

ITEM 614 - MAINTAINING TRAFFIC

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A SINGLE PERIOD NOT TO EXCEED 60 CONSECUTIVE CALENDAR DAYS, WHEN THROUGH TRAFFIC MAY BE DETOURED AS SHOWN ON SHEET 9. REFER TO PN 129 AND THE WINDOW CONTRACT TABLE ON SHEET 7 FOR ADDITIONAL INFORMATION.

TEMPORARY LANE CLOSURES PER STANDARD CONSTRUCTION DRAWING MT-97.10, OUTSIDE OF THE 75 DAY CLOSURE, ARE ACCEPTABLE AT THE APPROVAL OF THE ENGINEER.

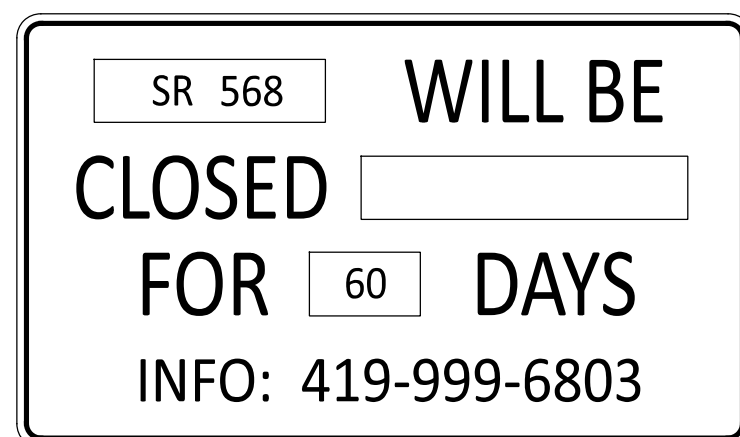
ACCESS TO ADJACENT PROPERTY WITHIN THE WORK LIMITS SHALL BE MAINTAINED BY THE CONTRACTOR AT ALL TIMES AS PER 614.02(a).

NOTICE OF CLOSURE SIGNS (W20-H13), SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. AFTER THE ROAD IS OPENED TO THROUGH TRAFFIC AND THE "NOTICE OF CLOSURE" SIGNS ARE NO LONGER NEEDED, THE CONTRACTOR SHALL REMOVE THE SIGNS.

NOTICE OF CLOSURE SIGN TIME TABLE

Table with 3 columns: ITEM, DURATION OF CLOSURE, SIGN DISPLAYED TO PUBLIC. Rows include ROAD CLOSURE with durations >= 2 WEEKS, >= 12 HOURS & < 2 WEEKS, and < 12 HOURS.

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION.



W20-H13

NOTE: THE CONTRACTOR IS TO SUPPLY THE DATE

THE ROADWAY CLOSURES SHALL BE ESTABLISHED, MAINTAINED, AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR AS PER STANDARD CONSTRUCTION DRAWING MT-101.60. THE DETOUR ROUTE SIGNING SHALL BE ESTABLISHED, MAINTAINED AND SUBSEQUENTLY REMOVED BY THE STATE OF OHIO. THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER 14 DAYS IN ADVANCE OF THE PLANNED ROAD CLOSURE TO ALLOW FOR COORDINATING THE DETOUR SIGNING.

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48 X 30 INCH ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AS DETAILED IN SCD MT-101.60 AT THE FOLLOWING LOCATIONS DURING PERIODS IN WHICH THE AFFECTED ROADS ARE CLOSED TO TRAFFIC.

SEE NEXT SHEET FOR LOCATIONS

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS AND SIGN SUPPORTS, AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AND TYPE III BARRICADES OF THE TYPE AND LOCATION AS FOLLOWS:

SEE NEXT SHEET FOR LOCATIONS

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE

Table with 3 columns: ITEM, DURATION OF CLOSURE, NOTICE DUE TO PERMITS & PIO. Rows include ROAD CLOSURES and LANE CLOSURES & RESTRICTIONS.

START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES 14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ITEM 614 - DETOUR SIGNING

THE CONTRACTOR SHALL PROVIDE, ERECT, MAINTAIN AND SUBSEQUENTLY REMOVE THE NO THRU TRAFFIC SIGNS (R11-3-66) AND LOCAL TRAFFIC ONLY SIGNS (R11-4-60) AND SIGN SUPPORTS, ON THE TOWNSHIP ROADS AS SHOWN ON SHEET 9, INCLUDING ANY TRAFFIC CONTROL DEVICES AS REQUIRED BY THE ODOT. THIS PAY ITEM HAS BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE WORK DESCRIBED ABOVE.

