

LOCATION MAP

BEGIN LATITUDE: 41° 34' 10.9" N LONGITUDE: 81° 25' 3.6" W

END LATITUDE: 41° 37' 15.9" N LONGITUDE: 81° 24' 35.8" W



## DESIGN DESIGNATION

CURRENT ADT (2026)	5,626
DESIGN YEAR ADT (2038)	5,626
DESIGN HOURLY VOLUME (2038)	UNKNOWN
DIRECTIONAL DISTRIBUTION	52%
TRUCKS (24 HOUR B&C)	N/A
DESIGN SPEED	40 MPH
LEGAL SPEED	40 MPH

## DESIGN FUNCTIONAL CLASSIFICATION:

05 MAJOR COLLECTOR (RURAL)

NHS PROJECT ----- N/A

## DESIGN EXCEPTIONS

NONE REQUIRED

## ADA DESIGN WAIVERS

NONE REQUIRED



## PLANS PREPARED BY:

STEPHEN HOVANCK & ASSOCIATES, INC.  
CONSULTING ENGINEERS AND PLANNERS  
TWO MERIT DRIVE  
RICHMOND HEIGHTS, OHIO 44143  
PH: (216) 731-6255 FAX: (216) 731-4483

# STATE OF OHIO

## DEPARTMENT OF TRANSPORTATION

LAK SR174-00.00

### CITY OF WILLOUGHBY HILLS

### LAKE COUNTY

## INDEX OF SHEETS:

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## FEDERAL PROJECT NUMBER

E191577

## RAILROAD INVOLVEMENT

NONE

## PROJECT DESCRIPTION

RESURFACING OF APPROXIMATELY 3.7 MILES OF RIVER ROAD FROM THE CORPORATION LIMIT NEAR ROGERS ROAD TO THE NORTHERN CORPORATION LINE. WORK WILL INCLUDE MILLING OF EXISTING ASPHALT AND REPLACEMENT, ALONG WITH FULL DEPTH REPAIRS, UTILITY CASTING ADJUSTMENTS, AND PAVEMENT MARKINGS THROUGHOUT THE PROJECT LENGTH.

## EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: N/A  
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A  
NOTICE OF INTENT EARTH DISTURBED AREA: N/A (NOI NOT REQUIRED)\*

\* ROUTINE MAINTENANCE PROJECT

## 2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL REQUIRE THE PART-TIME CLOSING OF THE HIGHWAY TO TRAFFIC, AS NOTED ON SHEETS 10-15. DURING WHICH TIME DETOURS WILL BE PROVIDED AS SHOWN HEREIN. PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
BP-2.5	7/19/24			800	7/18/25
BP-3.1	1/19/24			821	4/20/12
BP-5.1	7/18/25			832	7/18/25
CB-4	7/19/24			875	1/17/25
				921	7/19/24
MT-95.31	7/18/25				
MT-95.32	7/18/25				
MT-97.10	7/18/25				
MT-97.12	7/18/25				
MT-99.20	4/19/19				
MT-101.90	7/17/20				
MT-105.10	1/17/20				

ENGINEER'S SEAL:

STATE OF OHIO  
ANTHONY DIRENZO  
NO. 83900  
PROFESSIONAL ENGINEER  
SIGNED: *Anthony DiRenzo*  
DATE: 1/14/2026

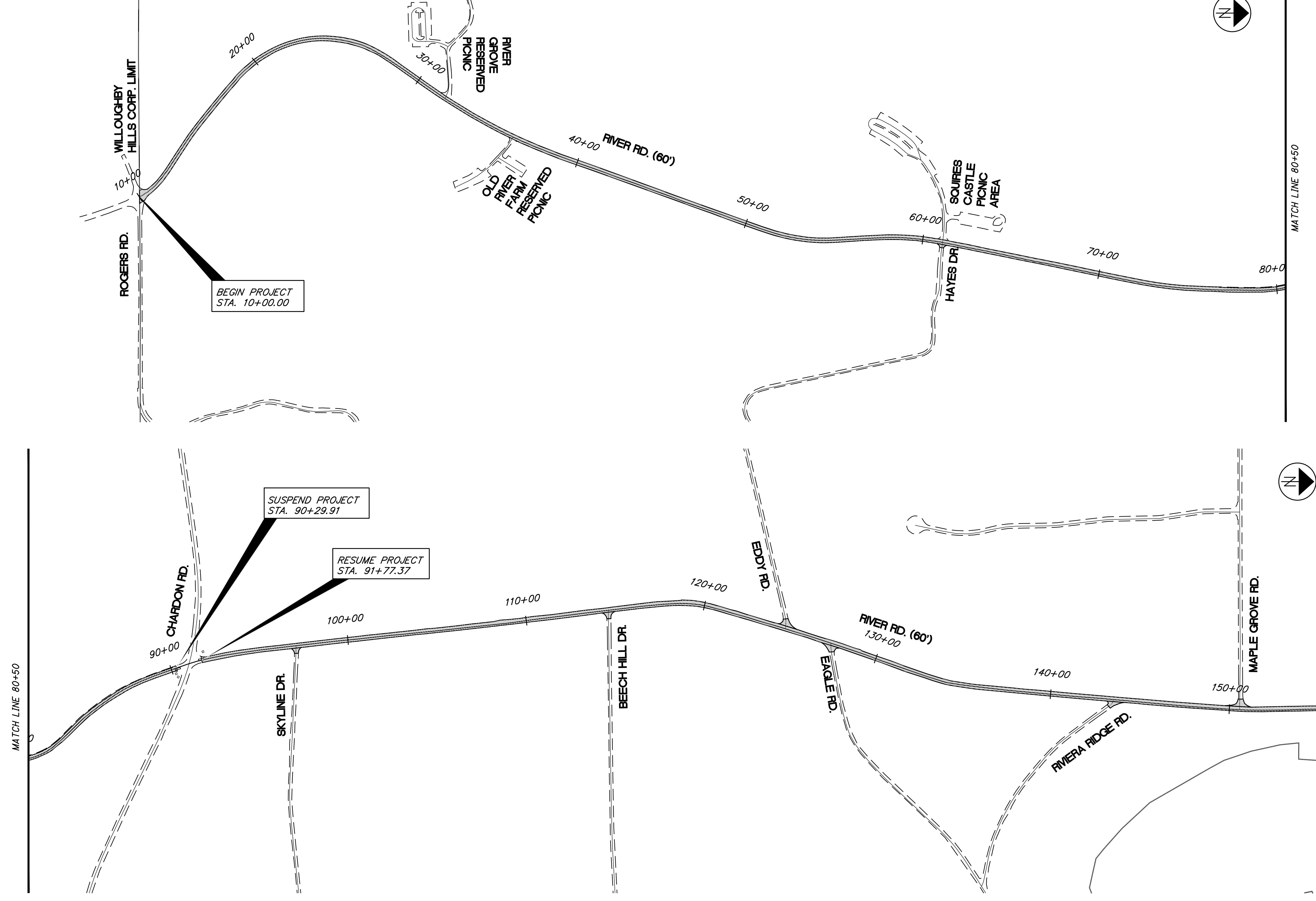
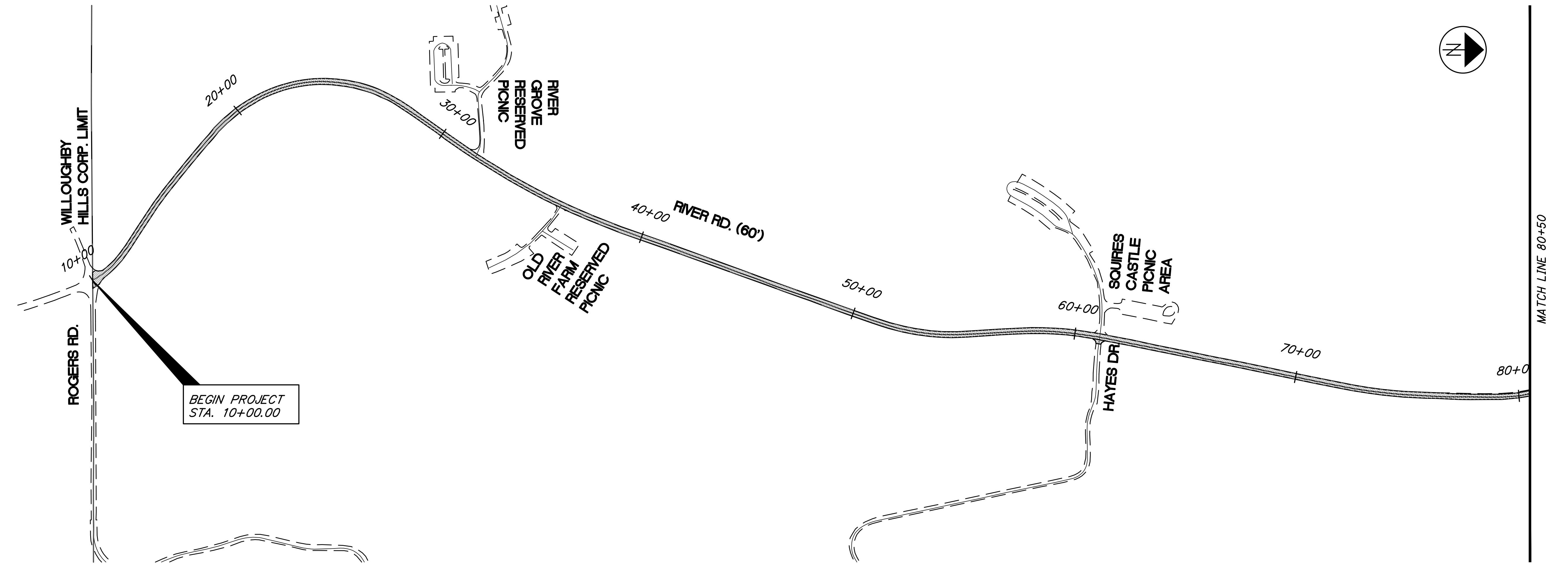
*John Picuri, P.E., P.S.*  
District 12 Deputy Director

*Pamela Boratyn*  
Pamela Boratyn  
Director, Department of Transportation

TITLE SHEET

DESIGN AGENCY	STEPHEN HOVANCK & ASSOCIATES, INC.
DESIGNER	MSL
REVIEWER	AWD 8/22/2025
PROJECT ID	105894
SHEET TOTAL	P.1 36

MATCH LINE 80+50



LAK SR174-00.00

2026/01/13  
G:\2024\A24-297 WILLOUGHBY HILLS RESURFACING OF RIVER ROAD\105894\DWG\Set\24-297-PS-03\_Schematic Plan 2.dwg

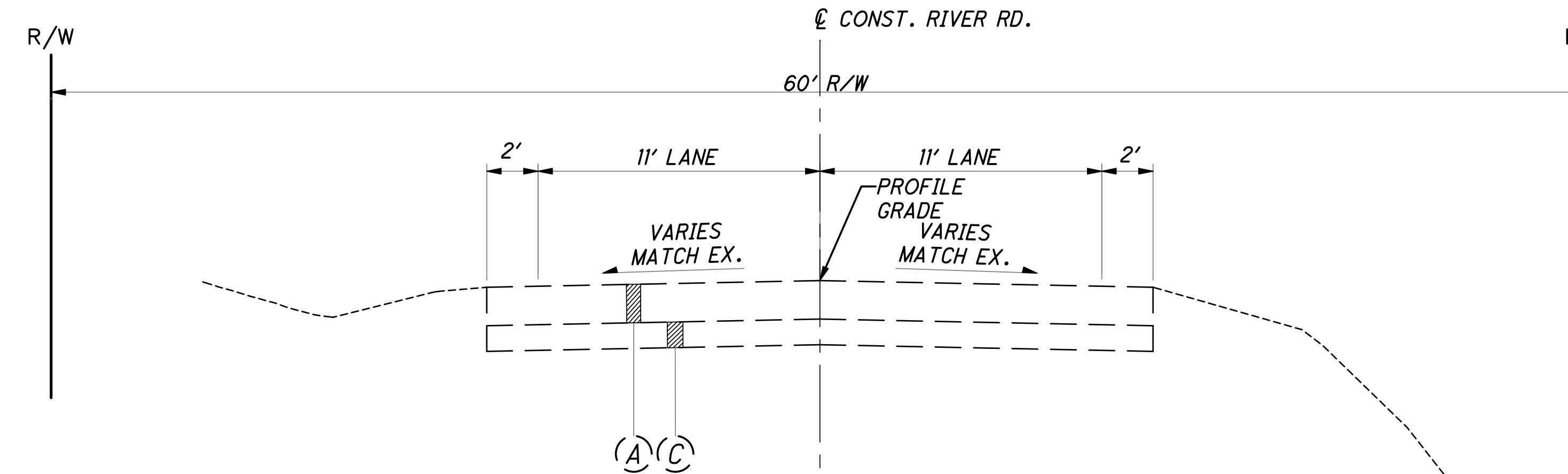


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STEPHEN HOVANECIK & ASSOCIATES, INC.  
DESIGNER  
MSL  
REVIEWER  
AWD 8/22/2025  
PROJECT ID  
105894  
SHEET TOTAL  
P.3 36

SCHEMATIC PLAN  
STA. 156+00 00 TO STA. 211+28.49

HORIZONTAL SCALE IN FEET  
0 125 250 500

## TYPICAL SECTIONS

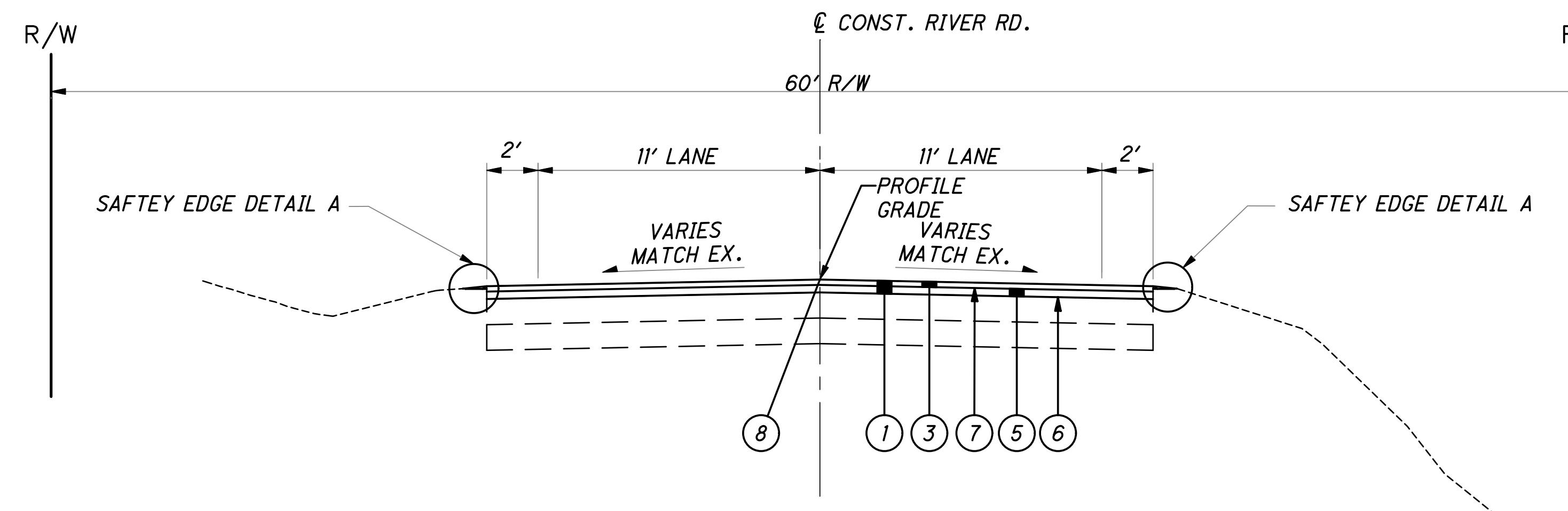


RIVER RD. - EXISTING SECTION

STA. 10+00.00 TO 76+79.21

&

STA. 92+19.16 TO 210+21.20

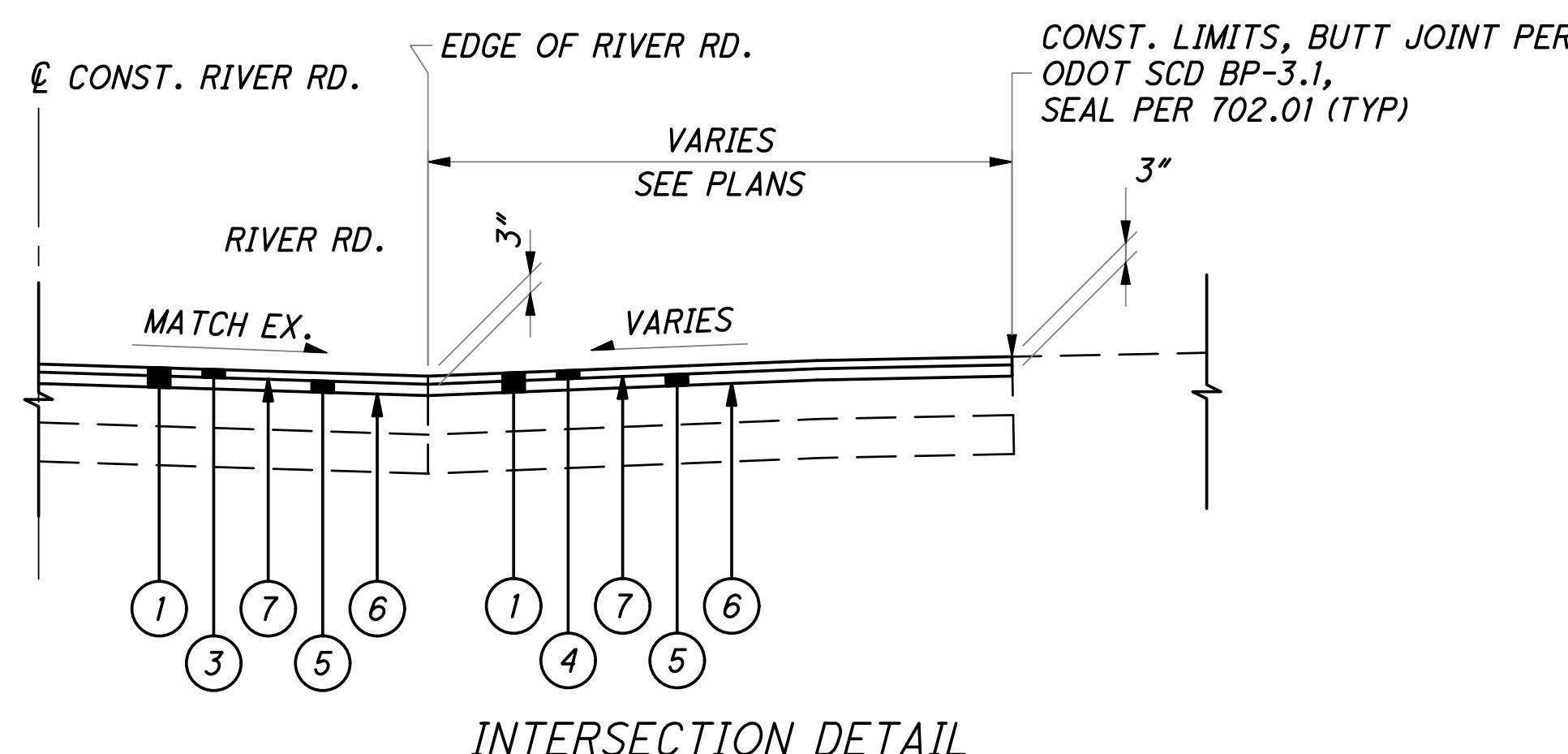


RIVER RD. - PROPOSED SECTION

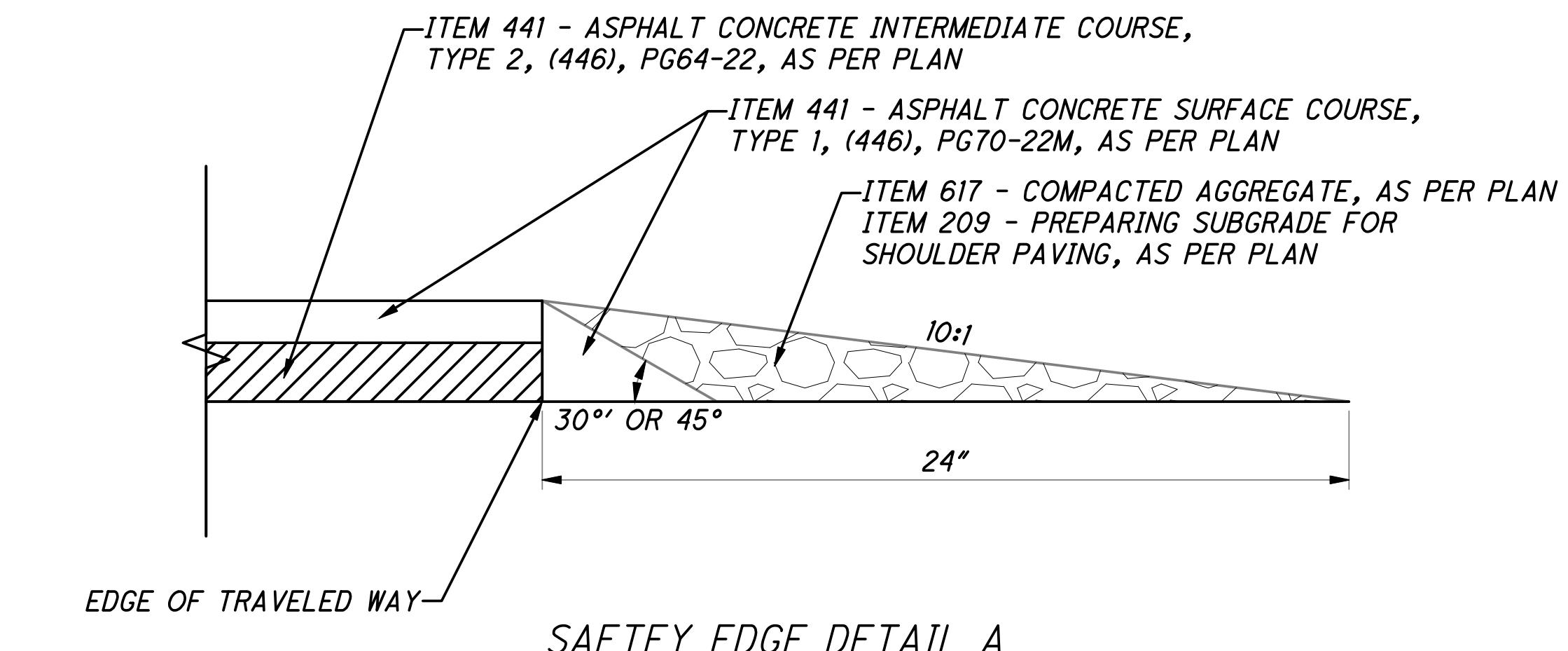
STA. 10+00.00 TO 76+79.21

&

STA. 92+19.16 TO 210+21.20



INTERSECTION DETAIL



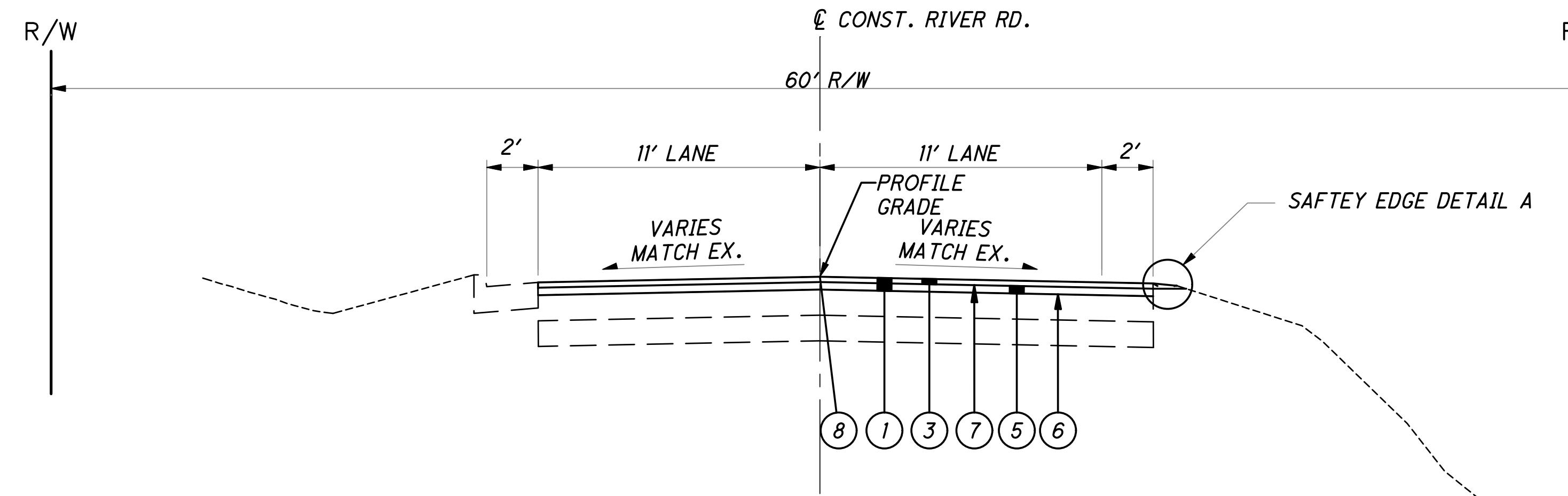
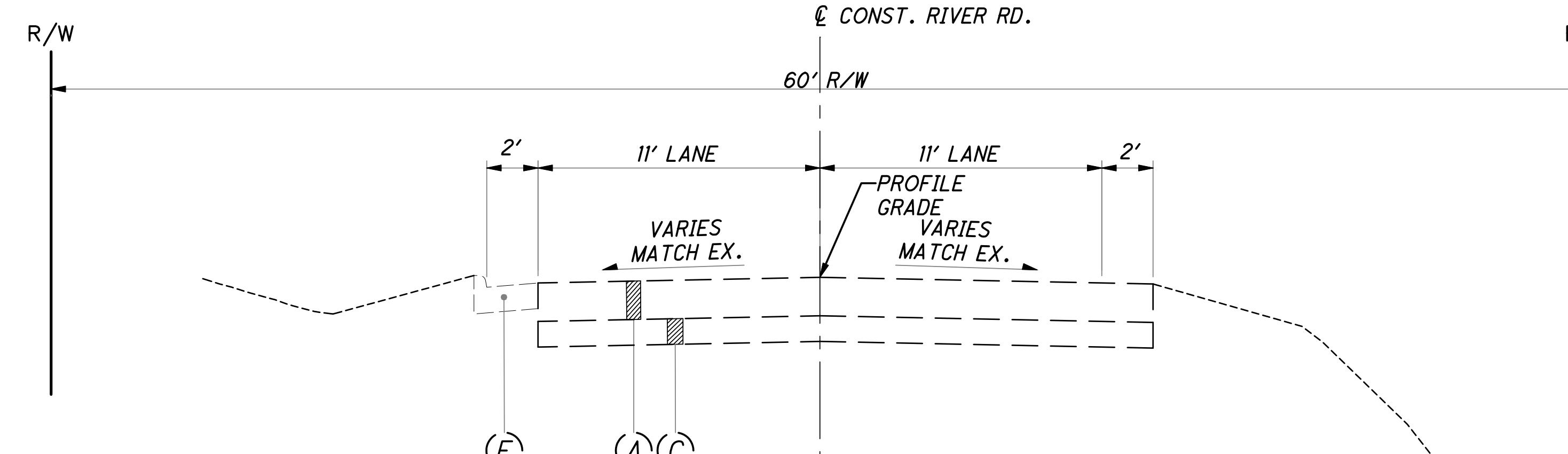
### EX. LEGEND

