

180452 Conformed Set
Dist 3

STATE OF OHIO

DEPARTMENT OF TRANSPORTATION

HUR / LOR-20-16.26 / 0.00 PM
LOR-511-21.14 PM

PROJECT DESCRIPTION

THIS PROJECT WILL INCLUDE PAVEMENT REPAIRS, PLANING AND PAVING WITH SMOOTH SEAL, INSTALLATION OF SAFETY EDGE, BRIDGE MAINTENANCE, AND REPLACING PAVEMENT MARKINGS.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: N/A ACRES
(MAINTENANCE PROJECT)
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A ACRES
(MAINTENANCE PROJECT)
NOTICE OF INTENT EARTH DISTURBED AREA: N/A ACRES
(MAINTENANCE PROJECT)

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 6111.02 OF THE OHIO REVISED CODE.

2016 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED _____
DATE 5/9/18 DISTRICT DEPUTY DIRECTOR

APPROVED _____
DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION



LOCATION MAP

DESIGN DESIGNATION: SEE SHEETS 2-3

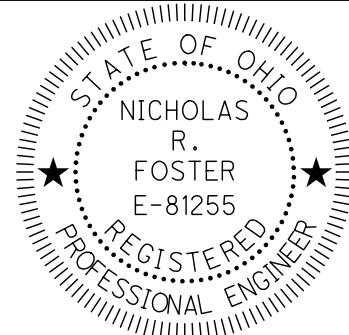
DESIGN EXCEPTIONS
NONE REQUIRED

VILLAGE OF WAKEMAN
CITY OF OBERLIN
NORWALK TOWNSHIP
TOWNSEND TOWNSHIP
WAKEMAN TOWNSHIP
CAMDEN TOWNSHIP
PITTSFIELD TOWNSHIP
NEW RUSSIA TOWNSHIP
CARLISLE TOWNSHIP
HURON COUNTY
LORAIN COUNTY

INDEX OF SHEETS:

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ENGINEERS SEAL:



SIGNED: Nicholas R Foster
DATE: 5/8/18

| STANDARD CONSTRUCTION DRAWINGS | | | | | | | | | | SUPPLEMENTAL SPECIFICATIONS | |
|--------------------------------|---------|-----------|----------|----------|----------|--------|---------|----------|---------|-----------------------------|----------|
| DM-4.3 | 1/15/16 | MT-101.60 | 1/20/17 | TC-41.20 | 10/18/13 | BP-3.1 | 7/18/14 | AS-1-15 | 7/17/15 | 800 | 4/20/18 |
| DM-4.4 | 1/15/16 | MT-101.70 | 1/17/14 | TC-42.20 | 10/18/13 | BP-4.1 | 1/20/17 | DBR-3-11 | 7/15/11 | 808 | 1/20/17 |
| | | MT-101.75 | 7/15/16 | TC-52.10 | 10/18/13 | BP-9.1 | 7/21/17 | PCB-91 | 1/18/13 | 821 | 4/20/12 |
| MT-95.30 | 7/21/17 | MT-101.90 | 7/17/15 | TC-52.20 | 7/15/16 | | | | | 832 | 1/17/14 |
| MT-95.50 | 7/21/17 | MT-104.10 | 10/16/15 | TC-64.10 | 1/20/17 | RM-1.1 | 7/18/14 | | | 846 | 4/17/15 |
| MT-96.11 | 1/20/17 | MT-105.10 | 7/19/13 | TC-65.10 | 1/17/14 | RM-4.2 | 4/18/14 | | | 897 | 1/16/15 |
| MT-96.20 | 7/15/16 | MT-120.00 | 1/20/17 | TC-65.11 | 7/15/16 | | | | | 908 | 10/20/17 |
| MT-96.26 | 7/19/13 | | | TC-71.10 | 1/20/17 | | | | | | |
| MT-97.10 | 7/18/14 | | | TC-72.20 | 7/15/16 | | | | | | |
| MT-97.12 | 1/20/17 | | | TC-82.10 | 7/17/15 | | | | | | |
| MT-98.10 | 1/20/17 | | | TC-84.20 | 10/18/13 | | | | | | |
| MT-98.11 | 1/20/17 | | | TC-84.21 | 10/18/13 | | | | | | |
| MT-98.20 | 7/18/14 | | | | | | | | | | |
| MT-98.22 | 1/20/17 | | | | | | | | | | |
| MT-98.29 | 1/20/17 | | | | | | | | | | |
| MT-99.20 | 7/19/13 | | | | | | | | | | |

PLANS PREPARED BY:
 OHIO DEPARTMENT OF TRANSPORTATION
DISTRICT THREE ENGINEERING

UNDERGROUND UTILITIES
CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

OHIO Utilities Protection SERVICE
Call Before You Dig
1-800-362-2764
(Non-members must be called directly)

OIL & GAS PRODUCERS UNDERGROUND PROTECTION SERVICE
1-800-925-0988

FEDERAL PROJECT NO.
E170414

PID NO.
87698

CONSTRUCTION PROJECT NO.

RAILROAD INVOLVEMENT
NONE

HUR / LOR-20-16.26 / 0.00
LOR-511-21.14

**DESIGN DESIGNATION
LOR-511-21.14-22.10**

CURRENT ADT (2018) ----- 5400
 DESIGN YEAR ADT (2026) ----- 5500
 DESIGN HOURLY VOLUME (2026) ----- 550
 DIRECTIONAL DISTRIBUTION ----- 56%
 TRUCKS (24 HOUR B&C) ----- 6%
 DESIGN SPEED ----- 45 MPH
 LEGAL SPEED ----- 45 MPH
 FUNCTIONAL CLASSIFICATION:
 MAJOR COLLECTOR
 NHS PROJECT ----- NO

**DESIGN DESIGNATION
LOR-511-22.10-22.16**

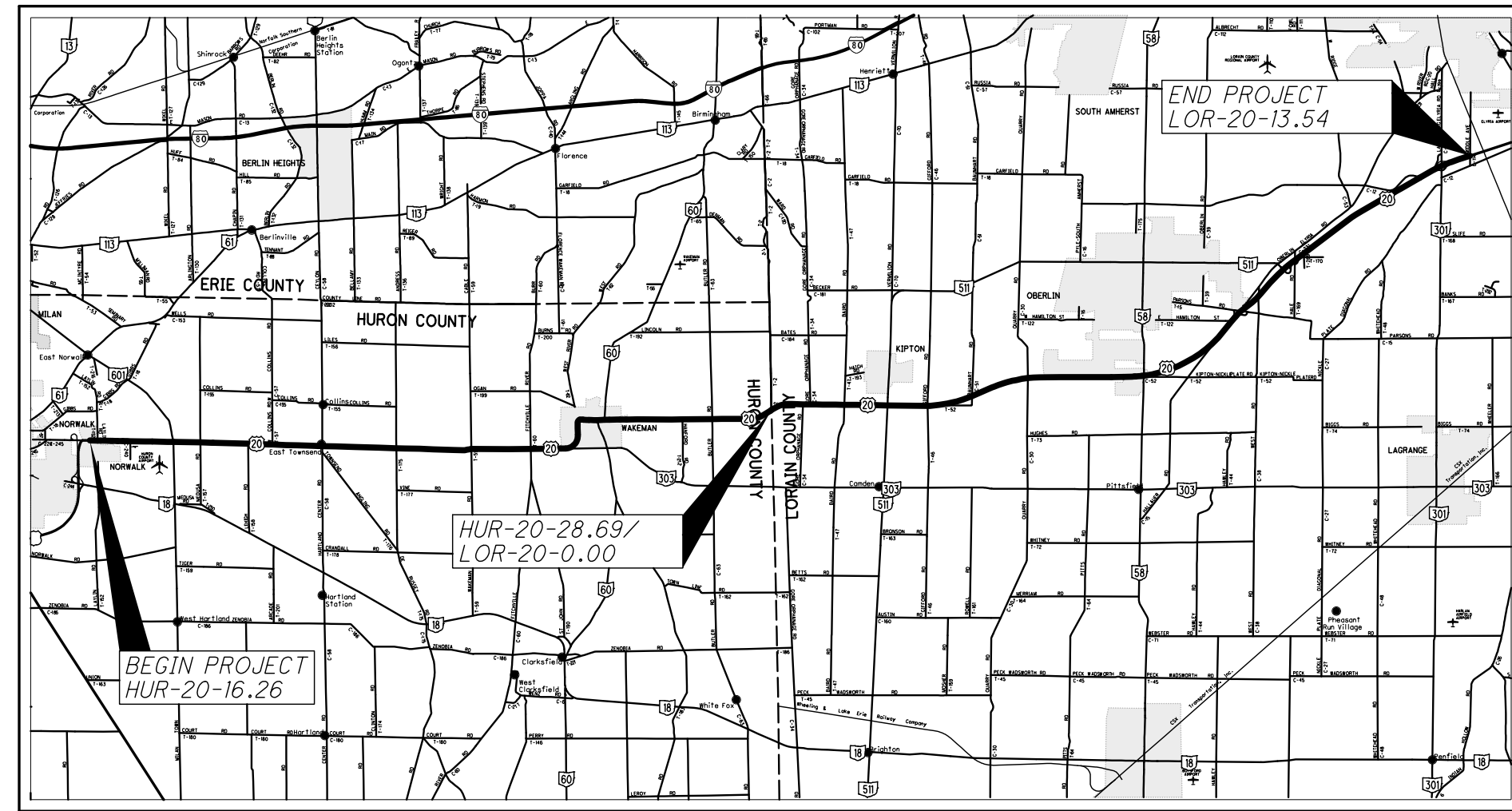
CURRENT ADT (2018) ----- 7500
 DESIGN YEAR ADT (2026) ----- 7900
 DESIGN HOURLY VOLUME (2026) ----- 790
 DIRECTIONAL DISTRIBUTION ----- 53%
 TRUCKS (24 HOUR B&C) ----- 12%
 DESIGN SPEED ----- 45 MPH
 LEGAL SPEED ----- 45 MPH
 FUNCTIONAL CLASSIFICATION:
 MAJOR COLLECTOR
 NHS PROJECT ----- NO

**DESIGN DESIGNATION
LOR-511-22.16-22.24**

CURRENT ADT (2018) ----- 7500
 DESIGN YEAR ADT (2026) ----- 7900
 DESIGN HOURLY VOLUME (2026) ----- 790
 DIRECTIONAL DISTRIBUTION ----- 53%
 TRUCKS (24 HOUR B&C) ----- 12%
 DESIGN SPEED ----- 65 MPH
 LEGAL SPEED ----- 65 MPH
 FUNCTIONAL CLASSIFICATION:
 MAJOR COLLECTOR
 NHS PROJECT ----- NO

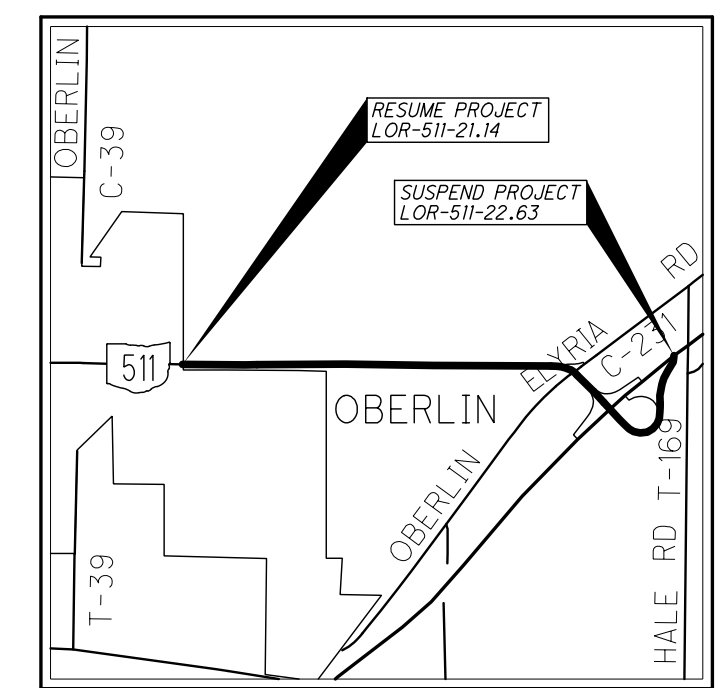
**DESIGN DESIGNATION
LOR-511-22.24-22.63**

CURRENT ADT (2018) ----- 2700
 DESIGN YEAR ADT (2026) ----- 2900
 DESIGN HOURLY VOLUME (2026) ----- 290
 DIRECTIONAL DISTRIBUTION ----- 56%
 TRUCKS (24 HOUR B&C) ----- 19%
 DESIGN SPEED ----- 65 MPH
 LEGAL SPEED ----- 65 MPH
 FUNCTIONAL CLASSIFICATION:
 MAJOR COLLECTOR
 NHS PROJECT ----- NO



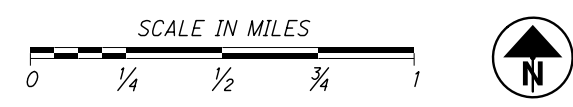
HUR-20 & LOR-20 LOCATION MAP

LATITUDE: 41°14'50" LONGITUDE: 82°27'04"



LOR-511 LOCATION MAP

LATITUDE: 41°17'38" LONGITUDE: 82°10'39"



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R

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R

L

BEGIN PROJECT
HUR-20-16.26

MATCH LINE A
HUR-20-28.69 = LOR-20-0.00

COMMERCE DR SLM 16.34

CITY OF NORWALK CORP SLM 16.49
CITY OF NORWALK CORP SLM 16.53
CITY OF NORWALK CORP SLM 16.65
CITY OF NORWALK CORP SLM 16.76

GORE ORPHANAGE RD (CR 34)
SLM 0.71

GORE ORPHANAGE RD (CR 34)
SLM 0.57

BAIRD RD (TR 47)
SLM 1.30

BAIRD RD (TR 47)
SLM 1.30

SR 511
SLM 2.05

SR 511
SLM 2.05

BRIDGE SLM 2.71
NO WORK

BRIDGE SLM 2.71
NO WORK

GIFFORD RD (TR 46)
SLM 2.74

GIFFORD RD (TR 46)
SLM 2.74

PLATE RD (TR 52)
SLM 3.03

01/NHS/PV

PLATE RD (TR 52)
SLM 3.56

BAUMHART RD (CR 51)
SLM 3.56

LOR-20

QUARRY RD (CR 30)
SLM 4.56

QUARRY RD (CR 30)
SLM 4.56

05/NHS/PV
06/NHS/PV

CITY OF OBERLIN CORP. LIMIT
SLM 6.23

SR 58
SLM 6.59

SR 58
SLM 6.59

CITY OF OBERLIN CORP. LIMIT
SLM 6.59

01/NHS/PV

BREAK LINE LOR-20-8.56
SEE SHEETS 5-6 FOR LOR-20-8.56-13.54

R

L

RESUME PROJECT LOR-511-21.14
LEAVE CITY OF OBERLIN

WILLOWBROOK DR SLM 21.31

BRIDGE SLM 21.43
NO WORK

SLM 21.42

CLARK LA SLM 21.78

OBERLIN-ELYRIA RD
(CR 231) SLM 22.16

BRIDGE SLM 22.28
(OVER US 20) - NO WORK

OBERLIN-ELYRIA RD
(CR 231)
SLM 22.16

LOR-511

SUSPEND PROJECT LOR-511-22.63

BEGIN PROJECT
HUR-20-16.26

LAYLIN RD (TR 152) SLM 16.49

07/NHS/PV
08/NHS/PV

BRIDGE SLM 17.74
04/NHS/BR

SR 601 SLM 17.82

MEDUSA RD (TR 157) SLM 18.29

LEHIGH RD (TR 158) SLM 19.13

WEST COLLINS RD (TR 57) SLM 19.45

HARTLAND CENTER RD (CR 58) SLM 20.42

DERUSSEY RD (TR 175) SLM 21.72

BRIDGE SLM 22.26
04/NHS/BR

BRIDGE SLM 22.83
04/NHS/BR

WAKEMAN TOWN LINE RD (TR 59) SLM 23.01

FITCHVILLE RIVER RD (CR 60) SLM 24.06

BRIDGE SLM 24.49
04/NHS/BR

VILLAGE OF WAKEMAN CORP SLM 24.69
SR 303 (TOWNSEND ST) SLM 24.76

CLARK ST SLM 24.93

ABBOTT ST SLM 25.15

PEARL ST SLM 25.22

DEPOT ST SLM 25.35

VERLIN ST SLM 25.43

PLEASANT ST SLM 25.49

RAILROAD ST SLM 25.61

COOPER ST SLM 25.55

SR 60 (RIVER ST) SLM 25.61

VILLAGE OF WAKEMAN CORP SLM 26.01

SR 60 SLM 25.74
VILLAGE OF WAKEMAN CORP SLM 26.12

HANFORD RD (TR 212) SLM 27.10

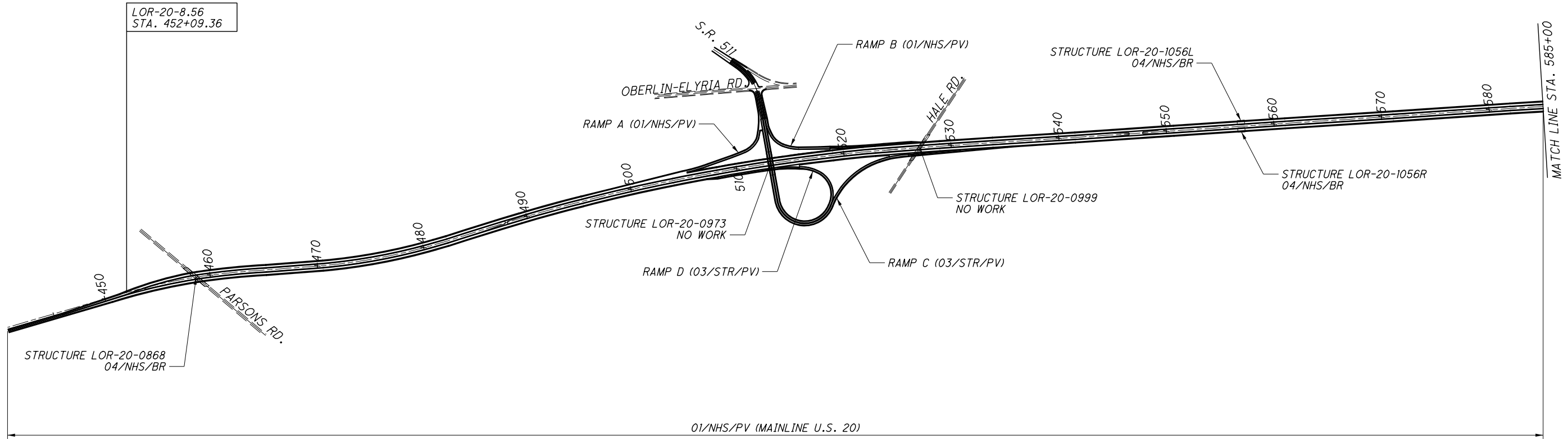
BUTLER RD (CR 63) SLM 27.62

HURON COUNTY
LORAIN COUNTY

COUNTY LINE RD (TR 2) SLM 28.69

MATCH LINE A
HUR-20-28.69 = LOR-20-0.00

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LOR-20-8.56
STA. 452+09.36

STRUCTURE LOR-20-0868
04/NHS/BR

PARSONS RD.

STRUCTURE LOR-20-0973
NO WORK

RAMP D (03/STR/PV)

RAMP A (01/NHS/PV)

S.R. 511

OBERLIN-ELYRIA RD.

RAMP B (01/NHS/PV)

STRUCTURE LOR-20-0999
NO WORK

RAMP C (03/STR/PV)

HALE RD.

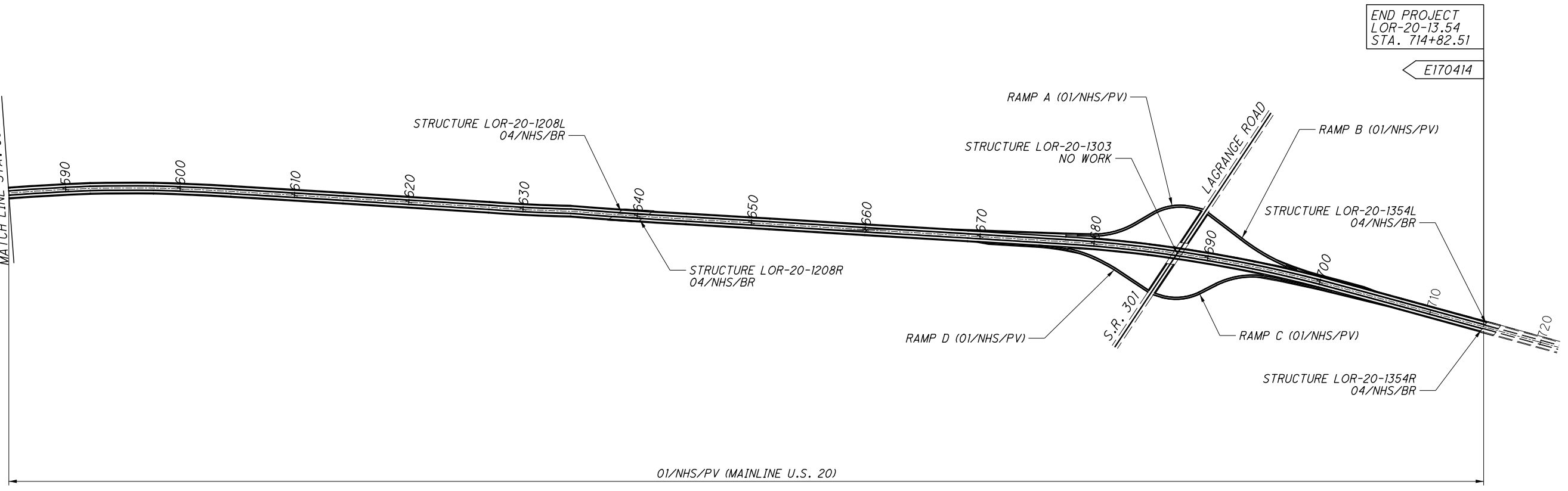
STRUCTURE LOR-20-1056L
04/NHS/BR

STRUCTURE LOR-20-1056R
04/NHS/BR

MATCH LINE STA. 585+00

01/NHS/PV (MAINLINE U.S. 20)

MATCH LINE STA. 585+00



END PROJECT
LOR-20-13.54
STA. 714+82.51

E170414

STRUCTURE LOR-20-1208L
04/NHS/BR

STRUCTURE LOR-20-1208R
04/NHS/BR

RAMP A (01/NHS/PV)

STRUCTURE LOR-20-1303
NO WORK

RAMP B (01/NHS/PV)

STRUCTURE LOR-20-1354L
04/NHS/BR

STRUCTURE LOR-20-1354R
04/NHS/BR

S.R. 301

RAMP C (01/NHS/PV)

LAGRANGE ROAD

RAMP D (01/NHS/PV)

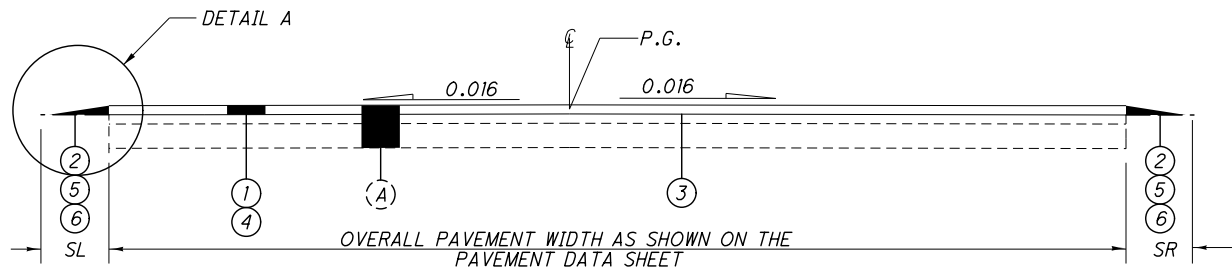
01/NHS/PV (MAINLINE U.S. 20)



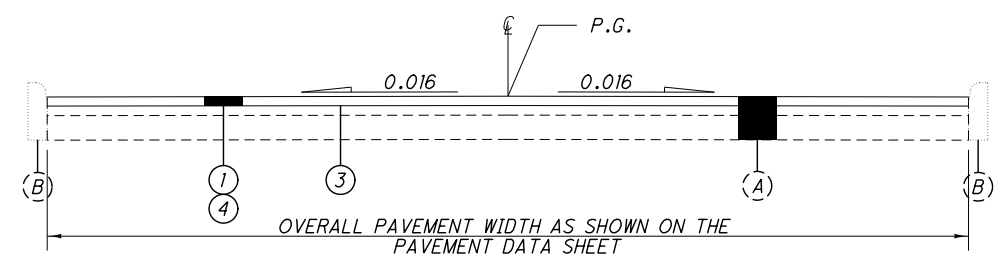
| | |
|------------|-----|
| CALCULATED | ACM |
| CHECKED | NRF |

SCHEMATIC PLAN
LOR-20 SLM 8.56 TO SLM 13.54

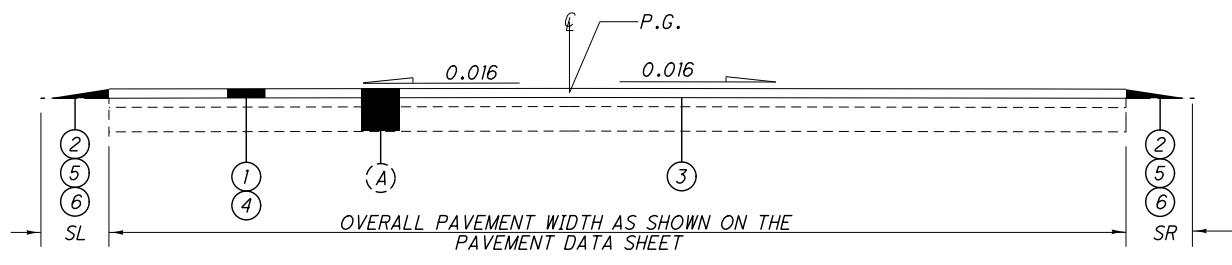
HUR / LOR-20-16.26 / 0.00
LOR-511-21.14



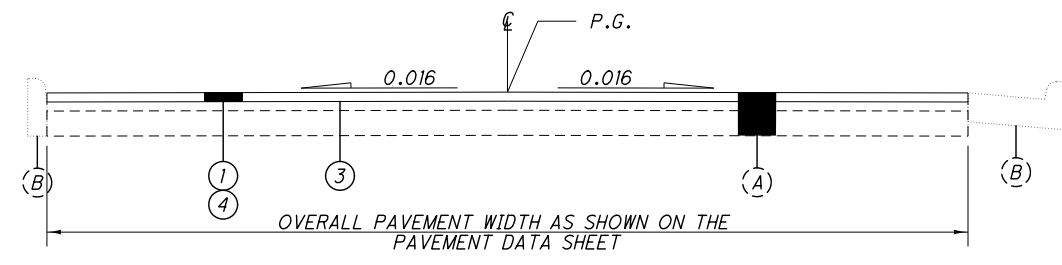
TYPICAL 1
SECTION APPLIES TO:
HUR-20-16.26-24.60
HUR-20-26.01-28.69
LOR-20-0.00-8.56
LOR-511-21.14-22.07



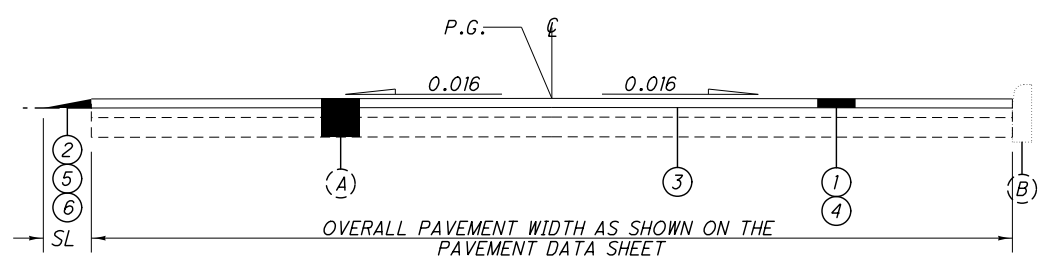
TYPICAL 4
SECTION APPLIES TO:
HUR-20-25.58-25.77



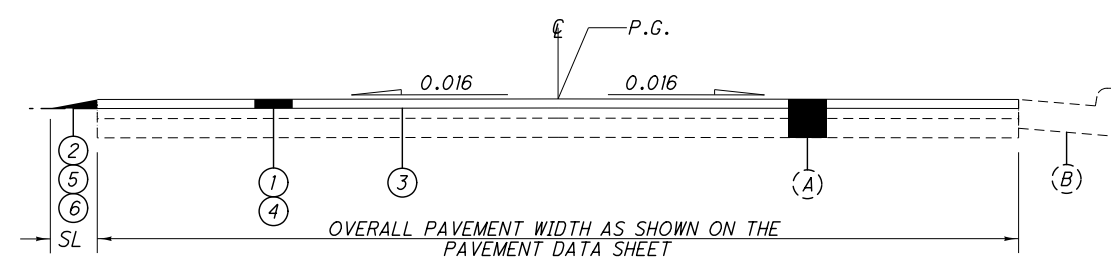
TYPICAL 2
SECTION APPLIES TO:
HUR-20-24.60-25.20



TYPICAL 5
SECTION APPLIES TO:
HUR-20-25.77-25.92



TYPICAL 3
SECTION APPLIES TO:
HUR-20-25.34-25.58



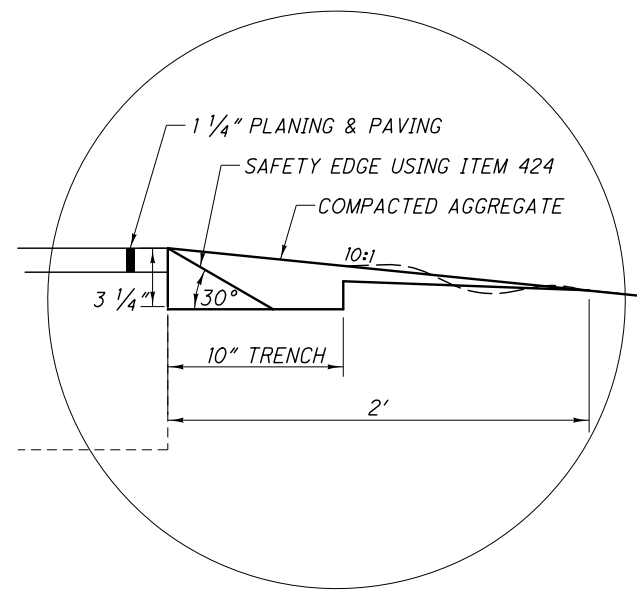
TYPICAL 6
SECTION APPLIES TO:
HUR-20-25.92-26.01

EXISTING LEGEND

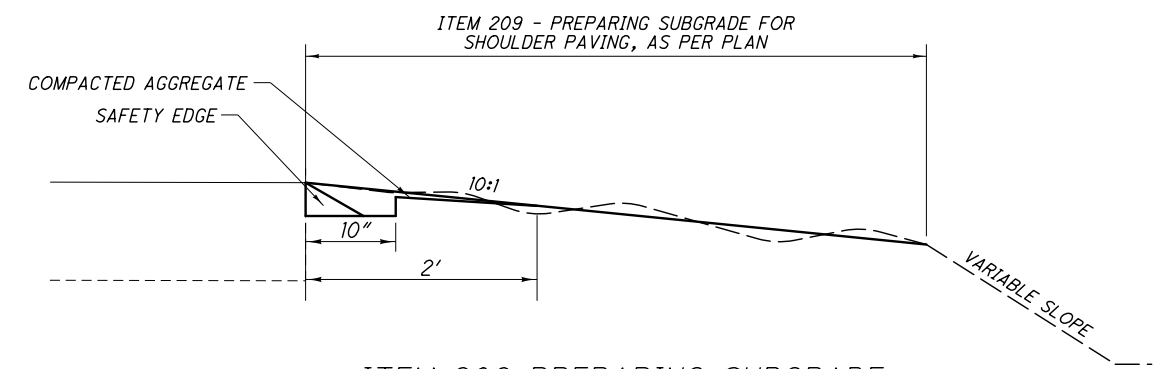
- (A) EXISTING PAVEMENT
- (B) EXISTING CURB

PROPOSED LEGEND

- ① ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (1.25")
- ② ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (SAFETY EDGE)
- ③ ITEM 407 - NON-TRACKING TACK COAT (APPLIED AT 0.08 GAL/SY)
- ④ ITEM 897 - PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (1.25")
- ⑤ ITEM 617 - COMPACTED AGGREGATE
- ⑥ ITEM 408 - PRIME COAT, AS PER PLAN (APPLIED AT 0.40 GAL/SY)

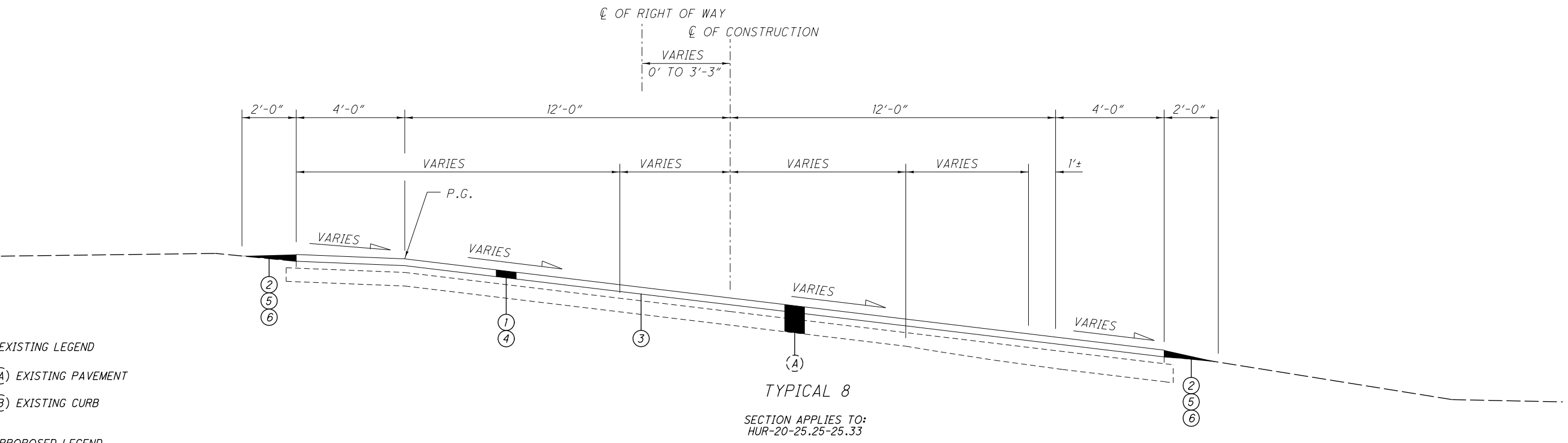
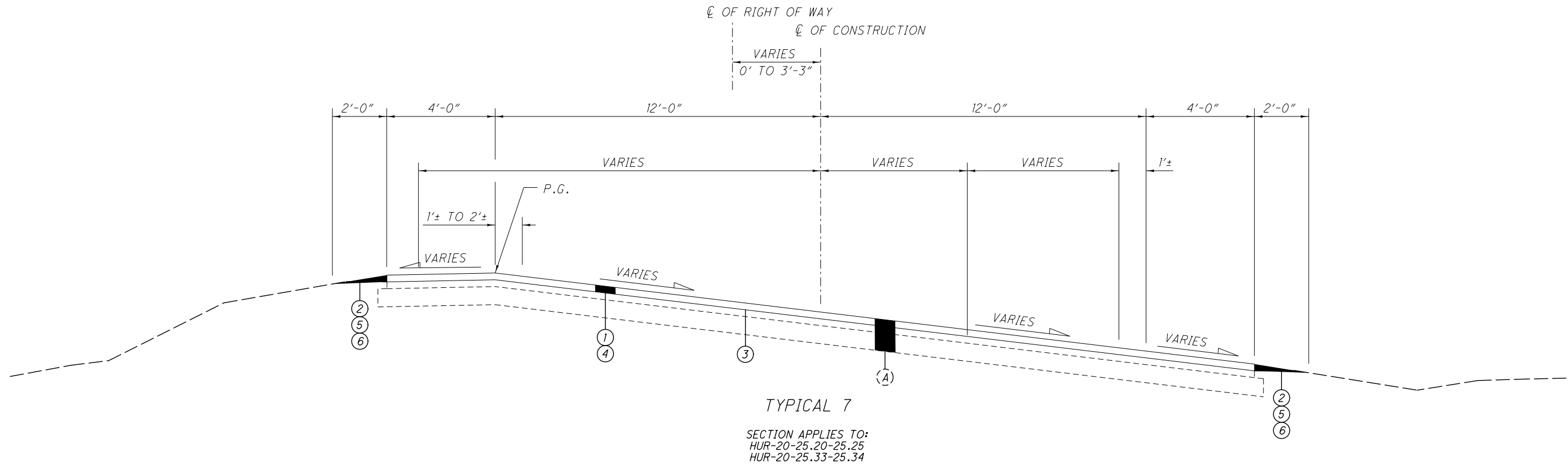


