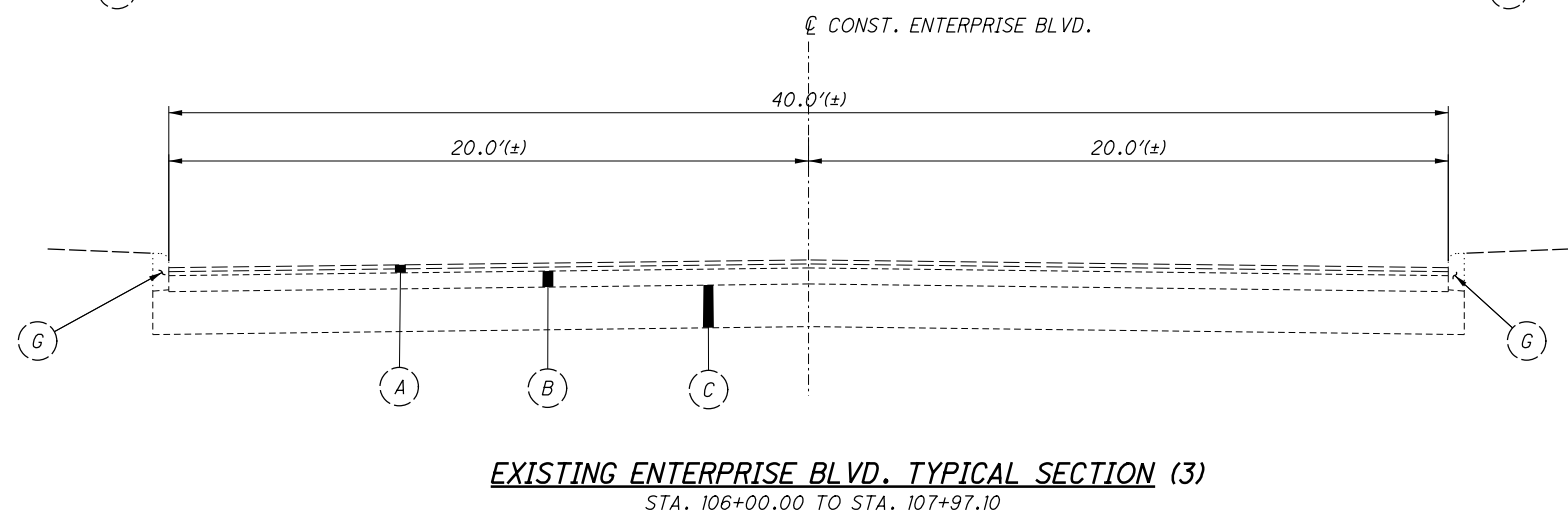
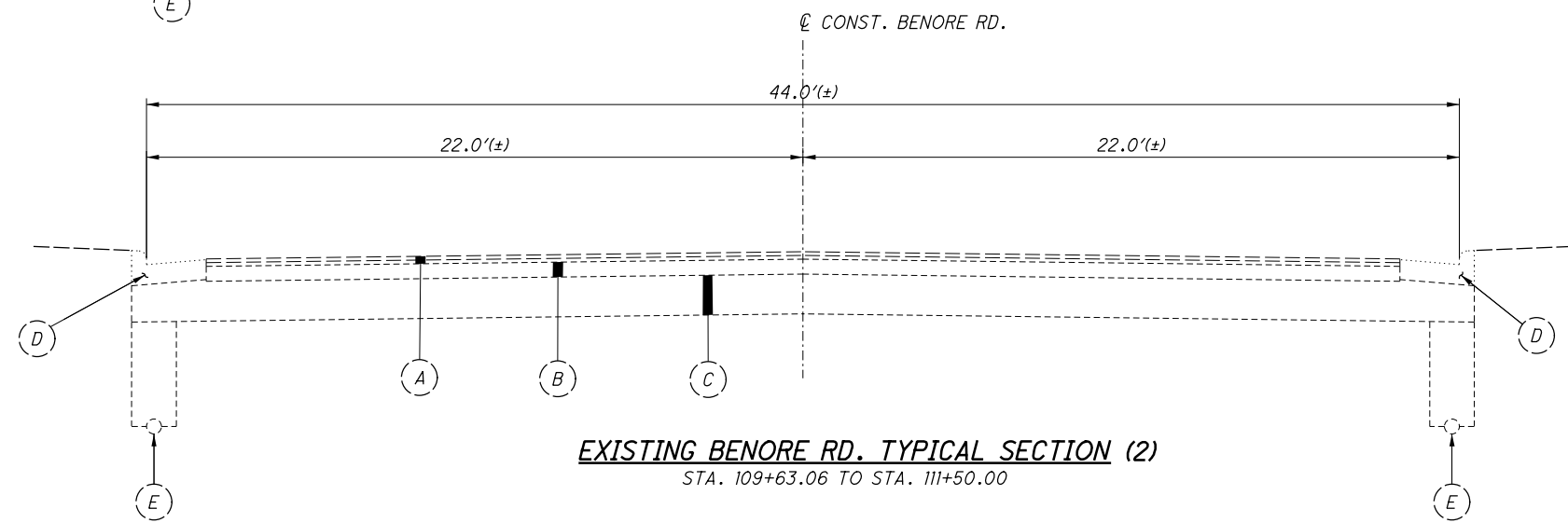
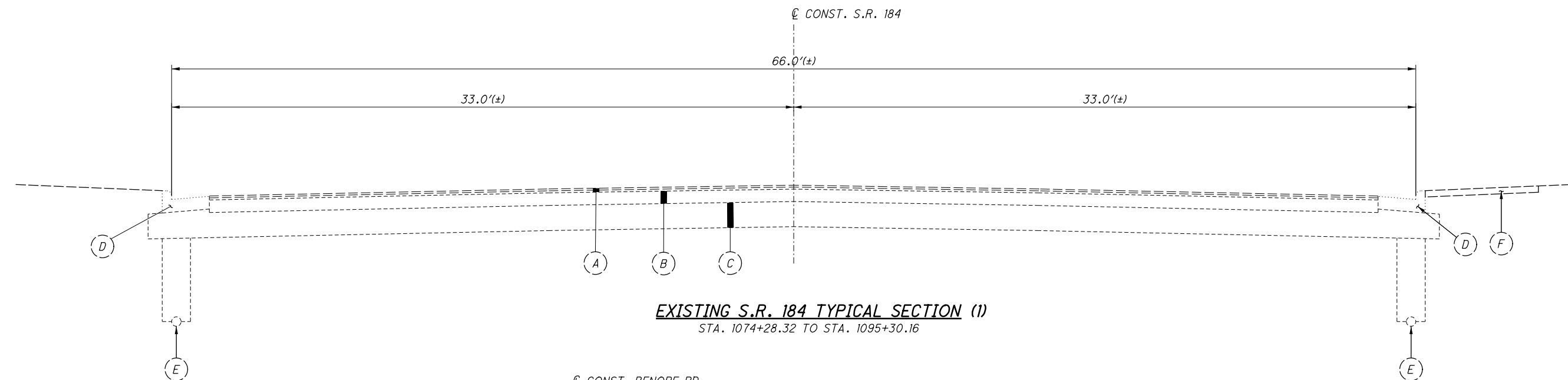


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- LEGEND**
- (A) 3"± ASPHALT CONCRETE
  - (B) 8"± ASPHALT CONCRETE BASE
  - (C) 16"± SUBBASE
  - (D) COMBINATION CURB AND GUTTER
  - (E) 6"± PIPE UNDERDRAINS
  - (F) SIDEWALK
  - (G) CURB

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EXISTING TYPICAL SECTIONS

LUC-184-8.92

**ROUNDING**

THE ROUNDING AT SLOPE BREAKPOINTS SHOWN ON THE TYPICAL SECTIONS APPLIES TO ALL CROSS-SECTIONS, EVEN THOUGH OTHERWISE SHOWN.

**UTILITIES**

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

AT&T OHIO INDEPENDENTS FIBER NETWORK  
130 N. ERIE ST, RM 714 13888 COUNTY RD 25A  
TOLEDO, OH 44883 WAPAKONETA, OH 45895  
419-245-7304 419-739-3114

BUCKEYE BROADBAND LEVEL 3 COMMUNICATIONS  
2700 OREGON RD. 1025 ELDORADO BLVD.  
NORTHWOOD, OH 43619 BROOMFIELD, CO 80021  
ATTN: MICHAEL SHEAHAN 512-742-1428  
419-724-3713

BUCKEYE PIPE LINE COMPANY SUNOCO LOGISTICS  
P.O. BOX 368 7155 INKSTER ROAD  
EMMAUS, PA 18049-0368 TAYLOR, MI 48180  
484-232-4000 313-670-1251

CITY OF TOLEDO - TOLEDO EDISON (USIC)  
DIVISION OF WATER 6099 ANGOLA RD.  
401 SOUTH ERIE STREET HOLLAND, OH 43528  
TOLEDO, OH 43602 419-249-5218  
419-936-2839

COLUMBIA GAS OF OHIO - MARATHON ASHLAND PIPE LINE  
TOLEDO COMPANY  
2901 E. MANHATTAN BLVD. 539 S. MAIN STREET  
TOLEDO, OH 43611 FINDLAY, OH 45840  
419-539-6066 419-422-2121

ENBRIDGE ENERGY LUCAS COUNTY ENGINEER'S OFFICE  
1409 HAMMOND AVE. 1 GOVERNMENT CENTER, SUITE 870  
SUPERIOR, WI 54880 TOLEDO, OH 43604-2259  
715-398-4500 419-213-4541 X4359

GAS RECOVERY SYSTEMS LUCAS COUNTY SANITARY  
10611 5 MILE RD. ENGINEERS  
NORTHVILLE, MI 48168 1111 SOUTH McCORD RD.  
248-305-7774 HOLLAND, OH 43528  
419-213-2926

CITY OF TOLEDO  
DIVISION OF ENGINEERING SERVICES  
600 JEFFERSON AVE. SUITE 300  
TOLEDO, OHIO 43604  
419-245-1315

CITY OF TOLEDO  
DIVISION OF TRANSPORTATION  
110 N. WESTWOOD  
TOLEDO, OHIO 43607  
419-245-1300

**CONSTRUCTION NOISE**

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, DO NOT OPERATE POWER-OPERATED CONSTRUCTION-TYPE DEVICES BETWEEN THE HOURS OF 9:00 PM AM AND 7:00 AM. IN ADDITION, DO NOT OPERATE AT ANY TIME ANY DEVICE IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT. CONTRACTOR WILL NEED TO APPLY FOR A NOISE WAIVER FROM CITY OF TOLEDO.

**SURVEYING PARAMETERS**

PRIMARY PROJECT CONTROL MONUMENTS GOVERN ALL POSITIONING ON ODOT PROJECTS. SEE SHEET 2 OF THE PLANS FOR A TABLE CONTAINING PROJECT CONTROL INFORMATION.

USE THE FOLLOWING PROJECT CONTROL, VERTICAL POSITIONING, AND HORIZONTAL POSITIONING PARAMETERS FOR ALL SURVEYING:

**PROJECT CONTROL**

POSITIONING METHOD: ODOT VRS

**VERTICAL POSITIONING**

ORTHOMETRIC HEIGHT DATUM: NAVD 88  
GEOID: 12A

**HORIZONTAL POSITIONING**

REFERENCE FRAME: NAD 83 (2011)  
ELLIPSOID: GRS 80  
MAP PROJECTION: LAMBERT CONFORMAL CONIC  
COORDINATE SYSTEM: OHIO STATE PLANE NORTH ZONE  
COMBINED SCALE FACTOR: 1.0000000  
ORIGIN OF COORDINATE SYSTEM: OHIO NORTH ZONE  
NORTHING = 0.000  
EASTING = 0.000

USE THE POSITIONING METHODS AND MONUMENT TYPE USED IN THE ORIGINAL SURVEY TO RESTORE ALL MONUMENTS RELATED TO PRIMARY PROJECT CONTROL THAT ARE DAMAGED OR DESTROYED BY CONSTRUCTION ACTIVITIES. RESTORE THE DAMAGED OR DESTROYED MONUMENTS IN ACCORDANCE WITH CMS 623.

UNITS ARE IN U.S. SURVEY FEET. USE THE FOLLOWING CONVERSION FACTOR: 1 METER = 3.280833333 U.S. SURVEY FEET.

**WORK LIMITS**

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

**CLEARING AND GRUBBING**

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

**REVIEW OF DRAINAGE FACILITIES**

BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE BY THE STATE, REPRESENTATIVES OF THE STATE AND THE CONTRACTOR, ALONG WITH LOCAL REPRESENTATIVES, SHALL MAKE AN INSPECTION OF ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCE SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING BY THE STATE.

ALL NEW CONDUITS, INLETS, CATCH BASINS, AND MANHOLES CONSTRUCTED AS A PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEAN CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE STATE.

ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE MENTIONED PARTIES SHALL BE MAINTAINED AND LEFT IN A CONDITION REASONABLY COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. ANY CHANGE IN THE CONDITION RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEMS.

**MANHOLES, CATCH BASINS AND INLETS REMOVED OR ABANDONED**

ALL CASTINGS SHALL BE CAREFULLY REMOVED AND STORED WITHIN THE RIGHT OF WAY FOR SALVAGE BY STATE FORCES.

PAYMENT FOR ALL OF THE ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 202 ITEM.

**ITEM SPECIAL - MISCELLANEOUS METAL**

EXISTING CASTINGS MAY PROVE TO BE UNSUITABLE FOR REUSE, AS DETERMINED BY THE ENGINEER. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE THE CASTINGS OF THE REQUIRED TYPE, SIZE AND STRENGTH (HEAVY OR LIGHT DUTY) FOR THE PARTICULAR STRUCTURE IN QUESTION. ALL MATERIAL SHALL MEET ITEM 611 OF THE SPECIFICATIONS AND SHALL HAVE THE PRIOR APPROVAL OF THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

SPECIAL, MISCELLANEOUS METAL 500 POUNDS

THE CONTRACTOR IS CAUTIONED TO USE EXTREME CARE IN THE REMOVAL, STORAGE AND REPLACEMENT OF ALL EXISTING CASTINGS. CASTINGS DAMAGED BY THE NEGLIGENCE OF THE CONTRACTOR, AS DETERMINED BY THE ENGINEER, SHALL BE REPLACED WITH THE PROPER NEW CASTINGS AT THE EXPENSE OF THE CONTRACTOR.

**TEMPORARY DRAINAGE ITEMS**

TEMPORARY DRAINAGE ITEMS LABELED ON THE MAINTENANCE OF TRAFFIC PLAN ARE ITEMIZED IN THE MOT GENERAL SUMMARY FOR INFORMATION ONLY AND INCLUDED IN ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, LUMP SUM.

**PART-WIDTH CONSTRUCTION**

BECAUSE OF THE NECESSITY TO BUILD THIS PROJECT UNDER TRAFFIC AND TO CONSTRUCT THE FULL PAVEMENT WIDTH IN STAGES, EXERCISE CARE TO PREVENT THE CONSTRUCTION OF A BUTT JOINT IN THE BASE COURSES. LAP LONGITUDINAL JOINTS AS SHOWN ON STANDARD CONSTRUCTION DRAWING BP-3.1.

**SEEDING AND MULCHING**

THE FOLLOWING QUANTITIES ARE PROVIDED TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDED AREAS:

|  |             |
|--|-------------|
| 659, SOIL ANALYSIS TEST                              | 2 EACH      |
| 659, TOPSOIL   | 189 CU. YD. |
| 659, SEEDING AND MULCHING - SEE SHEETS 52 ,56 AND 29 | 85 SQ. YD.  |
| 659, REPAIR SEEDING AND MULCHING                     | 85 SQ. YD.  |
| 659, INTER-SEEDING                                   | 85 SQ. YD.  |
| 659, COMMERCIAL FERTILIZER                           | 0.23 TON    |
| 659, LIME  | 0.04 ACRES  |
| 659, WATER   | 9 M. GAL.   |

SEEDING AND MULCHING SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE RIGHT-OF-WAY LINES, AND WITHIN THE CONSTRUCTION LIMITS FOR AREAS OUTSIDE THE RIGHT-OF-WAY LINES COVERED BY WORK AGREEMENT OR SLOPE EASEMENT. QUANTITY CALCULATIONS FOR SEEDING AND MULCHING ARE BASED ON THESE LIMITS.

