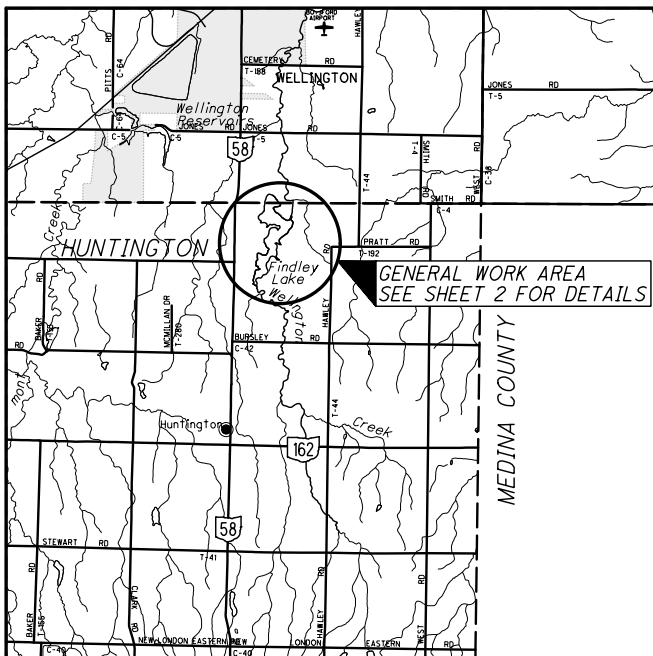


STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

**LOR-FINDLEY
SP-MAIN RD**
HUNTINGTON TOWNSHIP
WELLINGTON TOWNSHIP
LORAIN COUNTY



LOCATION MAP

LATITUDE: 42° 08' 02" N LONGITUDE: 82° 13' 02" W



- INTERSTATE HIGHWAY -----
- FEDERAL ROUTES -----
- STATE ROUTES -----
- COUNTY & TOWNSHIP ROADS -----
- OTHER ROADS -----

DESIGN DESIGNATION

NOT PROVIDED (STATE PARK ROADS)
SPEED LIMIT (DESIGN AND LEGAL) = 20MPH

DESIGN EXCEPTIONS

NONE

INDEX OF SHEETS:

TITLE SHEET	1
SCHEMATIC	2
TYPICAL SECTIONS	3
GENERAL NOTES	4 - 5
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PAVEMENT MARKINGS	9
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STRUCTURE DETAILS	12 - 13

PROJECT DESCRIPTION

THIS PROJECT CONSISTS OF RESURFACING VARIOUS ROUTES WITHIN FINDLEY STATE PARK IN LORAIN COUNTY WITH PAVEMENT PLANING, PAVEMENT REPAIRS, AND ASPHALT CONCRETE. IT WILL ALSO INCLUDE PAVEMENT MARKINGS AND OTHER MINOR IMPROVEMENTS.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	N/A
(MAINTENANCE PROJECT)	
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	N/A
(MAINTENANCE PROJECT)	
NOTICE OF INTENT EARTH DISTURBED AREA:	N/A
(MAINTENANCE PROJECT)	

2019 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

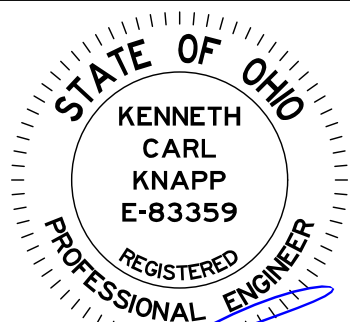
I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRED THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED
DATE 12/13/19 ROBERT WEAVER, P.E., P.S.
DISTRICT THREE DEPUTY DIRECTOR

APPROVED _____
DATE _____ JACK MARCHBANKS, Ph.D., DIRECTOR
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE 12/26/2019 JEREMY WENNER, P.E., CHIEF ENGINEER
DEPARTMENT OF NATURAL RESOURCES

ENGINEERS SEAL:



SIGNED:
DATE: 11 December 2019

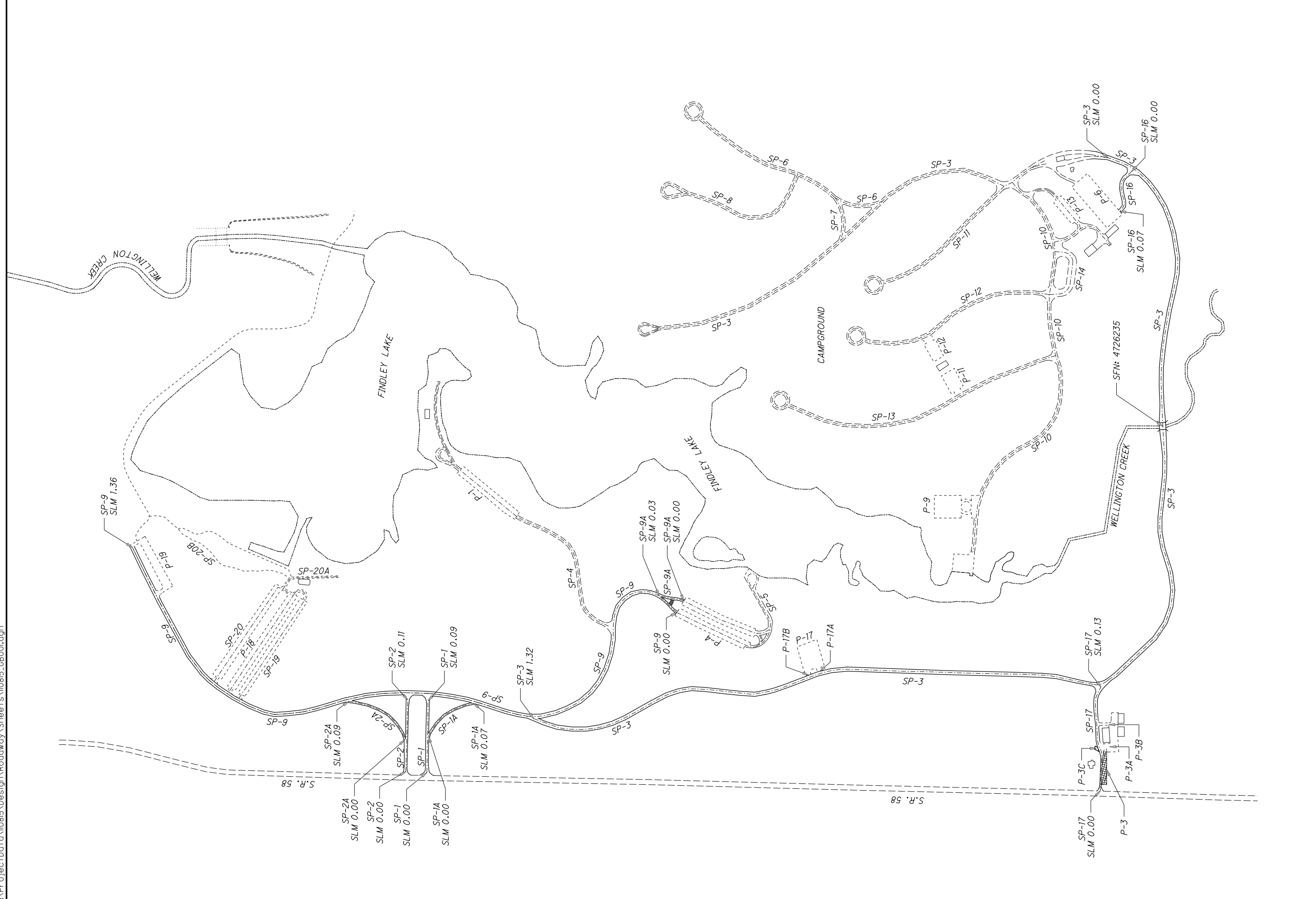
STANDARD CONSTRUCTION DRAWINGS		SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
BP-3.1	10/18/19	800-2019 1/17/20	
		832 10/19/18	
		846 4/17/15	
TC-41.20	10/18/13		
TC-41.50	10/18/13		
TC-42.20	10/18/13		
TC-52.10	10/18/13		
TC-52.20	7/20/18		

UNDERGROUND UTILITIES
Contact Two Working Days
Before You Dig

OHIO811.org
Before You Dig

OHIO811. 8-1-1. or 1-800-362-2764
(Non-members must be called directly)

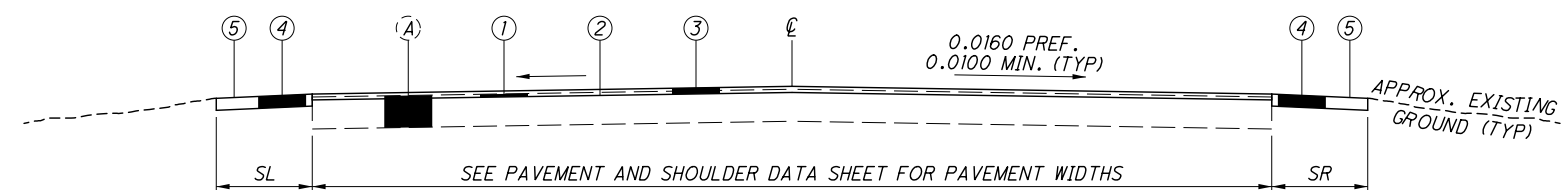
FEDERAL PROJECT NO. NON-FEDERAL
CONSTRUCTION PROJECT NO. 110815
RAILROAD INVOLVEMENT NONE
LOR-FINDLEY SP-MAIN ROAD
1/13



CALCULATED	TPG
CHECKED	KCK

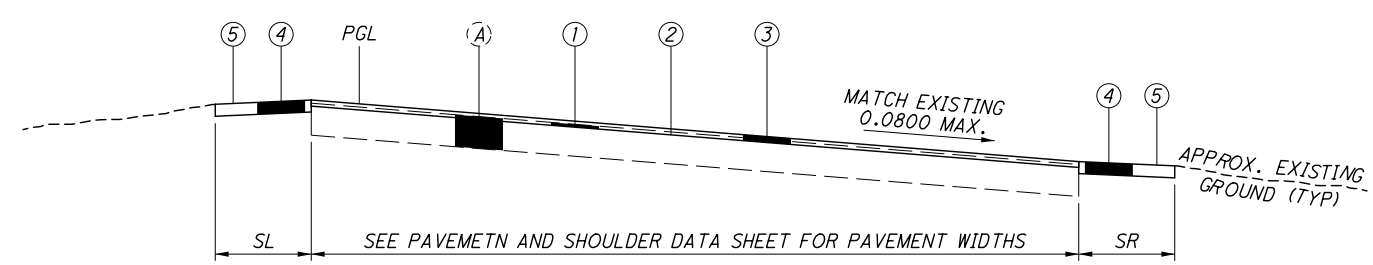
0 250 500
HORIZONTAL SCALE IN FEET

SCHEMATIC PLAN



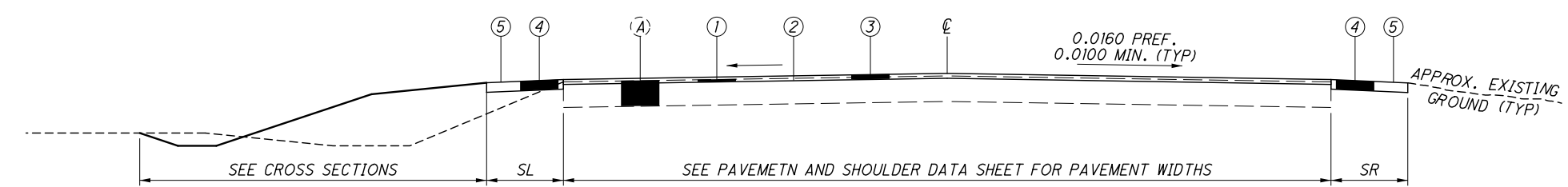
APPLIES TO:

SP-1	0.00-0.09	0.09 MILES
SP-2	0.00-0.06	0.06 MILES
SP-2	0.10-0.11	0.01 MILES
SP-3	0.00-1.32	1.32 MILES
SP-9	0.00-1.36	1.36 MILES
SP-9A	0.00-0.03	0.03 MILES
SP-16	0.00-0.07	0.07 MILES
SP-17	0.00-0.13	0.13 MILES
TOTAL		3.07 MILES



APPLIES TO:

SP-1A	0.00-0.07	0.07 MILES
SP-2A	0.00-0.09	0.09 MILES
TOTAL		0.16 MILES



APPLIES TO:

SP-2	0.06-0.10	0.04 MILES
TOTAL		0.04 MILES

EXISTING LEGEND
A - EXISTING PAVEMENT (SEE CORING DETAILS)

PROPOSED LEGEND
1 - ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (0.75")
2 - ITEM 407 - TACK COAT
3 - ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 9.5 MM, TYPE A (448), AS PER PLAN (PG64-22) (1.50")
4 - ITEM 617 - COMPACTED AGGREGATE (TYP) (3.00" AVG)
5 - ITEM 408 - PRIME COAT

TYPICAL SECTIONS

LOR-FINDLEY
SP-MAIN ROAD

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UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS.

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

GAS
 ASPIRE ENERGY
 300 TRACY BIRDGE ROAD
 ORRVILLE, OH 44667
 330.682.7726

COMMUNICATION
 FRONTIER COM
 83 TOWNSEND AVENUE
 NORWALK, OH 44857
 419.744.3613

COUNTY
 LORAIN COUNTY ENGINEER
 247 HADAWAY STREET
 ELYRIA, OH 44035
 440.329.5586

ELECTRIC
 LORAIN-MEDINA RURAL ELECTRIC
 P.O. BOX 158
 WELLINGTON, OH 44090
 800.222.8673

GAS
 COLUMBIA GAS OF OHIO
 1021 N MAIN STREET
 MANSFIELD, OH 44903
 419.528.1137

WATER
 RLCWA
 42401 S.R. 303
 LAGRANGE, OH 44050
 440.355.6060

CITY
 VILLAGE OF WELLINGTON
 115 WILLARD MEMORIAL SQUARE
 WELLINGTON, OH 44090
 440.647.4626

THE AFOREMENTIONED UTILITY COMPANIES AND AGENCIES HAVE VARIOUS FACILITIES IN THE AREA THAT WILL REMAIN IN PLACE DURING CONSTRUCTION.

EXTREME CAUTION SHOULD BE EXERCISED IN AREAS WITH UTILITIES. SECTIONS 105.07 AND 107.16 OF THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIALS SPECIFICATIONS REQUIRE, AMONG OTHER THINGS, THAT THE CONTRACTOR COOPERATE WITH ALL UTILITIES LOCATED WITHIN THE LIMITS OF THIS CONSTRUCTION PROJECT AND TAKE RESPONSIBILITY FOR THE PROTECTION OF THE UTILITY PROPERTY AND SERVICES.

ROUTINE MAINTENANCE

BETWEEN THE TIME THAT BIDS ARE TAKEN AND THE START OF CONSTRUCTION, THE MAINTAINING AGENCY MAY ENTER UPON THE PROJECT AND PERFORM ROUTINE MAINTENANCE SUCH AS CRACK SEALING, PATCHING, AND BERM AND SHOULDER REPAIR. THE EFFECTS, IF ANY, OF THE PERFORMANCE OF ROUTINE MAINTENANCE SHALL BE CONSIDERED AS INHERENT IN WORK OF THE CHARACTER PROVIDED FOR IN THE PLAN AND THE RESULTING CONDITIONS SHALL NOT BE CONSIDERED AS DIFFERING MATERIALLY FROM THOSE EXISTING AT THE TIME BIDS WERE TAKEN.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

SEQUENCE OF WORK

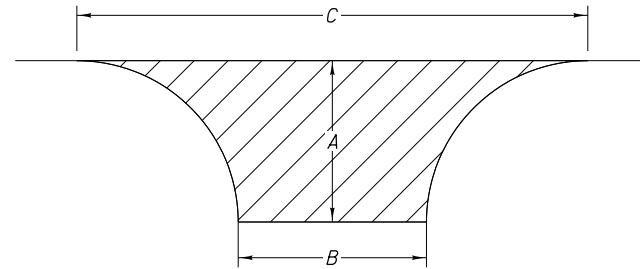
COORDINATE WITH THE ODNR PERSONNEL AND FINDLEY STATE PARK MANAGER FOR SPECIFIC DATES RELATED TO THE ALL RESTRICTIONS PRIOR TO BEGINNING CONSTRUCTION.

INTERSECTIONS AND DRIVES

RURAL-INTERSECTIONS SHALL BE PLANED AND PAVED TO THE END OF THE RADII OR AS DIRECTED BY THE ENGINEER. (TO PROVIDE A SMOOTH TRANSITION BETWEEN THE TWO HIGHWAYS, AND TO ELIMINATE WATER POCKETS).

ANY HAZARD OR UNSAFE CONDITION RESULTING FROM THE ABOVE WORK MUST BE CORRECTED IMMEDIATELY. THE CONTRACTOR IS REMINDED OF SECTIONS 105.01, 107.07 & 614.02A OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

THE PAVING DIMENSIONS FOR THE INTERSECTIONS ARE SHOWN IN THE CHART BELOW.



INTERSECTION NAME	A (FT.)	B (FT.)	C (FT.)	AREA (SY)
SP-1				
SP-1A	INCLUDED IN SP-1A PAVING			
SUBTOTAL				0
SP-2				
SP-2A	INCLUDED IN SP-2A PAVING			
SUBTOTAL				0
SP-3				
SP-16	INCLUDED IN SP-16 PAVING			
SP-17	INCLUDED IN SP-17 PAVING			
P-17A	15	23	30	42
P-17B	15	23	32	43
SP-9	INCLUDED IN SP-9 PAVING			
SUBTOTAL				85
SP-9				
SP-9A	INCLUDED IN SP-9A PAVING			
SP-4	20	45	83	128
SP-3	INCLUDED IN SP-3 PAVING			
SP-1A	INCLUDED IN SP-1A PAVING			
SP-1	INCLUDED IN SP-1 PAVING			
SP-2	INCLUDED IN SP-2 PAVING			
SP-2A	INCLUDED IN SP-2A PAVING			
SP-19	28	18	57	96
P-18	33	19	42	98
SP-20	36	21	23	87
P-19	14	25	61	58
SUBTOTAL				467
SP-17				
P-3A	10	38	67	53
P-3B	10	14	31	22
P-3C	IRREGULAR SHAPE			46
SUBTOTAL				121
TOTAL INTERSECTION AREA				673

PAVEMENT CORING INFORMATION

CORE #	ROUTE	APPROXIMATE LOCATION	LOCATION DESCRIPTION	THICKNESS (INCHES)	MATERIAL
C-1	SP-1	SLM 0.06	MIDPOINT - PARK ENTRANCE DRIVE	8.50	ASPHALT
C-2	SP-3	SLM 0.75	MIDPOINT	4.50	ASPHALT
C-3	SP-17	SLM 0.06	PARK OFFICE ROAD	5.25	ASPHALT
C-4	SP-3	SLM 0.32	EAST OF BRIDGE	6.00	ASPHALT
C-5	SP-16	SLM 0.03	NATURE CENTER ACCESS DRIVE	4.50	ASPHALT
C-6	SP-2A	SLM 0.05	EXIT RAMP	7.00	ASPHALT
C-7	SP-9	SLM 1.35	NORTH DAM ACCESS ROAD	6.00	ASPHALT
C-8	SP-9	SLM 0.11	MIDPOINT	7.00	ASPHALT

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE

THE INTENT OF THE PLANING IS TO MILL THE DEPTH SPECIFIED ON THE PAVEMENT AND SHOULDER DATA SHEET AT THE CENTER OF PAVEMENT AT NON-CURBED AREAS. THE PAVEMENT SLOPE SHALL BE 0.010 MINIMUM AND 0.016 PREFERRED, CONTINUOUS BETWEEN THE CROWN AND THE PROPOSED EDGELINE/SHOULDER. THE MILLING DEPTH SHALL BE CONTROLLED FROM THE CENTER OF PAVEMENT IN CONFORMANCE WITH THE ABOVE GUIDELINES.

SPECIAL ATTENTION SHALL BE GIVEN TO SUPERELEVATED CURVES. THE SUPERELEVATION SHALL BE MAINTAINED AND/OR RESTORED, IF NECESSARY, AS DIRECTED BY THE ENGINEER. IF THERE IS NO INFORMATION IN THE PLANS TO CHANGE THE SUPERELEVATION, THE INTENT IS TO MAINTAIN THE EXISTING SUPERELEVATION.

THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE TO ALL CATCH BASINS AND INLETS.

