

ESTIMATED QUANTITIES			
ITEM	DESCRIPTION	QUANTITY	UNIT
203	EMBANKMENT, USING NATURAL SOILS, 703.16.A	145	CU.YD. *
601	DETENTION BASIN FILTER, 601.10	1,005	SQ.YD. *
670	SLOPE EROSION PROTECTION	18,954	SQ.YD. *
832	EROSION CONTROL	304,764	EACH
832	STORM WATER POLLUTION PREVENTION INSPECTION	1	LS
832	STORMWATER POLLUTION PREVENTION SOFTWARE	1	LS
832	STORMWATER POLLUTION PREVENTION PLAN	1	LS

TOTALS CARRIED TO THE GENERAL SUMMARY

*NOTES:

ITEM 203 - SEE SHEET 203, STA. 75+00 LT FOR LOCATION

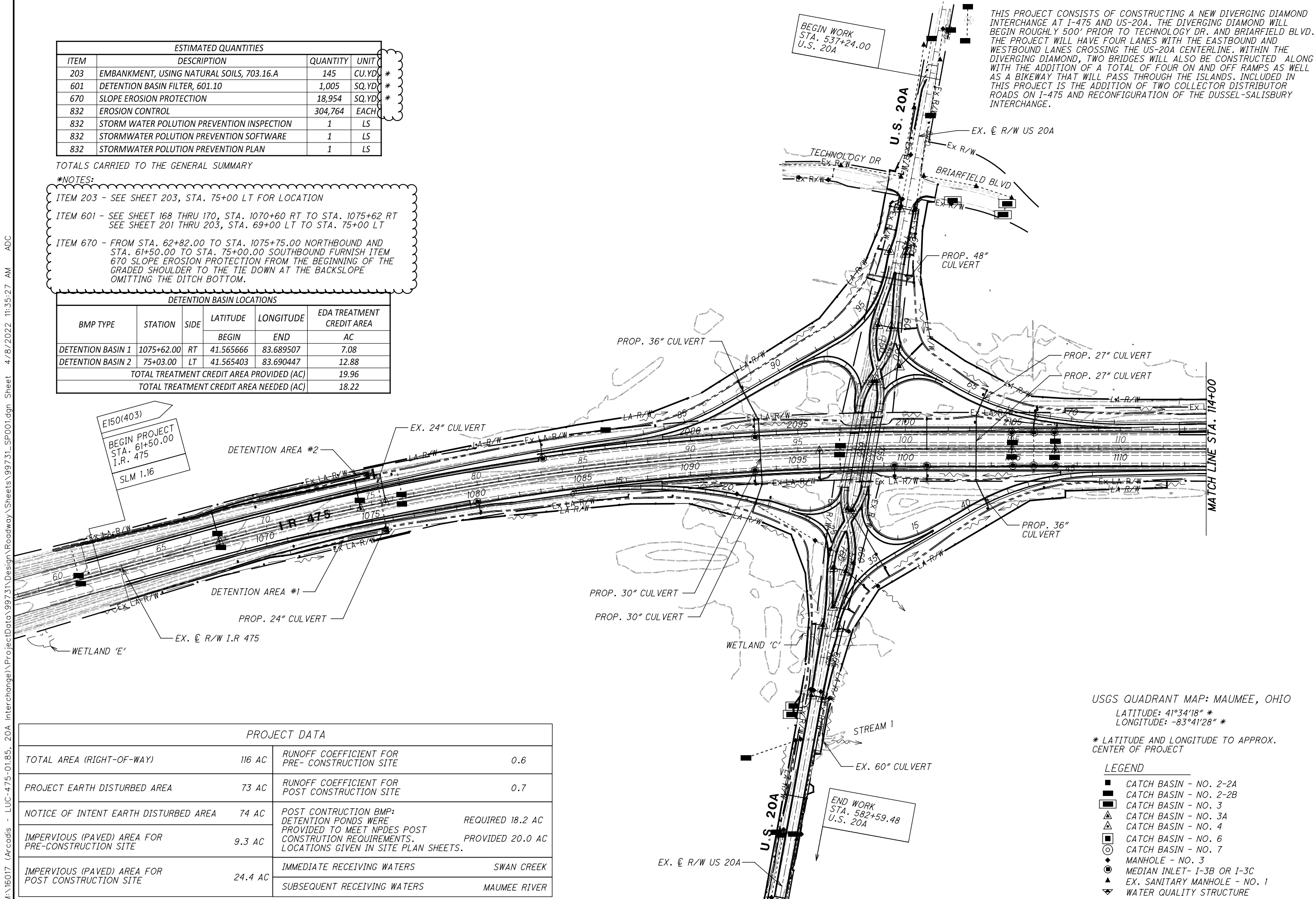
ITEM 601 - SEE SHEET 168 THRU 170, STA. 1070+60 RT TO STA. 1075+62 RT
SEE SHEET 201 THRU 203, STA. 69+00 LT TO STA. 75+00 LT

ITEM 670 - FROM STA. 62+82.00 TO STA. 1075+75.00 NORTHBOUND AND
STA. 61+50.00 TO STA. 75+00.00 SOUTHBOUND FURNISH ITEM
670 SLOPE EROSION PROTECTION FROM THE BEGINNING OF THE
GRADED SHOULDER TO THE TIE DOWN AT THE BACKSLOPE
OMITTING THE DITCH BOTTOM.

DETENTION BASIN LOCATIONS					
BMP TYPE	STATION	SIDE	LATITUDE	LONGITUDE	EDA TREATMENT CREDIT AREA
			BEGIN	END	
DETENTION BASIN 1	1075+62.00	RT	41.565666	83.689507	7.08
DETENTION BASIN 2	75+03.00	LT	41.565403	83.690447	12.88
TOTAL TREATMENT CREDIT AREA PROVIDED (AC)					19.96
TOTAL TREATMENT CREDIT AREA NEEDED (AC)					18.22

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THIS PROJECT CONSISTS OF CONSTRUCTING A NEW DIVERGING DIAMOND INTERCHANGE AT I-475 AND US-20A. THE DIVERGING DIAMOND WILL BEGIN ROUGHLY 500' PRIOR TO TECHNOLOGY DR. AND BRIARFIELD BLVD. THE PROJECT WILL HAVE FOUR LANES WITH THE EASTBOUND AND WESTBOUND LANES CROSSING THE US-20A CENTERLINE. WITHIN THE DIVERGING DIAMOND, TWO BRIDGES WILL ALSO BE CONSTRUCTED ALONG WITH THE ADDITION OF A TOTAL OF FOUR ON AND OFF RAMP AS WELL AS A BIKEWAY THAT WILL PASS THROUGH THE ISLANDS. INCLUDED IN THIS PROJECT IS THE ADDITION OF TWO COLLECTOR DISTRIBUTOR ROADS ON I-475 AND RECONFIGURATION OF THE DUSSEL-SALISBURY INTERCHANGE.



E150(403)
BEGIN PROJECT
STA. 61+50.00
I.R. 475
SLM 1.16

USGS QUADRANT MAP: MAUMEE, OHIO
LATITUDE: 41°34'18" *
LONGITUDE: -83°41'28" *
* LATITUDE AND LONGITUDE TO APPROX. CENTER OF PROJECT

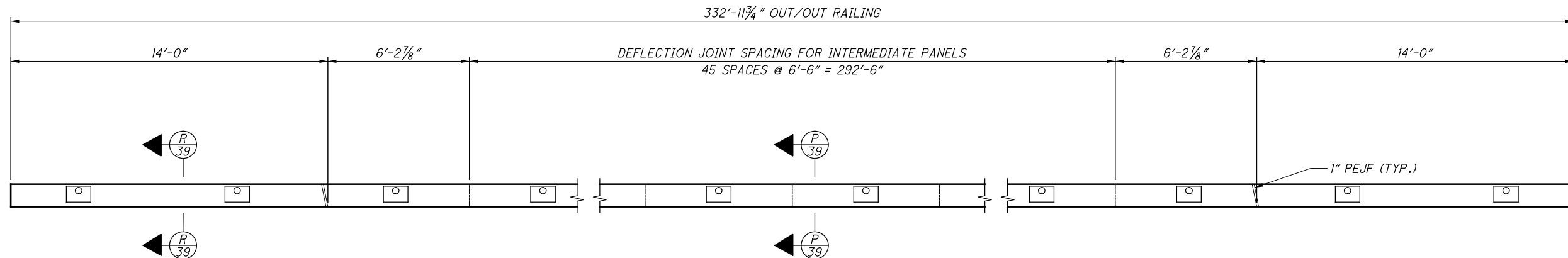
PROJECT DATA			
TOTAL AREA (RIGHT-OF-WAY)	116 AC	RUNOFF COEFFICIENT FOR PRE- CONSTRUCTION SITE	0.6
PROJECT EARTH DISTURBED AREA	73 AC	RUNOFF COEFFICIENT FOR POST CONSTRUCTION SITE	0.7
NOTICE OF INTENT EARTH DISTURBED AREA	74 AC	POST CONSTRUCTION BMP: DETENTION PONDS WERE PROVIDED TO MEET NPDES POST CONSTRUCTION REQUIREMENTS. LOCATIONS GIVEN IN SITE PLAN SHEETS.	REQUIRED 18.2 AC PROVIDED 20.0 AC
IMPERVIOUS (PAVED) AREA FOR PRE-CONSTRUCTION SITE	9.3 AC	IMMEDIATE RECEIVING WATERS	SWAN CREEK
IMPERVIOUS (PAVED) AREA FOR POST CONSTRUCTION SITE	24.4 AC	SUBSEQUENT RECEIVING WATERS	MAUMEE RIVER

- LEGEND**
- CATCH BASIN - NO. 2-2A
 - CATCH BASIN - NO. 2-2B
 - CATCH BASIN - NO. 3
 - ▲ CATCH BASIN - NO. 3A
 - ▲ CATCH BASIN - NO. 4
 - CATCH BASIN - NO. 6
 - CATCH BASIN - NO. 7
 - ◆ MANHOLE - NO. 3
 - ⊙ MEDIAN INLET- I-3B OR I-3C
 - ▲ EX. SANITARY MANHOLE - NO. 1
 - ▽ WATER QUALITY STRUCTURE

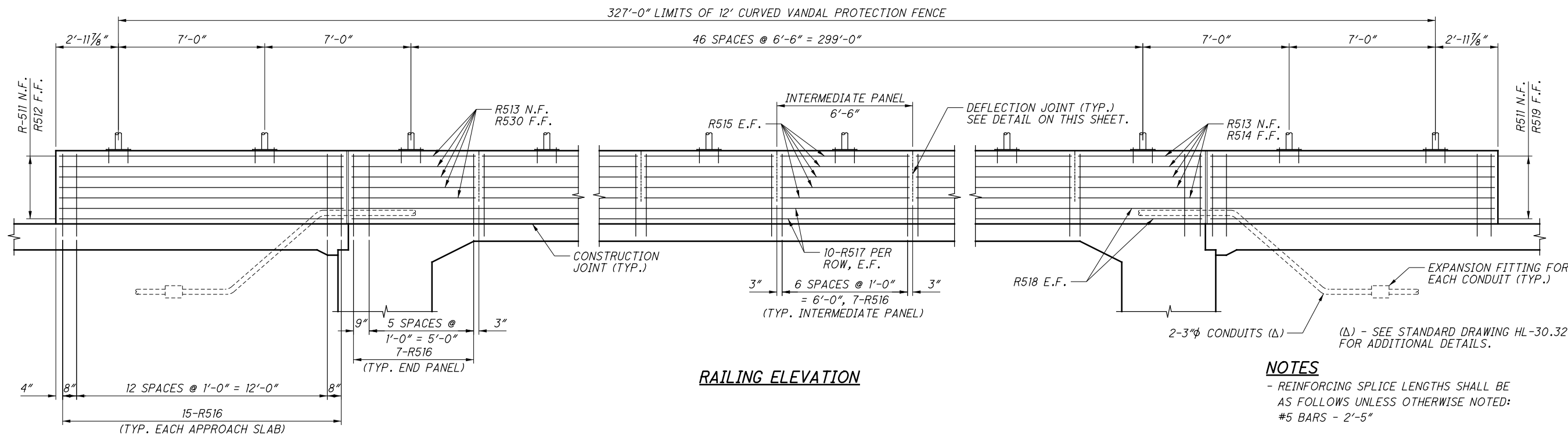
PROJECT SITE PLAN

LUC-475-01.85

52
637



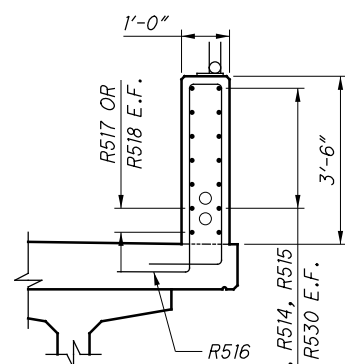
RAILING PLAN



RAILING ELEVATION

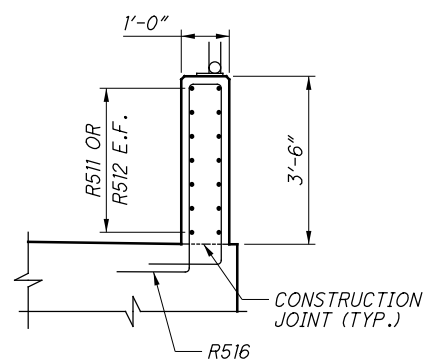
NOTES

- REINFORCING SPLICE LENGTHS SHALL BE AS FOLLOWS UNLESS OTHERWISE NOTED:
#5 BARS - 2'-5"
- SEE STD. DWG. BR-2-15 FOR SAWCUT DETAILS
- SEE STD. DWG. VPF-1-90 FOR DETAILS ON 12' CURVED VANDAL PROTECTION FENCE. USE BP-3 BASE PLATE AND PS-1 POST SECTION.
- PARAPET AESTHETIC TREATMENT SHALL BE INCLUDED FOR PAYMENT UNDER ITEM 511, CLASS QC2 CONCRETE WITH QC/QA, BRIDGE DECK (PARAPET), AS PER PLAN.