**ITEM 690-98400 - SPECIAL - MISC.: WATER PERMIT FEES, FIXED LUMP SUM (\$7,650.00)**

THIS ITEM OF WORK IS TO COMPENSATE THE CITY OF TOLEDO DIVISION OF WATER DISTRIBUTION FOR WATER PERMIT FEES FOR WORK ASSOCIATED WITH THIS PROJECT. THE WATER PERMIT FEES SHALL BE PAID BY THE CONTRACTOR PRIOR TO THE CITY COMMENCING WITH THE WORK. THE CONTRACTOR WILL NOT BE REQUIRED TO PERFORM ANY FIELD WORK ON THE PROJECT FOR THESE ITEMS.

| QTY | UNIT | DESCRIPTION                    | UNIT FEE   | TOTAL FEE  |
|-----|------|--------------------------------|------------|------------|
| 1   | EACH | FIRE HYDRANT REMOVED           | \$2,200.00 | \$2,200.00 |
| 1   | EACH | FIRE HYDRANT ADJUSTED TO GRADE | \$850.00   | \$850.00   |
| 1   | EACH | FIRE HYDRANT                   | \$4,600.00 | \$4,600.00 |

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**GENERAL NOTES**

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**ITEM 614, MAINTAINING TRAFFIC**

A MINIMUM OF 1 LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES ON S.R. 184 AND BENOIRE RD. BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, AND ITEM 615 ROADS FOR MAINTAINING TRAFFIC. ENTERPRISE BLVD. WILL BE CLOSED FOR THE DURATION OF THE PROJECT.

ALL WORK ASSOCIATED WITH MAINTENANCE OF TRAFFIC INCLUDING SET-UP, PAVING, SIDEWALK AND CURB RAMP WORK, PHASE CHANGES, TEMPORARY TRAFFIC SIGNALS AND TEAR DOWNS SHALL BE COMPLETED BETWEEN THE HOURS OF 8:00PM AND 6:00AM.

PEDESTRIAN TRAFFIC SHALL BE MAINTAINED AT ALL TIMES THROUGH CONSTRUCTION ZONE AS DETAILED IN THE PLANS.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE.

NOTICE OF CLOSURE SIGN TIME TABLE  
ITEM DURATION SIGN DISPLAYED  
OF CLOSURE TO PUBLIC

|                     |                        |                                   |
|---------------------|------------------------|-----------------------------------|
| RAMP &              | >=2 WEEKS              | 14 CALENDAR DAYS PRIOR TO CLOSURE |
| ROAD                | > 12 HOURS & < 2 WEEKS | 7 CALENDAR DAYS PRIOR TO CLOSURE  |
| CLOSURES < 12 HOURS |                        | 2 BUSINESS DAYS PRIOR TO CLOSURE  |

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS PHONE NUMBER SHALL BE (419) 373-4428.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC.

|           |  |             |
|-----------|--|-------------|
| ITEM 614, | ASPHALT CONCRETE FOR MAINTAINING TRAFFIC | 100 CU. YD. |
| ITEM 616, | WATER                                    | 20 M. GAL.  |

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48 X 30 INCH ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AS DETAILED IN SCD MT-101.60 AT THE FOLLOWING LOCATIONS DURING PERIODS IN WHICH THE AFFECTED ROADS ARE CLOSED TO TRAFFIC.

**PHASE 1:**

- 1) S.R. 184 EB, JUST EAST OF RAINTREE PARKWAY.
- 2) S.R. 184 EB, JUST WEST OF INTERSECTION OF S.R. 184 & BENOIRE/ENTERPRISE.
- 3) S.R. 184 EB, JUST EAST OF INTERSECTION OF S.R. 184 & BENOIRE/ENTERPRISE.

**PHASE 2:**

- 1) S.R. 184 WB, JUST WEST OF INTERSECTION OF S.R. 184 & BENOIRE/ENTERPRISE.
- 2) ENTERPRISE BLVD. AT INTERSECTION OF S.R. 184 & BENOIRE/ENTERPRISE.
- 3) S.R. 184 WB, JUST EAST OF INTERSECTION OF S.R. 184 & BENOIRE/ENTERPRISE.
- 4) S.R. 184 WB, JUST WEST OF INTERSECTION OF S.R. 184 & FUTURA/BENOIRE.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

**PLACEMENT OF ASPHALT CONCRETE**

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

**DUST CONTROL**

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

|           |       |            |
|-----------|-------|------------|
| ITEM 616, | WATER | 10 M. GAL. |
|-----------|-------|------------|

**ITEM 614, REPLACEMENT SIGN**

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 5 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

**ITEM 614, REPLACEMENT DRUM**

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL

AN ESTIMATED QUANTITY OF 5 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

**ITEM 615, ROADS FOR MAINTAINING TRAFFIC, AS PER PLAN**

IN ADDITION TO ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, ITEM 615, ROADS FOR MAINTAINING TRAFFIC SHALL INCLUDE ALL ITEMS NECESSARY TO PROVIDE A COMPLETE, FUNCTIONAL, AND SAFE INSTALLATION OF TEMPORARY ROAD FOR PUBLIC USE.

ITEM 615, ROADS FOR MAINTAINING TRAFFIC SHALL CONFORM TO C&MS ITEM 615 AND SHALL INCLUDE, BUT NOT LIMITED TO, INSTALLATION, MAINTENANCE, AND REMOVAL OF ALL EARTHWORK, GUARDRAIL, SIDEWALK, AND DRAINAGE NECESSARY TO PROVIDE THE TEMPORARY ROAD INSTALLATION.

ITEMS HAVE BEEN INCLUDED ON SHEETS 40-41 OF THE PLANS AS INFORMATION ONLY AND SHOULD BE INCLUDED IN ITEM 615, ROADS FOR MAINTAINING TRAFFIC, AS PER PLAN.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

|           |   |
|-----------|---|
| ITEM 615, | ROADS FOR MAINTAINING TRAFFIC, LUMP SUM AS PER PLAN |
|-----------|---|

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MAINTENANCE OF TRAFFIC GENERAL NOTES

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**DELINEATION OF PORTABLE AND PERMANENT BARRIER**

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL; AND, ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70. WHEN THE PB CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

ITEM 614, BARRIER REFLECTOR, TYPE 1 (ONE-WAY) SEE SHEET 14

ITEM 614, OBJECT MARKER, (TWO-WAY) SEE SHEET 19,26

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE ABOVE ITEMS.

**ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS**

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN A NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING THE SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 120 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

**NOTIFICATION OF TRAFFIC RESTRICTIONS**

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

| NOTIFICATION TIME TABLE                         |                       |  |
|---|-----------------------|--|
| ITEM  | DURATION OF CLOSURE   | NOTICE DUE TO PERMITS & PIO              |
| RAMP & ROAD CLOSURES                            | >= 2 WEEKS            | 21 CALENDAR DAYS PRIOR TO CLOSURE        |
|   | > 12HOURS & < 2 WEEKS | 14 CALENDAR DAYS PRIOR TO CLOSURE        |
|   | < 12 HOURS            | 4 CALENDAR DAYS PRIOR TO CLOSURE         |
| LANE CLOSURES & RESTRICTIONS                    | >= 2 WEEKS            | 14 CALENDAR DAYS PRIOR TO CLOSURE        |
|   | < 2 WEEKS             | 5 BUSINESS DAYS PRIOR TO CLOSURE         |
| START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES | N/A                   | 14 CALENDAR DAYS PRIOR TO IMPLEMENTATION |

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

THE PROJECT ENGINEER WILL FORWARD THIS INFORMATION TO THE FOLLOWING:

DISTRICT PUBLIC INFORMATION OFFICER (PIO) BY PHONE AT: (419) 373-4428 OR EMAIL AT: D02.pio@dot.ohio.gov

DISTRICT PERMIT SECTION BY PHONE AT: (419) 373-4301 OR EMAIL AT: D02.permits@dot.ohio.gov

PRIOR TO CLOSING ANY STREET OR TRAFFIC LANE, THE CONTRACTOR SHALL NOTIFY THE CITY OF TOLEDO, DIVISION OF TRANSPORTATION, AT 419-245-1300 OR BY EMAIL AT SHAWN.COMES@TOLEDO.OH.GOV AT LEAST SEVEN (7) DAYS PRIOR TO STARTING WORK.

**ITEM 204 - SUBGRADE COMPACTION AND PROOF ROLLING**

CONSTRUCT THE SUBGRADE AS FOLLOWS AND IN THE FOLLOWING SEQUENCE:

- SHAPE THE SUBGRADE TO WITHIN 0.2 FEET OF THE PLAN SUBGRADE ELEVATION.
- EXCAVATE AND REPLACE UNSUITABLE SUBGRADE BEFORE PROOF ROLLING. THE EXCAVATION LIMITS ARE SHOWN AND LABELED ON THE CROSS SECTIONS AS UNSUITABLE SUBGRADE. UNSUITABLE SUBGRADE INCLUDES UNSUITABLE SOIL (A-4B, A-2-5, A-5, A-7-5, AND SOIL WITH A LIQUID LIMIT GREATER THAN 65) AND ANY COAL, SHALE, OR ROCK WHICH NEEDS TO BE REMOVED ACCORDING TO SECTION 204.05 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS (C&MS). IF THERE IS UNSUITABLE SUBGRADE IN A SHALLOW FILL LOCATION, EXCAVATE AND REPLACE THE UNSUITABLE SUBGRADE BEFORE CONSTRUCTING THE SHALLOW FILL AND SHAPING THE SUBGRADE.
- COMPACT THE SUBGRADE ACCORDING TO C&MS 204.03.
- APPROXIMATE LIMITS FOR EXCAVATION OF UNSTABLE SUBGRADE ARE SHOWN AND LABELED ON THE CROSS SECTIONS AS UNSTABLE SUBGRADE. THE ENGINEER WILL IDENTIFY THE ACTUAL LIMITS OF EXCAVATION FOR UNSTABLE SUBGRADE BASED ON THE PROOF ROLLING RESULTS AND VISUAL OBSERVATIONS. PROOF ROLL THE COMPACTED SUBGRADE ACCORDING TO C&MS 204.06.
- EXCAVATE UNSTABLE SUBGRADE AS DIRECTED BY THE ENGINEER AND STABILIZE BY REPLACING WITH THE SPECIFIED MATERIALS ACCORDING TO C&MS 204.07. EXCAVATIONS WILL EXTEND 18 INCHES BEYOND THE EDGE OF THE SURFACE OF THE PAVEMENT, PAVED SHOULDERS, OR PAVED MEDIANS.
- PROOF ROLL THE STABILIZED AREAS ACCORDING TO C&MS 204.06 TO VERIFY STABILITY.
- FINE GRADE THE SUBGRADE TO THE SPECIFIED GRADE.

EXISTING UTILITIES WITHIN ANY UNSUITABLE SUBGRADE AREA SHALL NOT BE DISTURBED DURING EXCAVATION AND REPLACEMENT ACTIVITIES.

THE QUANTITIES FOR EXCAVATING THE UNSUITABLE SUBGRADE AND UNSTABLE SUBGRADE AS PER THE GEOTECHNICAL REPORT (STRUCTURE FOUNDATION EXPLORATION - FINAL DATED MAY 14, 2021) ARE BOTH PAID UNDER ITEM 204 EXCAVATION OF SUBGRADE.

ESTIMATED QUANTITIES HAVE BEEN PROVIDED BELOW AND CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

ITEM 204, EXCAVATION OF SUBGRADE 1,160 CY  
ITEM 204, GRANULAR EMBANKMENT, TYPE C 1,160 CY

CALCULATED  
AJK  
CHECKED  
JMZ

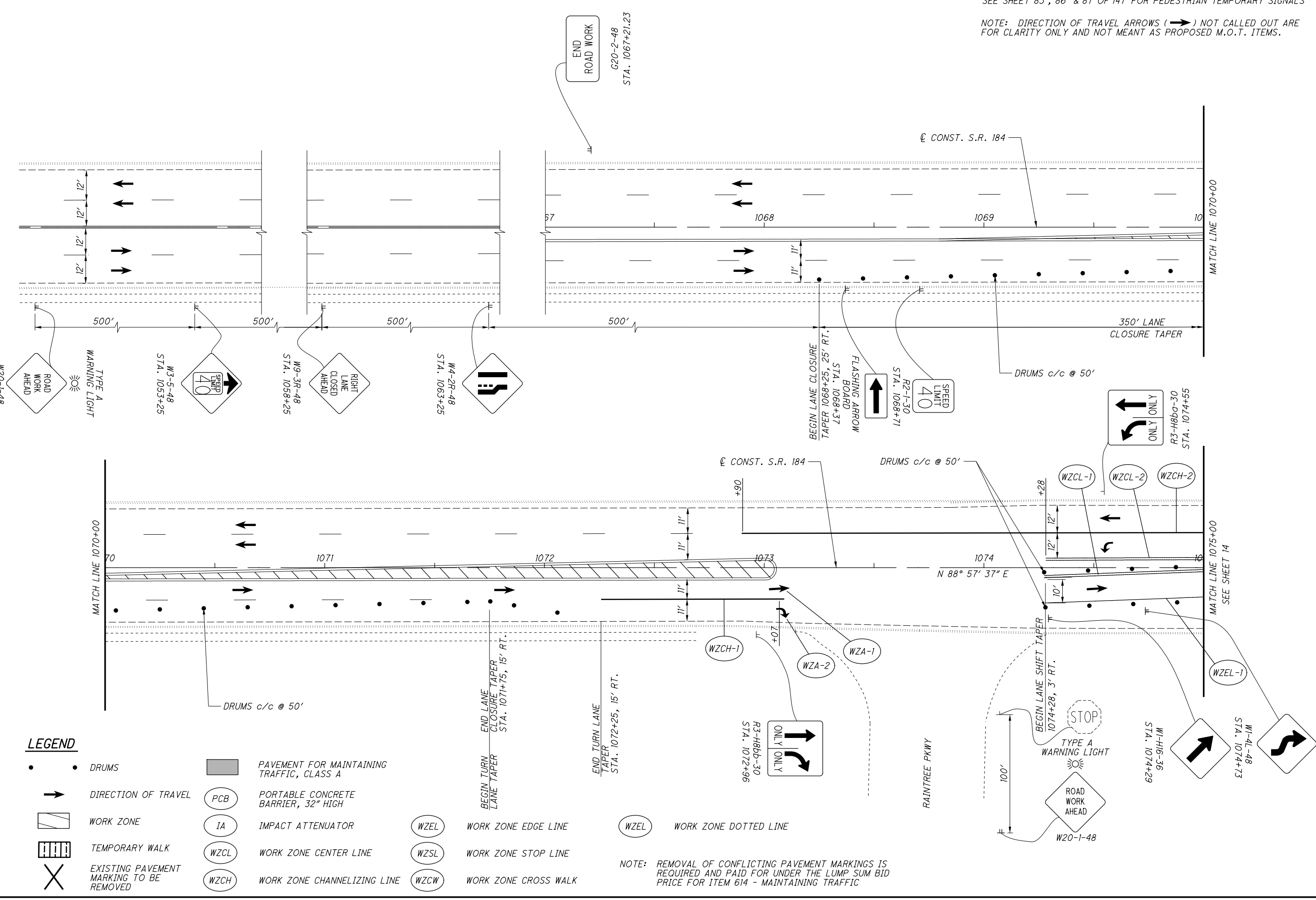
MAINTENANCE OF TRAFFIC GENERAL NOTES

LUC-184-8.92

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SEE SHEET 83, 86 & 87 OF 147 FOR PEDESTRIAN TEMPORARY SIGNALS  
 NOTE: DIRECTION OF TRAVEL ARROWS (→) NOT CALLED OUT ARE FOR CLARITY ONLY AND NOT MEANT AS PROPOSED M.O.T. ITEMS.

