

# GENERAL

## UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

<u>AT LOR-2-0097R:</u>		
CABLE	ELECTRIC	
BREEZELINE	OHIO EDISON	
105 BLAZE INDUSTRIAL PKWY	1717 ASHLAND ROAD	
BEREA, OH 44017	MANSFIELD, OH 44905	
866.496.9669	419.521.6214	
<u>AT LOR-2-0742R:</u>		
ELECTRIC	SEWER	WATER
CITY OF AMHERST UTILITIES DEPT	CITY OF AMHERST UTILITIES DEPT	CITY OF AMHERST UTILITIES DEPT
480 PARK AVENUE	480 PARK AVENUE	480 PARK AVENUE
AMHERST, OH 44001	AMHERST, OH 44001	AMHERST, OH 44001
440.988.4224	440.988.4224	440.988.4224
CABLE	COMMUNICATION	GAS
BREEZELINE	BRIGHTSPEED	COLUMBIA GAS OF OHIO
105 BLAZE INDUSTRIAL PKWY	203 W. 9TH STREET	3101 NORTH RIDGE RD E
BEREA, OH 44017	LORAIN, OH 44052	LORAIN, OH 44055
866.496.9669	440.244.8330	440.240.6144
ELECTRIC	TRAFFIC	
OHIO EDISON	ODOT DISTRICT THREE	
1717 ASHLAND ROAD	906 CLARK AVENUE	
MANSFIELD, OH 44905	ASHLAND, OH 44805	
419.521.6214	419.207.2868	

THE FOREMENTIONED UTILITY COMPANIES AND AGENCIES HAVE VARIOUS FACILITIES IN THE AREA THAT WILL REMAIN IN PLACE DURING CONSTRUCTION.

EXTREME CAUTION SHOULD BE EXERCISED IN AREAS WITH UTILITIES. SECTIONS 105.07 AND 107.16 OF THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS REQUIRE, AMONG OTHER THINGS, THAT THE CONTRACTOR COOPERATE WITH ALL UTILITIES LOCATED WITHIN THE LIMITS OF THIS CONSTRUCTION PROJECT AND TAKE RESPONSIBILITY FOR THE PROTECTION OF THE UTILITY PROPERTY AND SERVICES.

## EXISTING PLANS

EXISTING PLANS SHOWN BELOW MAY BE INSPECTED IN THE ODOT DISTRICT THREE OFFICE IN ASHLAND:

STRUCTURE LOR-2-0097R		STRUCTURE LOR-2-0742R	
PLAN TITLE	DATE	PLAN TITLE	DATE
ERI-2-29.12/LOR-2-0.00	1972	LOR-254-0.00-B	1966
ERI-2-30.51/LOR-2-0.00	1992	LOR-2-3.50	1994
ERI/LOR-2-30.46/0.00	2004	DO3-BY-FY2008(B)	2008
DO3-BH-FY2016(B)	2016	LOR-2-3.50	2009
		LOR-2-3.86	2020

## WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

## COORDINATION OF WORK BETWEEN CONTRACTORS

THE CONTRACTOR SHOULD BE AWARE THAT THERE MAY BE OTHER WORK BEING PERFORMED BY A SEPARATE CONTRACT. LOR-2-7.97 (PID: 79758) IS A ROADWAY MINOR REHAB CONTRACT APPROXIMATELY 0.50 MILE EAST OF THIS CONTRACT AND IS SCHEDULED TO BEGIN WORK IN THE 2024 CONSTRUCTION SEASON. COORDINATION OF WORK IS THE RESPONSIBILITY OF THE CONTRACTOR.

## ROUTINE MAINTENANCE

BETWEEN THE TIME THAT BIDS ARE TAKEN AND THE START OF CONSTRUCTION, THE MAINTAINING AGENCY MAY ENTER UPON THE PROJECT AND PERFORM ROUTINE MAINTENANCE SUCH AS CRACK SEALING, PATCHING, AND BERM AND SHOULDER REPAIR. THE EFFECTS, IF ANY, OF THE PERFORMANCE OF ROUTINE MAINTENANCE SHALL BE CONSIDERED AS INHERENT IN WORK OF THE CHARACTER PROVIDED FOR IN THE PLAN AND THE RESULTING CONDITIONS SHALL NOT BE CONSIDERED AS DIFFERING MATERIALLY FROM THOSE EXISTING AT THE TIME BIDS WERE TAKEN.

## PAVEMENT MARKING LOG

PRIOR TO REMOVING, GRINDING, OR OTHERWISE DESTROYING ANY EXISTING PAVEMENT MARKINGS, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CREATE AN EXISTING PAVEMENT MARKING LOG IN ORDER TO PLACE THE PROPOSED PAVEMENT MARKINGS IN THE SAME LOCATION AS THEIR EXISTING CONFIGURATION. SUBMIT THE EXISTING PAVEMENT MARKING LOG TO THE ENGINEER AND OBTAIN HIS OR HER APPROVAL PRIOR TO REMOVING, GRINDING, OR OTHERWISE DESTROYING THE EXISTING PAVEMENT MARKINGS.

ALL LABOR, MATERIAL, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THIS WORK SHOULD BE INCLUDED IN THE CONTRACT LUMP SUM BID PRICE FOR ITEM 614 – MAINTAINING TRAFFIC.

# STRUCTURES

## STANDARD DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS

REFER TO THE FOLLOWING STANDARD BRIDGE DRAWINGS AND SUPPLEMENTAL SPECIFICATION(S):

STANDARD DRAWING	DATE	SUPPLEMENTAL SPECIFICATION	DATE
PCB-91	REVISED 7-17-2020	800	01-19-24
		832	07-21-23
		848	1-15-21

## DESIGN SPECIFICATIONS

THESE STRUCTURES CONFORM TO THE 9TH EDITION OF THE “LRFD BRIDGE DESIGN SPECIFICATIONS” ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 2020 AND THE ODOT BRIDGE DESIGN MANUAL, 2020.

## DESIGN LOADING

N/A

## DESIGN DATA

STRUCTURE	MATERIAL DATA
LOR-2-0097R	PER SUPPLEMENTAL SPECIFICATION 848.06
LOR-2-0742R	CONCRETE CLASS QC MS – COMPRESSIVE STRENGTH = 4.5 KSI AT 28 DAYS EPOXY-COATED REINFORCING STEEL – MINIMUM YIELD STRENGTH = 60 KSI

## EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURES HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURES AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO C&MS SECTIONS 102.05, 105.02 AND 513.04. BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

## LOR-2-0097R CONSTRUCTION ACCESS

SHOULD ANY REPAIRS REQUIRE ACCESS TO THE UNDERSIDE OF STRUCTURE LOR-2-0097R, ACCESS SHALL BE ACCOMPLISHED BY MEANS APPROVED BY THE ENGINEER FACILITATING ACCESS FROM TOP OF THE BRIDGE DECK. NO WORK SHALL BE PERFORMED FROM THE GROUND UNDERNEATH THE BRIDGE.

## ITEM 202 – PORTIONS OF STRUCTURE REMOVED, AS PER PLAN

THIS ITEM SHALL INCLUDE THE ELEMENTS INDICATED IN THE PLANS AND GENERAL NOTES AND THAT ARE NOT SEPARATELY LISTED FOR PAYMENT. ITEMS TO BE REMOVED INCLUDE ALL EXISTING MATERIALS BEING REPLACED BY NEW CONSTRUCTION AND MISCELLANEOUS ITEMS THAT ARE NOT SHOWN TO BE INCORPORATED INTO THE FINAL CONSTRUCTION AND ARE DIRECTED TO BE REMOVED BY THE ENGINEER. THE USE OF EXPLOSIVES, HEADACHE BALLS AND/OR HOE-RAMS WILL NOT BE PERMITTED. THE METHOD OF REMOVAL AND THE WEIGHT OF HAMMER SHALL BE APPROVED BY THE ENGINEER. PERFORM ALL WORK IN A MANNER THAT WILL NOT CUT, ELONGATE OR DAMAGE THE EXISTING REINFORCING STEEL TO BE PRESERVED. CHIPPING HAMMERS SHALL NOT BE HEAVIER THAN THE NOMINAL 90-POUND CLASS. PNEUMATIC HAMMERS SHALL NOT BE PLACED IN DIRECT CONTACT WITH REINFORCING STEEL THAT IS TO BE RETAINED IN THE REBUILT STRUCTURE. SUBMIT CONSTRUCTION PLANS ACCORDING TO C&MS 501.05.

MAXIMUM REMOVAL LIMITS: SOUND THE CONCRETE TO DETERMINE THE LIMITS OF THE CONCRETE TO BE REMOVED AND COMPARE THESE LIMITS TO THE AREAS SHOWN IN THE PLANS. IF NEW AREAS ARE DISCOVERED OR IF THE DIMENSIONS OF THE PLAN AREAS INCREASE BY MORE THAN 25% IN ANY DIRECTION, DOCUMENT THE AREAS AND NOTIFY THE ENGINEER FOR EVALUATION TWO WEEKS PRIOR TO REMOVAL. THE ENGINEER WILL DETERMINE IF PATCHING IN DISCRETE SECTIONS/STAGES IS NEEDED OR IF THE INSTALLATION OF TEMPORARY FALSEWORK IS REQUIRED.

