

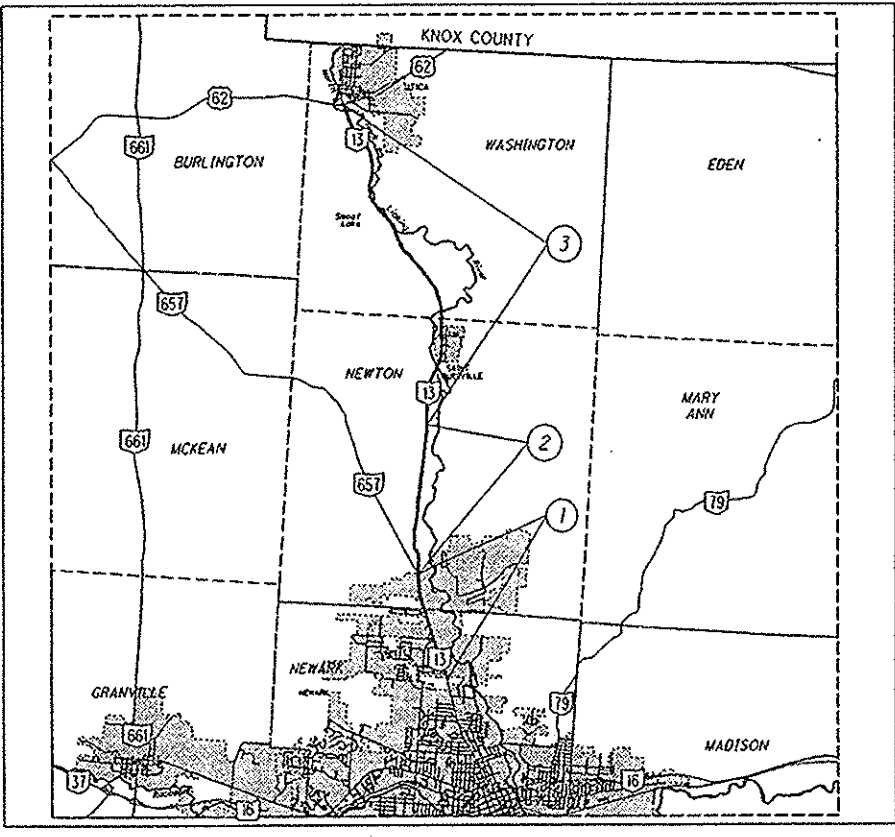
STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

LIC-13-12.06

CITY OF NEWARK
NEWARK, NEWTON AND WASHINGTON
TOWNSHIPS
LICKING COUNTY

PROJECT DESCRIPTION:
ASPHALT CONCRETE RESURFACING, AND RELATED
WORK, ON S.R. 13 IN LICKING COUNTY.

Project Earth Disturbed Area =
N/A (Maintenance Project)
Estimated Contractor Earth Disturbed Area =
N/A (Maintenance Project)
Notice of Intent Earth Disturbed Area =
N/A (Maintenance Project)



LOCATION MAP
LON/LAT: 82° 25' 27" / 40° 09' 45"
PORTION TO BE IMPROVED

DESIGN DESIGNATION	LOC. 1 12.06-13.98	LOC. 2 13.98-16.53	LOC. 3 16.53-22.53
Functional Classification	UPA	UPA	RPA
Opening Year ADT (2011)	12100	8500	7600
Design Year ADT (2023)	13500	9400	8500
Design Hourly Volume (2023)	1350	940	850
Directional Distribution	55%	55%	55%
Trucks (24 Hour B&C)	6%	8%	8%
Design Speed	55mph	55mph	55mph
Legal Speed	55mph	55mph	35mph

RPA = RURAL PRINCIPAL ARTERIAL
UPA = URBAN PRINCIPAL ARTERIAL

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LOCATION	COUNTY	ROUTE	BEGIN SLM	END SLM	LENGTH MILES	CITY/VILLAGE
1	LIC	13	12.06	13.98	1.92	NEWARK
2	LIC	13	13.98	16.53	2.55	
3	LIC	13	16.53	22.53	6.00	ST. LOUISVILLE / UTICA

2010 SPECIFICATIONS
THE STANDARD 2010 SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND THE PROPOSAL SHALL GOVERN THESE IMPROVEMENTS.
I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THESE IMPROVEMENTS WILL NOT REQUIRE THE CLOSING OF THE HIGHWAY AND PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS INDICATED IN THE PROPOSAL.

LIC - SR-13-12.06
110452 PID - 83109
Dist 5 7/21/2011
Contract Proposal available
@ www.contracts.dot.
state.oh.us/home

DESIGN EXCEPTIONS: NONE

UNDERGROUND UTILITIES
CONTACT BOTH SERVICES
CALL TWO WORKING DAYS
BEFORE YOU DIG
CALL 1-800-362-2764 (TOLL FREE)
OHIO UTILITIES PROTECTION SERVICE
NON-MEMBERS
MUST BE CALLED DIRECTLY
OIL & GAS PRODUCERS PROTECTIVE
SERVICE CALL: 1-800-925-0988

PLAN PREPARED BY:
OHIO DEPARTMENT OF TRANSPORTATION
DISTRICT 5 PRODUCTION OFFICE

ENGINEER'S SEAL
STATE OF OHIO
DOUGLAS N. MORGAN
E-63839
REGISTERED PROFESSIONAL ENGINEER
SIGNED: *Douglas N. Morgan*
DATE: 4-20-2011

STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	
BP-3.1	10-19-07	TC-65.10	1-21-05	800	4-15-11
BP-4.1	7-16-04	TC-65.11	1-21-05	817	7-16-10
BP-5.1	7-28-00	TC-71.10	1-21-11	832	5-5-09
		TC-73.10	1-19-01		
MT-97.10	10-15-10	TC-82.10	1-21-11		
MT-97.12	10-15-10				
MT-99.20	1-16-09				
MT-101.90	1-16-09				
MT-105.10	1-16-09				
				SPECIAL PROVISIONS	

APPROVED *[Signature]*
DATE 4/20/11 DISTRICT DEPUTY DIRECTOR
APPROVED *[Signature]*
DATE 5-4-11 DIRECTOR, DEPARTMENT OF TRANSPORTATION

FEDERAL PROJECT NO. E080 (428)
PID NO. 83109
CONSTRUCTION PROJECT NO.
RAILROAD INVOLVEMENT NONE
LIC-13-12.06
1/22

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

NOTIFICATION OF ROAD CLOSURE OR RESTRICTION

IN ORDER FOR ODOT TO PROPERLY PERMIT OVERSIZE LOADS, PREPARE PROPER SIGNING WHEN REQUIRED AND FURTHER TO NOTIFY THE GENERAL MOTORING PUBLIC, THE CONTRACTOR SHALL NOTIFY (IN WRITING THE DISTRICT 5 CONSTRUCTION ENGINEER WITH COPIES FOR THE DISTRICT 5 ROADWAY SERVICES MANAGER AND PROJECT ENGINEER NOT LESS THAN 21 DAYS BEFORE SUCH CLOSURE OR LANE RESTRICTIONS.

SEND NOTIFICATION TO:
DISTRICT 5 CONSTRUCTION ENGINEER
P.O. BOX 306
JACKSONSTOWN, OH 43030
PHONE: (740) 323-4400 EXT. 5241

ITEM 617 COMPACTED AGGREGATE, AS PER PLAN

ALL AGGREGATE SHALL BE 100% CRUSHED LIMESTONE. ALL QUALITY REQUIREMENTS EXCEPT SHALE SHALL BE WAIVED. OTHER GRADATION REQUIREMENTS SHALL BE AS SPECIFIED EXCEPT THE INDEX SHALL BE WAIVED. IF SO PERMITTED, THE CONTRACTOR MAY USE ASPHALT CONCRETE PAVEMENT (RACP MEETING REQUIREMENTS OF 617.02) IN LIEU OF CRUSHED LIMESTONE.

ITEM 407 TACK COAT

THE RATE OF APPLICATION OF THE 407 TACK COAT SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. PLAN QUANTITIES INDICATE AN AVERAGE APPLICATION RATE OF 0.075 GALLONS PER SQUARE YARD FOR ESTIMATING PURPOSES ONLY.

ITEM 407 TACK COAT FOR INTERMEDIATE COURSE

THE RATE OF APPLICATION OF THE 407 TACK COAT FOR INTERMEDIATE COURSE SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. PLAN QUANTITIES INDICATE AN AVERAGE APPLICATION RATE OF 0.05 GALLONS PER SQUARE YARD FOR ESTIMATING PURPOSES ONLY.

CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

ITEM 516 2" DEEP JOINT SEALER, AS PER PLAN

THE CONTRACTOR SHALL PLACE A 1" X 2.0" DEEP BEAD OF JOINT SEALER (AS PER 705.04) AT THE LOCATIONS SHOWN IN PLANS. THE CONTRACTOR SHALL SAW CUT A CHANNEL FOR THE JOINT SEALER. THE COST FOR SAW CUTTING THE CHANNEL FOR THE JOINT SEALER SHALL BE INCLUDED FOR PAYMENT WITH ITEM 516, 2" DEEP JOINT SEALER, AS PER PLAN.

MAIL BOX TURN OUTS

A QUANTITY OF ASPHALT CONCRETE HAS BEEN PROVIDED IN THE PLAN TO COVER MAIL BOX TURN-OUTS. TURN-OUTS SHALL BE PAVED AS SHOWN IN THE DETAIL IN DRAWING BP-4.1. ANY EXTRA GRADING OF THE SHOULDERS, PRIME OR TACK COAT, MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE MAIL BOX TURN OUTS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE ITEMS LISTED BELOW.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE **SUB-SUMMARIES** FOR THE ABOVE PURPOSES.

ITEM 448 ASPHALT CONCRETE INTERM. COURSE, TYPE 2, PG 64-22
LOCATION 1 - 9 CU.YD.
LOCATION 2 - 13 CU.YD.
LOCATION 3 - 31 CU.YD.

ITEM 448 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 70-22M
LOCATION 1 - 6 CU.YD.
LOCATION 2 - 10 CU.YD.
LOCATION 3 - 21 CU.YD.

ITEM 408 PRIME COAT, AS PER PLAN

THE CONTRACTOR SHALL APPLY ONE COAT OF MC-70 (AS PER SECTION 702) AT A RATE OF 0.40 GALLON PER SQUARE YARD TO THE COMPLETED AGGREGATE SHOULDER (ITEM 617) AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID BITUMINOUS MATERIAL ONTO THE EDGE OF PAVEMENT OR EDGE LINE. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO 107.10 OF THE SPECIFICATIONS.

THE FOLLOWING QUANTITIES OF PRIME COAT, AS PER PLAN HAVE BEEN CARRIED TO THE **SUB-SUMMARIES** AND SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT TO PERFORM THE ABOVE MENTIONED WORK.

ITEM 408 PRIME COAT, AS PER PLAN
LOCATION 1 - 3008 SQ.YD. x 0.40 GAL./SQ YD = 1,204 GAL
LOCATION 2 - 7,032 SQ.YD. x 0.40 GAL./SQ YD = 2,813 GAL
LOCATION 3 - 13,715 SQ.YD. x 0.40 GAL./SQ YD = 5,486 GAL

PAVEMENT MARKING

STOP LINES, CROSSWALK LINES, CHANNELIZING LINES, ETC., SHOWN IN THE PLANS ARE TAKEN FROM EXISTING MARKINGS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DOCUMENT EXISTING MARKING LOCATIONS (i.e. BY USE OF VIDEO, PICTURES) AND PLACE NEW PAVEMENT MARKINGS AS NEAR AS POSSIBLE TO THE EXISTING LOCATIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. DOCUMENTATION OF PAVEMENT MARKING SHALL BE SUPPLIED TO THE ENGINEER BEFORE COMMENCEMENT OF ANY OPERATION WHICH WILL REMOVE/OBLITERATE MARKINGS.

RESIDENTIAL AND COMMERCIAL DRIVES

AN ESTIMATED QUANTITY OF ITEM 448 ASPHALT CONCRETE, HAS BEEN INCLUDED IN THE PLAN TO BE USED AS DIRECTED BY THE ENGINEER TO PAVE APPROACH AREAS TO EXISTING DRIVEWAYS. PAVING SHALL TYPICALLY EXTEND 4' INTO THE DRIVEWAY (MEASURED FROM THE EDGE OF PAVEMENT OR PAVED SHOULDER IF PRESENT). THERE ARE 5 TYPES OF DRIVES: CONCRETE, ASPHALT, GRAVEL, GRAVEL WITH ASPHALT APRON AND FIELD/OIL WELL DRIVES. FIELD DRIVES AND OIL WELL DRIVES SHALL NOT BE PAVED. GRAVEL DRIVES SHALL BE PAVED BACK 4 INTO THE DRIVE-WAY UNLESS OTHERWISE DIRECTED BY THE ENGINEER. CONCRETE AND ASPHALT DRIVES SHALL HAVE BUTT JOINTS OR AS SHORT AN ASPHALT TAPER AS POSSIBLE (PREFERRED 4') AS DIRECTED BY THE ENGINEER SO AS TO PROVIDE A SMOOTH TRANSITION. GRAVEL DRIVES WITH ASPHALT APRONS SHALL ALSO HAVE BUTT JOINTS OR AS SHORT A ASPHALT TAPER AS POSSIBLE (PREFERRED 4') BUT ONLY IF THE EXISTING ASPHALT APRON IS IN AN ACCEPTABLE CONDITION TO BE PAVED OVER AS DIRECTED BY THE ENGINEER. IF THE ASPHALT APRON CANNOT BE PAVED OVER (FOR EXAMPLE, BROKEN INTO SMALL PIECES) AS DETERMINED BY THE ENGINEER, IT SHALL BE REMOVED BEFORE BEING PAVED BACK 4' INTO THE DRIVEWAY. ALL GRADING, PRIME OR TACK COAT, MATERIALS, LABOR, EQUIPMENT TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE DRIVES SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE ITEMS LISTED BELOW.

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE **SUB-SUMMARIES** FOR THE ABOVE DESCRIBED PURPOSE.

ITEM 448 ASPHALT CONCRETE INTERM. COURSE, TYPE 2, PG 64-22
LOCATION 1 - 23 CU.YD.
LOCATION 2 - 21 CU.YD.
LOCATION 3 - 50 CU.YD.

ITEM 448 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 70-22M
LOCATION 1 - 16 CU.YD.
LOCATION 2 - 15 CU.YD.
LOCATION 3 - 36 CU.YD.

