

ITEM 614, MAINTAINING TRAFFIC

THIS PROJECT IS A MAJOR REHABILITATION PROJECT TO REMOVE AND REPLACE THE EXISTING PAVEMENT ON US -6/US-24 FROM THE US -6 INTERCHANGE TO THE MAUMEE RIVER BRIDGE; INCLUDING BRIDGE REPAIR AND UPGRADES TO ALL INTERCHANGE RAMPS.

ITEM 614, MAINTAINING TRAFFIC (TIME LIMITATION ON A DETOUR)

A MINIMUM OF 1 LANES OF TRAFFIC IN EACH DIRECTION ON US-6/US-24 SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT AND ITEM 615 FOR MAINTAINING TRAFFIC, AND TEMPORARY SURFACES USING ITEMS 410 AND 614 UNLESS OTHERWISE NOTED.

RAMP CLOSURES SHALL BE LIMITED TO THE RESTRICTIONS SHOWN IN THE RAMP CLOSURE TABLE. ANY DISINCENTIVE IN THE AMOUNT SHOWN IN THE RAMP CLOSURE TABLE SHALL BE ASSESSED FOR EACH CALENDAR DAY THAT RAMPS REMAIN CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS:

NEW YEAR'S (OBSERVED)	GENERAL/REGULAR ELECTION (NOV)
THANKSGIVING	MEMORIAL DAY
CHRISTMAS (OBSERVED)	FOURTH OF JULY (OBSERVED)
LABOR DAY	EASTER

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY TIME ALL LANES
OR SPECIAL EVENT MUST BE OPEN TO TRAFFIC

SUNDAY 12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY 12:00N FRIDAY THROUGH 6:00 AM TUESDAY
12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY 12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY (GEN./REG. ELECTION)
5:00 AM TUESDAY THROUGH 12:00 AM WEDNESDAY
WEDNESDAY 12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY 12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)
6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY 12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY 12:00N FRIDAY THROUGH 6:00 AM MONDAY

DURING THE SAME PERIODS, MAINTAIN PEDESTRAIN ACCESS IF PEDESTRIAN ACCESS WAS PRESENT PRIOR TO CONSTRUCTION.

[NEWLY CONSTRUCTED LANE ADDITIONS, ONCE COMPLETED AND INITIALLY OPENED TO TRAFFIC, SHALL BE OPEN TO TRAFFIC DURING ALL SUBSEQUENT DESIGNATED HOLIDAYS AND SPECIAL EVENTS, AND RELATED PERIODS OF TIME, SPECIFIED ABOVE.]

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

ITEM 614, MAINTAINING TRAFFIC (LANE CLOSURE/REDUCTION REQUIRED)

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. [AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.]

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE
ITEM DURATION SIGN DISPLAYED
OF CLOSURE TO PUBLIC

RAMP & >=2 WEEKS 14 CALENDAR DAYS
PRIOR TO CLOSURE

ROAD > 12 HOURS 7 CALENDAR DAYS
& < 2 WEEKS PRIOR TO CLOSURE

CLOSURES <= 12 HOURS 2 BUSINESS DAYS
PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

ITEM 614, MAINTAINING TRAFFIC (ESTIMATED QUANTITIES)

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC.

ITEM 410, TRAFFIC COMPACTED SURFACE, TYPE A OR B 100 CU. YD.
ITEM 614, ASPHALT CONCRETE FOR MAINTAINING TRAFFIC 30 CU. YD.
ITEM 614 DETOUR SIGNING LS
ITEM 615 ROADS FOR MAINTAINING TRAFFIC LS

ITEMS ADJUSTED TO GRADE

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 611 - CATCH BASIN TO BE ADJUSTED TO GRADE 10 EA

MAINTAINING DRAINAGE DURING CONSTRUCTION

THE CONTRACTOR SHALL MAINTAIN DRAINAGE DURING CONSTRUCTION AT ALL TIMES. THIS SHALL BE ACHIEVED BY THE USE OF THE EXISTING DRAINAGE PIPES, DITCHES, CULVERTS, ETC. WHEN AT ALL POSSIBLE, AS WELL AS TEMPORARY AND PROPOSED DRAINAGE ITEMS.

