

ITEM 614 - MAINTAINING TRAFFIC

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON ALL EXISTING ROADWAYS IN ACCORDANCE WITH THE OHIO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

PN 127 - LANE VALUE CONTRACT

THE CONTRACTOR SHALL BE ASSESSED DISINCENTIVES AS DESIGNATED IN THE LANE VALUE CONTRACT TABLE BELOW FOR EACH UNIT OF TIME THE DESCRIBED CRITICAL LANE IS RESTRICTED FROM FULL USE BY THE TRAVELING PUBLIC WITHIN THE RESTRICTED TIME PERIOD. THE DISINCENTIVES WILL BE ASSESSED FOR ALL RESTRICTIONS OF THE CRITICAL WORK.

CRITICAL WORK IS SHOWN IN THE LANE VALUE CONTRACT TABLE BELOW.

CRITICAL WORK IS DEFINED AS HAVING THE DESIGNATED SECTIONS OPEN TO UNRESTRICTED TRAFFIC AS SHOWN IN THE TABLE, OR THE ENTIRE PROJECT IF NOT OTHERWISE LISTED.

UNRESTRICTED TRAFFIC IS DEFINED AS ALL TRAFFIC LANES BEING AVAILABLE FOR USE WITH TEMPORARY SAFETY FEATURES IN PLACE.

DESCRIPTION OF CRITICAL LANE TO BE MAINTAINED	RESTRICTED TIME PERIOD	TIME UNIT	DISINCENTIVE \$ PER TIME UNIT
2 LANES (EACH DIRECTION) OF LOR IR 90 FROM MM 17.33 TO MM 18.41	6 AM TO 7 PM	EACH MINUTE	\$250

THE CONTRACTOR SHALL DIVERT TRAFFIC FROM NORMAL CHANNELS BY PLASTIC DRUMS, PORTABLE BARRIER, TRAFFIC SIGNS, AND WORK ZONE PAVEMENT MARKINGS, AS SHOWN ON SHEETS 20-47.

A MINIMUM OF 2 LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT AND COMPLETED PAVEMENT. AN EXCEPTION WILL BE MADE TO THIS RULE DURING PRE-PHASE 1 TEMPORARY PAVEMENT CONSTRUCTION. DURING PRE-PHASE 1 A SINGLE LANE CLOSURE IN EACH DIRECTION WILL BE ALLOWED AT NIGHT (7PM-6AM). NIGHT CLOSURES MAY ALSO BE PERMITTED DURING PHASE 1 IF NEEDED.

THE CONTRACTOR SHALL INFORM THE DISTRICT OFFICE (419) 281-0513, EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.

THE CONTRACTOR SHALL FURNISH AND MAINTAIN ALL BARRICADES, SIGNS, SIGN SUPPORTS AND INCIDENTALS RELATED TO TRAFFIC CONTROL SO AS TO AVOID DAMAGE AND/OR INJURY TO VEHICLES AND PERSONS USING THE ROADWAY DURING CONSTRUCTION.

SIGNS FURNISHED SHALL BE IN NEW OR LIKE NEW CONDITIONS. LIKE NEW SIGNS SHALL BE SUBJECT TO THE APPROVAL OF THE PROJECT ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE AT ALL TIMES FOR PROVIDING AND MAINTAINING ALL SIGNS AND BARRICADES FOR THE MAINTENANCE OF TRAFFIC AND SAFETY OF HIS/HER WORK AT THE LOCATIONS SHOWN ON THESE PLANS OR AS DIRECTED BY THE ENGINEER.

EXISTING TRAFFIC CONTROL DEVICES LOCATED WITHIN THE WORK AREA, WHICH ARE REQUIRED FOR INTERIM OR PERMANENT TRAFFIC CONTROL, SHALL BE RELOCATED TO POINTS APPROVED BY THE ENGINEER. APPROPRIATE TRAFFIC CONTROL DEVICES SHALL BE MAINTAINED, IN COMPLIANCE WITH THE MANUAL, AT ALL TIMES WHILE TRAFFIC IS MAINTAINED. THE COST OF RELOCATION, IF REQUIRED, SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 - MAINTAINING TRAFFIC.

ITEM 614 - MAINTAINING TRAFFIC (CONT.)

TRUCK MOUNTED ATTENUATORS (TMA'S) SHALL BE USED AS SHOWN IN THE STANDARD CONSTRUCTION DRAWINGS.

NO WORK SHALL BE PERFORMED AND ALL LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	FOURTH OF JULY
NEW YEAR'S	LABOR DAY
MEMORIAL DAY	THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING)	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$250 FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED (PER THE LANE VALUE CONTRACT PN 127).

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

PRIOR TO OPENING TO TRAFFIC EACH LANE SHALL BE IN A SAFE, PASSABLE CONDITION. ALL TRANSVERSE JOINTS SHALL EXTEND ACROSS THE FULL LANE AND SHOULDER WIDTH AND EACH LANE SHALL BE FREE FROM UNEVEN LONGITUDINAL JOINTS. THE CONTRACTOR SHALL PROVIDE ASPHALT WEDGES FOR TRANSVERSE JOINTS WHEREVER THERE ARE PAVEMENT ELEVATION DIFFERENCES.

IF THE CONTRACTOR FAILS TO COMPLY WITH THE PROVISIONS FOR TRAFFIC CONTROL AS SET FORTH IN THESE PLANS AND PROVISIONS OF THE OHIO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THE FAILURE RESULTS IN A CONDITION AT THE WORK SITE WHICH IS UNSAFE FOR TRAFFIC, THE ENGINEER SHALL SUSPEND WORK UNTIL THE CONTRACTOR COMPLIES WITH THE NECESSARY REQUIREMENTS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

TRENCH FOR WIDENING

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

OVERNIGHT TRENCH CLOSING

THE BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN 1.5 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT EXCEPT FOR A SHORT LENGTH (25 FEET OR LESS) OF A WORK SECTION AT THE END OF THE TRENCH. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING SHALL BE BACKFILLED AT THE DIRECTION OF THE ENGINEER.

DRUM REQUIREMENTS

IN ADDITION TO THE REQUIREMENTS OF THE PLANS, SPECIFICATION AND PROPOSAL, DRUMS FURNISHED BY THE CONTRACTOR SHALL BE NEW AND UNUSED AT THE TIME OF ARRIVAL ON THE PROJECT. ANY DRUMS BROUGHT ON THE PROJECT, WHICH HAVE PREVIOUSLY BEEN USED ELSEWHERE, WILL NOT BE ACCEPTED.

PAYMENT FOR DRUMS SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED.

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616, WATER 55 M. GAL.

ITEM 614, REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 15 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

ITEM 614, REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 50 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

WORK ZONE INCREASED PENALTIES SIGN (R11-H5a)

R11-H5a SIGNS SHALL BE FURNISHED, ERECTED AND MAINTAINED IN GOOD CONDITION AND/OR REPLACED AS NECESSARY AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. SIGNS SHALL BE MOUNTED AT THE APPROPRIATE OFFSETS AND ELEVATIONS AS PRESCRIBED BY THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THEY SHALL BE MAINTAINED ON SUPPORTS MEETING CURRENT SAFETY CRITERIA.

SIGNS MAY BE ERECTED OR UNCOVERED NO MORE THAN FOUR HOURS BEFORE THE ACTUAL START OF WORK. THE SIGNS SHALL BE REMOVED OR COVERED NO LATER THAN FOUR HOURS FOLLOWING RESTORATION OF ALL LANES TO TRAFFIC WITH NO RESTRICTIONS, OR SOONER AS DIRECTED BY THE ENGINEER. TEMPORARY SIGN COVERING AND UNCOVERING DUE TO TEMPORARY LANE RESTORATIONS SHALL BE GUIDED BY THE FOUR-HOUR LIMITATIONS STATED ABOVE. SUCH LANE RESTORATIONS SHOULD BE EXPECTED TO REMAIN IN EFFECT FOR 30 OR MORE CONSECUTIVE CALENDAR DAYS, SUCH AS DURING WINTER SHUT-DOWNS.

THE R11-H5a SIGNS SHALL BE MOUNTED ON 2 NO. 3 POSTS WHEN LOCATED WITHIN CLEAR ZONES.

THE CONTRACTOR MAY USE SIGNS AND SUPPORTS IN USED, BUT GOOD, CONDITION PROVIDED THE SIGNS MEET CURRENT ODOT SPECIFICATIONS. SIGN FACES SHALL BE RETROREFLECTORIZED WITH TYPE G SHEETING COMPLYING WITH THE REQUIREMENTS OF C&MS 730.19.

WORK ZONE INCREASED PENALTIES SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS, INCLUDING THE SIGN AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION AS DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.

PAYMENT FOR ACCEPTED QUANTITIES, COMPLETE, IN PLACE WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, ERECTING, MAINTAINING, AND COVERING DURING SUSPENSION OF WORK AND REMOVAL OF THE SIGN AND SUPPORT.