CALCULATED ARA CHECKED JMZ



**LEGEND**

- DRUMS
- DIRECTION OF TRAVEL
- ▨ WORK ZONE
- ▩ TEMPORARY WALK
- ✕ EXISTING PAVEMENT MARKING TO BE REMOVED
- ▒ PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A
- PCB PORTABLE CONCRETE BARRIER, 32" HIGH
- IA IMPACT ATTENUATOR
- WZEL WORK ZONE EDGE LINE
- WZSL WORK ZONE STOP LINE
- WZCH WORK ZONE CHANNELIZING LINE
- WZCZ WORK ZONE CROSS WALK
- WZEL WORK ZONE DOTTED LINE

NOTE: REMOVAL OF CONFLICTING PAVEMENT MARKINGS IS REQUIRED AND PAID FOR UNDER THE LUMP SUM BID PRICE FOR ITEM 614 - MAINTAINING TRAFFIC

**MAINTENANCE OF TRAFFIC PHASE 1**  
**S.R. 184 - STA. 1048+25 TO STA. 1075+00**

**LUC-184-8.92**

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NOTE: REMOVAL OF CONFLICTING PAVEMENT MARKINGS IS REQUIRED AND PAID FOR UNDER THE LUMP SUM BID PRICE FOR ITEM 614 - MAINTAINING TRAFFIC

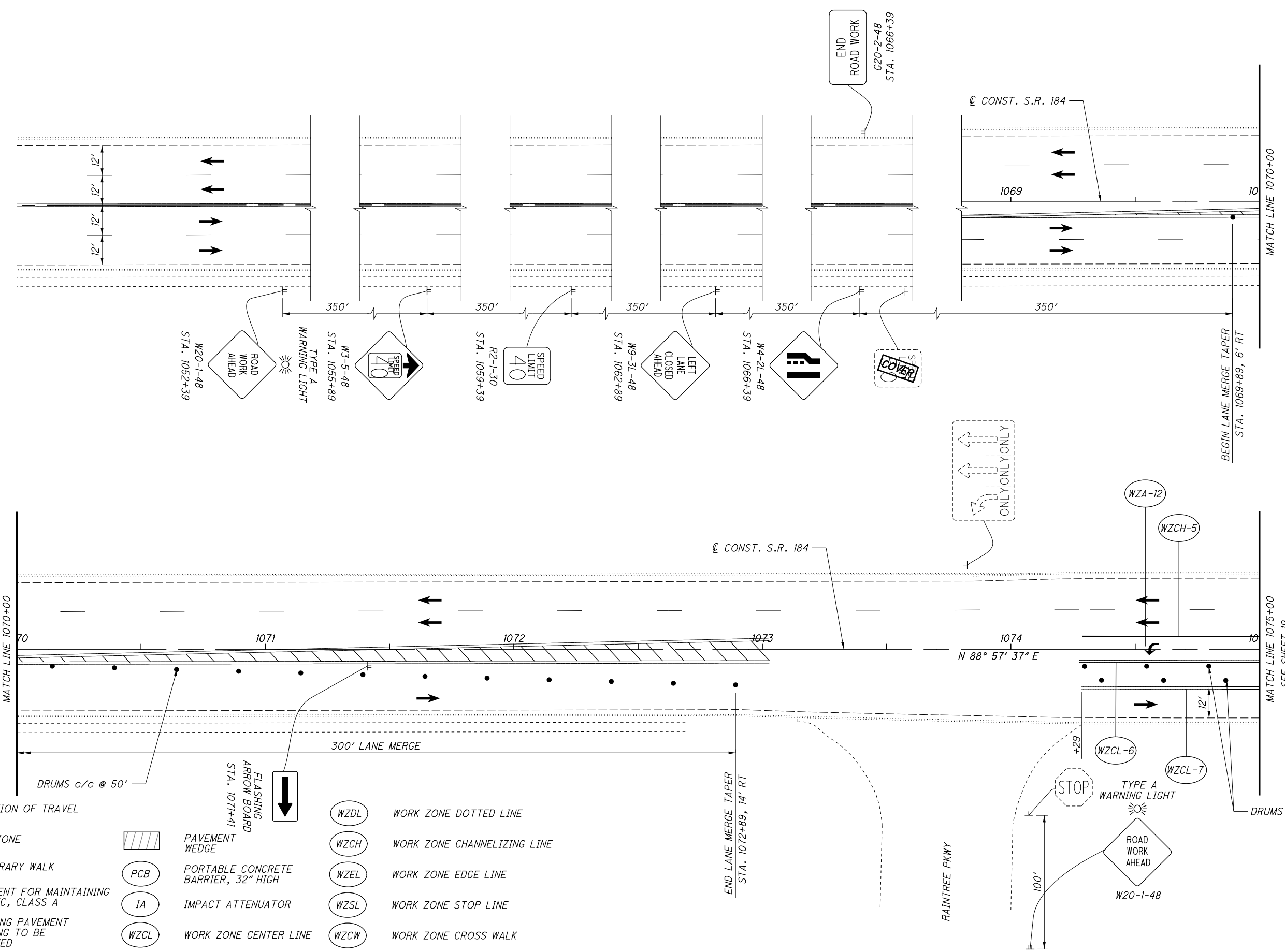
NOTE: DIRECTION OF TRAVEL ARROWS (→) NOT CALLED OUT ARE FOR CLARITY ONLY AND NOT MEANT AS PROPOSED M.O.T. ITEMS.

CALCULATED  
AJK  
CHECKED  
JMZ

0 50 100  
HORIZONTAL  
SCALE IN FEET

**MAINTENANCE OF TRAFFIC PHASE 2**  
**S.R. 184 - STA. 1052+39 TO STA. 1075+00**

**LUC-184-8.92**

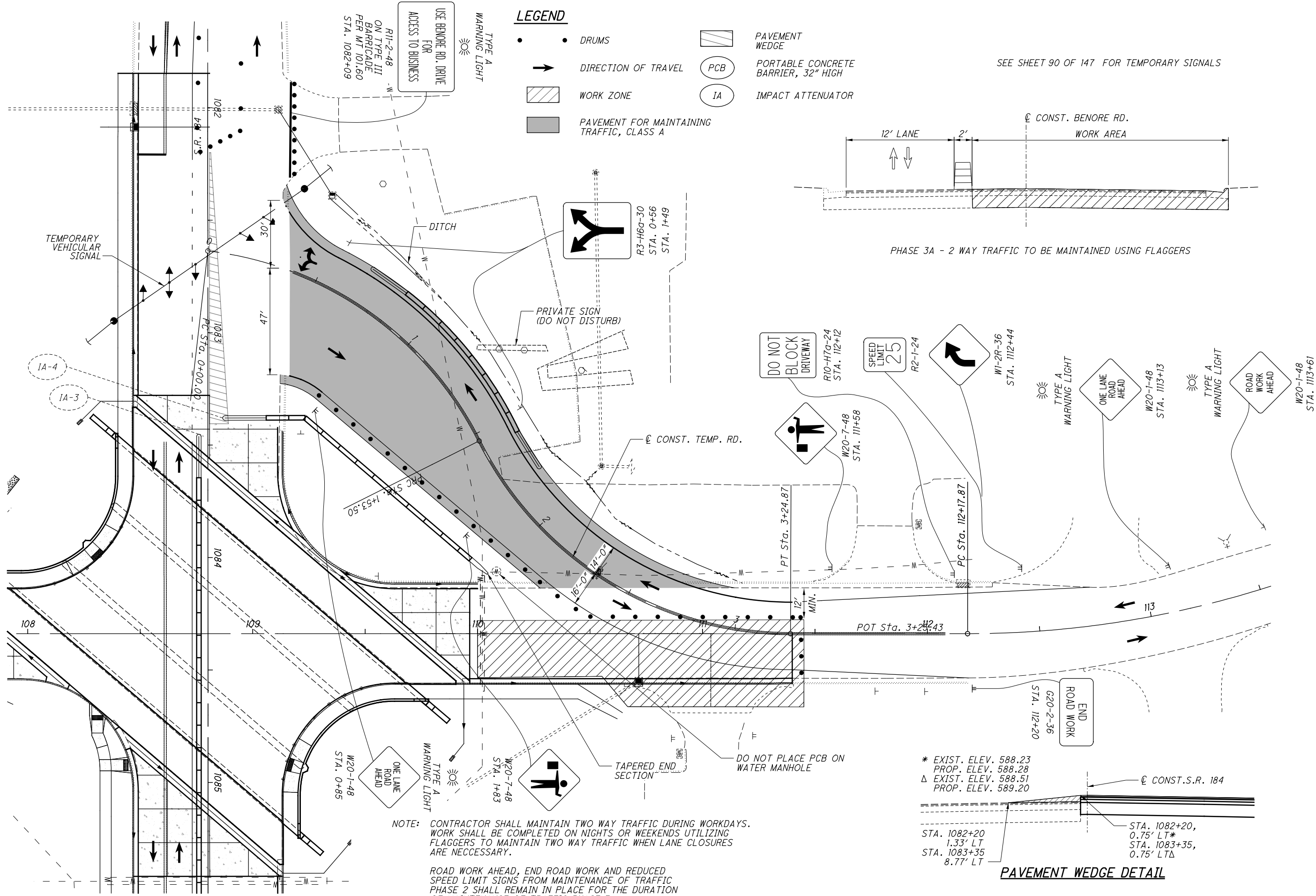


**LEGEND**

- DRUMS
- DIRECTION OF TRAVEL
- WORK ZONE
- TEMPORARY WALK
- PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A
- EXISTING PAVEMENT MARKING TO BE REMOVED
- DRUMS c/c @ 50'
- FLASHING ARROW BOARD STA. 1071+41
- PAVEMENT WEDGE
- PORTABLE CONCRETE BARRIER, 32" HIGH
- IMPACT ATTENUATOR
- WORK ZONE CENTER LINE
- WORK ZONE DOTTED LINE
- WORK ZONE CHANNELIZING LINE
- WORK ZONE EDGE LINE
- WORK ZONE STOP LINE
- WORK ZONE CROSS WALK

RAINTREE PKWY

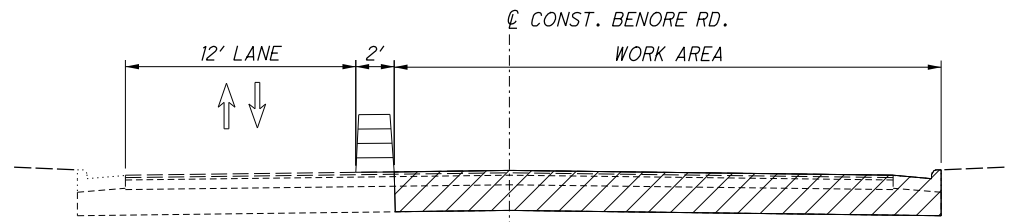
SEE SHEET 19



**LEGEND**

- • DRUMS
- DIRECTION OF TRAVEL
- ▨ WORK ZONE
- ▨ PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A
- ▨ PAVEMENT WEDGE
- PCB PORTABLE CONCRETE BARRIER, 32" HIGH
- IA IMPACT ATTENUATOR

SEE SHEET 90 OF 147 FOR TEMPORARY SIGNALS



PHASE 3A - 2 WAY TRAFFIC TO BE MAINTAINED USING FLAGGERS

NOTE: CONTRACTOR SHALL MAINTAIN TWO WAY TRAFFIC DURING WORKDAYS. WORK SHALL BE COMPLETED ON NIGHTS OR WEEKENDS UTILIZING FLAGGERS TO MAINTAIN TWO WAY TRAFFIC WHEN LANE CLOSURES ARE NECESSARY.

ROAD WORK AHEAD, END ROAD WORK AND REDUCED SPEED LIMIT SIGNS FROM MAINTENANCE OF TRAFFIC PHASE 2 SHALL REMAIN IN PLACE FOR THE DURATION OF MAINTENANCE OF TRAFFIC PHASE 3.

\* EXIST. ELEV. 588.23  
 PROP. ELEV. 588.28  
 Δ EXIST. ELEV. 588.51  
 PROP. ELEV. 589.20

STA. 1082+20  
 1.33' LT  
 STA. 1083+35  
 8.77' LT

STA. 1082+20,  
 0.75' LT\*  
 STA. 1083+35,  
 0.75' LTA

**PAVEMENT WEDGE DETAIL**



**MAINTENANCE OF TRAFFIC PHASE 3  
 BENOIRE RD - STA. 108+00 TO STA. 114+00**

**LUC-184-8.92**

SEE SHEET 94 OF 147 FOR SIGNING AND PAVEMENT AND TRAFFIC SIGNAL DETAILS

NOTE: REMOVAL OF CONFLICTING PAVEMENT MARKINGS IS REQUIRED AND PAID FOR UNDER THE LUMP SUM BID PRICE FOR ITEM 614 - MAINTAINING TRAFFIC

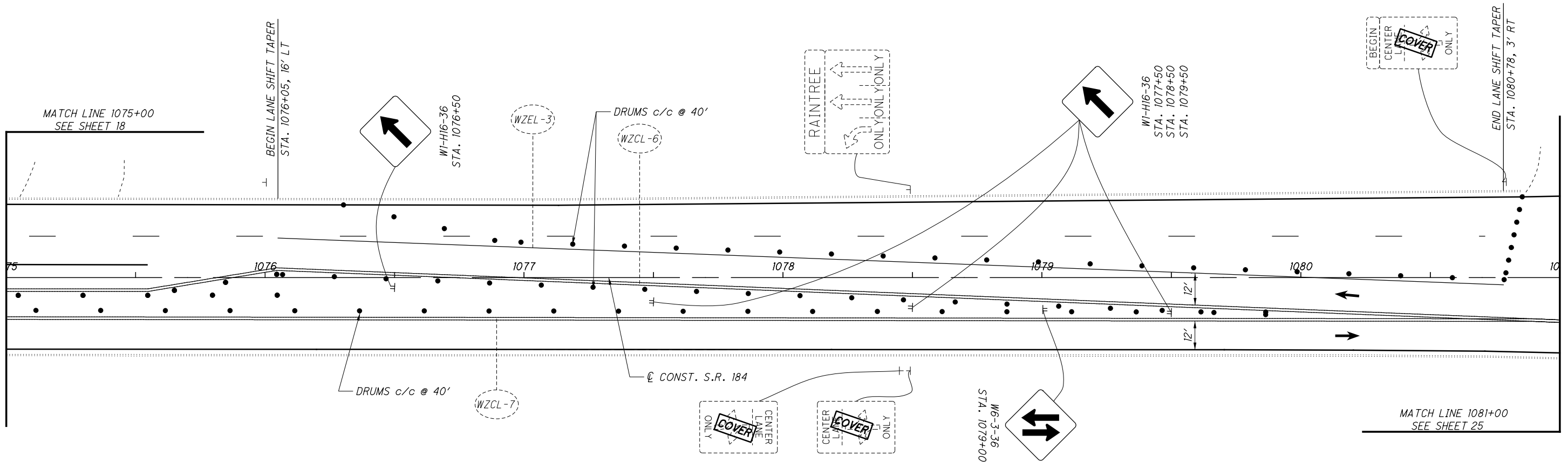
CALCULATED  
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SCALE IN FEET

**LEGEND**

- DRUMS
- DIRECTION OF TRAVEL
- ▨ WORK ZONE
- PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A
- PCB PORTABLE CONCRETE BARRIER, 32" HIGH
- WZCL WORK ZONE CENTER LINE
- WZSL WORK ZONE STOP LINE
- WZCH WORK ZONE CHANNELIZING LINE
- WZEL WORK ZONE EDGE LINE

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FOR MAINTENANCE OF TRAFFIC FROM STA. 1065+00 TO 1075+00 SEE PHASE 2 MAINTENANCE OF TRAFFIC ON SHEETS 18/147.

