

**MEDIAN BARRIER REPLACEMENT**

THIS ITEM WILL INCLUDE THE REMOVAL AND REPLACEMENT OF VARIOUS LOCATIONS OF MEDIAN BARRIER INCLUDED IN THIS PLAN. THE FOLLOWING QUANTITIES WILL BE INCLUDED FOR MEDIAN BARRIER REMOVAL AND REPLACEMENT:

ITEM 622 - CONCRETE BARRIER, TYPE D, AS PER PLAN 93 FT.

IT IS THE INTENT TO FOLLOW THE PLAN INSERT SHEET INCLUDED IN THIS PLAN FOR CONCRETE BARRIER WALL INSTALLATION. PLACEMENT OF MEDIAN BARRIER WALL SHALL IMMEDIATELY FOLLOW THE REMOVAL.

**AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS**

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED A HEIGHT OF 145 FT. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA), AND THE ODOT OFFICE OF AVIATION, WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL COORDINATION IS MET AND DOCUMENTATION HAS BEEN FURNISHED TO THE PROJECT ENGINEER. IF COORDINATION IS NOT OBTAINED, THEN THE PROJECT ENGINEER WILL HAVE THE AUTHORITY TO PROVIDE RESTRICTIONS AS REQUIRED.

THE TOLEDO HOSPITAL  
PROMEDICA HEALTH SYSTEM  
2142 N. COVE BLVD.  
TOLEDO, OHIO 43606  
419.291.4000

**ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (447), AS PER PLAN**

ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5MM TYPE A (447), AS PER PLAN SHALL FOLLOW THE SPECIFICATIONS FOR THE 442 ITEM EXCEPT FOR SECTION 442.04 ASPHALT BINDER, THE BINDER SHALL BE PG76-22M FOR THE SURFACE COURSE AND A MAXIMUM OF 10% OF RAP BY DRY WEIGHT OF MIX CAN BE USED. ADDITIONALLY, THE COARSE AGGREGATE FOR THIS ITEM SHALL CONTAIN A MINIMUM OF 50% AIR COOLED BLAST FURNACE SLAG, STEEL SLAG, OR CRUSHED IGNEOUS ROCK.

**ITEM 442, ASPHALT CONCRETE INTERMEDIATE COURSE, AS PER PLAN**

ITEM 442, ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE A (446), AS PER PLAN SHALL FOLLOW THE SPECIFICATIONS FOR THE 442 ITEM EXCEPT FOR SECTION 442.04 ASPHALT BINDER. THE BINDER SHALL BE PG76-22M FOR THE INTERMEDIATE COURSE AND A MAXIMUM OF 20% OF RAP BY DRY WEIGHT OF MIX CAN BE USED.

**ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN**

PER CMS 424.08, 448 DENSITY APPLIES TO THIS PROJECT. DENSITY WILL BE TESTED ACCORDING TO SUPPLEMENT 1055 PER CMS 448.02. THE DISINCENTIVE PORTION OF S-1055 (TABLE 1055.01-1 AND TABLE 1055.04) WILL BE WAIVED PROVIDING THAT THE CONTRACTOR MAKES EVERY EFFORT TO OBTAIN DENSITY AND DOES NOT USE VIBRATORY ROLLERS.

AN ESTIMATED QUANTITY OF 100 CY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO ACCOUNT FOR SURFACE IRREGULARITIES.

**PLANED SURFACES**

NO PLANED SURFACES SHALL BE OPEN TO THE PUBLIC FOR MORE THAN 5 DAYS. IF THE PLANED SURFACE IS OPEN FOR MORE THAN 5 DAYS, THEN IT IS THE CONTRACTOR'S RESPONSIBILITY TO REPAIR THE PAVEMENT FAILURES THAT OCCURRED AFTER THE 5 DAYS.

**RUMBLE STRIP REMOVAL BEFORE PAVING**

RUMBLE STRIPS WILL BE PLANED WITH ITEM 442, FOR THE I-75 LOCATION. THE QUANTITIES FOR PLANING AND PAVING THE RUMBLE STRIPS ARE PROVIDED BELOW. QUANTITIES ARE BASED ON 2' WIDE MILL.

I-75 NB EXISTING LENGTH OF RUMBLE STRIP:  
LENGTH = 16450 FT

I-75 SB EXISTING LENGTH OF RUMBLE STRIP:  
LENGTH = 16450 FT

RAMP LL TO I-75 NB EXISTING LENGTH OF RUMBLE STRIP:  
LENGTH = 1050 FT

TOTAL 33950 FT

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2 " I-75 NB = 3656 SY  
I-75 SB = 3656 SY  
RAMP LL TO I-75 NB = 234 SY  
= 7546 SY

ITEM 407 - NON TRACKING TACK COAT I-75 NB = 311 GAL  
I-75 SB = 311 GAL  
RAMP LL TO I-75 NB = 20 GAL  
= 696 GAL

ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 12MM, TYPE A (447), AS PER PLAN, 1 1/2" I-75 NB = 152 CY  
I-75 SB = 152 CY  
RAMP LL TO I-75 NB = 10 CY  
= 314 CY

RUMBLE STRIPS WILL BE PLANED WITH ITEM 441, FOR THE I-475 LOCATION. THE QUANTITIES FOR PLANING AND PAVING THE RUMBLE STRIPS ARE PROVIDED BELOW. QUANTITIES ARE BASED ON 2' WIDE MILL.