ITEM 614, WORK ZONE INCREASED PENALTIES SIGN 2 EACH

ITEM 615, ROADS FOR MAINTAINING TRAFFIC

PAYMENT FOR ITEM 615, ROADS FOR MAINTAINING TRAFFIC SHALL INCLUDE THE FOLLOWING EARTHWORK, AND ANY OTHER INCIDENTAL ITEMS REQUIRED TO CONSTRUCT THE TEMPORARY PAVEMENT IN PRE-PHASE 1.

ITEM 203, EXCAVATION 2370 CY

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TRAFFIC RESTRICTIONS PLAN

THE CONTRACTOR IS REQUIRED TO SUBMIT A PLAN OUTLINING ALL ANTICIPATED TRAFFIC RESTRICTIONS PRIOR TO THE START OF EACH SEASON. THIS INCLUDES LANE CLOSURES, TRAFFIC SHIFTS, SHORT TERM CLOSURES FOR BEAM REMOVAL OR ERECTION, ETC.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE

ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
RAMP & ROAD CLOSURE	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS AND < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 CALENDAR DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO CLOSURE

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ITEM 642, WORK ZONE SPEED ZONES (WZSZs)

THE FOLLOWING WORK ZONE SPEED ZONE (WZSZ) SPEED LIMIT REVISIONS HAVE BEEN APPROVED FOR USE ON THIS PROJECT WHEN WORK ZONE CONDITIONS AND FACTORS ARE MET AS DESCRIBED BELOW:

WZSZ REVISIONS NUMBER(S)	COUNTY-ROUTE-SECTION	DIRECTION
WZ-20487	LOR-90-17.33 TO LOR-90-17.84 (STA. 899+00 TO STA. 925+93)	EB & WB
WZ-20488	LOR-90-17.84 TO LOR-90-18.41 (STA. 925+93 TO STA. 956+20)	EB & WB

WZSZ LOCATIONS SHALL HAVE AN ORIGINAL (PRE-CONSTRUCTION) POSTED SPEED LIMIT OF 55 MPH OR GREATER, A QUALIFYING WORK ZONE CONDITION OF AT LEAST 0.5 MILE IN LENGTH, AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS, AND A WORK ZONE CONDITION IN PLACE THAT REDUCES THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS (I.E., LANE CLOSURE, LANE SHIFT, CROSSEVER, CONTRAFLOW AND/OR SHOULDER CLOSURE). THE LENGTH OF THE WORK ZONE CONDITION IS MEASURED FROM THE BEGINNING OF THE TAPER FOR THE SUBJECT WORK ZONE CONDITION IMPACTING THE TRAVEL LANES AND/OR SHOULDER TO THE END OF THE DOWNSTREAM TAPER, WHERE DRIVERS ARE RETURNED TO TYPICAL ALIGNMENT. AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS IS REQUIRED TO BALANCE THE ADDITIONAL EXPOSURE CREATED BY INSTALLING AND REMOVING WZSZ SIGNING WITH THE TIME NEEDED TO COMPLETE THE WORK.

IF THE WORK ZONE MEETS THESE MINIMUM CRITERIA, IT SHALL BE ANALYZED FURTHER USING TABLE 1 BELOW TO DETERMINE IF AND WHEN IT QUALIFIES FOR A SPEED LIMIT REDUCTION. DEPENDING ON THE ORIGINAL POSTED SPEED LIMIT, THE TYPE OF TEMPORARY TRAFFIC CONTROL USED, AND WHETHER OR NOT WORKERS ARE PRESENT, A WARRANTED WZSZ WILL VARY IN THE APPROVED SPEED LIMIT TO BE POSTED OVER TIME.

C&MS ITEM 614, PARAGRAPH 614.02(B), INDICATES THAT TWO DIRECTIONS OF A DIVIDED HIGHWAY ARE CONSIDERED SEPARATE HIGHWAY SECTIONS. THEREFORE, IF THE WORK ON A MULTI-LANE DIVIDED HIGHWAY IS LIMITED TO ONLY ONE DIRECTION, A SPEED LIMIT REDUCTION IN THE DIRECTION OF THE WORK DOES NOT AUTOMATICALLY CONSTITUTE A SPEED LIMIT REDUCTION IN THE OPPOSITE DIRECTION. EACH DIRECTION SHALL BE ANALYZED INDEPENDENTLY FROM EACH OTHER.

ALL WZSZS FLUCTUATE BETWEEN TWO APPROVED REDUCED SPEED LIMITS OR BETWEEN AN APPROVED REDUCED SPEED LIMIT AND THE ORIGINAL POSTED SPEED LIMIT. ONLY ONE OF TWO SIGNING STRATEGIES SHALL BE USED TO IMPLEMENT A WZSZ.

WZSZS USING DSL SIGN ASSEMBLIES SHALL BE IN ACCORDANCE WITH THIS NOTE, APPROVED LIST, SUPPLEMENTAL SPECIFICATIONS (SS) 808 AND 908, AND TRAFFIC SCD MT-104.10.

ONLY ONE WARRANTED SPEED LIMIT APPLIES AT ANY ONE TIME; SPEED LIMIT REDUCTIONS ARE NOT CUMULATIVE. WZSZS SHALL NOT BE USED FOR MOVING/MOBILE ACTIVITIES, AS DEFINED IN OMTCD PART 6.

WHEN LOOKING UP THE WARRANTED WORK ZONE SPEED LIMITS, ALWAYS USE THE ORIGINAL, PRECONSTRUCTION, POSTED SPEED LIMIT. DO NOT USE A PRIOR OR CURRENT WORK ZONE SPEED LIMIT AS A LOOK UP VALUE IN THE TABLE. POSITIVE PROTECTION IS GENERALLY REGARDED AS PORTABLE BARRIER OR OTHER RIGID BARRIER IN USE ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WITHOUT POSITIVE PROTECTION IS GENERALLY REGARDED AS USING DRUMS, CONES, SHADOW VEHICLE, ETC., ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WORKERS ARE

ITEM 642, WORK ZONE SPEED ZONES (WZSZs) (CONT.)

CONSIDERED AS BEING PRESENT WHEN ON-SITE, WORKING WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WHEN THE WORK ZONE CONDITION REDUCING THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS IS REMOVED, THE SPEED LIMIT DISPLAYED SHALL RETURN TO THE ORIGINAL POSTED SPEED LIMIT.

TABLE 1: WARRANTED WORK ZONE SPEED LIMITS (MPH) FOR WORK ZONES ON HIGH-SPEED (55 MPH OR GREATER) MULTI-LANE HIGHWAYS

ORIGINAL POSTED SPEED LIMIT	WITH POSITIVE PROTECTION		WITHOUT POSITIVE PROTECTION	
	WORKERS PRESENT	WORKERS NOT PRESENT	WORKERS PRESENT	WORKERS NOT PRESENT
70	60	65	55	65
65	55	60	50	60
60	55	60	50	60
55	50	55	45	55

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 808, DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY 36 SIGN MNTH ASSUMING 4 DSL SIGN ASSEMBLIES FOR 9 MONTHS

ITEM 622, PORTABLE BARRIER PLACEMENT

DURING THE PLACEMENT OF THE PORTABLE BARRIER, TRAFFIC WILL BE PROHIBITED FROM OCCUPYING THE TRAVEL LANE ADJACENT TO THE BARRIER. THE BARRIER WILL BE PLACED AT NIGHT PER THE WORK HOUR RESTRICTION NOTE AND IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE MAP. THE CLOSURE OF THE ADJACENT LANE WILL BE PER THE STANDARD DRAWING MT-95.30.

THE CONTRACTOR WILL SUBMIT PLAN TO THE ENGINEER FOR APPROVAL SEVEN (7) DAYS IN ADVANCE OF THE PLANNED LANE CLOSURE. WORK WILL NOT BEGIN UNTIL APPROVAL OF THE PLANS HAS BEEN GRANTED.

ALL COSTS INVOLVED IN PLACING THE PORTABLE CONCRETE BARRIER WILL BE INCLUDED IN THE CONTRACT PRICE BID FOR ITEM 622 PORTABLE BARRIER.

ITEM 253, PAVEMENT REPAIR

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN PROVIDED FOR ITEM 253, PAVEMENT REPAIR. THIS IS A CONTINGENCY ITEM AND SHALL ONLY BE USED AS DIRECTED BY THE ENGINEER.

THIS CONTINGENCY QUANTITY ASSUMES FULL DEPTH PAVEMENT REPAIR OF 20% OF THE PAVEMENT PLANING AREA. THE QUANTITY ALSO ASSUMES 90% OF THE VOLUME BEING FOR TRANSVERSE REPAIRS AND 10% OF THE VOLUME BEING FOR LONGITUDINAL REPAIRS.

ITEM 253, PAVEMENT REPAIR 2131 CY

ITEM 254, PATCHING PLANED SURFACE

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN PROVIDED FOR ITEM 254, PATCHING OF PLANED SURFACE. THIS IS A CONTINGENCY ITEM AND SHALL ONLY BE USED AS DIRECTED BY THE ENGINEER.