ITEM 621 RAISED PAVEMENT MARKER REMOVED

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED IN THE PLANS TO REMOVE RAISED PAVEMENT MARKERS FOR DISPOSAL BY THE CONTRACTOR. RPM REMOVAL SHALL NOT OCCUR SOONER THAN 10 DAYS PRIOR TO RESURFACING OF THE ROADWAY. ALL RPM'S REMOVED SHALL BECOME THE PROPERTY OF THE CONTRACTOR.

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE **SUB-SUMMARIES** FOR THE ABOVE DESCRIBED PURPOSE.

ITEM 621 RAISED PAVEMENT MARKER REMOVED
LOCATION 1 - 181 EACH
LOCATION 2 - 212 EACH
LOCATION 3 - 495 EACH

ITEM 614 WORK ZONE MARKING SIGN

IN ACCORDANCE WITH CMS SECTION 614.04, THE QUANTITIES OF WORK ZONE MARKING SIGN HAVE BEEN CARRIED TO THE **SUB-SUMMARIES** TO BE USED AS DIRECTED BY THE ENGINEER.

W8-H12a (NO EDGE LINES): LOCATION 1 - 2 EACH, LOCATION 2 - 2 EACH, LOCATION 3 - 6 EACH
W8-H15 (GROOVED PAVEMENT): LOCATION 1 - 11 EACH, LOCATION 2 - 6 EACH, LOCATION 3 - 9 EACH

ITEM 614 WORK ZONE MARKING SIGN
LOCATION 1 - 13 EACH
LOCATION 2 - 8 EACH
LOCATION 3 - 15 EACH

CALCULATED
LIVE
CHECKED
DNM

GENERAL NOTES

LIC - 13 - 12.06

ITEM 209 LINEAR GRADING

IN ORDER TO PROVIDE POSITIVE DRAINAGE FROM THE ROADWAY SURFACE TO THE SHOULDER BREAK, THE EXISTING ROADWAY SHOULDERS SHALL BE GRADED AND SHAPED USING A GRADER OF ADEQUATE SIZE TO PERFORM THE WORK TO THE SATISFACTION OF THE ENGINEER.

ALL EXCESS MATERIAL REMAINING AROUND GUARDRAIL AND OTHER AREAS AFTER THE GRADER WORK IS COMPLETED AND NOT DISPOSED OF ON THE SITE, SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. ALL EQUIPMENT, LABOR, OR INCIDENTALS REQUIRED TO COMPLETE THIS ITEM SHALL BE INCLUDED FOR PAYMENT IN THE UNIT PRICE BID FOR ITEM 209 LINEAR GRADING.

THIS WORK MAY BE INTERMITTENT AND SPREAD THROUGHOUT THE PROJECT LIMITS, AS DIRECTED BY THE ENGINEER. THE CONTRACTOR WILL ONLY BE PAID FOR INTERSECTIONS AND GAPS IF THEY ARE WITHIN THE LIMITS OF A SECTION MARKED BY THE ENGINEER FOR GRADING.

ALL LINEAR GRADING WORK SHALL BE DONE BEFORE PLACING THE ASPHALT SURFACE COURSE

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE SUB-SUMMARIES FOR THE ABOVE PURPOSES.

ITEM 209 LINEAR GRADING

LOCATION 1 - 1 MILE
LOCATION 2 - 1 MILE
LOCATION 3 - 3 MILE

BUTT JOINT

A BUTT JOINT WILL BE REQUIRED AT LOCATIONS SPECIFIED BELOW AND AT THE EXTRA AREAS WITH WEARING COURSE REMOVED.

BUTT JOINTS SHALL BE AS PER STANDARD CONSTRUCTION DRAWING BP-3.1 UNLESS OTHERWISE SHOWN IN THE PLANS.

MINIMUM BUTT JOINT LENGTHS SHALL BE 35' ON THE MAINLINE AND 10' ON THE EXTRA AREAS.

LOCATION	ROUTE	DESCRIPTION	S.L.M.	ITEM 614, ASPHALT CONCRETE FOR MAINTAINING TRAFFIC CU. YD.
1	S.R. 13	BEGIN WORK	12.06	1.5
1	S.R. 13	BRIDGE: LIC-13-1396	13.96	3.0
1		TOTAL		4.5
2	S.R. 13	BRIDGE: LIC-13-1496	14.96	2.8
3	S.R. 13	BRIDGE: LIC-13-1847	18.47	2.8
3	S.R. 13	BRIDGE: LIC-13-2050	20.50	2.8
3	S.R. 13	BRIDGE: LIC-13-2239	22.39	2.8
3	S.R. 13	END WORK	22.53	1.4
3		TOTAL		9.8

ITEM 254 PAVEMENT PLANING, ASPHALT CONCRETE

DEPTH OF PLANING SHALL BE 3.0" FULL WIDTH OF PAVEMENT FROM SLM 13.75 TO SLM 13.98 IN LOCATION 1, SLM 13.98 TO SLM 14.14 IN LOCATION 2 AND SLM 22.43 TO SLM 22.53 IN LOCATION 3.

THE ROADWAY SHALL BE PLANED SUCH THAT POSITIVE DRAINAGE IS CREATED FROM THE CENTER LINE TO THE EDGE OF PAVEMENT IN TANGENT SECTIONS AND SHALL FOLLOW EXISTING SUPERELEVATIONS WHERE APPLICABLE. ALL REQUIREMENTS OF ITEM 254 SHALL APPLY.

ITEM 253 PAVEMENT REPAIR

AN ESTIMATED QUANTITY FOR PAVEMENT REPAIR HAS BEEN INCLUDED IN THE PLAN TO BE USED AT THE LOCATIONS SHOWN IN THE TABLE BELOW, OR AS DIRECTED BY THE ENGINEER. REPAIRS SHALL TAKE PLACE PRIOR TO THE PAVING OPERATIONS. THE INTENT OF THIS OPERATION IS TO REPAIR THOSE AREAS OF PAVEMENT WHICH HAVE COMPLETELY FAILED (PUMPING OF SUB-BASE MATERIAL) AND NOT TO CORRECT SURFACE IRREGULARITIES. DEPTH OF EXCAVATION SHALL BE 6". AFTER EXCAVATION HAS BEEN COMPLETED, THE FACE OF THE REPAIR SHALL BE COATED WITH 407 TACK COAT. REPLACEMENT MATERIAL WILL BE 6" OF ITEM 301 ASPHALT CONCRETE BASE, PG64-22 (PLACED AND COMPACTED AS DIRECTED). REPAIR QUANTITIES MAY BE USED ON THE MAINLINE PAVEMENT OR ON PAVED SHOULDERS. ALL REPAIRS SHALL BE DONE EITHER BEFORE OR CONCURRENT WITH THE PARTIAL DEPTH REPAIRS.

ALL EXCAVATION, MATERIALS, LABOR, EQUIPMENT, TOOLS, TRAFFIC CONTROL AND INCIDENTALS NEEDED TO COMPLETE THE WORK DESCRIBED ABOVE SHALL BE PAID FOR UNDER ITEM 253 PAVEMENT REPAIR.

FULL DEPTH REPAIR					
BEGIN SLM	END SLM	SIDE	LENGTH	WIDTH	CU.YD.
12.780	12.810	RT	160	6	18
TOTAL LOCATION 1					18
16.560	16.586	RT	140	6	16
19.098	19.152	RT	285	6	32
21.715	21.740	RT	132	6	15
21.780	21.830	RT	264	6	30
TOTAL LOCATION 3					93
16.032	16.134	LT	539	6	60
TOTAL LOCATION 2					60
17.510	17.532	LT	120	6	14
17.610	17.640	LT	160	6	18
18.110	18.231	LT	640	6	71
TOTAL LOCATION 3					103

THE QUANTITIES SHOWN IN THE ABOVE TABLE HAVE BEEN INCREASED BY 20 PERCENT TO ALLOW FOR ANY EXTRA REPAIR AREAS AS DEEMED NECESSARY BY THE PROJECT ENGINEER.

THE FOLLOWING CONTINGENCY QUANTITIES HAVE BEEN CARRIED TO THE SUB-SUMMARIES FOR THE ABOVE DESCRIBED PURPOSE.

ITEM 253 PAVEMENT REPAIR
LOCATION 1 - 22 CU.YD.
LOCATION 2 - 72 CU.YD.
LOCATION 3 - 235 CU.YD.

ITEM 251 PARTIAL DEPTH REPAIR, MISC.: PAVED SHOULDER REPAIR

AN ESTIMATED QUANTITY FOR PARTIAL DEPTH PAVEMENT REPAIR HAS BEEN INCLUDED IN THE PLAN TO BE USED AT THE LOCATIONS SHOWN IN THE TABLE BELOW, OR AS DIRECTED BY THE ENGINEER. REPAIRS SHALL TAKE PLACE PRIOR TO THE PAVING OPERATIONS. THE ROADWAY SHALL BE EXCAVATED 3" IN DEPTH AND 6' (FEET) WIDE FROM OUTSIDE EDGE OF PAVED SHOULDER. AFTER 3" EXCAVATION, THE CONTRACTOR SHALL PLACE AND COMPACT 3" OF ITEM 301 ASPHALT CONCRETE BASE, PG64-22 TO BE FLUSH WITH EXISTING ROADWAY SURFACE.

ALL EXCAVATION, MATERIALS, LABOR, EQUIPMENT, TOOLS, TRAFFIC CONTROL AND INCIDENTALS NEEDED TO COMPLETE THE WORK DESCRIBED ABOVE SHALL BE PAID FOR UNDER ITEM 251 PARTIAL DEPTH REPAIR, MISC.: PAVED SHOULDER REPAIR.

PARTIAL DEPTH REPAIR					
BEGIN SLM	END SLM	SIDE	LENGTH	WIDTH	CU.YD.
12.900	12.940	RT	220	6	13
13.140	13.260	RT	640	6	36
12.875	13.510	LT	3360	6	187
13.660	13.780	LT	640	6	36
13.845	13.930	LT	450	6	25
TOTAL LOCATION 1					297
14.400	14.900	RT	2640	6	147
15.180	15.250	RT	370	6	21
15.340	16.470	RT	5970	6	332
14.090	14.950	LT	4550	6	253
14.980	15.450	LT	2490	6	139
15.635	16.535	LT	4760	6	265
TOTAL LOCATION 2					1157
16.530	17.530	RT	5280	6	294
17.780	17.990	RT	1110	6	62
18.700	18.860	RT	850	6	47
19.060	19.160	RT	530	6	30
19.360	19.620	RT	1380	6	77
19.820	20.120	RT	1590	6	89
20.750	21.000	RT	1320	6	74
21.520	21.580	RT	320	6	18
21.640	21.740	RT	530	6	30
21.800	21.930	RT	690	6	39
16.780	16.970	LT	1010	6	56
17.235	17.530	LT	1560	6	87
17.580	18.280	LT	3700	6	206
18.660	18.760	LT	530	6	30
18.870	19.360	LT	2590	6	144
19.580	19.800	LT	1170	6	65
21.090	21.960	LT	4600	6	256
22.020	22.390	LT	1960	6	109
TOTAL LOCATION 3					1713

THE FOLLOWING CONTINGENCY QUANTITIES HAVE BEEN CARRIED TO THE SUB-SUMMARIES FOR THE ABOVE DESCRIBED PURPOSE.

ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR
LOCATION 1 - 297 CU.YD.
LOCATION 2 - 1157 CU.YD.
LOCATION 3 - 1713 CU.YD.

GENERAL NOTES

LIC - 13 - 12.06

ITEM SPECIAL - REINFORCED MESH FOR TRANSVERSE AND/OR LONGITUDINAL JOINTS AND CRACKS

THIS ITEM SHALL BE USED TO REINFORCE LONGITUDINAL CRACKS LOCATED NEAR THE EDGE LINE ON SR 13. PLACE REINFORCED MESH 5.0' WIDE CENTERED OVER LONGITUDINAL JOINT CREATED BY PAVEMENT REPAIR OPERATIONS OR EXISTING LONGITUDINAL CRACK (AS SHOWN IN THE DETAILS BELOW). AFTER PLACING THE REINFORCING MESH, OVERLAY ENTIRE ROADWAY, INCLUDING PAVED SHOULDERS, WITH TOTAL OF 3.0" OF ASPHALT CONCRETE. THIS WORK WILL BE INTERMITTENT AND SPREAD THROUGH-OUT THE PROJECT. REINFORCING MATERIAL SHALL BE PLACED AT ALL LOCATION OF PAVEMENT REPAIR AND PARTIAL DEPTH PAVEMENT REPAIR AREAS. AN ADDITIONAL QUANTITY HAS BEEN INCLUDED TO BE USED AS DIRECTED BY THE ENGINEER TO PLACE OVER EXISTING CRACKS WHERE NO REPAIRS ARE NECESSARY.

REINFORCING MATERIAL SHALL BE GLASGRID #8502 OR EQUIVALENT AND SHALL BE PLACED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS. ALL MATERIALS, LABOR, EQUIPMENT, TRAFFIC CONTROL AND INCIDENTALS NECESSARY TO COMPLETE PLACING OF REINFORCING MESH SHALL BE INCLUDED FOR PAYMENT IN THE UNIT PRICE BID ITEM SPECIAL - REINFORCED MESH FOR TRANSVERSE AND/OR LONGITUDINAL JOINTS AND CRACKS.