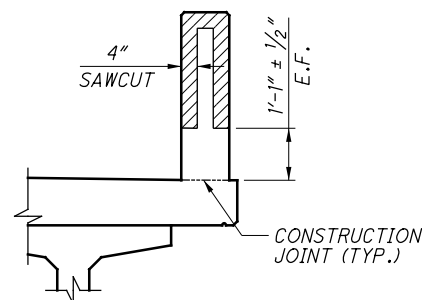
SECTION P-P

(DECK REINFORCING NOT SHOWN)

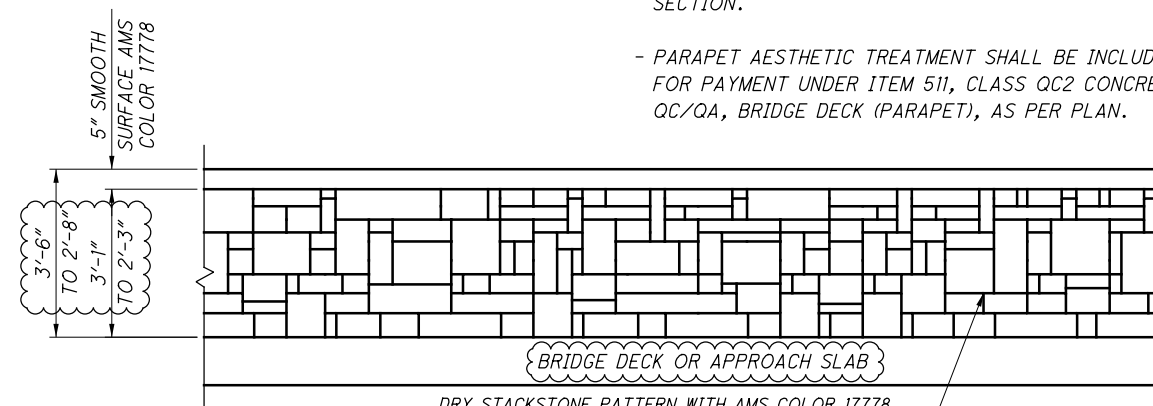


SECTION R-R

(APPROACH SLAB REINFORCING NOT SHOWN)



DEFLECTION JOINT DETAIL



DRY STACKSTONE PATTERN WITH AMS COLOR 17778
(CREATE 2 DISTINCT FORMS WITH A DIFFERENT RANDOM TEXTURE PATTERN, ALTERNATE USE OF EACH FORM)

AESTHETIC TREATMENT ELEVATION
(FULL LENGTH EXTERIOR PARAPET FASCIA BOTH STRUCTURES - 4 TOTAL FACES)



DESIGN AGENCY DGL Consulting Engineers, LLC 3455 Briarfield Blvd., Suite E Maumee, Ohio 43537 (419) 535-1015	DATE 3-9-18	DESIGNED SAM	DRAWN SAM	REVIEWED LLA
	STRUCTURE FILE NUMBER 4801025L/4801026R	CHECKED KWL	REVISED	
	SUPERSTRUCTURE DETAILS (19 OF 20)			
	BRIDGE NO. LUC-20A-1048 (L & R) US 20A OVER I-475/US 23			
LUC-475-01.85		PID No. 99731		
39/46		582		637

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ITEM 625 SPECIAL - MAINTAIN EXISTING LIGHTING

EXISTING ROADWAYS WHICH ARE TO REMAIN OPEN TO TRAFFIC DURING CONSTRUCTION OF THIS PROJECT AND ARE LIGHTED SHALL HAVE THE LIGHTING MAINTAINED AS DESCRIBED HEREIN. ANY NEW ROADWAYS THAT ARE TO BE LIGHTED SHALL BE LIT WITH NEW OR TEMPORARY LIGHTING PRIOR TO BEING OPENED TO TRAFFIC.

BEFORE ANY WORK IS STARED IN THE IMMEDIATE VICINITY OF ANY EXISTING LIGHTING CIRCUITS, REPRESENTATIVES OF THE STATE, THE MAINTAINING AGENCY, AND THE CONTRACTOR SHALL MAKE A VISUAL INSPECTION OF THE EXISTING ROADWAY LIGHTING CIRCUITS TO BE MAINTAINED.

DURING THIS INSPECTION, A WRITTEN RECORD OF THE CONDITION OF THE EXISTING LIGHTING SHALL BE MADE BY THE STATE'S REPRESENTATIVE. THIS WRITTEN REPORT SHALL NOTE INDIVIDUAL LUMINAIRES WHICH ARE NOT STANDING, AND INDIVIDUAL CIRCUITS WHICH ARE NOT IN WORKING ORDER. THE COMPLETED REPORT SHALL BE SIGNED BY THE REPRESENTATIVES OF THE STATE, THE MAINTAINING AGENCY, AND THE CONTRACTOR. IF, AS A RESULT OF THE INSPECTION, IT IS DETERMINED THAT THE CONDITION OF THE EXISTING SYSTEM IS BELOW THAT REQUIRED FOR THE SAFETY OF THE TRAVELING PUBLIC, THE MAINTAINING AGENCY SHALL MAKE REPAIRS NECESSARY TO RETURN THE SYSTEM TO AN ACCEPTABLE CONDITION. FOLLOWING THESE REPAIRS, THE SYSTEM SHALL AGAIN BE INSPECTED AND A REPORT MADE AND SIGNED AS OUTLINED HEREIN.

WHEN THE EXISTING SYSTEM IS IN AN ACCEPTABLE CONDITION, IT SHALL BE TURNED OVER TO THE CONTRACTOR WHO SHALL THEN BE REQUIRED TO MAINTAIN THE EXISTING LIGHTING TO THE CONDITION OUTLINED IN THIS REPORT WITH THE EXCEPTION OF THE KNOCKDOWNS DUE TO TRAFFIC ACCIDENTS.

REPLACEMENTS OF KNOCKED DOWN UNITS SHALL BE DONE ONLY WHEN THE ENGINEER HAS DETERMINED THAT THE REPLACEMENT OF THE KNOCKED DOWN UNIT IS NECESSARY AND SHALL BE PAID SEPARATELY ON A PER UNIT BASIS.

BETTERMENTS SHALL BE COVERED IN ITEMS OF WORK PERTAINING TO THE CONSTRUCTION OF PERMANENT IMPROVEMENTS. THE STATE SHALL GIVE THE CONTRACTOR ONE COPY OF THE EXISTING LIGHTING CIRCUITRY LAYOUT.

WHEN THE CONTRACTOR HAS TAKEN OVER THE MAINTENANCE OF THE EXISTING SYSTEM, HE SHALL PROVIDE ALL REQUIRED LAYOUT AND LOCATING OF EXISTING LIGHTING CIRCUITS WITHIN THE PROJECT.

WOOD POLES WITH OVERHEAD WIRING MAY BE USED. HOWEVER, TEMPORARY LIGHTING SHALL MEET FEDERAL AND STATE SAFETY CRITERIA. IF BREAKAWAY POLES ARE USED TO MEET THIS CRITERIA, THEN UNDERGROUND WIRING WILL BE USED. RECONDITIONED OR USED MATERIALS MAY BE FURNISHED FOR TEMPORARY LIGHTING.

ALL MATERIALS NECESSARY TO COMPLETE THE TEMPORARY LIGHTING SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR. A SEPARATE POWER SERVICE WILL BE PROVIDED BY THE CONTRACTOR FOR THE TEMPORARY LIGHTING SYSTEM. THE TEMPORARY LIGHTING SHALL NOT BE SPLICED INTO EXISTING LIGHTING CIRCUITS. THE CONTRACTOR SHALL PAY ALL HOOK-UP FEES AND ELECTRICAL COSTS FOR THE TEMPORARY SYSTEM. THESE COSTS SHALL BE PAID FOR UNDER THE LUMP SUM ITEM SPECIAL MAINTAIN EXISTING LIGHTING. WHEN NO LONGER NEEDED THE TEMPORARY LIGHTING INSTALLATION SHALL BE REMOVED AND PROPERLY DISPOSED OF BY THE CONTRACTOR.

ITEM 625 SPECIAL - MAINTAIN EXISTING LIGHTING (CONTINUED)

THE LUMP SUM BID FOR ITEM SPECIAL - MAINTAIN EXISTING LIGHTING, SHALL INCLUDE PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS, INCIDENTALS, AND TEMPORARY POWER SERVICES NECESSARY TO MAINTAIN THE EXISTING LIGHTING AS SPECIFIED HEREIN. THE UNIT BID PRICE FOR EACH ITEM SPECIAL - REPLACING EXISTING CONVENTIONAL LIGHTING UNIT, SHALL BE FULL PAYMENT FOR THE REPLACEMENT OF AN EXISTING UNIT WHICH HAS BEEN KNOCKED DOWN AFTER THE AFOREMENTIONED INSPECTION AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO PROVIDE A REPLACEMENT FOR SUCH UNIT. THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE DEPARTMENT'S ENGINEER.

ITEM SPECIAL - MAINTAIN EXISTING LIGHTING UNIT, LUMP

LANE VALUE CONTRACT (PN 127)

THE CONTRACTOR SHALL BE ASSESSED DISINCENTIVES AS DESIGNATED IN THE LANE VALUE CONTRACT TABLES/TIME LIMITATION NOTES FOR EACH UNIT OF TIME THE DESCRIBED CRITICAL LANE/RAMP IS RESTRICTED FROM FULL USE BY THE TRAVELING PUBLIC WITHIN THE RESTRICTED TIME PERIOD. THE LANE VALUE CONTRACT AMOUNT IS LOCATED IN THE TIME LIMITATION AND GENERAL MOT NOTES. THE DISINCENTIVES WILL BE ASSESSED FOR ALL RESTRICTIONS OF THE CRITICAL WORK.

CRITICAL WORK IS SHOWN IN THE LANE VALUE CONTRACT TABLE.

CRITICAL WORK IS DEFINED AS HAVING THE DESIGNATED SECTIONS OPEN TO UNRESTRICTED TRAFFIC AS SHOWN IN THE TABLE, OR THE ENTIRE PROJECT IF NOT OTHERWISE LISTED.

UNRESTRICTED TRAFFIC IS DEFINED AS ALL TRAFFIC LANES BEING AVAILABLE FOR USE WITH SPECIFIED STRIPING AND SAFETY FEATURES IN PLACE.

LANE VALUE CONTRACT TABLE				
ROUTE	RESTRICTION/NUMBER OF LANES TO BE MAINTAINED	RESTRICTED TIME PERIOD	TIME UNIT	DISINCENTIVE \$ PER TIME UNIT
I-475	2 LANES	WEEKDAYS: 6AM-9PM; WEEKENDS: 6AM-7PM	EACH MINUTE	\$215
I-475 (AFTER 3 LANES OPEN IN ONE OR BOTH DIRECTIONS)	3 LANES	WEEKDAYS: 7-9AM AND 2-7PM; WEEKENDS: 3-6PM	EACH MINUTE	\$215
	2 LANES	WEEKDAYS: 6AM-9PM; WEEKENDS: 6AM-7PM	EACH MINUTE	\$215
PART 3: US-24	2 LANES	WEEKDAYS: 7-9AM AND 2-7PM; WEEKENDS: 3-6PM	EACH MINUTE	\$215

ITEM 614 MAINTAINING TRAFFIC, MISC.: 6"X8" SOLID WOOD POST, AS PER PLAN

THIS ITEM SHALL CONSIST OF INSTALLING AND REMOVING GROUND MOUNTED 6"X8" SOLID WOOD POSTS. FIGURE 298-26 OF THE TRAFFIC ENGINEERING MANUAL(P. 2-219) SHOULD BE USED AS A GUIDE FOR INSTALLATION OF THE 6"X8" POSTS.

GRADE 2 SOUTHERN YELLOW PINE SHALL BE USED AND SHALL BE PRESSURE TREATED WITH CCA PRESERVATIVE.

PAYMENT WILL BE MADE PER UNIT PRICE BID PER EACH ITEM 614 MAINTAINING TRAFFIC, MISC.: 6"X8" SOLID WOOD POST, AS PER PLAN.

ITEM 614 MAINTAINING TRAFFIC, MISC.: SIGN (FLAT SHEET, PLYWOOD), AS PER PLAN

THIS ITEM CONSISTS OF INSTALLING AND REMOVING A FLAT SHEET SIGN ~~USING PLYWOOD. THE PLYWOOD SHALL BE A MINIMUM OF 3/4" THICK AND OF A MATERIAL APPROPRIATE FOR OUTDOOR USE, SUCH AS MARINE DENSITY OVERLAY, SIGNBOARD, OVERLAID PLYWOOD, OR HIGH DENSITY OVERLAY PLYWOOD.~~ ATTACHMENT AND MOUNTING MATERIALS SHALL BE IN CONFORMANCE WITH NCHRP REPORT 350. SEE SHEETS 70-171 FOR LOCATION AND SIZES OF THE TEMPORARY SIGNAGE. THIS SIGNAGE IS TO BE REMOVED ONCE THE PERMANENT SIGNS HAVE BEEN PLACED ON THEOVERHEAD SIGN SUPPORTS.

PAYMENT WILL BE MADE PER UNIT PRICE BID PER SQUARE FOOT OF ITEM 614 MAINTAINING TRAFFIC, MISC.: SIGN (FLAT SHEET, PLYWOOD), AS PER PLAN.

CALCULATED
BRO
CHECKED
DRJ

MAINTENANCE OF TRAFFIC GENERAL NOTES

LUC-475-01.85

SEQUENCE OF CONSTRUCTION

PID 95875 (MAINLINE I-475 WIDENING AND REPLACEMENT)

IT IS ASSUMED ALL DRAINAGE, LIGHTING, AND ANCILLARY ITEMS WILL BE CONSTRUCTED DURING THE PHASE THEY ARE LOCATED WITHIN.

SWITCHING BETWEEN PHASES SHALL BE COMPLETED DURING NIGHT TIME, WEEKEND OR OFF PEAK HOURS AS APPROVED BY THE ENGINEER.

SHOULDERS ON I-475 WILL REQUIRE CLOSING FOR INSTALLATION OF OVERHEAD AND GROUND MOUNTED SIGNS LOCATED OUTSIDE OF THE PROJECT LIMITS. TRAFFIC SHALL BE MAINTAINED PER MT-95.45.

THE CONTRACTOR HAS THE ABILITY TO DO ANY WORK CONCURRENTLY THAT WILL NOT IMPACT I-475 OR RAMP TRAFFIC BEYOND WHAT IS STATED IN THE PLANS AT ANY TIME.