**DETAIL A
SAFETY EDGE**



**ITEM 209 PREPARING SUBGRADE
FOR SHOULDER PAVING, AS PER PLAN**

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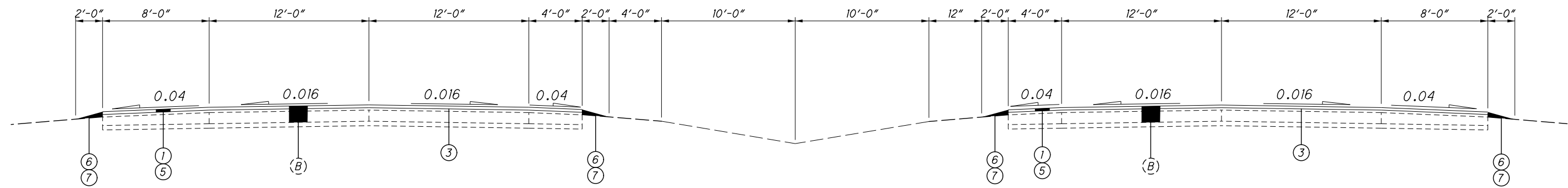
EXISTING LEGEND

- (A) EXISTING PAVEMENT
- (B) EXISTING CURB

PROPOSED LEGEND

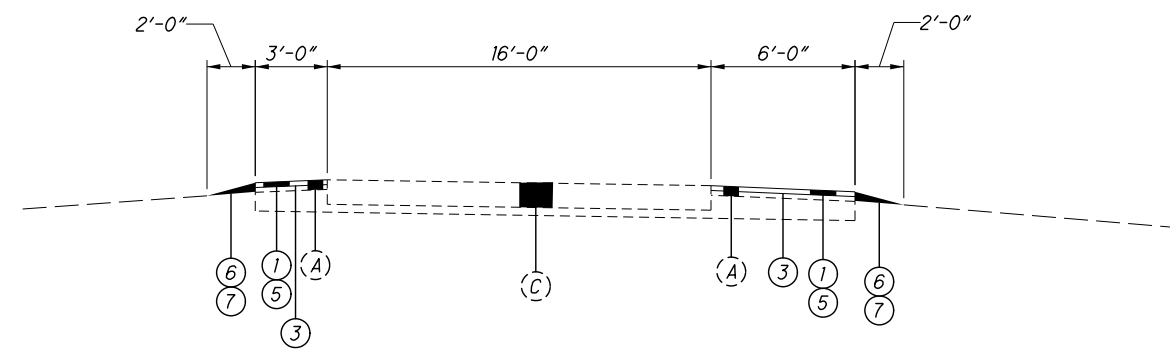
- ① ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (1.25")
- ② ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (SAFETY EDGE)
- ③ ITEM 407 - NON-TRACKING TACK COAT (APPLIED AT 0.08 GAL/SY)
- ④ ITEM 897 - PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (1.25")
- ⑤ ITEM 617 - COMPACTED AGGREGATE
- ⑥ ITEM 408 - PRIME COAT, AS PER PLAN (APPLIED AT 0.40 GAL/SY)

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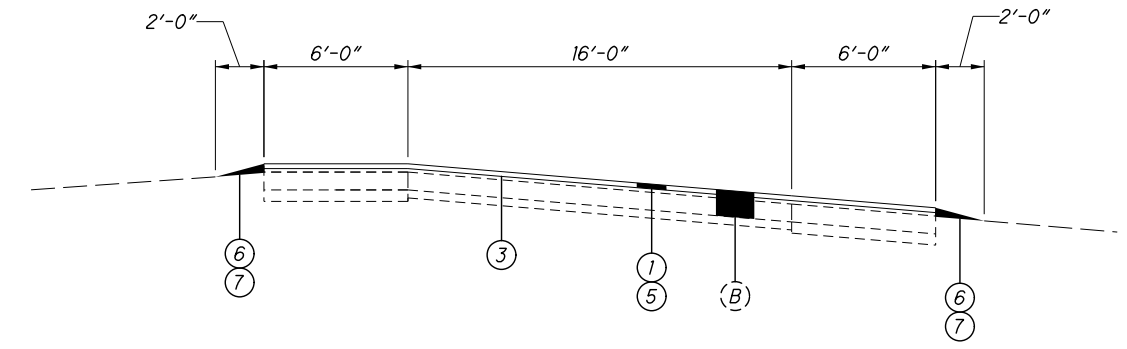


TYPICAL 9 - LOR-20 MAINLINE

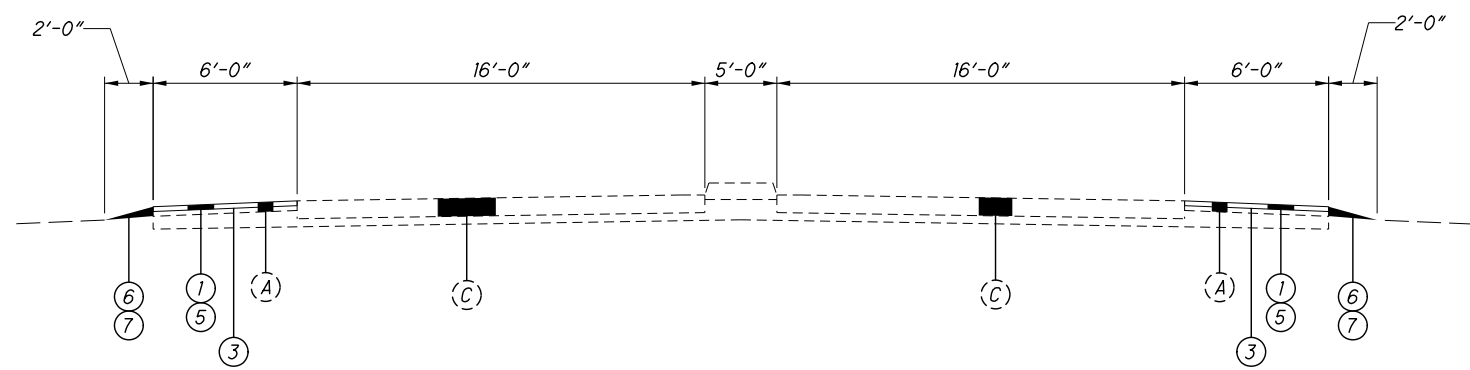
SECTION APPLIES TO:
LOR-20-8.56-13.54



SR 511 INTERCHANGE - RAMPS A, B



SR 301 INTERCHANGE - RAMPS A, B, C, D



SR 511 INTERCHANGE - RAMPS C, D

SECTION APPLIES TO:
LOR-511-22.07-22.63

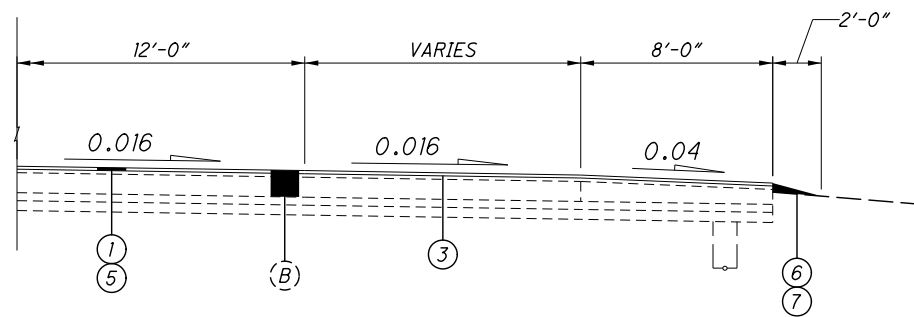
EXISTING LEGEND

- (A) EXISTING ASPHALT CONCRETE PAVEMENT
- (B) EXISTING COMPOSITE PAVEMENT
- (C) EXISTING REINFORCED CONCRETE PAVEMENT

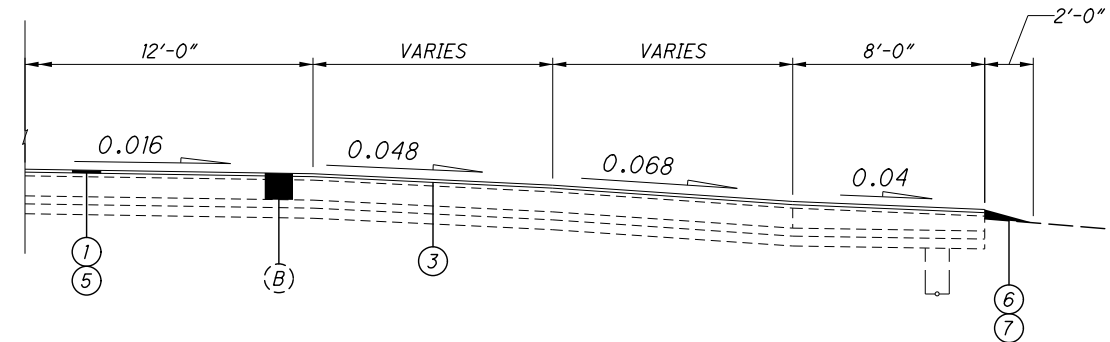
PROPOSED LEGEND

- (1) ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (1.25")
- (2) ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (SAFETY EDGE)
- (3) ITEM 407 - NON-TRACKING TACK COAT (APPLIED AT 0.08 GAL/SY)
- (4) NOT USED
- (5) ITEM 897 - PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (1.25")
- (6) ITEM 617 - COMPACTED AGGREGATE
- (7) ITEM 408 - PRIME COAT, AS PER PLAN (APPLIED AT 0.40 GAL/SY)

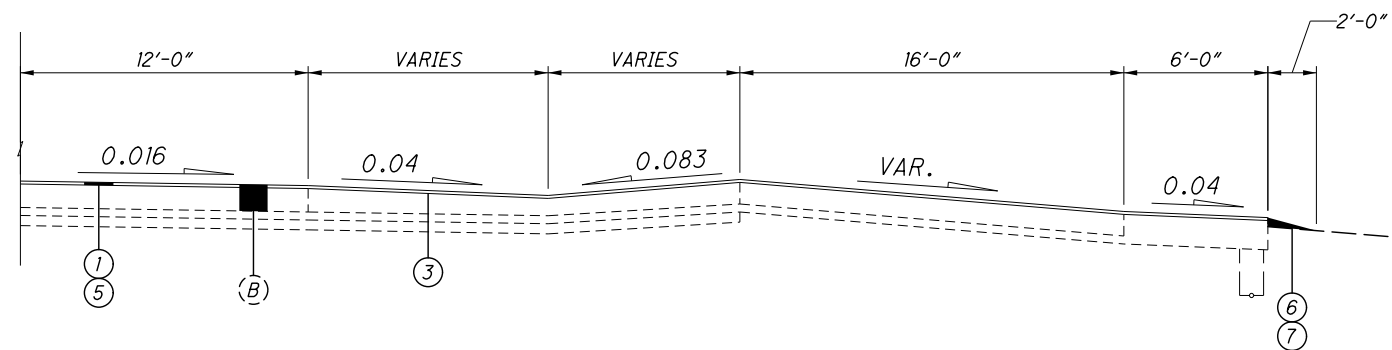
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LOR-20 - RAMP ACCELERATION/DECELERATION LANE



LOR-20 - RAMP ATTACHED TO MAINLINE



LOR-20 - RAMP DETACHED FROM MAINLINE

EXISTING LEGEND

- (A) EXISTING ASPHALT CONCRETE PAVEMENT
- (B) EXISTING COMPOSITE PAVEMENT
- (C) EXISTING REINFORCED CONCRETE PAVEMENT

PROPOSED LEGEND

- ① ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (1.25")
- ② ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (SAFETY EDGE)
- ③ ITEM 407 - NON-TRACKING TACK COAT (APPLIED AT 0.08 GAL/SY)
- ④ NOT USED
- ⑤ ITEM 897 - PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (1.25")
- ⑥ ITEM 617 - COMPACTED AGGREGATE
- ⑦ ITEM 408 - PRIME COAT, AS PER PLAN (APPLIED AT 0.40 GAL/SY)

CALCULATED
ACM
CHECKED
NRF

TYPICAL SECTIONS

HUR / LOR-20-16.26 / 0.00
LOR-511-21.14

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS.

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

HUR-20-16.26-28.69

CABLE
CHARTER COMMUNICATIONS
5520 WHIPPLE AVENUE NW
NORTH CANTON, OHIO 44720
330-494-9200

GAS
COLUMBIA GAS OF OHIO
1800 BROAD AVENUE
FINDLAY, OHIO 45840
419-427-3225

CITY
CITY OF NORWALK
38 WHITTLESEY AVENUE
NORWALK, OHIO 44857
419-663-6735

TRAFFIC
ODOT DISTRICT THREE
906 CLARK AVENUE
ASHLAND, OHIO 44805
419-207-7045

COMMUNICATION
FRONTIER COM
83 TOWNSEND AVENUE
NORWALK, OHIO 44857
419-744-3613

TOWNSHIP
CAMDEN TOWNSHIP
P.O. BOX 172
KIPTON, OHIO 44049
440-775-2015

COUNTY
HURON COUNTY ENGINEER DEPT.
150 JEFFERSON STREET
NORWALK, OHIO 44857
419-668-1997

VILLAGE
VILLAGE OF WAKEMAN
59 HYDE STREET, P.O. BOX 107
WAKEMAN, OHIO 44889
440-839-2970

ELECTRIC
OHIO EDISON
1717 ASHLAND ROAD
MANSFIELD, OHIO 44905
419-521-6213

WATER
NORTHERN OHIO RURAL WATER
P.O. BOX 96
COLLINS, OHIO 44826
419-668-7213

GAS
BUCKEYE OIL PIPELINE COMPANY
P.O. BOX 542
MANTUA, OHIO 44255
330-931-8309

WATER
RLCWA
42401 S.R. 303
LAGRANGE, OHIO 44050
440-355-6060

LOR-20-0.00-13.54 & LOR-511-21.14-22.63

CABLE
ARMSTRONG UTILITIES
1215 CLAREMONT AVENUE
ASHLAND, OHIO 44805
419-289-0161

COUNTY
LORAIN COUNTY ENGINEER
247 HADAWAY STREET
ELYRIA, OHIO 44035
440-329-5586

CABLE
CHARTER COMMUNICATIONS
5520 WHIPPLE AVENUE NW
NORTH CANTON, OHIO 44720
330-494-9200

ELECTRIC
LORAIN-MEDINA RURAL ELECTRIC
P.O. BOX 158
WELLINGTON, OHIO 44090
800-222-8673

CABLE
OBERLIN CABLE CO-OP
27 E. COLLEGE STREET
OBERLIN, OHIO 44074
440-775-4001

ELECTRIC
OHIO EDISON
1717 ASHLAND ROAD
MANSFIELD, OHIO 44905
419-521-6213

CITY
CITY OF OBERLIN
85 S. MAIN STREET
OBERLIN, OHIO 44074
440-775-1531

GAS
BUCKEYE OIL PIPELINE COMPANY
P.O. BOX 542
MANTUA, OHIO 44255
330-931-8309

COMMUNICATION
WINDSTREAM
560 TERNES AVENUE
ELYRIA, OHIO 44035
440-329-4245

GAS
ASPIRE ENERGY
300 TRACY BRIDGE ROAD
ORRVILLE, OHIO 44667
330-682-7726

COMMUNICATION
FRONTIER COM
83 TOWNSEND AVENUE
NORWALK, OHIO 44857
419-744-3613

GAS
COLUMBIA GAS OF OHIO
1021 N. MAIN STREET
MANSFIELD, OHIO 44903
419-528-1137

COMMUNICATION
AT&T TRANSMISSION
5980 WILCOX PLACE
DUBLIN, OHIO 43016
614-760-8320

GAS
TRANSCANADA
589 N. STATE ROAD
MEDINA, OHIO 44256
330-721-4163

COMMUNICATION
ONE COMMUNITY
800 W. ST. CLAIR, 2ND FLOOR
CLEVELAND, OHIO 44113
216-581-7972

GAS
DOMINION
320 SPRINGSIDE DRIVE
AKRON, OHIO 44333
800-362-7557

ELECTRIC
CITY OF OBERLIN MUNICIPAL LIGHT AND POWER SYSTEM
289 S. PROFESSOR STREET
OBERLIN, OHIO 44074
440-775-7260

UTILITIES (CONTINUED)

LOR-20-0.00-13.54 AND LOR-511-21.14-22.63 (CONT.)

TRAFFIC
ODOT DISTRICT THREE
906 CLARK AVENUE
ASHLAND, OHIO 44805
419-207-7045

TOWNSHIP
CAMDEN TOWNSHIP
PO BOX 172
KIPTON, OHIO 44049
440-775-2015

TOWNSHIP
CARLISLE TOWNSHIP
11969 LAGRANGE ROAD
LAGRANGE, OHIO 44050
440-458-4491

TOWNSHIP
NEW RUSSIA TOWNSHIP
46300 BUTTERNUT RIDGE ROAD
OBERLIN, OHIO 44074
440-775-7782

WATER
RLCWA
42401 S.R. 303
LAGRANGE, OHIO 44050
440-355-6060

THE AFOREMENTIONED UTILITY COMPANIES AND AGENCIES HAVE VARIOUS FACILITIES IN THE AREA THAT WILL REMAIN IN PLACE DURING CONSTRUCTION.

EXTREME CAUTION SHOULD BE EXERCISED IN AREAS WITH UTILITIES. SECTIONS 105.07 AND 107.16 OF THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIALS SPECIFICATIONS REQUIRE, AMONG OTHER THINGS, THAT THE CONTRACTOR COOPERATE WITH ALL UTILITIES LOCATED WITHIN THE LIMITS OF THIS CONSTRUCTION PROJECT AND TAKE RESPONSIBILITY FOR THE PROTECTION OF THE UTILITY PROPERTY AND SERVICES.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

EXISTING PLANS

EXISTING PLANS ENTITLED AS LISTED BELOW MAY BE INSPECTED IN THE ODOT DISTRICT 3 OFFICE IN ASHLAND.

| ROUTE | EXISTING PLAN NAME | DATE |
|----------------|--------------------|------|
| HUR-20 | HUR-20-16.04 | 2011 |
| HUR-20 | HUR-20-16.35 | 2008 |
| LOR-20/LOR-511 | LOR-20-8.56 | 2011 |

CONSTRUCTION NOTIFICATION

THE CONTRACTOR SHALL ADVISE THE PROJECT ENGINEER A MINIMUM OF FOURTEEN (14) DAYS PRIOR TO THE FOLLOWING: THE START OF CONSTRUCTION ACTIVITIES, LANE RESTRICTIONS, LANE CLOSURES, AND OR ROAD CLOSURES. THE PROJECT ENGINEER WILL FORWARD THIS INFORMATION TO THE FOLLOWING:

DISTRICT PUBLIC INFORMATION OFFICE (PIO) BY EMAIL AT D03.PIO@DOT.OHIO.GOV

DISTRICT PERMIT SECTION BY FAX AT (614) 887-4318 OR EMAIL AT LOUIS.TUMBLIN@DOT.OHIO.GOV

CENTRAL OFFICE SPECIAL HAUL PERMITS SECTION BY FAX AT (614) 728-4099 OR EMAIL AT HAULING.PERMITS@DOT.OHIO.GOV

THE PIO WILL, IN TURN, NOTIFY THE PUBLIC, THE LOCAL EMERGENCY SERVICES, AFFECTED SCHOOLS AND BUSINESSES, AND ANY OTHER IMPACTED LOCAL PUBLIC AGENCY OF ANY OF THE ABOVE MENTIONED ITEMS, VIA MEDIA SOURCES.

AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. THE CONTRACTOR IS ADVISED THAT NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED A HEIGHT OF 25 FEET WITHIN THE LIMITS OF HUR-020-16.86 TO HUR-020-18.36. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, THE CONTRACTOR IS ADVISED THAT FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA) WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. THE CONTRACTOR WILL BE REQUIRED TO FILE A NEW FAA FORM 7460-1, ADVISING THE FAA THAT AERONAUTICAL STUDY NO. (SEE BELOW LIST) IS BEING RESUBMITTED AND THAT AN ALTERATION TO THE ORIGINAL SUBMISSION IS REQUESTED. COPIES OF THE ALTERATION AND FORM 7460-1 SHALL BE FORWARDED TO THE ODOT OFFICE OF AVIATION. THE CONTRACTOR IS ADVISED THAT NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT UNTIL A COPY OF THE FAA APPROVAL AND ODOT OFFICE OF AVIATION PERMIT HAS BEEN FURNISHED TO THE PROJECT ENGINEER.

THE CONTRACTOR IS FURTHER ADVISED THAT THE FAA APPROVAL WILL TAKE A MINIMUM OF 45 DAYS. ALL SUBMISSIONS SHALL BE DIRECTED TO THESE OFFICES:

AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS (CONTINUED)

EXPRESS PROCESSING CENTER
THE FEDERAL AVIATION ADMINISTRATION
SOUTHWEST REGIONAL OFFICE
OBSTRUCTION EVALUATION SERVICE, AJR-32
2601 MEACHAN BLVD.
FORT WORTH, TX 76137-0520

ODOT
OFFICE OF AVIATION
2829 W DUBLIN-GRANVILLE RD.
COLUMBUS, OH 43235
614.793.5046

| AERONAUTICAL STUDY NUMBER | COUNTY | ROUTE | STRAIGHT LINE MILE | LAT-LONG | |
|---------------------------|--------|-------|--------------------|-----------|------------|
| | | | | LATITUDE | LONGITUDE |
| 2017-AGL-21862-OE | HUR | 20 | 16.86 | 41.249434 | -82.559595 |
| 2017-AGL-21863-OE | HUR | 20 | 17.36 | 41.249318 | -82.550102 |
| 2017-AGL-21864-OE | HUR | 20 | 17.86 | 41.249218 | -82.540625 |
| 2017-AGL-21865-OE | HUR | 20 | 18.36 | 41.249113 | -82.531046 |

FURTHER SPECIAL INSTRUCTIONS FOR AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS

THE FEDERAL AVIATION ADMINISTRATION OBSTRUCTION EVALUATION GROUP HAS DETERMINED THAT THE MANAGER OF THE NORWALK-HURON COUNTY AIRPORT (5A1) AND THE FAA BE ADVISED OF THE ACTUAL BEGINNING AND ENDING OF CONSTRUCTION WITHIN THE AREA OF INFLUENCE OF THE AIRPORT. DUE TO THIS REQUEST, THE CONTRACTOR IS INSTRUCTED TO CONTACT THE DISTRICT THREE FAA COORDINATOR (KENNY KNAPP) VIA EMAIL AT kenneth.knapp@dot.state.oh.us FIVE (5) BUSINESS DAYS BEFORE BEGINNING AND ENDING OF ALL CONSTRUCTION ACTIVITIES WITHIN THE LIMITS OF HUR-20-16.86 TO HUR-20-18.36.

SPECIAL NOTIFICATION OF BEGINNING AND ENDING OF CONSTRUCTION SHALL BE PROVIDED TO THE FAA COORDINATOR FIVE (5) DAYS PRIOR TO THE BEGINNING AND ENDING OF CONSTRUCTION WITHIN THE SECTION FROM HUR-20-16.86 TO HUR-20-18.36. ALONG WITH THE DATES OF CONSTRUCTION FOR THIS SECTION, THE CONTRACTOR SHALL PROVIDE THE NAME AND A CONTACT PHONE NUMBER FOR THE PERSON RESPONSIBLE FOR ENSURING COMPLIANCE WITH THE FAA GUIDELINES ON THE SITE. THIS PERSON SHALL BE ON-SITE FOR THE DURATION OF THE WORK WHILE WITHIN THIS SECTION OF THE PROJECT AND SHALL BE ABLE TO BE CONTACTED BY PHONE AT ALL TIMES. THIS RESPONSIBLE PERSON SHALL ENSURE THAT THE CONTRACTOR COMPLIES WITH ALL FAA AND ODOT REGULATIONS AS SET FORTH IN THIS PLAN AND PLAN PACKAGE AND SHALL IMMEDIATELY IMPLEMENT ANY ADDITIONAL MEASURES REQUESTED BY THE FAA OR IMPACTED AIRPORT.

IN ORDER TO COMPLY WITH ALL FAA REQUIREMENTS AND ALLEVIATE ANY ISSUES THAT MAY ARISE BETWEEN THE WORK ON THE HIGHWAY AND AIRWAY TRAFFIC, IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE NORWALK-HURON COUNTY AIRPORT MANAGER ADVISING THE MANAGER OF THE LOCATION, SCOPE, AND DURATION OF THE PROJECT A MINIMUM OF FIVE (5) DAYS PRIOR TO BEGINNING WORK WITHIN THE SECTION(S) OF ROADWAY AFFECTED BY THE AERONAUTICAL STUDIES LISTED ABOVE. PROVIDE ANY ADDITIONAL INFORMATION REQUESTED BY THE AIRPORT MANAGER. THE FAA HAS MANDATED THAT THE CONTRACTOR ABIDE BY ALL REGULATIONS AND REQUESTS SET FORTH BY THE AIRPORT MANAGER. A MINIMUM OF FIVE (5) DAYS PRIOR TO THE COMPLETION OF ALL WORK ON THE PROJECT, AGAIN CONTACT THE AIRPORT MANAGER IN ORDER TO NOTIFY THE MANAGER OF THE ACTUAL COMPLETION DATE OF THE PROJECT. ANY QUESTIONS REGARDING THIS REQUIREMENT MAY BE DIRECTED TO KENNY KNAPP, DISTRICT FAA COORDINATOR, AT 419-207-7175.

NORWALK-HURON COUNTY AIRPORT
C/O MELISSA JAMES, VICE PRESIDENT
P.O. BOX 406
NORWALK, OHIO 44857
PH: 419-668-5400
EMAIL: Melissa@HuronCountyAirport.co

THE CONTRACTOR IS ADVISED THAT THE FAA HAS REQUIRED SPECIAL MARKINGS BE PROVIDED ON ALL CONSTRUCTION EQUIPMENT WITHIN THE AREA OF HUR-20-16.86 TO HUR-20-18.36. ALL CONSTRUCTION EQUIPMENT, OTHER THAN PASSENGER VEHICLES, SHALL BE EQUIPPED WITH A RED LIGHT CONFORMING TO CHAPTER 5, RED OBSTRUCTION LIGHT SYSTEM (L-810 OR EQUIVALENT AND MINIMUM 32.5 CANDELAS) AND A FLAG CONFORMING TO CHAPTER 3, MARKING GUIDELINES IN ACCORDANCE WITH THE FAA'S ADVISORY CIRCULAR 70/7460-1K, OBSTRUCTION MARKING AND LIGHTING. A COPY OF THE PERTINENT SECTIONS OF THIS CIRCULAR WILL BE PROVIDED TO THE CONTRACTOR AT THE PRE-CONSTRUCTION MEETING.

ALL CONSTRUCTION EQUIPMENT SHALL BE REMOVED FROM THE ABOVE REFERENCED 1.5-MILE SECTION OF US 20 AND LOWERED TO ITS LOWEST VERTICAL POSITION (i.e. DUMP TRUCK WITH BED LOWERED) WHEN NOT IN USE.

FOR ANY INFORMATION NOT GIVEN HERE, REFERENCE THE FAA DETERMINATION DOCUMENTS AS PART OF THE PLAN PACKAGE SUBMITTAL. ANY QUESTIONS NOT ANSWERED BY THOSE DOCUMENTS MAY BE DIRECTED TO THE DISTRICT THREE FAA COORDINATOR AT THE ABOVE EMAIL OR BY PHONE AT 419.207.7175.

ALL EXTRA WORK, MATERIAL, AND EQUIPMENT NEEDED TO COMPLY WITH THE FAA'S REQUESTS, REQUIREMENTS, AND REGULATIONS SHALL BE PAID FOR UNDER THE LUMP SUM CONTRACT BID PRICE FOR ITEM 614 MAINTENANCE OF TRAFFIC.

CALCULATED
ACM
CHECKED
NRF

GENERAL NOTES

HUR / LOR-20-16.26 / 0.00
LOR-511-21.14

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ROUTINE MAINTENANCE

BETWEEN THE TIME THAT BIDS ARE TAKEN AND THE START OF CONSTRUCTION, THE MAINTAINING AGENCY MAY ENTER UPON THE PROJECT AND PERFORM ROUTINE MAINTENANCE SUCH AS CRACK SEALING, PATCHING, AND BERM AND SHOULDER REPAIR. THE EFFECTS, IF ANY, OF THE PERFORMANCE OF ROUTINE MAINTENANCE SHALL BE CONSIDERED AS INHERENT IN WORK OF THE CHARACTER PROVIDED FOR IN THE PLAN AND THE RESULTING CONDITIONS SHALL NOT BE CONSIDERED AS DIFFERING MATERIALLY FROM THOSE EXISTING AT THE TIME BIDS WERE TAKEN.

INTERIM COMPLETION DATE

IN ORDER TO ENSURE COMPLIANCE WITH THE TEMPERATURE RESTRICTIONS FOR ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, CONSIDER SEPTEMBER 30TH, 2018 AN INTERIM COMPLETION DATE FOR WORK INVOLVING THAT ITEM ON THE PROJECT. IF PAVING WORK WITH ITEM 424 IS REQUIRED AFTER SEPTEMBER 30TH, 2018, OBTAIN THE APPROVAL OF THE ENGINEER ON A DAY BY DAY BASIS PRIOR TO INITIATING SUCH WORK.

COORDINATION OF WORK BETWEEN CONTRACTORS

THE CONTRACTOR SHOULD BE AWARE THAT THERE MAY BE OTHER WORK BEING PERFORMED BY A SEPARATE CONTRACT. HUR-20-25.62 IS A BRIDGE REPLACEMENT PROJECT AND IS SCHEDULED TO BEGIN WORK IN THE 2019 CONSTRUCTION SEASON. LOR-511-0.00 IS A RESURFACING PROJECT AND IS SCHEDULED TO BEGIN WORK IN THE 2018 CONSTRUCTION SEASON. COORDINATION OF WORK IS THE RESPONSIBILITY OF THE CONTRACTOR.

ITEM 209 - PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN

PREPARE THE SHOULDER FOR PAVING A CONSISTENT SAFETY EDGE IN BOTH THICKNESS AND WIDTH.

PRIOR TO PAVING THE SAFETY EDGE, GRADE AN AREA 10 INCHES WIDE, BEGINNING AT THE EDGE OF THE PAVED ROADWAY, TO PROVIDE A LEVEL SURFACE FREE OF VEGETATION FOR CONSTRUCTION OF THE SAFETY EDGE. IF NECESSARY, EXCAVATE THE GRADED AREA TO THE DEPTH NECESSARY TO CONSTRUCT THE SAFETY EDGE. COMPACT THE GRADED SHOULDER ACCORDING TO 617.05 OR AS DIRECTED BY THE ENGINEER. THE GRADED SHOULDER BEYOND THE 10 INCH WIDE AREA FOR THE SAFETY EDGE SHALL BE GRADED AT A 10:1 SLOPE, OR AS DIRECTED BY THE ENGINEER. THE INTENT IS TO PROVIDE AN UNOBSTRUCTED AND POSITIVE FLOW OF STORM WATER FROM THE PAVEMENT TO THE DITCH.

SAFETY EDGE

IN ADDITION TO THE REQUIREMENTS OF 401.12, ATTACH A DEVICE TO THE SCREED OF THE PAVEMENT THAT CONFINES THE MATERIAL AT THE END GATE AND EXTRUDES THE ASPHALT MATERIAL IN SUCH A WAY THAT RESULTS IN A COMPACTED WEDGE SHAPE PAVEMENT EDGE OF APPROXIMATELY 30 DEGREES (NOT STEEPER THAN 40 DEGREES). ENSURE THE DEVICE MAINTAINS CONTACT WITH THE EXISTING SURFACE, AND ALLOW FOR AUTOMATIC TRANSITION TO CROSS ROADS, DRIVEWAYS AND OBSTRUCTIONS. DO NOT USE CONVENTIONAL SINGLE PLATE STRIKE OFF.

CONSTRUCTION OF SAFETY EDGE CAN BE OMITTED AT LOCATIONS WHERE EXISTING WIDTH OF GRADED SHOULDER OR BERM IS LESS THAN 12". PROJECTS WITH VARYING CONDITIONS SHOULD USE SAFETY EDGE WHERE POSSIBLE. PLAN PREPARATION HAS MADE EVERY REASONABLE ATTEMPT TO IDENTIFY POSSIBLE SAFETY EDGE LOCATIONS.

USE THE TRANSTECH SHOULDER WEDGE MAKER, THE CARLSON SAFETY EDGE END GATE, THE ADVANT-EDGER, THE TROXLER SAFETSLOPE OR A SIMILAR APPROVED-EQUAL DEVICE THAT PRODUCES THE SAME WEDGE CONSOLIDATION RESULTS. CONTACT INFORMATION FOR THESE WEDGE SHAPE COMPACTION DEVICES IS THE FOLLOWING:

TRANSTECH SYSTEMS, INC.
1594 STATE STREET
SCHENECTADY, NY 12304
1-800-724-6306
www.transtechsys.com

ADVANT-EDGE PAVING EQUIPMENT LLC
P.O. BOX 9163
NISKAYUNA, NY 12309-0163
518-280-6090
www.advantedgepaving.com

CARLSON SAFETY EDGE END GATE
18450 50TH AVENUE EAST
TACOMA, WA 98446
253-875-8000

TROXLER ELECTRONICS LABORATORIES INC.
3008 E. CORNWALLIS RD.
RESEARCH TRIANGLE PARK, NC 27709
1-877-TROXLER
www.troxlerlabs.com

IF ELECTING TO USE A SIMILAR DEVICE, PROVIDE PROOF THAT THE DEVICE HAS BEEN USED ON PREVIOUS PROJECTS WITH ACCEPTABLE RESULTS OR CONSTRUCT A TEST SECTION PRIOR TO THE BEGINNING OF WORK AND DEMONSTRATE WEDGE COMPACTION TO THE SATISFACTION OF THE ENGINEER. SHORT SECTIONS OF HANDWORK WILL BE ALLOWED WHEN NECESSARY FOR TRANSITIONS AND TURNOUTS OR OTHERWISE AUTHORIZED BY THE ENGINEER.

IN ADDITION TO THE REQUIREMENTS OF 401.16, MAKE THE FIRST ROLLER PASS 8 TO 12 INCHES AWAY FROM TAPERED EDGE. DO NOT ROLL THE TAPER.

ROLLER REQUIREMENTS WITHIN THE VILLAGE CORP LIMITS

WITHIN THE CORPORATION LIMITS OF THE VILLAGE OF WAKEMAN, THE CONTRACTOR SHALL NOT USE A VIBRATORY ROLLER TO COMPACT THE ASPHALT CONCRETE.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. (PREVIOUS CONSTRUCTION PLANS SHOWING THE ORIGINAL ALIGNMENT AND PROFILE, ARE AVAILABLE FOR INSPECTION AT THE ODOT DISTRICT 3 OFFICE). PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (442)
ITEM 253 - PAVEMENT REPAIR**

THESE ITEMS OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE.

PAVEMENT REPAIR SHALL BE PERFORMED AFTER PAVEMENT PLANING AND BEFORE PLACEMENT OF THE SURFACE COURSE. THE DEPTH OF REMOVAL SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT WITH A MAXIMUM DEPTH OF 4", BASED ON THE PAVEMENT DESIGN AND AN AVERAGE DEPTH OF 2" AND AN AVERAGE WIDTH OF 2 FT FOR ESTIMATING PURPOSES.

REPLACEMENT MATERIAL SHALL BE ITEM 301, OR ITEM 442 19MM, AS PER PLAN MATERIAL AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE. ITEM 301 ASPHALT CONCRETE CAN BE USED WHEN THE DEPTH OF THE REPAIR IS BETWEEN 3" AND 12" WITH A MAXIMUM PAVEMENT LIFT OF 6". ITEM 442 19MM, AS PER PLAN CAN BE USED WHEN THE DEPTH OF THE REPAIR IS BETWEEN 1.5" AND 3". PG 64-22 ASPHALT BINDER SHALL BE USED FOR ALL OF THE ASPHALT CONCRETE MATERIALS FOR THESE REPAIRS.

