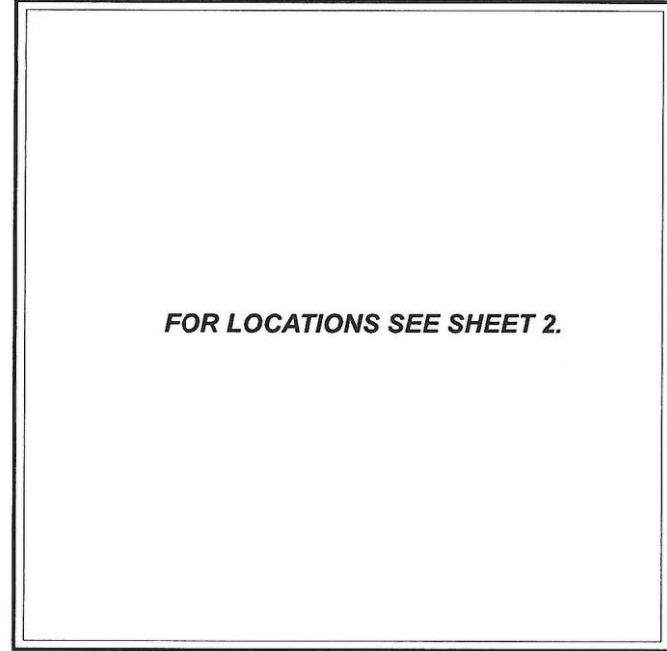


LOG - Pave-FY2022
 220015 PID - 110591
 Dist 7 1/13/2022

STATE OF OHIO
 DEPARTMENT OF TRANSPORTATION

LOG-PAVE-FY22

VILLAGE OF DEGRAFF
 VILLAGE OF HUNTSVILLE



LOCATION MAP

FEDERAL PROJECT NUMBER

E191 (386)

RAILROAD INVOLVEMENT

CSX

PROJECT DESCRIPTION

2" OVERLAY ON QUALIFYING SECTIONS WITHOUT CURB AND GUTTER,
 2" MILL AND FILL ON QUALIFYING SECTIONS WITHOUT CURB AND GUTTER,
 2" MILL AND FILL ON SECTIONS WITH CURB AND GUTTER.
 UPGRADE CURB RAMPS WHERE NECESSARY.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: N/A ACRES
 ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A ACRES
 NOTICE OF INTENT EARTH DISTURBED AREA: N/A ACRES
 "MAINTENANCE PROJECT"

2019 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

INDEX OF SHEETS:

TITLE SHEET	1
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TYPICAL SHEET	3
GENERAL NOTES	4,5
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PAVEMENT SUBSUMMARY	10
DRIVE DETAILS	11,12
TRAFFIC CONTROL SUBSUMMARY	13,14
STRUCTURE DETAILS	15

Contract Proposal available @
 www.contracts.dot.state.oh.us

TITLE SHEET

ADA DESIGN WAIVERS REQUIRED

UNDERGROUND UTILITIES
 Contact Two Working Days
 Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
 (Non members must be called directly)

PLAN PREPARED BY:
 ODOT DISTRICT 7 - ENGINEERING
 1001 ST. MARYS AVE.
 SIDNEY, OHIO 45365

ENGINEER'S SEAL:

SIGNED: *Chris Weiss*
 DATE: 9/28/2021

STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
BP-3.1	1/17/20	TC-41.20	10/18/13	800	10/15/21
BP-3.2	1/18/19	TC-42.20	10/18/13		
BP-7.1	7/17/20	TC-52.10	10/18/13	832	10/19/18
		TC-52.20	1/15/21		
DM-4.3	1/15/16	TC-65.10	1/17/14		
DM-4.4	1/15/16	TC-65.11	7/21/17		
		TC-71.10	7/16/21		
		TC-74.10	7/16/21		
MT-97.10	4/19/19				
MT-97.12	1/20/17				
MT-99.20	4/19/19				
MT-101.90	7/17/20				
MT-105.10	1/17/20				

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THESE IMPROVEMENTS WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY, AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED *Randall Chwalby* PE, PS.
 DATE 9-28-2021 DISTRICT DEPUTY DIRECTOR

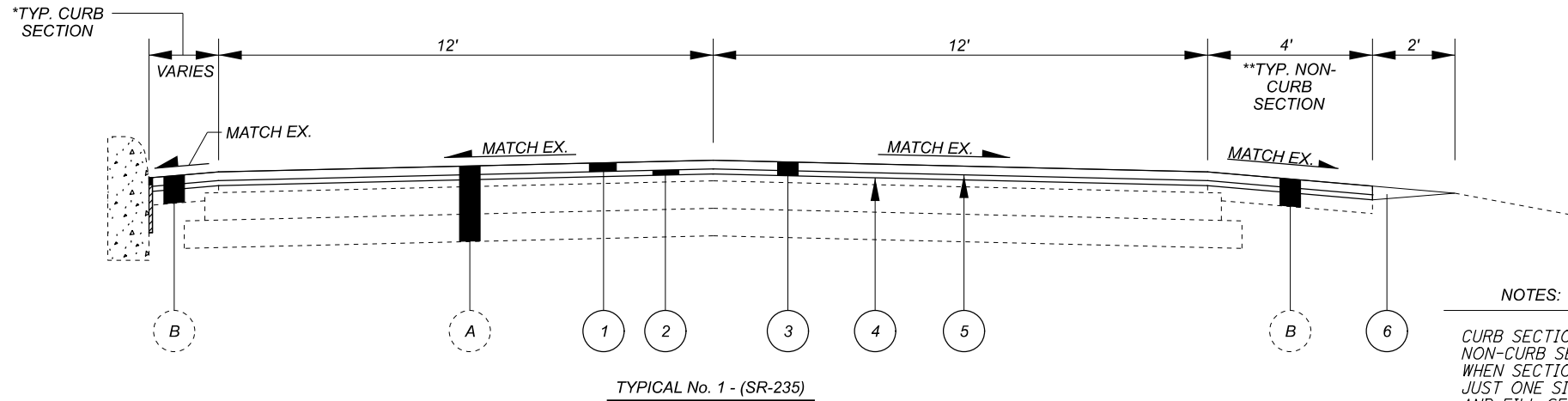
APPROVED *Jade Marchbanks*
 DATE 11-22-21 DIRECTOR, DEPARTMENT OF TRANSPORTATION

DESIGN AGENCY
DESIGNER
REB
REVIEWER
CWW 09-22-21
PROJECT ID
110591
SHEET TOTAL
P.1 15

MODEL: Sheet PAPER SIZE: 17x11 (in.) DATE: 9/28/2021 TIME: 8:40:45 AM USER: rbums2
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LOG-PAVE-FY22