THE PID 95875 PROJECT (WIDENING AND REPLACEMENT OF I-475) WILL BE CONSTRUCTED IN THREE MAIN PHASES:

PRE-PHASE 1

COMPLETE EARTHWORK AND TEMPORARY PAVEMENT ON THE SOUTHBOUND SIDE FROM STA. 51+11 TO 64+50. TWO LANES OF TRAFFIC SHALL BE MAINTAINED UTILIZING A SHOULDER CLOSURE PER STANDARD CONSTRUCTION DRAWING 95.45 DURING PERMITTED LANE CLOSURE TIMES.

PRE-PHASE 1A

AFTER PRE-PHASE 1, THE TWO SOUTHBOUND LANES SHALL BE SHIFTED TO THEIR PROPOSED PHASE 1 LOCATION TO ACCOMMODATE THE CONSTRUCTION OF TEMPORARY PAVEMENT IN THE MEDIAN, RAMP/MAINLINE Crossovers AND PLACEMENT OF PORTABLE CONCRETE BARRIER WALL. TRAFFIC SHALL BE MAINTAINED PER STANDARD CONSTRUCTION DRAWINGS MT-95.30 AND MT-95.45. SINGLE LANE CLOSURES WILL BE PERMITTED IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE NOTE ON SHEET 33.

PHASE 1

TWO LANES OF TRAFFIC ON NB AND SB I-475 SHALL BE MAINTAINED AT ALL TIMES.

TWO NORTHBOUND LANES OF TRAFFIC SHALL BE CROSSED OVER TO THE SOUTHBOUND DIRECTION PER THE PLANS. PHASE 1 WOULD WIDEN AND RECONSTRUCT THE ENTIRE PORTION OF THE NORTHBOUND DIRECTION UP TO AND INCLUDING THE INTERMEDIATE COURSE.

THE NORTHBOUND MONCLOVA BRIDGE WOULD BE CONSTRUCTED DURING THIS PHASE. SEE SEQUENCE OF CONSTRUCTION NOTES ON THIS SHEET FOR PID 96482 PROJECT FOR ADDITIONAL DETAILS FOR THE MONCLOVA BRIDGE CONSTRUCTION.

IN THE VICINITY OF THE US 20A BRIDGE TRAFFIC WOULD BE SHIFTED TO THE OUTSIDE TO ALLOW FOR THE CONSTRUCTION OF THE SUB STRUCTURE. SEE SEQUENCE OF CONSTRUCTION NOTES ON SHEET 45 FOR PID 99731 PROJECT FOR ADDITIONAL DETAILS FOR US 20A CONSTRUCTION.

SEQUENCE OF CONSTRUCTION (CONT)

PHASE 1 (CONTINUED)

NEAR THE COMPLETION OF PHASE 1, SHORT TERM LANE CLOSURES WOULD BE REQUIRED TO RESURFACE AND WIDEN THE PAVEMENT ADJACENT TO THE MAUMEE RIVER BRIDGE IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE NOTE ON SHEET 33.

ALL RAMPS SHALL BE MAINTAINED DURING PHASE 1 WITH EXCEPTION OF THE I-475 NB ENTRANCE RAMP FROM US 24-WB WHICH SHALL BE CLOSED FOR THE DURATION OF PHASE 1 AND SHORT TERM CLOSURES FOR THE OTHER US 24 RAMPS FOR RESURFACING AND BARRIER REMOVAL AND REPLACEMENT. SEE PLANS FOR DETAILS. SEE DETOUR PLANS FOR THE DETOUR OF THE WESTBOUND US 24 NORTHBOUND ENTRANCE RAMP.

PHASE 2

TWO LANES OF TRAFFIC ON NB AND SB I-475 SHALL BE MAINTAINED AT ALL TIMES.

TWO SOUTHBOUND LANES OF TRAFFIC SHALL BE CROSSED OVER TO THE NORTHBOUND DIRECTION PER THE PLANS ON TO THE COMPLETED PHASE 1 PAVEMENT. PHASE 2 WOULD WIDEN AND RECONSTRUCT THE ENTIRE PORTION OF THE SOUTHBOUND DIRECTION UP TO AND INCLUDING THE INTERMEDIATE COURSE.

THE SOUTHBOUND MONCLOVA BRIDGE WOULD BE CONSTRUCTED DURING THIS PHASE. SEE SEQUENCE OF CONSTRUCTION NOTES ON THIS SHEET FOR PID 96482 PROJECT FOR ADDITIONAL DETAILS FOR THE MONCLOVA BRIDGE CONSTRUCTION.

IN THE VICINITY OF THE US 20A BRIDGE SEE SEQUENCE OF CONSTRUCTION NOTES ON SHEET 45 FOR PID 99731 PROJECT FOR ADDITIONAL DETAILS FOR US 20A CONSTRUCTION.

NEAR THE COMPLETION OF PHASE 2, SHORT TERM LANE CLOSURES WOULD BE REQUIRED TO RESURFACE AND WIDEN THE PAVEMENT ADJACENT TO THE MAUMEE RIVER BRIDGE IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE NOTE ON SHEET 33.

ALL RAMPS SHALL BE MAINTAINED DURING PHASE 2 WITH EXCEPTION I-475 SB ENTRANCE RAMPS FROM US-24 EB AND US-24 WB WHICH SHALL BE CLOSED FOR THE DURATION OF PHASE 2 AND SHORT TERM CLOSURES FOR US 24 RAMPS FOR RESURFACING AND BARRIER REMOVAL AND REPLACEMENT. SEE PLANS FOR DETAILS.

PHASE 3

PHASE 3 SHALL CONSIST OF THE PLACEMENT OF THE FINAL SURFACE COURSE AND PERMANENT PAVEMENT MARKINGS AS WELL AS ANY BARRIER REPLACEMENT WITHIN THE US 24 INTERCHANGE. A MINIMUM OF 2 LANES OF TRAFFIC IN EACH DIRECTION ON I-475 AND ALL RAMPS SHALL BE MAINTAINED AT ALL TIMES. TRAFFIC SHALL BE MAINTAINED PER MT-95.30, OMTCD FIGURE 6H-37 AND MT-99.20. LANE CLOSURES SHALL BE IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE NOTE ON SHEET 33.

PID 96482 - MAINTENANCE OF TRAFFIC FOR MONCLOVA ROAD UNDERNEATH IR-475

NO OTHER TRAFFIC RESTRICTIONS ON MONCLOVA ROAD WILL BE ALLOWED EXCEPT AS DESCRIBED BELOW.

MONCLOVA ROAD SHALL REMAIN COMPLETELY OPEN TO BOTH EASTBOUND AND WESTBOUND TRAFFIC WHEN US20A IS CLOSED FOR CONSTRUCTION. NO FLAGGING OPERATIONS WILL BE ALLOWED ON MONCLOVA ROAD WHEN US20A IS CLOSED FOR CONSTRUCTION.

WESTBOUND MONCLOVA ROAD - CLOSURES AND FLAGGING OPERATIONS

WESTBOUND MONCLOVA ROAD CLOSURES AND FLAGGING OPERATIONS WILL NOT BE LIMITED EXCEPT WHEN US20A IS CLOSED AS NOTED ABOVE.

EASTBOUND MONCLOVA ROAD - CLOSURE

THE CONTRACTOR MAY CLOSE EASTBOUND MONCLOVA ROAD FOR THE NUMBER OF DAYS SHOWN IN TABLE 1 (THIS IS IN ADDITION TO THE ALLOWABLE FLAGGING DAYS). THE CONTRACTOR WILL BE PAID AN INCENTIVE FOR THE NUMBER OF ALLOWABLE CLOSURE DAYS NOT UTILIZED OR WILL BE ASSESSED A DISINCENTIVE FOR EACH DAY EXCEEDING THE ALLOWABLE CLOSURE DAYS. MULTIPLE CLOSURES ARE ALLOWED. COMPLETE CLOSURES OF MONCLOVA RD SHALL BE SPACED A MINIMUM OF 2 WEEKS APART REGARDLESS OF ANY SPECIFIC COMPLETE CLOSURE DURATION. CLOSURE DURATIONS IN TABLE 1 SHOW THE TOTAL CLOSURE DAYS ALLOWED FOR BOTH THE RIGHT AND LEFT STRUCTURES TOGETHER.

EASTBOUND MONCLOVA ROAD - FLAGGING OPERATIONS

THE CONTRACTOR MAY RESTRICT TRAFFIC WITH A FLAGGING OPERATION FOR THE NUMBER OF DAYS SHOWN IN TABLE 1 (THIS IS IN ADDITION TO THE ALLOWABLE CLOSURE DAYS). THE CONTRACTOR WILL BE PAID AN INCENTIVE FOR THE NUMBER OF ALLOWABLE FLAGGING DAYS NOT UTILIZED OR WILL BE ASSESSED A DISINCENTIVE FOR EACH DAY EXCEEDING THE ALLOWABLE FLAGGING DAYS.

FLAGGING DURATIONS IN TABLE 1 SHOW THE TOTAL FLAGGING DAYS ALLOWED FOR BOTH THE RIGHT AND LEFT STRUCTURES TOGETHER. FLAGGING FOR ANY PORTION OF THE DAY WILL COUNT AS A FULL FLAGGING DAY REGARDLESS OF THE ACTUAL FLAGGING DURATION.

STOPPAGE OF TRAFFIC BY FLAGGERS SHALL NOT EXCEED 5 MINUTES AT A TIME. ONCE THE 5-MINUTE INTERVAL IS COMPLETE, TRAFFIC SHALL BE ALLOWED TO FLOW UNTIL ALL QUEUED TRAFFIC HAS PASSED THROUGH THE PROJECT.

IF A FLAGGING OPERATION STOPS TRAFFIC FOR MORE THAN 5 MINUTES, DAMAGES IN THE AMOUNT OF \$25 PER MINUTE WILL BE ASSESSED AND THAT DAY WILL COUNT AS CLOSURE DAY AS WELL AS A FLAGGING DAY.

FLAGGERS SHALL GIVE PRIORITY TO ALL EMERGENCY VEHICLES DURING FLAGGING OPERATIONS. WORK SHALL IMMEDIATELY CEASE WHEN THE TRAFFIC QUEUE CONTAINS AN EMERGENCY VEHICLE WITH EMERGENCY SIGNALS OPERATING. TRAFFIC SHALL BE RELEASED TO ALLOW THE EMERGENCY VEHICLE TO PASS.

DESCRIPTION OF CRITICAL WORK	DURATION	TIME PERIOD	DISINCENTIVE \$ PER TIME PERIOD	INCENTIVE \$ PER TIME PERIOD	MAXIMUM INCENTIVE \$
COMPLETE CLOSURE OF MONCLOVA ROAD	70	CALENDAR DAYS	\$5,000	\$2,000	\$20,000
FLAGGING OPERATIONS ON MONCLOVA ROAD	90	CALENDAR DAYS	\$5,000	\$500	\$10,000

PID 96482 - MAINTENANCE OF TRAFFIC FOR MONCLOVA ROAD UNDERNEATH IR-475 (CONTINUED)

NOTIFICATION

COMPLETE CLOSURE AND FLAGGING OPERATIONS SHALL BE COMMUNICATED WITH MCLAREN ST. LUKE'S HOSPITAL, PROMEDICA TOLEDO HOSPITAL EMERGENCY AND URGENT CARE, AND LOCAL EMERGENCY/AMBULANCE SERVICES. THE PROJECT ENGINEER WILL COORDINATE THIS COMMUNICATION AND PROVIDE A LIST OF PARTIES TO BE INCLUDED. NOTIFICATION TIME FRAMES SHALL MEET THE REQUIREMENTS DETAILED IN THE NOTICE TO OFFICE OF COMMUNICATION TIME TABLE ON SHEET 33.

CLOSURE AND FLAGGING COMMUNICATIONS WILL INCLUDE THE TIME OF DAY THAT THE RESTRICTION WILL BEGIN AND END. RESTRICTING THE ROADWAY PRIOR TO THE STATED TIME OF DAY OR EXTENDING A RESTRICTION PAST THE STATED TIME OF DAY WILL RESULT IN A \$25/MINUTE DAMAGE. THE CONTRACTOR SHALL USE PORTABLE MESSAGE BOARDS AT THE FOLLOWING LOCATIONS PRIOR TO AND DURING A CLOSURE/FLAGGING EVENT. THE PROJECT ENGINEER WILL PROVIDE MESSAGES TO BE USED.