ITEM 254, PATCHING PLANED SURFACE 3488 SY

ITEM 642, TRAFFIC PAINT, AS PER PLAN

THIS WORK CONSISTS OF FURNISHING AND APPLYING WET REFLECTIVE (WR) OPTICS (BEADS OR ELEMENTS), GLASS BEADS, AND TRAFFIC PAINT ACCORDING TO 640, 740, AND THE ADDITIONAL REQUIREMENTS SPECIFIED BELOW.

FURNISH MATERIALS CONFORMING TO:

TRAFFIC PAINT.....	740.02
GLASS BEADS.....	740.09

FURNISH ONE OF THE FOLLOWING WET REFLECTIVE OPTICS: 3M CONNECTED ROADS ALL WEATHER ELEMENTS SERIES 50/51, POTTERS INDUSTRIES VISI-ULTRA, SWARCO DURALUX, OR APPROVED EQUAL.

IN ADDITION TO THE REQUIREMENTS OF 642.03, FURNISH EQUIPMENT CAPABLE OF APPLYING WR OPTICS AT THE TIME OF LINE PLACEMENT.

THE PAVEMENT SURFACE SHALL BE FREE OF LOOSE MATERIAL AND COMPLETELY DRY PRIOR TO THE APPLICATION OF THE PAVEMENT MARKINGS.

PLACE TRAFFIC PAINT AT A THICKNESS OF 20 MILS (0.51 MM). DROP WR OPTICS FROM THE FORWARD-MOST BEAD APPLICATOR GUN AT A MINIMUM RATE OF 5 POUNDS PER 100 SQUARE FEET (2.4 KILOGRAM PER 10 M2). DROP GLASS BEADS AT A MINIMUM RATE OF 8 POUNDS PER 100 SQUARE FEET (3.9 KILOGRAM PER 10 M2) FROM THE REAR BEAD APPLICATOR GUN.

THE DEPARTMENT WILL MEASURE PAVEMENT MARKINGS COMPLETE IN PLACE IN THE UNITS DESIGNATED. THE DEPARTMENT WILL MEASURE LINE QUANTITIES AS THE LENGTH OF THE COMPLETED MARKING, INCLUDING GAPS, INTERSECTIONS, AND OTHER SECTIONS OF PAVEMENT NOT NORMALLY MARKED.

THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES AT CONTRACT PRICES, OR PRICES ADJUSTED ACCORDING TO 641.11, MEASURED ACCORDING TO 641.12, WITH THE PROVISIONS SPECIFIED IN 641.13, AND AS FOLLOWS:

ITEM	UNIT	DESCRIPTION
642	MILE	EDGE LINE, 6 INCH, TYPE 1, AS PER PLAN
642	FOOT	CHANNELIZING LINE, 8 INCH, TYPE 1, AS PER PLAN

CALCULATED
MLV
CHECKED
JML

MAINTENANCE OF TRAFFIC NOTES

LOR-90-17.85

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SHEET NO.	REFERENCE NO.	ALIGNMENT	STATION		SIDE	614	614	614	614	614	614	614	614	614	614	614	615	622	622	642	642	642		
			INCREASED BARRIER DELINEATION	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS (UNIDIRECTIONAL)		WORK ZONE RAISED PAVEMENT MARKER, APP (20' SPACING, ONE-WAY WHITE)	WORK ZONE RAISED PAVEMENT MARKER, APP (20' SPACING, ONE-WAY YELLOW)	WORK ZONE RAISED PAVEMENT MARKER, APP (120' SPACING, ONE-WAY WHITE)	WORK ZONE RAISED PAVEMENT MARKER, APP (120' SPACING, ONE-WAY YELLOW)	BARRIER REFLECTOR, TYPE 1, (ONE WAY), WHITE	BARRIER REFLECTOR, TYPE 1, (TWO WAY), YELLOW/YELLOW	OBJECT MARKER, ONE WAY	OBJECT MARKER, TWO WAY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A	PORTABLE BARRIER, 32", UNANCHORED	PORTABLE BARRIER, 32", ANCHORED	EDGE LINE, 6", TYPE 1, AS PER PLAN (WHITE)	EDGE LINE, 6", TYPE 1, AS PER PLAN (YELLOW)	CHANNELIZING LINE, 8", TYPE 1, AS PER PLAN (WHITE)	FT	FT	MILE	MILE	FT
			FROM	TO		FT	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	SY	FT	FT	MILE	MILE	FT		
PHASE 1 QUANTITIES																								
21 - 28	EW-1	I.R. 90 WB	903+35	951+20	LT																			
21 - 28	EW-2	I.R. 90 EB	904+35	951+20	RT																			
21 - 28	CL-1	I.R. 90 WB	903+35	951+20	LT																			
21 - 28	CL-2	I.R. 90 EB	904+35	951+20	RT																			
21 - 28	EY-1	I.R. 90 WB	903+35	951+20	LT																			
21 - 28	EY-2	I.R. 90 EB	904+35	951+20	RT																			
21 - 22		I.R. 90 WB	903+35	907+00	LT			18																
22 - 27		I.R. 90 WB	907+00	948+00	LT				34															
27 - 28		I.R. 90 WB	948+00	951+20	LT			16																
21 - 22		I.R. 90 WB	903+35	907+00	LT			18																
22 - 27		I.R. 90 WB	907+00	948+00	LT				34															
27 - 28		I.R. 90 WB	948+00	951+20	LT			16																
21 - 22		I.R. 90 WB	903+35	907+00	LT				18															
22 - 27		I.R. 90 WB	907+00	948+00	LT					34														
27 - 28		I.R. 90 WB	948+00	951+20	LT			16																
21 - 22		I.R. 90 EB	904+35	907+10	RT			14																
22 - 27		I.R. 90 EB	907+10	948+60	RT				34															
27 - 28		I.R. 90 EB	948+60	951+20	RT			13																
21 - 22		I.R. 90 EB	904+35	907+10	RT			14																
22 - 27		I.R. 90 EB	907+10	948+60	RT				34															
27 - 28		I.R. 90 EB	948+60	951+20	RT			13																
21 - 22		I.R. 90 EB	904+35	907+10	RT				14															
22 - 27		I.R. 90 EB	907+10	948+60	RT					35														
27 - 28		I.R. 90 EB	948+60	951+20	RT			13																
21 - 24	PV-1	I.R. 90 WB	904+50	927+19	LT												1264							
21 - 24	PV-2	I.R. 90 EB	904+75	926+86	RT												1225							
25 - 28	PV-3	I.R. 90 WB	929+08	950+75	LT												1205							
25 - 27	PV-4	I.R. 90 EB	928+50	950+50	RT												1221							
24	PV-5	I.R. 90 WB	924+84.23	927+47	LT												292							
25	PV-6	I.R. 90 WB	928+29	929+85	LT												173							
24	PV-7	I.R. 90 EB	925+04.23	926+71	RT												185							
24 - 25	PV-8	I.R. 90 EB	928+29	929+35	RT												118							
PHASE 2 QUANTITIES																								
30 - 38	EW-3	I.R. 90 WB	906+04	948+95	LT																			
30 - 38	EW-4	I.R. 90 EB	905+84	949+30	RT																			
30 - 38	CL-3	I.R. 90 WB	906+04	948+95	LT																			
30 - 38	CL-4	I.R. 90 EB	905+84	949+30	RT																			
30 - 38	EY-3	I.R. 90 WB	906+04	948+95	LT																			
30 - 38	EY-4	I.R. 90 EB	905+84	949+30	RT																			
SUBTOTALS CARRIED TO SHEET 15									389								5,683			6.86		18,109		

MAINTENANCE OF TRAFFIC SUBSUMMARY

CALCULATED
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LOR-90-17.85

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SHEET NO.	REFERENCE NO.	ALIGNMENT	STATION		SIDE	614	614	614	614	614	614	614	614	614	614	614	615	622	622	642	642	642		
			FROM	TO		FT	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	SY	FT	FT	MILE	MILE	FT	
PHASE 2 QUANTITIES (CONTINUED)																								
30 - 31		I.R. 90 WB	906+04	909+64	LT			18																
31 - 37		I.R. 90 WB	909+64	944+85	LT				30															
37 - 38		I.R. 90 WB	944+85	948+95	LT			21																
30 - 31		I.R. 90 WB	906+04	909+64	LT			18																
31 - 37		I.R. 90 WB	909+64	944+85	LT				29															
37 - 38		I.R. 90 WB	944+85	948+95	LT			21																
30 - 31		I.R. 90 WB	906+04	909+64	LT				18															
31 - 37		I.R. 90 WB	909+64	944+85	LT				29															
37 - 38		I.R. 90 WB	944+85	948+95	LT			21																
30 - 31		I.R. 90 EB	905+84	909+44	RT			18																
31 - 37		I.R. 90 EB	909+44	945+15	RT				30															
37 - 38		I.R. 90 EB	945+15	949+30	RT			21																
30 - 31		I.R. 90 EB	905+84	909+44	RT			18																
31 - 37		I.R. 90 EB	909+44	945+15	RT				30															
37 - 38		I.R. 90 EB	945+15	949+30	RT			21																
31		I.R. 90 EB	908+54		RT																			
31 - 37	PB-1	I.R. 90 EB	908+74	945+15	RT			1																
31		I.R. 90 EB	908+74	909+44	RT	80																		
31 - 37		I.R. 90 EB	909+44	945+15	RT																			
31 - 38	PB-2	I.R. 90 WB	909+64	946+34	LT																			
31 - 37		I.R. 90 WB	909+64	944+85	LT																			
37 - 38		I.R. 90 WB	944+85	946+34	LT	160																		
38		I.R. 90 WB	946+54		LT			1																
PHASE 3 QUANTITIES																								
40 - 46	EW-5	I.R. 90 WB	909+74	944+85.00	LT																			
40 - 46	EW-6	I.R. 90 EB	909+44	945+05.00	RT																			
40 - 46	CL-5	I.R. 90 WB	909+74	944+85.00	LT																			
40 - 46	CL-6	I.R. 90 EB	909+44	94+50.00	RT																			
40 - 46	EY-5	I.R. 90 WB	909+74	944+85.00	LT																			
40 - 46	EY-6	I.R. 90 EB	909+44	945+05.00	RT																			
SUBTOTALS CARRIED TO SHEET 15						240	2		409					142		142			7,360		2.68		7,073	

