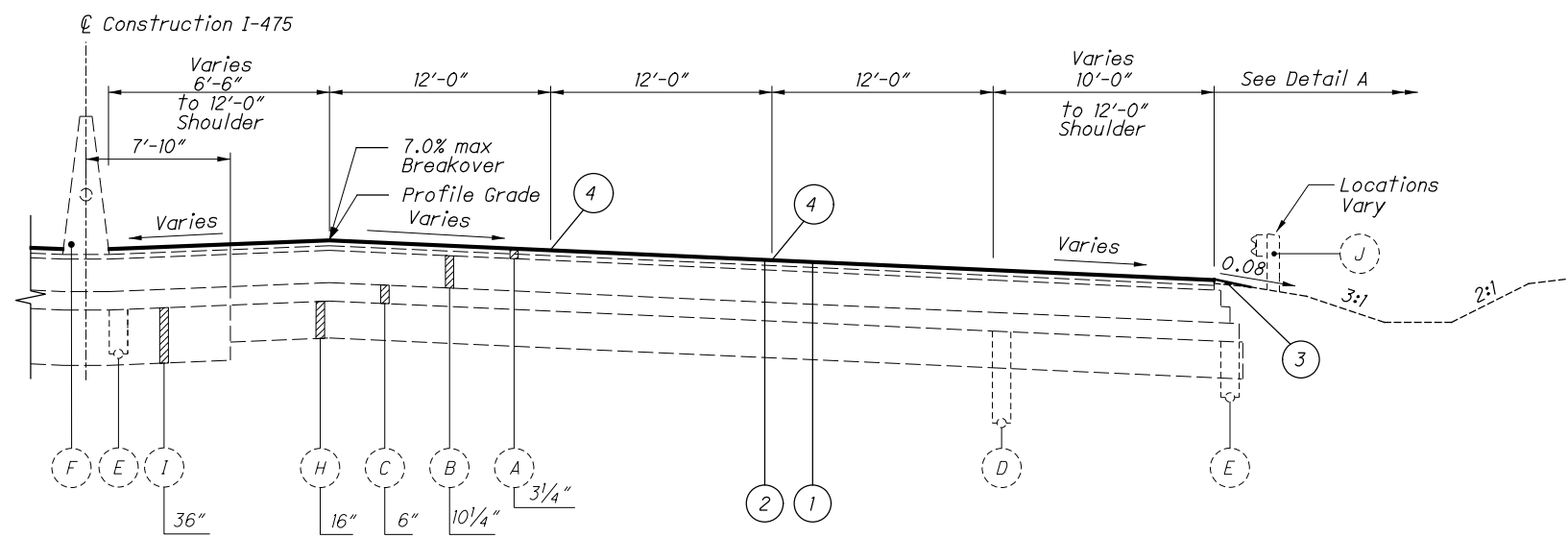
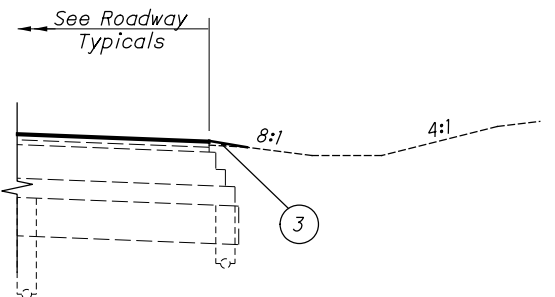


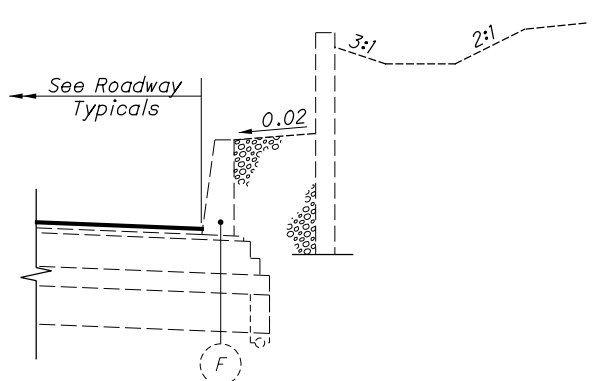
**SHOULDER SECTION - EASTBOUND I.R. 475**  
 I-475 Sta. 258+30.00 to Sta. 267+00.00 (Median Shoulder) = 870.00 FT.  
 I-475 Sta. 262+50.00 to Sta. 267+00.00 (Outside Shoulder) = 450.00 FT.



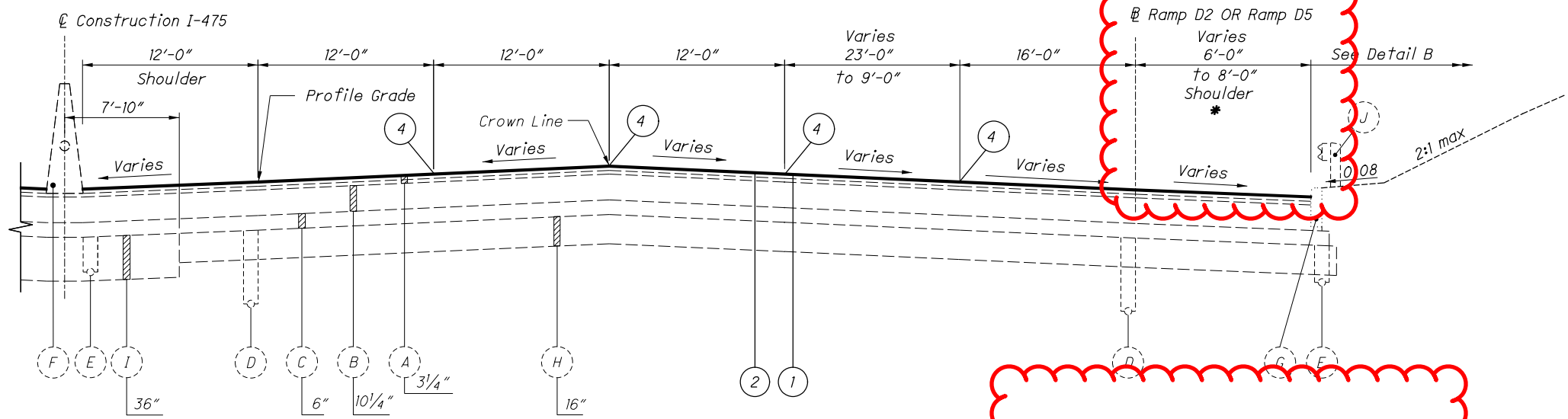
**TYPICAL SECTION - E**  
**SUPERELEVATED SECTION - EASTBOUND I-475**  
 I-475 Sta. 267+00.00 to Sta. 279+20.64 = 1220.64 FT.



**DETAIL A**  
 Applies From Sta. 273+63.14 to Sta. 277+70.36



**DETAIL B**  
 Applies From Sta. 281+59.57 to Sta. 282+68.55



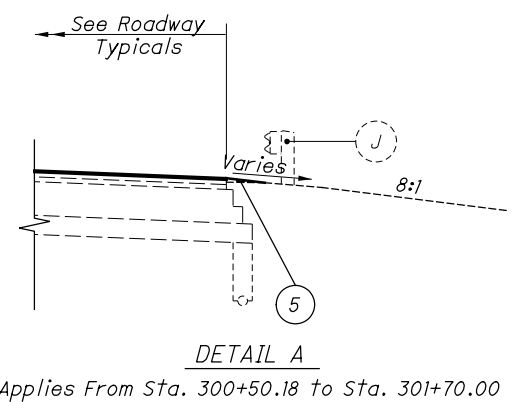
**TYPICAL SECTION - F**  
**NORMAL SECTION - EASTBOUND I-475**  
 I-475 Sta. 279+20.64 to Sta. 286+96.00 = 775.36 FT.  
 I-475 Sta. 295+55 to Sta. 300+50.18 = 495.18 FT.

ADDED NOTE

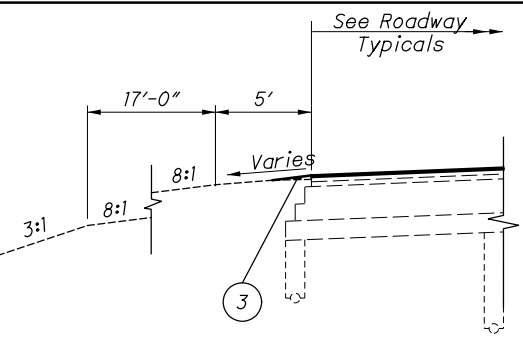
Ramp D2 OR Ramp D5  
 Varies  
 6'-0"  
 to 8'-0"  
 Shoulder

\* NOTE: STA. 295+55 TO STA. 300+50.18  
 PAVEMENT PLANING ASPHALT CONCRETE, 1"  
 (I-475 ADJACENT PAVEMENT AREA ONLY)

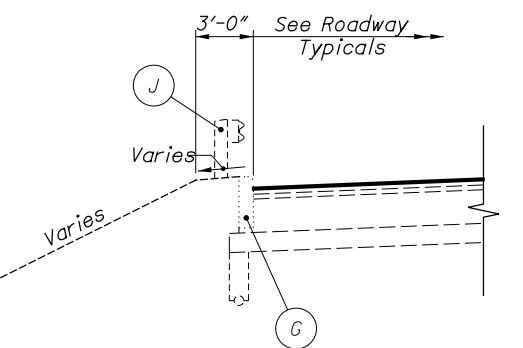
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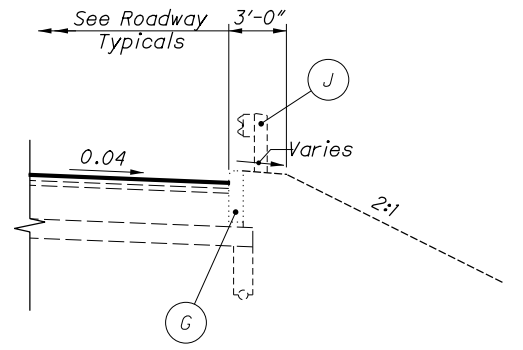
**DETAIL A**  
Applies From Sta. 300+50.18 to Sta. 301+70.00



