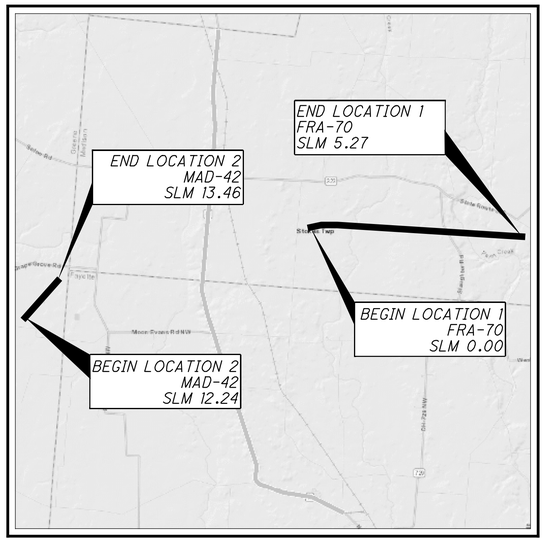
STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

FRA/MAD-70/42 SP FY24

BROWN TOWNSHIP, NORWICH TOWNSHIP, DEER CREEK TOWNSHIP

FRANKLIN COUNTY, MADISON COUNTY



LOCATION MAP

LATITUDE: 39°58'48"N LONGITUDE: 83°14'54"W

PORTION TO BE IMPROVED
INTERSTATE HIGHWAY
FEDERAL ROUTES
STATE ROUTES
COUNTY & TOWNSHIP ROADS
OTHER ROADS

DESIGN DESIGNATION

	FRA-70 / MAD-42
CURRENT ADT (2023)	*97,000 / 10,500*
DESIGN YEAR ADT (2030)	*102,000 / 11,500*
DESIGN HOURLY VOLUME (2030)	*8730 / 920*
DIRECTIONAL DISTRIBUTION	*52% / 51%*
TRUCKS (24 HOUR B&C)	*23,280 / 3,360*
DESIGN SPEED	*65-70 / 45*
LEGAL SPEED	*65-70 / 45*

DESIGN FUNCTIONAL CLASSIFICATION:
01 - INTERSTATE (URBAN) / 03 - OTHER PRINCIPAL ARTERIALS (RURAL)
NHS PROJECT — *YES*

DESIGN EXCEPTIONS

NONE REQUIRED

ADA DESIGN WAIVERS

NONE REQUIRED

UNDERGROUND UTILITIES
Contact Two Working Days Before You Dig
OHIO811.org Before You Dig
OHIO811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
ODOT IN HOUSE DESIGN

ENGINEER'S SEAL

STATE OF OHIO
ROBERT A. MCNEILL
E-78025
REGISTERED PROFESSIONAL ENGINEER

INDEX OF SHEETS:

FEDERAL PROJECT NUMBER

NON-FEDERAL

RAILROAD INVOLVEMENT

NONE

PROJECT DESCRIPTION

SPOT CONCRETE SLAB REPAIRS ON I-70 IN FRANKLIN COUNTY AND SR-42 IN MADISON COUNTY.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	*0.0 ACRES*
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	*0.0 ACRES*
NOTICE OF INTENT EARTH DISTURBED AREA:	*N/A**

** MAINTENANCE PROJECT*

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2019 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

Anthony C. Turowski, P.E.
District 06 Deputy Director

Jack Marchbanks, PhD
Director, Department of Transportation

STANDARD CONSTRUCTION DRAWINGS

BP-1.1	*7/28/00*	*MT-95.30*	*7/19/19*	*MT-105.10*	*1/17/20*	*TC-41.20*	*10/18/13*
BP-2.1	*1/21/22*	*MT-95.31*	*7/19/19*			*TC-42.20*	*10/18/13*
BP-2.2	*1/15/21*	*MT-95.32*	*4/19/19*			*TC-52.10*	*10/18/13*
BP-2.5	*1/21/22*	*MT-95.45*	*1/17/20*			*TC-65.10*	*1/17/14*
BP-2.6	*7/15/16*	*MT-95.50*	*7/21/17*			*TC-65.11*	*7/15/22*
BP-6.1	*7/19/18*	*MT-97.10*	*4/19/19*			*TC-72.20*	*7/20/18*
BP-8.1	*7/18/08*	*MT-98.10*	*1/17/20*				
		MT-98.11	*1/17/20*				
		MT-98.20	*4/19/19*				
		MT-98.21	*1/17/20*				
		MT-98.28	*1/17/20*				
		MT-98.29	*1/17/20*				
		MT-98.30	*7/16/21*				
		MT-99.20	*4/19/19*				
		MT-101.90	*7/17/20*				
		MT-104.10	*4/26/23*				

SUPPLEMENTAL SPECIFICATIONS

800-2019	*SEE PROPOSAL*
808	*1/18/19*
809	*1/20/23*
832	*7/15/22*
896	*7/21/17*
921	*4/20/12*

SPECIAL PROVISIONS

PID#108071 FRA/MAD-70/42 SP FY24

DESIGN AGENCY

DESIGNER: RAM
REVIEWER: XXX MM-DD-YY
PROJECT ID: 108071
SHEET: P.01 TOTAL: 18

FRA/MAD-70/72 SP FY24

MODEL: Sheet PAPERSIZE: 34x22 (in.) DATE: 6/8/2023 TIME: 3:00:00 PM USER: rmcneill
pw:\\ohiodot-pw.bentley.com:ohiodot-pw-02\Documents\01 Active Projects\District 06\Franklin\108071\400-Engineering\Roadway\Sheets\108071_GT001.dgn

ITEM 614 - MAINTAINING TRAFFIC:
ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CURRENT EDITION). COPIES ARE AVAILABLE FROM,

THE OHIO DEPARTMENT OF TRANSPORTATION
BUREAU OF TRAFFIC,
1980 WEST BROAD STREET
COLUMBUS, OHIO 43223

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK LISTED IN THE GENERAL SUMMARY FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER.