MONCLOVA RD. AND N. JEROME ROAD
BRIARFIELD AND 20A
RUSSELL RD. AND FALLEN TIMBERS LANE

CALCULATED
BRO
CHECKED
DRJ

MAINTENANCE OF TRAFFIC GENERAL NOTES

LUC-475-01.85

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REF NO.	SHEET NO.	STATION TO STATION		SIDE	614	614	614	614	614	614	614	614	614	614	614	614	614	614	615	622	622	622
					WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDOUS (UNIDIRECTIONAL)	MAINTAINING TRAFFIC, MISC.: 6"X8" SOLID WOOD POST, AS PER PLAN	MAINTAINING TRAFFIC, MISC.: SIGN (FLAT SHEET, PLYWOOD)	WORK ZONE LANE LINE, CLASS I, 4"	WORK ZONE CENTER LINE, CLASS I	WORK ZONE EDGE LINE, CLASS I, 6" (WHITE)	WORK ZONE EDGE LINE, CLASS I, 6" (WHITE)	WORK ZONE EDGE LINE, CLASS I, 6" (YELLOW)	WORK ZONE EDGE LINE, CLASS I, 6" (YELLOW)	WORK ZONE CHANNELIZING LINE, CLASS I, 12"	WORK ZONE CHANNELIZING LINE, CLASS I, 12", 807 PAINT	WORK ZONE DOTTED LINE, CLASS I (WHITE)	WORK ZONE DOTTED LINE, CLASS I (YELLOW)	WORK ZONE DOTTED LINE, CLASS I, 6", 807 PAINT (WHITE)	WORK ZONE LANE LINE, CLASS I, 6", 807 PAINT	WORK ZONE STOP LINE, CLASS I	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS 4, AS PER PLAN	PORTABLE BARRIER, 50", AS PER PLAN
STATION TO STATION					EACH	EACH	SF	MILE	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	SY	FT	EACH	FT
PID 99731 PHASE																						
SN-1	131	56+00.00	56+00.00	RT		2	32															
SN-2	131	56+00.00	56+00.00	RT			6															
SN-3	131	82+50.00	82+50.00	RT		2	32															
SN-4	131	82+50.00	82+50.00	RT			6															
WCH-1	131	108+31.00	111+56.00	LT											325	325						
WCH-2	131	108+31.00	111+56.00	LT											325	325						
SN-5	131	109+00.00	109+00.00	RT		2	40															
SN-6	131	109+00.00	109+00.00	RT			6															
WCH-3	131	115+21.00	116+96.00	RT											175	175						
WCH-4	131	115+21.00	116+96.00	RT											175	175						
WCH-5	131	117+66.00	124+06.00	LT											640	640						
WCH-6	131	117+66.00	124+06.00	LT											640	640						
WEY-1	131	111+56.00	125+00.00	LT						1344	1344											
WEY-2	131	116+96.00	125+00.00	RT						804	804											
WEY-3	131	124+06.00	125+00.00	LT						94	94											
WEW-1	131	100+51.00	125+00.00	LT				2449	2449													
WEW-2	131	111+00.00	125+00.00	RT				1400	1400													
WEW-3	131	124+06.00	125+00.00	LT				94	94													
PB-1	131	111+00.00	125+00.00	RT																	1400	
PB-2	131	116+69.00	121+71.00	RT																	502	
WDW-1	131	100+51.00	108+31.00	LT											780		780					
WDW-2	131	111+56.00	117+66.00	LT											610		610					
WDW-3	131	111+00.00	115+21.00	RT											421		421					
WIA-1	131	116+69.00	116+69.00	RT	1																	
TP-1	131	111+00.00	119+05.00	RT	1														1189			
SN-7	131	117+00.00	117+00.00	RT		1	32															
TR-2	131	123+56.00	125+00.00	RT																		
WEY-1	132	125+00.00	127+88.00	LT							288	288										
WEY-2	132	125+00.00	129+16.00	LT							416	416										
WEY-3	132	125+00.00	127+73.00	RT							273	273										
WEY-4	132	130+74.00	137+50.00	RT							676	676										
WEW-1	132	125+00.00	127+84.00	LT				284	284													
WEW-2	132	125+00.00	129+16.00	LT				416	416													
WEW-3	132	125+00.00	127+45.00	RT				245	245													
WEW-4	132	130+74.00	137+50.00	RT				676	676													
PB-1	132	125+00.00	127+17.00	RT																	217	
PB-2	132	130+99.00	137+50.00	RT																	651	
TP-1	132	125+00.00	127+45.00	RT														717				
WIA-1	132	130+99.00	130+99.00	RT	1																	
PB-1	133	137+50.00	150+00.00	RT																		1250
WEY-1	133	137+50.00	150+00.00	RT							1250	1250										
WEY-2	133	139+67.00	142+13.00	RT							246	246										
WEY-3	133	144+13.00	146+30.00	LT							217	217										
WEW-1	133	137+50.00	142+13.00	RT				463	463													
WEW-2	133	139+72.00	150+00.00	RT				1028	1028													
WEW-3	133	144+19.00	150+00.00	LT				581	581													
WCH-1	133	142+13.00	147+46.00	RT											533	533						
WCH-2	133	142+13.00	147+46.00	RT											533	533						
WCH-3	133	146+30.00	148+55.00	LT											225	225						
WCH-4	133	146+30.00	148+55.00	LT											225	225						
WDW-1	133	147+46.00	150+00.00	RT											254		254					
WDW-2	133	148+55.00	150+00.00	LT											145		145					
WEW-1	134	150+00.00	175+00.00	RT				2500	2500													
WEW-2	134	150+00.00	154+30.00	LT				430	430													
WEY-1	134	150+00.00	175+00.00	RT						2500	2500											
WDW-1	134	150+00.00	151+86.00	RT											186		186					
TOTALS CARRIED TO SHEET					69	2	7	154			10566	10566	8108	8108	3796	3796	2396		3024	1	4020	

CALCULATED TB CHECKED DRJ

MAINTENANCE OF TRAFFIC SUBSUMMARY

LUC-475-01.85

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855

REF NO.	SHEET NO.	STATION TO STATION		SIDE	614	614	614	614	614	614	614	614	614	614	614	614	614	615	622	622	622	CALCULATED TB CHECKED DRJ
					WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDOUS, UNIDIRECTIONAL	MAINTAINING TRAFFIC, MISC.: 6"X8" SOLID WOOD POST, AS PER PLAN	MAINTAINING TRAFFIC, MISC.: SIGN (FLAT SHEET, PLYWOOD)	WORK ZONE LANE LINE, CLASS I, 4"	WORK ZONE CENTER LINE, CLASS I	WORK ZONE EDGE LINE, CLASS I, 6" (WHITE)	WORK ZONE EDGE LINE, CLASS I, 6", 807 PAINT(WHITE)	WORK ZONE EDGE LINE, CLASS I, 6"(YELLOW)	WORK ZONE EDGE LINE, CLASS I, 6", 807 PAINT(YELLOW)	WORK ZONE CHANNELIZING LINE, CLASS I, 12"	WORK ZONE CHANNELIZING LINE, CLASS I, 12", 807 PAINT	WORK ZONE DOTTED LINE, CLASS I(WHITE)	WORK ZONE DOTTED LINE, CLASS I(YELLOW)	WORK ZONE DOTTED LINE, CLASS I, 6", 807 PAINT(WHITE)	WORK ZONE LANE LINE, CLASS I, 6", 807 PAINT	WORK ZONE STOP LINE, CLASS I	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN	
EACH				EACH	SF	MILE	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	MILE	FT	SY	FT	EACH	FT
PID 99731 PHASE 1A CONTINUED																						
WCH-1	141	150+00.00	150+71.00	RT											71	71						
WCH-2	141	150+00.00	150+71.00	RT											71	71						
WDW-1	141	150+71.00	157+31.00	RT													660		660			
WEY-1	141	150+00.00	175+00.00	RT																		
WEW-1	141	150+00.00	175+00.00	RT					2500	2500												
PB-1	141	150+00.00	175+00.00	RT																	2500	
PB-2	141	173+42.00	175+00.00	RT																	158	
PB-1	142	175+00.00	175+80.00	RT																	80	
WEW-1	142	175+00.00	175+80.00	RT				80	80													
WEY-1	142	175+00.00	175+80.00	RT						80	80											
PID 99731 PHASE 2																						
WEW-1	143	62+89.00	75+00.00	RT				1211	1211													
WDW-1	143	62+89.00	74+19.00	RT													1130		1130			
WCH-1	143	74+19.00	75+00.00	RT										81	81							
WCH-2	143	74+19.00	75+00.00	RT										81	81							
PB-1	144	87+53.00	100+00.00	LT																		1247
WCH-1	144	75+00.00	79+52.00	RT										452	452							
WCH-2	144	75+00.00	79+52.00	RT										452	452							
WEY-1	144	79+52.00	100+00.00	RT						2048	2048											
WEW-1	144	75+00.00	100+00.00	RT				2500	2500													
WEW-2	144	87+53.00	100+00.00	LT				1247	1247													
WDW-1	144	87+53.00	100+00.00	LT												1247		1247				
TP-1	144	88+73.00	100+00.00	LT															1185			
WEY-1	145	100+00.00	125+00.00	RT						2500	2500											
WEY-2	145	106+69.00	125+00.00	LT						1831	1831											
WEW-1	145	100+00.00	125+00.00	RT				2500	2500													
WEW-2	145	100+00.00	125+00.00	LT				2500	2500													
WDW-1	145	100+00.00	105+33.00	RT												533		533				
WDW-2	145	114+96.00	120+46.00	LT												550		550				
WCH-1	145	105+33.00	106+69.00	RT										136	136							
WCH-2	145	105+33.00	106+69.00	RT										136	136							
WCH-3	145	120+46.00	125+00.00	LT										454	454							
WCH-4	145	120+46.00	125+00.00	LT										454	454							
PB-1	145	100+00.00	125+00.00	LT																		2500
PB-2	145	106+81.00	125+00.00	LT																	1	1819
TP-1	145	100+00.00	108+61.00	LT															1357			
PB-1	146	125+00.00	125+29.00	LT																		29
PB-2	146	125+00.00	137+50.00	LT																		1250
WIA-1	146	125+29.00		LT	1																	
WCH-1	146	125+00.00	126+41.00	LT											141	141						
WCH-2	146	125+00.00	126+41.00	LT											141	141						
WEY-1	146	126+41.00	130+25.00	LT						384	384											
WEY-2	146	126+41.00	137+50.00	LT						1109	1109											
WEY-3	146	125+00.00	126+86.00	RT						186	186											
WEY-4	146	130+64.00	137+50.00	RT						686	686											
WEW-1	146	125+00.00	130+18.00	LT				518	518													
WEW-2	146	125+41.00	137+50.00	LT				1209	1209													
WEW-3	146	125+00.00	126+73.00	RT				173	173													
WEW-4	146	130+64.00	137+50.00	RT				686	686													
TOTALS CARRIED TO SHEET																						
				69	1				15124	15124	11324	11324	2670	2670	4120		4120		2542		1	9583

C:\Users\sgault\OneDrive - ARCADIS\Desk\top\LUC475\TOHOD\TO.PE0\Drawing\99731\Design\MOT\Sheets\99731_MS004.dgn Sheet 4/19/2022 3:06:08 PM sgault

REF NO.	SHEET NO.	STATION TO STATION		SIDE	614		614		614		614		614		614		614		614		615		622		622		622		
					EACH	EACH	SF	MILE	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT
PID 99731 PHASE 2 CONTINUED																													
WEY-1	147	137+50.00	140+49.00	LT							463	463																	
WEY-2	147	137+50.00	139+02.00	RT							152	152																	
WEY-3	147	138+73.00	142+13.00	RT							340	340																	
WEY-4	147	140+13.00	150+00.00	LT							987	987																	
WEW-1	147	137+50.00	140+33.00	LT					463	463																			
WEW-2	147	137+50.00	141+12.00	RT					362	362																			
WEW-3	147	138+79.00	150+00.00	RT					1121	1121																			
WEW-4	147	140+82.00	150+00.00	LT					918	918																			
WCH-1	147	139+02.00	150+00.00	RT									1098	1098															
WCH-2	147	139+02.00	150+00.00	RT									1098	1098															
WCH-3	147	142+13.00	147+46.00	RT									533	533															
WCH-4	147	142+13.00	147+46.00	RT									533	533															
WDW-1	147	147+46.00	150+00.00	RT											254		254												
TP-1	147	140+50.00	143+74.00	LT																	554								
PB-1	147	137+50.00	139+82.00	LT																							245		
PB-2	147	140+82.00	150+00.00	LT																							918		
PB-3	147	144+80.00	150+00.00	LT																							520		
WIA-1	147	139+82.00		LT	1																								
PB-1	148	150+00.00	151+76.00	LT																						1	176		
PB-2	148	150+00.00	157+74.00	LT																							774		
WDW-1	148	150+00.00	151+86.00	RT											186		186												
SN-8	148	151+00.00		RT		1	28																						
WDW-2	148	151+86.00	164+22.00	RT											1236		1236												
WDW-3	148	153+54.00	157+74.00	RT											420		420												
WIA-1	148	151+76.00		LT	1																								
WCH-1	148	150+00.00	151+86.00	RT									186	186															
WCH-2	148	150+00.00	151+86.00	RT									186	186															
WCH-3	148	151+78.00	153+54.00	RT									176	176															
WCH-4	148	151+78.00	153+54.00	RT									176	176															
WEY-1	148	150+00.00	151+78.00	LT							178	178																	
WEW-1	148	150+00.00	157+74.00	LT					774	774																			
WEW-2	148	150+00.00	164+22.00	RT					1422	1422																			
TP-1	148	150+26.00	157+74.00	LT																	966								
SN-9	148	158+00.00		RT			6																						
SN-10	148	158+00.00		RT		2	36																						
SN-11	148	170+00.00		RT			6																						
SN-12	148	170+00.00		RT		2	32																						
SN-13	148	198+00.00		RT			6																						
SN-14	148	198+00.00		RT		2	32																						
PID 99731 PHASE 2A																													
WEW-1	149	91+87.00	99+07.00	RT							720	720																	
TP-1	150	107+08.00	114+01.00	LT																									
WDW-1	150	105+33.00	109+61.00	LT																	1030								
WDW-2	150	115+44.00	122+04.00	LT											428		428												
WEW-1	150	105+33.00	125+00.00	LT					1967	1967																			
WEW-2	150	123+79.00	125+00.00	LT					121	121																			
WCH-1	150	109+61.00	111+36.00	RT									175	175															
WCH-2	150	109+61.00	111+36.00	RT									175	175															
WCH-3	150	122+04.00	123+79.00	LT									175	175															
WCH-4	150	122+04.00	123+79.00	LT									175	175															
PB-1	150	104+00.00	125+00.00	LT																							2100		
PB-2	150	111+08.00	125+00.00	LT																						1	1392		
PB-3	150	123+79.00	125+00.00	LT																					1	121			
PB-4	150	123+79.00	125+00.00	LT																							121		
WEY-1	150	111+36.00	125+00.00	LT									1364	1364															
WEY-2	150	123+79.00	125+00.00	LT									121	121															
TOTALS CARRIED TO SHEET	69	2	7	146							7868	7868	3605	3605	4686	4686	3184		3184						2550			3	6367