MAINTENANCE OF TRAFFIC SUBSUMMARY

CALCULATED
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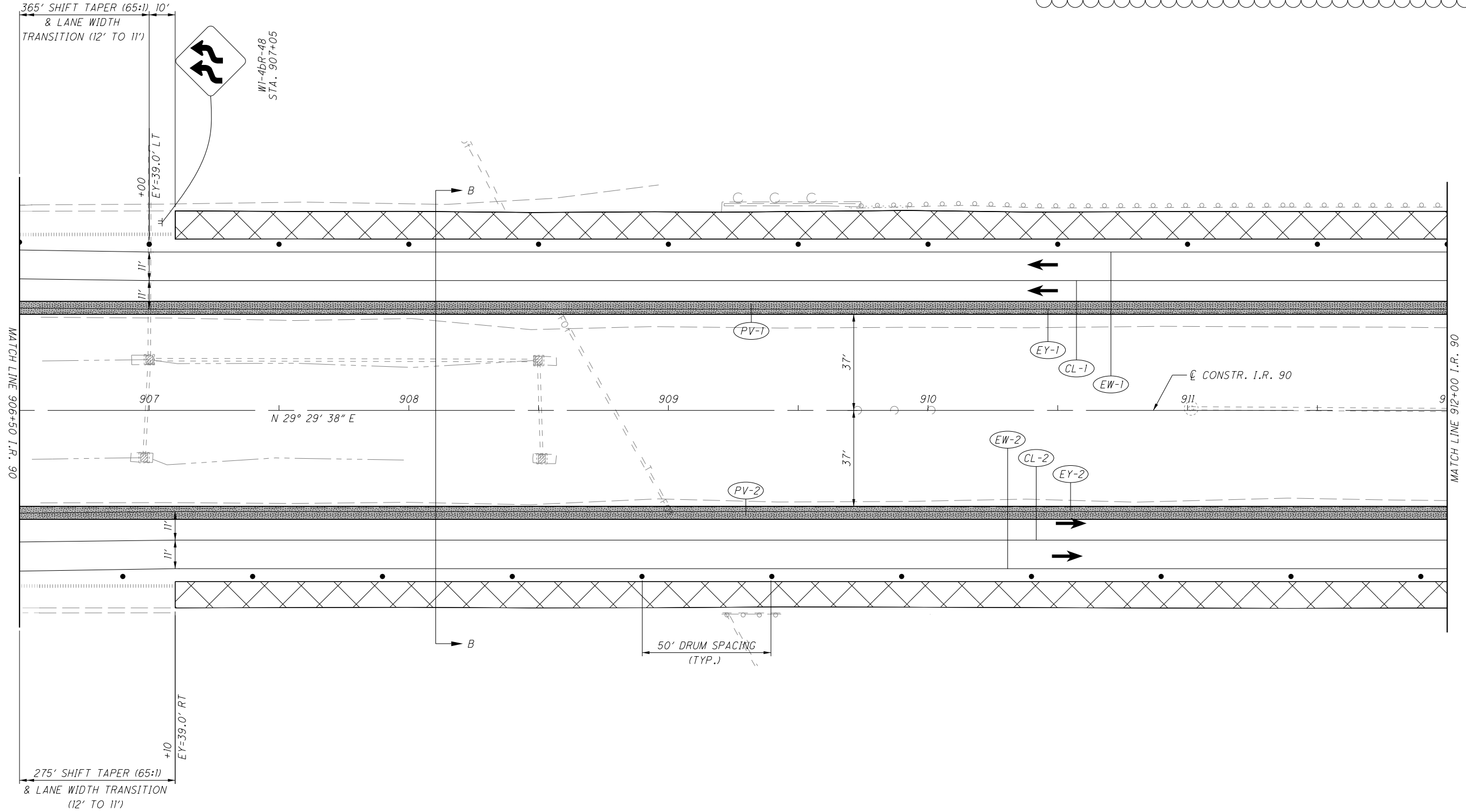
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SHEET NO.	REFERENCE NO.	ALIGNMENT	STATION		SIDE	614	614	614	614	614	614	614	614	614	614	615	622	622	642	642	642		
			INCREASED BARRIER DELINEATION	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS (UNIDIRECTIONAL)		WORK ZONE RAISED PAVEMENT MARKER, APP (20' SPACING, ONE-WAY WHITE)	WORK ZONE RAISED PAVEMENT MARKER, APP (20' SPACING, ONE-WAY YELLOW)	WORK ZONE RAISED PAVEMENT MARKER, APP (20' SPACING, ONE-WAY WHITE)	WORK ZONE RAISED PAVEMENT MARKER, APP (120' SPACING, ONE-WAY WHITE)	WORK ZONE RAISED PAVEMENT MARKER, APP (120' SPACING, ONE-WAY YELLOW)	BARRIER REFLECTOR, TYPE 1, (ONE WAY), WHITE	BARRIER REFLECTOR, TYPE 1, (TWO WAY), YELLOW/YELLOW	OBJECT MARKER, ONE WAY	OBJECT MARKER, TWO WAY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A	PORTABLE BARRIER, 32", UNANCHORED	PORTABLE BARRIER, 32", ANCHORED	EDGE LINE, 6", TYPE 1, AS PER PLAN (WHITE)	EDGE LINE, 6", TYPE 1, AS PER PLAN (YELLOW)	CHANNELIZING LINE, 8", TYPE 1, AS PER PLAN (WHITE)			
			FROM	TO		FT	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	SY	FT	FT	MILE	MILE	FT		
PHASE 3 QUANTITIES (CONTINUED)																							
40 - 43		I.R. 90 WB	909+74	924+74	LT			75															
43 - 44		I.R. 90 WB	924+74	929+85	LT				4														
44 - 46		I.R. 90 WB	929+85	944+85	LT			75															
40 - 43		I.R. 90 WB	909+74	924+74	LT			75															
43 - 44		I.R. 90 WB	924+74	929+85	LT				4														
44 - 46		I.R. 90 WB	929+85	944+85	LT			75															
40 - 43		I.R. 90 WB	909+74	924+74	LT					75													
43 - 44		I.R. 90 WB	924+74	929+85	LT						4												
44 - 46		I.R. 90 WB	929+85	944+85	LT						75												
40 - 43		I.R. 90 EB	909+44	925+04	RT			78															
43 - 44		I.R. 90 EB	925+04	929+45	RT				4														
44 - 46		I.R. 90 EB	929+45	945+05	RT			78															
40 - 43		I.R. 90 EB	909+44	925+04	RT			78															
43 - 44		I.R. 90 EB	925+04	929+45	RT				4														
44 - 46		I.R. 90 EB	929+45	945+05	RT			78															
42		I.R. 90 EB	920+23		RT		1																
42 - 43	PB-3	I.R. 90 EB	920+43	926+48	RT												601						
43 - 44	PB-5	I.R. 90 EB	928+48	928+83	RT													230					
44	PB-7	I.R. 90 EB	928+83	929+45	RT												70						
42 - 43		I.R. 90 EB	919+84	925+04	RT	515																	
43 - 44		I.R. 90 EB	925+04	929+45	RT							9		9									
43	PB-4	I.R. 90 WB	924+74	926+98	LT												230						
43 - 44	PB-6	I.R. 90 WB	926+98	929+20	LT													230					
45		I.R. 90 WB	934+59		LT		1																
44 - 45	PB-8	I.R. 90 WB	929+20	934+39	LT													521					
43 - 44		I.R. 90 WB	924+74	929+85	LT							10		10									
44 - 45		I.R. 90 WB	929+85	935+05	LT	515																	
SUBTOTALS FROM THIS SHEET						1,030	2		942			19		19			1,422	460					
SUBTOTALS FROM SHEET 13									389							5,683			6.86		18,109		
SUBTOTALS FROM SHEET 14						240	2		409				142	142				7,360		2.68		7,073	
TOTALS CARRIED TO GENERAL SUMMARY						1,270	4		1,740			19	142	19	142	5,683	1,422	7,820	9.54		25,182		