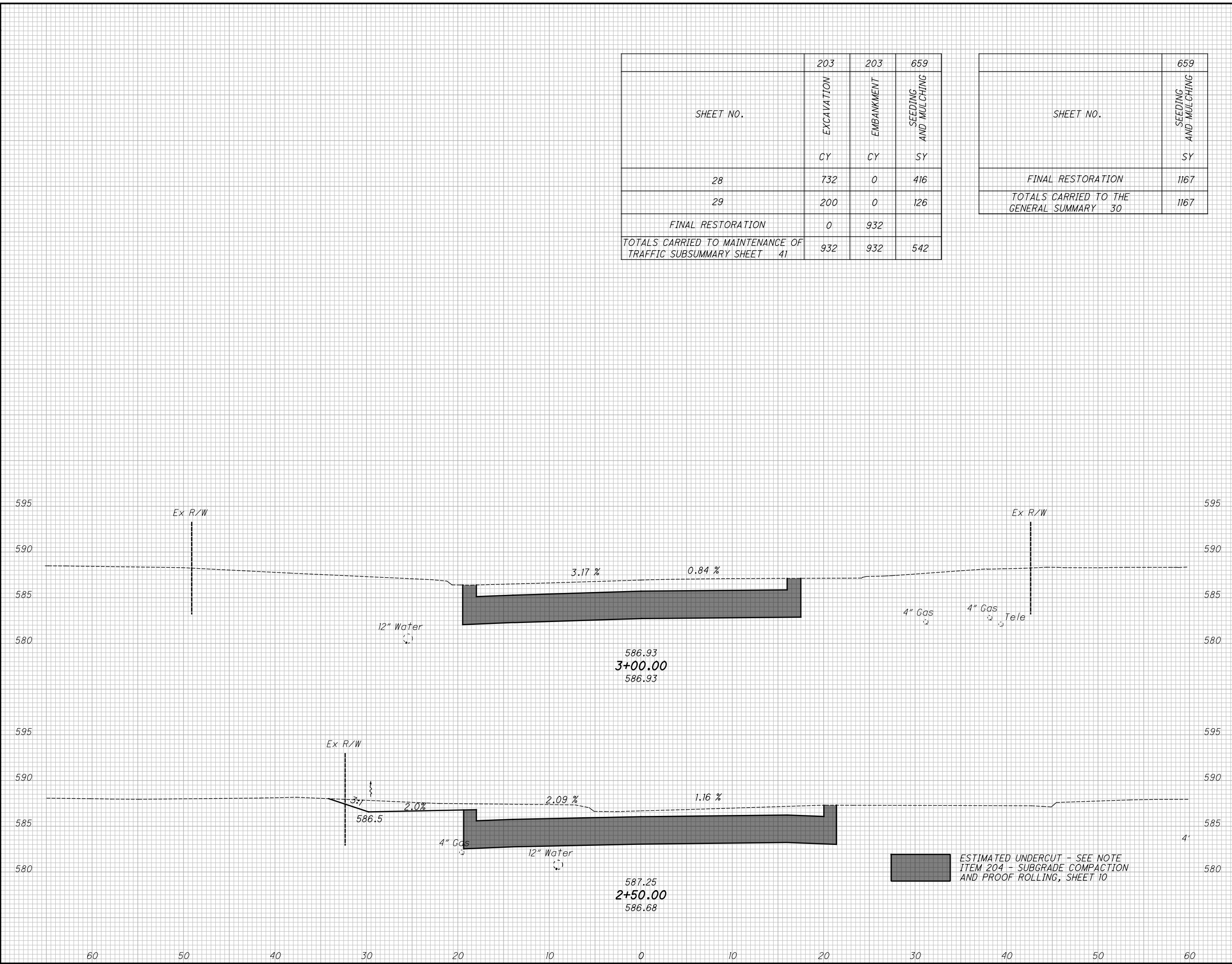
**MAINTENANCE OF TRAFFIC PHASE 4**  
**S.R. 184 - STA. 1076+00 TO STA. 1081+00**

**LUC-184-8.92**



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| SEEDING   |          |
|-----------|----------|
| END WIDTH | SO. YDS. |
| 126       | 79       |
| 60        | 47       |
| 50        | 0        |



| SHEET NO.   | EXCAVATION<br>CY | EMBANKMENT<br>CY | SEEDING<br>AND MULCHING<br>SY |
|---|------------------|------------------|-------------------------------|
| 28  | 732              | 0                | 416                           |
| 29  | 200              | 0                | 126                           |
| FINAL RESTORATION   | 0                | 932              |                               |
| TOTALS CARRIED TO MAINTENANCE OF<br>TRAFFIC SUBSUMMARY SHEET 41 | 932              | 932              | 542                           |

| SHEET NO.                                   | SEEDING<br>AND MULCHING<br>SY |
|---|-------------------------------|
| FINAL RESTORATION                           | 1167                          |
| TOTALS CARRIED TO THE<br>GENERAL SUMMARY 30 | 1167                          |

| END AREA | VOLUME |      |
|----------|--------|------|
|          | CUT    | FILL |
| 42       | 0      | 0    |
| 99       | 0      | 0    |
| 65       | 0      | 0    |
| 101      | 0      | 0    |
| 200      | 0      | 0    |

CROSS SECTIONS TEMPORARY ROAD  
STA. 2+50.00 TO STA. 3+00.00

LUC-184-8.92

29  
147

ESTIMATED UNDERCUT - SEE NOTE  
ITEM 204 - SUBGRADE COMPACTION  
AND PROOF ROLLING, SHEET 10



| REF NO.                           | SHEET NO. | STATION TO STATION |          |            | SIDE  | 202                    | 202                | 202                | 202                           | 202                               | 202                          | 202                     | 202                     | 204                         |                           | 606                       | 606                                 | 606                                 | 606   | 606   | 608                    | 608             | 608                      | 609  | 609                |
|-----------------------------------|-----------|--------------------|----------|------------|-------|------------------------|--------------------|--------------------|-------------------------------|-----------------------------------|------------------------------|-------------------------|-------------------------|-----------------------------|---------------------------|---------------------------|-------------------------------------|-------------------------------------|---|---|------------------------|-----------------|--------------------------|--|--------------------|
|                                   |           |                    |          |            |       | PAVEMENT REMOVED<br>SY | WALK REMOVED<br>SF | CURB REMOVED<br>FT | CURB AND GUTTER REMOVED<br>FT | PIPE REMOVED, 24" AND UNDER<br>FT | PIPE REMOVED, OVER 24"<br>FT | GUARDRAIL REMOVED<br>FT | MANHOLE REMOVED<br>EACH | CATCH BASIN REMOVED<br>EACH | SUBGRADE COMPACTION<br>SY | GUARDRAIL, TYPE MGS<br>FT | ANCHOR ASSEMBLY, MGS TYPE B<br>EACH | ANCHOR ASSEMBLY, MGS TYPE T<br>EACH | MGS BRIDGE TERMINAL ASSEMBLY,<br>TYPE 1<br>EACH | MGS BRIDGE TERMINAL ASSEMBLY,<br>TYPE 2<br>EACH | 4" CONCRETE WALK<br>SF | CURB RAMP<br>SF | DETECTABLE WARNING<br>SF | COMBINATION CURB AND GUTTER,<br>TYPE 2<br>FT | CURB, TYPE 6<br>FT |
| C-1                               | 44        | 1081+84.68         | TO       | 1083+87.76 | LT    |                        |                    |                    | 205                           |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 | 205                      |  |                    |
| C-2                               | 44        | 1081+84.68         | TO       | 1083+27.93 | RT    |                        |                    |                    | 143                           |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 | 143                      |  |                    |
| C-3                               | 45        | 1085+41.79         | TO       | 1086+86.86 | LT    |                        |                    |                    | 145                           |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 | 145                      |  |                    |
| C-4                               | 45        | 1085+41.79         | TO       | 1086+86.86 | RT    |                        |                    |                    | 203                           |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 | 203                      |  |                    |
| C-5                               | 48        | 106+10.70          | TO       | 108+49.05  | LT    |                        |                    | 153                |                               |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          | 153  |                    |
| C-6                               | 48        | 106+10.70          | TO       | 108+43.62  | RT    |                        |                    | 207                |                               |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          | 207  |                    |
| C-7                               | 49        | 109+44.56          | TO       | 111+39.92  | LT    |                        |                    |                    | 195                           |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 | 195                      |  |                    |
| C-8                               | 49        | 109+96.50          | TO       | 111+39.92  | RT    |                        |                    |                    | 143                           |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 | 143                      |  |                    |
| R-1                               | 44        | 1080+78.38         | TO       | 1083+44.83 | RT    |                        |                    |                    |                               |                                   | 270                          |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-2                               | 44        | 1082+00.76         |          |            | LT/RT |                        |                    |                    |                               |                                   |                              | 1                       |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-3                               | 44        | 1082+00.76         |          |            | LT/RT |                        |                    |                    |                               | 65                                |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-4                               | 44        | 1082+00.76         |          |            | RT    |                        |                    |                    |                               |                                   |                              | 1                       |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-5                               | 44        | 1082+00.76         |          |            | RT    |                        |                    |                    |                               | 55                                |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-6                               |           |                    | NOT USED |            |       |                        |                    |                    |                               |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-7                               | 44        | 1081+84.68         | TO       | 1083+54.00 | RT    |                        | 1023               |                    |                               |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-8                               | 45        | 1085+24.92         | TO       | 1087+91.76 | LT    |                        |                    |                    |                               |                                   | 269                          |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-9                               | 45        | 1084+83.94         | TO       | 1086+86.86 | RT    |                        | 1246               |                    |                               |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-10                              | 48        | 107+20.76          | TO       | 107+81.94  | LT    |                        |                    |                    |                               |                                   | 61                           |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-11                              | 48        | 107+64.24          |          |            | LT    |                        |                    |                    |                               |                                   |                              | 1                       |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-12                              | 48        | 107+64.24          | TO       | 107+90.82  | LT    |                        |                    |                    |                               | 48                                |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-13                              | 49        | 109+78.26          | TO       | 110+44.07  | RT    |                        |                    |                    |                               |                                   | 66                           |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| R-14                              | 49        | 110+76.33          |          |            | RT    |                        |                    |                    |                               |                                   |                              | 1                       |                         |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| G-1                               | 44        | 1081+70.51         | TO       | 1083+97.30 | RT    |                        |                    |                    |                               |                                   |                              |                         |                         |                             | 112.5                     | 1                         |                                     | 1                                   |   |   |                        |                 |                          |  |                    |
| G-2                               | 45        | 1085+28.76         | TO       | 1086+99.19 | LT    |                        |                    |                    |                               |                                   |                              |                         |                         |                             | 112.5                     | 1                         |                                     | 1                                   |   |   |                        |                 |                          |  |                    |
| G-3                               | 48        | 107+33.00          | TO       | 107+85.84  | LT    |                        |                    |                    |                               |                                   |                              |                         |                         |                             | 25                        |                           | 1                                   |                                     | 1   |   |                        |                 |                          |  |                    |
| G-4                               | 49        | 109+74.34          | TO       | 110+13.76  | RT    |                        |                    |                    |                               |                                   |                              |                         |                         |                             |                           |                           | 1                                   |                                     | 1   |   |                        |                 |                          |  |                    |
| P-1                               | 44        | 1081+84.68         | TO       | 1083+87.68 | LT/RT | 1193                   |                    |                    |                               |                                   |                              |                         | 1333                    |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| P-2                               | 45        | 1084+84.79         | TO       | 1086+86.86 | LT/RT | 1500                   |                    |                    |                               |                                   |                              |                         | 1339                    |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| P-3                               | 48        | 106+10.70          | TO       | 107+92.07  | LT/RT | 1076                   |                    |                    |                               |                                   |                              |                         | 866                     |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| P-4                               | 49        | 109+71.94          | TO       | 111+39.92  | LT/RT | 760                    |                    |                    |                               |                                   |                              |                         | 903                     |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| APPROACH SLABS                    |           | 1083+60.57         | TO       | 1083+80.57 | LT/RT |                        |                    |                    |                               |                                   |                              |                         | 302                     |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| APPROACH SLABS                    |           | 1084+89.15         | TO       | 1085+09.15 | LT/RT |                        |                    |                    |                               |                                   |                              |                         | 302                     |                             |                           |                           |                                     |                                     |   |   |                        |                 |                          |  |                    |
| SW-1                              | 44        | 1081+84.86         | TO       | 1083+27.93 | LT    |                        |                    |                    |                               |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   | 1023                   |                 |                          |  |                    |
| SW-2                              | 45        | 1084+83.94         | TO       | 1086+86.86 | RT    |                        |                    |                    |                               |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   | 1246                   |                 |                          |  |                    |
| SW-3                              | 48        | 109+96.50          | TO       | 110+64.64  | RT    |                        |                    |                    |                               |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   | 416                    |                 |                          |  |                    |
| DW-1                              | 44        | 1083+97            |          |            | RT    |                        |                    |                    |                               |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        | 12              |                          |  |                    |
| DW-2                              | 45        | 1084+91            |          |            | LT    |                        |                    |                    |                               |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        | 12              |                          |  |                    |
| CR-1                              | 45        | 1084+68            |          |            | RT    |                        |                    |                    |                               |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        | 78              |                          |  |                    |
| CR-2                              | 45        | 1084+92            |          |            | RT    |                        |                    |                    |                               |                                   |                              |                         |                         |                             |                           |                           |                                     |                                     |   |   |                        | 97              |                          |  |                    |
| TOTALS CARRIED TO GENERAL SUMMARY |           |                    |          |            |       | 4529                   | 2269               | 360                | 1035                          | 120                               | 48                           | 666                     | 1                       | 3                           | 5045                      | 250                       | 2                                   | 2                                   | 2   | 2   | 2685                   | 175             | 24                       | 1035   | 360                |

|                           |         |
|---------------------------|---------|
| CALCULATED                | CHECKED |
| CEL                       | JMZ     |
| <b>ROADWAY SUBSUMMARY</b> |         |
| <b>LUC-184-8.92</b>       |         |
| 34                        | 147     |

