINT, FILLING FROM THE BOTTOM UP. THE SEALANT SHALL COMPLETELY FILL THE JOINT IN SUCH A MANNER THAT, AFTER COOLING, THE LEVEL OF THE SEALANT WILL NOT BE HIGHER THAN 1/8" BELOW THE PAVEMENT SURFACE. ANY DEPRESSION IN THE COOLED SEAL GREATER THAN 1/4" SHALL BE BROUGHT UP TO THE SPECIFIED LIMIT BY FURTHER ADDITION OF HOT-POURED SEALANT. CARE SHALL BE TAKEN IN THE SEALING OF THE JOINTS SO THAT THE FINAL APPEARANCE WILL PRESENT A NEAT FINE LINE.

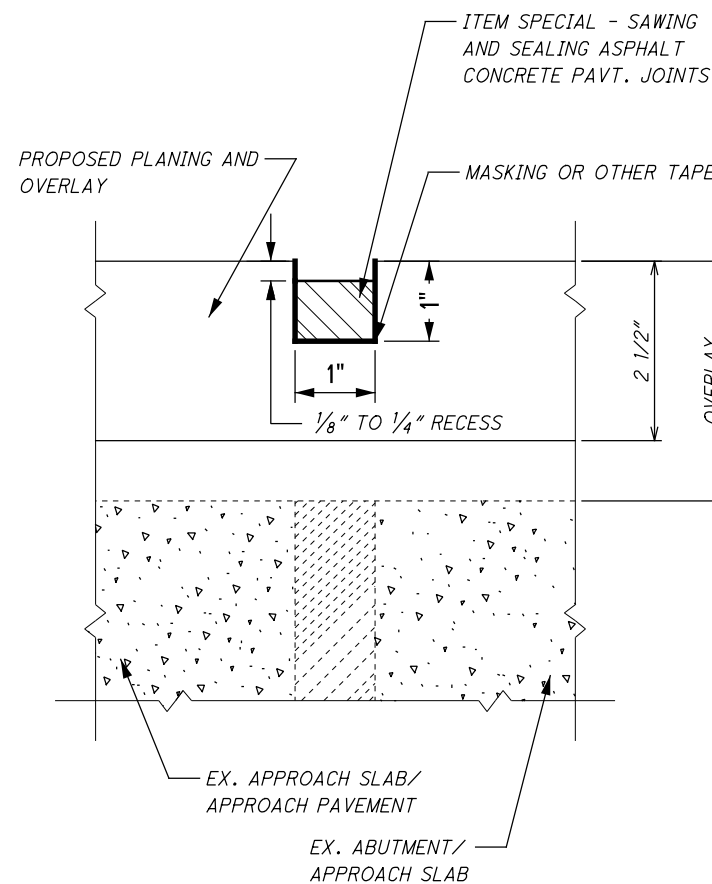
THE COLD APPLIED SEALANT MATERIALS (POLYURETHANE, SILICONE, AND POLYMERIC COMPOUNDS) SHALL BE INSTALLED AS PER MANUFACTURERS' RECOMMENDATIONS, EXCEPT AS MODIFIED BY THIS DRAWING. THE SEALANT SHALL BE INSTALLED WHEN THE AMBIENT TEMPERATURE IS 40 DEGREES F OR HIGHER. TRAFFIC SHALL NOT BE ALLOWED ON THE JOINT FOR ONE HOUR AFTER APPLICATION OF THE SEALANT.

**4) METHOD OF MEASUREMENT:**

THE QUANTITY TO BE PAID FOR UNDER THIS ITEM WILL BE THE NUMBER OF LINEAR FEET OF JOINTS SAWED AND SEALED AS PER THE ABOVE REQUIREMENTS.

**5) BASIS OF PAYMENT:**

THE UNIT PRICE PER LINEAR FOOT FOR ITEM 409 - "SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS" SHALL INCLUDE THE COST OF ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK, INCLUDING THE FURNISHING AND PLACING OF THE JOINT SEALER MATERIAL.



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DESIGN AGENCY  
THE OHIO DEPARTMENT OF  
TRANSPORTATION DISTRICT 6

REVIEWED DATE  
KRF 09/27/19  
STRUCTURE FILE NUMBER  
N/A

DRAWN RAM  
RAM REVISED  
DESIGNED RAM  
RAM CHECKED

STRUCTURE NOTES  
SAWING AND SEALING  
ASPHALT CONCRETE JOINTS

FAY-35-4.52  
PID No. 107802

2 / 12

53  
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SHEET NUMBER							PARTICIPATION		ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
FAY-35-6.43	FAY-35-13.56	FAY-35-13.76	FAY-35-16.66	FAY-35-16.79	FAY-35-17.21	01/STR/PV	02/NHS/BR							
56	57	58	59	60	61-63									
												FAY-35-0643 SFN: 2400359 STRUCTURE OVER MILLEDGEVILLE-OCTA RD (LEFT)		
1,184							1184	512	10050	1184	SY	SEALING CONCRETE SURFACES (NON-EPOXY)		
												FAY-35-0643 SFN: 2400448 STRUCTURE OVER MILLEDGEVILLE-OCTA RD (RIGHT)		
1,184							1184	512	10050	1184	SY	SEALING CONCRETE SURFACES (NON-EPOXY)		
	100							100	409	30001	FT	FAY-35-1356 SFN: 2400405 STRUCTURE OVER CSX RR (LEFT)		
												FAY-35-1356 SFN: 2400413 STRUCTURE OVER CSX RR (RIGHT)		
	100						100	409	30001	100	FT	SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN		
												FAY-35-1376 SFN: 2400499 STRUCTURE OVER US-22 (LEFT)		
			114					114	409	30001	FT	SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN		
												FAY-35-1376 SFN: 2400502 STRUCTURE OVER US-22 (RIGHT)		
			100					100	409	30001	FT	SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN		
												FAY-35-1666 SFN: 2400553 STRUCTURE OVER PAINT CREEK AND CR-58 (LEFT)		
				48				48	409	30001	FT	SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN		
				200				200	519	11101	SF	ITEM SPECIAL - PATCHING CONCRETE STRUCTURE, AS PER PLAN		
				17				17	846	00110	CF	POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM		
												FAY-35-1666 SFN: 2400561 STRUCTURE OVER PAINT CREEK AND CR-58 (RIGHT)		
				48				48	409	30001	FT	SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN		
				200				200	519	11101	SF	ITEM SPECIAL - PATCHING CONCRETE STRUCTURE, AS PER PLAN		
				17				17	846	00110	CF	POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM		
												FAY-35-1666 SFN: 2400596 STRUCTURE OVER ABANDONED RR (LEFT)		
					62			62	409	30001	FT	SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN		
					150			150	519	11101	SF	ITEM SPECIAL - PATCHING CONCRETE STRUCTURE, AS PER PLAN		
					22			22	846	00110	CF	POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM		
												FAY-35-1666 SFN: 2400618 STRUCTURE OVER ABANDONED RR (RIGHT)		
					62			62	409	30001	FT	SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN		
					150			150	519	11101	SF	ITEM SPECIAL - PATCHING CONCRETE STRUCTURE, AS PER PLAN		
					22			22	846	00110	CF	POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM		
												FAY-35-17.21 SFN: 2400642 STRUCTURE OVER SR-753 (LEFT)		
						134		134	848	10201	SY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN 2" THICKNESS		
						779		779	848	10201	SY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN 1 3/4" THICKNESS		
						913		913	848	20001	SY	SURFACE PREPERATION USING HYDRODEMOLITION, AS PER PLAN		
						15		15	848	30201	CY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN		
						25		25	848	50001	SY	HAND CHIPPING, AS PER PLAN		
						LS		LS	848	50101	LS	TEST SLAB, AS PER PLAN		
						779		779	848	50320	SY	EXISTING CONCRETE OVERLAY REMOVED, 1 1/4"		
						25		25	512	10300	SY	SEALING CONCRETE BRIDGE DECKS WITH HMWM RESIN		
						134		134	848	50300	SY	WEARING COURSE REMOVED, ASPHALT		