SAW CUT BOUNDARIES OF PROPOSED CONCRETE REMOVALS 2 ½” DEEP. REMOVE CONCRETE TO A ROUGH SURFACE. LEAVE THE EXISTING REINFORCING STEEL, WHERE REQUIRED IN THE PLANS, IN PLACE. PRIOR TO CONCRETE PLACEMENT ABRASIVELY CLEAN JOINT SURFACES AND EXISTING EXPOSED REINFORCEMENT TO REMOVE LOOSE AND DISINTEGRATED CONCRETE AND LOOSE RUST. THOROUGHLY CLEAN THE JOINT SURFACE AND EXPOSED REINFORCEMENT OF ALL DIRT, DUST, RUST OR OTHER FOREIGN MATERIAL BY THE USE OF WATER, AIR UNDER PRESSURE, OR OTHER METHODS THAT PRODUCE SATISFACTORY RESULTS. EXISTING REINFORCING STEEL DOES NOT HAVE TO HAVE A BRIGHT STEEL FINISH BUT REMOVE ALL PACK AND LOOSE RUST. THOROUGHLY DRENCH EXISTING CONCRETE SURFACES WITH CLEAN WATER AND ALLOW TO DRY TO A DAMP CONDITION BEFORE PLACING CONCRETE.

## ITEM 509 – EPOXY COATED STEEL REINFORCEMENT, AS PER PLAN

A QUANTITY OF ITEM 509 – EPOXY COATED STEEL REINFORCEMENT, AS PER PLAN, HAS BEEN INCLUDED IN THE STRUCTURE SUBSUMMARY FOR THE PURPOSE OF SUPPLEMENTING EXISTING REINFORCEMENT EXHIBING SECTION LOSS IN EXCESS OF 25%, OR OTHERWISE DEEMED DEFICIENT BY THE ENGINEER. SISTER THE CORRODED REINFORCEMENT WITH A NEW #4 BAR ADJACENT TO THE CORRODED STEEL. THE LENGTH OF THE NEW STEEL SHALL BE SUFFICIENT TO EXTEND 23” BEYOND THE LIMITS OF SECTION LOSS, OR TO THE LIMITS OF THE REPAIR AREA, WHICHEVER IS LESS.

## ITEM 511 – CONCRETE, MISC.: CLASS QC MS CONCRETE, AS PER PLAN

PROVIDE CONCRETE THAT MEETS THE SPECIFICATIONS OF ITEM 511 – CLASS QC MS CONCRETE. IN ADDITION TO THE REQUIREMENTS OF C&MS 499, A BEAM SPECIMEN TEST SHALL DEMONSTRATE BREAKING STRENGTH ABOVE 650 PSI PRIOR TO OPENING THE REPAIR AREA TO TRAFFIC. ADDITIONAL BEAM SPECIMENS MAY BE REQUIRED BEYOND THE TYPICAL THREE IN ORDER TO ENSURE THAT THE APPROPRIATE STRENGTH HAS BEEN ACHIEVED.

## ITEM 848 – REMOVAL DEBONDED OR DETERIORATED EXISTING VARIABLE THICKNESS CONCRETE OVERLAY, AS PER PLAN

THE PURPOSE OF THIS ITEM IS REMOVAL, BY HAND CHIPPING, ANY DEBONDED, UNSOUND, VARIABLE THICKNESS EXISTING RIGID CONCRETE OVERLAY BEFORE HYDRO-DEMOLITION IS PERFORMED. THE DEPARTMENT WILL PAY FOR THIS WORK BY THE CONTRACT BID PRICE PER SQUARE YARD FOR ITEM 848 – REMOVAL DEBONDED OR DETERIORATED EXISTING VARIABLE THICKNESS CONCRETE OVERLAY ACTUALLY PERFORMED AND WILL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THE WORK AS DIRECTED BY THE ENGINEER.

## ITEM 848 – SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, 3.25” THICK ITEM 848 – SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY

FOLLOW ALL PROVISIONS OF SUPPLEMENTAL SPECIFICATION 848, EXCEPT OVERLAY WORK MAY BE SCHEDULED UP UNTIL NOVEMBER 1, 2024, PROVIDED ALL OTHER PROVISIONS OF SS 848.30 ARE MET. DO NOT SCHEDULE OVERLAYS TO BE PLACED FOR THE TIME PERIOD FROM NOVEMBER 1ST TO MARCH 31ST.

# TRAFFIC CONTROL

## ITEM 642 – SPEED ZONE MEASUREMENT MARKING, AS PER PLAN


THE CONTRACTOR SHALL LOG THE LOCATIONS OF TWO INDIVIDUAL 4’ X 2’ WHITE SPEED ZONE MEASUREMENT MARKINGS FOUND ON THE DECK OF STRUCTURE LOR-2-0097R PRIOR TO REMOVAL OF THE EXISTING OVERLAY. FOLLOWING COMPLETION OF OVERLAY WORK, THESE MARKINGS SHALL BE REPLACED IN KIND UNDER ITEM 642 – SPEED ZONE MEASUREMENT MARKING, AS PER PLAN.

FOR PURPOSES OF THIS ITEM, “EACH” SHALL BE DEFINED AS AN INDIVIDUAL 4’ X 2’ WHITE MARKING PLACED IN THE EXACT LOCATION OF A LOGGED AND DESTROYED MARKING. THE BID PRICE PER EACH SHALL INCLUDE ALL LABOR, MATERIALS, AND EQUIPMENT REQUIRED FOR THE PERFORMANCE OF THIS WORK.

GENERAL NOTES

LOR-2-(0.97)(7.42)

MODEL: GENERAL NOTES PAPER SIZE: 34x22 (in.) DATE: 8/6/2024 TIME: 11:55:15 AM USER: jclarik8 pvc:\ohiodot-pw-bentley.com\ohiodot-pw-02\Documents\01 Active Projects\District 03\Lorain\119939\400-Engineering\Roadway\Sheets\119939\_GN001.dgn

DESIGN AGENCY	DISTRICT 3
	
BRIDGE ENGINEERING	
DESIGNER	JNC
REVIEWER	KAK 03-06-24
PROJECT ID	119939
SHEET	TOTAL
2	13



# MAINTENANCE OF TRAFFIC

## ITEM 614 – MAINTAINING TRAFFIC (GENERAL)

MAINTAIN AT LEAST ONE 10' MINIMUM LANE OF TRAFFIC AT ALL TIMES.

SUBMIT, IN WRITING, A SCHEDULE OF OPERATIONS TO THE ENGINEER AND RECEIVE APPROVAL BEFORE WORK IS STARTED ON THE PROJECT. PRIOR TO BEGINNING WORK, COORDINATE THE MAINTENANCE OF TRAFFIC OPERATIONS WITH THE LOCAL STATE HIGHWAY PATROL.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES 2012 EDITION WITH THE LATEST REVISIONS. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

### CONTRACTOR EQUIPMENT ACCESS AND WORK OPERATIONS

IN ADDITION TO THE REQUIREMENTS OF SECTION 614 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS THE FOLLOWING SHALL APPLY:

THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAVEL WHERE PRACTICAL. A FLAGGER SHALL BE USED WHERE THE CONTRACTOR'S EQUIPMENT MUST MERGE WITH THE TRAFFIC STREAM.

THE CONTRACTOR SHALL ARRANGE CONSTRUCTION OPERATIONS SO AS TO PREVENT ANY INTERFERENCE TO THE CONTINUOUS FLOW OF TRAFFIC. ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO THE CLOSED LANES UNLESS OTHERWISE APPROVED BY THE ENGINEER.

### ITEM 614 – MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED, AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

NEW YEAR'S (OBSERVED) MEMORIAL DAY FOURTH OF JULY (OBSERVED) LABOR DAY	GENERAL ELECTION DAY (NOVEMBER) THANKSGIVING CHRISTMAS (OBSERVED)
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THE PERIOD OF TIME THAT LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THANKSGIVING	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

DURING THE SAME PERIODS, MAINTAIN PEDESTRIAN ACCESS IF PEDESTRIAN ACCESS WAS PRESENT PRIOR TO CONSTRUCTION.

NEWLY CONSTRUCTED LANE ADDITIONS, ONCE COMPLETED AND INITIALLY OPENED TO TRAFFIC, SHALL BE OPEN TO TRAFFIC DURING ALL SUBSEQUENT DESIGNATED HOLIDAYS AND SPECIAL EVENTS, AND RELATED PERIODS F TIME, SPECIFIED ABOVE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

### ITEM 614 – MAINTAINING TRAFFIC (LANE CLOSURE/REDUCTION REQUIRED)

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A FIVE (5) CALENDAR DAYS, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

AT STRUCTURE LOR-2-0742R, SINGLE LANE CLOSURE OF SR 58 UNDER THE STRUCTURE USING DRUMS WILL BE PERMITTED IN A MANNER AND FOR A DURATION APPROVED BY THE ENGINEER. IF USED, THIS CLOSURE SHALL COMPLY WITH ALL THE REQUIREMENTS OF THE PREVIOUS PARAGRAPH.

### ITEM 614 – MAINTAINING TRAFFIC (ESTIMATED QUANTITIES)

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR MAINTENANCE OF TRAFFIC VIA SPOT REPAIR OF SHOULDER ASPHALT DURING LANE SHIFTS. ALL WORK WILL BE AS DIRECTED BY THE ENGINEER. WORK CAN INCLUDE BUT MAY NOT BE LIMITED TO POT HOLE PATCHING AND/OR REMOVAL/REPLACEMENT OF DETERIORATED PAVEMENT VIA PAVEMENT REPAIR. THE INTENT IS TO MAINTAIN SHOULDER PAVEMENT DURING LANE SHIFTS IN A CONDITION THAT IS REASONABLY SMOOTH AND FREE FROM HOLES, RUTS, RIDGES, BUMPS, AND STANDING WATER.