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| REF NO.                           | STATION TO STATION |    | SIDE    | 614  | 614                                      | 614                    | 614                            | 614                              | 614                                      | 614                          | 614                                       | 615                                       | 614  | 614   | 614   | 622                          | 202              | 203        | 203        | INCLUDED WITH ITEM 615 - ROADS FOR MAINTAINING TRAFFIC |                           |                                |      |      | 611  | 611 |
|-----------------------------------|--------------------|----|---------|--|--|------------------------|--------------------------------|----------------------------------|--|------------------------------|---|---|--|---|---|------------------------------|------------------|------------|------------|--|---------------------------|--------------------------------|------|------|------|-----|
|                                   |                    |    |         | WORK ZONE IMPACT ATTENUATOR, BIDIRECTIONAL | BARRIER REFLECTOR, TYPE 1, BIDIRECTIONAL | OBJECT MARKER, TWO WAY | WORK ZONE CENTER LINE, CLASS I | WORK ZONE EDGE LINE, CLASS I, 6" | WORK ZONE CHANNELIZING LINE, CLASS I, 8" | WORK ZONE STOP LINE, CLASS I | WORK ZONE STOP LINE, CLASS III, 642 PAINT | PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A | WORK ZONE DOTTED LINE, CLASS I, 6", 740.06, TYPE I | WORK ZONE CENTER LINE, CLASS III, 642 PAINT | WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT | PORTABLE BARRIER, UNANCHORED | PAVEMENT REMOVED | EXCAVATION | EMBANKMENT | SEEDING AND MULCHING                                   | MANHOLE ADJUSTED TO GRADE | MANHOLE RECONSTRUCTED TO GRADE | EACH | EACH |      |     |
|                                   |                    |    |         | EACH                                       | EACH                                     | EACH                   | MILE                           | MILE                             | FT                                       | FT                           | FT  | SY  | FT   | MILE  | MILE  | FT                           | SY               | CY         | CY         |  |                           |                                | SY   | EACH | EACH |     |
| PHASE 2                           |                    |    |         |  |  |                        |                                |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZCL-6                            | 1074+29            | TO | 1081+85 |  |  |                        | 0.14                           |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZCL-7                            | 1074+29            | TO | 1082+21 |  |  |                        | 0.15                           |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZCL-8                            | 0+39               | TO | 3+25    |  |  |                        | 0.05                           |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZCL-9                            | 1083+51            | TO | 1095+30 |  |  |                        | 0.19                           |                                  |  |                              |   |   |  | 0.04  |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZCL-10                           | 1087+30            | TO | 1095+30 |  |  |                        | 0.15                           |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZEL-3                            | 1076+05            | TO | 1082+21 |  |  |                        |                                | 0.09                             |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZEL-4                            | 0+26               | TO | 3+25    |  |  |                        |                                | 0.06                             |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZEL-5                            | 0+55               | TO | 3+25    |  |  |                        |                                | 0.07                             |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZEL-6                            | 1083+51            | TO | 1095+30 |  |  |                        |                                | 0.19                             |  |                              |   |   |  | 0.04  |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZCH-5                            | 1074+29            | TO | 1075+54 |  |  |                        |                                |                                  |  | 125                          |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZSL-8                            | 1082+21            |    |         |  |  |                        |                                |                                  |  |                              | 12  |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZSL-9                            | 0+60               |    |         |  |  |                        |                                |                                  |  |                              | 14  |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZSL-10                           | 1083+51            |    |         |  |  |                        |                                |                                  |  |                              |   | 11  |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| PCB-3                             | 1083+65            | TO | 1089+24 |  |  | 14                     |                                |                                  |  |                              |   |   |  |   |   |                              | 685              |            |            |  |                           |                                |      |      |      |     |
| PCB-4                             | 60+03              | TO | 2+00    |  |  | 4                      |                                |                                  |  |                              |   |   |  |   |   |                              | 197              |            |            |  |                           |                                |      |      |      |     |
| PCB-6                             | 0+85               |    | 1+58    |  |  | 2                      |                                |                                  |  |                              |   |   |  |   |   |                              | 80               |            |            |  |                           |                                |      |      |      |     |
| IA-3                              | 1083+45            | TO | 1083+65 |  |  | 1                      |                                |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| IA-4                              | 0+28               | TO | 0+48    |  |  | 1                      |                                |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| IA-5                              | 1089+24            | TO | 1089+43 |  |  | 1                      |                                |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| IA-6                              | 0+69               |    | 0+85    |  |  | 1                      |                                |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| IA-7                              | 1+58               |    | 1+79    |  |  | 1                      |                                |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| TP-1                              | 0+34               | TO | 2+45    |  |  |                        |                                |                                  |  |                              |   | 1141                                      |  |   |   |                              | 1141             | 932        | 932        |  |                           |                                | 542  | 1    | 1    |     |
| WZDL-1                            | 1082+21            | TO | 1083+45 |  |  |                        |                                |                                  |  |                              |   |   | 124  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| PHASE 4                           |                    |    |         |  |  |                        |                                |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZCL-11                           | 109+46             | TO | 111+15  |  |  |                        |                                |                                  |  |                              |   |   |  | 0.03  |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZCL-12                           | 1081+68            | TO | 1083+49 |  |  |                        |                                |                                  |  |                              |   |   |  | 0.03  |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZCL-13                           | 1085+00            | TO | 1085+50 |  |  |                        |                                |                                  |  |                              |   |   |  | 0.01  |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZEL-7                            | 1081+68            | TO | 111+45  |  |  |                        |                                |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZEL-8                            | 109+57             | TO | 1086+87 |  |  |                        |                                |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZEL-8                            | 109+57             | TO | 1086+87 |  |  |                        |                                |                                  |  |                              |   |   |  | 0.10  |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZEL-8                            | 109+57             | TO | 1086+87 |  |  |                        |                                |                                  |  |                              |   |   |  | 0.05  |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZCH-6                            | 1084+85            | TO | 1086+37 |  |  |                        |                                |                                  |  | 152                          |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZSL-11                           | 109+46             |    |         |  |  |                        |                                |                                  |  |                              |   | 14  |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZSL-12                           | 1083+49            |    |         |  |  |                        |                                |                                  |  |                              |   | 13  |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| WZSL-13                           | 1084+85            |    |         |  |  |                        |                                |                                  |  |                              |   | 32  |  |   |   |                              |                  |            |            |  |                           |                                |      |      |      |     |
| PCB-5                             | 1081+56            | TO | 1083+72 |  |  | 5                      |                                |                                  |  |                              |   |   |  |   |   |                              |                  |            |            |  |                           |                                |      |      | 227  |     |
| TOTALS CARRIED TO GENERAL SUMMARY |                    |    |         | 5  | 25                                       | 25                     | 0.68                           | 0.40                             | 277                                      | 26                           | 70  | 1141                                      | 124  | 0.11  | 0.19  | 1189                         |                  |            |            |  |                           | FOR INFORMATION ONLY           |      |      |      |     |

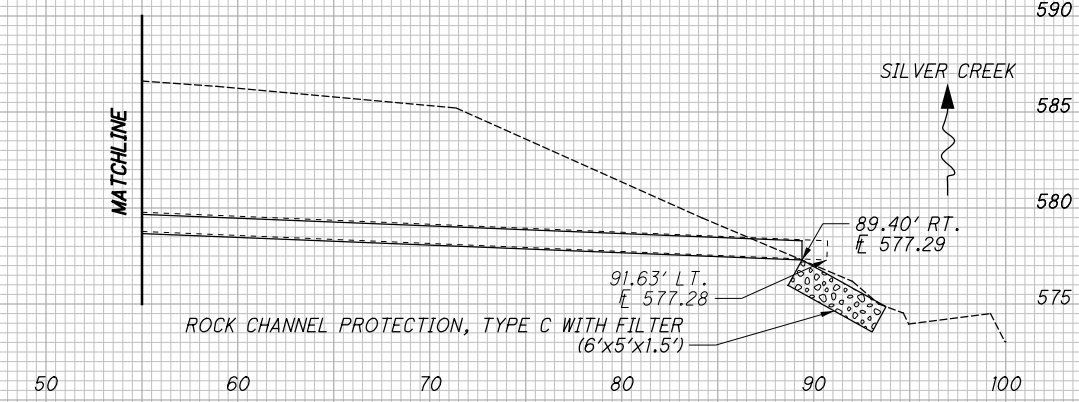
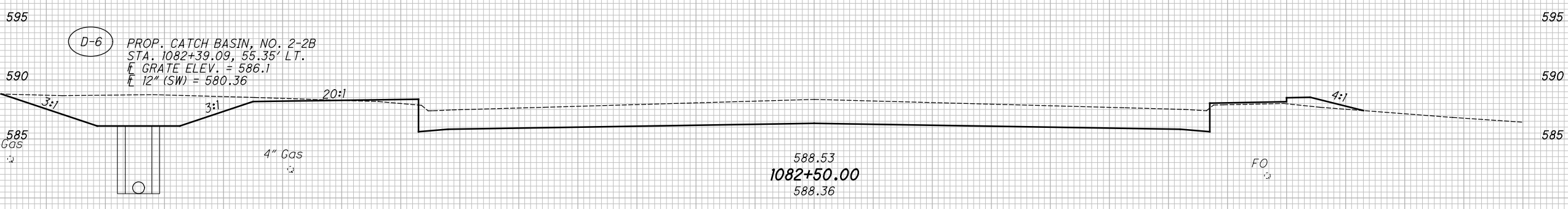
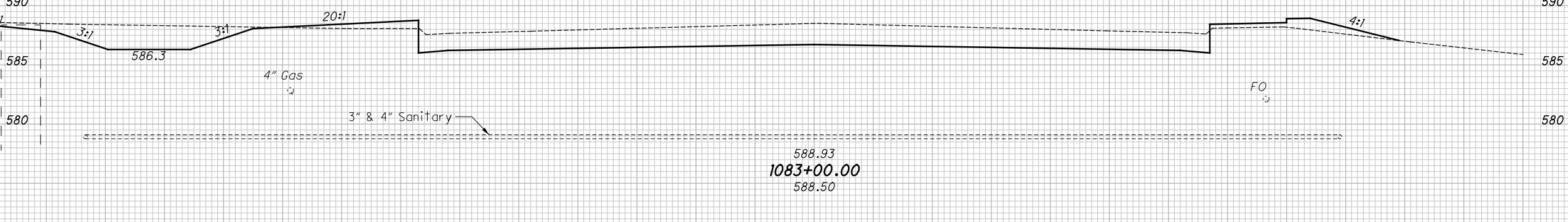
**MAINTENANCE OF TRAFFIC SUBSUMMARY**  
**PHASE 2, PHASE 3 & PHASE 4**  
**LUC-184-8.92**

CALCULATED  
TDB  
CHECKED  
CEL

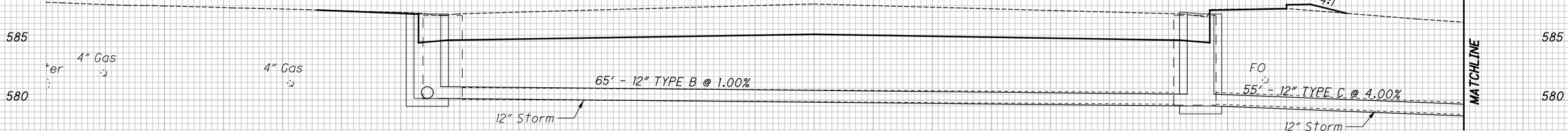
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| SEEDING | END   |          |
|---------|-------|----------|
|         | WIDTH | SO. YDS. |
|         | 45    | 247      |
|         | 44    | 168      |
|         | 17    | 14       |
|         | 0     | 429      |

**S-1** RECONSTRUCTED TO GRADE  
 EX. SANITARY MANHOLE,  
 STA. 1083+09.08, 67.21' LT.  
 GRATE ELEV. = 588.51  
 30" (S) / 24" (E) = 561.75  
 PROP. MOT GRATE ELEV. = 587.37  
 PROP. FINAL GRATE ELEV. = 588.43



**D-1** PROP. CATCH BASIN, NO. 6  
 STA. 1082+08.57, 32.19' LT.  
 GRATE ELEV. = 587.32  
 12" (NE) = 580.14  
 12" (S) = 580.14  
 TO BE REMOVED  
 EX. CATCH BASIN, NO. 2-2B  
 STA. 1082+00.76, 31.46' LT.  
 GRATE ELEV. = 587.16  
 12" (S) = 580.06



**D-3** PROP. CATCH BASIN, NO. 3A  
 STA. 1082+08.57, 32.94' RT.  
 GRATE ELEV. = 587.32  
 12" (N) = 579.49  
 12" (S) = 579.49  
 TO BE REMOVED  
 EX. CATCH BASIN, NO. 3  
 STA. 1082+00.76, 32.41' RT.  
 GRATE ELEV. = 587.05  
 12" (S) = 579.65

| END | AREA |      | VOLUME |      | CALCULATED | CHECKED |
|-----|------|------|--------|------|------------|---------|
|     | CUT  | FILL | CUT    | FILL |            |         |
| 45  | 120  | 57   |        |      |            |         |
| 247 |      |      | 231    | 86   |            |         |
| 44  | 129  | 36   |        |      |            |         |
| 168 |      |      | 213    | 35   |            |         |
| 17  | 100  | 2    | 57     | 1    |            |         |
| 14  | 100  | 2    |        |      |            |         |
| 429 |      |      | 501    | 122  |            |         |

588.15  
 1082+00.00  
 588.15  
 STA. 1081+84.68 BEGIN PROJECT

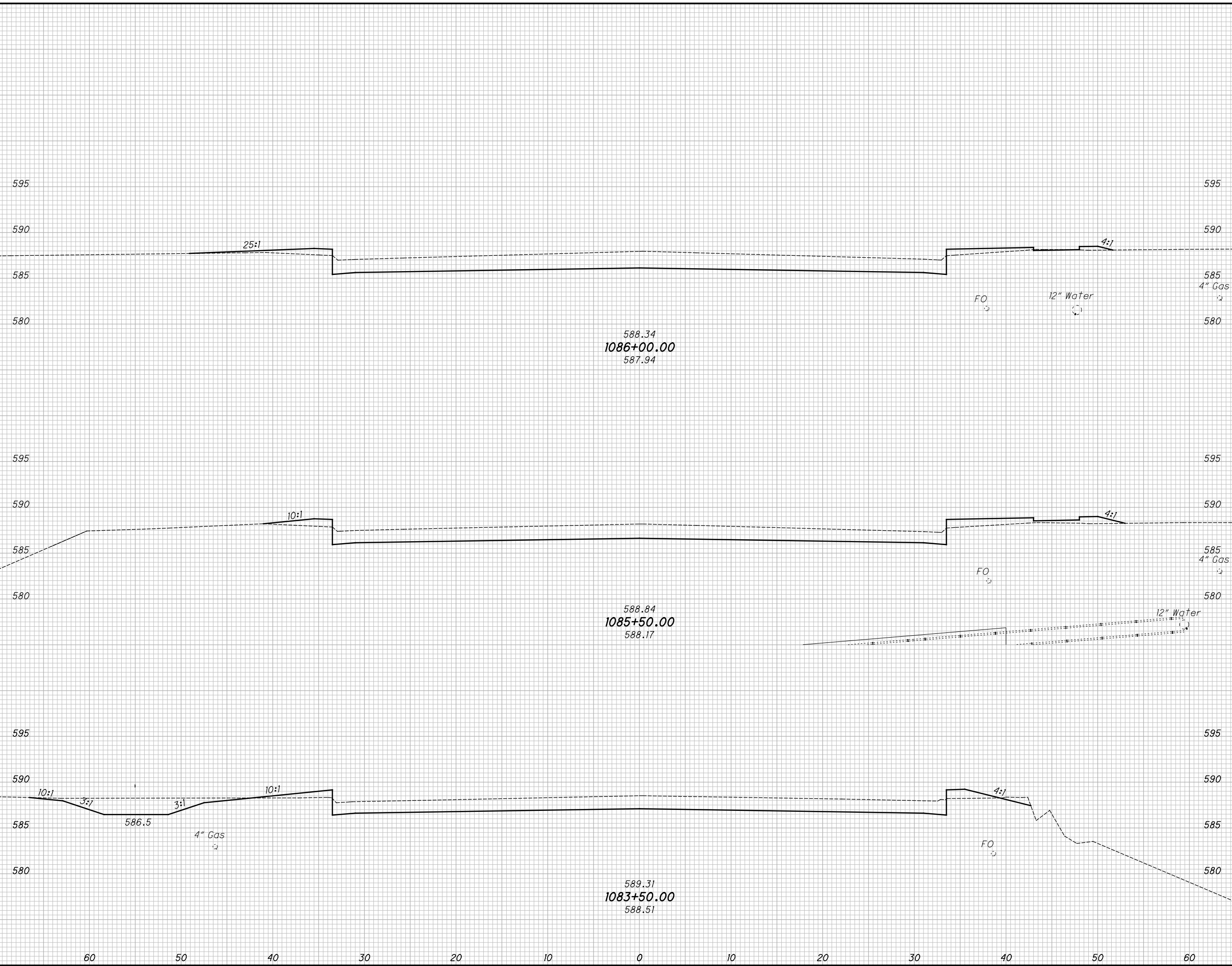
CROSS SECTIONS S.R. 184  
 STA. 1082+00.00 TO STA. 1083+00.00

LUC-184-8.92

50  
 147

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| SEEDING   |          |
|-----------|----------|
| END WIDTH | SO. YDS. |
| 396       | 255      |
| 60        | 46       |
| 50        | 22       |
| 40        | 29       |