I 475 EB EXISTING LENGTH OF RUMBLE STRIP:  
LENGTH = 1346 FT (x2) = 2692 FT

I 475 WB EXISTING LENGTH OF RUMBLE STRIP:  
LENGTH = 7192 FT (x2) = 14384 FT

RAMP S-W TO I 475 WB EXISTING LENGTH OF RUMBLE STRIP:  
LENGTH = 900 FT (x2) = 1800 FT

RAMP N-W TO I 475 WB EXISTING LENGTH OF RUMBLE STRIP:  
LENGTH = 775 FT (x2) = 1550 FT

TOTAL 22026 FT

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2 " I 475 EB 598 SY  
I 475 WB 3197 SY  
RAMP S-W TO I 475 WB 400 SY  
RAMP N-W TO I 475 WB 345 SY  
= 4540 SY

ITEM 407 - NON TRACKING TACK COAT I 475 EB 51 GAL  
I 475 WB 272 GAL  
RAMP S-W TO I 475 WB 34 GAL  
RAMP N-W TO I 475 WB 30 GAL  
= 387 GAL

ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22, 1 1/2" I 475 EB 25 CY  
I 475 WB 133 CY  
RAMP S-W TO I 475 WB 17 CY  
RAMP N-W TO I 475 WB 15 CY  
= 190 CY

THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR RUMBLE STRIP REMOVAL:

ITEM 254, PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2" 12086 SY

ITEM 407, NON-TRACKING TACK COAT 1083 GAL

ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22, 1 1/2" 190 CY

ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 12MM, TYPE A (447), AS PER PLAN, 1 1/2" 314 CY

**ITEM 255, FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, MISC.: CLASS QC 3**

MATERIALS: PROVIDE MATERIALS CONFORMING TO THE FOLLOWING REQUIREMENTS:

PORTLAND CEMENT CONCRETE 499.03, CLASS QC 3, W/MACRO-FIBERS\*

QC3: FLEXURAL STRENGTH: 300 PSI IN 12 HOURS

PERMEABILITY: 2000 COULOMBS

MACROFIBERS: MINIMUM 4.0 LB/CY

COARSE AGGREGATE (NO. 57 & NO.8) 703.02 & 703.13  
FINE AGGREGATE (NATURAL SAND) 703.02  
PORTLAND CEMENT, TYPE I 701.04  
FLY ASH OR NATURAL POZZOLAN 701.13  
SLAG CEMENT 701.09  
WATER 499.02  
CHEMICAL ADMIXTURE 705.12  
AIR-ENTRAINING ADMIXTURE 705.10  
MACRO-FIBERS FOR CONCRETE 705.29  
LIQUID MEMBRANE-FORMING COMPOUNDS FOR CONCRETE CURING 705.07

\*USE A MINIMUM DOSAGE RATE OF FIBERS OF 4.0 LB/YD3 OF CONCRETE. ENSURE THE FINAL PROPOSED MIX IS WORKABLE AND ABLE TO BE PRODUCED SUCH THAT BALLING OR CLUMPING OF THE FIBERS IS NOT A PROBLEM AS DETERMINED BY THE ENGINEER. A DEMONSTRATION OF THE MIX PRODUCTION, OR TRIAL MIX, MAY BE REQUIRED BY THE ENGINEER PRIOR TO PLACING ANY OF THE MIX ON THE PROJECT.

PAVEMENT SHALL BE PLANED BEFORE PAVEMENT IS REMOVED. THE FOLLOWING ESTIMATED QUANTITY IS TO BE USED FOR PAVEMENT REMOVAL AND RIGID REPLACEMENT AS DIRECTED BY THE ENGINEER. THE CONTRACTOR MAY BRING THE CONCRETE TO THE MILLED SURFACE.

ITEM 255 - FULL DEPTH PAVEMENT REPAIR AND RIGID REPLACEMENT, MISC.: CLASS QC 3  
60-6x12 JOINTS NB = 480 SQ. YD.  
75-6x12 JOINTS SB = 600 SQ. YD.  
TOTAL = 1080 SQ. YD.

ITEM 255 - FULL DEPTH PAVEMENT SAWING  
2160 LF (NB)  
2700 FL (SB)  
TOTAL = 4860 LF

QUANTITIES CARRIED TO THE GENERAL SUMMARY.

**LONGITUDINAL JOINTS (FLEXIBLE PAVEMENT)**

LOCATE LONGITUDINAL JOINTS IN THE SURFACE COURSE SUBJECT TO THE FOLLOWING REQUIREMENTS:

- PLACE THE MAINLINE PAVEMENT SURFACE COURSE WITH A SINGLE COLD LONGITUDINAL JOINT LOCATED AT THE LANE LINES. NO OTHER COLD JOINTS ARE PERMITTED IN THE SURFACE COURSE OF THE MAINLINE PAVEMENT.

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GENERAL NOTES

LUC-75 / 475-7.82 / 14.50

**ITEM 614, MAINTAINING TRAFFIC**

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	FOURTH OF JULY
NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING
EASTER	

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
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SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

PLEASE REFER TO THE SEQUENCE OF CONSTRUCTION LOCATED ON MOT NOTE SHEET 23 AND THE MAINTENANCE OF TRAFFIC DETAIL SHEETS INCLUDED IN THIS PLAN FOR LANE CLOSURES AND RESTRICTIONS OF I-75 AND I-475.

**WORK WITHIN CONSTRUCTION ZONE**

THE CONTRACTOR MUST REMOVE ALL EQUIPMENT WHEN WORK IS NOT BEING PERFORMED OR THE CONTRACTOR MUST SET UP AN APPROVED MOT PLAN THAT INCLUDES A STAGING AREA WITH A MOT TRUCK AND APPROPRIATE IMPACT ATTENUATOR.

**CONCRETE MEDIAN BARRIER REPLACEMENT**

REMOVING, GRADING AND INSTALLING THE REPLACEMENT BARRIER IN A CONTINUOUS OPERATION SHALL BE LIMITED TO 50 LINEAR FEET AND SHALL AT ALL TIMES BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THE ENGINEER SHALL BE SATISFIED THAT ALL INSTALLATIONS WILL AFFORD MAXIMUM PROTECTION FOR TRAFFIC.