- (A) EX. ASPHALT CONCRETE, 9" - 11" ±
- (B) EX. REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT, 8"
- (C) EX. AGGREGATE OR SLAG BASE, 2" - 3" ±
- (D) EX. SUBBASE, 6"
- (E) EX. CONCRETE CURB

### PROP. LEGEND

- (1) ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (3") (SEE NOTE SHEET 8)
- (2) ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN (SEE NOTE SHEET 9)
- (3) ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG70-22M, AS PER PLAN (1-1/4") (SEE NOTE SHEET 9)
- (4) ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), PG64-22, AS PER PLAN (1-1/4") (SEE NOTE SHEET 9)
- (5) ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446), PG64-22, AS PER PLAN (1-3/4") (SEE NOTE SHEET 9)
- (6) ITEM 407 - TACK COAT, 702.13 (APPLIED AT A RATE OF 0.09 GAL/SY)
- (7) ITEM 407 - NON-TRACKING TACK COAT (APPLIED AT A RATE OF 0.06 GAL/SY)
- (8) ITEM 875 - LONGITUDINAL JOINT ADHESIVE

**NOTE:**  
EXISTING GUARD RAILS ARE NOT SHOWN AND NOT TO BE DISTURBED

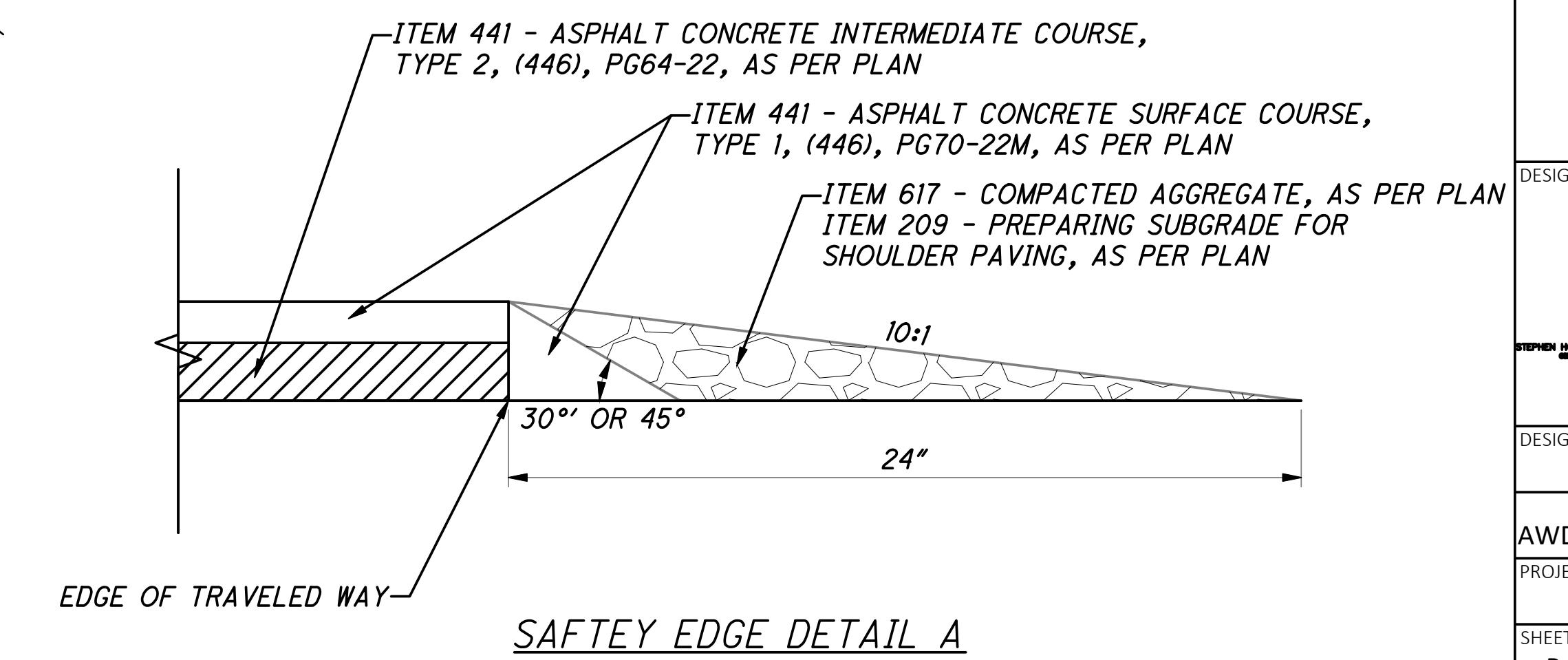
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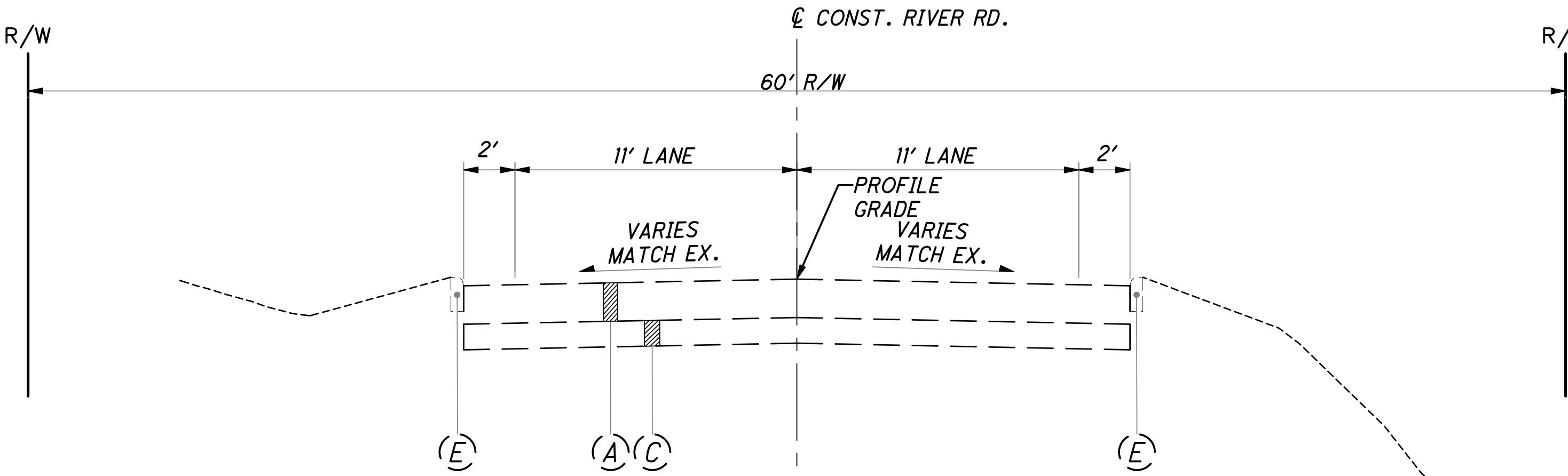
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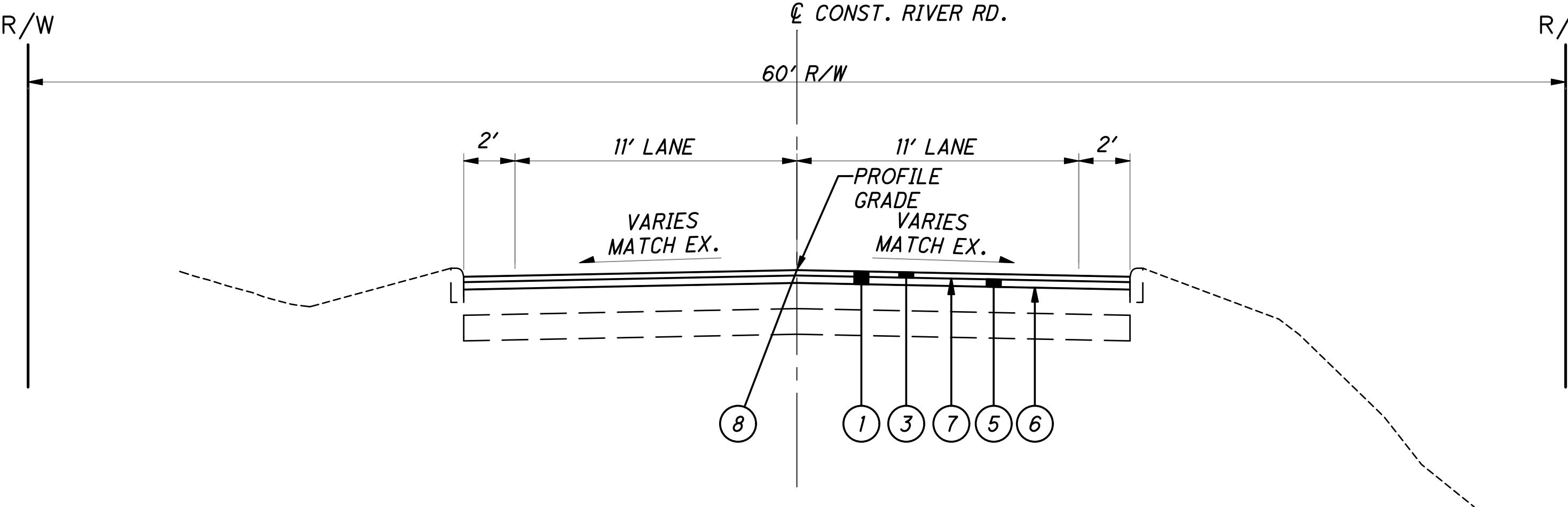


## TYPICAL SECTIONS



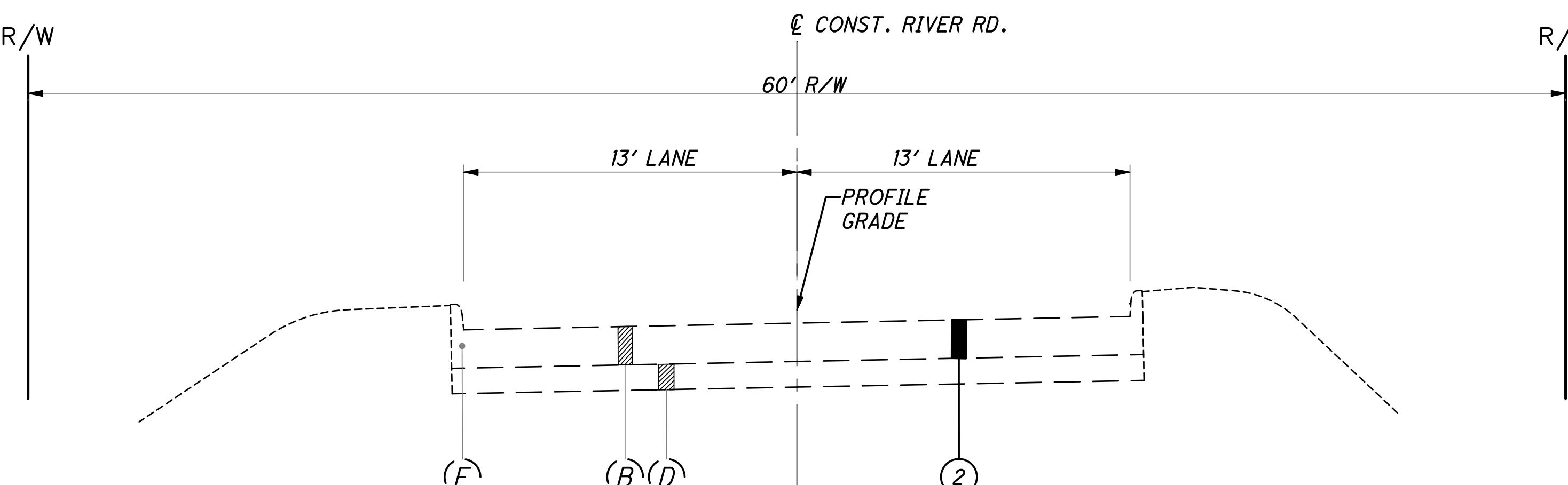
RIVER RD. - EXISTING SECTION

STA. 91+77.37 TO 92+19.16



RIVER RD. - PROPOSED SECTION

STA. 91+77.37 TO 92+19.16



RIVER RD. - EXISTING & PROPOSED SECTION

STA. 210+21.20 TO STA. 211+28.49

### EX. LEGEND

- (A) EX. ASPHALT CONCRETE, 9" - 11" ±
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- (D) EX. SUBBASE, 6"
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**NOTE:**  
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## GENERAL

### UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

**WATER:**  
LAKE COUNTY DEPT. OF UTILITIES  
WATER DIVISION  
105 MAIN STREET  
PAINESVILLE, OHIO 44077  
SARAH A. CEROVSKI  
440-350-2652

**TELEPHONE:**  
AT&T  
13630 LORAIN AVENUE,  
ROOM 200  
CLEVELAND, OHIO 44111  
JAMES JANIS  
216-476-6142  
FAX: (216) 476-6013  
pj8191@att.com

**NATURAL GAS:**  
ENBRIDGE GAS OHIO  
Relocation@dominionenergy.com  
320 SPRINGSIDE DRIVE, SUITE 320  
AKRON, OH 44333  
PHONE: 330-664-2409

**ELECTRIC:**  
CEI FIRST ENERGY  
6896 MILLER RD., SUITE 101  
BRECKSVILLE, OHIO 44141  
JOHN M. ZASSICK  
OFFICE: 440-546-8706  
CELL: 216-538-1580  
jmzassick@firstenergycorp.com

THERE ARE NO UNDERGROUND UTILITIES SHOWN IN PAVEMENT AREAS ON THIS PLAN. THE NATURE OF THE PAVING WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

### CONSTRUCTION NOISE

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, DO NOT OPERATE POWER-OPERATED CONSTRUCTION-TYPE DEVICES BETWEEN THE HOURS OF 9:00PM AND 7:30AM M-SA OR 8:00 PM AND 10:00 AM SUNDAY. IN ADDITION, DO NOT OPERATE AT ANYTIME ANY DEVICE IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT.

THE CONTRACTOR SHALL APPLY FOR A NOISE VARIANCE TO PERFORM NIGHT PAVING. IN THE EVENT A NOISE VARIANCE IS NOT GRANTED BY THE CITY, WORK SHALL COMMENCE DURING THE DAYTIME HOURS AT NO ADDITIONAL COSTS. ANY ADDITIONAL MOT COST WILL BE INCLUDED IN THE LINE ITEM FOR MOT, NO SEPARATE PAYMENT SHALL BE MADE.

### PROTECTION OF RIGHT-OF-WAY LANDSCAPING

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL RECORD A VIDEO SURVEY OF ALL LANDSCAPING ITEMS WITHIN THE RIGHT OF WAY. A RECORD OF THIS VIDEO WILL BE KEPT IN THE PROJECT ENGINEER'S FILES. PRIOR TO FINAL ACCEPTANCE, A FINAL REVIEW OF LANDSCAPING ITEMS WILL BE MADE.

CONSTRUCT ALL ACTIVITIES, EQUIPMENT STORAGE, AND STAGING TO WITHIN THE CONSTRUCTION LIMITS. THE DOCUMENT SUBMITTED MUST CLEARLY IDENTIFY THE AREA AND EXPLAIN THE PROPOSED USE AND RESTORATION OF THE AREA. THE REQUEST MUST BE APPROVED, IN WRITING, BEFORE THE CONTRACTOR HAS PERMISSION TO USE THE AREA.

ANY ITEMS DAMAGED BEYOND THE CONSTRUCTION LIMITS AS DEFINED ABOVE WILL BE REPLACED IN KIND OR AS APPROVED BY THE PROJECT ENGINEER.

### WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

### CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DETERMINED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER.

### CONSTRUCTION NOTIFICATION

THE CONTRACTOR WILL ADVISE THE PROJECT ENGINEER A MINIMUM OF FOURTEEN (14) DAYS PRIOR TO START OF CONSTRUCTION ACTIVITIES, LANE CLOSURE, AND ROAD CLOSURES. THE PROJECT ENGINEER WILL FORWARD THE INFORMATION TO THE CITY AND ANY THEIR LOCAL OFFICIALS RESPONSIBLE FOR PUBLIC NOTIFICATION

### STATIONING AND LOCATIONS

STATIONING AND LOCATIONS INDICATED ON THESE PLANS ARE APPROXIMATE. ALL LOCATIONS AND ITEMS CALLED OUT BY STATION ARE SUBJECT TO ADJUSTMENT IN THE FIELD AS DETERMINED BY THE ENGINEER.

### EXISTING TYPICAL SECTIONS

EXISTING TYPICAL SECTIONS HAVE BEEN DEVELOPED FROM SITE MEASUREMENTS AND RECORD PLANS AND ARE BELIEVED TO REPRESENT THE WIDTH AND COMPOSITION OF THE EXISTING PAVEMENT, BUT THE STATE DOES NOT GUARANTEE ACCURACY OF THE SAME.

### STAGING AND STORAGE

EQUIPMENT AND MATERIALS SHALL NOT BE STAGED OR STORED WITHIN PUBLICLY OWNED PARK PROPERTY. ALL STAGING AND STORAGE AREAS OUTSIDE OF THE RIGHT-OF-WAY SHALL BE PRE-APPROVED BY PROPERTY OWNER.

### ITEM SPECIAL - PRE-CONSTRUCTION VIDEOGRAPHY

THE CONTRACTOR SHALL PROVIDE A PRE-CONSTRUCTION VIDEO PRIOR TO DELIVERY OF ANY EQUIPMENT, MATERIAL OR SUPPLIES AS FOLLOWS:

- A. **SCOPE:** COLOR VIDEO & AUDIO DOCUMENTATION OF SURFACE FEATURES AND IMPROVEMENTS WITHIN PROJECT LIMITS AND STORAGE AREAS, INCLUDING ADJACENT AREA. PRIOR TO WITHIN PROJECT LIMITS AND STORAGE AREAS, INCLUDING ADJACENT AREA. PRIOR TO AUDIO-VIDEO TAPING OF THE PROJECT, ALL AREAS TO BE INVENTORIED SHALL BE INVESTIGATED VISUALLY WITH NOTATIONS MADE OF ITEMS NOT READILY VISIBLE BY TAPING METHODS.
- B. **PURPOSE:** DOCUMENTATION OF EXISTING CONDITIONS PRIOR TO CONSTRUCTION FOR USE IN DETERMINING RESTORATION WORK AFTER CONSTRUCTION.
- C. **QUALIFICATIONS:** MINIMUM TWO (2) YEARS OF AUDIO-VIDEO TAPING EXPERIENCE ON SIMILAR TYPE CONSTRUCTION PROJECTS FOR PUBLIC AGENCIES.
- D. **COVERAGE AREA:** SHALL INCLUDE, BUT NOT LIMITED TO, ALL EXISTING DRIVEWAYS, SIDEWALKS, CURBS, STREETS, SIGNS, LANDSCAPING, TREES, CATCH BASINS, FENCES, VISIBLE UTILITIES, AND ALL BUILDINGS LOCATED WITHIN THE CONSTRUCTION ZONE OF INFLUENCE.
  - a. **STREET-** FULL WIDTH OF RIGHT-OF-WAY UNLESS NOTED OTHERWISE BY THE ENGINEER.
  - b. **WORK AGREEMENT AREAS-** ALL ADJACENT AREAS LYING WITHIN THE CONSTRUCTION ZONE OF INFLUENCE.
  - c. **BUILDING EXTERIORS-** ALL EXTERIOR SURFACES IDENTIFIED BY THE ENGINEER TO RECEIVE COVERAGE.
  - d. **FRONT AND SIDE YARD AREAS-** OF RESIDENTIAL HOMES WITHIN THE CONSTRUCTION ZONE OF INFLUENCE.
- E. **EQUIPMENT:** CAMERA SHALL BE CAPABLE OF PROVIDING ACCEPTANCE RESOLUTION COLOR VIDEO, SUFFICIENT ZOOM, PAN AND TILT CAPABILITIES, VIDEO SIGNAL- TO-NOISE RATIO OF AT LEAST 40 DB, AND A HORIZONTAL RESOLUTION OF 270 LINES.
- F. **DELIVERABLE:** CONTRACTOR SHALL PROVIDE TWO (2) COPIES ON CD OR DVD. THE DISKS SHALL BE LABELED WITH PROJECT NAME, LOCATION, AND DATE. THE AUDIO-VIDEO CONTENTS SHALL BE LABELED WITH PROJECT NAME, LOCATION, AND DATE. THE AUDIO-VIDEO CONTENTS SHALL BE INVENTORIED BY RUN SEGMENT, IDENTIFIED BY LOCATION, ADDRESS, OR STATIONING.
- G. **ENTERING PROPERTY:** PRIVATE PROPERTY SHALL NOT BE ENTERED WITHOUT PERMISSION OF THE PROPERTY OWNER.
- H. **OWNERSHIP OF DISKS:** ALL DISKS AND THEIR CONTENTS SHALL BECOME PROPERTY OF THE CITY OF WILLOUGHBY HILLS.
- I. **SITE RECORDING CONDITIONS-** SHALL BE DONE DURING GOOD AND CLEAR VISIBILITY. THE AVERAGE RATE OF SPEED DURING RECORDING SHALL BE APPROXIMATELY SIXTY (60) FEET PER MINUTE.

