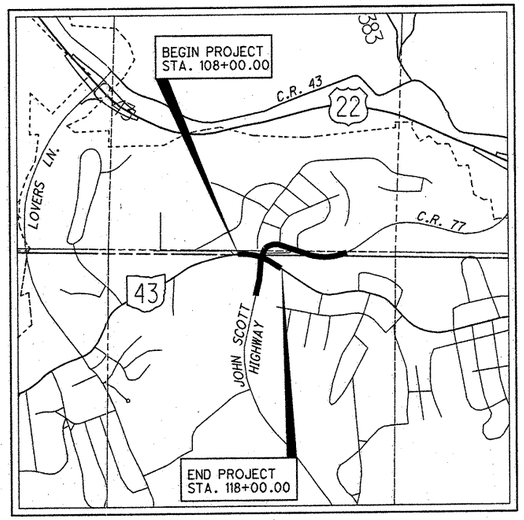


LOCATION MAP

LATITUDE: 40°22'40" LONGITUDE: 80°40'10"

SCALE IN MILES

0 1/4 1/2 3/4 1

N

PORTION TO BE IMPROVED
INTERSTATE & DIVIDED HIGHWAY
UNDIVIDED STATE & FEDERAL ROUTES
OTHER ROADS

DESIGN DESIGNATION	S.R. 43	C.R. 77
CURRENT ADT (2010)	N/A	N/A
DESIGN YEAR ADT (2030)	N/A	N/A
DESIGN HOURLY VOLUME (2030)	N/A	N/A
DIRECTIONAL DISTRIBUTION	N/A	N/A
TRUCKS (24 HOUR B&C)	N/A	N/A
DESIGN SPEED	35 MPH	35 MPH
LEGAL SPEED	35 MPH	35 MPH
DESIGN FUNCTIONAL CLASSIFICATION:	URBAN ARTERIAL	URBAN ARTERIAL
NHS PROJECT	NO	

DESIGN EXCEPTIONS:

NONE REQUIRED

CITY OF STEUBENVILLE

DEPARTMENT OF PUBLIC WORKS

JEF-43-1.97

PART 2

SUNSET BLVD. & JOHN SCOTT HIGHWAY INTERSECTION SAFETY UPGRADE

JEFFERSON COUNTY

CITY PROJECT NO. 2006-131

FOR PART 1, SEE JEF-43-1.97 SUNSET BOULEVARD RESURFACING IMPROVEMENTS

INDEX OF SHEETS:

PROJECT DESCRIPTION

IMPROVEMENT TO THE INTERSECTION OF S.R. 43 (SUNSET BOULEVARD) AND C.R. 77 (JOHN SCOTT HIGHWAY) VIA PAVEMENT RESURFACING AND RESTRIPING OF INTERSECTION APPROACHES, AND INSTALLATION OF ADA-COMPLIANT CURB RAMPS. THIS PROJECT WAS ORIGINALLY BID AS PART OF JEF-43-3.45, PID 82135.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA = N/A (MAINTENANCE PROJECT)
ESTIMATED CONTRACTOR EARTH DISTURBED AREA = N/A (MAINTENANCE PROJECT)
NOTICE OF INTENT EARTH DISTURBED AREA = N/A (MAINTENANCE PROJECT)

2008 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED _____ STEUBENVILLE CITY ENGINEER
DATE 3/30/10

APPROVED _____ STEUBENVILLE CITY MANAGER
DATE 03.30.10

Final Plans 4/5/10

STANDARD CONSTRUCTION DRAWINGS					SUPPLEMENTAL SPECIFICATIONS
					SEE PART 1
		SEE PART 1			
					SPECIAL PROVISIONS
					SEE PART 1

ENGINEERS SEAL:

STATE OF OHIO
KEVIN JOSEPH GRATHWOL
E-58282
REGISTERED
PROFESSIONAL ENGINEER

SIGNED: Kevin J Grathwol
DATE: 3/29/10

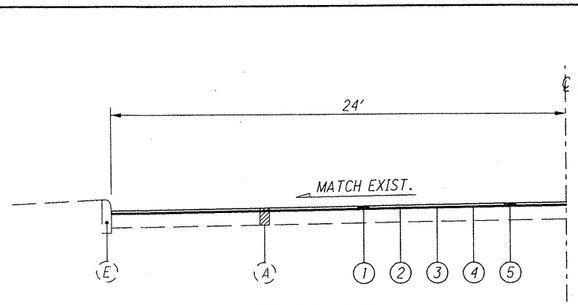
PLAN PREPARED BY:

W.E. STILSON CONSULTING GROUP
355 E. Campus View Blvd.
Columbus, Ohio 43235

UNDERGROUND UTILITIES
CONTACT BOTH SERVICES
CALL TWO WORKING DAYS
BEFORE YOU DIG
CALL
1-800-362-2764
(TOLL FREE)
OHIO UTILITIES PROTECTION SERVICE
NON-MEMBERS
MUST BE CALLED DIRECTLY
OIL & GAS PRODUCERS PROTECTIVE SERVICE CALL: 1-800-925-0988

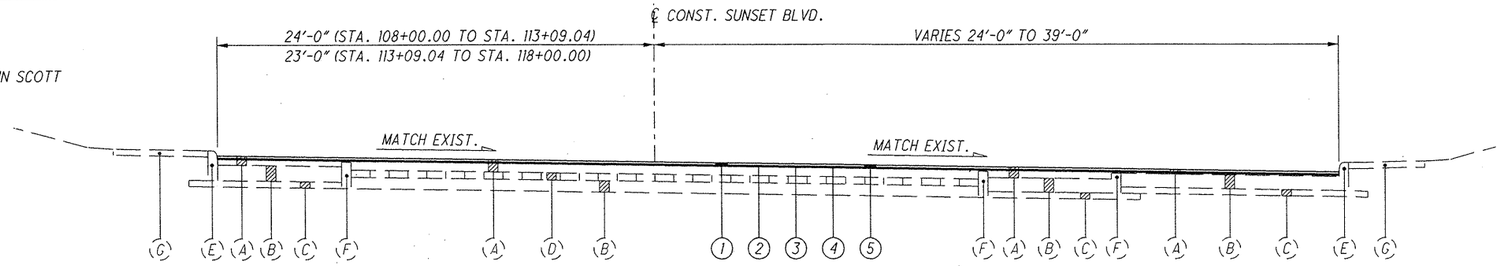
FEDERAL PROJECT NO. E091281
PID NO. 25577
CONSTRUCTION PROJECT NO.
RAILROAD INVOLVEMENT NONE
JEF-43-1.97
1/10

$TIME$
$DATE$
$FILE$



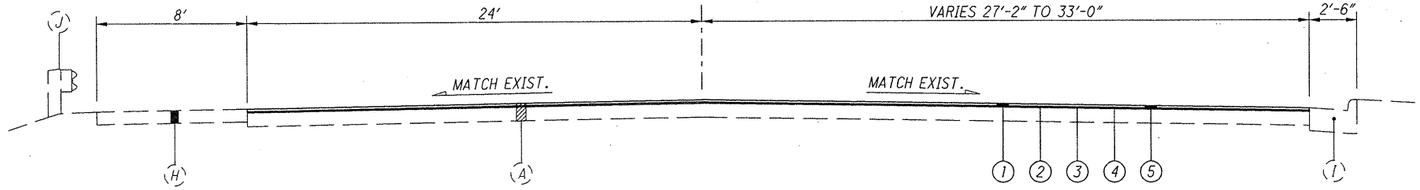
STATION LIMITS LEFT

STA. 5+76.00 TO STA. 9+61.94 = 385.94 FT.



RESURFACING SECTION - SUNSET BLVD.

STA. 108+00.00 TO STA. 118+00.00 = 1000.00 FT.



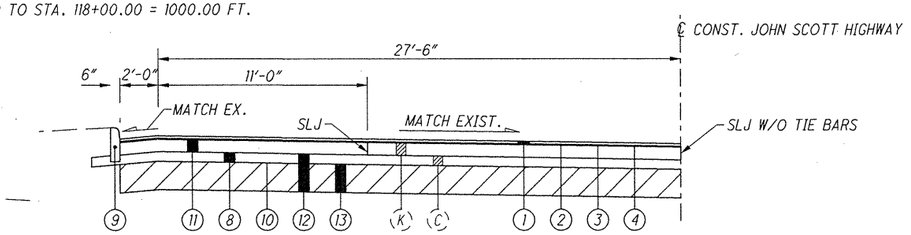
RESURFACING SECTION - JOHN SCOTT HIGHWAY

STATION LIMITS LEFT

STA. 5+00.00 TO STA. 5+76.00 = 76.00 FT.

STATION LIMITS RIGHT

STA. 5+00.00 TO STA. 9+61.94 = 461.94 FT.