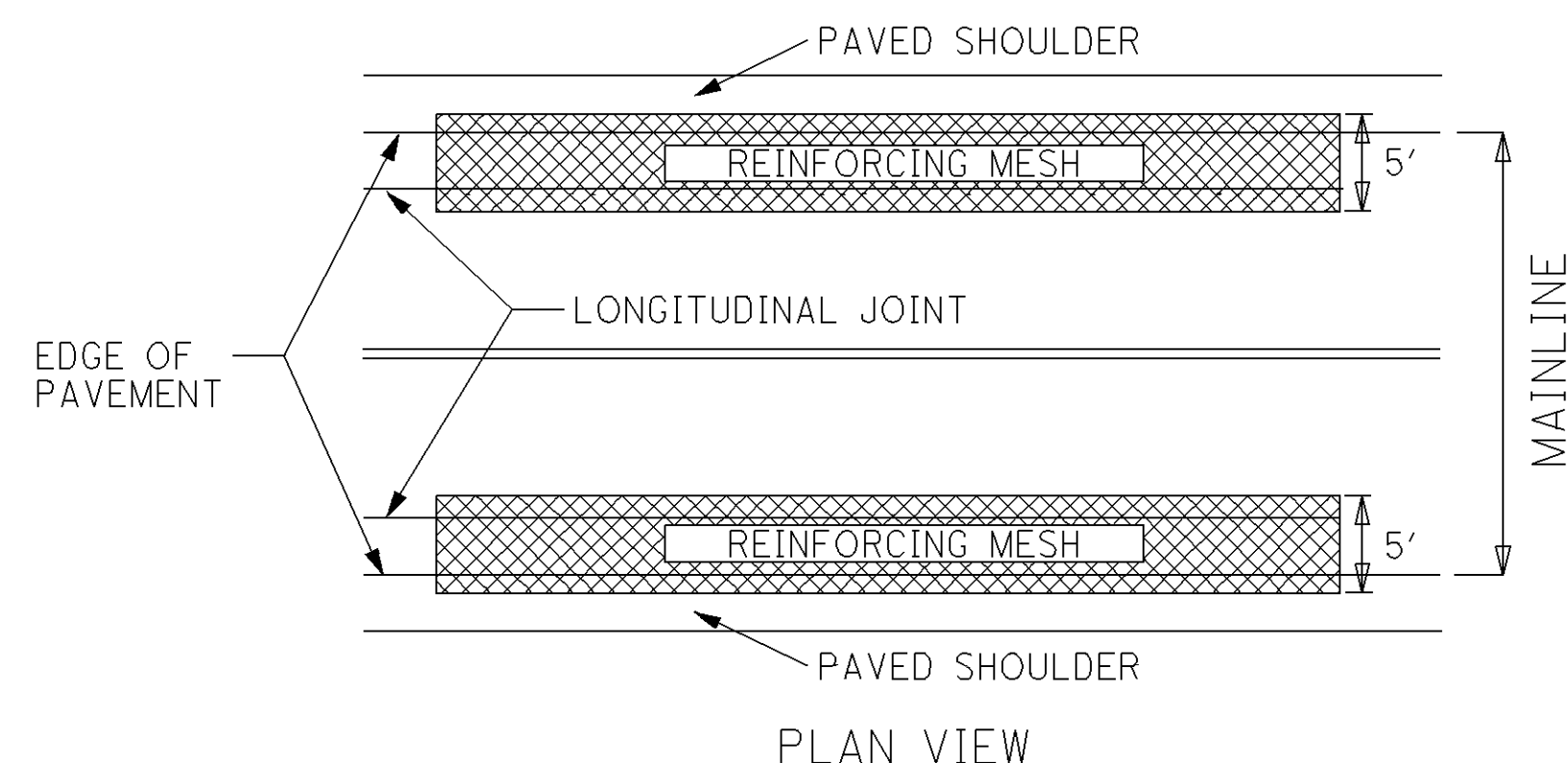
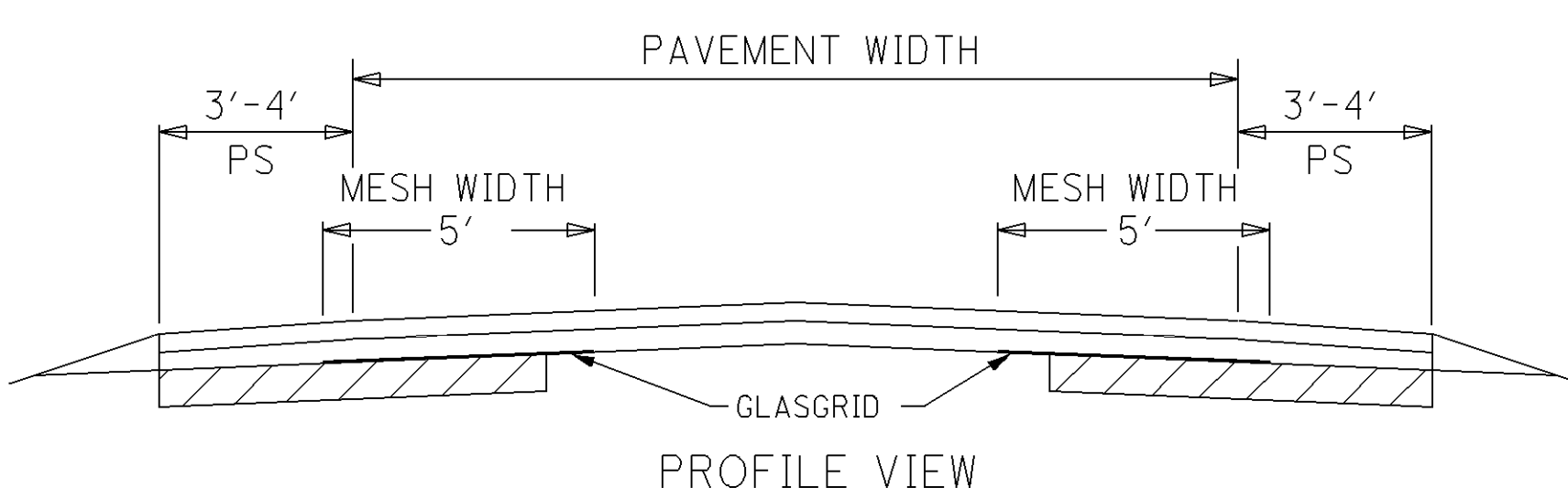
ITEM SPECIAL - REINFORCED MESH FOR TRANSVERSE AND/OR LONGITUDINAL JOINTS AND CRACKS

LOCATION 1 - 3039 SQ. YD.

LOCATION 2 - 11845 SQ. YD.

LOCATION 2 - 20773 SQ. YD.

 PAVEMENT REPAIR/PARTIAL DEPTH REPAIR
ITEM 301 ASPHALT CONCRETE BASE, PG 64-22



ITEM 614 MAINTAINING TRAFFIC

AT NO TIME SHALL TRAFFIC BE MAINTAINED ON THE PLANED SURFACE, AT LEAST ONE COURSE OF ASPHALT CONCRETE SHALL BE IN PLACE BEFORE OPENING TO TRAFFIC.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT, IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

CATCH BASINS, MANHOLES AND WATER VALES ADJUSTED TO GRADE

THESE ITEMS SHALL BE USED TO ADJUST CATCH BASINS, MANHOLES AND VALVE BOXES LOCATED THROUGH -OUT THE PROJECT LIMITS AS DIRECTED BY THE ENGINEER. ALL MATERIALS, LABOR EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE WORK DESCRIBED SHALL BE INCLUDED FOR PAYMENT WITH THE ITEMS LISTED BELOW. ANY GAS VALVE BOXES AND TELEPHONE COMPANY MANHOLES ON THIS PROJECT SHALL BE ADJUSTED TO GRADE BY THE RESPECTIVE OWNERS.

LOCATION 1:

ITEM 604 MANHOLE ADJUSTED TO GRADE - 10 EACH

ITEM 638 VALVE BOX ADJUSTED TO GRADE - 3 EACH

LOCATION 1 QUANTITIES

ALL OF LOCATION 1 IS CONSIDERED AN URBAN PRINCIPAL ARTERIAL, HOWEVER, THERE ARE SEVERAL PLACES WHERE THE CITY OF NEWARK CORPORATION LINE INCORPORATES HALF OF THE PAVEMENT OR ALL OF THE PAVEMENT FOR VERY SMALL DISTANCES. IN ORDER TO REDUCE CONFUSION, WE HAVE DECIDED NOT TO LIST ALL OF THESE LOCATIONS. INSTEAD, WE HAVE SEPARATED LOCATION 1 INTO PERCENTAGES. WE HAVE DETERMINED THAT ONLY SIXTY-EIGHT PERCENT OF LOCATION 1 IS WITHIN THE CITY OF NEWARK. THEREFORE, SIXTY-EIGHT PERCENT OF THE QUANTITIES CALCULATED FOR LOCATION 1 WILL REMAIN IN LOCATION 1. THE REMAINING THIRTY-TWO PERCENT OF THE QUANTITIES WILL BE INCLUDED IN LOCATION 2.

ITEM 632 DETECTOR LOOP, AS PER PLAN

ALL STOP LINE INDUCTANCE DETECTOR LOOPS SHALL BE THE POWER HEAD CONFIGURATION SHOWN ON TC-82.10. THE WIDTH SHALL BE AS SPECIFIED ON TC-82.10 AND THE LENGTH SHALL BE AS CURRENTLY CALLED FOR IN THE PLANS. THE STOP LINE DETECTOR LOOPS SHALL NOT BE WIRED TO ANY OTHER LOOPS AND SHALL HAVE ITS OWN DETECTOR CHANNEL.

ALL DILEMMA ZONE INDUCTANCE DETECTOR LOOPS CALLED FOR IN THE PLANS SHALL BE THE ANGULAR DESIGN DETECTION (ADD) LOOP AS SHOWN ON TC-82.10. DIMENSIONS SHALL BE AS SPECIFIED ON TC-82.10.

SYSTEM LOOPS SHALL BE AS DEPICTED IN THE PLANS.

ALL STOP LINE DETECTION SHALL BE TESTED FOR A BICYCLE TARGET AND ALL DILEMMA DETECTION ZONES SHALL BE TESTED FOR A MOTORCYCLE TARGET.

ALL DETECTOR LOOPS SHALL BE CUT INTO THE PLANED SURFACE OR THE PROPOSED INTERMEDIATE COURSE AT A DEPTH OF 4" FROM THE PROPOSED SURFACE ELEVATION. IF THE CONTRACTOR SO CHOOSES, THEY MAY CUT THE DETECTOR LOOPS INTO THE EXISTING ASPHALT BEFORE PLANING BUT SHALL MAKE SURE THE MATERIAL USED TO FILL THE SAW CUT IS LEFT FAR ENOUGH BELOW THE SURFACE COURSE THAT IT WILL NOT BE DISTURBED DURING THE PLANING OPERATION. THE CONTRACTOR SHALL TEST ALL LEAD-IN CABLES PRIOR TO MAKING THE FINAL SPLICE.

PLACEMENT SHALL BE AS PER SPECIFICATION 632.10. FINAL LOCATIONS, SIZE AND ORIENTATION SHALL BE PROVIDED TO THE CONTRACTOR AT THE PRE-CONSTRUCTION MEETING. ALL MATERIALS, LABOR, TOOLS, EQUIPMENT, TRAFFIC CONTROL AND INCIDENTALS NECESSARY TO PERFORM THE WORK DESCRIBED ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 632, DETECTOR LOOP, AS PER PLAN.

LOCATION 1 - 3 EACH

S.R.13 N.B. @ 21ST. ST. - 1 DILEMMA ZONE , 1 STOP LINE

S.R. 13 S.B. @ 21ST. ST. - 1 DILEMMA ZONE

ITEM 614 MAINTAINING TRAFFIC

A MINIMUM OF 1 LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT AND STANDARD DRAWINGS MT-97.10 AND MT-97.12.

AT NO TIME SHALL TRAFFIC BE MAINTAINED ON THE PLANED SURFACE, AT LEAST ONE COURSE OF ASPHALT CONCRETE SHALL BE IN PLACE BEFORE OPENING TO TRAFFIC.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT, IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614 PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN, ON SITE, FOR THE DURATION OF THE PROJECT. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS MAINTAINED BY THE DIRECTOR (OFFICE OF MATERIALS MANAGEMENT). THE APPROVED LIST OF PORTABLE CHANGEABLE MESSAGE SIGNS CAN BE FOUND ON THE ODOT WEBSITE BY CLICKING ON THE SERVICES MENU, THEN LICKING ON MATERIALS MANAGEMENT. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 650 FT. AND 475 FT., RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. PCMS TRAILERS SHOULD BE DELINEATED ON A PERMANENT BASIS BY AFFIXING RETROREFLECTIVE MATERIAL, IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER AS SEEN BY ONCOMING ROAD USERS.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET(S) OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED, FACING AWAY FROM ALL TRAFFIC, AND SHALL DISPLAY ONE OR MORE TYPE G YELLOW RETROREFLECTIVE SHEETING SURFACES OF 9-INCH BY 15-INCH MINIMUM SIZE FACING TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ITEM 614 PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN (cont'd)

(THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN 2 HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.)

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.)

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF CMS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

A TOTAL OF 2 PCMS SHALL BE REQUIRED FOR THIS PROJECT.

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO GENERAL SUMMARY:

ITEM 614 PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

- LOCATION 1 – 20 DAYS**
- LOCATION 2 – 40 DAYS**
- LOCATION 3 – 90 DAYS**

ITEM 614 LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE

IN ADDITION TO THE REQUIREMENTS OF 614 AND THE LATEST EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED LAW ENFORCEMENT OFFICER AND OFFICIAL PATROL CAR WITH WORKING TOP MOUNTED EMERGENCY FLASHING LIGHTS WILL BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS:

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED IN THIS NOTE WILL NOT GENERALLY BE PERMITTED AT PROJECT COST UNLESS PRIOR APPROVAL HAS BEEN OBTAINED FROM THE ENGINEER. LEOS SHOULD NOT BE USED WHERE THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD) INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS:

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED. IN GENERAL, LEOS SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH A LIST OF THE APPROPRIATE LAW ENFORCEMENT AGENCY(S), INCLUDING ADDRESS AND TELEPHONE NUMBER.

THE LEO SHOULD REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING THE SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF THE SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHOULD NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF THE SHIFT.

LAW ENFORCEMENT OFFICERS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR). **THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:**

ITEM 614 LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE

LOCATION 1 - 20 HRS, LOCATION 2 – 30 HRS, LOCATION 3 – 50 HRS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR.