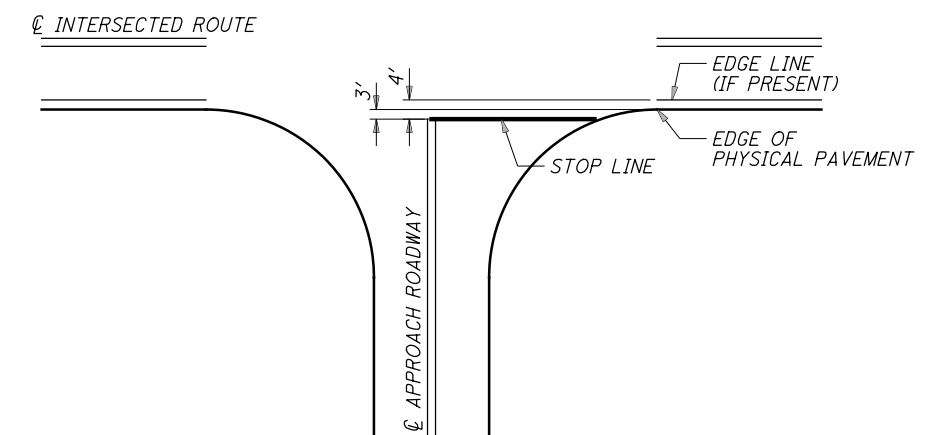
THE PROGRESSION OF THE PLANING SHALL PROCEED IN SUCH A MANNER THAT NORMAL TRAFFIC WILL NOT BE REQUIRED TO RUN OVER THE PLANED ROADWAY SURFACE MORE THAN FOURTEEN (14) CALENDAR DAYS. FOR EACH CALENDAR DAY BEYOND THE 14 DAYS THAT THE ROADWAY REMAINS EXPOSED TO THE PLANED SURFACE, THE CONTRACTOR WILL BE ASSESSED A DISINCENTIVE FEE OF \$400 PER DAY.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT PLANING, ASPHALT CONCRETE. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE.

THE PAVEMENT AND SHOULDER DATA SHEET CONTAINS QUANTITIES FOR PAVEMENT PLANING LABELED AS 1.50-0.75 INCHES IN DEPTH. THIS AREA IS TO BE USED AS DIRECTED BY THE ENGINEER AS TAPER SECTIONS TO MEET EXISTING GRADE AT AREAS SUCH AS CROSS ROADS, ADJACENT TO BUILDINGS, ETC. WHERE A BUMP IN THE PAVEMENT IS NOT DESIRABLE.

STOP BAR PLACEMENT DETAILS

AT NORMAL STOP CONTROLLED INTERSECTIONS, THE STOP BAR SHOULD BE PLACED 4 FEET FROM THE EDGE LINE OF THE INTERSECTING ROADWAY IN ORDER TO ACHIEVE MAXIMUM INTERSECTION SIGHT DISTANCE. IF NO EDGE LINE IS TO BE PLACED ON THE INTERSECTED ROADWAY, PLACE THE STOP LINE 3 FEET FROM THE PHYSICAL EDGE OF PAVEMENT.



CALCULATED TPG CHECKED KCK
GENERAL NOTES
 LOR-FINDLEY SP-MAIN ROAD
 4
 13

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ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE)

ITEM 253 - PAVEMENT REPAIR

THESE ITEMS OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE. CORING HAS BEEN PERFORMED TO HELP DETERMINE THE COMPONENTS THAT MAY BE ENCOUNTERED DURING THIS ITEM OF WORK. THE PAVEMENT CORING INFORMATION IS LOCATED UNDER THE CORING DATA NOTE.

WITHIN AREAS THAT ARE TO BE PLANED, PAVEMENT REPAIR SHALL BE PERFORMED AFTER PAVEMENT PLANING. COMPLETE ALL PAVEMENT REPAIRS PRIOR TO PLACEMENT OF THE SURFACE COURSE. THE DEPTH OF REMOVAL SHALL BE SUFFICIENT TO REMOVE ALL DETRIMENTALLY DETERIORATED PAVEMENT AS MARKED AND APPROVED BY THE ENGINEER. REPLACEMENT MATERIAL SHALL BE ITEM 301 MATERIAL USING A PG64-22 BINDER AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PLANED PAVEMENT SURFACE. ALL AGGREGATE SHALL BE 100% LIMESTONE. THE CONTRACTOR SHALL BE CAPABLE OF PERFORMING REPAIRS AT A MINIMUM WIDTH OF 2 FEET.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES, ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE) IS CONSIDERED TO BE A MAXIMUM DEPTH OF 4 INCHES. ITEM 253 PAVEMENT REPAIR IS CONSIDERED ALL OTHER REPAIRS. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER CUBIC YARD, BY TICKET WEIGHT CONVERSION, OF ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE) OR ITEM 253 - PAVEMENT REPAIR. THE FOLLOWING ESTIMATED QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

FOR INFORMATIONAL PURPOSES ONLY, IT IS ESTIMATED THAT 85% OF THE TOTAL PAVEMENT REPAIRS ARE CONSIDERED LONGITUDINAL REPAIRS AND 15% OF THE TOTAL PAVEMENT REPAIRS ARE CONSIDERED TRANSVERSE.

SP-9, SP-9A, SP-2A, SP-2, SP-1, SP-1A, SP-3, SP-17, AND SP-16

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE)	491	CU YD
ITEM 253 - PAVEMENT REPAIR	25	CU YD

SP-4

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE)	26	CU YD
ITEM 253 - PAVEMENT REPAIR	5	CU YD

TOTAL CARRIED TO GENERAL SUMMARY

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE)	517	CU YD
ITEM 253 - PAVEMENT REPAIR	30	CU YD

ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 9.5 MM. TYPE A (448), AS PER PLAN (PG64-22)

ALL OPEN TRANSVERSE JOINTS SHALL BE TAPERED TO MEET EXISTING PAVEMENT BEFORE INTRODUCING TRAFFIC. A "BUMP" SIGN (W8-1-36) SHALL BE ERECTED ON EACH SIDE OF TRANSVERSE JOINTS LEFT OPEN OVER NIGHT, INCLUDING A SPEED ADVISORY SIGN. THESE SIGNS SHALL BE REMOVED IMMEDIATELY AFTER JOINT HAS BEEN CLOSED. PLACEMENT OF SIGNS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

CARE SHALL BE TAKEN TO MATCH EXISTING PAVEMENT ELEVATIONS AT EXISTING PAVED BERMS, DRIVES, INTERSECTIONS, ETC.

REQUIREMENTS OF 442 APPLY EXCEPT AS FOLLOWS:
MIX DESIGN: FOR N_{des} USE 50 GYRATIONS, FOR N_{max} USE 75 GYRATIONS.
MINIMUM TOTAL PG BINDER CONTENT IS 6.3 PERCENT.
MINIMUM VIRGIN PG BINDER CONTENT IS 5.2 PERCENT.
CHOOSE OPTIMUM BINDER CONTENT AT AIR Voids OF 3.5%.
USE A PG 64-22 BINDER.
AGGREGATE SHALL BE 100% LIMESTONE.

WHEN AN AGGREGATE SOURCE IS SPECIALLY DESIGNATED WITH AN SR ON THE AGGREGATE GRAVITY LIST DO NOT USE THE AGGREGATE EXCEPT AS ALLOWED FOR MEDIUM TRAFFIC IN THE GUIDELINES FOR MAINTAINING ADEQUATE PAVEMENT FRICTION IN SURFACE PAVEMENT.
QUALITY CONTROL: DO NOT PERFORM N_{max} IN QUALITY CONTROL TESTING. DO NOT TAKE EXTRA ASPHALT BINDER SAMPLES AS OUTLINED IN CMS 442.05.

AGGREGATES USED ON PARK ROADS

ALL AGGREGATES USED IN ANY PAVEMENT MATERIALS SHALL BE 100% LIMESTONE. ALL REQUIREMENTS REGARDING AGGREGATE FOUND IN THE C&MS APPLY.

GUARDRAIL AT SP-3 STRUCTURE

GUARDRAIL MAY BE REMOVED IN ORDER TO PLANE AND PAVE FULL WIDTH OVER THE STRUCTURE, DEPENDING ON THE NEEDS OF THE CONTRACTOR. THE GUARDRAIL RAIL ELEMENT, INCLUDING ALL PERTINENT HARDWARE, MAY BE REMOVED FOR REUSE DURING TIMES WHEN WORK IS ACTIVELY BEING COMPLETED ON THE ADJACENT PAVEMENT. AT NO TIME IS TRAFFIC PERMITTED TO FLOW OVER THE STRUCTURE IN THE LANE ADJACENT TO THE REMOVED GUARDRAIL. TRAFFIC MUST BE MAINTAINED IN THE LANE FURTHEST FROM THE REMOVED GUARDRAIL. AT NO TIME IS IT PERMISSIBLE FOR THE GUARDRAIL ON BOTH SIDES OF THE STRUCTURE TO BE REMOVED CONCURRENTLY. THE GUARDRAIL MUST BE RE-ERECTED TO THE SATISFACTION OF THE ENGINEER WHEN WORK IS NOT ACTIVELY BEING COMPLETED OVER THE STRUCTURE, INCLUDING OVERNIGHT. ALL GUARDRAIL MUST BE RETURNED TO ITS CURRENT STATE OR BETTER AT THE COMPLETION OF ALL PAVING WORK OVER THE STRUCTURE.

ALL WORK INCLUDING ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THE ABOVE WORK SHALL BE INCLUDED IN THE CONTRACT LUMP SUM BID PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

INTERIM COMPLETION DATE

IF NECESSARY TO EXTEND CONSTRUCTION PAST JUNE 2020 DUE TO WEATHER, MATERIAL SUPPLY, OR ANY OTHER DELAY IN CONSTRUCTION, SUSPEND OPERATIONS DURING THE MONTH OF JULY 2020, EXCEPT AS COORDINATED WITH THE FINDLEY STATE PARK MANAGER. COMPLY WITH ALL REQUESTS OF THE FINDLEY STATE PARK MANAGER FOR STOPPAGE OF WORK AND RESTRICTIONS ON WORK LOCATIONS WITHIN THIS TIME PERIOD. CONSTRUCTION ACTIVITIES MAY RESUME NORMAL OPERATION ON AUGUST 1ST, 2020. FOR EVERY DAY THAT THE CONTRACTOR PERFORMS WORK IN THE MONTH OF JULY WITHOUT THE APPROVAL OF THE FINDLEY STATE PARK MANAGER, A DISINCENTIVE FEE OF \$1000 PER DAY WILL BE ASSESSED.

ITEM 617 - COMPACTED AGGREGATE

DO NOT USE ANY MATERIAL CONTAINING OR CONSISTING OF SLAG FOR USE AS COMPACTED AGGREGATE ON THIS PROJECT. ALL OTHER MATERIALS LISTED IN 703.18, WITH A PREFERENCE FOR RAP, MAY BE CONSIDERED FOR USE.

ITEM 408 - PRIME COAT, AS PER PLAN

THE CONTRACTOR SHALL APPLY ONE COAT OF MC-70 (AS PER SECTION 702) AT A RATE OF 0.40 GAL/SY TO THE COMPLETED AGGREGATE SHOULDER (ITEM 617) AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID BITUMINOUS MATERIAL ONTO THE EDGE OF PAVEMENT OR EDGE LINE. ADDITIONALLY, THE CONTRACTOR SHALL PREVENT THE SPRAYING OF LIQUID BITUMINOUS MATERIAL ONTO THE SURROUNDING VEGETATION WHILE STILL ENSURING FULL COVERAGE OF THE COMPLETED COMPACTED AGGREGATE MATERIAL. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO 107.10 OF THE SPECIFICATIONS.