STATION LIMITS LEFT

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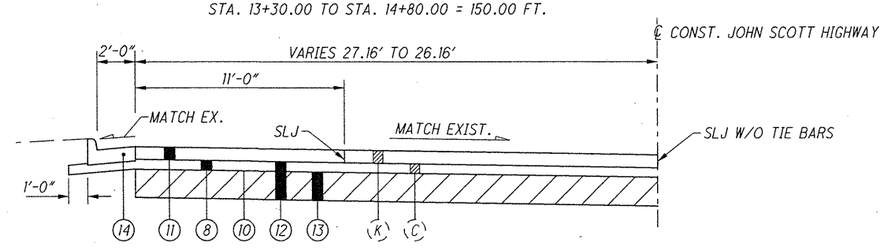
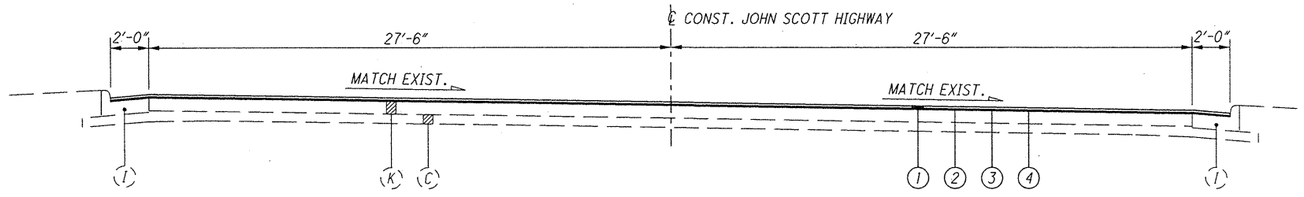


STATION LIMITS LEFT

STA. 15+00.00 TO STA. 15+30.00 = 30.00 FT.

RIGID PAVEMENT REPLACEMENT SECTION - JOHN SCOTT HIGHWAY



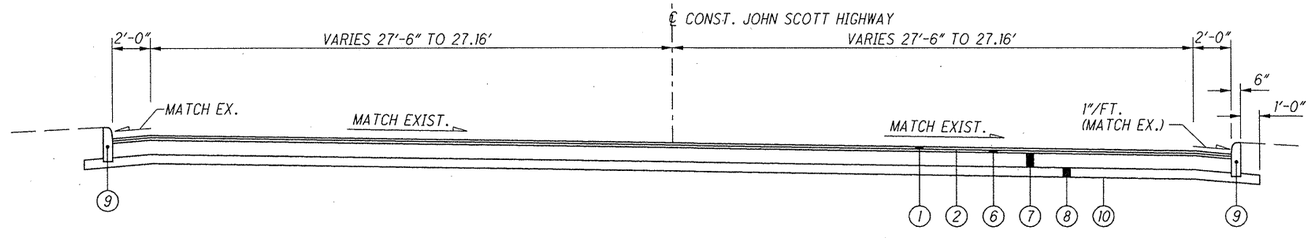
RESURFACING SECTION - JOHN SCOTT HIGHWAY

STATION LIMITS LEFT

STA. 10+42.00 TO STA. 13+30.00 = 288.00 FT.

STATION LIMITS RIGHT

STA. 10+42.00 TO STA. 14+80.00 = 438.00 FT.



FULL DEPTH PAVEMENT REPLACEMENT - JOHN SCOTT HIGHWAY

STA. 10+22.00 TO STA. 10+42.00 = 20.00 FT.

STA. 14+80.00 TO STA. 15+00.00 = 20.00 FT.

LEGEND

- (A) EXISTING ASPHALT
- (B) EXISTING CONCRETE BASE
- (C) EXISTING SUB-BASE
- (D) EXISTING BRICK
- (E) EXISTING CONCRETE CURB
- (F) PREVIOUS CONCRETE CURB
- (G) EXISTING CONCRETE WALK
- (H) EXISTING AGGREGATE SHOULDER
- (I) EXISTING CONCRETE CURB & GUTTER
- (J) EXISTING GUARDRAIL
- (K) EXISTING 8" CONCRETE PAVEMENT

- (1) ITEM 442 - 1 ½" ASPHALT CONCRETE SURFACE COURSE, 12.5mm, TYPE B (448), AS PER PLAN
- (2) ITEM 407 - TACK COAT FOR INTERMEDIATE COURSE (0.04 GAL./SQ.YD.)
- (3) ITEM 448 - ¾" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG64-22
- (4) ITEM 407 - TACK COAT (0.075 GAL./SQ.YD.)
- (5) ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (DEPTH = 2 ¼")
- (6) ITEM 448 - 1 ½" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG64-22
- (7) ITEM 301 - 9" ASPHALT CONCRETE BASE, PG64-22
- (8) ITEM 304 - 6" AGGREGATE BASE
- (9) ITEM 609 - CURB, TYPE 6
- (10) ITEM 204 - SUBGRADE COMPACTION
- (11) ITEM 255 - 8" FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS MS, AS PER PLAN
- (12) ITEM 204 - EXCAVATION OF SUBGRADE (24" DEPTH)
- (13) ITEM 204 - EMBANKMENT (18" DEPTH)
- (14) ITEM 609 - COMBINATION CURB AND GUTTER, TYPE 2

CALCULATED LAM
CHECKED KJG

TYPICAL SECTIONS

JEF-43-1.97

2
10

$TIMES
SQUATES
$FILES

GENERAL

A PRECONSTRUCTION CONFERENCE SCHEDULED BY THE CITY OF STEUBENVILLE ENGINEER SHALL BE HELD PRIOR TO ANY WORK STARTING. IN ADDITION THE CONTRACTOR SHALL PROVIDE (2) TWO WORKING DAYS NOTICE TO THE CITY OF STEUBENVILLE ENGINEER PRIOR TO BEGINNING WORK TO ARRANGE FOR INSPECTION.

EXISTING DATA

EACH CONTRACTOR SHALL VISIT THE SITE PERSONALLY TO ASCERTAIN THE NATURE OF THE WORK AND THOROUGHLY FAMILIARIZE HIMSELF WITH THE SITE PRIOR TO BID SUBMISSION.

EXISTING STRUCTURES, GRADES, PIPING, ETC. ARE INDICATED IN APPROXIMATE LOCATION ON THE PLAN. INFORMATION SHOWN IS NOT GUARANTEED TO BE CORRECT AND COMPLETE. THE DATA SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR. THE EXISTENCE OF FACILITIES ABOVE OR BELOW GROUND, WHICH MAY NOT BE SHOWN, WILL NOT BE A BASIS FOR A CLAIM FOR EXTRA WORK.

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

COLUMBIA GAS OF OHIO
300 LURAY DRIVE
WINTERSVILLE, OH 43953
740-266-2125

CITY OF STEUBENVILLE
238 S. LAKE ERIE AVE.
STEUBENVILLE, OH 43952
740-283-6041

AMERICAN ELECTRIC POWER
110 JOHN SCOTT HIGHWAY
STEUBENVILLE, OH 43952
740-266-3026

AT&T-OHIO
3935 NORTHPOINTE DR.
ZANESVILLE OH 43701
740-454-3455

COMCAST
100 WELDAY, SUITE A
WINTERSVILLE, OH 43953
740-346-2265

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. DUE TO THE NATURE OF THE WORK REQUIRED BY THIS PROJECT, THIS PLAN WAS DEVELOPED VIA EXISTING PLAN INFORMATION OBTAINED FROM THE CITY OF STEUBENVILLE IN LIEU OF DETAILED FIELD SURVEY. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT, OTHER THAN PLACEMENT OF PROPOSED SIGNAGE, WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

AT LEAST TWO WORKING DAYS PRIOR TO COMMENCING CONSTRUCTION OPERATIONS IN AN AREA WHICH MAY INVOLVE UNDERGROUND UTILITY FACILITIES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER, THE OHIO UTILITY PROTECTION SERVICE (1-800-362-2764) AND THE OWNERS OF EACH UNDERGROUND UTILITY FACILITY SHOWN IN THE PLANS. THE OWNER OF THE UNDERGROUND UTILITY FACILITY SHALL WITHIN FORTY EIGHT HOURS, EXCLUDING SATURDAYS, SUNDAYS AND LEGAL HOLIDAYS, AFTER NOTICE IS RECEIVED, STAKE, MARK, OR OTHERWISE DESIGNATE THE LOCATION OF THE UNDERGROUND UTILITY FACILITIES IN THE CONSTRUCTION AREA IN SUCH A MANNER AS TO INDICATE THEIR COURSE TOGETHER WITH APPROXIMATE DEPTH AT WHICH THEY WERE INSTALLED. THE MARKING OR LOCATING SHALL BE COORDINATED TO STAY APPROXIMATELY TWO DAYS AHEAD OF THE PLANNED CONSTRUCTION.