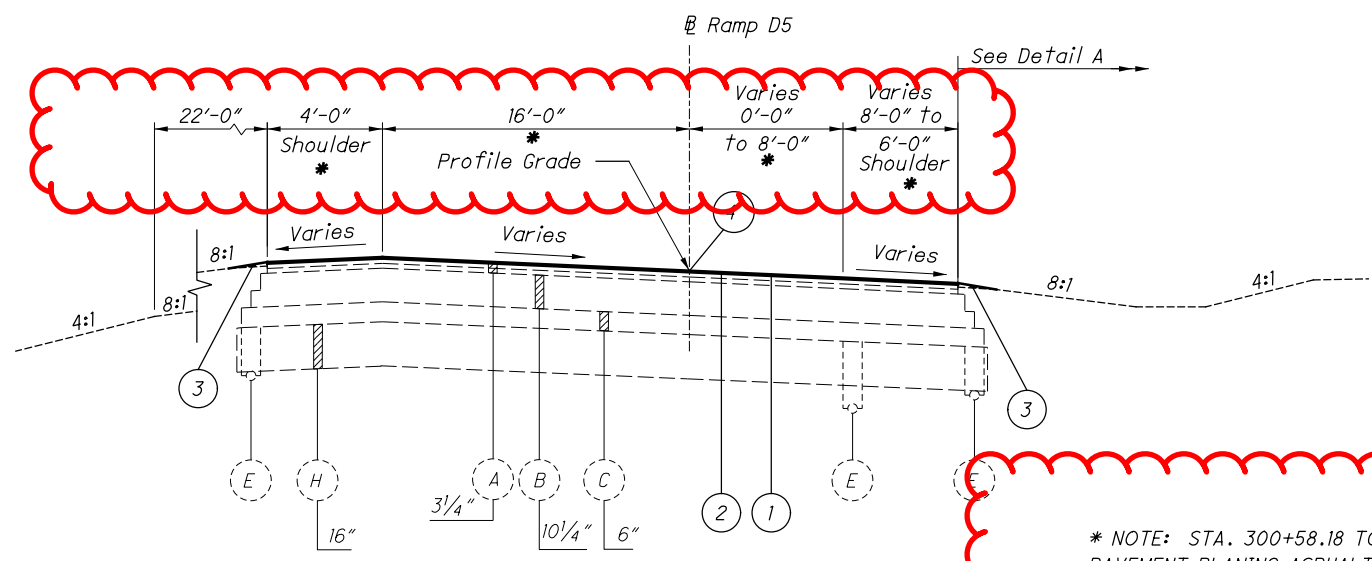
**DETAIL B**  
Applies From Sta. 313+04.25 to Sta. 315+63.37



**DETAIL C**  
Applies From Sta. 317+02.14 to Sta. 317+30.29



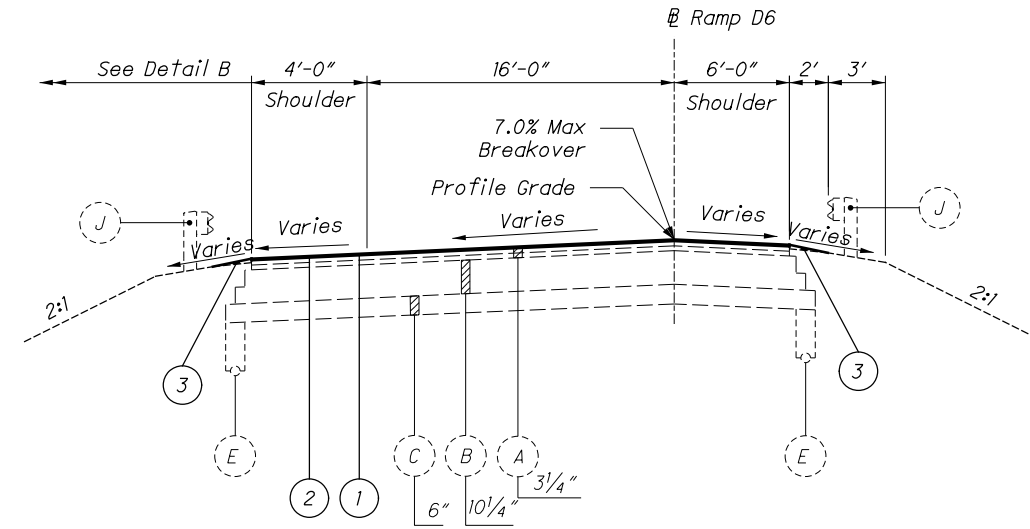
**DETAIL D**  
Applies From Sta. 317+05.11 to Sta. 317+30.29



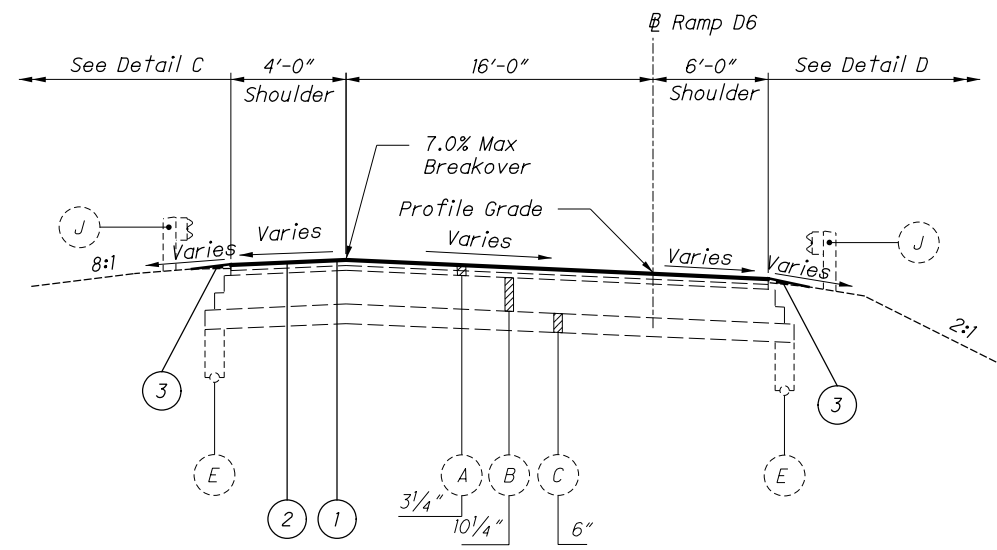
**TYPICAL SECTION - O**  
NORMAL SECTION - Ramp D5  
Ramp D5 Sta. 300+50.18 to Sta. 310+54.00 = 1003.82 FT.

\* NOTE: STA. 300+58.18 TO STA. 303+54  
PAVEMENT PLANING ASPHALT CONCRETE, 1"  
(I-475 ADJACENT PAVEMENT AREA ONLY)

ADDED NOTE



**TYPICAL SECTION - P**  
SUPERELEVATED SECTION - Ramp D6  
Ramp D6 Sta. 311+19.00 to Sta. 315+63.37 = 444.37 FT.



**TYPICAL SECTION - Q**  
SUPERELEVATED SECTION - Ramp D6  
Ramp D6 Sta. 315+63.37 to Sta. 317+30.29 = 166.92 FT.  
Sta. 318+90.43 to Sta. 321+31.65 = 241.22 FT.

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**MEDIAN BARRIER REPLACEMENT**

THIS ITEM WILL INCLUDE THE REMOVAL AND REPLACEMENT OF VARIOUS LOCATIONS OF MEDIAN BARRIER INCLUDED IN THIS PLAN. THE FOLLOWING QUANTITIES WILL BE INCLUDED FOR MEDIAN BARRIER REMOVAL AND REPLACEMENT:

ITEM 622 - CONCRETE BARRIER, TYPE D, AS PER PLAN 93 FT.

IT IS THE INTENT TO FOLLOW THE PLAN INSERT SHEET INCLUDED IN THIS PLAN FOR CONCRETE BARRIER WALL INSTALLATION. PLACEMENT OF MEDIAN BARRIER WALL SHALL IMMEDIATELY FOLLOW THE REMOVAL.

**AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS**

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED A HEIGHT OF 145 FT. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA), AND THE ODOT OFFICE OF AVIATION, WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL COORDINATION IS MET AND DOCUMENTATION HAS BEEN FURNISHED TO THE PROJECT ENGINEER. IF COORDINATION IS NOT OBTAINED, THEN THE PROJECT ENGINEER WILL HAVE THE AUTHORITY TO PROVIDE RESTRICTIONS AS REQUIRED.

THE TOLEDO HOSPITAL  
 PROMEDICA HEALTH SYSTEM  
 2142 N. COVE BLVD.  
 TOLEDO, OHIO 43606  
 419.291.4000

**ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (447), AS PER PLAN**

ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5MM TYPE A (447), AS PER PLAN SHALL FOLLOW THE SPECIFICATIONS FOR THE 442 ITEM EXCEPT FOR SECTION 442.04 ASPHALT BINDER, THE BINDER SHALL BE PG76-22M FOR THE SURFACE COURSE AND A MAXIMUM OF 10% OF RAP BY DRY WEIGHT OF MIX CAN BE USED. ADDITIONALLY, THE COARSE AGGREGATE FOR THIS ITEM SHALL CONTAIN A MINIMUM OF 50% AIR COOLED BLAST FURNACE SLAG, STEEL SLAG, OR CRUSHED IGNEOUS ROCK.

**ITEM 442, ASPHALT CONCRETE INTERMEDIATE COURSE, AS PER PLAN**

ITEM 442, ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE A (446), AS PER PLAN SHALL FOLLOW THE SPECIFICATIONS FOR THE 442 ITEM EXCEPT FOR SECTION 442.04 ASPHALT BINDER. THE BINDER SHALL BE PG76-22M FOR THE INTERMEDIATE COURSE AND A MAXIMUM OF 20% OF RAP BY DRY WEIGHT OF MIX CAN BE USED.

**ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN**

PER CMS 424.08, 448 DENSITY APPLIES TO THIS PROJECT. DENSITY WILL BE TESTED ACCORDING TO SUPPLEMENT 1055 PER CMS 448.02. THE DISINCENTIVE PORTION OF S-1055 (TABLE 1055.01-1 AND TABLE 1055.04) WILL BE WAIVED PROVIDING THAT THE CONTRACTOR MAKES EVERY EFFORT TO OBTAIN DENSITY AND DOES NOT USE VIBRATORY ROLLERS.

AN ESTIMATED QUANTITY OF 100 CY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO ACCOUNT FOR SURFACE IRREGULARITIES.

**PLANED SURFACES**

NO PLANED SURFACES SHALL BE OPEN TO THE PUBLIC FOR MORE THAN 5 DAYS. IF THE PLANED SURFACE IS OPEN FOR MORE THAN 5 DAYS, THEN IT IS THE CONTRACTOR'S RESPONSIBILITY TO REPAIR THE PAVEMENT FAILURES THAT OCCURRED AFTER THE 5 DAYS.

**RUMBLE STRIPE REMOVAL BEFORE PAVING**

RUMBLE STRIPS WILL BE PLANED WITH ITEM 441, FOR THE I-475 LOCATION. THE QUANTITIES FOR PLANING AND PAVING THE RUMBLE STRIPS ARE PROVIDED BELOW. QUANTITIES ARE BASED ON 2' WIDE MILL. QUANTITIES TO BE CARRIED TO THE GENERAL SUMMARY.

I 475 EB EXISTING LENGTH OF RUMBLE STRIP:  
 LENGTH = 1346 FT (x2) = 2692 FT

I 475 WB EXISTING LENGTH OF RUMBLE STRIP:  
 LENGTH = 7192 FT (x2) = 14384 FT

RAMP S-W TO I 475 WB EXISTING LENGTH OF RUMBLE STRIP:  
 LENGTH = 900 FT (x2) = 1800 FT

RAMP N-W TO I 475 WB EXISTING LENGTH OF RUMBLE STRIP:  
 LENGTH = 775 FT (x2) = 1550 FT

TOTAL 22026 FT

**ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2 "**

I 475 EB 598 SY  
 I 475 WB 3197 SY  
 RAMP S-W TO I 475 WB 400 SY  
 RAMP N-W TO I 475 WB 345 SY  
 ITEM 254 TOTAL = 4540 SY

**ITEM 407 - NON TRACKING TACK COAT**

I 475 EB 51 GAL  
 I 475 WB 272 GAL  
 RAMP S-W TO I 475 WB 34 GAL  
 RAMP N-W TO I 475 WB 30 GAL  
 ITEM 407 TOTAL = 387 GAL

**ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22, 1 1/2"**

I 475 EB 25 CY  
 I 475 WB 133 CY  
 RAMP S-W TO I 475 WB 17 CY  
 RAMP N-W TO I 475 WB 15 CY  
 ITEM 441 TOTAL = 190 CY

**ITEM 255, FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, MISC.: CLASS QC 3**

MATERIALS: PROVIDE MATERIALS CONFORMING TO THE FOLLOWING REQUIREMENTS:

PORTLAND CEMENT CONCRETE  
 499.03, CLASS QC 3, W/MACRO-FIBERS\*  
 QC3: FLEXURAL STRENGTH: 300 PSI IN 12 HOURS  
 PERMEABILITY: 2000 COULOMBS  
 MACROFIBERS: MINIMUM 4.0 LB/CY  
 COARSE AGGREGATE (NO. 57 & NO.8) 703.02 & 703.13  
 FINE AGGREGATE (NATURAL SAND) 703.02  
 PORTLAND CEMENT, TYPE I 701.04  
 FLY ASH OR NATURAL POZZOLAN 701.13  
 SLAG CEMENT 701.09  
 WATER 499.02  
 CHEMICAL ADMIXTURE 705.12  
 AIR-ENTRAINING ADMIXTURE 705.10  
 MACRO-FIBERS FOR CONCRETE 705.29  
 LIQUID MEMBRANE-FORMING COMPOUNDS FOR CONCRETE CURING 705.07

\*USE A MINIMUM DOSAGE RATE OF FIBERS OF 4.0 LB/YD<sup>3</sup> OF CONCRETE. ENSURE THE FINAL PROPOSED MIX IS WORKABLE AND ABLE TO BE PRODUCED SUCH THAT BALLING OR CLUMPING OF THE FIBERS IS NOT A PROBLEM AS DETERMINED BY THE ENGINEER. A DEMONSTRATION OF THE MIX PRODUCTION, OR TRIAL MIX, MAY BE REQUIRED BY THE ENGINEER PRIOR TO PLACING ANY OF THE MIX ON THE PROJECT.

PAVEMENT SHALL BE PLANED BEFORE PAVEMENT IS REMOVED. THE FOLLOWING ESTIMATED QUANTITY IS TO BE USED FOR PAVEMENT REMOVAL AND RIGID REPLACEMENT AS DIRECTED BY THE ENGINEER. THE CONTRACTOR MAY BRING THE CONCRETE TO THE MILLED SURFACE.

ITEM 255 - FULL DEPTH PAVEMENT REPAIR AND RIGID REPLACEMENT, MISC.: CLASS QC 3  
 60-6x12 JOINTS NB = 480 SQ. YD.  
 75-6x12 JOINTS SB = 600 SQ. YD.  
 TOTAL = 1080 SQ. YD.

ITEM 255 - FULL DEPTH PAVEMENT SAWING  
 2160 LF (NB)  
 2700 LF (SB)  
 TOTAL = 4860 LF

QUANTITIES CARRIED TO THE GENERAL SUMMARY.

**LONGITUDINAL JOINTS (FLEXIBLE PAVEMENT)**

LOCATE LONGITUDINAL JOINTS IN THE SURFACE COURSE SUBJECT TO THE FOLLOWING REQUIREMENTS:

- PLACE THE MAINLINE PAVEMENT SURFACE COURSE WITH A SINGLE COLD LONGITUDINAL JOINT LOCATED AT THE LANE LINES. NO OTHER COLD JOINTS ARE PERMITTED IN THE SURFACE COURSE OF THE MAINLINE PAVEMENT.

MODIFIED QUANTITIES

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CALCULATED  
 ALF  
 CHECKED  
 JMF

**GENERAL NOTES**

LUC-75 / 475-7.82 / 14.50



**SEQUENCE OF OPERATIONS**

**CONSTRUCTION SEQUENCING FOR LUC-75.**

MILL AND FILL RUMBLE STRIPS AS NECESSARY PRIOR TO THE START OF EACH STAGE.

THE INTENT IS THAT LONGITUDINAL JOINTS SHALL BE PLACED ON THE LANE LINE, EDGE LINE, OR CENTER OF A TRAVELED LANE.

WEDGING AT TRANSVERSE AND LONGITUDINAL JOINTS SHALL BE INSTALLED PER SCD MT-101.90.

STAGE 1: LIMITED TO FOUR WEEKENDS FROM FRIDAY 9PM TO MONDAY 6AM. THE RAMP FROM I-280 NB TO I-75 NB MAY BE CLOSED. A MINIMUM OF TWO LANES ON I-75 NB MUST BE MAINTAINED AT ALL TIMES EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. ALL LANE SHIFTS AND LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETE OVERLAY OF FORWARD AND REAR APPROACH SLABS FOR STRUCTURE LUC-75-0849.

COMPLETE OVERLAY OF FORWARD AND REAR APPROACH SLABS FOR STRUCTURE LUC-75-0861.

COMPLETE DECK SEALING OF STRUCTURE LUC-75-0891.

COMPLETE PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE FOLLOWING LOCATIONS:  
RAMP LL STA 19+54/RAMP A STA 351+32- RAMP LL STA 30+03/RAMP A 362+15.  
LUC-75 NB FROM STA 363+89 - STA 386+15

STAGE 2: THIS WORK SHALL BE LIMITED TO 10 DAYS. I-75 NB SHALL REMAIN THREE LANES FROM THE I-280 INTERCHANGE UNTIL THE LANE REDUCTION AT STA 407+16, WHERE I-75 NB CAN BE REDUCED TO TWO LANE. ADDITIONAL CLOSURES PERMITTED BY THE PLCS ARE ACCEPTABLE. I-75 SB SHALL REMAIN A MINIMUM OF TWO LANES EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. ALL LANE SHIFTS AND LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETE MEDIAN WALL REPAIR, PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE FOLLOWING LOCATIONS:

THE INSIDE PORTION AND LEFT LANE OF LUC-75 NB FROM STA 389+88.52 TO STA 448+50.

THE INSIDE PORTION AND LEFT LANE OF LUC-75 SB FROM STA 366+88.33 TO STA 448+50.

STAGE 3: THIS WORK SHALL BE LIMITED TO 10 DAYS. I-75 NB & I-75 SB SHALL REMAIN A MINIMUM OF TWO LANES IN EACH DIRECTION EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. ALL LANE SHIFTS AND LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETE PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE FOLLOWING LOCATIONS:

THE CENTER LANE, RIGHT LANE, AND OUTSIDE PORTION OF LUC-75 NB FROM STA 389+88.52 TO STA 448+50.

THE CENTER LANE, RIGHT LANE, AND OUTSIDE PORTION OF LUC-75 SB FROM 366+88.33 TO STA 448+50.

STAGE 4: THIS WORK SHALL BE LIMITED TO 21 DAYS. A MINIMUM OF TWO LANES IN EACH DIRECTION MUST BE MAINTAINED AT ALL TIMES EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. LANE SHIFTS AND LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETED THE PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE INSIDE PORTION FOR I-75 NB & SB FROM STA 450+64.22 TO STA 539+50.

STAGE 5: THIS WORK SHALL BE LIMITED TO 21 DAYS. A MINIMUM OF TWO LANES IN EACH DIRECTION MUST BE MAINTAINED AT ALL TIMES EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. LANE SHIFTS AND THE LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETE THE PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE OUTSIDE PORTION OF LUC-75 NB AND SB FROM STA. 450+64.22 TO STA. 593.50.