**WORK ZONE MARKINGS AND SIGNS**

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

ITEM 614 - WORK ZONE MARKING SIGN	60 EACH
ITEM 614 - WORK ZONE SPEED LIMIT SIGN	12 EACH
ITEM 614 - WORK ZONE LANE LINE, CLASS I, 6", 642 PAINT	38 MILE
ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT	31 MILE
ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT	7768 FT
ITEM 614 - WORK ZONE DOTTED LINE, CLASS III, 6", 642 PAINT	6573 FT
ITEM 614 - WORK ZONE STOP LINE, CLASS III, 642 PAINT	203 FT
ITEM 614 - WORK ZONE LANE LINE, CLASS I, 6", 807 PAINT	39 MILE
ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 6", 807 PAINT	32 MILE
ITEM 614 - WORK ZONE LANE LINE, CLASS I, 6", 740.06, TYPE 1	2 MILE
ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 12", 740.06, TYPE 1	5800 FT

ONLY CLASS I WORK ZONE PAVEMENT MARKINGS ON INTERSTATES AND MULTILANE HIGHWAYS EXPECTED TO BE IN PLACE LONGER THAN 14 DAYS WILL BE WET REFLECTIVE. WET REFLECTIVE WORK ZONE MARKINGS WILL NOT BE RECESSED. DUE TO CONTRACTORS MEANS AND METHODS, NOT KNOWING IF TEMPORARY MARKINGS WOULD BE IN PLACE LONGER THAN 14 DAYS, BOTH QUANTITIES WERE INCLUDED WITH THE CONTRACT.

**ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)**

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. [AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.]

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE		
ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO
		PUBLIC
RAMP & ROAD CLOSURES	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	>= 12 HRS & < 7 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HRS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS PHONE NUMBER 419-373-4428.

**ITEM 614, REPLACEMENT DRUM**

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 30 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

**ITEM 614, REPLACEMENT SIGN**

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ECT.

AN ESTIMATED QUANTITY OF 5 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

**WORK ZONE INCREASED PENALTIES SIGN (R11-H5A)**

R11-H5A-48 SIGNS SHALL BE FURNISHED, ERECTED, AND MAINTAINED IN GOOD CONDITION AND/OR REPLACED AS NECESSARY AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. SIGNS SHALL BE MOUNTED AT THE APPROPRIATE OFFSETS AND ELEVATIONS AS PRESCRIBED BY THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THEY SHALL BE MAINTAINED ON SUPPORTS MEETING CURRENT SAFETY CRITERIA.

THE SIGNS MAY BE ERECTED OR UNCOVERED NO MORE THAN FOUR HOURS BEFORE THE ACTUAL START OF WORK. THE SIGNS SHALL BE REMOVED OR COVERED NO LATER THAN FOUR HOURS FOLLOWING RESTORATION OF ALL LANES TO TRAFFIC WITH NO RESTRICTIONS, OR SOONER AS DIRECTED BY THE ENGINEER. TEMPORARY SIGN COVERING AND UNCOVERING DUE TO TEMPORARY LANE RESTORATIONS SHALL BE GUIDED BY THE FOUR-HOUR LIMITATIONS STATED ABOVE. SUCH LANE RESTORATIONS SHOULD BE EXPECTED TO REMAIN IN EFFECT FOR 30 OR MORE CONSECUTIVE CALENDAR DAYS, SUCH AS DURING WINTER SHUTDOWNS.

(THE SIGNS ON THE MAINLINE SHALL BE DUAL MOUNTED UNLESS NOT PHYSICALLY POSSIBLE. THE FIRST SIGN SHALL BE PLACED BETWEEN THE ROAD WORK AHEAD (W20-1) SIGN AND THE NEXT SIGN IN THE SEQUENCE. SIGNS SHALL BE ERECTED ON EACH ENTRANCE RAMP AND EVERY 2 MILES THROUGH THE CONSTRUCTION WORK LIMITS. SIGNS ON THE MAINLINE SHALL BE R11-H5A-48. SIGNS USED ON THE RAMPS SHALL BE R11-H5A-24. R11-H5A-24 SIGNS MAY BE USED IN THE MEDIAN IN LIEU OF R11-H5A-48 SIGNS IF IT IS NOT PHYSICALLY POSSIBLE TO PROVIDE R11-H5A-48 SIGNS IN THE MEDIAN.)

THE CONTRACTOR MAY USE SIGNS AND SUPPORTS IN USED, BUT GOOD, CONDITION PROVIDED THE SIGNS MEET CURRENT ODOT SPECIFICATIONS. SIGN FACES SHALL BE RETROREFLECTORIZED WITH TYPE G SHEETING COMPLYING WITH THE REQUIREMENTS OF C&MS 730.19.

WORK ZONE INCREASED PENALTIES SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS, INCLUDING THE SIGN AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION AS DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.