ITEM SPECIAL - PRE- CONSTRUCTION VIDEOGRAPHY LS

## ROADWAY

### ITEM 201 - CLEARING AND GRUBBING, AS PER PLAN

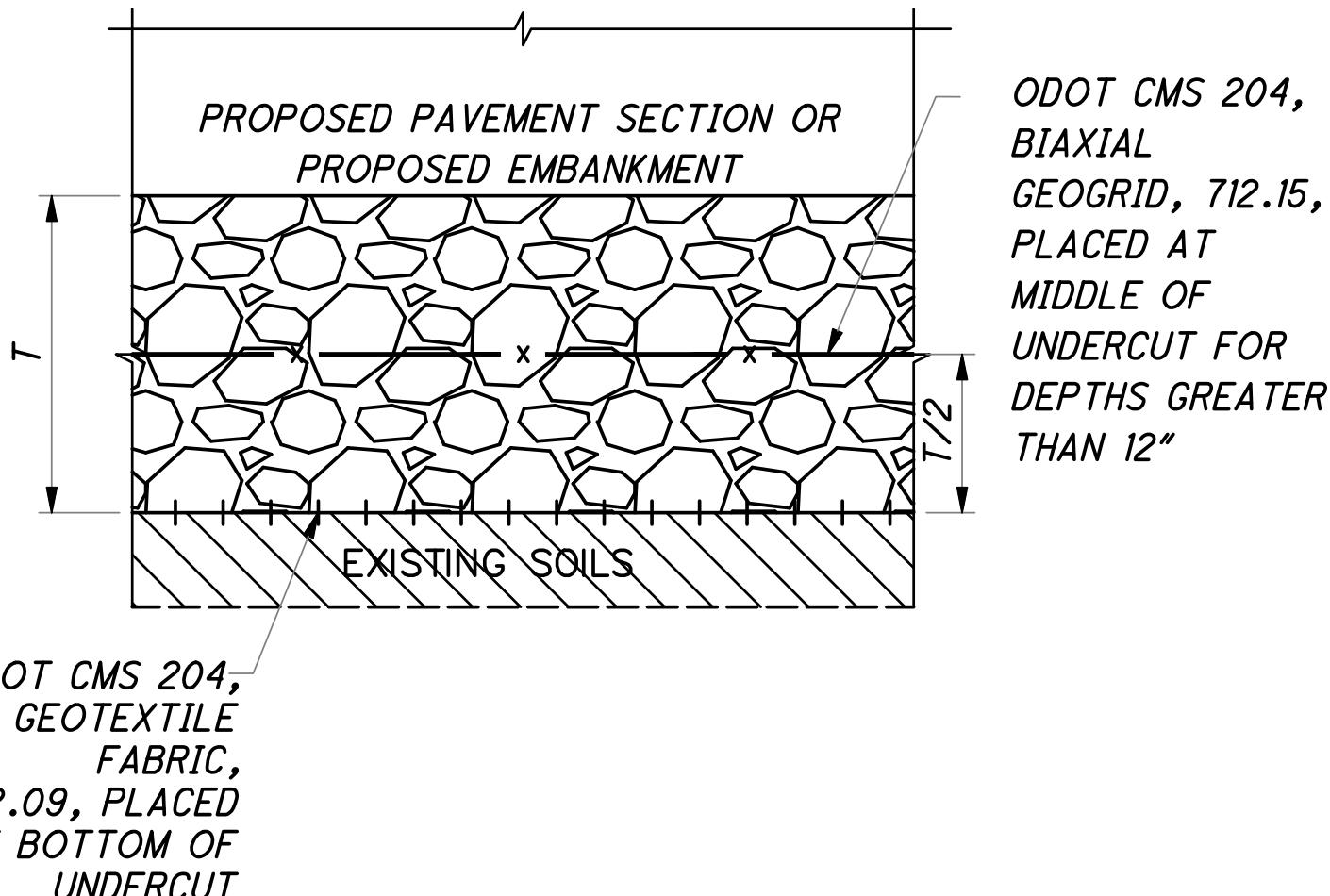
ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING, AS PER PLAN.

### UNSUITABLE OR UNSTABLE SOILS FOR PAVEMENT STABILIZATION

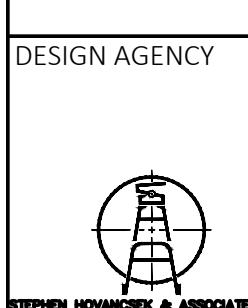
GEOTEXTILE FABRIC SHALL BE IN ACCORDANCE WITH ODOT 712.09. GEOGRID SHALL BE IN ACCORDANCE WITH ODOT 712.15. ALL MATERIALS MUST BE CONTAINED ON THE ODOT OFFICE OF MATERIALS MANAGEMENT, APPROVED MATERIALS LIST AND/OR QUALIFIED PRODUCTS LIST.

THE FOLLOWING ITEMS AND QUANTITIES ARE TO BE USED AS DIRECTED BY THE ENGINEER TO ADDRESS UNSTABLE OR UNSUITABLE SOILS ENCOUNTERED IN THE AREAS OF PAVEMENT CONSTRUCTION:

ITEM 204 - EXCAVATION OF SUBGRADE	170 CY
ITEM 204 - GRANULAR MATERIAL, TYPE B	170 CY
ITEM 204 - GEOTEXTILE FABRIC	340 SY
ITEM 204 - GEOGRID	340 SY



## GENERAL NOTES



DESIGNER  
MSL

REVIEWER

AWD 8/22/2025  
PROJECT ID  
105894

SHEET TOTAL  
P.7 36

## ROADWAY CONT.

### ITEM 623 - CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF C&MS 623, THE FOLLOWING SHALL APPLY:

IT IS THE CONTRACTOR'S RESPONSIBILITY TO IDENTIFY, PRESERVE, AND PROTECT THE EXISTING MONUMENT ASSEMBLIES WITHIN THE LIMITS OF THE PROJECT. THE CONTRACTOR SHALL DOCUMENT ALL WORK IN THESE REGARDS TO THE SATISFACTION OF THE ENGINEER

ANY MONUMENT ASSEMBLIES THAT ARE DISTURBED, DAMAGED, OR DESTROYED AS A RESULT OF THE CONTRACTOR'S WORK, AS DETERMINED BY THE ENGINEER, SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

AFTER COMPLETION OF ALL WORK, BUT PRIOR TO FINAL ACCEPTANCE OF THE PROJECT, AN OHIO PROFESSIONAL SURVEYOR SHALL DETERMINE THE MINIMUM VERTICAL CLEARANCES OF ALL EXISTING AND NEW BRIDGES WITHIN THE PROJECT LIMITS. AT A MINIMUM, MEASUREMENTS SHALL BE TAKEN ALONG EACH FASCIA BEAM AT THE EDGE OF SHOULDERS, EDGE LINES, LANE LINES, AND CROWN OF THE ROADWAY BELOW. PLEASE NOTE THAT THE I-90 OVERPASS HAS A LOW CLEARANCE SIGN OF 14', THIS CLEARANCE MUST NOT BE REDUCED AFTER RESURFACING. THE ODOT DISTRICT 12 VERTICAL CLEARANCE SURVEY FORM SHALL BE USED, WHERE APPLICABLE, TO DOCUMENT THE MEASUREMENTS. WHERE THE ODOT DISTRICT 12 VERTICAL CLEARANCE SURVEY FORM IS NOT APPLICABLE, THE MEASUREMENTS SHALL BE DOCUMENTED ON A CONTRACTOR-DEVELOPED FORM THAT CLOSELY RESEMBLES THE ODOT DISTRICT 12 VERTICAL CLEARANCE SURVEY FORM AND ACCURATELY DEPICTS THE BRIDGE AND BELOW LANE AND SHOULDER CONFIGURATION. THE COMPLETED FORM SHALL BEAR THE STAMP OR SEAL OF THE OHIO PROFESSIONAL SURVEYOR WHO HAS TAKEN THE MEASUREMENTS AND SHALL BE SUBMITTED TO THE PROJECT ENGINEER PRIOR TO FINAL ACCEPTANCE OF THE PROJECT.

THE ODOT DISTRICT 12 VERTICAL CLEARANCE SURVEY FORM CAN BE DOWNLOADED FROM THE FOLLOWING WEBSITE:

<https://ftp.dot.state.oh.us/pub/contracts/Attach/LAK-105894>

## EROSION CONTROL

### ITEM 832 - EROSION CONTROL

THE FOLLOWING ESTIMATED QUANTITY IS PROVIDED FOR TEMPORARY SEDIMENT AND EROSION CONTROL IN ACCORDANCE WITH THE APPLICABLE REQUIREMENTS OF ODOT SUPPLEMENTAL SPECIFICATION 832:

#### ITEM 832 - EROSION CONTROL

7,500 EACH

## DRAINAGE

### REVIEW OF DRAINAGE FACILITIES

BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE BY THE STATE REPRESENTATIVES, SHALL MAKE AN INSPECTION OF ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCES SHALL BE DETERMINED FROM FIELD OBSERVATIONS. THE RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING BY THE STATE.

### ITEM 611 - CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN

THE FOLLOWING ESTIMATED QUANTITY IS TO BE USED WHERE DIRECTED BY THE ENGINEER.

### ITEM 611 - CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN

2 EACH

### ITEM SPECIAL - MISCELLANEOUS METAL

EXISTING CASTINGS MAY PROVE TO BE UNSUITABLE FOR REUSE, AS DETERMINED BY THE ENGINEER. IT SHALL BE CONTRACTOR'S RESPONSIBILITY TO PROVIDE THE CASTINGS OF THE REQUIRED TYPE, SIZE AND STRENGTH (HEAVY OR LIGHT DUTY) FOR PARTICULAR STRUCTURE IN QUESTION. ALL MATERIAL SHALL MEET ITEM 611 OF THE SPECIFICATIONS AND SHALL HAVE THE PRIOR APPROVAL OF THE ENGINEER.

REPLACEMENT OF OTHER CASTINGS (I.E. VALVE BOXES, MONUMENT BOXES, ETC.) UNSUITABLE FOR REFUSE WILL ALSO BE PAID UNDER THIS ITEM.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS DETERMINED BY THE ENGINEER:

### ITEM SPECIAL - MISCELLANEOUS METAL

2,000 POUNDS

THE CONTRACTOR IS CAUTIONED TO USE EXTREME CARE IN THE REMOVAL, STORAGE AND REPLACEMENT OF ALL EXISTING CASTINGS. CASTINGS DAMAGED BY THE NEGLIGENCE OF THE CONTRACTOR, AS DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH PROPER NEW CASTING AT THE EXPENSE OF THE CONTRACTOR.

## PAVEMENT

### PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY WITH A UNIFORM THICKNESS AS SHOWN ON THE TYPICAL SECTIONS.

### FLEXIBLE START WINDOW CONTRACT

USE THE FOLLOWING TABLE AS PREFERRED TO IN THE PROPOSAL:

DESCRIPTION OF CRITICAL WORK	CALENDAR DAYS TO COMPETE	DISINCENTIVE \$ PER DAY	WORK WINDOW	
			START	END
ODOT ITEM 441 (PAVING ITEMS) & MOT DETOURS	90	PER C&MS 108.07	CONTRACT EXECUTION DATE	PROJECT COMPLETION DATE

### ASPHALT CONCRETE SURFACE COURSE SEALING REQUIREMENTS

IN ADDITION TO THE GUTTER SEALING REQUIREMENTS SPECIFIED IN ODOT SCD BP-3.1 AND IN 401.08 (D), THE CONTRACTOR SHALL SEAL THE FOLLOWING LOCATIONS:

- ALL CASTINGS INCLUDING BUT NOT LIMITED TO MONUMENTS, MANHOLES, WATER VALVES, CATCH BASINS AND CURB INLETS.
- BUTT JOINTS INCLUDING BRIDGE APPROACHES. FEATHERED JOINTS ARE NOT PERMITTED.
- BUTT JOINT BETWEEN PAVED SHOULDER AND DRIVEWAY ASPHALT.
- PERIMETER OF ALL PAVEMENT REPAIRS OR OTHER ASPHALT INLAYS WHEN PAVEMENT REPAIRS/INLAYS ARE NOT OVERLAIDED WITH AN ASPHALT CONCRETE SURFACE COURSE.
- ALL COLD LONGITUDINAL JOINTS BETWEEN PAVED SHOULDER AND GUARDRAIL ASPHALT.

THE MATERIAL USED SHALL BE CERTIFIED 702.21 PG BINDER. THE WIDTH OF THE SEALER SHALL BE 2-3 INCHES.

ANY ADDITIONAL COSTS ASSOCIATED WITH THE WORK IDENTIFIED IN THIS NOTE SHALL BE INCLUDED IN THE APPROPRIATE ASPHALT CONCRETE SURFACE COURSE ITEM OF WORK.

### ITEM 254 PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (3")

THE INTENT OF THE PLANING IS TO MILL DOWN 3 INCHES OF THE SURFACE ASPHALT CONTINGENT ON THE FOLLOWING: MAXIMUM CROSS SLOPE SHALL BE 0.02 WHILE THE MINIMUM CROSS SLOPE SHALL BE 0.01. THE PREFERRED CROSS SLOPE IS 0.16.

THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE TO ALL CATCH BASINS AND INLETS.

THE PROGRESSION OF THE PLANING SHALL PROCEED IN SUCH A MANNER THAT NORMAL TRAFFIC WILL NOT BE REQUIRED TO RUN OVER THE PLANED ROADWAY SURFACE MORE THAN FOURTEEN (14) CALENDAR DAYS. SEE C&MS 108.07 FOR \$/DAY PLANED SURFACE DISINCENTIVE.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT PLANING, ASPHALT CONCRETE. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF ITEM 254- PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (3").

### BUTT JOINT

BUTT JOINTS SHALL NOT BE CUT AND LEFT OPEN TO TRAFFIC. THEY SHALL BE FILLED IN WITH A TEMPORARY ASPHALT CONCRETE WEDGE PAID FOR UNDER ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN, LUMP SUM.

CONSTRUCTION "BUMP" (W8-1-36) AND ADVISORY SPEED (W13-1-24) SIGNS SHALL BE ERECTED AND MAINTAINED DURING THE PERIOD THE BUTT JOINT IS LEFT OPEN. THESE SIGNS SHALL BE PAID FOR UNDER THE LUMP SUM ITEM FOR ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

### ITEM 251- PARTIAL DEPTH PAVEMENT REPAIR (441), AS PER PLAN

THE ENGINEER SHALL IDENTIFY AREAS REQUIRING PARTIAL DEPTH REPAIR AFTER INITIAL PLANING IS COMPLETE. ALL APPLICABLE PROVISIONS OF ITEM 251, AS SET FORTH IN THE CONSTRUCTION AND MATERIAL SPECIFICATIONS, SHALL APPLY EXCEPT AS MODIFIED HEREIN.

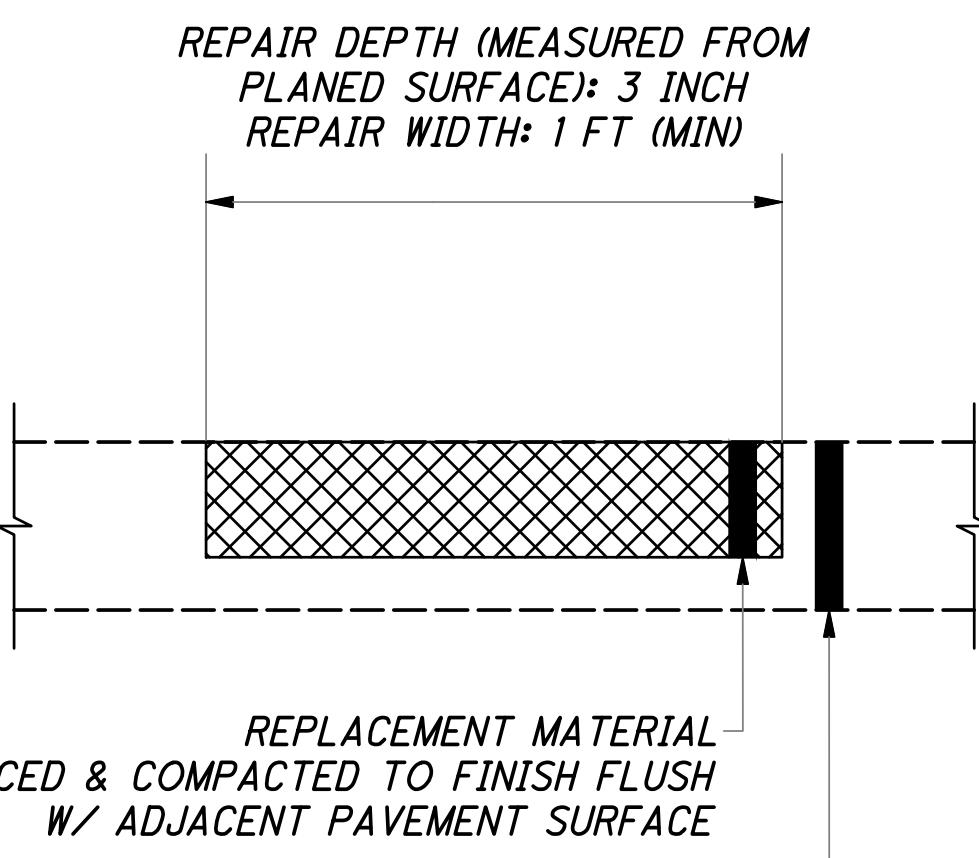
PAVEMENT REPAIR SHALL BE PERFORMED AFTER PAVEMENT PLANING AND BEFORE PLACEMENT OF THE INTERMEDIATE AND/OR SURFACE COURSE. THE CONTRACTOR SHALL BE CAPABLE OF PERFORMING PAVEMENT REPAIRS TO A MINIMUM OF 1 FOOT WIDE.

REPLACEMENT MATERIAL SHALL BE TYPE 2 MATERIAL PER C&MS 441 AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIRS. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF ITEM 251- PARTIAL DEPTH PAVEMENT REPAIR (441), AS PER PLAN. THE FOLLOWING ESTIMATED QUANTITY IS PROVIDED IN GENERAL SUMMARY TO BE USED AT LOCATIONS DETERMINED BY THE ENGINEER:

### ITEM 251- PARTIAL DEPTH PAVEMENT REPAIR (441), AS PER PLAN

400 SY



## GENERAL NOTES

ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR (441), AS PER PLAN

DESIGN AGENCY  
STEPHEN HOVANESK & ASSOCIATES, INC.

DESIGNER MSL  
REVIEWER AWD 8/22/2025  
PROJECT ID 105894  
SHEET TOTAL P.8 36

## PAVEMENT CONT.

### ITEM 253- PAVEMENT REPAIR, AS PER PLAN

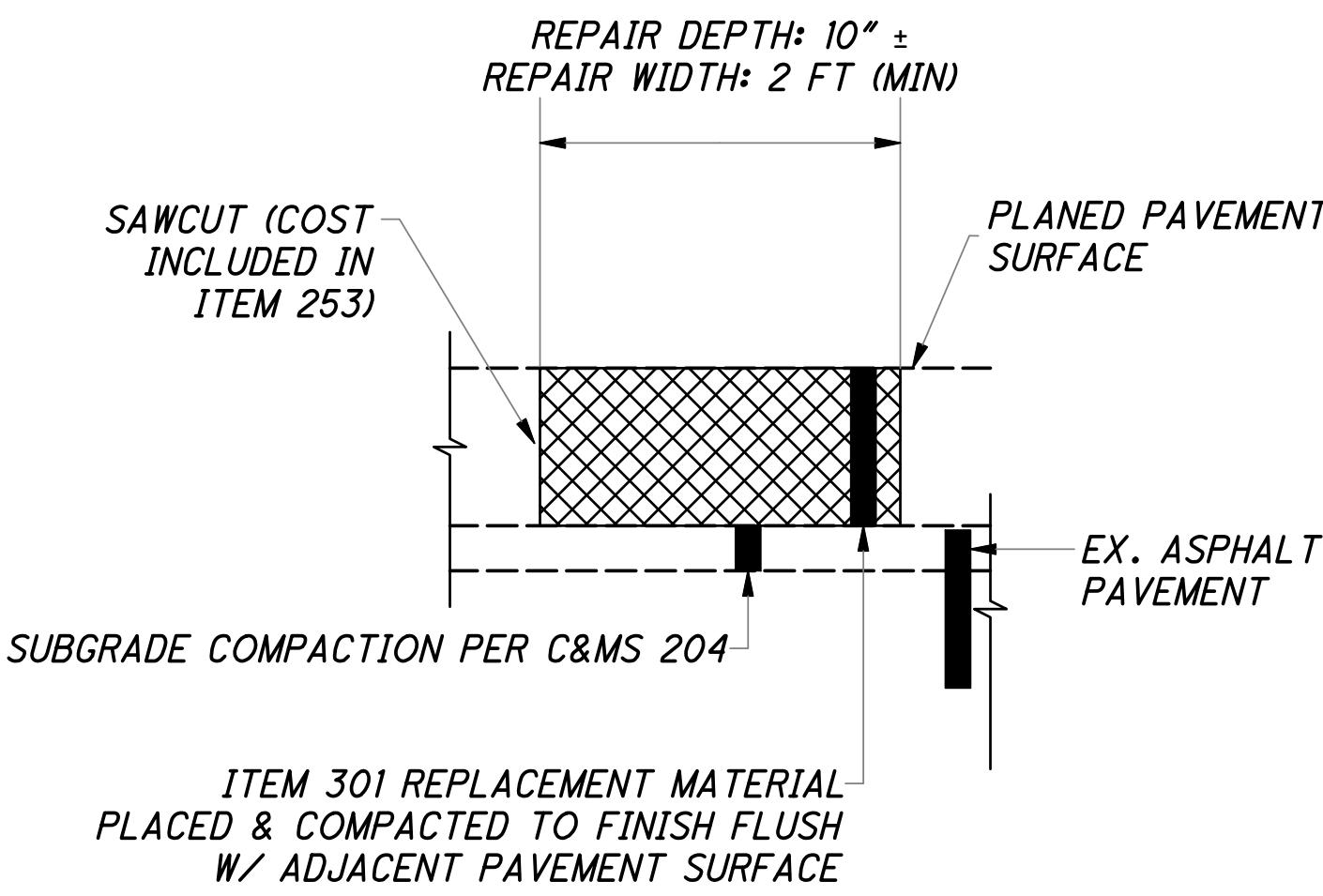
THIS ITEM SHALL CONSIST OF REMOVING THE EXISTING PAVEMENT, FULL DEPTH, AND REPLACING WITH FLEXIBLE PAVEMENT.

FOR ESTIMATING PURPOSES, THE DEPTH SHALL BE  $10'' \pm$  THE ENGINEER WILL DESIGNATE THE LOCATION AND LIMITS OF THE AREAS TO BE REPAIRED PRIOR TO ANY FULL DEPTH SAWCUTTING. CONTRACTOR TO SAWCUT THE LIMITS OF THE FULL DEPTH REPAIR AT THE LIMITS OF THE AREA DESIGNATED BY THE ENGINEER TO BE REPAIRED.

REPLACEMENT MATERIAL SHALL BE ASPHALT CONCRETE BASE PER C&MS 301, AND SHALL BE PLACED AND COMPAKTED TO FINISH FLUSH WITH ADJACENT PAVEMENT SURFACE. THE MAXIMUM SINGLE LIFT IS 6".

PAYMENT SHALL INCLUDE LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIRS. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER CUBIC YARD OF ITEM 253- PAVEMENT REPAIR, AS PER PLAN. THE FOLLOWING ESTIMATED QUANTITY IS PROVIDED IN THE GENERAL SUMMARY TO BE USED AT LOCATIONS DETERMINED BY THE ENGINEER:

ITEM 253- PAVEMENT REPAIR, AS PER PLAN 300 CY



### ITEM 253- PAVEMENT REPAIR, AS PER PLAN

### ITEM 255- FULL DEPTH PAVEMENT REPAIR AND RIGID REPLACEMENT, TYPE 2, CLASS QC MS, AS PER PLAN

WHEN THIS ITEM IS CALLED FOR ON THE PLANS OR IN THE PROPOSAL, ALL APPLICABLE PROVISIONS OF ITEM 255, AS SET FORTH IN THE CONSTRUCTION AND MATERIAL SPECIFICATIONS, SHALL APPLY EXCEPT AS MODIFIED HEREIN.