DESIGNATED LOCAL DETOUR ROUTE

IN ADDITION TO THE OFFICIAL, SIGNED DETOUR ROUTE, A LOCAL ROUTE HAS BEEN DETERMINED TO BE THE SECONDARY, UNSIGNED DETOUR ROUTE OR "DESIGNATED LOCAL DETOUR ROUTE." THIS ROUTE IS SHOWN ON THIS SHEET. DURING THE TIME THAT TRAFFIC IS DETOURED, THE CONTRACTOR SHALL MAINTAIN THIS ROUTE IN A CONDITION WHICH IS REASONABLY SMOOTH AND FREE FROM HOLES, RUTS, RIDGES, BUMPS, DUST & STANDING WATER. ONCE THE DETOUR IS REMOVED AND TRAFFIC RETURNED TO ITS NORMAL PATTERN, THE DESIGNATED LOCAL DETOUR ROUTE SHALL BE RESTORED TO A CONDITION THAT IS EQUIVALENT TO THAT WHICH EXISTED PRIOR TO ITS USE FOR THIS PURPOSE. ALL SUCH WORK SHALL BE PERFORMED WHEN AND AS DETERMINED BY THE ENGINEER. THE REPLACEMENT PAVEMENT FOR ITEM 253 PAVEMENT REPAIR SHALL CONSIST OF 1-1/4" ITEM 441, ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449) PG64-22 AND ITEM 407, TACK COAT PLACED ON 5" OF ITEM 301, ASPHALT CONCRETE BASE, PG64-22 (449).

THE FOLLOWING ESTIMATED QUANTITIES ARE PROVIDED FOR USE AS DETERMINED BY THE ENGINEER TO MAINTAIN AND SUBSEQUENTLY RESTORE THE DESIGNATED LOCAL DETOUR ROUTE.

- ITEM 253 - PAVEMENT REPAIR = 20 CY
ITEM 407 - TACK COAT = 20 GAL
ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), PG64-22 = 10 CY
ITEM 617 - COMPACTED AGGREGATE = 50 CY

ITEM 614 - WORK ZONE PAVEMENT MARKING

WORK ZONE PAVEMENT MARKING SHALL BE COMPLETE AND IN PLACE ON ALL NEW PAVEMENT PRIOR TO EXPOSING IT TO TRAFFIC. THE FOLLOWING ESTIMATED QUANTITY FOR THIS ITEM HAS BEEN PROVIDED FOR USE AS DIRECTED BY THE ENGINEER.

ITEM 614 - WORK ZONE CENTER LINE, CLASS II = 0.27 MILE

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING CONTINGENCY QUANTITY HAS BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616 - WATER = 4 MGAL

COORDINATION OF CONTRACTORS

SINCE THE MAINTENANCE OF TRAFFIC AND WORK ON THIS PROJECT MAY OVERLAP OTHER PROJECTS, IT IS ESSENTIAL THAT EACH CONTRACTOR CONDUCT THEIR WORK AND COOPERATE WITH EACH OTHER IN SUCH A MANNER AS NOT TO HINDER THE PROGRESS OR COMPLETION OF THE WORK BEING PERFORMED BY THE OTHER CONTRACTOR.

SPECIFICALLY, COORDINATION SHALL BE MADE WITH THE CONTRACTOR OF THE HAN-GREENWAY TRAIL EXTENSION 3 PROJECT, PID 117097. THE GREENWAY TRAIL PROJECT WILL BE CONSTRUCTING A NEW MULTIUSE PATH ALONG S.R. 568. THE NEW PATH INCLUDES A BRIDGE OVER GLAUNER DITCH, JUST NORTH OF THE S.R. 568 BRIDGE. IF THE CONTRACTOR FOR THE GREENWAY TRAIL PROJECT CHOOSES TO START WORK BEFORE THE COMPLETION OF THIS BRIDGE REPAIR PROJECT, THEY WILL BE PERMITTED TO UTILIZE THE EXISTING S.R. 568 RIGHT-OF-WAY TO CONSTRUCT THE MULTIUSE PATH. THEY WILL BE REQUIRED TO MAINTAIN A MINIMUM OF 11' OF EXISTING PAVEMENT WIDTH ON EACH SIDE OF THE BRIDGE TO MAINTAIN ACCESS TO AND FROM THE S.R. 568 BRIDGE BEING REPAIRED OVER GLAUNER DITCH. ADDITIONALLY, THE GREENWAY TRAIL PROJECT WILL NOT BE PERMITTED TO WORK WITHIN THE EXISTING S.R. 568 RIGHT-OF-WAY BETWEEN STA. 186+00 AND STA. 197+00. ANY DEVIATION FROM THIS MUST BE AGREED TO BY BOTH CONTRACTORS AND ODOT'S PROJECT ENGINEER.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS LOCATIONS ARE SHOWN ON SHEET 9. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 6 SNMT