IF ANY CONFLICTS BETWEEN PROPOSED SIGNING AND EXISTING UNDERGROUND UTILITIES ARE IDENTIFIED FOLLOWING MARKING OF THE EXISTING UTILITIES, THE LOCATION OF THE PROPOSED SIGN(S) IN CONFLICT SHALL BE ADJUSTED BY THE ENGINEER AS DEEMED NECESSARY TO AVOID SAID CONFLICT.

STANDARD CONSTRUCTION DETAILS

STANDARD CONSTRUCTION DETAILS FOR "ODOT" ITEMS ARE AVAILABLE FROM THE OHIO DEPARTMENT OF TRANSPORTATION WEB SITE AT THE FOLLOWING ADDRESS:
HTTP://WWW.DOT.STATE.OH.US/DRRC

STANDARD CONSTRUCTION DETAILS FOR "CITY STD." ITEMS ARE INCLUDED IN THE SPECIFICATION BOOK.

EXISTING PLANS

EXISTING PLANS ENTITLED JEF-22-13.85 AND JEF-22-13.90 MAY BE INSPECTED IN THE CITY ENGINEER'S OFFICE IN STEUBENVILLE.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PREVIOUS CONSTRUCTION PLANS (NOTED ABOVE) SHOWING THE ORIGINAL ALIGNMENT AND PROFILE, ARE AVAILABLE FOR INSPECTION AT THE CITY OF STEUBENVILLE. PERFORM PROPOSED ASPHALT PLANING OPERATIONS AND PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY WITH A UNIFORM THICKNESS OF 1-1/2 INCHES AS SHOWN ON THE TYPICAL SECTIONS.

INSPECTION, TESTING, PAYMENT

ALL IMPROVEMENT WORK PERFORMED BY THE CONTRACTOR SHALL BE IMMEDIATELY INSPECTED FOLLOWING COMPLETION AND THE CONTRACTOR SHALL SCHEDULE ALL WORK REQUIRED IN A MANNER WHICH WILL MAKE POSSIBLE COMPLETE INSPECTION OF THE WORK INCLUDING TESTING. NO ESTIMATE FOR PAYMENT WILL BE PREPARED FOR WORK WHICH HAS NOT BEEN INSPECTED AND LOGGED BY THE ENGINEER IN ANY AND ALL RESPECTS AND THIS CONDITION WILL NOT BE WAVED BY THE OWNER UNDER ANY CIRCUMSTANCES.

FAILURE OR NEGLECT ON THE PART OF THE ENGINEER TO CONDEMN OR REJECT INFERIOR WORK OR MATERIALS SHALL NOT BE CONSTRUED TO IMPLY ACCEPTANCE OF SUCH WORK OR MATERIALS IF SUCH BAD OR INFERIOR MATERIALS OR WORK BECOME EVIDENT AT ANY TIME PRIOR TO FINAL ACCEPTANCE OF THE WORK AND THE RELEASE OF THE CONTRACTOR BY THE OWNER. NOR SHALL IT BE CONSTRUED AS BARRING THE OWNER AT ANY SUBSEQUENT TIME FROM RECOVERY OF DAMAGES IN SUCH SUM OF MONEY AS MAY BE NEEDED TO BUILD ANEW ALL PORTIONS OF THE WORK IN WHICH FRAUD WAS PRACTICED OR IMPROPER MATERIALS HIDDEN, WHENEVER FOUND.

SAFETY PROCEDURES

THE CONTRACTOR SHALL COMPLY WITH ALL FEDERAL OCCUPATIONAL, SAFETY AND HEALTH ADMINISTRATION (OSHA) REQUIREMENTS DURING ALL PHASES OF CONSTRUCTION.

NOTIFICATION OF SAFETY FORCES

THE CONTRACTOR SHALL NOTIFY THE CITY POLICE AND FIRE DEPARTMENTS AND LOCAL SCHOOL DISTRICT TRANSPORTATION SUPERVISOR AT LEAST 48 HOURS IN ADVANCE OF ANY TRAFFIC CHANGE.

PRESERVATION OF PROPERTY CORNERS AND SURVEY MARKERS

THE CONTRACTOR SHALL PRESERVE ALL CORNERSTONES, IRON PINS, CONCRETE MONUMENTS OR ANY TYPE OF LAND MONUMENT. HE SHALL HAVE ALL LAND MONUMENTS IN THE PROXIMITY OF THE WORK REFERENCED. HE SHALL REPLACE DESTROYED OR DAMAGED MONUMENTS AND SHALL FURNISH A CERTIFICATION BY AN OHIO REGISTERED SURVEYOR THAT THE MONUMENTS HAVE BEEN RESTORED.

USE OF PRIVATE PROPERTY

THE CONTRACTOR CAN USE PRIVATE PROPERTY FOR THE STORAGE OF EQUIPMENT AND MATERIALS ONLY WITH A WRITTEN AGREEMENT WITH THE PROPERTY OWNER. THE CONTRACTOR SHALL PROVIDE THIS AGREEMENT TO THE ENGINEER PRIOR TO ANY USE OF PRIVATE PROPERTY.

ITEM 608 - 4" CONCRETE WALK, AS PER PLAN

THIS ITEM SHALL CONFORM TO THE REQUIREMENTS OF ODOT ITEM 608 WITH MODIFICATIONS AS PER THE CITY OF STEUBENVILLE GENERAL SPECIFICATION FOR FIBROUS CONCRETE ALONG WITH ADDITIONAL REQUIREMENTS AS PER THE CITY OF STEUBENVILLE SPECIFICATIONS FOR SIDEWALK.

ALL NEW WALK AND CURB RAMPS SHALL BE INSTALLED OVER A 3" THICK LAYER OF ITEM 304, AGGREGATE BASE. THE COST OF SAID AGGREGATE BASE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 608, 4" CONCRETE WALK, AS PER PLAN.

ITEM 608 - CURB RAMP, TYPE B1(B2), AS PER PLAN

THIS ITEM SHALL CONFORM TO THE REQUIREMENTS OF ODOT ITEM 608 WITH MODIFICATIONS AS PER THE CITY OF STEUBENVILLE GENERAL SPECIFICATION FOR FIBROUS CONCRETE ALONG WITH ADDITIONAL REQUIREMENTS AS PER THE CITY OF STEUBENVILLE SPECIFICATIONS FOR SIDEWALK.

ALL NEW WALK AND CURB RAMPS SHALL BE INSTALLED OVER A 3" THICK LAYER OF ITEM 304, AGGREGATE BASE. THE COST OF SAID AGGREGATE BASE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 608, CURB RAMP, TYPE B1(B2), AS PER PLAN.

ITEM 442, ASPHALT CONCRETE SURFACE COURSE, 12.5 mm, TYPE B (448), AS PER PLAN

COURSE AGGREGATE USED IN ITEM 442, ASPHALT CONCRETE SURFACE COURSE, 12.5 mm, TYPE B (448), AS PER PLAN, SHALL BE AIR-COOLED BLAST FURNACE SLAG.

MANHOLES AND VALVE BOXES ADJUSTED TO GRADE

THE FOLLOWING CONTINGENCY QUANTITIES HAVE BEEN PROVIDED IN THE PLANS FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 604, MANHOLE ADJUSTED TO GRADE 6 EACH

ITEM 638, VALVE BOX ADJUSTED TO GRADE 3 EACH

CONTRACTOR'S RESPONSIBILITIES

CONTRACTOR SHALL SUPERVISE AND DIRECT THE WORK COMPETENTLY AND EFFICIENTLY DEVOTING SUCH ATTENTION THERETO AND APPLYING SUCH SKILLS AND EXPERTISE AS MAY BE NECESSARY TO PERFORM THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE MEANS, METHODS, TECHNIQUES, SEQUENCES AND PROCEDURES OF CONSTRUCTION. CONTRACTOR SHALL NOT BE RESPONSIBLE FOR THE NEGLIGENCE OF OTHERS IN THE DESIGN OR SELECTION OF A SPECIFIC MEANS, METHOD, TECHNIQUE, SEQUENCE OR PROCEDURE OF CONSTRUCTION WHICH IS INDICATED IN AND REQUIRED BY THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE TO SEE THAT THE FINISHED WORK COMPLIES ACCURATELY WITH THE CONTRACT DOCUMENTS.

CONTRACTOR SHALL KEEP ON THE WORK AT ALL TIMES DURING ITS PROGRESS A COMPETENT RESIDENT SUPERINTENDENT, WHO SHALL NOT BE REPLACED WITHOUT WRITTEN NOTICE TO OWNER AND ENGINEER EXCEPT UNDER EXTRAORDINARY CIRCUMSTANCES. THE RESIDENT MUST BE PRESENT DURING ALL WORK, THIS NOT ONLY INCLUDES THE CONTRACTOR'S WORK BUT ALSO ALL OF HIS SUB-CONTRACTORS WORK. THE SUPERINTENDENT WILL BE THE CONTRACTOR'S REPRESENTATIVE AT THE SITE AND SHALL HAVE AUTHORITY TO ACT ON BEHALF OF CONTRACTOR. ALL COMMUNICATIONS GIVEN TO THE SUPERINTENDENT SHALL BE BINDING AS IF GIVEN TO THE CONTRACTOR.