STRUCTURE QUANTITIES

FAY -35 -4.52  
PID No. 107802

3 / 12

54  
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DESIGN AGENCY  
OHIO DEPARTMENT OF  
TRANSPORTATION DISTRICT 6

DATE  
REVIEWED  
STRUCTURE FILE NUMBER  
N/A

DRAWN  
RAM  
REVISED

DESIGNED  
RAM  
CHECKED

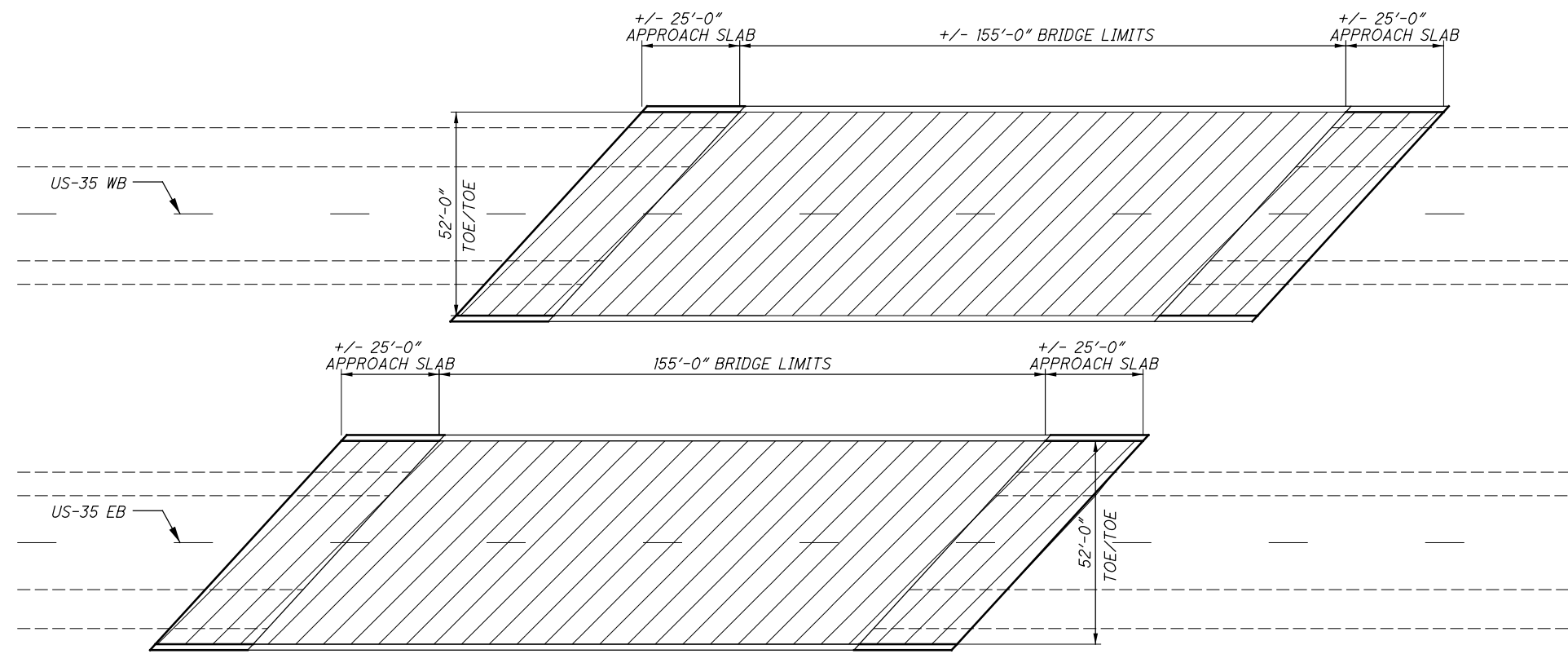
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SHEET NUMBER							PARTICIPATION		ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION
FAY-35-6.43	FAY-35-13.56	FAY-35-13.76	FAY-35-16.66	FAY-35-16.79	FAY-35-17.21	01/STR/PV	02/NHS/BR						
56	57	58	59	60	61-63								
													<b>FAY-35-17.21 SFN: 2400650</b>
													<b>STRUCTURE OVER SR-753 (RIGHT)</b>
						134	134	848	10201	134	SY		SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN 2" THICKNESS
						779	779	848	10201	779	SY		SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN 1 3/4" THICKNESS
						913	913	848	20001	913	SY		SURFACE PREPERATION USING HYDRODEMOLITION, AS PER PLAN
						15	15	848	30201	15	CY		SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN
						25	25	848	50001	25	SY		HAND CHIPPING, AS PER PLAN
						LS	LS	848	50101	LS			TEST SLAB, AS PER PLAN
						779	779	848	50320	779	SY		EXISTING CONCRETE OVERLAY REMOVED, 1 1/4"
						25	25	512	10300	25	SY		SEALING CONCRETE BRIDGE DECKS WITH HMWM RESIN
						134	134	848	50300	134	SY		WEARING COURSE REMOVED, ASPHALT

