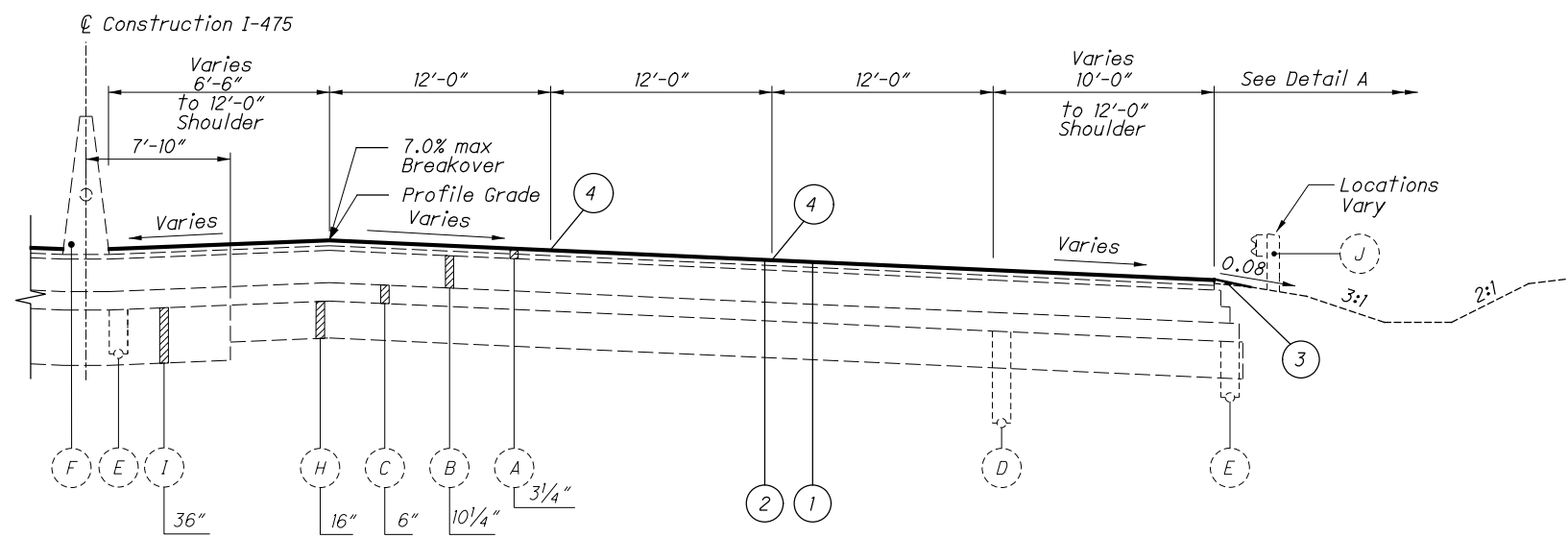
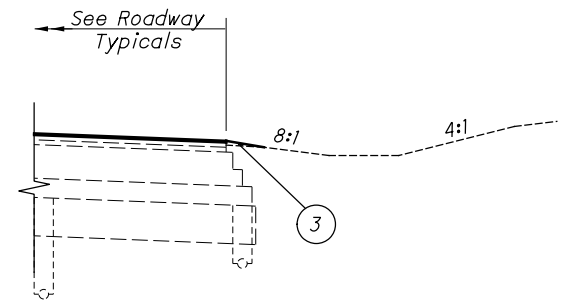


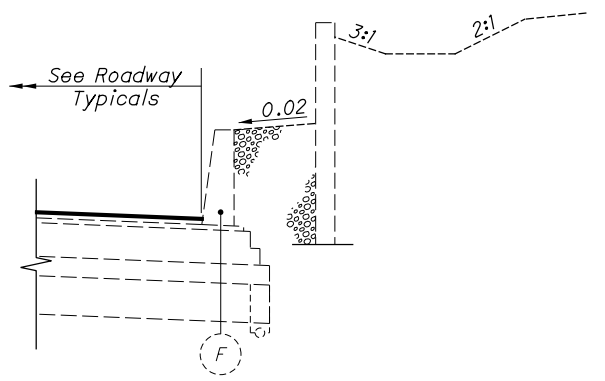
**SHOULDER SECTION - EASTBOUND I.R. 475**  
 I-475 Sta. 258+30.00 to Sta. 267+00.00 (Median Shoulder) = 870.00 FT.  
 I-475 Sta. 262+50.00 to Sta. 267+00.00 (Outside Shoulder) = 450.00 FT.



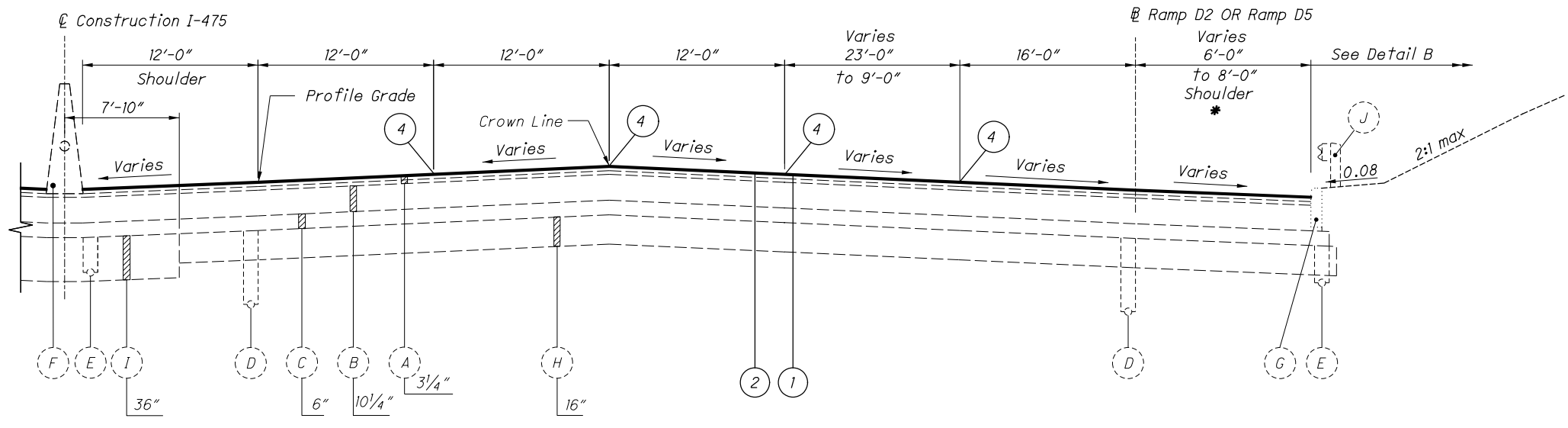
**TYPICAL SECTION - E**  
**SUPERELEVATED SECTION - EASTBOUND I-475**  
 I-475 Sta. 267+00.00 to Sta. 279+20.64 = 1220.64 FT.



**DETAIL A**  
 Applies From Sta. 273+63.14 to Sta. 277+70.36



**DETAIL B**  
 Applies From Sta. 281+59.57 to Sta. 282+68.55

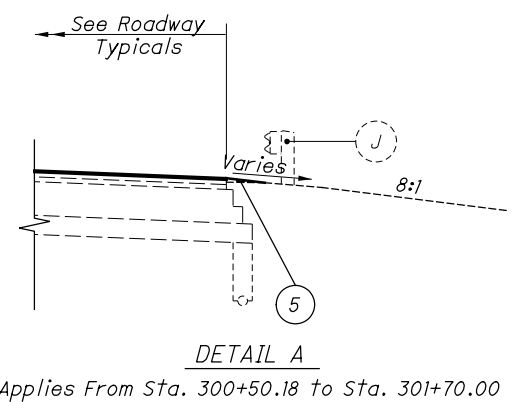


**TYPICAL SECTION - F**  
**NORMAL SECTION - EASTBOUND I-475**  
 I-475 Sta. 279+20.64 to Sta. 286+96.00 = 775.36 FT.  
 I-475 Sta. 295+55 to Sta. 300+50.18 = 495.18 FT.

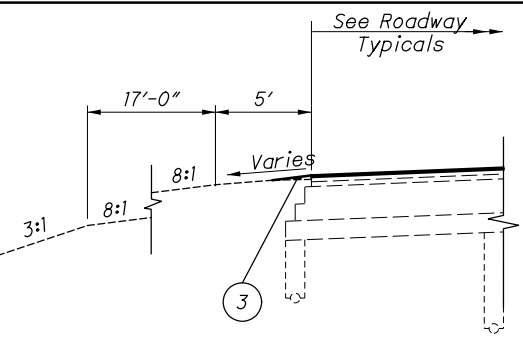
\* NOTE: STA. 295+55 TO STA. 300+50.18  
 PAVEMENT PLANING ASPHALT CONCRETE, 1"  
 (I-475 ADJACENT PAVEMENT AREA ONLY)

NOTE: SEE SHEET NO. 7 FOR PAVEMENT LEGEND.

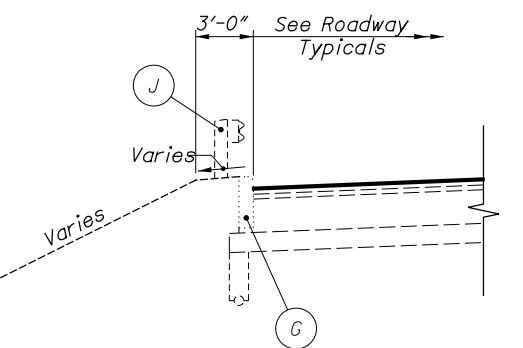
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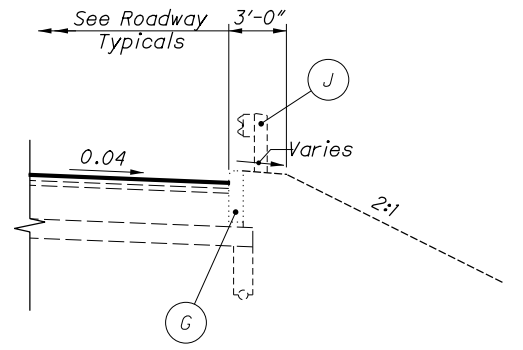
**DETAIL A**  
Applies From Sta. 300+50.18 to Sta. 301+70.00