THE CONTRACTOR SHALL PROVIDE AT LEAST THREE (3) RESPONSIBLE WORKERS FROM THE PROJECT TO ANSWER EMERGENCY CALLS (AND THE CALL ORDER IN WHICH THEY RECEIVED SUCH CALLS) AND PERFORM EMERGENCY SERVICE DURING NON-WORKING HOURS FOR ANY CONDITION RESULTING FROM THE CONTRACTORS CONSTRUCTION ACTIVITIES WHICH REPRESENT A HAZARD TO THE PUBLIC. THESE WORKERS SHALL BE PROVIDED WITH PRIVATE TELEPHONE SERVICE TO BE USED EXCLUSIVELY FOR THIS PURPOSE AND SHALL MAKE THEMSELVES AVAILABLE AT ANY TIME OF THE DAY OR NIGHT AND ANY DAY OF THE WEEK FOR THIS EMERGENCY WORK. THESE WORKERS SHALL ALSO BE PROVIDED WITH A PROPER VEHICLE, SUPPLIES AND MATERIALS AND BE GIVEN SUFFICIENT AUTHORITY TO ADEQUATELY PERFORM THIS EXTENDED ASSIGNMENT.

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS MS, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF ITEM 255, THE FOLLOWING SHALL APPLY:

1) ALL JOINTS SHALL BE SKEWED 4 FEET RIGHT FORWARD FOR A 24 FOOT WIDTH TO MATCH THE EXISTING JOINT ORIENTATION. DOWEL BARS SHALL BE INSTALLED PARALLEL TO THE LONGITUDINAL JOINT. TIE BARS OR HOOK BOLTS INSTALLED ACROSS THE LONGITUDINAL JOINT SHALL BE PARALLEL TO THE TRANVERSE JOINTS.

2) COST OF ALL FULL DEPTH PAVEMENT SAWING SHALL BE INCLUDED IN THE COST OF THE PAVEMENT REPLACEMENT. NO SEPARATE PAYMENT WILL BE MADE FOR PAVEMENT SAWING.

3) FOLLOWING REMOVAL OF THE EXISTING CONCRETE PAVEMENT, EXISTING SUBBASE AND SUBGRADE SHALL BE UNDERCUT TO A DEPTH OF TWO FEET BELOW EXISTING PAVEMENT, AND REPLACED WITH 18" OF ITEM 204, EMBANKMENT AND 6" OF ITEM 304 AGGREGATE BASE AS SHOWN ON THE TYPICAL SECTIONS. SEPARATE PAYMENT FOR UNDERCUTTING AND BACKFILLING OF UNDERCUT AREAS SHALL BE PROVIDED UNDER ITEMS 204 AND 304 AS QUANTIFIED IN THE PLANS.

4) CONTRACTOR SHALL BE RESPONSIBLE FOR TESTING OF THE CONCRETE, TO INCLUDE AIR CONTENT, SLUMP, AND PREPARATION AND TESTING OF BEAMS VIA THREE POINT FLEXURAL TEST TO VERIFY ATTAINMENT OF REQUIRED MODULUS OF RUPTURE OF 400 POUNDS PER SQUARE INCH PRIOR TO OPENING OF THE ROADWAY TO TRAFFIC.

ITEM 604, CATCH BASIN, NO. 3, AS PER PLAN

IN ORDER TO ACCOMMODATE EXISTING PIPE SIZES, INVERTS, AND CONFIGURATIONS, CATCH BASIN SHALL BE CAST-IN-PLACE.

THE PROPOSED ELEVATIONS FOR THE STRUCTURE SHALL BE COORDINATED WITH THE CITY ENGINEER PRIOR TO BEGINNING WORK ON THIS ITEM.

PAVEMENT RESTORATION FOR DRAINAGE STRUCTURE INSTALLATIONS

THE FOLLOWING QUANTITY IS PROVIDED FOR PAVEMENT RESTORATION FOLLOWING INSTALLATION OF ITEM 604, CATCH BASIN, NO. 3, AS PER PLAN.

ITEM 305, 9" CONCRETE BASE 5 SQ. YD.

THE ABOVE QUANTITY IS BASED ON A 305 THICKNESS OF NINE INCHES AND A WIDTH OF TWO FEET AROUND THE PERIMETER OF THE DRAINAGE STRUCTURE.

PROVIDE ANY MATERIALS USED OUTSIDE THE LIMITS STATED ABOVE AT NO ADDITIONAL COST.

PAVEMENT RESTORATION FOR PIPE INSTALLATIONS

THE FOLLOWING QUANTITY HAS BEEN PROVIDED FOR PAVEMENT RESTORATION FOLLOWING INSTALLATION OF PIPES UNDER ITEM 603.

ITEM 305, 9" CONCRETE BASE 14 SQ. YD.

THE ABOVE QUANTITY IS BASED ON A 305 THICKNESS OF NINE INCHES AND A PAVEMENT RESTORATION WIDTH THAT INCLUDES THE TRENCH WIDTH PLUS TWO FEET ON EACH SIDE OF THE TRENCH. SEE STANDARD CONSTRUCTION DRAWING DM-1.4 FOR TRENCH WIDTH FORMULA AND CALCULATION.

PROVIDE ANY MATERIALS USED OUTSIDE THE LIMITS STATED ABOVE AT NO ADDITIONAL COST.

ITEM SPECIAL - MISC.: PAVEMENT REINFORCEMENT MESH GRID

THIS WORK CONSISTS OF PLACEMENT OF A SELF ADHESIVE GLASS FIBER MESH OVER EXISTING CONCRETE PAVEMENT ON JOHN SCOTT HIGHWAY (EXCLUDING RECONSTRUCTED PAVEMENT SECTIONS) PRIOR TO PLACEMENT OF ASPHALT CONCRETE SURFACE COURSE.

MATERIAL:

FURNISH HIGH STRENGTH OPEN FIBER MESH GRID BETWEEN PAVEMENT LAYERS THAT MEETS THE FOLLOWING PROPERTIES:

PROPERTIES	
MATERIAL	FIBER GLASS REINFORCEMENT WITH MODIFIED POLYMER COATING AND PRESSURE SENSITIVE ADHESIVE BACKING
TENSILE STRENGTH ACROSS WIDTH TEST METHOD ASTM D 6637	560 LBS/IN.
TENSILE STRENGTH ACROSS LENGTH TEST METHOD ASTM D 6637	560 LBS/IN.
ELONGATION AT BREAK TEST METHOD ASTM D 6637	< 3 %
MELTING POINT TEST METHOD ASTM D 276	> 425° F
MASS/UNIT AREA TEST METHOD ASTM D 5261-92	11 OZ. / SQ. YD.
GRID PATTERN	0.5 IN. x 0.5 IN.

BEFORE INSTALLATION SUBMIT A LETTER TO THE ENGINEER WITH A STATEMENT CERTIFYING MATERIAL RECEIVED MEETS THE ABOVE PROPERTIES. SUBMIT TO THE ENGINEER ACTUAL DATED (SALES FLYER DATA NOT ACCEPTABLE) TEST DATA WITH THE CERTIFICATION LETTER.

CONSTRUCTION:

1) TACK EXISTING CONCRETE PAVEMENT TO BE RESURFACED

2) PLACE ASPHALT CONCRETE INTERMEDIATE LEVELING COURSE

3) TACK ASPHALT CONCRETE INTERMEDIATE LEVELING COURSE

3) PLACE PAVEMENT REINFORCEMENT MESH GRID OVER INTERMEDIATE LEVELING COURSE ACROSS THE ENTIRE WIDTH OF PAVEMENT TO BE RESURFACED.

4) PLACE ASPHALT CONCRETE SURFACE COURSE

ENSURE ALL AREAS WHERE MESH IS TO BE PLACED ARE FREE OF ALL DIRT AND OTHER LOOSE MATERIALS BY SWEEPING OR OTHER APPROVED METHOD. PLACE THE MESH ON A PAVEMENT SURFACE THAT IS BETWEEN 40°F AND 140°F. ALLOW FOR TACK CURE BEFORE PLACING MESH.