- (A) EXISTING 3" AC, ON 4" BITUMINOUS AGGREGATE BASE, ON 4" AGGREGATE BASE
- (B) EXISTING 8" AGGREGATE BASE, WITH SEAL COAT AND BITUMINOUS PRIME COAT WITH #8 COVER AGGREGATE
- (1) ITEM 441 - 1/4" ASPHALT CONCRETE, SURFACE COURSE, TYPE 1 (448), PG70-22M
- (2) ITEM 441 - 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448)
- (3) ITEM 254 - PAVEMENT PLANNING, ASPHALT CONCRETE (2")
- (4) ITEM 407 - NON-TRACKING TACK COAT (0.085 GAL/SY)
- (5) ITEM 407 - NON-TRACKING TACK COAT (0.055 GAL/SY)
- (6) ITEM 617 - COMPACTED AGGREGATE (AVERAGE 2")



* S.L.M. 4.78 TO S.L.M. 5.61 (2" MILL AND 2" FILL)
 ** S.L.M. 5.61 TO S.L.M. 6.17 (1" MILL AND 2" OVERLAY)

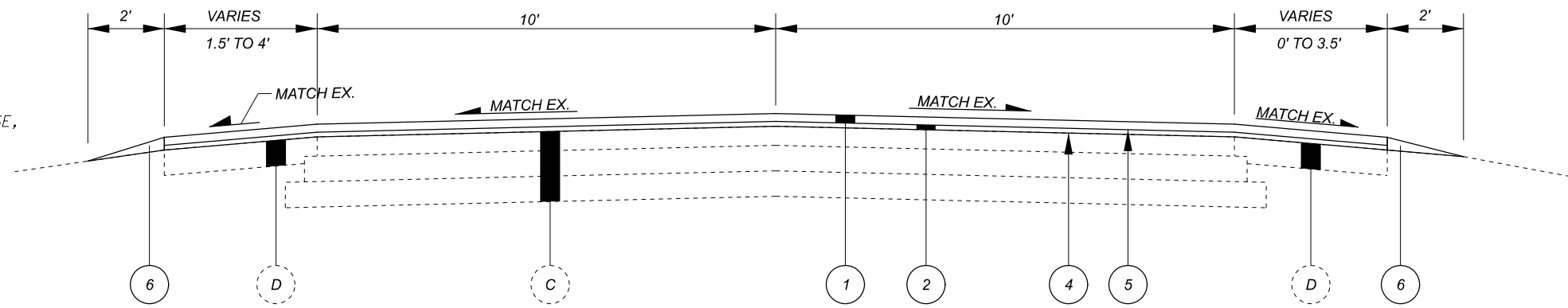
NOTES:

CURB SECTIONS WILL BE 2" MILL AND FILL / NON-CURB SECTIONS SHALL BE 2" OVERLAY. WHEN SECTIONS OF ROADWAY HAS CURB ON JUST ONE SIDE IT WILL BE TREATED AS A MILL AND FILL SECTION.

SEE TYPICAL 2 FOR OVERLAY DETAILS.

A QUANTITY FOR ITEM 301 - ASPHALT CONCRETE BASE, PG64-22, AS PER PLAN HAS BEEN CARRIED TO ESTIMATED QUANTITIES FOR USE BY THE ENGINEER TO WIDEN OR LENGTHEN EXISTING MAILBOX APPROACHES.

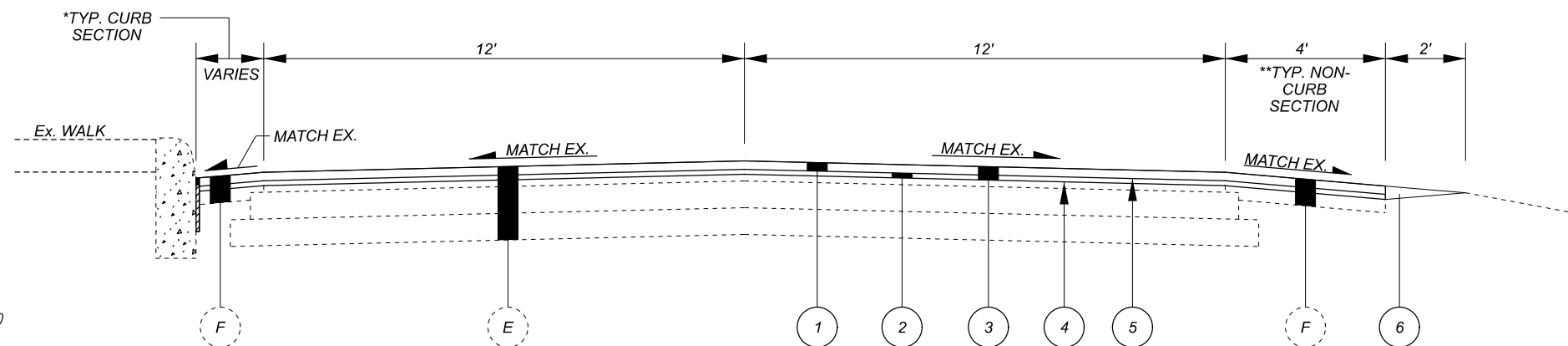
- (C) EXISTING 3" AC, ON 3"-5" CRUSHED AGGREGATE BASE, ON 17" BITUMINOUS SURFACE
- (D) EXISTING 3" ASPHALT CONCRETE, ON 5" CRUSHED AGGREGATE BASE, ON 4" SUBBASE
- (1) ITEM 441 - 1/4" ASPHALT CONCRETE, SURFACE COURSE, TYPE 1 (448), PG70-22M
- (2) ITEM 441 - 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448)
- (4) ITEM 407 - NON-TRACKING TACK COAT (0.085 GAL/SY)
- (5) ITEM 407 - NON-TRACKING TACK COAT (0.055 GAL/SY)
- (6) ITEM 617 - COMPACTED AGGREGATE (2" AVG.)



S.L.M. 11.07 TO S.L.M. 12.21 (2" OVERLAY)

LEGEND

- (E) EXISTING 3" AC, ON 3" BITUMINOUS AGGREGATE BASE, ON 4" AGGREGATE BASE
- (F) EXISTING 8" AGGREGATE BASE, ON 4" SUBBASE
- (1) ITEM 441 - 1/4" ASPHALT CONCRETE, SURFACE COURSE, TYPE 1 (448), PG70-22M
- (2) ITEM 441 - 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448)
- (3) ITEM 254 - PAVEMENT PLANNING, ASPHALT CONCRETE (VARIES 2" TO 1")
- (4) ITEM 407 - NON-TRACKING TACK COAT (0.085 GAL/SY)
- (5) ITEM 407 - NON-TRACKING TACK COAT (0.055 GAL/SY)
- (6) ITEM 617 - COMPACTED AGGREGATE (1" AVG.)