FOR THE ITEM 442 19 MM, AS PER PLAN MATERIAL, REQUIREMENTS OF 442 APPLY EXCEPT AS FOLLOWS:
MIX DESIGN: FOR Ndes USE 50 GYRATIONS, FOR Nmax USE 75 GYRATIONS. USE A PG 64-22 BINDER.
MAXIMUM RECLAIMED ASPHALT CONCRETE PAVEMENT IS 30 PERCENT.
APPLY 703.05 FOR COARSE AND FINE AGGREGATE EXCEPT GRADATION FOR FINE AGGREGATE DOES NOT APPLY.
QUALITY CONTROL: DO NOT PERFORM Nmax IN QUALITY CONTROL TESTING. DO NOT TAKE EXTRA ASPHALT BINDER SAMPLES AS OUTLINED IN CMS 442.05.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR (442) IS TO BE A MAXIMUM OF 4" DEEP AND ITEM 253 PAVEMENT REPAIR IS FOR DEPTHS GREATER THAN 4". PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER CUBIC YARD, (BY TICKET WEIGHT CONVERSION), OF ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (442) OR ITEM 253 - PAVEMENT REPAIR.

ITEM 408 - PRIME COAT, AS PER PLAN

THE CONTRACTOR SHALL APPLY ONE COAT OF MC-70 (AS PER SECTION 702) AT A RATE OF 0.40 GAL/SY TO THE COMPLETED AGGREGATE SHOULDER (ITEM 617) AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE TYPICAL SECTIONS OF THIS PLAN. THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID BITUMINOUS MATERIAL ONTO THE EDGE OF PAVEMENT OR EDGE LINE. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO 107.10 OF THE SPECIFICATIONS.

ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B

OMIT ITEM 424 ON STRUCTURES WITH CONCRETE WEARING SURFACES.

ITEM SPECIAL - AIR SPEED ZONE MARKING

EXCEPT AS NOTED, THIS ITEM IS TO MEET CMS 644. THE SPEED MEASUREMENT MARKINGS ARE TO BE WHITE AND 24 INCHES WIDE (MEASURED IN THE DIRECTION OF TRAVEL) AND FOUR (4) FEET IN LENGTH.

PLACE THE MARKINGS AT 0.25 MILE INTERVALS OVER A ONE (1) MILE LENGTH OF ROADWAY ENTIRELY ON THE PAVED SHOULDERS. THE FIRST ZONE IS TO START AT LOR-20-10.50 EB AND END AT LOR-20-11.50 EB. THE SECOND ZONE IS TO START AT LOR-20-11.50 WB AND END AT LOR-20-10.50 WB.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO HAVE THE MARKINGS LAID OUT BY A STATE OF OHIO REGISTERED SURVEYOR. A RECORD IS TO BE KEPT AND ONE ORIGINAL SIGNED AND SEALED DOCUMENT IS TO BE SENT TO THE DISTRICT 3 TRAFFIC ENGINEER AND ONE COPY FOR THE DISTRICT CONSTRUCTION ENGINEER.

MEASUREMENT AND PAYMENT: THE FIVE (5) MARKINGS PLACED ON EACH OF THE TWO SHOULDERS IN EACH 1 MILE OF ROADWAY PER EACH DIRECTION OF TRAVEL EQUAL ONE ZONE. ONE ZONE WILL BE MEASURED AS 1 EACH. PAYMENT FOR ALL MATERIALS, LABOR, EQUIPMENT AND SURVEYING FOR ACCEPTED WORK IS TO BE INCLUDED PER EACH IN ITEM SPECIAL - AIR SPEED ZONE MARKING.

ITEM 620 - DELINEATOR, MISC.: REMOVAL & REERECTED

THIS ITEM IS TO BE USED FOR THE REMOVAL, STORAGE, AND REERECTION OF THE DELINEATORS (QUICK KURB) ALONG US 20 JUST EAST OF SR 58 FOR THE PLANING AND PAVING OPERATIONS. THE DELINEATORS CAN BE REMOVED FOR A MAXIMUM OF 21 CONSECUTIVE DAYS BEFORE THEY SHALL BE REINSTALLED. FOR EVERYDAY PAST THE 21 CONSECUTIVE DAYS THE DELINEATORS ARE NOT REINSTALLED, A DISINCENTIVE FEE OF \$1000 PER DAY WILL BE ASSESSED TO THE CONTRACTOR. PAYMENT SHALL BE PER FOOT AND INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE ALL OF THE ABOVE WORK.

ITEMS 251 & 253 ESTIMATED QUANTITIES

| FUNDING | COUNTY | ROUTE | SLM | ITEM 251 | ITEM 253 |
|-----------|--------|-------|---------------|----------|----------|
| 01/NHS/PV | HUR | 20 | 16.26 - 18.00 | 39 CY | 1 CY |
| 01/NHS/PV | HUR | 20 | 18.00 - 19.00 | 34 CY | 1 CY |
| 01/NHS/PV | HUR | 20 | 19.00 - 20.00 | 48 CY | 1 CY |
| 01/NHS/PV | HUR | 20 | 20.00 - 21.00 | 41 CY | 1 CY |
| 01/NHS/PV | HUR | 20 | 21.00 - 22.00 | 34 CY | 1 CY |
| 01/NHS/PV | HUR | 20 | 22.00 - 23.00 | 54 CY | 1 CY |
| 01/NHS/PV | HUR | 20 | 23.00 - 24.00 | 48 CY | 1 CY |
| 01/NHS/PV | HUR | 20 | 24.00 - 24.60 | 48 CY | 1 CY |
| 01/NHS/PV | HUR | 20 | 24.60 - 26.12 | 82 CY | 1 CY |
| 01/NHS/PV | HUR | 20 | 26.12 - 27.00 | 31 CY | 1 CY |
| 01/NHS/PV | HUR | 20 | 27.00 - 28.00 | 41 CY | 3 CY |
| 01/NHS/PV | HUR | 20 | 28.00 - 28.69 | 24 CY | 1 CY |
| TOTAL | | | | 524 CY | 14 CY |

| FUNDING | COUNTY | ROUTE | SLM | ITEM 251 | ITEM 253 |
|-----------|--------|-------|---------------|----------|----------|
| 01/NHS/PV | LOR | 20 | 0.00 - 1.00 | 27 CY | 1 CY |
| 01/NHS/PV | LOR | 20 | 1.00 - 2.00 | 24 CY | 1 CY |
| 01/NHS/PV | LOR | 20 | 2.00 - 3.00 | 85 CY | 1 CY |
| 01/NHS/PV | LOR | 20 | 3.00 - 4.00 | 136 CY | 1 CY |
| 01/NHS/PV | LOR | 20 | 4.00 - 5.00 | 85 CY | 1 CY |
| 01/NHS/PV | LOR | 20 | 5.00 - 6.23 | 116 CY | 1 CY |
| 01/NHS/PV | LOR | 20 | 6.59 - 7.50 | 51 CY | 1 CY |
| 01/NHS/PV | LOR | 20 | 7.50 - 8.56 | 51 CY | 1 CY |
| 01/NHS/PV | LOR | 20 EB | 8.56 - 9.00 | 14 CY | 1 CY |
| 01/NHS/PV | LOR | 20 EB | 9.00 - 10.00 | 34 CY | 1 CY |
| 01/NHS/PV | LOR | 20 EB | 10.00 - 11.00 | 34 CY | 1 CY |
| 01/NHS/PV | LOR | 20 EB | 11.00 - 12.00 | 34 CY | 1 CY |
| 01/NHS/PV | LOR | 20 EB | 12.00 - 13.00 | 34 CY | 1 CY |
| 01/NHS/PV | LOR | 20 EB | 13.00 - 13.54 | 17 CY | 1 CY |
| 01/NHS/PV | LOR | 20 WB | 8.56 - 9.00 | 14 CY | 1 CY |
| 01/NHS/PV | LOR | 20 WB | 9.00 - 10.00 | 34 CY | 1 CY |
| 01/NHS/PV | LOR | 20 WB | 10.00 - 11.00 | 34 CY | 1 CY |
| 01/NHS/PV | LOR | 20 WB | 11.00 - 12.00 | 34 CY | 1 CY |
| 01/NHS/PV | LOR | 20 WB | 12.00 - 13.00 | 34 CY | 1 CY |
| 01/NHS/PV | LOR | 20 WB | 13.00 - 13.54 | 17 CY | 1 CY |
| TOTAL | | | | 909 CY | 20 CY |

| FUNDING | COUNTY | ROUTE | SLM | ITEM 251 | ITEM 253 |
|-----------|--------|-------|---------------|----------|----------|
| 02/S<2/PV | LOR | 511 | 21.14 - 21.42 | 14 CY | 1 CY |
| TOTAL | | | | 14 CY | 1 CY |

| FUNDING | COUNTY | ROUTE | SLM | ITEM 251 | ITEM 253 |
|-----------|--------|-------|---------------|----------|----------|
| 03/STR/PV | LOR | 511 | 21.42 - 22.63 | 35 CY | 1 CY |
| TOTAL | | | | 35 CY | 1 CY |

| FUNDING | COUNTY | ROUTE | SLM | ITEM 251 | ITEM 253 |
|-----------|--------|-------|-------------|----------|----------|
| 06/NHS/PV | LOR | 20 | 6.23 - 6.59 | 16 CY | 1 CY |
| TOTAL | | | | 16 CY | 1 CY |

| FUNDING | COUNTY | ROUTE | SLM | ITEM 251 | ITEM 253 |
|-----------|--------|-------|---------------|----------|----------|
| 08/NHS/PV | HUR | 20 | 16.26 - 16.84 | 2 CY | 1 CY |
| TOTAL | | | | 2 CY | 1 CY |

| FUNDING SPLIT SUB-TOTALS | | | | ITEM 251 | ITEM 253 |
|--------------------------|--|--|--|----------|----------|
| 01/NHS/PV | | | | 1433 CY | 34 CY |
| 02/S<2/PV | | | | 14 CY | 1 CY |
| 03/STR/PV | | | | 35 CY | 1 CY |
| 06/NHS/PV | | | | 16 CY | 1 CY |
| 08/NHS/PV | | | | 2 CY | 1 CY |
| TOTAL | | | | 1500 CY | 38 CY |

CALCULATED
ACM
CHECKED
NRF

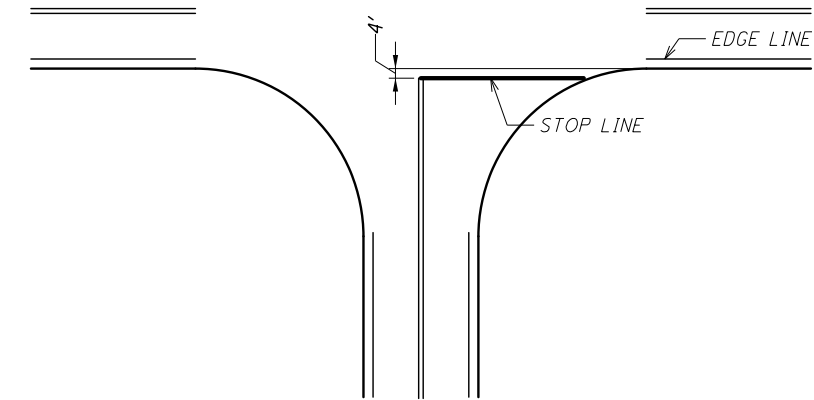
GENERAL NOTES

HUR / LOR-20-16.26 / 0.00
LOR-511-21.14

PAVEMENT CORING INFORMATION

Table with 10 columns: COUNTY, ROUTE, SLM, ASPHALT DEPTH (IN.), CONCRETE DEPTH (IN.), BRICK DEPTH (IN.), LOCATION, DIRECTION, YEAR CORED. Lists data for various locations like HUR and LOR.

STOP LINE PLACEMENT FOR NORMAL INTERSECTIONS



AT NORMAL STOP CONTROLLED INTERSECTIONS, THE STOP BAR SHOULD BE PLACED 4 FEET FROM THE EDGE LINE OF THE INTERSECTING ROADWAY IN ORDER TO ACHIEVE MAXIMUM INTERSECTION SIGHT DISTANCE.

ITEM 897 - PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A

THE INTENT OF THE PLANING IS TO MILL THE DEPTH SPECIFIED ON THE TYPICAL SECTIONS AT THE CENTER OF PAVEMENT AT NON-CURBED AREAS. THE PAVEMENT SLOPE SHALL BE 0.010 MINIMUM AND 0.016 PREFERRED.

SPECIAL ATTENTION SHALL BE GIVEN TO SUPERELEVATED CURVES. THE SUPERELEVATION SHALL BE MAINTAINED AND/OR RESTORED, IF NECESSARY, AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE TO ALL CATCH BASINS AND INLETS.

THE PROGRESSION OF THE PLANING SHALL PROCEED IN SUCH A MANNER THAT NORMAL TRAFFIC WILL NOT BE REQUIRED TO RUN OVER THE PLANED ROADWAY SURFACE MORE THAN FOURTEEN (14) CALENDAR DAYS.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT PLANING, ASPHALT CONCRETE. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF ITEM 897 - PAVEMENT PLANING, ASPHALT CONCRETE.

ITEM 897 - PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (CURBED AND CURB AND GUTTERED SECTIONS)

THE INTENT OF THE PLANING IS TO MILL THE SPECIFIED DEPTH ALONG THE CURB CONTINGENT ON THE FOLLOWING: THE MAXIMUM CROSS SLOPE SHALL BE 0.02 WHILE THE MINIMUM CROSS SLOPE SHALL BE 0.01.

SPECIAL ATTENTION SHALL BE GIVEN TO SUPERELEVATED CURVES. THE SUPERELEVATION SHALL BE MAINTAINED AND/OR RESTORED, IF NECESSARY, AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE TO ALL CATCH BASINS AND INLETS.

THE PROGRESSION OF THE PLANING SHALL PROCEED IN SUCH A MANNER THAT NORMAL TRAFFIC WILL NOT BE REQUIRED TO RUN OVER THE PLANED ROADWAY SURFACE MORE THAN FOURTEEN (14) CALENDAR DAYS.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT PLANING, ASPHALT CONCRETE. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF ITEM 897 - PAVEMENT PLANING, ASPHALT CONCRETE.

ITEM 897 - PATCHING PLANED SURFACE

AN ESTIMATED QUANTITY OF ITEM 897 - PATCHING PLANED SURFACE HAS BEEN SET UP TO BE USED AS DIRECTED BY THE ENGINEER AS DESCRIBED IN SUPPLEMENTAL SPECIFICATION 897.

INTERSECTIONS AND DRIVES

RURAL-INTERSECTIONS SHALL BE PLANED AND PAVED TO THE END OF THE RADII OR AS DIRECTED BY THE ENGINEER. (TO PROVIDE A SMOOTH TRANSITION BETWEEN THE TWO HIGHWAYS, AND TO ELIMINATE WATER POCKETS).

URBAN-INTERSECTIONS SHALL BE PLANED AND PAVED TO THE BACK OF CROSSWALKS OR AS DIRECTED BY THE ENGINEER. (TO PROVIDE A SMOOTH TRANSITION BETWEEN THE TWO HIGHWAYS, AND TO ELIMINATE WATER POCKETS).

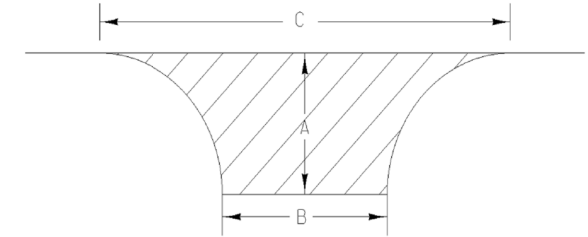
EXISTING PAVED DRIVES SHALL BE PLANED AND PAVED SO AS TO PROVIDE A SMOOTH TRANSITION BETWEEN THE HIGHWAY AND THE DRIVE, (DISTANCE FROM EDGE OF ROADWAY MAY VARY AT EACH DRIVE) AS DIRECTED BY THE ENGINEER.

EXISTING AGGREGATE DRIVE APRONS SHALL BE PLANED AND PAVED. THE SLOPE OF THIS APRON SHALL BE THE SAME AS THE ADJACENT PAVEMENT SLOPE OR AS DIRECTED BY THE ENGINEER. ANY GRADING NEEDED TO PAVE THE APRON SHALL BE INCLUDED IN THE RELATED ASPHALT ITEM FOR PAYMENT.

ANY HAZARD OR UNSAFE CONDITION RESULTING FROM THE ABOVE WORK MUST BE CORRECTED IMMEDIATELY. THE CONTRACTOR IS REMINDED OF SECTIONS 105.01, 107.07 & 614.02A OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

INTERSECTIONS AND DRIVES (CONT.)

THE PAVING DIMENSIONS FOR THE INTERSECTIONS ARE SHOWN IN THE CHART BELOW.



Main table listing intersection data with columns: FUNDING SPLIT, INTERSECTION NAME, CTY, RTE, SLM, A (FT), B (FT), C (FT), AREA (SY). Includes sub-totals and a grand total of 6729.

CALCULATED
ACM
CHECKED
NRF

GENERAL NOTES

HUR / LOR-20-16.26 / 0.00
LOR-511-21.14

ITEM SPECIAL, MAILBOX SUPPORT SYSTEM

THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF EXISTING NON-STANDARD MAILBOX SUPPORTS AND FURNISHING AND ERECTING MAILBOX SUPPORTS AND ANY ASSOCIATED HARDWARE IN ACCORDANCE WITH THE DETAILS SHOWN, AND ATTACHING AN OWNER SUPPLIED MAILBOX, AT LOCATIONS DETERMINED BY THE ENGINEER.

IN ABSENCE OF A NEW BOX SUPPLIED BY THE OWNER THE CONTRACTOR SHALL SALVAGE THE EXISTING BOX AND PLACE IT ON THE NEW SUPPORT. DUE CARE SHALL BE EXERCISED IN SUCH AN OPERATION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY BOX DAMAGED BY IMPROPER HANDLING, AS JUDGED AND DIRECTED BY THE ENGINEER.

THE BOX SHALL BE SECURELY AND NEATLY ATTACHED BY THE CONTRACTOR TO THE NEW SUPPORT. THE CONTRACTOR SHALL SUPPLY ALL NECESSARY ATTACHMENT HARDWARE (NUTS, BOLTS, PLATES, SPACERS AND WASHERS) AS NECESSARY TO ACCOMMODATE THE COMPLETE INSTALLATION. SUPPORT HARDWARE SHALL ACCOMMODATE EITHER A SINGLE OR A DOUBLE MAILBOX INSTALLATION, AND NO MORE THAN TWO MAILBOXES MAY BE MOUNTED ON A SINGLE POST. [HARDWARE SHALL BE COMMERCIAL GRADE GALVANIZED STEEL.]

WOOD POSTS SHALL BE NOMINAL 4 IN. x 4 IN. (S4S) OR 4 1/2 IN. DIAMETER ROUND, AND CONFORM TO 710.14. STEEL POSTS SHALL BE NOMINAL PIPE SIZE 2 IN. I.D., AND CONFORM TO AASHTO M 181.

POSTS SHALL BE SET AS PER THE FIRST PARAGRAPH OF 606.03, AND SHALL IN NO INSTANCE BE ENCASED IN CONCRETE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WORK WITH THE LOCAL POST MASTER AND NOTIFYING THE PROPERTY OWNERS PRIOR TO WORK.

GROUP MAILBOX SUPPORTS SHALL BE PLACED ON 3 FT. CENTERS AND THE TURNOUT LENGTHENED TO ACCOMMODATE THE GROUPING.

WHERE GUARDRAIL EXISTS, MAILBOXES AND THEIR SUPPORTS SHALL BE PLACED BEHIND THE GUARDRAIL. SUPPORTS MUST STILL MEET THE BREAKAWAY REQUIREMENTS LISTED ABOVE.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DESCRIBED ABOVE.

ITEM SPECIAL-MAILBOX SUPPORT SYSTEM, SINGLE 01/NHS/PV - U.S. 20 6 EACH

ITEM SPECIAL-MAILBOX SUPPORT SYSTEM, DOUBLE 01/NHS/PV - U.S. 20 2 EACH

LOCATIONS OF MAILBOX SUPPORT SYSTEM TO BE REPLACED

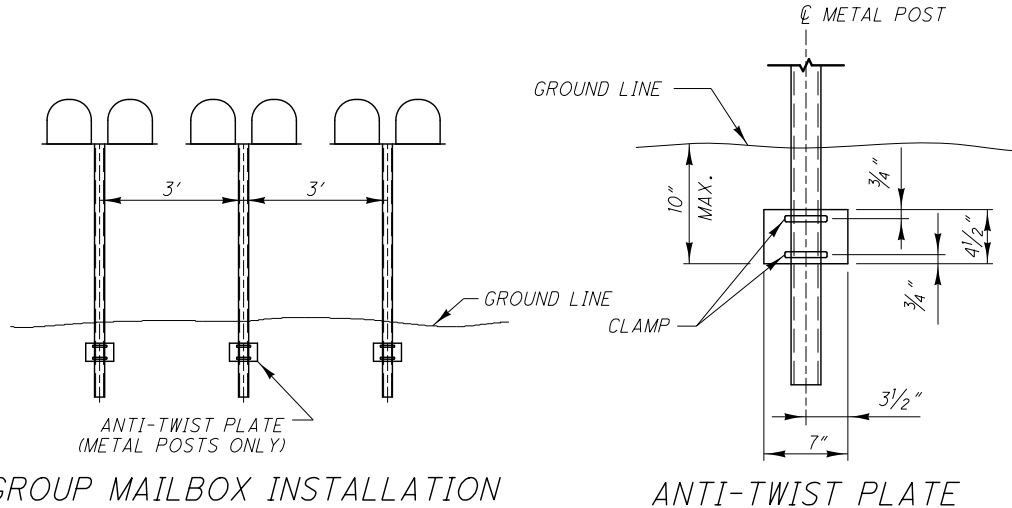
ADDRESSES AND/OR LOCATIONS OF MAILBOX SUPPORT SYSTEM TO BE REPLACED:

- 01/NHS/PV - HUR-20
- 875 US 20 - SOUTH SIDE OF US 20
- 1103 US 20 - SOUTH SIDE OF US 20
- 1141 US 20 - SOUTH SIDE OF US 20
- 1161 US 20 - SOUTH SIDE OF US 20
- 3325 US 20 - SOUTH SIDE OF US 20
- 69 E MAIN ST - NORTH SIDE OF US 20 (WAKEMAN)
- 5631 US 20 - SOUTH SIDE OF US 20

- 01/NHS/PV - LOR-20
- 51167 US 20 - NORTH SIDE OF US 20

MAILBOX APPROACHES

THE EXISTING MAILBOX APPROACHES SHALL BE PAVED WITH THE SAME TREATMENT AS THE SURROUNDING PAVEMENT. NO PROPOSED MAILBOX APPROACHES ARE TO BE INCLUDED IN THIS PROJECT.



GROUP MAILBOX INSTALLATION

ANTI-TWIST PLATE

ITEM 611 - CASTINGS ADJUSTED TO GRADE

THE CASTING TO BE ADJUSTED MAY OR MAY NOT HAVE AN EXISTING FRAME. THE WORK SHALL CONSIST OF ADJUSTING THE EXISTING CASTING TO THE SATISFACTION OF THE ENGINEER. IT IS NOT INTENDED TO PLACE NEW FRAMES WHERE NONE CURRENTLY EXIST. THE CONTRACTOR IS REMINDED TO FIELD CHECK ALL ADJUSTMENT TO GRADE ITEMS PRIOR TO BIDDING, AS NO ADDITIONAL COMPENSATION WILL BE GRANTED FOR LABOR AND MATERIALS REQUIRED TO SATISFACTORILY ADJUST CASTINGS WITHOUT FRAMES.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY AND LISTED UNDER THE APPROPRIATE ADJUSTMENT ITEM:

| HUR-20 (01/NHS/PV) | | | | LOR-20 (01/NHS/PV) | |
|--------------------|--------------|-------|-----------------|---------------------|--------------|
| SLM | CASTING TYPE | SLM | CASTING TYPE | SLM | CASTING TYPE |
| 25.14 | MANHOLE | 25.73 | CATCH BASIN (2) | 6.60 | MANHOLE |
| 25.43 | MANHOLE | 25.74 | MANHOLE | | |
| 25.49 | CATCH BASIN | 25.76 | INLET | LOR-511 (02/SK2/PV) | |
| 25.50 | MANHOLE (2) | 25.77 | MANHOLE | SLM | CASTING TYPE |
| 25.55 | CATCH BASIN | 25.81 | INLET | 21.23 | MANHOLE |
| 25.56 | INLET (2) | 25.85 | MANHOLE | | |
| 25.60 | INLET (2) | 25.88 | INLET | LOR-20 (05/NHS/PV) | |
| 25.61 | INLET | 25.89 | MANHOLE | SLM | CASTING TYPE |
| 25.72 | INLET (2) | 25.94 | INLET | 6.43 | MANHOLE |
| 25.73 | INLET | 26.03 | MANHOLE | | |

ITEM 623 - MONUMENT BOX RECONSTRUCTED TO GRADE, AS PER PLAN

THE CONTRACTOR AND THE ENGINEER SHALL FIELD VERIFY THE LOCATION OF ALL EXISTING MONUMENT BOXES LISTED IN THE PLANS PRIOR TO BEGINNING ANY WORK ON THE MONUMENT BOXES. THE USE OF METAL DETECTOR RODS MAY BE NECESSARY TO LOCATE BURIED MONUMENTATION. ANY MONUMENT BOX LISTED IN THIS NOTE THAT IS IMMEDIATELY VISIBLE ON THE SURFACE OF THE EXISTING PAVEMENT, OR IS UNCOVERED DURING THE PLANING PROCESS, SHALL BE ADJUSTED TO GRADE IF WITHIN TOLERANCE OF THE ADJUSTMENT COLLAR. ANY MONUMENT NOT FITTING THE ABOVE CRITERIA SHALL BE TREATED AS RECONSTRUCTED TO GRADE.

THE ENGINEER SHALL MAKE THE FINAL DETERMINATION OF WHETHER EACH MONUMENT BOX IS TO BE RECONSTRUCTED OR ADJUSTED AFTER THE PLACEMENT OF THE FINAL ASPHALT CONCRETE PAVEMENT SURFACE. ANY MONUMENT BOX THAT DOES NOT HAVE AN EXISTING ADJUSTABLE FRAME AND LID, OR THAT EXHIBITS SUBSTANTIAL DETERIORATION AS DETERMINED BY THE ENGINEER REQUIRING MORE WORK THAN WOULD BE CONSIDERED NORMAL FOR ITEM 623 - MONUMENT BOX ADJUSTED TO GRADE SHALL BE RECONSTRUCTED. ANY EXISTING MONUMENT THAT DOES NOT HAVE AN EXISTING SALVAGEABLE MONUMENT BOX AROUND THE PIN SHALL BE RECONSTRUCTED USING A NEW MONUMENT BOX AS PER RM-1.1, MAINTAINING THE EXISTING MONUMENTATION LOCATION.

ALL WORK RELATED TO RECONSTRUCTING OR ADJUSTING MONUMENT BOXES TO GRADE WILL BE IN ACCORDANCE WITH SPECIFICATIONS 611.10.C, 623.04, AND 623.05 OF THE ODOT CONSTRUCTION AND MATERIAL SPECIFICATIONS.

ALL LABOR, MATERIAL, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THIS WORK IS TO BE PAID USING THE CONTRACT BID PRICE PER EACH FOR ITEM 623 - MONUMENT BOX RECONSTRUCTED TO GRADE, AS PER PLAN. A LIST OF LOCATIONS AND TOTAL QUANTITY AS SHOWN BELOW IS CARRIED TO THE GENERAL SUMMARY:

HUR-20-16.84-28.69 (01/NHS/PV)

| | | | | |
|-----------|-------|-------|-------|-------|
| SLM 17.72 | 17.80 | 21.02 | 25.14 | 25.23 |
| 25.32 | 25.42 | | | |

ITEM 623 - MONUMENT BOX RECONSTRUCTED TO GRADE, AS PER PLAN (CONT.)

ITEM 623 - MONUMENT BOX RECONSTRUCTED TO GRADE, AS PER PLAN 01/NHS/PV 7 EACH

ITEM 614 - MAINTAINING TRAFFIC LANE CLOSURE/REDUCTION REQUIRED

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ITEM 614 - MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

- ALL ROUTES:
- CHRISTMAS FOURTH OF JULY
 - NEW YEARS LABOR DAY
 - MEMORIAL DAY THANKSGIVING

THE FOLLOWING SUMMIT MOTORSPORTS PARK RACE EVENTS: SUMMIT RACING EQUIPMENT NHRA NATIONALS (LATE JUNE) BLUE SUEDE CRUISE (MID JULY) AMES PERFORMANCE PONTIAC NATIONALS (EARLY AUGUST) KELLY SERVICES NIGHT UNDER FIRE (MID AUGUST) NMCA ALL-AMERICAN NATIONALS (LATE AUGUST) HALLOWEEN CLASSIC (MID OCTOBER)

EVENT DATES ARE APPROXIMATES AND ARE BASED OFF THE 2018 EVENT DATES. TIMES ARE SUBJECT TO CHANGE FOR 2019. ONLY THE TWO-LANE SECTION OF US 20 IS TO FOLLOW THE NO WORK DURING THE ABOVE LISTED SUMMIT MOTORSPORTS PARK SPECIAL RACE EVENTS. THIS SECTION ALONG WITH THE REMAINDER OF THE PROJECT IS STILL TO FOLLOW ALL HOLIDAY WORK RESTRICTIONS AS OUTLINED IN THIS NOTE.

THE CONTRACTOR SHALL CONTACT THE RACEWAY TO OBTAIN INFORMATION ON THE DATES FOR RACE EVENTS: SUMMIT MOTORSPORTS PARK 1300 STATE ROUTE 18 NORWALK, OH 44857 419-668-5555 HTTP://SUMMITMOTORSPORTSPARK.COM

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

| DAY OF THE WEEK | TIME ALL LANES MUST BE OPEN TO TRAFFIC |
|-----------------|---|
| SUNDAY | 12:00N FRIDAY THROUGH 6:00 AM MONDAY |
| MONDAY | 12:00N FRIDAY THROUGH 6:00 AM TUESDAY |
| TUESDAY | 12:00N MONDAY THROUGH 6:00 AM WEDNESDAY |
| WEDNESDAY | 12:00N TUESDAY THROUGH 6:00 AM THURSDAY |
| THURSDAY | 12:00N WEDNESDAY THROUGH 6:00 AM MONDAY |
| FRIDAY | 12:00N THURSDAY THROUGH 6:00 AM MONDAY |
| SATURDAY | 12:00N FRIDAY THROUGH 6:00 AM MONDAY |

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE FEE OF \$50 FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

BUTT JOINTS

BUTT JOINTS SHALL NOT BE CUT AND LEFT OPEN TO TRAFFIC. THEY SHALL BE FILLED IN WITH A TEMPORARY ASPHALT CONCRETE WEDGE USING ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC.

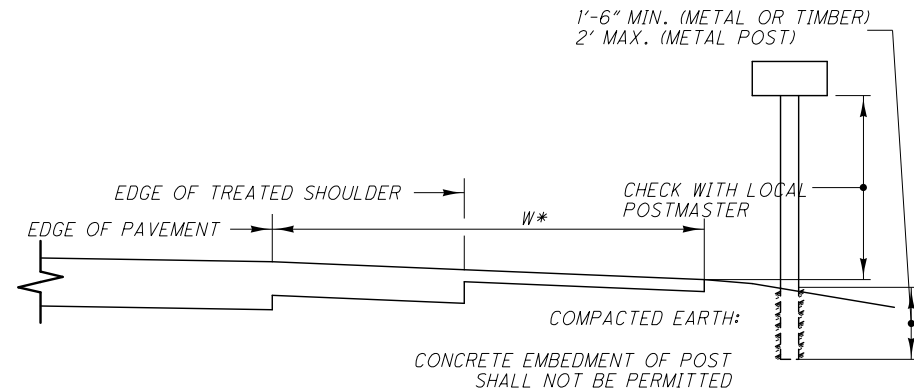
CONSTRUCTION "BUMP" (W8-1-36) AND "ADVISORY SPEED" (W13-1-24) SIGNS SHALL BE ERECTED AND MAINTAINED DURING THE PERIOD THE BUTT JOINT IS LEFT OPEN. THESE SIGNS SHALL BE PAID FOR UNDER THE LUMP SUM ITEM FOR ITEM 614 MAINTAINING TRAFFIC.

ITEM 614 - MAINTAINING TRAFFIC (4-LANE ROUTES)

ALL ADVANCE WARNING SIGNS FOR ANY CONDITION WHICH RESTRICTS TRAFFIC SHALL BE ERECTED BEFORE ANY SUCH RESTRICTION IS PUT INTO EFFECT. ALL SUCH SIGNS SHALL BE COVERED OR REMOVED FROM THE VIEW OF TRAFFIC WHEN THEY ARE NOT APPLICABLE, WITH THE APPROVAL OF THE ENGINEER.

IF THE CONTRACTOR FAILS TO COMPLY WITH THE PROVISIONS FOR TRAFFIC CONTROL AS SET FORTH IN THESE PLANS OR WITH PROVISIONS OF THE ODOTCD, AND SUCH FAILURE RESULTS IN A CONDITION AT THE WORK SITE WHICH IS UNSAFE FOR TRAFFIC, THE ENGINEER SHALL SUSPEND WORK UNTIL THE CONTRACTOR COMPLIES WITH THE NECESSARY REQUIREMENTS.

ALL MAINTENANCE OF TRAFFIC SIGNS ARE PAID UNDER ITEM 614 - MAINTAINING TRAFFIC.



CROSS SECTION / ELEVATION VIEW

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ITEM 614 - MAINTAINING TRAFFIC: GENERAL

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH ITEM 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, PLAN DETAILS, STANDARD DRAWINGS, AND AS OUTLINED IN THE CONSTRUCTION AND MAINTENANCE SECTION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES CURRENT EDITION WITH THE LATEST REVISIONS. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED ON THIS PLAN.

THE FOLLOWING REQUIREMENTS SHALL ALSO APPLY:
THE CONTRACTOR SHALL SUBMIT, IN WRITING, A SCHEDULE OF OPERATIONS TO THE ENGINEER AND RECEIVE APPROVAL BEFORE WORK IS STARTED ON THE PROJECT. PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COORDINATE THE MAINTENANCE OF TRAFFIC OPERATIONS WITH THE LOCAL STATE HIGHWAY PATROL.

NIGHT WORK IS PERMITTED.

THE CONTRACTOR IS REQUIRED TO MAINTAIN ALL PAVEMENT THROUGHOUT THE PROJECT UNDER ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC DURING THE PERIOD FROM THE START OF WORK TO THE COMPLETION OF ALL WORK.

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO CONSTRUCT A TEMPORARY ASPHALT WEDGE FROM THE EXISTING PAVEMENT TO THE PLANED SURFACE AT BUTT JOINTS AND OTHER LOCATIONS THAT RESULT IN A DROP-OFF. THIS QUANTITY SHALL ALSO BE USED AT PLANED SURFACES WHERE A TEMPORARY ASPHALT WEDGE IS NEEDED AROUND CASTINGS. BEFORE RESURFACING OF THE PAVEMENT, THE TEMPORARY WEDGE SHALL BE REMOVED AND THE COST SHALL BE CONSIDERED INCIDENTAL TO ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC.

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC

01/NHS/PV = 60 CU YD
02/S&2/PV = 5 CU YD
03/STR/PV = 5 CU YD
05/NHS/PV = 5 CU YD
07/NHS/PV = 5 CU YD

ITEM 614 - WORK ZONE MARKING SIGN (2-LANE ROUTES)

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR TEMPORARY WORK ZONE MARKING SIGNS PER THE REQUIREMENTS OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS, 614.04.