CALCULATED
LME
CHECKED
DNM

GENERAL NOTES

LIC - 13 - 12.06

PAVEMENT DATA

LOCATION	COUNTY	ROUTE	BEGIN LOG POINT SLM	END LOG POINT SLM	LENGTH		PAVEMENT WIDTH (FEET)	TYPICAL	PAVEMENT AREA	254		407		448 ASPHALT CONCRETE				614	
					MILES	LIN. FT.				SQ. YD.	SQ. YD.	GAL.	GAL.	INCHES	CU. YD.	INCHES	CU. YD.	MILE	MILE
1	LIC	S.R. 13	12.06	13.75	1.69	8,923.20	24.0	1	23,795.2		1,784.7	1,189.8	1.75	1,156.8	1.25	826.3	1.69	1.69	
1	LIC	S.R. 13	13.75	13.98	0.23	1,214.40	24.0	1	3,238.4	3,238.4	242.9	162.0	1.75	157.5	1.25	112.5	0.23	0.23	
1	LIC	S.R. 13	12.32	12.42	RIGHT TURN LANE			1	481.0		36.1	24.1	1.75	23.4	1.25	16.8			
1	LIC	S.R. 13	12.74	12.88	LEFT/RIGHT TURN LANES			1	1,055.0		79.2	52.8	1.75	51.3	1.25	36.7			
1	LIC	S.R. 13	13.25	13.34	LEFT TURN LANE			1	417.0		31.3	20.9	1.75	20.3	1.25	14.5			
1	LIC	S.R. 13	13.48	13.67	LEFT TURN LANE			1	834.0		62.6	41.7	1.75	40.6	1.25	29.0			
1	LIC	S.R. 13	13.78	13.83	RIGHT TURN LANE			2	313.0	313.0	23.5	15.7	1.75	15.3	1.25	10.9			
DEDUCT FOR BRIDGES (FROM SHEET 10)									(495.5)	(422.7)	(37.2)	(24.8)	1.75	(24.1)	1.25	(17.3)	(0.04)	(0.04)	
TOTALS											3,128.7	2,223.1	1,482.2		1,441.1		1,029.4	1.88	1.88
TOTALS URBAN PAVING 68% (CARRIED TO LOCATION 1 SUB-SUMMARY)											2,127.5	1,511.7	1,007.9		979.9		700.0	1.28	1.28
TOTALS GENERAL PAVING 32% (CARRIED WITH LOCATION 2 BELOW)											1,001.2	711.4	474.3		461.2		329.4	0.60	0.60
2	LIC	S.R. 13	13.98	14.14	0.16	844.80	24.0	1	2,252.8	2,252.8	169.0	112.7	1.75	109.6	1.25	78.3	0.16	0.16	
2	LIC	S.R. 13	14.14	16.53	2.39	12,619.20	24.0	1	33,651.2		2,523.9	1,682.6	1.75	1,635.9	1.25	1,168.5	2.39	2.39	
2	LIC	S.R. 13	13.98	14.11	LEFT TURN LANE			1	577.0	577.0	43.3	28.9	1.75	28.1	1.25	20.1			
DEDUCT FOR BRIDGES (FROM SHEET 10)									(422.7)	(31.8)	(21.2)	1.75	(20.6)	1.25	(14.7)	(0.04)	(0.04)		
TOTALS (CARRIED TO LOCATION 2 SUB-SUMMARY)											3,831.0	3,415.8	2,277.3		2,214.2		1,581.6	3.11	3.11
3	LIC	S.R. 13	16.53	22.39	5.86	30,940.80	24.0	1	82,508.8		6,188.2	4,125.5	1.75	4,010.9	1.25	2,864.9	5.86	5.86	
3	LIC	S.R. 13	22.39	22.53	0.14	739.20	24.0	1	1,971.2	1,971.2	147.9	98.6	1.75	95.9	1.25	68.5	0.14	0.14	
3	LIC	S.R. 13	18.77	19.09	LEFT TURN LANE			1	1,407.0		105.6	70.4	1.75	68.4	1.25	48.9			
3	LIC	S.R. 13	18.84	18.89	RIGHT TURN LANE			1	427.0		32.1	21.4	1.75	20.8	1.25	14.9			
DEDUCT FOR BRIDGES (FROM SHEET 10)									(2,488.5)	(694.7)	(186.7)	(124.5)	1.75	(121.0)	1.25	(86.5)	(0.04)	(0.04)	
TOTALS (CARRIED TO LOCATION 3 SUB-SUMMARY)											1,276.5	6,287.1	4,191.4		4,075.0		2,910.7	5.96	5.96

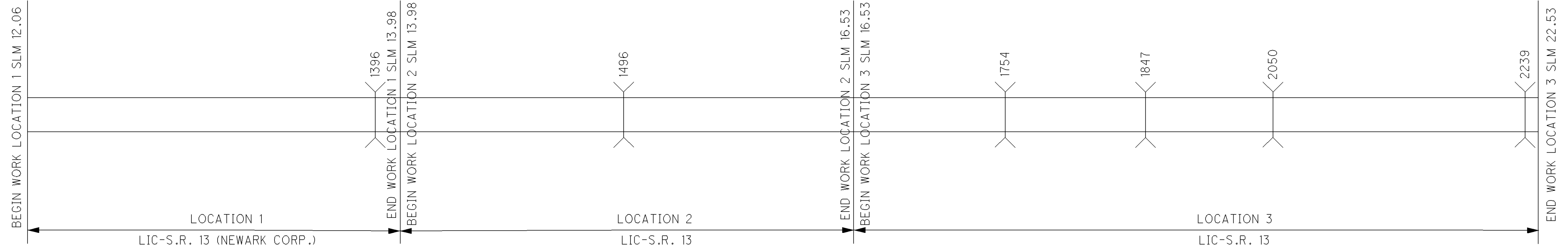
NOTE:
 THE PAVEMENT WIDTHS SHOWN IN THE "PAVEMENT DATA" TABLE ABOVE ARE THE WIDTHS WHICH HAVE BEEN DETERMINED TO HAVE SUFFICIENT ROADWAY BASE FOR PAVING. IF ACTUAL ROADWAY WIDTHS DIFFER, THE ROADWAY SHALL BE PAVED ONLY THE WIDTH SHOWN IN THE AFOREMENTIONED TABLE. IF THE EXISTING ROADWAY IS WIDER THAN THAT WHICH IS SHOWN IN THE TABLE, PAVING SHALL BE CENTERED ABOUT THE FULL WIDTH OF THE ROADWAY AND ANY EXCESS EXISTING PAVEMENT ON THE EDGES SHALL BE COVERED WITH ITEM 617 COMPACTED AGGREGATE. PAVING IN CURBED ROADWAY SECTIONS SHALL BE FROM CURB TO CURB.

SEE SHEET 7 FOR TYPICALS AND STRAIGHT LINE INFORMATION

CALCULATED
 LME
 CHECKED
 DNM

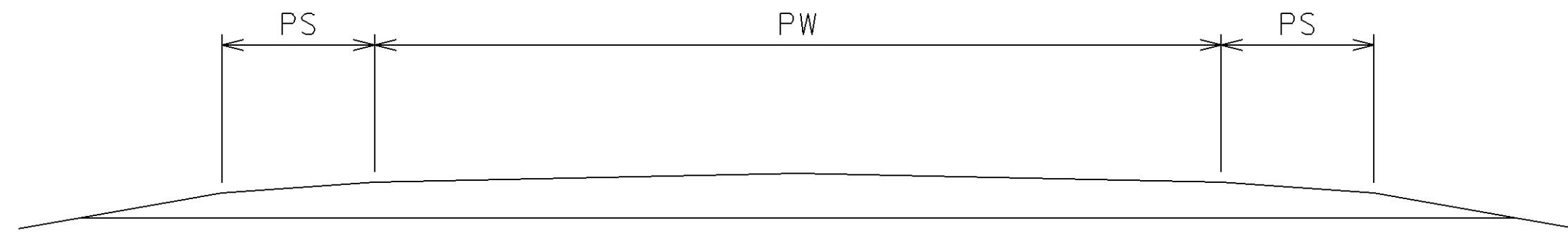
ASPHALT CONCRETE DATA

LIC-13-12.06

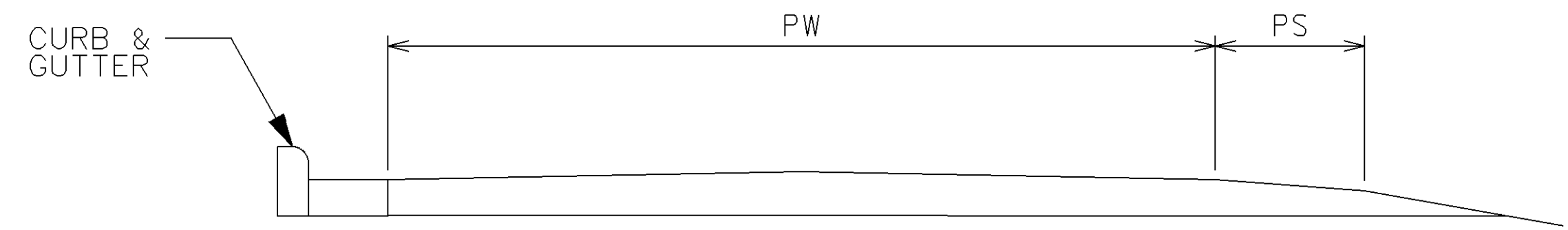


PW = PAVEMENT WIDTH
 PS = PAVED SHOULDER
 AS = AGGREGATE SHOULDER

TYPICAL 1



TYPICAL 2



CALCULATED	LME
CHECKED	DNM

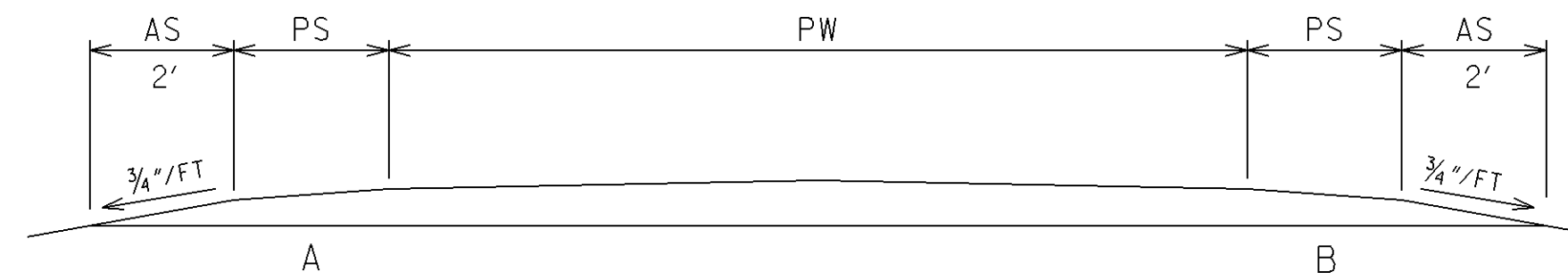
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LIC-13-12.06

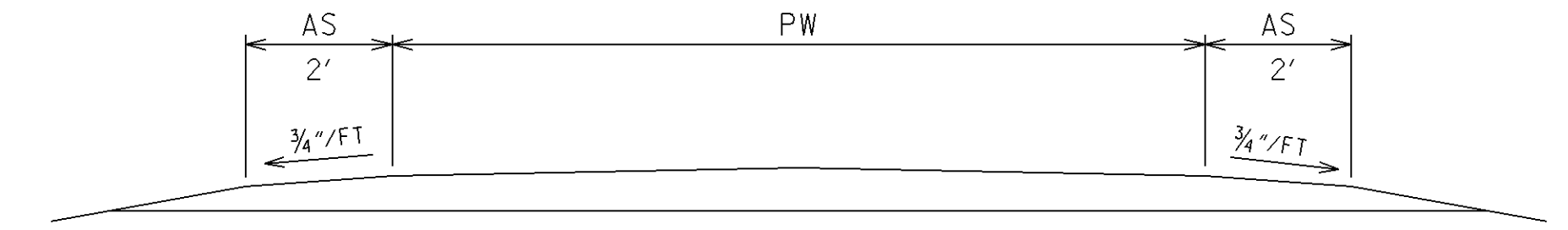
NOTE:
 THE SHOULDER WIDTHS SHOWN IN THE "SHOULDER DATA" TABLE BELOW ARE THE WIDTHS WHICH HAVE BEEN DETERMINED TO HAVE SUFFICIENT BASE FOR PAVING. IF ACTUAL SHOULDER WIDTHS DIFFER, THE SHOULDERS SHALL BE PAVED ONLY THE WIDTH SHOWN IN THE AFOREMENTIONED TABLE. IF THE EXISTING ROADWAY IS WIDER THAN THAT WHICH IS SHOWN IN THE TABLE, PAVING SHALL BE CENTERED ABOUT THE FULL WIDTH OF THE ROADWAY AND ANY EXCESS EXISTING PAVEMENT ON THE EDGES SHALL BE COVERED WITH ITEM 617 COMPACTED AGGREGATE. PAVING IN CURBED ROADWAY SECTIONS SHALL BE FROM CURB TO CURB.

PW = PAVEMENT WIDTH
 PS = PAVED SHOULDER
 AS = AGGREGATE SHOULDER

TYPICAL 1



TYPICAL 2



SHOULDER DATA

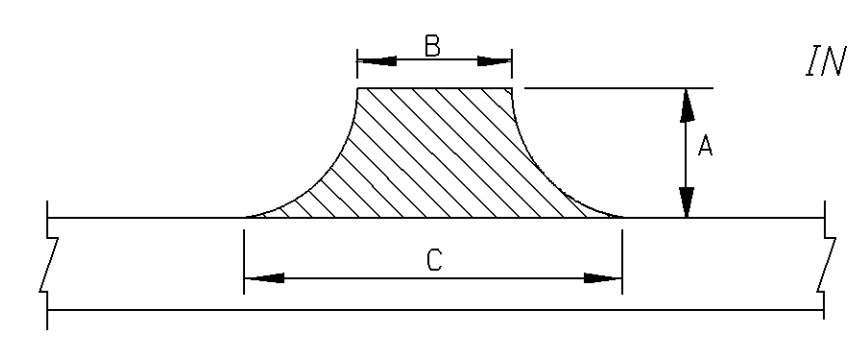
LOCATION	COUNTY	ROUTE	BEGIN LOG POINT SLM	END LOG POINT SLM	LENGTH		TYPICAL	PROPOSED WIDTH (FT.)		SHOULDER AREA	254		407		448 ASPHALT CONCRETE				617					
											MILES	LIN. FT.	A	B	SQ. YD.	SQ. YD.	GAL.	GAL.	INCHES	CU. YD.	INCHES	CU. YD.	INCHES	CU. YD.
1	LIC	S.R. 13	12.06	13.75	1.69	8923.2	1	4	4	7,931.7		594.9	396.6	1.75	385.6	1.25	275.4	1.75	192.8					
1	LIC	S.R. 13	13.75	13.98	0.23	1214.4	1	4	4	1,079.5	1,079.5	81.0	54.0	1.75	52.5	1.25	37.5	1.75	26.3					
DEDUCT FOR BRIDGES (FROM SHEET 10)										(165.2)	(165.2)	(12.4)	(8.3)	1.75	(8.0)	1.25	(5.7)	1.75	(4.1)					
TOTALS											914.3	663.5	442.3		430.1		307.2		215.0					
TOTALS URBAN PAVING 68% (CARRIED TO LOCATION 1 SUB-SUMMARY)											621.7	451.2	300.8		292.5		208.9		146.2					
TOTALS GENERAL PAVING 32% (CARRIED WITH LOCATION 2 BELOW)											292.6	212.3	141.5		137.6		98.3		68.8					
2	LIC	S.R. 13	13.98	14.14	0.16	844.8	1	4	4	750.9	750.9	56.3	37.5	1.75	36.5	1.25	26.1	1.75	18.3					
2	LIC	S.R. 13	14.14	14.40	0.26	1372.8	1	4	4	1,220.3		91.5	61.0	1.75	59.3	1.25	42.4	1.75	29.7					
2	LIC	S.R. 13	14.40	16.53	2.13	11246.4	1	3	3	7,497.6		562.3	374.9	1.75	364.5	1.25	260.3	1.75	243.0					
DEDUCT FOR BRIDGES (FROM SHEET 10)										(105.7)		(7.9)	(5.3)	1.75	(5.1)	1.25	(3.7)	1.75	(18.0)					
TOTALS (CARRIED TO LOCATION 2 SUB-SUMMARY)											1,043.5	914.5	609.6		592.8		423.4		341.8					
3	LIC	S.R. 13	16.53	18.09	1.56	8236.8	1	3	3	5,491.2		411.8	274.6	1.75	266.9	1.25	190.7	1.75	178.0					
3	LIC	S.R. 13	18.09	18.19	0.10	528.0	1	3	7	586.7		44.0	29.3	1.75	28.5	1.25	20.4	1.75	11.5					
3	LIC	S.R. 13	18.19	22.39	4.20	22176.0	1	3	3	14,784.0		1,108.8	739.2	1.75	718.7	1.25	513.3	1.75	479.2					
3	LIC	S.R. 13	22.39	22.53	0.14	739.2	1	3	3	492.8	492.8	37.0	24.6	1.75	24.0	1.25	17.1	1.75	16.0					
DEDUCT FOR BRIDGES (FROM SHEET 10)										(622.1)	(173.7)	(46.7)	(31.1)	1.75	(30.2)	1.25	(21.6)	1.75	(18.0)					
TOTALS (CARRIED TO LOCATION 3 SUB-SUMMARY)											319.1	1,554.9	1,036.6		1,007.9		719.9		666.7					