CONSTRUCTION OPERATIONS SHALL NOT BEGIN UNTIL ALL TRAFFIC CONTROL IS IN PLACE AND APPROVED BY ODOT PERSONNEL. THE CONSTRUCTION INSPECTOR SHALL APPROVE ALL TEMPORARY TRAFFIC CONTROL DEVICES FOR CONDITION AND LOCATION BEFORE THE CONTRACTOR WILL BE ALLOWED TO BEGIN WORK. IF THE CONTRACTOR DOES NOT COMPLY WITH THE STANDARDS, HIS PERMIT SHALL BE REVOKED AND ALL WORK SHALL BE TERMINATED.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

PUBLIC OUTREACH AND NOTIFICATION:
THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE DISTRICT 6 PUBLIC INFORMATION OFFICE VIA EMAIL AT D06.PIO@DOT.OHIO.GOV TO COORDINATE EFFORTS TO NOTIFY ADJACENT RESIDENTS AND BUSINESSES OF THE UPCOMING PROJECT. ADVANCE NOTIFICATION SHALL OCCUR NO LATER THAN FOURTEEN (14) DAYS PRIOR TO THE FIRST DAY OF WORK. ALL NOTIFICATIONS SHALL BE MADE UTILIZING THE TEMPLATE PROVIDED BY THE DISTRICT 6 PUBLIC INFORMATION OFFICE.

WORK SITE LIGHTING:
FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR, AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

NOTIFICATION OF TRAFFIC RESTRICTIONS
THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE PIO (D06.PIO@DOT.OHIO.GOV). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE BUT IS NOT LIMITED TO ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME FRAME TABLE

ITEM	*DURATION OF CLOSURE*	*NOTIFICATION DUE TO DISTRICT 6 COMMUNICATIONS OFFICE*	*SIGN DISPLAYED TO PUBLIC*
RAMP & ROAD CLOSURES	*>=2 WEEKS*	*21 CALENDAR DAYS PRIOR TO CLOSURE*	*14 CALENDAR DAYS PRIOR TO CLOSURE*
	>12 HOURS & <2 WEEKS	*14 CALENDAR DAYS PRIOR TO CLOSURE*	*7 CALENDAR DAYS PRIOR TO CLOSURE*
	<=12 HOURS	*4 BUSINESS DAYS PRIOR TO CLOSURE*	*2 BUSINESS DAYS PRIOR TO CLOSURE*
LANE CLOSURES & RESTRICTIONS	*>=2 WEEKS*	*14 CALENDAR DAYS PRIOR TO CLOSURE*	
	<2 WEEKS	*5 BUSINESS DAYS PRIOR TO CLOSURE*	
START OF CONSTRUCTION AND TRAFFIC PATTERN CHANGES	*N/A*	*14 CALENDAR DAYS PRIOR TO IMPLEMENTATION*	

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME FRAME TABLE.

USE OF STANDARD DRAWINGS:
FOR THE PURPOSE OF THIS PROJECT, "MOVING OPERATION" SHALL BE LIMITED TO PAVEMENT MARKING STRIPING.

IT MAY BE NECESSARY TO EXTEND THE ADVANCE WARNING AND BUFFER ZONES BEYOND THE MINIMUM DISTANCES SHOWN ON THE STANDARD DRAWINGS. THIS MAY BE DUE TO HORIZONTAL ALIGNMENT, VERTICAL ALIGNMENT, RAMP LOCATIONS, OR OTHER SIGHT OBSTRUCTIONS. LOCATIONS OF THE TAPER ZONES MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER, BUT TAPER LENGTHS MUST MEET THE MINIMUM STANDARDS. TAPERS SHOULD BE PLACED IN TANGENT SECTIONS WHENEVER POSSIBLE. ADDITIONAL YIELD SIGNS MAY BE REQUIRED FOR RAMPS WITHIN 1,000 FEET OF A WORK ZONE. PAYMENT SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

FOR ANY MULTILANE HIGHWAY, DEVICE SPACING SHALL BE A MAXIMUM OF 40' CENTER ON CENTER IN THE TAPERS AND 80' CENTER ON CENTER IN THE TANGENT SECTIONS.

LANES OPEN DURING HOLIDAYS AND SPECIAL EVENTS:
NO WORK SHALL BE PERFORMED AND THE SAME NUMBER OF LANES AS WERE AVAILABLE AT THE START OF THE PROJECT SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

HOLIDAYS
CHRISTMAS (OBSERVED) FOURTH OF JULY
NEW YEAR'S (OBSERVED) LABOR DAY
MEMORIAL DAY THANKSGIVING
TOTAL SOLAR ECLPISE (4/8/24)
GENERAL/REGULAR ELECTION DAY (NOV)

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THE PERIOD:

DAY OF HOLIDAY	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THANKSGIVING	5:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

DURING THE SAME PERIODS, MAINTAIN PEDESTRIAN ACCESS IF PEDESTRIAN ACCESS WAS PRESENT PRIOR TO CONSTRUCTION.

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA WIDE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS ON MAD-42, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF $100 FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS ON FRA-70, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE AS PER THE LANE VALUE CONTRACT TABLE FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

DROPOFFS IN WORK ZONES:
THE DROPOFF ADJACENT TO THE TRAVELED LANE SHALL MEET THE CRITERIA OUTLINED IN STANDARD DRAWING MT-101.90. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR MATERIALS, LABOR OR EQUIPMENT NECESSARY TO MEET THE REQUIREMENTS OF MT-101.90.

APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION(S):

PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE APPROVED MOT EXCEPTION(S) PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

APPROVED MOT EXCEPTION(S) INCLUDE:

CLOSURE FROM 3 LANES TO 2 LANES ON I-70 FOR HOURS BEYOND THE PERMITTED LANE CLOSURE TIMES. THE CONTRACTOR WILL BE PERMITTED TO CLOSE 1 OF 3 LANES UTILIZING ONE OF THE FOLLOWING PLANS TO PERFORM THE PROPOSED PAVEMENT REPAIRS:

1. WEEKDAY LANE CLOSURE:
SUNDAY 6 PM TO THURSDAY 2 PM

WORK	DIRECTION	# INSTANCES
MS CONCRETE REPAIRS	EASTBOUND	2
MS CONCRETE REPAIRS	WESTBOUND	1

2. WEEKEND LANE CLOSURE:
FRIDAY 7 PM TO MONDAY 5 AM

WORK	DIRECTION	# INSTANCES
MS CONCRETE REPAIRS	EASTBOUND	4
MS CONCRETE REPAIRS	WESTBOUND	2

PAVEMENT DEMOLITION AND REMOVAL WILL ONLY BE PERMITTED TO OCCUR WITH A 2 LANE CLOSURE DURING THE HOURS PERMITTED IN THE LANE VALUE CLONTRACT TABLE ON SHEET 12/18. EXTENDED HOURS FOR THE 1 LANE CLOSURE ARE TO BE UTILIZED FOR POURING/FINISHING/CURING CONCRETE, ASPHALT REPAIRS, OR OTHER MISCELLANEOUS WORK.

A MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF 30 CALENDAR DAYS PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER AS WELL AS THE CONTRACTOR, AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.

IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO THE OFFICE OF ROADWAY ENGINEERING, STATEWIDE TMC, DWZTM AND SPECIAL HAULING PERMITS AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTION(S) REFERENCED ABOVE. REFERENCE "EXCEPTION REQUEST APPROVAL DATED 3/27/2023 FOR PID 108071" IN THE NOTIFICATION AND OTHER CORRESPONDENCE.

ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTION(S) LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE MOT EXCEPTION COMMITTEE (MOTEC). IN THE EVENT THAT SUCH CHANGES ARE PROPOSED, THE REQUEST SHALL BE COORDINATED THROUGH THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM) A MINIMUM OF 30 CALENDAR DAYS PRIOR TO THE DESIRED IMPLEMENTATION DATE. IF THE DISTRICT AGREES WITH THE PROPOSED CHANGES THE DWZTM SHALL SEEK APPROVAL FROM THE MOTEC. IN THE EVENT THE PROPOSED CHANGES ARE APPROVED IN WRITING, THE CLOSURES ARE STILL SUBJECT TO NOTIFICATION REQUIREMENTS WITHIN THIS NOTE PRIOR TO IMPLEMENTATION.

MAINTENANCE OF TRAFFIC NOTES

DESIGN AGENCY

DESIGNER RAM
REVIEWER XXX MM-DD-YY
PROJECT ID 108071
SHEET P.09 TOTAL 18

FRA/MAD-70/42 SP FY24

MODEL: Sheet PAPERSIZE: 34x22 (in.) DATE: 6/8/2023 TIME: 3:00:58 PM USER: rmcneill
pw:\\ohiodot-pw.bentley.com:ohiodot-pw-02\Documents\01 Active Projects\District 06\Franklin\108071\400-Engineering\MOT\Sheets\108071_MN001.dgn

EXTRA ADVANCE WARNING SIGNS:
AN ADVANCE WARNING SIGN GROUP CONSISTS OF TWO W20-1 (ROAD WORK AHEAD) SIGNS, TWO W20-5 (RIGHT/LEFT LANE CLOSED AHEAD) SIGNS WITH W16-3A DISTANCE PLATES, AND TWO W3-H7 (WATCH FOR STOPPED TRAFFIC) SIGNS AND REQUIRED FLASHING LIGHTS.

THE CONTRACTOR SHALL PROVIDE, ERECT, MAINTAIN AND REMOVE AN EXTRA ADVANCE WARNING SIGN GROUP AS SHOWN ON TRAFFIC SCD MT-95.50. THE W16-3A DISTANCE PLATES SHALL READ "2 MILES". THE RIGHT (LEFT) LANE CLOSED AHEAD SIGNS SHALL BE LOCATED 2 MILES FROM THE BEGINNING OF THE LANE TAPER. SPACING OF THE OTHER SIGNS SHALL BE AS SHOWN ON TRAFFIC SCD MT-95.40.

THE CONTRACTOR SHALL PROVIDE, ERECT, MAINTAIN AND REMOVE AN ADDITIONAL EXTRA ADVANCE WARNING SIGN GROUP DURING THE MOTEC APPROVED LANE CLOSURES.

THE CONTRACTOR SHALL HAVE AN ADDITIONAL EXTRA ADVANCE WARNING SIGN GROUP (6 SIGNS AND 2 DISTANCE PLATES) AVAILABLE FOR USE WHEN DIRECTED BY THE ENGINEER. THE DISTANCE PLATES FOR THIS GROUP SHALL BE ABLE TO BE MODIFIED IN THE FIELD TO SHOW APPROPRIATE WHOLE MILES TO THE LANE TAPER.

PAYMENT FOR PROVIDING, ERECTING, MAINTAINING AND REMOVING EXTRA ADVANCE WARNING SIGN GROUPS SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614, MAINTAINING TRAFFIC.