ESTIMATING 3 PCMS SIGNS FOR 2 MONTHS

DESIGN AGENCY




Table with 2 columns: DESIGNER (MJM), REVIEWER (RJM), PROJECT ID (98521), SHEET (P.8), TOTAL (66)

SHEET NUM.												PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.
6	8	14	15	45	50	64	01/BRO/13	EXT	TOTAL									
					80			80	519	11100	80	SF	PATCHING CONCRETE STRUCTURE					
					181			181	526	25000	181	SY	REINFORCED CONCRETE APPROACH SLABS (T=15")					
					22			22	843	50000	22	SF	PATCHING CONCRETE STRUCTURES WITH TROWELABLE MORTAR					
					1,299			1,299	848	10200	1,299	SY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, (T=2")					
					1,299			1,299	848	20000	1,299	SY	SURFACE PREPARATION USING HYDRODEMOLITION, (T=1")					
					43			43	848	30200	43	CY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY					
					77			77	848	50000	77	SY	HAND CHIPPING					
					LS			LS	848	50100	LS		TEST SLAB					
					3			3	848	50200	3	CY	FULL-DEPTH REPAIR					
					181			181	848	50300	181	SY	WEARING COURSE REMOVED, ASPHALT					
													STRUCTURE OVER 20 FOOT SPAN (HAN-568-0363)					
						LS		LS	202	11203	LS		PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN	63				
						123		123	202	38500	123	FT	BRIDGE RAILING REMOVED					
						7,871		7,871	509	10001	7,871	LB	EPOXY COATED STEEL REINFORCEMENT, AS PER PLAN	63				
						80		80	509	20001	80	LB	CONCRETE REINFORCEMENT, REPLACEMENT OF EXISTING CONCRETE REINFORCEMENT, AS PER PLAN	63				
						26		26	511	34410	26	CY	CLASS QC2 CONCRETE, SUPERSTRUCTURE					
						74		74	512	10100	74	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)					
						28		28	512	10300	28	SY	SEALING CONCRETE BRIDGE DECKS WITH HMWM RESIN					
						92		92	516	31000	92	FT	JOINT SEALER					
						130		130	517	70100	130	FT	RAILING (THREE STEEL TUBE BRIDGE RAILING)					
						146		146	SPECIAL	51822300	146	FT	STEEL DRIP STRIP, (SEE STANDARD CONSTRUCTION DRAWING DS-1-92)					
						41		41	519	11100	41	SF	PATCHING CONCRETE STRUCTURE					
						1,757		1,757	SPECIAL	53000300	1,757	LB	STRUCTURES MISC.: WELDED WIRE REINFORCEMENT	63,65				
						510		510	848	10200	510	SY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, (T=5")					
						560		560	848	20000	560	SY	SURFACE PREPARATION USING HYDRODEMOLITION, (T=1")					
						17		17	848	30200	17	CY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY					
						31		31	848	50000	31	SY	HAND CHIPPING					
						LS		LS	848	50100	LS		TEST SLAB					
						3		3	848	50200	3	CY	FULL-DEPTH REPAIR					
						560		560	848	50300	560	SY	WEARING COURSE REMOVED, ASPHALT					
													MAINTENANCE OF TRAFFIC					
	20							20	253	02000	20	CY	PAVEMENT REPAIR					
	20							20	407	10000	20	GAL	TACK COAT					
	10							10	441	70000	10	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), PG 64-22					
	6							6	614	18601	6	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN					
	0.27							0.27	614	21400	0.27	MLE	WORK ZONE CENTER LINE, CLASS II					
	4							4	616	10000	4	MGAL	WATER					
	50							50	617	10100	50	CY	COMPACTED AGGREGATE					
													INCIDENTALS					
								LS	614	11000	LS		MAINTAINING TRAFFIC					
								LS	614	12420	LS		DETOUR SIGNING					
								LS	623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING					
								LS	624	10000	LS		MOBILIZATION					