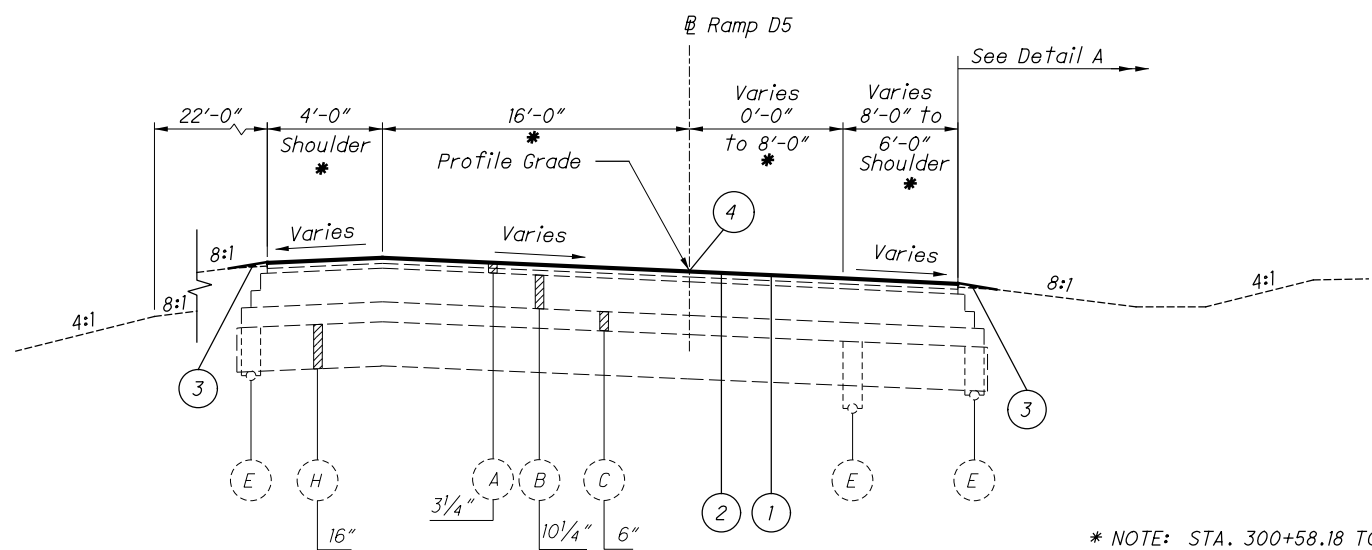
**DETAIL B**  
Applies From Sta. 313+04.25 to Sta. 315+63.37



**DETAIL C**  
Applies From Sta. 317+02.14 to Sta. 317+30.29

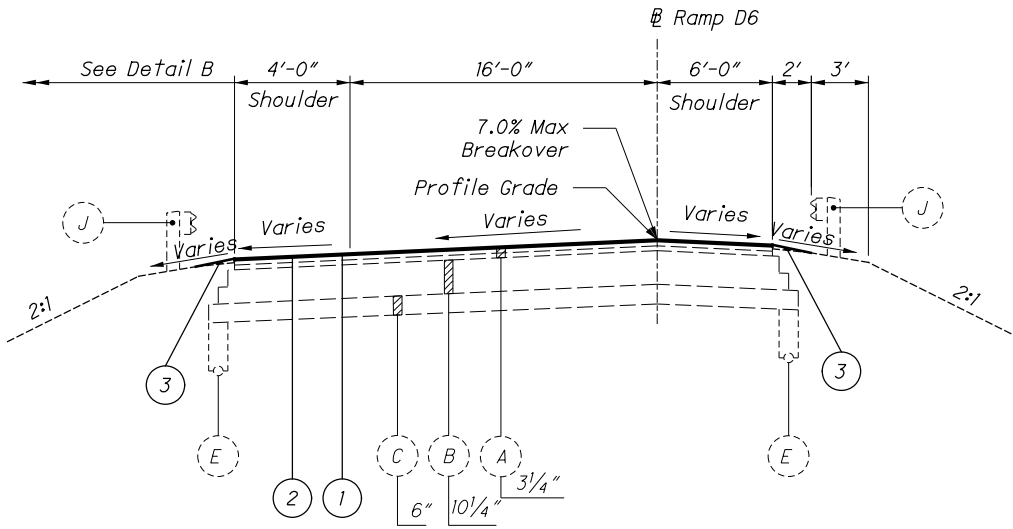


**DETAIL D**  
Applies From Sta. 317+05.11 to Sta. 317+30.29

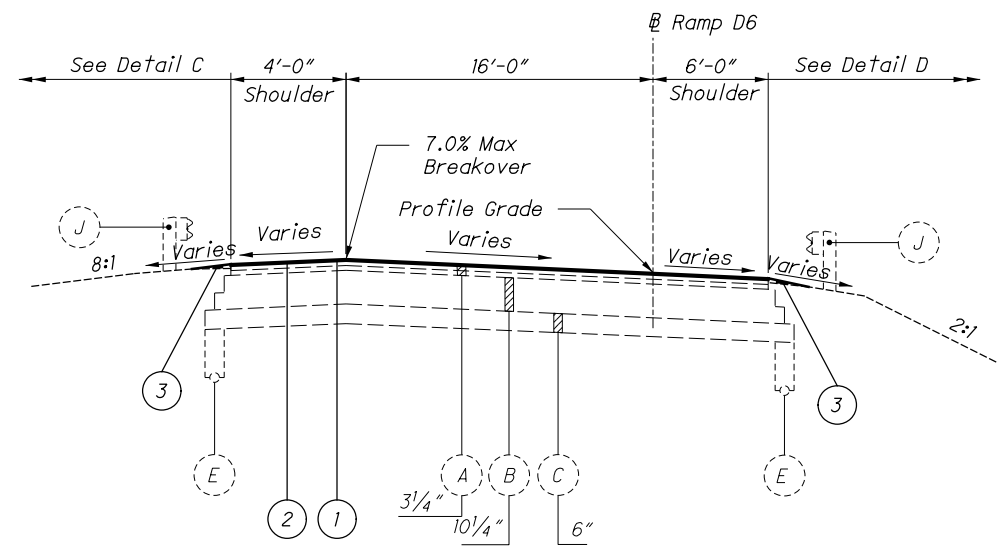


**TYPICAL SECTION - O**  
NORMAL SECTION - Ramp D5  
Ramp D5 Sta. 300+50.18 to Sta. 310+54.00 = 1003.82 FT.

\* NOTE: STA. 300+58.18 TO STA. 303+54  
PAVEMENT PLANING ASPHALT CONCRETE, 1"  
(I-475 ADJACENT PAVEMENT AREA ONLY)



**TYPICAL SECTION - P**  
SUPERELEVATED SECTION - Ramp D6  
Ramp D6 Sta. 311+19.00 to Sta. 315+63.37 = 444.37 FT.



**TYPICAL SECTION - Q**  
SUPERELEVATED SECTION - Ramp D6  
Ramp D6 Sta. 315+63.37 to Sta. 317+30.29 = 166.92 FT.  
Sta. 318+90.43 to Sta. 321+31.65 = 241.22 FT.

NOTE: SEE SHEET NO. 7 FOR PAVEMENT LEGEND.

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**MEDIAN BARRIER REPLACEMENT**

THIS ITEM WILL INCLUDE THE REMOVAL AND REPLACEMENT OF VARIOUS LOCATIONS OF MEDIAN BARRIER INCLUDED IN THIS PLAN. THE FOLLOWING QUANTITIES WILL BE INCLUDED FOR MEDIAN BARRIER REMOVAL AND REPLACEMENT:

ITEM 622 - CONCRETE BARRIER, TYPE D, AS PER PLAN 93 FT.

IT IS THE INTENT TO FOLLOW THE PLAN INSERT SHEET INCLUDED IN THIS PLAN FOR CONCRETE BARRIER WALL INSTALLATION. PLACEMENT OF MEDIAN BARRIER WALL SHALL IMMEDIATELY FOLLOW THE REMOVAL.

**AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS**

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED A HEIGHT OF 145 FT. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA), AND THE ODOT OFFICE OF AVIATION, WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL COORDINATION IS MET AND DOCUMENTATION HAS BEEN FURNISHED TO THE PROJECT ENGINEER. IF COORDINATION IS NOT OBTAINED, THEN THE PROJECT ENGINEER WILL HAVE THE AUTHORITY TO PROVIDE RESTRICTIONS AS REQUIRED.

THE TOLEDO HOSPITAL  
PROMEDICA HEALTH SYSTEM  
2142 N. COVE BLVD.  
TOLEDO, OHIO 43606  
419.291.4000

**ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (447), AS PER PLAN**

ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5MM TYPE A (447), AS PER PLAN SHALL FOLLOW THE SPECIFICATIONS FOR THE 442 ITEM EXCEPT FOR SECTION 442.04 ASPHALT BINDER, THE BINDER SHALL BE PG76-22M FOR THE SURFACE COURSE AND A MAXIMUM OF 10% OF RAP BY DRY WEIGHT OF MIX CAN BE USED. ADDITIONALLY, THE COARSE AGGREGATE FOR THIS ITEM SHALL CONTAIN A MINIMUM OF 50% AIR COOLED BLAST FURNACE SLAG, STEEL SLAG, OR CRUSHED IGNEOUS ROCK.

**ITEM 442, ASPHALT CONCRETE INTERMEDIATE COURSE, AS PER PLAN**

ITEM 442, ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE A (446), AS PER PLAN SHALL FOLLOW THE SPECIFICATIONS FOR THE 442 ITEM EXCEPT FOR SECTION 442.04 ASPHALT BINDER. THE BINDER SHALL BE PG76-22M FOR THE INTERMEDIATE COURSE AND A MAXIMUM OF 20% OF RAP BY DRY WEIGHT OF MIX CAN BE USED.

**ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN**

PER CMS 424.08, 448 DENSITY APPLIES TO THIS PROJECT. DENSITY WILL BE TESTED ACCORDING TO SUPPLEMENT 1055 PER CMS 448.02. THE DISINCENTIVE PORTION OF S-1055 (TABLE 1055.01-1 AND TABLE 1055.04) WILL BE WAIVED PROVIDING THAT THE CONTRACTOR MAKES EVERY EFFORT TO OBTAIN DENSITY AND DOES NOT USE VIBRATORY ROLLERS.

AN ESTIMATED QUANTITY OF 100 CY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO ACCOUNT FOR SURFACE IRREGULARITIES.

**PLANED SURFACES**

NO PLANED SURFACES SHALL BE OPEN TO THE PUBLIC FOR MORE THAN 5 DAYS. IF THE PLANED SURFACE IS OPEN FOR MORE THAN 5 DAYS, THEN IT IS THE CONTRACTOR'S RESPONSIBILITY TO REPAIR THE PAVEMENT FAILURES THAT OCCURRED AFTER THE 5 DAYS.

**RUMBLE STRIPE REMOVAL BEFORE PAVING**

RUMBLE STRIPS WILL BE PLANED WITH ITEM 441, FOR THE I-475 LOCATION. THE QUANTITIES FOR PLANING AND PAVING THE RUMBLE STRIPS ARE PROVIDED BELOW. QUANTITIES ARE BASED ON 2' WIDE MILL. QUANTITIES TO BE CARRIED TO THE GENERAL SUMMARY.