PLACE MESH UNDER TENSION TO PREVENT RIPPLING. REMOVE RIPPLES BY PULLING, OR IF NECESSARY (IN CURVES FOR EXAMPLE), BY CUTTING AND FLATTENING THE MESH. OVERLAP TRANSVERSE JOINTS OF THE MESH 3 TO 6 INCHES. OVERLAP LONGITUDINAL JOINTS OF THE MESH BY 1 INCH. ROLL THE MESH SURFACE 2 PASSES WITH A RUBBER COATED DRUM ROLLER, RUBBER TIRED ROLLER OR OTHER METHOD ACCEPTABLE TO THE MANUFACTURER. CLEAN RUBBER ROLLER IF BUILDUP ON THE RUBBER SURFACE INTERFERES WITH MESH PLACEMENT. DO NOT USE A STEEL DRUM ROLLER.

PLACED MESH WILL HANDLE SPEED CONTROLLED EMERGENCY OR CONSTRUCTION TRAFFIC BUT DAMAGED SECTIONS MUST BE REMOVED AND/OR REPAIRED. DO NOT ALLOW MUD OR OTHER MATERIAL TO COLLECT ON THE MESH PRIOR TO ASPHALT CONCRETE PLACEMENT. COVER MESH WITH ASPHALT CONCRETE THE SAME DAY UNLESS WEATHER BECOMES UNSUITABLE.

THE FOLLOWING QUANTITY HAS BEEN INCLUDED IN THE GENERAL SUMMARY FOR WORK NOTED ABOVE:

ITEM SPECIAL - MISC.: PAVEMENT REINFORCEMENT MESH GRID 2380 SQ. YD.

CALCULATED LAM CHECKED KJG

GENERAL NOTES

JEF - 43 - 1.97

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S:\PRODUCTION\PROJECTS\099905.00 Steubenville JEF-43-0.96\Roadway\John Scott Resurfacing\GEN-NOTES.dwg, Notes1, 3/31/2010 11:30:21 AM

ITEM 614, MAINTAINING TRAFFIC

ALL LANES SHALL BE OPEN TO TRAFFIC BETWEEN THE HOURS OF 7:00 A.M TO 9:00 A.M. AND 4:00 P.M. TO 6:00 P.M., MONDAY THROUGH FRIDAY, EXCEPT AS NOTED BELOW AND ON SHEET 4A.

DURING PERIODS OF TIME WHEN THE CONTRACTOR IS WORKING WITHIN OR ADJACENT TO THE ROADWAY, A MINIMUM OF ONE (1) LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT AND THE COMPLETED PAVEMENT. REQUIRED LANE CLOSURES SHALL BE IN ACCORDANCE WITH STANDARD CONSTRUCTION DRAWINGS MT-95.31 AND MT-95.32. ALL TRAFFIC LANES SHALL BE RE-OPENED TO TRAFFIC AT THE END OF THE WORKING DAY.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

THE CONTRACTOR SHALL CLOSE THE WESTBOUND/SOUTHBOUND LANES OF JOHN SCOTT HIGHWAY AS DETAILED ON SHEET 4A FOR A PERIOD NOT TO EXCEED 14 CONSECUTIVE CALENDAR DAYS IN ORDER TO COMPLETE FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT. LIQUIDATED DAMAGES SHALL BE ASSESSED IN ACCORDANCE WITH CMS 108.07 FOR EACH CALENDAR DAY THAT THE LANES REMAIN CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

THE CONTRACTOR SHALL NOTIFY THE CITY OF STEUBENVILLE SEVEN (7) DAYS PRIOR TO ANY LOOP DETECTOR BEING TAKEN OUT OF SERVICE.

ALL PERMANENT AND TEMPORARY PAVEMENT MARKINGS ARE TO BE MAINTAINED THROUGHOUT THE PROJECT. ANY DAMAGED OR FADED PAVEMENT MARKINGS SHALL BE CORRECTED WITHIN EIGHT HOURS AFTER THE CONTRACTOR'S NOTIFICATION OF THE PROBLEM. IF WEATHER IS NOT CONDUCIVE TO PAVEMENT MARKING INSTALLATION, THEN REPAIRS SHALL BE MADE ON THE FIRST WEATHER PERMISSIVE DAY AFTER NOTIFICATION.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NIGHTTIME PAVING

IN ORDER TO MINIMIZE INCONVENIENCE TO THE TRAVELING PUBLIC THROUGH THIS HEAVILY TRAVELED INTERSECTION, PAVEMENT MILLING, RESURFACING AND FULL-DEPTH RECONSTRUCTION OPERATIONS (EXCEPT AS NOTED ON SHEET 4A) SHALL BE PERFORMED BETWEEN THE HOURS OF 9:00 P.M. TO 7:00 A.M. ALL TRAFFIC LANES SHALL BE RE-OPENED TO TRAFFIC BY 7:00 A.M.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

COORDINATION OF RESURFACING AND PLANING OPERATIONS

ONCE THE PAVEMENT PLANING OPERATION HAS BEGUN, IT SHALL PROCEED CONTINUOUSLY UNTIL ALL ELEMENTS OF THE WORK ASSOCIATED WITH THE PAVEMENT PLANING OPERATION ARE COMPLETED. THE PAVEMENT PLANING OPERATION SHALL BE COMPLETED IN A TIMELY MANNER AS DIRECTED BY THE ENGINEER. IN ORDER TO MEET THE REQUIREMENTS OF STANDARD CONSTRUCTION DRAWING MT-101.90 (DROP-OFFS IN WORK ZONES) AND BETTER MAINTAIN THE INTEGRITY OF THE UNDERLYING PAVEMENT, PLACEMENT OF THE INTERMEDIATE RESURFACING COURSE SHALL PROCEED IN A CONTINUOUS OPERATION BEHIND THE PAVEMENT PLANING OPERATION, SUCH THAT THE PLANED SURFACE IS EXPOSED TO TRAFFIC NO MORE THAN ONE (1) DAY PRIOR TO PLACEMENT OF THE INTERMEDIATE RESURFACING COURSE.

THROUGHOUT RESURFACING OPERATIONS, THE CONTRACTOR SHALL PROTECT EXPOSED MANHOLE CASTINGS VIA MANHOLE PROTECTOR RINGS OR ASPHALT WEDGES.

ITEM 614 – LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

- DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP). IN GENERAL, LEOS SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.

- WHEN CONSTRUCTION VEHICLES ARE ENTERING/EXITING THE ZONE DIRECTLY FROM/INTO AN OPEN LANE OF TRAFFIC. IF A LANE HAS BEEN CLOSED TO PROVIDE AN ACCELERATION/ DECELERATION LANE FOR THE VEHICLE, THE LEO WILL NOT BE REQUIRED.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THESE SERVICES WITH:

CITY OF STEUBENVILLE POLICE DEPARTMENT
123 SOUTH 3RD STREET
STEUBENVILLE, OHIO 43952
TEL. (740) 283-6000, EXT. 2300 – MAIN LINE
TEL. (740) 283-6000, EXT. 2900 – EMERGENCY ONLY POLICE DISPATCH (OR 911)

LEOS WITH PATROL CAR REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	24 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

WORK ZONE MARKINGS AND SIGNS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF CMS 614.04 AND 614.11.

ITEM 614, WORK ZONE CENTER LINE, CLASS II	0.48 MILES
ITEM 614, WORK ZONE LANE LINE, CLASS II	0.67 MILES
ITEM 614, WORK ZONE STOP LINE, CLASS I	230 FEET
ITEM 614, WORK ZONE CHANNELIZING LINE, CLASS I	1250 FEET

PROVIDING CONTINUOUS ACCESS TO RESIDENCES AND BUSINESSES

INGRESS AND EGRESS TO INDIVIDUAL PROPERTIES LOCATED WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED AT ALL TIMES. PROPERTIES WITH MULTIPLE DRIVES MAY HAVE ONE DRIVE CLOSED AT A TIME WHILE WORK IS PERFORMED IN THE AREA OF THE CLOSED DRIVE. IF DRIVEWAY CLOSURE IS NECESSARY TO ENABLE WORK ON OR IN FRONT OF A DRIVEWAY, THE CONTRACTOR SHALL GIVE THE PROPERTY OWNER NOTIFICATION 48 HOURS IN ADVANCE OF SUCH CLOSURE AND SHALL MAKE EVERY EFFORT TO ACCOMMODATE THE OWNER'S NEED FOR ACCESS. CLOSURE IS PERMITTED ONLY DURING WORK HOURS AND ACCESS MUST BE RETURNED AT THE END OF EACH WORKING DAY.

FAILURE TO COMPLY

FOR ANY FAILURE TO COMPLY WITH PROVISIONS FOR TRAFFIC CONTROL SET OUT IN THESE PLANS AND NOTES OR WITH THE PROVISIONS OF THE MANUAL, THE ROADWAY IN THE VICINITY OF THE WORK AREA SHALL BE CONSIDERED IN A CONDITION UNACCEPTABLE FOR THE SAFETY AND CONVENIENT USE BY THE TRAVELING PUBLIC. ANY FAILURE TO KEEP THE ROADWAY IN THE VICINITY OF THE WORKING AREA IN A CONDITION ACCEPTABLE FOR THE SAFE AND CONVENIENT USE BY THE TRAVELING PUBLIC SHALL BE CONSIDERED A BREACH OF THIS CONTRACT. WORK SHALL BE SUSPENDED UNTIL THE CONTRACTOR COMPLIES WITH THE PROVISIONS OF THE AFOREMENTIONED ITEMS.