GENERAL SUMMARY

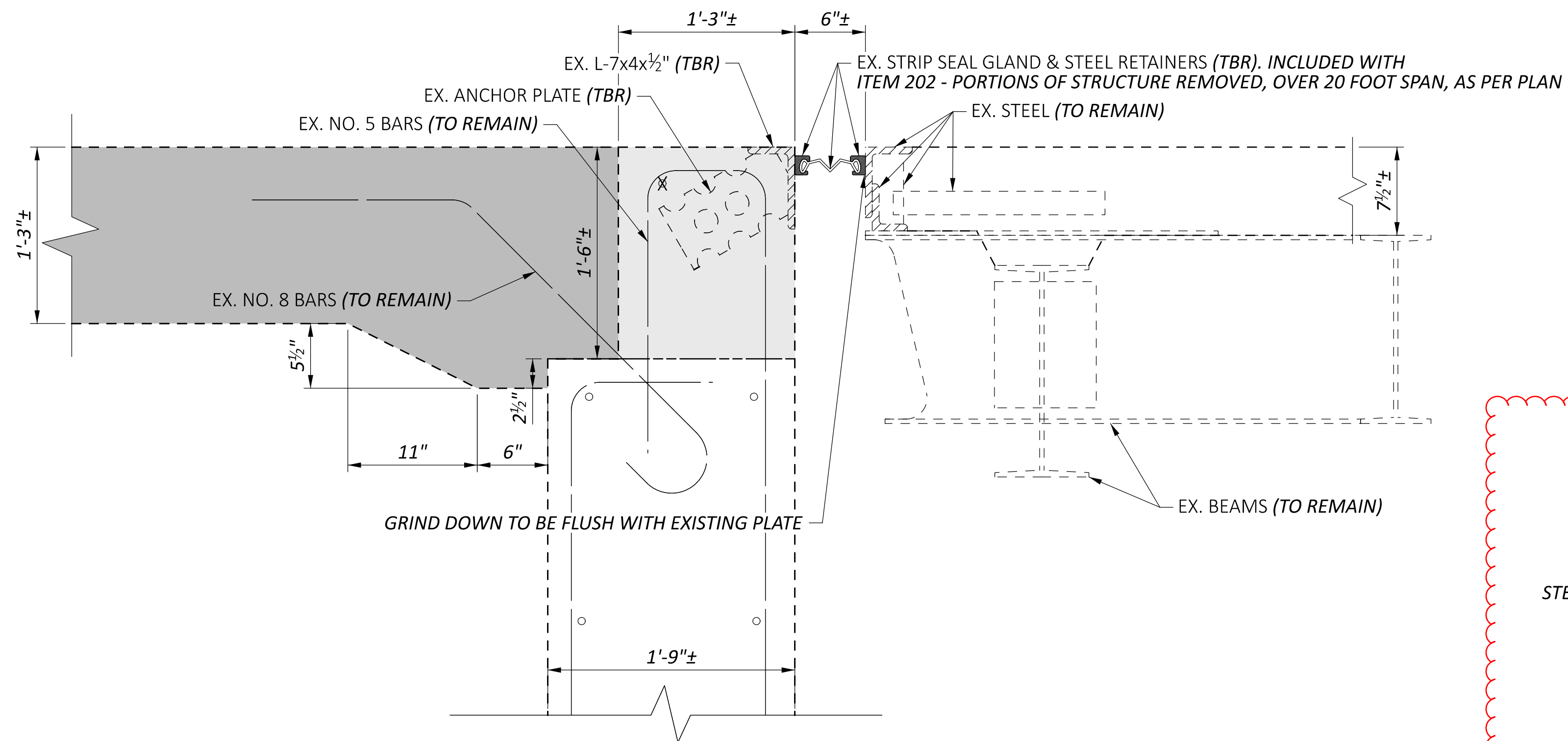
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DESIGN AGENCY