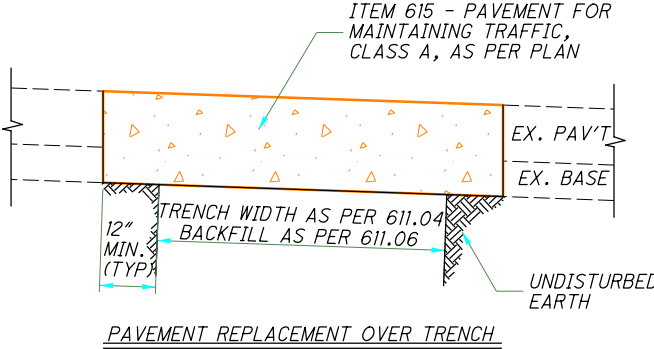
GENERALLY, THE EXISTING DRAINAGE DEVICES SHALL REMAIN IN PLACE AND IN OPERATION UNTIL THE PROPOSED FEATURES ARE CONSTRUCTED AND OPERATIONAL. WHEN EXISTING DEVICES ARE NO LONGER NEEDED THEY SHALL BE REMOVED IF POSSIBLE OR PROPERLY PLUGGED AND FILLED. AT PROPOSED CULVERT CROSSINGS AND/OR MEDIAN STORM CROSSINGS, THE CONTRACTOR MAY HAVE THE OPTION OF INSTALLING TEMPORARY SHEETING OR BORING AND JACKING A PORTION OF THE PIPE UNDER THE ACTIVE LANES AT THE PHASE CUT LINE FOR THE PART-WIDTH CONSTRUCTION PHASES.

UNLESS SEPARATELY ITEMIZED IN THE PLANS, ALL LABOR, EQUIPMENT, MATERIALS REQUIRED TO MAINTAIN DRAINAGE DURING CONSTRUCTION, INCLUDING SUBSEQUENT REMOVAL OF ANY TEMPORARY ITEMS, SHALL BE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

615, PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN

CONDUIT CROSSINGS

THE CONTRACTOR SHALL CONSTRUCT CONDUIT CROSSINGS OF EXISTING PAVEMENT DURING WEEKEND CLOSURES, AS APPROVED BY THE ENGINEER, OR DURING SUBSEQUENT PHASES PER THE SEQUENCE OF TRAFFIC NOTES. AT THE CONCLUSION OF THE CONDUIT INSTALLATION, THE CONTRACTOR SHALL BACKFILL AND PLACE TEMPORARY PAVEMENT PER THE DETAIL BELOW OR PER THE TRENCH REPAIR DETAIL LOCATED IN THE PLANS.



THE FOLLOWING ESTIMATED QUANTITY OF 400.3 SQ. YD. HAS BEEN INCLUDED IN THE GENERAL SUMMARY.

ITEM 614, MAINTAINING TRAFFIC (CLOSING PARAGRAPH FOR NOTE)

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

PLACEMENT OF ASPHALT CONCRETE

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

TRENCH FOR WIDENING

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

OVERNIGHT TRENCH CLOSING

THE BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN 2 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT EXCEPT FOR A SHORT LENGTH (25 FEET OR LESS) OF A WORK SECTION AT THE END OF THE TRENCH. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING SHALL BE BACKFILLED AT THE DIRECTION OF THE ENGINEER.

DRUM REQUIREMENTS

IN ADDITION TO THE REQUIREMENTS OF THE PLANS, SPECIFICATION AND PROPOSAL, DRUMS FURNISHED BY THE CONTRACTOR SHALL BE NEW AND UNUSED AT THE TIME OF ARRIVAL ON THE PROJECT. ANY DRUMS BROUGHT ON THE PROJECT, WHICH HAVE PREVIOUSLY BEEN USED ELSEWHERE, WILL NOT BE ACCEPTED.

PAYMENT FOR DRUMS SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED.

PERMITTED LANE CLOSURE SCHEDULE (PLCS)

LANE CLOSURE(S) SHALL CONFORM TO THE PLCS. PUBLISHED PLCS INFORMATION CAN BE FOUND ON THE ODOT WEBSITE AT: [HTTPS://WWW.TRANSPORTATION.OHIO.GOV/WPS/PORTAL/GOV/ODOT/WORKING/DATA-TOOLS/RESOURCES/PERMITTED-LANE-CLOSURE](https://www.transportation.ohio.gov/wps/portal/gov/odot/working/data-tools/resources/permitted-lane-closure)

THE MONTHLY PUBLISHED SCHEDULES REQUIRED TO BE USED, FOR EACH PLCS SEGMENT WITHIN THE PROJECT AREA, ARE THOSE THAT COMPRISE THE CONSECUTIVE 12-MONTH PERIOD BEGINNING 15 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE AND ENDING 4 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE. THESE SAME 12 MONTHS APPLY FOR THE LIFE OF THE PROJECT AND SHALL BE APPLIED TO EACH RESPECTIVE MONTH OF CONSTRUCTION (MONTH OF LANE CLOSURE(S) SHALL MATCH MONTH OF PLCS USED). LANE CLOSURE(S) IN PLACE FOR MULTIPLE MONTHS SHALL ALWAYS COMPLY WITH THE CURRENT RESPECTIVE MONTH.