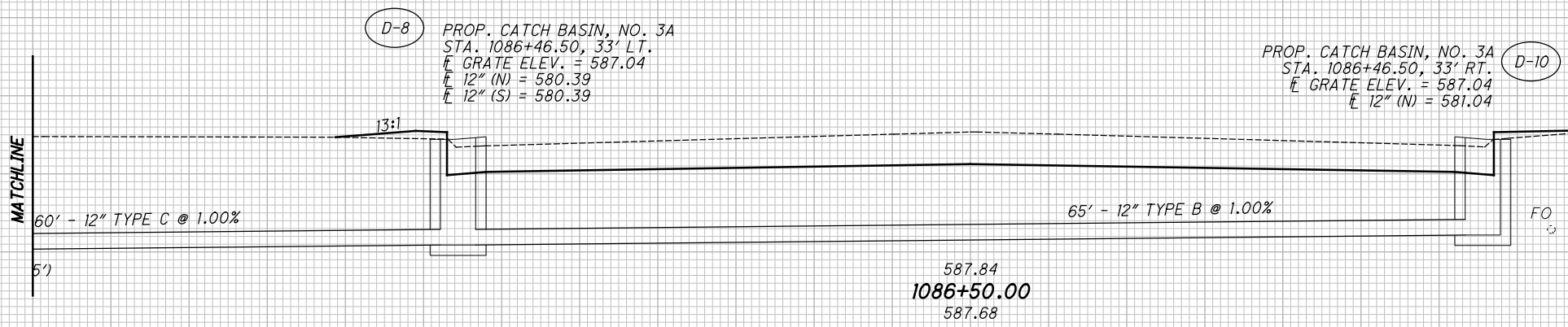
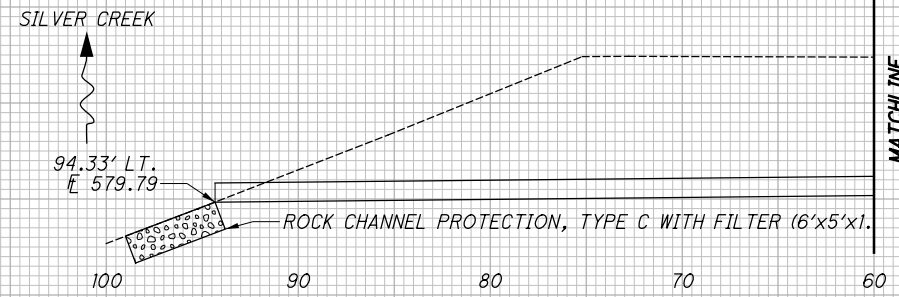
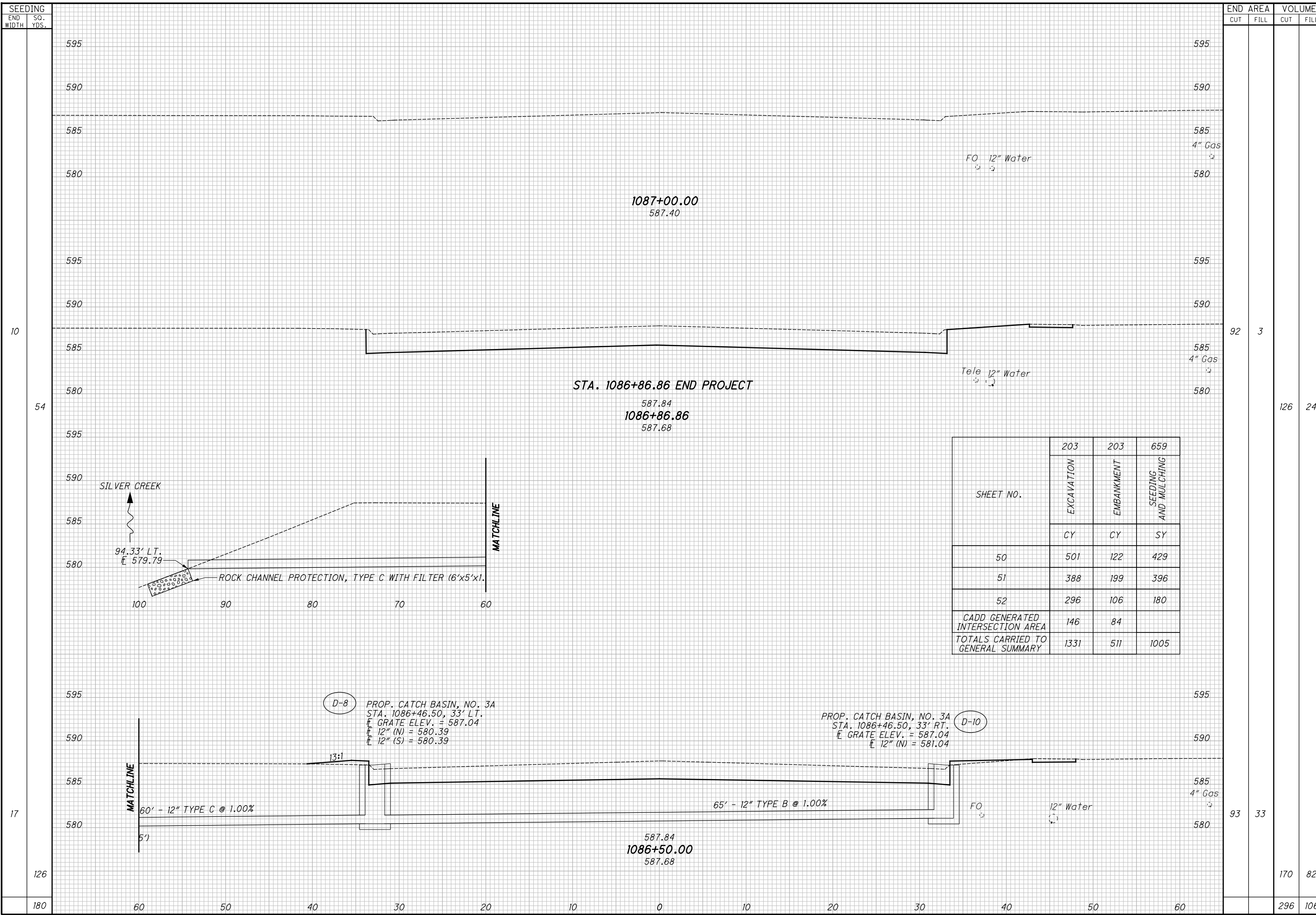
| END AREA |      | VOLUME |      |
|----------|------|--------|------|
| CUT      | FILL | CUT    | FILL |
| 91       | 56   | 170    | 82   |
| 93       | 33   |        |      |
| 116      | 69   | 218    | 117  |
|          |      | 388    | 199  |

**CROSS SECTIONS S.R. 184  
STA. 1083+50.00 TO STA. 1086+00.00**

**LUC-184-8.92**

51  
147

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| SHEET NO.                         | 203        | 203        | 659                  |
|-----------------------------------|------------|------------|----------------------|
|                                   | EXCAVATION | EMBANKMENT | SEEDING AND MULCHING |
|                                   | CY         | CY         | SY                   |
| 50                                | 501        | 122        | 429                  |
| 51                                | 388        | 199        | 396                  |
| 52                                | 296        | 106        | 180                  |
| CADD GENERATED INTERSECTION AREA  | 146        | 84         |                      |
| TOTALS CARRIED TO GENERAL SUMMARY | 1331       | 511        | 1005                 |

| END AREA | VOLUME |      | CALCULATED CEL | CHECKED JMZ |
|----------|--------|------|----------------|-------------|
|          | CUT    | FILL |                |             |
| 92       | 3      |      |                |             |
| 93       | 33     |      |                |             |
| 170      | 82     |      |                |             |
| 296      | 106    |      |                |             |

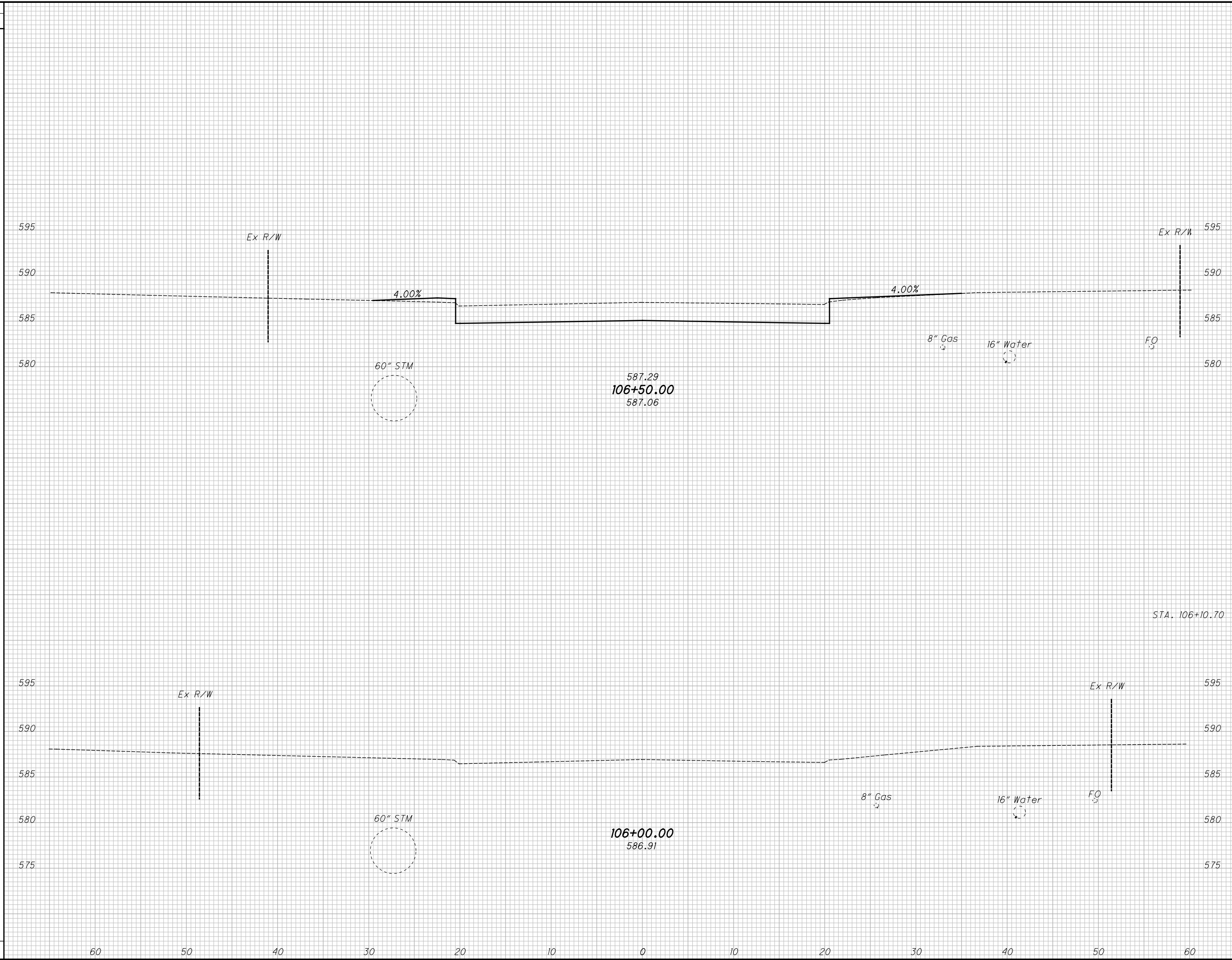
CROSS SECTIONS S.R. 184  
STA. 1086+50.00 TO STA. 1087+00.00

LUC-184-8.92

52  
147

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| SEEDING   |          |
|-----------|----------|
| END WIDTH | SO. YDS. |
| 24        |          |
| 65        |          |
| 0         |          |
| 65        |          |



| END AREA |      | VOLUME |      |
|----------|------|--------|------|
| CUT      | FILL | CUT    | FILL |
| 55       | 15   | 80     | 19   |
| 55       | 11   | 11     | 2    |
| 0        | 0    |        |      |
| 91       | 21   |        |      |

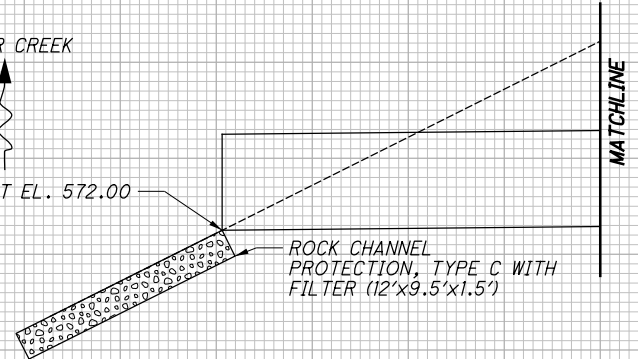
**CROSS SECTIONS ENTERPRISE BLVD.**  
**STA. 106+00.00 TO STA. 106+50.00**  
**LUC-184-8.92**  
 53  
 147



SEEDING  
 END SO.  
 WIDTH YDS.  
 19 118 279

| END AREA |      | VOLUME |      | CALCULATED CEL | CHECKED JMZ |
|----------|------|--------|------|----------------|-------------|
| CUT      | FILL | CUT    | FILL |                |             |
| 55       | 47   | 101    | 69   |                |             |
| 55       | 27   | 101    | 39   |                |             |
|          |      | 208    | 108  |                |             |

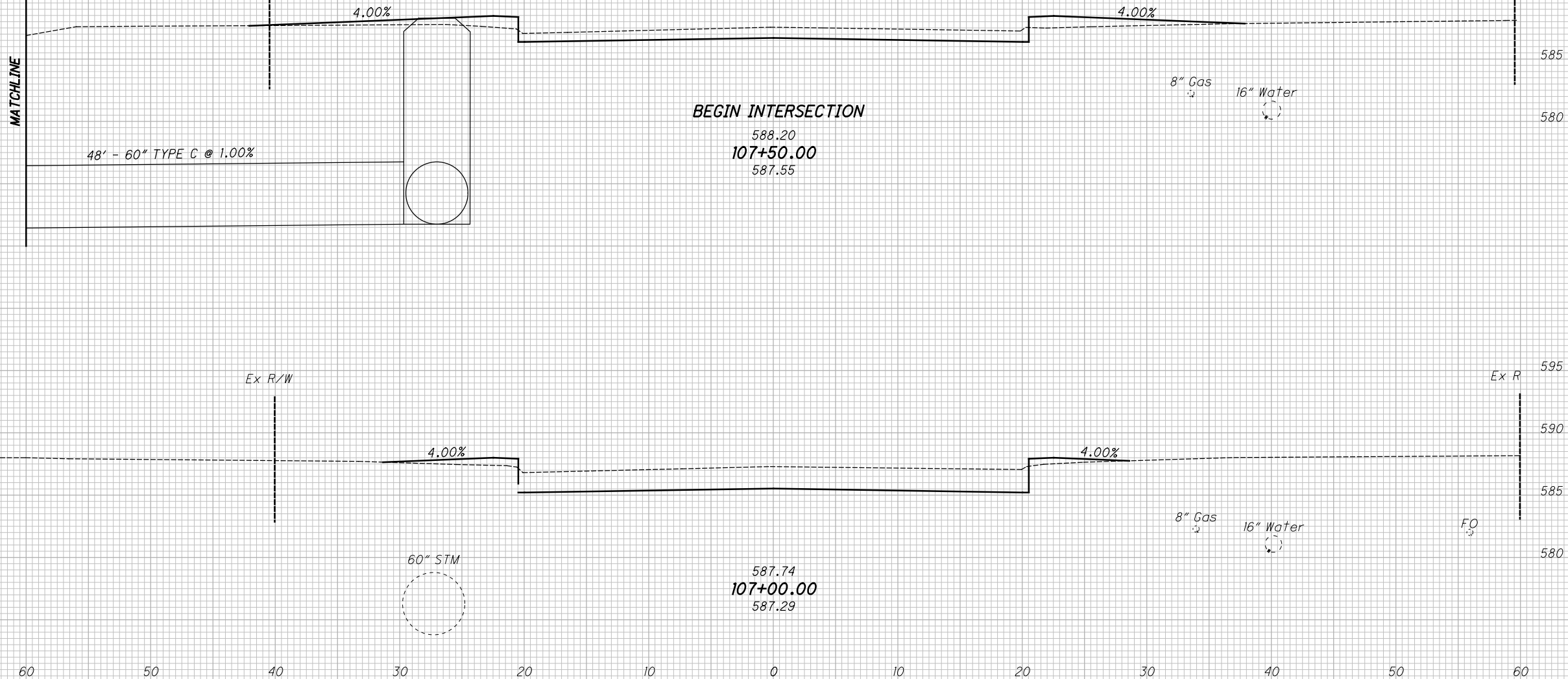
SILVER CREEK  
 ↑  
 OUTLET EL. 572.00



(D-11) PROP. MANHOLE, NO. 3  
 STA. 107+57.30, 26.81' LT.  
 GRATE ELEV. = 588.28  
 E 60" (S) = 572.5(±)  
 E 60" (NW) = 572.5(±)