I 475 EB EXISTING LENGTH OF RUMBLE STRIP:  
LENGTH = 1346 FT (x2) = 2692 FT

I 475 WB EXISTING LENGTH OF RUMBLE STRIP:  
LENGTH = 7192 FT (x2) = 14384 FT

RAMP S-W TO I 475 WB EXISTING LENGTH OF RUMBLE STRIP:  
LENGTH = 900 FT (x2) = 1800 FT

RAMP N-W TO I 475 WB EXISTING LENGTH OF RUMBLE STRIP:  
LENGTH = 775 FT (x2) = 1550 FT

TOTAL 22026 FT

**ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2 "**

I 475 EB 598 SY  
I 475 WB 3197 SY  
RAMP S-W TO I 475 WB 400 SY  
RAMP N-W TO I 475 WB 345 SY  
ITEM 254 TOTAL = 4540 SY

**ITEM 407 - NON TRACKING TACK COAT**

I 475 EB 51 GAL  
I 475 WB 272 GAL  
RAMP S-W TO I 475 WB 34 GAL  
RAMP N-W TO I 475 WB 30 GAL  
ITEM 407 TOTAL = 387 GAL

**ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448),**

PG64-22, 1 1/2"  
I 475 EB 25 CY  
I 475 WB 133 CY  
RAMP S-W TO I 475 WB 17 CY  
RAMP N-W TO I 475 WB 15 CY  
ITEM 441 TOTAL = 190 CY

**ITEM 255, FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, MISC.: CLASS QC 3**

MATERIALS: PROVIDE MATERIALS CONFORMING TO THE FOLLOWING REQUIREMENTS:

PORTLAND CEMENT CONCRETE  
499.03, CLASS QC 3, W/MACRO-FIBERS\*

QC3: FLEXURAL STRENGTH: 300 PSI IN 12 HOURS

PERMEABILITY: 2000 COULOMBS

MACROFIBERS: MINIMUM 4.0 LB/CY

COARSE AGGREGATE (NO. 57 & NO.8) 703.02 & 703.13  
FINE AGGREGATE (NATURAL SAND) 703.02  
PORTLAND CEMENT, TYPE I 701.04  
FLY ASH OR NATURAL POZZOLAN 701.13  
SLAG CEMENT 701.09  
WATER 499.02  
CHEMICAL ADMIXTURE 705.12  
AIR-ENTRAINING ADMIXTURE 705.10  
MACRO-FIBERS FOR CONCRETE 705.29  
LIQUID MEMBRANE-FORMING COMPOUNDS FOR CONCRETE CURING 705.07

\*USE A MINIMUM DOSAGE RATE OF FIBERS OF 4.0 LB/YD<sup>3</sup> OF CONCRETE. ENSURE THE FINAL PROPOSED MIX IS WORKABLE AND ABLE TO BE PRODUCED SUCH THAT BALLING OR CLUMPING OF THE FIBERS IS NOT A PROBLEM AS DETERMINED BY THE ENGINEER. A DEMONSTRATION OF THE MIX PRODUCTION, OR TRIAL MIX, MAY BE REQUIRED BY THE ENGINEER PRIOR TO PLACING ANY OF THE MIX ON THE PROJECT.

PAVEMENT SHALL BE PLANED BEFORE PAVEMENT IS REMOVED. THE FOLLOWING ESTIMATED QUANTITY IS TO BE USED FOR PAVEMENT REMOVAL AND RIGID REPLACEMENT AS DIRECTED BY THE ENGINEER. THE CONTRACTOR MAY BRING THE CONCRETE TO THE MILLED SURFACE.

ITEM 255 - FULL DEPTH PAVEMENT REPAIR AND RIGID REPLACEMENT, MISC.: CLASS QC 3  
60-6x12 JOINTS NB = 480 SQ. YD.  
75-6x12 JOINTS SB = 600 SQ. YD.  
TOTAL = 1080 SQ. YD.

ITEM 255 - FULL DEPTH PAVEMENT SAWING  
2160 LF (NB)  
2700 FL (SB)  
TOTAL = 4860 LF

QUANTITIES CARRIED TO THE GENERAL SUMMARY.

**LONGITUDINAL JOINTS (FLEXIBLE PAVEMENT)**

LOCATE LONGITUDINAL JOINTS IN THE SURFACE COURSE SUBJECT TO THE FOLLOWING REQUIREMENTS:

- PLACE THE MAINLINE PAVEMENT SURFACE COURSE WITH A SINGLE COLD LONGITUDINAL JOINT LOCATED AT THE LANE LINES. NO OTHER COLD JOINTS ARE PERMITTED IN THE SURFACE COURSE OF THE MAINLINE PAVEMENT.

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GENERAL NOTES

LUC-75 / 475-7.82 / 14.50

**ITEM 614, MAINTAINING TRAFFIC**

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	FOURTH OF JULY
NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING
EASTER	

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR EVENT      TIME ALL LANES MUST BE OPEN TO TRAFFIC

SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

PLEASE REFER TO THE SEQUENCE OF CONSTRUCTION LOCATED ON MOT NOTE SHEET 23 AND THE MAINTENANCE OF TRAFFIC DETAIL SHEETS INCLUDED IN THIS PLAN FOR LANE CLOSURES AND RESTRICTIONS OF I-75 AND I-475.

**WORK WITHIN CONSTRUCTION ZONE**

THE CONTRACTOR MUST REMOVE ALL EQUIPMENT WHEN WORK IS NOT BEING PERFORMED OR THE CONTRACTOR MUST SET UP AN APPROVED MOT PLAN THAT INCLUDES A STAGING AREA WITH A MOT TRUCK AND APPROPRIATE IMPACT ATTENUATOR.

**CONCRETE MEDIAN BARRIER REPLACEMENT**

REMOVING, GRADING AND INSTALLING THE REPLACEMENT BARRIER IN A CONTINUOUS OPERATION SHALL BE LIMITED TO 50 LINEAR FEET AND SHALL AT ALL TIMES BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THE ENGINEER SHALL BE SATISFIED THAT ALL INSTALLATIONS WILL AFFORD MAXIMUM PROTECTION FOR TRAFFIC.

**WORK ZONE MARKINGS AND SIGNS**

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

ITEM 614 - WORK ZONE MARKING SIGN	60 EACH
ITEM 614 - WORK ZONE SPEED LIMIT SIGN	12 EACH
ITEM 614 - WORK ZONE LANE LINE, CLASS I, 6", 642 PAINT	30 MILE
ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT	31 MILE
ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT	7768 FT
ITEM 614 - WORK ZONE DOTTED LINE, CLASS III, 6", 642 PAINT	6573 FT
ITEM 614 - WORK ZONE STOP LINE, CLASS III, 642 PAINT	203 FT
ITEM 614 - WORK ZONE LANE LINE, CLASS I, 6", 807 PAINT	31 MILE
ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 6", 807 PAINT	32 MILE
ITEM 614 - WORK ZONE LANE LINE, CLASS I, 6", 740.06, TYPE 1	2 MILE
ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 12", 740.06, TYPE 1	5800 FT

ONLY CLASS I WORK ZONE PAVEMENT MARKINGS ON INTERSTATES AND MULTILANE HIGHWAYS EXPECTED TO BE IN PLACE LONGER THAN 14 DAYS WILL BE WET REFLECTIVE. WET REFLECTIVE WORK ZONE MARKINGS WILL NOT BE RECESSED. DUE TO CONTRACTORS MEANS AND METHODS, NOT KNOWING IF TEMPORARY MARKINGS WOULD BE IN PLACE LONGER THAN 14 DAYS, BOTH QUANTITIES WERE INCLUDED WITH THE CONTRACT.

**ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)**

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. [AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.]

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE		
ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO
		PUBLIC
RAMP & ROAD CLOSURES	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	>= 12 HRS & < 7 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HRS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS PHONE NUMBER 419-373-4428.

**ITEM 614, REPLACEMENT DRUM**

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 30 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

**ITEM 614, REPLACEMENT SIGN**

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ECT.

AN ESTIMATED QUANTITY OF 5 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

**WORK ZONE INCREASED PENALTIES SIGN (R11-H5A)**

R11-H5A-48 SIGNS SHALL BE FURNISHED, ERECTED, AND MAINTAINED IN GOOD CONDITION AND/OR REPLACED AS NECESSARY AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. SIGNS SHALL BE MOUNTED AT THE APPROPRIATE OFFSETS AND ELEVATIONS AS PRESCRIBED BY THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THEY SHALL BE MAINTAINED ON SUPPORTS MEETING CURRENT SAFETY CRITERIA.

THE SIGNS MAY BE ERECTED OR UNCOVERED NO MORE THAN FOUR HOURS BEFORE THE ACTUAL START OF WORK. THE SIGNS SHALL BE REMOVED OR COVERED NO LATER THAN FOUR HOURS FOLLOWING RESTORATION OF ALL LANES TO TRAFFIC WITH NO RESTRICTIONS, OR SOONER AS DIRECTED BY THE ENGINEER. TEMPORARY SIGN COVERING AND UNCOVERING DUE TO TEMPORARY LANE RESTORATIONS SHALL BE GUIDED BY THE FOUR-HOUR LIMITATIONS STATED ABOVE. SUCH LANE RESTORATIONS SHOULD BE EXPECTED TO REMAIN IN EFFECT FOR 30 OR MORE CONSECUTIVE CALENDAR DAYS, SUCH AS DURING WINTER SHUTDOWNS.

(THE SIGNS ON THE MAINLINE SHALL BE DUAL MOUNTED UNLESS NOT PHYSICALLY POSSIBLE. THE FIRST SIGN SHALL BE PLACED BETWEEN THE ROAD WORK AHEAD (W20-I) SIGN AND THE NEXT SIGN IN THE SEQUENCE. SIGNS SHALL BE ERECTED ON EACH ENTRANCE RAMP AND EVERY 2 MILES THROUGH THE CONSTRUCTION WORK LIMITS. SIGNS ON THE MAINLINE SHALL BE R11-H5A-48. SIGNS USED ON THE RAMPS SHALL BE R11-H5A-24. R11-H5A-24 SIGNS MAY BE USED IN THE MEDIAN IN LIEU OF R11-H5A-48 SIGNS IF IT IS NOT PHYSICALLY POSSIBLE TO PROVIDE R11-H5A-48 SIGNS IN THE MEDIAN.)

THE CONTRACTOR MAY USE SIGNS AND SUPPORTS IN USED, BUT GOOD, CONDITION PROVIDED THE SIGNS MEET CURRENT ODOT SPECIFICATIONS. SIGN FACES SHALL BE RETROREFLECTORIZED WITH TYPE G SHEETING COMPLYING WITH THE REQUIREMENTS OF C&MS 730.19.

WORK ZONE INCREASED PENALTIES SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS, INCLUDING THE SIGN AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION AS DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.

PAYMENT FOR ACCEPTED QUANTITIES, COMPLETE, IN PLACE WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, ERECTING, MAINTAINING, COVERING DURING SUSPENSION OF WORK, AND REMOVAL OF THE SIGN AND SUPPORT.