DESIGN AGENCY ODOT DEPARTMENT OF TRANSPORTATION DISTRICT 6
DATE MM/DD/YY XXX
REVIEWED STRUCTURE FILE NUMBER 00000
DRAWN XXX
REVISIONS REVISED XXX
DESIGNED XXX
CHECKED XXX
<b>STRUCTURE QUANTITIES</b>
<b>FAY -35 -4.52</b>
<b>PID No. 107802</b>
4 / 12
55 63

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**NOTES:**  
 NOTE 1:  
 DO NOT PAVE DECK. SEAL DECK AND APPROACH SLABS (NON-EPOXY). DO NOT SEAL PARAPETS.



**BRIDGE PLAN**  
 FAY-35-6.43 (2400359/2400448)  
 OVER MILLEDGEVILLE-OCTA ROAD

 STRUCTURE WORK

LOCATION				QUANTITIES				REMARKS	STRUCTURE NOTES
COUNTY	ROUTE	STATION	DECK AND APPROACH AREA	512					
				SEALING CONCRETE SURFACES (NON-EPOXY)					
			SQ FT	SY					
FAY	35	6.43 L	10,660.0	1184.0			BRIDGE DECK AND APPROACH SLABS	FAY-35-0643 SFN: 2400359/2400448	
FAY	35	6.43 R	10,660.0	1184.0			BRIDGE DECK AND APPROACH SLABS		
TOTALS CARRIED TO STRUCTURE QUANTITIES				2368.0					

DESIGN AGENCY: THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 6

DATE: 09/27/19

REVIEWED: KRF

STRUCTURE FILE NUMBER: 2400359/2400448

DRAWN: RAM

CHECKED: KRF

DESIGNED: RAM

REVISED: XXX

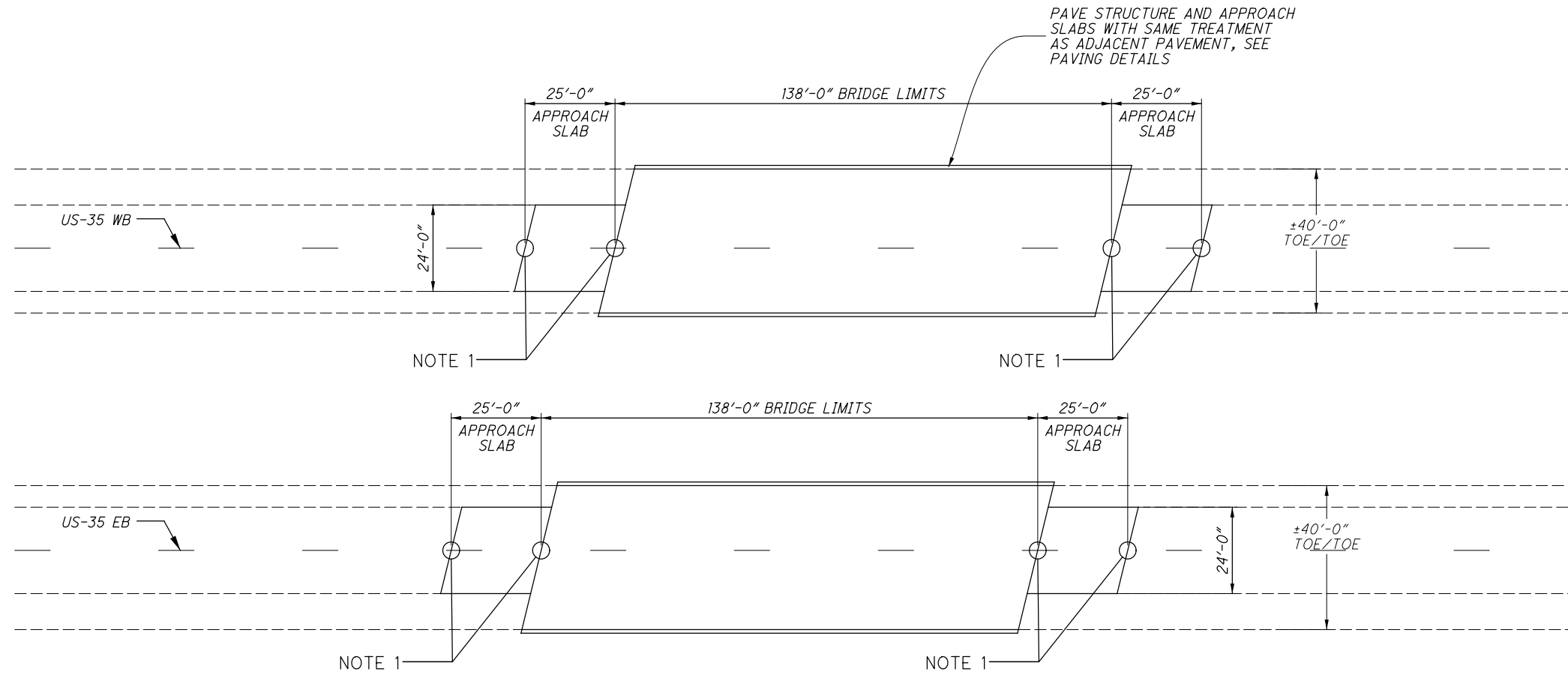
**BRIDGE PLAN**  
 FAY-35-6.43  
 OVER PAINT MILLEDGEVILLE-OCTA RD

**FAY-35-6.43**  
 PID No. 107802

5 / 12

56  
63

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**NOTES:**  
 NOTE 1:  
 SAW AND SEAL ASPHALT  
 CONCRETE JOINTS AT  
 EACH END OF APPROACH  
 SLAB