CALCULATED
TPG
CHECKED
KCK

GENERAL NOTES

LOR-FINDLEY
SP-MAIN ROAD

**ITEM 614 – MAINTAINING TRAFFIC (GENERAL)
(TEM 642-2)**

MAINTAIN ONE 10' LANE OF TRAFFIC AT ALL TIMES.

SUBMIT, IN WRITING, A SCHEDULE OF OPERATIONS TO THE ENGINEER AND RECEIVE APPROVAL BEFORE WORK IS STARTED ON THE PROJECT. PRIOR TO BEGINNING WORK, COORDINATE THE MAINTENANCE OF TRAFFIC OPERATIONS WITH THE LOCAL STATE HIGHWAY PATROL.

BUTT JOINTS

DO NOT CUT BUTT JOINTS AND ALLOW THEM TO BE LEFT OPEN TO TRAFFIC. FILL THE BUTT JOINTS WITH A TEMPORARY ASPHALT CONCRETE WEDGE USING ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC IN ACCORDANCE WITH THE TAPER RATES SET FORTH IN SCD BP-3.1.

ERECT AND MAINTAIN CONSTRUCTION "BUMP" (W8-1-36) AND "ADVISORY SPEED" (W13-1-24) SIGNS DURING THE PERIOD THE BUTT JOINT IS LEFT OPEN. PAYMENT FOR THESE SIGNS WILL BE MADE UNDER THE LUMP SUM BID PRICE FOR ITEM 614 MAINTAINING TRAFFIC.

**ITEM 614 – MAINTAINING TRAFFIC (CLOSING PARAGRAPH FOR NOTE)
(TEM 642-12)**

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES CURRENT EDITION WITH THE LATEST REVISIONS. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

**ITEM 614 – MAINTAINING TRAFFIC (LANE CLOSURE/REDUCTION REQUIRED)
(TEM 642-7)**

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

**ITEM 614 – MAINTAINING TRAFFIC (ESTIMATED QUANTITIES)
(TEM 642-9)**

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR MAINTENANCE OF TRAFFIC. INCLUDE THE COST FOR THE REMOVAL OF ALL MAINTENANCE OF TRAFFIC MATERIALS IN THE CONTRACT BID PRICE FOR EACH ITEM BELOW. REMOVE THE MATERIALS AT THE DIRECTION OF THE ENGINEER WHEN NO LONGER OPERATIONALLY NEEDED.

ITEM 614 – ASPHALT CONCRETE FOR MAINTAINING TRAFFIC
10 CU YD

**ITEM 614 – MAINTAINING TRAFFIC (SIGNS AND BARRICADES)
(TEM 642-11)**

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS AND SIGN SUPPORTS, AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AND TYPE III BARRICADES OF THE TYPE AND LOCATION AS PER THE ENGINEER.

**NOTIFICATION OF TRAFFIC RESTRICTIONS
(TEM 642-58)**

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS AND PIO*
RAMP AND/OR ROAD CLOSURES	2 WEEKS OR GREATER	21 CALENDAR DAYS
	12 HOURS TO 2 WEEKS	14 CALENDAR DAYS
	12 HOURS OR LESS	4 BUSINESS DAYS
LANE CLOSURES AND RESTRICTIONS	2 WEEKS OR GREATER	14 CALENDAR DAYS
	LESS THAN 2 WEEKS	5 BUSINESS DAYS
START OF CONSTRUCTION AND TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS

* - PRIOR TO CLOSURE DATE, UNLESS NOTED OTHERWISE

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ODNR CONTACT INFORMATION

REFER TO THE FOLLOWING CONTACT INFORMATION WHEN OBTAINING INFORMATION FROM THE ODNR ENGINEER AND/OR FINDLEY STATE PARK MANAGER:

RYAN BERTANI, EI (PREFERRED)
PROJECT ENGINEER
2045 MORSE ROAD, BUILDING E-3
COLUMBUS, OH 43229
O: 614.265.6717
M: 614.558.8921

PATTI BARNET (SECONDARY)
ASSISTANT PROJECT MANAGER
2045 MORSE ROAD, BUILDING E-3
COLUMBUS, OH 43229
O: 614.265.6508
M: 614.561.9184

ROCKY CARPENTER (FOR DAILY COORDINATION)
FINDLEY STATE PARK MANAGER
25381 STATE ROUTE 58
WELLINGTON, OH 44090
O: 440.647.5749 EXT 100
M: 330.203.6091

ADVISE THESE INDIVIDUALS OF THE DATE, TIME, AND LOCATION OF THE PRECONSTRUCTION MEETING A MINIMUM OF 14 DAYS PRIOR TO THE MEETING. IN ADDITION, ADVISE THESE INDIVIDUALS A MINIMUM OF 30 DAYS PRIOR TO THE ANTICIPATED BEGINNING, SUSPENSION, RESUMPTION, AND COMPLETION OF CONSTRUCTION ON THIS PROJECT, RESPECTIVELY. COORDINATE THE LOCATION OF EQUIPMENT AND MATERIAL STORAGE AND CONTRACTOR EMPLOYEE VEHICLE PARKING A MINIMUM OF 5 DAYS PRIOR TO MAKING THESE IMPACTS. THE ABOVE LISTED INDIVIDUALS SHALL BE INVITED TO ALL PROJECT UPDATE MEETINGS. COORDINATE ALL WORK ON A MINIMUM OF A WEEKLY BASIS WITH THE FINDLEY STATE PARK MANAGER, OR MORE FREQUENTLY IF REQUESTED. COMPLY WITH ANY REQUESTS FROM THE ABOVE LISTED ODNR PERSONNEL.

CALCULATED
TPG
CHECKED
KCK

MAINTENANCE OF TRAFFIC

LOR-FINDLEY
SP-MAIN ROAD

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SHEET NUM.								PART.		ALT	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE	CALCULATED	TPG	CHECKED	KCK
5	6	8	9	10	11	12		01/NFP/ PV	(X)		EXT	TOTAL		SHEET		NO.				
														ROADWAY						
					66			66			202	23000	66	SY	PAVEMENT REMOVED					
				2				2			202	35100	2	FT	PIPE REMOVED, 24" AND UNDER					
				4	119			123			203	10000	123	CY	EXCAVATION					
				48				48			203	20000	48	CY	EMBANKMENT					
		6.56						6.56			209	60500	6.56	MILE	LINEAR GRADING					
														DRAINAGE						
				5				5			611	01800	5	FT	8" CONDUIT, TYPE B					
														EROSION CONTROL						
				2				2			659	00100	2	EACH	SOIL ANALYSIS TEST					
				10				10			659	00300	10	CY	TOPSOIL					
				86				86			659	10000	86	SY	SEEDING AND MULCHING					
				5				5			659	14000	5	SY	REPAIR SEEDING AND MULCHING					
				5				5			659	15000	5	SY	INTER-SEEDING					
				0.02				0.02			659	20000	0.02	TON	COMMERCIAL FERTILIZER					
				0.02				0.02			659	31000	0.02	ACRE	LIME					
				1				1			659	35000	1	MGAL	WATER					
								5,000			832	30000	5,000	EACH	EROSION CONTROL					
														PAVEMENT						
517								517			251	01042	517	CY	PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE)					
					60			60			252	01500	60	FT	FULL DEPTH PAVEMENT SAWING					
30								30			253	02000	30	CY	PAVEMENT REPAIR					
		39,320						39,320			254	01000	39,320	SY	PAVEMENT PLANING, ASPHALT CONCRETE (0.75")					
		211						211			254	01600	211	SY	PATCHING PLANED SURFACE					
		3,556						3,556			407	10000	3,556	GAL	TACK COAT					
		2,963						2,963			408	10001	2,963	GAL	PRIME COAT, AS PER PLAN					5
		1,655						1,655			442	10501	1,655	CY	ASPHALT CONCRETE SURFACE COURSE, 9.5 MM, TYPE A (448), AS PER PLAN (PG64-22) (1.50")					5
		625			130			755			617	10100	755	CY	COMPACTED AGGREGATE					
														TRAFFIC CONTROL						
				5.92				5.92			642	00100	5.92	MILE	EDGE LINE, 4", TYPE 1 (WHITE)					
				0.46				0.46			642	00100	0.46	MILE	EDGE LINE, 4", TYPE 1 (YELLOW)					
				3.13				3.13			642	00300	3.13	MILE	CENTER LINE, TYPE 1					
														TRAFFIC CONTROL ALTERNATES						
				311				311	X		642	00500	311	FT	STOP LINE, TYPE 1 (ALTERNATE 1)					
				10				10	X		642	01322	10	EACH	WRONG WAY ARROW, TYPE 1 (ALTERNATE 1)					
				311				311	X		644	00500	311	FT	STOP LINE (ALTERNATE 2)					
				10				10	X		644	01360	10	EACH	WRONG WAY ARROW (ALTERNATE 2)					
														STRUCTURE REPAIR (SFN: 4726235)						
						34		34			846	00111	34	CF	POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM, AS PER PLAN					12
														MAINTENANCE OF TRAFFIC						
	10							10			614	13000	10	CY	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC					
				3.16				3.16			614	21100	3.16	MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT					
				311				311			614	26200	311	FT	WORK ZONE STOP LINE, CLASS I, 642 PAINT					
														INCIDENTALS						
								LS			614	11000	LS		MAINTAINING TRAFFIC					
								LS			623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING					
								LS			624	10000	LS		MOBILIZATION					