(FOR EXAMPLE: IF THE SALE DATE FOR THE PROJECT WAS MARCH OF 2021, THE MONTHLY PUBLISHED SCHEDULES FOR EACH APPLICABLE PLCS SEGMENT WOULD BE DECEMBER 2019 TO NOVEMBER 2020. IF THIS WAS A THREE-YEAR PROJECT, YEAR THREE WOULD STILL BE USING THE DECEMBER 2019 TO NOVEMBER 2020 MONTHLY SCHEDULES. IF THE PROJECT DESIRED TO CLOSE TWO LANES IN JUNE 2021, REFERENCE WOULD BE MADE TO THE JUNE 2020 SCHEDULE(S) FOR THE RESPECTIVE PLCS SEGMENT(S). IF THE SAME TWO LANES WERE DESIRED TO BE CLOSED AGAIN IN JULY 2021, REFERENCE WOULD BE MADE TO THE JULY 2020 SCHEDULE(S) FOR THE RESPECTIVE PLCS SEGMENT(S).)

MORE RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE AT THE DISCRETION OF THE ENGINEER IN ORDER TO COMPLY WITH THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

DESIGN AGENCY



DESIGNER

DEK

REVIEWER

JJM MM-DD-YY

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110524

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LESS RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE SUBJECT TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)) AND SHALL NOT BE IMPLEMENTED UNTIL, AND UNLESS, APPROVED BY THE PROPER ODOT AUTHORITY. [EXISTING MOT EXCEPTIONS THAT HAVE ALREADY BEEN APPROVED IN ACCORDANCE TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY AND STANDARD PROCEDURE ARE DETAILED IN THE APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION(S) PLAN NOTE.]

ALLOWABLE LANE CLOSURE HOURS FOR FACILITIES NOT COVERED BY THE PLCS, IF ANY, SHALL BE AS SPECIFIED ELSEWHERE IN THE PLANS.

DROPOFFS IN THE WORK ZONE

THE OPTIONAL WEDGE TREATMENT AS DETAIL ON MT-101.90 WILL BE REQUIRED AND SHALL BE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 614. MAINTAINING TRAFFIC FOR MAINLINE AND RAMP WORK.

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616, WATER 600 M. GAL.

WORK ZONE MARKINGS AND SIGNS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

ITEM 614, WORK ZONE MARKING SIGN	50 EACH
ITEM 614, W.Z. DOTTED LINE, CLASS I, 6", 807 PAINT	2800 FEET
ITEM 614, W.Z. DOTTED LINE, CLASS I, 6"	2800 FEET
ITEM 614, W.Z. EDGE LINE, CLASS I, 6", 807 PAINT	6 MILE
ITEM 614, W.Z. EDGE LINE, CLASS I, 6"	6 MILE
ITEM 614, W.Z. CHANNELIZING LINE, CLASS I, 12", 807 PAINT	1000 FEET
ITEM 614, W.Z. CHANNELIZING LINE, CLASS I, 12"	1000 FEET
ITEM 614, W.Z. CHANNELIZING LINE, CLASS III, 12", 642 PAINT	1000 FEET
ITEM 614, W.Z. STOP LINE, CLASS I	200 FEET
ITEM 614, W.Z. STOP LINE, CLASS III, 642 PAINT	200 FEET
ITEM 614, W.Z. ARROW, CLASS I	12 EACH
ITEM 614, W.Z. ARROW, CLASS III, 642 PAINT	12 EACH

ITEM 622, PORTABLE BARRIER, 50", AS PER PLAN

THIS WORK SHALL CONSIST OF FURNISHING, MAINTAINING, AND SUBSEQUENTLY REMOVING A 50-INCH PORTABLE BARRIER AT THE LOCATIONS SHOWN ON THE PLANS. FOR DETAILS, SEE SCD RM-4.1.

PORTABLE STEEL BARRIER IS AN APPROVED ALTERNATIVE TO PORTABLE CONCRETE BARRIER. FOR INFORMATION ON APPROVED VENDORS, SEE THE APPROVED PRODUCTS LIST MAINTAINED BY THE OFFICE OF ROADWAY ENGINEERING.

PORTABLE BARRIER, 32 INCHES HIGH WITH AN 18-INCH MINIMUM HEIGHT GLARE SCREEN MAY BE USED AT THE OPTION OF THE CONTRACTOR. THE GLARE SCREEN SHALL BE CONSTRUCTED USING ONE OF THE SCREENS PROVIDED ON THE APPROVED LIST, AVAILABLE ON THE OFFICE OF ROADWAY ENGINEERING WEBSITE.

PADDLE OR INTERMITTENT TYPE GLARE SCREENS SHALL BE DESIGNED USING A 20 DEGREE CUT-OFF ANGLE BASED ON TANGENT ALIGNMENT. THAT SPACING SHALL BE USED THROUGHOUT THE BARRIER LENGTH WITHOUT REGARD TO BARRIER CURVATURE.