TO BE REMOVED  
 EX. MANHOLE  
 STA. 107+62.24, 26.48' LT.  
 GRATE ELEV. = 587.7(±)  
 E 60" (S) = 572.5(±)  
 E 60" (NW) = 572.5(±)

595  
 590  
 585  
 580  
 595  
 590  
 585  
 580



BEGIN INTERSECTION  
 588.20  
 107+50.00  
 587.55

587.74  
 107+00.00  
 587.29

CROSS SECTIONS ENTERPRISE BLVD.  
 STA. 107+00.00 TO STA. 107+50.00

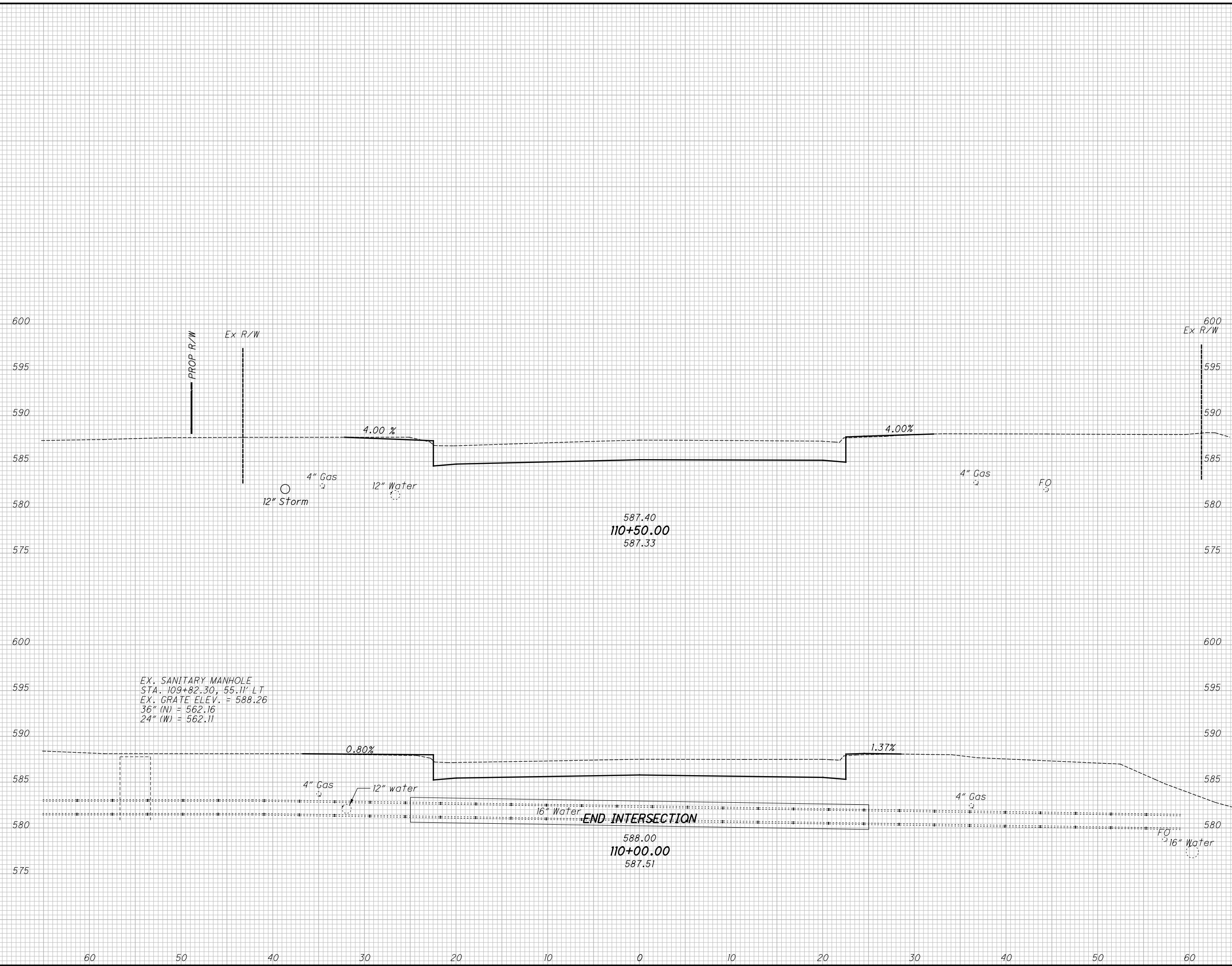
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54  
 147

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| SEEDING   |          |
|-----------|----------|
| END WIDTH | SO. YDS. |
| 19        |          |
| 169       |          |
| 42        |          |
| 169       |          |



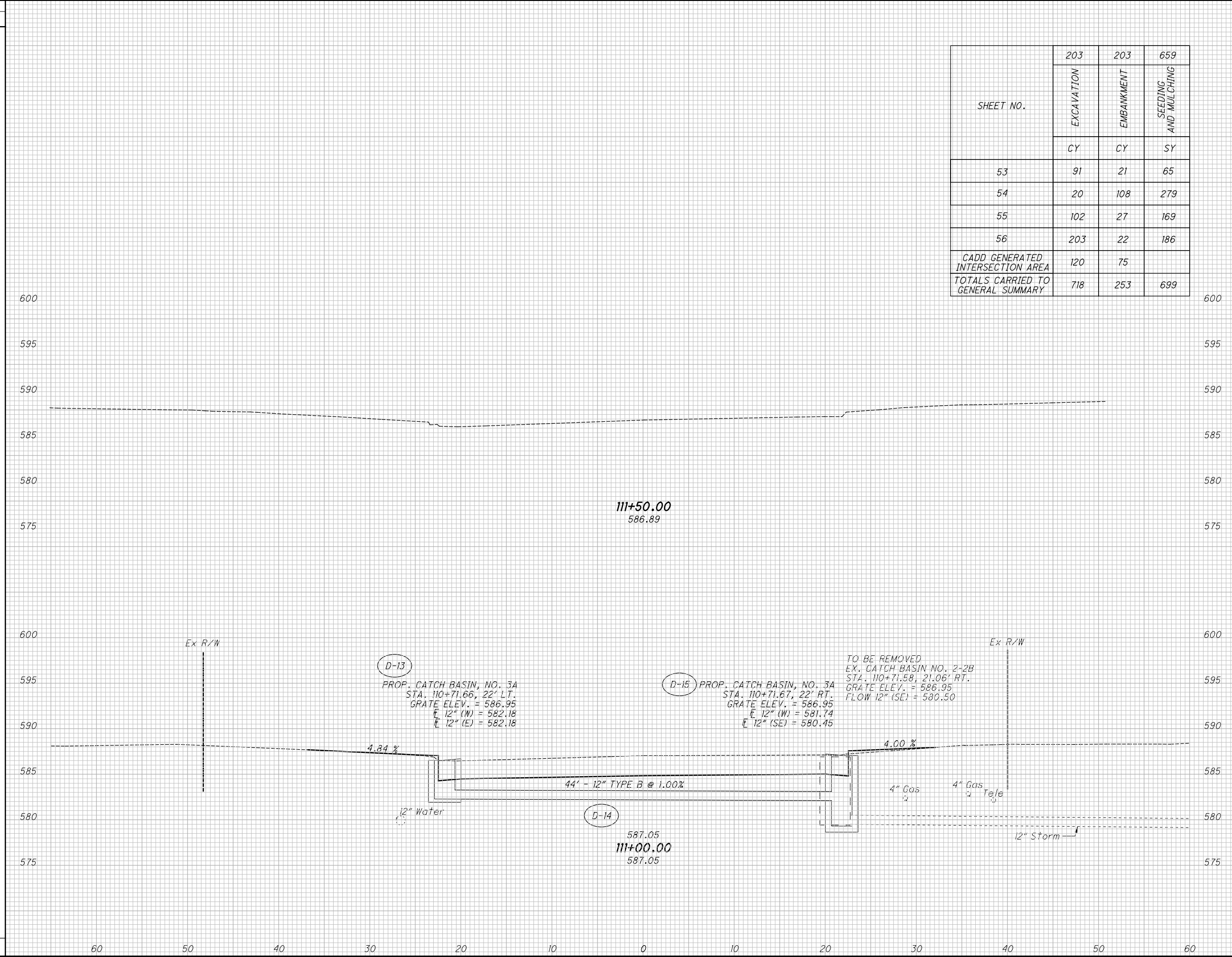
| END AREA |      | VOLUME |      |
|----------|------|--------|------|
| CUT      | FILL | CUT    | FILL |
| 56       | 6    | 102    | 27   |
| 55       | 23   | 102    | 27   |

CROSS SECTIONS BENORE ROAD  
 STA. 110+00.00 TO STA. 110+50.00

LUC-184-8.92

55  
147

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| SHEET NO.                         | 203        | 203        | 659                  |
|-----------------------------------|------------|------------|----------------------|
|                                   | EXCAVATION | EMBANKMENT | SEEDING AND MULCHING |
|                                   | CY         | CY         | SY                   |
| 53                                | 91         | 21         | 65                   |
| 54                                | 20         | 108        | 279                  |
| 55                                | 102        | 27         | 169                  |
| 56                                | 203        | 22         | 186                  |
| CADD GENERATED INTERSECTION AREA  | 120        | 75         |                      |
| TOTALS CARRIED TO GENERAL SUMMARY | 718        | 253        | 699                  |

| END AREA | VOLUME | CALCULATED | CHECKED |     |      |     |      |
|----------|--------|------------|---------|-----|------|-----|------|
|          |        |            |         | CUT | FILL | CUT | FILL |
| 55       | 0      | 101        | 8       |     |      |     |      |
| 55       | 9      | 102        | 14      |     |      |     |      |
|          |        | 203        | 22      |     |      |     |      |

**CROSS SECTIONS BENORE ROAD**  
**STA. 111+00.00**  
**LUC-184-8.92**  
 56  
 147

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**ESTIMATED QUANTITIES (01/NHS/BR)**

CALC. BY: ARA  
CHKD. BY: AJK  
DATE: 2/22/2021  
DATE: 2/23/2021

| ITEM    | ITEM EXTENSION | TOTAL  | UNIT | DESCRIPTION   | REAR ABUTMENT | FORWARD ABUTMENT | PIERS | SUPER-STRUCTURE | GENERAL | REF. SHEET NUMBER |
|---------|----------------|--------|------|---|---------------|------------------|-------|-----------------|---------|-------------------|
| 202     | 11003          | LS     |      | STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN             |               |                  |       |                 | LS      | 3                 |
| 202     | 22900          | 737    | SY   | APPROACH SLAB REMOVED   | 362           | 375              |       |                 |         |                   |
| 202     | 23500          | 1777   | SY   | WEARING COURSE REMOVED  | 362           | 375              |       | 1040            |         |                   |
| 503     | 11101          | LS     |      | COFFERDAMS AND EXCAVATION BRACING, AS PER PLAN                |               |                  |       |                 | LS      | 3                 |
| 503     | 21100          | 673    | CY   | UNCLASSIFIED EXCAVATION                                       | 337           | 336              |       |                 |         |                   |
| 505     | 11100          | LS     |      | PILE DRIVING EQUIPMENT MOBILIZATION                           |               |                  |       |                 | LS      |                   |
| 507     | 00500          | 2430   | FT   | 12" CAST-IN-PLACE REINFORCED CONCRETE PILES, DRIVEN           | 1215          | 1215             |       |                 |         |                   |
| 507     | 00550          | 2700   | FT   | 12" CAST-IN-PLACE REINFORCED CONCRETE PILES, FURNISHED        | 1350          | 1350             |       |                 |         |                   |
| 507     | 00700          | 2600   | FT   | 16" CAST-IN-PLACE REINFORCED CONCRETE PILES, DRIVEN           |               |                  | 2600  |                 |         |                   |
| 507     | 00750          | 2800   | FT   | 16" CAST-IN-PLACE REINFORCED CONCRETE PILES, FURNISHED        |               |                  | 2800  |                 |         |                   |
| 509     | 10000          | 435485 | LB   | EPOXY COATED REINFORCING STEEL                                | 13769         | 13717            | 12915 | 395084          |         |                   |
| 511     | 33312          | 1165   | CY   | CLASS QC2 CONCRETE WITH QC/QA, SUPERSTRUCTURE                 |               |                  |       | 1165            |         |                   |
| 511     | 43512          | 233    | CY   | CLASS QC1 CONCRETE WITH QC/QA, ABUTMENT INCLUDING FOOTING     | 117           | 116              |       |                 |         |                   |
| 512     | 10050          | 167    | SY   | SEALING OF CONCRETE SURFACES (NON-EPOXY)                      |               |                  |       | 167             |         |                   |
| 512     | 10100          | 761    | SY   | SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)                 | 59            | 62               | 187   | 453             |         |                   |
| 512     | 33000          | 6      | SY   | TYPE 2 WATERPROOFING  | 2             | 2                |       | 2               |         |                   |
| 516     | 13200          | 276    | SF   | 1/2" PREFORMED EXPANSION JOINT FILLER                         |               |                  |       | 276             |         |                   |
| 516     | 13600          | 305    | SF   | 1" PREFORMED EXPANSION JOINT FILLER                           |               |                  |       | 305             |         |                   |
| 516     | 14014          | 391    | FT   | INTEGRAL ABUTMENT EXPANSION JOINT SEAL                        |               |                  |       | 391             |         |                   |
| 517     | 75120          | 202    | FT   | RAILING (CONCRETE PARAPET WITH TWIN STEEL TUBE RAILING)       |               |                  |       | 202             |         |                   |
| 518     | 21200          | 276    | CY   | POROUS BACKFILL WITH GEOTEXTILE FABRIC                        | 140           | 136              |       |                 |         |                   |
| 518     | 40000          | 417    | FT   | 6" PERFORATED CORRUGATED PLASTIC PIPE                         | 209           | 208              |       |                 |         |                   |
| 518     | 40010          | 60     | FT   | 6" NON-PERFORATED CORRUGATED PLASTIC PIPE, INCLUDING SPECIALS | 30            | 30               |       |                 |         |                   |
| 523     | 20000          | 8      | EACH | DYNAMIC LOAD TESTING  | 2             | 2                | 4     |                 |         |                   |
| 523     | 20500          | 8      | EACH | RESTRIKE  | 2             | 2                | 4     |                 |         |                   |
| 526     | 15010          | 595    | SY   | REINFORCED CONCRETE APPROACH SLABS WITH QC/QA (T=13")         | 298           | 297              |       |                 |         |                   |
| 526     | 90010          | 380    | FT   | TYPE A INSTALLATION   | 190           | 190              |       |                 |         |                   |
| SPECIAL | 45IE30000      | 388    | FT   | PRESSURE RELIEF JOINT, TYPE A                                 | 194           | 194              |       |                 |         |                   |
| 601     | 32204          | 907    | CY   | ROCK CHANNEL PROTECTION, TYPE C WITH GEOTEXTILE FABRIC        | 458           | 449              |       |                 |         |                   |
| 625     | 33000          | 1      | EACH | STRUCTURE GROUNDING SYSTEM                                    |               |                  |       |                 | 1       | 3                 |