01/NHS/PV - HUR-20-16.84-28.69, LOR-20-0.00-6.23, LOR-20-6.59-8.56 & LOR-20 INTERCHANGE RAMPS
WORK ZONE MARKING SIGN: (W8-H12A-36) NO EDGE LINE = 57 EACH
WORK ZONE MARKING SIGN: (R4-1-24) DO NOT PASS = 51 EACH
WORK ZONE MARKING SIGN: (R4-2-24) PASS WITH CARE = 45 EACH

02/S&2/PV - LOR-511-21.14-21.43
WORK ZONE MARKING SIGN: (W8-H12A-36) NO EDGE LINE = 2 EACH
WORK ZONE MARKING SIGN: (R4-1-24) DO NOT PASS = 1 EACH
WORK ZONE MARKING SIGN: (R4-2-24) PASS WITH CARE = 1 EACH

03/STR/PV - LOR-511-21.43-22.17 & OBERLIN-ELYRIA RD
WORK ZONE MARKING SIGN: (W8-H12A-36) NO EDGE LINE = 5 EACH
WORK ZONE MARKING SIGN: (R4-1-24) DO NOT PASS = 5 EACH
WORK ZONE MARKING SIGN: (R4-2-24) PASS WITH CARE = 2 EACH

05/NHS/PV - LOR-20-6.23-6.59 (CITY OF OBERLIN)
WORK ZONE MARKING SIGN: (W8-H12A-36) NO EDGE LINE = 4 EACH
WORK ZONE MARKING SIGN: (R4-1-24) DO NOT PASS = 4 EACH

07/NHS/PV - HUR-20-16.26-16.84 (CITY OF NORWALK)
WORK ZONE MARKING SIGN: (W8-H12A-36) NO EDGE LINE = 4 EACH
WORK ZONE MARKING SIGN: (R4-1-24) DO NOT PASS = 3 EACH
WORK ZONE MARKING SIGN: (R4-2-24) PASS WITH CARE = 2 EACH

TOTAL (01/NHS/PV) = 153 EACH
TOTAL (02/S&2/PV) = 4 EACH
TOTAL (03/STR/PV) = 12 EACH
TOTAL (05/NHS/PV) = 8 EACH
TOTAL (07/NHS/PV) = 9 EACH

FLOODLIGHTING (4-LANE ROUTES)

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT UNIT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

MAINTENANCE OF TRAFFIC SCHEME (4-LANE ROUTES)

THE CONTRACTOR SHALL SCHEDULE THEIR WORK AND METHODS IN ORDER TO MEET THE INTENT OF THE PLANS. THE PAVEMENT SURFACES TO BE USED BY THE TRAVELING PUBLIC SHALL BE ABLE TO DRAIN FREELY. ALL COSTS TO MAINTAIN THE ROADWAY AS PER THE CONSTRUCTION AND MATERIALS SPECIFICATIONS AND THE PLANS SHALL BE INCLUDED IN ITEM 614 LUMP SUM MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED.

THE CONTRACTOR SHALL SUBMIT A MAINTENANCE OF TRAFFIC PLAN TO THE ENGINEER FOR APPROVAL PRIOR TO IMPLEMENTING ANY LANE CLOSURES ON THE 4-LANE SECTION OF US ROUTE 20 IN LORAIN COUNTY. INCLUDE IN THIS PLAN, AT A MINIMUM, THE FOLLOWING:

1. A PLAN FOR THE SEQUENCE OF CONSTRUCTION FOR MILLING, PAVEMENT REPAIRS, AND PAVING OPERATIONS FOR EACH LANE BY DIRECTION AND FOR ALL RAMPS.
2. A PLAN FOR THE SCHEDULE OF LANE CLOSURES, SHOWING THE MAXIMUM LENGTH DURATION OF THESE CLOSURES.

WORK OPERATIONS (4-LANE ROUTES)

IN ADDITION TO THE REQUIREMENTS OF SECTION 614 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS THE FOLLOWING SHALL APPLY:

THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAVEL WHERE PRACTICAL. A FLAGGER SHALL BE USED WHERE THE CONTRACTOR'S EQUIPMENT MUST MERGE WITH THE TRAFFIC STREAM.

THE CONTRACTOR SHALL ARRANGE CONSTRUCTION OPERATIONS SO AS TO PREVENT ANY INTERFERENCE TO THE CONTINUOUS FLOW OF TRAFFIC. ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO THE CLOSED LANES UNLESS OTHERWISE APPROVED BY THE ENGINEER.

WORKING HOURS RESTRICTION

THE 4-LANE SEGMENT OF US-20 IN LORAIN COUNTY IS A RESTRICTED LANE CLOSURE ROUTE DUE TO HIGH TRAFFIC VOLUMES. A LANE CLOSURE IS DEFINED AS ANY RESTRICTION OF A LANE OF TRAFFIC INCLUDING, BUT IS NOT LIMITED TO, SET-UP AND TEARDOWN OF TRAFFIC CONTROL ZONES.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS IN PROGRESS, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

DURING THE PROJECT DURATION, LANE CLOSURES SHALL BE PERMITTED AS LISTED ON THE ODOT PLCM WEB SITE AT <http://plcm.dot.state.oh.us>.

ANY SINGLE LANE CLOSURES MUST CONFORM TO THE HOUR RESTRICTIONS AS SET FORTH ON THE ODOT PLCM WEB SITE. DURING TIMES THAT LANE CLOSURES ARE NOT PERMITTED, ALL LANES SHALL BE OPEN AND CAUSE NO IMPEDANCE TO TRAFFIC.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THE ABOVE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE FEE OF \$75 PER MINUTE THE REQUIREMENTS ARE NOT MET.

ITEM 614 - REPLACEMENT SIGN

FLAT SHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

THIS ITEM IS TO BE CONSIDERED INCIDENTAL TO MAINTAINING TRAFFIC ON THE PROJECT AND WILL BE PAID FOR UNDER THE LUMP SUM CONTRACT BID PRICE FOR ITEM 614 MAINTAINING TRAFFIC. IT SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED SIGN IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL SIGN.

ITEM 614 - REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

THIS ITEM IS TO BE CONSIDERED INCIDENTAL TO MAINTAINING TRAFFIC ON THE PROJECT AND WILL BE PAID FOR UNDER THE LUMP SUM CONTRACT BID PRICE FOR ITEM 614 MAINTAINING TRAFFIC. IT SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

CONSTRUCTION EQUIPMENT MEDIAN CROSSING

CONSTRUCTION EQUIPMENT SHALL CROSS THE MEDIAN ONLY AT THE EXISTING INTERSECTIONS AND U-TURN CROSSOVERS. NO ADDITIONAL EQUIPMENT CROSSINGS ARE PERMITTED.

MAINTENANCE OF TRAFFIC FOR MARKING PAVEMENT REPAIRS

PROVIDE LANE CLOSURES AS PER THE MAINTENANCE OF TRAFFIC NOTES IN THESE PLANS A MINIMUM OF 24 HOURS PRIOR TO PERFORMING PAVEMENT REPAIRS TO ALLOW THE ENGINEER TO IDENTIFY AND MARK THE AREAS OF PAVEMENT IN NEED OF REPAIRS.

PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NEEDED TO PERFORM THE ABOVE LISTED WORK IS CONSIDERED INCIDENTAL TO MAINTAINING TRAFFIC ON THE PROJECT AND WILL BE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 614 MAINTAINING TRAFFIC.

ITEM 614 - WORK ZONE SPEED ZONES (WZSZS)

THE FOLLOWING WORK ZONE SPEED ZONE (WZSZ) SPEED LIMIT REVISION(S) HAVE BEEN APPROVED FOR USE ON THIS PROJECT WHEN WORK ZONE CONDITIONS AND FACTORS ARE MET AS DESCRIBED BELOW:

| WZSZ REVISION NUMBER | COUNTY, ROUTE, SECTION | DIRECTION |
|----------------------|------------------------|-----------|
| 20527 | LOR-20-8.56 TO 13.54 | EB & WB |

POTENTIAL WZSZ LOCATIONS SHALL HAVE AN ORIGINAL (PRE-CONSTRUCTION) POSTED SPEED LIMIT OF =55 MPH, A QUALIFYING WORK ZONE CONDITION OF AT LEAST 0.5 MILE IN LENGTH, AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS, AND A WORK ZONE CONDITION IN PLACE THAT REDUCES THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS (I.E., LANE CLOSURE, LANE SHIFT, CROSSOVER, CONTRAFLOW, AND/OR SHOULDER CLOSURE). THE LENGTH OF THE WORK ZONE CONDITION IS MEASURED FROM THE BEGINNING OF THE TAPER FOR THE SUBJECT WORK ZONE CONDITION IMPACTING THE TRAVEL LANES AND/OR SHOULDER TO THE END OF THE DOWNSTREAM TAPER, WHERE DRIVERS ARE RETURNED TO TYPICAL ALIGNMENT. AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS IS REQUIRED TO BALANCE THE ADDITIONAL EXPOSURE CREATED BY INSTALLING AND REMOVING WZSZ SIGNING WITH THE TIME NEEDED TO COMPLETE THE WORK.

IF THE WORK ZONE MEETS THESE MINIMUM CRITERIA, IT SHALL BE ANALYZED FURTHER USING TABLE 1 BELOW TO DETERMINE IF AND WHEN IT QUALIFIES FOR A SPEED LIMIT REDUCTION. DEPENDING ON THE ORIGINAL POSTED SPEED LIMIT, THE TYPE OF TEMPORARY TRAFFIC CONTROL USED, AND WHETHER OR NOT WORKERS ARE PRESENT, A WARRANTED WZSZ WILL VARY IN THE APPROVED SPEED LIMIT TO BE POSTED OVER TIME.

C&MS ITEM 614, PARAGRAPH 614.02(B), INDICATES THAT TWO DIRECTIONS OF A DIVIDED HIGHWAY ARE CONSIDERED SEPARATE HIGHWAY SECTIONS. THEREFORE, IF THE WORK ON A MULTI-LANE DIVIDED HIGHWAY IS LIMITED TO ONLY ONE DIRECTION, A SPEED LIMIT REDUCTION IN THE DIRECTION OF THE WORK DOES NOT AUTOMATICALLY CONSTITUTE A SPEED LIMIT REDUCTION IN THE OPPOSITE DIRECTION. EACH DIRECTION SHALL BE ANALYZED INDEPENDENTLY FROM EACH OTHER.

ALL WZSZS FLUCTUATE BETWEEN TWO APPROVED REDUCED SPEED LIMITS OR BETWEEN AN APPROVED REDUCED SPEED LIMIT AND THE ORIGINAL POSTED SPEED LIMIT. ONLY ONE OF TWO SIGNING STRATEGIES SHALL BE USED TO IMPLEMENT A WZSZ.

[WZSZS USING DSL SIGN ASSEMBLIES SHALL BE IN ACCORDANCE WITH THIS NOTE, APPROVED LIST, SUPPLEMENTAL SPECIFICATION (SS) 808, AND TRAFFIC SCD MT-104.10.]

ONLY ONE WARRANTED SPEED LIMIT APPLIES AT ANY ONE TIME; SPEED LIMIT REDUCTIONS ARE NOT CUMULATIVE. WZSZS SHALL NOT BE USED FOR MOVING/MOBILE ACTIVITIES, AS DEFINED IN ODOT PART 6.

WHEN LOOKING UP THE WARRANTED WORK ZONE SPEED LIMITS, ALWAYS USE THE ORIGINAL, PRECONSTRUCTION, POSTED SPEED LIMIT. DO NOT USE A PRIOR OR CURRENT WORK ZONE SPEED LIMIT AS A LOOK UP VALUE IN THE TABLE. POSITIVE PROTECTION IS GENERALLY REGARDED AS PORTABLE BARRIER OR OTHER RIGID BARRIER IN USE ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WITHOUT POSITIVE PROTECTION IS GENERALLY REGARDED AS USING DRUMS, CONES, SHADOW VEHICLE, ETC., ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WORKERS ARE CONSIDERED AS BEING PRESENT WHEN ON-SITE, WORKING WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WHEN THE WORK ZONE CONDITION REDUCING THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS IS REMOVED, THE SPEED LIMIT DISPLAYED SHALL RETURN TO THE ORIGINAL POSTED SPEED LIMIT.

| ORIGINAL POSTED SPEED LIMIT | WITH POSITIVE PROTECTION | | WITHOUT POSITIVE PROTECTION | |
|-----------------------------|--------------------------|---------------------|-----------------------------|---------------------|
| | WORKERS PRESENT | WORKERS NOT PRESENT | WORKERS PRESENT | WORKERS NOT PRESENT |
| 70 | 60 | 65 | 55 | 65 |
| 65 | 55 | 60 | 50 | 60 |
| 60 | 55 | 60 | 50 | 60 |
| 55 | 50 | 55 | 45 | 55 |

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY (01/NHS/PV):

ITEM 614, DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY-----24 SIGN MONTHS ASSUMING 6 DSL SIGN ASSEMBLY(IES) FOR 4 MONTH(S) EACH.

ITEM 614 - LANE DROP-OFFS

MAINTAIN LANE DROP-OFFS AS PER STANDARD CONSTRUCTION DRAWING MT-101.90.

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN, AND REMOVE WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN(S) ON SITE FOR THE DURATION OF THE PROJECT. THE SIGN(S) SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS MAINTAINED BY THE DIRECTOR (OFFICE OF MATERIALS MANAGEMENT). THE APPROVED LIST OF PORTABLE CHANGEABLE MESSAGE SIGNS CAN BE FOUND ON THE ODOT WEB SITE BY CLICKING ON THE SERVICES MENU, THEN CLICKING ON MATERIALS MANAGEMENT. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 650 FT AND 475 FT RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM TO DIM THE SIGN DURING DARKNESS AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN(S) SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. PCMS TRAILERS SHALL BE DELINEATED ON A PERMANENT BASIS BY AFFIXING CONSPICUITY TAPE CONFORMING TO CMS 614.03 IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER AS SEEN BY ONCOMING ROAD USERS.

THE PROBABLE PCMS LOCATIONS WILL BE DETERMINED BY THE ENGINEER PRIOR TO BEGINNING WORK ON THIS PROJECT. PLACEMENT, OPERATIONS, MAINTENANCE, AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION, YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED TO FACE AWAY FROM TRAFFIC AND SHALL DISPLAY A MINIMUM OF ONE YELLOW RETROREFLECTIVE SHEETING SURFACE, A MINIMUM OF 9 INCHES BY 15 INCHES IN SIZE, FACING TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT AND TO REVISE SIGN MESSAGES IF NECESSARY.

THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PREPROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PREPROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED, OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL, IN ACTIVE CELLULAR PHONE AREAS, ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS, AND REVISION TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA LINK INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF CMS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE WILL BE DEDUCTED FROM MONEYS DUE OR TO BECOME DUE TO THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24 HOUR PER DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE, AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK. THE CONTRACTOR SHALL ONLY BE PAID FOR PCMS UNITS WHEN THEY ARE IN OPERATION ON THE PROJECT AS SPECIFIED IN THE PLANS OR BY THE ENGINEER.

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN
01/NHS/PV = 8 SIGN-MONTH

ITEM 614 - LIMIT OF TRAFFIC ON PLANED SURFACE

TRAFFIC IS PERMITTED TO RUN ON THE PLANED SURFACE FOR A MAXIMUM OF 14 CONSECUTIVE DAYS. FOR EVERY DAY PAST THE 14 CONSECUTIVE DAYS THAT TRAFFIC IS FORCED TO RUN ON THE PLANED SURFACE, A DISINCENTIVE FEE OF \$1000 PER DAY WILL BE ASSESSED TO THE CONTRACTOR.

ITEM 614 - WORKSITE TRAFFIC SUPERVISOR

THIS ITEM IS FOR THE PLANING AND PAVING OF THE FOUR-LANE SECTION OF US 20, THE RAMP CLOSURES ON US 20, AND THE BRIDGE WORK ASSOCIATED WITH HUR-20-2449.

SUBJECT TO APPROVAL OF THE ENGINEER, THE CONTRACTOR SHALL EMPLOY AND IDENTIFY (SOMEONE OTHER THAN THE SUPERINTENDENT) A CERTIFIED WORKSITE TRAFFIC SUPERVISOR (WTS) BEFORE STARTING WORK IN THE FIELD. THE WTS MAY BE CERTIFIED FROM ONE OF THE FOLLOWING ORGANIZATIONS:

1. AMERICAN TRAFFIC SAFETY SERVICE ASSOCIATION (ATSSA), PHONE NUMBER 1-800-272-8772, CERTIFIED TRAFFIC CONTROL SUPERVISOR (TCS).
2. NATIONAL HIGHWAY INSTITUTE, DESIGN AND OPERATION OF WORK ZONE TRAFFIC CONTROL, PHONE NUMBER 1-703- 235-0528.
3. THE OHIO CONTRACTORS ASSOCIATION, TRAFFIC CONTROL SUPERVISOR (OCA/TCS) WORK ZONE CLASS, ONLY IF TAKEN AFTER MAY 5, 2004, PHONE NUMBER 1-614-599-7915.
4. OHIO LABORERS TRAINING, TRAFFIC CONTROL SUPERVISORS CLASS, PHONE NUMBER 1-740-599-7915.

A COPY OF EACH WTS'S CERTIFICATION AND 24-HOUR CONTACT INFORMATION SHALL BE PROVIDED TO THE ENGINEER AT THE PRECONSTRUCTION CONFERENCE. IF THE DESIGNATED WTS WILL NOT BE AVAILABLE FULL TIME (24/7) THE CONTRACTOR MAY DESIGNATE AN ALTERNATE WTS TO BE AVAILABLE WHEN THE PRIMARY IS OFF DUTY. EACH WTS SHALL HAVE A CURRENT WTS CERTIFICATION (WITH AN EXPIRATION DATE NO MORE THAN 5 YEARS FROM THE DATE OF ISSUE) FROM ANY OF THE APPROVED ORGANIZATIONS.

THE WTS POSITION HAS THE RESPONSIBILITY OF MONITORING TRAFFIC CONTROL DEFICIENCIES FOR THE ENTIRE WORK ZONE. THE DUTIES OF THE WTS ARE AS FOLLOWS:

1. BE AVAILABLE ON A 24-HOUR PER DAY BASIS, AND BE ABLE TO BE ON SITE FOR ALL EMERGENCY TRAFFIC CONTROL NEEDS WITHIN ONE HOUR OF NOTIFICATION BY POLICE OR PROJECT STAFF AND BE PREPARED TO EFFECT CORRECTIVE MEASURES IMMEDIATELY ON EXISTING WORK ZONE TRAFFIC CONTROL DEVICES.
2. ATTEND PRECONSTRUCTION MEETING AND ALL PROJECT MEETINGS WHERE TRAFFIC CONTROL MANAGEMENT IS DISCUSSED.
3. BE AVAILABLE FOR MEETINGS OR DISCUSSIONS WITH THE ENGINEER UPON REQUEST OR WITHIN 36 HOURS.
4. BE AWARE OF, AND COORDINATE IF NECESSARY, ALL TRAFFIC CONTROL OPERATIONS, INCLUDING THOSE OF SUBCONTRACTORS AND SUPPLIERS.
5. COORDINATE PROJECT ACTIVITIES WITH ALL LAW ENFORCEMENT OFFICERS (LEOS). A WTS SHALL ALSO BE THE MAIN CONTACT PERSON WITH THE LEO'S WHILE THEY ARE ON THE PROJECT.
6. COORDINATE MEETINGS WITH ODOT PERSONNEL, LEO'S AND OTHER APPLICABLE ENTITIES BEFORE EACH PLAN PHASE SWITCH TO DISCUSS WORK ZONE TRAFFIC CONTROL.
7. ENSURE COMPLIANCE WITH THE CONTRACT DOCUMENTS FOR SIGNS, BARRICADES, TEMPORARY CONCRETE BARRIER, PAVEMENT MARKINGS, PORTABLE MESSAGE SIGNS, AND OTHER TRAFFIC CONTROL DEVICES ON A DAILY BASIS; AND FACILITATE ANY CORRECTIVE ACTION NECESSARY.
8. NOTIFY THE CONTRACTOR OF THE NEED FOR CLEANING AND MAINTENANCE OF ALL TRAFFIC CONTROL DEVICES, INCLUDING THE COVERING AND REMOVAL OF INAPPLICABLE SIGNS.
9. INSPECT, EVALUATE, PROPOSE NECESSARY MODIFICATIONS TO, AND DOCUMENT THE EFFECTIVENESS OF, THE TRAFFIC CONTROL DEVICES AND/OR TRAFFIC OPERATIONS ON A DAILY BASIS (7 DAYS A WEEK). IN ADDITION, A WEEKLY NIGHT INSPECTION OF THE WORK ZONE SETUP FOR DAYTIME WORK OPERATIONS; AND ONE DAYTIME INSPECTION PER WEEK FOR NIGHTTIME PROJECTS. THIS SHALL INCLUDE (BUT NOT BE LIMITED TO) DOCUMENTATION ON THE FOLLOWING PROJECT EVENTS:
 - A. INITIAL TRAFFIC CONTROL SETUP (DAY AND NIGHT REVIEW).
 - B. DAILY TRAFFIC CONTROL SETUP AND REMOVAL.
 - C. WHEN CONSTRUCTION STAGING CAUSES A CHANGE IN THE TRAFFIC CONTROL SETUP.
 - D. CRASH OCCURRENCES WITHIN THE CONSTRUCTION AREA.
 - E. REMOVAL OF TRAFFIC CONTROL DEVICES AT THE END OF A PHASE OR PROJECT.
 - F. ALL OTHER EMERGENCY TRAFFIC CONTROL NEEDS.
10. COMPLETE THE DEPARTMENT APPROVED LONG TERM INSPECTION FORM (CA-D-8) AFTER EACH INSPECTION AS REQUIRED IN # 9 AND SUBMIT IT TO THE ENGINEER THE FOLLOWING WORK DAY. THESE REPORTS SHALL INCLUDE A CHECKLIST OF ALL TRAFFIC CONTROL MAINTENANCE ITEMS TO BE REVIEWED. A COPY OF THE FORM WILL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING. ANY DEFICIENCIES OBSERVED SHALL BE NOTED, ALONG WITH RECOMMENDED CORRECTIVE ACTIONS AND THE DATES BY WHICH SUCH CORRECTIONS WERE, OR WILL BE, COMPLETED. A COPY OF THIS DOCUMENT CAN BE FOUND IN THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION INSPECTION FORMS MANUAL DATED 10/15/06 OR CURRENT REVISION.

ITEM 614 - WORKSITE TRAFFIC SUPERVISOR (CONT.)

11. VERIFY THAT ALL FLAGGING OPERATIONS ARE BEING CONDUCTED PER THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

12. HAVE COPIES OF THE ODOT TEMPORARY TRAFFIC CONTROL MANUAL AND APPLICABLE STANDARDS AND SPECIFICATIONS INCLUDED IN THE CONTRACT DOCUMENTS AVAILABLE AT ALL TIMES ON THE PROJECT.

THE DEPARTMENT WILL NOT PAY THE UNIT PRICE BID FOR THE WTS FOR ANY DAY ON WHICH THE CONTRACTOR FAILS TO PERFORM THE DUTIES SET FORTH ABOVE. SHOULD THE CONTRACTOR'S FAILURE TO PERFORM ANY OF THE DUTIES DESCRIBED ABOVE RESULT IN A MAINTENANCE OF TRAFFIC SAFETY ISSUE, THE DEPARTMENT WILL DEDUCT THE PRORATED DAILY AMOUNT FOR ITEM 614 MAINTENANCE OF TRAFFIC FROM THE CONTRACTOR'S NEXT SCHEDULED ESTIMATE.

IF THREE OR MORE FAILURES TO PERFORM THE DUTIES SET FORTH ABOVE OCCUR, THE WTS SHALL BE IMMEDIATELY REMOVED FROM THE WORK IN ACCORDANCE WITH C&MS 108.05.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED FOR THE WORKSITE TRAFFIC SUPERVISOR:

ITEM 614 - WORKSITE TRAFFIC SUPERVISOR 01/NHS/PV: 2 MONTHS
04/NHS/BR: 1 MONTH

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE LATEST EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED LAW ENFORCEMENT OFFICER (AND OFFICIAL PATROL CAR WITH MOUNTED EMERGENCY FLASHING LIGHTS) SHALL BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS AS DIRECTED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED.

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION.

LAW ENFORCEMENT OFFICERS (LEO'S) SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED. THE LEO'S ARE CONSIDERED TO BE EMPLOYED BY THE CONTRACTOR AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR ACTIONS. ALTHOUGH THEY ARE EMPLOYED BY THE CONTRACTOR, THE PROJECT ENGINEER SHALL HAVE CONTROL OVER THEIR PLACEMENT. THE OFFICIAL PATROL CAR SHALL BE A PUBLIC SAFETY VEHICLE AS REQUIRED BY THE OHIO REVISED CODE. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEO'S SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THESE SERVICES AND PROVIDE 72 HOURS ADVANCE NOTICE AS REQUIRED BY THE HIGHWAY PATROL LISTED BELOW:

STATE HIGHWAY PATROL
HURON COUNTY POST
300 SOUTH NORWALK ROAD
NORWALK, OHIO 44857
419-668-4087

STATE HIGHWAY PATROL
LORAIN COUNTY POST
38000 CLETUS DRIVE
ELYRIA, OHIO 44035
440-365-5045

LAW ENFORCEMENT OFFICERS WITH PATROL CAR REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE
01/NHS/PV = 168 HOURS
05/NHS/PV = 16 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

IF THE CONTRACTOR WISHES TO UTILIZE LEO'S FOR FLAGGING AND TRAFFIC CONTROL OTHER THAN FOR THAT REQUIRED IN THESE PLANS, THEY MAY DO SO AT THEIR OWN EXPENSE.

ITEM 614 - DETOUR SIGNING

THE FOLLOWING QUANTITY IS INCLUDED FOR THE CONTRACTOR TO PROVIDE THE DETOUR SIGNING AS SHOWN AS PER 614.06 (B):

ITEM 614 - DETOUR SIGNING LUMP (01/NHS/PV)

NOTIFICATIONS OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW. NOTIFICATIONS SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS. UPON RECEIPT OF NOTIFICATION BY THE CONTRACTOR, THE PROJECT ENGINEER WILL ARRANGE NOTIFICATION OF THE FOLLOWING ORGANIZATIONS, IN WRITING, IN ACCORDANCE WITH THE BELOW TABLE:

- LORAIN COUNTY ENGINEER'S OFFICE
- NEW RUSSIA TOWNSHIP TRUSTEES (TOWNSHIP ROADS ONLY)
- LOCAL POLICE, FIRE, AND EMERGENCY MEDICAL SERVICES
- LOCAL SCHOOL DISTRICTS
- LORAIN COUNTY SHERIFF'S OFFICE
- ODOT DISTRICT THREE OFFICE OF ROADWAY SERVICES
- ODOT DISTRICT THREE PUBLIC INFORMATION OFFICE
- SPECIAL HAULING PERMITS SECTION (Hauling.Permits@dot.ohio.gov)

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE

| ITEM | DURATION OF CLOSURE | NOTICE LEAD TIME REQUIRED* |
|---|-----------------------|--|
| RAMP AND/OR ROAD CLOSURES | TWO WEEKS OR GREATER | 21 CALENDAR DAYS |
| | 12 HOURS TO TWO WEEKS | 14 CALENDAR DAYS |
| | 12 HOURS OR LESS | 4 BUSINESS DAYS |
| LANE CLOSURES AND RESTRICTIONS | TWO WEEKS OR GREATER | 14 CALENDAR DAYS |
| | LESS THAN TWO WEEKS | 5 BUSINESS DAYS |
| START OF CONSTRUCTION AND TRAFFIC PATTERN CHANGES | N/A | 14 CALENDAR DAYS PRIOR TO IMPLEMENTATION |

* - PRIOR TO CLOSURE DATE, UNLESS NOTED OTHERWISE
 ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ITEM 614 - MAINTENANCE OF TRAFFIC (NOTICE OF CLOSURE SIGNS)

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN ONE WEEK.

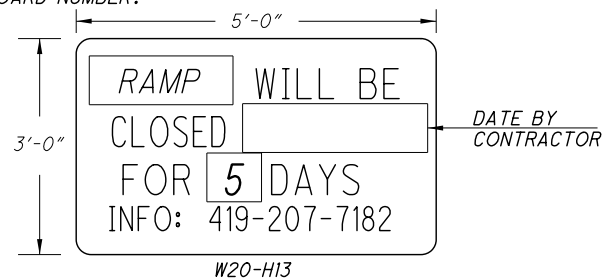
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NOTICE OF CLOSURE SIGN TIME TABLE

| ITEM | DURATION OF CLOSURE | SIGN DISPLAYED TO PUBLIC* |
|---------------------------|-----------------------|---------------------------|
| RAMP AND/OR ROAD CLOSURES | TWO WEEKS OR GREATER | 14 CALENDAR DAYS |
| | 12 HOURS TO TWO WEEKS | 7 CALENDAR DAYS |
| | 12 HOURS OR LESS | 2 BUSINESS DAYS |

* - PRIOR TO CLOSURE DATE, UNLESS NOTED OTHERWISE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH MOTORISTS MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.



ITEM 614 - MAINTAINING TRAFFIC

DETOUR LIMITATION:
 TWO WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED FIVE (5) CONSECUTIVE CALENDAR DAYS. THROUGH TRAFFIC WILL BE DETOURED AS SHOWN ON THIS SHEET.

THE CONTRACTOR WILL INSTALL, MAINTAIN, AND SUBSEQUENTLY REMOVE THE DETOUR SIGNING.

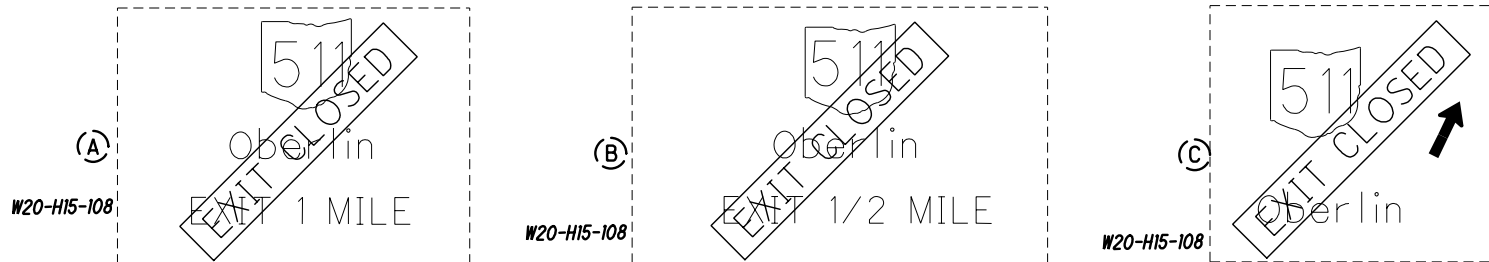
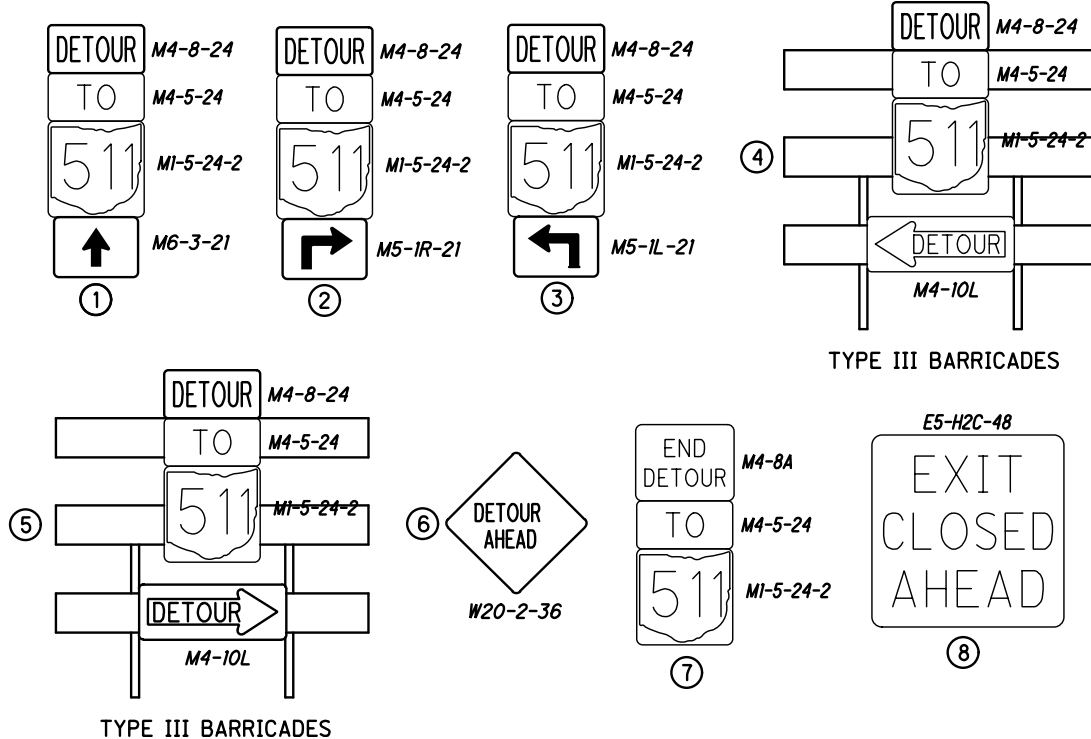
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INTERIM COMPLETION DATE:
 THE FIVE (5) CONSECUTIVE CALENDAR DAYS SHALL BE CONSIDERED AN INTERIM COMPLETION DATE, AND FOR EACH CALENDAR DAY BEYOND THE FIVE (5) CONSECUTIVE CALENDAR DAYS THAT THE ROADWAY REMAINS CLOSED TO TRAFFIC, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN ACCORDANCE WITH THE TRAFFIC ENGINEERING MANUAL.

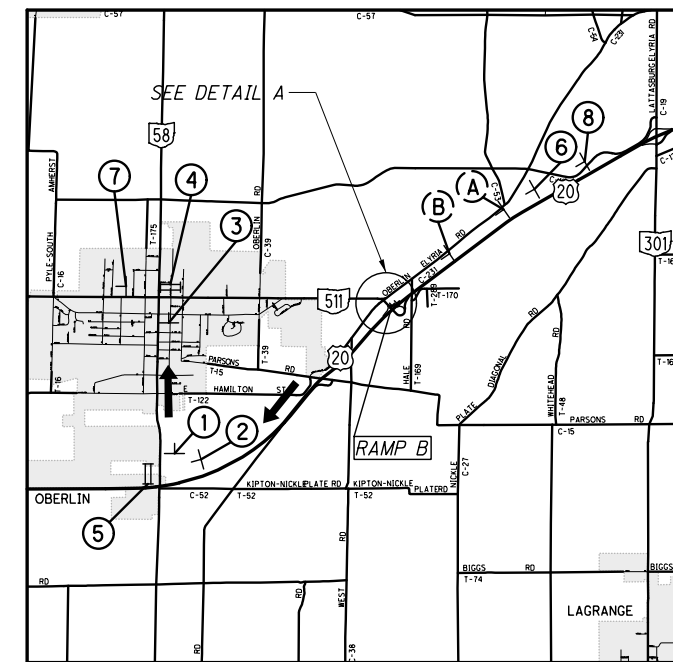
ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES, AS PER SECTION 614.02 (A).

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATION, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

SIGN LEGEND



LOR-20/511 RAMP B DETOUR MAP

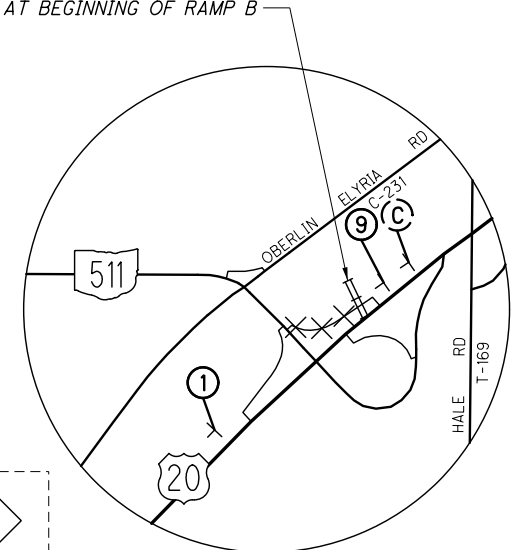


MAP LEGEND

- X - PROJECT LOCATION
- ↑ ↓ - OFFICIAL STATE SIGNED DETOUR
- || - GATES AND BARRICADES, AS PER MT-101.60



PLACE BARRICADES AT BEGINNING OF RAMP B



DETAIL A

NOTIFICATIONS OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW. NOTIFICATIONS SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS. UPON RECEIPT OF NOTIFICATION BY THE CONTRACTOR, THE PROJECT ENGINEER WILL ARRANGE NOTIFICATION OF THE FOLLOWING ORGANIZATIONS, IN WRITING, IN ACCORDANCE WITH THE BELOW TABLE:

- LORAIN COUNTY ENGINEER'S OFFICE
- NEW RUSSIA TOWNSHIP TRUSTEES (TOWNSHIP ROADS ONLY)
- LOCAL POLICE, FIRE, AND EMERGENCY MEDICAL SERVICES
- LOCAL SCHOOL DISTRICTS
- LORAIN COUNTY SHERIFF'S OFFICE
- ODOT DISTRICT THREE OFFICE OF ROADWAY SERVICES
- ODOT DISTRICT THREE PUBLIC INFORMATION OFFICE
- SPECIAL HAULING PERMITS SECTION (Hauling.Permits@dot.ohio.gov)

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE

| ITEM | DURATION OF CLOSURE | NOTICE LEAD TIME REQUIRED* |
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| START OF CONSTRUCTION AND TRAFFIC PATTERN CHANGES | N/A | 14 CALENDAR DAYS PRIOR TO IMPLEMENTATION |

* - PRIOR TO CLOSURE DATE, UNLESS NOTED OTHERWISE
 ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ITEM 614 - MAINTENANCE OF TRAFFIC (NOTICE OF CLOSURE SIGNS)

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN ONE WEEK.