CALCULATED
 MAK
 CHECKED
 DRJ
LUC-475-01.85
 66
 855

REF NO.	SHEET NO.	STATION TO STATION		SIDE	614	614	614	614	614	614	614	614	614	614	614	614	614	614	615	622	622	622		
					WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDOUS UNIDIRECTIONAL	MAINTAINING TRAFFIC, MISC.: 6"x8" SOLID WOOD POST, AS PER PLAN	MAINTAINING TRAFFIC, MISC.: SIGN (FLAT SHEET, PLYWOOD)	WORK ZONE LANE LINE, CLASS I, 4"	WORK ZONE CENTER LINE, CLASS I	WORK ZONE EDGE LINE, CLASS I, 6" (WHITE)	WORK ZONE EDGE LINE, CLASS I, 6", 807 PAINT(WHITE)	WORK ZONE EDGE LINE, CLASS I, 6"(YELLOW)	WORK ZONE EDGE LINE, CLASS I, 6", 807 PAINT(YELLOW)	WORK ZONE CHANNELIZING LINE, CLASS I, 12"	WORK ZONE CHANNELIZING LINE, CLASS I, 12", 807 PAINT	WORK ZONE DOTTED LINE, CLASS I(WHITE)	WORK ZONE DOTTED LINE, CLASS I(YELLOW)	WORK ZONE DOTTED LINE, CLASS I, 6", 807 PAINT(WHITE)	WORK ZONE LANE LINE, CLASS I, 6", 807 PAINT	WORK ZONE STOP LINE, CLASS I	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN	PORTABLE BARRIER, 50", AS PER PLAN	PORTABLE BARRIER, "Y" CONNECTOR	PORTABLE BARRIER, UNANCHORED
PID 99731 PHASE 2A CONTINUED					EACH	EACH	SF	MILE	FT	FT	FT	FT	FT	FT	FT	FT	FT	MILE	FT	SY	FT	EACH	FT	
PB-1	151	125+00.00	131+00.00	LT																			600	
PB-2	151	125+00.00	127+90.00	LT																			290	
PB-3	151	125+00.00	133+69.00	LT																			869	
PB-4	151	125+00.00	133+37.00	LT																			837	
WEY-1	151	125+00.00	132+18.00	LT																				
WEY-2	151	125+00.00	136+84.00	LT									718	718										
WEW-1	151	125+00.00	132+11.00	LT						711	711													
WEW-2	151	125+00.00	136+84.00	LT						1184	1184													
TP-1	151	126+61.00	130+29.00	LT																542				
WIA-1	151	127+90.00		LT	1																			
WIA-2	151	131+00.00		LT	1																			
WIA-3	151	133+37.00		LT	1																			
WEW-1	152	141+75.00	150+00.00	LT							825	825												
WEY-1	152	141+58.00	150+00.00	LT									842	842										
PB-1	152	141+85.00	150+00.00	LT																			815	
WEY-1	153	150+00.00	156+34.00	LT									634	634										
PB-1	153	151+43.00	162+29.00	LT																	1		1086	
PB-2	153	150+00.00	156+34.00	LT																			634	
WEW-1	153	150+00.00	162+29.00	LT						1229	1229													
WIA-1	153	156+34.00		RT	1																			
WCH-1	153	156+34.00	158+09.00	RT																				
WCH-2	153	156+34.00	158+09.00	RT									175	175										
TP-1	153	154+88.00	162+29.00	LT																			961	
WDW-1	153	158+09.00	162+29.00	LT												420		420						
PID 99731 US 20A PHASE 3																								
WCH-1	192	536+70.00	537+65.00	LT																				
WEW-1	192	536+70.00	540+00.00	LT							330													
WEW-2	192	536+70.00	540+00.00	RT							330													
WCL-1	192	537+65.00	540+00.00	LT						235														
WCL-2	192	536+70.00	540+00.00	LT & CL						330														
WEW-1	193	540+00.00	542+00.00	LT							200													
WEW-2	193	540+00.00	542+22.00	LT & RT							222													
WEW-3	193	542+57.00	550+00.00	LT & RT							743													
WEW-4	193	542+68.00	550+00.00	LT							732													
WCL-1	193	540+00.00	541+50.00	LT						150														
WCL-2	193	540+00.00	540+70.00	LT						70														
WCL-3	193	543+35.00	550+00.00	LT						665														
WS-1	193	541+50.00		LT																		11		
WS-2	193	542+33.00		RT																		12		
WS-3	193	542+45.00		RT																		12		
WS-4	193	543+35.00		LT																		12		
WIA-1	193	543+88.00		LT	1																			
PB-1	193	543+88.00	550+00.00	LT																			612	
WEW-1	194	550+00.00	560+00.00	LT & RT							1000													
WEW-2	194	550+00.00	560+00.00	LT & RT							1000													
WCL-1	194	550+00.00	560+00.00	LT & RT						1000														
PB-1	194	550+00.00	552+75.00	LT & RT																			275	
PB-2	194	556+98.00	560+00.00	LT & RT																			302	
WIA-1	194	552+75.00		RT	1																			
WIA-2	194	556+98.00		RT	1																			
TOTALS CARRIED TO SHEET					69	7				2450	8506	3949	3378	3378	445	350	420		420		47	1503	1	6320

MAINTENANCE OF TRAFFIC SUBSUMMARY

LUC-475-01.85

CALCULATED TB CHECKED DRJ

REF NO.	SHEET NO.	STATION TO STATION	SIDE	614																	615	622	622	622
				614	614	614	614	614	614	614	614	614	614	614	614	614	614	614	614	614	615	622	622	622
PID 99731 US 20A PHASE 3				WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS (UNIDIRECTIONAL)	MAINTAINING TRAFFIC, MISC.: 6"X8" SOLID WOOD POST, AS PER PLAN	MAINTAINING TRAFFIC, MISC.: SIGN (FLAT SHEET, PLYWOOD)	WORK ZONE LANE LINE, CLASS I, 4"	WORK ZONE CENTER LINE, CLASS I	WORK ZONE EDGE LINE, CLASS I, 6" (WHITE)	WORK ZONE EDGE LINE, CLASS I, 6" (WHITE)	WORK ZONE EDGE LINE, CLASS I, 6" (YELLOW)	WORK ZONE EDGE LINE, CLASS I, 6", 807 PAINT(YELLOW)	WORK ZONE CHANNELIZING LINE, CLASS I, 12"	WORK ZONE CHANNELIZING LINE, CLASS I, 12", 807 PAINT	WORK ZONE DOTTED LINE, CLASS I(WHITE)	WORK ZONE DOTTED LINE, CLASS I, 6", 807 PAINT(YELLOW)	WORK ZONE DOTTED LINE, CLASS I, 6", 807 PAINT(WHITE)	WORK ZONE LANE LINE, CLASS I, 6", 807 PAINT	WORK ZONE STOP LINE, CLASS I	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN	PORTABLE BARRIER, 50", AS PER PLAN	PORTABLE BARRIER, "Y" CONNECTOR	PORTABLE BARRIER, UNANCHORED	
				EACH	EACH	SF	MILE	FT	FT	FT	FT	FT	FT	FT	FT	FT	MILE	FT	SY	FT	EACH	FT		
PB-1	195	560+00.00	566+75.00	LT																				
WEW-1	195	560+00.00	570+00.00	LT					1000															
WEW-2	195	560+00.00	570+00.00	LT					1000															
WCL-1	195	560+00.00	570+00.00	LT				1000																
WIA-1	195	566+75.00		LT	1																			
WEW-1	196	570+00.00	580+00.00	LT					1000															
WEW-2	196	570+00.00	577+15.00	LT & RT					715															
WEW-3	196	577+54.00	580+00.00	RT					246															
WCL-1	196	570+00.00	577+00.00	LT				700																
WCL-2	196	577+65.00	580+00.00	LT				235																
WCL-3	196	578+95.00	580+00.00	LT				105																
WDY-1	196	577+00.00	577+65.00	LT												65								
WS-1	196	577+30.00	577+41.00	RT														12						
WS-2	196	577+39.00	577+60.00	RT														21						
WCH-1	196	577+39.00	577+41.00	RT							25													
WDW-1	196	578+45.00	580+00.00	LT												155								
WL-1	196	578+95.00	580+00.00	RT			105																	
WEW-1	197	580+00.00	583+45.00	LT					345															
WEW-2	197	580+00.00	583+95.00	RT					395															
WDW-1	197	580+00.00	583+45.00	LT												345								
WCL-1	197	580+00.00	584+55.00	LT				455																
WCL-2	197	580+00.00	584+55.00	LT & RT				455																
WL-1	197	580+00.00	585+35.00	RT																				
WL-2	197	583+45.00	585+35.00	LT			535																	
							190																	
TOTALS CARRIED TO SHEET				69	1		830	2950	4701					25		500	65		33					675

CALCULATED TB CHECKED DRJ
LUC-475-01.85
 68
 855

MAINTENANCE OF TRAFFIC SUBSUMMARY



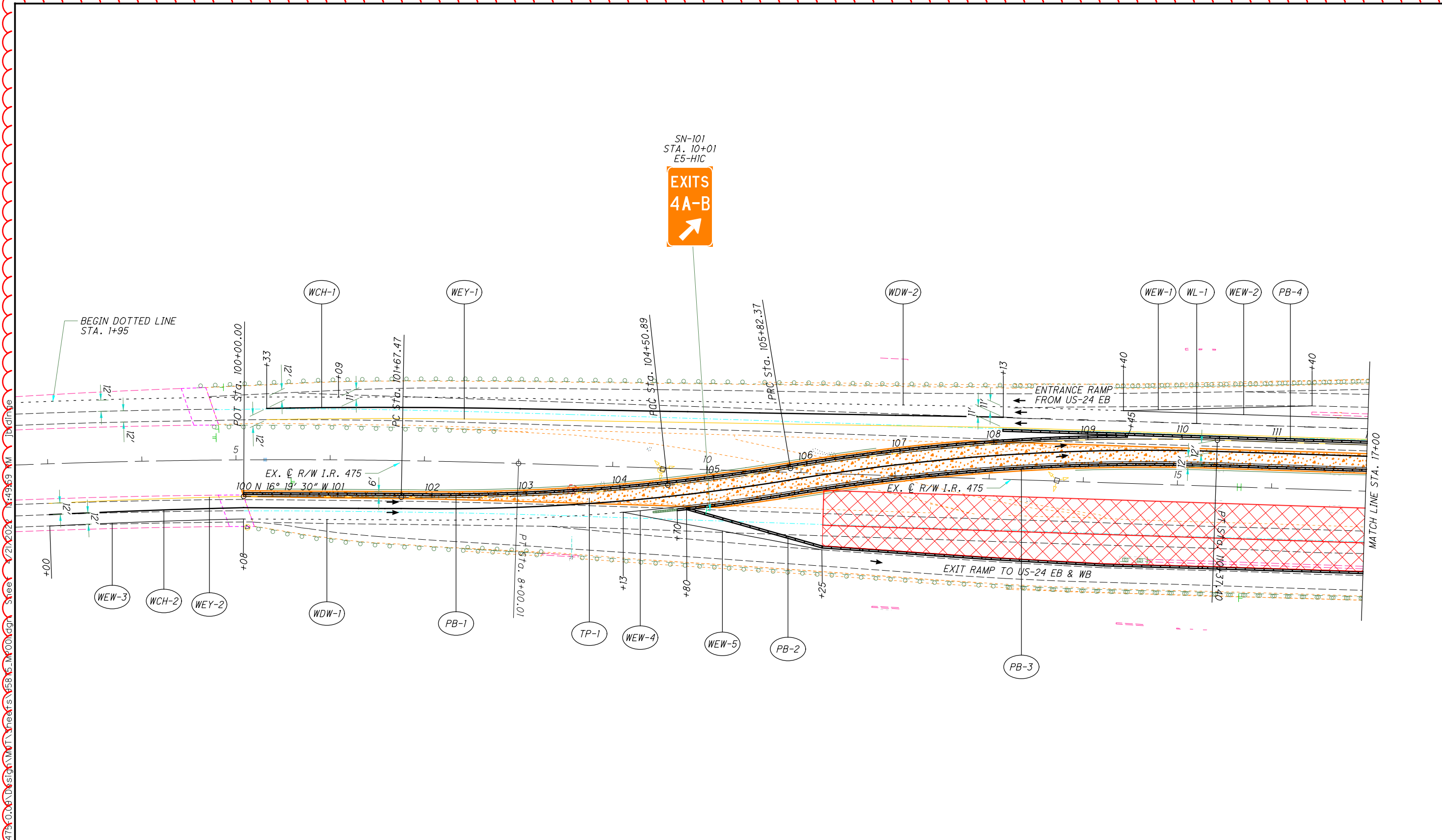
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MK
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DEK