STAGE 6: PLACE THE FINAL SURFACE COURSE. A MINIMUM OF 3 LANE MUST BE MAINTAINED AT ALL TIMES FROM 7AM TO 7PM. THE PERMITTED LANE CLOSURE SCHEDULE SHALL BE FOLLOWED ALL OTHER TIMES.

INSTALL PERMANENT PAVEMENT MARKINGS AND ALL OTHER SAFTEY ITEMS.

**CONSTRUCTION SEQUENCING FOR LUC-475.**

**STAGE 1: JOINT REPAIRS**

FOR THREE (3) WEEKENDS, CONTRACTOR WILL BE ALLOWED MOT LANE REDUCTION AND CLOSURE PERIODS AS DETAILED BELOW TO PERFORM JOINT REPAIR WORK IN LIEU OF ODOT PERMITTED LANE CLOSURE TABLE TIMEFRAMES ALL WORK NOT COMPLETED DURING THESE 3 WEEKENDS WILL BE SUBJECT TO ODOT'S PERMITTED LANE CLOSURE TABLE. I 475 EXIT RAMPS TO PROMEDICA PARKWAY SHALL REMAIN OPEN AT ALL TIMES.

MILL AND FILL RUMBLE STRIPS PRIOR TO PERFORMING JOINT REPAIR WORK. (TO BE PERFORMED PRIOR TO THE START OF THESE 3 WEEKENDS)

WEEKEND 1: WB REPAIRS ON I-75/I-475 WB SYSTEM RAMPS, UP TO STATION 344+00 +/-.

-CLOSE THE SYSTEM RAMP FROM I-75 NB TO I-475 WB FOR HALF OF THE WEEKEND TO COMPLETE REPAIRS ALONG THE RAMP. REOPEN I75 NB TO I-475 WB SYSTEM RAMP TO WB 475, PRIOR TO PERFORMING 2ND HALF OF WEEKEND WORK.  
-CLOSE THE SYSTEM RAMP FROM I-75 SB TO I-475 WB FOR WTHE REMAINING HALF OF THE WEEKEND TO COMPLETE REPAIRS ALONG THE RAMP. (THIS RAMP CLOSURE MUST START BETWEEN THE HOURS OF 9PM AND 6AM.)

WEEKEND 2: REPAIRS ON WB I-475 MAINLINE

-CLOSE THE SYSTEM RAMP FROM I-75 NB TO I-475 WB.  
-PERFORM THE PAVEMENT REPAIRS (STATION 344+00+/- TO WEST END OF PROJECT). MAINTAIN MINIMUM TWO OPEN LANES OF TRAFFIC BY USE OF RIGHT SHOULDER AND RIGHT LANE ON I-475 WB. CONTRACTOR MAY REMOVE RIGHT LANE EDGE LINE UP TO 48 HOURS PRIOR TO THE START OF WEEKEND WORK. INSTALL NO EDGE LINE SIGNS. PLACE DRUMS IN CLOSED SHOULDER AREA. PLACE TEMP LANE LINE BETWEEN SHOULDER AND RIGHT LANE, PRIOR TO SHIFTING TRAFFIC TO SHOULDER FOR WEEKEND WORK. AT CONCLUSION OF WEEKEND WORK, RE-CLOSE SHOULDER WITH DRUMS. PLACE TEMPORARY EDGE LINE WITHIN

48 HOURS OF COMPLETING WEEKEND WORK.  
-THE RAMP FROM PROMEDICA TO I-475 WB WILL NEED TO BE CLOSED TO REDUCE CONGESTION

WEEKEND 3: REPAIRS NEAR DOUGLAS RD OFF RAMP AND PROMEDICA ON RAMP  
-CLOSE THE RIGHT LANE OF I-475 AFTER THE PROMEDICA EXIST RAMP TO MAKE REPAIRS ON THE PROMEDICA ENTRANCE RAMP, DOUGLAS EXIST RAMP, AND RIGHT LANE OF I-475 WB.  
-I-475 EB: REDUCE TO ONE LANE (FROM WEST END OF PROJECT UP TO STATION 287+00) FOR 24 HOURS DURING ONE OF THE THREE ALLOTTED WEEKENDS TO COMPLETE THE PAVEMENT REPAIRS. ALL LANES OF EB 475 SHALL REOPEN AS SOON AS JOINT REPAIR WORK IS COMPLETED.

RAMPS AT PROMEDICA PKWY. WILL INCLUDE ONE OVERNIGHT CLOSURE FROM 9PM TO 6AM. DAMAGES WILL OCCUR AT \$2500/15 MINUTES FOR EACH 15 MINUTES THE RAMP REMAINS CLOSED AFTER THE DURATION.

ONLY ONE PROMEDICA PKWY. RAMP SHALL BE CLOSED AT A TIME. THE RAMPS ON PROMEDICA PKWY. SHALL NOT BE CLOSED AT THE SAME TIME AS THE RAMPS AT DOUGLAS RD.

PROMEDICA PKWY. EB EXIT RAMP DETOUR:  
SECOR RD. TO MONROE ST. TO PROMEDICA PKWY.

PROMEDICA PKWY. WB EXIT RAMP DETOUR:  
WB-475 TO DOUGLAS RD. TO EB-475 TO PROMEDICA PKWY.

PROMEDICA PKWY. EB ENTRANCE RAMP DETOUR:  
WB-475 TO DOUGLAS RD. TO EB-475.

PROMEDICA PKWY. WB ENTRANCE RAMP DETOUR:  
PROMEDICA PKWY. TO CENTRAL AVE. TO MONROE ST. TO SECOR RD. TO WB-475.

**STAGE 2: RESURFACING**

RESURFACE I-475 AND COMPLETE ALL SAFETY ITEMS.

A MINIMUM OF 3 LANES MUST BE MAINTAINED AT ALL TIMES FROM 7AM TO 7PM. A MINIMUM OF 2 LANES MUST BE MAINTAINED ON ALL SYSTEM RAMPS AND MUST BE MAINTAINED FROM 7AM TO 7PM. THE PERMITTED LANE CLOSURE SCHEDULE SHALL BE FOLLOWED ALL OTHER TIMES.

DESCRIPTION OF CRITICAL LANE/RAMP TO BE MAINTAINED	TIME UNIT	DISINCENTIVE, MONEY TIME UNIT
MAINTAIN A MINIMUM 2 LANES OF I-75	/MIN	\$100
MAINTAIN A MINIMUM OF 3 LANES OF I-75 NB DURING STAGE 2	/MIN	\$100
ALL STAGE COMPLETED WITHIN TIMEFRAME DESCRIBED IN THE CONSTRUCTION SEQUENCING	/HR	\$1000
SYSTEM RAMP CLOSURES	/15 MIN	\$1000

Ramp Closure	Maintenance of Traffic Operation	Detour
<b>Related to I-75 Work</b>		
I-280 NB to I-75 NB	May be closed up to 4 weekends according to the construction sequencing.	I-75 SB to Expressway Dr to I-75 NB
Ramp LL (Manhattan to I-75 NB)	May be closed up to 4 weekends according to the construction sequencing.	Manhattan Blvd to Phillips Ave to I-75 NB
I-75 NB to Ottawa River Rd	This ramp may be closed up to 5 nights from 9PM to 6AM.	I-75 NB to Alexis Rd to Suder Ave
Ottawa River Rd to I-75 SB	This ramp may be closed up to 5 nights from 9PM to 6AM.	Suder Ave to Alexis Rd to I-75 SB
I-75 NB to Alexis Rd	This ramp may be closed up to 5 nights from 9PM to 6AM.	I-75 NB to Erie Rd to I-75 SB
Alexis Rd to I-75 NB	This ramp may be closed up to 5 nights from 9PM to 6AM.	I-75 SB to Expressway Dr to I-75 NB
I-75 SB to Alexis Rd	This ramp may be closed up to 5 nights from 9PM to 6AM.	I-75 SB to Expressway Dr to I-75
Alexis Rd to I-75 SB	This ramp may be closed up to 5 nights from 9PM to 6AM.	I-75 NB to Erie Rd to I-75 SB
<b>Related to I-475 Work</b>		
I-75 NB to I-475 WB	May be closed up to one full weekend and up to 29 hours of an additional weekend according to the construction sequencing.	I-75 NB to Phillips Ave to I-75 SB
I-75 SB to I-475 WB	May be closed up to 29 hours of one weekend according to the construction sequencing.	I-75 SB to Detroit Ave to I-75 NB
Promedica Pkwy to I-475 WB	May be closed for two weekends according to the construction sequencing.	Promedica Pkwy to Central Ave to Monroe St. to I-475 WB
I-475 WB to Douglas Rd	May be closed for one weekend according to the construction sequencing.	I-475 WB to Secor Rd to I-475 EB