CALCULATED	MLV
	CHECKED
JML	
MAINTENANCE OF TRAFFIC SUBSUMMARY	
LOR-90-17.85	
15	
196	

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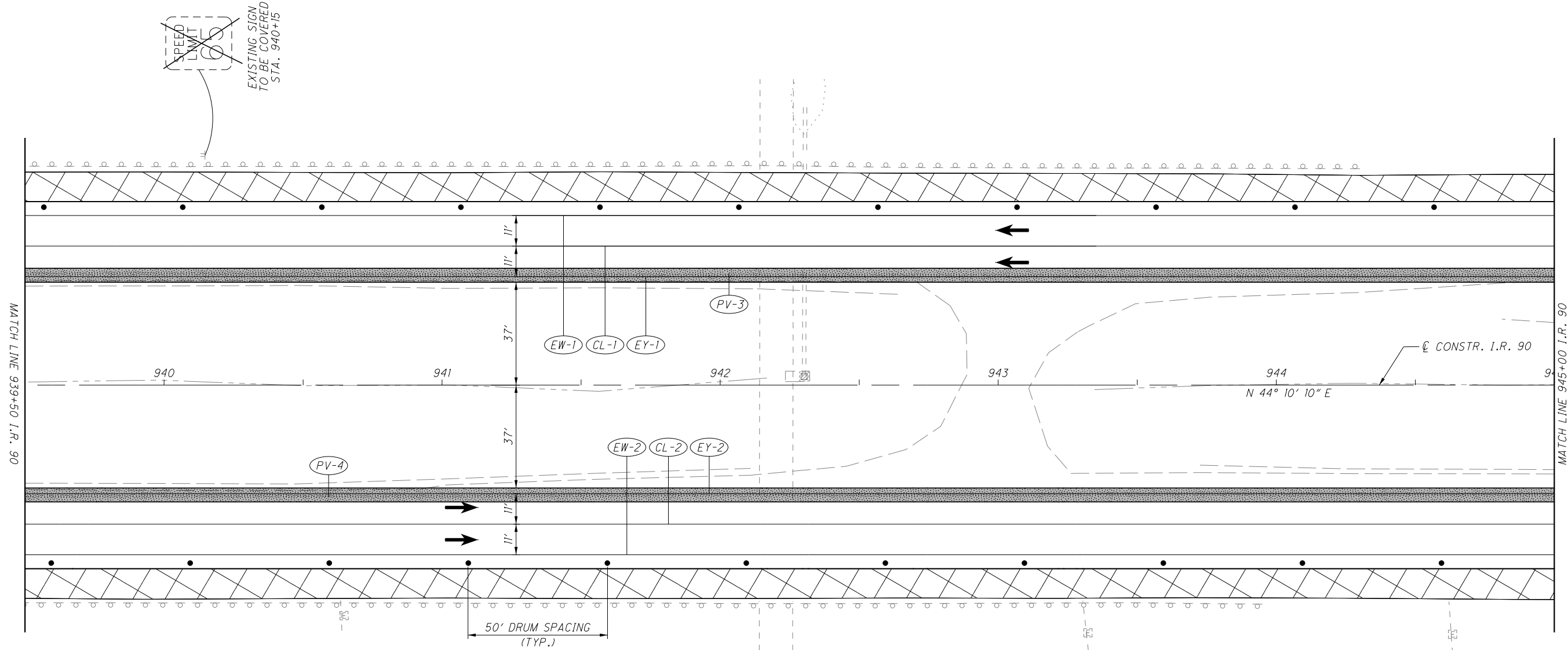
- NOTES:
1. FOR LEGEND, SEE SHEET 20
 2. FOR TYPICAL SECTION, SEE SHEET 17
 3. FOR EXISTING SIGNS, SEE SIGNING AND PAVEMENT MARKINGS SHEETS, III - 119
 4. INSIDE SHOULDER REPLACEMENT WITH TEMPORARY PAVEMENT TO BE INSTALLED PRIOR TO PHASE 1 START
 5. TEMPORARY PAVEMENT CONSTRUCTED OUTSIDE PERMANENT PAVEMENT LIMITS MAY REMAIN AFTER CONSTRUCTION

CALCULATED
MLV
CHECKED
JML

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC - PHASE 1
STA. 906+50 TO STA. 912+00

LOR-90-17.85



~~SPEED
LIMIT
55~~
EXISTING SIGN
TO BE COVERED
STA. 940+15

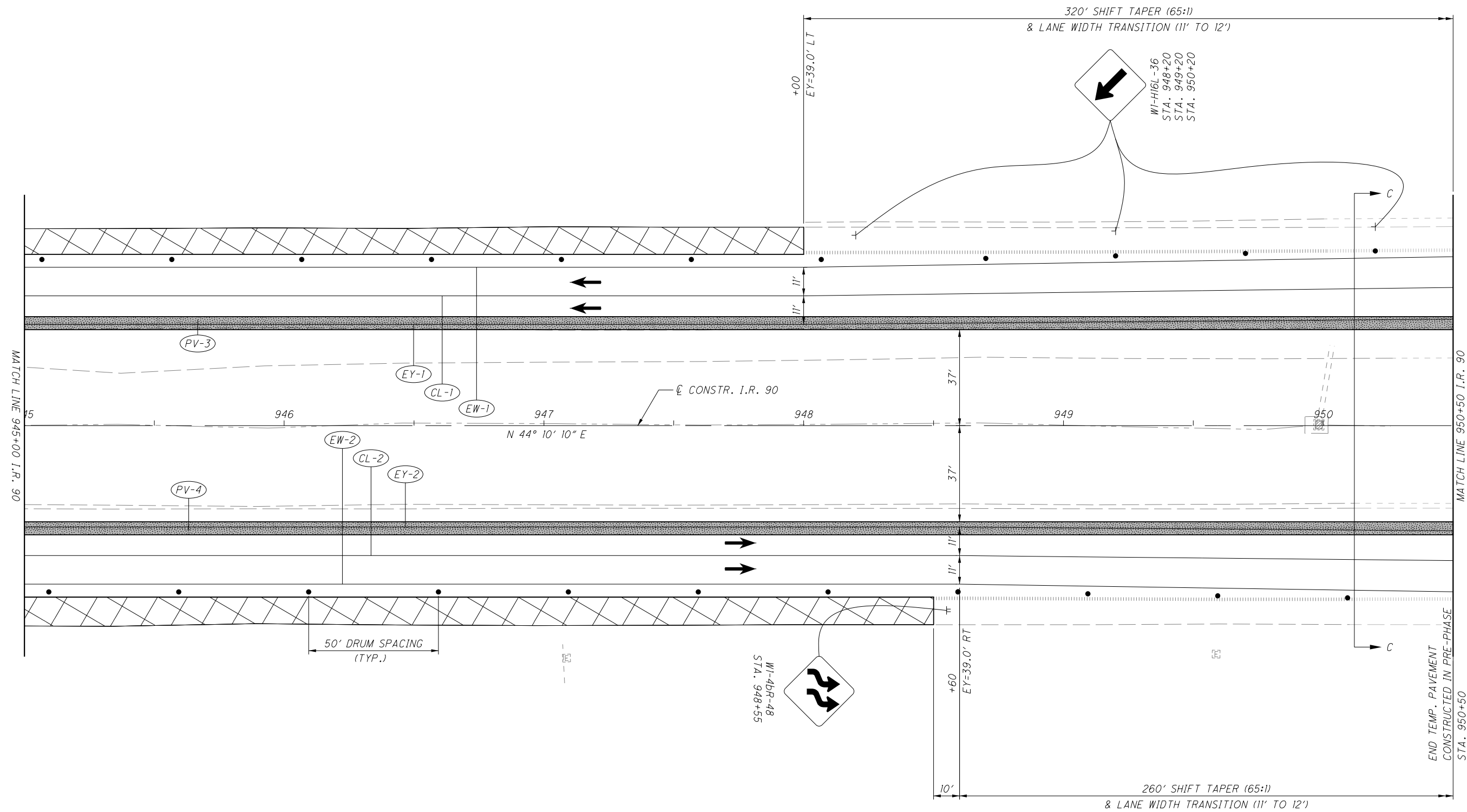
- NOTES:
1. FOR LEGEND, SEE SHEET 20
 2. FOR TYPICAL SECTION, SEE SHEET 17
 3. FOR EXISTING SIGNS, SEE SIGNING AND PAVEMENT MARKINGS SHEETS, 111 - 119
 4. INSIDE SHOULDER REPLACEMENT WITH TEMPORARY PAVEMENT TO BE INSTALLED PRIOR TO PHASE I START
 5. TEMPORARY PAVEMENT CONSTRUCTED OUTSIDE PERMANENT PAVEMENT LIMITS MAY REMAIN AFTER CONSTRUCTION

CALCULATED
MLV
CHECKED
JML

0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC - PHASE 1
STA. 939+50 TO STA. 945+00