ITEM 614, WORK ZONE INCREASED PENALTIES SIGN 23 EACH

WORK ZONE INCREASED PENALTIES SIGNS WILL BE PLACED AT THE FOLLOWING LOCATIONS:

I-280 LANE B	2 EACH
RAMP LL	1 EACH
I-75 NB LANES	4 EACH
RAMP D (ALEXIS RD)	1 EACH
I-75 SB LANES	4 EACH
RAMP B (ALEXIS RD)	1 EACH
RAMP NN (OTTAWA RIVER)	1 EACH
I-475 EB LANES	2 EACH
RAMP D2 (DOUGLAS RD)	1 EACH
RAMP D6 (PROMEDICA PKWY)	1 EACH
I-475 WB LANES/RAMP SW	2 EACH
RAMP NW	2 EACH
RAMP D3 (PROMEDICA PKWY)	1 EACH

QUANTITY CARRIED TO THE GENERAL SUMMARY.

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**SEQUENCE OF OPERATIONS**

**CONSTRUCTION SEQUENCING FOR LUC-75.**

MILL AND FILL RUMBLE STRIPS AS NECESSARY PRIOR TO THE START OF EACH STAGE.

THE INTENT IS THAT LONGITUDINAL JOINTS SHALL BE PLACED ON THE LANE LINE, EDGE LINE, OR CENTER OF A TRAVELED LANE.

WEDGING AT TRANSVERSE AND LONGITUDINAL JOINTS SHALL BE INSTALLED PER SCD MT-101.90.

STAGE 1: LIMITED TO FOUR WEEKENDS FROM FRIDAY 9PM TO MONDAY 6AM. THE RAMP FROM I-280 NB TO I-75 NB MAY BE CLOSED. A MINIMUM OF TWO LANES ON I-75 NB MUST BE MAINTAINED AT ALL TIMES EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. ALL LANE SHIFTS AND LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETE OVERLAY OF FORWARD AND REAR APPROACH SLABS FOR STRUCTURE LUC-75-0849.

COMPLETE OVERLAY OF FORWARD AND REAR APPROACH SLABS FOR STRUCTURE LUC-75-0861.

COMPLETE DECK SEALING OF STRUCTURE LUC-75-0891.

COMPLETE PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE FOLLOWING LOCATIONS:

RAMP LL STA 19+54/RAMP A STA 351+32- RAMP LL STA 30+03/RAMP A 362+15.  
LUC-75 NB FROM STA 363+89 - STA 386+15

STAGE 2: THIS WORK SHALL BE LIMITED TO 10 DAYS. I-75 NB SHALL REMAIN THREE LANES FROM THE I-280 INTERCHANGE UNTIL THE LANE REDUCTION AT STA 407+16, WHERE I-75 NB CAN BE REDUCED TO TWO LANE. ADDITIONAL CLOSURES PERMITTED BY THE PLCS ARE ACCEPTABLE. I-75 SB SHALL REMAIN A MINIMUM OF TWO LANES EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. ALL LANE SHIFTS AND LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETE MEDIAN WALL REPAIR, PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE FOLLOWING LOCATIONS:

THE INSIDE PORTION AND LEFT LANE OF LUC-75 NB FROM STA 389+88.52 TO STA 448+50.

THE INSIDE PORTION AND LEFT LANE OF LUC-75 SB FROM STA 366+88.33 TO STA 448+50.

STAGE 3: THIS WORK SHALL BE LIMITED TO 10 DAYS. I-75 NB & I-75 SB SHALL REMAIN A MINIMUM OF TWO LANES IN EACH DIRECTION EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. ALL LANE SHIFTS AND LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETE PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE FOLLOWING LOCATIONS:

THE CENTER LANE, RIGHT LANE, AND OUTSIDE PORTION OF LUC-75 NB FROM STA 389+88.52 TO STA 448+50.

THE CENTER LANE, RIGHT LANE, AND OUTSIDE PORTION OF LUC-75 SB FROM 366+88.33 TO STA 448+50.

STAGE 4: THIS WORK SHALL BE LIMITED TO 21 DAYS. A MINIMUM OF TWO LANES IN EACH DIRECTION MUST BE MAINTAINED AT ALL TIMES EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. LANE SHIFTS AND LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETED THE PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE INSIDE PORTION FOR I-75 NB & SB FROM STA 450+64.22 TO STA 539+50.

STAGE 5: THIS WORK SHALL BE LIMITED TO 21 DAYS. A MINIMUM OF TWO LANES IN EACH DIRECTION MUST BE MAINTAINED AT ALL TIMES EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. LANE SHIFTS AND THE LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETE THE PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE OUTSIDE PORTION OF LUC-75 NB AND SB FROM STA. 450+64.22 TO STA. 593.50.

STAGE 6: PLACE THE FINAL SURFACE COURSE. A MINIMUM OF 3 LANE MUST BE MAINTAINED AT ALL TIMES FROM 7AM TO 7PM. THE PERMITTED LANE CLOSURE SCHEDULE SHALL BE FOLLOWED ALL OTHER TIMES.

INSTALL PERMANENT PAVEMENT MARKINGS AND ALL OTHER SAFTEY ITEMS.

**CONSTRUCTION SEQUENCING FOR LUC-475.**

STAGE 1: JOINT REPAIRS

FOR THREE (3) WEEKENDS, CONTRACTOR WILL BE ALLOWED MOT LANE REDUCTION AND CLOSURE PERIODS AS DETAILED BELOW TO PERFORM JOINT REPAIR WORK IN LIEU OF ODOT PERMITTED LANE CLOSURE TABLE TIMEFRAMES ALL WORK NOT COMPLETED DURING THESE 3 WEEKENDS WILL BE SUBJECT TO ODOT'S PERMITTED LANE CLOSURE TABLE. I 475 EXIT RAMPS TO PROMEDICA PARKWAY SHALL REMAIN OPEN AT ALL TIMES.

MILL AND FILL RUMBLE STRIPS PRIOR TO PERFORMING JOINT REPAIR WORK. (TO BE PREFORMED PRIOR TO THE START OF THESE 3 WEEKENDS)

WEEKEND 1: WB REPAIRS ON I-75/I-475 WB SYSTEM RAMPS, UP TO STATION 344+00 +/-.

-CLOSE THE SYSTEM RAMP FROM I-75 NB TO I-475 WB FOR HALF OF THE WEEKEND TO COMPLETE REPAIRS ALONG THE RAMP. REOPEN I75 NB TO I-475 WB SYSTEM RAMP TO WB 475, PRIOR TO PERFORMING 2ND HALF OF WEEKEND WORK.  
-CLOSE THE SYSTEM RAMP FROM I-75 SB TO I-475 WB FOR WTHE REMAINING HALF OF THE WEEKEND TO COMPLETE REPAIRS ALONG THE RAMP. (THIS RAMP CLOSURE MUST START BETWEEN THE HOURS OF 9PM AND 6AM.)

WEEKEND 2: REPAIRS ON WB I-475 MAINLINE

-CLOSE THE SYSTEM RAMP FROM I-75 NB TO I-475 WB.  
-PERFORM THE PAVEMENT REPAIRS (STATION 344+00+/- TO WEST END OF PROJECT). MAINTAIN MINIMUM TWO OPEN LANES OF TRAFFIC BY USE OF RIGHT SHOULDER AND RIGHT LANE ON I-475 WB. CONTRACTOR MAY REMOVE RIGHT LANE EDGE LINE UP TO 48 HOURS PRIOR TO THE START OF WEEKEND WORK. INSTALL NO EDGE LINE SIGNS. PLACE DRUMS IN CLOSED SHOULDER AREA. PLACE TEMP LANE LINE BETWEEN SHOULDER AND RIGHT LANE, PRIOR TO SHIFTING TRAFFIC TO SHOULDER FOR WEEKEND WORK. AT CONCLUSION OF WEEKEND WORK, RE-CLOSE SHOULDER WITH DRUMS. PLACE TEMPORARY EDGE LINE WITHIN

48 HOURS OF COMPLETING WEEKEND WORK.

-THE RAMP FROM PROMEDICA TO I-475 WB WILL NEED TO BE CLOSED TO REDUCE CONGESTION

WEEKEND 3: REPAIRS NEAR DOUGLAS RD OFF RAMP AND PROMEDICA ON RAMP  
-CLOSE THE RIGHT LANE OF I-475 AFTER THE PROMEDICA EXIST RAMP TO MAKE REPAIRS ON THE PROMEDICA ENTRANCE RAMP, DOUGLAS EXIST RAMP, AND RIGHT LANE OF I-475 WB.  
-I-475 EB: REDUCE TO ONE LANE (FROM WEST END OF PROJECT UP TO STATION 287+00) FOR 24 HOURS DURING ONE OF THE THREE ALLOTTED WEEKENDS TO COMPLETE THE PAVEMENT REPAIRS. ALL LANES OF EB 475 SHALL REOPEN AS SOON AS JOINT REPAIR WORK IS COMPLETED.

RAMPS AT PROMEDICA PKWY. WILL INCLUDE ONE OVERNIGHT CLOSURE FROM 9PM TO 6AM. DAMAGES WILL OCCUR AT \$2500/15 MINUTES FOR EACH 15 MINUTES THE RAMP REMAINS CLOSED AFTER THE DURATION.

ONLY ONE PROMEDICA PKWY. RAMP SHALL BE CLOSED AT A TIME. THE RAMPS ON PROMEDICA PKWY. SHALL NOT BE CLOSED AT THE SAME TIME AS THE RAMPS AT DOUGLAS RD.

PROMEDICA PKWY. EB EXIT RAMP DETOUR:  
SECOR RD. TO MONROE ST. TO PROMEDICA PKWY.

PROMEDICA PKWY. WB EXIT RAMP DETOUR:  
WB-475 TO DOUGLAS RD. TO EB-475 TO PROMEDICA PKWY.

PROMEDICA PKWY. EB ENTRANCE RAMP DETOUR:  
WB-475 TO DOUGLAS RD. TO EB-475.

PROMEDICA PKWY. WB ENTRANCE RAMP DETOUR:  
PROMEDICA PKWY. TO CENTRAL AVE. TO MONROE ST. TO SECOR RD. TO WB-475.

STAGE 2: RESURFACING

RESURFACE I-475 AND COMPLETE ALL SAFETY ITEMS.

A MINIMUM OF 3 LANES MUST BE MAINTAINED AT ALL TIMES FROM 7AM TO 7PM. A MINIMUM OF 2 LANES MUST BE MAINTAINED ON ALL SYSTEM RAMPS AND MUST BE MAINTAINED FROM 7AM TO 7PM. THE PERMITTED LANE CLOSURE SCHEDULE SHALL BE FOLLOWED ALL OTHER TIMES.