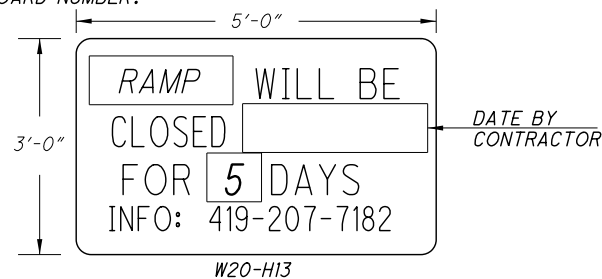
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ITEM 614 - MAINTAINING TRAFFIC

DETOUR LIMITATION: TWO WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED THIRTY (30) CONSECUTIVE CALENDAR DAYS. THROUGH TRAFFIC WILL BE DETOURED AS SHOWN ON THIS SHEET.

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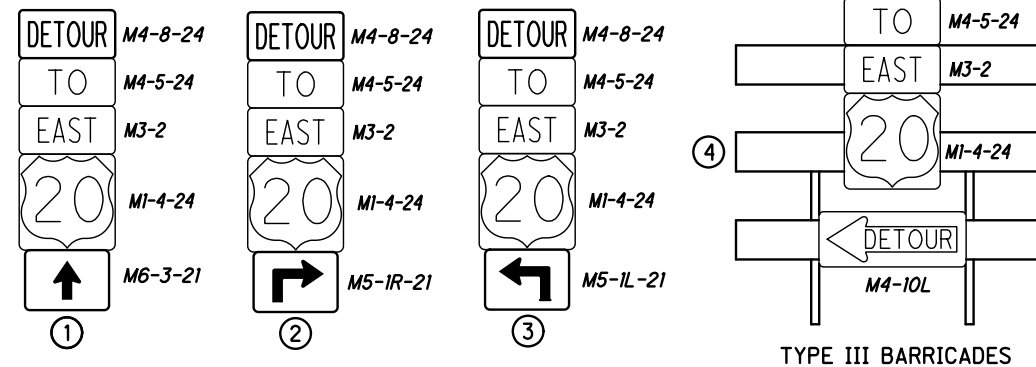
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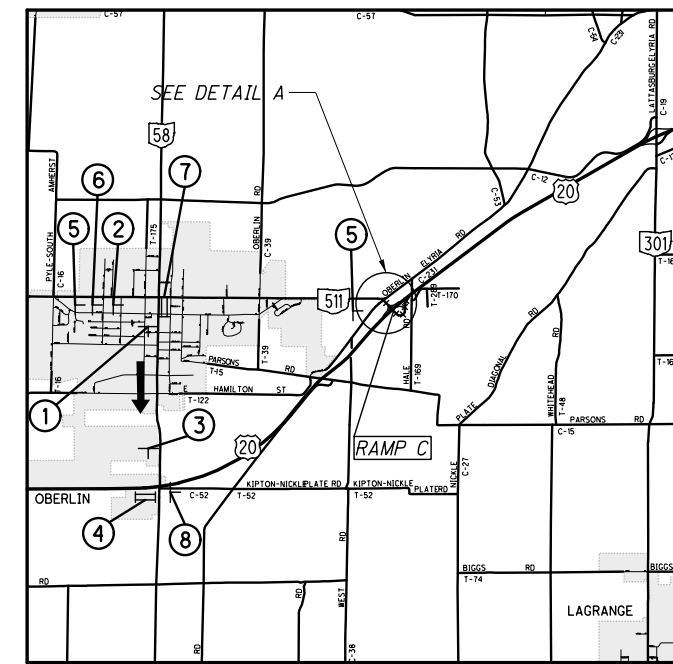
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SIGN LEGEND



LOR-20/511 RAMP C DETOUR MAP



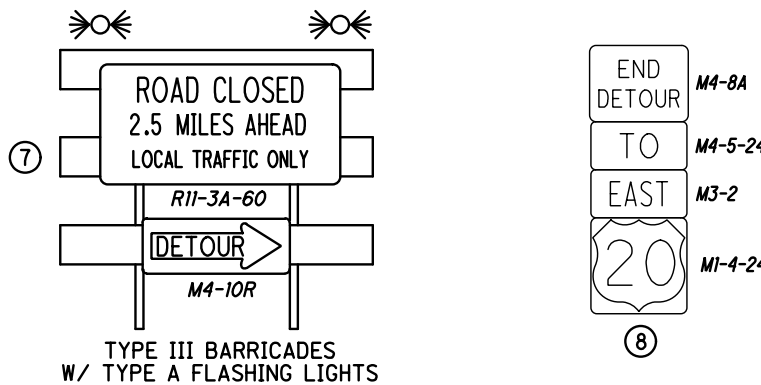
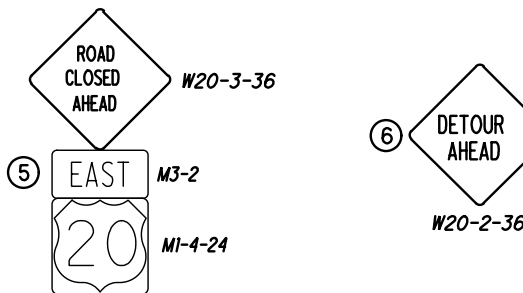
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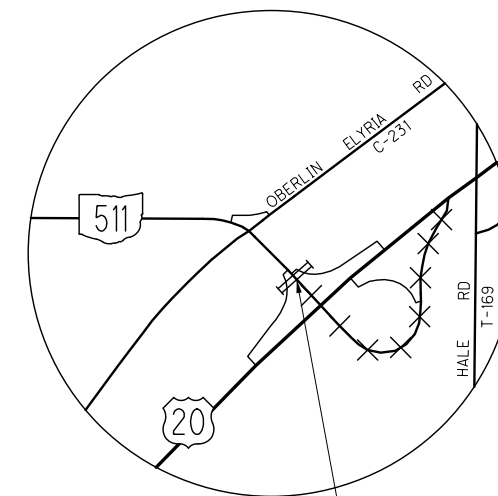
SCALE IN MILES



TYPE III BARRICADES



TYPE III BARRICADES W/ TYPE A FLASHING LIGHTS



DETAIL A

PLACE BARRICADES AT BEGINNING OF RAMP C

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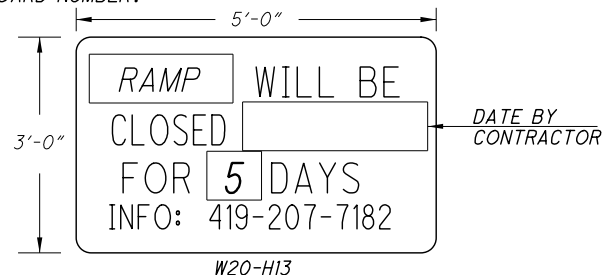
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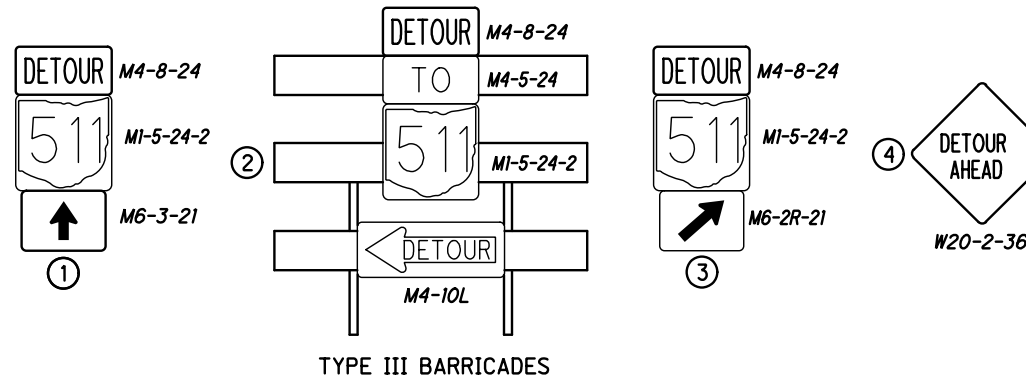
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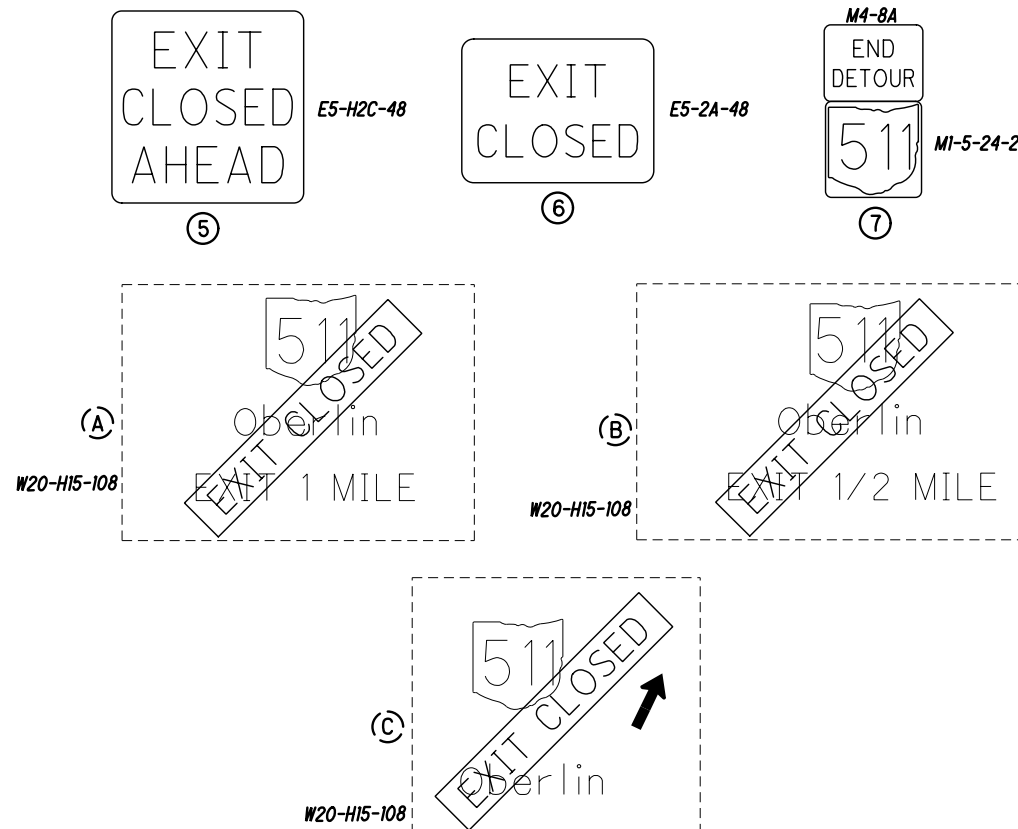
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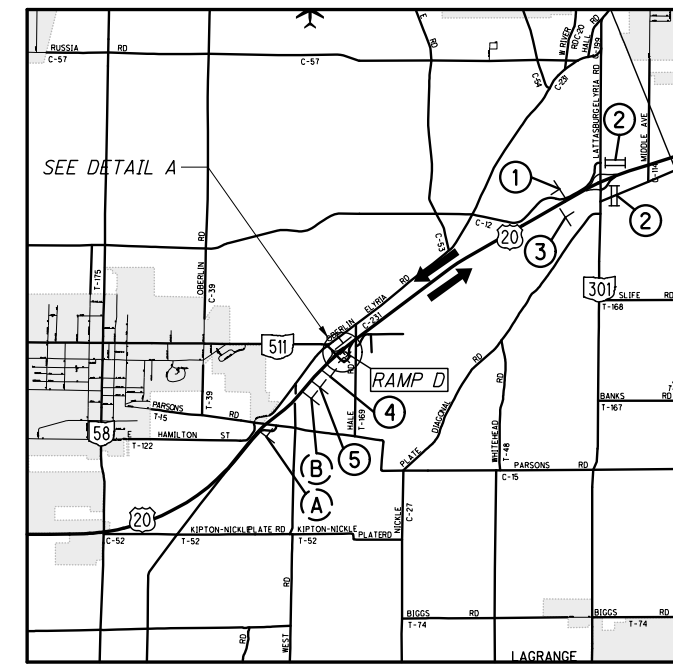
SIGN LEGEND



TYPE III BARRICADES



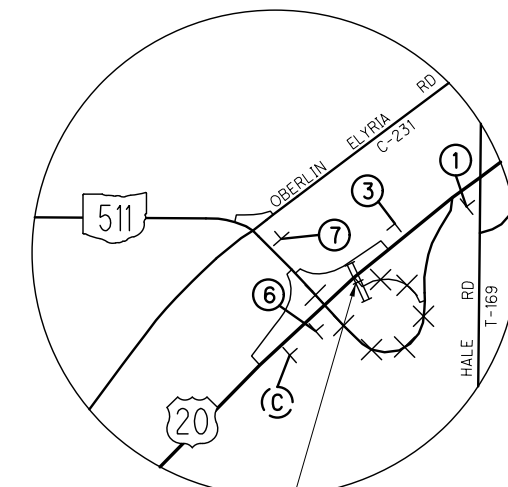
LOR-20/511 RAMP D DETOUR MAP



MAP LEGEND

- X - PROJECT LOCATION
- ↑ - OFFICIAL STATE SIGNED DETOUR
- || - GATES AND BARRICADES, AS PER MT-101.60

SCALE IN MILES

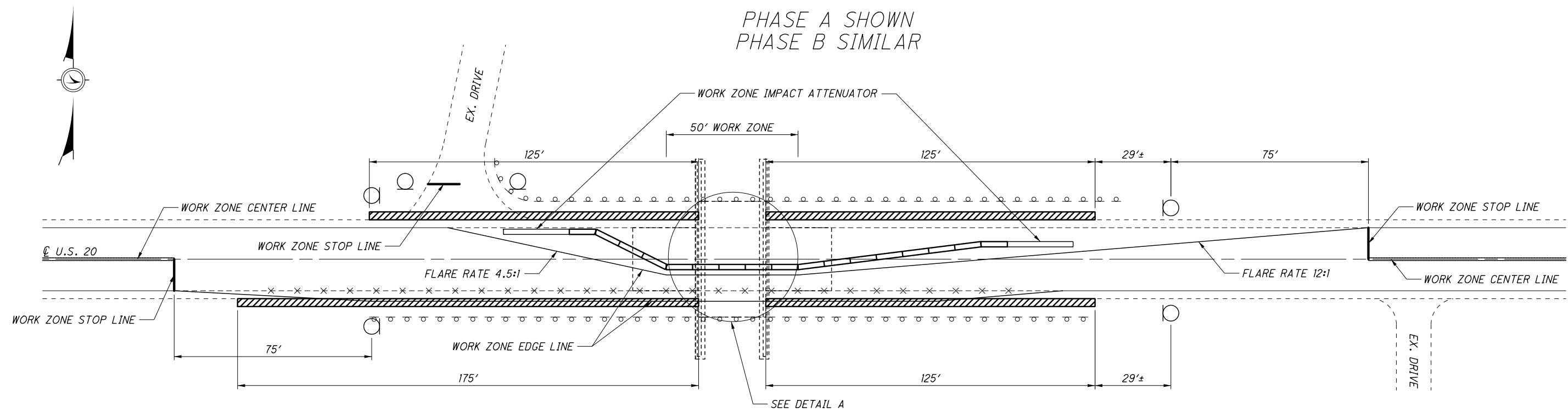


DETAIL A

PLACE BARRICADES AT BEGINNING OF RAMP D

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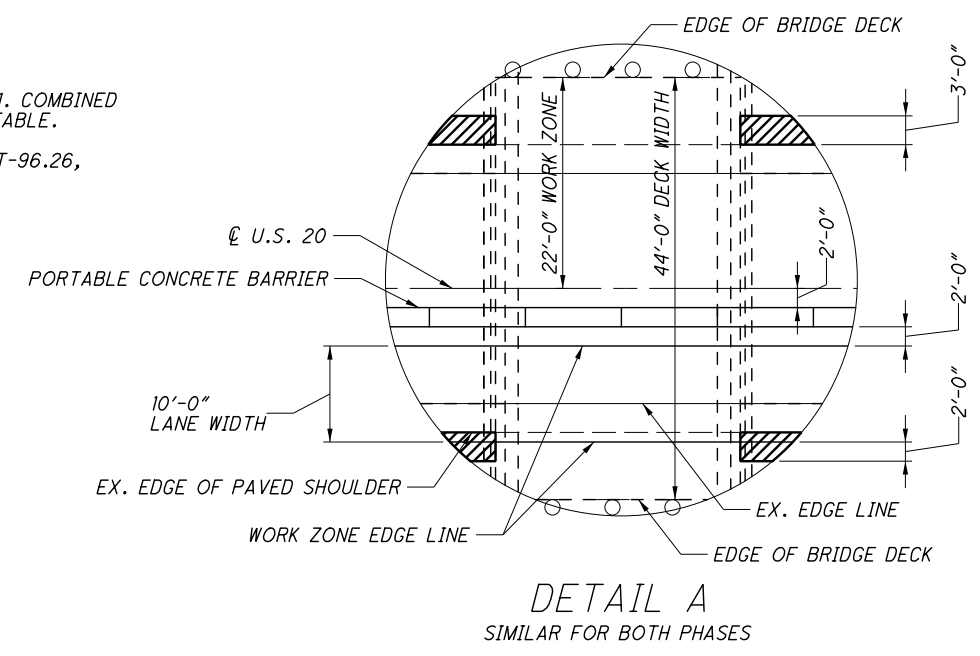
PHASE A SHOWN
PHASE B SIMILAR



NOTES:

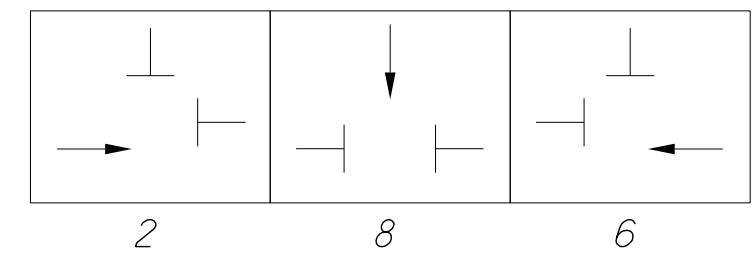
- 1) PHASE A IS SHOWN. FOR PHASE B DETAILS, SEE SCD MT-96.11. COMBINED PHASE A AND PHASE B QUANTITIES ARE SHOWN IN THE BELOW TABLE.
- 2) FOR ADDITIONAL DETAILS, SEE SCDS MT-96.11, MT-96.20, MT-96.26, MT-101.70 AND ALSO SUPPLEMENTAL SPECIFICATION 961.

- = SIGNAL SUPPORT LOCATION
- ▨ = ITEM 615 - PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A



DETAIL A
SIMILAR FOR BOTH PHASES

SIGNAL PHASING DIAGRAM



SIGNAL TIMING

VEHICULAR DETECTION SHALL BE APPROVED BY THE ENGINEER AND INSTALLED PRIOR TO ACTIVATING SIGNAL PHASING. A TWO PHASE CONTROLLER WITH CABINET CAPABLE OF BEING SET WITH THE FOLLOWING SPLITS SHALL BE FURNISHED:

| PHASE | GREEN | YELLOW | ALL-RED | RECALL |
|---------|-------|--------|---------|--------|
| 2 (EB) | 21 | 5 | 16 | MIN |
| 8 (SB)* | 8 | 3 | 5 | NONE |
| 6 (WB) | 21 | 5 | 16 | MIN |

*PHASES ARE ACTUATED ONLY (SKIPPED IF NO VEHICLE PRESENT)

| ESTIMATED QUANTITIES HUR-20-2226 (SFN: 3901416) | | | |
|---|----------|------|--|
| ITEM | QUANTITY | UNIT | DESCRIPTION |
| 614 | 4 | EACH | WORK ZONE IMPACT ATTENUATOR |
| 614 | 19 | EACH | BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL) |
| 614 | 14 | EACH | BARRIER REFLECTOR, TYPE 2 (BIDIRECTIONAL) |
| 614 | 12 | EACH | OBJECT MARKER, TWO WAY |
| 614 | 0.19 | MILE | WORK ZONE CENTER LINE, CLASS I, 740.06, TYPE I |
| 614 | 0.27 | MILE | WORK ZONE EDGE LINE, CLASS I, 6", 740.06, TYPE I |
| 614 | 72 | FT | WORK ZONE STOP LINE, CLASS I, 740.06, TYPE I |
| 615 | 183 | SY | PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A |
| 622 | 320 | FT | PORTABLE BARRIER, 32" |
| 622 | 100 | FT | PORTABLE BARRIER, 32", BRIDGE MOUNTED (UNANCHORED) |

ALL QUANTITIES CARRIED TO STRUCTURE SUB-SUMMARY (04/NHS/BR)

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| SHEET NUM. | | | | | | | | PART. | | | | | | | | ITEM | ITEM EXT | GRAND TOTAL | UNIT | DESCRIPTION | SEE SHEET NO. | | |
|---|----|----|----|---------|----|----|-----|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|------|----------|-------------|----------|-------------|---------------|--|----|
| 11 | 13 | 14 | 15 | 24 | 25 | 27 | 31 | 01/NHS/PV | 02/SC2/PV | 03/STR/PV | 04/NHS/BR | 05/NHS/PV | 06/NHS/PV | 07/NHS/PV | 08/NHS/PV | | | | | | | 09/NHS/BR | |
| ROADWAY | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | 65.51 | | | | 62 | 0.56 | 1.93 | | 0.72 | | 0.3 | | | | 209 | 72051 | 65.51 | MILE | PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN | 11 |
| | 7 | | | | | | | 7 | | | | | | | | | | 623 | 39601 | 7 | EACH | MONUMENT BOX RECONSTRUCTED TO GRADE, AS PER PLAN | 12 |
| | 6 | | | | | | | 6 | | | | | | | | | | SPECIAL | 69050100 | 6 | EACH | MAILBOX SUPPORT SYSTEM, SINGLE | 13 |
| | 2 | | | | | | | 2 | | | | | | | | | | SPECIAL | 69050200 | 2 | EACH | MAILBOX SUPPORT SYSTEM, DOUBLE | 13 |
| DRAINAGE | | | | | | | | | | | | | | | | | | | | | | | |
| | 4 | | | | | | | 4 | | | | | | | | | | 611 | 98630 | 4 | EACH | CATCH BASIN ADJUSTED TO GRADE | |
| | 12 | | | | | | | 12 | | | | | | | | | | 611 | 99150 | 12 | EACH | INLET ADJUSTED TO GRADE | |
| | 12 | | | | | | | 10 | 1 | | | 1 | | | | | | 611 | 99654 | 12 | EACH | MANHOLE ADJUSTED TO GRADE | |
| PAVEMENT | | | | | | | | | | | | | | | | | | | | | | | |
| 1,500 | | | | | | | | 1,433 | 14 | 35 | | | | 16 | 2 | | | 251 | 01030 | 1,500 | CY | PARTIAL DEPTH PAVEMENT REPAIR (442) | |
| 38 | | | | | | | | 34 | 1 | 1 | | | | 1 | 1 | | | 253 | 02000 | 38 | CY | PAVEMENT REPAIR | |
| | | | | 51,971 | | | | 49,554 | 401 | 1,059 | | 840 | | | 117 | | | 407 | 20000 | 51,971 | GAL | NON-TRACKING TACK COAT | |
| | | | | 30,596 | | | | 29,097 | 263 | 757 | | 338 | | | 141 | | | 408 | 10001 | 30,596 | GAL | PRIME COAT, AS PER PLAN | 11 |
| | | | | 288 | | | | 288 | | | | | | | | | | 409 | 30001 | 288 | FT | SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN | 29 |
| | | | | 22,557 | | | | 21,508 | 174 | 459 | | 365 | | | 51 | | | 424 | 12000 | 22,557 | CY | FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (1.25") | |
| | | | | 570 | | | | 532 | 7 | 17 | | 10 | | | 4 | | | 424 | 12000 | 570 | CY | FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (SAFETY EDGE) | |
| | | | | 4,441 | | | | 4,210 | 45 | 115 | | 48 | | | 23 | | | 617 | 10100 | 4,441 | CY | COMPACTED AGGREGATE | |
| | | | | 19.7 | | | | 19.7 | | | | | | | | | | 618 | 40600 | 19.7 | MILE | RUMBLE STRIPS, (ASPHALT CONCRETE) | |
| | | | | 42.52 | | | | 39.64 | 0.56 | 1.3 | | 0.72 | | | 0.3 | | | 618 | 41000 | 42.52 | MILE | EDGE LINE, RUMBLE STRIPE (ASPHALT CONCRETE) | |
| | | | | 1.74 | | | | 1.74 | | | | | | | | | | 618 | 43000 | 1.74 | MILE | CENTER LINE, RUMBLE STRIPE (ASPHALT CONCRETE) | |
| | | | | 649,634 | | | | 619,424 | 5,012 | 13,232 | | 10,499 | | | 1,467 | | | 897 | 01010 | 649,634 | SY | PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (1.25") | |
| | | | | 3,252 | | | | 3,097 | 27 | 67 | | 53 | | | 8 | | | 897 | 02000 | 3,252 | SY | PATCHING PLANED SURFACE | |
| TRAFFIC CONTROL | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | 330 | | | | | | | | | | 620 | 70010 | 330 | FT | DELINEATOR, MISC.: REMOVAL & REERECTED | 11 |
| | | | | 2,540 | | | | 2,393 | 18 | 77 | | 48 | | | 4 | | | 621 | 00100 | 2,540 | EACH | RPM | |
| | | | | 2,528 | | | | 2,381 | 18 | 77 | | 48 | | | 4 | | | 621 | 54000 | 2,528 | EACH | RAISED PAVEMENT MARKER REMOVED | |
| | | | | 43.68 | | | | 40.95 | 0.56 | 1.3 | | 0.72 | | | 0.15 | | | 642 | 00094 | 43.68 | MILE | EDGE LINE, 6" | |
| | | | | 22.5 | | | | 20.48 | 0.93 | 0.65 | | 0.36 | | | 0.08 | | | 642 | 00300 | 22.5 | MILE | CENTER LINE, TYPE 1 | |
| | | | | 22.11 | | | | 22.11 | | | | | | | | | | 644 | 00104 | 22.11 | MILE | EDGE LINE, 6" | |
| | | | | 10.42 | | | | 10.42 | | | | | | | | | | 644 | 00204 | 10.42 | MILE | LANE LINE, 6" | |
| | | | | 5,587 | | | | 3,898 | | | | 1,689 | | | | | | 644 | 00404 | 5,587 | FT | CHANNELIZING LINE, 12" | |
| | | | | 1,178 | | | | 981 | | 15 | | 182 | | | | | | 644 | 00500 | 1,178 | FT | STOP LINE | |
| | | | | 577 | | | | 577 | | | | | | | | | | 644 | 00600 | 577 | FT | CROSSWALK LINE | |
| | | | | 2,653 | | | | 1,989 | | 98 | | 566 | | | | | | 644 | 00700 | 2,653 | FT | TRANSVERSE/DIAGONAL LINE | |
| | | | | 2 | | | | 2 | | | | | | | | | | 644 | 01110 | 2 | EACH | SCHOOL SYMBOL MARKING, 96" | |
| | | | | 1,111 | | | | 1,111 | | | | | | | | | | 644 | 01200 | 1,111 | FT | PARKING LOT STALL MARKING | |
| | | | | 28 | | | | 11 | | | | 17 | | | | | | 644 | 01300 | 28 | EACH | LANE ARROW | |
| | | | | 2 | | | | 2 | | | | | | | | | | SPECIAL | 64440000 | 2 | EACH | AIR SPEED ZONE MARKING | 11 |
| | | | | 3.27 | | | | 2.58 | | 0.69 | | | | | | | | 646 | 10010 | 3.27 | MILE | EDGE LINE, 6" | |
| | | | | 0.15 | | | | 0.05 | | 0.1 | | | | | | | | 646 | 10110 | 0.15 | MILE | LANE LINE, 6" | |
| | | | | 0.3 | | | | 0.17 | | 0.13 | | | | | | | | 646 | 10200 | 0.3 | MILE | CENTER LINE | |
| | | | | 2,100 | | | | 1,697 | | 403 | | | | | | | | 646 | 10310 | 2,100 | FT | CHANNELIZING LINE, 12" | |
| | | | | 219 | | | | 79 | | 140 | | | | | | | | 646 | 10400 | 219 | FT | STOP LINE | |
| | | | | 1,314 | | | | 1,260 | | 54 | | | | | | | | 646 | 10600 | 1,314 | FT | TRANSVERSE/DIAGONAL LINE | |
| | | | | 421 | | | | 421 | | | | | | | | | | 646 | 10800 | 421 | SF | ISLAND MARKING | |
| | | | | 10 | | | | 8 | | 2 | | | | | | | | 646 | 20300 | 10 | EACH | LANE ARROW | |
| | | | | 15 | | | | | | 15 | | | | | | | | 646 | 20800 | 15 | FT | YIELD LINE | |
| TRAFFIC SIGNALS | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | 8 | | | | | | 8 | | | | | | 632 | 26501 | 8 | EACH | DETECTOR LOOP, AS PER PLAN | 27 |
| STRUCTURE REPAIR (HUR-20-1774 SFN 3901394) | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | 153 | | | | 153 | | | | | | | 202 | 38602 | 153 | FT | BRIDGE RAILING REMOVED FOR REUSE | |
| | | | | | | | 29 | | | | 29 | | | | | | | 407 | 20000 | 29 | GAL | NON-TRACKING TACK COAT | |
| | | | | | | | 101 | | | | 101 | | | | | | | 409 | 30001 | 101 | FT | SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN | 29 |
| | | | | | | | 13 | | | | 13 | | | | | | | 424 | 12000 | 13 | CY | FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (1.25") | |
| | | | | | | | 153 | | | | 153 | | | | | | | 517 | 75600 | 153 | FT | DEEP BEAM BRIDGE RETROFIT RAILING | |
| | | | | | | | 360 | | | | 360 | | | | | | | 897 | 01010 | 360 | SY | PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (1.25") | |

GENERAL SUMMARY

**HUR / LOR-20-16.26 / 0.00
LOR-511-21.14**

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| SHEET NUM. | | | | | | | | PART. | | | | | | | | | ITEM | ITEM EXT | GRAND TOTAL | UNIT | DESCRIPTION | SEE SHEET NO. | |
|-------------------------------|----|-----|-----|----|--------|----|------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|------|----------|-------------|--------|-------------|--|----|
| 11 | 13 | 14 | 15 | 24 | 25 | 27 | 31 | 01/NHS/PV | 02/SC2/PV | 03/STR/PV | 04/NHS/BR | 05/NHS/PV | 06/NHS/PV | 07/NHS/PV | 08/NHS/PV | 09/NHS/BR | | | | | | | |
| MAINTENANCE OF TRAFFIC | | | | | | | | | | | | | | | | | | | | | | | |
| | | | 184 | | | | | 168 | | | | 16 | | | | | | 614 | 1110 | 184 | hour | LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE | |
| | | | 3 | | | | | 2 | | | 1 | | | | | | | 614 | 11500 | 3 | MNTH | WORKSITE TRAFFIC SUPERVISOR | |
| | | | LS | | | | 4 | LS | | | 4 | | | | | | | 614 | 12350 | 4 | EACH | WORK ZONE IMPACT ATTENUATOR | |
| | | 186 | | | | | | LS | | | | | | | | | | 614 | 12420 | LS | | DETOUR SIGNING | |
| | | | | | | | | 153 | 4 | 12 | | 8 | | | 9 | | | 614 | 12460 | 186 | EACH | WORK ZONE MARKING SIGN | |
| | | 80 | | | | | | 60 | 5 | 5 | | 5 | | | 5 | | | 614 | 13000 | 80 | CY | ASPHALT CONCRETE FOR MAINTAINING TRAFFIC | |
| | | | | | | | | 19 | | | 19 | | | | | | | 614 | 13310 | 19 | EACH | BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL) | |
| | | | | | | | | 14 | | | 14 | | | | | | | 614 | 13312 | 14 | EACH | BARRIER REFLECTOR, TYPE 2 (BIDIRECTIONAL) | |
| | | | | | | | | 12 | | | 12 | | | | | | | 614 | 13360 | 12 | EACH | OBJECT MARKER, TWO WAY | |
| | | | 8 | | | | | 8 | | | | | | | | | | 614 | 18601 | 8 | SNMT | PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN | 15 |
| | | 24 | | | | | | 24 | | | | | | | | | | 614 | 18700 | 24 | SNMT | DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY | |
| | | | | | 19.32 | | | 19.22 | | 0.1 | | | | | | | | 614 | 20560 | 19.32 | MILE | WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT | |
| | | | | | | | 0.19 | | | | 0.19 | | | | | | | 614 | 21200 | 0.19 | MILE | WORK ZONE CENTER LINE, CLASS I, 740.06, TYPE I | |
| | | | | | 67.94 | | | 63.44 | 0.84 | 2.35 | | 1.08 | | | 0.23 | | | 614 | 21550 | 67.94 | MILE | WORK ZONE CENTER LINE, CLASS III, 642 PAINT | |
| | | | | | | | 0.27 | | | | 0.27 | | | | | | | 614 | 22210 | 0.27 | MILE | WORK ZONE EDGE LINE, CLASS I, 6", 740.06, TYPE I | |
| | | | | | 42.78 | | | 42.78 | | | | | | | | | | 614 | 22360 | 42.78 | MILE | WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT | |
| | | | | | 10,566 | | | 8,474 | | 403 | | 1,689 | | | | | | 614 | 23680 | 10,566 | FT | WORK ZONE CHANNELIZING LINE, CLASS III, 8", 642 PAINT | |
| | | | | | 4,248 | | | 3,530 | | 152 | | 566 | | | | | | 614 | 25620 | 4,248 | FT | WORK ZONE TRANSVERSE/DIAGONAL LINE, CLASS III, 642 PAINT | |
| | | | | | | | 72 | | | | 72 | | | | | | | 614 | 26400 | 72 | FT | WORK ZONE STOP LINE, CLASS I, 740.06, TYPE I | |
| | | | | | 1,528 | | | 1,191 | | 155 | | 182 | | | | | | 614 | 26610 | 1,528 | FT | WORK ZONE STOP LINE, CLASS III, 642 PAINT | |
| | | | | | 577 | | | 577 | | | | | | | | | | 614 | 27620 | 577 | FT | WORK ZONE CROSSWALK LINE, CLASS III, 642 PAINT | |
| | | | | | 2 | | | 2 | | | | | | | | | | 614 | 31750 | 2 | EACH | WORK ZONE SCHOOL SYMBOL MARKING, 96", CLASS III, 642 PAINT | |
| | | | | | | | 183 | | | | 183 | | | | | | | 615 | 20000 | 183 | SY | PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A | |
| | | | | | | | 320 | | | | 320 | | | | | | | 622 | 41000 | 320 | FT | PORTABLE BARRIER, 32" | |
| | | | | | | | 100 | | | | 100 | | | | | | | 622 | 41020 | 100 | FT | PORTABLE BARRIER, 32", BRIDGE MOUNTED (UNANCHORED) | |
| INCIDENTALS | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | LS | LS | LS | LS | LS | LS | LS | LS | LS | | 614 | 11000 | LS | | MAINTAINING TRAFFIC | |
| | | | | | | | | 7 | 1 | 1 | 3 | | | | | | | 619 | 16010 | 12 | MNTH | FIELD OFFICE, TYPE B | |
| | | | | | | | | LS | LS | LS | LS | LS | LS | LS | LS | LS | | 623 | 10000 | LS | | CONSTRUCTION LAYOUT STAKES AND SURVEYING | |
| | | | | | | | | LS | LS | LS | LS | LS | LS | LS | LS | LS | | 624 | 10000 | LS | | MOBILIZATION | |