GENERAL SUMMARY

LOR-FINDLEY
SP-MAIN ROAD

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PLAN SPLIT	COUNTY	ROUTE	LOG POINT TO LOG POINT		LENGTH		AVERAGE WIDTH	TYPICAL SECTION	PAVEMENT AREA	254			407		442		AGGREGATE SHOULDER PROPOSED WIDTH		AGGREGATE SHOULDER AREA	209		617		408	
					MILE	FEET				PAVEMENT PLANING, ASPHALT CONCRETE	PATCHING PLANED SURFACE	TACK COAT	ASPHALT CONCRETE SURFACE COURSE, 9.5 MM, TYPE A (448), AS PER PLAN	SL	SR	LINEAR GRADING	COMPACTED AGGREGATE	PRIME COAT, AS PER PLAN							
			SLM		FT	SO YD	INCHES	SO YD	SO YD	GAL/SY	GALLON	INCH	CU YD	FT	FT	SO YD	MILE	CU YD	GAL/SY	GALLON					
01/NFP/PV	LOR	SP-1	0.00	0.01	0.01	25	39.50	1	110	1.50-0.75	110	1	0.09	10	1.50	5	2.0	2.0	11.12	0.02	1	0.40	5		
01/NFP/PV	LOR	SP-1	0.01	0.08	0.07	367	18.00	1	734	0.75	734	4	0.09	67	1.50	31	2.0	2.0	163.12	0.14	14	0.40	66		
01/NFP/PV	LOR	SP-1	0.08	0.09	0.01	25	40.50	1	113	0.75	113	1	0.09	11	1.50	5	2.0	2.0	11.12	0.02	1	0.40	5		
01/NFP/PV	LOR	SP-1A	0.00	0.07	0.07	338	12.00	2	451	0.75	451	3	0.09	41	1.50	19	2.0	2.0	150.23	0.14	13	0.40	61		
01/NFP/PV	LOR	SP-2	0.00	0.01	0.01	25	34.00	1	95	1.50-0.75	95	1	0.09	9	1.50	4	2.0	2.0	11.12	0.02	1	0.40	5		
01/NFP/PV	LOR	SP-2	0.01	0.06	0.05	235	18.00	1	470	0.75	470	3	0.09	43	1.50	20	2.0	2.0	104.45	0.10	9	0.40	42		
01/NFP/PV	LOR	SP-2	0.06	0.10	0.04	195	18.00	3	390	0.75	390	2	0.09	36	1.50	17	2.0	2.0	86.67	0.08	8	0.40	35		
01/NFP/PV	LOR	SP-2	0.10	0.11	0.01	25	39.50	1	110	0.75	110	1	0.09	10	1.50	5	2.0	2.0	11.12	0.02	1	0.40	5		
01/NFP/PV	LOR	SP-2A	0.00	0.09	0.09	442	11.00	2	541	0.75	541	3	0.09	49	1.50	23	2.0	2.0	196.45	0.18	17	0.40	79		
01/NFP/PV	LOR	SP-3	0.00	0.01	0.01	25	22.00	1	62	1.50-0.75	62	1	0.09	6	1.50	3	2.0	2.0	11.12	0.02	1	0.40	5		
01/NFP/PV	LOR	SP-3	0.01	0.33	0.32	1680	25.00	1	4,667	0.75	4667	24	0.09	421	1.50	195	2.0	2.0	746.67	0.64	63	0.40	299		
01/NFP/PV	LOR	SP-3	0.33	0.66	0.33	1705	26.00	1	4,926	0.75	4926	25	0.09	444	1.50	206	2.0	2.0	757.78	0.66	64	0.40	304		
01/NFP/PV	LOR	SP-3	0.66	0.99	0.33	1705	21.00	1	3,979	0.75	3979	20	0.09	359	1.50	166	2.0	2.0	757.78	0.66	64	0.40	304		
01/NFP/PV	LOR	SP-3	0.99	1.32	0.33	1705	21.00	1	3,979	0.75	3979	20	0.09	359	1.50	166	2.0	2.0	757.78	0.66	64	0.40	304		
01/NFP/PV	LOR	SP-3	EXTRA AREA FOR ANCILLARY PAVEMENT						85		0.75-1.50	85	1	0.09	8	1.50	4								
01/NFP/PV	LOR	SP-9	0.00	0.01	0.01	25	12.00	1	34	1.50-0.75	34	1	0.09	4	1.50	2	2.0	2.0	11.12	0.02	1	0.40	5		
01/NFP/PV	LOR	SP-9	0.01	0.04	0.03	113	12.00	1	151	0.75	151	1	0.09	14	1.50	7	2.0	2.0	50.23	0.06	5	0.40	21		
01/NFP/PV	LOR	SP-9	0.04	0.27	0.23	1213	21.00	1	2,831	0.75	2831	15	0.09	255	1.50	118	2.0	2.0	539.12	0.46	45	0.40	216		
01/NFP/PV	LOR	SP-9	0.27	0.54	0.27	1419	20.00	1	3,154	0.75	3154	16	0.09	284	1.50	132	2.0	2.0	630.67	0.54	53	0.40	253		
01/NFP/PV	LOR	SP-9	0.54	0.81	0.27	1419	20.00	1	3,154	0.75	3154	16	0.09	284	1.50	132	2.0	2.0	630.67	0.54	53	0.40	253		
01/NFP/PV	LOR	SP-9	0.81	1.08	0.27	1419	20.00	1	3,154	0.75	3154	16	0.09	284	1.50	132	2.0	2.0	630.67	0.54	53	0.40	253		
01/NFP/PV	LOR	SP-9	1.08	1.35	0.27	1394	20.00	1	3,098	0.75	3098	16	0.09	279	1.50	130	2.0	2.0	619.56	0.54	52	0.40	248		
01/NFP/PV	LOR	SP-9	1.35	1.36	0.01	25	20.00	1	56	0.75-1.50	56	1	0.09	6	1.50	3	2.0	2.0	11.12	0.02	1	0.40	5		
01/NFP/PV	LOR	SP-9	EXTRA AREA FOR ANCILLARY PAVEMENT						467		0.75-1.50	467	3	0.09	43	1.50	20								
01/NFP/PV	LOR	SP-9A	0.00	0.01	0.01	25	12.00	1	34	1.50-0.75	34	1	0.09	4	1.50	2	2.0	2.0	11.12	0.02	1	0.40	5		
01/NFP/PV	LOR	SP-9A	0.01	0.04	0.03	117	12.00		156	0.75	156	1	0.09	15	1.50	7	2.0	2.0	52	0.06	5	0.40	21		
01/NFP/PV	LOR	SP-16	0.00	0.06	0.06	266	27.00	1	798	0.75	798	4	0.09	72	1.50	34	2.0	2.0	118.23	0.12	10	0.40	48		
01/NFP/PV	LOR	SP-16	0.06	0.07	0.01	25	13.00	1	37	0.75-1.50	37	1	0.09	4	1.50	2	2.0	2.0	11.12	0.02	1	0.40	5		
01/NFP/PV	LOR	SP-17	0.00	0.01	0.01	25	27.50	1	77	1.50-0.75	77	1	0.09	7	1.50	4	2.0	2.0	11.12	0.02	1	0.40	5		
01/NFP/PV	LOR	SP-17	0.01	0.12	0.11	568	18.33	1	1,158	0.75	1158	6	0.09	105	1.50	49	2.0	2.0	252.45	0.22	22	0.40	101		
01/NFP/PV	LOR	SP-17	0.12	0.13	0.01	25	46.00		128	0.75	128	1	0.09	12	1.50	6	2.0	2.0	11.12	0.02	1	0.40	5		
01/NFP/PV	LOR	SP-17	EXTRA AREA FOR ANCILLARY PAVEMENT						121		0.75	121	1	0.09	11	1.50	6								
TOTALS CARRIED TO GENERAL SUMMARY									39320		39320	211		3556		1655				6.56		625		2963	

PAVEMENT AND SHOULDER DATA

LOR-FINDLEY
SP-MAIN ROAD

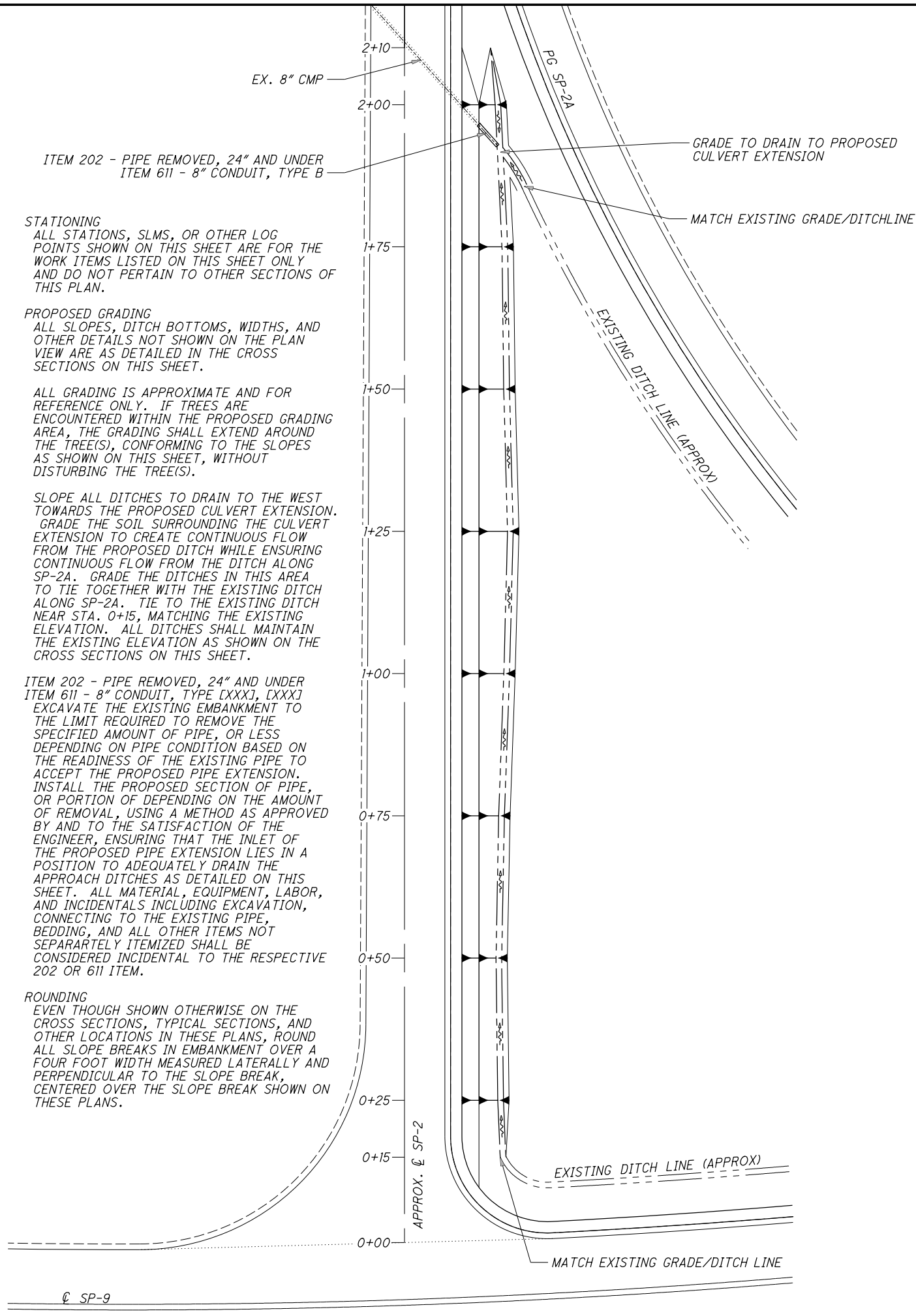
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PLAN SPLIT	COUNTY	ROUTE	LOG POINT TO LOG POINT		LENGTH		614		642						644 (ALTERNATE BID ITEMS)					
			SLM	MILE	FEET	WORK ZONE CENTER LINE, CLASS 1, 642 PAINT	WORK ZONE STOP LINE, CLASS 1, 642 PAINT	EDGE LINE, 4", TYPE 1 (WHITE)	EDGE LINE, 4", TYPE 1 (YELLOW)	CENTER LINE, TYPE 1 (SOLID LINE EQUIVALENT)	CENTER LINE, TYPE 1	WRONG WAY ARROW, TYPE 1	STOP LINE, TYPE 1	EDGE LINE, 4" (WHITE)	EDGE LINE, 4" (YELLOW)	CENTER LINE (SOLID LINE EQUIVALENT)	CENTER LINE	WRONG WAY ARROW	STOP LINE	
				MILE	FEET	MILE	FT	MILE	MILE	MILE	MILE	EACH	FT	MILE	MILE	MILE	MILE	EACH	FT	
01/NFP/PV	LOR	SP-1	0.00	0.09	0.09	450	0.09	35.00	0.09	0.09	0.180	0.09	2	35	0.09	0.09	0.18	0.09	2	35.00
01/NFP/PV	LOR	SP-1A	0.00	0.07	0.07	338	0.07		0.07	0.07	0.140	0.07	1		0.07	0.07	0.14	0.07	1	
01/NFP/PV	LOR	SP-2	0.00	0.09	0.09	450	0.09	40.00	0.09	0.09	0.180	0.09	2	40	0.09	0.09	0.18	0.09	2	40.00
01/NFP/PV	LOR	SP-2A	0.00	0.09	0.09	442	0.09		0.09	0.09	0.180	0.09	1		0.09	0.09	0.18	0.09	1	
01/NFP/PV	LOR	SP-3	0.00	1.30	1.30	6820	1.3	69.00	2.6		2.600	1.3		69	2.60		2.60	1.30		69.00
01/NFP/PV	LOR	SP-9	0.00	1.31	1.31	6885	1.31	52.00	2.62		2.620	1.31		52	2.62		2.62	1.31		52.00
01/NFP/PV	LOR	SP-9	1.31	1.34	0.03	142			0.03	0.03			1		0.03				1	
01/NFP/PV	LOR	SP-9A	0.00	0.03	0.03	142	0.03		0.03	0.03			1		0.03	0.03			1	
01/NFP/PV	LOR	SP-16	0.00	0.06	0.06	291	0.06	57.00	0.06	0.06	0.120	0.06	2	57	0.06	0.06	0.12	0.06	2	57.00
01/NFP/PV	LOR	SP-17	0.00	0.12	0.12	618	0.12	58.00	0.24		0.240	0.12		58	0.24		0.24	0.12		58.00
TOTALS CARRIED TO GENERAL SUMMARY							3.16	311	5.92	0.46	6.26	3.13	10	311	5.92	0.43	6.26	3.13	10	311