INTERSTATE ENTRANCE RAMP CLOSURES (FRANKLIN COUNTY ONLY):
IF THE INTERSTATE IS REDUCED TO ONE THROUGH LANE, ALL NON-INTERSTATE ENTRANCE RAMPS ENTERING DIRECTLY INTO THE WORK ZONE TRANSITION AREA, ACTIVITY AREA OR TERMINATION AREA AS DEFINED BY THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD) OR ENTERING WITHIN 1000 FEET OF THE FIRST ARROW BOARD SHALL BE CLOSED IN ACCORDANCE WITH THE SHORT DURATION RAMP CLOSURES NOTE.

SHORT DURATION RAMP CLOSURES:
FOR THE PURPOSE OF PERFORMING THE REQUIRED WORK OR WHEN REQUIRED BY THE INTERSTATE ENTRANCE RAMP CLOSURE NOTE, RAMPS MAY BE CLOSED FOR SHORT DURATIONS AND DETOURED IN ACCORDANCE WITH THE RAMP CLOSURE TABLE IF APPROVED BY THE ENGINEER. RAMP CLOSURES ARE SUBJECT TO DISINCENTIVES.

FOR ALL SERVICE RAMP CLOSURES LASTING MORE THAN 12 HOURS BUT LESS THAN 60 HOURS AND/OR, FOR ALL SYSTEM RAMP CLOSURES LASTING MORE THAN 12 HOURS BUT LESS THAN 24 HOURS THE CONTRACTOR SHALL PROVIDE THE FOLLOWING:
- A MINIMUM OF TWO PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) PLACED, AS DIRECTED BY THE ENGINEER, TO WARN DRIVERS OF THE CLOSURE AND TO PROVIDE THE DESIGNATED DETOUR ROUTE.
- POSITIVE GUIDANCE ALONG THE DETOUR ROUTE WITH DETOUR SIGNS (M4-9 SERIES) IN ACCORDANCE WITH THE DETOUR SIGNS NOTE.

FOR ALL RAMP CLOSURES LASTING LESS THAN 12 HOURS, THE CONTRACTOR SHALL PROVIDE THE FOLLOWING:
- A MINIMUM OF TWO PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) PLACED, AS DIRECTED BY THE ENGINEER, TO WARN DRIVERS OF THE CLOSURE AND TO PROVIDE THE DESIGNATED DETOUR ROUTE.

WHEN CLOSING ENTRANCE RAMPS, CORRESPONDING LEAD-IN LANES AND TURN LANES SHALL ALSO BE CLOSED.

IF A DESIGNATED DETOUR ROUTE IS NOT PROVIDED IN THE PLANS, TRAFFIC SHALL BE DIRECTED TO THE NEXT INTERCHANGE, IF AVAILABLE, TO TURN AROUND. IF THE USE OF THE NEXT INTERCHANGE IS NOT POSSIBLE, AN ALTERNATIVE DETOUR ROUTE SHALL BE PROVIDED BY THE ENGINEER.

SERVICE RAMP: INTERCHANGE RAMPS BETWEEN FREEWAYS (OR EXPRESSWAYS) AND NON-FREEWAYS (OR NONEXPRESSWAYS). THESE RAMPS PROVIDE ACCESS (CONNECTIONS) BETWEEN FREEWAYS/EXPRESSWAYS AND OTHER PRINCIPAL/MINOR ARTERIALS, COLLECTORS OR LOCAL ROADS.

SYSTEM RAMP: INTERCHANGE RAMPS (OR CONNECTORS) BETWEEN FREEWAYS (OR EXPRESSWAYS) AND FREEWAYS (OR EXPRESSWAYS).

FOR EACH UNIT OF TIME A RAMP IS CLOSED BY THE CONTRACTOR'S ACTION WHILE NOT OTHERWISE PERMITTED BY THE RAMP CLOSURE RESTRICTION TABLES - THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN ACCORDANCE TO THE LANE VALUE CONTRACT TABLE FOR THE SECTION OF MAINLINE IN WHICH THE RAMP IS LOCATED.

Ramp Closure Restrictions
Interstate Route 70 in Franklin County

Secondary Route: Hilliard Rome Rd SLM along 70: 5.25

Ramp	Movement	No Closures Allowed		Detour Routes	
		Mon-Fri	Sat-Sun	Primary Detour Route	Secondary Detour Route
A	CR-3 to I-70 WB	5AM-8PM	No Restriction	Hilliard Rome Rd. to 70 E (Ramp F) to 270 N to 70 W	None
B	I-70 WB to CR-3 SB	5AM-11PM	8AM-7PM	70 W to OH-142 to 70 E to Hilliard Rome Rd. S (Ramp E)	None
C	I-70 WB to CR-3 NB	5AM-12AM	8AM-8PM	70 W to OH-142 to 70 E to Hilliard Rome Rd. N (Ramp E)	70 W to 270 S to US-40 W to Hilliard Rome Rd. N
D	CR-3 NB to I-70 EB	5AM-8PM	8AM-7PM	Hilliard Rome Rd. N to 70 W to OH-142 to 70 E	None
E	I-70 EB to CR-3 SB	5AM-7PM	No Restriction	70 E to 270 N to 70 W to Hilliard Rome Rd. S (Ramp B)	70 E to 270 S to US-40 W to Hilliard Rome Rd.
F	CR-3 SB to I-70 EB	5AM-10PM	8AM-7PM	Hilliard Rome Rd. S to US-40 E to 270 N to 70 E	Hilliard Rome Rd. S to 70 W (Ramp A) to OH-142 to 70 E

Ramp Closure Restrictions
Interstate Route 70 in Madison County

Secondary Route: US Route 42 SLM along 70:

Ramp	Movement	No Closures Allowed		Detour Routes	
		Mon-Fri	Sat-Sun	Primary Detour Route	Secondary Detour Route
A	US-42 to I-70 WB	5AM-9PM	8AM-7PM	US-42 to I-70 EB (Ramp C) to OH-29 WB (Ramp D) to I-70 WB (Ramp A)	US-42 NB to OH-29 EB to I-70 WB (Ramp A)
B	I-70 WB to US-42	5AM-9PM	8AM-7PM	I-70 WB to OH-56 SB (Ramp B) to I-70 EB (Ramp C) to US-42 (Ramp D)	I-70 WB to OH-54 SB (Ramp C) to I-70 EB (Ramp D) to US-42 (Ramp D)
C	US-42 to I-70 EB	5AM-9PM	8AM-7PM	US-42 NB to OH-29 EB to I-70 EB (Ramp C)	US-42 to I-70 WB (Ramp A) to OH-56 SB (Ramp B) to I-70 EB (Ramp C)
D	I-70 EB to US-42	5AM-9PM	8AM-7PM	I-70 EB to OH-29 (Ramp D) to OH-29 WB to US-42	I-70 EB to OH-29 (Ramp D) to I-70 WB (Ramp A) to US-42 (Ramp B)

LANE VALUE CONTRACT TABLE:
THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE AS DESIGNATED IN THE LANE VALUE CONTRACT TABLE FOR EACH UNIT OF TIME A LANE/SHOULDER/RAMP IS CLOSED BY THE CONTRACTOR'S ACTION WHILE NOT OTHERWISE PERMITTED BY THE LANE VALUE CONTRACT TABLE.

LANE VALUE CONTRACT TABLE

Section (SLM)	Existing Number of Lanes per Direction	Lane closures are NOT permitted:				Disincentive Amounts per minute per lane
		Lane Reduction	Mon to Fri	Sat	Sun	
FRA-70						
Madison County Line (0.00) to Hilliard Rome Road (4.68)	3	3 to 2	5AM-9AM & 2PM-7PM	3PM-6PM	3PM-6PM	$275
		3 to 1	5AM-8PM	5AM-9PM	6AM-11PM	$275
Hilliard Rome Road (4.68) to I-270 (West) (6.78)	3	3 to 2	5AM-7PM	3PM-6PM	3PM-6PM	$370
		3 to 1	5AM-10PM	5AM-9PM	6AM-11PM	$370
Short term shoulder closures are permitted any time except 5AM-9AM and 3PM-6PM Monday-Friday.						

LANE VALUE CONTRACT TABLE

SECTION (SLM)	EXISTING NUMBER OF LANES PER DIRECTION	LANE CLOSURES ARE **NOT** PERMITTED:				DISINCENTIVE AMOUNTS PER MINUTE PER LANE
		LANE REDUCTION	MON TO FRI	SAT	SUN	
MAD-70						
CLARK COUNTY INE (0.00) TO SR-29 (10.28)	3	3 TO 2	NO RESTRICTION	3PM-6PM	NO RESTRICTION	$250
		3 TO 1	5AM-7PM	5AM-8PM	8AM-9PM	$250
SR-29 (10.28) TO FRANKLIN COUNTY LINE (15.58) WB	3	3 TO 2	3PM-5PM	3PM-7PM	NO RESTRICTION	$275
		3 TO 1	5AM-7PM	5AM-8PM	8AM-9PM	$275
SR-29 (10.28) TO FRANKLIN COUNTY LINE (15.58) EB	3	3 TO 2	NO RESTRICTION	3PM-7PM	NO RESTRICTION	$275
		3 TO 1	5AM-7PM	5AM-8PM	8AM-9PM	$275
SHOULDER CLOSURES ARE PERMITTED ANY TIME EXCEPT 5AM-9AM & 3PM-6PM MONDAY-FRIDAY						

MADISON COUNTY US-42, 2-LANE SECTION:
ALL LANE OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT FOR PERIODS WHEN ROADWAY REPAIRS ARE BEING COMPLETED. TO ENSURE THAT TRAFFIC CAN FLOW SMOOTHLY AT THE END OF THE WORKING DAY, THE LENGTH OF WORK ZONES SHOULD BE LIMITED TO THE AMOUNT OF WORK THAT CAN BE COMPLETED IN A DAY. THIS MEANS THAT WORK ZONES SHALL NOT BE LONGER THAN THE AMOUNT OF WORK THAT CAN BE PERFORMED IN A DAY, SO THAT THE LANE CAN BE OPENED BACK UP TO TRAFFIC AT THE END OF THE DAY. TRAFFIC SHALL BE MAINTAINED BY FLAGGERS FOR CLOSING 1 LANE OF THE 2-LANE HIGHWAY FOR PAVING OPERATION AS PER STANDARD DRAWING MT-97.10 - FLAGGER CLOSING 1 LANE OF A 2-LANE HIGHWAY - STATIONARY OPERATION.

MADISON COUNTY US-42, 4-LANE SECTION:
ALL LANES OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT FOR PERIODS WHEN ROADWAY REPAIRS ARE BEING COMPLETED. TO ENSURE THAT TRAFFIC CAN FLOW IN BOTH DIRECTIONS, TRAFFIC SHALL BE MAINTAINED DURING ROADWAY REPAIR WORK PER STANDARD DRAWING MT-95.30 - CLOSING RIGHT OR LEFT LANE OF A MULTI-LANE DIVIDED HIGHWAY WITH DRUMS, MT-95.31 - CLOSING RIGHT LANES OF A MULTI-LANE UNDIVIDED HIGHWAY WITH DRUMS, OR MT-95.32 - CLOSING LEFT LANES OF A MULTI-LANE UNDIVIDED HIGHWAY WITH DRUMS, WITH THE EXCEPTION THAT EXISTING ONE LANE OF TRAFFIC ON BOTH ENDS OF THE MULTILANE SECTION MAY BE SHIFTED INTO AND OUT OF THE OPEN LANE IN LIEU OF THE REQUIRED TAPERS.