L013_MPS_001.DGN 4-5-11

SHOULDER TREATMENT DATA

LIC-13-12.06

CALCULATED
 LME
 CHECKED
 DNM



INTERSECTIONS

$$AREA = \left[A \frac{(B + C)}{2} \right] / 9$$

EXTRA AREAS

LOCATION	COUNTY	ROUTE	SIDE	DESCRIPTION	INTERSECTIONS			AREA SQ. YD.	202 WEARING COURSE REMOVED SQ. YD.	407		448 ASPHALT CONCRETE			
					DETAIL DIMENSION					TACK COAT @ 0.075 GAL./SQ. YD. GAL.	TACK COAT FOR INTERMEDIATE COURSE @ 0.05 GAL./SQ. YD. GAL.	THICKNESS IN.	INTERMEDIATE COURSE, TYPE 2, PG 64-22 CU. YD.	THICKNESS IN.	SURFACE COURSE, TYPE 1, PG 64-22 CU. YD.
					A	B	C								
					FT.	FT.	FT.								
1	LIC	S.R. 13	LT	GLADYS AVE	30	20	67	145.0	145.0	10.9	7.3	1.75	7.1	1.25	5.1
1	LIC	S.R. 13	LT	GREENFIELD AVE	30	21	63	140.0	140.0	10.5	7.0	1.75	6.9	1.25	4.9
1	LIC	S.R. 13	LT	MYRTLE AVE	30	23	73	160.0	160.0	12.0	8.0	1.75	7.8	1.25	5.6
1	LIC	S.R. 13	LT	HILLVIEW CIRCLE	56	21	100	376.5	376.5	28.3	18.9	1.75	18.4	1.25	13.1
1	LIC	S.R. 13	LT	21ST STREET	55	50	165	657.0	657.0	49.3	32.9	1.75	32.0	1.25	22.9
1	LIC	S.R. 13	LT	DANIELLE DRIVE	17	45	65	103.9	103.9	7.8	5.2	1.75	5.1	1.25	3.7
1	LIC	S.R. 13	LT	GREGORY DRIVE	50	25	100	347.3	347.3	26.1	17.4	1.75	16.9	1.25	12.1
1	LIC	S.R. 13	LT	NORTH VERNON AVE	35	19	66	165.3	165.3	12.4	8.3	1.75	8.1	1.25	5.8
1	LIC	S.R. 13	LT	GLENRIDGE DRIVE	26	33	56	128.6	128.6	9.7	6.5	1.75	6.3	1.25	4.5
1	LIC	S.R. 13	LT	ASBURY AVE	25	21	56	107.0	107.0	8.1	5.4	1.75	5.3	1.25	3.8
TOTALS									2,330.6	175.1	116.9		113.9		81.5
TOTALS URBAN PAVING 68% (CARRIED TO LOCATION 1 SUB-SUMMARY)									1,584.8	119.1	79.5		77.5		55.4
TOTALS GENERAL PAVING 32% (CARRIED WITH LOCATION 2 BELOW)									745.8	56.0	37.4		36.4		26.1
2	LIC	S.R. 13	LT	S.R. 657	65	24	100	447.8	447.8	33.6	22.4	1.75	21.8	1.25	15.6
2	LIC	S.R. 13	LT	CHESTNUT HILLS ROAD	45	28	100	320.0	320.0	24.0	16.0	1.75	15.6	1.25	11.2
2	LIC	S.R. 13	RT	WILLIAMS ROAD (HATFIELD LN)	25	16	41	79.2	79.2	6.0	4.0	1.75	3.9	1.25	2.8
2	LIC	S.R. 13	RT	SNYDER ROAD	20	21	45	73.4	73.4	5.6	3.7	1.75	3.6	1.25	2.6
2	LIC	S.R. 13	LT	PARANA DRIVE	40	22	75	215.6	215.6	16.2	10.8	1.75	10.5	1.25	7.5
2	LIC	S.R. 13	LT	FLAMINGO DRIVE	30	24	68	153.4	153.4	11.6	7.7	1.75	7.5	1.25	5.4
TOTALS (CARRIED TO LOCATION 2 SUB-SUMMARY)									2,035.2	153.0	102.0		99.3		71.2
3	LIC	S.R. 13	RT	ROCKY RIDGE ROAD	45	19	70	222.5	222.5	16.7	11.2	1.75	10.9	1.25	7.8
3	LIC	S.R. 13	LT	SAINT JOSEPH ROAD	30	21	64	141.7	141.7	10.7	7.1	1.75	6.9	1.25	5.0
3	LIC	S.R. 13	LT	WEAVER ROAD	30	19	66	141.7	141.7	10.7	7.1	1.75	6.9	1.25	5.0
3	LIC	S.R. 13	RT	WARTHEN STREET	20	35	78	125.6	125.6	9.5	6.3	1.75	6.2	1.25	4.4
3	LIC	S.R. 13	RT	MORELAND STREET (LOCHES RD)	25	21	55	105.6	105.6	8.0	5.3	1.75	5.2	1.25	3.7
3	LIC	S.R. 13	RT	DOG HOLLOW ROAD	30	26	83	181.7	181.7	13.7	9.1	1.75	8.9	1.25	6.4
3	LIC	S.R. 13	LT	SMOOTS ROAD	50	24	100	344.5	344.5	25.9	17.3	1.75	16.8	1.25	12.0
3	LIC	S.R. 13	RT	GINGER HILL ROAD	25	19	70	123.7	123.7	9.3	6.2	1.75	6.1	1.25	4.3
TOTALS (CARRIED TO LOCATION 3 SUB-SUMMARY)									1,387.0	104.5	69.6		67.9		48.6

EXTRA AREA DATA

LIC-13-12.06

L013_MEA_001.DGN 4-5-11

BRIDGE TREATMENT

LOCATION 1

DETAIL ① LIC-13-1396 - BUTT JOINT AT APPROACH SLABS

LOCATION 2

DETAIL ① LIC-13-1496 - BUTT JOINT AT APPROACH SLABS

LOCATION 3

DETAIL ② LIC-13-1754 - REMOVE ASPHALT AND WATERPROOFING, PLACE WATERPROOFING AND 3" ASPHALT CONCRETE

DETAIL ① LIC-13-1847 - BUTT JOINT AT APPROACH SLABS

DETAIL ① LIC-13-2050 - BUTT JOINT AT APPROACH SLABS

DETAIL ① LIC-13-2239 - BUTT JOINT AT APPROACH SLABS

BRIDGE DEDUCTIONS (MAINLINE)

LOCATION 1

LIC-13-1396 - [(145.8' X 24') + 2(20' X 24)] / 9 = 495.5 S.Y.

LOCATION 2

LIC-13-1496 - [(128.5' X 24') + 2(15' X 24)] / 9 = 422.7 S.Y.

LOCATION 3

LIC-13-1754 - [(213.0' X 24') + 2(25' X 24)] / 9 = 701.3 S.Y.

LIC-13-1847 - [(184.0' X 24') + 2(25' X 24)] / 9 = 624.0 S.Y.

LIC-13-2050 - [(125.7' X 24') + 2(25' X 24)] / 9 = 468.5 S.Y.

LIC-13-2239 - [(210.5' X 24') + 2(25' X 24)] / 9 = 694.7 S.Y.

TOTAL = 2488.5 S.Y.

BRIDGE DEDUCTIONS (SHOULDERS)

LOCATION 1

LIC-13-1396 - [(145.8' X 8') + 2(20' X 8)] / 9 = 165.2 S.Y.

LOCATION 2

LIC-13-1496 - [(128.5' X 6') + 2(15' X 6')] / 9 = 105.7 S.Y.

LOCATION 3

LIC-13-1754 - [(213.0' X 6') + 2(25' X 6')] / 9 = 175.3 S.Y.

LIC-13-1847 - [(184.0' X 6') + 2(25' X 6')] / 9 = 156.0 S.Y.

LIC-13-2050 - [(125.7' X 6') + 2(25' X 6')] / 9 = 117.1 S.Y.

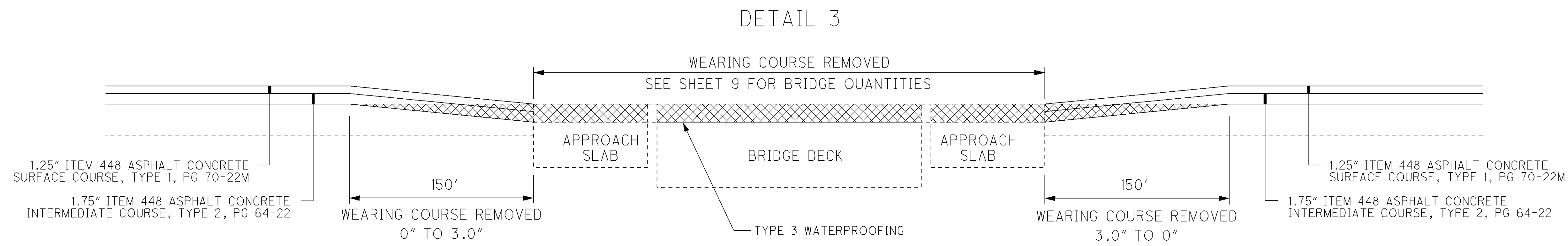
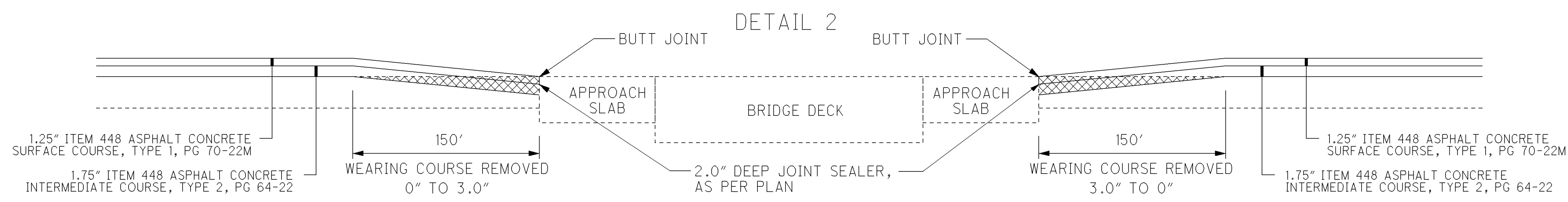
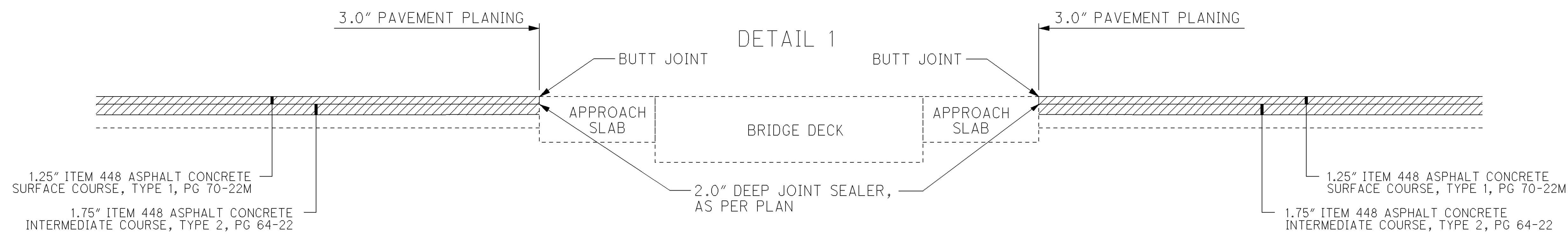
LIC-13-2239 - [(210.5' X 6') + 2(25' X 6')] / 9 = 173.7 S.Y.

TOTAL = 622.1 S.Y.