**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 5+00 TO STA. 17+50**

LUC-475-0.09

70
855

SN-101
STA. 10+01
E5-HIC



LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

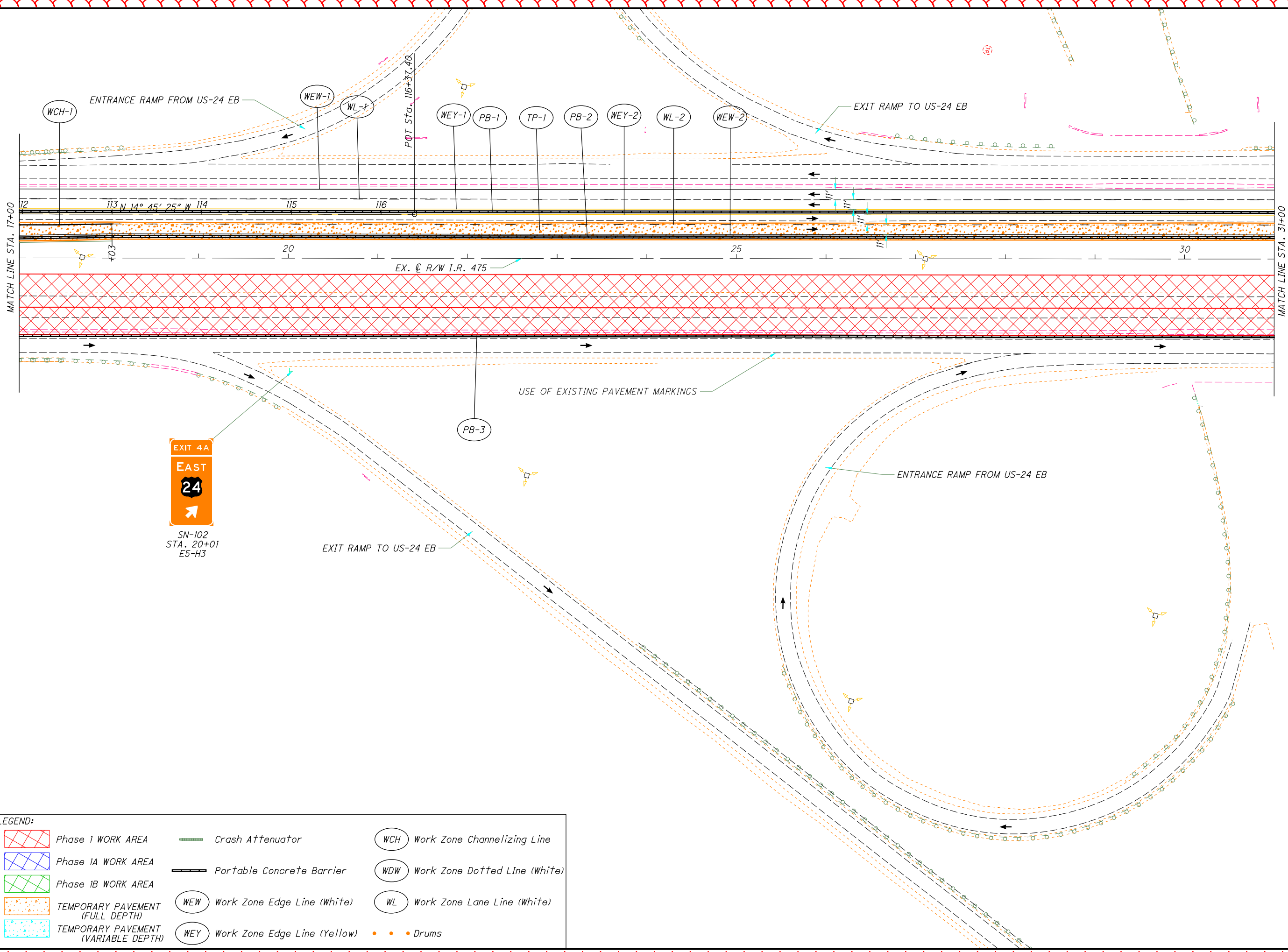


CALCULATED MK
CHECKED DEK

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 17+50 TO STA. 30+91.88

LUC-475-0.09

71
855

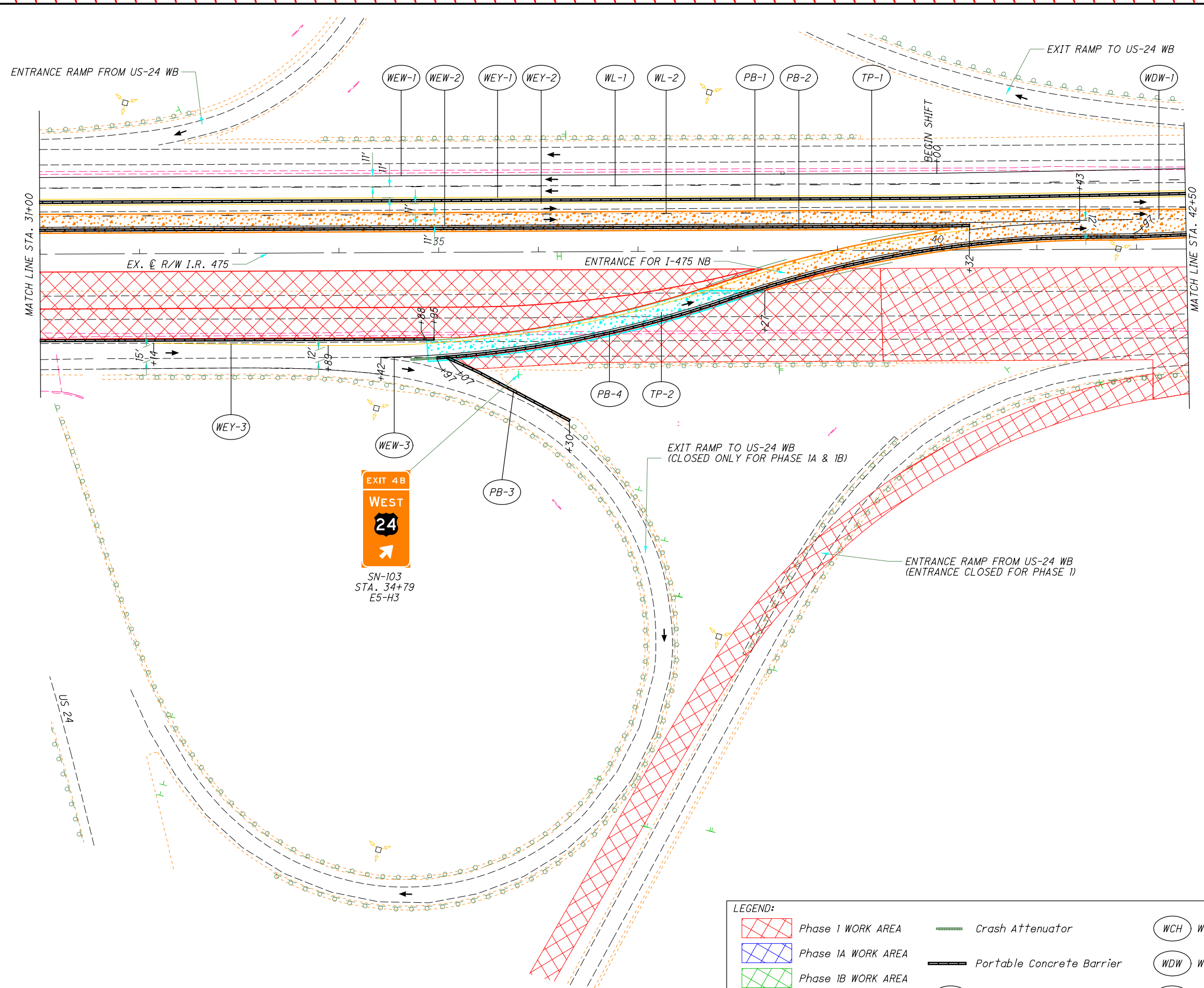


LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

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EXIT 4B
WEST
24
 SN-103
 STA. 34+79
 E5-H3

LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

0 50 100
 HORIZONTAL SCALE IN FEET

CALCULATED MK
 CHECKED DEK

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 31+91.88 TO STA. 42+50

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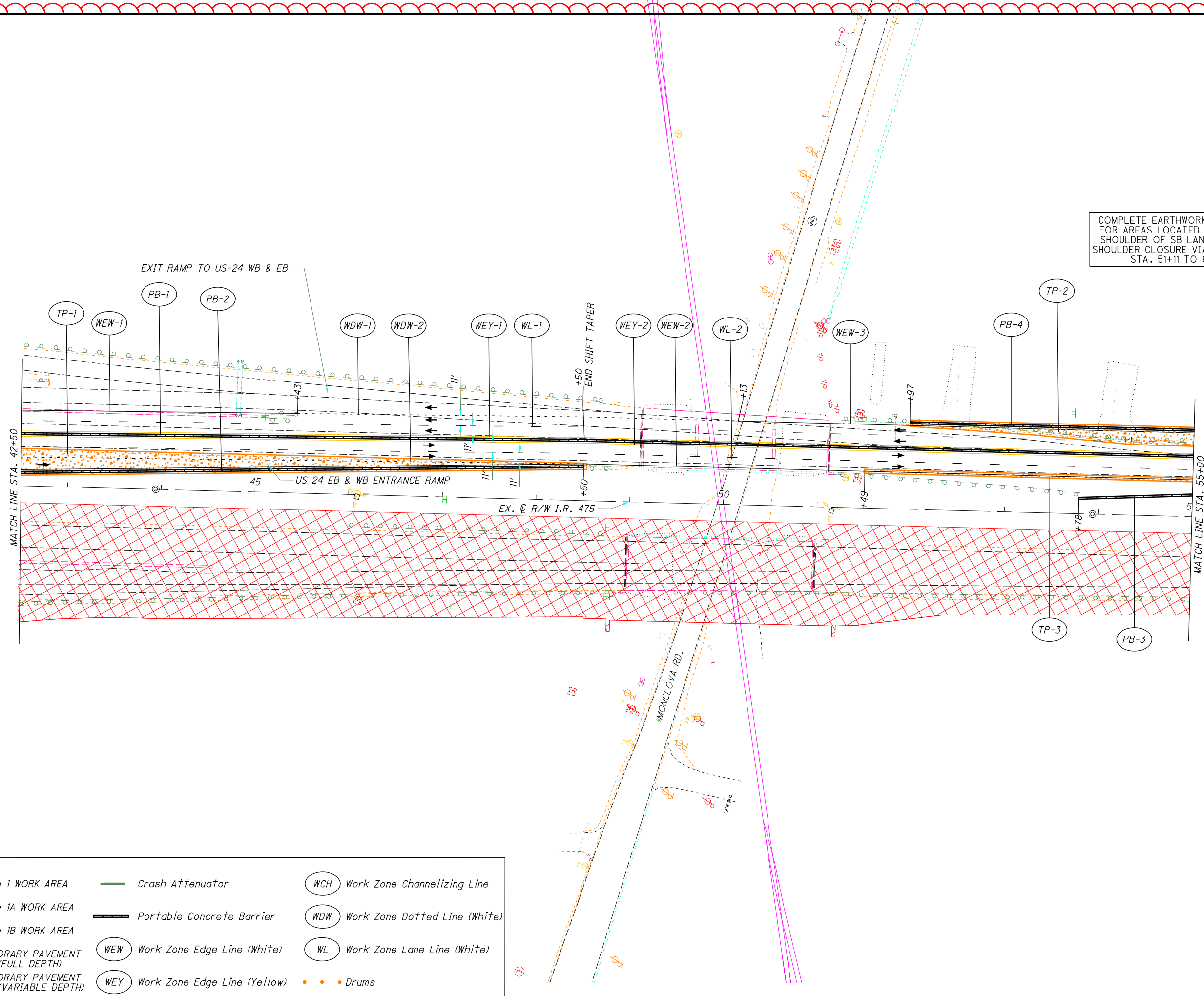
CALCULATED
MK
CHECKED
DEK

**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 42+50 TO STA. 55+00**

LUC-475-0.09

73
855

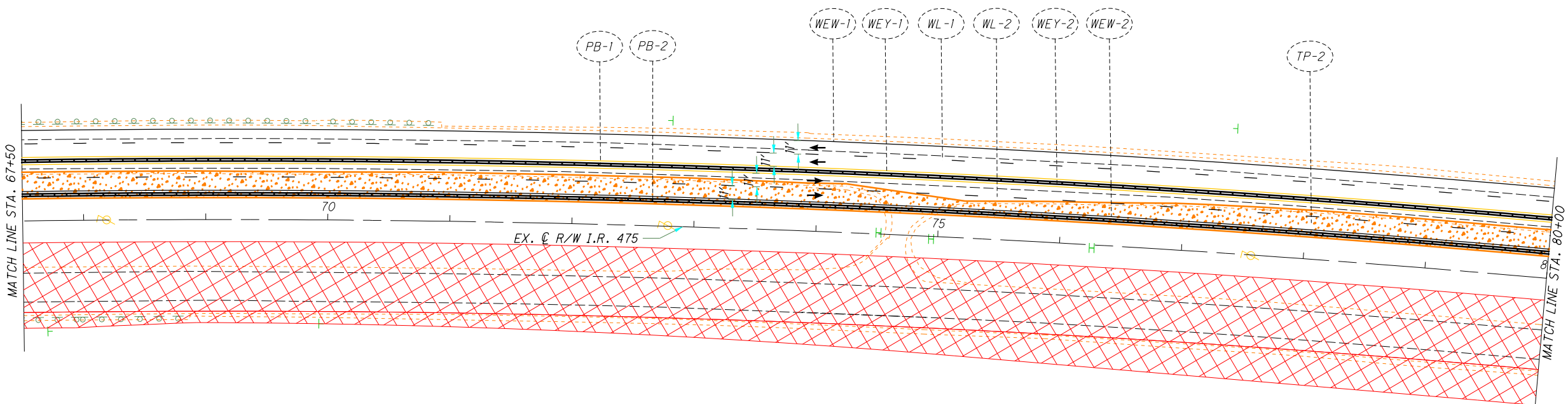
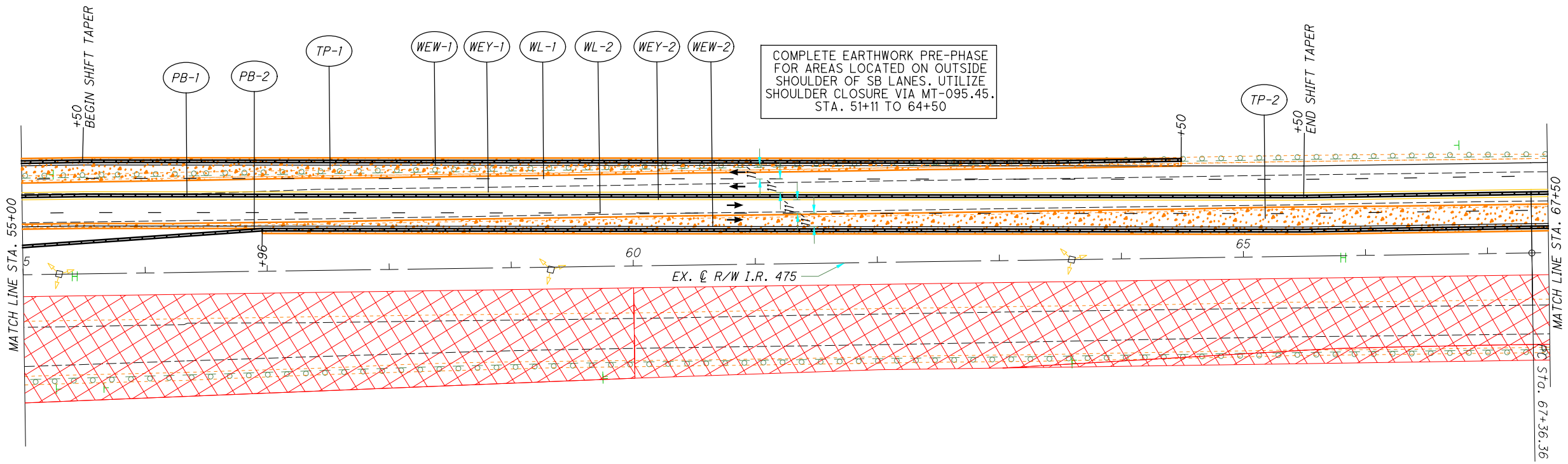
COMPLETE EARTHWORK PRE-PHASE FOR AREAS LOCATED ON OUTSIDE SHOULDER OF SB LANES. UTILIZE SHOULDER CLOSURE VIA MT-095.45. STA. 51+11 TO 64+50



LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		WCH Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		WDW Work Zone Dotted Line (White)
	Phase 1B WORK AREA		WEW Work Zone Edge Line (White)		WL Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		WEY Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