MAINTENANCE OF TRAFFIC NOTES

DESIGN AGENCY

DESIGNER RAM
REVIEWER XXX MM-DD-YY
PROJECT ID 108071
SHEET P.12 TOTAL 18

FRA/MAD-70/42 SP FY24

MODEL: Sheet 04 PAPERSIZE: 34x22 (in.) DATE: 6/8/2023 TIME: 3:01:02 PM USER: rmcneill
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SHEET NUMBER									PARTICIPATION		ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
		7-8	9-12	14	18				01/IMS/05							
															PAVEMENT	
				4,903					*4,903*		*251*	*01021*	*4,903*	*SY*	*PARTIAL DEPTH PAVEMENT REPAIR (442), AS PER PLAN, 4.5"*	*7*
				2,488					*2,488*		*255*	*10161*	*2,488*	*SY*	*FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN, 12.25"*	*7*
				635					*635*		*255*	*10161*	*635*	*SY*	*FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN, 14.5"*	*7*
				320					*320*		*255*	*10501*	*320*	*SY*	*FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS RRCM, AS PER PLAN, 12.25"*	*7*
				776					*776*		*255*	*10501*	*776*	*SY*	*FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS RRCM, AS PER PLAN, 14.5"*	*7*
				8,932					*8,932*		*255*	*20000*	*8,932*	*FT*	*FULL DEPTH PAVEMENT SAWING*	
				4,219					*4,219*		*257*	*10001*	*4,219*	*SY*	*DIAMOND GRINDING PORTLAND CEMENT CONCRETE PAVEMENT, AS PER PLAN*	*7*
															TRAFFIC CONTROL	
					152				*152*		*621*	*00100*	*152*	*EACH*	*RPM*	
					152				*152*		*621*	*54000*	*152*	*EACH*	*RAISED PAVEMENT MARKER REMOVED*	
					3.24				*3.24*		*642*	*00104*	*3.24*	*MILE*	*EDGE LINE, 6", TYPE 1*	
					3.28				*3.28*		*642*	*00204*	*3.28*	*MILE*	*LANE LINE, 6", TYPE 1*	
					342				*342*		*642*	*00404*	*342*	*FT*	*CHANNELIZING LINE, 12", TYPE 1*	
															MAINTENANCE OF TRAFFIC	
			500						*500*		*614*	*11110*	*500*	*HOUR*	*LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE*	*11*
			8						*8*		*614*	*18601*	*8*	*SNMT*	*PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN*	*10*
			3.28						*3.28*		*614*	*20560*	*3.28*	*MILE*	*WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT*	
			3.24						*3.24*		*614*	*22360*	*3.24*	*MILE*	*WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT*	
			342						*342*		*614*	*23690*	*342*	*FT*	*WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT*	
			9						*9*		*808*	*18700*	*9*	*SNMT*	*DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY*	
			12						*12*		896	00012	*12*	*SNMT*	PORTABLE NON-INTRUSIVE TRAFFIC SENSOR, CLASS II	
			6						*6*		896	00020	*6*	*SNMT*	PORTABLE CHANGEABLE MESSAGE SIGN	
															INCIDENTALS	
									LS		*614*	*11000*	*LS*		*MAINTAINING TRAFFIC*	
									LS		*623*	*10001*	*LS*		*CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN*	*7*
									LS		*624*	*10000*	*LS*		*MOBILIZATION*	

GENERAL SUMMARY

DESIGN AGENCY

DESIGNER
RAM

REVIEWER
XXX MM-DD-YY

PROJECT ID
108071

SHEET P.13 TOTAL 18

FRA/MAD-70/42 SP FY24

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SHEET NUMBER	LOCATION	ROUTE	BEGIN SLM	END SLM	SIDE	251 PARTIAL DEPTH PAVEMENT REPAIR (442), AS PER PLAN, 4.5"	255 FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN, 12.25"	255 FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN, 14.5"	255 FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS RRCM, AS PER PLAN, 12.25"	255 FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS RRCM, AS PER PLAN, 14.5"	255 FULL DEPTH PAVEMENT SAWING	257 DIAMOND GRINDING PORTLAND CEMENT CONCRETE PAVEMENT, AS PER PLAN	COMMENTS
						SY	SY	SY	SY	SY	FT	SY	
15	1	FRA-70	0.00	5.27	EB AND WB	356	2,488		320		4,500	2,808	
16	2	MAD-42	12.24	13.46	NE	1,870		363		569	2,892	932	
17	2	MAD-42	12.24	13.46	SB	2,677		272		207	1,540	479	
TOTALS CARRIED TO GENERAL SUMMARY						4903	2,488	635	320	776	8,932	4,219	