ITEM 614 – ASPHALT CONCRETE FOR MAINTAINING TRAFFIC 20 CU YD

### PERMITTED LANE CLOSURE SCHEDULE (PLCS)

LANE CLOSURES SHALL CONFORM TO THE PLCS.  
HTTPS://ODOT.MS2SOFT.COM/TDMS.U/PLCS/ADMINCHEDULES?LOC=ODOT

THE MONTHLY PUBLISHED SCHEDULES REQUIRED TO BE USED, FOR EACH PLCS SEGMENT WITHIN THE PROJECT AREA, ARE THOSE THAT COMPRISE THE CONSECUTIVE 12-MONTH PERIOD BEGINNING 15 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE AND ENDING 4 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE. THESE SAME 12 MONTHS APPLY FOR THE LIFE OF THE PROJECT AND SHALL BE APPLIED TO EACH RESPECTIVE MONTH OF CONSTRUCTION (MONTH OF LANE CLOSURES SHALL MATCH MONTH OF PLCS USED). LANE CLOSURES IN PLACE FOR MULTIPLE MONTHS SHALL ALWAYS COMPLY WITH THE CURRENT RESPECTIVE MONTH.

(FOR EXAMPLE: IF THE SALE DATE FOR THE PROJECT WAS MARCH OF 2021, THE MONTHLY PUBLISHED SCHEDULES FOR EACH APPLICABLE PLCS SEGMENT WOULD BE DECEMBER 2019 TO NOVEMBER 2020. IF THIS WAS A THREE-YEAR PROJECT, YEAR THREE WOULD STILL BE USING THE DECEMBER 2019 TO NOVEMBER 2020 MONTHLY SCHEDULES. IF THE PROJECT DESIRED TO CLOSE TWO LANES IN JUNE 2021, REFERENCE WOULD BE MADE TO THE JUNE 2020 SCHEDULES FOR THE RESPECTIVE PLCS SEGMENTS. IF THE SAME TWO LANES WERE DESIRED TO BE CLOSED AGAIN IN JULY 2021, REFERENCE WOULD BE MADE TO THE JULY 2020 SCHEDULES FOR THE RESPECTIVE PLCS SEGMENTS.)

MORE RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE AT THE DISCRETION OF THE ENGINEER IN ORDER TO COMPLY WITH THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

LESS RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE SUBJECT TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)) AND SHALL NOT BE IMPLEMENTED UNTIL, AND UNLESS, APPROVED BY THE PROPER ODOT AUTHORITY. EXISTING MOT EXCEPTIONS THAT HAVE ALREADY BEEN APPROVED IN ACCORDANCE TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY AND STANDARD PROCEDURE ARE DETAILED IN THE APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS PLAN NOTE.

ALLOWABLE LANE CLOSURE HOURS FOR FACILITIES NOT COVERED BY THE PLCS, IF ANY, SHALL BE AS SPECIFIED ELSEWHERE IN THE PLANS.

### ITEM 622 – PORTABLE BARRIER PLACEMENT AND REMOVAL

DURING THE PLACEMENT AND REMOVAL OF PORTABLE BARRIER, TRAFFIC WILL BE PROHIBITED FROM OCCUPYING THE TRAVEL LANE ADJACENT TO THE BARRIER. THE BARRIER WILL BE PLACED AT NIGHT AND IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE MAP. THE CLOSURE OF THE ADJACENT LANE WILL BE PER THE STANDARD CONSTRUCTION DRAWING MT-95.30.

THE CONTRACTOR WILL SUBMIT A PLAN TO THE ENGINEER FOR APPROVAL A MINIMUM OF SEVEN (7) DAYS IN ADVANCE OF THE PLANNED LANE CLOSURE. WORK WILL NOT BEGIN UNTIL APPROVAL OF THE PLAN HAS BEEN GRANTED.

ALL COSTS INVOLVED IN PLACEMENT AND REMOVAL OF THE PORTABLE CONCRETE BARRIER WILL BE INCLUDED IN THE CONTRACT PRICE PER LINEAR FOOT OF ITEM 622 – PORTABLE BARRIER, ANCHORED AND ITEM 622 – PORTABLE BARRIER, UNANCHORED.

### FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

### ITEM 614 – WORK ZONE IMPACT ATTENUATOR FOR 24" WIDE HAZARDS (UNIDIRECTIONAL OR BIDIRECTIONAL)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

### ITEM 614 – PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET(S) OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614 – PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN 3 SIGN MONTHS  
(ASSUMING 1 SIGN FOR 3 MONTHS)

### DELINEATION OF PORTABLE AND PERMANENT BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL; AND, ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70.

THE INCREASED BARRIER DELINEATION SHALL CONSIST OF EITHER DELINEATION PANELS OR THE TRIPLE STACKING OF WORK ZONE BARRIER REFLECTORS.

DELINEATION PANELS SHALL CONSIST OF PANELS OF DELINEATION, APPROXIMATELY 34 INCHES LONG AND 6 INCHES WIDE AND SHALL BE "CRIMPED." PANELS SHALL BE INSTALLED AND SPACED PER TRAFFIC SCD MT101.70.

TRIPLE-STACKED BARRIER REFLECTORS SHALL CONSIST OF ALIGNING THREE BARRIER REFLECTORS VERTICALLY, AT LOCATIONS WHERE A SINGLE BARRIER REFLECTOR WOULD BE OTHERWISE ATTACHED. THERE SHALL BE NO OPEN SPACE BETWEEN THE ADJACENT BARRIER REFLECTORS. THE TRIPLE-STACKED BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THEY SHALL BE SPACED AND ALIGNED PER TRAFFIC SCD MT101.70.

ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:


PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE ABOVE ITEMS.

ALONG RUNS OF INCREASED BARRIER DELINEATION WHERE THIS ITEM IS PROVIDED, THE QUANTITY SHALL BE MEASURED AS THE ENTIRE LENGTH OF THE RUN OF INCREASED BARRIER DELINEATION, INCLUDING THE SPACES BETWEEN THE INDIVIDUAL DELINEATION PANELS OR STACKS OF BARRIER REFLECTORS.

LOR-2-(0.97)(7.42)

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MAINTENANCE OF TRAFFIC

DESIGN AGENCY	DISTRICT 3
	BRIDGE ENGINEERING
DESIGNER	JNC
REVIEWER	JDF
PROJECT ID	03-06-24
	119939
SHEET	TOTAL
3	13



**ITEM 614 – LAW ENFORCEMENT OFFICER WITH PATROL CAR) FOR ASSISTANCE, AS PER PLAN**

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMTUCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMTUCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

FOR OPERATIONS WITHOUT POSITIVE PROTECTION OCCURRING WITHIN 10 FEET OF AN OPEN TRAVELED LANE THAT MEET ALL OF THE FOLLOWING CRITERIA:

ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND

AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND,

AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

“WITHOUT POSITIVE PROTECTION” MEANS USE OF DRUMS, CONES, SHADOW VEHICLE, ETC, WITHOUT PROTECTION FROM PORTABLE BARRIER OR OTHER RIGID BARRIER ALONG THE WORK AREA. THIS PHRASE DOES NOT APPLY TO CASES WHERE POSITIVE PROTECTION IS REQUIRED. MOBILE OPERATIONS ARE REGARDED AS “WITHOUT POSITIVE PROTECTION”. FOR WORK ZONES USING A COMBINATION OF BARRIER AND TEMPORARY TRAFFIC CONTROL DEVICES (CONES, DRUMS, ETC), THE DESIGNATION SHALL BE BASED UPON THE TYPE OF DEVICES USED IN THE AREA THAT WORKERS ARE LOCATED.

IF MULTIPLE ACTIVE LOCALIZED QUALIFYING WORK AREAS OCCUR WITHOUT POSITIVE PROTECTION, PER MAINLINE TRAFFIC DIRECTION, PROVIDE A UNIFORMED LEO AND OFFICIAL PATROL CAR IN ADVANCE OF:

THE FIRST ACTIVE WORK AREA THAT DRIVERS WILL ENCOUNTER; OR

THE ACTIVE WORK AREA Laterally CLOSEST TO THE OPEN TRAVELED LANE; OR

OTHER LOCATION AS APPROVED BY THE ENGINEER.

THE UNIFORMED LEO AND OFFICIAL PATROL CAR MAY RELOCATE AMONG THE LISTED LOCATIONS AS APPROPRIATE AS THE OPERATIONS PROCEED IN THE LOCALIZED QUALIFYING WORK AREAS.

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST’S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS’ DUTIES AND PLACEMENT AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. ESTIMATED QUANTITIES HAVE BEEN PROVIDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY.

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

**NOTIFICATION OF TRAFFIC RESTRICTIONS**

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV), THE DISTRICT PUBLIC INFORMATION OFFICE (PIO), THE DISTRICT DETOUR NOTIFICATION EMAIL LIST (D03.DETOUR.NOTIFICATION@DOT.OHIO.GOV), AND THE DISTRICT LANE CLOSURE NOTIFICATION EMAIL LIST (D03.LANE.CLOSURE@DOT.OHIO.GOV). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS AND PIO*
RAMP AND/OR ROAD CLOSURES	2 WEEKS OR GREATER	21 CALENDAR DAYS
	12 HOURS TO 2 WEEKS	14 CALENDAR DAYS
	12 HOURS OR LESS	4 BUSINESS DAYS
LANE CLOSURES AND RESTRICTIONS	2 WEEKS OR GREATER	14 CALENDAR DAYS
	LESS THAN 2 WEEKS	5 BUSINESS DAYS
START OF CONSTRUCTION AND TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS

\* - PRIOR TO CLOSURE DATE, UNLESS NOTED OTHERWISE

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

**ITEM 614 – MAINTAINING TRAFFIC (WINTER TIME LIMITATIONS)**

ALL EXISTING LANES, INCLUDING RAMPS, SHALL BE OPEN AND AVAILABLE TO TRAFFIC IN THE ORIGINAL ALIGNMENT BETWEEN NOVEMBER 1, 2024 AND MARCH 31, 2025. SHOULD THE CONTRACTOR FAIL TO MEET THESE REQUIREMENTS, A DISINCENTIVE SHALL BE ASSESSED ACCORDING TO THE APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS NOTE IN THESE PLANS.