* S.L.M. 0.00 TO S.L.M. 0.60 (2" MILL AND 2" FILL)
 ** S.L.M. 0.60 TO S.L.M. 1.29 (1" MILL AND 2" FILL)

DESIGN AGENCY



DESIGNER

REB

REVIEWER

CWW 09-22-21

PROJECT ID

110591

SHEET

TOTAL

P.3 | 15

ALIGNMENT AND PROFILE

THE WORK PROPOSED BY THIS PROJECT CONSISTS OF PLANING AND RESURFACING OF THE EXISTING PAVEMENT. THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED AND THE PROFILE OF THE PROPOSED SURFACE WILL BE SIMILAR TO THAT OF THE EXISTING PAVEMENT.

ITEM 253 - PAVEMENT REPAIR, AS PER PLAN

PAVEMENT REPAIR SHALL BE IN ACCORDANCE WITH ITEM 253 - PAVEMENT REPAIR, WITH THE FOLLOWING ADDITIONS:

THE ENGINEER SHALL DESIGNATE THE LOCATIONS AND LIMITS OF THE AREAS TO BE REPAIRED. THE AREAS SHALL BE ROUGHLY RECTANGULAR IN SHAPE AND SAWED OR MILLED TO A NEAT LINE. THE DEPTH OF REMOVAL, AS DIRECTED BY THE ENGINEER, SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT. THE ENTIRE AREA INCLUDING VERTICAL FACES SHALL BE COATED PRIOR TO PLACING THE REPLACEMENT MATERIAL PER 253.03. THE REPLACEMENT MATERIAL SHALL BE ITEM 301 - ASPHALT CONCRETE BASE, PG64-22.

THE ESTIMATED PAVEMENT REPAIR AREAS SHALL BE A MINIMUM OF 4 FEET IN WIDTH AND 4 INCHES IN DEPTH MEASURED FROM THE MILLED SURFACE OR AS DIRECTED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

PART 1 - PAVEMENT REPAIR, AS PER PLAN = 2650 SQ. YD.
PART 2 - PAVEMENT REPAIR, AS PER PLAN = 1500 SQ. YD.

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE

AN ESTIMATED QUANTITY OF ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE HAS BEEN CARRIED TO THE GENERAL SUMMARY AND INCLUDED IN THE PLANS TO BE USED AS DIRECTED BY THE ENGINEER.

THE APPROXIMATE DEPTH OF PAVEMENT PLANING SHALL VARY FROM TWO INCH (2") TO ONE INCH (1"), AS SHOWN ON TYPICAL SECTIONS.

THE APPROXIMATE WIDTH OF THE PAVEMENT PLANING SHALL VARY:

LOG-235 FROM THIRTY ONE FEET (31') TO FORTY FEET (40').
LOG-508 FROM TWENTY FIVE FEET (25') TO THIRTY SEVEN FEET (37').

NO MILLED SURFACE SHALL BE EXPOSED FOR MORE THAN 48 HOURS UNLESS APPROVED BY THE ENGINEER.

THERE ARE SEVEN (7) INTERSECTIONS WITHIN THE LIMITS OF THE PAVEMENT PLANNING. THERE ARE SEVENTEEN (17) STREETS/RAMPS THAT THE PROJECT WILL ADDRESS.

THERE ARE APPROXIMATELY FOUR (4) CASTING WITHIN THE PLANNING AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING ALL LABOR AND EQUIPMENT NECESSARY TO MAINTAIN TRAFFIC AROUND THE CASTINGS AND SHALL INCLUDE THE COST IN THE UNIT PRICE BID FOR: ITEM 614 - MAINTAINING TRAFFIC.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

ITEM 611 - MANHOLE ADJUSTED TO GRADE = 1 EACH (235)
ITEM 638 - VALVE BOX ADJUSTED TO GRADE = 5 EACH (235)
ITEM 611 - MANHOLE ADJUSTED TO GRADE = 3 EACH (508)
ITEM 638 - VALVE BOX ADJUSTED TO GRADE = 2 EACH (508)

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (CON'T)

ANY CASTINGS THAT REQUIRE ADJUSTING DURING CONSTRUCTION ARE THE RESPONSIBILITY OF THE CONTRACTOR. ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY FOR THE ADJUSTMENT OF CASTINGS SHALL BE INCLUDED IN ITEMS 611, AND ITEM 638.

ITEM 254 - PATCHING PLANED SURFACE, AS PER PLAN

PAVEMENT AREAS DESIGNATED FOR PATCHING AFTER PAVEMENT PLANING OPERATION SHALL BE MILLED TWO INCHES (2") IN DEPTH AND PATCHED PER 254.04.

AN ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

SR 235 - PATCHING PLANED SURFACE, AS PER PLAN = 75 SY
SR 274 - PATCHING PLANED SURFACE, AS PER PLAN = 50 SY
SR 508 - PATCHING PLANED SURFACE, AS PER PLAN = 50 SY

ITEM 254 - PATCHING PLANED SURFACE, AS PER PLAN = 175 SY

ITEM 441 - ASPHALT CONCRETE, SURFACE COURSE, TYPE 1, (448), PG70-22M

THE MATERIAL USED FOR RESURFACING SHALL CONSIST OF ONE AND ONE QUARTER INCH (1.25") OF ITEM 441 ASPHALT CONCRETE, SURFACE COURSE, TYPE 1, (448). THE BINDER SHALL BE PG70-22M.

PAVEMENT MARKINGS

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DOCUMENT THE LAYOUT OF THE EXISTING PAVEMENT MARKINGS INCLUDING EXISTING LANE AND SHOULDER WIDTHS IN A LOG AND SUBMIT TO THE DEPARTMENT FOR ACCEPTANCE. THE DEPARTMENT WILL NOT ALLOW THE CONTRACTOR TO PERFORM ANY PAVEMENT WORK FUNCTIONS (MILLING, OVERLAY, ETC.) UNTIL ACCEPTANCE OF THE SUBMITTED EXISTING MARKING LOG.

MARKINGS SHALL BE REPLACED IN KIND EXCEPT WHERE EXISTING MARKINGS DO NOT MEET THE CURRENT STANDARD CONSTRUCTION DRAWINGS. THE CONTRACTOR SHALL COORDINATE AND CORROBORATE THE PROPOSED LAYOUT OF ALL PAVEMENT MARKINGS PER APPLICABLE STANDARD CONSTRUCTION DRAWINGS WITH ODOT.