DESIGN AGENCY  
PENNONI ASSOCIATES INC.  
2 SUMMIT PARK DRIVE, SUITE 335  
INDEPENDENCE, OHIO 44131

DATE  
2/20/21  
REVIEWED  
MDP  
STRUCTURE FILE NUMBER  
4805144

DRAWN  
JTS  
REVISOR  
JTS  
CHECKED  
BFS

**ESTIMATED QUANTITIES**  
BRIDGE NO. LUC-184-0892  
S.R. 184 (ALEXIS RD.) OVER SILVER CREEK

**LUC-184-8.92**  
PID No. 101327

5 / 48

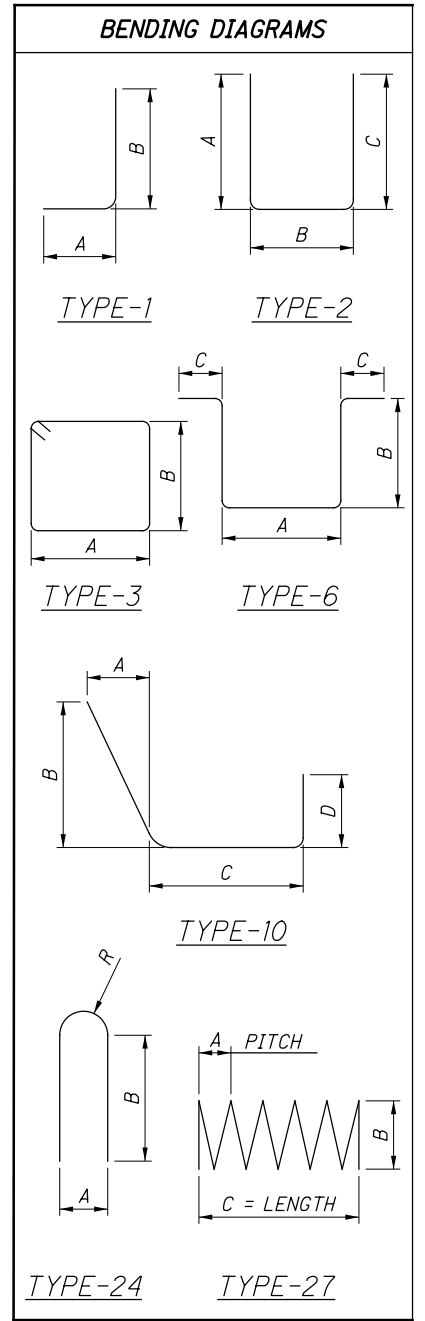
101  
147

REINFORCING STEEL LIST

| MARK         | NUMBER | LENGTH  | WEIGHT | TYPE | DIMENSIONS |        |        |   |   |   | SERIES INC. |
|--------------|--------|---------|--------|------|------------|--------|--------|---|---|---|-------------|
|              | TOTAL  |         |        |      | A          | B      | C      | D | E | R |             |
| <b>PIERS</b> |        |         |        |      |            |        |        |   |   |   |             |
| P401         | 80     | 9'-2"   | 490    | 3    | 2'-5"      | 1'-11" |        |   |   |   |             |
| SP402        | 40     |         | 7479   | 27   | 1'-0"      | 1'-0"  | 30'-0" |   |   |   |             |
| P501         | 4      | 4'-2"   | 17     | 2    | 0'-6"      | 3'-5"  | 0'-6"  |   |   |   |             |
| P502         | 8      | 10'-5"  | 87     | 24   | 2'-4"      | 3'-5"  |        |   |   |   |             |
| P503*        | 24     | 30'-0"  | 751    | STR  |            |        |        |   |   |   |             |
| P504         | 1      | 18'-10" | 20     | STR  |            |        |        |   |   |   |             |
| P505         | 1      | 16'-10" | 18     | STR  |            |        |        |   |   |   |             |
| P506         | 2      | 29'-0"  | 60     | STR  |            |        |        |   |   |   |             |
| P507         | 2      | 27'-0"  | 56     | STR  |            |        |        |   |   |   |             |
| P508         | 167    | 13'-0"  | 2264   | 6    | 2'-8"      | 4'-5"  | 1'-0"  |   |   |   |             |
| P509         | 105    | 11'-0"  | 1205   | 6    | 2'-8"      | 3'-5"  | 1'-0"  |   |   |   |             |
| P510         | 4      | 10'-1"  | 42     | 6    | 1'-9"      | 3'-5"  | 1'-0"  |   |   |   |             |
| P511         | 2      | 5'-1"   | 11     | STR  |            |        |        |   |   |   |             |
| P512         | 2      | 4'-8"   | 10     | STR  |            |        |        |   |   |   |             |
| P513         | 1      | 25'-4"  | 26     | STR  |            |        |        |   |   |   |             |
| P514         | 1      | 23'-4"  | 24     | STR  |            |        |        |   |   |   |             |
| P515         | 1      | 28'-2"  | 29     | STR  |            |        |        |   |   |   |             |
| P516         | 1      | 26'-2"  | 27     | STR  |            |        |        |   |   |   |             |
| P517         | 1      | 14'-7"  | 15     | STR  |            |        |        |   |   |   |             |
| P518         | 1      | 12'-7"  | 13     | STR  |            |        |        |   |   |   |             |
| P601*        | 32     | 30'-0"  | 1442   | STR  |            |        |        |   |   |   |             |
|              | 1      | 29'-7"  |        |      |            |        |        |   |   |   |             |
| P602         | SER OF | TO      | 180    | STR  |            |        |        |   |   |   | 3.25"       |
|              | 4      | 30'-5"  |        |      |            |        |        |   |   |   |             |
|              | 1      | 7'-0"   |        |      |            |        |        |   |   |   |             |
| P603         | SER OF | TO      | 51     | STR  |            |        |        |   |   |   | 12.25"      |
|              | 4      | 10'-1"  |        |      |            |        |        |   |   |   |             |
|              | 1      | 6'-4"   |        |      |            |        |        |   |   |   |             |
| P604         | SER OF | TO      | 48     | STR  |            |        |        |   |   |   | 12.75"      |
|              | 4      | 9'-6"   |        |      |            |        |        |   |   |   |             |
|              | 1      | 30'-3"  |        |      |            |        |        |   |   |   |             |
| P605         | SER OF | TO      | 184    | STR  |            |        |        |   |   |   | 3.75"       |
|              | 4      | 31'-2"  |        |      |            |        |        |   |   |   |             |
| P606         | 320    | 30'-10" | 14820  | 1    | 1'-0"      | 30'-0" |        |   |   |   |             |
| P1101*       | 40     | 25'-0"  | 5313   | STR  |            |        |        |   |   |   |             |
| P1102        | 4      | 5'-2"   | 110    | STR  |            |        |        |   |   |   |             |
| P1103        | 4      | 3'-5"   | 73     | STR  |            |        |        |   |   |   |             |
| P1104        | 4      | 9'-1"   | 193    | STR  |            |        |        |   |   |   |             |
| P1105        | 4      | 7'-4"   | 156    | STR  |            |        |        |   |   |   |             |
|              |        | TOTAL   | 12915  | LBS  |            |        |        |   |   |   |             |

REINFORCING STEEL LIST

| MARK                  | NUMBER | LENGTH  | WEIGHT | TYPE | DIMENSIONS |        |        |       |   |   | SERIES INC. |
|-----------------------|--------|---------|--------|------|------------|--------|--------|-------|---|---|-------------|
|                       | TOTAL  |         |        |      | A          | B      | C      | D     | E | R |             |
| <b>SUPERSTRUCTURE</b> |        |         |        |      |            |        |        |       |   |   |             |
| S401                  | 38     | 30'-0"  | 762    | STR  |            |        |        |       |   |   |             |
|                       | 1      | 16'-10" |        |      |            |        |        |       |   |   |             |
| S402                  | SER OF | TO      | 94     | STR  |            |        |        |       |   |   | 2.25"       |
|                       | 8      | 18'-2"  |        |      |            |        |        |       |   |   |             |
| S403                  | 32     | 9'-0"   | 192    | STR  |            |        |        |       |   |   |             |
|                       | 1      | 8'-10"  |        |      |            |        |        |       |   |   |             |
| S404                  | SER OF | TO      | 57     | STR  |            |        |        |       |   |   | 6.25"       |
|                       | 8      | 12'-6"  |        |      |            |        |        |       |   |   |             |
| S405                  | 2      | 26'-11" | 36     | STR  |            |        |        |       |   |   |             |
| S406                  | 2      | 16'-1"  | 21     | STR  |            |        |        |       |   |   |             |
|                       | 1      | 7'-1"   |        |      |            |        |        |       |   |   |             |
| S407                  | SER OF | TO      | 39     | STR  |            |        |        |       |   |   | 0.5"        |
|                       | 8      | 7'-4"   |        |      |            |        |        |       |   |   |             |
|                       | 1      | 19'-1"  |        |      |            |        |        |       |   |   |             |
| S408                  | SER OF | TO      | 115    | STR  |            |        |        |       |   |   | 8.25"       |
|                       | 8      | 23'-11" |        |      |            |        |        |       |   |   |             |
| S409                  | 468    | 3'-11"  | 1224   | 2    | 1'-3"      | 1'-7"  | 1'-3"  |       |   |   |             |
| S501                  | 6      | 4'-6"   | 28     | 10   | 1'-1"      | 1'-4"  | 1'-0"  | 2'-0" |   |   |             |
| S502                  | 1      | 12'-0"  | 13     | 3    | 1'-11"     | 3'-10" |        |       |   |   |             |
| S503                  | 1      | 12'-4"  | 13     | 3    | 2'-1"      | 3'-10" |        |       |   |   |             |
| S504                  | 11     | 13'-2"  | 151    | 3    | 2'-6"      | 3'-10" |        |       |   |   |             |
| S505                  | 176    | 9'-3"   | 1698   | 2    | 3'-10"     | 1'-10" | 3'-10" |       |   |   |             |
| S506                  | 36     | 10'-10" | 407    | 3    | 2'-6"      | 2'-8"  |        |       |   |   |             |
| S507                  | 50     | 11'-2"  | 582    | 3    | 2'-6"      | 2'-10" |        |       |   |   |             |
| S508                  | 33     | 11'-6"  | 396    | 3    | 2'-6"      | 3'-0"  |        |       |   |   |             |
| S509                  | 26     | 11'-10" | 321    | 3    | 2'-6"      | 3'-2"  |        |       |   |   |             |
| S510                  | 2      | 8'-11"  | 19     | 2    | 3'-10"     | 1'-6"  | 3'-10" |       |   |   |             |
| S511                  | 2      | 8'-10"  | 18     | 2    | 3'-10"     | 1'-5"  | 3'-10" |       |   |   |             |
| S512                  | 2      | 9'-1"   | 19     | 2    | 3'-10"     | 1'-8"  | 3'-10" |       |   |   |             |
| S513                  | 1      | 10'-2"  | 11     | 3    | 2'-0"      | 2'-10" |        |       |   |   |             |
| S514                  | 1      | 10'-0"  | 10     | 3    | 1'-11"     | 2'-10" |        |       |   |   |             |
| S515                  | 1      | 10'-8"  | 11     | 3    | 2'-3"      | 2'-10" |        |       |   |   |             |
| S516                  | 21     | 11'-8"  | 256    | 3    | 2'-11"     | 2'-8"  |        |       |   |   |             |
| S517                  | 52     | 9'-1"   | 493    | 2    | 3'-7"      | 2'-2"  | 3'-7"  |       |   |   |             |
| S518                  | 21     | 11'-4"  | 248    | 3    | 2'-11"     | 2'-6"  |        |       |   |   |             |
| S519                  | 20     | 8'-11"  | 186    | 2    | 3'-6"      | 2'-2"  | 3'-6"  |       |   |   |             |
| S520                  | 128    | 8'-9"   | 1168   | 2    | 3'-5"      | 2'-2"  | 3'-5"  |       |   |   |             |
| S521                  | 44     | 11'-0"  | 505    | 3    | 2'-11"     | 2'-4"  |        |       |   |   |             |
| S522                  | 18     | 10'-8"  | 200    | 3    | 2'-11"     | 2'-2"  |        |       |   |   |             |
| S523                  | 36     | 8'-5"   | 316    | 2    | 3'-3"      | 2'-2"  | 3'-3"  |       |   |   |             |
| S524                  | 19     | 10'-4"  | 205    | 3    | 2'-11"     | 2'-0"  |        |       |   |   |             |
| S525                  | 32     | 8'-3"   | 275    | 2    | 3'-2"      | 2'-2"  | 3'-2"  |       |   |   |             |
| S526                  | 11     | 10'-0"  | 115    | 3    | 2'-11"     | 1'-10" |        |       |   |   |             |
| S527                  | 24     | 12'-4"  | 309    | 3    | 2'-11"     | 3'-0"  |        |       |   |   |             |
| S528                  | 2      | 11'-10" | 25     | 3    | 2'-8"      | 3'-0"  |        |       |   |   |             |
| S529                  | 2      | 11'-0"  | 23     | 3    | 2'-3"      | 3'-0"  |        |       |   |   |             |
| S530                  | 2      | 10'-6"  | 22     | 3    | 2'-0"      | 3'-0"  |        |       |   |   |             |
| S531                  | 2      | 10'-4"  | 22     | 3    | 1'-11"     | 3'-0"  |        |       |   |   |             |
| S532                  | 6      | 4'-5"   | 28     | 10   | 1'-6"      | 1'-4"  | 0'-7"  | 2'-0" |   |   |             |
| S533                  | 2      | 4'-1"   | 9      | STR  |            |        |        |       |   |   |             |
| S534                  | 1      | 4'-9"   | 5      | STR  |            |        |        |       |   |   |             |
| S535                  | 120    | 8'-11"  | 1116   | 2    | 3'-8"      | 1'-10" | 3'-8"  |       |   |   |             |
| S536                  | 23     | 10'-6"  | 252    | 3    | 2'-6"      | 2'-6"  |        |       |   |   |             |
| S537                  | 2      | 8'-1"   | 17     | 2    | 3'-5"      | 1'-6"  | 3'-5"  |       |   |   |             |
| S538                  | 2      | 8'-0"   | 17     | 2    | 3'-5"      | 1'-5"  | 3'-5"  |       |   |   |             |
| S539                  | 2      | 8'-3"   | 17     | 2    | 3'-5"      | 1'-8"  | 3'-5"  |       |   |   |             |
| S540                  | 1      | 9'-6"   | 10     | 3    | 2'-0"      | 2'-6"  |        |       |   |   |             |



REINFORCING STEEL NOTES

- SERIES BARS - EACH BAR VARIES BY TABULATED AMOUNT.
  - ALL DIMENSIONS ARE OUT TO OUT.
  - TYPE 'STR' INDICATES A STRAIGHT BAR.
  - THE BAR SIZE NUMBER IS INDICATED IN THE MARK COLUMNS. THE FIRST ONE OR TWO DIGITS OF EACH MARK INDICATES THE BAR SIZE NUMBER. FOR EXAMPLE, A501 IS A #5 BAR SIZE AND P1001 IS A #10 BAR SIZE.
  - ALL REINFORCING STEEL SHALL BE EPOXY COATED.
  - 16" C.I.P. REINF. CONC. PILE REINFORCING IS PROVIDED FOR INFORMATION ONLY. ALL REINFORCING STEEL REQUIRED FOR 16" C.I.P PILE IS TO BE INCLUDED FOR PAYMENT UNDER ITEM 507 - 16" CAST-IN-PLACE REINFORCED CONCRETE PILES, FURNISHED.
- \* BAR UTILIZING MECHANICAL CONNECTORS  
 ◇ SEE NOTE 6

DESIGN AGENCY: PENNONI ASSOCIATES INC.  
 2 SUMMIT PARK DRIVE, SUITE 335  
 INDEPENDENCE, OHIO 44131

DATE: 2/20/21  
 STRUCTURE FILE NUMBER: 4805144

REVIEWED: MDP  
 DRAWN: CJK

DESIGNED: ARA  
 CHECKED: BPS

REINFORCING STEEL LIST - 2  
 BRIDGE NO. LUC-184-0892  
 S.R. 184 (ALEXIS RD.) OVER SILVER CREEK

LUC-184-8.92  
 PID No. 101327

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