COOPERATION BETWEEN CONTRACTORS / CONSTRUCTION SEQUENCE

CURRENT WITH THIS PROJECT, THE CITY OF STEUBENVILLE WILL BE COMPLETING TRAFFIC SIGNAL IMPROVEMENTS UNDER SEPARATE PROJECT JEF-43-1.09 THAT WILL REPLACE ALL EXISTING LOOP DETECTION FROM BELLEVIEW BOULEVARD TO LOVERS LANE WITH VIDEO DETECTION. SAID PLANS REQUIRE THE SIGNAL CONTRACTOR TO COMPLETE WORK ON CLOSED LOOP SYSTEM "A" THAT EXTENDS FROM RICHLAND AVENUE TO LOVERS LANE PRIOR TO BEGINNING WORK ON SYSTEM "B" THAT IS BEYOND THE LIMTS OF THE PROPOSED RESURFACING IMPROVEMENTS. IN ADDITION, THE SIGNAL PLANS DIRECT THE CONTRACTOR TO COMPLETE WORK AT THE HIGH TRAFFIC VOLUME INTERSECTIONS OF JOHN SCOTT HIGHWAY AND LOVERS LANE (JOHNSON ROAD INTERSECTION ALREADY INCLUDES VIDEO DETECTION) PRIOR TO WORKING AT THE REMAINING SECONDARY INTERSECTIONS.

IN ORDER TO MINIMIZE DISRUPTION OF THE SR-43 TRAFFIC SIGNAL SYSTEM DURING CONSTRUCTION OF THESE IMPROVEMENTS, THE CONTRACTOR SHALL NOT BEGIN PAVEMENT PLANING AND RESURFACING OPERATIONS PRIOR TO AUGUST 1, 2010. FURTHERMORE, THE CONTRACTOR SHALL BEGIN PAVEMENT PLANING AND RESURFACING AT THE EAST END OF THE PROJECT (PART 1) AND PROCEDE WEST. IT IS THE INTENT OF THE CITY TO HAVE VIDEO DETECTION IMPROVEMENTS COMPLETED AND OPERATIONAL AT THE INTERSECTION OF JOHN SCOTT HIGHWAY PRIOR TO PAVING OPERATIONS REMOVING THE EXISTING LOOPS AT THE INTERSECTION.

IN ACCORDANCE WITH SECTION 105.08 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS, SEPARATE CONTRACTORS WORKING WITHIN THE LIMITS OF THE PROJECT SHALL CONDUCT THEIR WORK WITHOUT INTERFERING WITH OR HINDERING THE PROGRESS OR COMPLETION OF WORK BEING PERFORMED BY OTHER CONTRACTORS AND SHALL COOPERATE WITH EACH OTHER AS DIRECTED BY THE ENGINEER.

CALCULATED LAM | CHECKED KJG

MAINTENANCE OF TRAFFIC NOTES

JEF - 43 - 1.97

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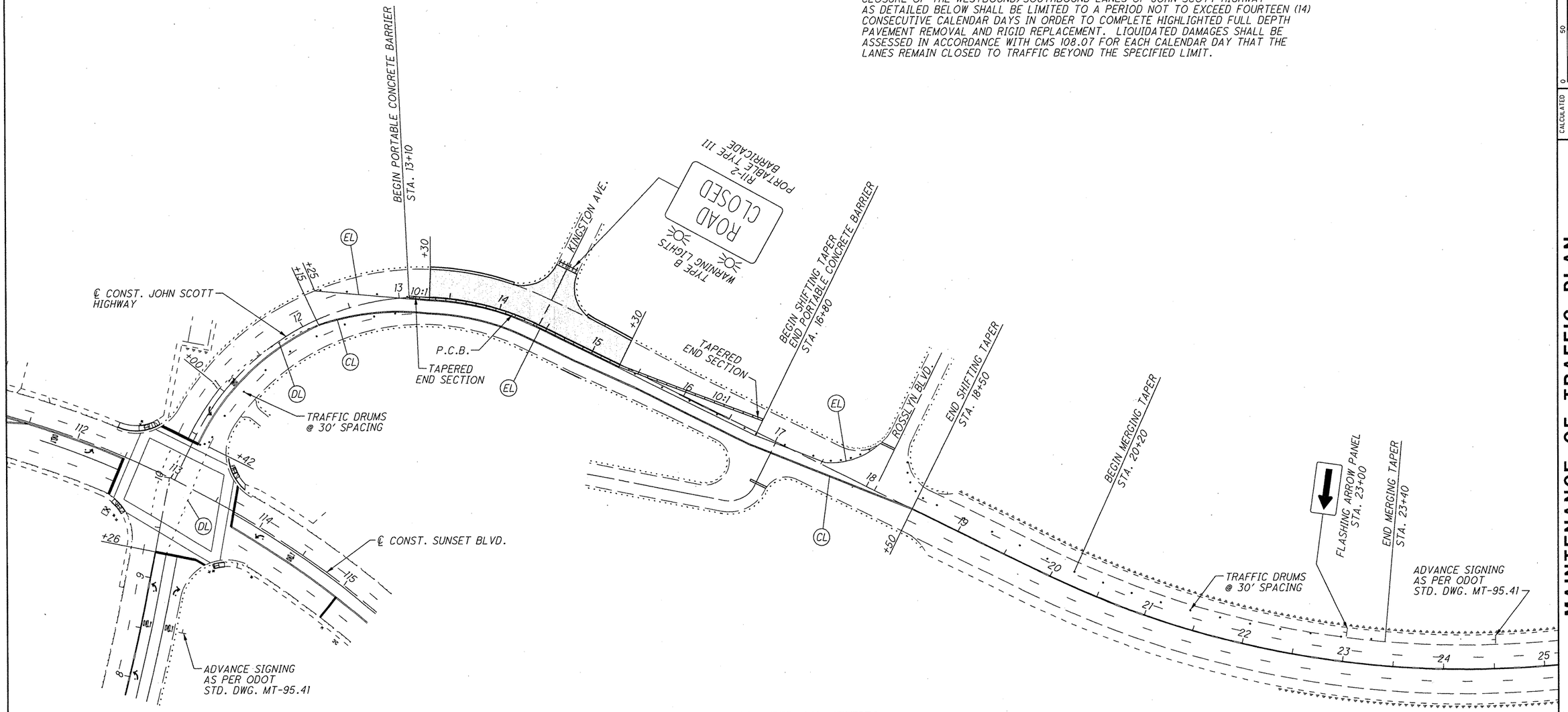
NOTE:

CLOSURE OF THE WESTBOUND/SOUTHBOUND LANES OF JOHN SCOTT HIGHWAY AS DETAILED BELOW SHALL BE LIMITED TO A PERIOD NOT TO EXCEED FOURTEEN (14) CONSECUTIVE CALENDAR DAYS IN ORDER TO COMPLETE HIGHLIGHTED FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT. LIQUIDATED DAMAGES SHALL BE ASSESSED IN ACCORDANCE WITH CMS 108.07 FOR EACH CALENDAR DAY THAT THE LANES REMAIN CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.



ESTIMATED QUANTITIES

ITEM	TOTAL	UNIT	ITEM DESCRIPTION
614	9	EACH	BARRIER REFLECTOR, TYPE B2
614	8	EACH	OBJECT MARKER, TWO WAY
614	0.12	MILE	WORK ZONE, CENTER LINE, CLASS I
614	0.11	MILE	WORK ZONE, EDGE LINE, CLASS I
614	231	FT.	WORK ZONE, DOTTED LINE, CLASS I
622	370	FT.	PORTABLE CONCRETE BARRIER, 32"
644	0.12	MILE	LANE LINE
644	0.08	MILE	CENTER LINE
644	20	FT.	TRANSVERSE / DIAGONAL LINE