GENERAL SUMMARY

**HUR / LOR-20-16.26 / 0.00
LOR-511-21.14**

I:\ProjectData\87698\Design\Roadway\Sheets\87698_GC00.dgn

I:\ProjectData\87698\Design\Roadway\Sheets\87698_G0001.dgn

| PLAN SPLIT | COUNTY | ROUTE | LOG POINT | | LENGTH | | AVERAGE WIDTH FT | # TYPICAL NUMBER (SEE SHEETS 7-10 FOR TYPICALS) | PAVEMENT AREA SY | 407 | 409 | 424 | 424 | 897 | 897 | 618 | 618 | 618 | AGGREGATE SHOULDER PROPOSED WIDTH | | AGGREGATE SHOULDER AREA | 209 | 617 | 408 | CALCULATED ACM CHECKED NRF | |
|---|---------------------------------------|-------|-----------------------|-------|--------|---|---------------------|---|---------------------|---|--|--|---|----------------------------|---|---|------------------------------------|-------|--------------------------------------|-------|-------------------------|------------------------|--|--------|-------------------------------------|--|
| | | | TO | MILE | FEET | NON-TRACKING TACK COAT @ 0.08 GAL/SY | | | | SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN | FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B | FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B | PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A | PATCHING PLANED SURFACE | EDGE LINE, RUMBLE STRIPES, ASPHALT CONCRETE | CENTER LINE, RUMBLE STRIPES, ASPHALT CONCRETE | RUMBLE STRIPS, ASPHALT CONCRETE | SL | SR | SY | MILE | COMPACTED AGGREGATE | PRIME COAT, AS PER PLAN @ 0.40 GAL/SY | | | |
| | | | LOG POINT | | | | | | | | | | | | | | | | | | | | 2 | | | |
| | | | STRAIGHT LINE MILEAGE | | | | | | | | | | | | | | | | | | | | | INCHES | | |
| 01/NHS/PV | LOR | 20 | 8.56 | 8.59 | 0.03 | 158.4 | 74.00 | 9 | 1,302 | 104 | | | | 1302 | 7 | | | 0.06 | 2.0 | 2.0 | 70 | 0.06 | 4 | 28 | | |
| 01/NHS/PV | LOR | 20 | 8.59 | 10.56 | 1.97 | 10401.6 | 72.00 | 9 | 83,213 | 6657 | 72 | 2889 | | 83213 | 416 | | | 7.88 | 2.0 | 2.0 | 9246 | 7.88 | 514 | 3698 | | |
| STRUCTURE LOR-20-1056L&R PAVEMENT DATA INCLUDED IN STRUCTURE QUANTITIES | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 01/NHS/PV | LOR | 20 | 10.58 | 10.59 | 0.01 | 52.8 | 72.00 | 9 | 422 | 34 | 72 | 15 | | 422 | 2 | | | 0.04 | 2.0 | 2.0 | 47 | 0.04 | 3 | 19 | | |
| 01/NHS/PV | LOR | 20 | 10.59 | 12.08 | 1.49 | 7867.2 | 72.00 | 9 | 62,938 | 5035 | 72 | 2185 | | 62938 | 315 | | | 5.96 | 2.0 | 2.0 | 6993 | 5.96 | 389 | 2797 | | |
| STRUCTURE LOR-20-1208L&R PAVEMENT DATA INCLUDED IN STRUCTURE QUANTITIES | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 01/NHS/PV | LOR | 20 | 12.10 | 12.13 | 0.03 | 158.4 | 80.00 | 9 | 1,408 | 113 | 72 | 49 | | 1408 | 7 | | | 0.12 | 2.0 | 2.0 | 141 | 0.12 | 8 | 56 | | |
| 01/NHS/PV | LOR | 20 | 12.13 | 12.59 | 0.46 | 2428.8 | 72.00 | 9 | 19,430 | 1554 | | 675 | | 19430 | 97 | | | 1.84 | 2.0 | 2.0 | 2159 | 1.84 | 120 | 864 | | |
| 01/NHS/PV | LOR | 20 | 12.59 | 13.49 | 0.90 | 4752 | 72.00 | 9 | 38,016 | 3041 | | 1320 | | 38016 | 190 | | | 3.60 | 2.0 | 2.0 | 4224 | 3.60 | 235 | 1690 | | |
| 01/NHS/PV | LOR | 20 | 13.49 | 13.54 | 0.05 | 264 | 72.00 | 9 | 2,112 | 169 | | 73 | | 2112 | 11 | | | 0.20 | 2.0 | 2.0 | 235 | 0.20 | 13 | 94 | | |
| 02/S<2/PV | LOR | 511 | 21.14 | 21.42 | 0.28 | 1478.4 | 30.00 | 1 | 4,928 | 394 | | 171 | 7 | 4928 | 25 | 0.56 | | | 2.0 | 2.0 | 657 | 0.56 | 37 | 263 | | |
| 03/STR/PV | LOR | 511 | 21.42 | 22.07 | 0.65 | 3432 | 30.00 | 1 | 11,440 | 915 | | 397 | 17 | 11440 | 57 | 1.30 | | | 2.0 | 2.0 | 1525 | 1.30 | 85 | 610 | | |
| 03/STR/PV | LOR-511-22.07-22.17 SHOULDERS | | | | | | | | 564 | 45 | | 20 | | 564 | 3 | | | | 2.0 | 2.0 | 91 | 0.16 | 5 | 36 | | |
| LOR-511-22.17 TO 22.63 INCLUDED IN SR 511 RAMP CALCULATIONS | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 01/NHS/PV | SR 511 - RAMP A SHOULDERS | | | | | | | | 815 | 65 | | 28 | | 815 | 4 | | | | 2.0 | 2.0 | 324 | 0.28 | 18 | 130 | | |
| 01/NHS/PV | SR 511 - RAMP B SHOULDERS | | | | | | | | 820 | 66 | | 28 | | 820 | 4 | | | | 2.0 | 2.0 | 320 | 0.27 | 18 | 128 | | |
| 01/NHS/PV | SR 511 - RAMP C SHOULDERS | | | | | | | | 1,425 | 114 | | 49 | | 1425 | 7 | | | | 2.0 | 2.0 | 558 | 0.48 | 31 | 223 | | |
| 01/NHS/PV | SR 511 - RAMP D SHOULDERS | | | | | | | | 1,220 | 98 | | 42 | | 1220 | 6 | | | | 2.0 | 2.0 | 438 | 0.37 | 24 | 175 | | |
| 01/NHS/PV | SR 511 - RAMP ACCEL/DECEL LANES | | | | | | | | 7,041 | 563 | | 244 | | 7041 | 35 | | | | | | | | | | | |
| 01/NHS/PV | SR 301 - RAMP A FULL WIDTH | | | | | | | | 1,307 | 105 | | 45 | | 1307 | 7 | | | | 2.0 | 2.0 | 218 | 0.19 | 12 | 87 | | |
| 01/NHS/PV | SR 301 - RAMP B FULL WIDTH | | | | | | | | 1,080 | 86 | | 38 | | 1080 | 5 | | | | 2.0 | 2.0 | 180 | 0.15 | 10 | 72 | | |
| 01/NHS/PV | SR 301 - RAMP C FULL WIDTH | | | | | | | | 1,320 | 106 | | 46 | | 1320 | 7 | | | | 2.0 | 2.0 | 220 | 0.19 | 12 | 88 | | |
| 01/NHS/PV | SR 301 - RAMP D FULL WIDTH | | | | | | | | 1,040 | 83 | | 36 | | 1040 | 5 | | | | 2.0 | 2.0 | 173 | 0.15 | 10 | 69 | | |
| 01/NHS/PV | SR 301 - RAMP ACCEL/DECEL LANES | | | | | | | | 7,125 | 570 | | 247 | | 7125 | 36 | | | | | | | | | | | |
| 01/NHS/PV | LOR-20 EXTRA AREA FOR U-TURN MEDIANS | | | | | | | | 1,651 | 132 | | 57 | | 1651 | 8 | | | | 2.0 | 2.0 | 40 | 0.03 | 2 | 16 | | |
| 03/STR/PV | OBERLIN-ELYRIA RD EASTBOUND SHOULDERS | | | | | | | | 513 | 41 | | 18 | | 513 | 3 | | | | 2.0 | 2.0 | 171 | 0.29 | 10 | 68 | | |
| 03/STR/PV | OBERLIN-ELYRIA RD WESTBOUND SHOULDERS | | | | | | | | 358 | 29 | | 12 | | 358 | 2 | | | | 2.0 | 2.0 | 104 | 0.18 | 6 | 42 | | |
| 01/NHS/PV | EXTRA AREA FOR INTERSECTIONS | | | | | | | | 6,436 | 515 | | 223 | | 6436 | 32 | | | | | | | | | | | |
| 01/NHS/PV | EXTRA AREA FOR PAVED DRIVES | | | | | | | | 1,188 | 95 | | 41 | | 1188 | 6 | | | | | | | | | | | |
| 01/NHS/PV | EXTRA AREA FOR AGGREGATE DRIVES | | | | | | | | 3,033 | | | | | | | | | | | | | | | 169 | | |
| 01/NHS/PV | EXTRA AREA FOR EX. MAILBOX APPROACHES | | | | | | | | 1,410 | 113 | | 49 | | 1410 | 7 | | | | | | | | | | | |
| 02/S<2/PV | EXTRA AREA FOR PAVED DRIVES | | | | | | | | 54 | 4 | | 2 | | 54 | 1 | | | | | | | | | | | |
| 02/S<2/PV | EXTRA AREA FOR AGGREGATE DRIVES | | | | | | | | 144 | | | | | | | | | | | | | | | | 8 | |
| 02/S<2/PV | EXTRA AREA FOR EX. MAILBOX APPROACHES | | | | | | | | 30 | 2 | | 1 | | 30 | 1 | | | | | | | | | | | |
| 03/STR/PV | EXTRA AREA FOR INTERSECTIONS | | | | | | | | 76 | 6 | | 3 | | 76 | 1 | | | | | | | | | | | |
| 03/STR/PV | EXTRA AREA FOR PAVED DRIVES | | | | | | | | 171 | 14 | | 6 | | 171 | 1 | | | | | | | | | | | |
| 03/STR/PV | EXTRA AREA FOR AGGREGATE DRIVES | | | | | | | | 171 | | | | | | | | | | | | | | | | 10 | |
| 03/STR/PV | EXTRA AREA FOR EX. MAILBOX APPROACHES | | | | | | | | 110 | 9 | | 4 | | 110 | 1 | | | | | | | | | | | |
| 05/NHS/PV | EXTRA AREA FOR PAVED DRIVES | | | | | | | | 9 | 1 | | 0 | | 9 | 1 | | | | | | | | | | | |
| 05/NHS/PV | EXTRA AREA FOR AGGREGATE DRIVES | | | | | | | | 2 | | | | | | | | | | | | | | | | 1 | |
| 07/NHS/PV | EXTRA AREA FOR INTERSECTIONS | | | | | | | | 217 | 17 | | 8 | | 217 | 1 | | | | | | | | | | | |
| 07/NHS/PV | EXTRA AREA FOR PAVED DRIVES | | | | | | | | 54 | 4 | | 2 | | 54 | 1 | | | | | | | | | | | |
| 07/NHS/PV | EXTRA AREA FOR AGGREGATE DRIVES | | | | | | | | 54 | | | | | | | | | | | | | | | | 3 | |
| 07/NHS/PV | EXTRA AREA FOR EX. MAILBOX APPROACHES | | | | | | | | 70 | 6 | | 2 | | 70 | 1 | | | | | | | | | | | |
| 01/NHS/PV | DEDUCT AREA FOR CONCRETE BRIDGE DECKS | | | | | | | | -1,935 | -155 | | -67 | | -1935 | -10 | | | | | | | | | | | |
| TOTAL FOR PLAN SPLIT (01/NHS/PV) | | | | | | | | | 622,457 | 49,554 | 288 | 21,508 | 532 | 619,424 | 3,097 | 39.64 | 1.74 | 19.70 | 72,742 | 62.00 | 4,210 | 29,097 | | | | |
| TOTAL FOR PLAN SPLIT (02/S<2/PV) | | | | | | | | | 5,156 | 401 | | 174 | 7 | 5,012 | 27 | 0.56 | | 657 | 0.56 | 45 | 263 | | | | | |
| TOTAL FOR PLAN SPLIT (03/STR/PV) | | | | | | | | | 13,403 | 1,059 | | 459 | 17 | 13,232 | 67 | 1.30 | | 1,892 | 1.93 | 115 | 757 | | | | | |
| TOTAL FOR PLAN SPLIT (05/NHS/PV) | | | | | | | | | 10,501 | 840 | | 365 | 10 | 10,499 | 53 | 0.72 | | 845 | 0.72 | 48 | 338 | | | | | |
| TOTAL FOR PLAN SPLIT (07/NHS/PV) | | | | | | | | | 1,521 | 117 | | 51 | 4 | 1,467 | 8 | 0.30 | | 352 | 0.30 | 23 | 141 | | | | | |
| TOTALS CARRIED TO GENERAL SUMMARY | | | | | | | | | 653,038 | 51,971 | 288 | 22,557 | 570 | 649,634 | 3,252 | 42.52 | 1.74 | 19.70 | 76,488 | 65.51 | 4,441 | 30,596 | | | | |

PAVEMENT AND SHOULDER DATA

HUR / LOR-20-16.26 / 0.00
LOR-511-21.14

AUXILIARY & LONG LINE MARKINGS

Table with columns for PARTICIPATION, COUNTY, ROUTE, STATION / SLM, HIGHWAY MILES, and various marking categories (614, 646, 642, 644) including quantities and costs.

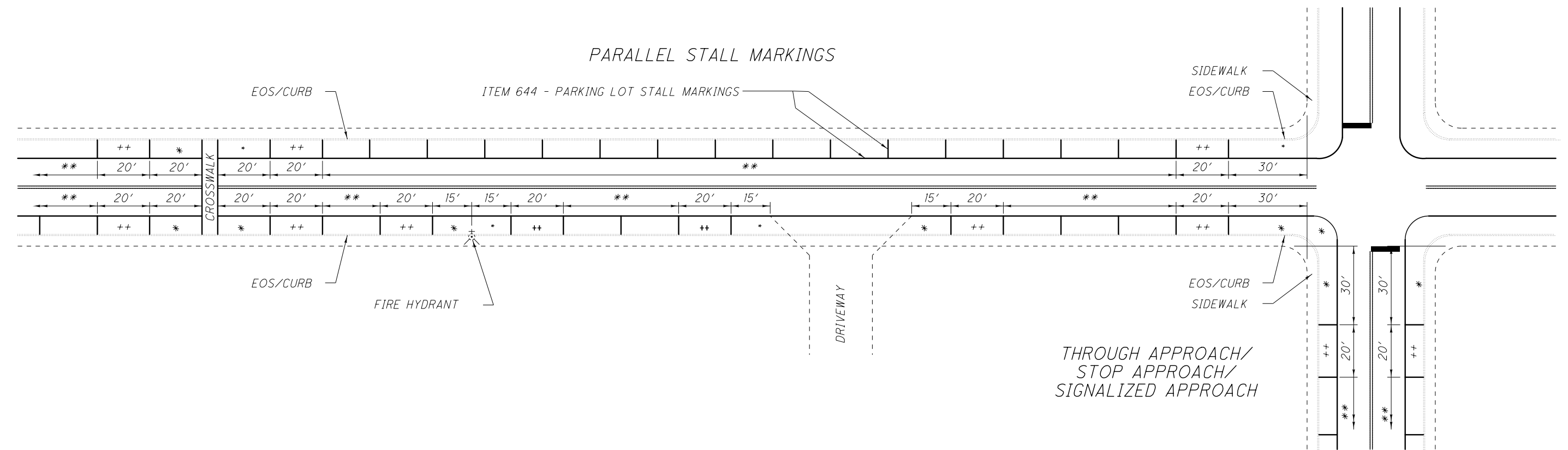
RAISED PAVEMENT MARKERS

Table with columns for PARTICIPATION, COUNTY, ROUTE, STATION/SLM, DETAIL, RAISED PAVEMENT MARKER REMOVED, ONE-WAY, TWO-WAY, and REMARKS. Includes a description of marker types and removal counts.

Vertical text on the left margin: I:\ProjectData\87698\Design\Traffic\Sheets\87698_T5001.dgn

PARALLEL STALL MARKINGS

ITEM 644 - PARKING LOT STALL MARKINGS



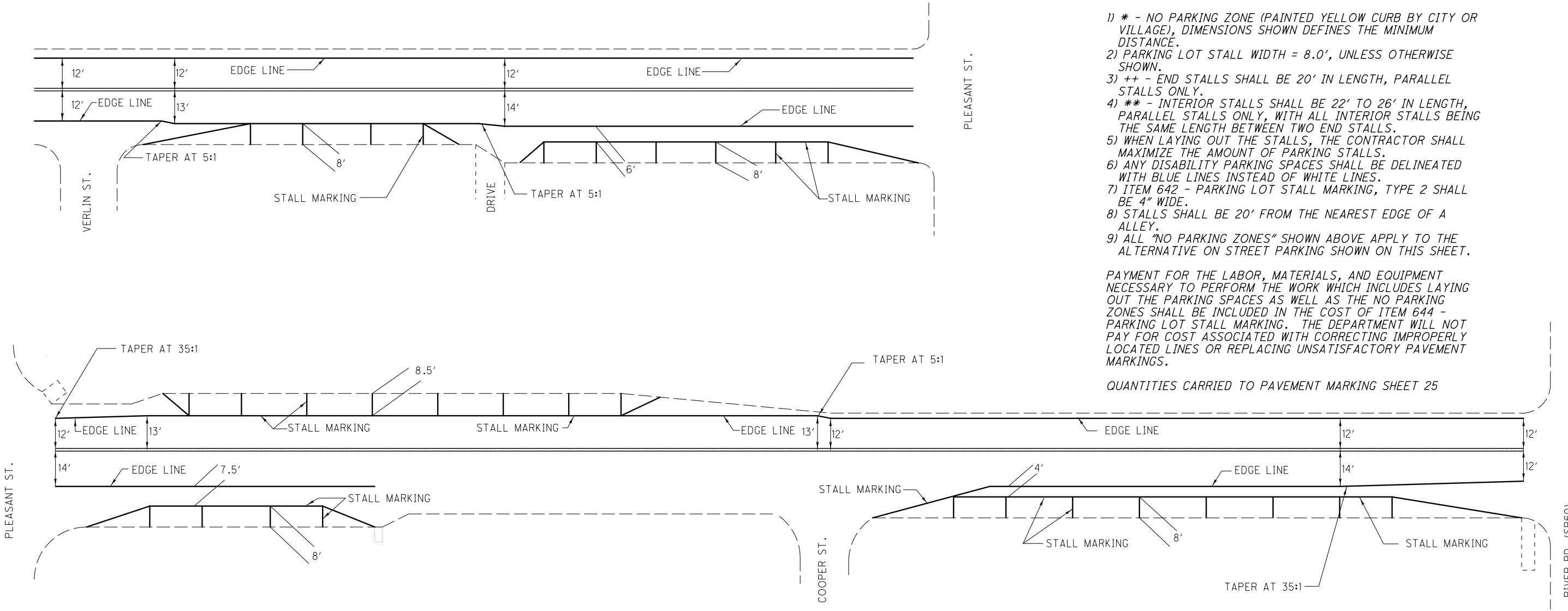
THROUGH APPROACH/
STOP APPROACH/
SIGNALIZED APPROACH

NOTES

- 1) * - NO PARKING ZONE (PAINTED YELLOW CURB BY CITY OR VILLAGE), DIMENSIONS SHOWN DEFINES THE MINIMUM DISTANCE.
- 2) PARKING LOT STALL WIDTH = 8.0', UNLESS OTHERWISE SHOWN.
- 3) ++ - END STALLS SHALL BE 20' IN LENGTH, PARALLEL STALLS ONLY.
- 4) ** - INTERIOR STALLS SHALL BE 22' TO 26' IN LENGTH, PARALLEL STALLS ONLY, WITH ALL INTERIOR STALLS BEING THE SAME LENGTH BETWEEN TWO END STALLS.
- 5) WHEN LAYING OUT THE STALLS, THE CONTRACTOR SHALL MAXIMIZE THE AMOUNT OF PARKING STALLS.
- 6) ANY DISABILITY PARKING SPACES SHALL BE DELINEATED WITH BLUE LINES INSTEAD OF WHITE LINES.
- 7) ITEM 642 - PARKING LOT STALL MARKING, TYPE 2 SHALL BE 4" WIDE.
- 8) STALLS SHALL BE 20' FROM THE NEAREST EDGE OF A ALLEY.
- 9) ALL "NO PARKING ZONES" SHOWN ABOVE APPLY TO THE ALTERNATIVE ON STREET PARKING SHOWN ON THIS SHEET.

PAYMENT FOR THE LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO PERFORM THE WORK WHICH INCLUDES LAYING OUT THE PARKING SPACES AS WELL AS THE NO PARKING ZONES SHALL BE INCLUDED IN THE COST OF ITEM 644 - PARKING LOT STALL MARKING. THE DEPARTMENT WILL NOT PAY FOR COST ASSOCIATED WITH CORRECTING IMPROPERLY LOCATED LINES OR REPLACING UNSATISFACTORY PAVEMENT MARKINGS.

QUANTITIES CARRIED TO PAVEMENT MARKING SHEET 25



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ITEM 632 - DETECTOR LOOP, AS PER PLAN

AN ESTIMATED QUANTITY OF ITEM 632, DETECTOR LOOP, AS PER PLAN, HAS BEEN PROVIDED FOR THE PURPOSE OF REPLACING DAMAGED DETECTOR LOOPS AND/OR UPGRADING DETECTOR LOOPS TO IMPROVE MOTORCYCLE DETECTION. IT IS IMPERATIVE THAT REPLACEMENT OF DETECTOR LOOPS BE INSTALLED AND FULLY FUNCTIONAL IN THE SHORTEST POSSIBLE TIME. THE CONTRACTOR SHALL HAVE REPLACEMENT DETECTOR LOOPS INSTALLED AND FULLY FUNCTIONAL WITHIN 7 CALENDAR DAYS OF DESTRUCTION OF THE EXISTING DETECTOR LOOPS.

THE CONTRACTOR SHALL NOTIFY MATT BLANKENSHIP, ODOT DISTRICT 3 ROADWAY SERVICES MANAGER, (PHONE 419-207-7045) 5 WORKING DAYS IN ADVANCE OF ANY PLANING OPERATIONS OR PAVEMENT REPAIR WORK. THIS NOTIFICATION IS NEEDED FOR DISTRICT 3 TO SCHEDULE TEMPORARY SIGNAL TIMING MODIFICATIONS FOR THE TIME PERIOD WHEN THE DETECTOR LOOPS ARE OUT OF OPERATION. THE CONTRACTOR SHALL THEN RENOTIFY MR. BLANKENSHIP WITHIN 2 WORKING DAYS AFTER THE NEW DETECTOR LOOPS ARE REPLACED SO THAT HE CAN RESCHEDULE DISTRICT CREWS TO RESTORE SIGNAL TIMINGS TO THE ORIGINAL SETTINGS. IN ADDITION, THE CONTRACTOR SHALL ALSO NOTIFY CRAIG DEVORE, ODOT DISTRICT 3 PLANNING AND ENGINEERING DEPT. (PHONE 419-207-7169) WHEN THE NEW LOOPS ARE INSTALLED.

FAILURE TO COMPLY WITH THE ABOVE STATED REQUIREMENTS WILL RESULT IN THE ASSESSMENT OF A DISINCENTIVE FEE OF \$500.00 PER DAY TO THE CONTRACTOR FOR EACH CALENDAR DAY BEYOND THE SPECIFIED LIMIT.

THE NEW DETECTOR LOOPS SHALL BE PLACED PER THE PLAN DETAILS AFTER THE PLANING AND PAVEMENT REPAIR OPERATIONS ARE COMPLETED WITHIN THE AFFECTED AREAS. THE DETECTOR LOOPS SHALL NOT BE CUT INTO THE SURFACE COURSE.

IN ADDITION TO THE REQUIREMENTS OF CMS 632.11, THE CONTRACTOR SHALL PROVIDE A POSITIVE AND EFFECTIVE MEANS FOR REMOVAL OF SOLID RESIDUE RESULTING FROM THE DRY SAW BLADE CUTTING OF LOOP DETECTOR SLOTS IN THE PAVEMENT. THE RESIDUE SHALL BE REMOVED BY VACUUM OR OTHER EFFECTIVE MEANS, BEFORE IT IS BLOWN BY TRAFFIC ACTION OR WIND. RESIDUE FROM DRY CUTTING SHALL NOT BE REMOVED BY COMPRESSED AIR. AS AN ALTERNATE, THE CONTRACTOR MAY USE WET CUTTING.

LOOP DETECTOR WIRE TO LEAD-IN CABLE SPLICES WITHIN EPOXY ENCAPSULATED SPLICE ENCLOSURES SHALL BE JOINED BY AN APPROVED CONNECTOR AND SOLDERED PER CMS 632.23 & 725.15. ALL COSTS ASSOCIATED WITH THE SOLDERED SPLICE CONNECTION AND EPOXY SPLICE KIT SHALL BE INCLUDED WITH THE DETECTOR LOOP.

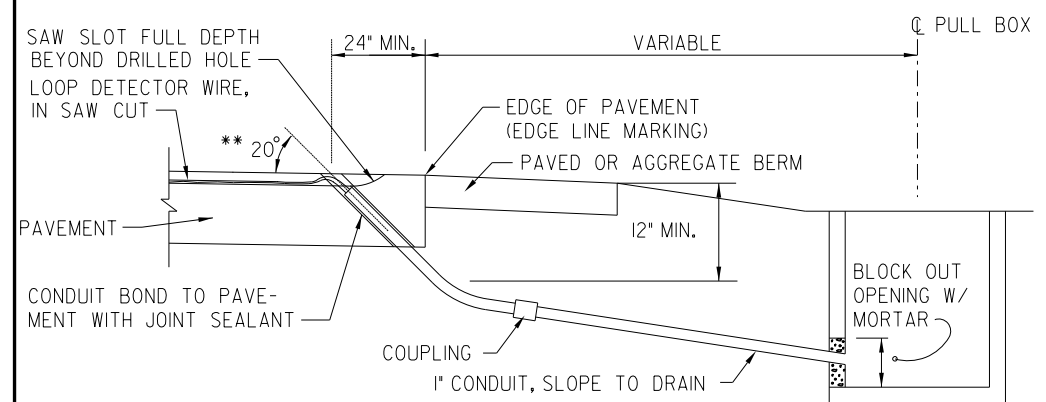
IF THE PULL BOX IS NOT SPECIFIED IN THE PLANS, THE SPLICE SHALL BE MADE IN THE FIRST ENTERED POLE OR PEDESTAL, EXCEPT WHERE THE CONTROLLER CABINET IS MOUNTED ON THE POLE OR PEDESTAL, IN WHICH CASE THE LOOP WIRES SHALL BE ROUTED DIRECTLY INTO THE CABINET UNLESS SPECIFIED DIFFERENTLY IN THE PLANS. LOOP DETECTOR WIRE ROUTED THROUGH CONDUIT, PULL BOXES, POLES, AND PEDESTALS SHALL BE TWISTED PER CMS 632.23.

FURNISH ALL MATERIALS ACCORDING TO THE DEPARTMENT'S QUALIFIED PRODUCTS LIST (QPL).

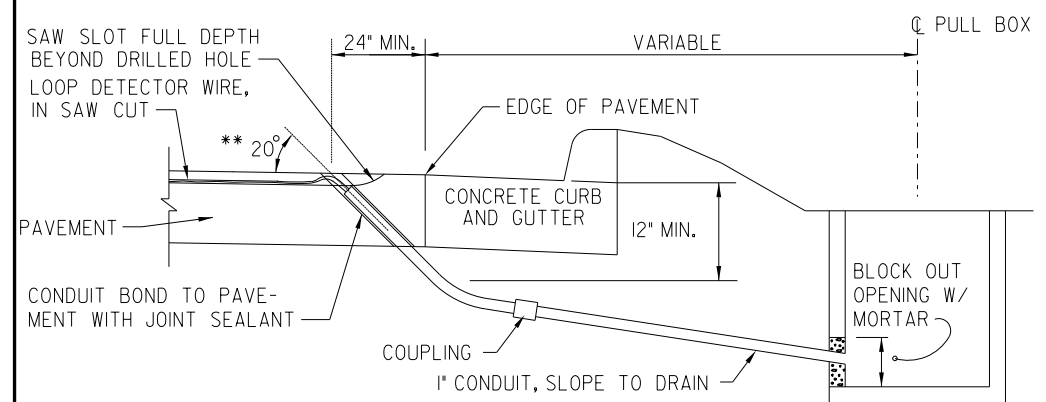
SEE DETAILS ON THIS SHEET FOR ADDITIONAL REQUIREMENTS.

PAYMENT FOR ALL OF THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH FOR ITEM 632, DETECTOR LOOP, AS PER PLAN.

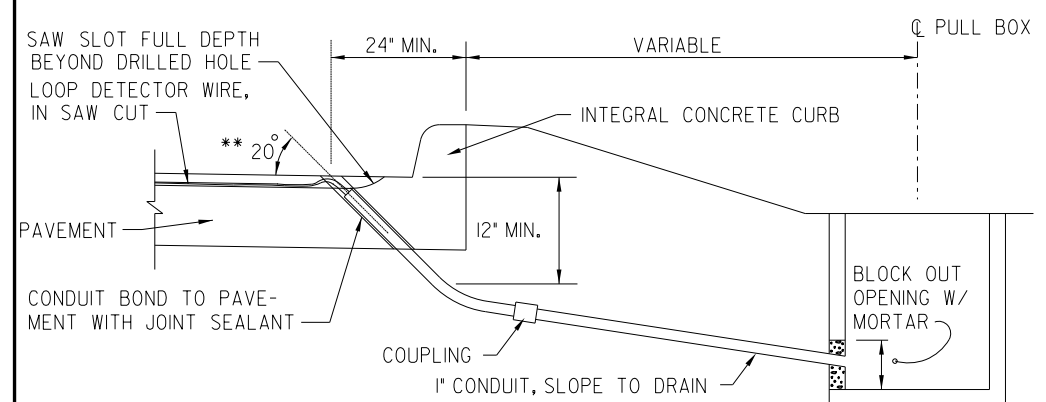
(05/NHS/PV)
ITEM 632 - DETECTOR LOOP, AS PER PLAN 8 EACH



DRILLED HOLE LOCATION DETAIL WITH PAVED OR AGGREGATE BERM



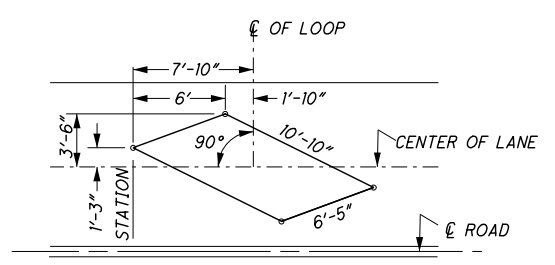
DRILLED HOLE LOCATION DETAIL WITH CONCRETE CURB AND GUTTER



DRILLED HOLE LOCATION DETAIL WITH INTEGRAL CONCRETE CURB

* CONDUIT SHALL BE 1" DIAMETER 725.04.
* THE RANGE OF THIS ANGLE SHALL BE FROM 15 TO 30 DEGREES.

NOTE: SEE STANDARD DRAWING TC-82.10 FOR ADDITIONAL NOTES AND DETAILS



ANGULAR DESIGN DETECTION (ADD) LOOP DETAIL FOR LANE WIDTH 11' & LARGER

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DETECTOR LOOP REPLACEMENT

NOTE:
THIS SHEET IS FOR INFORMATIONAL PURPOSES ONLY. IT IS INTENDED TO BE REPRESENTATIVE OF LOCATIONS AND QUANTITIES OF THE POSSIBLY AFFECTED EXISTING DETECTOR LOOPS ONLY. IT IS NOT INDICATIVE OF PLACEMENT, QUANTITY, OR LOCATION OF ANY OTHER ITEMS.

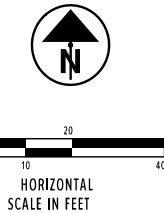
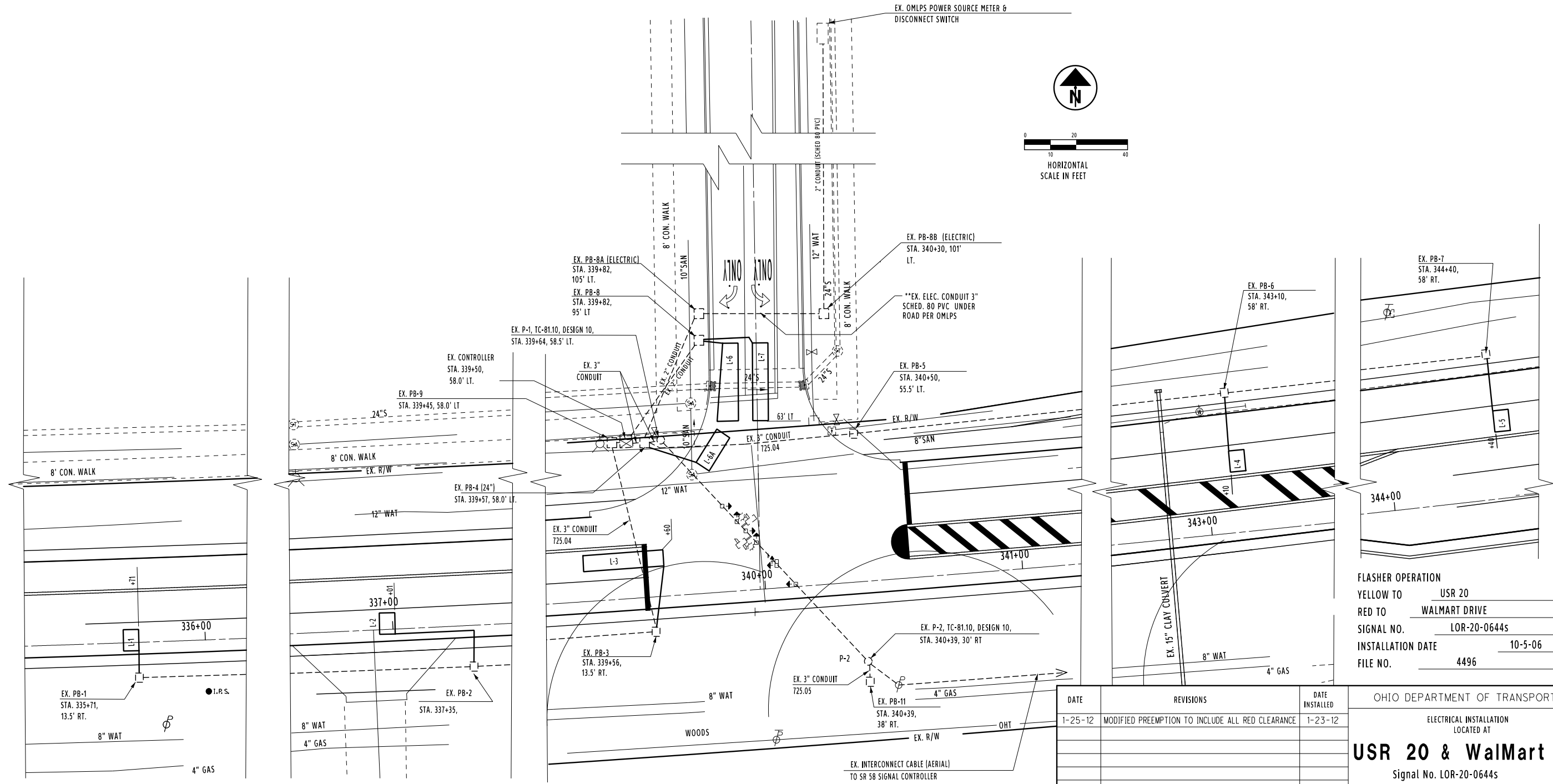
REPLACE LOOPS L-1, L-2, L-3, L-4, L-5, L-6, L-6A, L-7

ITEM 632 - DETECTOR LOOP, AS PER PLAN..... 8 EACH 05/NHS/PV

QUANTITY CARRIED TO THE DETECTOR LOOP GENERAL NOTES SHEET.