**BRIDGE PLAN**  
 FAY-35-1356 (2400405/2400413)  
 OVER CSX RAILROAD  
 SKEW: 13°43'15"

LOCATION				QUANTITIES				REMARKS	STRUCTURE NOTES
C	R	S	D						
O	O	L	E						
U	U	M	C						
N	T		K						
T	E		A						
Y			R						
			E						
			A						
			SQ FT						
				409					FAY-35-1356 SFN: 2400405/2400413
					SAWING AND SEALING ASPHALT CONCRETE JOINTS, AS PER PLAN				
				FT					
FAY	35	13.56 L	5,791.0	100				BRIDGE DECK AND APPROACH SLABS	
FAY	35	13.56 R	5,791.0	100				BRIDGE DECK AND APPROACH SLABS	
TOTALS CARRIED TO STRUCTURE QUANTITIES				200					

**BRIDGE PLAN**  
 FAY-35-1356  
 OVER CSX RAILROAD

**FAY-35-4.52**  
 PID No. 107802

6 / 12

57  
63

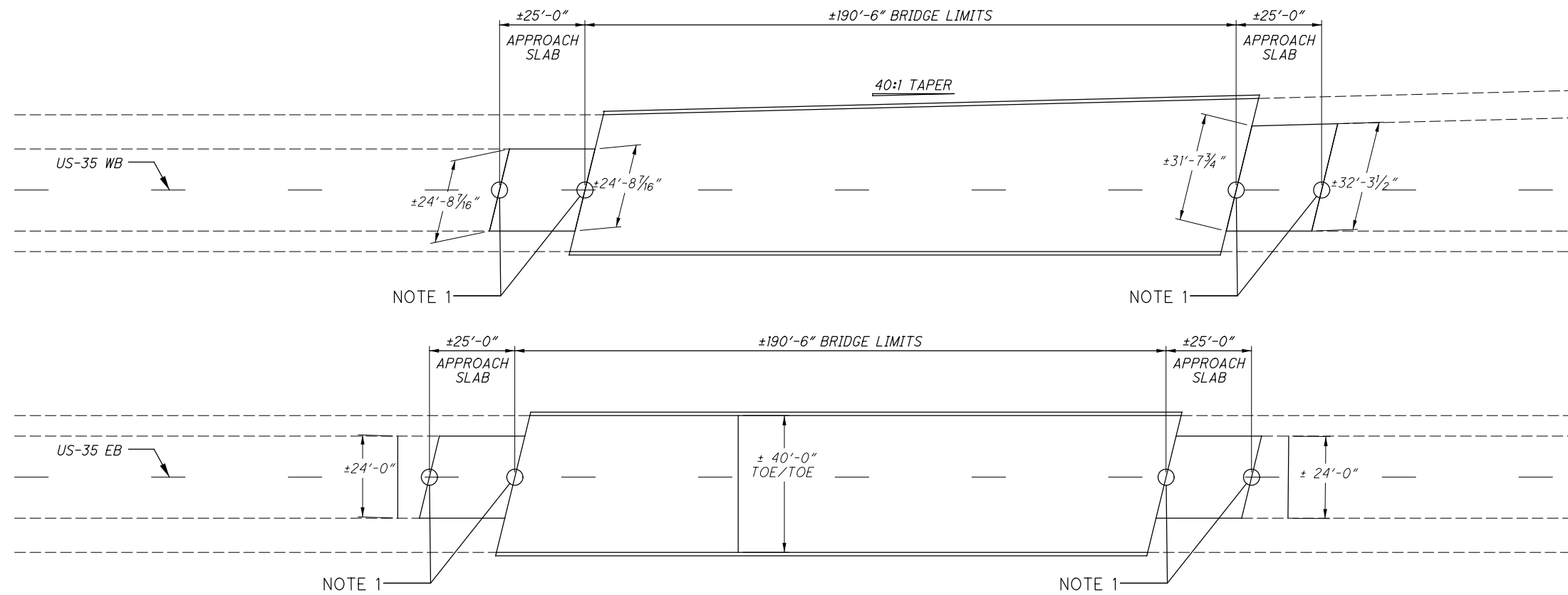
DESIGN AGENCY  
 THE OHIO DEPARTMENT OF  
 TRANSPORTATION DISTRICT 6

REVIEWED  
 KRF 09/27/19  
 STRUCTURE FILE NUMBER  
 2400405/2400413

DRAWN  
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 REVISION  
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 RAM  
 CHECKED  
 KRF

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PAVE STRUCTURE AND APPROACH SLABS WITH SAME TREATMENT AS ADJACENT PAVEMENT. SEE PAVING DETAILS.

**NOTES:**  
NOTE 1:  
SAW AND SEAL ASPHALT CONCRETE JOINTS

**BRIDGE PLAN**  
FAY-35-1376 (2400502/2400499)  
OVER US-22  
SKEW: 16°-12'-28" LF

LOCATION				QUANTITIES				REMARKS	STRUCTURE NOTES
C	R	S	D	409				FAY-35-1376 SFN: 2400499/2400502	
O	O	L	E						
U	U	M	C	SAWING AND SEALING ASPHALT CONCRETE JOINTS, AS PER PLAN					
N	T		K						
T	E		A						
Y			R						
			E						
			A						
			SQ FT						
FAY	35	13.76 L	9,914.0	114					
FAY	35	13.76 R	8,008.0	100					
TOTALS CARRIED TO STRUCTURE QUANTITIES				214					