NOVEMBER 1, 2024, SHALL BE CONSIDERED AN INTERIM COMPLETION DATE. WORK TO BE COMPLETED PRIOR TO THIS DATE INCLUDES:

- ALL OVERLAY AND ASSOCIATED WORK ON THE RIGHT SIDE OF STRUCTURE LOR-2-0097R (PHASE A)
- ALL REPAIRS AND INCIDENTAL WORK ON STRUCTURE LOR-2-0742

PENALTIES FOR FAILURE TO COMPLETE THE WORK ABOVE BY THE SPECIFIED DATE SHALL BE ASSESSED COMMENSURATE WITH, BUT NOT IN ADDITION TO, THE DISINCENTIVES LISTED IN THE APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS NOTE FOR THOSE SAME LANE REDUCTIONS.

**APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS**

PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE BEEN APPROVED BY THE MOT EXCEPTION COMMITTEE (MOTEC) OR THE PROJECT IMPACT ADVISORY COUNCIL (PIAC) PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

**APPROVED MOT EXCEPTION(S) INCLUDE:**

- LANE REDUCTION AT LOR-2-0.97 FROM 2 LANES TO 1 LANE FOR UP TO 30 DAYS EACH FOR PHASE A AND B, RESPECTIVELY. A \$72,000 PER DAY DISINCENTIVE SHALL BE ASSESSED FOR LANE RESTRICTIONS AT THIS LOCATION BEYOND THE 30-DAY DURATION PER PHASE. UTILIZE A QUEUE WARNING SYSTEM.
- LANE REDUCTION AT LOR-2-7.42 FROM 2 LANES TO 1 LANE FOR UP TO 30 DAYS. A \$72,000 PER DAY DISINCENTIVE SHALL BE ASSESSED FOR LANE RESTRICTIONS AT THIS LOCATION BEYOND THE 60-DAY DURATION. UTILIZE A QUEUE WARNING SYSTEM.

A MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF FOURTEEN (14) CALENDAR DAYS PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS FOURTEEN (14) CALENDERS DAY MINIMUM MAY BE WAIVED IF THE MEETING IS HELD BEFORE OR CONCURRENTLY WITH THE PRECONSTRUCTION MEETING. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER, AS WELL AS THE CONTRACTOR, WORKSITE TRAFFIC SUPERVISOR (WTS) AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.

IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTIONS REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO THE OFFICE OF ROADWAY ENGINEERING, STATEWIDE TMC, DWZTM AND SPECIAL HAULING PERMITS AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTIONS REFERENCED ABOVE. REFERENCE “EXCEPTION REQUEST APPROVAL DATED MARCH 15, 2024 FOR PID 119939” IN THE NOTIFICATION AND OTHER CORRESPONDENCE.

ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTIONS LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE APPLICABLE ODOT CENTRAL OFFICE COMMITTEE (MOTEC OR PIAC). IN THE EVENT THAT SUCH CHANGES ARE PROPOSED, THE REQUEST SHALL BE COORDINATED THROUGH THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM) A MINIMUM OF 30 CALENDAR DAYS PRIOR TO THE DESIRED IMPLEMENTATION DATE. IF THE DISTRICT AGREES WITH THE PROPOSED CHANGES THE DWZTM SHALL SEEK APPROVAL FROM THE APPLICABLE ODOT CENTRAL OFFICE COMMITTEE. IN THE EVENT THE PROPOSED CHANGES ARE APPROVED IN WRITING, THE CLOSURES ARE STILL SUBJECT TO NOTIFICATION REQUIREMENTS WITHIN THIS NOTE PRIOR TO IMPLEMENTATION.

**WORK ZONE QUEUE DETECTION WARNING SYSTEM**

THE CONTRACTOR SHALL FURNISH, INSTALL, AND MAINTAIN AN APPROVED WORK ZONE QUEUE DETECTION WARNING SYSTEM (WZQDWS) AS PER SUPPLEMENTAL SPECIFICATION 896.

THE PROBABLE INITIAL LOCATIONS OF THE WZQDWS DEVICES ARE SHOWN ON SHEETS 5, 7, AND 9 OF THE PLAN. IT IS EXPECTED THAT THESE LOCATIONS WILL VARY BASED ON PLANNED OR UNPLANNED PHASE AND TRAFFIC PATTERN CHANGES. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE DEVICES BY THE CONTRACTOR SHALL BE DIRECTED BY THE ENGINEER.

THE FOLLOWING TRAFFIC SENSOR THRESHOLDS AND PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) MESSAGES SHALL BE USED:

GREATER THAN OR EQUAL TO 50 MPH – USE FOUR CORNER FLASHING CAUTION MODE

BETWEEN 50 MPH AND 25 MPH – TRAFFIC AHEAD XX MPH / SLOW DOWN

BELOW OR EQUAL TO 25 MPH – TRAFFIC AHEAD XX MPH / PREPARE TO STOP

FOUR CORNER FLASHING CAUTION MODE SHALL CONSIST OF THE USE OF ONE ASTERISK IN EACH CORNER OF THE PCMS DISPLAY (4 TOTAL ASTERISKS).

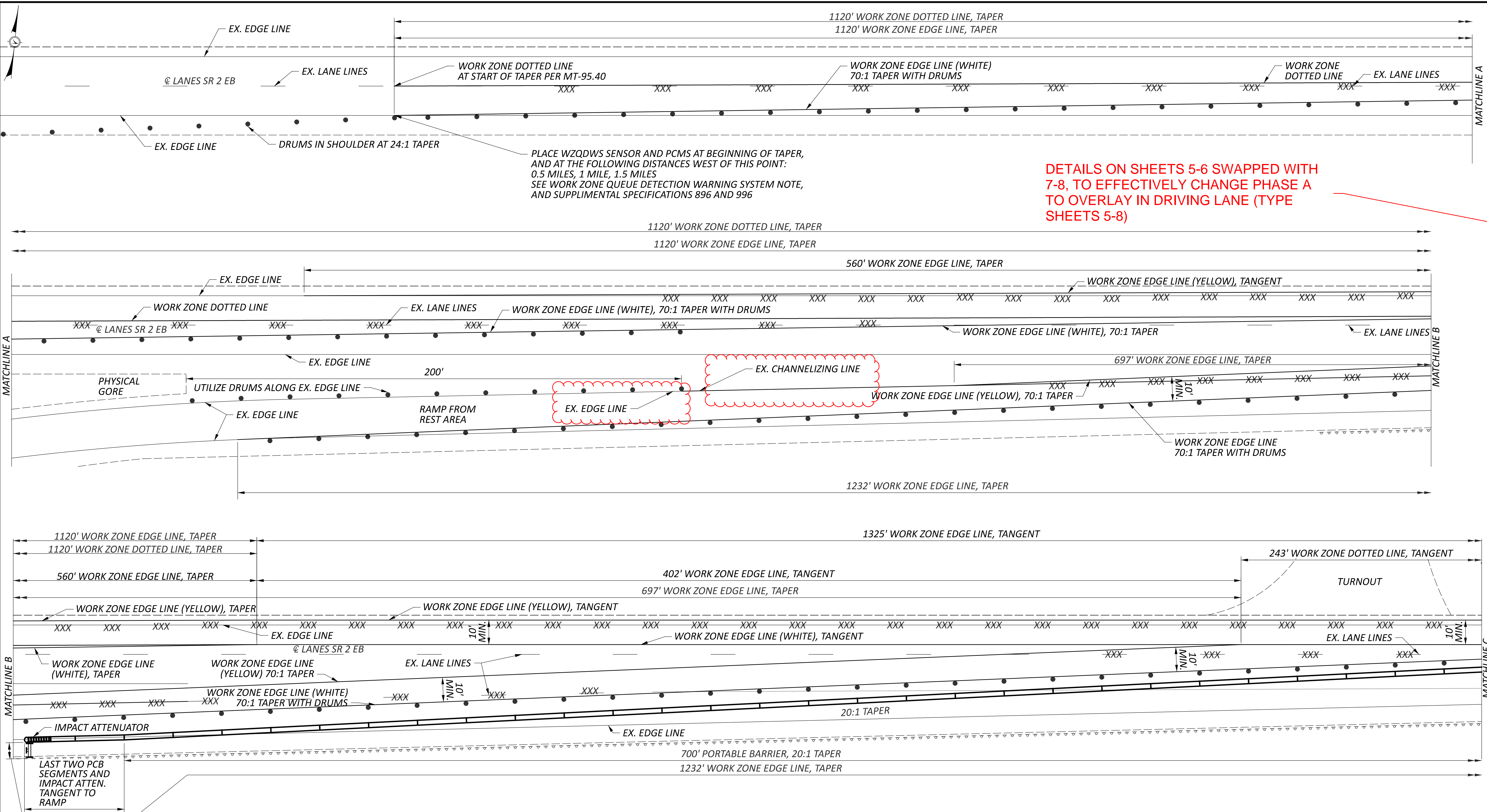
XX SHALL BE ROUNDED UP TO THE NEAREST MULTIPLE OF 5 MPH MINUS 1. OCCUPANCY MAY BE DIRECTED TO BE USED BASED ON CERTAIN TRAFFIC CONDITIONS AND SCENARIOS. ODOT WILL DIRECT THE CONTRACTOR OF THE THRESHOLDS TO BE USED FOR THOSE AREAS WHERE OCCUPANCY IS DIRECTED TO BE USED.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 896, PORTABLE NON-INTRUSIVE TRAFFIC SENSOR, CLASS II 17 SIGN MONTH  
ASSUMING 4 SENSORS FOR 1 MONTH (LOR-2-0.97 PHASE A)  
ASSUMING 4 SENSORS FOR 1 MONTH (LOR-2-0.97 PHASE B)  
ASSUMING 9 SENSORS FOR 1 MONTH (LOR-2-7.42)