THE GLARE SCREEN SYSTEM SHALL BE SECURELY FASTENED TO THE 32-INCH PORTABLE BARRIER USING THE HARDWARE AND PROCEDURES SPECIFIED BY THE MANUFACTURER.

FOR DIRECTIONS ON HOW TO INSTALL THE GLARE SCREEN AND THE BARRIER, SEE THE MANUFACTURER'S INSTRUCTIONS.

PAYMENT SHALL INCLUDE ALL LABOR, MATERIAL, AND EQUIPMENT NECESSARY TO PERFORM THE WORK AND SHALL BE PAID FOR AT THE CONTRACT PRICE PER FOOT FOR ITEM 622, PORTABLE BARRIER, 50", AS PER PLAN.

ITEM 614, REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 25 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

ITEM 614, REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 250 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

WORK ZONE SPEED ZONES (WZSZS)

THE FOLLOWING WORK ZONE SPEED ZONE (WZSZ) SPEED LIMIT REVISION(S) HAVE BEEN APPROVED FOR USE ON THIS PROJECT WHEN WORK ZONE CONDITIONS AND FACTORS ARE MET AS DESCRIBED BELOW:

WZSZ REVISION NUMBER(S) COUNTY-ROUTE-SECTION(S) DIRECTION(S) WZ-15270, HEN-24-(4.15-10.83), EB/WB

POTENTIAL WZSZ LOCATIONS SHALL HAVE AN ORIGINAL (PRE-CONSTRUCTION) POSTED SPEED LIMIT OF 55 MPH OR GREATER, A QUALIFYING WORK ZONE CONDITION OF AT LEAST 0.5 MILE IN LENGTH, AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS, AND A WORK ZONE CONDITION IN PLACE THAT REDUCES THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS (I.E., LANE CLOSURE, LANE SHIFT, CROSSOVER, CONTRAFLOW AND/OR SHOULDER CLOSURE). THE LENGTH OF THE WORK ZONE CONDITION IS MEASURED FROM THE BEGINNING OF THE TAPER FOR THE SUBJECT WORK ZONE CONDITION IMPACTING THE TRAVEL LANES AND/OR SHOULDER TO THE END OF THE DOWNSTREAM TAPER, WHERE DRIVERS ARE RETURNED TO TYPICAL ALIGNMENT. AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS IS REQUIRED TO BALANCE THE ADDITIONAL EXPOSURE CREATED BY INSTALLING AND REMOVING WZSZ SIGNING WITH THE TIME NEEDED TO COMPLETE THE WORK.

IF THE WORK ZONE MEETS THESE MINIMUM CRITERIA, IT SHALL BE ANALYZED FURTHER USING TABLE 1 BELOW TO DETERMINE IF AND WHEN IT QUALIFIES FOR A SPEED LIMIT REDUCTION. DEPENDING ON THE ORIGINAL POSTED SPEED LIMIT, THE TYPE OF TEMPORARY TRAFFIC CONTROL USED, AND WHETHER OR NOT WORKERS ARE PRESENT, A WARRANTED WZSZ WILL VARY IN THE APPROVED SPEED LIMIT TO BE POSTED OVER TIME.

C&MS ITEM 614, PARAGRAPH 614.02(B), INDICATES THAT TWO DIRECTIONS OF A DIVIDED HIGHWAY ARE CONSIDERED SEPARATE HIGHWAY SECTIONS. THEREFORE, IF THE WORK ON A MULTI-LANE DIVIDED HIGHWAY IS LIMITED TO ONLY ONE DIRECTION, A SPEED LIMIT REDUCTION IN THE DIRECTION OF THE WORK DOES NOT AUTOMATICALLY CONSTITUTE A SPEED LIMIT REDUCTION IN THE OPPOSITE DIRECTION. EACH DIRECTION SHALL BE ANALYZED INDEPENDENTLY FROM EACH OTHER.

ALL WZSZS FLUCTUATE BETWEEN TWO APPROVED REDUCED SPEED LIMITS OR BETWEEN AN APPROVED REDUCED SPEED LIMIT AND THE ORIGINAL POSTED SPEED LIMIT. ONLY ONE OF TWO SIGNING STRATEGIES SHALL BE USED TO IMPLEMENT A WZSZ.

WZSZS USING DSL SIGN ASSEMBLIES SHALL BE IN ACCORDANCE WITH THIS NOTE, APPROVED LIST, SUPPLEMENTAL SPECIFICATIONS (SS) 808 AND 908, AND TRAFFIC SCD MT-104.10.