NO PERMANENT PAVEMENT MARKINGS, RAISED PAVEMENT MARKERS, NOR RUMBLE STRIPS SHALL BE PLACED UNTIL THE ODOT PROJECT ENGINEER HAS APPROVED THE LOCATION AND/OR LAYOUT OF THE WORK ZONE PAVEMENT MARKINGS.

PROTECTION OF THE GREAT MIAMI RIVER WATER TRAIL:

AN ACCESS POINT FOR THE GREAT MIAMI RIVER WATER TRAIL IS LOCATED OFF SR 235 AT S.L.M. 04.70. THE CONTRACTOR SHALL NOT RESTRICT ACCESS TO THE ACCESS POINT.

ITEM 659 - SEEDING AND MULCHING, AS PER PLAN

THE CONTRACTOR SHALL BE RESPONSIBLE TO SEED AND MULCH WHERE INDICATED IN THESE PLANS. ALL ITEMS NECESSARY TO PROMOTE GROWTH SHALL BE INCLUDED IN THIS PAY ITEM. ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM OF WORK SHALL BE PAID FOR UNDER THE UNIT BID PER SQUARE YARD FOR ITEM 659 - SEEDING AND MULCHING, AS PER PLAN.

ITEM 608 - CURB, TYPE 6, AS PER PLAN

INCLUDED WITH THIS PAY ITEM THE CONTRACTOR SHALL ALSO BE RESPONSIBLE TO REBUILD PAVEMENT VOID FROM CURB REMOVAL.

COOPERATION WITH RAILROAD (CSX TRANSPORTATION) SECTION:

LOCATION #1

- 1). DOT# 538724K
- 2). RRMP# QI 150.25

LOCATION #2

- 1). ID 538722W
- 2). MP QI 150.20

THE CONTRACTOR SHALL COORDINATE ALL WORK ON, OVER OR ADJACENT TO THE RAILROADS WITHIN PROJECT LIMITS. THE CONTRACTOR SHALL CONTACT CSXT, AT LEAST 30 DAYS IN ADVANCE, IN ORDER TO COORDINATE THE NECESSARY WORK. UNDER NO CIRCUMSTANCES SHALL THERE BE ANY WORK PERFORMED WITHIN THE RAILROAD RIGHT-OF-WAY WITHOUT THE PROPER WRITTEN AUTHORIZATION AND/OR FLAGGING PROTECTION FROM THE RAILROAD. SEE "PAVING AT RAILROAD CROSSING" NOTE FOR ADDITIONAL DETAILS.

PAVING AT RAILROAD CROSSING AND UNDERPATH:

THE RAILROAD CROSSING AT LOG-508-SLM 0.20: SUSPEND AND RESUME RESURFACING AT THE EXISTING RAILROAD CROSSING APPROXIMATELY 3 FEET FROM NEAREST RAIL. THE CONTRACTOR SHALL VERIFY THE ACTUAL JOINT LOCATIONS WITH THE ENGINEER BEFORE PERFORMING WORK.

ROADWAY FLAGGERS SHALL BE PRESENT ON EACH SIDE OF THE RAILROAD CROSSING, ANYTIME TRAFFIC IS DIRECTED INTO OPPOSING TRAFFIC LANES AT THE RAILROAD CROSSING.

CHANNELIZING/MOT DEVICES SHALL BE PLACE A MINIMUM OF 15 FEET FROM THE CENTER LINE OF RAILROAD TRACKS.

THE CONTRACTOR SHALL PROVIDE PRE AND POST VERTICAL CLEARANCE MEASUREMENTS FOR RAILROAD UNDER GRADE BRIDGE (LOCATION #1) ONCE PAVING OPERATIONS ARE COMPLETED.

ITEM 202 - REMOVAL MISC.; DETECTABLE WARNING

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ONLY REMOVING EXISTING DETECTABLE WARNINGS WHERE INDICATED IN THESE PLANS. WORKING CLOSELY WITH THE PROJECT ENGINEER THE CONTRACTOR SHALL STAY WITHIN THE EXISTING FOOTPRINT OF DETECTABLE WARNING, IF NECESSARY MINIMAL ALTERATIONS MAY BE MADE TO PREPARE EACH LOCATION TO ACCEPT A STANDARD 2'x4' OR 2'x5' DETECTABLE WARNING, UNLESS OTHERWISE STATED IN THESE PLANS. ALL OTHER LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM OF WORK SHALL BE PAID FOR UNDER THE UNIT BID PER SQUARE FOOT FOR ITEM 202 - REMOVAL MISC.; DETECTABLE WARNING.

ITEM 608 - CURB RAMP, AS PER PLAN

THE CONTRACTOR SHALL BE RESPONSIBLE TO MODIFY AN EXISTING CURB RAMP WHERE INDICATED IN THESE PLANS. WORKING CLOSELY WITH THE PROJECT ENGINEER THE CONTRACTOR SHALL MODIFY THE EXISTING CURB RAMP BY ADDING A FLARED WING TO ONE SIDE. THE PURPOSE IS NOT TO REBUILD THE ENTIRE RAMP. MODIFICATION SHALL BE KEPT CLOSE TO STANDARD AS POSSIBLE. ALL OTHER LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM OF WORK SHALL BE PAID FOR UNDER THE UNIT BID PER SQUARE FOOT FOR ITEM 608 - CURB RAMP, AS PER PLAN.

ITEM 202 - CURB REMOVED, AS PER PLAN

THE CONTRACTOR SHALL BE RESPONSIBLE TO REMOVE CURB WHERE INDICATED IN THESE PLANS. CONTRACTOR SHALL BE PERMITTED TO REMOVE A MAXIMUM OF 2 FEET FULL DEPTH PAVEMENT REMOVAL TO ALLOW FORMS TO BE SET. PAVEMENT REMOVAL SHALL BE INCLUDED IN THIS PAY ITEM.

ENVIRONMENTAL NOTE

PROTECTION OF WATER RESOURCES: THE CONTRACTOR SHALL NOT DISCHARGE TOXIC OR HAZARDOUS MATERIALS SUCH AS SEALANTS, PAINT, SOLVENTS, CLEANING AGENTS, EARTHEN MATERIALS, WASTE-WATER, FUELS OR DEBRIS OF ANY KIND TO A WATERWAY. IF REFUELING OF IMMOBILE EQUIPMENT IS NECESSARY WITHIN THE FLOODPLAIN OR NEAR ANY TRIBUTARY DRAINAGE WAYS, DITCHES, OR STREAM, THE CONTRACTOR SHALL PROVIDE SECONDARY CONTAINMENT WITH ENOUGH CAPACITY TO COMPLETELY CONTAIN AND COLLECT ALL POTENTIAL LIQUID WASTES IN THE EVENT OF A SPILL.