#### 255.04 CORRECTION OF SUBGRADE (AND SUBBASE).

SUITABLE SUBBASE DISTURBED IN AREAS WHERE CONCRETE PAVEMENT IS REMOVED SHALL BE SHAPED AND COMPAKTED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST. UNSUITABLE SUBBASE, AS DETERMINED BY THE ENGINEER, SHALL BE REMOVED AND REPLACES WITH "ITEM 304 - AGGREGATE BASE, AS PER PLAN" TO THE DEPTH OF ADJACENT SUBBASE.

WHERE UNSUITABLE SUBGRADE MATERIAL IS ENCOUNTERED, IT SHALL BE REMOVED TO THE DEPTH DETERMINED BY THE ENGINEER, AND REPLACED IN EIGHT (8) INCH MAXIMUM (LOOSE DEPTH) MECHANICALLY COMPAKTED LAYERS. SUITABLE EMBANKMENT MATERIAL (204.02) REQUIRED TO REPLACE THE UNDERCUT SUBGRADE SHALL, TO THE EXTENT POSSIBLE, EXHIBIT THE SAME PHYSICAL PROPERTIES AS THE ADJACENT SOUND SUBGRADE MATERIALS. HOWEVER, USE OF SLAG, IN ANY FORM, IS NOT PERMITTED. ALL EXPOSED OR RECONSTRUCTED SUBGRADE SOILS SHALL BE COMPAKTED TO THE SATISFACTION OF THE ENGINEER. IN CONJUNCTION WITH THE ABOVE SUBGRADE WORK, AN ESTIMATED QUANTITY OF ITEM 204 - GEOTEXTILE FABRIC AND ITEM 204 - GEOGRID ARE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER.

REMOVAL AND DISPOSAL OF UNSUITABLE SUBBASE OR SUBGRADE MATERIAL SHALL NOT BE CONSIDERED INCIDENTAL TO ITEM 255 AND NO SEPARATE PAYMENT WILL BE MADE.

#### 255.061 CURB REPLACEMENT

INTEGRAL CONCRETE CURB REMOVED IN CONJUNCTION WITH THE PAVEMENT REMOVAL OPERATIONS SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM 255 PAVEMENT REMOVAL. INTEGRAL CONCRETE CURB TO BE REMOVED SHALL BE REPLACED (IN KIND) AND PAID FOR UNDER ITEM 609 - CURB, TYPE 2-A USING CLASS QC MS CONCRETE.

#### 255.09 METHOD OF MEASUREMENT.

THE REPLACEMENT MATERIAL FOR UNSUITABLE SUBBASE SHALL BE FURNISHED, PLACED, MEASURED AND PAID FOR IN ACCORDANCE WITH ITEM 304 - AGGREGATE BASE, AS PER PLAN.

#### 255.10 BASIS FOR PAYMENT.

PAYMENT FOR ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, TYPE 2, CLASS QC MS, AS PER PLAN IS FULL COMPENSATION FOR FURNISHING ALL MATERIALS AND LABOR PER 255.10 AND AS SPECIFIED HEREIN, INCLUDING BUT NOT LIMITED TO INTEGRAL CONCRETE CURB AND/OR PAVEMENT REMOVAL, SUBBASE/SUBGRADE CORRECTION AND/OR REMOVAL, AS NECESSARY, FURNISHING AND PLACING DOWELS, TIE BARS, MESH AND THE CLASS QC MS CONCRETE.

THE FOLLOWING ESTIMATED QUANTITIES TO BE USED AS DIRECTED BY THE ENGINEER, ARE CARRIED TO THE GENERAL SUMMARY FOR THIS WORK AND PAYMENT IS INCLUDED IN THE CONTRACT UNIT PRICE BID FOR:

ITEM 204 - GEOTEXTILE FABRIC	<u>70 SY</u>
ITEM 204 - GEOGRID	<u>70 SY</u>
ITEM 204 - GRANULAR MATERIAL, TYPE B	<u>8 CY</u>
ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, TYPE 2, CLASS QC MS, AS PER PLAN	<u>110 SY</u>
ITEM 255 - FULL DEPTH PAVEMENT SAWING	<u>150 FT</u>
ITEM 304 - AGGREGATE BASE, AS PER PLAN	<u>24 CY</u>
ITEM 609 - CURB, TYPE 2-A	<u>100 FT</u>

#### PLANED SURFACES

THE DURATION OF THE ITEM BETWEEN MILLING AND PLACEMENT OF THE INTERMEDIATE COURSE SHALL BE NO LONGER THAN FOURTEEN (14) DAYS, UNLESS MOT NOTES STATES OTHERWISE. THE LIMIT SHALL BEGIN ON THE FIRST DAY OF PLANING AND SHALL CONTINUE BASED ON CALENDAR DAYS, MINUS ANY BAD WEATHER DAYS, UNTIL COMPLETION OF THE ASPHALT CONCRETE INTERMEDIATE COURSE.

#### ITEM 304 - AGGREGATE BASE, AS PER PLAN

MATERIAL FURNISHED FOR THIS ITEM SHALL BE LIMITED TO CRUSHED CARBONATE STONE.

### ITEM 202 - CURB REMOVED, AS PER PLAN

ALL REQUIREMENTS OF ODOT CMS 202 SHALL APPLY EXCEPT AS MODIFIED HEREIN:

ALL SAWCUTTING SHALL BE IN ACCORDANCE WITH ODOT CMS 255 AND SHALL BE CONSIDERED INCIDENTAL TO THIS ITEM.

THE FOLLOWING ESTIMATED CONTINGENCY QUANTITIES HAVE BEEN CARRIED THROUGH TO THE GENERAL SUMMARY, FOR USE AS DIRECTED BY THE ENGINEER:

ITEM 202 - CURB REMOVED, AS PER PLAN 100 FT

### ITEM 209 - PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN

PREPARE THE SHOULDER FOR PAVING A CONSISTENT SAFETY EDGE IN BOTH THICKNESS AND WIDTH. PRIOR TO PAVING THE SAFETY EDGE, GRADE AN AREA  $10''$  WIDE, BEGINNING AT THE EDGE OF THE PAVED ROADWAY, TO PROVIDE A LEVEL SURFACE FREE OF VEGETATION FOR CONSTRUCTION OF THE SAFETY EDGE. IF NECESSARY, EXCAVATE THE GRADED AREA TO THE DEPTH NECESSARY TO CONSTRUCT THE SAFETY EDGE. COMPACT THE GRADED SHOULDER ACCORDING TO 617.05, OR AS DIRECTED BY THE ENGINEER. THIS ITEM OF WORK SHALL ALSO INCLUDE THE GRADING OF THE SHOULDER BEYOND THE LIMITS OF THE SAFETY EDGE TO ELIMINATE HIGH SPOTS AND PROVIDE POSITIVE SHEET FLOW OFF THE PAVEMENT INTO ROADSIDE DITCHES OR DRAINAGE STRUCTURES. THIS ITEM IS INTENDED FOR PREPARATION OF THE SHOULDER FOR PLACEMENT OF ITEM 617 - COMPAKTED AGGREGATE, AS PER PLAN. ANY DEBRIS COLLECTED SHALL BE REMOVED AND DISPOSED OF AS SPECIFIED IN SECTION 105.16 & 105.17 OF THE CMS. PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT BID PRICE FOR ITEM 209 - PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO PERFORM THE WORK.

### ITEM 617 - COMPAKTED AGGREGATE, AS PER PLAN

THIS ITEM SHALL BE USED TO PLACE COMPAKTED AGGREGATE ALONG THE SHOULDERS. MATERIAL SHALL BE LIMITED TO RECLAIMED ASPHALT PAVEMENT (RAP). THE ACTUAL DEPTH OF COMPAKTED AGGREGATE PLACED WILL VARY DEPENDING UPON EXISTING CONDITIONS. FOR ESTIMATING PURPOSES, AN AVERAGE DEPTH OF TWO INCH ( $2''$ ) AT A TWO FOOT WIDTH HAS BEEN USED. WATER, IF NEEDED, SHALL BE APPLIED ACCORDING TO 617.05 AND SHALL BE INCLUDED WITH ITEM 617 - COMPAKTED AGGREGATE, AS PER PLAN.

### ITEM 441 ASPHALT CONCRETE INTERMEDIATE COURSE TYPE 2, (446), PG64-22, AS PER PLAN

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT.

ALL OPEN TRANSVERSE JOINTS SHALL BE TAMPERED TO MEET EXISTING PAVEMENT BEFORE INTRODUCING TRAFFIC, A "BUMP" SIGN (MB-1-36) SHALL BE ERECTED ON EACH SIDE OF TRANSVERSE JOINTS LEFT OPEN OVER NIGHT, INCLUDING A SPEED ADVISORY SIGN. THESE SIGNS SHALL BE REMOVED IMMEDIATELY AFTER JOINT HAS BEEN CLOSED. PAYMENT OF SIGNS SHALL BE PAID FOR UNDER THE LUMP SUM ITEM FOR ITEM 614 MAINTAINING TRAFFIC. REQUIREMENTS OF 441 APPLY.

AN ADDITIONAL QUANTITY OF ITEM 441 ASPHALT CONCRETE INTERMEDIATE COURSE TYPE 2 APP HAS BEEN INCLUDED TO BE USED AS A LEVELING COURSE FOR CORRECTION OF CROWN, PROFILE AND OTHER IRREGULARITIES AT LOCATIONS DETERMINED BY THE ENGINEER.

ITEM 441 ASPHALT CONCRETE INTERMEDIATE COURSE TYPE 2 (446) AS PER PLAN 50 CY

### ITEM 441 ASPHALT CONCRETE SURFACE COURSE TYPE 1, (446), PG70-22M, AS PER PLAN ITEM

### 441 ASPHALT CONCRETE SURFACE COURSE TYPE 1, (449), PG 64-22, AS PER PLAN

ALL REQUIREMENTS OF ODOT CMS 441 SHALL APPLY EXCEPT AS MODIFIED HEREIN:

BINDER SHALL BE PG70-22 FOR RIVER ROAD, AND PG 64-22 FOR INTERSECTIONS AS SHOWN IN PAGES 21-28. THE COARSE VIRGIN AGGREGATE FOR THIS ITEM SHALL CONSIST OF A BLEND OF 60% MINIMUM AIR COOLED BLAST FURNACE SLAG (ACBFS) OR TRAP ROCK FROM ONTARIO WITH LIMESTONE COMPRISING THE REMAINING PERCENTAGE.

CARE SHALL BE TAKEN TO MATCH EXISTING PAVEMENT ELEVATIONS AT EXISTING PAVED BERMS, DRIVE APRONS, INTERSECTIONS, ETC.

### ITEM 609 - COMBINATION CURB AND GUTTER, TYPE 2, AS PER PLAN ITEM 609 - CURB, TYPE 4-C, AS PER PLAN

EXISTING CURB SHALL BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER. MATCH THE EXISTING HEIGHT OF THE NEW CURB TO THE HEIGHT OF THE ADJACENT EXISTING CURB, AND TRANSITION THE FULL REVEAL (4" FOR TYPE 4-C & 6" FOR TYPE- 2) AT A RATE OF 1" CURB HEIGHT PER FOOT OF CURB LENGTH.

### ITEM 875 - LONGITUDINAL JOINT ADHESIVE

APPLY ASPHALTIC JOINT ADHESIVE ALONG CENTERLINE OF ROAD AND ANY OTHER LONGITUDINAL COLD JOINTS PRODUCED FROM PAVING OPERATIONS, OR AS DIRECTED BY THE ENGINEER. FURNISH MATERIALS IN ACCORDANCE WITH ODOT SUPPLEMENTAL SPECIFICATION 875.

### ITEM 632- DETECTOR LOOP, AS PER PLAN

DETECTOR LOOPS DISTURBED BY PAVEMENT PLANING, PAVEMENT REPAIR, OR BUTT JOINT OPERATIONS SHALL BE ABANDONED IN PLACE. THE NEW DETECTOR LOOP WIRE SHALL BE CUT INTO THE SURFACE COURSE OF THE ASPHALT. NEW LOOP DETECTORS SHALL BE PLACED AT THE SAME LOCATIONS AND BE THE SAME SIZE AND TYPE OF THE EXISTING, OR AS DETERMINED BY THE ENGINEER.

THE CONTRACTOR SHALL CONFIRM THAT ALL DETECTOR LOOPS HAVE BEEN DISCONNECTED BY THE CITY PRIOR TO THE START OF ANY PLANING OPERATIONS OR PAVEMENT REPAIR WORK THAT WILL DAMAGE LOOP INSTALLATIONS.

THE CONTRACTOR SHALL NOTIFY CITY OF WILLOUGHBY HILLS ENGINEER 216-377-3838 FIVE WORKING DAYS IN ADVANCE OF ANY PLANING OPERATIONS OR PAVEMENT REPAIR WORK THAT WILL DAMAGE DETECTOR LOOP INSTALLATIONS. THIS NOTIFICATION IS NEEDED TO DISCONNECT THE LOOP DETECTOR LOOPS ARE OUT OF OPERATION. THE CONTRACTOR SHALL THEN RENOTIFY THE CITY WITHIN TWO WORKING DAYS AFTER THE DAMAGED DETECTOR LOOPS ARE REPLACED SO THAT SIGNAL TIMINGS CAN BE RESTORED TO THE ORIGINAL SETTINGS.

THE NEW DETECTOR LOOPS WIRES SHALL BE RUN INTO EXISTING CONTROL BOX, POLE, OR PULLBOX, WHICHEVER IS APPLICABLE. INCLUDED IN THIS ITEM IS THE POURED EPOXY TYPE CABLE SPLICE KIT (CONFORMING TO 725.15) THAT MUST BE USED IN MAKING THESE CONNECTIONS. ALL NECESSARY MATERIAL, LABOR, SPLICE KITS AND EQUIPMENT FOR PROPER INSTALLATION AND OPERATION SHALL BE INCIDENTAL TO PAYMENT OF FOR ITEM 632, DETECTOR LOOP, AS PER PLAN.

## MAINTENANCE OF TRAFFIC NOTES

### ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN

THE MAKING OF THIS IMPROVEMENT REQUIRES THAT RIVER ROAD BE PARTIALLY CLOSED TO THROUGH TRAFFIC. ONE-WAY NORTHBOUND TRAFFIC WILL BE MAINTAINED DURING CONSTRUCTION. SOUTHBOUND RIVER ROAD TRAFFIC WILL BE DETOURED TO SOM CENTER ROAD. ON-STREET PARKING WILL NOT BE PERMITTED DURING CONSTRUCTION.

THE CONTRACTOR SHALL MAINTAIN ACCESS TO ADJACENT BUSINESSES THROUGH THE DURATION OF THE PROJECT. LOCAL ACCESS FOR ABUTTING PROPERTIES SHALL BE MAINTAINED AT ALL TIMES THROUGH THE USE OF PARTIAL WIDTH CONSTRUCTION AND BY MAINTAINING ONE (1) MINIMUM TEN (10) FOOT LANE FOR NORTHBOUND TRAFFIC.

THE CONTRACTOR SHALL MAINTAIN SAFE AND SATISFACTORY ACCESS TO ABUTTING PROPERTY AND ADJACENT STREETS. THE CONTRACTOR SHALL MAINTAIN SAFE AND CLEAR PEDESTRIAN MOVEMENT THROUGH THE CONSTRUCTION ZONE AT ALL TIMES. THE CONTRACTOR SHALL MAINTAIN ADEQUATE PEDESTRIAN WALKS AT ALL INTERSECTIONS, INCLUDING ASPHALT CONCRETE WALKS, WHERE DIRECTED BY THE ENGINEER.

TRAFFIC ON THE MAINLINE (ONE LANE IN THE NORTHBOUND DIRECTION), SIDE STREETS AND DRIVES SHALL BE MAINTAINED AT ALL TIMES, UNLESS OTHERWISE APPROVED BY THE ENGINEER, AND/OR THE DRIVE OWNER.

ALL CONSTRUCTION TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE MOST RECENT EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. ALL SIGNS, SUPPORTS, PAVEMENT MARKINGS AND OTHER TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR.

ALL CONSTRUCTION TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, LIGHTING, FLAGGERS, WORK ZONE PAVEMENT MARKINGS/STRIPING, DRUMS, ETC. SHALL BE PROVIDED SO AS TO AVOID DAMAGE AND/OR INJURY TO VEHICLES AND PERSONS USING THE ROADWAY DURING CONSTRUCTION.

EXISTING TRAFFIC CONTROL DEVICES (SIGNS AND/OR TRAFFIC SIGNALS), LOCATED WITHIN THE WORK AREA, WHICH ARE REQUIRED FOR INTERIM OR PERMANENT TRAFFIC CONTROL, SHALL BE RELOCATED TO POINTS APPROVED BY THE ENGINEER.

APPROPRIATE TRAFFIC CONTROL DEVICES SHALL BE MAINTAINED, IN COMPLIANCE WITH THE MANUAL, AT ALL TIMES WHILE TRAFFIC IS MAINTAINED. THE COST OF RELOCATION, IF REQUIRED, SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN.

NO ROAD SHALL BE CLOSED UNTIL DEEMED NECESSARY BY THE ENGINEER. THE PLACEMENT OF THE SIGNS AND DRUMS SHALL BE MADE SIMULTANEOUSLY. NO DRUMS SHALL BE PLACED WITHOUT PROPER SIGNAGE IN PLACE. SIGNS SHALL BE COVERED OR REMOVED FROM THE VIEW OF TRAFFIC WHENEVER THEY ARE NOT APPLICABLE.

THE CONTRACTOR SHALL DIVERT TRAFFIC FROM NORMAL CHANNELS BY PLASTIC DRUMS, FLASHING ARROW PANELS COMPLYING WITH ODOT CMS 614 AND TRAFFIC SIGNS AND WORK ZONE PAVEMENT MARKINGS AS SHOWN ON THESE PLANS. PAYMENTS FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

### NOTIFICATION

THE CONTRACTOR SHALL NOTIFY IN WRITING THE FOLLOWING AGENCIES AT LEAST TWO (2) WEEKS PRIOR TO THE START OF CONSTRUCTION, AND AT LEAST SEVENTY-TWO (72) HOURS BEFORE IMPLEMENTING ANY SUBSTANTIAL CHANGES IN TRAFFIC PATTERNS OF CLOSING OF ANY STREET TO TRAFFIC:

WILLOUGHBY HILLS POLICE DEPT. 440-942-9111  
WILLOUGHBY HILLS FIRE DEPT. 440-942-7207  
WILLOUGHBY EASTLAKE SCHOOL DIST. 440-283-4120  
MAJOR WASTE DISPOSAL 440-254-4929

### ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, TWO (2) PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS), ON SITE, WITH MINIMUM LEGIBILITY DISTANCES OF 475 FEET, TWO (2) WEEKS PRIOR TO PROJECT MOBILIZATION.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. PCMS TRAILERS SHALL BE DELINEATED ON A PERMANENT BASIS BY AFFIXING CONSPICUITY TAPE CONFORMING TO ODOT C&MS 614.03, IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER AS SEEN BY ONCOMING ROAD USERS. THE PCMS SHALL BE LOCATED BY THE CONTRACTOR AT THE DIRECTION OF THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

ALL MESSAGES TO BE DISPLAYED ON THE PCMS WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE PCMS SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, AND MESSAGE ADDITIONS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.

THE PCMS SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF ODOT C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN TWELVE (12) HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND/OR IN THE CITY TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE CITY DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONIES DUE, OR TO BECOME DUE THE CONTRACTOR ON THIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THE PCMS FOR THE DURATION OF THE PROJECT.

PAYMENT FOR THIS ITEM, INCLUDING BUT NOT LIMITED TO, ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE, AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

### ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

9 SNMT

### COORDINATION

THE ENGINEER AND CONTRACTOR SHALL WORK WITH THE LOCAL BUSINESS OWNERS, AND RESIDENTS TO ENSURE ACCESS TO ALL PROPERTIES AT ALL TIMES. HOWEVER, SOME INCONVENIENCES WILL OCCUR. NO ADDITIONAL PAYMENT WILL BE MADE FOR THE COORDINATION OR PROVIDING ACCESS TO THE BUSINESSES AND RESIDENCES.

### CONSTRUCTION TRAFFIC

ALL CONSTRUCTION TRAFFIC SHALL USE ACCEPTABLE TRUCK ROUTES TO ACCESS THE CONSTRUCTION AREA. USE OF LOCAL RESIDENTIAL STREETS IS STRICTLY PROHIBITED UNLESS ALLOWED IN WRITING BY THE LOCAL ENFORCEMENT AUTHORITY.

### NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAME SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

### NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE

ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	2 WEEKS OR MORE BETWEEN 12 HOURS AND 2 WEEKS 12 HOURS OR LESS	21 CALENDAR DAYS PRIOR TO CLOSURE 14 CALENDAR DAYS PRIOR TO CLOSURE 4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	2 WEEKS OR MORE LESS THAN 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE 5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TABLE

### TEMPORARY MAINTENANCE OF EXISTING TRAFFIC SIGNALS

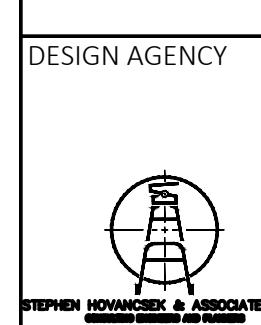
INCIDENTAL TO THE REQUIREMENTS FOR MAINTAINING TRAFFIC IN ACCORDANCE WITH 614.03, EXISTING TRAFFIC SIGNALS AT THE FOLLOWING INTERSECTIONS SHALL BE TEMPORARILY MAINTAINED UNTIL THE PROJECT IS COMPLETE AND THE EXISTING OPERATION CAN BE RESTORED:

RIVER ROAD AND CHARDON ROAD - NORTHBOUND

THE EXISTING SIGNAL HEADS (OR ADDITIONAL TRAFFIC SIGNAL HEADS SUPPLIED BY THE CONTRACTOR) SHALL BE POSITIONED SO AS TO PROVIDE A MINIMUM OF TWO TRAFFIC SIGNAL HEADS OVER THE PORTION OF THE ROADWAY USED BY NORTHBOUND TRAFFIC, AND THE OPERATION SHALL BE MODIFIED AS NEEDED. THE NUMBER, LOCATION, VISIBILITY, AND HEIGHT OF ALL TRAFFIC SIGNALS SHALL BE IN ACCORDANCE WITH THE PROVISIONS OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (OMUTCD).

ANY COSTS FOR THE TEMPORARY MAINTENANCE OF EXISTING TRAFFIC SIGNALS SHALL BE INCLUDED IN THE LUMP SUM COST BID FOR ITEM 614 - MAINTAINING TRAFFIC.

MOT NOTES



TRAFFIC SIGNING

ADVANCE TRAFFIC SIGNING, INCLUDING DETOUR SIGNING, CONSTRUCTION WORK ZONE APPROACH SIGNING, BARRICADES AND SIGNS ON BARRICADES BEYOND THE WORK LIMITS SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR.

TEMPORARY PAVING OF VERTICAL SURFACES

IN ORDER TO PROVIDE FOR LOCAL ACCESS, LONGITUDINAL VERTICAL FACES ABUTTING DRIVES SHALL BE TEMPORARILY RAMPED. TRANSVERSE VERTICAL FACES SHALL BE TEMPORARILY RAMPED A MINIMUM OF TEN (10) FEET IN LENGTH AND TRAFFIC SHALL BE WARNED WITH WB-1 "BUMP" SIGNS IN ADVANCE OF THE RAMPED AREAS.