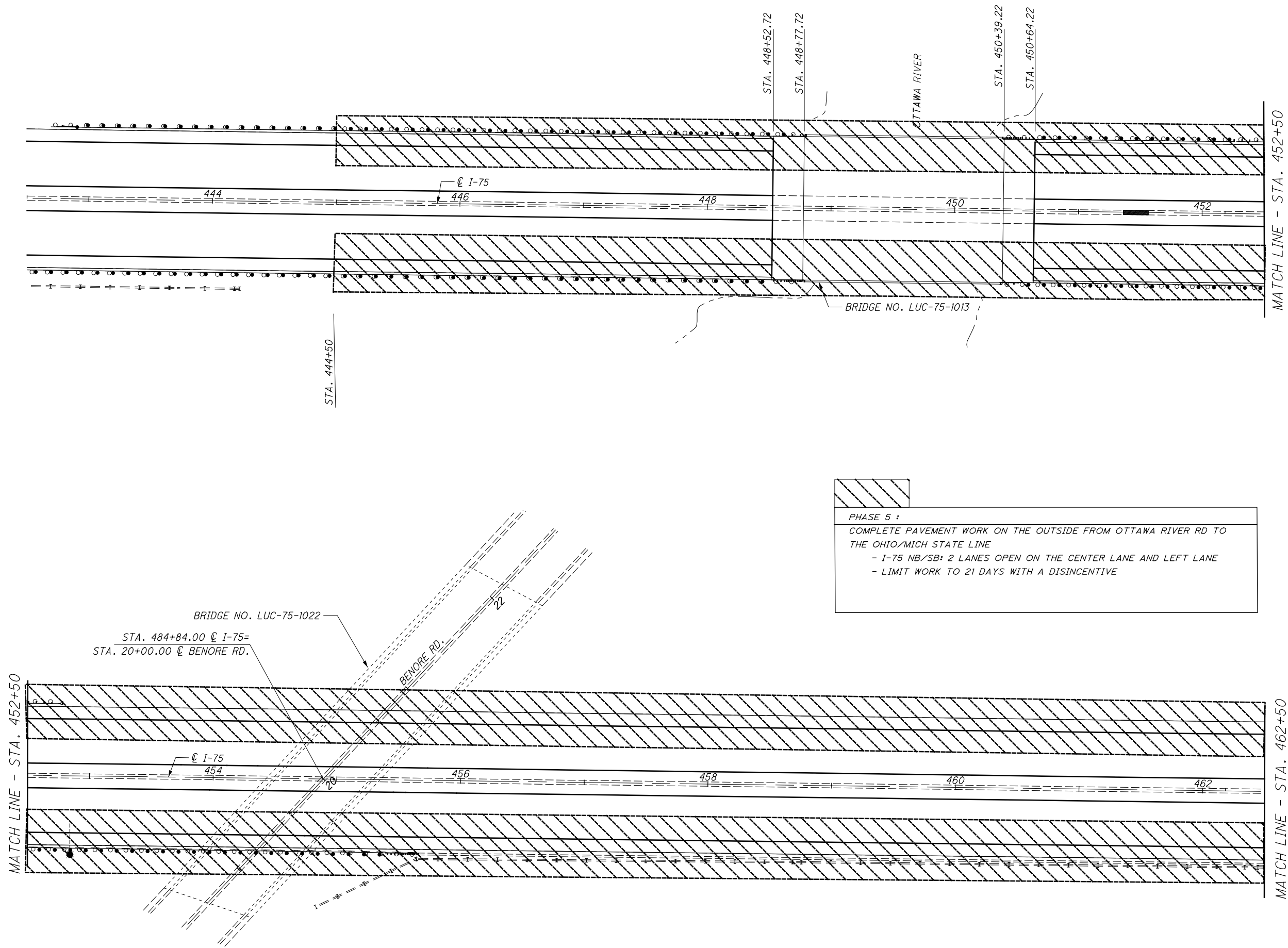
PAYMENT FOR ACCEPTED QUANTITIES, COMPLETE, IN PLACE WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, ERECTING, MAINTAINING, COVERING DURING SUSPENSION OF WORK, AND REMOVAL OF THE SIGN AND SUPPORT.

ITEM 614, WORK ZONE INCREASED PENALTIES SIGN 23 EACH

WORK ZONE INCREASED PENALTIES SIGNS WILL BE PLACED AT THE FOLLOWING LOCATIONS:

I-280 LANE B	2 EACH
RAMP LL	1 EACH
I-75 NB LANES	4 EACH
RAMP D (ALEXIS RD)	1 EACH
I-75 SB LANES	4 EACH
RAMP B (ALEXIS RD)	1 EACH
RAMP NN (OTTAWA RIVER)	1 EACH
I-475 EB LANES	2 EACH
RAMP D2 (DOUGLAS RD)	1 EACH
RAMP D6 (PROMEDICA PKWY)	1 EACH
I-475 WB LANES/RAMP SW	2 EACH
RAMP NW	2 EACH
RAMP D3 (PROMEDICA PKWY)	1 EACH

QUANTITY CARRIED TO THE GENERAL SUMMARY.



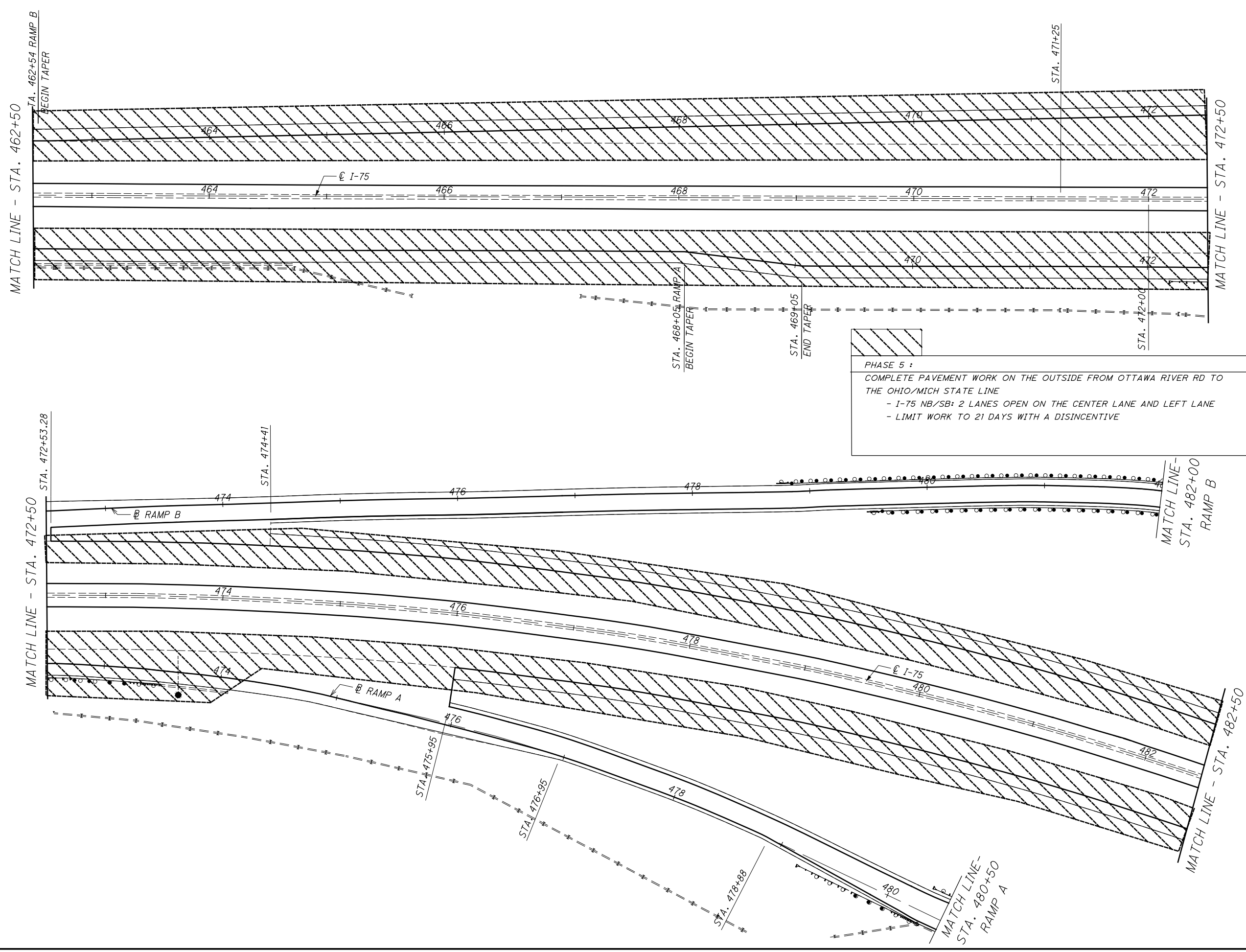
**PHASE 5 :**  
 COMPLETE PAVEMENT WORK ON THE OUTSIDE FROM OTTAWA RIVER RD TO THE OHIO/MICH STATE LINE  
 - I-75 NB/SB: 2 LANES OPEN ON THE CENTER LANE AND LEFT LANE  
 - LIMIT WORK TO 21 DAYS WITH A DISINCENTIVE