PAVEMENT SUBSUMMARY

FRA/MAD-70/42 SP FY24

DESIGN AGENCY

DESIGNER RAM

REVIEWER XXX MM-DD-YY

PROJECT ID 108071

SHEET P.14 TOTAL 18

LOCATION				DESIGN						251	251	255	255	255	255	257	
LOCATION	ROUTE	BEGIN SLM	SIDE	LENGTH OF REPAIR	WIDTH OF REPAIR	DEPTH OF REPAIR	1	2	RAMP/ SHOULDER/ TURN LANE	PARTIAL DEPTH PAVEMENT REPAIR (442), AS PER PLAN, 4.5"	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN, 12.25"	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN, 14.5"	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS RRCM, AS PER PLAN, 12.25"	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS RRCM, AS PER PLAN, 14.5"	FULL DEPTH PAVEMENT SAWING	DIAMOND GRINDING PORTLAND CEMENT CONCRETE PAVEMENT, AS PER PLAN	COMMENTS
										SY	SY	SY	SY	SY	FT	SY	
2	MAD-42	13.37	NB	6	15	14.5	X							10.0	42	10.0	
2	MAD-42	13.36	NB	6	15	14.5	X							10.0	42	10.0	NEAR PILOT CAR ENTRANCE
2	MAD-42	13.29	NB	12	15	14.5	X							20.0	54	20.0	BETWEEN MCMAHAN DRIVES
2	MAD-42	13.29	NB	25	15	14.5	X							41.7	80	41.7	TWO CLOSE JOINT FAILURES
2	MAD-42	13.27	NB	6	15	14.5	X							10.0	42	10.0	IN SOUTHERN MCMAHAN DRIVE
2	MAD-42	13.26	NB	6	15	14.5	X							10.0	42	10.0	DEEP RUTTING
2	MAD-42	13.25	NB	50	14	4.5	X			77.8							
2	MAD-42	13.25	NB	10	15	14.5	X							16.7	50	16.7	END OF TWO LANE
2	MAD-42	13.24	NB	57	15	4.5	X			95.0							LOTS OF CLOSE FAILURES
2	MAD-42	13.20	NB	125	15	4.5	X			208.3							LOTS OF CLOSE FAILURES
2	MAD-42	13.19	NB	10	15	14.5	X					16.7			50	16.7	IN LANE AND TAPER
2	MAD-42	13.18	NB	10	15	14.5	X					16.7			50	16.7	IN LANE AND TAPER
2	MAD-42	13.17	NB	10	15	14.5	X					16.7			50	16.7	IN LANE AND TAPER
2	MAD-42	13.17	NB	10	15	14.5	X					16.7			50	16.7	
2	MAD-42	13.15	NB	10	15	14.5	X					16.7			50	16.7	
2	MAD-42	13.13	NB	10	15	14.5	X					16.7			50	16.7	RIGHT @ TOP OF 70WB RAMP
2	MAD-42	13.13	NB	10	15	14.5	X					16.7			50	16.7	RIGHT @ TOP OF 70WB RAMP
2	MAD-42	13.12	NB	65	9	14.5			X	65.0					148	0.0	RIGHT @ TOP OF 70WB RAMP
2	MAD-42	13.12	NB	10	15	14.5	X					16.7			50	16.7	
2	MAD-42	13.10	NB	66	15	14.5	X			110.0							LOTS OF CLOSE FAILURES
2	MAD-42	13.08	NB	10	12	14.5	X	X	X					39.9	132	39.9	
2	MAD-42	13.07	NB	40	12	14.5	X	X	X					159.9	312	159.9	
2	MAD-42	13.07	NB	19	12	4.5	X	X		50.6							CLOSE TO BRIDGE - LANE 2 AND BRIDGE GORE
2	MAD-42	12.94	NB	20	12	14.5		X				26.7			64	26.7	
2	MAD-42	12.92	NB	264	12	14.5	X			352.0							
2	MAD-42	12.92	NB	10	12	4.5		X		13.3							
2	MAD-42	12.91	NB	10	12	14.5	X	X				26.6			88	26.6	AT THE TOP OF 42 TO 70 EB RAMP
2	MAD-42	12.91	NB	10	12	14.5		X				13.3			44	13.3	
2	MAD-42	12.90	NB	42	10	14.5			X					46.7	104	46.7	AT THE TOP OF 42 TO 70 EB RAMP AND SHOULDER
2	MAD-42	12.90	NB	6	12	14.5	X	X				16.0			72	16.0	AT THE TOP OF 42 TO 70 EB RAMP AND SHOULDER
2	MAD-42	12.90	NB	6	12	14.5	X	X				16.0			72	16.0	JUST BEFORE RAMP
2	MAD-42	12.88	NB	40	12	14.5	X	X				106.6			208	106.6	
2	MAD-42	12.87	NB	6	12	14.5		X				8.0			36	8.0	
2	MAD-42	12.86	NB	76	6	14.5	X			50.7							ALONG WHITE LINE
2	MAD-42	12.86	NB	6	12	14.5	X	X				16.0			72	16.0	
2	MAD-42	12.85	NB	6	15	14.5			X					10.0	42	10.0	PRIOR TO SLIP RAMP
2	MAD-42	12.85	NB	6	15	14.5			X					10.0	42	10.0	PRIOR TO SLIP RAMP
2	MAD-42	12.84	NB	108	6	14.5	X			72.0							ALONG WHITE LINE
2	MAD-42	12.84	NB	6	15	14.5	X	X	X					30.0	126	30.0	
2	MAD-42	12.83	NB	6	15	14.5		X	X					20.0	84	20.0	
2	MAD-42	12.83	NB	6	15	14.5	X	X	X					30.0	126	30.0	
2	MAD-42	12.81	NB	105	15	4.5	X	X	X	525.0							SPRAY COVERING FAILURES
2	MAD-42	12.80	NB	6	12	14.5	X	X						16.0	72	16.0	
2	MAD-42	12.79	NB	64	12	4.5	X			170.6							
2	MAD-42	12.79	NB	6	12	14.5	X	X						16.0	72	16.0	
2	MAD-42	12.78	NB	60	12	4.5	X			80.0							
2	MAD-42	12.78	NB	6	12	14.5	X	X						16.0	72	16.0	
2	MAD-42	12.73	NB	26	14	14.5	X							40.4	80	40.4	
2	MAD-42	12.67	NB	6	12	14.5	X							8.0	36	8.0	
2	MAD-42	12.42	NB	6	12	14.5	X							8.0	36	8.0	BETWEEN TWO SPEEDWAY CAR DRIVES
									QUANTITIES CARRIED TO PAVEMENT SUBSUMMARY	1,870		363		569	2,892	932	