ONLY ONE WARRANTED SPEED LIMIT APPLIES AT ANY ONE TIME; SPEED LIMIT REDUCTIONS ARE NOT CUMULATIVE. WZSZS SHALL NOT BE USED FOR MOVING/MOBILE ACTIVITIES, AS DEFINED IN OMUTCD PART 6.

WORK ZONE SPEED ZONES (WZSZS) (CONT)

WHEN LOOKING UP THE WARRANTED WORK ZONE SPEED LIMITS, ALWAYS USE THE ORIGINAL, PRECONSTRUCTION, POSTED SPEED LIMIT. DO NOT USE A PRIOR OR CURRENT WORK ZONE SPEED LIMIT AS A LOOK UP VALUE IN THE TABLE. POSITIVE PROTECTION IS GENERALLY REGARDED AS PORTABLE BARRIER OR OTHER RIGID BARRIER IN USE ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WITHOUT POSITIVE PROTECTION IS GENERALLY REGARDED AS USING DRUMS, CONES, SHADOW VEHICLE, ETC., ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WORKERS ARE CONSIDERED AS BEING PRESENT WHEN ON-SITE, WORKING WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WHEN THE WORK ZONE CONDITION REDUCING THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS IS REMOVED, THE SPEED LIMIT DISPLAYED SHALL RETURN TO THE ORIGINAL POSTED SPEED LIMIT.

TABLE 1: WARRANTED WORK ZONE SPEED LIMITS (MPH) FOR WORK ZONES ON HIGH-SPEED (55 MPH OR GREATER) MULTI-LANE HIGHWAYS

ORIGINAL POSTED SPEED LIMIT	WITH POSITIVE PROTECTION		WITHOUT POSITIVE PROTECTION	
	WORKERS PRESENT	WORKERS NOT PRESENT	WORKERS PRESENT	WORKERS NOT PRESENT
70	60	65	55	65
65	55	60	50	60
60	55	60	50	60
55	50	55	45	55

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

[ITEM 808, DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY 527 SIGN MNTH]
[ASSUMING 17 DSL SIGN ASSEMBLY(IES) FOR 31 MONTH(S)]

WORK ZONE INCREASED PENALTIES SIGN (R11-H5A)

R11-H5A-48 SIGNS SHALL BE FURNISHED, ERECTED, AND MAINTAINED IN GOOD CONDITION AND/OR REPLACED AS NECESSARY AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. SIGNS SHALL BE MOUNTED AT THE APPROPRIATE OFFSETS AND ELEVATIONS AS PRESCRIBED BY THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THEY SHALL BE MAINTAINED ON SUPPORTS MEETING CURRENT SAFETY CRITERIA.

THE SIGNS MAY BE ERECTED OR UNCOVERED NO MORE THAN FOUR HOURS BEFORE THE ACTUAL START OF WORK. THE SIGNS SHALL BE REMOVED OR COVERED NO LATER THAN FOUR HOURS FOLLOWING RESTORATION OF ALL LANES TO TRAFFIC WITH NO RESTRICTIONS, OR SOONER AS DIRECTED BY THE ENGINEER. TEMPORARY SIGN COVERING AND UNCOVERING DUE TO TEMPORARY LANE RESTORATIONS SHALL BE GUIDED BY THE FOUR-HOUR LIMITATIONS STATED ABOVE. SUCH LANE RESTORATIONS SHOULD BE EXPECTED TO REMAIN IN EFFECT FOR 30 OR MORE CONSECUTIVE CALENDAR DAYS, SUCH AS DURING WINTER SHUT-DOWNS.

(THE SIGNS ON THE MAINLINE SHALL BE DUAL MOUNTED UNLESS NOT PHYSICALLY POSSIBLE. THE FIRST SIGN SHALL BE PLACED BETWEEN THE ROAD WORK AHEAD (W20-1) SIGN AND THE NEXT SIGN IN THE SEQUENCE. SIGNS SHALL BE ERECTED ON EACH ENTRANCE RAMP AND EVERY 2 MILES THROUGH THE CONSTRUCTION WORK LIMITS. SIGNS ON THE MAINLINE SHALL BE R11-H5A-48. SIGNS USED ON THE RAMPS SHALL BE R11-H5A-24. R11-H5A-24 SIGNS MAY BE USED IN THE MEDIAN IN LIEU OF R11-H5A-48 SIGNS IF IT IS NOT PHYSICALLY POSSIBLE TO PROVIDE R11-H5A-48 SIGNS IN THE MEDIAN.)

THE R11-H5A-48 SIGNS SHALL BE MOUNTED ON 2 NO. 3 POSTS WHEN LOCATED WITHIN CLEAR ZONES.

THE CONTRACTOR MAY USE SIGNS AND SUPPORTS IN USED, BUT GOOD, CONDITION PROVIDED THE SIGNS MEET CURRENT ODOT SPECIFICATIONS.