DESCRIPTION OF CRITICAL LANE/RAMP TO BE MAINTAINED	TIME UNIT	DISINCENTIVE, MONEY TIME UNIT
MAINTAIN A MINIMUM 2 LANES OF I-75	/MIN	\$100
MAINTAIN A MINIMUM OF 3 LANES OF I-75 NB DURING STAGE 2	/MIN	\$100
ALL STAGE COMPLETED WITHIN TIMEFRAME DESCRIBED IN THE CONSTRUCTION SEQUENCING	/HR	\$1000
SYSTEM RAMP CLOSURES	/15 MIN	\$1000

Ramp Closure	Maintenance of Traffic Operation	Detour
<b>Related to I-75 Work</b>		
I-280 NB to I-75 NB	May be closed up to 4 weekends according to the construction sequencing.	I-75 SB to Expressway Dr to I-75 NB
Ramp LL (Manhattan to I-75 NB)	May be closed up to 4 weekends according to the construction sequencing.	Manhattan Blvd to Phillips Ave to I-75 NB
I-75 NB to Ottawa River Rd	This ramp may be closed up to 5 nights from 9PM to 6AM.	I-75 NB to Alexis Rd to Suder Ave
Ottawa River Rd to I-75 SB	This ramp may be closed up to 5 nights from 9PM to 6AM.	Suder Ave to Alexis Rd to I-75 SB
I-75 NB to Alexis Rd	This ramp may be closed up to 5 nights from 9PM to 6AM.	I-75 NB to Erie Rd to I-75 SB
Alexis Rd to I-75 NB	This ramp may be closed up to 5 nights from 9PM to 6AM.	I-75 SB to Expressway Dr to I-75 NB
I-75 SB to Alexis Rd	This ramp may be closed up to 5 nights from 9PM to 6AM.	I-75 SB to Expressway Dr to I-75
Alexis Rd to I-75 SB	This ramp may be closed up to 5 nights from 9PM to 6AM.	I-75 NB to Erie Rd to I-75 SB
<b>Related to I-475 Work</b>		
I-75 NB to I-475 WB	May be closed up to one full weekend and up to 29 hours of an additional weekend according to the construction sequencing.	I-75 NB to Phillips Ave to I-75 SB
I-75 SB to I-475 WB	May be closed up to 29 hours of one weekend according to the construction sequencing.	I-75 SB to Detroit Ave to I-75 NB
Promedica Pkwy to I-475 WB	May be closed for two weekends according to the construction sequencing.	Promedica Pkwy to Central Ave to Monroe St to Secor Rd. to I-475 WB
I-475 WB to Douglas Rd	May be closed for one weekend according to the construction sequencing.	I-475 WB to Secor Rd to I-475 EB

CALCULATED  
ALF  
CHECKED  
JMF

**MAINTENANCE OF TRAFFIC NOTES**

**LUC-75 / 475-7.82 / 14.50**

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SHEET NUM.														PART.		ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	
15	16	18	19	19A	20	21	22	23A	58	58A	59	82A	82C	01/IMS/PV	02/IMS/BR							
<b>ROADWAY</b>																						
LUMP																LUMP	201	11000	LS		CLEARING AND GRUBBING	
										3,271						3,271	202	23500	3,271	SY	WEARING COURSE REMOVED	
											15,035.5					15,035.5	202	38000	15,035.5	FT	GUARDRAIL REMOVED	
											17					17	202	42010	17	EACH	ANCHOR ASSEMBLY REMOVED, TYPE E	
											18					18	202	42041	18	EACH	ANCHOR ASSEMBLY REMOVED, TYPE T, AS PER PLAN	15
											3					3	202	42050	3	EACH	ANCHOR ASSEMBLY REMOVED, TYPE B	
											35					35	202	47000	35	EACH	BRIDGE TERMINAL ASSEMBLY REMOVED	
																LUMP	202	98000	LS		REMOVAL MISC.: TRAFFIC MONITORING EQUIPMENT	19A
											167					167	209	15000	167	STA	RESHAPING UNDER GUARDRAIL	
									7.56	6.01						13.57	209	60200	13.57	STA	LINEAR GRADING	
											13,070					13,070	606	15050	13,070	FT	GUARDRAIL, TYPE MGS	
											20					20	606	26150	20	EACH	ANCHOR ASSEMBLY, MGS TYPE E, MASH 2016	
											18					18	606	26550	18	EACH	ANCHOR ASSEMBLY, MGS TYPE T	
											25					25	606	35002	25	EACH	MGS BRIDGE TERMINAL ASSEMBLY, TYPE 1	
											10					10	606	35102	10	EACH	MGS BRIDGE TERMINAL ASSEMBLY, TYPE 2	
2	93															93	622	24001	93	FT	CONCRETE BARRIER, TYPE D, AS PER PLAN	16
																2	623	39500	2	EACH	MONUMENT BOX ADJUSTED TO GRADE	
<b>EROSION CONTROL</b>																						
											2,904					2,904	659	10000	2,904	SY	SEEDING AND MULCHING	
											0.39					0.39	659	20000	0.39	TON	COMMERCIAL FERTILIZER	
											16					16	659	35000	16	MGAL	WATER	
																1,000	832	30000	1,000	EACH	EROSION CONTROL	
<b>DRAINAGE</b>																						
8																8	611	98630	8	EACH	CATCH BASIN ADJUSTED TO GRADE	
3																3	611	99654	3	EACH	MANHOLE ADJUSTED TO GRADE	
<b>PAVEMENT</b>																						
										241,995						241,995	254	01000	241,995	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 3 1/4"	
	4,540															4,540	254	01000	4,540	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2"	
											14,328					14,328	254	01000	14,328	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1"	
	1,080															1,080	255	10200	1,080	SY	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, MISC.:CLASS QC3	16
	4,860															4,860	255	20000	4,860	FT	FULL DEPTH PAVEMENT SAWING	
	387									33,879	8,972					43,238	407	20000	43,238	GAL	NON-TRACKING TACK COAT	
	100										2,932					3,032	424	12001	3,032	CY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN	16
	190															190	441	50000	190	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	
										15,202						15,202	442	00100	15,202	CY	ANTI-SEGREGATION EQUIPMENT	
										10,083						10,083	442	10301	10,083	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), AS PER PLAN	16
											277					12,041	442	10101	12,041	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, 19 MM, TYPE A (446), AS PER PLAN, PG72-22M	16
											693	480				1,173	617	10100	1,173	CY	COMPACTED AGGREGATE	
											2,774					2,774	SPECIAL	69012060	2,774	SY	PAVEMENT OVERLAY FABRIC COMPOSITE	19
											5,867					5,867	875	10000	5,867	LB	LONGITUDINAL JOINT ADHESIVE	
											3,330					3,330	897	01010	3,330	SY	PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A, 3"	
<b>WATER WORK</b>																						
2																2	638	10800	2	EACH	VALVE BOX ADJUSTED TO GRADE	
<b>LIGHTING</b>																						
																102	625	26253	102	EACH	LUMINAIRE, CONVENTIONAL, SOLID STATE (LED), AS PER PLAN, COOPER GALLEON OR TYPE H30	18
																1	625	34001	1	EACH	POWER SERVICE, AS PER PLAN	18
																LUMP	SPECIAL	62540000	LS		MAINTAIN EXISTING LIGHTING	18
																1	SPECIAL	62540010	1	EACH	REPLACEMENT OF EXISTING LIGHTING UNIT	18