DETOUR RD CHANGED

NOTE ADDED

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SHEET NUM.														PART.		ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	
15	16	18	19	19A	20	21	22	23A	58	58A	59	82A	82C	01/IMS/PV	02/IMS/BR							
<b>ROADWAY</b>																						
LUMP																LUMP	201	11000	LS	CLEARING AND GRUBBING		
										3,271						3,271	202	23500	3,271	SY	WEARING COURSE REMOVED	
											15,035.5					15,035.5	202	38000	15,035.5	FT	GUARDRAIL REMOVED	
																17	42010	17	EACH	ANCHOR ASSEMBLY REMOVED, TYPE E		
																18	42041	18	EACH	ANCHOR ASSEMBLY REMOVED, TYPE T, AS PER PLAN	15	
																3	42050	3	EACH	ANCHOR ASSEMBLY REMOVED, TYPE B		
																35	47000	35	EACH	BRIDGE TERMINAL ASSEMBLY REMOVED		
																LUMP	202	98000	LS	REMOVAL MISC.: TRAFFIC MONITORING EQUIPMENT	19A	
																167	15000	167	STA	RESHAPING UNDER GUARDRAIL		
									7.56	6.01						13.57	209	60200	13.57	STA	LINEAR GRADING	
																13,070	15050	13,070	FT	GUARDRAIL, TYPE MGS		
																20	26150	20	EACH	ANCHOR ASSEMBLY, MGS TYPE E, MASH 2016		
																18	26550	18	EACH	ANCHOR ASSEMBLY, MGS TYPE T		
																25	35002	25	EACH	MGS BRIDGE TERMINAL ASSEMBLY, TYPE 1		
																10	35102	10	EACH	MGS BRIDGE TERMINAL ASSEMBLY, TYPE 2		
																93	24001	93	FT	CONCRETE BARRIER, TYPE D, AS PER PLAN	16	
2	93															2	39500	2	EACH	MONUMENT BOX ADJUSTED TO GRADE		
<b>EROSION CONTROL</b>																						
																2,904	10000	2,904	SY	SEEDING AND MULCHING		
																0.39	20000	0.39	TON	COMMERCIAL FERTILIZER		
																16	35000	16	MGAL	WATER		
																1,000	30000	1,000	EACH	EROSION CONTROL		
<b>DRAINAGE</b>																						
8																8	98630	8	EACH	CATCH BASIN ADJUSTED TO GRADE		
3																3	99654	3	EACH	MANHOLE ADJUSTED TO GRADE		
<b>PAVEMENT</b>																						
																241,995	01000	241,995	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 3/4"		
	4,540															4,540	01000	4,540	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2"		
																14,328	01000	14,328	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1"		
	1,065															1,065	02000	1,065	SY	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, MISC.: CLASS QC3	16	
	4,860															4,860	20000	4,860	FT	FULL DEPTH PAVEMENT SAWING		
	387															33,879	20000	43,238	GAL	NON-TRACKING TACK COAT		
	100															2,932	12001	3,032	CY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN	16	
	190															190	50000	190	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22		
																15,202	00100	15,202	CY	ANTI-SEGREGATION EQUIPMENT		
																10,065	10301	10,065	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), AS PER PLAN	16	
																277	10101	12,041	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, 19 MM, TYPE A (446), AS PER PLAN, PG72-22M	16	
																693	10100	1,173	CY	COMPACTED AGGREGATE		
																2,774	SPECIAL	69012060	2,774	SY	PAVEMENT OVERLAY FABRIC COMPOSITE	19
																5,867	875	10000	5,867	LB	LONGITUDINAL JOINT ADHESIVE	
																3,330	897	01010	3,330	SY	PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A, 3"	
<b>WATER WORK</b>																						
2																2	638	10800	2	EACH	VALVE BOX ADJUSTED TO GRADE	
<b>LIGHTING</b>																						
																102	625	26253	102	EACH	LUMINAIRE, CONVENTIONAL, SOLID STATE (LED), AS PER PLAN, COOPER GALLEON OR TYPE H30LPHANE MONGOS	
																1	625	34001	1	EACH	POWER SERVICE, AS PER PLAN	18
																LUMP	SPECIAL	62540000	LS	MAINTAIN EXISTING LIGHTING	18	
																1	SPECIAL	62540010	1	EACH	REPLACEMENT OF EXISTING LIGHTING UNIT	18

QUANTITIES REVISED

QUANTITIES REVISED

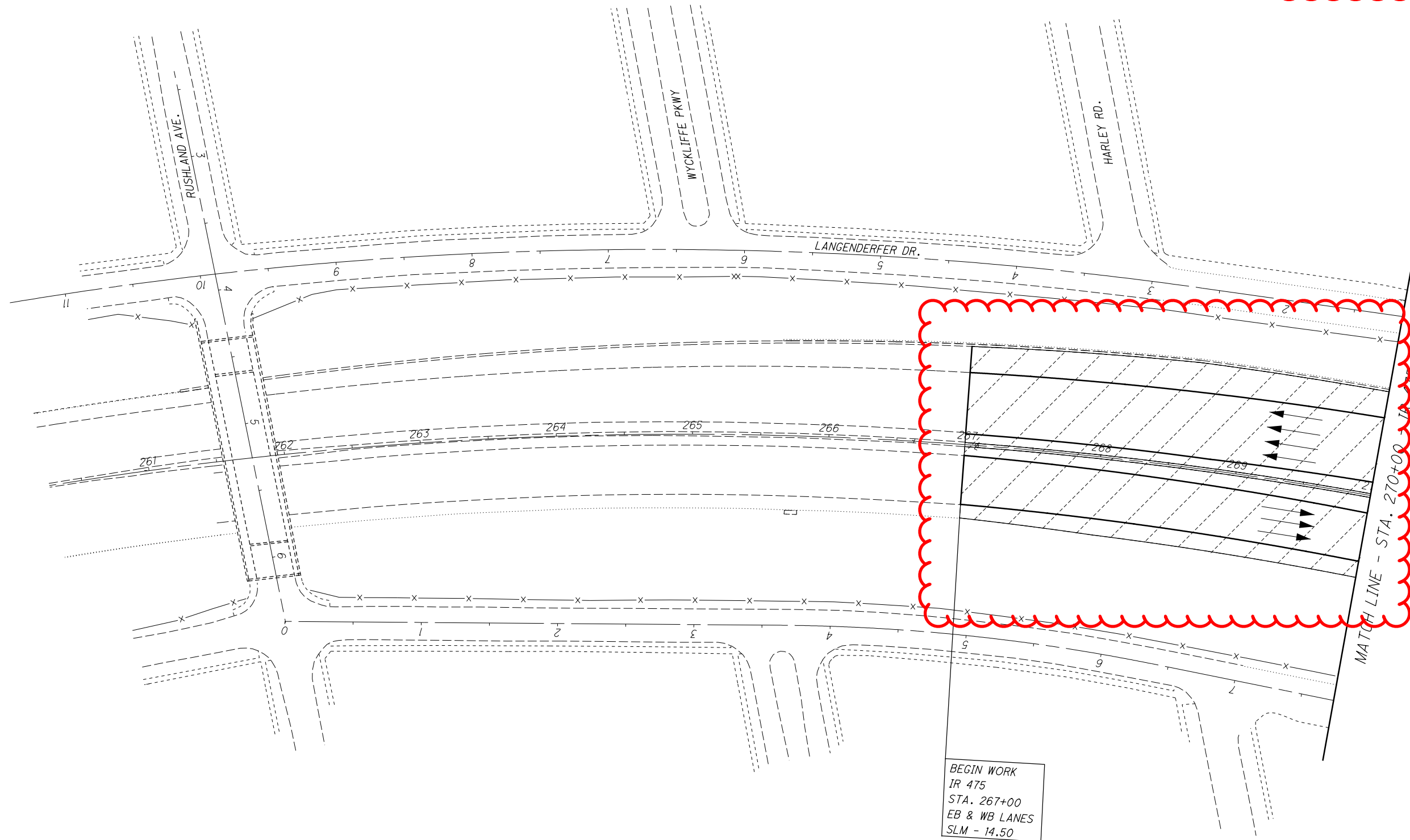
GENERAL SUMMARY

LUC-75 / 475-7.82 / 14.50

I:\ProjectData\LUC-75-8-42\Design\Roadway\Sheets\88563\_GS002.dgn Sheet 3/3/2021 10:47:39 AM afintel

STATION RANGE I-475			SIDE	TYPICAL SECTION	DISTANCE (D) FT	AVERAGE WIDTH (W) FT	CADD GENERATED AREA SY	202	206	254	407	424	617	875
TO	FROM	TO						FROM	TO	FROM	TO	FROM	TO	FROM
								WEARING COURSE REMOVED SY	PAVEMENT PLANNING, ASPHALT CONCRETE, 1" SY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN, 1" CY	COMPACTED AGGREGATE CY	LONGITUDINAL JOINT ADHESIVE LB		
267+00	TO	279+20.64	EB	E	1220.64	57.3	7765		3820		216	45	1526	
279+20.64	TO	282+68.55	EB	F	347.91	67.0	2590				72	13	435	
282+68.55	TO	286+96	EB	F	427.45	78.5	3728	212			104	2	107	
295+55	TO	300+50.18	EB	F	495.18	18.0	990		768		28	2	124	
267+00	TO	272+51	WB	A	551.00	76.00	4653		5000		129	20	138	
272+51	TO	272+72	WB	A	21.00	74.00	173				5	1	5	
272+72	TO	277+70.36	WB	A	498.36	72.00	3987				111	18	125	
277+70.36	TO	282+44.38	WB	B	474.02	72.00	3792				105	18	119	
282+44.38	TO	295+52.97	WB	B	1308.59	84.00	12214				339	48	327	
295+52.97	TO	302+03.11	WB	B	650.14	88.50	6393				178			
302+03.11	TO	304+53.57	WB	B	250.46	74.00	2059				57	9	63	
304+53.57	TO	316+51.29	WB	C	1197.72	75.00	9981	253			277	44	299	
316+51.29	TO	319+14.24	WB	C	89.24	78.00	773	253			21	3	22	
319+14.24	TO	321+17.04	WB	C	202.80	69.00	1555				43	8	51	
321+17.04	TO	322+56.44	WB	C	139.40	60.00	929	462			26	5	35	
322+56.44	TO	322+89.46	WB	C	33.02	80.00	294	464			8	1	8	
322+89.46	TO	330+24	WB	C	370.07	80.00	3290				91	7	185	
330+24	TO	330+66.69	WB	D	42.69	76.00	360				10	2	43	
330+66.69	TO	340+91.69	WB	D	1025.00	72.00	8200				228	38		
340+91.69	TO	345+41.69	WB	D	450.00	75.50	3775		2917		105	17		
345+41.69	TO	349+85.34	RAMP NW	H	443.65	41.00	2021				56	8	222	
349+85.34	TO	353+16.5	RAMP NW	H	331.16	52.25	1923	324			53	6	166	
345+41.69	TO	349+85.34	RAMP SW	G	443.65	45.50	2243				62	16	111	
349+85.34	TO	354+40.8	RAMP SW	G	455.46	39.25	1986	113			55	17	114	
271+20	TO	271+50	RAMP D1	I	30.00	58.50	195	149			5	1	8	
271+50	TO	276+20	RAMP D1	I	470.00	34.00	1776				49	17	118	
276+20	TO	276+70	RAMP D1	I	50.00	30.00	167				5	2	25	
276+70	TO	277+70.36	RAMP D1	I	100.36	27.00	301				8	4	50	
277+70.36	TO	282+44.38	RAMP D1	I	474.02	24.00	1264				35	9	237	
272+36	TO	274+07	RAMP D2	J	171.00	42.50	808	142			22	3	43	
272+36	TO	274+07	SLIP	J	171.00	33.00	627	149			17	3	43	
274+07	TO	279+20.64	RAMP D2	J	513.64	26.00	1484				41	10	128	
279+20.64	TO	282+68.55	RAMP D2	J	347.91	27.00	1044				29	13	87	
302+03.11	TO	304+53.57	RAMP D3	K	250.46	25.00	696				19	9	63	
304+53.57	TO	310+20	RAMP D3	K	566.43	26.00	1636	73			45	21	142	
311+20	TO	311+70	RAMP D4	L	50.00	78.25	435	196			12	1	13	
311+70	TO	313+91	RAMP D4	L	219.00	34.00	827				23	4	55	
313+91	TO	314+46.05	RAMP D4	L	103.54	34.00	391	97			11	2	26	
314+46.05	TO	314+46.05	RAMP D4	L	55.05	31.00	190	108			5	1	14	
317+35.95	TO	319+14.24	RAMP D4	M,N	178.29	28.00	555				15		45	
319+14.24	TO	321+17.04	RAMP D4	N	202.80	35.50	800				22		51	
321+17.04	TO	322+56.44	RAMP D4	N	139.40	23.00	356				10		35	
300+50.18	TO	301+90	RAMP D5	O	139.82	26.00	404				0	0	0	
301+90	TO	303+54	RAMP D5	O	164.00	36.50	665		1113		11	3	35	
303+54	TO	307+01	RAMP D5	O	347.00	27.00	1041				18		41	
307+01	TO	307+51	RAMP D5	O	50.00	30.00	167				29	6	87	
307+51	TO	310+00	RAMP D5	O	249.00	34.00	941				5	1	13	
310+00	TO	310+58	RAMP D5	O	58.00	84.25	543	203			26	5	62	
311+19	TO	316+93	RAMP D6	P,Q	574.00	26.00	1658	73			11	11	144	
316+93	TO	317+30.29	RAMP D6	Q	37.29	28.00	116				3	1	9	
318+90.43	TO	321+31.65	RAMP D6	Q	241.22	29.67	795		710.00		22	4	60	
<b>01/STR/PV TOTALS CARRIED TO GENERAL SUMMARY</b>								<b>3271</b>	<b>401</b>	<b>14328</b>	<b>8972</b>	<b>2932</b>	<b>480</b>	<b>5867</b>