**BRIDGE PLAN**  
FAY-35-13.76  
OVER US-22

DESIGN AGENCY: THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 6

DATE: 09/27/19  
REVIEWED: KRF  
DRAWN: RAM  
DESIGNED: RAM  
CHECKED: KRF

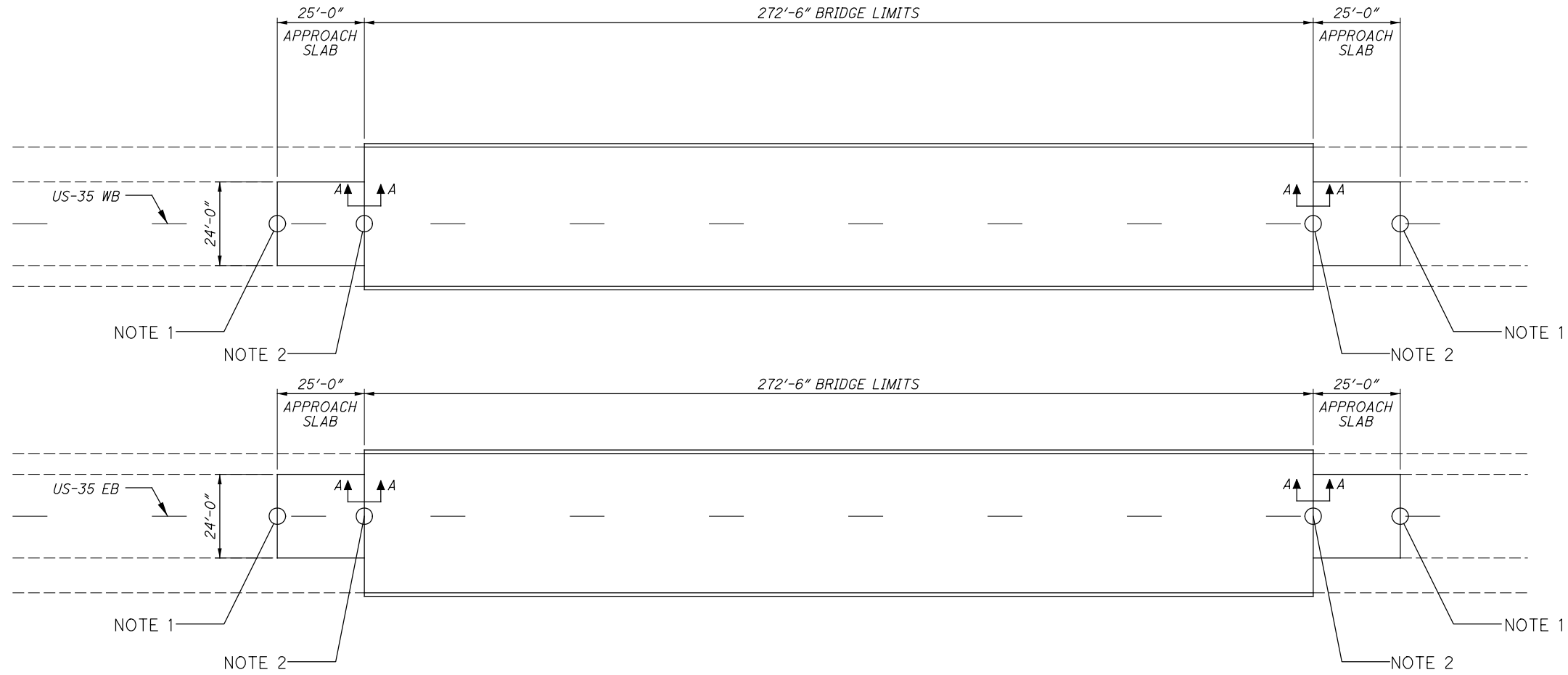
STRUCTURE FILE NUMBER: 2400499/2400502  
REVISED: XXX

**FAY-35-4.52**  
PID No. 107802

7 / 12

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63

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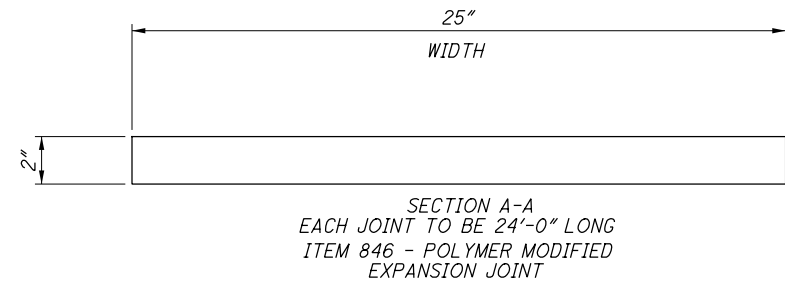


PAVE STRUCTURE AND APPROACH SLABS WITH SAME TREATMENT AS ADJACENT PAVEMENT. SEE PAVING DETAILS.

- NOTES:**
- NOTE 1:  
SAW AND SEAL ASPHALT CONCRETE JOINTS
- NOTE 2:  
INSTALL POLYMER MODIFIED ASPHALT EXPANSION JOINTS.
- NOTE 3:  
ITEM 519 - PATCHING CONCRETE STRUCTURES AS DIRECTED BY ENGINEER ON INSIDE FACE OF PARAPETS ONLY.
- NOTE 4:  
LANE CLOSURE DURING ALLOWABLE HOURS ONLY. SHOULDER MAY REMAIN CLOSED DURING PARAPET PATCHING OPERATIONS.

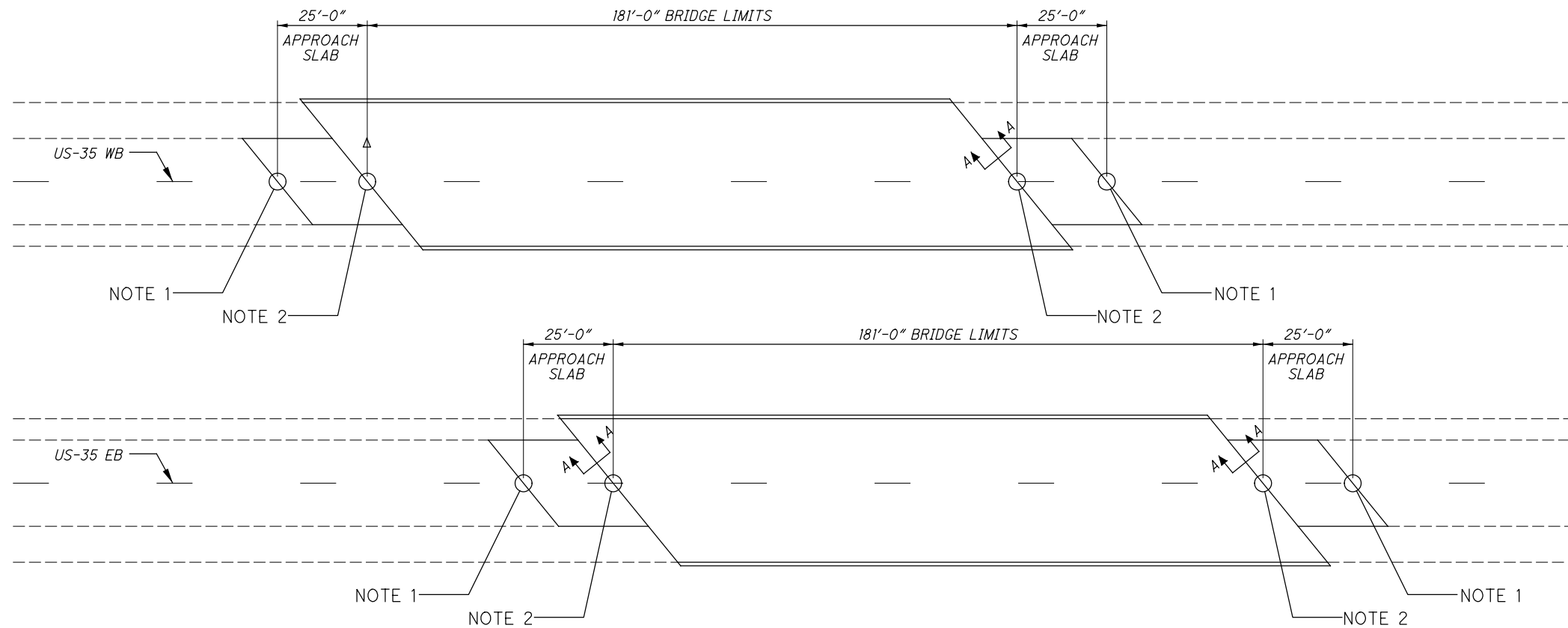
**BRIDGE PLAN**  
FAY-35-16.66 (2400553/2400561)  
OVER PAINT CREEK AND CR-58  
SKEW: NONE