GENERAL SUMMARY

LUC-75 / 475-7.82 / 14.50

57  
115

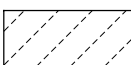
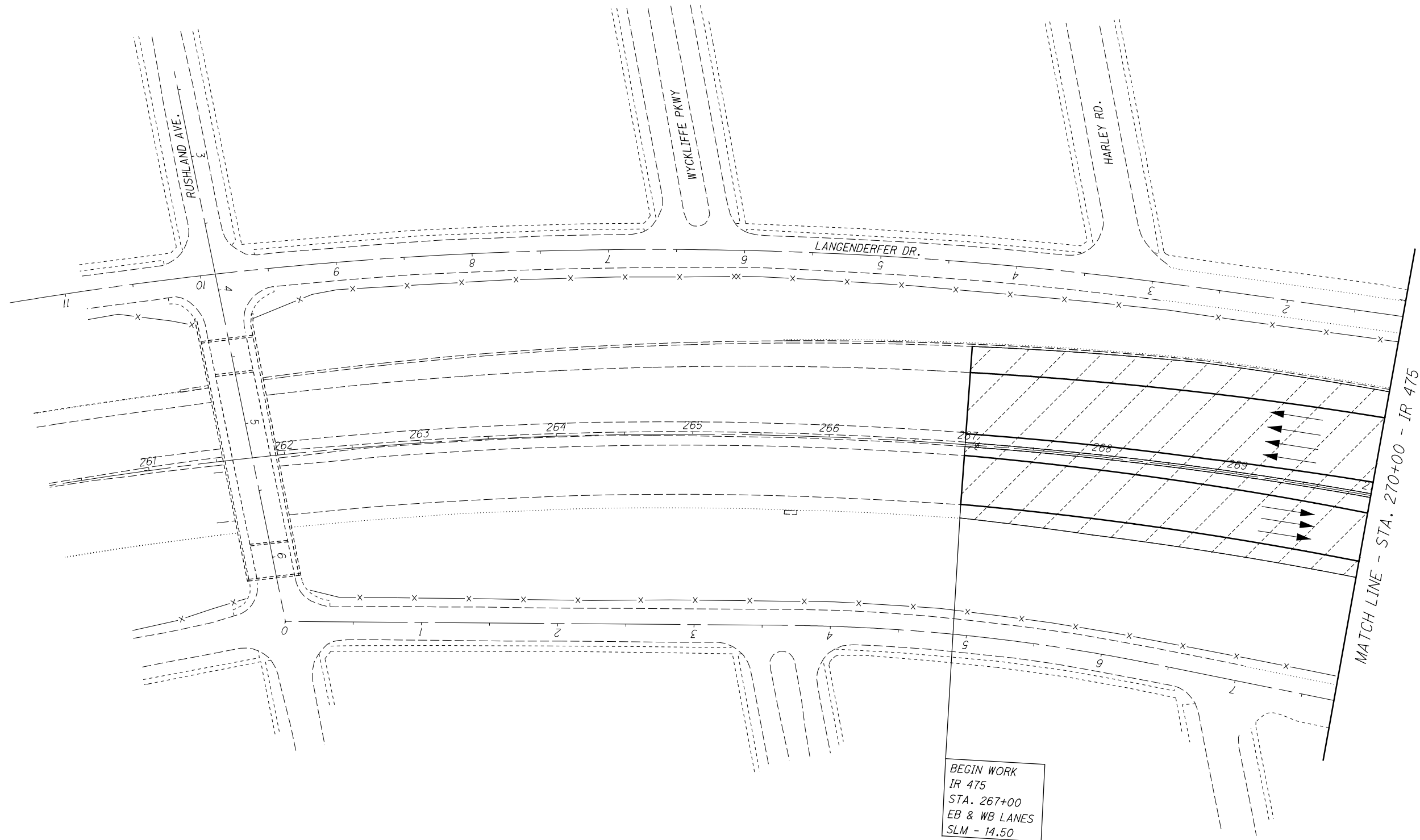
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STATION RANGE I-475			SIDE	TYPICAL SECTION	DISTANCE (D)	AVERAGE WIDTH (W)	CADD GENERATED AREA	202	209	254	407	424	617	875
WEARING COURSE REMOVED	LINEAR GRADING	PAVEMENT PLANING, ASPHALT CONCRETE, 1"						NON-TRACKING TACK COAT	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN, 1"	COMPACTED AGGREGATE	LONGITUDINAL JOINT ADHESIVE			
SY	MILE	SY	GAL	CY	CY	LB								
267+00	TO	279+20.64	EB	E	1220.64	57.3	7765		0.46	3820	660	216	45	1526
279+20.64	TO	282+68.55	EB	F	347.91	67.0	2590		0.13		220	72	13	435
282+68.55	TO	286+96	EB	F	427.45	78.5	3728	212			317	104	2	107
295+55	TO	300+50.18	EB	F	495.18	18.0	990			768	84	28	2	124
267+00	TO	272+51	WB	A	551.00	76.00	4653		0.21		395	129	20	138
272+51	TO	272+72	WB	A	21.00	74.00	173		0.01	5000	15	5	1	5
272+72	TO	277+70.36	WB	A	498.36	72.00	3987		0.19		339	111	18	125
277+70.36	TO	282+44.38	WB	B	474.02	72.00	3792		0.18		322	105	18	119
282+44.38	TO	295+52.97	WB	B	1308.59	84.00	12214		0.50		1038	339	48	327
295+52.97	TO	302+03.11	WB	B	650.14	88.50	6393				543	178		
302+03.11	TO	304+53.57	WB	B	250.46	74.00	2059		0.09		175	57	9	63
304+53.57	TO	316+51.29	WB	C	1197.72	75.00	9981	253	0.45		848	277	44	299
316+51.29	TO	319+14.24	WB	C	89.24	78.00	773	253	0.03		66	21	3	22
319+14.24	TO	321+17.04	WB	C	202.80	69.00	1555		0.08		132	43	8	51
321+17.04	TO	322+56.44	WB	C	139.40	60.00	929	462	0.05		79	26	5	35
322+56.44	TO	322+89.46	WB	C	33.02	80.00	294	464	0.01		25	8	1	8
326+53.93	TO	330+24	WB	C	370.07	80.00	3290		0.14		280	91	7	185
330+24	TO	330+66.69	WB	D	42.69	76.00	360		0.02		31	10	2	43
330+66.69	TO	340+91.69	WB	D	1025.00	72.00	8200		0.39		697	228	38	
340+91.69	TO	345+41.69	WB	D	450.00	75.50	3775		0.17	2917	321	105	17	
345+41.69	TO	349+85.34	RAMP NW	H	443.65	41.00	2021		0.17		172	56	8	222
349+85.34	TO	353+16.5	RAMP NW	H	331.16	52.25	1923	324	0.13		163	53	6	166
345+41.69	TO	349+85.34	RAMP SW	G	443.65	45.50	2243		0.17		191	62	16	111
349+85.34	TO	354+40.8	RAMP SW	G	455.46	39.25	1986	113	0.17		169	55	17	114
271+20	TO	271+50	RAMP D1	I	30.00	58.50	195	149	0.01		17	5	1	8
271+50	TO	276+20	RAMP D1	I	470.00	34.00	1776		0.18		151	49	17	118
276+20	TO	276+70	RAMP D1	I	50.00	30.00	167		0.02		14	5	2	25
276+70	TO	277+70.36	RAMP D1	I	100.36	27.00	301		0.04		26	8	4	50
277+70.36	TO	282+44.38	RAMP D1	I	474.02	24.00	1264		0.18		107	35	9	237
272+36	TO	274+07	RAMP D2	J	171.00	42.50	808	142	0.06		69	22	3	43
272+36	TO	274+07	SLIP	J	171.00	33.00	627	149	0.06		53	17	3	43
274+07	TO	279+20.64	RAMP D2	J	513.64	26.00	1484		0.19		126	41	10	128
279+20.64	TO	282+68.55	RAMP D2	J	347.91	27.00	1044		0.13		89	29	13	87
302+03.11	TO	304+53.57	RAMP D3	K	250.46	25.00	696		0.09		59	19	9	63
304+53.57	TO	310+20	RAMP D3	K	566.43	26.00	1636	73	0.21		139	45	21	142
311+20	TO	311+70	RAMP D4	L	50.00	78.25	435	196	0.02		37	12	1	13
311+70	TO	313+91	RAMP D4	L	219.00	34.00	827		0.08		70	23	4	55
314+46.05	TO	315+49.59	RAMP D4	L	103.54	34.00	391	97	0.04		33	11	2	26
313+91	TO	314+46.05	RAMP D4	L	55.05	31.00	190	108	0.02		16	5	1	14
317+35.95	TO	319+14.24	RAMP D4	M,N	178.29	28.00	555		0.07		47	15		45
319+14.24	TO	321+17.04	RAMP D4	N	202.80	35.50	800		0.08		68	22		51
321+17.04	TO	322+56.44	RAMP D4	N	139.40	23.00	356		0.05		30	10		35
									0.00		0	0		0
300+50.18	TO	301+90	RAMP D5	O	139.82	26.00	404		0.05	1113	34	11	3	35
301+90	TO	303+54	RAMP D5	O	164.00	36.50	665		0.06		57	18		41
303+54	TO	307+01	RAMP D5	O	347.00	27.00	1041		0.13		88	29	6	87
307+01	TO	307+51	RAMP D5	O	50.00	30.00	167		0.02		14	5	1	13
307+51	TO	310+00	RAMP D5	O	249.00	34.00	941		0.09		80	26	5	62
310+00	TO	310+58	RAMP D5	O	58.00	84.25	543	203	0.02		46	15	1	15
311+19	TO	316+93	RAMP D6	P,Q	574.00	26.00	1658	73	0.22		141	46	11	144
316+93	TO	317+30.29	RAMP D6	Q	37.29	28.00	116		0.01		10	3	1	9
318+90.43	TO	321+31.65	RAMP D6	Q	241.22	29.67	795		0.09	710.00	68	22	4	60
<b>01/STR/PV TOTALS CARRIED TO GENERAL SUMMARY</b>								3271	6.01	14328	8972	2932	480	5867

CALCULATED  
ALF  
CHECKED  
JMF

**PAVEMENT CALCULATIONS I - 475**

**LUC-75 / 475-7.82 / 14.50**



MILL 1" FROM  
 STA. 267+00 TO  
 STA. 273+50 FOR  
 BRIDGE CLEARANCE



0 20 40 80  
 HORIZONTAL  
 SCALE IN FEET

CALCULATED	ALF
CHECKED	JMF

**PLAN SHEET I-475**  
**STA. 267+00 TO STA. 270+00**

**LUC-75 / 475-7.82 / 14.50**

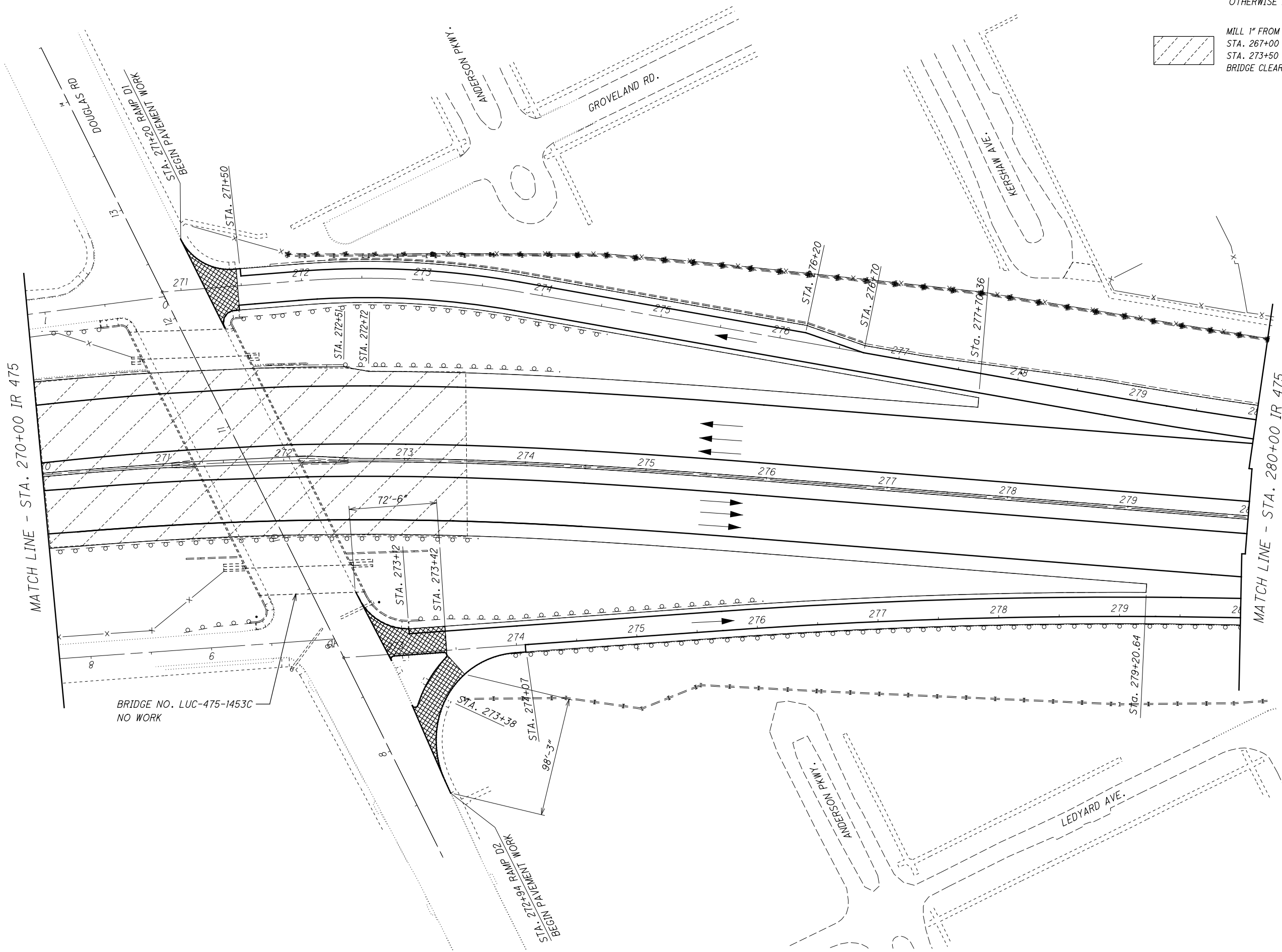


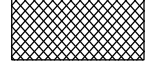
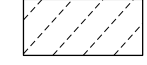
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MATCH LINE - STA. 270+00 IR 475

MATCH LINE - STA. 280+00 IR 475

BRIDGE NO. LUC-475-1453C  
NO WORK



 = BUTT JOINT AS PER STD. DWG. BP-3.1 (25' TYP. UNLESS OTHERWISE NOTED)  
 MILL 1" FROM STA. 267+00 TO STA. 273+50 FOR BRIDGE CLEARANCE