LOOP DETECTOR CHART

| ZONE NO. | SIZE (FT.) | MODE | DELAY (SEC.) | CAMERA | ASSOCIATED CONTROLLER PHASE | COMMENTS |
|----------|------------|----------|--------------|--------|-----------------------------|------------------|
| L-1 | 6X8 | PULSE | | | 2 | REPLACE EX. LOOP |
| L-2 | 6X8 | PULSE | | | 2 | |
| L-3 | 6X30 | PRESENCE | 3* | | 5 | |
| L-4 | 6X8 | PULSE | | | 6 | |
| L-5 | 6X8 | PULSE | | | 6 | |
| L-6 | 6X8X30 | PRESENCE | 10* | | 4 | |
| L-6A | 6X15 | PRESENCE | 10* | | 4 | |
| L-7 | 6X30 | PRESENCE | 3 | | 4 | REPLACE EX. LOOP |



FLASHER OPERATION
 YELLOW TO USR 20
 RED TO WALMART DRIVE
 SIGNAL NO. LOR-20-0644s
 INSTALLATION DATE 10-5-06
 FILE NO. 4496

| DATE | REVISIONS | DATE INSTALLED |
|---------|--|----------------|
| 1-25-12 | MODIFIED PREEMPTION TO INCLUDE ALL RED CLEARANCE | 1-23-12 |
| | | |
| | | |
| | | |
| | | |
| | | |

OHIO DEPARTMENT OF TRANSPORTATION
 ELECTRICAL INSTALLATION
 LOCATED AT
USR 20 & Walmart Drive
 Signal No. LOR-20-0644s
 DISTRICT 3 COUNTY LORAIN

DRAWN
RJR
10/06

REVIEWED
28
38

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LOOP DETECTOR DETAILS
 LOR US 20 & WALMART DRIVE
 HUR / LOR-20-16.26 / 0.00
 LOR-511-21.14

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURES HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURES AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURES AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05 AND 105.02.

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURES. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS WHICH HAVE BEEN VERIFIED IN THE FIELD.

EXISTING PLANS

THE FOLLOWING EXISTING PLANS MAY BE INSPECTED AT THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT THREE OFFICE LOCATED AT 906 CLARK AVENUE, ASHLAND, OHIO 44805.

| STRUCTURE NUMBER: | EXISTING PLAN NAME: | DATE: |
|-------------------|-----------------------|-------|
| HUR-20-1774 | HUR/LOR-20-16.35/0.00 | 2008 |
| HUR-20-2226 | HUR/LOR-20-16.35/0.00 | 2008 |
| HUR-20-2283 | HUR/LOR-20-16.35/0.00 | 2008 |
| HUR-20-2449 | HUR/LOR-20-16.35/0.00 | 2008 |
| LOR-20-0216 | HUR/LOR-20-16.35/0.00 | 2008 |
| LOR-20-0868 | LOR-20-8.56 | 2012 |
| LOR-20-1056L&R | LOR-20-8.56 | 2012 |
| LOR-20-1208L&R | LOR-20-8.56 | 2012 |
| LOR-20-1354L&R | D03-BH-FY2009(B) | 2008 |

DESIGN DATA

-CONCRETE CLASS QC2 - COMPRESSIVE STRENGTH 4,500 PSI
-REINFORCED STEEL - ASTM A615 OR A996, GRADE 60, MINIMUM YIELD STRENGTH 60,000 PSI.

DESIGN SPECIFICATIONS

DESIGN SPECIFICATIONS: THIS STRUCTURE CONFORMS TO "STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 2002, INCLUDING THE 2003-2007 INTERIM SPECIFICATIONS AND THE ODOT BRIDGE DESIGN MANUAL.

DECK PROTECTION METHOD

ASPHALT CONCRETE DECK OVERLAY (STRUCTURES HUR-20-1774, LOR-20-0216, LOR-20-1056L&R, LOR-20-1208L&R). SEE PAVEMENT AND SHOULDER DATA SHEET FOR QUANTITIES.

NO DECK PROTECTION METHOD ON ALL OTHER STRUCTURES

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

IN-STREAM WORK RESTRICTION

THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO AVOID CONSTRUCTION IN AND/OR LIMIT DEMOLITION DEBRIS FROM ENTERING STREAMS OR WETLANDS. ANY MATERIAL THAT DOES FALL INTO STREAMS OR WETLANDS SHALL BE REMOVED AS SOON AS POSSIBLE.

ALL PROJECTS INVOLVING JURISDICTIONAL WATERS OF THE UNITED STATES STREAMS, RIVERS, NON-ISOLATED WETLANDS) AND/OR ISOLATED WETLANDS ARE SUBJECT TO REGULATION UNDER SECTIONS 404 AND 401 OF THE CLEAN WATER ACT, AND POSSIBLY OHIO EPA ISOLATED WETLAND LAW. IT IS ANTICIPATED THAT NO IN-STREAM WORK, OR WORK UNDER THE STREAM'S ORDINARY HIGH WATER MARK (OHWM) WILL BE NEEDED. THEREFORE NO WATERWAY PERMITS HAVE BEEN GRANTED AND NO IN-STREAM WORK IS ALLOWED.

SHOULD WORK (EITHER TEMPORARY OR PERMANENT) IN THE STREAM BE NEEDED; IT WILL REQUIRE A PERMIT AND AUTHORIZATION BY THE UNITED STATES ARMY CORPS OF ENGINEERS (USACE). THE CONTRACTOR SHALL NOT UTILIZE FILLS BELOW OHWM UNTIL SUCH ACTIVITY IS AUTHORIZED BY THE USACE. DETAILS OF THIS REQUIREMENT ARE DESCRIBED IN ODOT'S SUPPLEMENTAL SPECIFICATION 832.09.

USACE DEFINITION OF OHWM - THE ORDINARY HIGH WATER MARK IS THE LINE ON THE SHORES ESTABLISHED BY THE FLUCTUATIONS OF WATER AND INDICATED BY PHYSICAL CHARACTERISTICS SUCH AS A CLEAR, NATURAL LINE IMPRESSED ON THE BANK; SHELVEING; CHANGES IN THE CHARACTER OF THE SOIL; DESTRUCTION OF TERRESTRIAL VEGETATION; THE PRESENCE OF LITTER AND DEBRIS; OR THE APPROPRIATE MEANS THAT CONSIDER THE CHARACTERISTICS OF THE SURROUNDING AREAS.

PLACING ASPHALT CONCRETE FEATHERING ON APPROACHES TO BRIDGES

SPECIAL CARE SHALL BE TAKEN WHEN PLACING THE ASPHALT CONCRETE BUTT JOINT TO CREATE A SMOOTH TRANSITION FROM THE EXISTING APPROACH PAVEMENT TO THE BRIDGE DECK OR APPROACH SLAB. THE CONTRACTOR'S ATTENTION IS CALLED TO STANDARD DRAWING BP-3.1 FOR REQUIRED TOLERANCES.

PAVING AT STRUCTURES

STRUCTURES HUR-20-2226, HUR-20-2283, HUR-20-2449, HUR-20-2562, LOR-20-1354L&R:
SUSPEND AND RESUME AT CONCRETE BRIDGE DECKS AND APPROACH SLABS.

STRUCTURES LOR-20-1208L&R:
SUSPEND AND RESUME AT STEEL EXPANSION JOINTS.

PLACEMENT OF ADJACENT CONCRETE POURS

DO NOT PLACE ADJACENT CONCRETE POURS SIMULTANEOUSLY. ALLOW SUFFICIENT TIME FOR THE FIRST POUR TO CURE TO THE POINT FORMS CAN BE STRIPPED WITHOUT DETRIMENT TO THE POUR BEFORE PLACING THE SECOND POUR. ALL CONSTRUCTION JOINTS NOT SPECIFICALLY LABELED IN THE PLANS AS OPTIONAL ARE TO BE PERFORMED AS DETAILED ABOVE. SHOULD THE CONTRACTOR FAIL TO PERFORM THE CONSTRUCTION JOINT AS DESCRIBED, THE ENGINEER WILL DIRECT THE CONTRACTOR TO REMOVE THE INADEQUATELY PLACED CONCRETE AND REPLACE IT AS DESCRIBED ABOVE AT NO COST TO THE DEPARTMENT. PAYMENT WILL NOT BE MADE FOR INADEQUATELY PLACED CONCRETE NOT REPLACED.

ALL LABOR, MATERIAL, EQUIPMENT, AND INCIDENTALS NEEDED TO PERFORM THE DESCRIBED WORK IS TO BE CONSIDERED INCIDENTAL TO THE RESPECTIVE CONCRETE ITEM AND WILL BE PAID FOR UNDER THAT CONTRACT BID PRICE.

EXISTING REINFORCING STEEL

EXISTING REINFORCING STEEL, WHEN SHOWN, IS DETAILED FOR REPRESENTATION PURPOSES ONLY. IT IS NOT DETAILED TO SCALE. WHEN PERFORMING ALL REPAIR OR PATCHING WORK, TAKE UTMOST CARE TO NOT DAMAGE THE EXISTING REINFORCING STEEL. SHOULD THE EXISTING REINFORCING STEEL BE DAMAGED IN THE COURSE OF PERFORMING THE WORK, REPLACE THE DAMAGED STEEL AT NO COST TO THE DEPARTMENT. COAT ALL EXPOSED REINFORCING STEEL WITH AN APPROVED EPOXY COATING MATERIAL AND ALLOW SUFFICIENT CURING TIME PRIOR TO PLACING NEW CONCRETE. COST FOR THE ABOVE WORK WILL BE CONSIDERED INCIDENTAL TO THE APPROPRIATE REPAIR OR PATCHING ITEM.

STANDARD BRIDGE DRAWINGS & SUPPLEMENTAL SPECIFICATIONS

REFER TO STANDARD BRIDGE DRAWINGS AS-1-15 DATED 7/17/15, DBR-3-11 DATED 7/15/11 AND PCB-91 DATED 1/18/13.

ITEM 202. PORTIONS OF STRUCTURE REMOVED, AS PER PLAN

THIS ITEM SHALL INCLUDE THE ELEMENTS INDICATED IN THE PLANS AND GENERAL NOTES. ITEMS TO BE REMOVED INCLUDE ALL EXISTING MATERIALS BEING REPLACED BY NEW CONSTRUCTION AND MISCELLANEOUS ITEMS THAT ARE NOT SHOWN TO BE INCORPORATED INTO THE FINAL CONSTRUCTION AND ARE DIRECTED TO BE REMOVED BY THE ENGINEER. THE USE OF EXPLOSIVES, HEADACHE BALLS AND/OR HOE-RAMS WILL NOT BE PERMITTED. THE METHOD OF REMOVAL SHALL BE APPROVED BY THE ENGINEER. PERFORM ALL WORK IN A MANNER THAT WILL NOT CUT, ELONGATE OR DAMAGE THE EXISTING REINFORCING STEEL TO BE PRESERVED. CHIPPING HAMMERS SHALL NOT BE HEAVIER THAN THE NOMINAL 90-POUND CLASS. PNEUMATIC HAMMERS SHALL NOT BE PLACED IN DIRECT CONTACT WITH REINFORCING STEEL THAT IS TO BE RETAINED IN THE REBUILT STRUCTURE. SUBMIT CONSTRUCTION PLANS ACCORDING TO CMS 501.05.

CUT LINE CONSTRUCTION JOINT PREPARATION: SAW CUT BOUNDARIES OF PROPOSED CONCRETE REMOVALS 1 INCH DEEP. REMOVE CONCRETE TO A ROUGH SURFACE. LEAVE THE EXISTING REINFORCING STEEL, IF REQUIRED IN THE PLANS, IN PLACE. PRIOR TO CONCRETE PLACEMENT. ABRASIVELY CLEAN JOINT SURFACES AND EXISTING EXPOSED REINFORCEMENT TO REMOVE LOOSE AND DISINTEGRATED CONCRETE AND LOOSE RUST. THOROUGHLY CLEAN THE JOINT SURFACE AND EXPOSED REINFORCEMENT OF ALL DIRT, DUST, RUST OR OTHER FOREIGN MATERIAL BY THE USE OF WATER, AIR UNDER PRESSURE, OR OTHER METHODS THAT PRODUCE SATISFACTORY RESULTS. EXISTING REINFORCING STEEL DOES NOT HAVE TO HAVE A BRIGHT STEEL FINISH, BUT REMOVE ALL PACK AND LOOSE RUST. THOROUGHLY DRENCH EXISTING CONCRETE SURFACES WITH CLEAN WATER AND ALLOW TO DRY TO A DAMP CONDITION BEFORE PLACING CONCRETE.

PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID PER CUBIC YARD OF ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN.

ITEM 202 - REMOVAL MISC.: JOINT SEAL

THIS ITEM SHALL BE USED TO REMOVE THE EXISTING JOINT SEAL LOCATED BETWEEN THE APPROACH SLAB AND THE DECK OR BACKWALL.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER FOOT FOR THE ABOVE ITEM, WHICH WILL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 202 - REMOVAL MISC.: POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM (STRUCTURE LOR-20-0217)

THIS ITEM SHALL BE USED TO REMOVE THE EXISTING POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM LOCATED BETWEEN THE APPROACH SLAB AND THE DECK OR BACKWALL.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER FOOT FOR THE ABOVE ITEM, WHICH WILL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 202 - BRIDGE RAILING REMOVED FOR REUSE

THIS ITEM SHALL BE USED TO REMOVE AND REINSTALL THE EXISTING BRIDGE RAILING TO FACILITATE FULL WIDTH PAVING OVER THE STRUCTURE. BRIDGE RAILING POSTS ARE TO REMAIN IN PLACE. GUARDRAIL AND BRIDGE RAILING MUST BE IN PLACE IF TRAFFIC IS TO BE PERMITTED IN THE ADJACENT LANE.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER FOOT FOR THE ABOVE ITEM, WHICH WILL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

ITEM 409 - SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN

WHERE SHOWN IN THESE PLANS, PROVIDE A TWO (2) INCH WIDE BY THREE (3) INCH DEEP SAWED AND SEALED ASPHALT CONCRETE PAVEMENT JOINT. REMOVE ALL MATERIAL FROM THE DESCRIBED AREA AND ENSURE IT IS FREE OF DEBRIS. FILL THE VOID IN ACCORDANCE WITH C&MS 409.

PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIAL, AND INCIDENTALS NEEDED TO COMPLETE THIS WORK SHALL BE MADE AT THE CONTRACT BID PRICE PER FOOT FOR ITEM 409 - SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN.

**ITEM 511 - CLASS QC2 CONCRETE, BRIDGE DECK (PARAPET)
ITEM 511 - CLASS QC2 CONCRETE, MISC.: APPROACH SLAB REPAIR**

THIS ITEM SHALL BE USED AT LOCATIONS INDICATED IN THE PLAN.

THE CONCRETE SHALL BE CLASS QC2. THE COURSE AGGREGATE SHALL BE LIMESTONE.

ALL EXISTING SURFACES WITH WHICH THE CONCRETE IS TO BOND SHALL BE CLEANED BY ABRASIVE BLASTING. THESE SURFACES SHALL BE MADE FREE OF SPALLS, LAITANCE, PAINT, RUST, AND OTHER CONTAMINANTS DETRIMENTAL TO ACHIEVING AN ADEQUATE BOND.

PAYMENT FOR THE ABOVE SHALL BE MADE AT THE UNIT BID PRICE PER CUBIC YARD FOR THE ABOVE LISTED ITEMS AND WILL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NEEDED TO COMPLETE THE WORK.

ITEM 517 - DEEP BEAM BRIDGE RETROFIT RAILING

THIS ITEM SHALL INCLUDE THE RETROFIT OF ALL EXISTING DEEP BEAM BRIDGE RAILING ON STRUCTURES HUR-20-1774, HUR-20-2226, HUR-20-2283, HUR-20-2449, AND LOR-20-0217. THE REMOVAL AND REPLACEMENT OF ALL BOLTS AND HARDWARE NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THIS ITEM. THE RAIL ELEMENTS SHALL BE PLACED IN ACCORDANCE WITH STANDARD CONSTRUCTION DRAWING DBR-3-11, DATED 7/15/2011. VERIFY THE LOCATION OF THE REPAIR IN THE FIELD PRIOR TO BEGINNING WORK. THE REPAIR SHOULD BE CENTERED ON THE BRIDGE ON BOTH ENDS.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER FOOT FOR ITEM 517 - DEEP BEAM BRIDGE RETROFIT RAILING, WHICH WILL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

CALCULATED
ACM
CHECKED
NRF

STRUCTURE NOTES

HUR / LOR-20-16.26 / 0.00
LOR-511-21.14

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ITEM 846 - POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM (STRUCTURE LOR-20-0217)

THIS ITEM WILL BE USED TO SEAL THE EXPANSION/CONTRACTION JOINTS AS PER SUPPLEMENTAL SPECIFICATION 846 AND THE MANUFACTURER'S REQUIREMENTS USING A POLYMER MODIFIED ASPHALT SYSTEM. THE CONTRACTOR WILL OBTAIN THE SERVICES OF ONE OF THE FOLLOWING APPROVED APPLICATORS WHO WILL FURNISH AND INSTALL THE NEW BRIDGE EXPANSION JOINT SYSTEM AFTER ALL PAVING ON THE AFFECTED STRUCTURE HAS BEEN COMPLETED.

| PRODUCT NAME | SUPPLIER | ADDRESS | PHONE NO. |
|------------------------------------|-----------------------------------|---|----------------|
| THORMA-JOINT | DYNAMIC SURFACE APPLICATIONS, LTD | 373 VILLAGE RD. PENNSDALE, PA 17756 | (570) 546-6041 |
| MATRIX 502 | CRAFCO, INC. | 420 N. ROOSEVELT AVE. CHANDLER, AZ 85226 | (800) 528-8242 |
| EXPANDEX JOINT SYSTEM | WATSON-BOWMAN ACME | 95 PINEVIEW DR. AMHERST, NY 14228 | (716) 691-7566 |
| APJ ASPHALTIC PLUG EXPANSION JOINT | WYOMING EQUIPMENT SALES | 281 WEST SIXTH STREET WEST WYOMING, PA 18644 | (570) 693-2810 |

PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIAL, AND INCIDENTALS NEEDED TO COMPLETE THIS WORK SHALL BE MADE AT THE CONTRACT BID PRICE PER CUBIC FOOT FOR ITEM 846 - POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM.

ITEM 614 - MAINTAINING TRAFFIC FOR STRUCTURES (2-LANE)

TWO WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT THROUGH TRAFFIC ON THESE STRUCTURES MAY HAVE A LANE CLOSURE DURING NORMAL WORKING HOURS USING FLAGGERS AS SHOWN ON STANDARD DRAWING MT-97.10.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614 - MAINTAINING TRAFFIC (FOR STRUCTURE HUR-20-2226)

TWO WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT THROUGH TRAFFIC ON THIS STRUCTURE SHALL HAVE A SIGNALIZED LANE CLOSURE AS SHOWN ON SHEET 18 FOR A MAXIMUM OF 30 CONSECUTIVE CALENDAR DAYS (TOTAL BOTH PHASES). THE 30 CONSECUTIVE DAYS SHALL BE CONSIDERED AS AN INTERIM COMPLETION DATE (SECTION 108) AND FOR EACH CALENDAR DAY BEYOND THE 30 DAYS THAT THE HIGHWAY REMAINS IN A SIGNALIZED CLOSURE, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE FEE OF \$1500 PER DAY.

NO EQUIPMENT OR MATERIAL SHALL BE LOCATED OTHER THAN BEHIND THE PORTABLE CONCRETE BARRIER.

ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES AS PER 614.02 (A).

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

TEMPORARY TRAFFIC SIGNAL ACTIVATION FOR PARTIAL ROADWAY CLOSURE (STRUCTURE HUR-20-2226)

THE CONTRACTOR SHALL NOTIFY ODOT DISTRICT 3 PUBLIC INFORMATION OFFICER (PIO) A MINIMUM FOURTEEN (14) CALENDAR DAYS ADVANCE NOTICE BEFORE ACTIVATING A TEMPORARY TRAFFIC SIGNAL TO STOP-AND-GO OPERATION FOR PARTIAL ROADWAY CLOSURE.

THE PIO CONTACT INFORMATION IS AS FOLLOWS:

KAITLYN MAYNARD
PUBLIC INFORMATION OFFICER
ODOT DISTRICT 3
906 CLARK AVENUE
ASHLAND, OH 44805
PHONE 419-207-7182

IN ADDITION, THE TEMPORARY TRAFFIC SIGNAL SHALL BE ACTIVATED PER THE REQUIREMENTS OF ODOT SCD MT-120.00. THE TEMPORARY TRAFFIC SIGNAL SHALL OPERATE IN FLASH MODE FIVE (5) TO SEVEN (7) DAYS PRIOR TO ACTIVATING TO STOP-AND-GO OPERATION. SIGNAL ACTIVATION SHALL NOT OCCUR ON WEEKENDS, MONDAYS, FRIDAYS, OR ANY DAY IMMEDIATELY BEFORE OR AFTER A STATE OBSERVED HOLIDAY.

ALL COSTS ASSOCIATED WITH THE ABOVE DESCRIBED WORK SHALL BE INCLUDED WITH ITEM 614 - MAINTAINING TRAFFIC.

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| SUB-SUMMARY OF STRUCTURE REPAIR ITEMS | | | | | | | | | | | | | | | |
|---------------------------------------|-----------|--|------|-------------|-------------|--------------|-------------|-------------|-------------|--------------|--------------|--------------|--------------|--------------|--------------|
| ITEM | EXTENSION | DESCRIPTION | UNIT | HUR-20-1774 | HUR-20-2226 | HUR-20-2283 | HUR-20-2449 | LOR-20-0217 | LOR-20-0868 | LOR-20-1056L | LOR-20-1056R | LOR-20-1208L | LOR-20-1208R | LOR-20-1354L | LOR-20-1354R |
| | | | | (04/NHS/BR) | (04/NHS/BR) | (09/NHS/BR) | (09/NHS/BR) | (04/NHS/BR) | (04/NHS/BR) | (04/NHS/BR) | (04/NHS/BR) | (04/NHS/BR) | (04/NHS/BR) | (04/NHS/BR) | (04/NHS/BR) |
| | | | | SHEET 32 | SHEET 33 | NOT DETAILED | | SHEET 34 | SHEET 35 | SHEET 36 | | SHEET 37 | | SHEET 38 | |
| | | | | | | | | | | | | | | 2 | |
| 202 | 11301 | PORTIONS OF STRUCTURE REMOVED, AS PER PLAN | CY | | 4 | | | | | | | | | | |
| 202 | 38603 | BRIDGE RAILING REMOVED FOR REUSE | FT | 153 | | | | 132 | | | | | | | |
| 202 | 98200 | REMOVAL MISC.: JOINT SEAL | FT | | | | | | 82 | | | | | 80 | 80 |
| 202 | 98200 | REMOVAL MISC.: POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM | FT | | | | | 80 | | | | | | | |
| 407 | 20000 | NON-TRACKING TACK COAT | GAL | 29 | | | | 22 | | 14 | 14 | 47 | 47 | | |
| 409 | 30001 | SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN | FT | 101 | | | | | | 80 | 80 | | | | |
| 424 | 12000 | FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (1.25") | CY | 13 | | | | 9 | | 6 | 6 | 21 | 21 | | |
| 511 | 34448 | CLASS QC2 CONCRETE, BRIDGE DECK (PARAPET) | CY | | | | | | | | | | | 2 | |
| 511 | 53012 | CLASS QC2 CONCRETE, MISC.: APPROACH SLAB REPAIR | CY | | 4 | | | | | | | | | | |
| 516 | 31000 | JOINT SEALER | FT | | 48 | | | | 82 | | | | | 80 | 80 |
| 517 | 75600 | DEEP BEAM BRIDGE RETROFIT RAILING | FT | 153 | 76 | 30 | 38 | 132 | | | | | | | |
| 519 | 11100 | PATCHING CONCRETE STRUCTURE | SF | | 96 | | | | | | | | | | |
| 614 | 12350 | WORK ZONE IMPACT ATTENUATOR | EACH | | 4 | | | | | | | | | | |
| 614 | 13310 | BARRIER REFLECTOR, TYPE 1 (BIDIRECTIONAL) | EACH | | 19 | | | | | | | | | | |
| 614 | 13312 | BARRIER REFLECTOR, TYPE 2 (BIDIRECTIONAL) | EACH | | 14 | | | | | | | | | | |
| 614 | 13360 | OBJECT MARKER, TWO WAY | EACH | | 12 | | | | | | | | | | |
| 614 | 21200 | WORK ZONE CENTER LINE, CLASS I, 740.06, TYPE I | MILE | | 0.19 | | | | | | | | | | |
| 614 | 22210 | WORK ZONE EDGE LINE, CLASS I, 6", 740.06, TYPE I | MILE | | 0.27 | | | | | | | | | | |
| 614 | 26400 | WORK ZONE STOP LINE, CLASS I, 740.06, TYPE I | FT | | 72 | | | | | | | | | | |
| 615 | 20000 | PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A | SY | | 183 | | | | | | | | | | |
| 622 | 41000 | PORTABLE BARRIER, 32" | FT | | 320 | | | | | | | | | | |
| 622 | 41020 | PORTABLE BARRIER, 32", BRIDGE MOUNTED (UNANCHORED) | FT | | 100 | | | | | | | | | | |
| 846 | 00110 | POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM | CF | | | | | 64 | | | | | | | |
| 897 | 01010 | PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (1.25") | SY | 360 | | | | 271 | | 180 | 180 | 593 | 593 | | |

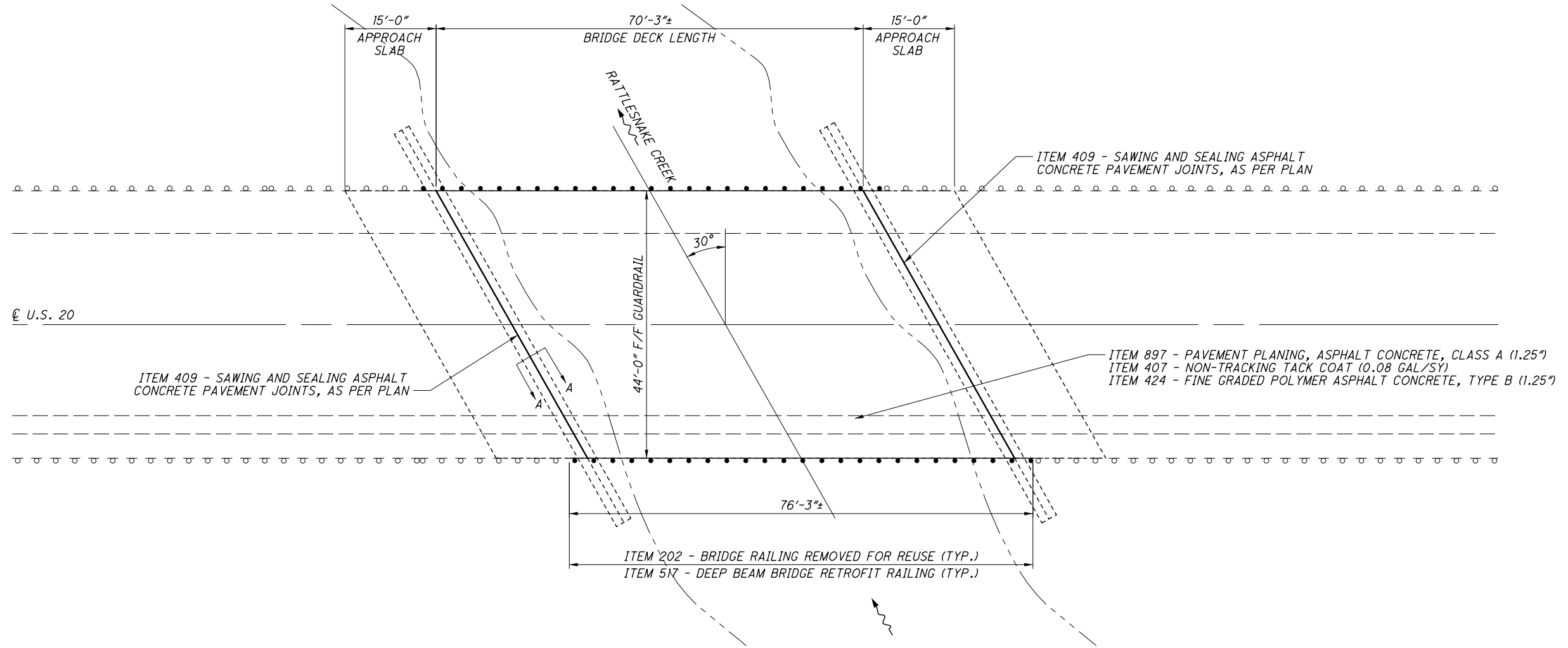
ALL QUANTITIES CARRIED TO GENERAL SUMMARY

CALCULATED
ACM
CHECKED
NRF

STRUCTURE SUB-SUMMARY

HUR / LOR-20-16.26 / 0.00
LOR-511-21.14

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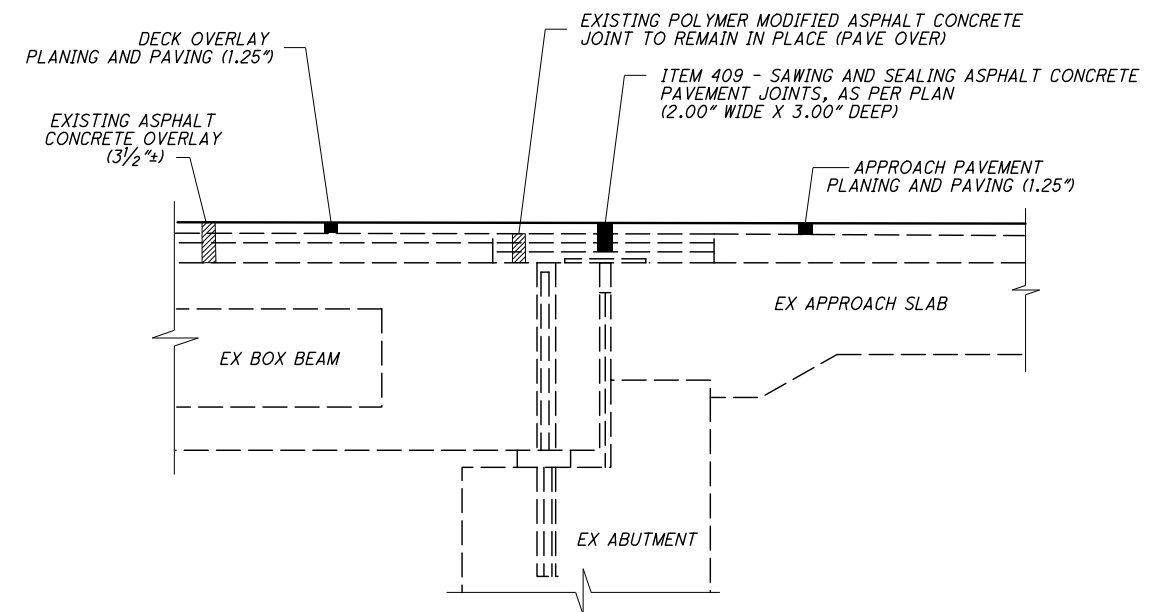


NOTES

1) ITEM 202 - BRIDGE RAILING REMOVED FOR REUSE
 THIS WORK IS TO FACILITATE FULL WIDTH PLANING AND PAVING OVER THE STRUCTURE. THE INTENT IS TO REMOVE THE RAIL ELEMENTS ONLY DURING THE PLANING AND PAVING OPERATIONS. DO NOT ALLOW TRAFFIC IN THE LANE ADJACENT TO THE GUARDRAIL WHEN THE RAIL IS REMOVED. REINSTALL THE RAIL IF TRAFFIC IS TO BE PERMITTED IN THE ADJACENT LANE.

2) PERFORM ITEM 517 DEEP BEAM BRIDGE RETROFIT RAILING AS DETAILED ON SHEET 29 AND AFTER PLANING AND PAVING OPERATIONS ARE COMPLETED OVER THE STRUCTURE. VERIFY THE LOCATION OF THE REPAIR IN THE FIELD PRIOR TO BEGINNING WORK. THE REPAIR SHOULD BE CENTERED ON THE BRIDGE ON BOTH ENDS.

ALL MATERIAL, EQUIPMENT, LABOR, AND INCIDENTALS NEEDED TO COMPLETE THIS WORK WILL BE PAID FOR UNDER THE CONTRACT BID PRICE PER FOOT FOR ITEM 517 DEEP BEAM BRIDGE RETROFIT RAILING.



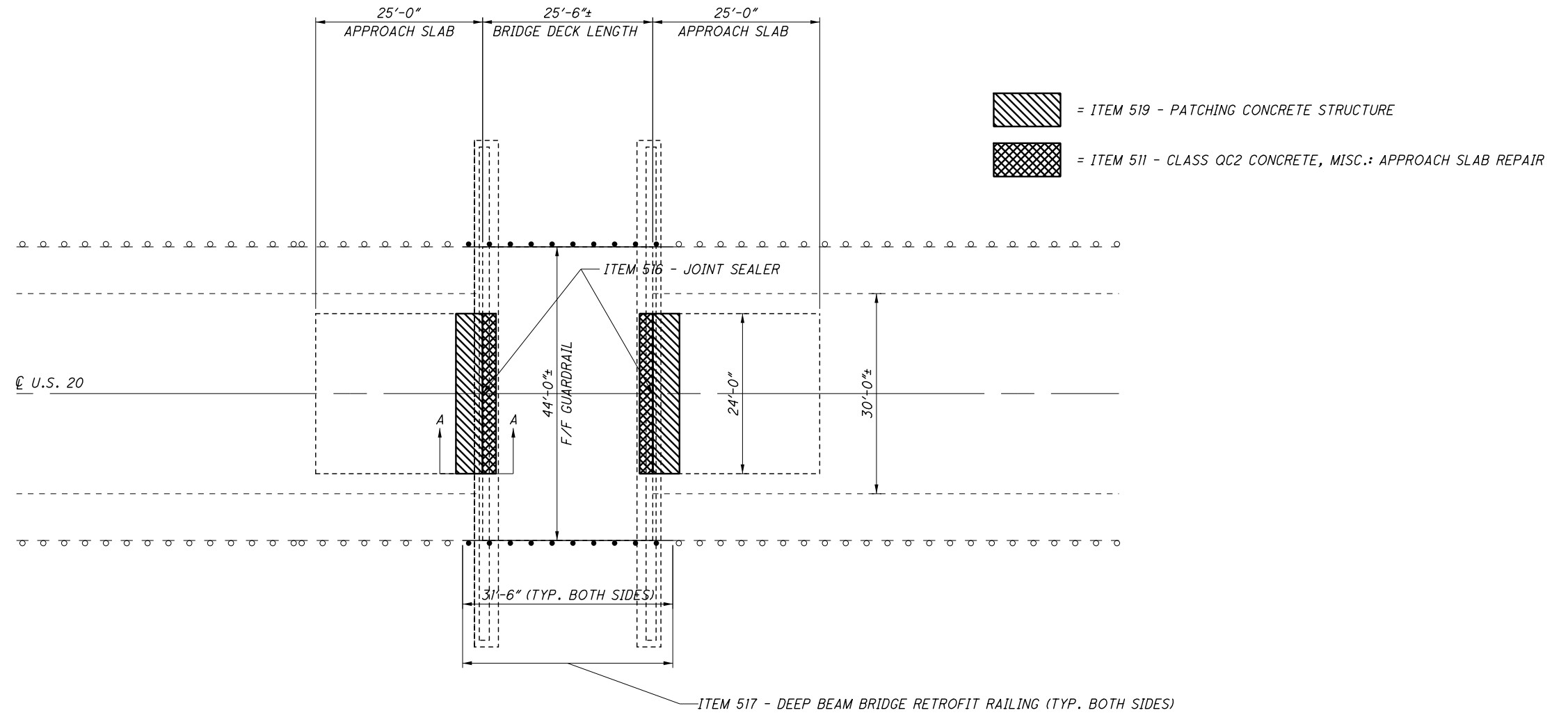
SECTION A-A

| ESTIMATED QUANTITIES HUR-20-1774 (SFN: 3901394) | | | |
|---|----------|------|--|
| ITEM | QUANTITY | UNIT | DESCRIPTION |
| 202 | 153 | FT | BRIDGE RAILING REMOVED FOR REUSE |
| 407 | 29 | GAL | NON-TRACKING TACK COAT (0.08 GAL/SY) |
| 409 | 101 | FT | SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN |
| 424 | 13 | CY | FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (1.25") |
| 517 | 153 | FT | DEEP BEAM BRIDGE RETROFIT RAILING |
| 897 | 360 | SY | PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (1.25") |

ALL QUANTITIES CARRIED TO STRUCTURE SUB-SUMMARY (04/NHS/BR)



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NOTES

1) PERFORM ITEM 517 DEEP BEAM BRIDGE RETROFIT RAILING AS DETAILED ON SHEET 29. VERIFY THE LOCATION OF THE REPAIR IN THE FIELD PRIOR TO BEGINNING WORK. THE REPAIR SHOULD BE CENTERED ON THE BRIDGE ON BOTH ENDS.

ALL MATERIAL, EQUIPMENT, LABOR, AND INCIDENTALS NEEDED TO COMPLETE THIS WORK WILL BE PAID FOR UNDER THE CONTRACT BID PRICE PER FOOT FOR ITEM 517 DEEP BEAM BRIDGE RETROFIT RAILING.