ANY AND ALL CONSTRUCTION DEBRIS, EARTHEN DEBRIS, EXCESS ASPHALT OR CONCRETE, WOOD DEBRIS FROM CLEARING, EXCESS FILL MATERIAL, AND TRASH SHOULD BE DISPOSED OF AT AN APPROVED UPLAND SITE OR LAND FILL ABOVE FEMA 100-YEAR FLOOD ELEVATIONS. DISPOSAL OF ANY SUCH MATERIALS WITHIN 1000 FEET OF A SCENIC RIVER IS PROHIBITED.

IN ACCORDANCE WITH ORC 3750.06, SPILLS MUST BE REPORTED TO THE LOCAL FIRE DEPARTMENT (911); THE LOCAL EMERGENCY COORDINATOR (937-324-7615), AND THE OHIO SPILL LINE (1-800-282-9378).

THE CONTRACTOR SHALL KEEP ALL IDLE EQUIPMENT, FUELS, LUBRICANTS, AND ANY STORAGE FOR/OF POTENTIALLY TOXIC OR HAZARDOUS MATERIALS OUT OF THE FEMA DESIGNATED SPECIAL FLOOD HAZARD AREA AND NOT WITHIN 1000 FEET OF A SCENIC RIVER.

IF ANY ISSUE OF INCIDENT OCCURS WITHIN CSXT ROW, PLEASE CONTACT THE CSXT PUBLIC SAFETY COORDINATION CENTER AT 800-232-0144.

WEARING COURSE REMOVED CALCULATIONS				
PART.	LOCATION	WIDTH	LENGTH	AREA
1	NORTH OF ELLIS ST. LOG-235-5.42	30.0' AVG.	100'	333 SY
1	END PROJECT LOG-235-6.06	30.0' AVG.	100'	333 SY
1	SR 235 INTERSECTIONS	3 INTERSECTIONS X 20 SY/INT.		60 SY
1	SR 235 DRIVES	14 DRIVES X 8 SY/DRIVE		112 SY
2	BEGIN PROJECT LOG-274-11.02	27.0' AVG.	100'	300 SY
2	END PROJECT LOG-274-12.21	28.0' AVG.	100'	311 SY
2	SR 274 INTERSECTIONS	4 INTERSECTIONS X 20 SY/INT.		80 SY
2	SR 274 DRIVES	40 DRIVES X 8 SY/DRIVE		320 SY
1	SOUTH OF CR 63 LOG-508-0.60	26.0' AVG.	50'	144 SY
1	SOUTH CORP. LINE LOG-508-0129	26.0' AVG.	100'	289 SY
1	SR 508 INTERSECTIONS	4 INTERSECTIONS X 20 SY/INT.		80 SY
1	SR 508 DRIVES	10 DRIVES X 8 SY/DRIVE		248 SY
				TOTAL 2610 SY

WEARING COURSE REMOVED QUANTITIES ARE INCLUDED WITH THE PAVEMENT CALCULATIONS

DESIGN AGENCY



DESIGNER
REB
REVIEWER
CWW 09-22-21
PROJECT ID
110591
SHEET TOTAL
P.4 15

PROPOSED DETECTABLE WARNING

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING DETECTABLE WARNING WHERE INDICATED IN THESE NOTES AND PLANS. WORKING CLOSELY WITH THE PROJECT ENGINEER THE CONTRACTOR SHALL REMOVE THE EXISTING BRICK INLAYS ALONG SR 508, AND MODIFY THE EXISTING VOID IF NECESSARY TO WET PLACE A NEW DETECTABLE WARNING MADE FROM AN APPROVED PRODUCT. OTHER LOCATIONS MENTIONED SHALL CONSIST OF REMOVING SECTIONS OF EXISTING WALK AND REPLACING THE WALK TO INCLUDE A DETECTABLE WARNING MADE FROM AN APPROVED PRODUCT. THE CONTRACTOR SHALL NOT BE PERMITTED TO REMOVE LARGE SECTIONS OF EXISTING WALK DUE TO ITS CURRENT CONDITION.

SEE TOTALS PER LOCATION FOR PAY ITEMS.

ALL OTHER LABOR, REMOVALS, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM OF WORK SHALL BE PAID FOR UNDER THE UNIT BID PER SQUARE FOOT FOR ITEM 608- CONCRETE WALK.

WORK LOCATIONS NOT SHOWN IN THESE PLANS: (FOR SR 235)

- 1). NW CORNER OF SR 235 AND MILL STREET, SAW CUT AND REMOVE 4' OF EXISTING WALK THAT BUTTS UPTO MILL ST; AND INSTALL NEW WALK TO INCLUDE A 2'x4' DETECTABLE WARNING FROM APPROVED MATERIAL.
- 2). SW CORNER OF SR 235 AND MILL STREET, SAW CUT AND REMOVE 6' OF EXISTING WALK THAT BUTTS UPTO MILL ST; AND INSTALL NEW WALK TO INCLUDE A 2'x4' DETECTABLE WARNING FROM APPROVED MATERIAL.
- 3). NE CORNER OF SR 235 AND MILL STREET, SAW CUT AND REMOVE 4' OF EXISTING WALK THAT BUTTS UPTO MILL ST; AND INSTALL NEW WALK TO INCLUDE A 2'x4' DETECTABLE WARNING FROM APPROVED MATERIAL.
- 4). SE CORNER OF SR 235 AND MILL STREET, SAW CUT AND REMOVE 8.5' OF EXISTING WALK THAT BUTTS UPTO MILL ST, AND INSTALL NEW WALK TO INCLUDE A 2'x4' DETECTABLE WARNING FROM APPROVED MATERIAL.
- 5). SW CORNER OF SR 235 AND CHURCH STREET, SAW CUT AND REMOVE 4' OF EXISTING WALK THAT BUTTS UPTO CHURCH ST; AND INSTALL NEW WALK TO INCLUDE A 2'x4' DETECTABLE WARNING FROM APPROVED MATERIAL.
- 6). SE CORNER OF SR 235 AND CHURCH STREET, SAW CUT AND REMOVE 4' OF EXISTING WALK THAT BUTTS UPTO CHURCH ST; AND INSTALL NEW WALK TO INCLUDE A 2'x4' DETECTABLE WARNING FROM APPROVED MATERIAL.
- 7). NW CORNER OF SR 235 AND BOGGS STREET, INSTALL NEW WALK BUTTING UPTO BOGGS ST, AND THE EXISTING WALK. FOLLOW THE EXISTING RADIUS OF THE ROADWAY CREATE A PIE WEDGE SHAPE BACK TO EXISTING WALK. INCLUDE A 2'x4' DETECTABLE WARNING FROM APPROVED MATERIAL, ALIGNED WITH NE CORNER.
- 8). SE CORNER OF SR 235 AND BOGGS STREET, REMOVE EXCESS WALK NORTH OF THE EXISTING CURB RAMP. REMOVE 8'± OF EXISTING CURB FROM CURB RAMP AROUND RADIUS TO THE EAST. CONSTRUCT A NEW FLARE ON THE NORTH SIDE OF THE EXISTING RAMP RAMP, INSTALL NEW STANDARD HEIGHT CURB TO CONNECT TO FLARE AND FILL VOID FROM REMOVED WALK WITH EMBANKMET AND SEED AND MULCH. SEE NOTE FOR CURB REMOVAL AS PER PLAN.