WORK ZONE INCREASED PENALTIES SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS, INCLUDING THE SIGN AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION AS DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.

PAYMENT FOR ACCEPTED QUANTITIES, COMPLETE, IN PLACE WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, ERECTING, MAINTAINING, COVERING DURING SUSPENSION OF WORK, AND REMOVAL OF THE SIGN AND SUPPORT.

ITEM 614, WORK ZONE INCREASED PENALTIES SIGN 20 EACH

DESIGN AGENCY



DESIGNER

DEK

REVIEWER

JJM MM-DD-YY

PROJECT ID

110524

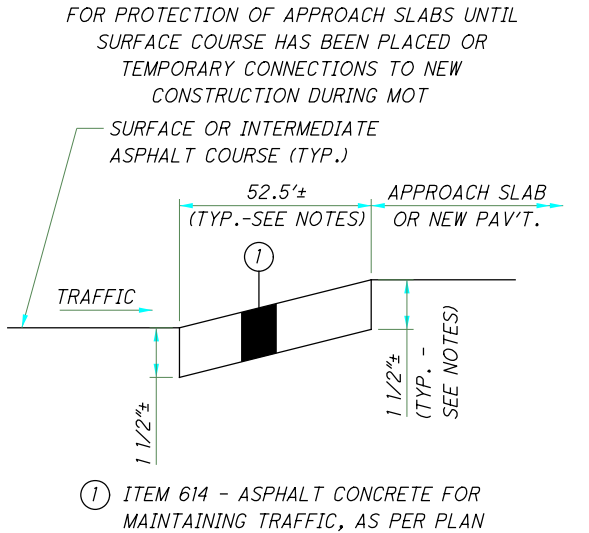
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TOTAL

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PAVEMENT WEDGING



WORK ZONE PAVING AT APPROACH SLABS AND PROPOSED PAVEMENT DETAILS (NOT TO SCALE)

NOTES:
THE CONTRACTOR SHALL PLACE A BUTT JOINT AS DETAILED BELOW AT EACH MAINLINE BRIDGE AND/OR PHASE JOINT LINE, FOR FURTHER INFORMATION SEE SCD-3.1.

PAVEMENT ELEVATION DIFFERENCE BETWEEN OLD AND NEW MAY VARY AT SOME LOCATIONS. THE CONTRACTOR SHALL FURNISH WEDGING AS NEEDED TO MEET THE REQUIRED GRADE.

THE CONTRACTOR SHALL COMPLETE THE ABOVE WORK, PRIOR TO OPENING THE AREA TO TRAFFIC. PRIOR TO PLACING THE SURFACE COURSE, REMOVE THE WEDGE DOWN TO THE SURFACE OF THE INTERMEDIATE COURSE. PLACE SURFACE COURSE AS PER THE PLANS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED AS SHOWN BELOW. THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC, AS PER PLAN 282 CY

TRAFFIC INCIDENT MANAGEMENT (TIM) DURING MOT

OHIO TIM IS OHIO'S TRAFFIC INCIDENT MANAGEMENT PROGRAM WHICH IS COMMITTED TO MAINTAINING THE SAFE AND EFFECTIVE FLOW OF TRAFFIC DURING EMERGENCIES AS TO PREVENT FURTHER DAMAGE, INJURY OR UNDUE DELAY OF THE MOTORING PUBLIC. IN ADDITION TO COMPLYING WITH THE PROVISION OF OMUTCD CHAPTER 6I, CONTROL OF TRAFFIC THROUGH TRAFFIC INCIDENT MANAGEMENT AREAS, THE CONTRACTOR SHALL ACTIVELY PARTICIPATE IN TIM PLANNING AND IMPLEMENTATION AS OUTLINED BELOW.

1. SUPERINTENDENT SHALL IDENTIFY THE INDIVIDUAL PERSONS ON THE PROJECT WHO WILL, OR MAY NEED TO, PERFORM THE DUTIES HEREIN. AT A MINIMUM, INCLUDE THE SUPERINTENDENT, FOREMEN AND SUPERVISORS (OR EQUIVALENT) AS WELL AS THE WORKSITE TRAFFIC SUPERVISOR (WTS; IF APPLICABLE TO THE PROJECT). THESE INDIVIDUALLY IDENTIFIED PERSONS SHALL COLLECTIVELY BE KNOWN AS CONTRACTOR TRAFFIC INCIDENT MANAGEMENT (TIM) CONTACTS. NOTIFY THE PROJECT ENGINEER OF THE CONTRACTOR TIM CONTACTS (ALONG WITH CONTACT INFORMATION FOR EACH) AT OR BEFORE THE PRECONSTRUCTION MEETING.