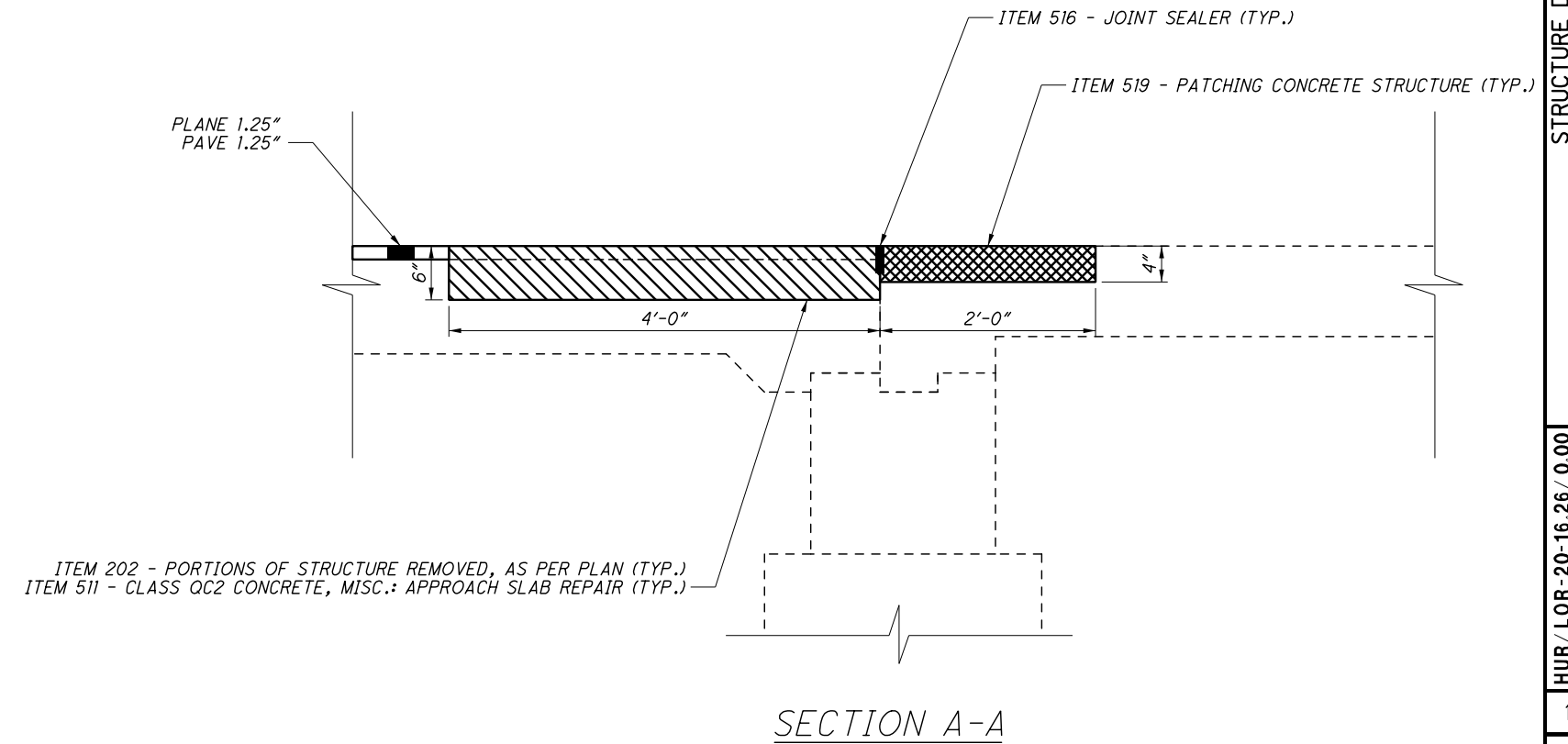
2) SUSPEND AND RESUME PAVING OPERATIONS AT APPROACH SLAB REPAIRS ON BOTH ENDS OF STRUCTURE.

3) DO NOT DISTURB EXISTING REINFORCING STEEL IN THE APPROACH SLABS OR DECK.

4) SEE SHEET 19 FOR MAINTENANCE OF TRAFFIC DETAILS.

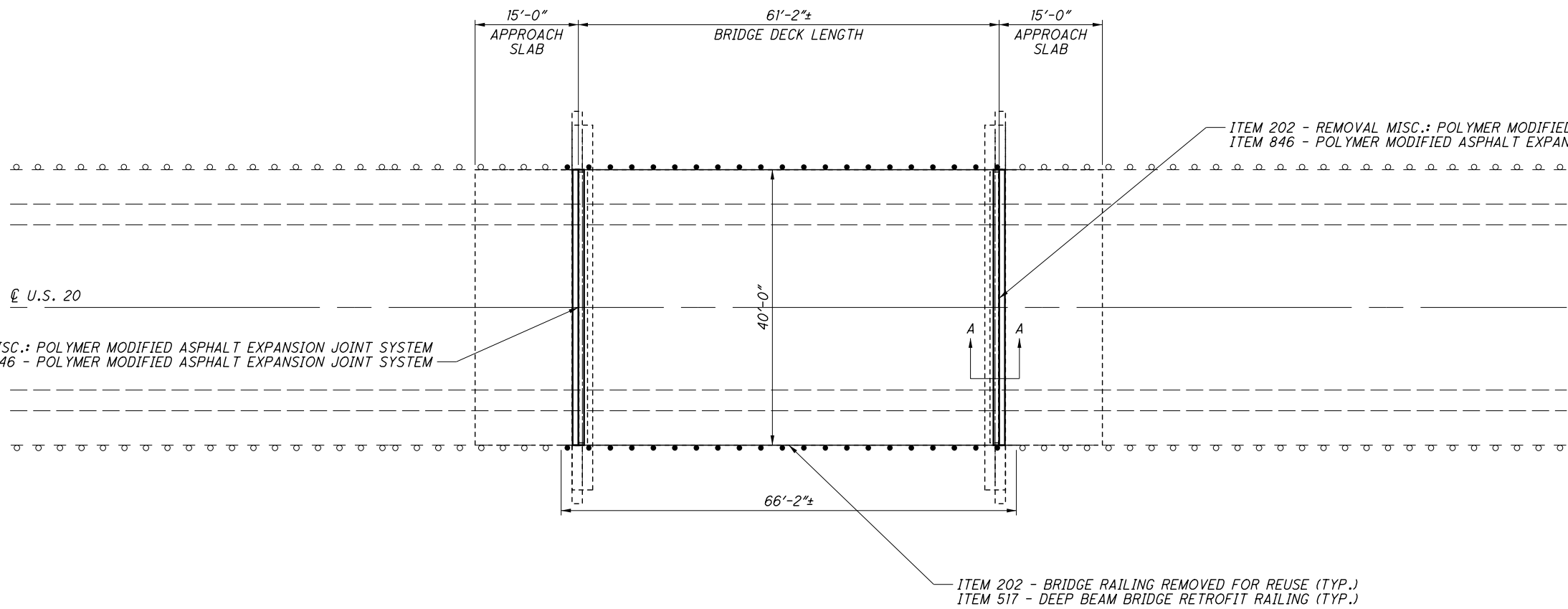
| ESTIMATED QUANTITIES HUR-20-2226 (SFN: 3901416) | | | |
|---|----------|------|---|
| ITEM | QUANTITY | UNIT | DESCRIPTION |
| 202 | 4 | CY | PORTIONS OF STRUCTURE REMOVED, AS PER PLAN |
| 511 | 4 | CY | CLASS QC2 CONCRETE, MISC.: APPROACH SLAB REPAIR |
| 516 | 48 | FT | JOINT SEALER |
| 517 | 76 | FT | DEEP BEAM BRIDGE RETROFIT RAILING |
| 519 | 96 | SF | PATCHING CONCRETE STRUCTURE |

ALL QUANTITIES CARRIED TO STRUCTURE SUB-SUMMARY (04/NHS/BR)



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ITEM 202 - REMOVAL MISC.: POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM
ITEM 846 - POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM

NOTES

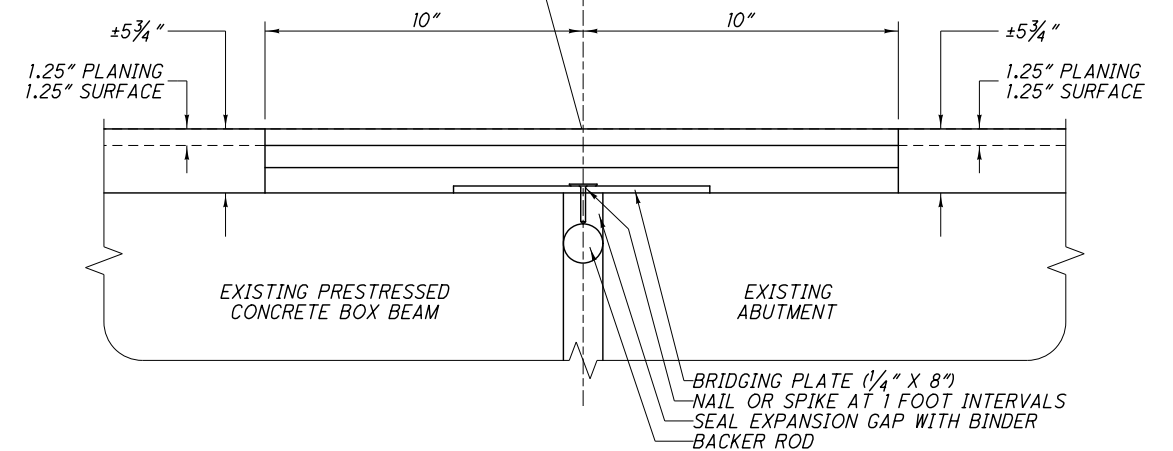
1) ITEM 202 - BRIDGE RAILING REMOVED FOR REUSE
THIS WORK IS TO FACILITATE FULL WIDTH PLANING AND PAVING OVER THE STRUCTURE. THE INTENT IS TO REMOVE THE RAIL ELEMENTS ONLY DURING THE PLANING AND PAVING OPERATIONS. DO NOT ALLOW TRAFFIC IN THE LANE ADJACENT TO THE GUARDRAIL WHEN THE RAIL IS REMOVED. REINSTALL THE RAIL IF TRAFFIC IS TO BE PERMITTED IN THE ADJACENT LANE.

2) PERFORM ITEM 517 DEEP BEAM BRIDGE RETROFIT RAILING AS DETAILED ON SHEET 29. VERIFY THE LOCATION OF THE REPAIR IN THE FIELD PRIOR TO BEGINNING WORK. THE REPAIR SHOULD BE CENTERED ON THE BRIDGE ON BOTH ENDS.

ALL MATERIAL, EQUIPMENT, LABOR, AND INCIDENTALS NEEDED TO COMPLETE THIS WORK WILL BE PAID FOR UNDER THE CONTRACT BID PRICE PER FOOT FOR ITEM 517 DEEP BEAM BRIDGE RETROFIT RAILING.

4) SEE SUPPLEMENTAL SPECIFICATION 846 FOR DETAILS NOT SHOWN ON THIS SHEET FOR WORK RELATED TO POLYMER MODIFIED ASPHALT EXPANSION JOINTS.

ITEM 202 - REMOVAL MISC.: POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM
ITEM 846 - POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM



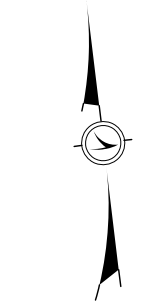
| ESTIMATED QUANTITIES LOR-20-0217 (SFN: 4701852) | | | |
|---|----------|------|--|
| ITEM | QUANTITY | UNIT | DESCRIPTION |
| 202 | 132 | FT | BRIDGE RAILING REMOVED FOR REUSE |
| 202 | 80 | FT | REMOVAL MISC.: POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM |
| 407 | 22 | GAL | NON-TRACKING TACK COAT (0.08 GAL/SY) |
| 424 | 9 | CY | FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (1.25") |
| 517 | 132 | FT | DEEP BEAM BRIDGE RETROFIT RAILING |
| 846 | 64 | CF | POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM |
| 897 | 271 | SY | PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (1.25") |

ALL QUANTITIES CARRIED TO STRUCTURE SUB-SUMMARY (04/NHS/BR)

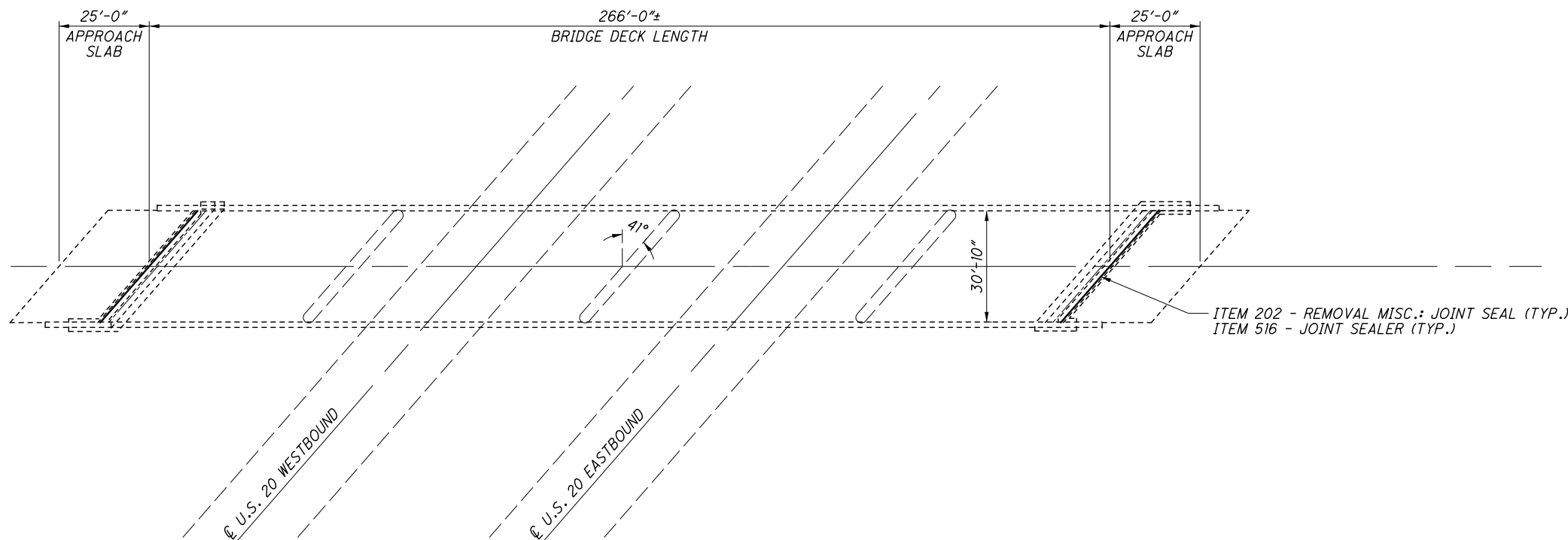
SECTION A-A



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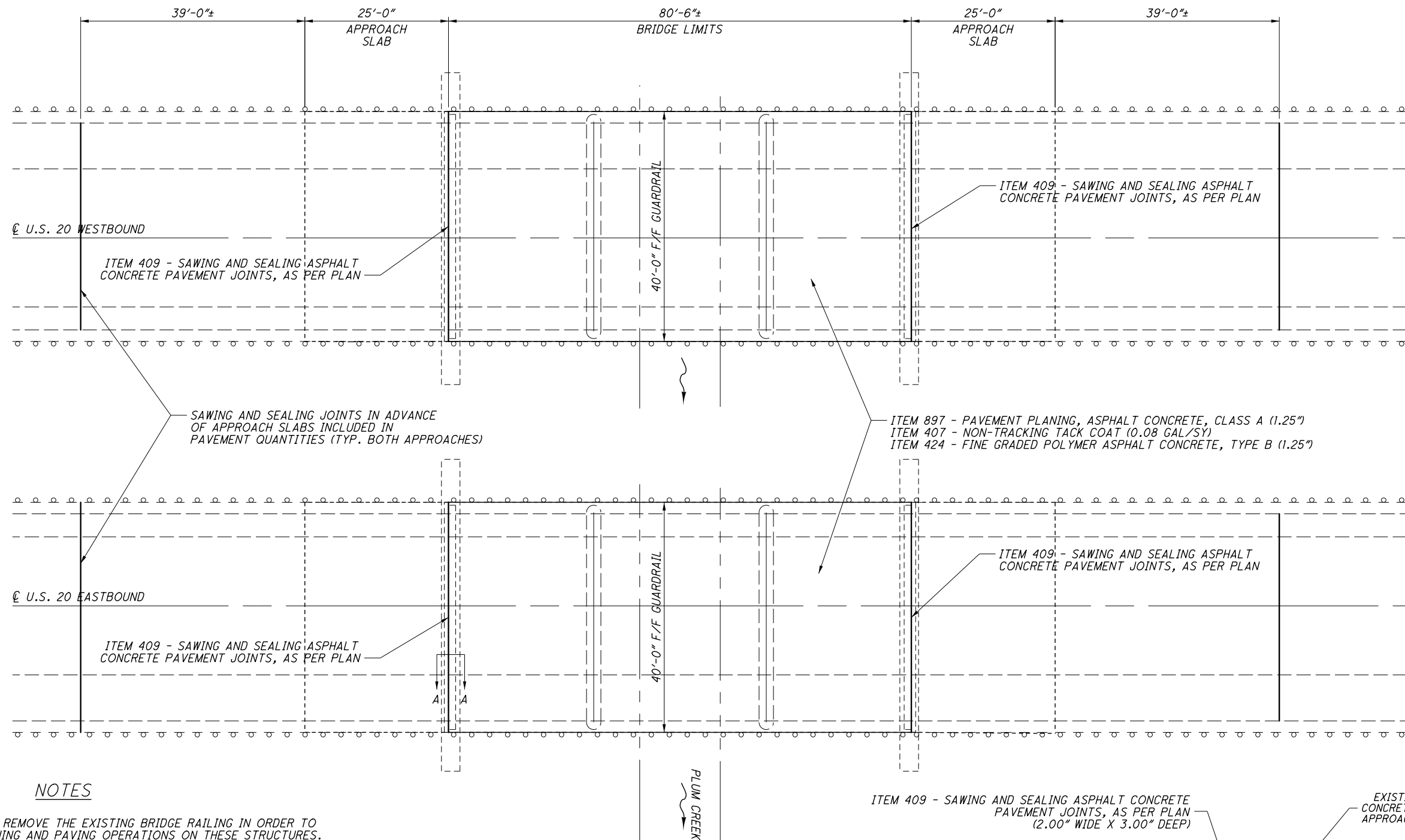
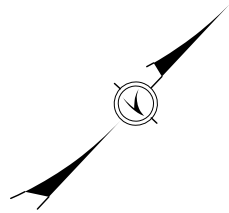
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ITEM 202 - REMOVAL MISC.: JOINT SEAL (TYP.)
ITEM 516 - JOINT SEALER (TYP.)

| ESTIMATED QUANTITIES LOR-20-0868 (SFN: 4701909) | | | |
|---|----------|------|---------------------------|
| ITEM | QUANTITY | UNIT | DESCRIPTION |
| 202 | 82 | FT | REMOVAL MISC.: JOINT SEAL |
| 516 | 82 | FT | JOINT SEALER |

ALL QUANTITIES CARRIED TO STRUCTURE SUB-SUMMARY (04/NHS/BR)



NOTES

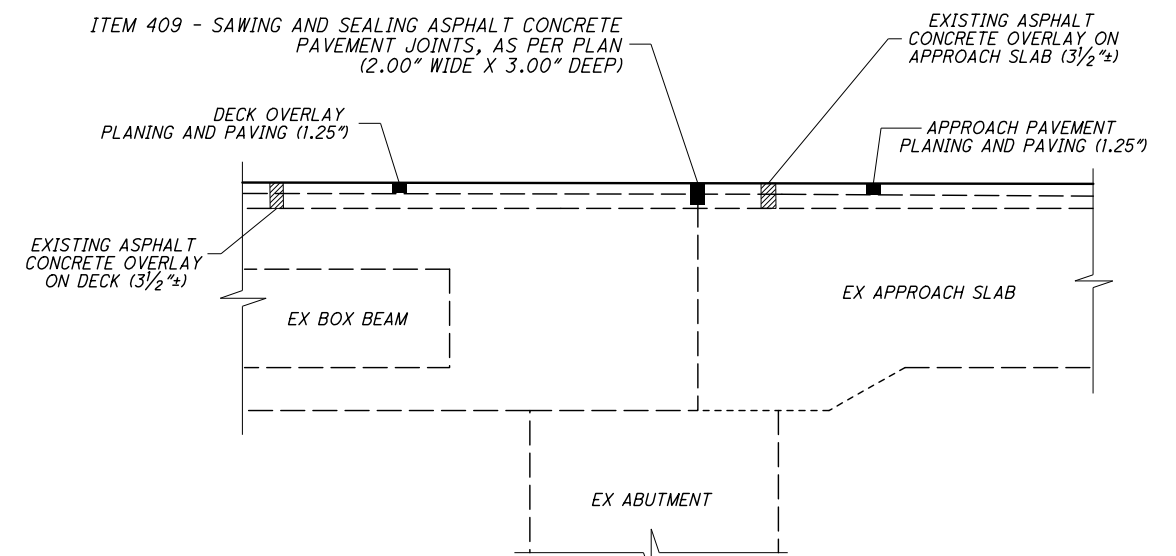
1) THE CONTRACTOR SHALL REMOVE THE EXISTING BRIDGE RAILING IN ORDER TO PERFORM FULL WIDTH PLANING AND PAVING OPERATIONS ON THESE STRUCTURES. THE EXISTING BRIDGE RAILING POSTS SHALL REMAIN IN PLACE. THE CONTRACTOR SHALL REINSTALL THE BRIDGE RAILING BY THE END OF THE WORKING DAY. THE EXISTING BRIDGE RAILING SHALL ONLY BE REMOVED WHEN WORKERS ARE PRESENT AND WITH THE APPROVAL OF THE ENGINEER.

SHOULD THE CONTRACTOR FAIL TO COMPLY WITH THE ABOVE REQUIREMENTS, AND SUCH FAILURE RESULTS IN A CONDITION AT THE WORK SITE WHICH IS UNSAFE FOR TRAFFIC, THE ENGINEER SHALL SUSPEND WORK UNTIL THE CONTRACTOR COMPLIES WITH THE NECESSARY REQUIREMENTS.

ALL MATERIAL, EQUIPMENT AND LABOR NEEDED TO COMPLETE THE ABOVE WORK SHALL BE CONSIDERED INCIDENTAL TO MAINTAINING TRAFFIC AND WILL BE PAID FOR UNDER THE LUMP SUM CONTRACT BID PRICE FOR ITEM 614 MAINTAINING TRAFFIC.

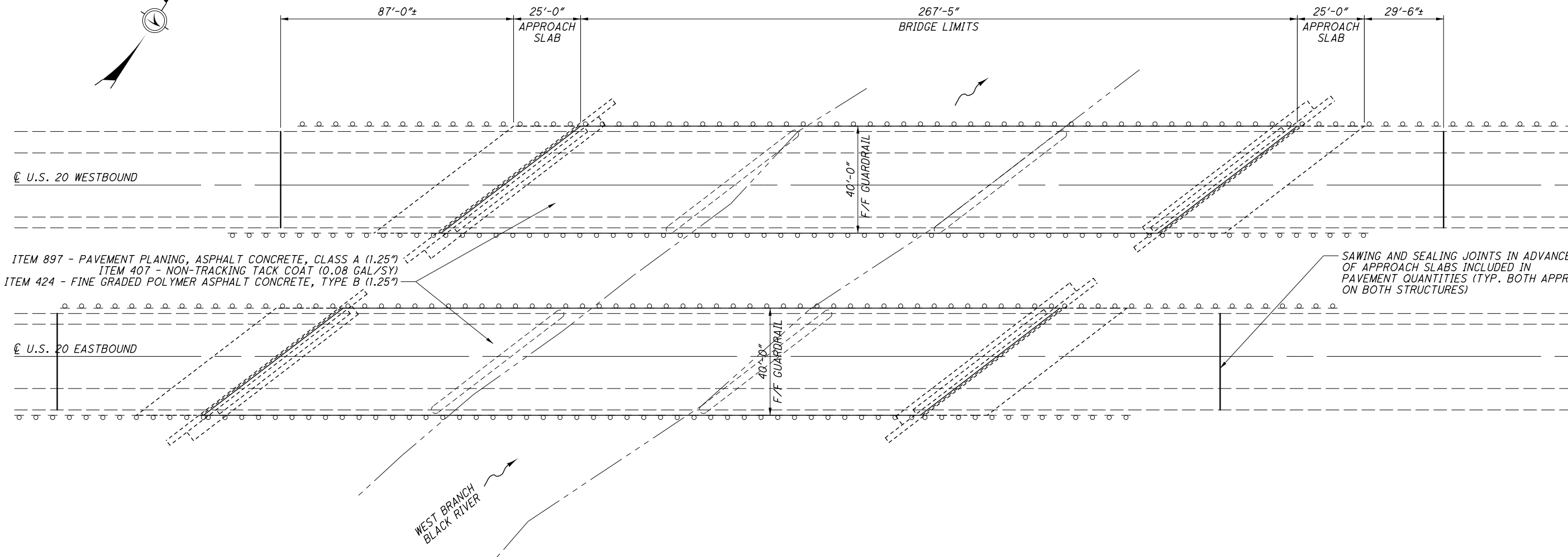
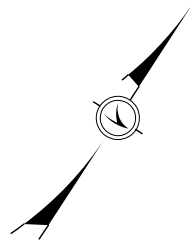
| ESTIMATED QUANTITIES LOR-20-1056 (L&R) (SFN: 4701933/4701941) | | | |
|---|----------|------|--|
| ITEM | QUANTITY | UNIT | DESCRIPTION |
| 407 | 28 | GAL | NON-TRACKING TACK COAT (0.08 GAL/SY) |
| 409 | 160 | FT | SAWING AND SEALING ASPHALT CONCRETE PAVEMENT JOINTS, AS PER PLAN |
| 424 | 12 | CY | FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (1.25") |
| 897 | 360 | SY | PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (1.25") |

ALL QUANTITIES CARRIED TO STRUCTURE SUB-SUMMARY (04/NHS/BR)



SECTION A-A

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ITEM 897 - PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (1.25")
 ITEM 407 - NON-TRACKING TACK COAT (0.08 GAL/SY)
 ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (1.25")

NOTES

1) THE CONTRACTOR SHALL REMOVE THE EXISTING BRIDGE RAILING IN ORDER TO PERFORM FULL WIDTH PLANING AND PAVING OPERATIONS ON THESE STRUCTURES. THE EXISTING BRIDGE RAILING POSTS SHALL REMAIN IN PLACE. THE CONTRACTOR SHALL REINSTALL THE BRIDGE RAILING BY THE END OF THE WORKING DAY. THE EXISTING BRIDGE RAILING SHALL ONLY BE REMOVED WHEN WORKERS ARE PRESENT AND WITH THE APPROVAL OF THE ENGINEER.

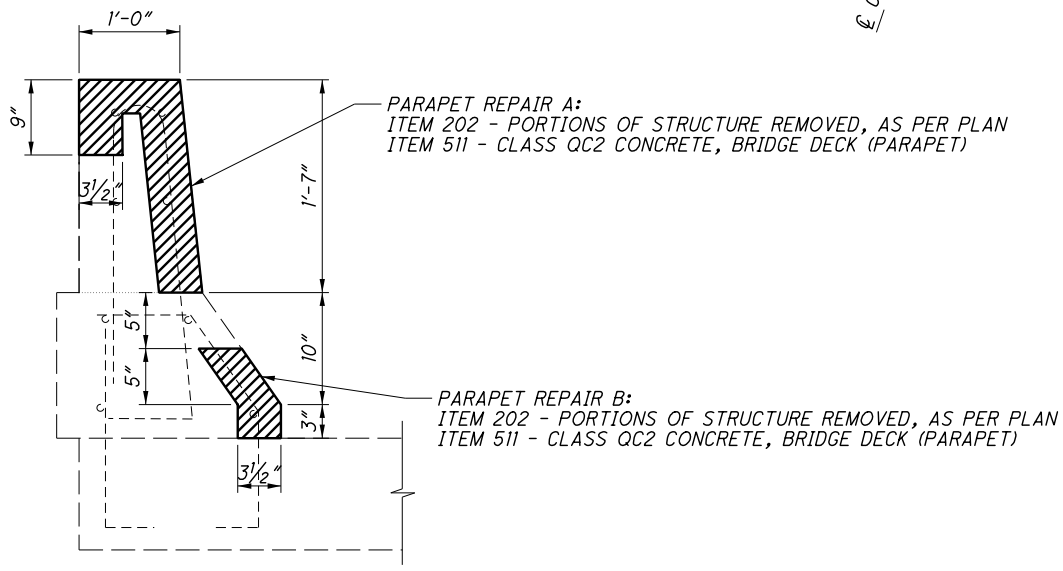
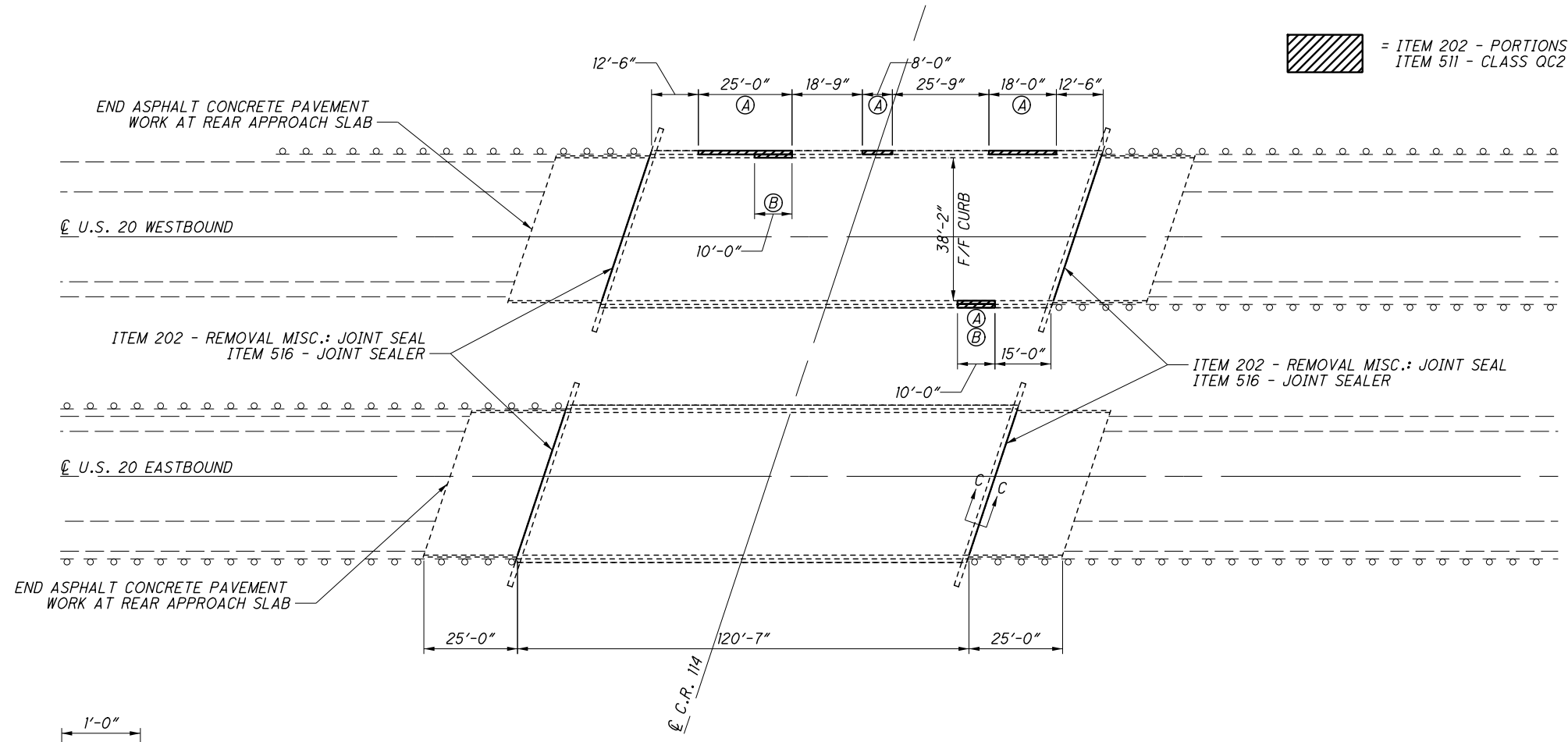
SHOULD THE CONTRACTOR FAIL TO COMPLY WITH THE ABOVE REQUIREMENTS, AND SUCH FAILURE RESULTS IN A CONDITION AT THE WORK SITE WHICH IS UNSAFE FOR TRAFFIC, THE ENGINEER SHALL SUSPEND WORK UNTIL THE CONTRACTOR COMPLIES WITH THE NECESSARY REQUIREMENTS.

ALL MATERIAL, EQUIPMENT AND LABOR NEEDED TO COMPLETE THE ABOVE WORK SHALL BE CONSIDERED INCIDENTAL TO MAINTAINING TRAFFIC AND WILL BE PAID FOR UNDER THE LUMP SUM CONTRACT BID PRICE FOR ITEM 614 MAINTAINING TRAFFIC.

| ESTIMATED QUANTITIES LOR-20-1208 (L&R) (SFN: 4701968/4701976) | | | |
|---|----------|------|--|
| ITEM | QUANTITY | UNIT | DESCRIPTION |
| 407 | 94 | GAL | NON-TRACKING TACK COAT (0.08 GAL/SY) |
| 424 | 42 | CY | FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (1.25") |
| 897 | 1186 | SY | PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A (1.25") |

ALL QUANTITIES CARRIED TO STRUCTURE SUB-SUMMARY (04/NHS/BR)

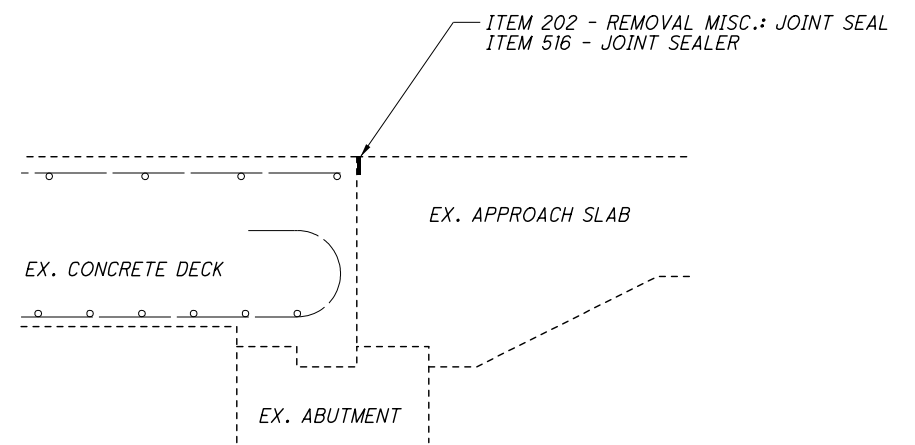
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PARAPET REPAIR DETAIL

NOTES

- 1) USE EXTREME CARE WHEN PERFORMING ALL ITEMS THAT REQUIRE ANY REMOVAL OF THE EXISTING STRUCTURE AS TO NOT DAMAGE ANY EXISTING REINFORCING STEEL; THE REINFORCING STEEL IS TO REMAIN IN PLACE AND NOT BE REMOVED IN THE REMOVAL PROCESS. REPAIR ANY DAMAGED STEEL AT NO COST TO THE DEPARTMENT AND TO THE SATISFACTION OF THE ENGINEER.
- 2) APPROXIMATE DIMENSIONS ARE SHOWN ON THIS PLAN AND ARE FOR ESTIMATING PURPOSES ONLY. VERIFY THESE DIMENSIONS IN THE FIELD AND ADJUST ACCORDINGLY IF NECESSARY.



SECTION C-C

| ESTIMATED QUANTITIES LOR-20-1354 (L&R) (SFN: 4701089/4701119) | | | |
|---|----------|------|--|
| ITEM | QUANTITY | UNIT | DESCRIPTION |
| 202 | 2 | CY | PORTIONS OF STRUCTURE REMOVED, AS PER PLAN |
| 202 | 160 | FT | REMOVAL MISC.: JOINT SEAL |
| 511 | 2 | CY | CLASS QC2 CONCRETE, BRIDGE DECK (PARAPET) |
| 516 | 160 | FT | JOINT SEALER |

ALL QUANTITIES CARRIED TO STRUCTURE SUB-SUMMARY (04/NHS/BR)

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