LOR-90-17.85



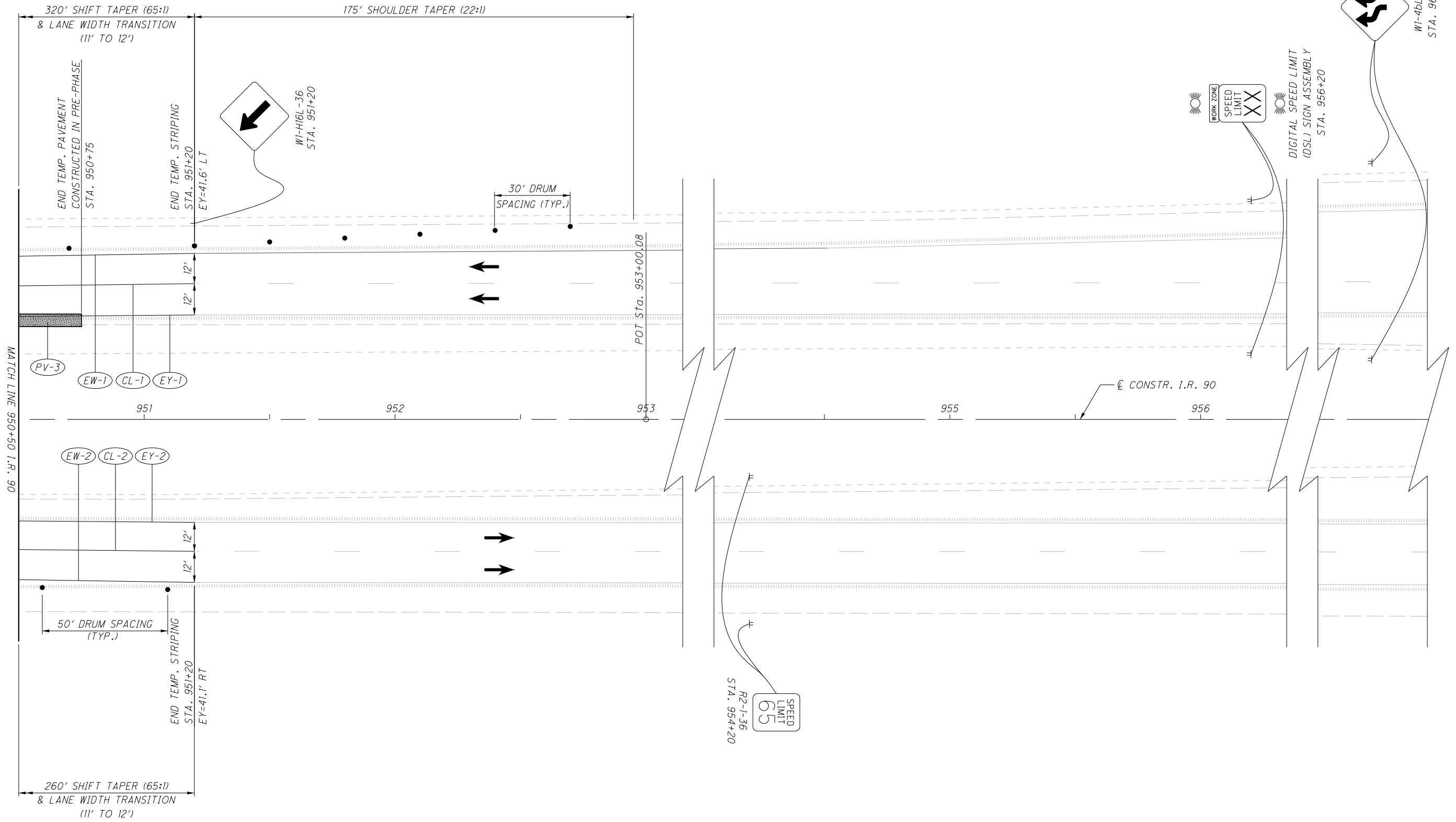
NOTES:
 1. FOR LEGEND, SEE SHEET 20
 2. FOR TYPICAL SECTION, SEE SHEET 17
 3. FOR EXISTING SIGNS, SEE SIGNING AND PAVEMENT MARKINGS SHEETS, 111 - 119
 4. INSIDE SHOULDER REPLACEMENT WITH TEMPORARY PAVEMENT TO BE INSTALLED PRIOR TO PHASE 1 START
 5. TEMPORARY PAVEMENT CONSTRUCTED OUTSIDE OF PERMANENT PAVEMENT LIMITS MAY REMAIN AFTER CONSTRUCTION

CALCULATED	ML V
CHECKED	JML

0 20 40
 HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC - PHASE 1
STA. 945+00 TO STA. 950+50

LOR-90-17.85



NOTES:
 1. FOR LEGEND, SEE SHEET 20
 2. FOR TYPICAL SECTION, SEE SHEET 17
 3. FOR EXISTING SIGNS, SEE SIGNING AND PAVEMENT MARKINGS SHEETS, 111 - 119
 4. INSIDE SHOULDER REPLACEMENT WITH TEMPORARY PAVEMENT TO BE INSTALLED PRIOR TO PHASE 1 START
 5. TEMPORARY PAVEMENT CONSTRUCTED OUTSIDE PERMANENT PAVEMENT LIMITS MAY REMAIN AFTER CONSTRUCTION

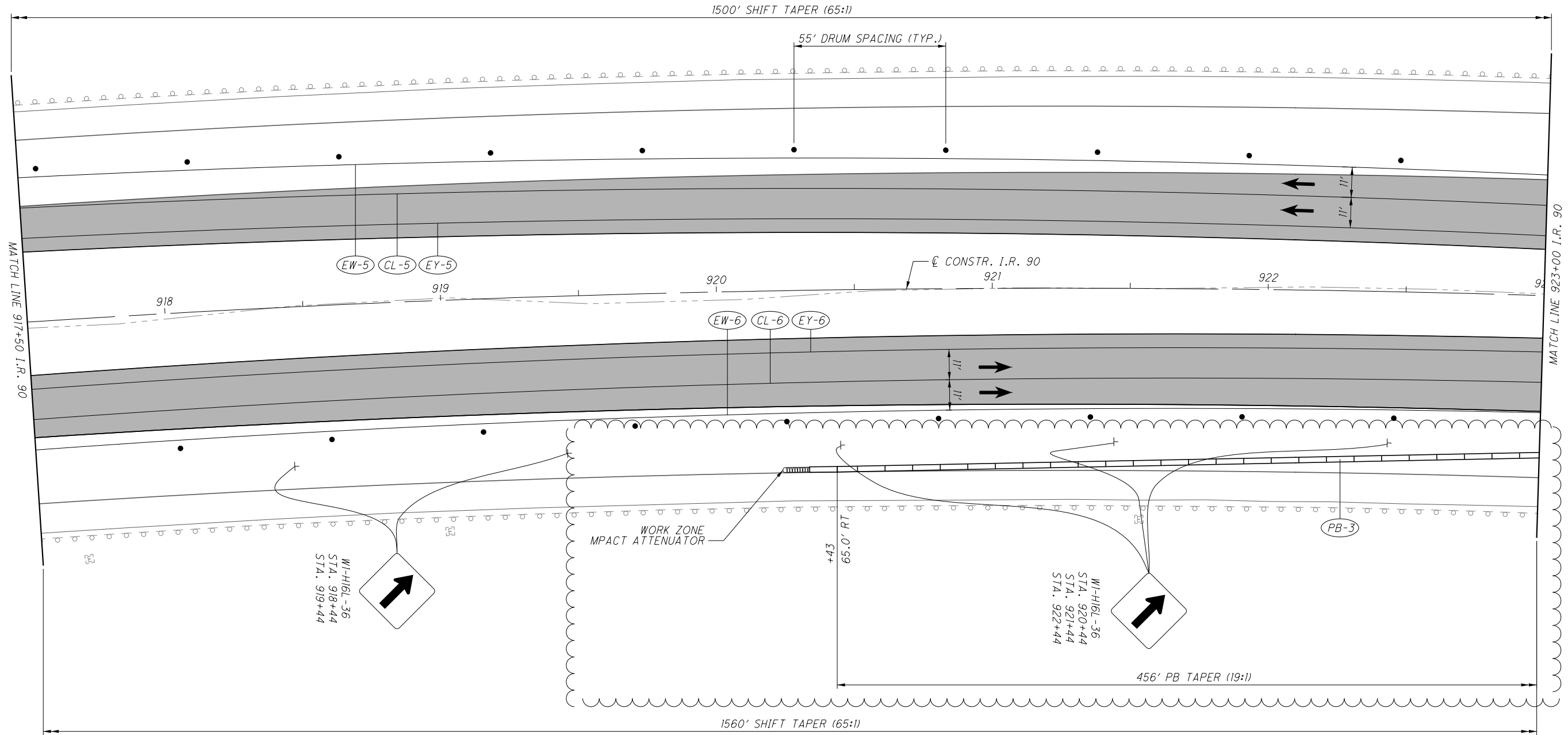
CALCULATED MLV
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0 20 40
 HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC - PHASE 1
STA. 950+50 TO STA. 961+20