CALCULATED ALF  
 CHECKED JMF

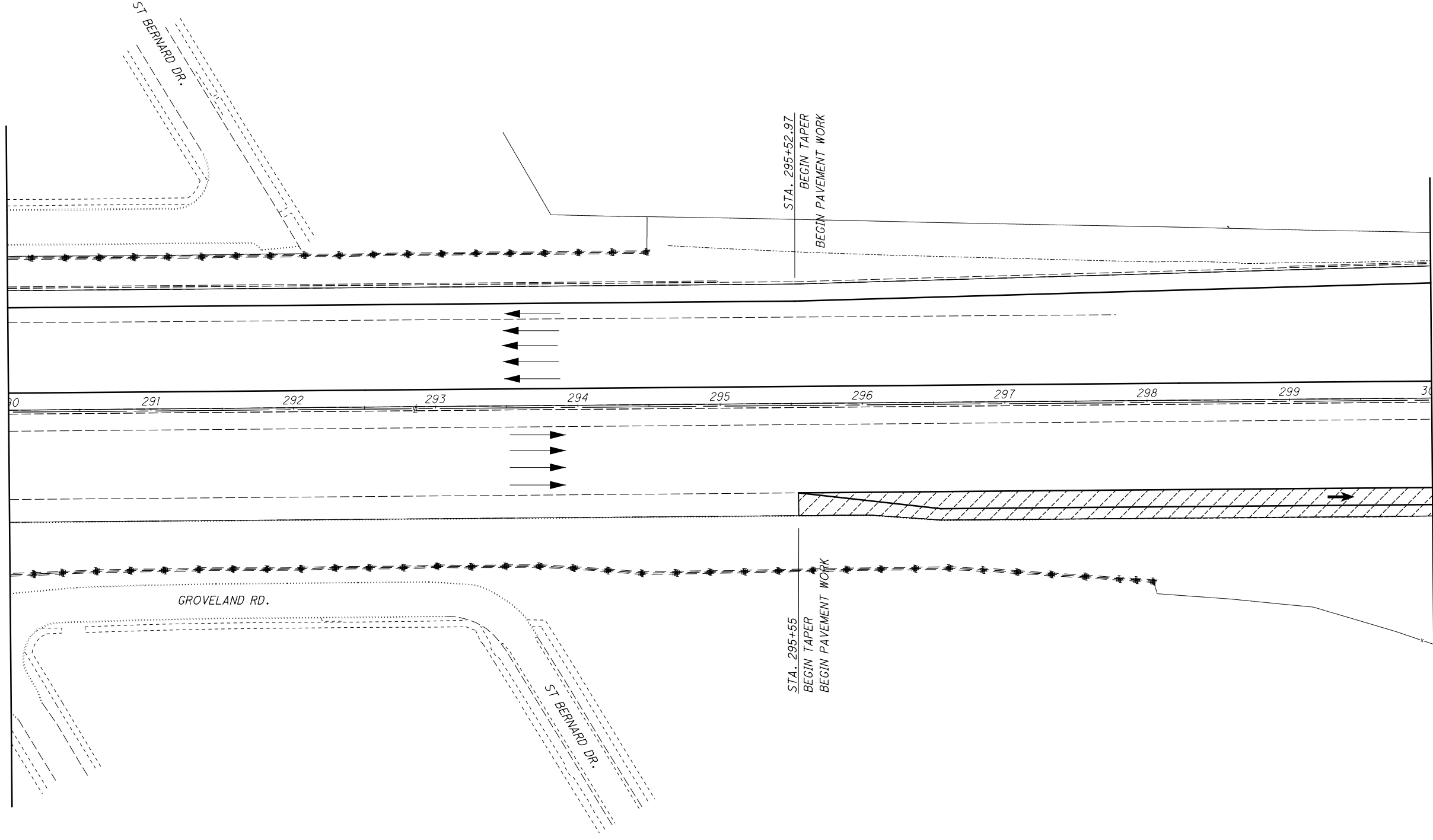
0 20 40 80  
 HORIZONTAL SCALE IN FEET

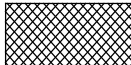
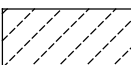
**PLAN SHEET I-475**  
**STA. 270+00 TO STA. 280+00**

**LUC-75 / 475-7.82 / 14.50**

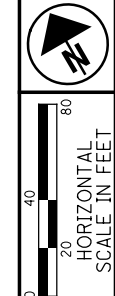
I:\ProjectData\LUC\88563\LUC-75-8-42\Design\Roadway\Sheets\88563\_GP04.dgn Sheet 3/3/2021 10:53:31 AM afintel

MATCH LINE - STA. 290+00 IR 475



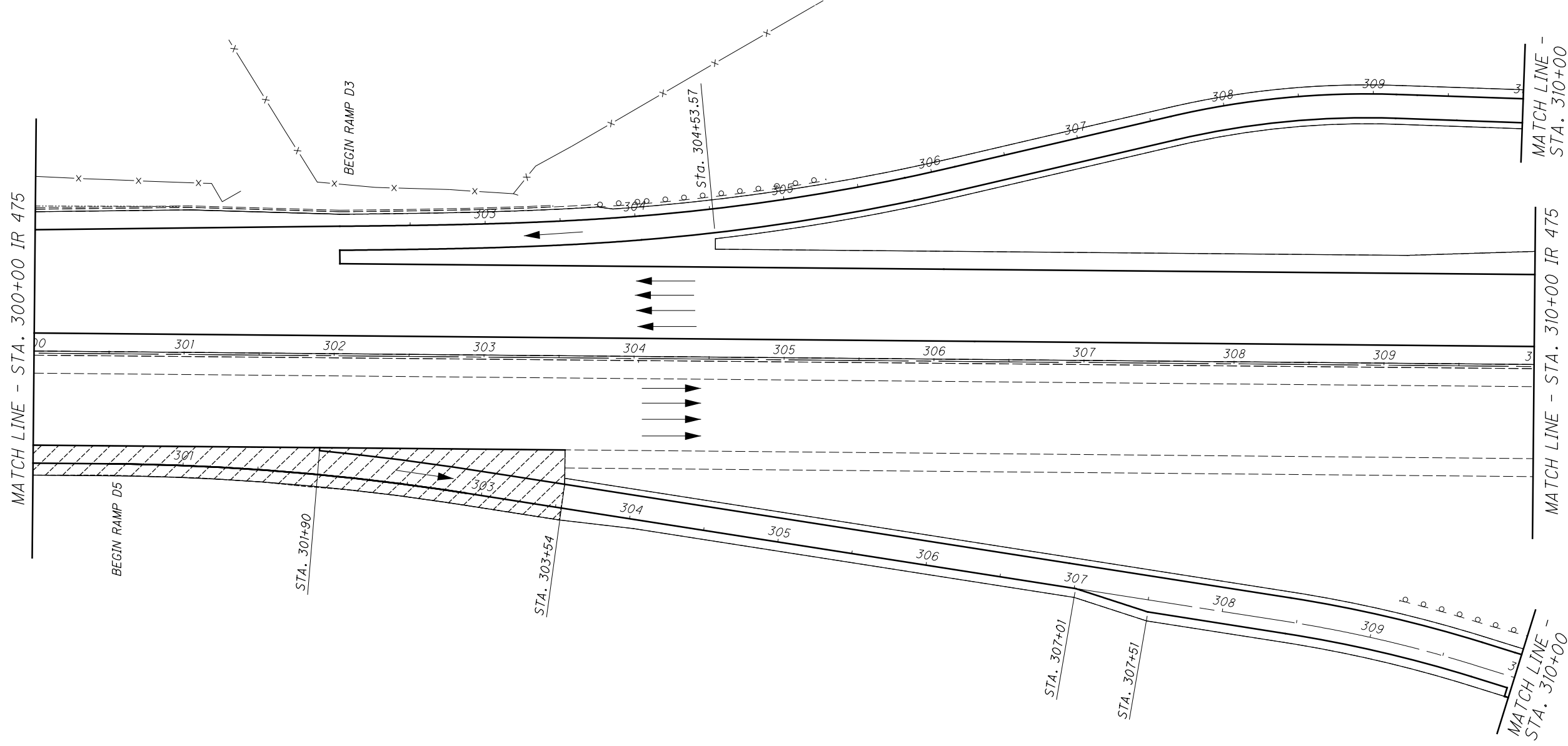
 = BUTT JOINT AS PER STD. DWG. BP-3.1 (25' TYP. UNLESS OTHERWISE NOTED)  
 = MILL 1" FROM STA. 295+55 TO STA. 303+54


CALCULATED
ALF
CHECKED
JMF




**PLAN SHEET I-475**  
**STA. 290+00 TO STA. 300+00**

**LUC-75/ 475-7.82/ 14.50**



 = 1" PAVEMENT PLANING

 = MILL 1" FROM  
STA. 295+55 TO  
STA. 303+54

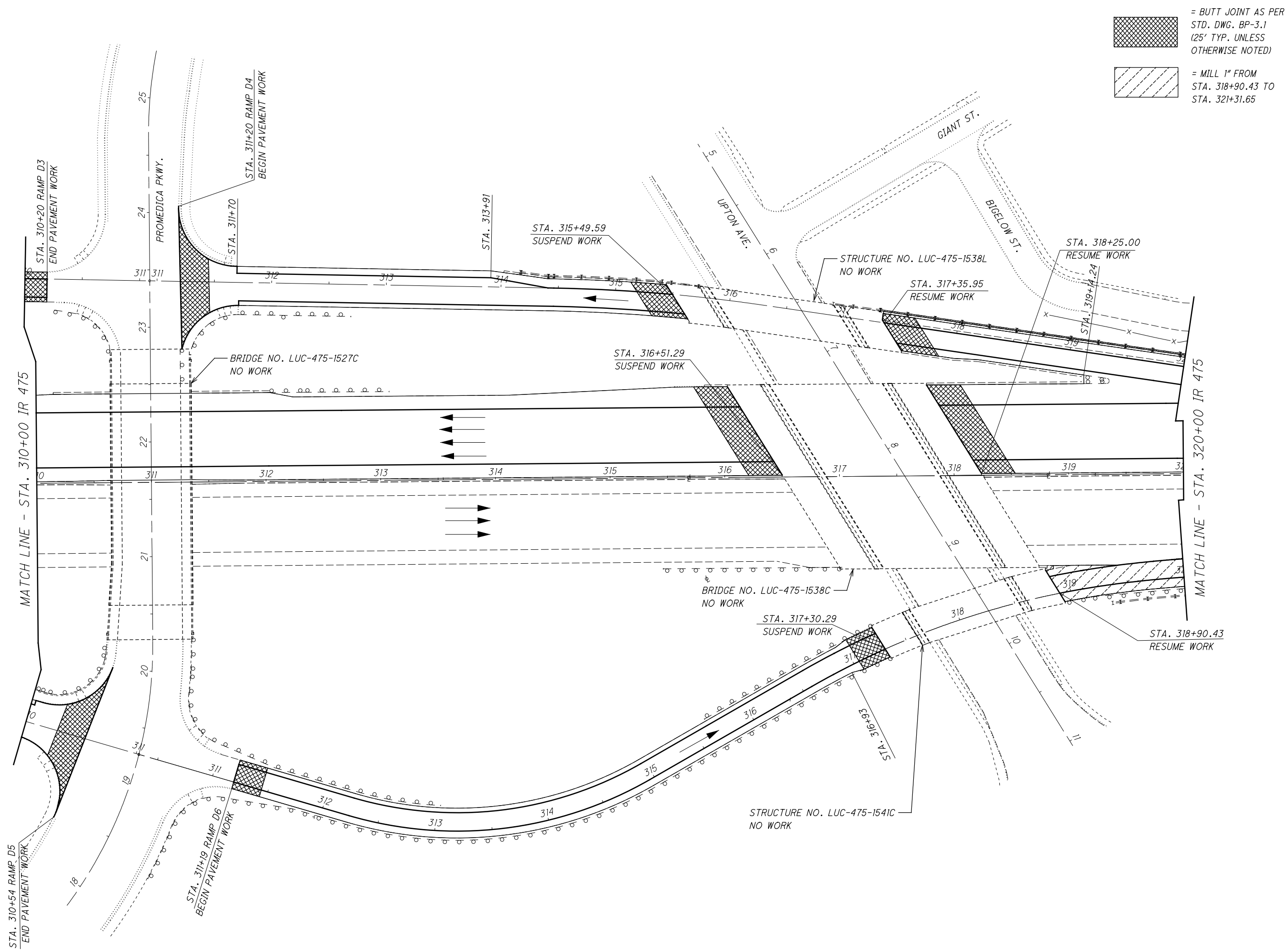
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ALF