2. SUPERINTENDENT SHALL NOTIFY THE ENGINEER IMMEDIATELY IF ANY CONTRACTOR TIM CONTACT IS ADDED, REMOVED OR THE CONTACT INFORMATION CHANGES OVER THE COURSE OF THE PROJECT.

3. PRIOR THE FIRST DAY OF WORK IN THE FIELD, EACH CONTRACTOR TIM CONTACT ON THE PROJECT SHALL HAVE ATTENDED AND SUCCESSFULLY COMPLETED OHIO TIM TRAINING PROVIDED BY THE DEPARTMENT OR DESIGNEE. TRAINING INFORMATION CAN BE FOUND AT WWW.OHIOTIM.COM.

4. SUPERINTENDENT, AT A MINIMUM, SHALL ATTEND AND ACTIVELY PARTICIPATE IN A DEPARTMENT SCHEDULED TIM MEETING BEFORE CONSTRUCTION WORK BEGINS AND BEFORE EACH PHASE CHANGE. THESE MEETINGS WILL RESULT IN A DEPARTMENT ISSUED PROJECT SPECIFIC TRAFFIC INCIDENT MANAGEMENT PLAN (TIMP). AT THE TIM MEETINGS THE ATTENDING CONTRACTOR TIM CONTACTS SHALL:

A. COLLABORATE WITH ODOT AND SAFETY FORCES;

B. SHARE PROJECT SPECIFIC DETAILS THAT IMPACT TIM RESPONDERS; AND

C. RECOMMEND WAYS TO INCORPORATE NECESSARY EMERGENCY ACCESS AND OTHER TIM ELEMENTS FOR TIM RESPONDERS GIVEN PROJECT SPECIFIC WORK BEING COMPLETED AND PROJECT SPECIFIC PHASING.

5. CONTRACTOR TIM CONTACTS SHALL IMPLEMENT COMPONENTS OF THE RESULTING TIMP (SUCH AS APPROVED EMERGENCY INGRESS/EGRESS POINTS, ETC), AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH 109.05.

6. CONTRACTOR TIM CONTACTS SHALL PERFORM, AT A MINIMUM, THE FOLLOWING FUNCTIONS WHEN AN INCIDENT/CRASH OCCURS:

A. IF OBSERVED OR PRESENT WHEN OCCURS, CALL 911 AND THEN NOTIFY THE TRAFFIC MANAGEMENT CENTER (TMC) TO PROVIDE THE FOLLOWING:

I. LOCATION, INCLUDING MILEPOST NUMBER AND DIRECTION OF TRAVEL

II. NUMBER AND TYPE OF VEHICLES INVOLVED, IF KNOWN

III. ESTIMATED EXTENT OF DAMAGE OR INJURY, IF KNOWN

IV. ESTIMATED NUMBER OF PATIENTS INVOLVED, IF KNOWN

V. ANY POTENTIAL HAZARDOUS CONDITIONS, IF KNOWN

VI. THE PLACARD NUMBER ON ANY HAZARDOUS MATERIALS PLACARD FROM A SAFE DISTANCE, IF APPLICABLE AND VISIBLE

B. FOLLOWING AN INCIDENT/CRASH:

I. INITIATE TRAFFIC MANAGEMENT/PROVIDE TEMPORARY TRAFFIC CONTROL AS INDICATED IN THE TIMP, AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH 109.05.

II. RECOMMEND ROADWAY REPAIR NEEDS.

III. PROVIDE REPAIR RESOURCES AND INITIATE REPAIRS, AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH 109.05.

IV. ATTEND AND PARTICIPATE IN AN AFTER ACTION REVIEW (AAR).

ALL COSTS, UNLESS OTHERWISE SPECIFIED, RESULTING FROM THE ABOVE REQUIREMENTS SHALL BE CONSIDERED TO BE INCLUDED IN THE LUMP SUM PRICE FOR ITEM 614, MAINTAINING TRAFFIC. FAILURE TO PERFORM THE REQUIREMENTS OF THIS PLAN NOTE WILL RESULT IN A DAILY FINE OF 2% OF ITEM 614, MAINTAINING TRAFFIC AND MAY RESULT IN ONE OR MORE CONTRACTOR TIM CONTACTS BEING REMOVED FROM THE LIST OF OHIO TIM TRAINED INDIVIDUALS (AT THE SOLE DISCRETION OF THE OHIO TIM EXECUTIVE COMMITTEE). IN THE EVENT AN INDIVIDUAL IS REMOVED FROM THE OHIO TIM TRAINED LIST, THE INDIVIDUAL WILL BE REMOVED FROM CONTRACTOR TIM CONTACT RESPONSIBILITIES ON ALL PROJECTS.