QUANTITIES  
MODIFIED



MILL 1" FROM  
STA. 267+00 TO  
STA. 273+50 FOR  
BRIDGE CLEARANCE

CALCULATED ALF  
CHECKED JMF

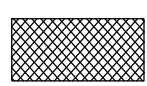
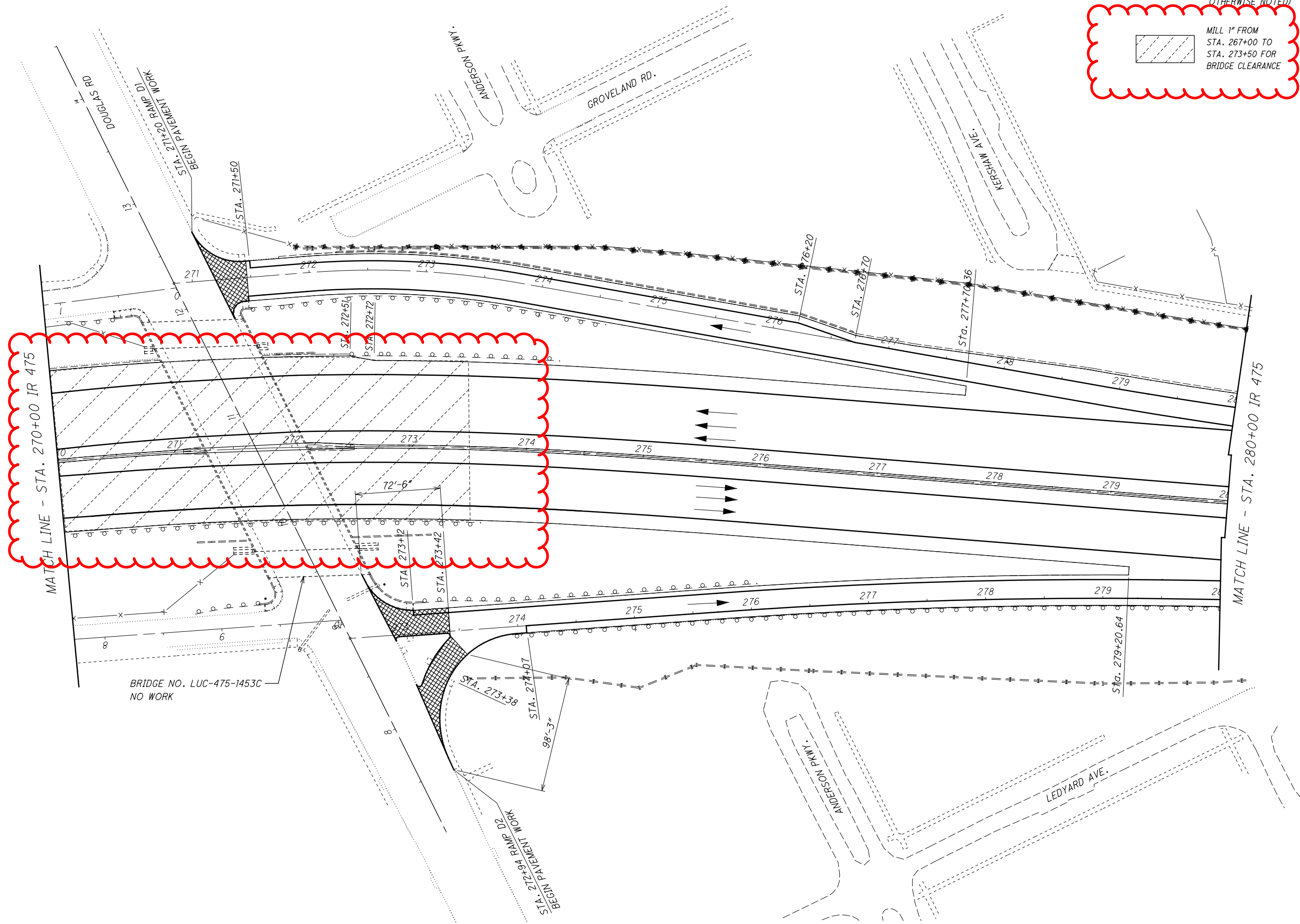
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HORIZONTAL  
SCALE IN FEET

**PLAN SHEET I-475**  
**STA. 267+00 TO STA. 270+00**

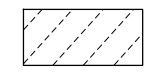
LUC-75 / 475-7.82 / 14.50

BEGIN WORK  
IR 475  
STA. 267+00  
EB & WB LANES  
SLM - 14.50





= BUTT JOINT AS PER  
STD. DWG. BP-3.1  
(25' TYP. UNLESS  
OTHERWISE NOTED)