LOR-90-17.85

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- NOTES:
1. FOR LEGEND, SEE SHEET 20
 2. FOR TYPICAL SECTION, SEE SHEET 19
 3. FOR EXISTING SIGNS, SEE SIGNING AND PAVEMENT MARKINGS SHEETS, 111 - 119

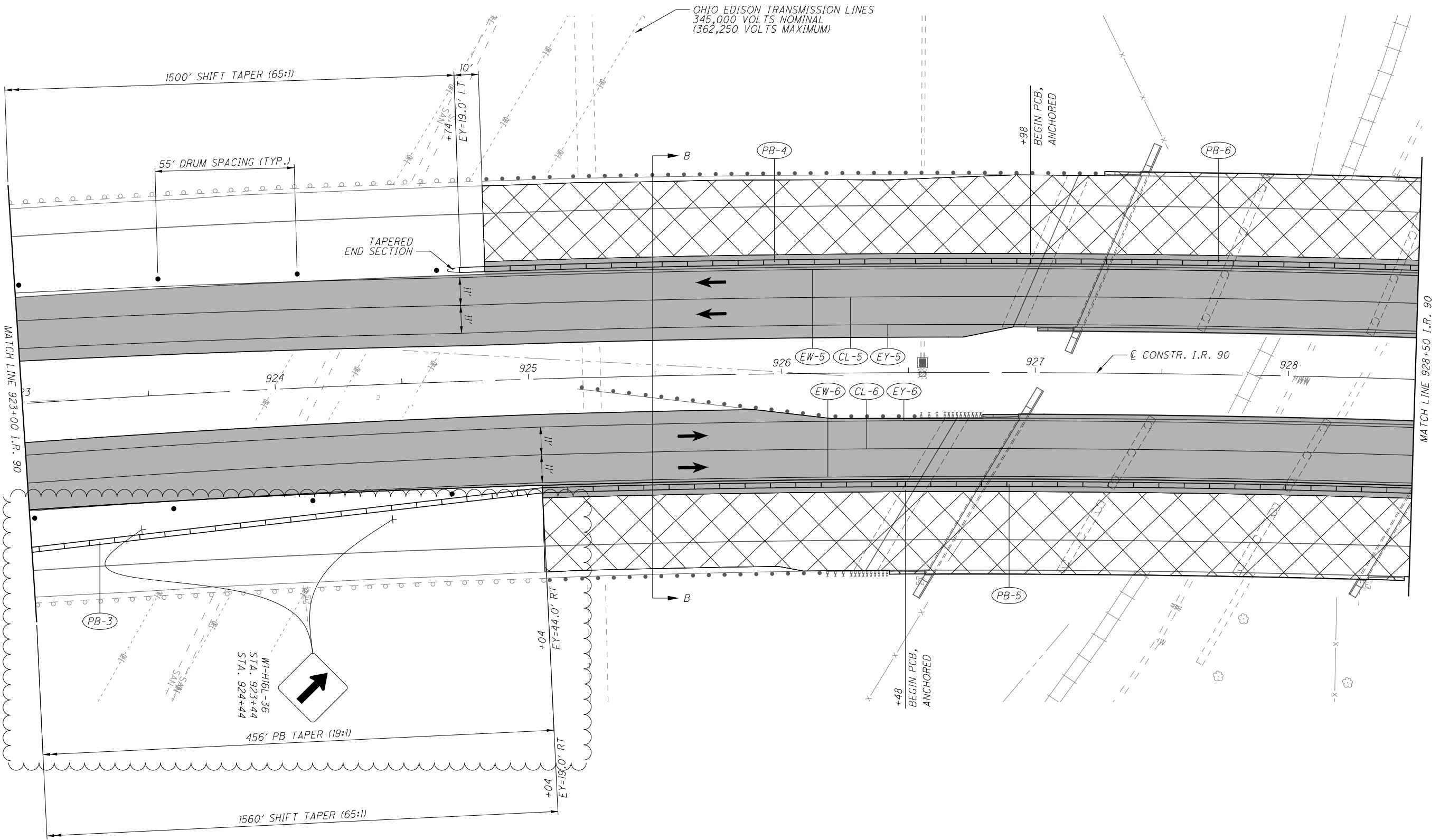
CALCULATED
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CHECKED
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0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC - PHASE 3
STA. 917+50 TO STA. 923+00

LOR-90-17.85

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MATCH LINE 923+00 I.R. 90

MATCH LINE 928+50 I.R. 90

OHIO EDISON TRANSMISSION LINES
345,000 VOLTS NOMINAL
(362,250 VOLTS MAXIMUM)

1500' SHIFT TAPER (65:1)

55' DRUM SPACING (TYP.)

TAPERED
END SECTION

W-1-HIBL - 36
STA. 923+44
STA. 924+44

456' PB TAPER (19:1)

1560' SHIFT TAPER (65:1)

NOTES:

1. FOR LEGEND, SEE SHEET 20
2. FOR TYPICAL SECTION, SEE SHEET 19
3. FOR EXISTING SIGNS, SEE SIGNING AND PAVEMENT MARKINGS SHEETS, III - 119

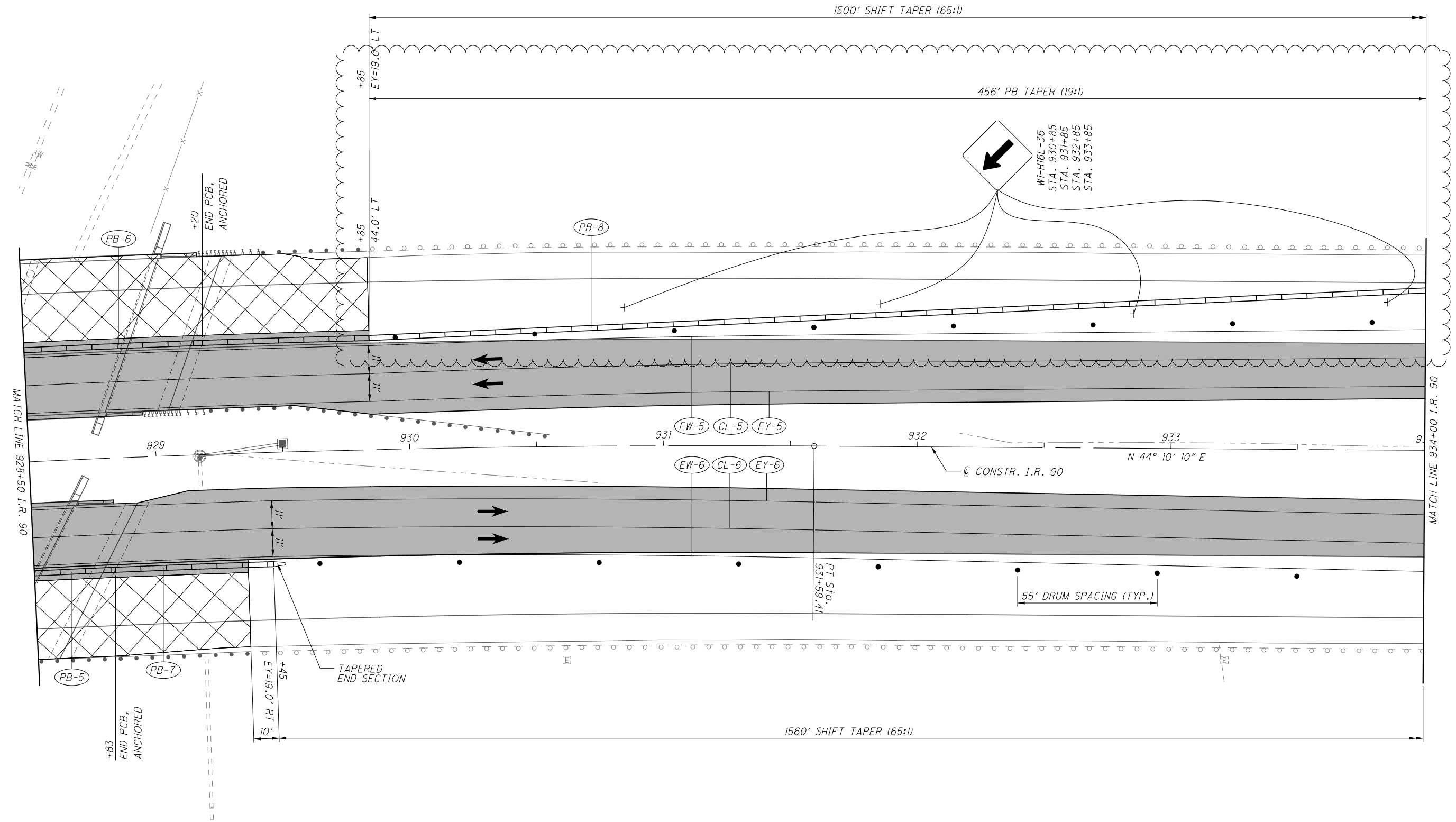
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0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC - PHASE 3
STA. 923+00 TO STA. 928+50