DEDUCTIONS = PAVEMENT/SHOULDER WIDTHS X (BRIDGE LENGTH + APPROACH SLABS)

BRIDGE DATA

LOCATION	COUNTY, ROUTE, BRIDGE NO.	LENGTH (BRIDGE LIMITS)	WIDTH	AREA	APPROACH SLAB LENGTH	APPROACH SLAB WIDTH	APPROACH SLAB AREA (INCLUDES BOTH APPROACH SLABS)	DETAILS (SHEET 11)	MAINLINE DEDUCTIONS (CARRIED TO SHEET 6)	SHOULDER DEDUCTIONS (CARRIED TO SHEET 8)	202		407		448 ASPHALT CONCRETE				512	516			
											WEARING COURSE REMOVED	TACK COAT FOR INTERMEDIATE COURSE @ 0.05 GAL./S.Y.	TACK COAT @ 0.075 GAL./S.Y.	S E M E N T	INTERMEDIATE COURSE, TYPE 2, PG 64-22	S E M E N T	SURFACE COURSE, TYPE 1, PG 70-22M	TYPE 3 WATERPROOFING	2" DEEP JOINT SEALER, AS PER PLAN				
																				SQ.YD.	GAL.	GAL.	INCHES
1	LIC-13-1396	145.8	40	648.0	20	40.0	177.8	1	495.5	165.2										64.0			
TOTALS (CARRIED TO LOCATION 1 SUB-SUMMARY)																					64.0		
2	LIC-13-1496	128.5	40	571.2	15	40.0	133.4	2	422.7	105.7	1,000.0										60.0		
TOTALS (CARRIED TO LOCATION 2 SUB-SUMMARY)											1,000.0												60.0
3	LIC-13-1754	213	40	946.7	25	40.0	222.3	3	701.3	175.3	2,169.0	58.5	87.7	1.75	56.8	1.25	40.6		946.7				
3	LIC-13-1847	184	44	899.6	25	44.0	244.5	2	624.0	156.0	1,000.0										60.0		
3	LIC-13-2050	125.7	44	614.6	25	44.0	244.5	2	468.5	117.1	1,000.0										60.0		
3	LIC-13-2239	210.5	36	842.0	25	36.0	200.0	2	694.7	173.7	1,000.0										60.0		
SUB-TOTALS									2,488.5	622.1													
TOTALS (CARRIED TO LOCATION 3 SUB-SUMMARY)											5,169.0	58.5	87.7		56.8		40.6		946.7		180.0		



ITEM 817 EDGE LINE										
LOCATION	COUNTY	ROUTE	S.L.M.		TOTAL LENGTH (MILES)	INFORMATION ONLY			TOTAL EDGE LINE MILES	REMARKS
			FROM	TO		WHITE EDGE LINE QUANTITIES				
						TOTAL MILES	HIGHWAY MILES	RAMP MILES		
1	LIC	S.R. 13	12.06	13.98	1.92	3.84	3.84		3.84	
TOTAL									3.84	
TOTALS URBAN PAVING 68% (CARRIED TO LOCATION 1 SUB-SUMMARY)									2.61	
TOTALS GENERAL PAVING 32% (CARRIED WITH LOCATION 2 BELOW)									1.23	
2	LIC	S.R. 13	13.98	16.53	2.55	5.10	5.10		5.10	
TOTAL (CARRIED TO LOCATION 2 SUB-SUMMARY)									6.33	
3	LIC	S.R. 13	16.53	22.53	6.00	12.00	12.00		12.00	
TOTAL (CARRIED TO LOCATION 3 SUB-SUMMARY)									12.00	

ITEM 817 CENTER LINE										
LOCATION	COUNTY	ROUTE	S.L.M.		TOTAL LENGTH (MILES)	INFORMATION ONLY		TOTAL CENTER LINE MILES	REMARKS	
			FROM	TO		CENTER LINE QUANTITIES				
						TOTAL MILES	EQUIVALENT SOLID LINE			
1	LIC	S.R. 13	12.06	13.98	1.92	1.92	2.626		2.08	0.16 MILE ADDED FOR TURN LANE
TOTAL									2.08	
TOTALS URBAN PAVING 68% (CARRIED TO LOCATION 1 SUB-SUMMARY)									1.41	
TOTALS GENERAL PAVING 32% (CARRIED WITH LOCATION 2 BELOW)									0.67	
2	LIC	S.R. 13	13.98	16.53	2.55	2.55	4.077		2.61	0.06 MILE ADDED FOR TURN LANE
TOTAL (CARRIED TO LOCATION 2 SUB-SUMMARY)									3.28	
3	LIC	S.R. 13	16.53	22.53	6.00	6.00	9.979		6.27	0.27 MILE ADDED FOR TURN LANE
TOTAL (CARRIED TO LOCATION 3 SUB-SUMMARY)									6.27	

644 THERMOPLASTIC AUXILIARY MARKING

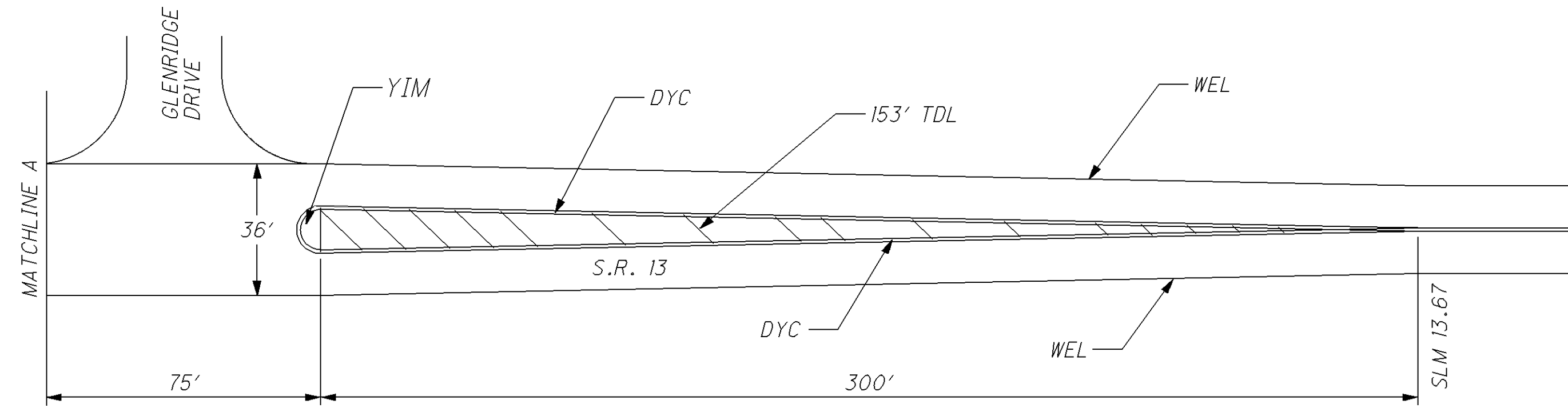
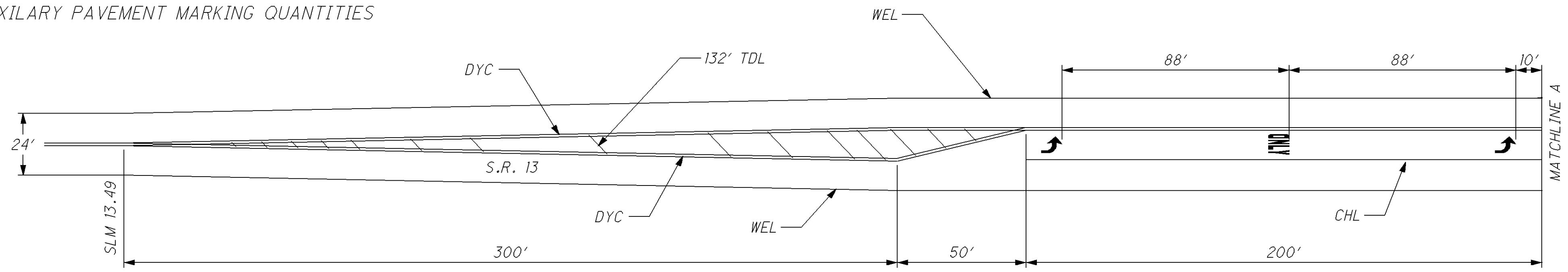
LOCATION	COUNTY	ROUTE	DESCRIPTION	SIDE	SLM	TRANVERSE/DIAGONAL LINES (24")		STOP LINE (24")	12" CROSSWALK LINE	8" CHANNELIZING LINE	WORD ON PAVEMENT		SCHOOL SYMBOL MARKING		LANE ARROWS					ISLAND MARKING	RAILROAD MARKING SYMBOL	REMARKS		
						WHITE	YELLOW				ONLY				COMBINATION		TURN							
											72"	96"			72"	96"	LT./TH.	RT./TH.	LT.				RT.	THRU
						FT.	FT.				FT.	FT.	FT.	FT.	FT.	FT.	FT.	FT.	FT.				FT.	FT.
1	LC	S.R. 13	GLADYS AVE	LT				12															PLACE 21' FROM SR 13 CENTER LINE	
1	LC	S.R. 13	GREENFIELD AVE	LT				12															PLACE 25' FROM SR 13 CENTER LINE	
1	LC	S.R. 13	MYRTLE AVE	LT				12															PLACE 24' FROM SR 13 CENTER LINE	
1	LC	S.R. 13	HILLVIEW CIRCLE	LT				12															PLACE 24' FROM SR 13 CENTER LINE	
1	LC	S.R. 13	WIDENING AT PARK DRIVE							100		1						1					SEE DETAIL SHEET 14	
1	LC	S.R. 13	ON S.R. 13 BEFORE 21st ST	CL				24		165		2					2		2				SEE DETAIL SHEET 14	
1	LC	S.R. 13	21ST STREET	LT																				
1	LC	S.R. 13	ON S.R. 13 AFTER 21st ST	CL				23		100		2							1	1				SEE DETAIL SHEET 14
1	LC	S.R. 13	DANIELLE DRIVE	LT				20																PLACE 24' FROM SR 13 CENTER LINE
1	LC	S.R. 13	WIDENING AT GREGORY DRIVE							95		2					1		1					SEE DETAIL SHEET 14
1	LC	S.R. 13	GREGORY DRIVE	LT				26																PLACE 23' FROM SR 13 CENTER LINE
1	LC	S.R. 13	NORTH VERNON AVE	LT				16																PLACE 25' FROM SR 13 CENTER LINE
1	LC	S.R. 13	GLENRIDGE DRIVE	LT			285			200		1					2				57			SEE DETAIL SHEET 15
1	LC	S.R. 13	ASBURY AVE	LT						150		1							2					SEE DETAIL SHEET 15
TOTALS (CARRIED TO LOCATION 1 SUB-SUMMARY)							285	157		810		9					5	4	4	57				
TOTALS URBAN PAVING 68% (CARRIED TO LOCATION 1 SUB-SUMMARY)							194	107		551		6						3	3	3	39			
TOTALS GENERAL PAVING 32% (CARRIED WITH LOCATION 2 BELOW)							91	50		259		3					2	1	1	18				
2	LC	S.R. 13	ON S.R. 13 BEFORE S.R. 657	CL			106			75		1					1							SEE DETAIL SHEET 15
2	LC	S.R. 13	S.R. 657	LT				26																PLACE 20' FROM SR 13 CENTER LINE
2	LC	S.R. 13	ON S.R. 13 AFTER S.R. 657	CL			133														57			SEE DETAIL SHEET 15
2	LC	S.R. 13	CHESTNUT HILLS ROAD	LT				18																PLACE 20' FROM SR 13 CENTER LINE
2	LC	S.R. 13	WILLIAMS ROAD	RT				12																PLACE 22' FROM SR 13 CENTER LINE
2	LC	S.R. 13	SNYDER ROAD	RT				13																PLACE 20' FROM SR 13 CENTER LINE
2	LC	S.R. 13	PARANA DRIVE	LT				25																PLACE 24' FROM SR 13 CENTER LINE
2	LC	S.R. 13	FLAMINGO DRIVE	LT				21																PLACE 21' FROM SR 13 CENTER LINE
TOTALS (CARRIED TO LOCATION 2 SUB-SUMMARY)							330	165		334		4					3	1	1	75				
3	LC	S.R. 13	ON S.R. 13										1											PLACE AS DIRECTED
3	LC	S.R. 13	ROCKY RIDGE ROAD	RT				19																PLACE 24' FROM SR 13 CENTER LINE
3	LC	S.R. 13	ON S.R. 13										1											PLACE AS DIRECTED
3	LC	S.R. 13	SAINT JOSEPH ROAD	LT				17																PLACE 23' FROM SR 13 CENTER LINE
3	LC	S.R. 13	WEAVER ROAD	LT				19																PLACE 24' FROM SR 13 CENTER LINE
3	LC	S.R. 13	WARTHEN STREET	RT				22																PLACE 21' FROM SR 13 CENTER LINE
3	LC	S.R. 13	MORELAND STREET (LOCHES RD)	RT				18																PLACE 24' FROM SR 13 CENTER LINE
3	LC	S.R. 13	DOG HOLLOW ROAD	RT				28																PLACE 21' FROM SR 13 CENTER LINE
3	LC	S.R. 13	ON S.R. 13 SLM 18.77-SLM 19.09	CL			475			470		2					2	2			57			SEE DETAIL SHEET 16
3	LC	S.R. 13	SMOOTS ROAD	LT				24																PLACE 22' FROM SR 13 CENTER LINE
3	LC	S.R. 13	GINGER HILL ROAD	RT				17																PLACE 24' FROM SR 13 CENTER LINE
TOTALS (CARRIED TO LOCATION 3 SUB-SUMMARY)							475	164		470		2	2				2	2		57				

CALCULATED
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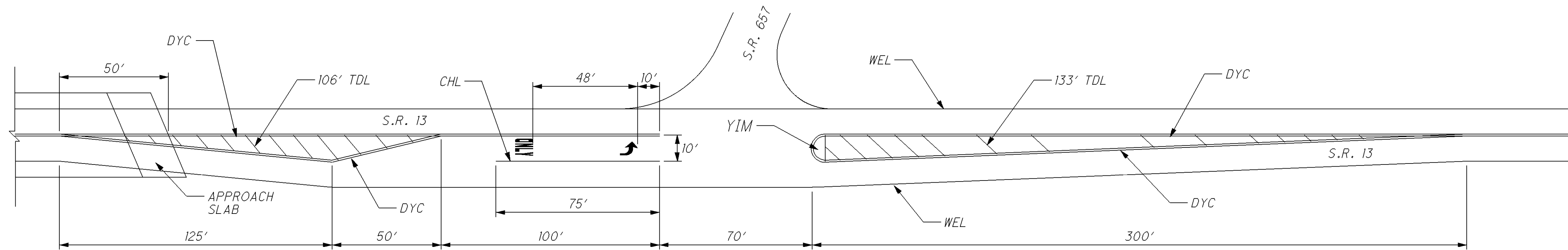
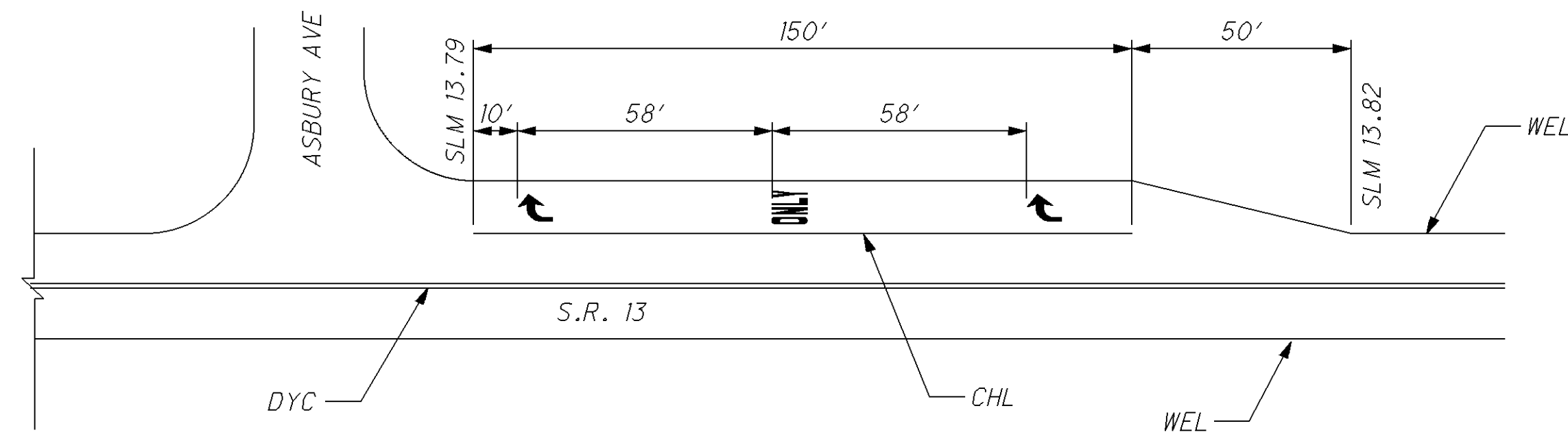
PAVEMENT MARKING DATA