ITEM 896, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 17 SIGN MONTH  
ASSUMING 4 SIGNS FOR 1 MONTH (LOR-2-0.97 PHASE A)  
ASSUMING 4 SIGNS FOR 1 MONTH (LOR-2-0.97 PHASE B)  
ASSUMING 9 SIGNS FOR 1 MONTH (LOR-2-7.42)





DETAILS ON SHEETS 5-6 SWAPPED WITH 7-8, TO EFFECTIVELY CHANGE PHASE A TO OVERLAY IN DRIVING LANE (TYPE SHEETS 5-8)

MAINTENANCE OF TRAFFIC  
 LOR-2-0097R LANE CLOSURE DETAILS - PHASE A

LOR-2-0097R MAINTENANCE OF TRAFFIC SUBSUMMARY

ITEM	DESCRIPTION	UNIT	QUANTITY		
			PHASE A	PHASE B	TOTAL
614E11111	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE, AS PER PLAN	HOUR	40	40	80
614E11630	INCREASED BARRIER DELINEATION	FT	1419	1899	3318
614E12384	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (BIDIRECTIONAL)	EACH	1	1	2
614E13310	BARRIER REFLECTOR, TYPE 1 (1WAY)	EACH	19	29	48
614E13350	OBJECT MARKER, ONE WAY	EACH	19	29	48
614E22360	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT (WHITE)	MILE	0.35	0.66	1.01
614E22360	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT (YELLOW)	MILE	0.57	0.52	1.09
614E24612	WORK ZONE DOTTED LINE, CLASS III, 6", 642 PAINT	FT	2030	1363	3393
622E41100	PORTABLE BARRIER, UNANCHORED	FT	930	1410	2340

PROVIDE ALL MARKINGS, DRUMS, BARRIERS, AND SIGNAGE PER SCD MT-95.30, MT-95.40, MT-98.10, MT-98.11, MT-101.70, MT-101.75, MT-101.90, AND PCB-91, EXCEPT AS OTHERWISE DETAILED IN THE PLANS. ANY WORK NOT SPECIFICALLY ITEMIZED IN THE SUBSUMMARY SHALL BE PAID FOR UNDER THE BID LUMP SUM PRICE FOR ITEM 614 - MAINTAINING OF TRAFFIC

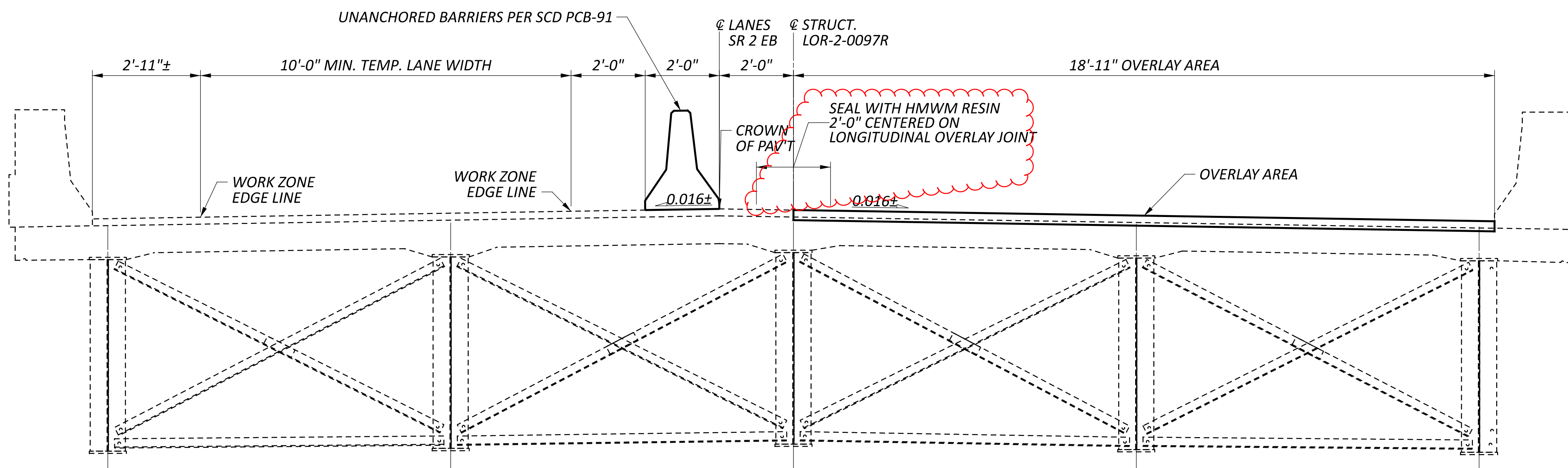
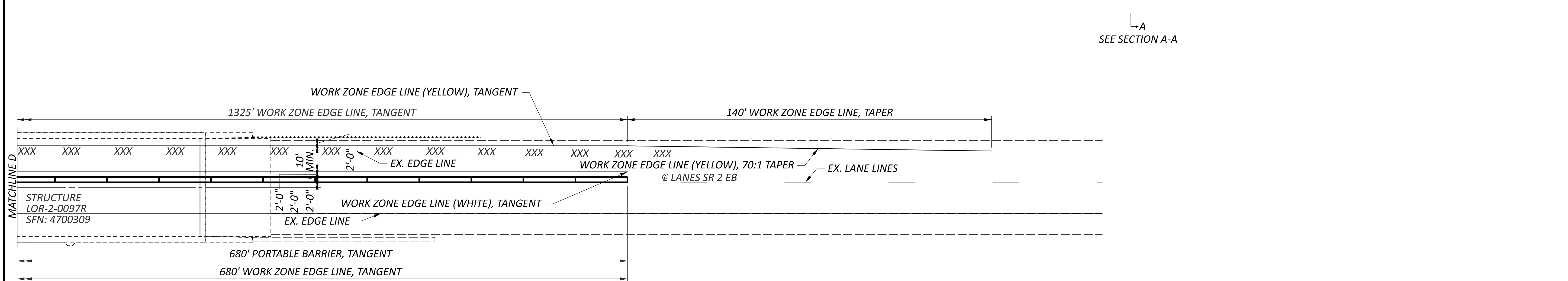
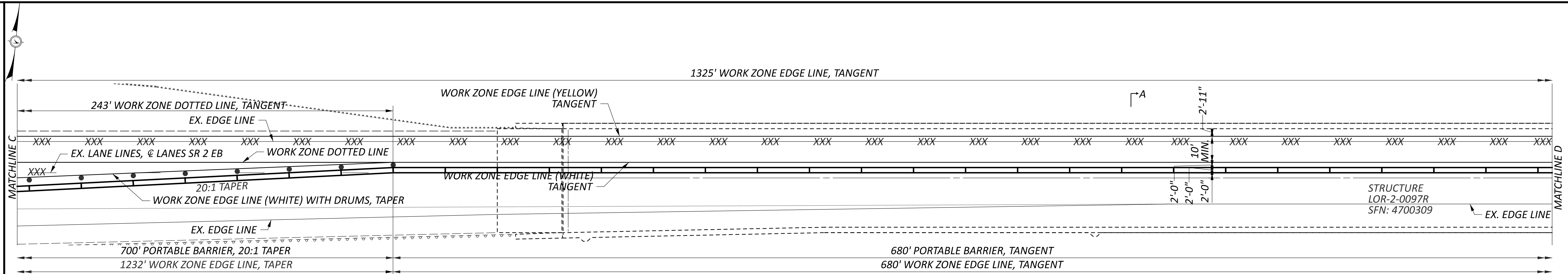
LEGEND

- PORTABLE CONCRETE BARRIER
- EXISTING MARKINGS COVERED OR REMOVED\*
- DRUMS
- TYPE III BARRICADE

ALL DRUMS SPACED AT 20' MIN.  
 PLACE SIGNAGE PER SCD MT-95.40 & MT-98.10  
 \* WHERE WORK ZONE MARKINGS ARE TO BE PLACED DIRECTLY OVER THE LOCATION OF EXISTING MARKINGS, REMOVE MARKINGS PER C&MS 614.11.G.1.a. ALL OTHER CONFLICTING MARKINGS SHALL BE COVERED PER C&MS 614.11.G.1.b.