ALL CASTINGS ENCOUNTERED SHALL BE SET TO GRADE AND PAID FOR UNDER VARIOUS ITEMS DESCRIBED ELSEWHERE IN THE GENERAL NOTES OR SPECIFICATIONS. WHEN EXPOSED TO TRAFFIC, CASTINGS MAY BE REQUIRED TO BE ADJUSTED MORE THAN ONCE, BUT PAYMENTS FOR ADJUSTING WILL ONLY BE PAID ONCE. WHEN CONSTRUCTION IS ADJACENT TO DRIVES, MAINTAIN ACCESS TO DRIVES BY ONLY CONSTRUCTING THE PORTIONS OF THE ROADWAY NOT IN CONFLICT WITH THE DRIVES. ADDITIONAL CONSTRUCTION JOINTS SHALL BE ALLOWED BY THE ENGINEER. IN ORDER TO PROVIDE FOR LOCAL ACCESS, TEMPORARY RAMPING SHALL BE INSTALLED WITH ASPHALT CONCRETE FOR MAINTAINING TRAFFIC. PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO PROVIDE FOR TEMPORARY RAMPING SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC.

THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC:

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC 100 CY

SEQUENCE OF CONSTRUCTION

THE CONTRACTOR SHALL CONTACT AND WORK WITH THE CITY OF WILLOUGHBY HILLS TO MAKE SIGNAL PHASING AND TIMING ADJUSTMENTS, AS NEEDED TO MOVE TRAFFIC AS EFFICIENTLY AS POSSIBLE. TURN LANES SHALL BE MAINTAINED WHEREVER POSSIBLE AND BY MOVING DRUMS AS APPROVED OR AS DIRECTED BY THE PROJECT REPRESENTATIVE. PAYMENT INCLUDED IN ITEM 614-MAINTAINING TRAFFIC, AS PER PLAN.

THE CONTRACTOR SHALL MAINTAIN NORTHBOUND TRAFFIC AT ALL TIMES DURING THE PAVING OPERATIONS/WORK, SOUTHBOUND TRAFFIC SHALL BE DETOURED TO SOM CENTER ROAD.

CONSTRUCTION PHASINGPHASE 1

PHASE 1 SHALL CONSIST OF ALL WORK BETWEEN ROGERS ROAD AND CHARDON ROAD (US-6).

PHASE 1A

THE CONTRACTOR SHALL CLOSE THE SOUTHBOUND LANE OF RIVER ROAD AND DIVERT TRAFFIC TO SOM CENTER ROAD. THE NORTHBOUND LANE MUST BE MAINTAINED AT ALL TIME. THE CONTRACTOR SHALL PERFORM ALL ROADWAY IMPROVEMENTS IN THE SOUTHBOUND LANE BETWEEN ROGERS ROAD AND CHARDON ROAD (US-6).

PHASE 1B

THE CONTRACTOR SHALL SHIFT RIVER ROAD NORTHBOUND TRAFFIC TO THE SOUTHBOUND LANE. SOUTHBOUND TRAFFIC SHALL CONTINUE TO BE DIVERTED TO SOM CENTER ROAD. THE CONTRACTOR SHALL PERFORM ALL ROADWAY IMPROVEMENTS IN THE NORTHBOUND LANE BETWEEN ROGERS ROAD AND CHARDON ROAD (US-6).

PHASE 1C

THE CONTRACTOR SHALL PERFORM ALL FINAL PAVEMENT MARKING OPERATIONS. TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH MT-99.20.

PHASE 2

PHASE 2 SHALL CONSIST OF ALL WORK BETWEEN CHARDON ROAD (US-6) AND GULLY BROOK PARK (END PROJECT LIMITS).

CONSTRUCTION PHASING (CONT.)PHASE 2A

THE CONTRACTOR SHALL CLOSE THE SOUTHBOUND LANE OF RIVER ROAD AND DIVERT TRAFFIC TO SOM CENTER ROAD. THE NORTHBOUND LANE MUST BE MAINTAINED AT ALL TIME. THE CONTRACTOR SHALL PERFORM ALL ROADWAY IMPROVEMENTS IN THE SOUTHBOUND LANE BETWEEN CHARDON ROAD (US-6) AND GULLY BROOK PARK.

PHASE 2B

THE CONTRACTOR SHALL SHIFT RIVER ROAD NORTHBOUND TRAFFIC TO THE SOUTHBOUND LANE. SOUTHBOUND TRAFFIC SHALL CONTINUE TO BE DIVERTED TO SOM CENTER ROAD. THE CONTRACTOR SHALL PERFORM ALL ROADWAY IMPROVEMENTS IN THE NORTHBOUND LANE BETWEEN CHARDON ROAD (US-6) AND GULLY BROOK PARK.

PHASE 2C

THE CONTRACTOR SHALL PERFORM ALL FINAL PAVEMENT MARKING OPERATIONS. TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH MT-99.20.

REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES.

ITEM 616, WATER 50 MGAL  
ITEM 616, CALCIUM CHLORIDE 14 TON

ADVANCED WORK ZONE INFORMATION

ADVANCE WORK ZONE INFORMATION SIGNS, AS USED IN THIS NOTE, ARE FIXED MESSAGE TYPES. THE SIGNS ARE TO BE LOCATED AT EXTREME DISTANCE FROM THE WORK AREA, AS SHOWN IN THE PLANS.

THE SIGNS SHALL BE BLACK ON ORANGE (INCLUDING A BLACK BORDER). THE LAYOUT SHALL BE IN CONFORMANCE WITH TEM CHAPTER 211.

WHEN REGULATORY INFORMATION IS PROVIDED, IT SHALL BE DISPLAYED SEPARATELY AS A STANDARD BLACK-ON-WHITE SIGN. MIXING OF BLACK-ON-WHITE REGULATORY INFORMATION ON A BLACK-ON-ORANGE INFORMATION SIGN IS PROHIBITED.

IF THE MOTORIST IS BEING DETOURED OR IF AN ALTERNATE ROUTE IS PROVIDED, THE ROUTE SHOULD BE SIGNED WITH ASSEMBLIES CONSISTING OF THE APPROPRIATE BLACK-ON-ORANGE DETOUR OR ALT MARKER WITH A STANDARD ROUTE MARKER AND ARROW PLATE. IF MORE TARGET VALUE IS DESIRED, THIS TRAIL BLAZER INFORMATION MAY BE SHOWN ON AN ORANGE PANEL (OMUTCD SECTION 2D.32).

ROUTE SIGN ASSEMBLIES SHALL BE SIZED ACCORDING TO THE TYPE OF ROAD ON WHICH THEY ARE LOCATED IN ACCORDANCE WITH THE OMUTCD.

SUPPORTS FOR SIGN INSTALLATIONS SHALL CONFORM TO ALL EXISTING STANDARDS FOR PERMANENT SIGNS. THESE SIGNS SHOULD NOT BE ATTACHED TO EXISTING SUPPORTS.

WHERE THE PLANS CALL FOR AN OVERLAY TO COVER A PORTION OF AN EXISTING SIGN, THE OVERLAY SHALL BE BLACK-ON-ORANGE. LETTER SIZES SHOULD BE THE SAME AS ON THE EXISTING SIGNS. WHEN LANE ARROWS ARE TO BE COVERED, A BLANK OVERLAY SHOULD BE PLACED OVER EACH OF THE AFFECTED ARROWS.

ADVANCED WORK ZONE INFORMATION (CONT.)

ALL ADVANCE WORK ZONE INFORMATION SIGN INSTALLATIONS LOCATED OUTSIDE OF THE PROJECT WORK LIMITS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN

WORK ZONE MARKINGS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC:

614 WORK ZONE EDGE LINE, CLASS 1, 4", 642 PAINT, WHITE  
614 WORK ZONE CENTERLINE, CLASS 1, 642 PAINT, YELLOW  
614 WORK ZONE STOP LINE, CLASS 1, 642 PAINT, WHITE

7.39 MILE  
3.68 MILE  
103 FEET

FAILURE TO COMPLY

IF THERE IS ANY FAILURE TO COMPLY WITH THE PROVISIONS FOR TRAFFIC CONTROL, AS OUTLINED IN THESE PLANS AND NOTES OR WITH THE PROVISIONS OF THE OMUTCD, THE ROADWAY IN THE VICINITY OF THE WORK AREA SHALL NOT BE CONSIDERED IN A CONDITION FOR SAFE AND CONVENIENT USE BY THE TRAVELING PUBLIC. ANY FAILURE TO KEEP THE ROADWAY IN THE VICINITY OF THE WORK AREA IN A CONDITION FOR THE SAFE AND CONVENIENT USE BY THE TRAVELING PUBLIC SHALL BE CONSIDERED A BREACH OF CONTRACT. WORK SHALL BE SUSPENDED UNTIL THE CONTRACTOR COMPLIES WITH THE PROVISIONS OF THE AFOREMENTIONED ITEMS.

HOLIDAY TRAFFIC LIMITATION

NO WORK SHALL BE PERFORMED, AND ALL EXISTING LANES WHICH ARE NOT INSIDE A DESIGNATED WORK ZONE SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

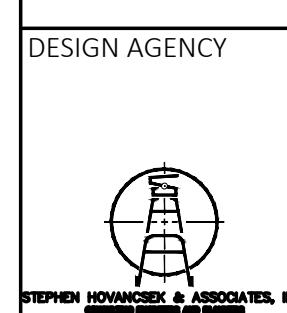
NEW YEARS DAY (JANUARY 1)  
MARTIN LUTHER KING DAY (THIRD MONDAY IN JANUARY)  
EASTER SUNDAY (DATE VARIES)  
MEMORIAL DAY (LAST MONDAY IN MAY)  
INDEPENDENCE DAY (JULY 4)  
LABOR DAY (FIRST MONDAY IN SEPTEMBER)  
THANKSGIVING DAY (FOURTH THURSDAY IN NOVEMBER)  
CHRISTMAS DAY (DECEMBER 25)

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES IN ACCORDANCE WITH 108.07.

CONTRACTOR'S VEHICLES AND EQUIPMENT

ALL VEHICLES AND EQUIPMENT BELONGING TO THE CONTRACTOR OR THE CONTRACTOR'S WORKERS MUST BE PARKED IN ACCORDANCE WITH CITY REGULATIONS, VEHICLES AND EQUIPMENT MAY NOT BE PARKED IN PRIVATE PARKING LOTS, PUBLIC SIDE STREETS, OR OTHER PRIVATE PROPERTY UNLESS WRITTEN APPROVAL OF THE OWNER AND THE ENGINEER HAS BEEN GRANTED. THE CONTRACTOR OR CONTRACTOR'S WORKERS MAY NOT PARK VEHICLES OR EQUIPMENT OR STORE MATERIALS WHERE IT IS DEEMED BY THE ENGINEER TO BE A SAFETY HAZARD; NOR IN MANNER WHICH OBSTRUCTS SIGNS, BARRIERS, BARRICADES, OR OTHER TRAFFIC CONTROL DEVICES OR INTERFERES WITH ACCESS TO ABUTTING PROPERTIES.



DESIGN AGENCY	STEPHEN NOWAKOWSKI & ASSOCIATES, INC.
DESIGNER	MSL
REVIEWER	
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CONSTRUCTION WARNING SIGNS

IMMEDIATELY PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL INSTALL THE CONSTRUCTION WARNING SIGNS SHOWN ON SHEETS 14-15.

AS A MINIMUM, G20-1 SIGNS FACING TRAFFIC ENTERING THE PROJECT, AND G20-2 SIGNS FACING TRAFFIC LEAVING THE PROJECT, SHALL BE PLACED AS SHOWN IN THE MANUAL. ADDITIONAL G20-1 SIGNS SHALL BE PLACED AFTER EACH MAJOR INTERSECTION, IN BOTH DIRECTIONS, AND AFTER EACH SUSPENSION AND RESUMPTION OF WORK.

ADDITIONALLY, A W20-1 SIGN SHALL BE PLACED ON EACH INTERSECTING STREET A MINIMUM OF 200 FEET IN ADVANCE OF THE PROJECT, AND ON THE APPROACHES TO THE PROJECT A MINIMUM OF 500 FEET IN ADVANCE OF THE WORK LIMITS. G20-2 SIGNS SHALL ALSO BE INSTALLED ON EACH MAJOR INTERSECTING STREET, FACING TRAFFIC LEAVING THE PROJECT, A MINIMUM OF 200 FEET FROM THE PROJECT.

THE TRAFFIC CONTROL DEVICES SHOWN ON MT-97.11, MT-95.31 AND MT-95.32 SHALL BE IN ADDITION TO THOSE INDICATED ABOVE. IF DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL ALSO INSTALL W8-11 "UNEVEN LANES", W8-7 "LOOSE GRAVEL," AND/OR W21-2 "FRESH OIL/TAR" SIGNS.

FLUORESCENT ORANGE TYPE G SIGN SHEETING SHALL BE USED FOR ALL DETOUR AND CONSTRUCTION WARNING SIGNS.

INSTALLATION OF WORK ZONE PAVEMENT MARKINGS AND SIGNS

ALL WORK ZONE PAVEMENT MARKINGS AND SIGNS REQUIRED FOR A PARTICULAR LANE CLOSURE OR TRAFFIC PATTERN SHALL BE INSTALLED ON A SINGLE WORK DAY, AND THE CORRESPONDING TRAFFIC PATTERN, AS DETAILED ON THE PLANS, SHALL BE IMPLEMENTED IMMEDIATELY. IN ADDITION, THE REQUIREMENTS OF 614.11(G) SHALL APPLY. THE CONTRACTOR SHALL BE RESPONSIBLE, WITH THE ASSISTANCE OF THE ENGINEER AND RECORDS OF THE DIVISION OF TRAFFIC ENGINEERING, FOR THE ENTIRE LAYOUT AND LOCATIONS OF THE REQUIRED PAVEMENT MARKINGS.

LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW SHALL NOT BE PERMITTED AT PROJECT COST NOR TIME COMPENSATION. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

- DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.
- DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).
- DURING PERIODS WHERE TRAFFIC NEEDS TO BE DIRECTED CONTRARY TO A TRAFFIC CONTROL DEVICE (FLAGGER, SIGN E.G. STOP SIGN, STREET OR HIGHWAY SIGNS, ETC), SIGNAL OR OTHER DEVICE USED TO REGULATE, WARN OR GUIDE TRAFFIC). TRAFFIC IN THIS INSTANCE INCLUDES VEHICULAR, PEDESTRIAN AND/OR SHARED USE PATH USERS.

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) MAY BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

- FOR LANE CLOSURES THAT MEET ALL OF THE CRITERIA LISTED BELOW: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONT.)

## CRITERIA:

- ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND,
- AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND,
- AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS AND/OR IN CONTRARY TO OTHER TRAFFIC CONTROL DEVICES IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE SHIFT DURATION SHALL NOT BE LESS THAN THE LEO'S MINIMUM SHOW-UP TIME REQUIRED BY THEIR LAW ENFORCEMENT AGENCY. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

## ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE

160 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

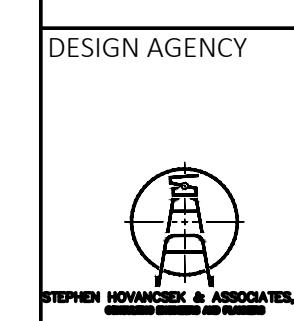
REPLACEMENT SIGNS

FLAT SHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS, AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE USED, BUT GOOD, CONDITION SUBJECT TO THE APPROVAL OF THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

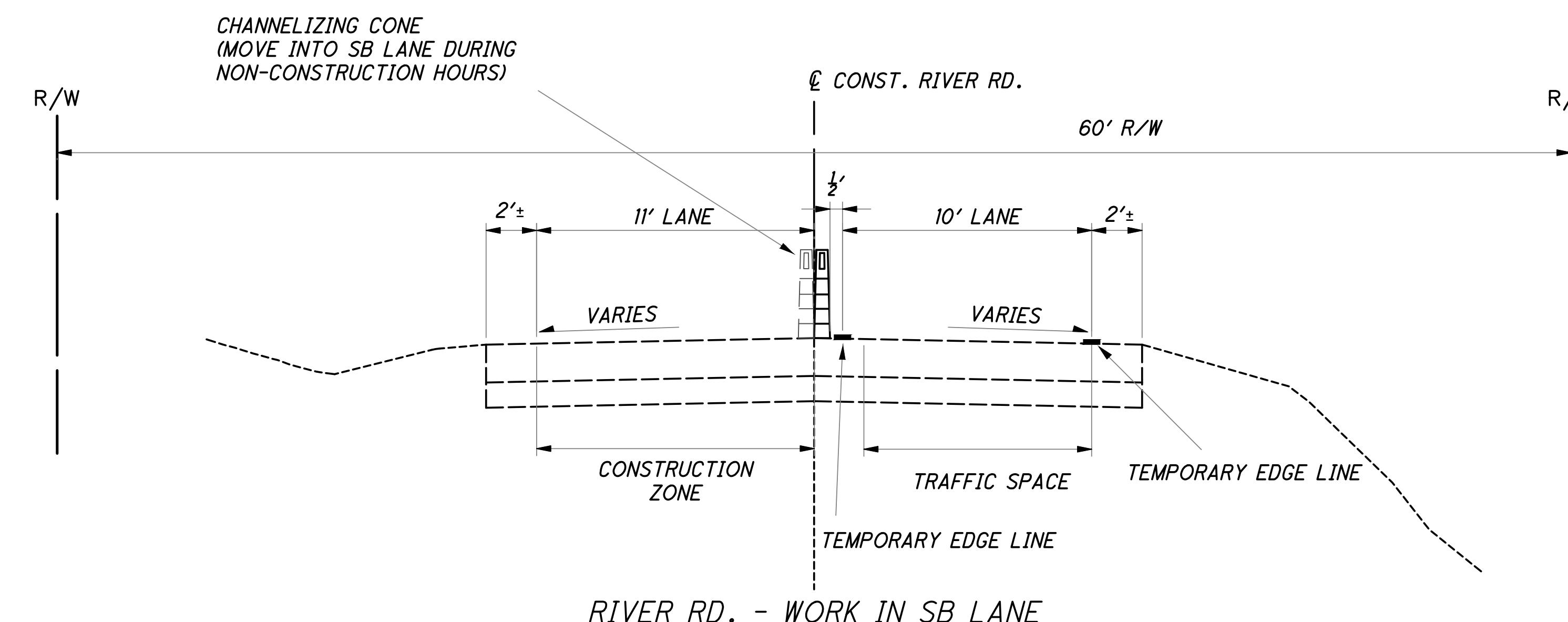
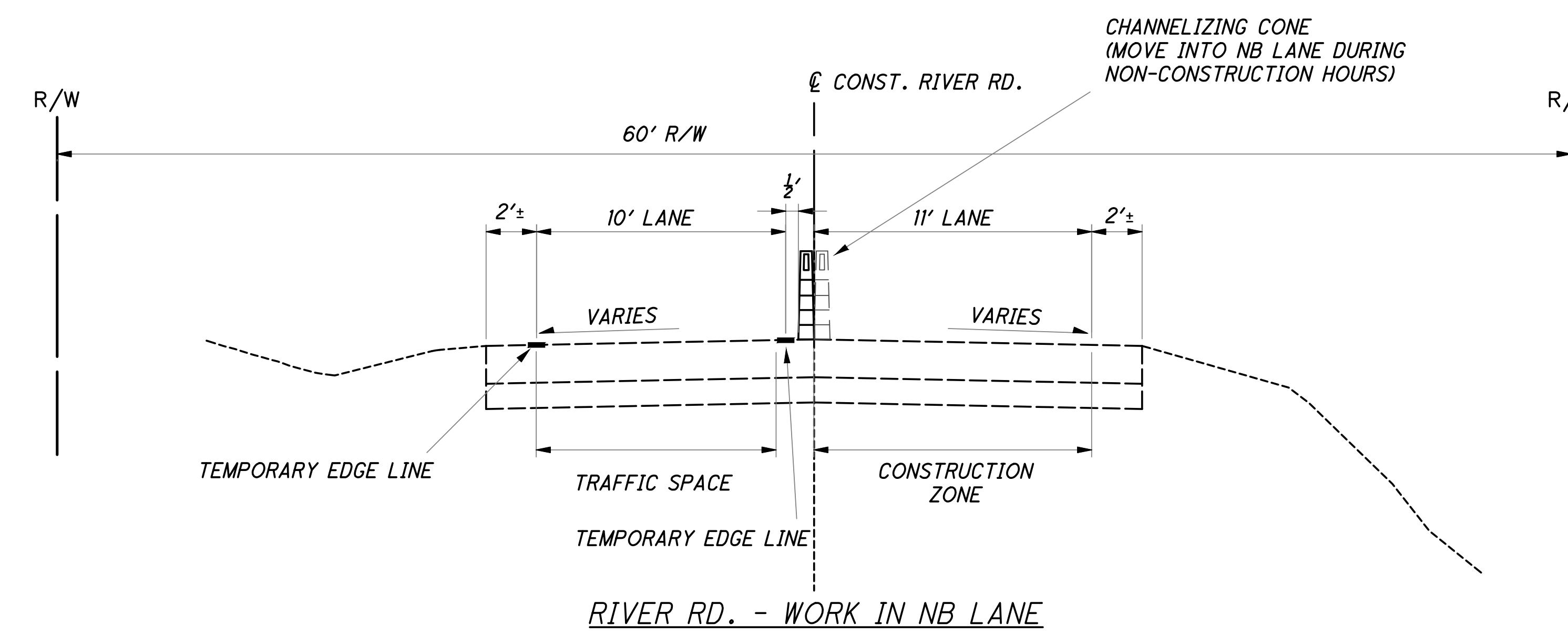
DRIVEWAY ACCESS

THIS WORK SHALL BE IN CONJUNCTION WITH ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN AND ALL COSTS INCURRED FOR THIS ITEM SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN - NO ADDITIONAL PAYMENTS WILL BE MADE. ACCESS TO ALL PROPERTY OWNERS, INCLUDING RESIDENCES AND BUSINESSES, SHALL BE MADE AVAILABLE AT ALL TIMES DURING CONSTRUCTION.

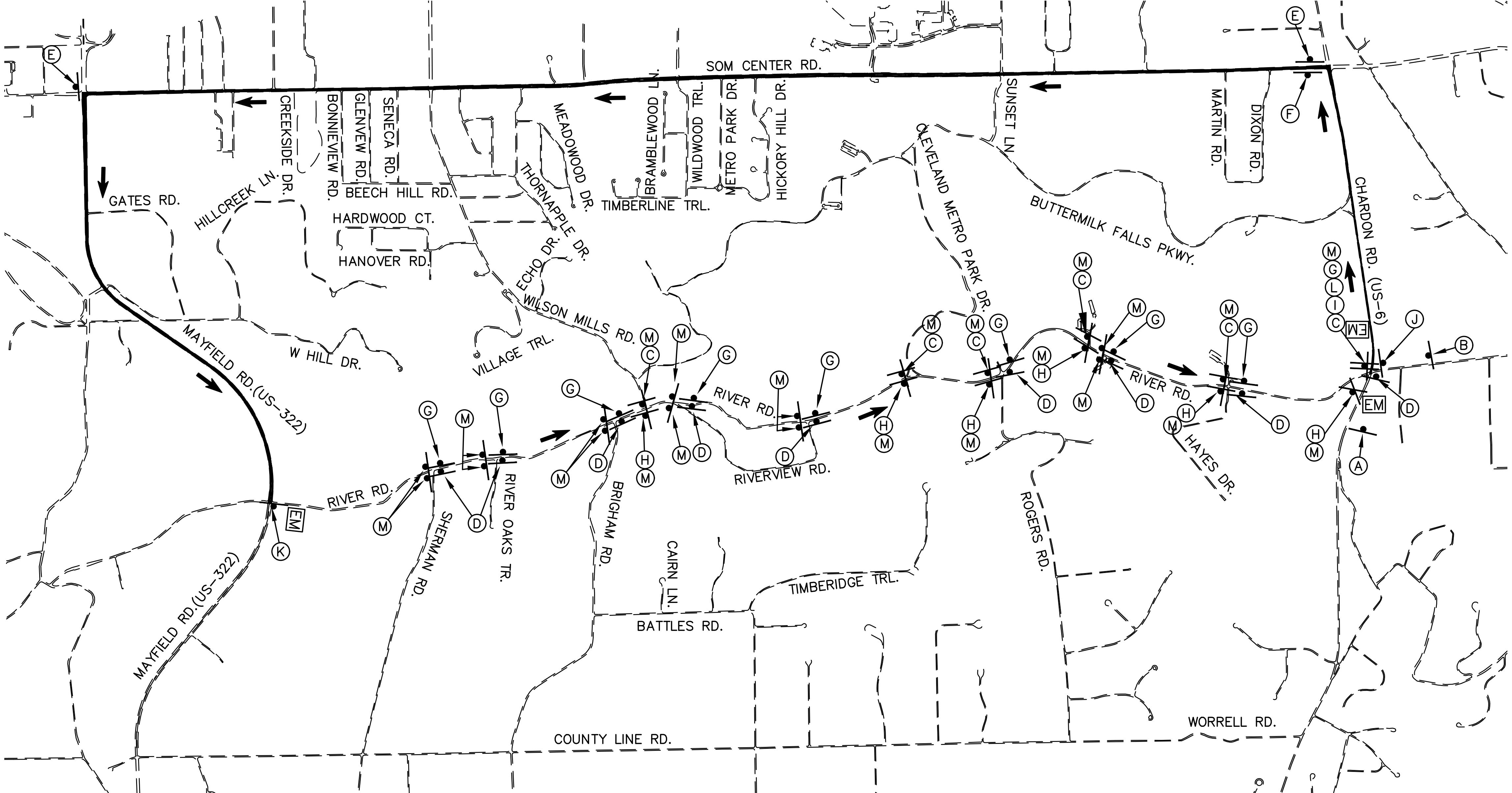


DESIGN AGENCY  
STEPHEN NOVAKOSKI & ASSOCIATES, INC.