LOR-90-17.85

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- NOTES:
1. FOR LEGEND, SEE SHEET 20
 2. FOR TYPICAL SECTION, SEE SHEET 19
 3. FOR EXISTING SIGNS, SEE SIGNING AND PAVEMENT MARKINGS SHEETS, 111 - 119

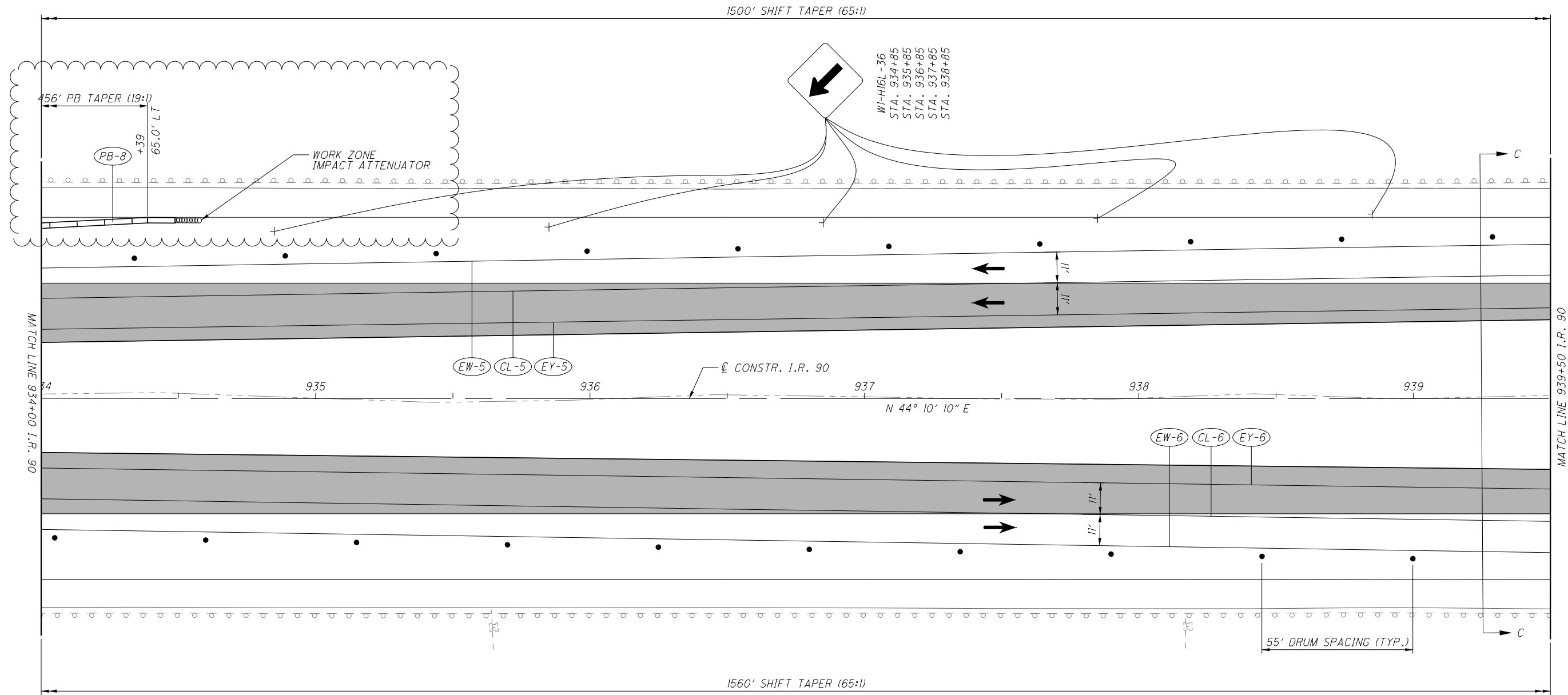
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0 20 40
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC - PHASE 3
STA. 928+50 TO STA. 934+00

LOR-90-17.85

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WI-H16L-36
 STA. 934+85
 STA. 935+85
 STA. 936+85
 STA. 937+85
 STA. 938+85

- NOTES:
1. FOR LEGEND, SEE SHEET 20
 2. FOR TYPICAL SECTION, SEE SHEET 19
 3. FOR EXISTING SIGNS, SEE SIGNING AND PAVEMENT MARKINGS SHEETS, III - 119

CALCULATED
 MLV
 CHECKED
 JML

0 20 40
 HORIZONTAL
 SCALE IN FEET

MAINTENANCE OF TRAFFIC - PHASE 3
STA. 934+00 TO STA. 939+50

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SHEET NUM.					PART.	ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
8	9	10	11	15							
				1,270		614	11630	1,270	FT	MAINTENANCE OF TRAFFIC INCREASED BARRIER DELINEATION	
				4		614	12380	4	EACH	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (UNIDIRECTIONAL)	
2						614	12484	2	EACH	WORK ZONE INCREASED PENALTIES SIGN	
15						614	12500	15	EACH	REPLACEMENT SIGN	
50						614	12600	50	EACH	REPLACEMENT DRUM	
				1,740		614	12801	1,740	EACH	WORK ZONE RAISED PAVEMENT MARKER, AS PER PLAN	10
				19		614	13310	19	EACH	BARRIER REFLECTOR, TYPE 1, (ONE WAY)	
				142		614	13310	142	EACH	BARRIER REFLECTOR, TYPE 1, (TWO WAY)	
				19		614	13350	19	EACH	OBJECT MARKER, ONE WAY	
				142		614	13360	142	EACH	OBJECT MARKER, TWO WAY	
LS	18					614	18601	18	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	9
				5,683		615	10000	LS		ROADS FOR MAINTAINING TRAFFIC	
						615	20000	5,683	SY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A	
55						616	10000	55	MGN	WATER	
				1,550		622	41100	1,550	FT	PORTABLE BARRIER, UNANCHORED	
				7,820		622	41110	7,820	FT	PORTABLE BARRIER, ANCHORED	
				9.54		642	00105	9.54	MILE	EDGE LINE, 6", TYPE 1, AS PER PLAN	11
				25,182		642	00401	25,182	FT	CHANNELIZING LINE, 8", TYPE 1, AS PER PLAN	11
			36			808	18700	36	SNMT	DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY	
						100	00300	LS		INCIDENTALS PREMIUM ON RAILROADS' PROTECTIVE PUBLIC LIABILITY AND PROPERTY DAMAGE	
						614	11000	LS		LIABILITY INSURANCE	
						614	11110	1,000	HOUR	MAINTAINING TRAFFIC LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
		1,000				619	16020	15	MNTH	FIELD OFFICE, TYPE C	
						623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING	
						624	10000	LS		MOBILIZATION	

GENERAL SUMMARY

LOR-90-17.85

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SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	202	606	606	606	625	202	202	607								
			GUARDRAIL REMOVED	GUARDRAIL TYPE MGS		BRIDGE TERMINAL ASSEMBLY, TYPE 1	BRIDGE TERMINAL ASSEMBLY, TYPE 2	PULL BOX REMOVED	CATCH BASIN REMOVED	PIPE REMOVED, 24" AND UNDER	FENCE REMOVED AND REBUILT, AS PER PLAN	FT	FT	EACH	EACH	EACH	FT	FT			
			FROM	TO		FT	FT	EACH	EACH	EACH	EACH	FT	FT								
63-65	R-1	IR-90	923+96.54	926+90.65	RT	295															
63-65	R-2	IR-90	924+84.23	927+46.86	LT	266															
63-65	G-1	IR-90	924+84.23	927+28.75	LT		248		1												
65	R-3	IR-90	925+04.23	926+67.76	RT	161															
65	G-2	IR-90	925+04.23	926+41.99	RT		112	1													
65	G-3	IR-90	925+19.06	926+79.39	RT		136	1													
65	R-4	IR-90	936+54.30		RT					1											
65	R-5	IR-90	928+27.38	929+35.00	RT	106															
65	R-6	IR-90	928+33.21		RT					1											
65	G-4	IR-90	928+46.04	929+35.00	RT		88	1													
65-67	R-7	IR-90	928+81.47	931+85.81	LT	307															
65	R-8	IR-90	928+96.18	929+85.00	LT	90															
65-67	G-5	IR-90	928+95.29	930+55.19	LT		137	1													
65	G-6	IR-90	929+18.79	929+85.00	LT		43	1													
65	R-9	IR-90	926+55.53	926+55.57	LT					1	8										
65	R-10	IR-90	929+17.26	929+17.27	RT					1	4										
65	F-1	IR-90	926+24.72	926+57.35	RT								60								
65	F-2	IR-90	927+30.24	927+51.31	LT								48								
65	F-3	IR-90	928+21.25	928+21.48	RT								57								
65	F-4	IR-90	929+02.13	929+16.39	LT								42								
TOTALS CARRIED TO GENERAL SUMMARY						1225	764	4	2	2	2	12	207								

ROADWAY SUBSUMMARY	LOR-90-17.85						
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CALCULATED	HB						
CHECKED	SNP						
52	196						