 DESIGNER: RJM
 REVIEWER: EED
 PROJECT ID: 98521
 SHEET: P.11 TOTAL: 66

TRAFFIC SIGNS AND SIGN SUPPORTS												
REFERENCE NO.	SHEET	ROUTE	STATION	SIDE	CODE	SIZE (INCHES)	630					
							GROUND MOUNTED SUPPORT, NO. 3 POST		SIGN POST REFLECTOR	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	REMOVAL OF GROUND MOUNTED SIGN AND REERECTION
							FT	EACH				
S-101	46	S.R. 568	151+59.76	LT	W1-7-48	48 X 24	13	13	2		2	1
S-102	46	S.R. 568	152+12.58	RT	D3-1-24	24 X 8	12				1	1
S-103	46	S.R. 568	152+08.08	RT	R1-1-36	36 X 36	14	14	2		1	1
S-104	46	S.R. 568	152+47.26	RT	I-3-36	36 X 18	12.5				1	1
S-105	46	S.R. 568	151+02.50	RT	R1-1-36	36 X 36	14	14	2		1	1
S-106	46	S.R. 568	152+43.65	RT	I-H25a-12	12 X 12	10				1	1
S-107	46	S.R. 568	155+63.12	LT	I-3-36	36 X 18	12.5				1	1
S-108	46	S.R. 568	159+22.33	RT	W11-3-30	30 X 30				1	2	
					D10-H8-12	12 X 12	10					1
S-201	47	S.R. 586	191+16.87	RT	I-H25a-12	12 X 12	10				1	1
TOTALS CARRIED TO GENERAL SUMMARY							149.0		6	1	11	9

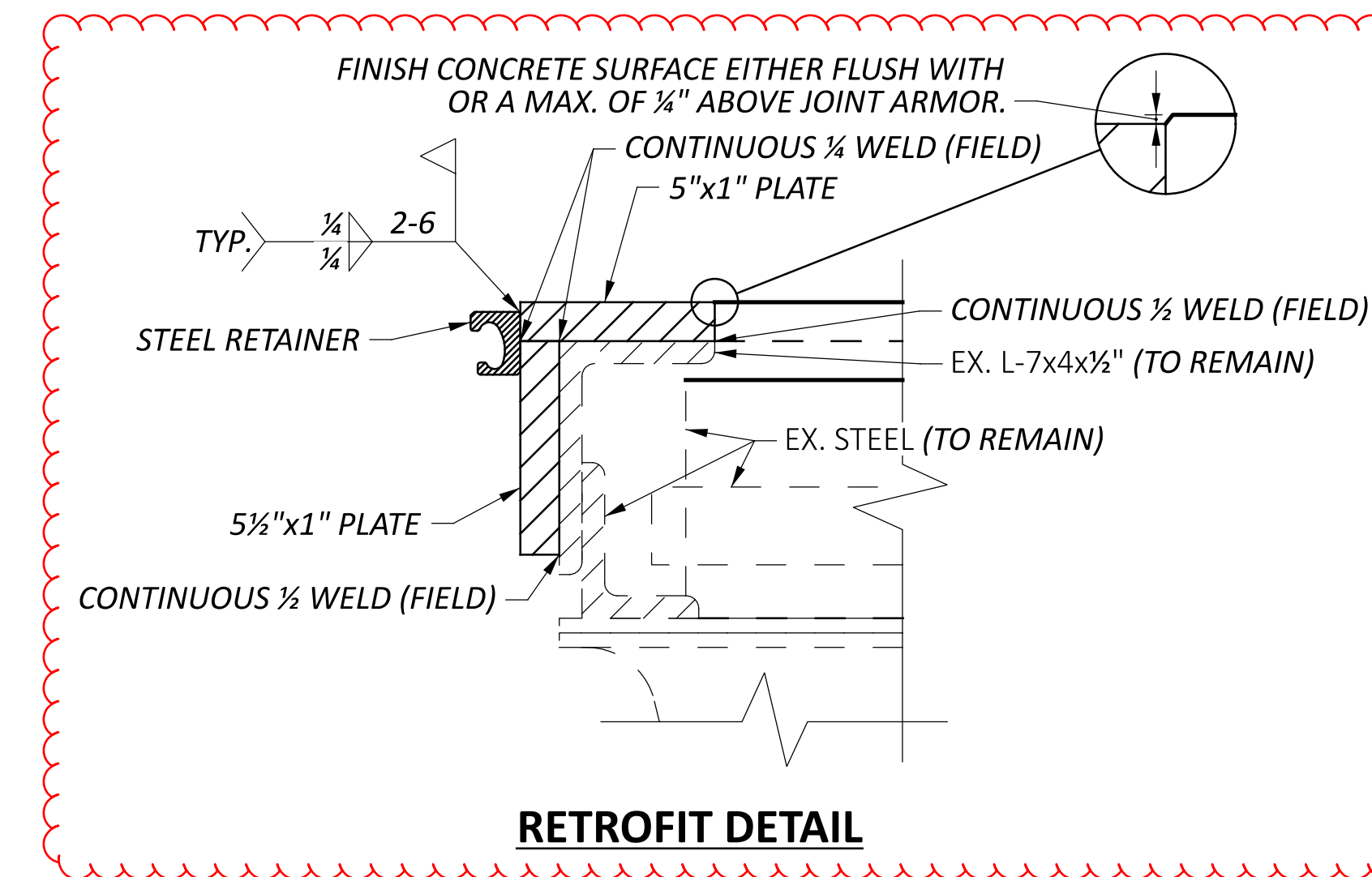
RPM's AND TRAFFIC PAINT						
SHEET	ROUTE	621		642		644
		RPM	RAISED PAVEMENT MARKER REMOVED	EDGE LINE, 6", TYPE 1	CENTER LINE, TYPE 1	STOP LINE
		Y/Y EACH	EACH	MILES	MILES	FT
46	S.R. 568	5	8	0.26	0.12	
46	T.R. 234			0.04	0.01	32
47	S.R. 568	6	6	0.22	0.11	
TOTALS CARRIED TO GENERAL SUMMARY		11	14	0.52	0.24	32