DESIGNER	MSL
REVIEWER	AWD 8/22/2025
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P.12	36



# MOT DETOUR PLAN PHASE 1



NOTE:  
CONTRACTOR TO INSTALL ONE-WAY SIGNS FOR ALL DRIVEWAYS ON RIVER RD.

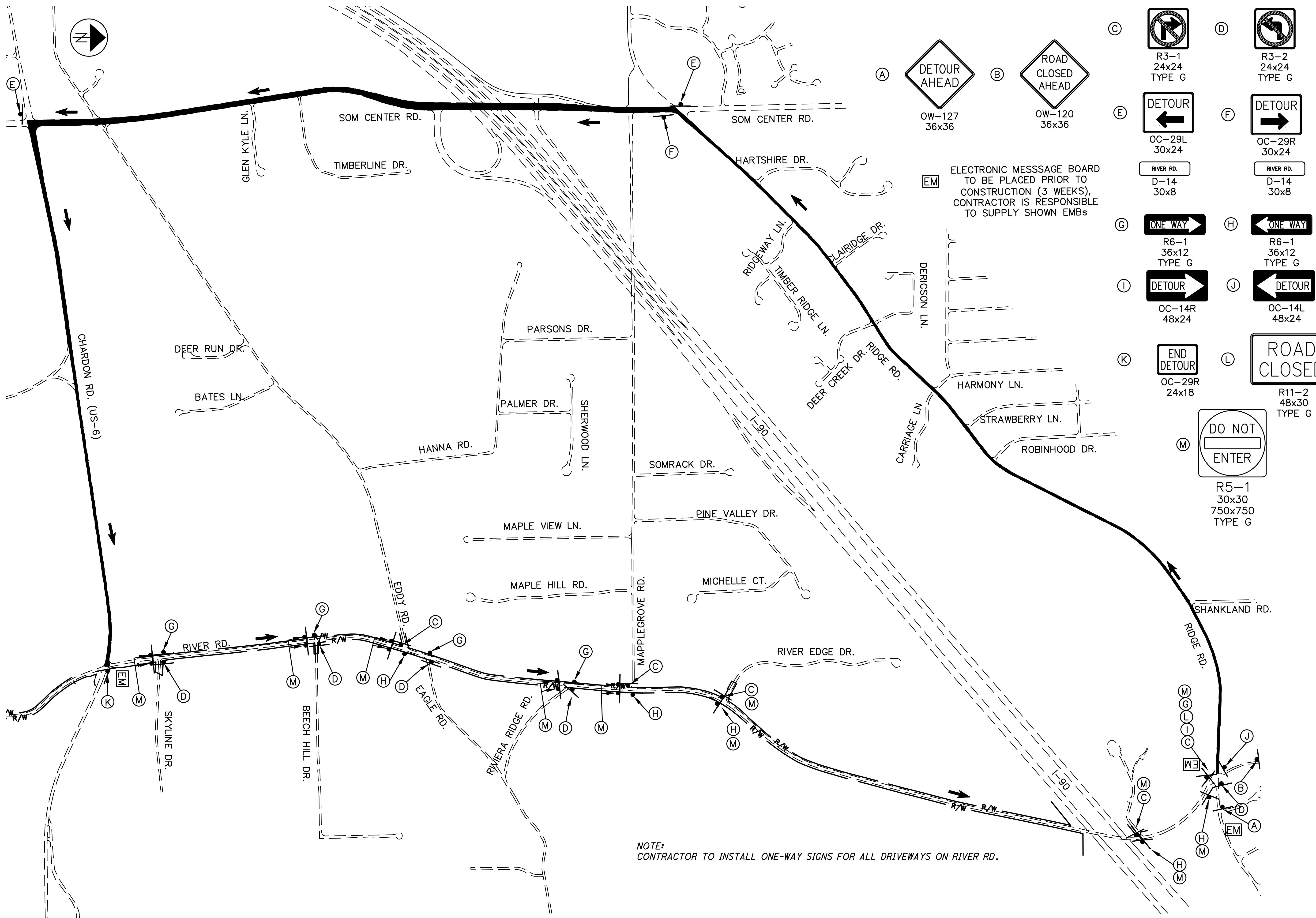
EM ELECTRONIC MESSAGE BOARD  
TO BE PLACED PRIOR TO  
CONSTRUCTION (3 WEEKS),  
CONTRACTOR IS RESPONSIBLE  
TO SUPPLY SHOWN EMBs

DESIGN AGENCY  
STEPHEN NOWAKOWSKI & ASSOCIATES, INC.

DESIGNER MSL  
REVIEWER AWD 8/22/2025  
PROJECT ID 105894  
SHEET TOTAL P.14 36

(A)	DETOUR AHEAD	(B)	ROAD CLOSED AHEAD
	OW-127 36x36		OW-120 36x36
(C)		(D)	
	R3-1 24x24 TYPE G		R3-2 24x24 TYPE G
(E)		(F)	
	OC-29L 30x24		OC-29R 30x24
	RIVER RD.		RIVER RD.
	D-14 30x8		D-14 30x8
(G)		(H)	
	R6-1 36x12 TYPE G		R6-1 36x12 TYPE G
(I)		(J)	
	OC-14R 48x24		OC-14L 48x24
(K)			
	OC-29R 24x18		
(L)			
	R11-2 48x30 TYPE G		
(M)			
	R5-1 30x30 750x750 TYPE G		

## MOT DETOUR PLAN PHASE 2



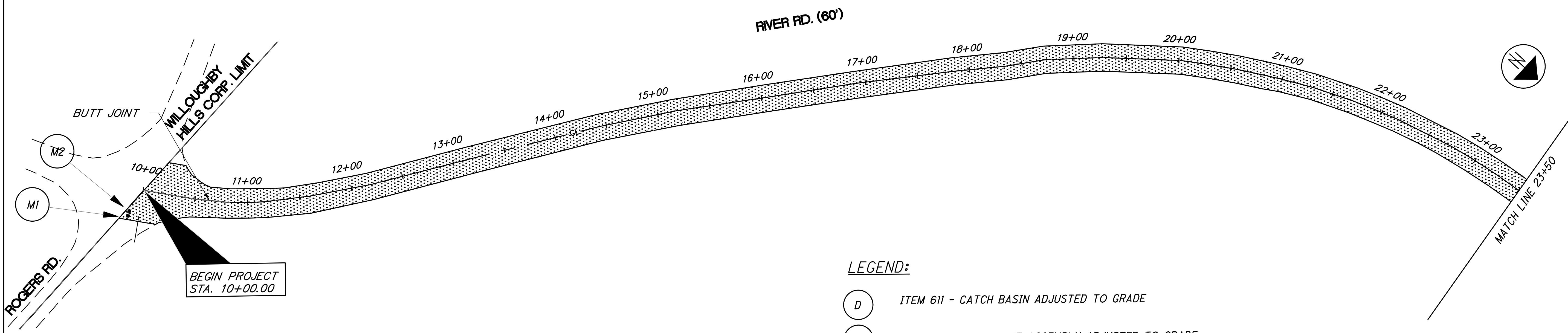


SHEET NO.	REFERENCE NO.	STATION	SIDE	611	623	632	638	609	609					
				FROM	TO	EACH	EACH	EACH	EACH	FT	FT			
21	M1	9+90	9+90	RT		1								
21	M2	9+90	9+90	RT		1								
22	W1	60+70	60+70	RT				1						
22	W2	61+07	61+07	RT				1						
23	W3	89+58	89+58	RT				1						
23	L1	90+20	90+50	RT										
23	L2	91+60	91+90	LT				1						
23	C1	76+79	90+30	LT					75					
23	C2	91+77	92+19	LT/RT					25					
23	D1	78+35	78+35	LT	1									
23	D2	79+97	79+97	LT	1									
23	D3	81+79	81+79	LT	1									
23	D4	83+77	83+77	LT	1									
23	D5	87+06	87+06	LT	1									
23	D6	91+87	91+87	LT	1									
24	W4	97+18	97+18	RT			1							
25	W5	124+35	124+35	LT				1						
25	W6	124+38	124+38	LT				1						
25	W7	124+81	124+81	LT				1						
25	W8	127+58	127+58	RT				1						
25	W9	143+70	143+70	RT				1						
25	W10	150+75	150+75	LT				1						
25	W11	151+04	151+04	LT				1						
26	W12	160+68	160+68	LT				1						
27	W13	197+58	197+58	LT				1						
28	W14	210+41	210+41	LT				1						
TOTALS CARRIED TO GENERAL SUMMARY				6	2	2	14	75	25					

SHEET NO.	REFERENCE NO.	STATION		SIDE	642	642 CENTER LINE, TYPE 1		642					
		FROM	TO		EDGE LINE, 4" TYPE 1	DOUBLE SOLID LINE, YELLOW	SINGLE SOLID/SINGLE DASHED LINE, YELLOW	SINGLE DASHED LINE, YELLOW	STOP LINE, TYPE 1				
					MILE	MILE	MILE	MILE	FEET				
29	EL	10+14	37+50	RT	0.52								
29	DY	10+45	29+63	LT/RT		0.36							
29	EL	10+34	37+50	LT	0.51								
29	DYD	29+63	36+03	LT/RT			0.12						
29	SYD	36+03	37+50	LT/RT				0.03					
30	EL	37+50	60+89	LT	0.44								
30	SYD	37+50	48+38	LT/RT				0.21					
30	EL	37+50	60+89	RT	0.44								
30	DY	48+38	60+89	LT/RT		0.24							
30	EL	61+47	66+50	LT	0.10								
30	DY	61+47	62+25	LT/RT		0.01							
30	EL	61+47	66+50	RT	0.10								
30	DYD	62+25	64+44	LT/RT			0.04						
30	SYD	64+44	66+50	LT/RT				0.04					
31	EL	66+50	90+30	LT	0.45								
31	SYD	66+50	73+51	LT/RT				0.13					
31	EL	66+50	90+30	RT	0.45								
31	DYD	73+51	75+11	LT/RT			0.03						
31	DY	75+11	90+30	LT/RT		0.29							
31	EL	91+77	95+00	LT	0.06								
31	DY	91+77	95+00	LT/RT		0.06							
31	EL	91+77	95+00	RT	0.06								
32	EL	95+00	124+00	LT	0.55								
32	DY	95+00	96+96	LT/RT		0.04							
32	EL	95+00	96+83	RT	0.03								
32	SL	97+09	97+19	RT					10				
32	DY	97+25	100+99	LT/RT		0.07							
32	EL	97+35	114+32	RT	0.32								
32	SYD	100+99	105+39	LT/RT				0.08					
32	DYD	105+39	110+49	LT/RT			0.10						
32	DY	110+49	114+40	LT/RT		0.07							
32	SL	114+57	114+70	RT					13				
32	DY	114+80	124+00	LT/RT		0.17							
32	EL	114+97	124+00	RT	0.17								
33	EL	124+00	124+43	LT	0.01								
33	DY	124+00	124+25	LT/RT		0.00							
33	EL	124+00	126+84	RT	0.05								
33	SL	124+45	124+63	LT					18				
33	EL	124+94	149+88	LT	0.47								
33	DY	125+14	126+84	LT/RT		0.03							
33	SL	127+50	127+68	RT				0.30		18			
33	DY	127+70	143+38	LT/RT									
33	EL	127+89	143+21	RT	0.29								
<b>SUBTOTALS THIS SHEET (CARRIED TO SHEET 19)</b>					5.03	1.65	0.29	0.49	59				

SHEET NO.	REFERENCE NO.	STATION	SIDE	642	642				642	STOP LINE, TYPE 1	FEET	22
				EDGE LINE, 4" TYPE 1	DOUBLE SOLID LINE, YELLOW	SINGLE SOLID/SINGLE DASHED LINE, YELLOW	SINGLE DASHED LINE, YELLOW					
		FROM	TO	MILE	MILE	MILE	MILE					
33	SL	143+53	143+75	RT								
33	DY	143+92	150+30	LT/RT		0.12						
33	EL	144+03	153+00	RT	0.17							
33	SL	150+40	150+62	LT								
33	DY	150+91	153+00	LT/RT		0.04						
33	EL	151+05	153+00	LT	0.04							
34	EL	153+00	160+50	LT	0.14							
34	DY	153+00	160+50	LT/RT		0.14						
34	EL	153+00	181+50	RT	0.54							
34	EL	161+24	181+50	LT	0.38							
35	EL	181+50	207+50	LT	0.49							
35	DY	181+50	207+50	LT/RT		0.49						
35	EL	181+50	207+50	RT	0.49							
36	EL	207+50	210+21	LT	0.05							
36	DY	207+50	211+28	LT/RT		0.07						
36	EL	207+50	210+21	RT	0.05							
<b>SUBTOTALS (THIS SHEET)</b>				2.36	1.25	0	0	44				
<b>SUBTOTALS SHEET 18</b>				5.03	1.65	0.29	0.49	59				
<b>TOTALS CARRIED TO GENERAL SUMMARY</b>				7.39	3.68			103				

SHEET NO.	STATION TO STATION		CADD GENERATED AREA		PREPARING SUBGRADE FOR SHOULDER PAVING. AS PER PLAN		PAVEMENT PLANNING, ASPHALT CONCRETE / CONCRETE, AS PER PLAN		407		407		441		441		441		617		875				
					SY	MILE	SY	GAL	GAL	CY	CY	CY	CY	CY	CY	CY	CY	CY	CY	LB	LB	LB	LB	LB	LB
21	10+00	37+50	8,044	1.05	8,044	724	483	288	392	51	550														
22	37+50	66+50	8,313	1.1	8,313	749	499	296	2	405	54	580													
23	66+50	95+00	7,322	1.08	7,322	659	440	264		356	53	570													
24	95+00	124+00	8,754	1.1	8,754	788	526	305	9	426	54	580													
25	124+00	153+00	9,721	1.1	9,721	875	584	317	30	473	54	580													
26	153+00	181+50	8,620	1.08	8,620	776	518	304	5	420	53	570													
27	181+50	207+50	7,319	0.99	7,319	659	440	263		356	49	520													
28	207+50	210+21	805	0.11	805	73	49	29		40	6	54													



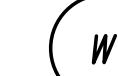
## LEGEND:



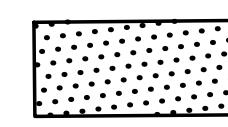
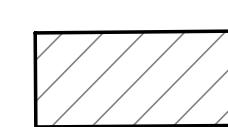
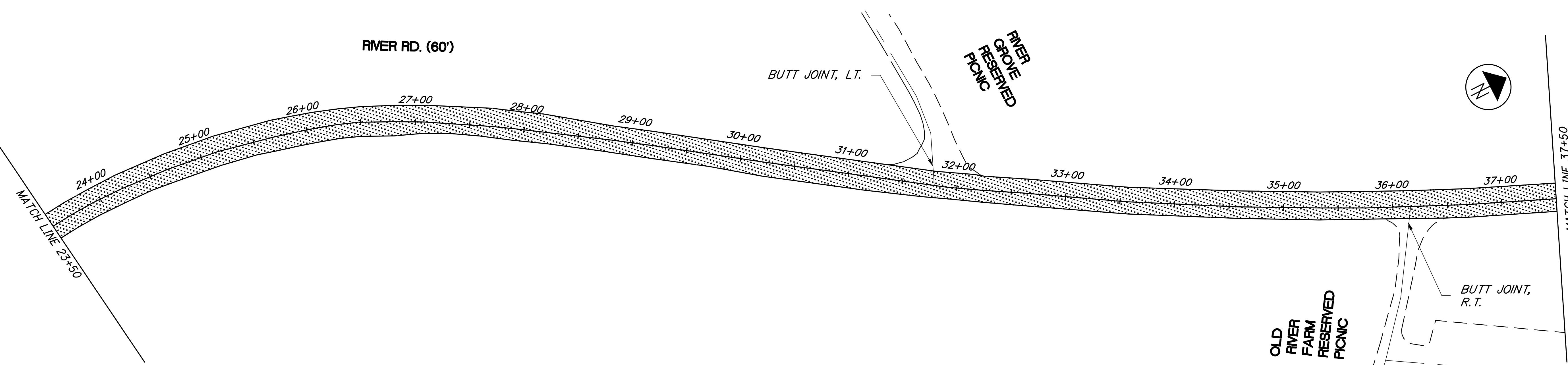
ITEM 611 - CATCH BASIN ADJUSTED TO GRADE

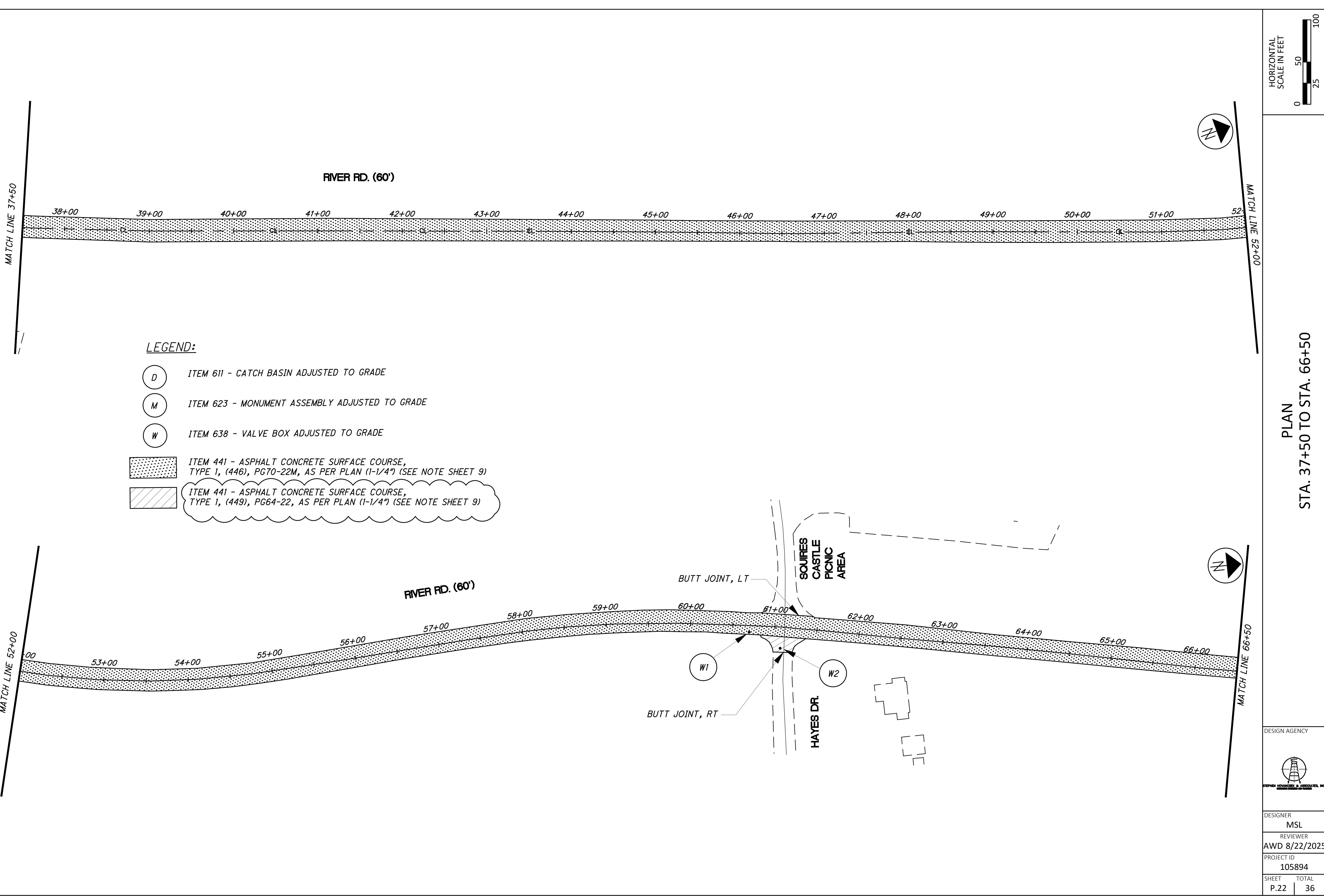


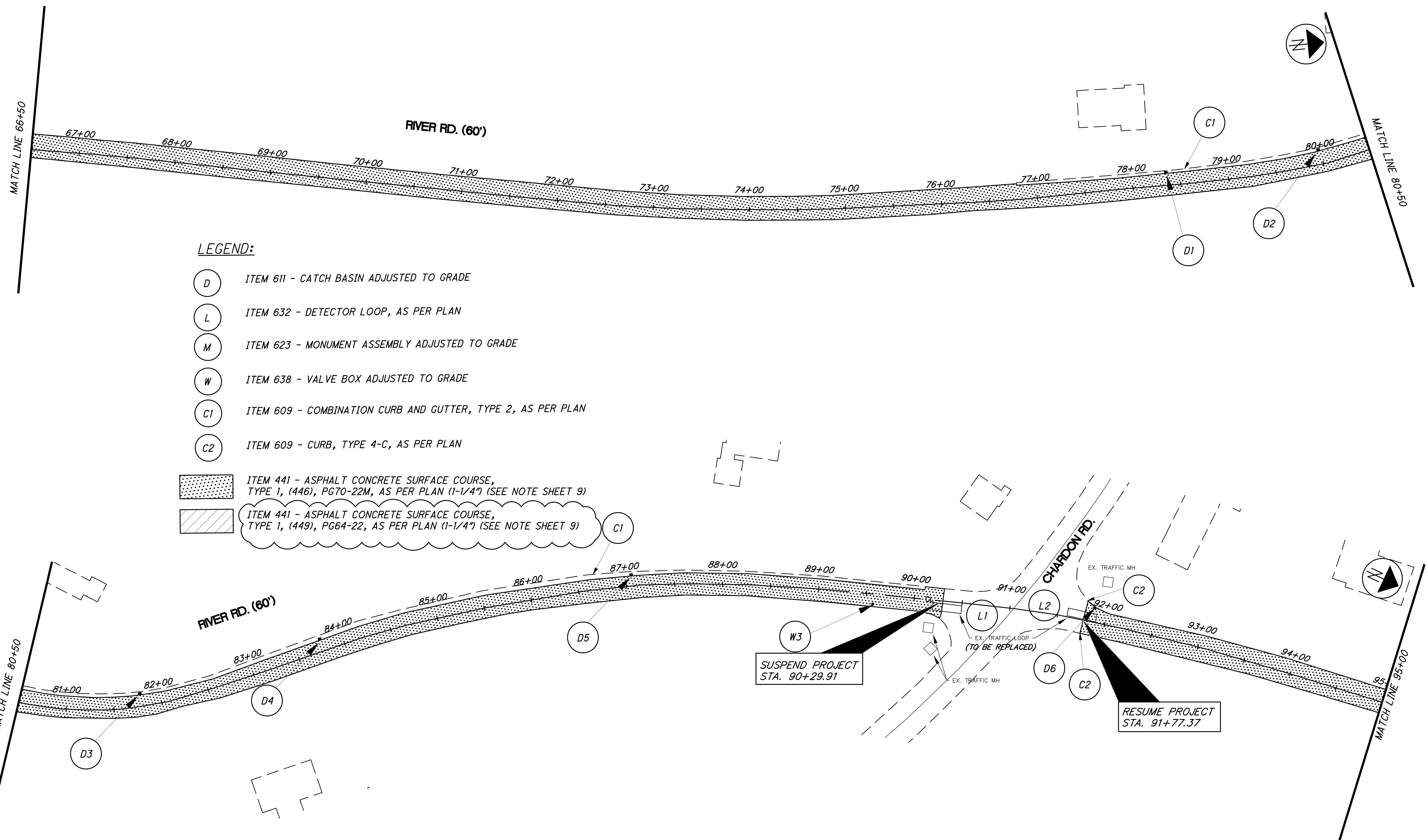
ITEM 623 - MONUMENT ASSEMBLY ADJUSTED TO GRADE