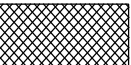
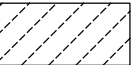
CHECKED  
JMF

0  
0  
0  
HORIZONTAL  
SCALE IN FEET

**PLAN SHEET I-475**  
**STA. 300+00 TO STA. 310+00**

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-  = BUTT JOINT AS PER STD. DWG. BP-3.1 (25' TYP. UNLESS OTHERWISE NOTED)
-  = MILL 1" FROM STA. 318+90.43 TO STA. 321+31.65

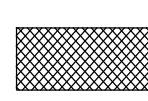
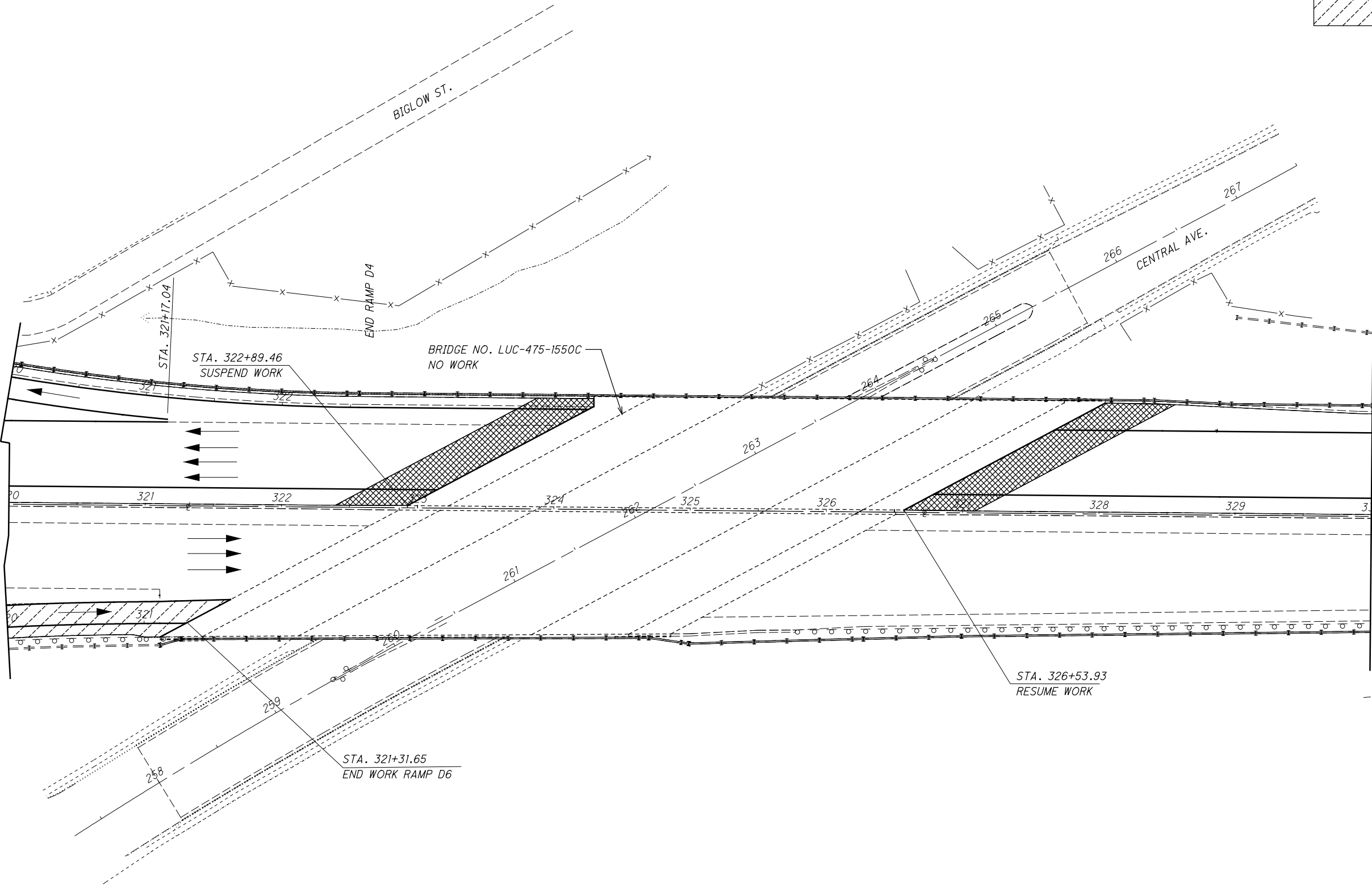
CALCULATED ALF  
CHECKED JMF

0 20 40 80  
HORIZONTAL SCALE IN FEET

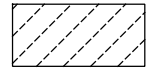
**PLAN SHEET I-475**  
**STA. 310+00 TO STA. 320+00**

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MATCH LINE - STA. 320+00 IR 475



= BUTT JOINT AS PER STD. DWG. BP-3.1 (25' TYP. UNLESS OTHERWISE NOTED)



= MILL 1" FROM STA. 318+90.43 TO STA. 321+31.65

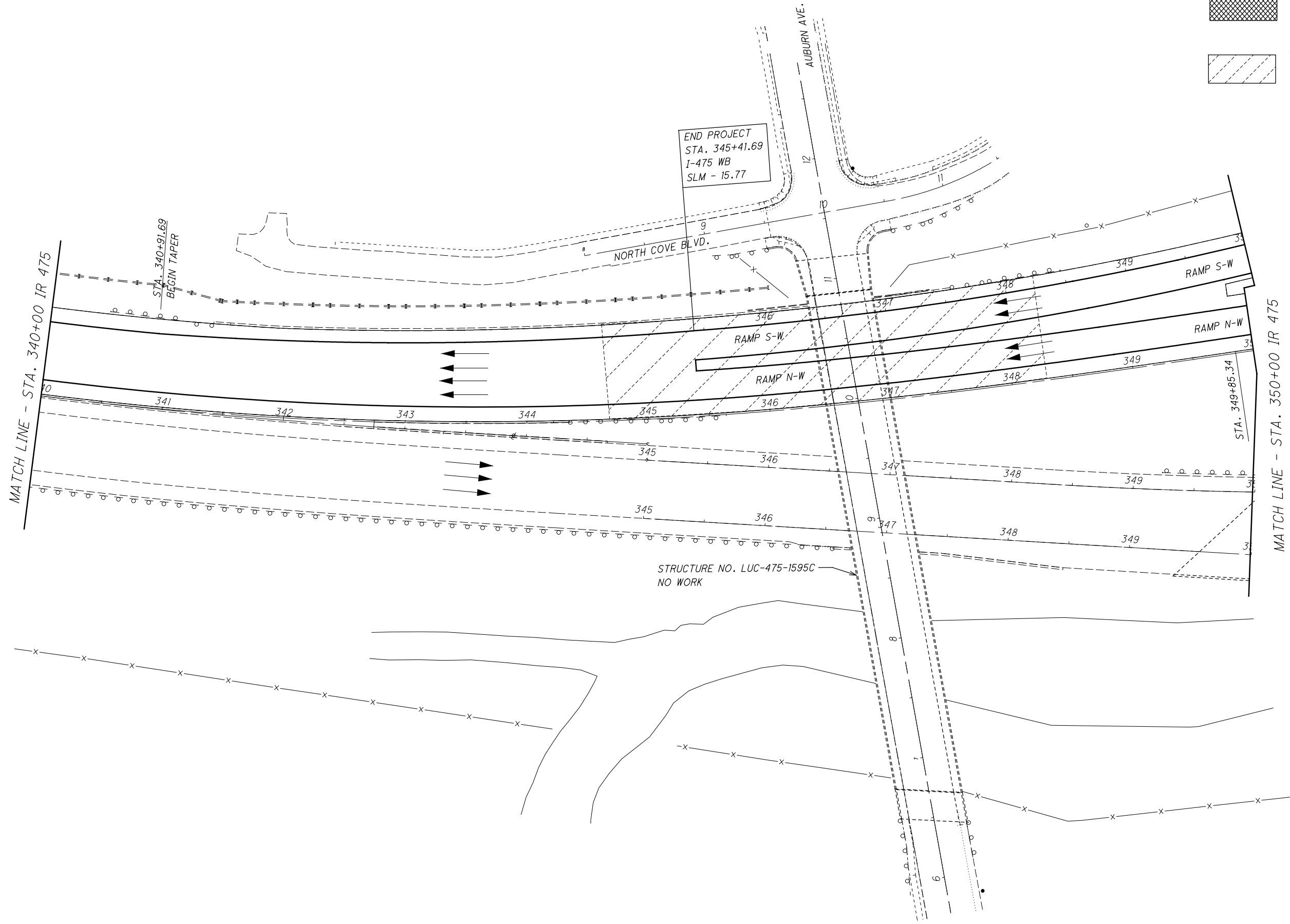
CALCULATED ALF  
CHECKED JMF

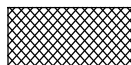
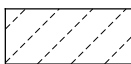
0 40 80  
HORIZONTAL SCALE IN FEET

**PLAN SHEET I-475**  
**STA. 320+00 TO STA. 330+00**

LUC-75 / 475-7.82 / 14.50

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 = BUTT JOINT AS PER STD. DWG. BP-3.1 (25' TYP. UNLESS OTHERWISE NOTED)  
 MILL 1" FROM STA. 344+75 TO STA. 348+25 FOR BRIDGE CLEARANCE

CALCULATED ALF  
 CHECKED JMF  
  
  
 HORIZONTAL SCALE IN FEET

**PLAN SHEET I-475**  
**LUC-75 / 475-7.82 / 14.50**

**LUC-75 / 475-7.82 / 14.50**  