TOTALS CARRIED TO GENERAL SUMMARY

LEGEND

(CL) WORK ZONE CENTER LINE, CLASS I
(DL) WORK ZONE DOTTED LINE, CLASS I
(EL) WORK ZONE EDGE LINE, CLASS I

CALCULATED LMO
CHECKED KJG

0 25 50 100 HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC PLAN
SUNSET BOULEVARD & JOHN SCOTT HIGHWAY

JEF-43-1.97

4A
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$TIME$
$DATE$
$FILE$

SHEET NUMBER	OFFICE CALC'S	ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
						ROADWAY	
	330	202	23000	330	SQ YD	PAVEMENT REMOVED	
442		202	30000	442	SQ FT	WALK REMOVED	
17		202	32000	17	FT	CURB REMOVED	
236		202	32500	236	FT	CURB AND GUTTER REMOVED	
24		202	35100	24	FT	PIPE REMOVED, 24" AND UNDER	
1		202	58100	1	EACH	CATCH BASIN REMOVED	
	939	204	10000	939	SQ YD	SUBGRADE COMPACTION	
	376	204	13000	376	CU YD	EXCAVATION OF SUBGRADE	
	282	204	20000	282	CU YD	EMBANKMENT	
445		608	10001	445	SQ FT	4" CONCRETE WALK, AS PER PLAN	3
2		608	52131	2	EACH	CURB RAMP, TYPE B1, AS PER PLAN	3
2		608	52141	2	EACH	CURB RAMP, TYPE B2, AS PER PLAN	3
						DRAINAGE	
24		603	04400	24	FT	12" CONDUIT, TYPE B	
1		604	00401	1	EACH	CATCH BASIN, NO. 3, AS PER PLAN	3
6		604	34500	6	EACH	MANHOLE ADUSTED TO GRADE	
						PAVEMENT	
	9612	254	01000	9612	SQ YD	PAVEMENT PLANING, ASPHALT CONCRETE	
	564	255	10151	564	SQ YD	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS MS, AS PER PLAN	3
	89	301	46000	89	CU YD	ASPHALT CONCRETE BASE, PG64-22	
	161	304	20000	161	CU YD	AGGREGATE BASE	
19		305	13000	19	SQ YD	9" CONCRETE BASE	
	943	407	10000	943	GALLON	TACK COAT	
	517	407	14000	517	GALLON	TACK COAT FOR INTERMEDIATE COURSE	
	539	442	20051	539	CU YD	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE B (448), AS PER PLAN	3
	277	448	46050	277	CU YD	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG64-22	
42		609	12000	42	FT	COMBINATION CURB AND GUTTER, TYPE 2	
211		609	26000	211	FT	CURB, TYPE 6	
2380		SPECIAL	69098300	2380	SQ YD	SPECIAL - MISC.: PAVEMENT REINFORCEMENT MESH GRID	3

CALCULATED L.MO
CHECKED K.JG

GENERAL SUMMARY

JEF-43-1.97

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$TIME$
$DATE$
$FILE$

SHEET NUMBER													ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
		3	4	4A			9											
		3											638	10800	3	EACH	VALVE BOX ADJUSTED TO GRADE	
																	TRAFFIC CONTROL	
							53						621	10000	53	EACH	RPM, LOW PROFILE, YELLOW/YELLOW	
							81						621	10020	81	EACH	RPM, LOW PROFILE, WHITE/RED	
							34						621	54000	34	EACH	RAISED PAVEMENT MARKER REMOVED	
				0.12			0.68						644	00200	0.80	MILE	LANE LINE	
				0.08			0.49						644	00300	0.57	MILE	CENTER LINE	
							1349						644	00400	1349	FT	CHANNELIZING LINE	
							234						644	00500	234	FT	STOP LINE	
							651						644	00600	651	FT	CROSSWALK LINE	
				20			300						644	00700	320	FT	TRANSVERSE / DIAGONAL LINE	
							143						644	00900	143	SQ FT	ISLAND MARKING	
							14						644	01300	14	EACH	LANE ARROW	
							6						644	01400	6	EACH	WORD ON PAVEMENT, 72"	
							282						644	01510	282	FT	DOTTED LINE, 6"	
																	MAINTENANCE OF TRAFFIC	
			24										614	11110	24	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
				9									614	13302	9	EACH	BERRIER REFLECTOR, TYPE B2	
				8									614	13360	8	EACH	OBJECT MARKER, TWO WAY	
			0.67										614	20400	0.67	MILE	WORK ZONE LANE LINE, CLASS II	
				0.12									614	21000	0.12	MILE	WORK ZONE CENTER LINE, CLASS I	
			0.48										614	21400	0.48	MILE	WORK ZONE CENTER LINE, CLASS II	
				0.11									614	22000	0.11	MILE	WORK ZONE EDGE LINE, CLASS I	
			1250										614	23000	1250	FT	WORK ZONE CHANNELIZING LINE, CLASS I	
				231									614	24000	231	FT	WORK ZONE DOTTED LINE, CLASS I	
			230										614	26000	230	FT	WORK ZONE STOP LINE, CLASS I	
				370									622	40020	370	FT	PORTABLE CONCRETE BARRIER, 32"	
			LUMP										614	11000	LUMP		MAINTAINING TRAFFIC	
													624	10000	LUMP		MOBILIZATION	

CALCULATED LMO
CHECKED KJG

GENERAL SUMMARY

JEF-43-1.97

$TIME$

$DATE$
$FILE$

REF NO.	SHEET NO.	STATION FROM	STATION TO	SIDE		202 WALK REMOVED	202 CURB REMOVED	202 CURB AND GUTTER REMOVED	202 PIPE REMOVED, 24" AND UNDER	202 CATCH BASIN REMOVED		603 12" CONDUIT, TYPE B	604 CATCH BASIN, NO. 3, AS PER PLAN		608 4" CONCRETE WALK, AS PER PLAN	608 CURB RAMP, TYPE B1, AS PER PLAN	608 CURB RAMP, TYPE B2, AS PER PLAN		609 COMBINATION CURB AND GUTTER, TYPE 2	609 CURB, TYPE 6		
						SQ. FT.	FT.	FT.	FT.	EACH		FT.	EACH		SQ. FT.	EACH	EACH		FT.	FT.		
D1	8	114+05	114+25	RT.					24	1		24	1									
P1	8	10+22	10+42	LT.				44												44		
P2	8	10+22	10+42	RT.				35												35		
P3	8	13+30	14+05	LT.				75												75		
P4	8	14+80	15+00	RT.				20												20		
P5	8	14+80	15+00	LT.				20												20		
P6	8	15+00	15+30	LT.				30											30			
R1	8	112+55	112+72	RT.		68	17								77		1			17		
R2	8	112+51	112+66	LT.		220									220		1					
R3	8	113+47	113+58	LT.		100									94	1						
R4	8	113+79	113+91	RT.		54		12							54	1			12			
TOTALS CARRIED TO GENERAL SUMMARY						442	17	236	24	1		24	1		445	2	2		42	211		

CALCULATED LMO
CHECKED KJG

ROADWAY SUBSUMMARY

JEF-43-1.97

DATES
FILES
TIMES

FULL DEPTH PAVEMENT RECONSRUCTION (FLEXIBLE)
RIGID PAVEMENT REPLACEMENT
PAVEMENT FEATHER

STA. 112+98.29, ℄ CONST. SUNSET BLVD. =
STA. 10+00.39, ℄ CONST. JOHN SCOTT HIGHWAY

STA. 113+03.98, ℄ CONST. SUNSET BLVD. =
STA. 10+00.00, ℄ CONST. JOHN SCOTT HIGHWAY

℄ CONST. SUNSET BLVD.
℄ CONST. JOHN SCOTT HIGHWAY
ARGONNE AVE. (ASPH.)
KINGSTON AVE. (ASPH.)
+00 BEGIN PAVEMENT PLANING & RESURFACING
+30 BEGIN RIGID PAVEMENT REPLACE. LT.
+00 BEGIN RIGID PAVEMENT REPLACE. LT.
12' CONC.
12' CONC.
14' CONC.
10' GRAVEL
ASPH. PARKING
43' CONC.
24' CONC.
12' ASPH.
30' ASPH.
24' ASPH.
36' ASPH.
N 77°12'53" E
S 73°38'58" E
N 16°21'02" E
S 52°27'07" E
S 54°34'28" E
N 12°27'32" E
N 01°13'29" E
104°47'03"
90°00'00"
106
107
108
109
110
111
112
113
114
115
116
117
24'
39'
33'
38'
23'
27.5'
27.16'
26.16'
C.B.
M.H.
12" STM
18" STM
12"-B
P1
P2
P3
P4
P5
P6
D1
R1
R2
R3
R4
PC Sta. 108+09.36
+32.49 BEGIN TAPER RT.
+40.22 END TAPER RT.
PT Sta. 8+80.20
PC Sta. 10+44.03
+42, END FULL DEPTH REPLACEMENT
BEGIN PAVEMENT RESURFACING
+22, BEGIN FULL DEPTH REPLACEMENT
PT Sta. 14+36.73
+80 BEGIN FULL DEPTH RECONSTRUCTION (FLEXIBLE), LT. & RT.
+89.74 BEGIN TAPER, 27.50' LT. & RT.
+00 END FULL DEPTH RECONSTRUCTION (FLEXIBLE) LT. & RT.
+30 END RIGID PAVEMENT REPLACMENT LT.
PC Sta. 117+14.85
PT Sta. 117+99.75
PT Sta. 116+48.24
+60.15 BEGIN TAPER RT.
+00 END PAVEMENT PLANING & RESURFACING
PC Sta. 5+09.06
+00 BEGIN PAVEMENT PLANING & RESURFACING
+20 BEGIN TAPER RT.
+45 THEORETICAL END TAPER RT.
MATCH EX.
CURB RAMP, TYPE B2
CURB RAMP, TYPE B1