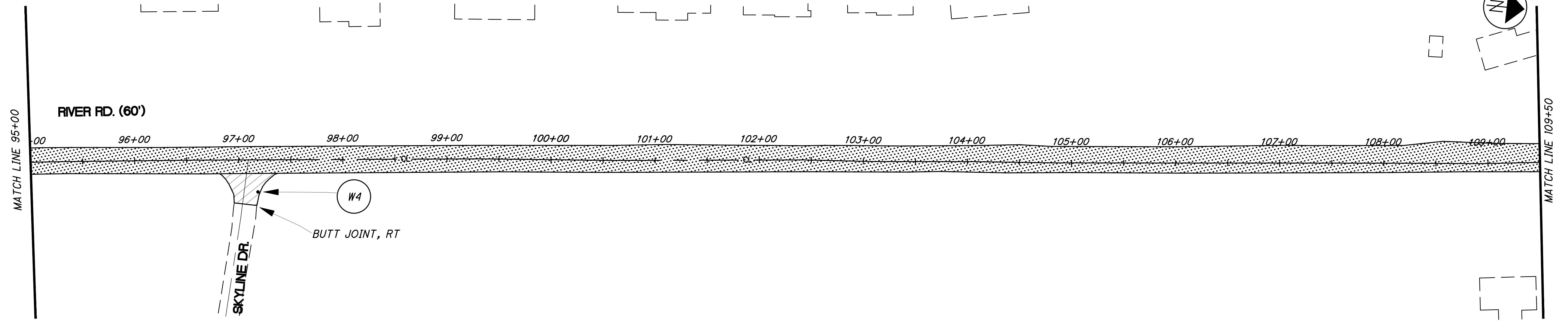
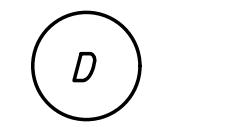


ITEM 638 - VALVE BOX ADJUSTED TO GRADE

ITEM 441 - ASPHALT CONCRETE SURFACE COURSE,  
TYPE 1, (446), PG70-22M, AS PER PLAN (1-1/4") (SEE NOTE SHEET 9)ITEM 441 - ASPHALT CONCRETE SURFACE COURSE,  
TYPE 1, (449), PG64-22, AS PER PLAN (1-1/4") (SEE NOTE SHEET 9)PLAN  
STA. 10+00.00 TO STA. 37+50HORIZONTAL  
SCALE IN FEET  
0 25 50 100DESIGN AGENCY  
STEPHEN HOVANESK & ASSOCIATES, INC.DESIGNER MSL  
REVIEWER AWD 8/22/2025  
PROJECT ID 105894  
SHEET TOTAL P.21 36





LEGEND:

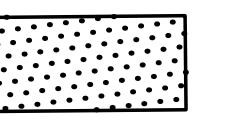
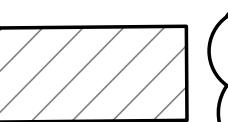
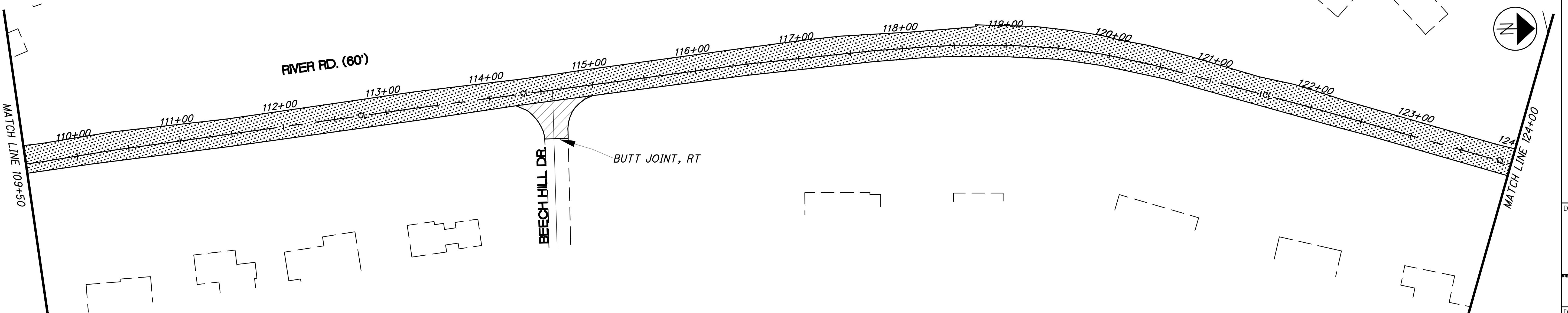
ITEM 611 - CATCH BASIN ADJUSTED TO GRADE



ITEM 623 - MONUMENT ASSEMBLY ADJUSTED TO GRADE



ITEM 638 - VALVE BOX ADJUSTED TO GRADE

ITEM 441 - ASPHALT CONCRETE SURFACE COURSE,  
TYPE 1, (446), PG70-22M, AS PER PLAN (1-1/4") (SEE NOTE SHEET 9)ITEM 441 - ASPHALT CONCRETE SURFACE COURSE,  
TYPE 1, (449), PG64-22, AS PER PLAN (1-1/4") (SEE NOTE SHEET 9)

PLAN STA. 95+00 TO STA. 124+00

HORIZONTAL SCALE IN FEET  
0 25 50 100

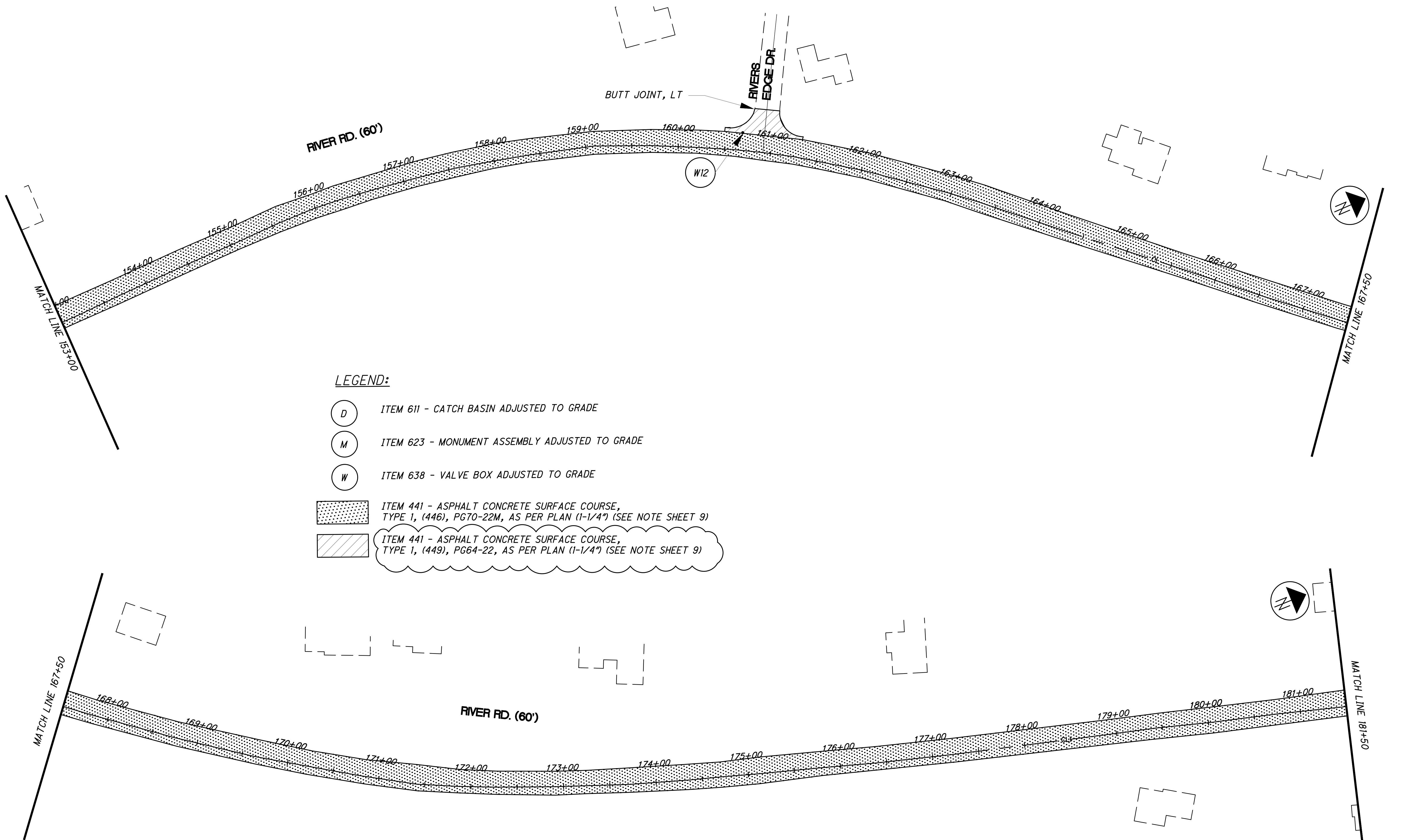
DESIGN AGENCY	STEPHEN HOVANESK & ASSOCIATES, INC.
DESIGNER	MSL
REVIEWER	AWD 8/22/2025
PROJECT ID	105894
SHEET TOTAL	P.24 36

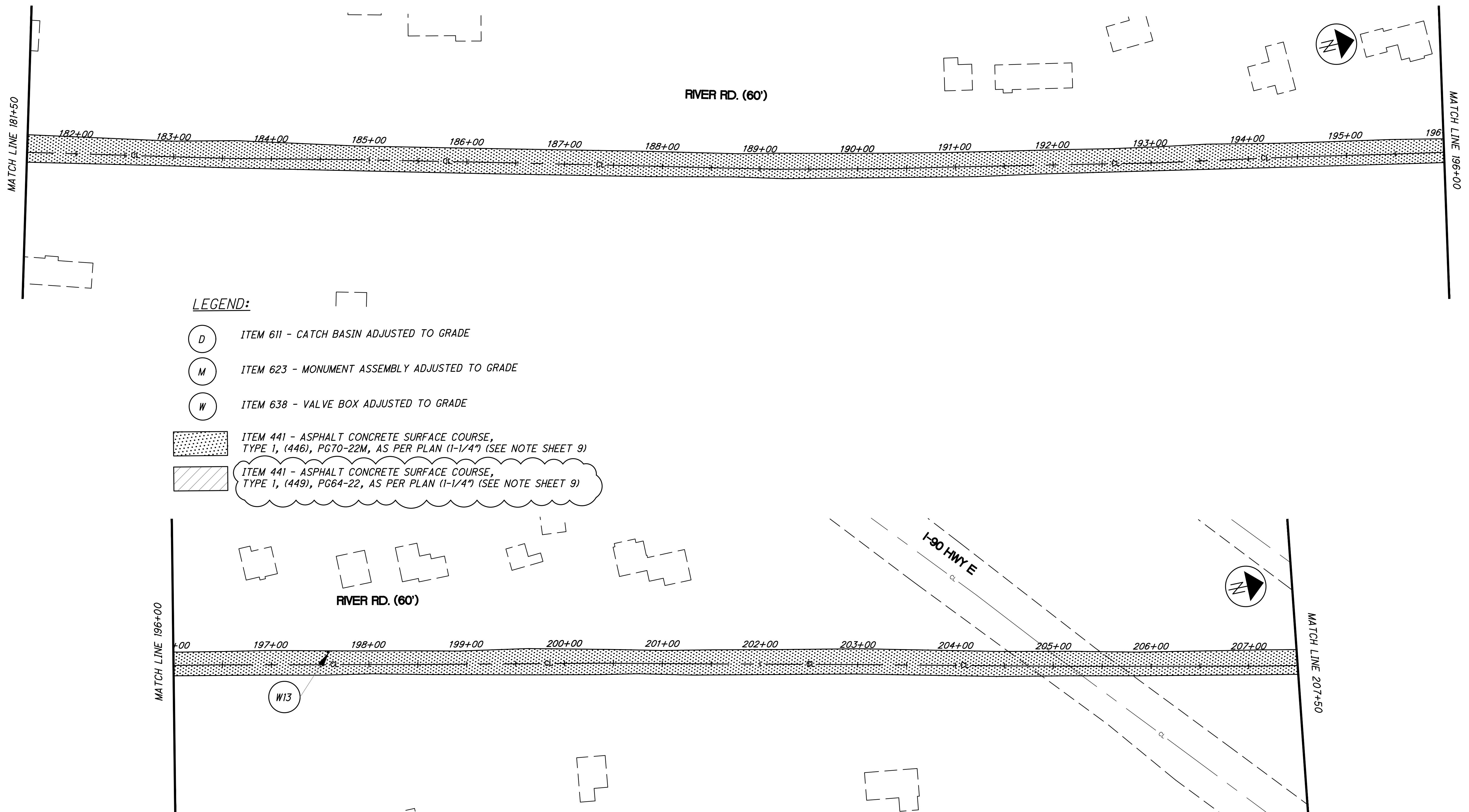
LAK SR174-00.00

0 1

# PLAN STA. 124+00 TO STA. 153+00

DESIGNER	MSL
REVIEWER	
AWD 8/22/2025	
PROJECT ID	
105894	
SHEET	TOTAL
R 25	36





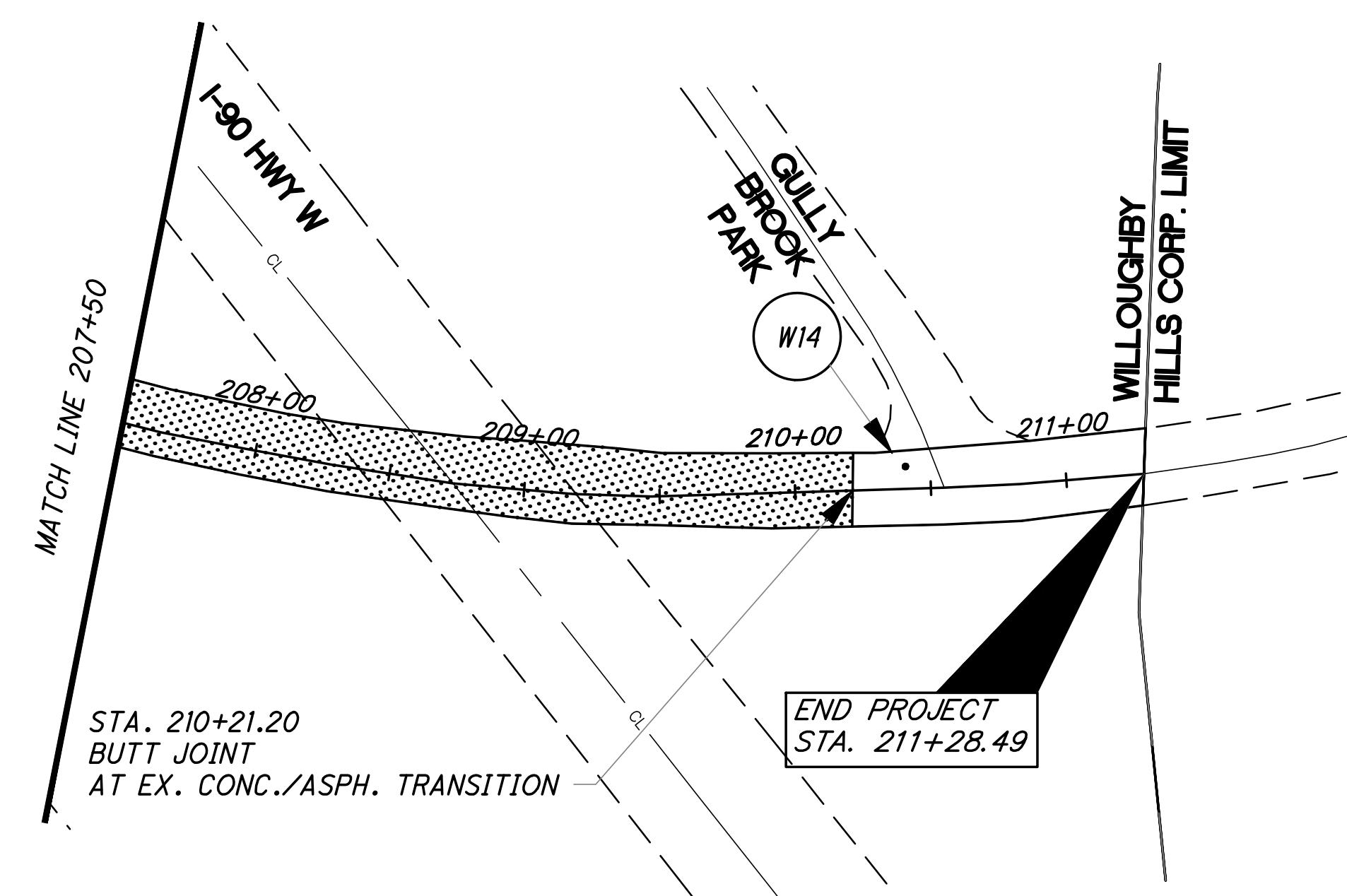
DESIGN AGENCY  
STEPHEN HOVANESK & ASSOCIATES, INC.

DESIGNER  
MSL

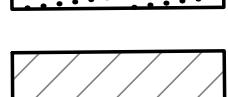
REVIEWER  
AWD 8/22/2025

PROJECT ID  
105894

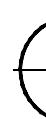
SHEET TOTAL  
P.27 36



## LEGENDA

-  *ITEM 611 - CATCH BASIN ADJUSTED TO GRADE*
-  *ITEM 623 - MONUMENT ASSEMBLY ADJUSTED TO GRADE*
-  *ITEM 638 - VALVE BOX ADJUSTED TO GRADE*
-  *ITEM 441 - ASPHALT CONCRETE SURFACE COURSE,  
TYPE 1, (446), PG70-22M, AS PER PLAN (1-1/4") (SEE NOTE SHEET 9)*
-  *ITEM 441 - ASPHALT CONCRETE SURFACE COURSE,  
TYPE 1, (449), PG64-22, AS PER PLAN (1-1/4") (SEE NOTE SHEET 9)*

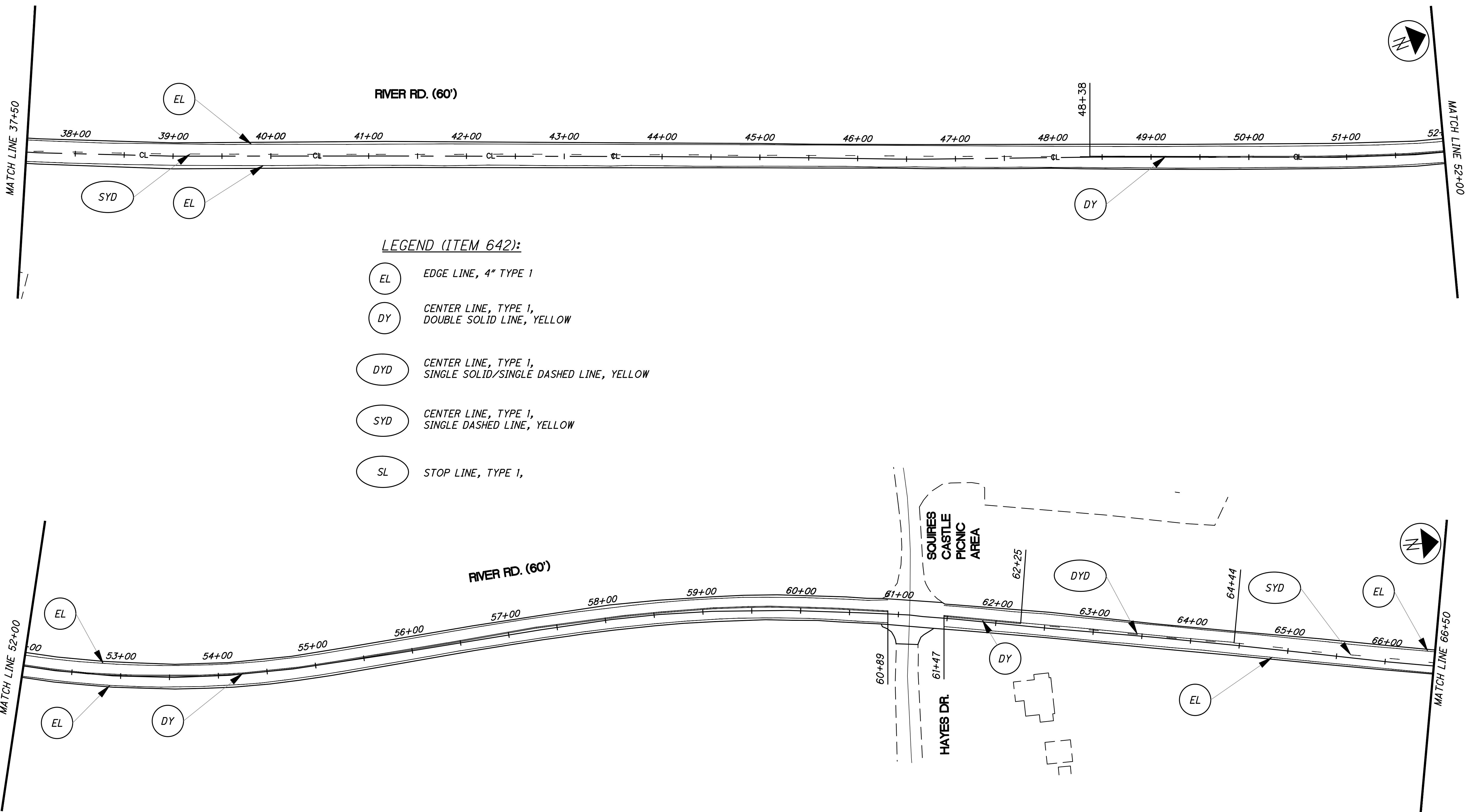
*NOTE: THE I-90 OVERPASS HAS A LOW CLEARANCE SIGN OF 14', THIS CLEARANCE MUST NOT BE REDUCED AFTER RESURFACING*

DESIGN AGENCY	
 <b>PHEN HOVANCSK &amp; ASSOCIATES, INC.</b>	
DESIGNER	
<b>MSL</b> REVIEWER	
<b>WD 8/22/2025</b>	
PROJECT ID	
<b>105894</b>	
HEET	TOTAL
<b>P.28</b>	<b>36</b>

# STA. 207+50 TO STA. 211+28.49

## PLAN



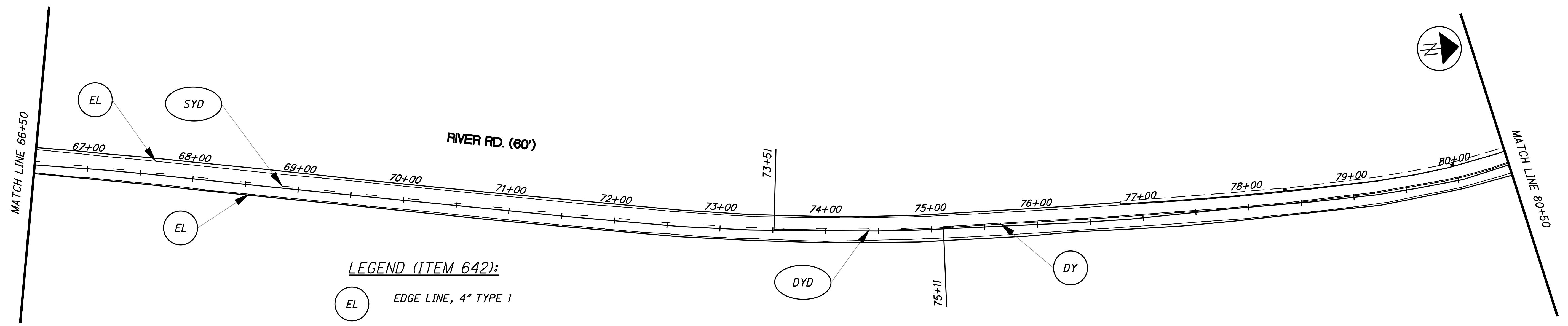


**PAVEMENT MARKING PLAN**  
**STA. 37+50 TO STA. 66+50**

DESIGN AGENCY  
STEPHEN HOVANECZ & ASSOCIATES, INC.