CALCULATED	KCK
CHECKED	TPG
PAVEMENT MARKINGS	
LOR-FINDLEY	
SP-MAIN ROAD	
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ITEM 202 - PIPE REMOVED, 24" AND UNDER
ITEM 611 - 8" CONDUIT, TYPE B

STATIONING
ALL STATIONS, SLMS, OR OTHER LOG
POINTS SHOWN ON THIS SHEET ARE FOR THE
WORK ITEMS LISTED ON THIS SHEET ONLY
AND DO NOT PERTAIN TO OTHER SECTIONS OF
THIS PLAN.

PROPOSED GRADING
ALL SLOPES, DITCH BOTTOMS, WIDTHS, AND
OTHER DETAILS NOT SHOWN ON THE PLAN
VIEW ARE AS DETAILED IN THE CROSS
SECTIONS ON THIS SHEET.

ALL GRADING IS APPROXIMATE AND FOR
REFERENCE ONLY. IF TREES ARE
ENCOUNTERED WITHIN THE PROPOSED GRADING
AREA, THE GRADING SHALL EXTEND AROUND
THE TREE(S), CONFORMING TO THE SLOPES
AS SHOWN ON THIS SHEET, WITHOUT
DISTURBING THE TREE(S).

SLOPE ALL DITCHES TO DRAIN TO THE WEST
TOWARDS THE PROPOSED CULVERT EXTENSION.
GRADE THE SOIL SURROUNDING THE CULVERT
EXTENSION TO CREATE CONTINUOUS FLOW
FROM THE PROPOSED DITCH WHILE ENSURING
CONTINUOUS FLOW FROM THE DITCH ALONG
SP-2A. GRADE THE DITCHES IN THIS AREA
TO TIE TOGETHER WITH THE EXISTING DITCH
ALONG SP-2A. TIE TO THE EXISTING DITCH
NEAR STA. 0+15, MATCHING THE EXISTING
ELEVATION. ALL DITCHES SHALL MAINTAIN
THE EXISTING ELEVATION AS SHOWN ON THE
CROSS SECTIONS ON THIS SHEET.

ITEM 202 - PIPE REMOVED, 24" AND UNDER
ITEM 611 - 8" CONDUIT, TYPE [XXX], [XXX]
EXCAVATE THE EXISTING EMBANKMENT TO
THE LIMIT REQUIRED TO REMOVE THE
SPECIFIED AMOUNT OF PIPE, OR LESS
DEPENDING ON PIPE CONDITION BASED ON
THE READINESS OF THE EXISTING PIPE TO
ACCEPT THE PROPOSED PIPE EXTENSION.
INSTALL THE PROPOSED SECTION OF PIPE,
OR PORTION OF DEPENDING ON THE AMOUNT
OF REMOVAL, USING A METHOD AS APPROVED
BY AND TO THE SATISFACTION OF THE
ENGINEER, ENSURING THAT THE INLET OF
THE PROPOSED PIPE EXTENSION LIES IN A
POSITION TO ADEQUATELY DRAIN THE
APPROACH DITCHES AS DETAILED ON THIS
SHEET. ALL MATERIAL, EQUIPMENT, LABOR,
AND INCIDENTALS INCLUDING EXCAVATION,
CONNECTING TO THE EXISTING PIPE,
BEDDING, AND ALL OTHER ITEMS NOT
SEPARATELY ITEMIZED SHALL BE
CONSIDERED INCIDENTAL TO THE RESPECTIVE
202 OR 611 ITEM.

ROUNDING
EVEN THOUGH SHOWN OTHERWISE ON THE
CROSS SECTIONS, TYPICAL SECTIONS, AND
OTHER LOCATIONS IN THESE PLANS, ROUND
ALL SLOPE BREAKS IN EMBANKMENT OVER A
FOUR FOOT WIDTH MEASURED Laterally AND
PERPENDICULAR TO THE SLOPE BREAK,
CENTERED OVER THE SLOPE BREAK SHOWN ON
THESE PLANS.

SEEDING		CUT SF	FILL SF	CUT CY	FILL CY
FT	SY				
8.38				0.13	1.17
10.05		0.47	4.21		
29.69				0.50	4.93
11.33		0.60	6.45		
31.92				0.56	6.63
11.66		0.61	7.87		
33.34				0.53	7.92
12.35		0.54	9.25		
33.36				0.52	7.94
11.67		0.58	7.90		
31.10				0.48	6.31
10.72		0.45	5.73		
24.90				0.42	5.02
7.21		0.46	5.12		
24.64				0.45	4.86
10.54		0.51	5.39		
14.63				0.23	2.49
85.52	MINOR GRID SIZE 1'-0" X 1'-0"			3.82	47.28

ITEM	QUANTITY	UNIT	DESCRIPTION
202	2	FT	PIPE REMOVED, 24" AND UNDER
203	4	CU YD	EXCAVATION
203	48	CU YD	EMBANKMENT
611	5	FT	8" CONDUIT, TYPE B
659	86	SQ YD	SEEDING AND MULCHING
659	0.02	TON	COMMERCIAL FERTILIZER
659	0.02	ACRE	LIME
659	1	M GALLON	WATER
659	5	SQ YD	REPAIR SEEDING AND MULCHING
659	5	SQ YD	INTERSEEDING
659	10	CU YD	TOPSOIL
659	2	EACH	SOIL ANALYSIS TEST

ALL QUANTITIES CARRIED TO GENERAL SUMMARY



22
5.5
0
HORIZONTAL SCALE
(PLAN VIEW ONLY)