DESIGN AGENCY  
 DISTRICT 3  
  
 BRIDGE ENGINEERING  
 DESIGNER  
 JNC  
 REVIEWER  
 JDF 03-06-24  
 PROJECT ID  
 119939  
 SHEET TOTAL  
 5 13





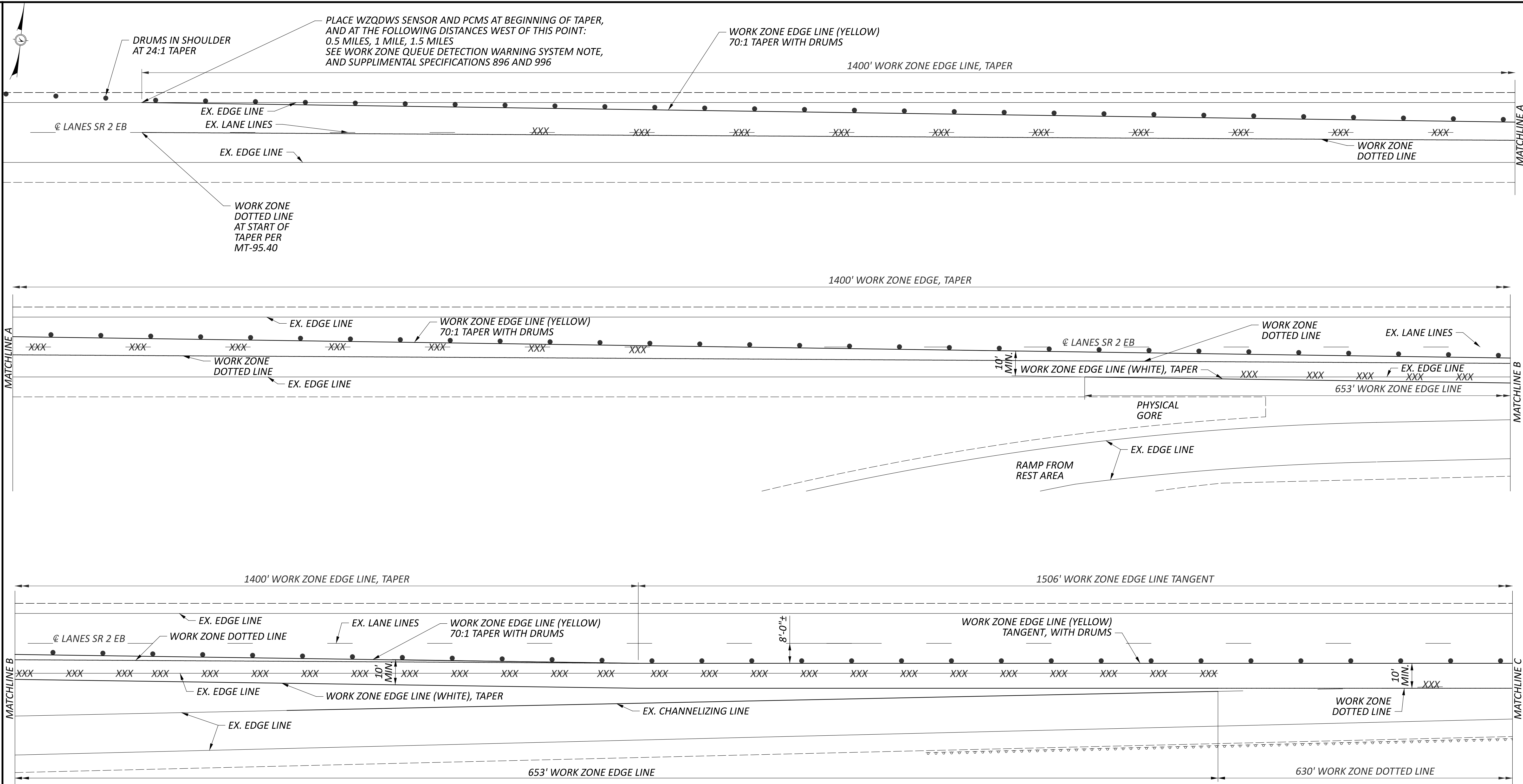
SECTION A-A

**LEGEND**

- PORTABLE CONCRETE BARRIER
  - EXISTING MARKINGS COVERED OR REMOVED\*
  - DRUMS
  - TYPE III BARRICADE
- ALL DRUMS SPACED AT 20' MIN.  
 PLACE SIGNAGE PER SCD MT-95.40 & MT-98.10
- \* WHERE WORK ZONE MARKINGS ARE TO PLACED DIRECTLY OVER THE LOCATION OF EXISTING MARKINGS, REMOVE MARKINGS PER C&MS 614.11.G.1.a. ALL OTHER CONFLICTING MARKINGS SHALL BE COVERED PER C&MS 614.11.G.1.b.

DESIGN AGENCY	DISTRICT 3
BRIDGE ENGINEERING	
DESIGNER	JNC
REVIEWER	JDF
PROJECT ID	03-06-24
SHEET	119939
TOTAL	6 / 13





**LEGEND**

- PORTABLE CONCRETE BARRIER
- XXX - EXISTING MARKINGS COVERED OR REMOVED\*
- - DRUMS
- || - TYPE III BARRICADE

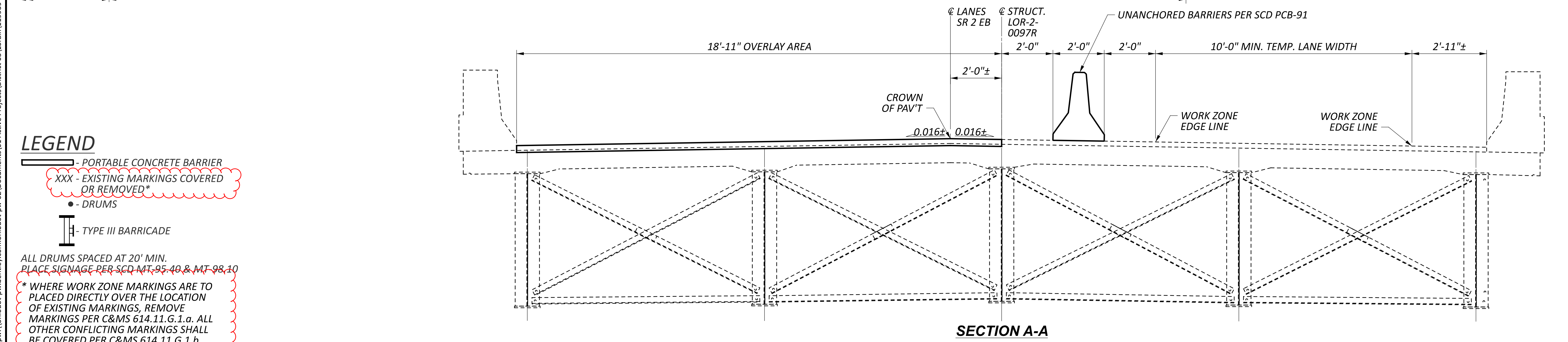
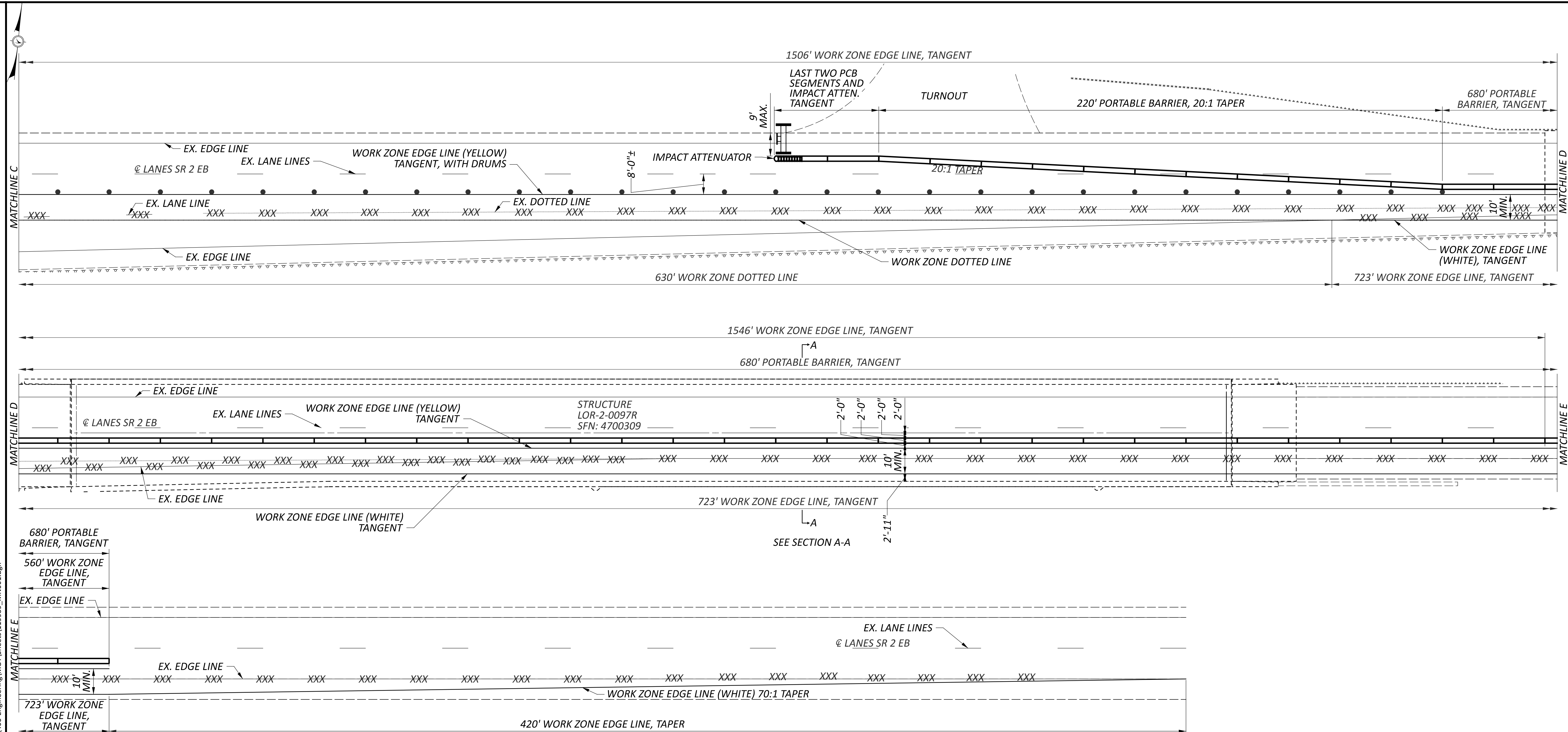
ALL DRUMS SPACED AT 20' MIN.  
 PLACE SIGNAGE PER SCD MT-95.40 & MT-98.10

\* WHERE WORK ZONE MARKINGS ARE TO PLACED DIRECTLY OVER THE LOCATION OF EXISTING MARKINGS, REMOVE MARKINGS PER C&MS 614.11.G.1.a. ALL OTHER CONFLICTING MARKINGS SHALL BE COVERED PER C&MS 614.11.G.1.b.