COMPLETE EARTHWORK PRE-PHASE FOR AREAS LOCATED ON OUTSIDE SHOULDER OF SB LANES. UTILIZE SHOULDER CLOSURE VIA MT-095.45. STA. 51+11 TO 64+50



LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

CALCULATED MK
CHECKED DEK

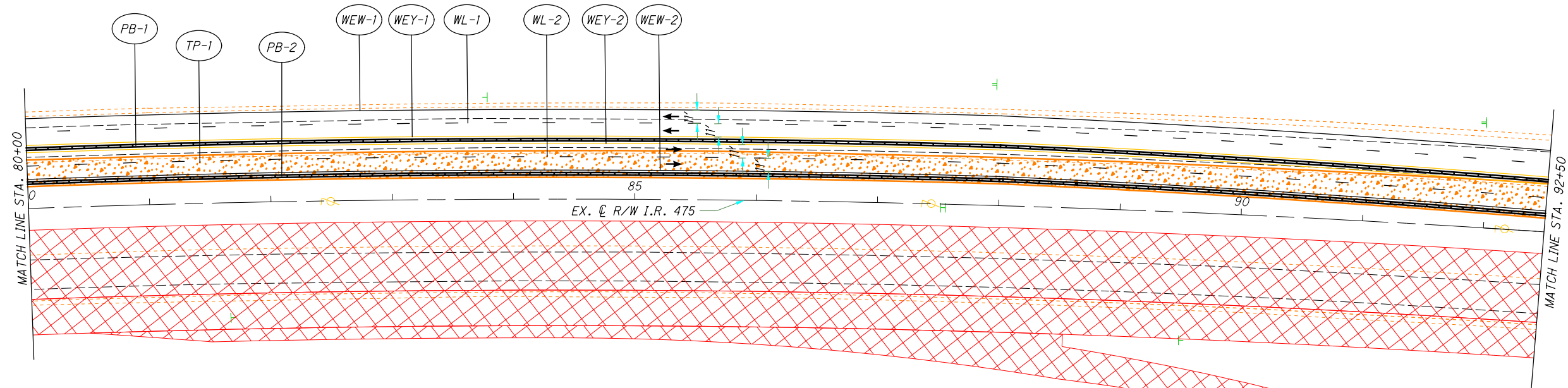
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HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 55+00 TO STA. 80+00

LUC-475-0.09

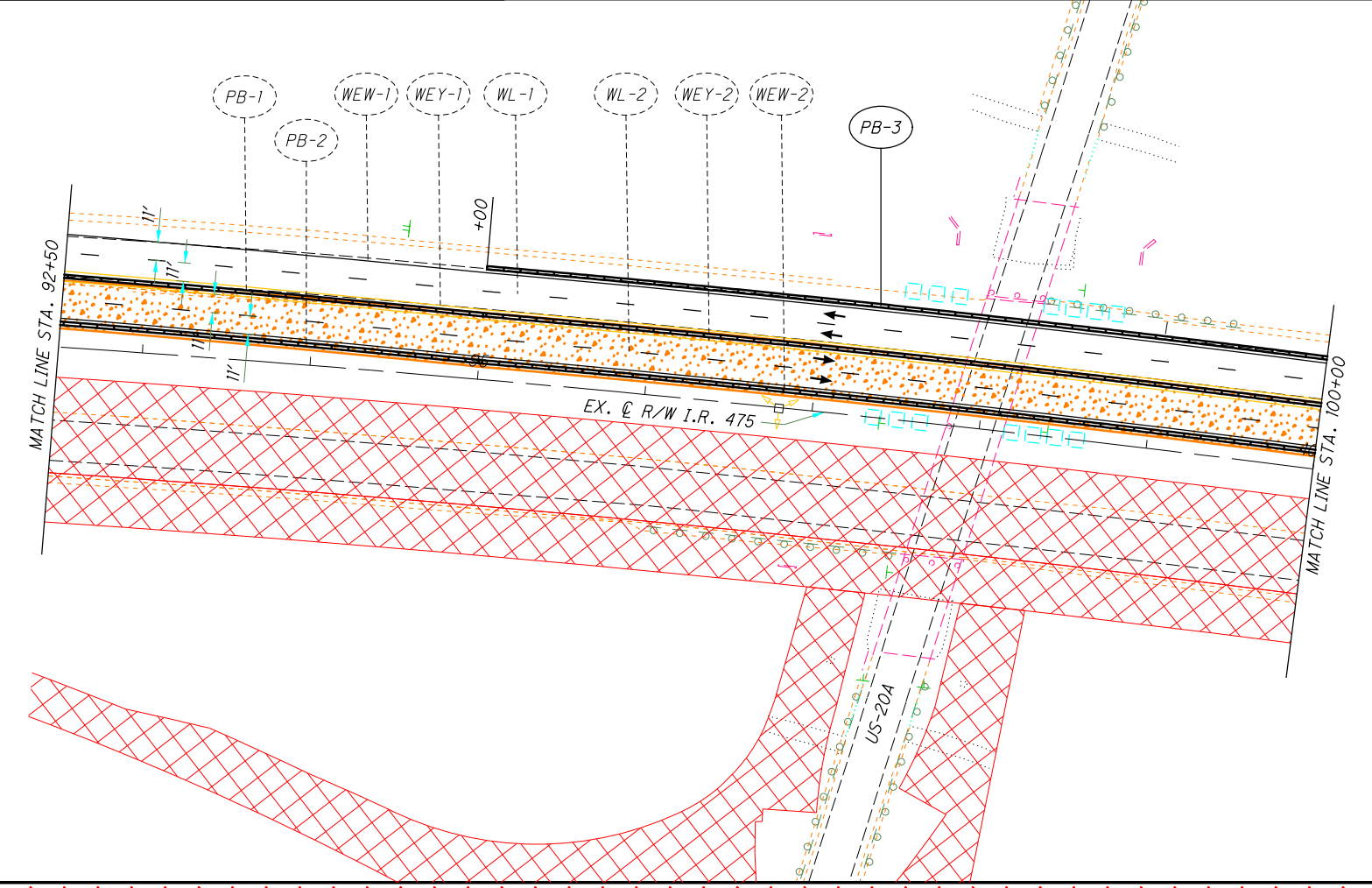
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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		WCH Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		WDW Work Zone Dotted Line (White)
	Phase 1B WORK AREA		WEW Work Zone Edge Line (White)		WL Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		WEY Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

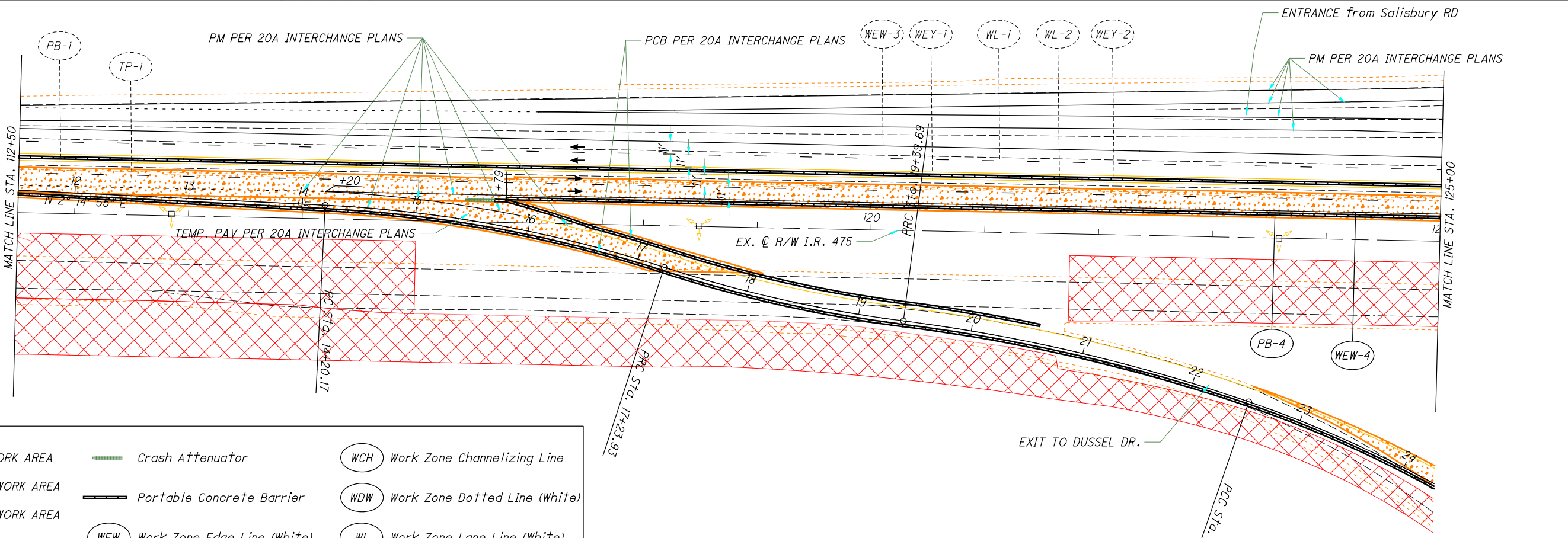
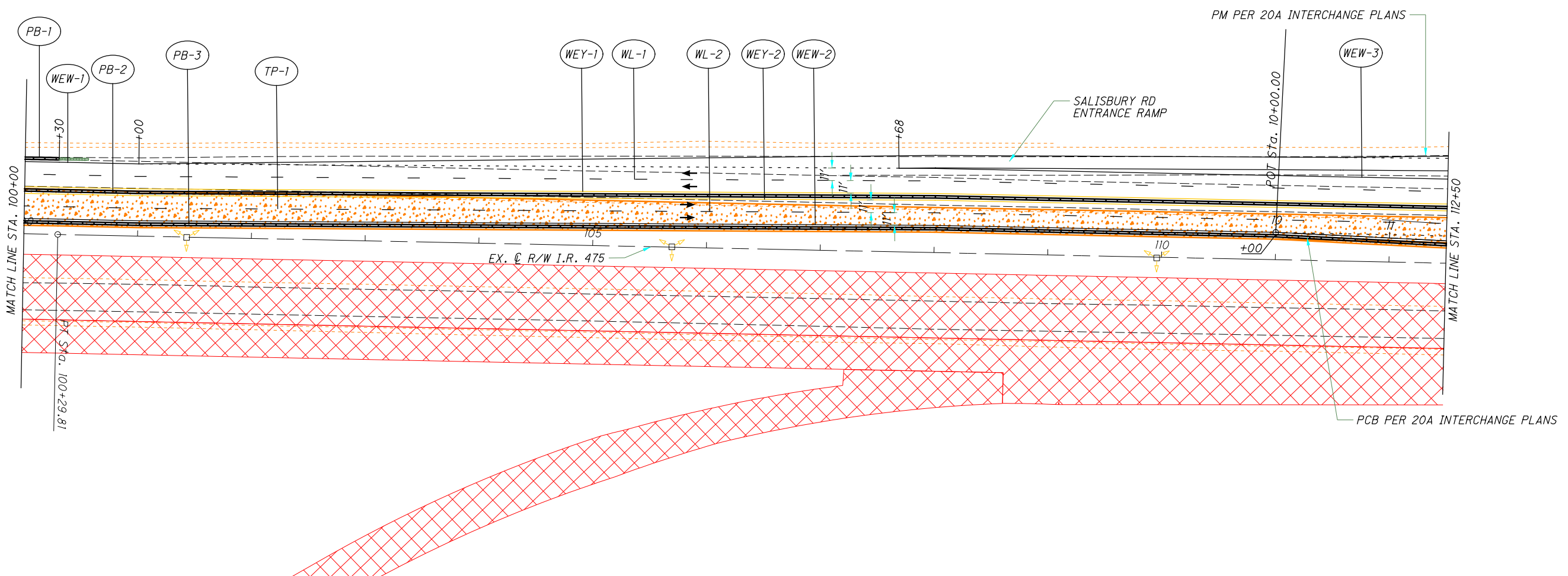


CALCULATED MK
 CHECKED DEK
 HORIZONTAL SCALE IN FEET
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MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 80+00 TO STA. 100+00

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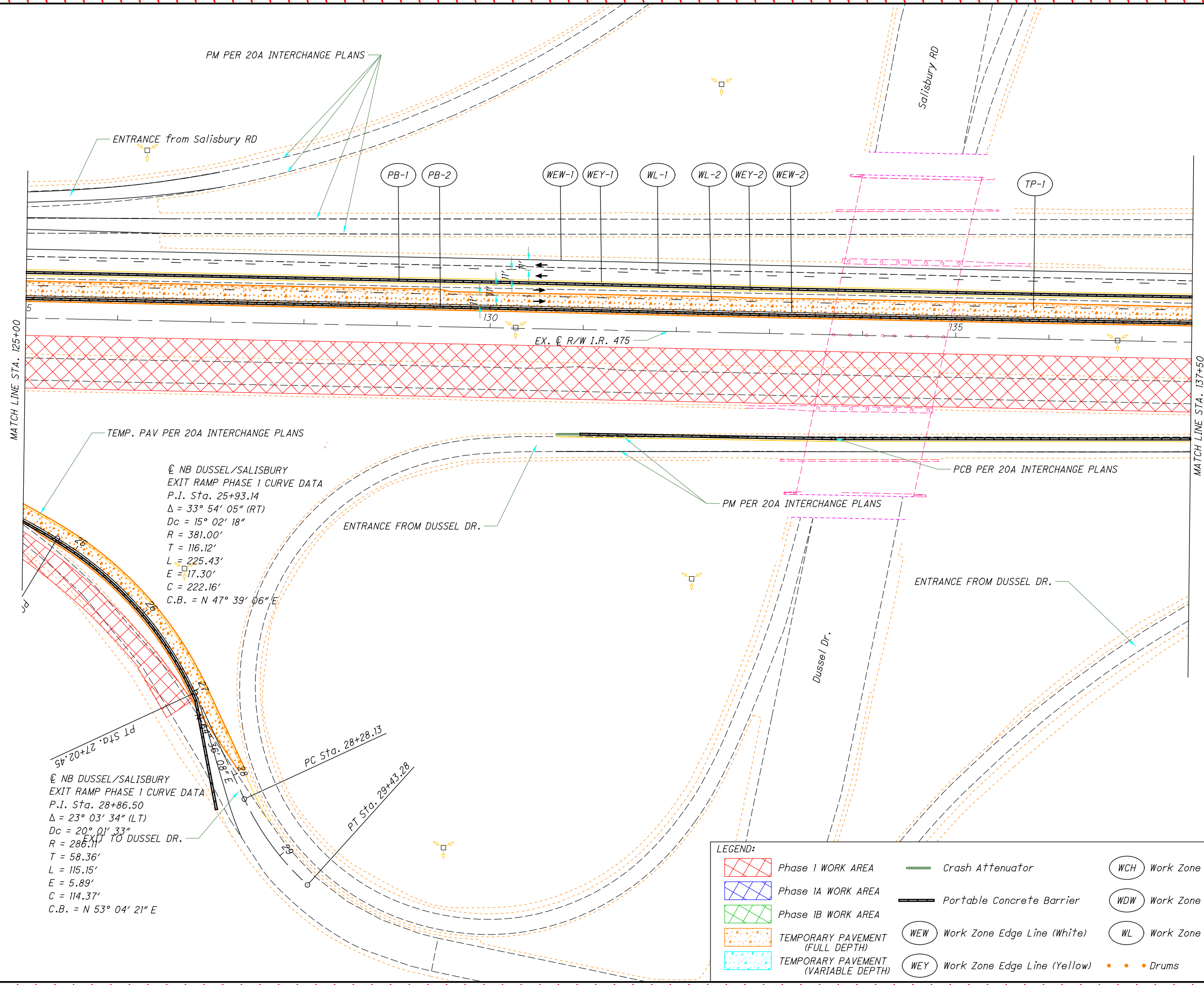
	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