LIC-13-12.06

SEE SHEET 12 FOR EDGE LINE AND CENTER LINE PAVEMENT MARKING QUANTITIES
 SEE SHEET 13 FOR AUXILIARY PAVEMENT MARKING QUANTITIES



SL = STOP LINE
 WEL=WHITE EDGE LINE
 DYC=DOUBLE YELLOW CENTER LINE
 CHL=CHANNELIZING LINE
 TDL=TRANSVERSE/DIAGONAL LINE

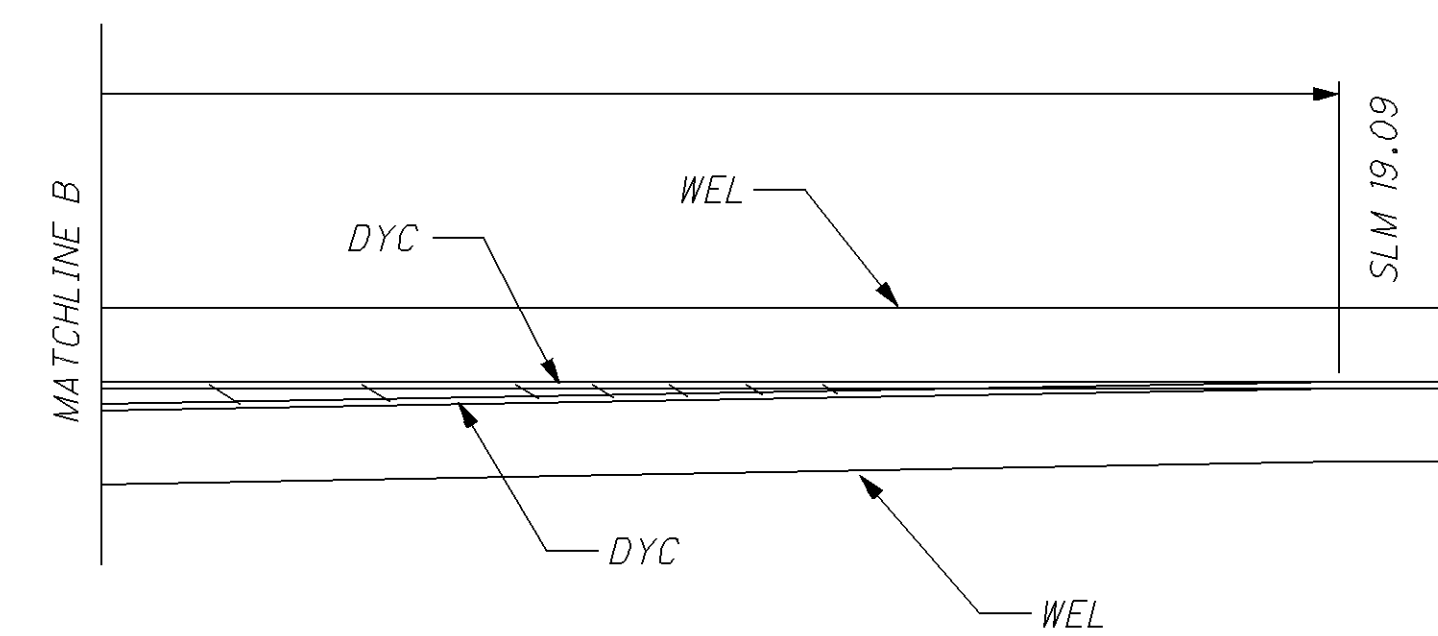
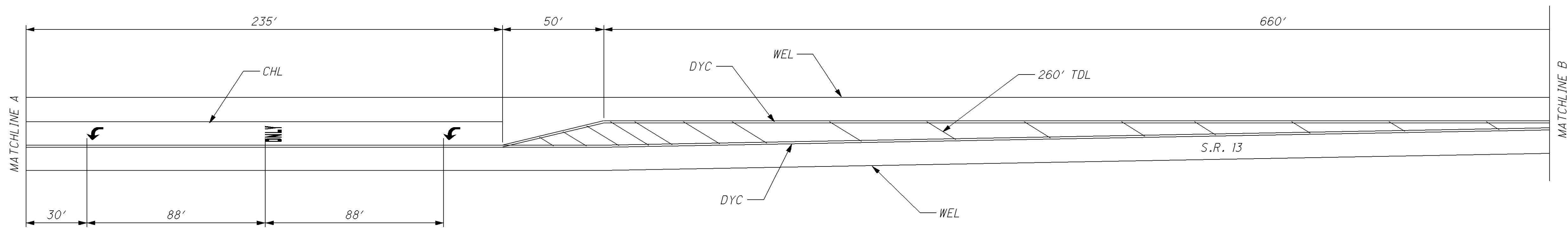
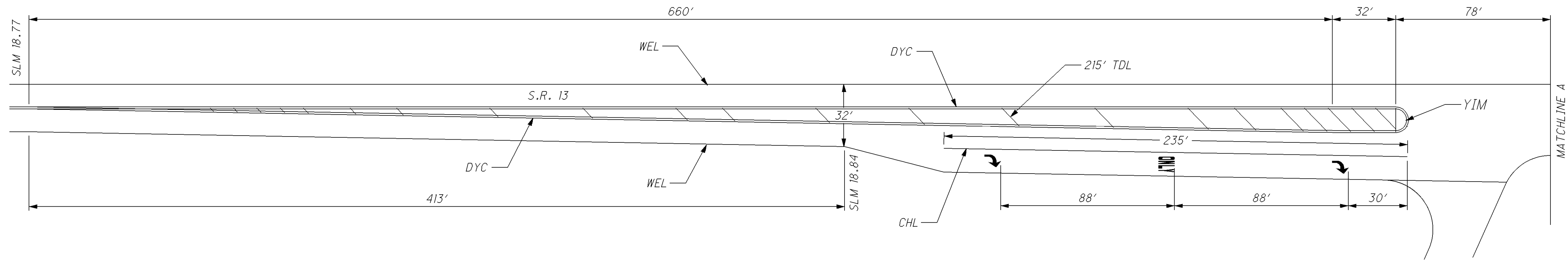


CALCULATED
 LIME
 CHECKED
 DNM

PAVEMENT MARKING DETAILS

LIC-13-12.06

SEE SHEET 12 FOR EDGE LINE AND CENTER LINE PAVEMENT MARKING QUANTITIES
 SEE SHEET 13 FOR AUXILARY PAVEMENT MARKING QUANTITIES



SL = STOP LINE
 WEL=WHITE EDGE LINE
 DYC=DOUBLE YELLOW CENTER LINE
 CHL=CHANNELIZING LINE
 TDL=TRANSVERSE/DIAGONAL LINE

L013_PMD_001.dgn 4/18/2011

CALCULATED
LIME
CHECKED
DNM

PAVEMENT MARKING DETAILS

LIC-13-12.06

DETAIL	SEE STD. DWG. TC-65.11
1	TAPERED ACCELERATION LANE
2	DECELERATION LANE
3	MULTILANE DIVIDED/ CONTROLLED ACCESS

DETAIL	SEE STD. DWG. TC-65.11
4	4 LANE DIVIDED TO 2 LANE TRANSITION
5	4 LANE UNDIVIDED TO 2 LANE TRANSITION
6	ONE LANE BRIDGE
7	STOP APPROACH
8	THRU APPROACH
9	TWO WAY LEFT TURN LANE

DETAIL	SEE STD. DWG. TC-65.11
10	APPROACH W/LT. TURN LANE
11	HORIZONTAL CURVE 40'
12	HORIZONTAL CURVE ALT.
GAP	CENTERLINE AT 80' TYP.
REM	SEE REMARKS COLUMN

CALCULATED
LME
CHECKED
DNM

ITEM 621 RPM SUB-SUMMARY

LOCATION	COUNTY	ROUTE	BEGIN LOG POINT SLM	END LOG POINT SLM	LENGTH		DETAIL	621	PRISMATIC RETRO-REFLECTOR COLORS					REMARKS	
								RPM	INFORMATION ONLY						
									EACH	ONE-WAY		TWO-WAY			
				MILES	LIN.FT.		WHITE	YELLOW		YELLOW / YELLOW	WHITE / RED	YELLOW / RED			
1	LIC	S.R. 13	12.06	12.19	0.13	686	GAP	9			9				
1	LIC	S.R. 13	12.19	12.21	0.02	106	11	3			3			PC 12.19 PT 12.21 L=106' DEG=5	
1	LIC	S.R. 13	12.21	12.32	0.11	581	GAP	8			8				
1	LIC	S.R. 13	12.32	12.42	0.10	528	REM	10			7	3		RIGHT TURN AT T.J. EVANS PARK	
1	LIC	S.R. 13	12.42	12.74	0.32	1,690	GAP	22			22				
1	LIC	S.R. 13	12.74	12.88	0.14	739	REM	18			10	8		TURN LANES AT 21ST STREET	
1	LIC	S.R. 13	12.88	13.25	0.37	1,954	GAP	25			25				
1	LIC	S.R. 13	13.25	13.34	0.09	475	REM	9			6	3		LEFT TURN AT GREGORY DRIVE	
1	LIC	S.R. 13	13.34	13.49	0.15	792	GAP	10			10				
1	LIC	S.R. 13	13.49	13.67	0.18	950	7	30			24	6		LEFT TURN AT GLENRIDGE DRIVE	
1	LIC	S.R. 13	13.67	13.79	0.12	634	GAP	8			8				
1	LIC	S.R. 13	13.79	13.82	0.03	158	REM	7			2	5		RIGHT TURN AT ASBURY AVENUE	
1	LIC	S.R. 13	13.82	13.98	0.16	845	GAP	11			11				
SUB-TOTALS												145	25		
TOTALS								170							
TOTALS URBAN PAVING 68% (CARRIED TO LOCATION 1 SUB-SUMMARY)								116							
TOTALS GENERAL PAVING 32% (CARRIED WITH LOCATION 2 BELOW)								54							
2	LIC	S.R. 13	13.98	14.11	0.13	686	7	23			20	3		LEFT TURN AT S.R. 657	
2	LIC	S.R. 13	14.11	14.18	0.07	370	GAP	5			5				
2	LIC	S.R. 13	14.18	14.41	0.23	1,214	12	37			37			PC 14.27 PT 14.32 L=264' DEG=11	
2	LIC	S.R. 13	14.41	14.54	0.13	686	11	18			18			PC 14.42 PT 14.54 L=634' DEG=6	
2	LIC	S.R. 13	14.54	14.64	0.10	528	GAP	7			7				
2	LIC	S.R. 13	14.64	14.65	0.01	53	11	2			2			PC 14.64 PT 14.65 L=106' DEG=9	
2	LIC	S.R. 13	14.65	16.46	1.81	9,557	GAP	120			120				
2	LIC	S.R. 13	16.46	16.50	0.04	211	11	6			6			PC 16.46 PT 16.50 L=211' DEG=7	
2	LIC	S.R. 13	16.50	16.53	0.03	158	GAP	2			2				
SUB-TOTALS												217	3		
TOTAL (CARRIED TO LOCATION 2 SUB-SUMMARY)								274							

RAISED PAVEMENT MARKER DATA

LIC-13-12.06

DETAIL	SEE STD. DWG. TC-65.II
1	TAPERED ACCELERATION LANE
2	DECELERATION LANE
3	MULTILANE DIVIDED/ CONTROLLED ACCESS

DETAIL	SEE STD. DWG. TC-65.II
4	4 LANE DIVIDED TO 2 LANE TRANSITION
5	4 LANE UNDIVIDED TO 2 LANE TRANSITION
6	ONE LANE BRIDGE
7	STOP APPROACH
8	THRU APPROACH
9	TWO WAY LEFT TURN LANE

DETAIL	SEE STD. DWG. TC-65.II
10	APPROACH W/LT. TURN LANE
11	HORIZONTAL CURVE 40'
12	HORIZONTAL CURVE ALT.
GAP	CENTERLINE AT 80' TYP.
REM	SEE REMARKS COLUMN

CALCULATED
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ITEM 621 RPM SUB-SUMMARY