CALCULATED ALF  
 CHECKED JMF

**MAINTENANCE OF TRAFFIC DETAIL PHASE 5**  
**STA. 444+50 TO STA. 462+50**

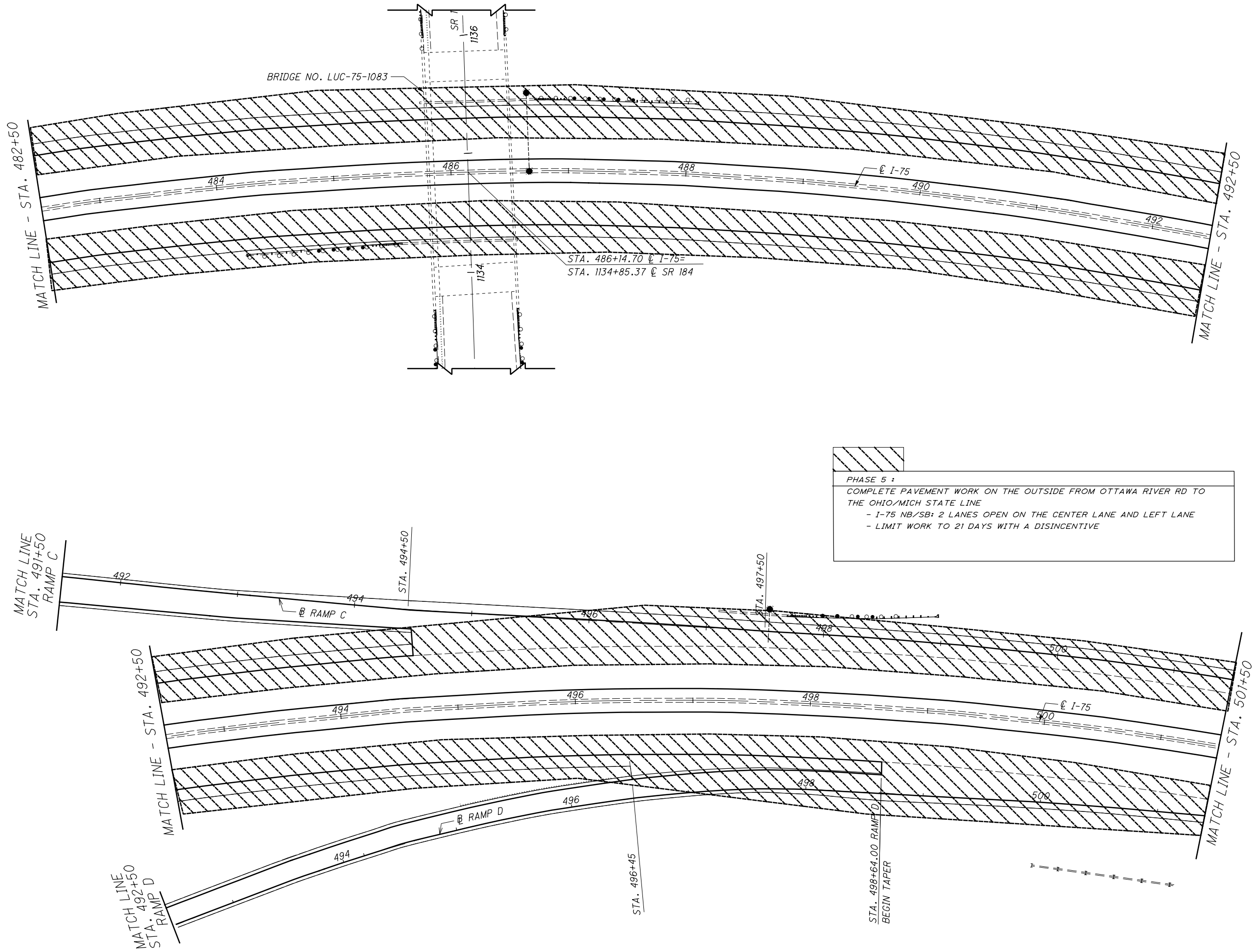
**LUC-75 / 475-7.82 / 14.50**



CALCULATED ALF  
 CHECKED JMF

**MAINTENANCE OF TRAFFIC DETAIL PHASE 5**  
**STA. 462+50 TO STA. 482+50**

**LUC-75 / 475-7.82 / 14.50**

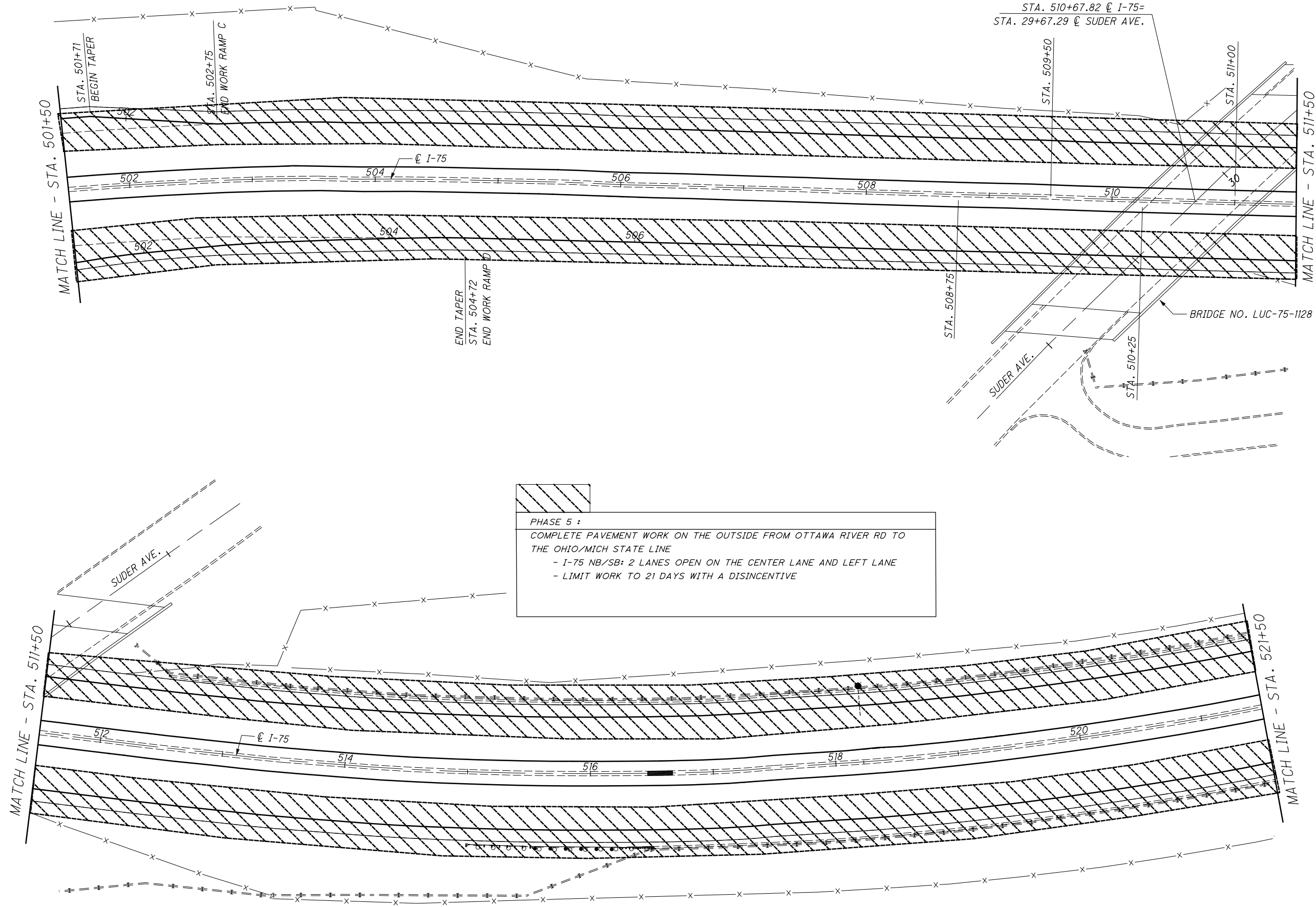


CALCULATED	ALF
CHECKED	JMF

0 20 40 80  
HORIZONTAL SCALE IN FEET

**MAINTENANCE OF TRAFFIC DETAIL PHASE 5**  
**STA. 482+50 TO STA. 501+50**

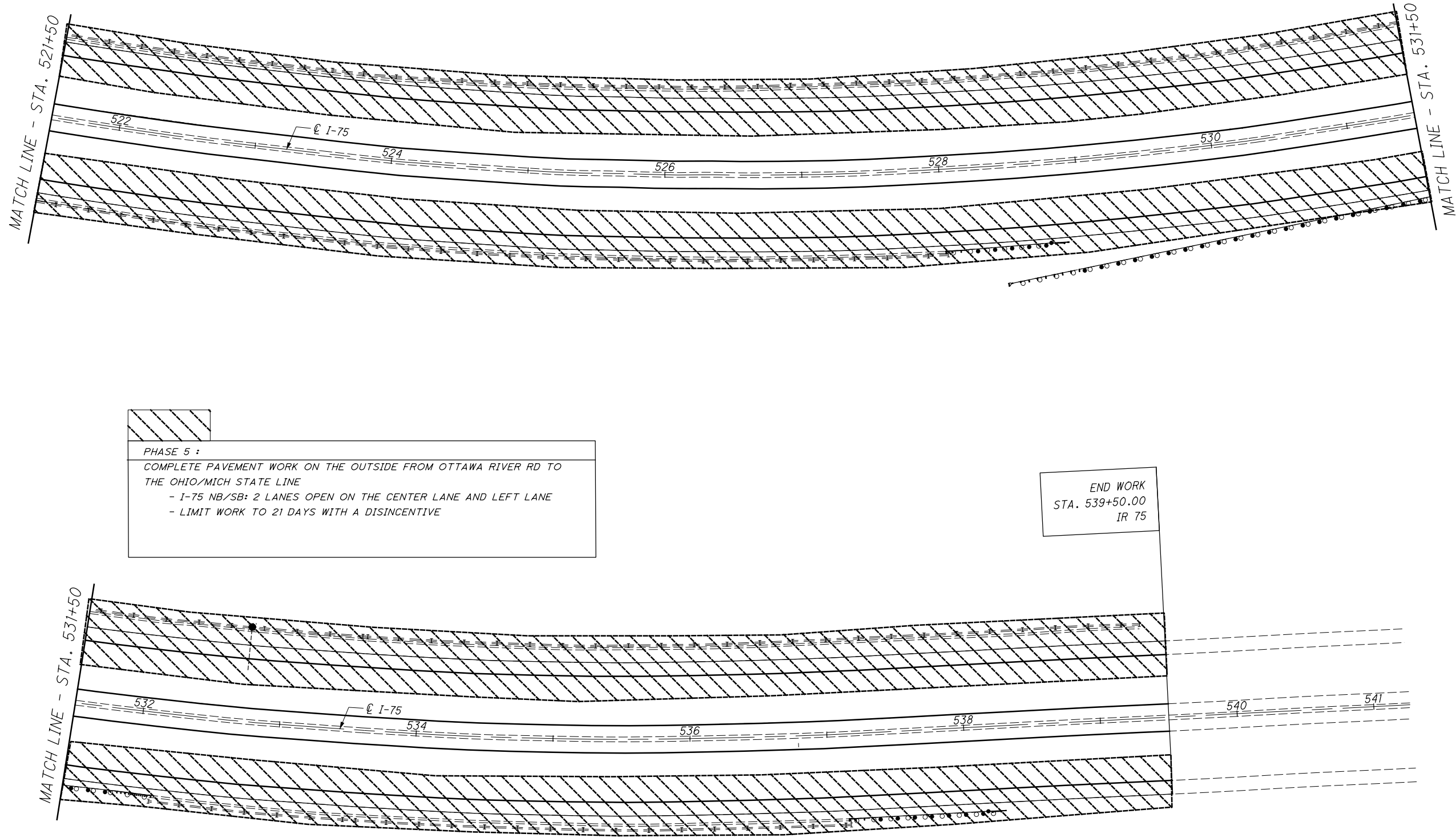
**LUC-75/475-7.82/14.50**



CALCULATED ALF  
 CHECKED JMF

**MAINTENANCE OF TRAFFIC DETAIL PHASE 5**  
**STA. 501+50 TO STA. 521+50**

LUC-75 / 475-7.82 / 14.50



**PHASE 5 :**  
 COMPLETE PAVEMENT WORK ON THE OUTSIDE FROM OTTAWA RIVER RD TO THE OHIO/MICH STATE LINE  
 - I-75 NB/SB: 2 LANES OPEN ON THE CENTER LANE AND LEFT LANE  
 - LIMIT WORK TO 21 DAYS WITH A DISINCENTIVE

END WORK  
 STA. 539+50.00  
 IR 75



CALCULATED ALF  
 CHECKED JMF

**MAINTENANCE OF TRAFFIC DETAIL PHASE 5**  
**STA. 521+50 TO STA. 539+50**

LUC-75 / 475-7.82 / 14.50

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SHEET NUM.														PART.		ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	
15	16	18	19	19A	20	21	22	23A	58	58A	59	82A	82C	01/IMS/PV	02/IMS/BR							
<b>ROADWAY</b>																						
LUMP																LUMP	201	11000	LS		CLEARING AND GRUBBING	
										3,271						3,271	202	23500	3,271	SY	WEARING COURSE REMOVED	
											15,035.5					15,035.5	202	38000	15,035.5	FT	GUARDRAIL REMOVED	
											17					17	202	42010	17	EACH	ANCHOR ASSEMBLY REMOVED, TYPE E	
											18					18	202	42041	18	EACH	ANCHOR ASSEMBLY REMOVED, TYPE T, AS PER PLAN	15