LOCATION				QUANTITIES				REMARKS	STRUCTURE NOTES
C	R	S	D	409	519	846		FAY-35-1666 SFN: 2400553/2400561	
O	O	L	E						
U	T	M	C						
N	E		K		ITEM SPECIAL -	POLYMER			
T			A		PATCHING	MODIFIED			
Y			R		CONCRETE	ASPHALT			
			E		JOINTS, AS PER	EXPANSION JOINT			
			A		PLAN	SYSTEM			
				FT	SF	CF			
				SQ FT					
FAY	35	16.66 L	11,445.0	48					
					200		PATCHING PARAPETS		
						17	EXPANSION JOINT AT APPROACHES		
FAY	35	16.66 R	11,445.0	48					
					200		PATCHING PARAPETS		
						17	EXPANSION JOINT AT APPROACHES		
<b>TOTALS CARRIED TO STRUCTURE QUANTITIES</b>				96	400	34			





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PAVE STRUCTURE AND APPROACH SLABS WITH SAME TREATMENT AS ADJACENT PAVEMENT. SEE PAVING DETAILS.

**NOTES:**

NOTE 1:  
SAW AND SEAL ASPHALT CONCRETE JOINTS

NOTE 2:  
INSTALL POLYMER MODIFIED ASPHALT EXPANSION JOINTS.

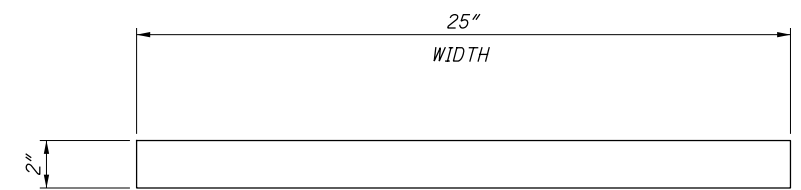
NOTE 3:  
ITEM 519 - PATCHING CONCRETE STRUCTURES AS DIRECTED BY ENGINEER ON INSIDE FACE OF PARAPETS ONLY.

NOTE 4:  
LANE CLOSURE DURING ALLOWABLE HOURS ONLY. SHOULDER MAY REMAIN CLOSED DURING THE PARAPET PATCHING OPERATIONS.

**BRIDGE PLAN**

FAY-35-1679 (2400596/2400618)  
OVER ABANDONED R/R  
SKEW: 39°-12'-23" RF

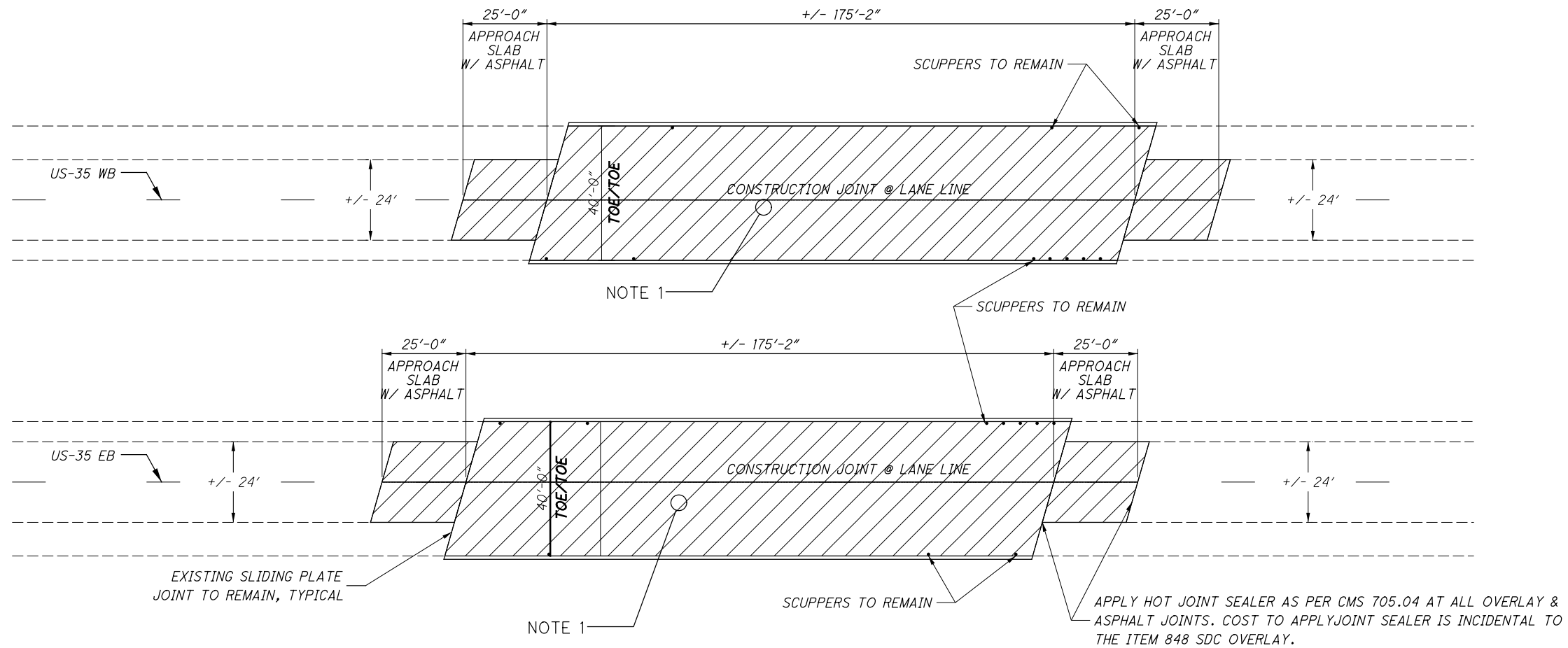
LOCATION				QUANTITIES				REMARKS	STRUCTURE NOTES
C	R	S	D	409	519	846		FAY-35-1679 SFN: 2400596/2400618	
O	O	L	E						
U	U	M	C						
N	T		K	SAWING AND SEALING ASPHALT CONCRETE JOINTS, AS PER PLAN	ITEM SPECIAL - PATCHING CONCRETE STRUCTURE, AS PER PLAN	POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM			
T	E		A						
Y			R	FT	SF	CF			
			E						
			A						
			A						
				SQ FT					
FAY	35	16.79 L	7,621.0	62					
					150		PATCHING PARAPETS		
						22	EXPANSION JOINT AT APPROACHES		
FAY	35	16.79 R	7,621.0	62					
					150		PATCHING PARAPETS		
						22	EXPANSION JOINT AT APPROACHES		
<b>TOTALS CARRIED TO STRUCTURE QUANTITIES</b>				124	300	44			