CURVE DATA - SUNSET BLVD.
P.I. STA. 112+58.03
Δ = 50° 20' 00" (RT)
Dc = 6° 00' 00"
R = 954.93'
T = 448.67'
L = 838.88'
E = 100.15'

CURVE DATA - JOHN SCOTT HIGHWAY
P.I. STA. 12+94.03
Δ = 90° 00' 00" (RT)
Dc = 22° 55' 06"
R = 250.00'
T = 250.00'
L = 392.70'
E = 103.55'

CURVE DATA - JOHN SCOTT HIGHWAY
P.I. STA. 6+95.22
Δ = 11° 08' 03" (LT)
Dc = 3° 00' 00"
R = 1,909.86'
T = 186.16'
L = 371.14'
E = 9.05'

CURVE DATA - SUNSET BLVD.
P.I. STA. 117+57.30
Δ = 2° 07' 21" (LT)
Dc = 2° 30' 00"
R = 2,291.83'
T = 42.45'
L = 84.90'
E = 0.39'

N
0 20 40 80
HORIZONTAL SCALE IN FEET
CALCULATED LAM
CHECKED KJG

ROADWAY PLAN
SUNSET BOULEVARD & JOHN SCOTT HIGHWAY

JEF-43-1.97

8
10

DATES
FILES
TIMES

SHEET NO.	LOCATION	STATION FROM	STATION TO	SIDE	644 LANE LINE MILE	644 CENTER LINE MILE	644 CHANNELIZING LINE FT.	644 STOP LINE FT.	644 CROSSWALK LINE FT.	644 TRANSVERSE/DIAGONAL LINE FT.	644 ISLAND MARKING SQ. FT.	644 LANE ARROW EACH	644 WORD ON PAVEMENT, 72" EACH	644 DOTTED LINE 6" FT.
10	SUNSET BLVD.	108+00	118+00	LT./RT.	0.38	0.25	454	142	291	170		4	2	
10	JOHN SCOTT HIGHWAY	5+00	15+30	LT./RT.	0.30	0.24	895	92	360	130	143	10	4	282
	TOTALS CARRIED TO GENERAL SUMMARY				**0.68**	**0.49**	**1349**	**234**	**651**	**300**	**143**	**14**	**6**	**282**

SHEET NO.	LOCATION	STATION FROM	STATION TO	SIDE	INTERVAL FT.	REMARKS	621 RAISED PAVEMENT MARKER REMOVED EACH		621 RPM, LOW PROFILE, YELLOW/YELLOW EACH	621 RPM, LOW PROFILE, WHITE/RED EACH
	SUNSET BLVD.									
10		108+00	112+49	RT.	40	CENTER LINE			20	
10		108+00	112+49	LT./RT.	80	LANE LINE				12
10		111+65	112+49	RT.	40	CHANNELIZING LINE				3
10										
10		113+70	116+48.25	RT.	40	CENTER LINE			7	
10		116+48.25	118+00	RT.	80	CENTER LINE			3	
10		113+70	118+00	LT./RT.	80	LANE LINE				12
10		113+70	117+40	RT.	40	CHANNELIZING LINE				10
	JOHN SCOTT HIGHWAY									
10		5+00	5+94	LT./RT.	80	CENTER LINE			4	
10		6+52	9+26	LT./RT.	80	CENTER LINE			6	
10		5+00	9+26	LT.	80	LANE LINE				6
10		5+26	9+26	RT.	40	CHANNELIZING LINE				11
10		7+16	9+26	RT.	40	CHANNELIZING LINE				6
10		10+42	14+12	LT./RT.	40	CENTER LINE	11		11	
10		14+78	15+00	RT.	80	CENTER LINE	1		1	
10		14+78	15+30	LT.	80	CENTER LINE	1		1	
10		10+42	14+12	LT.	80	LANE LINE	5			5
10		14+78	15+00	LT.	80	LANE LINE	1			1
10		10+42	15+00	RT.	80	LANE LINE	6			6
10		10+42	12+67	LT.	40	CHANNELIZING LINE	6			6
10		13+52	14+12	RT.	40	CHANNELIZING LINE	3			3
	TOTALS CARRIED TO GENERAL SUMMARY						**34**		**53**	**81**

CALCULATED LMO
CHECKED KJG

PAVEMENT MARKING SUBSUMMARY

JEF-43-1.97

$TIME$
$DATE$
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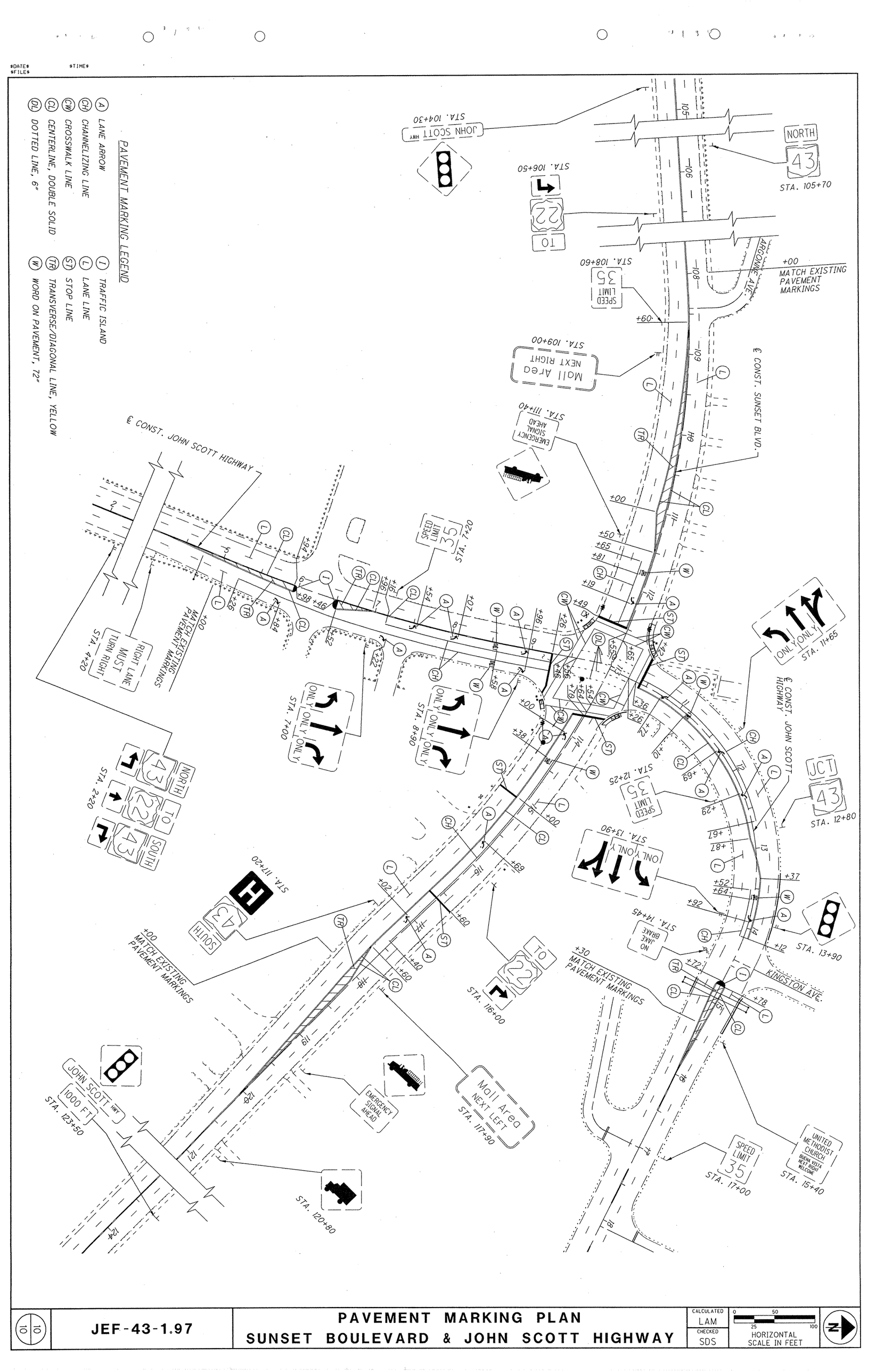
$DATE$
$FILE$ $TIME$



10/10	JEF-43-1.97	PAVEMENT MARKING PLAN SUNSET BOULEVARD & JOHN SCOTT HIGHWAY	CALCULATED LAM CHECKED SDS	0 50 25 100 HORIZONTAL SCALE IN FEET