											3					3	202	42050	3	EACH	ANCHOR ASSEMBLY REMOVED, TYPE B	
											35					35	202	47000	35	EACH	BRIDGE TERMINAL ASSEMBLY REMOVED	
																LUMP	202	98000	LS		REMOVAL MISC.: TRAFFIC MONITORING EQUIPMENT	19A
											167					167	209	15000	167	STA	RESHAPING UNDER GUARDRAIL	
									7.56	6.01						13.57	209	60500	13.57	MILE	LINEAR GRADING	
											13,070					13,070	606	15050	13,070	FT	GUARDRAIL, TYPE MGS	
											20					20	606	26150	20	EACH	ANCHOR ASSEMBLY, MGS TYPE E, MASH 2016	
											18					18	606	26550	18	EACH	ANCHOR ASSEMBLY, MGS TYPE T	
											25					25	606	35002	25	EACH	MGS BRIDGE TERMINAL ASSEMBLY, TYPE 1	
											10					10	606	35102	10	EACH	MGS BRIDGE TERMINAL ASSEMBLY, TYPE 2	
2	93															93	622	24001	93	FT	CONCRETE BARRIER, TYPE D, AS PER PLAN	16
																2	623	39500	2	EACH	MONUMENT BOX ADJUSTED TO GRADE	
<b>EROSION CONTROL</b>																						
											2,904					2,904	659	10000	2,904	SY	SEEDING AND MULCHING	
											0.39					0.39	659	20000	0.39	TON	COMMERCIAL FERTILIZER	
											16					16	659	35000	16	MGAL	WATER	
																1,000	832	30000	1,000	EACH	EROSION CONTROL	
<b>DRAINAGE</b>																						
8																8	611	98630	8	EACH	CATCH BASIN ADJUSTED TO GRADE	
3																3	611	99654	3	EACH	MANHOLE ADJUSTED TO GRADE	
<b>PAVEMENT</b>																						
										241,995						241,995	254	01000	241,995	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 3 1/4"	
	12,086															12,086	254	01000	12,086	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2"	
											14,328					14,328	254	01000	14,328	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1"	