SECTION A-A  
ITEM 846 - POLYMER MODIFIED EXPANSION JOINT  
EACH JOINT TO BE 31'-0" LONG

DESIGN AGENCY: THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 6  
 DATE: 09/27/19  
 REVIEWED: KRF  
 STRUCTURE FILE NUMBER: 2400596/2400618  
 DRAWN: RAM  
 CHECKED: KRF  
 REVISED: XXX  
**BRIDGE PLAN**  
 FAY-35-16.79  
 OVER ABANDONED RAILROAD  
**FAY-35-4.52**  
 PID No. 107802  
 9 / 12  
 60  
 63

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**NOTES:**

NOTE 1:  
INSTALL NEW SUPERPLASTICIZED DENSE CONCRETE OVERLAY ON BRIDGE DECK AND APPROACH SLABS TO MATCH EXISTING BRIDGE ELEVATION

NOTE 2:  
SEAL DECK & APPROACH CONSTRUCTION JOINT WITH 1'-0" WIDE HMWM RESIN SEALER

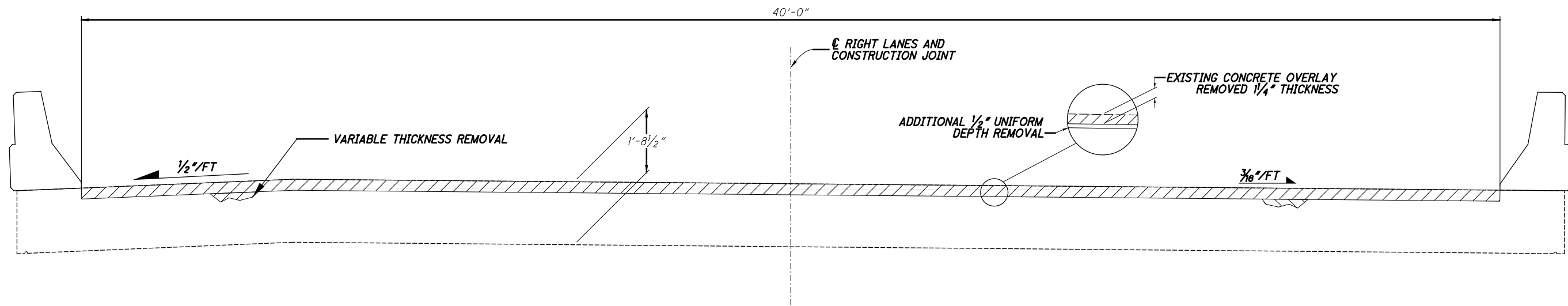
**BRIDGE PLAN**

FAY-35-17.21 (2400642/2400650)  
OVER SR-753  
SKEW 16°LF

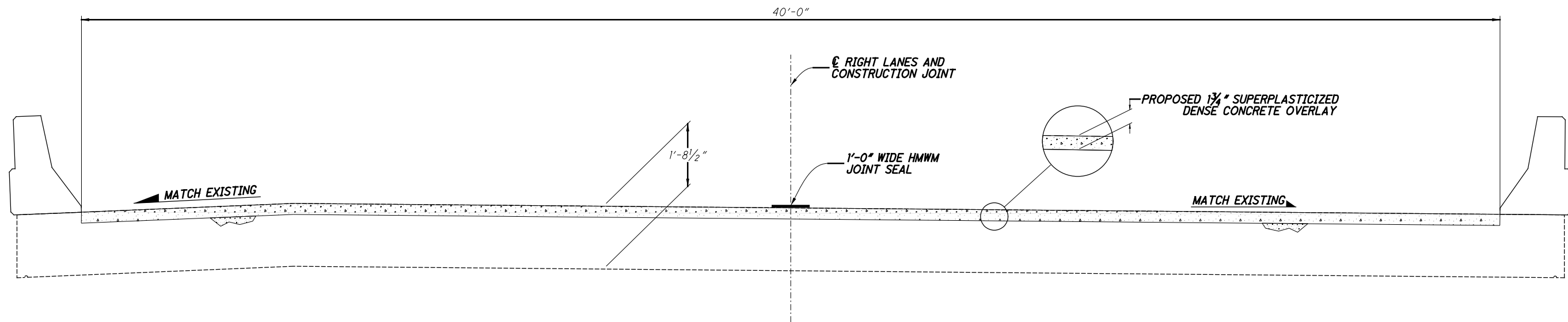


LOCATION				QUANTITIES								REMARKS	STRUCTURE NOTES	
C	R	S	D	848	848	848	848	848	848	848	848	512		FAY-35-17.21 SFN: 2400642/2400650
O	O	L	E	SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN 1 3/4" THICKNESS (DECK)	SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, AS PER PLAN 2" THICKNESS (APPR SLABS)	SURFACE PREPERATION USING HYDRODEMOLITION, AS PER PLAN	SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY, AS PER PLAN	EXISTING CONCRETE OVERLAY REMOVED 1 1/4"	HAND CHIPPING, AS PER PLAN	TEST SLAB, AS PER PLAN	WEARING COURSE REMOVED ASPHALT	SEALING CONCRETE BRIDGE DECKS WITH HMWM RESIN		