PROPOSED WALK AND DETECTABLE WARNING (con't)

- 9). NE CORNER OF SR 235 AND BOGGS STREET, INSTALL 12'± OF 4' WALK, TO INCLUDE A 2'x4' DETECTABLE WARNING FROM AN APPROVED MATERIAL, TO ALIGN WITH THE PROPOSED NW CORNER.
- 10). SW CORNER OF SR 235 AND KOKE STREET, INSTALL 9'± OF 4' WALK, TO INCLUDE A 2'x4' DETECTABLE WARNING FROM AN APPROVED MATERIAL.
- 11). SE CORNER OF SR 235 AND KOKE/MIAMI STREET, REMOVE 5'± OF EXISTING WALK IN TWO LOCATIONS, ONE BUTTING UPTO SR 235 AND THE OTHER BUTTING UPTO MIAMI STREET. INSTALL NEW WALK AT BOTH LOCATION TO INCLUDE A 2'x4' DETECTABLE WARNING FROM AN APPROVED MATERIAL.

WORK LOCATIONS NOT SHOWN IN THESE PLANS: (FOR SR 508)

- 1). SOUTH EAST CORNER OF SR 235 AND SR 508, REMOVE THE EXTRA DETECTABLE WARNING AND FILL VOID WITH CONCRETE.
- 2). MID BLOCK ON SR 508 BETWEEN HAYES STREET AND SOUTH STREET REMOVE THE EXISTING BRICK INLAY AT THE ALLEY, ON BOTH SIDES OF THE STREET. REPLACE WITH APPROVED DETECTABLE WARNING MATERIAL.
- 3). AT THE INTERSECTION OF SR 508 AND SOUTH STREET, BOTH SIDES OF INTERSECTION, SAW CUT 6' X 3' AND REMOVE EXISTING CONCRETE, AND PLACE A 2' X 5' APPROVED DETECTABLE WARNING MATERIAL.
- 4). SR 508 EAST SIDE OF ROADWAY, A MINIMUM OF 12 FEET FROM THE CENTER OF THE AT GRADE RAILROAD CROSSING, SAW CUT 6' X 3' AND REMOVE EXISTING CONCRETE, AND PLACE A 2' X 5' APPROVED DETECTABLE WARNING MATERIAL.
- 5). SR 508 WEST SIDE OF ROADWAY, A MINIMUM OF 12' NORTH OF THE CENTER OF THE AT GRADE RAILROAD CROSSING. SAW CUT 6' X 3' AND REMOVE EXISTING CONCRETE, AND PLACE A 2' X 5' APPROVED DETECTALBE WARNING MATERIAL.
- 6). SR 508 SE CORNER FROM RAILROAD, INSTALL 6'± OF PROPOSED WALK, TO BUTT AGAINST EXISTING PAVEMENT AT ALLEY. IT MAY BE NECESSARY TO REMOVE 4'± OF EXISTING WALK. INCLUDED IN PROPOSED WALK A 2'x4" DETECTABLE WARNING OF APPROVED MATERIAL.
- 7). NO WORK PERMITTED SOUTH OF THE TRACKS, WITHIN A MINIMUM OF 12 FEET FROM THE CENTER OF THE AT GRADE RAILROAD.

THE CONTRACTOR SHALL NOT BE PERMITTED TO WORK OUTSIDE THE EXISTING FOOT PRINT OF THE EXISTING SIDE WALKS. FOR MORE INFORMATION SEE DETAILS SHOWN ON SHEET 8, AND NOTE ON SHEET 7.

TOTALS PER SR 235 LOCATIONS:

- @ NW CORNER SR 235 AND MILL STREET:
ITEM 202 - WALK REMOVED..... 16 S.F.
ITEM 608 - CONCRETE WALK, 4"..... 16 S.F.
ITEM 608 - DETECTABLE WARNING..... 8 S.F.
- @ SW CORNER OF SR 235 AND MILL STREET:
ITEM 202 - WALK REMOVED 24 S.F.
ITEM 608 - CONCRETE WALK, 4" 24 S.F.
ITEM 608 - DETECTABLE WARNING 8 S.F.
- @ NE CORNER OF SR 235 AND MILL STREET:
ITEM 202 - WALK REMOVED 16 S.F.
ITEM 608 - CONCRETE WALK, 4" 16 S.F.
ITEM 608 - DETECTABLE WARNING 8 S.F.
- @ SE CORNER OF SR 235 AND MILL STREET:
ITEM 202 - WALK REMOVED 34 S.F.
ITEM 608 - CONCRETE WALK, 4" 34 S.F.
ITEM 608 - DETECTABLE WARNING 8 S.F.
- @ SW CORNER OF SR 235 AND CHURCH STREET:
ITEM 202 - WALK REMOVED 16 S.F.
ITEM 608 - CONCRETE WALK, 4" 16 S.F.
ITEM 608 - DETECTABLE WARNING 8 S.F.
- @ SE CORNER OF SR 235 AND CHURCH STREET:
ITEM 202 - WALK REMOVED 16 S.F.
ITEM 608 - CONCRETE WALK, 4" 16 S.F.
ITEM 608 - DETECTABLE WARNING 8 S.F.
- @ NW CORNER OF SR 235 AND BOGGS STREET:
ITEM 608 - CONCRETE WALK, 4" 60 S.F.
ITEM 608 - DETECTABLE WARNING 8 S.F.
- @ SE CORNER IF SR 235 AND BOGGS STREET:
ITEM 202 - WALK REMOVED 17 S.F.
ITEM 202 - CURB REMOVED, AS PER PLAN ... 8 FT.
ITEM 203 - EMBANKMENT32 C.Y.
ITEM 608 - CURB RAMP, AS PER PLAN ... 5 S.F.
ITEM 609 - CURB, TYPE 6 8 FT
ITEM 659 - SEEDING AND MULCHING, AS PER PLAN 2 S.Y.
- @ NE CORNER OF SR 235 AND BOGGS STREET:
ITEM 608 - CONCRETE WALK, 4" 48 S.F.
ITEM 608 - DETECTABLE WARNING 8 S.F.
- @ SW CORNER OF SR 235 AND KOKE STREET:
ITEM 608 - CONCRETE WALK, 4" 36 S.F.
ITEM 608 - DETECTABLE WARNING 8 S.F.
- @ SE CORNER OF SR 235 AND KOKE/MIAMI STREET:
ITEM 202 - WALK REMOVED 40 S.F.
ITEM 608 - CONCRETE WALK, 4" 40 S.F.
ITEM 608 - DETECTABLE WARNING 16 S.F.