	1,080															1,080	255	10200	1,080	SY	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, MISC.:CLASS QC3	16
	4,860															4,860	255	20000	4,860	FT	FULL DEPTH PAVEMENT SAWING	
	1,083									33,879	8,972					43,934	407	20000	43,934	GAL	NON-TRACKING TACK COAT	
	100										2,932					3,032	424	12001	3,032	CY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN	16
	190															190	441	50000	190	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	
										15,202						15,202	442	00100	15,202	CY	ANTI-SEGREGATION EQUIPMENT	
	314									10,083						10,397	442	10301	10,397	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), AS PER PLAN	16
											277					11,764	442	10101	12,041	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, 19 MM, TYPE A (446), AS PER PLAN, PG72-22M	16
											693	480				1,173	617	10100	1,173	CY	COMPACTED AGGREGATE	
											2,774					2,774	SPECIAL	69012060	2,774	SY	PAVEMENT OVERLAY FABRIC COMPOSITE	19
											5,867					5,867	875	10000	5,867	LB	LONGITUDINAL JOINT ADHESIVE	
											3,330					3,330	897	01010	3,330	SY	PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A, 3"	
<b>WATER WORK</b>																						
2																2	638	10800	2	EACH	VALVE BOX ADJUSTED TO GRADE	
<b>LIGHTING</b>																						
																102	625	26253	102	EACH	LUMINAIRE, CONVENTIONAL, SOLID STATE (LED), AS PER PLAN, COOPER GALLEON OR TYPE H30	18
																1	625	34001	1	EACH	POWER SERVICE, AS PER PLAN	18
																LUMP	SPECIAL	62540000	LS		MAINTAIN EXISTING LIGHTING	18
																1	SPECIAL	62540010	1	EACH	REPLACEMENT OF EXISTING LIGHTING UNIT	18