ITEM 614 MAINTAINING TRAFFIC, MISC.: 6"X8" SOLID WOOD POST, AS PER PLAN

THIS ITEM SHALL CONSIST OF INSTALLING AND REMOVING GROUND MOUNTED 6"X8" SOLID WOOD POSTS. FIGURE 298-26 OF THE TRAFFIC ENGINEERING MANUAL (P. 2-219) SHOULD BE USED AS A GUIDE FOR INSTALLATION OF THE 6"X8" POSTS.

GRADE 2 SOUTHERN YELLOW PINE SHALL BE USED AND SHALL BE PRESSURE TREATED WITH CCA PRESERVATIVE.

PAYMENT WILL BE MADE PER UNIT PRICE BID PER EACH ITEM 614 MAINTAINING TRAFFIC, MISC.: 6"X8" SOLID WOOD POST, AS PER PLAN.

ITEM 614 MAINTAINING TRAFFIC, MISC.: SIGN (FLAT SHEET), AS PER PLAN

THIS ITEM CONSISTS OF INSTALLING AND REMOVING A FLAT SHEET SIGN APPROPRIATE FOR OUTDOOR USE. ATTACHMENT AND MOUNTING MATERIALS SHALL BE IN CONFORMANCE WITH NCHRP REPORT 350. SEE SHEETS 135Z, 135AA, 135AB, and 135AC FOR LOCATION AND SIZES OF TEMPORARY SIGNAGE.

PAYMENT WILL BE MADE PER UNIT PRICE PER SQUARE FOOT OF ITEM 614 MAINTAINING TRAFFIC, MISC.: SIGN (FLAT SHEET), AS PER PLAN

RUMBLE STRIP REMOVAL BEFORE PAVING

RUMBLE STRIPS WILL BE PLANED WITH ITEM 441 THE QUANTITIES FOR PLANING AND PAVING THE RUMBLE STRIPS ARE PROVIDED BELOW. QUANTITIES ARE BASED ON 2' WIDE MILL. QUANTITIES TO BE CARRIED TO THE GENERAL SUMMARY.

EXISTING LENGTH OF RUMBLE STRIP:
LENGTH = 41400 FT

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2 " 9,200 SY

ITEM 407 - TACK COAT 782 GAL

ITEM 441, ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449) 383 CY

Allowable Closure Table				
Interchange	Ramp	Phase of Closure	Allowable Duration of Closure*	Disincentive \$
US-24 & US-6 (West Interchange)	US-6/US-24 WB to US-6	Phase 1A	120 Days	\$5000 per day
	US-6 to US-24 WB	Phase 1A	60 Days	\$1000 per day
	US-24 EB to US-6	Phase 2C	60 Days	\$1000 per day
	US-6 to US-24 EB	Phase 2C	90 Days	\$5000 per day
US-6 & SR-108	US-24 WB to SR-108	Phase 1B	90 Days	\$3500 per day
	SR-108 to US-24 WB	Phase 1B	210 Days	\$4000 per day
	US-24 EB to SR-108	Phase 2B	120 Days	\$3500 per day
	SR-108 to US-24 EB	Phase 2B	90 Days	\$5000 per day
US-6 & Industrial Dr	US-24 WB to Industrial Dr	Phase 1	Friday 8PM to Monday 6AM** Overnight 8PM-6AM***	\$100 per 15 minute increment
	Industrial Dr to US-24 WB	Phase 1	Friday 8PM to Monday 6AM** Overnight 8PM-6AM***	\$100 per 15 minute increment
	US-24 EB to Industrial Dr	Phase 2	Friday 8PM to Monday 6AM** Overnight 8PM-6AM***	\$100 per 15 minute increment
	Industrial Dr to US-24 EB	Phase 2	Friday 8PM to Monday 6AM** Overnight 8PM-6AM***	\$175 per 15 minute increment
US-24 & US-6 (East Interchange)	US-6 WB to US-24 WB	Pre-Phase 2	Total: 105 Days (Pre-Phase 2: 30 Days, Phase 2A: 75 Days)	
		Phase 2A		\$10000 per day
US-6 & CR-424	US-24 EB to US-6 EB	Phase 2A	90 Days	\$10000 per day
	US-6 WB to CR-424	Phase 2	105 Days	\$150 per day
	US-6 EB to CR-424	Phase 2	90 Days	\$150 per day
	CR-424 to US-6 EB	Phase 2	90 Days	\$500 per day

*Ramp closures shall be scheduled to ensure there are no detour route conflicts.

** Pavement replacement at gore area.

*** Multiple overnight closures are permitted for ramp resurfacing



GENERAL SUMMARY (SHEET 4 OF 4)	
DESIGN AGENCY	
	
DESIGNER	JRB
REVIEWER	XXX MM-DD-YY
PROJECT ID	110524
SHEET	TOTAL
P.139	1108