LOCATION	COUNTY	ROUTE	BEGIN LOG POINT SLM	END LOG POINT SLM	LENGTH		DETAIL	621	PRISMATIC RETRO-REFLECTOR COLORS					REMARKS
								RPM	INFORMATION ONLY					
					EACH	ONE-WAY			TWO-WAY					
					MILES	LIN.FT.		WHITE	YELLOW	YELLOW / YELLOW	WHITE / RED	YELLOW / RED		
3	LIC	S.R. 13	16.53	17.93	1.40	7,392	GAP	93			93			
3	LIC	S.R. 13	17.93	17.98	0.05	264	11	7			7			PC 17.93 PT 17.98 L=264' DEG=9
3	LIC	S.R. 13	17.98	18.00	0.02	106	GAP	2			2			
3	LIC	S.R. 13	18.00	18.05	0.05	264	11	7			7			PC 18.00 PT 18.05 L=264' DEG=9
3	LIC	S.R. 13	18.05	18.39	0.34	1,795	GAP	23			23			
3	LIC	S.R. 13	18.39	18.46	0.07	370	11	10			10			PC 18.39 PT 18.46 L=370' DEG=8
3	LIC	S.R. 13	18.46	18.90	0.44	2,323	GAP/7	62			48	14		TURN LANES SLM 18.77 TO SLM 19.09
3	LIC	S.R. 13	18.90	18.95	0.05	264	11	7			7			PC 18.90 PT 18.95 L=264' DEG=7
3	LIC	S.R. 13	18.95	19.62	0.67	3,538	GAP	45			45			
3	LIC	S.R. 13	19.62	19.67	0.05	264	11	7			7			PC 19.62 PT 19.67 L=264' DEG=7
3	LIC	S.R. 13	19.67	19.86	0.19	1,003	12	33			33			PC 19.71 PT 19.77 L=317' DEG=10
3	LIC	S.R. 13	19.86	20.23	0.37	1,954	GAP	25			25			
3	LIC	S.R. 13	20.23	20.27	0.04	211	11	6			6			PC 20.23 PT 20.27 L=211' DEG=7
3	LIC	S.R. 13	20.27	20.55	0.28	1,478	GAP	19			19			
3	LIC	S.R. 13	20.55	20.58	0.03	158	11	4			4			PC 20.55 PT 20.58 L=158' DEG=8
3	LIC	S.R. 13	20.58	20.62	0.04	211	GAP	3			3			
3	LIC	S.R. 13	20.62	20.66	0.04	211	11	6			6			PC 20.62 PT 20.66 L=211' DEG=7
3	LIC	S.R. 13	20.66	20.76	0.10	528	GAP	7			7			
3	LIC	S.R. 13	20.76	20.88	0.12	634	11	16			16			PC 20.76 PT 20.88 L=634' DEG=6
3	LIC	S.R. 13	20.88	21.04	0.16	845	GAP	11			11			
3	LIC	S.R. 13	21.04	21.08	0.04	211	11	6			6			PC 21.04 PT 21.08 L=211' DEG=9
3	LIC	S.R. 13	21.08	21.48	0.40	2,112	GAP	27			27			
3	LIC	S.R. 13	21.48	21.68	0.20	1,056	12	29			29			PC 21.57 PT 21.59 L=106' DEG=18
3	LIC	S.R. 13	21.68	21.88	0.20	1,056	12	33			33			PC 21.74 PT 21.79 L=264' DEG=10
3	LIC	S.R. 13	21.88	22.12	0.24	1,267	12	38			38			PC 21.98 PT 22.03 L=264' DEG=10
3	LIC	S.R. 13	22.12	22.39	0.27	1,426	GAP	18			18			END AT UTICA CORP.
SUB-TOTALS											530	14		
TOTAL (CARRIED TO LOCATION 3 SUB-SUMMARY)								544						

RAISED PAVEMENT MARKER DATA

LIC - 13 - 12.06

L013_TRM_002.DGN 4-5-11

LOCATION 1											ITEM	ITEM EXT.	LOCATION 1 TOTAL	UNIT	DESCRIPTION
Sht. 2	Sht. 3	Sht. 4	Sht. 5	Sht. 6	Sht. 8	Sht. 9	Sht. 10	Sht. 12	Sht. 13	Sht. 17					
						1,585					202	23500	1,585	SQ YD	WEARING COURSE REMOVED
	1										209	60500	1	MILE	LINEAR GRADING
	297										251	98000	297	CU YD	PARTIAL DEPTH REPAIR, MISC.: PAVED SHOULDER REPAIR
	22										253	02000	22	CU YD	PAVEMENT REPAIR
				2,128	622						254	01000	2,750	SQ YD	PAVEMENT PLANING, ASPHALT CONCRETE
				1,512	452	120					407	10000	2,084	GALLON	TACK COAT
				1,008	301	80					407	14000	1,389	GALLON	TACK COAT FOR INTERMEDIATE COURSE
1,204											408	10001	1,204	GALLON	PRIME COAT, AS PER PLAN
32				980	293	78					448	46050	1,383	CU YD	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG64-22
22				700	209						448	46904	931	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M
						56					448	47020	56	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22
							64				516	31011	64	FT	2" DEEP JOINT SEALER, AS PER PLAN
		10									604	34500	10	EACH	MANHOLE ADJUSTED TO GRADE
			20								614	11110	20	HOURL	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE
13											614	12460	13	EACH	WORK ZONE MARKING SIGN
	5.0										614	13000	5.0	CU YD	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC
			20								614	18401	20	DAY	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN
				1.28							614	21400	1.28	MILE	WORK ZONE CENTER LINE, CLASS II
				1.28							614	21550	1.28	MILE	WORK ZONE CENTER LINE, CLASS III, 642 PAINT
						147					617	10101	147	CU YD	COMPACTED AGGREGATE, AS PER PLAN
										116	621	00100	116	EACH	RPM
181											621	54000	181	EACH	RAISED PAVEMENT MARKER REMOVED
		3									632	26501	3	EACH	DETECTOR LOOP, AS PER PLAN
		3									638	10800	3	EACH	VALVE BOX ADJUSTED TO GRADE
									551		644	00400	551	FT	CHANNELIZING LINE
									107		644	00500	107	FT	STOP LINE
									194		644	00700	194	FT	TRANSVERSE/DIAGONAL LINE
									39		644	00900	39	SQ FT	ISLAND MARKING
									9		644	01300	9	EACH	LANE ARROW
									6		644	01410	6	EACH	WORD ON PAVEMENT, 96"
		3,039									690	12050	3,039	SQ YD	SPECIAL - REINFORCED MESH FOR TRANSVERSE AND/OR LONGITUDINAL JOINTS AND CRACKS
								2.61			817	00100	2.61	MILE	EDGE LINE
								1.41			817	00300	1.41	MILE	CENTER LINE

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LOCATION 1 SUB - SUMMARY

LIC-13-12.06

L013_LSS.001.DGN 4-5-11

LOCATION 2											ITEM	ITEM EXT.	LOCATION 2 TOTAL	UNIT	DESCRIPTION
Sht. 2	Sht. 3	Sht. 4	Sht. 5	Sht. 6	Sht. 8	Sht. 9	Sht. 10	Sht. 12	Sht. 13	Sht. 17					
						2,036	1,000				202	23500	3,036	SQ YD	WEARING COURSE REMOVED
	1										209	60500	1	MILE	LINEAR GRADING
	1,157										251	98000	1,157	CU YD	PARTIAL DEPTH REPAIR, MISC.: PAVED SHOULDER REPAIR
	72										253	02000	72	CU YD	PAVEMENT REPAIR
				3,831	1,044						254	01000	4,875	SQ YD	PAVEMENT PLANING, ASPHALT CONCRETE
				3,416	915	153					407	10000	4,484	GALLON	TACK COAT
				2,278	610	102					407	14000	2,990	GALLON	TACK COAT FOR INTERMEDIATE COURSE
2,813											408	10001	2,813	GALLON	PRIME COAT, AS PER PLAN
34				2,215	593	100					448	46050	2,942	CU YD	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG64-22
25				1,582	424						448	46904	2,031	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M
						72					448	47020	72	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22
							60				516	31011	60	FT	2" DEEP JOINT SEALER, AS PER PLAN
8			30								614	11110	30	hour	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE
	3.0										614	12460	8	EACH	WORK ZONE MARKING SIGN
			40								614	13000	3.0	CU YD	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC
											614	18401	40	DAY	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN
				3.11							614	21400	3.11	MILE	WORK ZONE CENTER LINE, CLASS II
				3.11							614	21550	3.11	MILE	WORK ZONE CENTER LINE, CLASS III, 642 PAINT
					342						617	10101	342	CU YD	COMPACTED AGGREGATE, AS PER PLAN
										274	621	00100	274	EACH	RPM
212											621	54000	212	EACH	RAISED PAVEMENT MARKER REMOVED
									334		644	00400	334	FT	CHANNELIZING LINE
									165		644	00500	165	FT	STOP LINE
									330		644	00700	330	FT	TRANSVERSE/DIAGONAL LINE
									75		644	00900	75	SQ FT	ISLAND MARKING
									5		644	01300	5	EACH	LANE ARROW
									4		644	01410	4	EACH	WORD ON PAVEMENT, 96"
		11,845									690	12050	11,845	SQ YD	SPECIAL - REINFORCED MESH FOR TRANSVERSE AND/OR LONGITUDINAL JOINTS AND CRACKS
								6.33			817	00100	6.33	MILE	EDGE LINE
								3.28			817	00300	3.28	MILE	CENTER LINE

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LOCATION 2 SUB-SUMMARY

LIC-13-12.06

LOCATION 3											ITEM	ITEM EXT.	LOCATION 3 TOTAL	UNIT	DESCRIPTION
Sht. 2	Sht. 3	Sht. 4	Sht. 5	Sht. 6	Sht. 8	Sht. 9	Sht. 10	Sht. 12	Sht. 13	Sht. 18					
						1,387	5,169				202	23500	6,556	SQ YD	WEARING COURSE REMOVED
	3										209	60500	3	MILE	LINEAR GRADING
	1,713										251	98000	1,713	CU YD	PARTIAL DEPTH REPAIR, MISC.: PAVED SHOULDER REPAIR
	235										253	02000	235	CU YD	PAVEMENT REPAIR
				1,277	319						254	01000	1,596	SQ YD	PAVEMENT PLANING, ASPHALT CONCRETE
				6,288	1,555	105	59				407	10000	8,007	GALLON	TACK COAT
				4,192	1,037	70	88				407	14000	5,387	GALLON	TACK COAT FOR INTERMEDIATE COURSE
5,486											408	10001	5,486	GALLON	PRIME COAT, AS PER PLAN
81				4,075	1,008	68	57				448	46050	5,289	CU YD	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG64-22
57				2,911	720		41				448	46904	3,729	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M
						49					448	47020	49	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22
							947				512	33010	947	SQ YD	TYPE 3 WATERPROOFING
							180				516	31011	180	FT	2" DEEP JOINT SEALER, AS PER PLAN
			50								614	11110	50	HOURL	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE
15											614	12460	15	EACH	WORK ZONE MARKING SIGN
	10.0										614	13000	10.0	CU YD	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC
			90								614	18401	90	DAY	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN
				5.96							614	21400	5.96	MILE	WORK ZONE CENTER LINE, CLASS II
				5.96							614	21550	5.96	MILE	WORK ZONE CENTER LINE, CLASS III, 642 PAINT
					667						617	10101	667	CU YD	COMPACTED AGGREGATE, AS PER PLAN
										544	621	00100	544	EACH	RPM
495											621	54000	495	EACH	RAISED PAVEMENT MARKER REMOVED
								470			644	00400	470	FT	CHANNELIZING LINE
								164			644	00500	164	FT	STOP LINE
								475			644	00700	475	FT	TRANSVERSE/DIAGONAL LINE
								57			644	00900	57	SQ FT	ISLAND MARKING
								2			644	01110	2	EACH	SCHOOL SYMBOL MARKING, 96"
								4			644	01300	4	EACH	LANE ARROW
								2			644	01410	2	EACH	WORD ON PAVEMENT, 96"
		20,773									690	12050	20,773	SQ YD	SPECIAL - REINFORCED MESH FOR TRANSVERSE AND/OR LONGITUDINAL JOINTS AND CRACKS
								12.00			817	00100	12.00	MILE	EDGE LINE
								6.27			817	00300	6.27	MILE	CENTER LINE

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LOCATION 3 SUB-SUMMARY

LIC-13-12.06

L013_LSS.003.DGN 4-5-11

LOCATION 1 (80% FED / 20% LOCAL) URBAN	LOCATION 2 (80% FED / 20% STATE) URBAN	LOCATION 3 (80% FED / 20% STATE) RURAL			ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET
1,585	3,036	6,556			202	23500	11,177	SQ YD	WEARING COURSE REMOVED	
1	1	3			209	60500	5	MILE	LINEAR GRADING	
297	1,157	1,713			251	98000	3,167	CU YD	PARTIAL DEPTH REPAIR, MISC.: PAVED SHOULDER REPAIR	
22	72	235			253	02000	329	CU YD	PAVEMENT REPAIR	
2,750	4,875	1,596			254	01000	9,221	SQ YD	PAVEMENT PLANING, ASPHALT CONCRETE	
2,084	4,484	8,007			407	10000	14,575	GALLON	TACK COAT	
1,389	2,990	5,387			407	14000	9,766	GALLON	TACK COAT FOR INTERMEDIATE COURSE	
1,204	2,813	5,486			408	10001	9,503	GALLON	PRIME COAT, AS PER PLAN	2
1,383	2,942	5,289			448	46050	9,614	CU YD	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG64-22	
931	2,031	3,729			448	46904	6,691	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M	
56	72	49			448	47020	177	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22	
		947			512	33010	947	SQ YD	TYPE 3 WATERPROOFING	
64	60	180			516	31011	304	FT	2" DEEP JOINT SEALER, AS PER PLAN	2
10					604	34500	10	EACH	MANHOLE ADJUSTED TO GRADE	
20	30	50			614	11110	100	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
13	8	15			614	12460	36	EACH	WORK ZONE MARKING SIGN	
5	3	10			614	13000	18	CU YD	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC	
20	40	90			614	18401	150	DAY	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	5
1.28	3.11	5.96			614	21400	10.35	MILE	WORK ZONE CENTER LINE, CLASS II	
1.28	3.11	5.96			614	21550	10.35	MILE	WORK ZONE CENTER LINE, CLASS III, 642 PAINT	
147	342	667			617	10101	1,156	CU YD	COMPACTED AGGREGATE, AS PER PLAN	2
116	274	544			621	00100	934	EACH	RPM	
181	212	495			621	54000	888	EACH	RAISED PAVEMENT MARKER REMOVED	
3					632	26501	3	EACH	DETECTOR LOOP, AS PER PLAN	4
3					638	10800	3	EACH	VALVE BOX ADJUSTED TO GRADE	
551	334	470			644	00400	1,355	FT	CHANNELIZING LINE	
107	165	164			644	00500	436	FT	STOP LINE	
194	330	475			644	00700	999	FT	TRANSVERSE/DIAGONAL LINE	
39	75	57			644	00900	171	SQ FT	ISLAND MARKING	
		2			644	01110	2	EACH	SCHOOL SYMBOL MARKING, 96"	
9	5	4			644	01300	18	EACH	LANE ARROW	
6	4	2			644	01410	12	EACH	WORD ON PAVEMENT, 96"	
3,039	11,845	20,773			690	12050	35,657	SQ YD	SPECIAL - REINFORCED MESH FOR TRANSVERSE AND/OR LONGITUDINAL JOINTS AND CRACKS	
2.61	6.33	12.00			817	00100	20.94	MILE	EDGE LINE	
1.41	3.28	6.27			817	00300	10.96	MILE	CENTER LINE	
					103	05000	LUMP		PREMIUM FOR CONTRACT PERFORMANCE BOND AND FOR PAYMENT BOND	
					614	11000	LUMP		MAINTAINING TRAFFIC	
					619	16000	3	MONTH	FIELD OFFICE, TYPE A	
					623	10000	LUMP		CONSTRUCTION LAYOUT STAKES	
					624	10000	LUMP		MOBILIZATION	

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GENERAL SUMMARY

LIC-13-12.06