ADA WAIVER

AN APPROVED ADA DESIGN WAIVER IS REQUIRED ON THIS PROJECT. THE FOLLOWING FEATURES LISTED BELOW CANNOT FEASIBLY BE CONSTRUCTED TO MEET ADA GUIDLINES.

ADA DESIGN WAIVER

ADA FEATURE	APPROVAL DATE
RMPO007590	12/28/21
RMPO007574	12/28/21
RMPO007573	12/28/21

TOTALS PER SR 508 LOCATIONS:

- @ SE CORNER OF SR 508 AND SR 235 :
ITEM 202 - REMOVAL MISC.: DETECTABLE WARNING. 10 S.F.
ITEM 608 - CONCRETE WALK, 4" 10 S.F.
- @ NW CORNER OF SR 508 AND HAYES STREET:
ITEM 202 - REMOVAL MISC.: DETECTABLE WARNING. 16 S.F.
ITEM 608 - CONCRETE WALK, 4" 16 S.F.
ITEM 608 - DETECTABLE WARNING 16 S.F.
- @ NE CORNER OF SR 508 AND HAYES STREET:
ITEM 202 - REMOVAL MISC.: DETECTABLE WARNING. 16 S.F.
ITEM 608 - CONCRETE WALK, 4" 16 S.F.
ITEM 608 - DETECTABLE WARNING 16 S.F.
- @ SW CORNER OF SR 508 AND HAYES STREET:
ITEM 202 - REMOVAL MISC.: DETECTABLE WARNING. 16 S.F.
ITEM 608 - CONCRETE WALK, 4" 16 S.F.
ITEM 608 - DETECTABLE WARNING 16 S.F.
- @ SE CORNER OF SR 508 AND HAYES STREET:
ITEM 202 - REMOVAL MISC.: DETECTABLE WARNING. 16 S.F.
ITEM 608 - CONCRETE WALK, 4" 16 S.F.
ITEM 608 - DETECTABLE WARNING 16 S.F.
- @ SR 508 MIDBLOCK ALLEY BETWEEN HAYES AND SOUTH STREETS:
ITEM 202 - REMOVAL MISC.: DETECTABLE WARNING. 32 S.F.
ITEM 608 - CONCRETE WALK, 4" 32 S.F.
ITEM 608 - DETECTABLE WARNING 32 S.F.
- @ NE CORNER OF SR 508 AND SMITH STREET:
ITEM 202 - WALK REMOVED 18 S.F.
ITEM 608 - CONCRETE WALK, 4" 18 S.F.
ITEM 608 - DETECTABLE WARNING 10 S.F.
- @ SE CORNER OF SR 508 AND SMITH STREET:
ITEM 202 - WALK REMOVED 18 S.F.
ITEM 608 - CONCRETE WALK, 4" 18 S.F.
ITEM 608 - DETECTABLE WARNING 10 S.F.
- @ NW CORNER EXISTING RAILROAD, 12' FROM CENTER OF TRACK:
ITEM 202 - WALK REMOVED 18 S.F.
ITEM 608 - CONCRETE WALK, 4" 18 S.F.
ITEM 608 - DETECTABLE WARNING 10 S.F.
- @ NE CORNER EXISTING RAILROAD, 12' FROM CENTER OF TRACK:
ITEM 202 - WALK REMOVED 18 S.F.
ITEM 608 - CONCRETE WALK, 4" 18 S.F.
ITEM 608 - DETECTABLE WARNING 10 S.F.
- @ SE CORNER EXISTING RAILROAD, 12' FROM CENTER OF TRACK:
ITEM 202 - WALK REMOVED 16 S.F.
ITEM 608 - CONCRETE WALK, 4" 24 S.F.
ITEM 608 - DETECTABLE WARNING 8 S.F.

TOTALS FOR SR 235/508 CARRIED TO GENERAL SUMMARY

- ITEM 202 - WALK REMOVED 351 S.F.
- ITEM 202 - CURB REMOVED, AS PER PLAN 8 S.F.
- ITEM 202 - REMOVAL MISC.: DETECTABLE WARNING .. 106 S.F.
- ITEM 203 - EMBANKMENT32 C.Y.
- ITEM 608 - CONCRETE WALK, 4" 508 S.F.
- ITEM 608 - DETECTABLE WARNING 232 S.F.
- ITEM 608 - CURB RAMP, AS PER PLAN 5 S.F.
- ITEM 609 - CURB, TYPE 6, AS PER PLAN 8 FT.
- ITEM 659 - SEEDING AND MULCHING, AS PER PLAN 2 S.Y.

DESIGN AGENCY



DESIGNER
REB
REVIEWER
CWW 09-22-21
PROJECT ID
110591
SHEET TOTAL
P.5 15

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS AS SHOWN IN THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN ONE (1) HOUR FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRE-CONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE. THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN (CONTINUED)

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 6 SIGN MONTH (ASSUMING THREE PCMS FOR TWO MONTHS)

PEDESTRIAN DETOUR

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING THE SIGNS AND BARRICADES FOR THE PROPOSED PEDESTRIAN DETOURS IN ACCORDANCE WITH STD. MT-110.10, AND THESE PLANS. ANY DEVIATION MUST FIRST BE APPROVED BY THE ENGINEER. ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THIS WORK SHALL BE INCLUDED IN THE UNIT BID PER LUMP SUM FOR ITEM 614 - MAINTAINING TRAFFIC.

CORRESPONDENCE WITH LOCATIONS SHOWN IN PLANS: SEE DETECTABLE WARNING, AS PER PLAN NOTE SHEET 5.