MAINTENANCE OF TRAFFIC  
 LOR-2-0097R LANE CLOSURE DETAILS - PHASE B

DESIGN AGENCY	DISTRICT 3
DESIGNER	JNC
REVIEWER	JDF
PROJECT ID	03-06-24
SHEET	119939
TOTAL	7 / 13





**LEGEND**

- PORTABLE CONCRETE BARRIER
  - EXISTING MARKINGS COVERED OR REMOVED\*
  - DRUMS
  - TYPE III BARRICADE
- ALL DRUMS SPACED AT 20' MIN.  
PLACE SIGNAGE PER SCD MT-95-40 & MT-98-10
- \* WHERE WORK ZONE MARKINGS ARE TO PLACED DIRECTLY OVER THE LOCATION OF EXISTING MARKINGS, REMOVE MARKINGS PER C&MS 614.11.G.1.a. ALL OTHER CONFLICTING MARKINGS SHALL BE COVERED PER C&MS 614.11.G.1.b.

**SECTION A-A**

DESIGN AGENCY	DISTRICT 3
DESIGNER	
REVIEWER	JNC
PROJECT ID	119939
SHEET	8
TOTAL	13

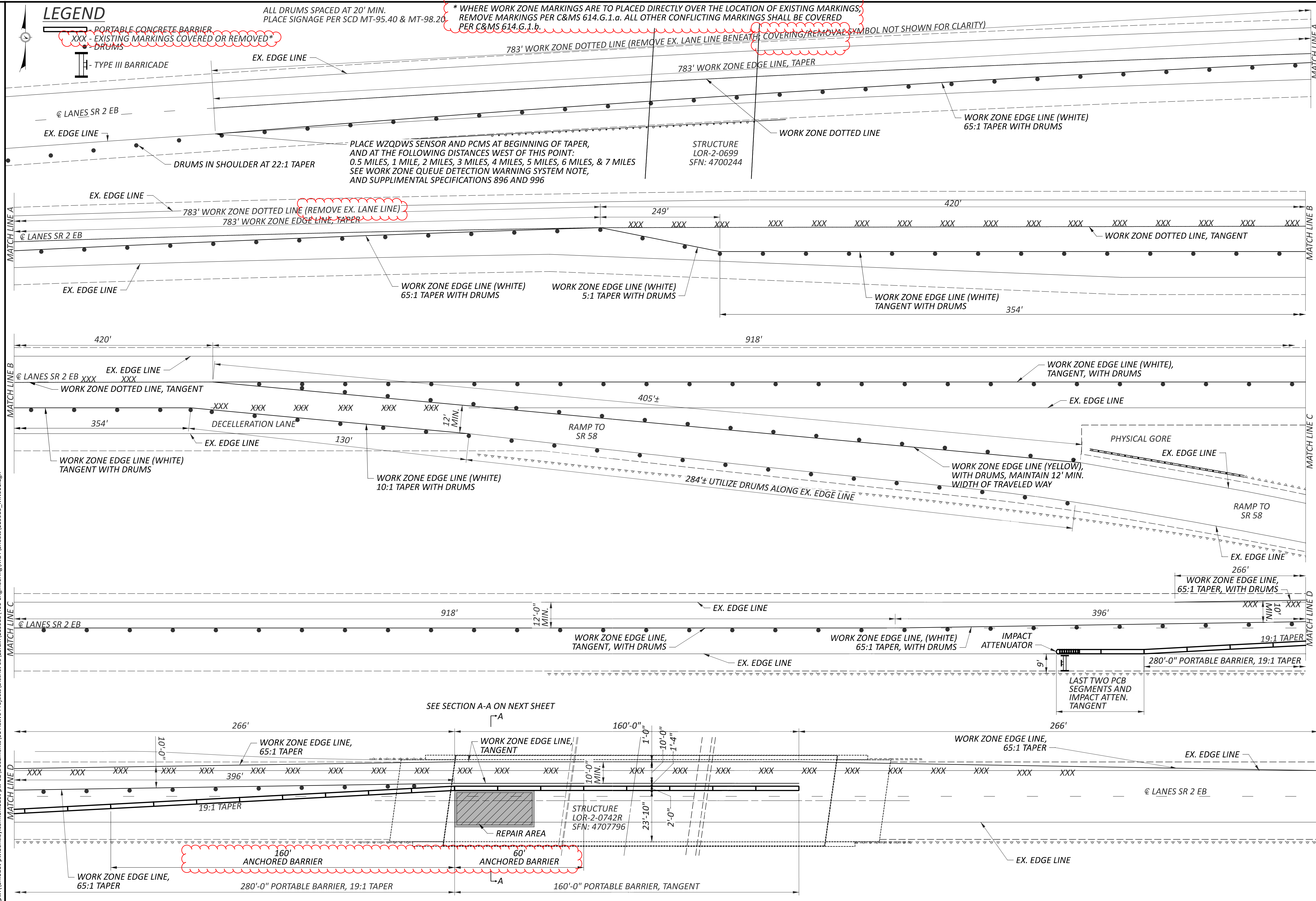


**LEGEND**

- PORTABLE CONCRETE BARRIER
- XXX - EXISTING MARKINGS COVERED OR REMOVED\*
- DRUMS
- TYPE III BARRICADE

ALL DRUMS SPACED AT 20' MIN.  
PLACE SIGNAGE PER SCD MT-95.40 & MT-98.20

\* WHERE WORK ZONE MARKINGS ARE TO PLACED DIRECTLY OVER THE LOCATION OF EXISTING MARKINGS,  
REMOVE MARKINGS PER C&MS 614.G.1.a. ALL OTHER CONFLICTING MARKINGS SHALL BE COVERED  
PER C&MS 614.G.1.b.



SEE SECTION A-A ON NEXT SHEET

WORK ZONE EDGE LINE, TANGENT

WORK ZONE EDGE LINE, TANGENT

WORK ZONE EDGE LINE, 65:1 TAPER

EX. EDGE LINE

REPAIR AREA

ANCHORED BARRIER

ANCHORED BARRIER

280'-0" PORTABLE BARRIER, 19:1 TAPER

160'-0" PORTABLE BARRIER, TANGENT

DESIGN AGENCY	DISTRICT 3
DESIGNER	BRIDGE ENGINEERING
REVIEWER	JNC
PROJECT ID	JDF 03-06-24
SHEET	119939
TOTAL	9   13








LOR-2-(0.97)(7.42)

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SHEET NUM.						PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.
3	4	5	10	12	13	01/NHS/13	EXT	TOTAL				
											<b>EROSION CONTROL</b>	
						1,000	832	30000	1,000	EACH	EROSION CONTROL	
											<b>TRAFFIC CONTROL</b>	
				0.1		0.1	642	00104	0.1	MILE	EDGE LINE, 6", TYPE 1 (WHITE)	
				0.1	0.12	0.22	642	00104	0.22	MILE	EDGE LINE, 6", TYPE 1 (YELLOW)	
				0.1	0.2	0.3	642	00204	0.3	MILE	LANE LINE, 6", TYPE 1	
				260		260	642	01510	260	FT	DOTTED LINE, 6", TYPE 1	
				2		2	642	40001	2	EACH	SPEED MEASUREMENT MARKING, AS PER PLAN	2
											<b>MAINTENANCE OF TRAFFIC</b>	
		80	40			120	614	11111	120	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE, AS PER PLAN	4
		3,318	460			3,778	614	11630	3,778	FT	INCREASED BARRIER DELINEATION	
		2	1			3	614	12384	3	EACH	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (BIDIRECTIONAL)	
			LS			LS	614	12420	LS		DETOUR SIGNING	
20						20	614	13000	20	CY	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC	
		48	5			53	614	13310	53	EACH	BARRIER REFLECTOR, TYPE 1, 1WAY	
		48	5			53	614	13350	53	EACH	OBJECT MARKER, ONE WAY	
3						3	614	18601	3	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	3
		1.01	0.65			1.66	614	22360	1.66	MILE	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT (WHITE)	
		1.09	0.1			1.19	614	22360	1.19	MILE	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT (YELLOW)	
		3,393	1,204			4,597	614	24612	4,597	FT	WORK ZONE DOTTED LINE, CLASS III, 6", 642 PAINT	
		2,340	240			2,580	622	41100	2,580	FT	PORTABLE BARRIER, UNANCHORED	
			220			220	622	41110	220	FT	PORTABLE BARRIER, ANCHORED	
	17					17	896	00012	17	SNMT	PORTABLE NON-INTRUSIVE TRAFFIC SENSOR, CLASS II	
	17					17	896	00020	17	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN	
											<b>STRUCTURE OVER 20 FOOT SPAN (SFN: 4700309)</b>	
				478		478	512	10300	478	SY	SEALING CONCRETE BRIDGE DECKS WITH HMWM RESIN	
				2,028		2,028	848	10200	2,028	SY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, 3.25" THICK	
				2,028		2,028	848	20000	2,028	SY	SURFACE PREPARATION USING HYDRODEMOLITION	
				19		19	848	30200	19	CY	SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY	
				61		61	848	50000	61	SY	HAND CHIPPING	
				LS		LS	848	50100	LS		TEST SLAB	
				14		14	848	50200	14	CY	FULL-DEPTH REPAIR	
				2,028		2,028	848	50320	2,028	SY	EXISTING CONCRETE OVERLAY REMOVED	2
				609		609	848	50341	609	SY	REMOVAL OF DEBONDED OR DETERIORATED EXISTING VARIABLE THICKNESS CONCRETE OVERLAY, AS PER PLAN	2
											<b>STRUCTURE OVER 20 FOOT SPAN (SFN:4707796)</b>	
					15	15	202	11301	15	CY	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	2
					144	144	509	10001	144	LB	EPOXY COATED STEEL REINFORCEMENT, AS PER PLAN	2
					15	15	511	71100	15	CY	CONCRETE, MISC.: CLASS QC MS CONCRETE, AS PER PLAN	2
					70	70	512	10300	70	SY	SEALING CONCRETE BRIDGE DECKS WITH HMWM RESIN	
											<b>INCIDENTALS</b>	
						LS	614	11000	LS		MAINTAINING TRAFFIC	
						2	619	16010	2	MNTH	FIELD OFFICE, TYPE B	
						LS	623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING	
						LS	624	10000	LS		MOBILIZATION	