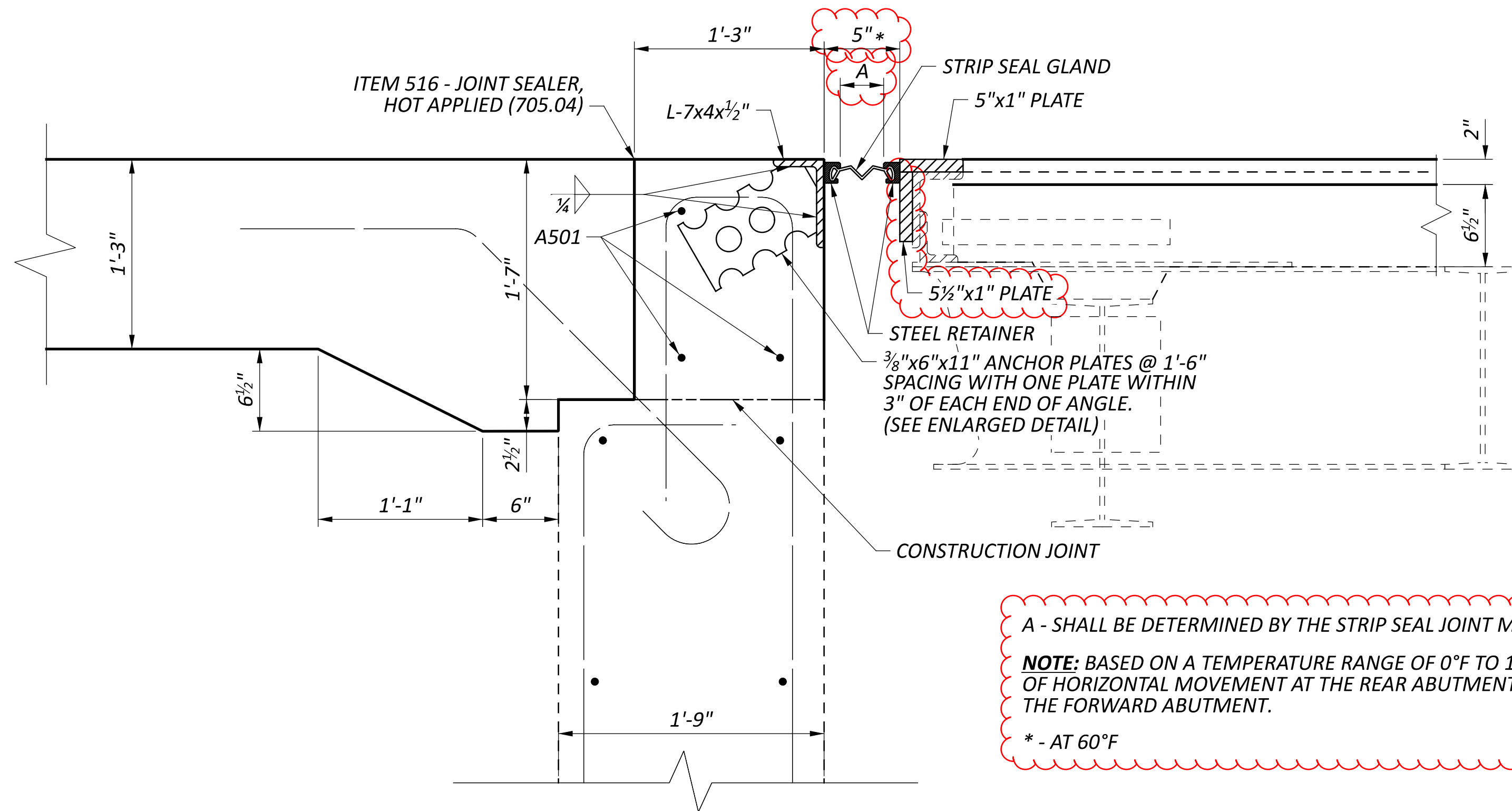


EXISTING MODIFIED EXPANSION JOINT DETAILS

	- ITEM 202 - APPROACH SLAB REMOVED (T=15")
	- ITEM 202 - PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN
	- EX. BARS TO BE REMOVED
TBR	- TO BE REMOVED

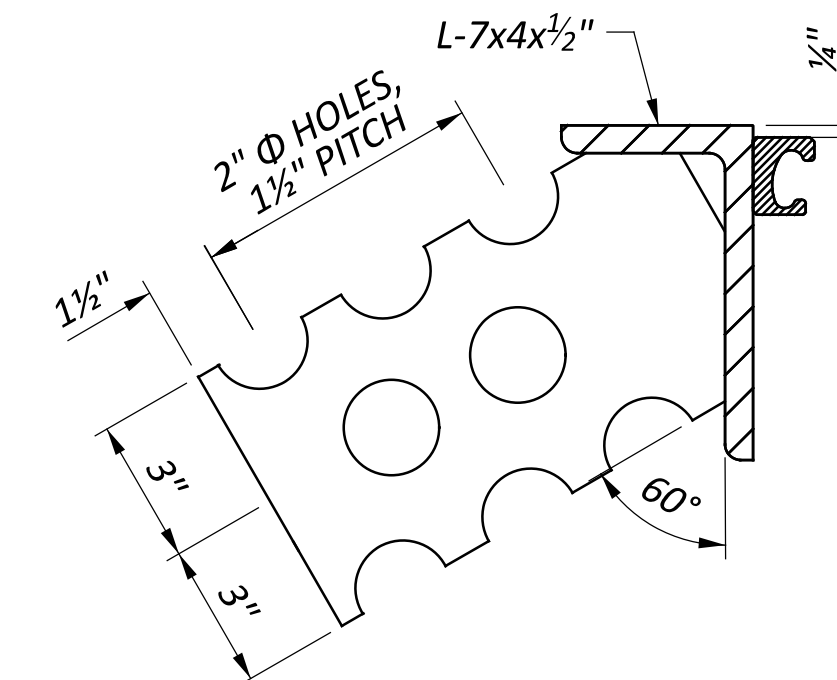


RETROFIT DETAIL



PROPOSED MODIFIED EXPANSION JOINT DETAILS

A - SHALL BE DETERMINED BY THE STRIP SEAL JOINT MANUFACTURER. SEE SCD EXJ-4-87.
NOTE: BASED ON A TEMPERATURE RANGE OF 0°F TO 120°F, IT IS ESTIMATED THERE WILL BE 1.3"± OF HORIZONTAL MOVEMENT AT THE REAR ABUTMENT AND 2"± OF HORIZONTAL MOVEMENT AT THE FORWARD ABUTMENT.
 * - AT 60°F



ANCHOR PLATE DETAIL

SFN	3204839
DESIGN AGENCY	
DESIGNER	CHECKER
RJM	EED
REVIEWER	
MJM	06-23-23
PROJECT ID	98521
SUBSET	TOTAL
13	14
SHEET	TOTAL
P.60	66