DESIGNER  
MSL  
REVIEWER  
AWD 8/22/2025  
PROJECT ID  
105894  
SHEET TOTAL  
P.30 | 36

HORIZONTAL SCALE IN FEET  
0 25 50 100

LEGEND (ITEM 642):

EDGE LINE, 4" TYPE 1

CENTER LINE, TYPE 1,  
DOUBLE SOLID LINE, YELLOW

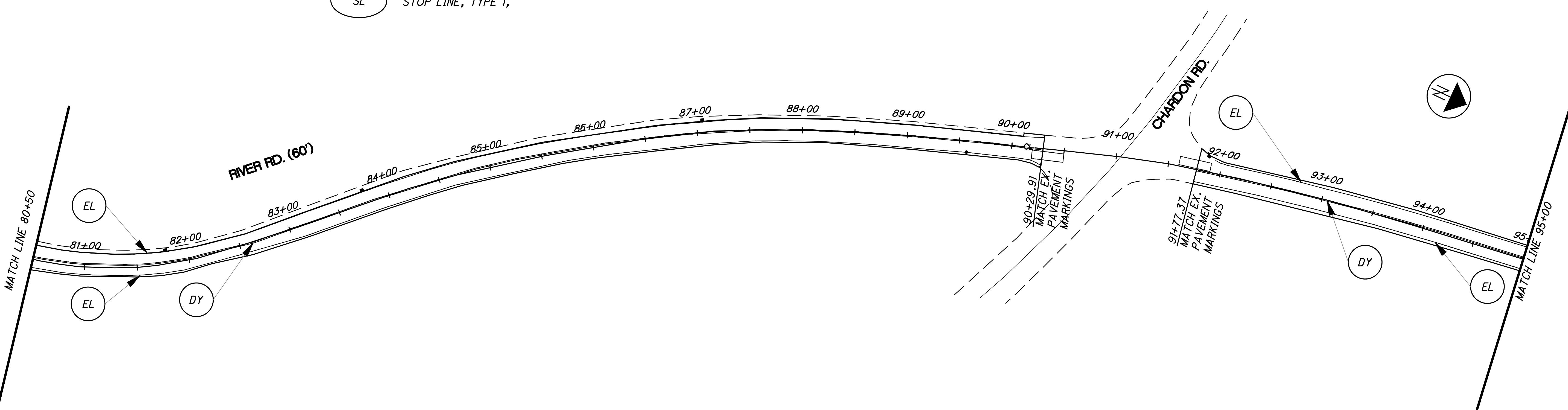
CENTER LINE, TYPE 1,  
SINGLE SOLID/SINGLE DASHED LINE, YELLOW

CENTER LINE, TYPE 1,  
SINGLE DASHED LINE, YELLOW

STOP LINE, TYPE 1,

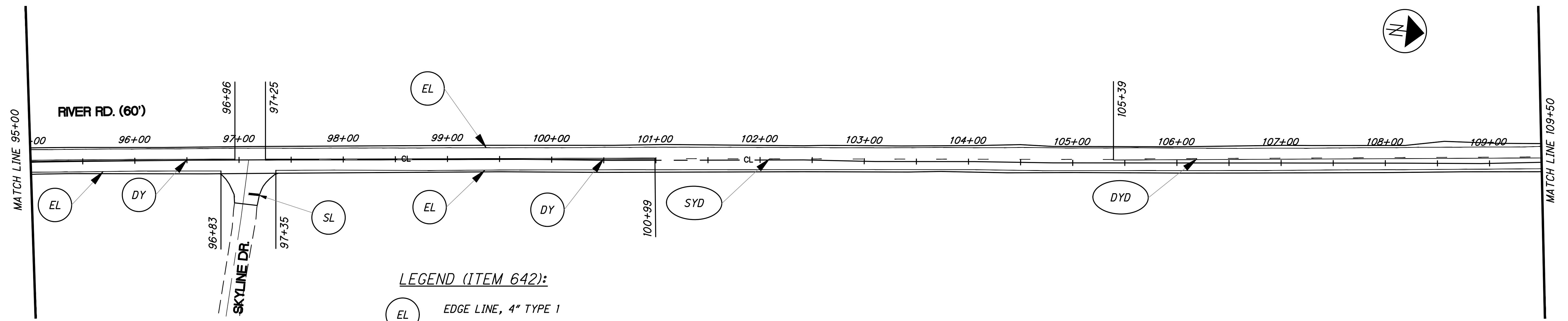
PAVEMENT MARKING PLAN  
STA. 66+50 TO STA. 95+00

HORIZONTAL SCALE IN FEET  
0 25 50 100



DESIGN AGENCY  
STEPHEN HOVANEC & ASSOCIATES, INC.

DESIGNER MSL  
REVIEWER AWD 8/22/2025  
PROJECT ID 105894  
SHEET TOTAL P.31 36

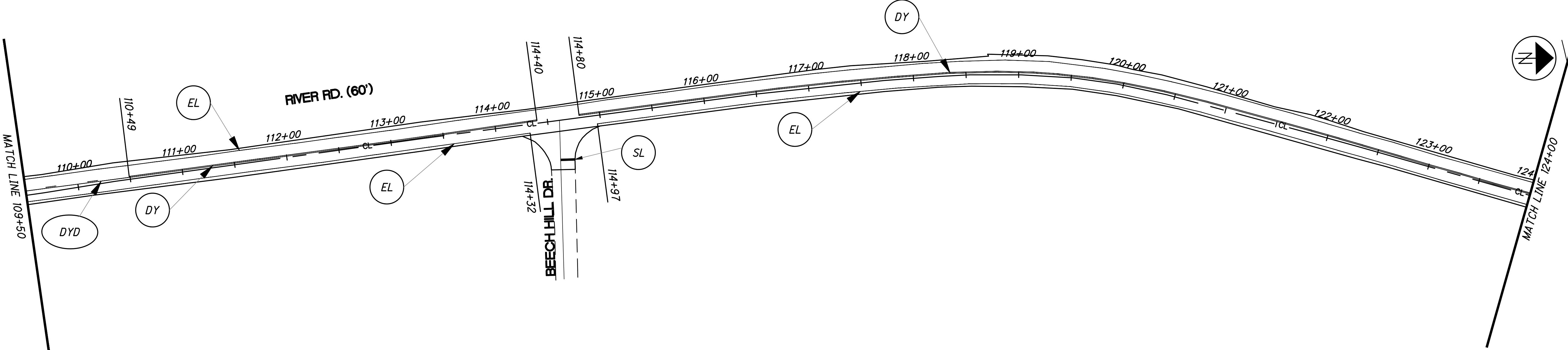


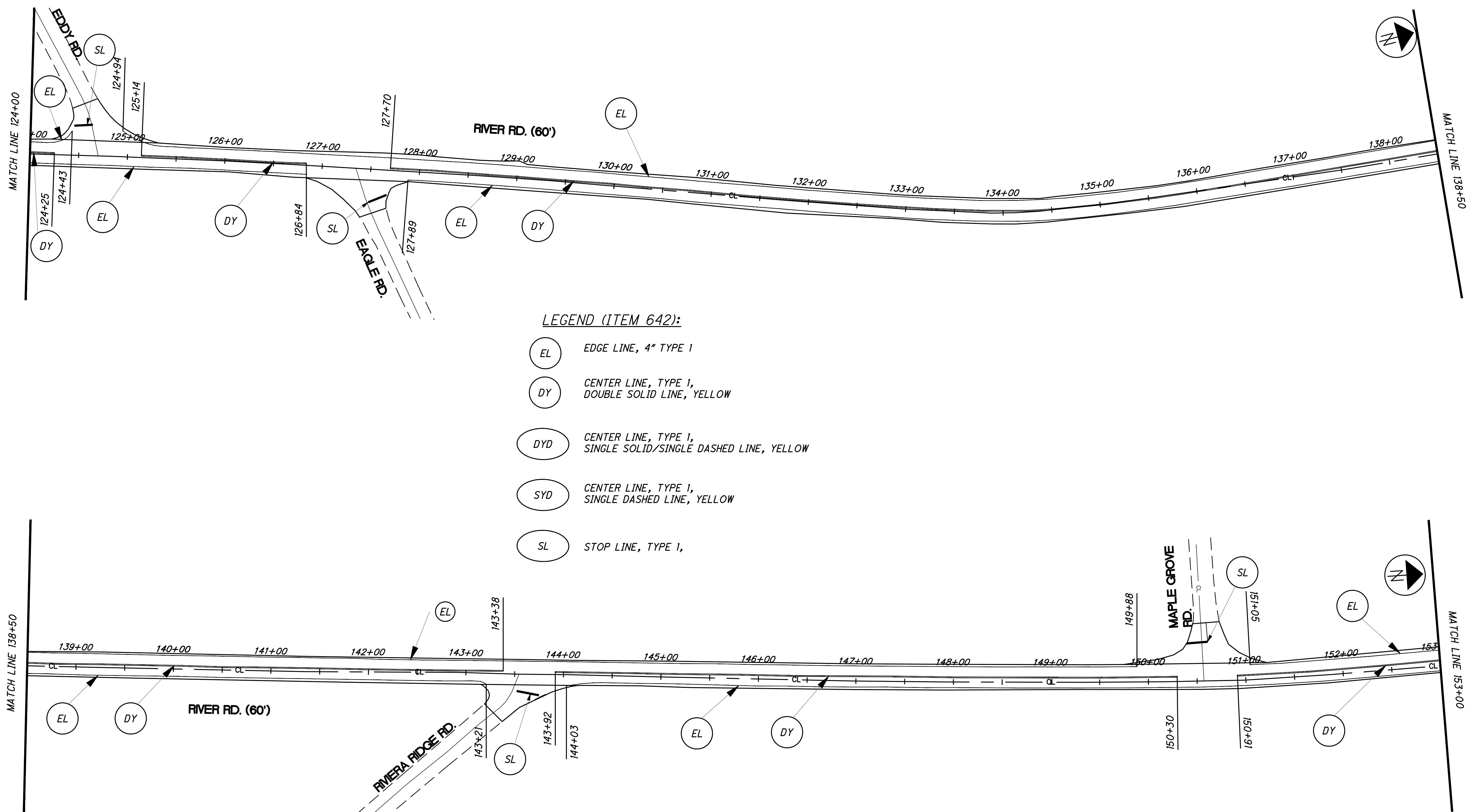
**PAVEMENT MARKING PLAN  
STA. 95+00 TO STA. 124+00**

HORIZONTAL SCALE IN FEET  
0 25 50 100

DESIGN AGENCY  
STEPHEN HOVANEC & ASSOCIATES, INC.

DESIGNER  
MSL  
REVIEWER  
AWD 8/22/2025  
PROJECT ID  
105894  
SHEET TOTAL  
P.32 36



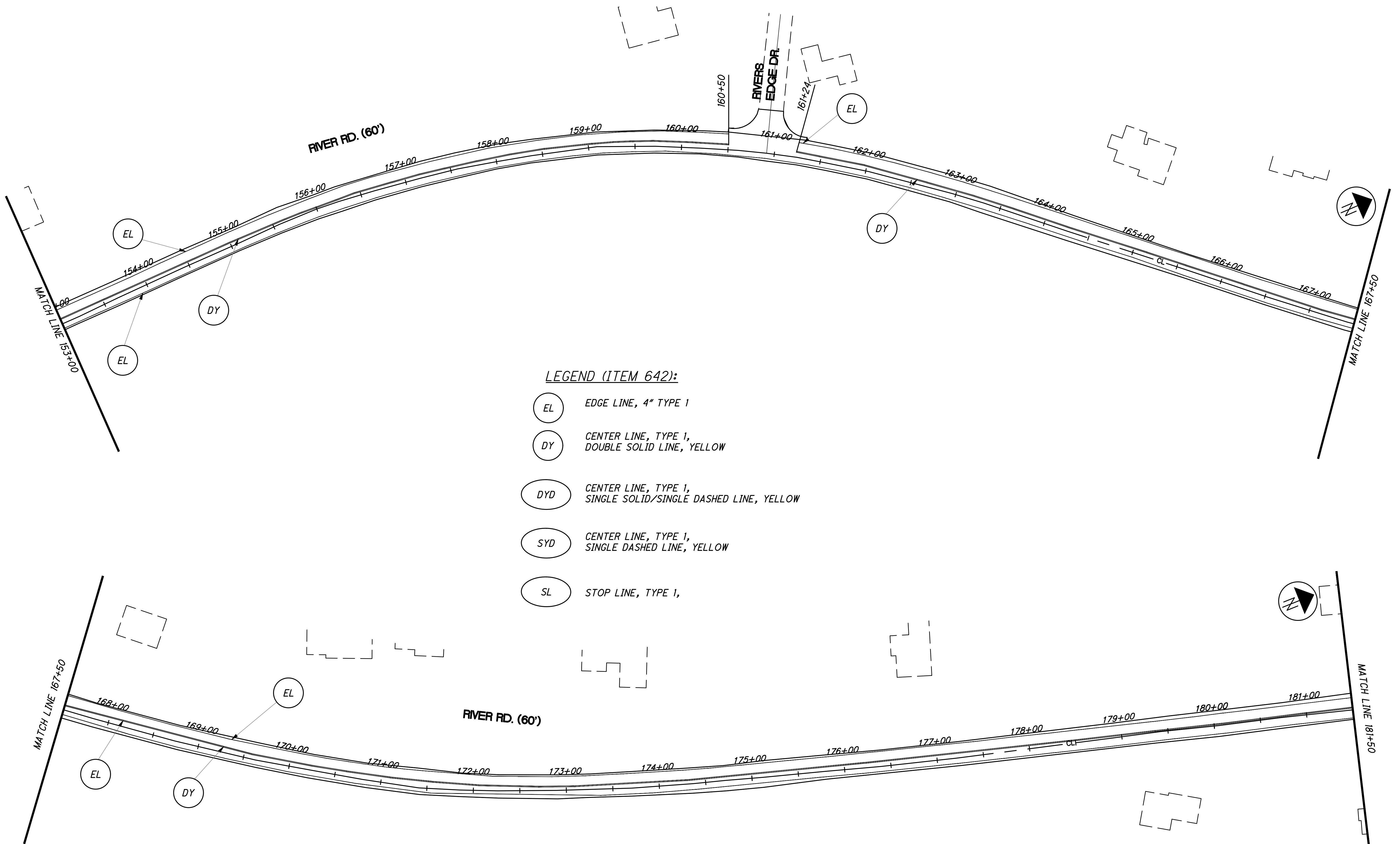


**PAVEMENT MARKING PLAN**  
STA 124+00 TO STA 153+00

DESIGN AGENCY  
STEPHEN HOVANEC & ASSOCIATES, INC.

DESIGNER  
MSL  
REVIEWER  
AWD 8/22/2025  
PROJECT ID  
105894  
SHEET TOTAL  
P.33 36

HORIZONTAL SCALE IN FEET  
0 25 50 100

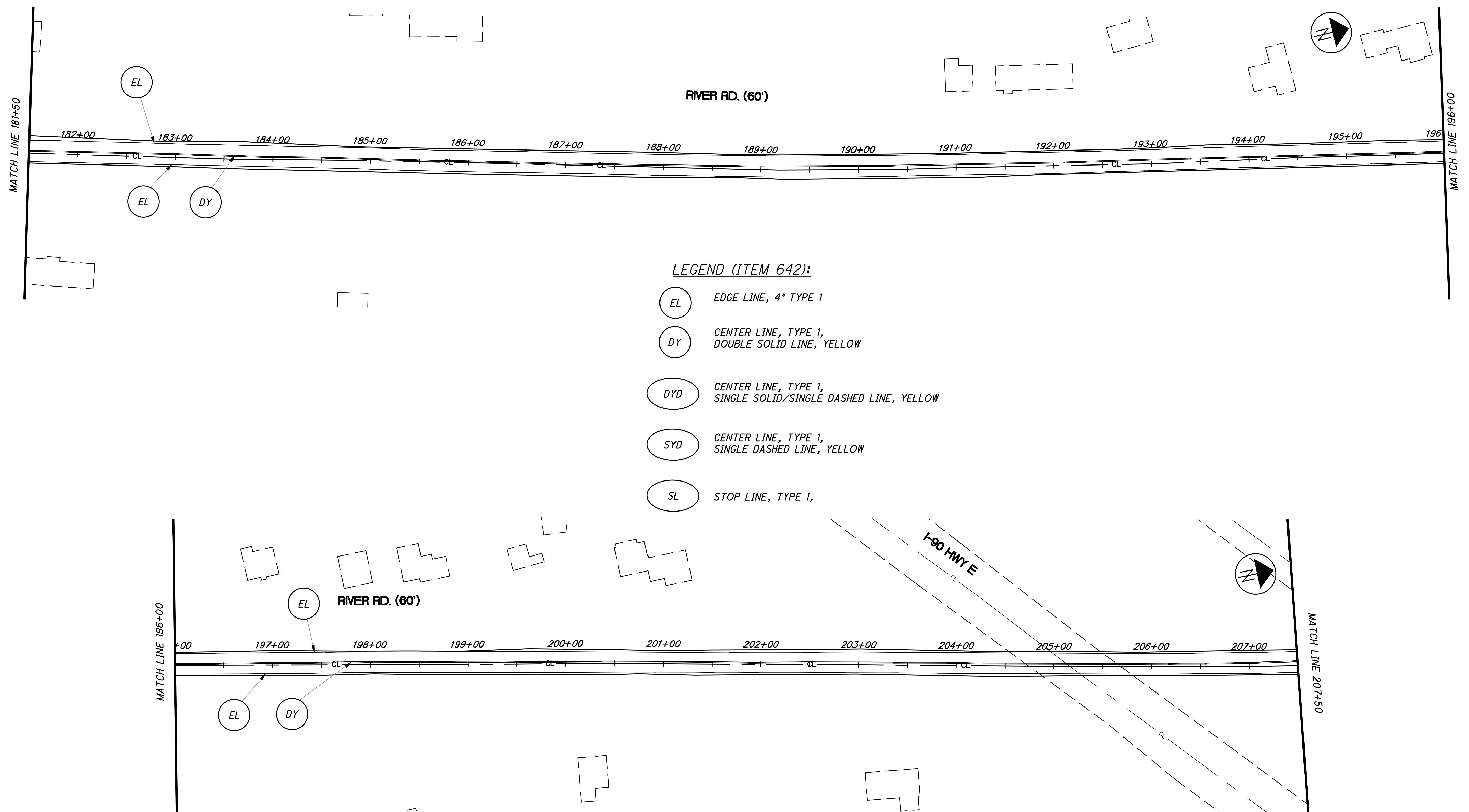


PAVEMENT MARKING PLAN  
STA 153+00 TO STA 181+50

HORIZONTAL SCALE IN FEET  
0 25 50 100

DESIGN AGENCY  
STEPHEN HOVANEC & ASSOCIATES, INC.

DESIGNER MSL  
REVIEWER AWD 8/22/2025  
PROJECT ID 105894  
SHEET TOTAL P.34 36



**PAVEMENT MARKING PLAN  
STA 181+50 TO STA 207+50**

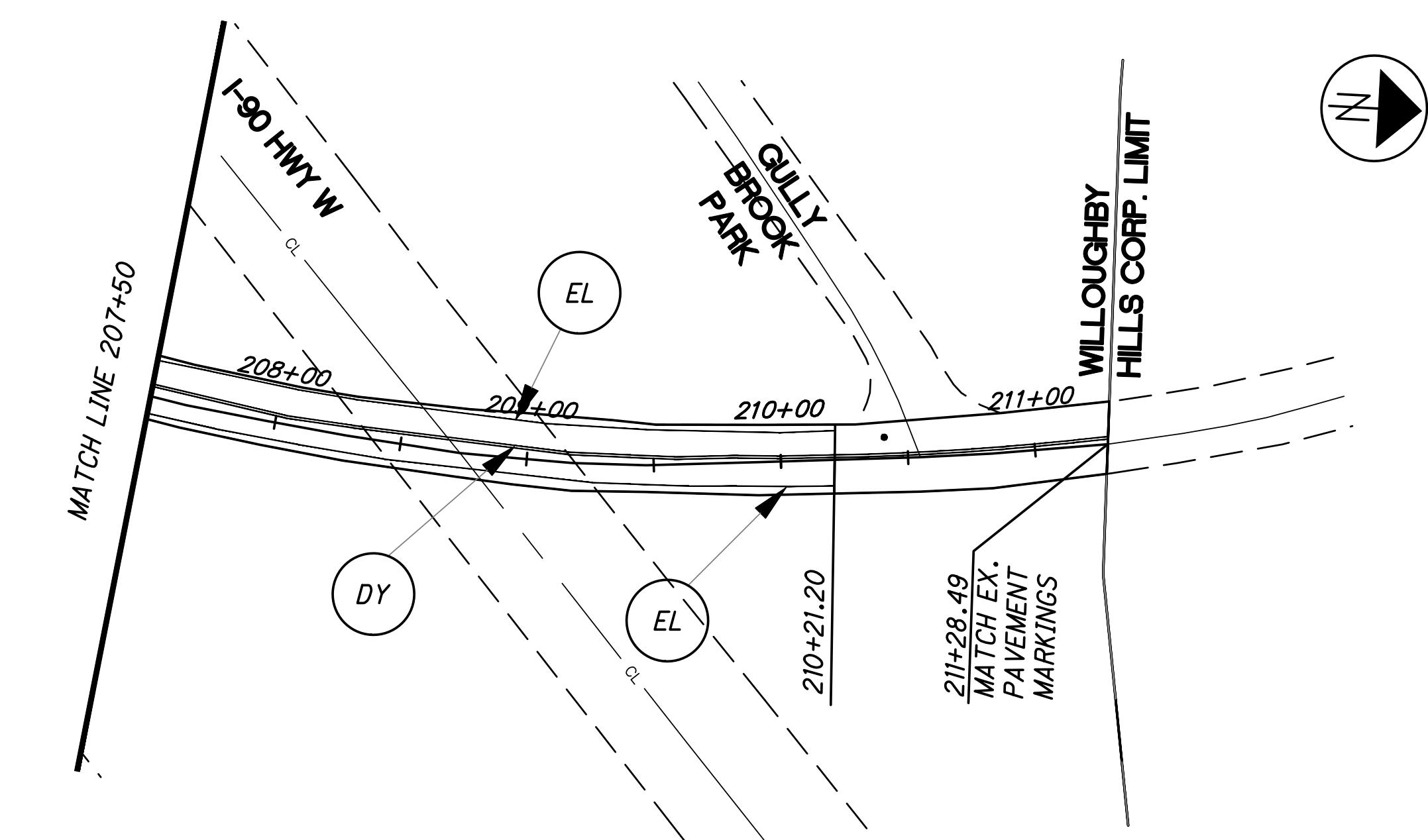
DESIGN AGENCY  
STEPHEN HOVANECZ & ASSOCIATES, INC.

DESIGNER MSL  
REVIEWER AWD 8/22/2025  
PROJECT ID 105894  
SHEET TOTAL P.35 36

HORIZONTAL SCALE IN FEET  
0 25 50 100

LAK SR174-00.00

2026/01/13  
G:\2024\A24-297 WILLOUGHBY HILLS RESURFACING OF RIVER ROAD\PLT 105894\DWG\Set24-297-PLT 105894.dwg



LEGEND (ITEM 642):

- (EL) EDGE LINE, 4" TYPE 1
- (DY) CENTER LINE, TYPE 1, DOUBLE SOLID LINE, YELLOW
- (DYD) CENTER LINE, TYPE 1, SINGLE SOLID/SINGLE DASHED LINE, YELLOW
- (SYD) CENTER LINE, TYPE 1, SINGLE DASHED LINE, YELLOW
- (SL) STOP LINE, TYPE 1,

PAVEMENT MARKING PLAN  
STA. 207+50 TO STA. 211+28.49

DESIGN AGENCY  
STEPHEN HOVANECZ & ASSOCIATES, INC.

DESIGNER MSL  
REVIEWER AWD 8/22/2025  
PROJECT ID 105894  
SHEET TOTAL P.36 36

HORIZONTAL SCALE IN FEET  
0 25 50 100