MILL 1" FROM  
STA. 267+00 TO  
STA. 273+50 FOR  
BRIDGE CLEARANCE



0 20 40 80  
HORIZONTAL  
SCALE IN FEET

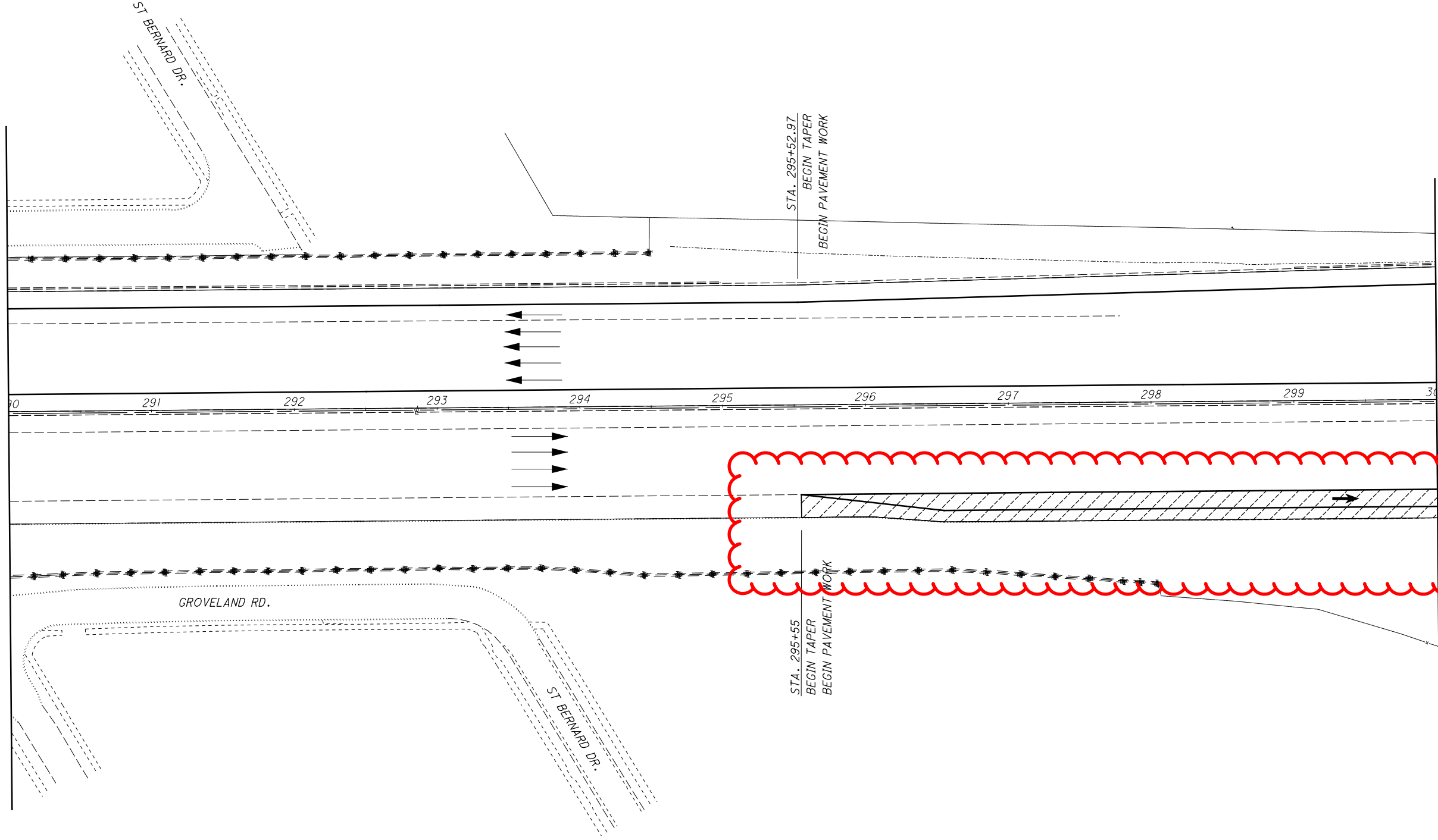
CALCULATED  
ALF  
CHECKED  
JMF

**PLAN SHEET I-475**  
**STA. 270+00 TO STA. 280+00**

**LUC-75 / 475-7.82 / 14.50**

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MATCH LINE - STA. 290+00 IR 475



ST BERNARD DR.

GROVELAND RD.

ST BERNARD DR.

STA. 295+55  
BEGIN TAPER  
BEGIN PAVEMENT WORK

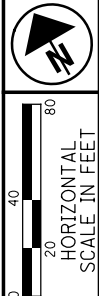
STA. 295+52.97  
BEGIN TAPER  
BEGIN PAVEMENT WORK

MATCH LINE - STA. 300+00 IR 475

= BUTT JOINT AS PER  
STD. DWG. BP-3.1  
(25' TYP. UNLESS  
OTHERWISE NOTED)

= MILL 1" FROM  
STA. 295+55 TO  
STA. 303+54

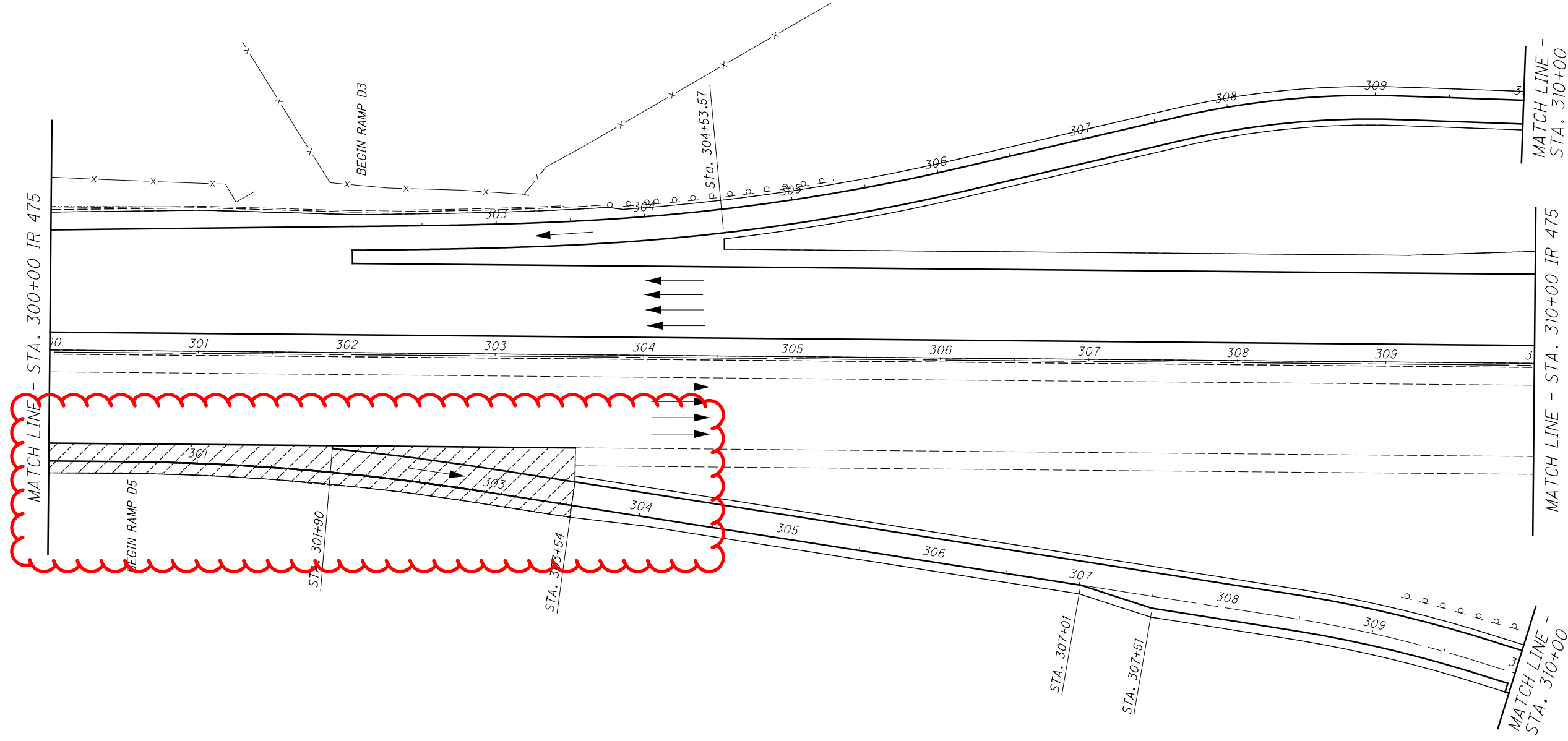
CALCULATED  
ALF  
CHECKED  
JMF





PLAN SHEET I-475  
STA. 290+00 TO STA. 300+00

LUC-75/475-7.82/14.50

75  
115



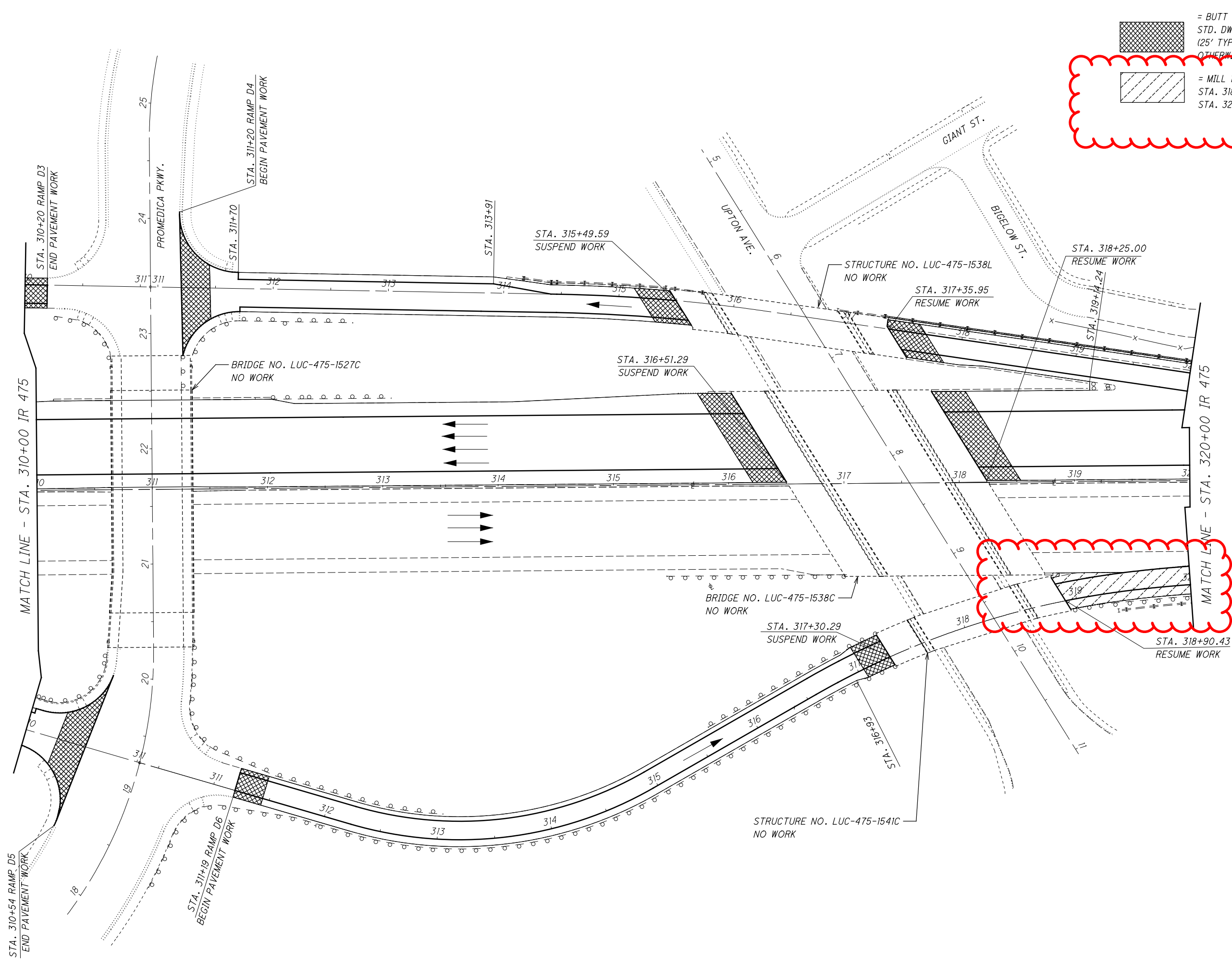
 = 1" PAVEMENT PLANING

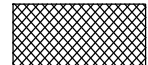
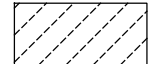
 = MILL 1" FROM STA. 295+55 TO STA. 303+54