PAVEMENT REPAIR PLAN

FRA/MAD-70/42 SP FY24

MODEL: Sheet 03 PAPERSIZE: 34x22 (in.) DATE: 6/8/2023 TIME: 3:01:46 PM USER: rmcneill
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DESIGN AGENCY

DESIGNER RAM

REVIEWER XXX MM-DD-YY

PROJECT ID 108071

SHEET P.16 TOTAL 18

PAVEMENT REPAIR PLAN

LOCATION				DESIGN						251	251	255	255	255	255	257	
LOCATION	ROUTE	BEGIN SLM	SIDE	LENGTH OF REPAIR	WIDTH OF REPAIR	DEPTH OF REPAIR	LANE 1	LANE 2	LANE SHOULDER	PARTIAL DEPTH PAVEMENT REPAIR (442), AS PER PLAN, 4.5"	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN, 12.25"	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN, 14.5"	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS RRCM, AS PER PLAN, 12.25"	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS RRCM, AS PER PLAN, 14.5"	FULL DEPTH PAVEMENT SAWING	DIAMOND GRINDING PORTLAND CEMENT CONCRETE PAVEMENT, AS PER PLAN	COMMENTS
										SY	SY	SY	SY	SY	FT	SY	
2	MAD-42	12.39	SB	10	12	4.5	X			13.3							PRIOR TO SPEEDWAY TRUCK ENTRANCE
2	MAD-42	12.42	SB	6	12	14.5	X							8.0	36	8.0	BETWEEN TWO SPEEDWAY CAR DRIVES
2	MAD-42	12.43	SB	6	12	14.5	X							8.0	36	8.0	JUST PRIOR TO SPEEDWAY CAR DRIVE
2	MAD-42	12.63	SB	10	12	14.5	X							13.3	44	13.3	RIGHT @ SOUTH EDGE OF TA TRUCK ENTRANCE
2	MAD-42	12.68	SB	263	20	4.5	X			584.4							PREVIOUS AC PATCH - WORSE NEAR CENTER LINE
2	MAD-42	12.68	SB	6	12	14.5	X							8.0	36	8.0	
2	MAD-42	12.70	SB	500	20	4.5	X			1,111.1							PREVIOUS PATCHES
2	MAD-42	12.89	SB	10	12	4.5	X			13.3							RIGHT @ TOP OF RAMP - 70 EB TO 42
2	MAD-42	12.90	SB	10	12	4.5	X			13.3							
2	MAD-42	12.91	SB	10	12	4.5	X	X		26.6							
2	MAD-42	12.92	SB	10	12	4.5	X	X		26.6							
2	MAD-42	12.93	SB	10	12	4.5	X	X	X	39.9							
2	MAD-42	12.94	SB	15	12	4.5	X	X	X	60.0							
2	MAD-42	12.95	SB	6	12	4.5			X	8.0							
2	MAD-42	12.95	SB	6	12	4.5	X	X		16.0							
2	MAD-42	12.96	SB	18	12	4.5	X	X		48.0							
2	MAD-42	13.06	SB	35	12	14.5	X	X				93.4			188	93.4	
2	MAD-42	13.07	SB	10	12	14.5	X	X				26.6			88	26.6	COULD BE SWITCHED TO AC
2	MAD-42	13.08	SB	10	12	14.5		X				13.3			44	13.3	
2	MAD-42	13.09	SB	10	12	14.5		X				13.3			44	13.3	
2	MAD-42	13.09	SB	50	12	4.5	X	X		133.4							
2	MAD-42	13.11	SB	11	12	14.5	X	X				29.4			92	29.4	
2	MAD-42	13.14	SB	31	12	14.5	X	X				82.6			172	82.6	AT TOP OF 70 WB RAMP - NEW AC REPAIR
2	MAD-42	13.16	SB	10	12	14.5		X				13.3			44	13.3	
2	MAD-42	13.17	SB	10	12	14.5		X						13.3	44	13.3	
2	MAD-42	13.18	SB	10	12	14.5		X						13.3	44	13.3	
2	MAD-42	13.19	SB	10	12	14.5	X	X						26.6	88	26.6	
2	MAD-42	13.19	SB	10	20	14.5	X	X						22.2	120	22.2	
2	MAD-42	13.19	SB	10	20	14.5	X	X						22.2	120	22.2	
2	MAD-42	13.20	SB	10	20	14.5	X	X						22.2	120	22.2	
2	MAD-42	13.22	SB	216	20	4.5	X	X		480.0							
2	MAD-42	13.27	SB	6	15	14.5	X							10.0	42	10.0	IN SOUTHERN MCMAHAN DRIVE
2	MAD-42	13.29	SB	12	15	14.5	X							20.0	54	20.0	BETWEEN MCMAHAN DRIVES
2	MAD-42	13.36	SB	6	15	14.5	X							10.0	42	10.0	NEAR PILOT CAR ENTRANCE
2	MAD-42	13.37	SB	6	15	14.5	X							10.0	42	10.0	
2	MAD-42	13.37	SB	62	15	4.5	X			103.3							
QUANTITIES CARRIED TO PAVEMENT SUBSUMMARY										2,677		272		207	1,540	479	

FRA/MAD-70/42 SP FY24

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DESIGN AGENCY

DESIGNER RAM

REVIEWER XXX MM-DD-YY

PROJECT ID 108071

SHEET P.17 TOTAL 18