- 1). SOUTH EAST CORNER OF SR 235 AND SR 508: PEDESTRIANS TRAVELING ALONG THE EAST SIDE OF SR 508, FROM SOUTH TO NORTH SHALL BE DETOURED AT HAYES ST SENDING PEDESTRIANS FROM THE NE CORNER TO THE NW CORNER TO CONTINUE SOUTH TO NORTH TRAVEL ALONG THE WEST SIDE OF SR 508.
- 2). MID BLOCK ALLEY BETWEEN HAYES STREET AND SOUTH STEET: NW CORNER AND NE CORNER TO BE DONE TOGETHER, SENDING PEDESTRIANS TRAVELING NORTH TO SOUTH ON EITHER SIDES OF THE STREET, EAST OR WEST ON HAYES STREET TO THE EXISTING ALLEY MID BLOCK, TRAVELING SOUTH IN ALLEY TO CONNECTING ALLEY MID BLOCK, TRAVELING EAST OR WEST BACK TO SR 508.
- 3). NE CORNER OF SOUTH STREET: PEDESTRIANS TRAVELING SOUTH TO NORTH ALONG THE EAST SIDE OF SR 508 SHALL BE DETOURED FROM THE SE CORNER OF SOUTH STREET TRAVELING EAST ON SOUTH STREET TO THE ALLEY MID BLOCK, THEN TRAVELING NORTH THROUGH ALLEY TO CONNECTING ALLEY MID BLOCK, THEN TRAVELING WEST THROUGH ALLEY TO SR 508.
SE CORNER OF SOUTH STREET: PEDESTRIANS TRAVELING SOUTH TO NORTH ALONG THE EAST SIDE OF SR 508 SHALL BE DETOURED AT A POINT 70' SOUTH OF THE RAILROAD, SENDING PEDESTRIAN TRAVEL SOUTH BACK TO S. BOGGS STREET, CROSSING SR 508 TO THE WEST, TRAVELING WEST ON S. BOGGS AROUND THE CURVE AND CONTINUE TO ALLEY MID BLOCK, THEN TRAVELING EAST IN ALLEY TO SR 508.
- 4). SR 508 EAST SIDE OF ROADWAY, 12 FEET FROM CENTER OF THE RAILROAD TRACKS. THE DETOUR SHALL BE THE SAME DETOUR SET FOR THE SE CORNER OF SOUTH STREET DIRECTLY ABOVE.
- 5). SR 508 WEST SIDE OF ROADWAY, 12' NORTH OF CENTER OF RAILROAD TRACKS. THE DETOUR SHALL BE SET 80' SOUTH OF TRACKS, SENDING PEDESTRIAN TRAVEL BACK SOUTH TO S. BOGGS ST., THEN FOLLOWING THE SET DETOUR PLACED FOR THE SE CORNER OF SOUTH STREET ABOVE.

DESIGN AGENCY



DESIGNER

REB

REVIEWER

NKH 09-20-21

PROJECT ID


110591

SHEET TOTAL

P.7 15

SHEET NUM.					PART.		ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
4	5	10	13	14	01/STR/PV	02/NFA/PV						
ROADWAY												
		2,610			1,599	1,011	202	23500	2,610	SY	WEARING COURSE REMOVED	
	351				351		202	30000	351	SF	WALK REMOVED	
	8				8		202	32001	8	FT	CURB REMOVED, AS PER PLAN	4
	106				106		202	98400	106	SF	REMOVAL MISC.: DETECTABLE WARNING	4
	0.32				0.32		203	20000	0.32	CY	EMBANKMENT	
	508				508		608	10000	508	SF	4" CONCRETE WALK	
	5				5		608	52001	5	SF	CURB RAMP, AS PER PLAN	4
	232				232		608	53020	232	SF	DETECTABLE WARNING	
EROSION CONTROL												
	2				2		659	00501	2	SY	SEEDING AND MULCHING, CLASS 1, AS PER PLAN	4
DRAINAGE												
4					4		611	99654	4	EACH	MANHOLE ADJUSTED TO GRADE	
PAVEMENT												
4,150					2,650	1,500	253	01001	4,150	SY	PAVEMENT REPAIR, AS PER PLAN	6
	9,420				9,420		254	01000	9,420	SY	PAVEMENT PLANING, ASPHALT CONCRETE (1" DEPTH)	
	26,360				26,360		254	01000	26,360	SY	PAVEMENT PLANING, ASPHALT CONCRETE (2" DEPTH)	
175						175	254	01601	175	SY	PATCHING PLANED SURFACE, AS PER PLAN	6
	8				8		301	46001	8	CY	ASPHALT CONCRETE BASE, PG64-22, AS PER PLAN (4" DEPTH)	3
	8,879				6,035	2,844	407	20000	8,879	GAL	NON-TRACKING TACK COAT	
	2,418				1,712	706	441	50100	2,418	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG70-22M (1 1/4")	
	1,449				1,026	423	441	50200	1,449	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448) (3/4")	
	8				8		609	26001	8	FT	CURB, TYPE 6, AS PER PLAN	4
	354				199	155	617	10100	354	CY	COMPACTED AGGREGATE (2" AVG.)	
WATER WORK												
7					7		638	10800	7	EACH	VALVE BOX ADJUSTED TO GRADE	
TRAFFIC CONTROL												
				64			621	00100	64	EACH	RPM	
				64			621	54000	64	EACH	RAISED PAVEMENT MARKER REMOVED	
		8.21			5.77	2.44	642	00104	8.21	MILE	EDGE LINE, 6", TYPE 1	
		3.82			2.6	1.22	642	00300	3.82	MILE	CENTER LINE, TYPE 1	
		38			38		644	00400	38	FT	CHANNELIZING LINE, 8"	
		135			135		644	00500	135	FT	STOP LINE	
		1,189			1,189		644	00620	1,189	FT	CROSSWALK LINE, 12"	
		22			22		644	00700	22	FT	TRANSVERSE/DIAGONAL LINE WHITE	
		2			2		644	01000	2	EACH	RAILROAD SYMBOL MARKING	
		1			1		644	01110	1	EACH	SCHOOL SYMBOL MARKING, 96"	
		640			640		644	01200	640	FT	PARKING LOT STALL MARKING	
		2			2		644	01600	2	EACH	HANDICAP SYMBOL MARKING	
STRUCTURE REPAIR (LOG-508-0029)												
	48				48		516	31001	48	FT	JOINT SEALER, AS PER PLAN(2" DEEP)	15
MAINTENANCE OF TRAFFIC												
		43			29	14	614	12460	43	EACH	WORK ZONE MARKING SIGN	
		9.08			6.64	2.44	614	21000	9.08	MILE	WORK ZONE CENTER LINE, CLASS I	
		405			405		614	26610	405	FT	WORK ZONE STOP LINE, CLASS III, 642 PAINT	
		3			3		614	31750	3	EACH	WORK ZONE SCHOOL SYMBOL MARKING, 96", CLASS III, 642 PAINT	
INCIDENTALS												
							614	11000	LS		MAINTAINING TRAFFIC	
							623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING	
							624	10000	LS		MOBILIZATION	

DESIGN AGENCY



DESIGNER: REB
 REVIEWER: CWW
 PROJECT ID: 110591
 SHEET: P.9 TOTAL: 15