GENERAL SUMMARY

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SHEET NUM.														PART.		ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
15	16	18	19	19A	20	21	22	23A	58	58A	59	82A	82C	01/IMS/PV	02/IMS/BR						
<b>TRAFFIC CONTROL</b>																					
												867	413	1,280		621	00100	1,280	EACH	RPM	
												867	413	1,280		621	54000	1,280	EACH	RAISED PAVEMENT MARKER REMOVED	
				225										225		625	25900	225	FT	CONDUIT, JACKED OR DRILLED, 3"	
				2										2		625	30700	2	EACH	PULL BOX, 725.08, 18"	
728														728		626	00102	728	EACH	BARRIER REFLECTOR, TYPE 1, ONE WAY	
											204			204		626	00116	204	EACH	BARRIER REFLECTOR, TYPE 5, UNIDIRECTIONAL	
											89			89		626	00116	89	EACH	BARRIER REFLECTOR, TYPE 5, BIDIRECTIONAL	
												14.78	6	20.78		642	00104	20.78	MILE	EDGE LINE, 6", TYPE 1	
												14.48	5.5	19.98		642	00204	19.98	MILE	LANE LINE, 6", TYPE 1	
1														1		642	40000	1	EACH	SPEED MEASUREMENT MARKING	15
												530	956	1,486		644	00400	1,486	FT	CHANNELIZING LINE, 8"	
												5,482	6,178	11,660		644	00404	11,660	FT	CHANNELIZING LINE, 12"	
												39	86	125		644	00500	125	FT	STOP LINE	
												48	264	312		644	00601	312	FT	CROSSWALK LINE, AS PER PLAN	15
												366	301	667		644	00720	667	FT	CHEVRON MARKING	
												9	27	36		644	01300	36	EACH	LANE ARROW	
												2	3	5		644	01360	5	EACH	WRONG WAY ARROW	
												4,611	2,191	6,802		644	01510	6,802	FT	DOTTED LINE, 6"	
<b>RETAINING WALLS</b>																					
		1,365												1,365		512	10100	1,365	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)	
		965												965		512	74000	965	SY	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES	
<b>STRUCTURE OVER 20 FOOT SPAN (LUC-75-0849)</b>																					
SEE SHEETS 105-108 FOR QUANTITIES.																					
<b>STRUCTURE OVER 20 FOOT SPAN (LUC-75-0861)</b>																					
SEE SHEETS 109-110 FOR QUANTITIES.																					
<b>STRUCTURE OVER 20 FOOT SPAN (LUC-75-0891)</b>																					
SEE SHEETS 111-112 FOR QUANTITIES.																					
<b>STRUCTURE OVER 20 FOOT SPAN (LUC-75-1013)</b>																					
SEE SHEETS 113-115 FOR QUANTITIES.																					
<b>MAINTENANCE OF TRAFFIC</b>																					
						500								500		614	11110	500	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
								2,301						2,301		614	11630	2,301	FT	INCREASED BARRIER DELINEATION	23A
					60									60		614	12460	60	EACH	WORK ZONE MARKING SIGN	
					12									12		614	12470	12	EACH	WORK ZONE SPEED LIMIT SIGN	
					23									23		614	12484	23	EACH	WORK ZONE INCREASED PENALTIES SIGN	
														5		614	12500	5	EACH	REPLACEMENT SIGN	
					30									30		614	12600	30	EACH	REPLACEMENT DRUM	
														942		614	13310	942	EACH	BARRIER REFLECTOR, TYPE 1, ONE-WAY	23A
														210		614	13318	210	EACH	BARRIER REFLECTOR, TYPE 5, ONE-WAY	23A
														437		614	13350	437	EACH	OBJECT MARKER, ONE WAY	23A
														353		614	13360	353	EACH	OBJECT MARKER, TWO WAY	23A
														10		614	18600	10	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN	
														38		614	20110	38	MILE	WORK ZONE LANE LINE, CLASS I, 6", 642 PAINT	
														2		614	20210	2	MILE	WORK ZONE LANE LINE, CLASS I, 6", 740.06, TYPE I	
														31		614	22110	31	MILE	WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT	
														5,800		614	23410	5,800	FT	WORK ZONE CHANNELIZING LINE, CLASS I, 12", 740.06, TYPE I	
														7,768		614	23690	7,768	FT	WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT	
														6,573		614	24618	6,573	FT	WORK ZONE DOTTED LINE, CLASS III, 12", 642 PAINT	
														203		614	26610	203	FT	WORK ZONE STOP LINE, CLASS III, 642 PAINT	
														39		614	20056	39	MILE	WORK ZONE LANE LINE, CLASS I, 6", 807 PAINT	
														32		614	22056	32	MILE	WORK ZONE EDGE LINE, CLASS I, 6", 807 PAINT	
								30						30		808	18700	30	SNMT	DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY	
<b>INCIDENTALS</b>																					
														LUMP		108	30000	LS		CPM PROGRESS SCHEDULE SHORT DURATION PROJECTS	
														LUMP		614	11000	LS		MAINTAINING TRAFFIC	
														LUMP		623	10001	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN	15
														LUMP		624	10000	LS		MOBILIZATION	

**GENERAL SUMMARY**

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