U	U	M	A	SY	SY	SY	CY	SY	SY	LS	SY	SY		
T	E		E											
Y			A											
			SQ FT											
FAY	35	17.21 L	7,350.0	779	134	913	15	779	25		134	25	BRIDGE DECK AND APPROACH SLABS	
FAY	35	17.21 R	7,350.0	779	134	913	15	779	25		134	25	BRIDGE DECK AND APPROACH SLABS	
<b>TOTALS CARRIED TO STRUCTURE QUANTITIES</b>				1,558	268	1,826	30	1,558	50	LS	268	50		

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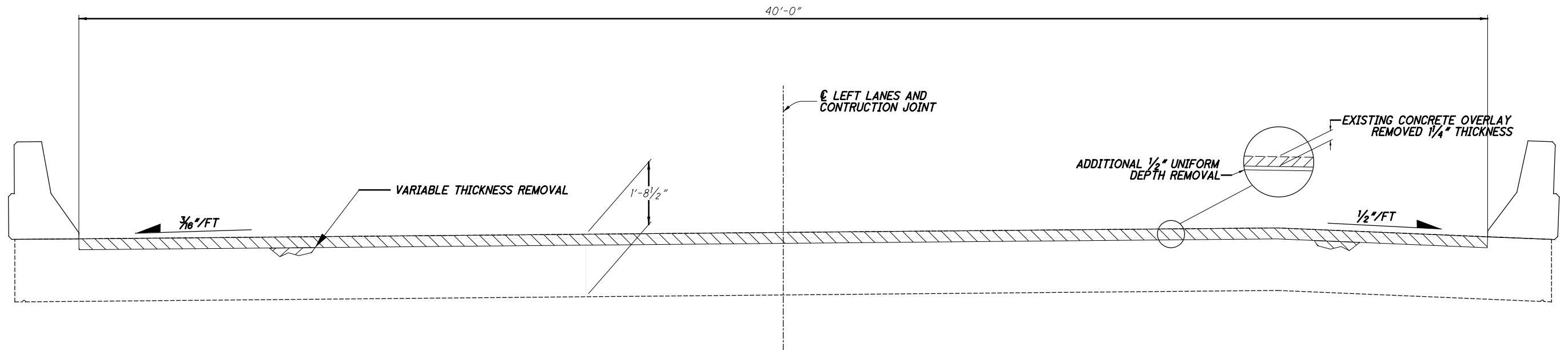
EXISTING TRANSVERSE SECTION-RIGHT BRIDGE



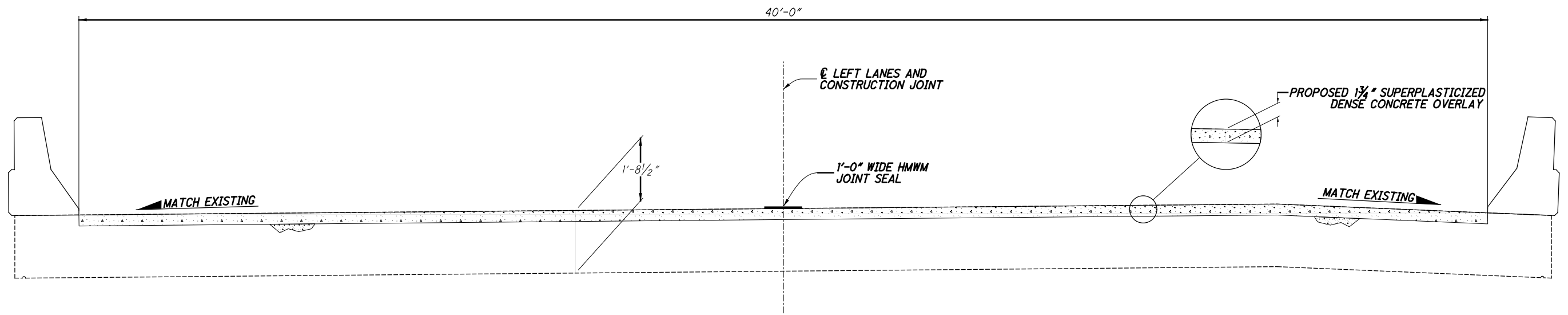
PROPOSED TRANSVERSE SECTION-RIGHT BRIDGE

DESIGNED		DRAWN		REVIEWED		DATE		DESIGN AGENCY	
CAB	KRF	CAB	KRF	KRF	KRF	09/27/19		ODOT DEPARTMENT OF	
								TRANSPORTATION DISTRICT 6	
CHECKED		REVISED		STRUCTURE FILE NUMBER					
KRF		XXX	XXX	2400650					
<b>TRANSVERSE SECTION</b>									
BRIDGE NO. FAY-35-17.21									
<b>FAY-35-17.21</b>									
<b>PID No. 107802</b>									
11 / 12									
62									
63									

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EXISTING TRANSVERSE SECTION-LEFT BRIDGE



PROPOSED TRANSVERSE SECTION-LEFT BRIDGE

DESIGNED		CAB	CHECKED	KRF
DRAWN		CAB	REVISED	XXX
REVIEWED	KRF	DATE	09/27/19	STRUCTURE FILE NUMBER
2400642		DESIGN AGENCY	ODOT DEPARTMENT OF TRANSPORTATION DISTRICT 6	
<b>TRANSVERSE SECTION</b>				
BRIDGE NO. FAY-35-17.21				
<b>FAY-35-17.21</b>		<b>PID No. 107802</b>		
12 / 12		63 / 63		