CALCULATED ALF  
CHECKED JMF

0 0 0  
HORIZONTAL SCALE IN FEET

**PLAN SHEET I-475**  
**STA. 300+00 TO STA. 310+00**



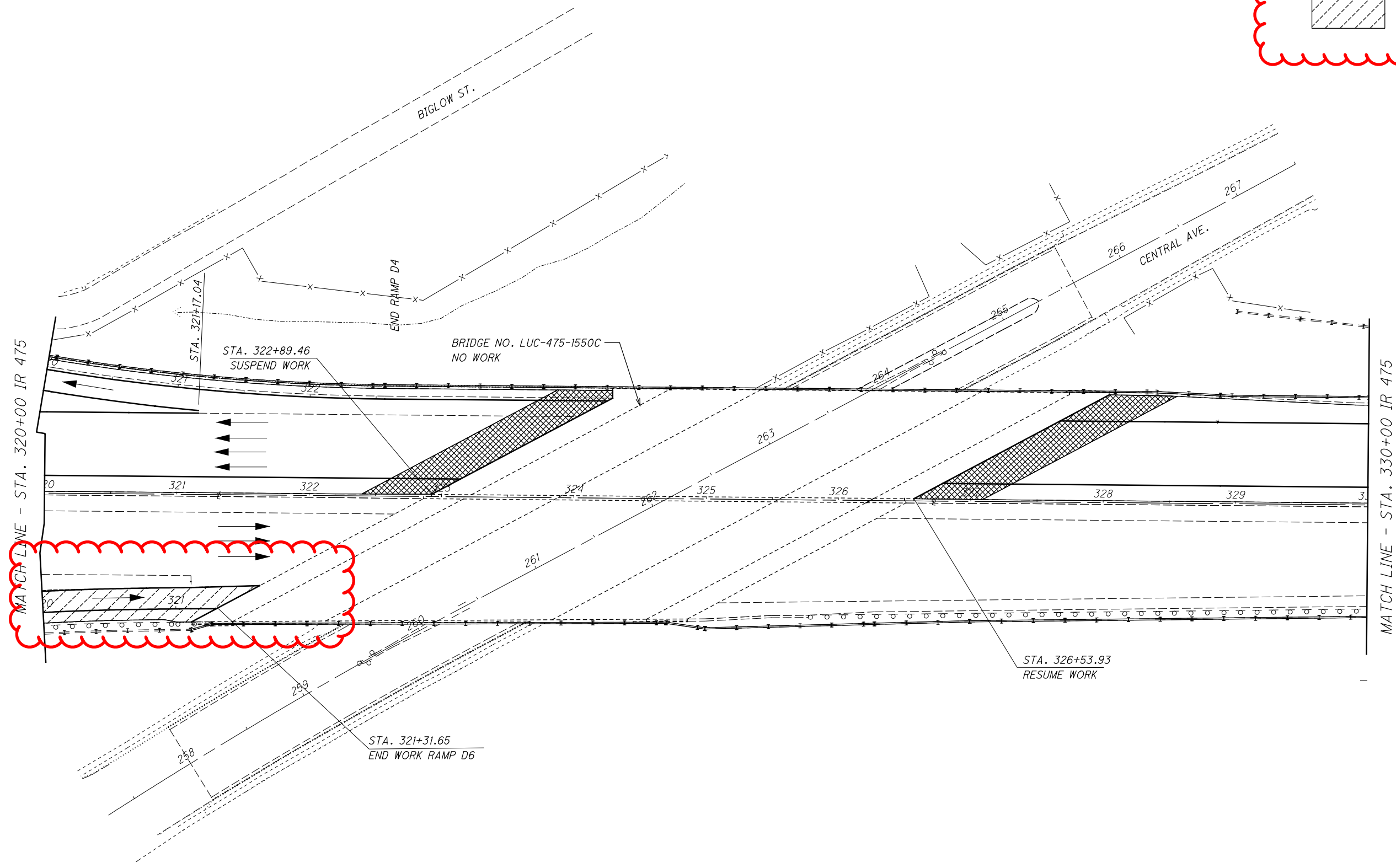
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 = MILL 1" FROM STA. 318+90.43 TO STA. 321+31.65

CALCULATED ALF  
 CHECKED JMF  
 0 20 40 80  
 HORIZONTAL SCALE IN FEET

**PLAN SHEET I-475**  
**STA. 310+00 TO STA. 320+00**

LUC-75 / 475-7.82 / 14.50

I:\ProjectData\LUC\88563\LUC-75-8.42\Design\Roadway\Sheets\88563\_GP01.dgn Sheet 3/3/2021 10:20:36 AM afintel



= BUTT JOINT AS PER  
STD. DWG. BP-3.1  
(25' TYP. UNLESS  
OTHERWISE NOTED)

= MILL 1" FROM  
STA. 318+90.43 TO  
STA. 321+31.65

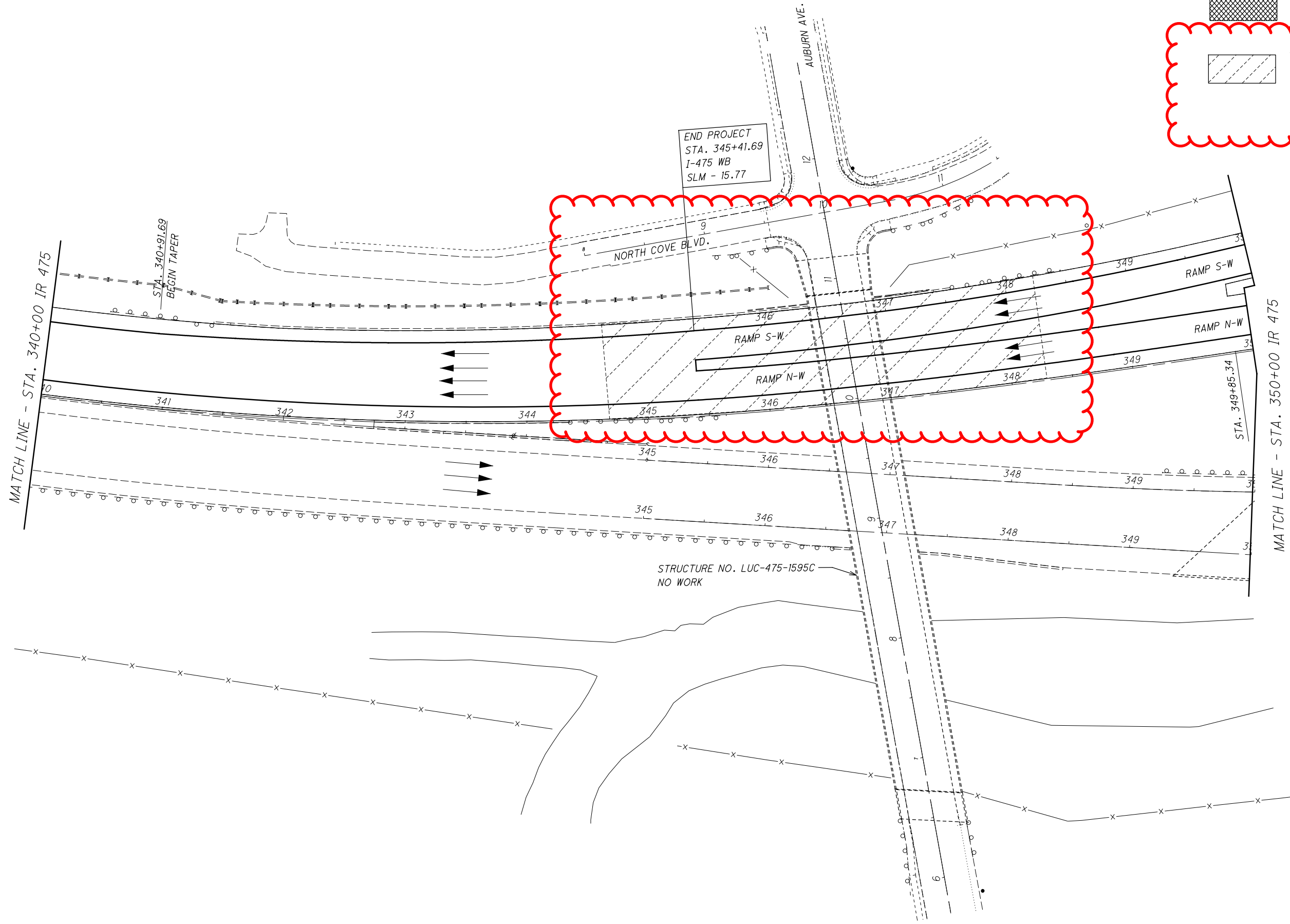
CALCULATED  
ALF  
CHECKED  
JMF

0 20 40 80  
HORIZONTAL  
SCALE IN FEET

PLAN SHEET I-475  
STA. 320+00 TO STA. 330+00

LUC-75 / 475-7.82 / 14.50

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END PROJECT  
 STA. 345+41.69  
 I-475 WB  
 SLM - 15.77

STA. 340+91.69  
 BEGIN TAPER

NORTH COVE BLVD.

AUBURN AVE.

RAMP S-W

RAMP N-W

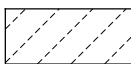
STRUCTURE NO. LUC-475-1595C  
 NO WORK

MATCH LINE - STA. 340+00 IR 475

MATCH LINE - STA. 350+00 IR 475



= BUTT JOINT AS PER  
 STD. DWG. BP-3.1  
 (25' TYP. UNLESS  
 OTHERWISE NOTED)



MILL 1" FROM  
 STA. 344+75 TO  
 STA. 348+25 FOR  
 BRIDGE CLEARANCE



0 20 40 80  
 HORIZONTAL  
 SCALE IN FEET

CALCULATED ALF  
 CHECKED JMF

PLAN SHEET I-475  
 LUC-75 / 475-7.82 / 14.50

LUC-75 / 475-7.82 / 14.50

80  
 115