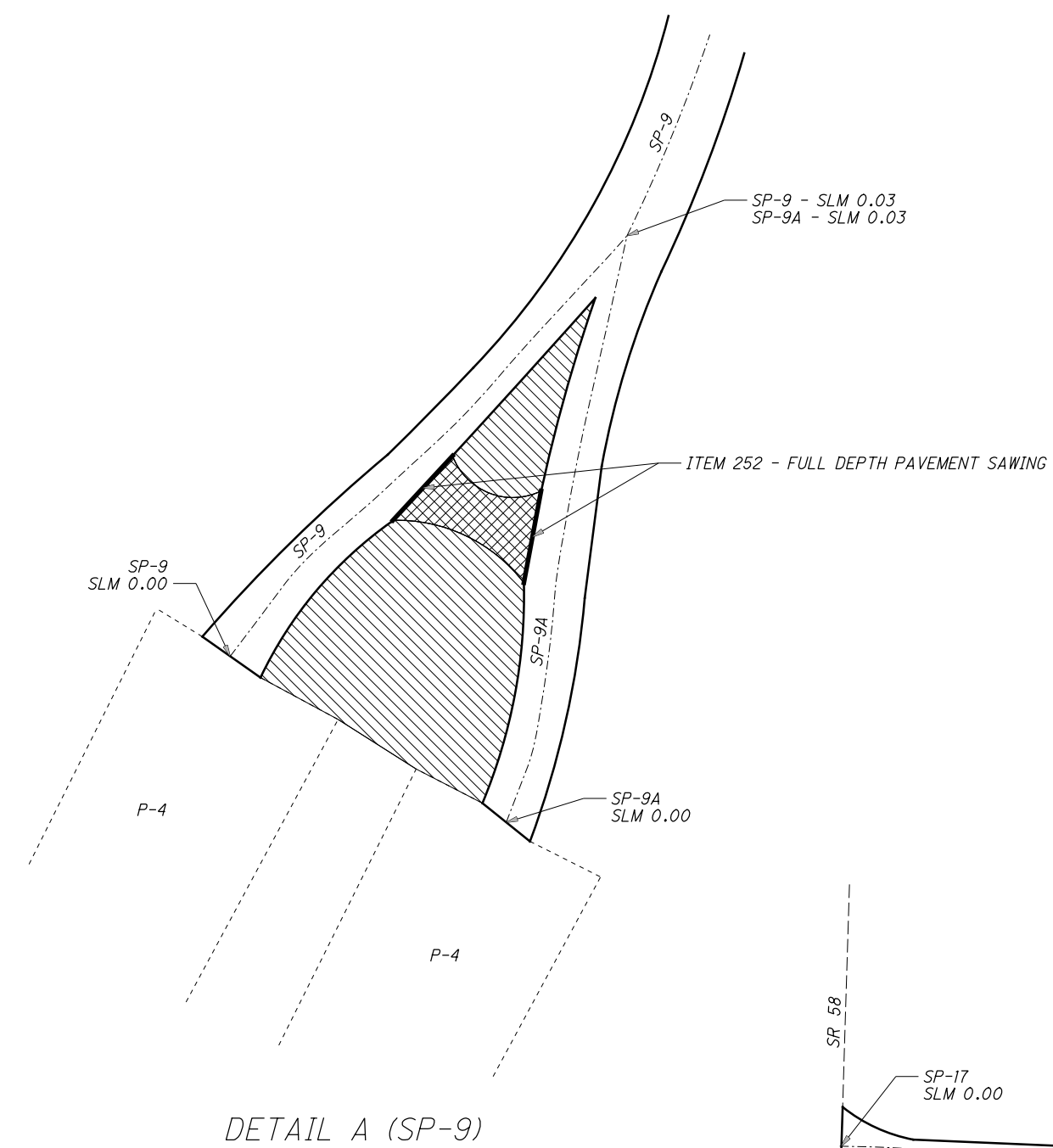
CALCULATED TPG CHECKED KCK

**SLOPE REPAIR DETAILS
ALONG SP-2**

**LOR-FINDLEY
SP-MAIN ROAD**

10
13

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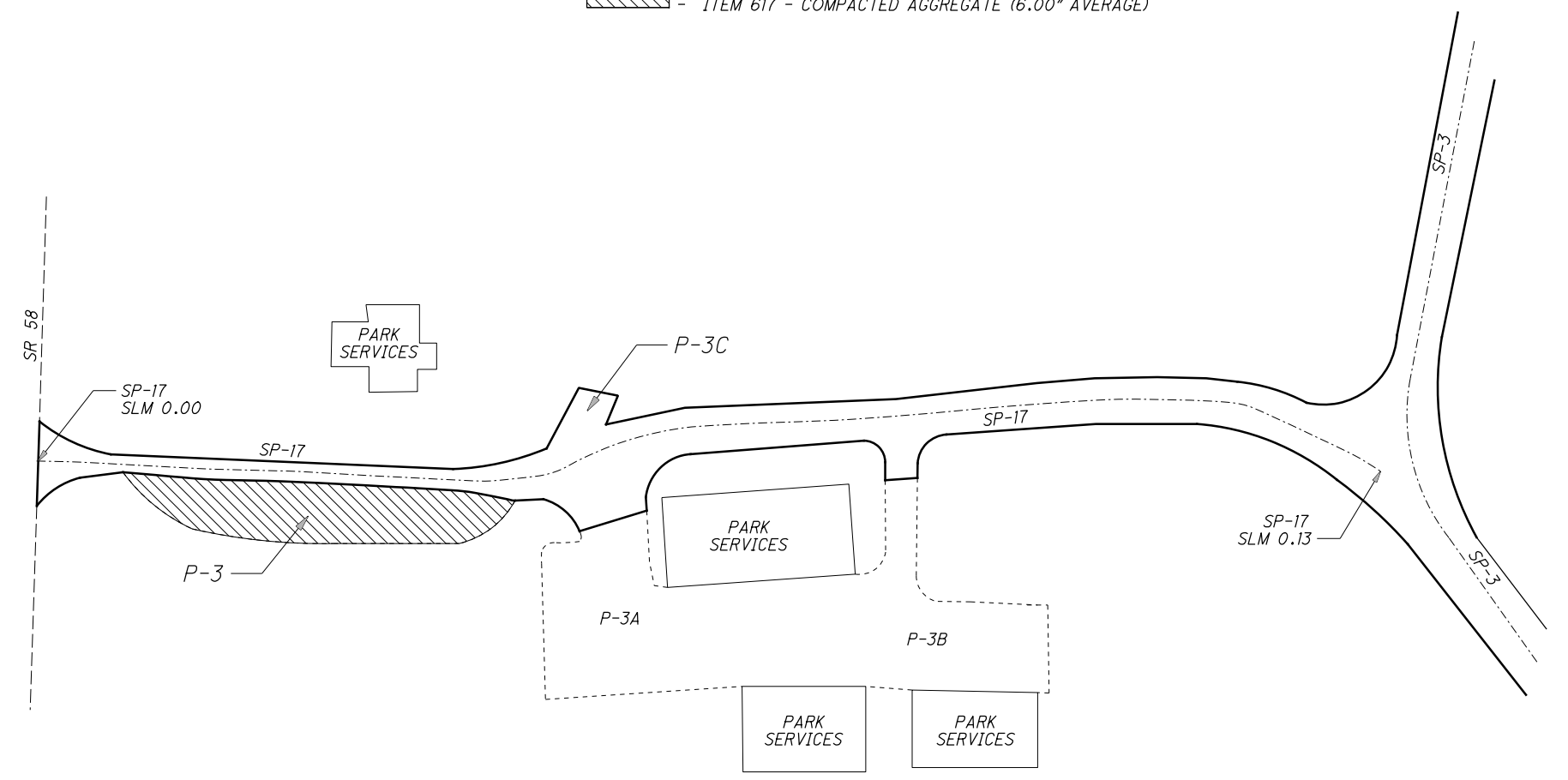
ESTIMATED QUANTITIES

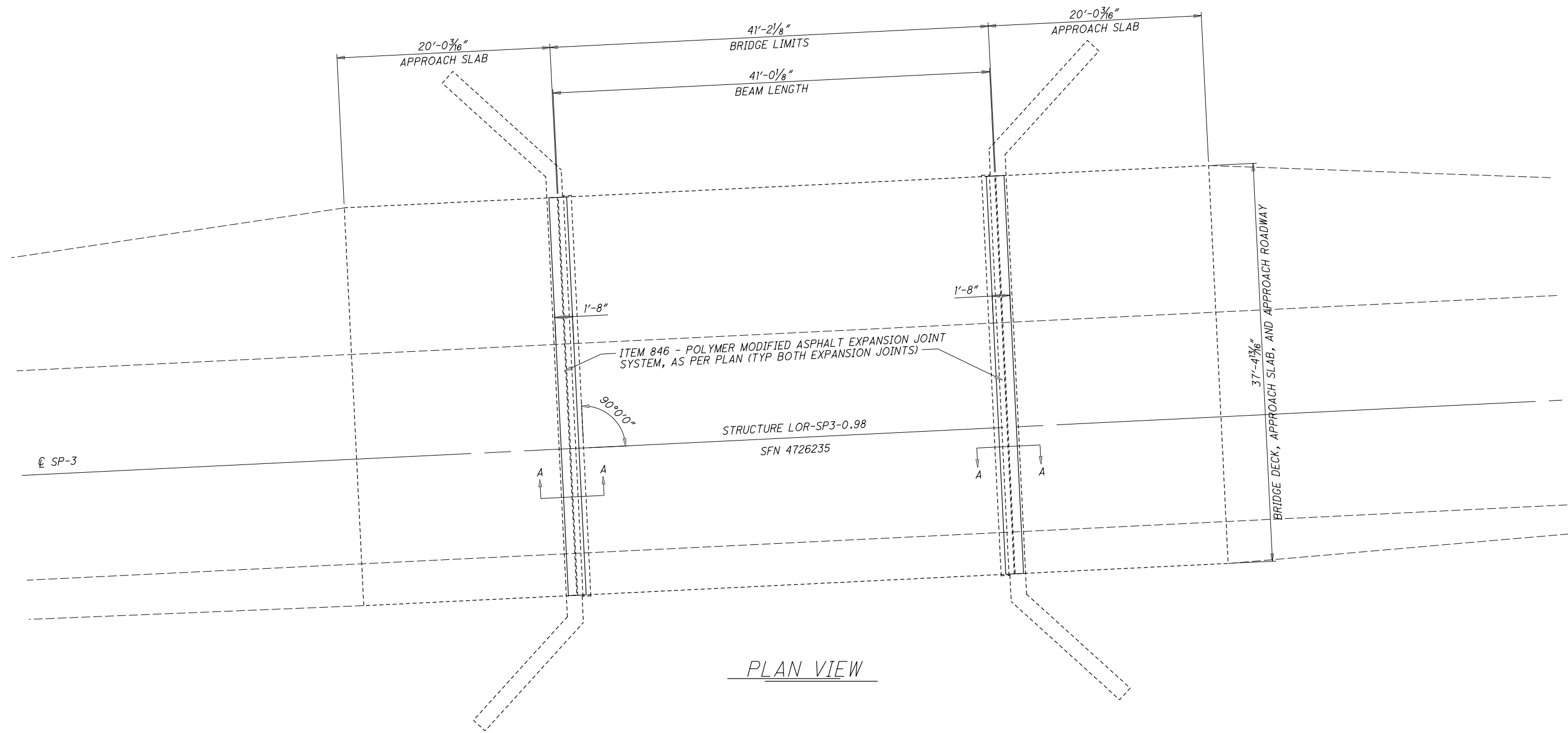
ITEM	QUANTITY	UNIT	DESCRIPTION
202	66	SQ YD	PAVEMENT REMOVED, ASPHALT
203	119	CU YD	EXCAVATION (6.00" AVERAGE)
252	60	FT	FULL DEPTH PAVEMENT SAWING
617	130	CU YD	COMPACTED AGGREGATE (6.00" AVERAGE)

ALL QUANTITIES CARRIED TO GENERAL SUMMARY

LEGEND

- ITEM 202 - PAVEMENT REMOVED, ASPHALT AND
ITEM 617 - COMPACTED AGGREGATE (6.00" AVERAGE)
- ITEM 203 - EXCAVATION (6.00" AVERAGE DEPTH) AND
ITEM 617 - COMPACTED AGGREGATE (6.00" AVERAGE)





PLAN VIEW

ESTIMATED QUANTITIES

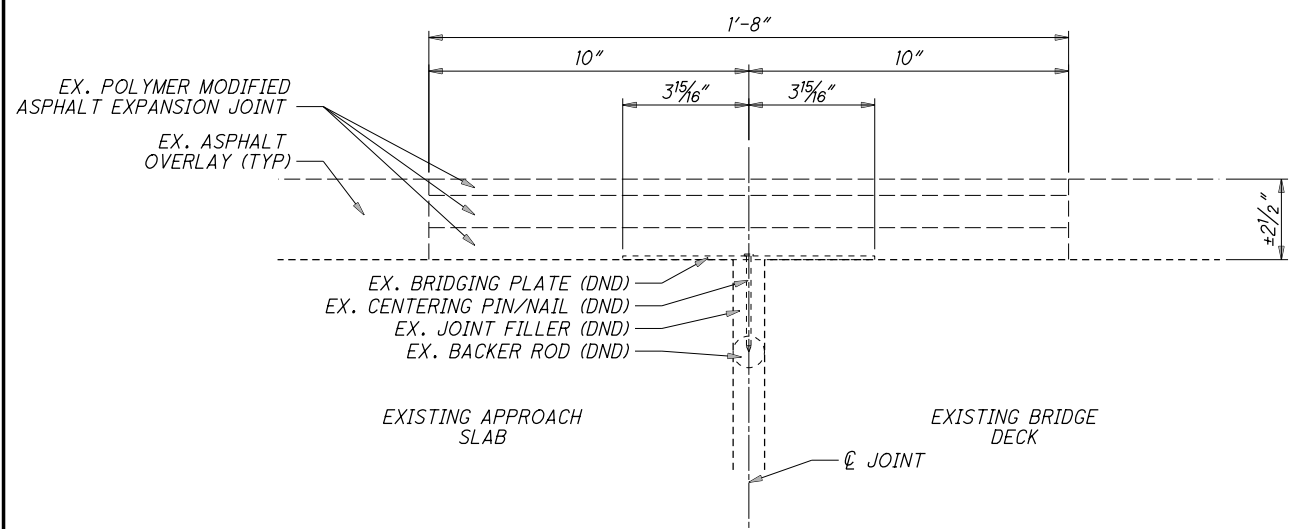
ITEM	QUANTITY	UNIT	DESCRIPTION
846	34	CU FT	POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM, AS PER PLAN