GENERAL SUMMARY

DESIGN AGENCY  
**DISTRICT 3**



BRIDGE  
 ENGINEERING

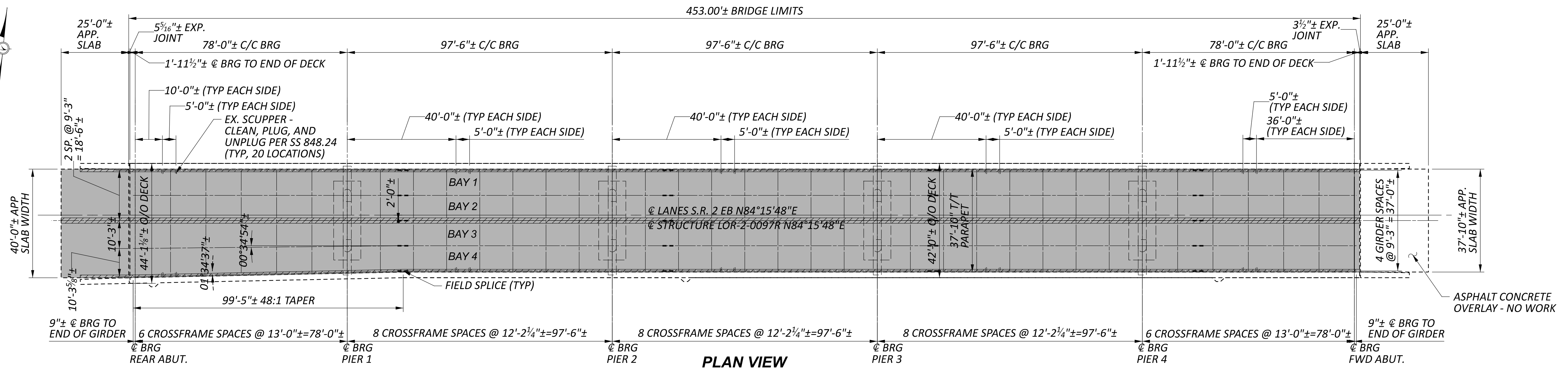
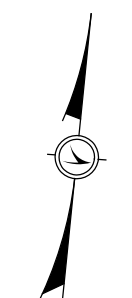
DESIGNER  
**JNC**

REVIEWER  
**KAK 03-06-24**

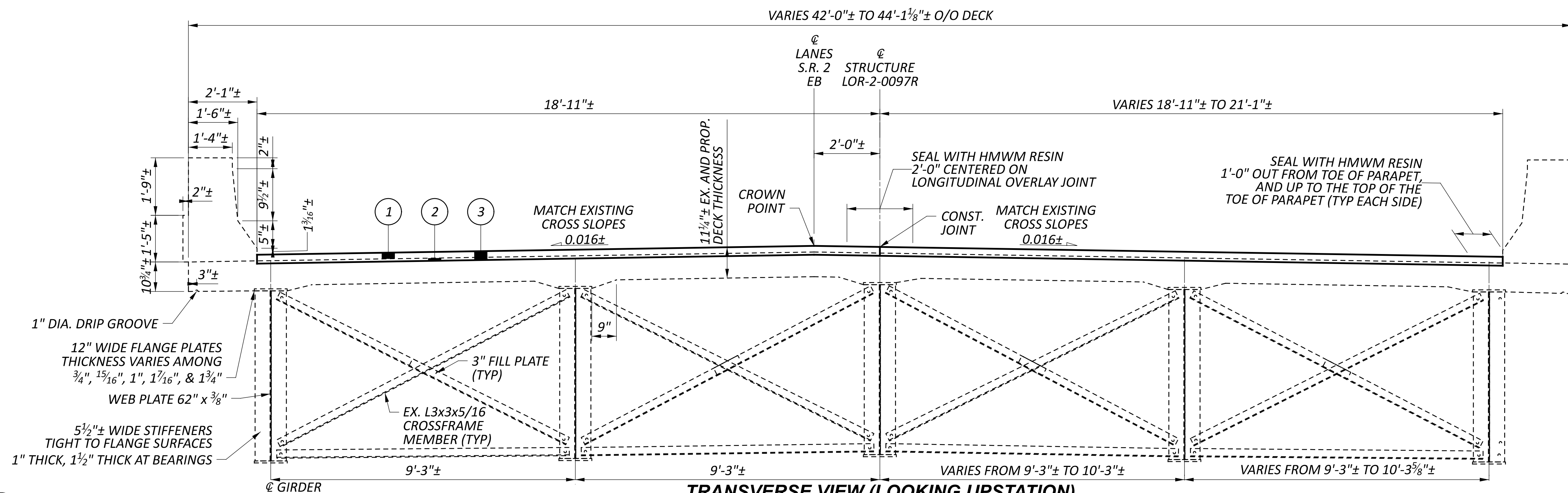
PROJECT ID  
**119939**

SHEET TOTAL  
 11 | 13





PLAN VIEW



TRANSVERSE VIEW (LOOKING UPSTATION)

**LEGEND**

- ITEM 848 - EXISTING CONCRETE OVERLAY REMOVED, AS PER PLAN, 2.25" NOMINAL THICKNESS
- ITEM 848 - REMOVAL OF DEBONDED OR DETERIORATED EXISTING VARIABLE THICKNESS CONCRETE OVERLAY, AS PER PLAN
- ITEM 848 - SURFACE PREPARATION USING HYDRODEMOLITION
- ITEM 848 - FULL DEPTH REPAIR
- ITEM 848 - SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, 3.25" THICK
- ITEM 848 - SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY
- ITEM 848 - HAND CHIPPING

- ITEM 512 - SEALING CONCRETE BRIDGE DECKS WITH HMWM RESIN\*

- 1 ITEM 848 - EXISTING CONCRETE OVERLAY REMOVED, AS PER PLAN, 2.25" NOMINAL THICKNESS
- ITEM 848 - REMOVAL OF DEBONDED OR DETERIORATED EXISTING VARIABLE THICKNESS CONCRETE OVERLAY
- 2 ITEM 848 - SURFACE PREPARATION USING HYDRODEMOLITION (1")
- ITEM 848 - HAND CHIPPING
- 3 ITEM 848 - SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, 3.25" THICK
- ITEM 848 - SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY
- ITEM 848 - FULL DEPTH REPAIR TO BE USED ACCORDING TO SS 848.23 AND AS DIRECTED BY THE ENGINEER

\* THE ESTIMATED QUANTITY OF ITEM 512 - SEALING OF CONCRETE BRIDGE DECKS WITH HMWM RESIN INCLUDED IN THE SUBSUMMARY AND CARRIED TO THE GENERAL SUMMARY CONSTITUTES TWO APPLICATIONS. SHOULD THE PHASES OF THE OVERLAY WORK NOT BE PERFORMED IN IMMEDIATE SEQUENCE DUE TO WEATHER LIMITATIONS, THE JOINT BETWEEN THE NEW OVERLAY AND EXISTING WEARING SURFACE SHALL BE SEALED PRIOR TO OVERWINTERING. FOLLOWING COMPLETION BOTH OVERLAY PHASES, THE JOINT BETWEEN THE TWO PHASES OF THE OVERLAY SHALL THEN BE SEALED.

**LOR-2-0097R STRUCTURE SUBSUMMARY**

ITEM	DESCRIPTION	UNIT	QUANTITY
512E10300	SEALING CONCRETE BRIDGE DECKS WITH HMWM RESIN	SY	478
642E00104	EDGE LINE, 6", TYPE 1 (WHITE)	MILE	0.1
642E00104	EDGE LINE, 6", TYPE 1 (YELLOW)	MILE	0.1
642E00204	LANE LINE, 6", TYPE 1	MILE	0.1
642E01510	DOTTED LINE, 6", TYPE 1	FT	260
642E40001	SPEED MEASUREMENT MARKING, AS PER PLAN	EACH	2
848E10200	SUPERPLASTICIZED DENSE CONCRETE OVERLAY USING HYDRODEMOLITION, 3.25" THICK	SY	2028
848E20000	SURFACE PREPARATION USING HYDRODEMOLITION	SY	2028
848E30200	SUPERPLASTICIZED DENSE CONCRETE OVERLAY (VARIABLE THICKNESS), MATERIAL ONLY	CY	19
848E50000	HAND CHIPPING	SY	61
848E50100	TEST SLAB	LS	LS
848E50200	FULL DEPTH REPAIR	CY	14
848E50320	EXISTING CONCRETE OVERLAY REMOVED, AS PER PLAN, 2.25" NOMINAL THICKNESS	SY	2028
848E50341	REMOVAL OF DEBONDED OR DETERIORATED EXISTING VARIABLE THICKNESS CONCRETE OVERLAY, AS PER PLAN	SY	609
ALL QUANTITIES CARRIED TO GENERAL SUMMARY			

STRUCTURE DETAILS  
STRUCTURE PLAN AND SUBSUMMARY  
STRUCTURE LOR-2-0097R

SFN 4700309  
 DESIGN AGENCY DISTRICT 3  
  
 BRIDGE ENGINEERING  
 DESIGNER CHECKER  
 JNC KCK  
 REVIEWER  
 KAK 03-06-24  
 PROJECT ID 119939  
 SUBSET TOTAL  
 1 1  
 SHEET TOTAL  
 12 13