ALL QUANTITIES CARRIED TO GENERAL SUMMARY

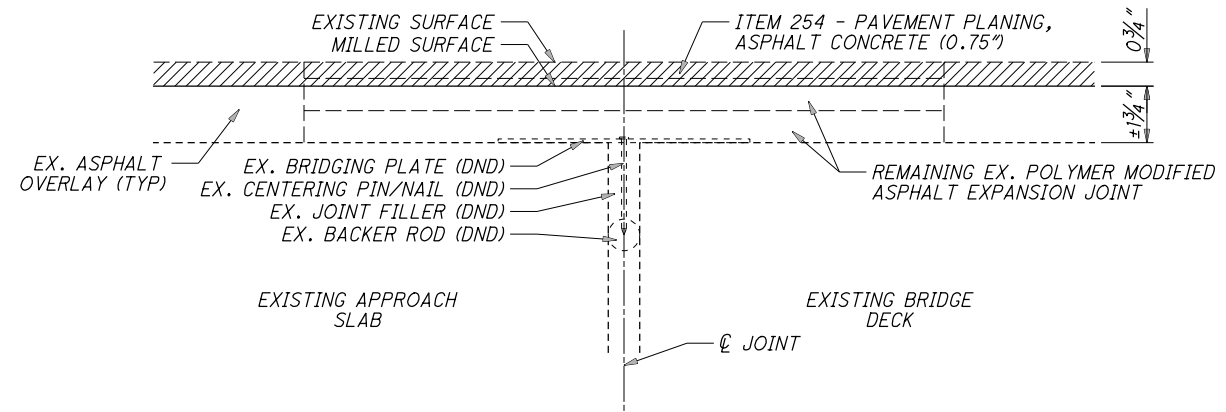
NOTES

- ITEMS OF WORK NOT ITEMIZED ON THIS SHEET
 ALL ITEMS OF WORK, INCLUDING PAVEMENT PLANING, ASPHALT CONCRETE PLACEMENT, AND OTHER ITEMS OF WORK NOT LISTED OR SPECIFICALLY ITEMIZED ON THIS SHEET ARE TO BE TREATED AS LISTED AND QUANTIFIED ELSEWHERE IN THESE PLANS.
- ITEM 846 - POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM, AS PER PLAN
 ALL ELEMENTS OF SUPPLEMENTAL SPECIFICATION 846 APPLY EXCEPT AS FOLLOWS:
- 1) THE BRIDGING PLATE, CENTERING PINS/NAILS, BACKER ROD, AND JOINT FILLER SHALL BE SALVAGED FROM THE EXISTING POLYMER MODIFIED EXPANSION JOINTS AND NOT DISTURBED DURING ANY PORTION OF THE REMOVAL OF THE EXISTING POLYMER MODIFIED ASPHALT MATERIAL OR INSTALLATION OF THE PROPOSED POLYMER MODIFIED ASPHALT MATERIAL. IF ANY SECTIONS OF ANY OF THESE ITEMS ARE REMOVED DURING THE REMOVAL OF THE EXISTING POLYMER MODIFIED ASPHALT MATERIAL, REPLACE THE REMOVED ITEM WITH A NEW EQUIVALENT CONFORMING TO THE REQUIREMENTS OF SUPPLEMENTAL SPECIFICATION 846 AT THE DIRECTION OF, AND TO THE SATISFACTION OF, THE ENGINEER.
 - 2) THE REMOVAL OF THE EXISTING POLYMER MODIFIED ASPHALT MATERIAL, INCLUDING ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THE REMOVAL SHALL BE CONSIDERED INCIDENTAL TO ITEM 846 - POLYMER MODIFIED ASPHALT EXPANSION JOINT SYSTEM, AS PER PLAN.
- ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THE ABOVE WORK SHALL BE INCLUDED IN THE CONTRACT BID PRICE PER CUBIC FOOT FOR ITEM 846 - POLYMER MODIFIED EXPANSION JOINT SYSTEM, AS PER PLAN.

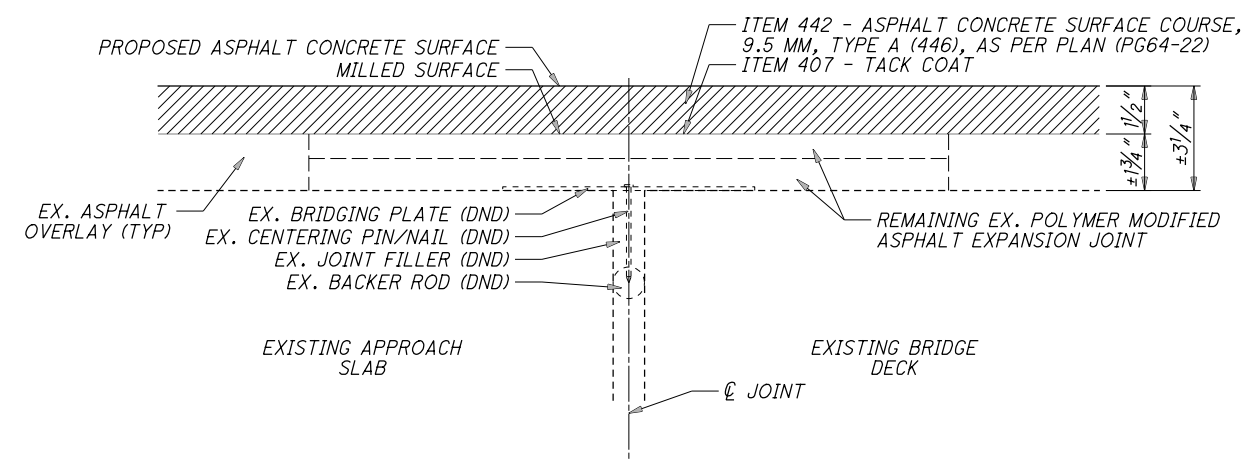
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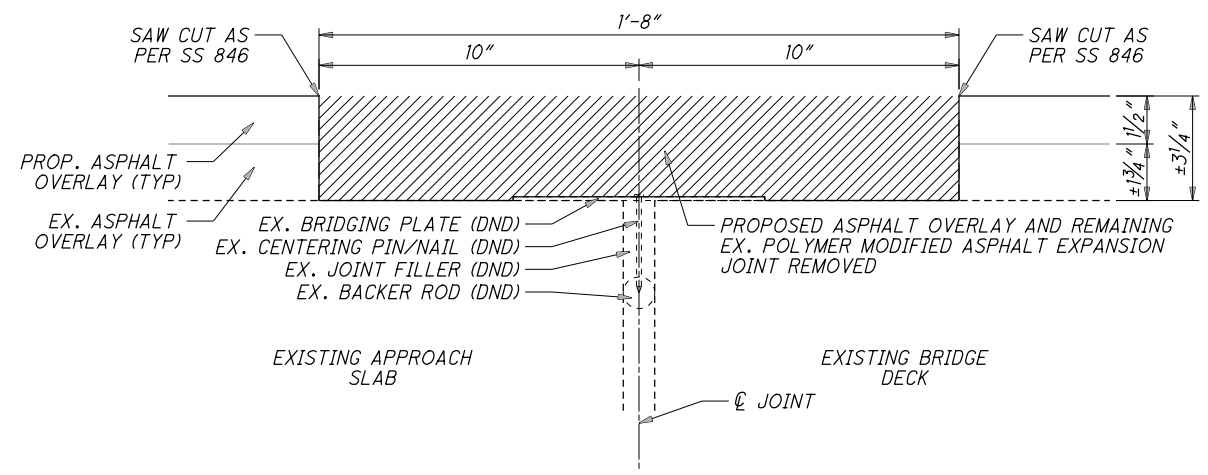
PHASE 1
SECTION A-A
EXISTING CONDITION



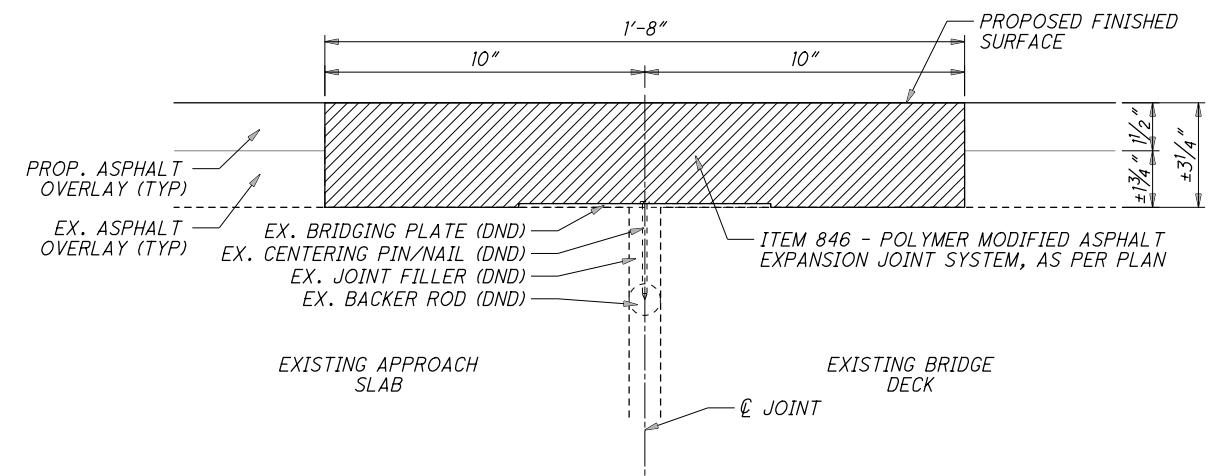
PHASE 2
SECTION A-A
MILLED SURFACE



PHASE 3
SECTION A-A
SURFACE COURSE PLACED



PHASE 4
SECTION A-A
EXISTING JOINT REMOVED



PHASE 5
SECTION A-A
PROPOSED JOINT PLACED

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