0 50 100
 HORIZONTAL SCALE IN FEET
 CALCULATED MK
 CHECKED DEK

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 100+00 TO STA. 125+00

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NB DUSSEL/SALISBURY
 EXIT RAMP PHASE 1 CURVE DATA
 P.I. Sta. 25+93.14
 $\Delta = 33^\circ 54' 05''$ (RT)
 $Dc = 15^\circ 02' 18''$
 $R = 381.00'$
 $T = 116.12'$
 $L = 225.43'$
 $E = 17.30'$
 $C = 222.16'$
 $C.B. = N 47^\circ 39' 06'' E$

NB DUSSEL/SALISBURY
 EXIT RAMP PHASE 1 CURVE DATA
 P.I. Sta. 28+86.50
 $\Delta = 23^\circ 03' 34''$ (LT)
 $Dc = 20^\circ 01' 33''$
 $R = 286.11'$
 $T = 58.36'$
 $L = 115.15'$
 $E = 5.89'$
 $C = 114.37'$
 $C.B. = N 53^\circ 04' 21'' E$

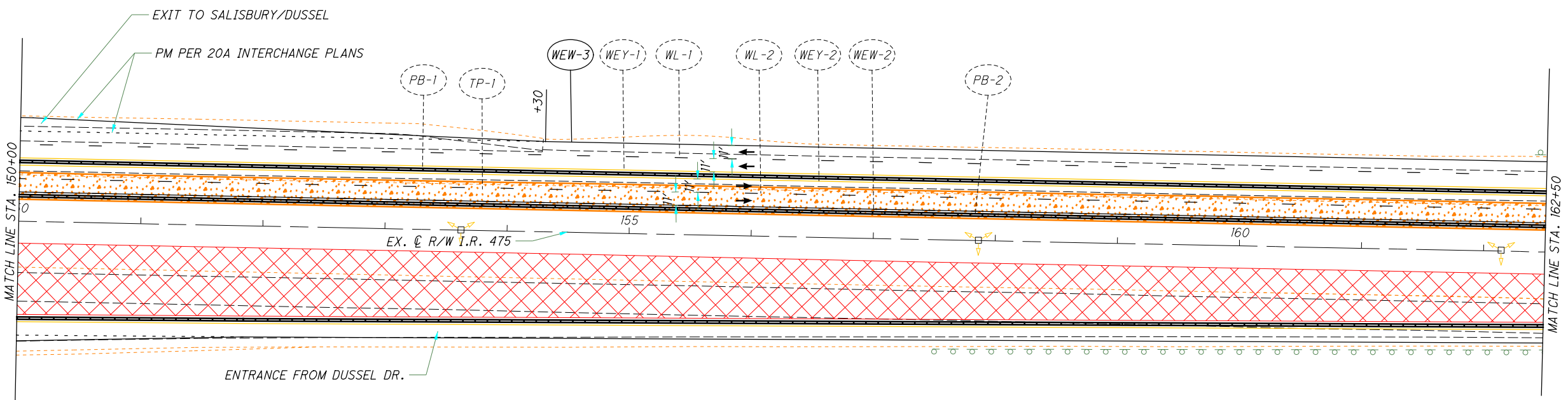
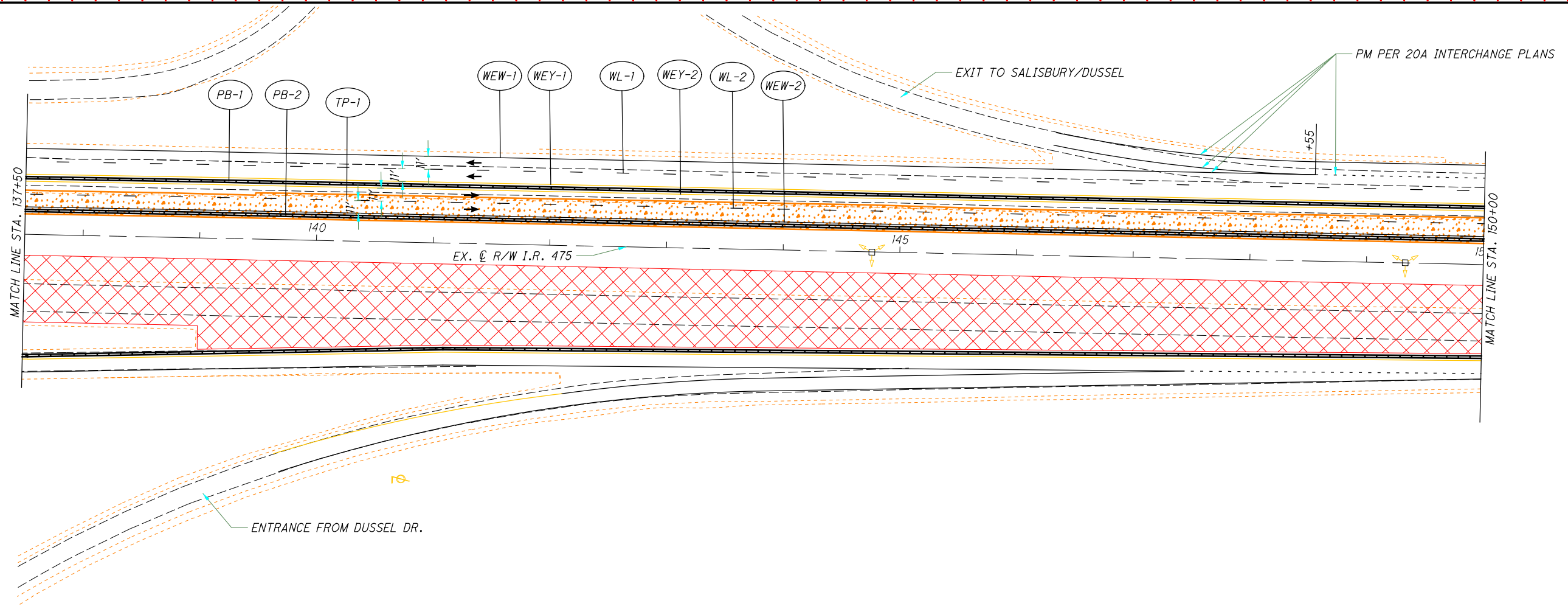
LEGEND:					
	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

CALCULATED MK
 CHECKED DEK

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 125+00 TO STA. 137+50

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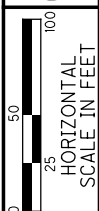
	Phase I WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase IA WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase IB WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

CALCULATED
MK
CHECKED
DEK

0 50 100
HORIZONTAL SCALE IN FEET

**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 137+50 TO STA. 162+50**

LUC-475-0.09

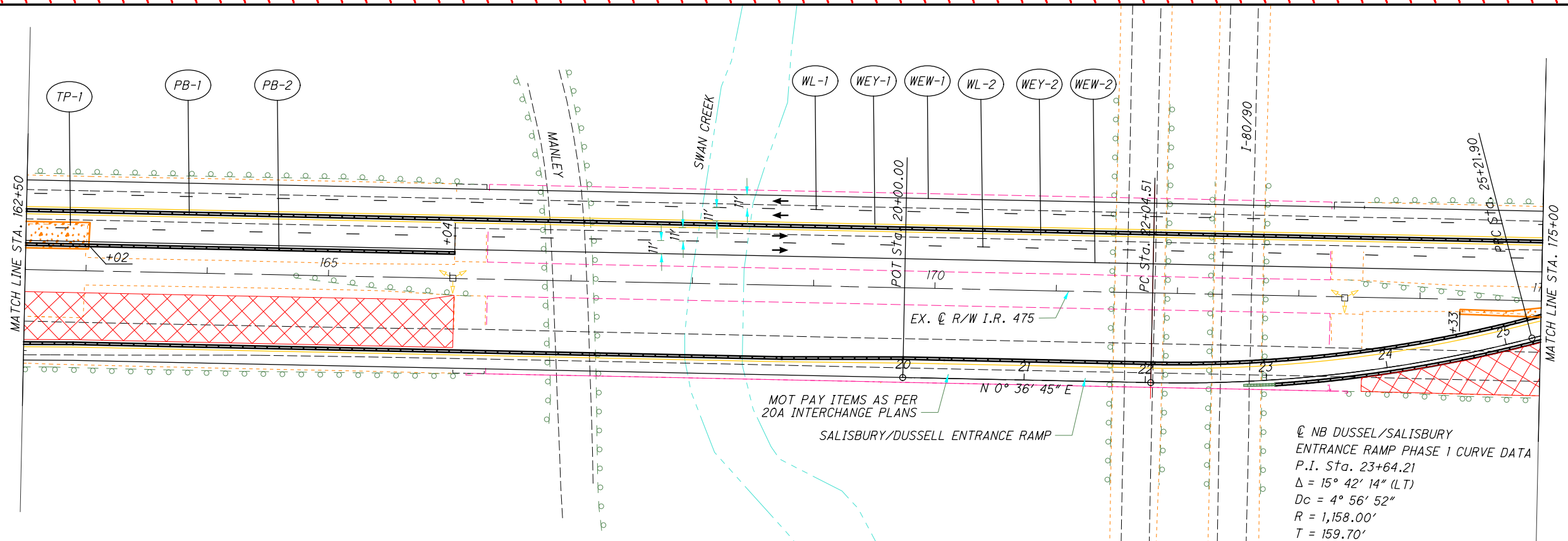


CALCULATED MK
CHECKED DEK

**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 162+00 TO STA. 187+50**

LUC-475-0.09

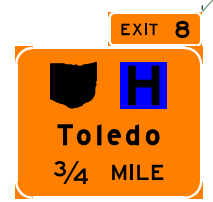
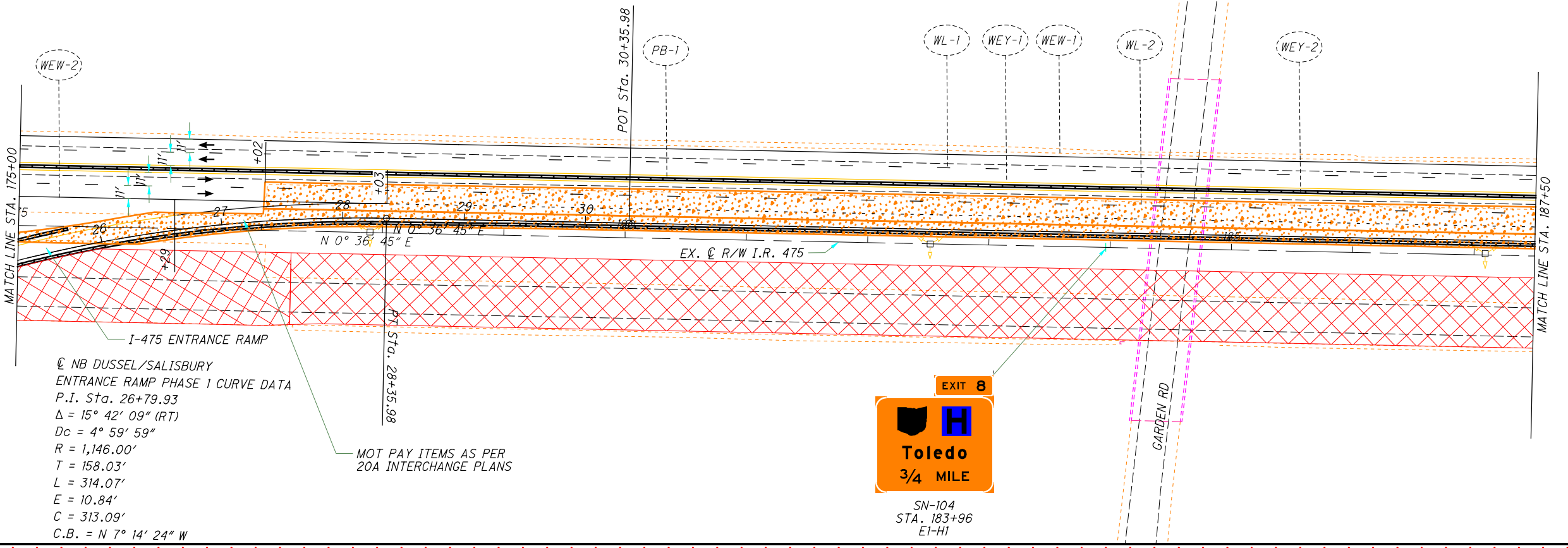
79
855



$\text{C} \text{ NB DUSSEL/SALISBURY ENTRANCE RAMP PHASE 1 CURVE DATA}$
 P.I. Sta. 23+64.21
 $\Delta = 15^{\circ} 42' 14''$ (LT)
 $D_c = 4^{\circ} 56' 52''$
 $R = 1,158.00'$
 $T = 159.70'$
 $L = 317.39'$
 $E = 10.96'$
 $C = 316.40'$
 $\text{C.B.} = \text{N } 7^{\circ} 14' 22'' \text{ W}$

LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				



SN-104
STA. 183+96
E1-H1

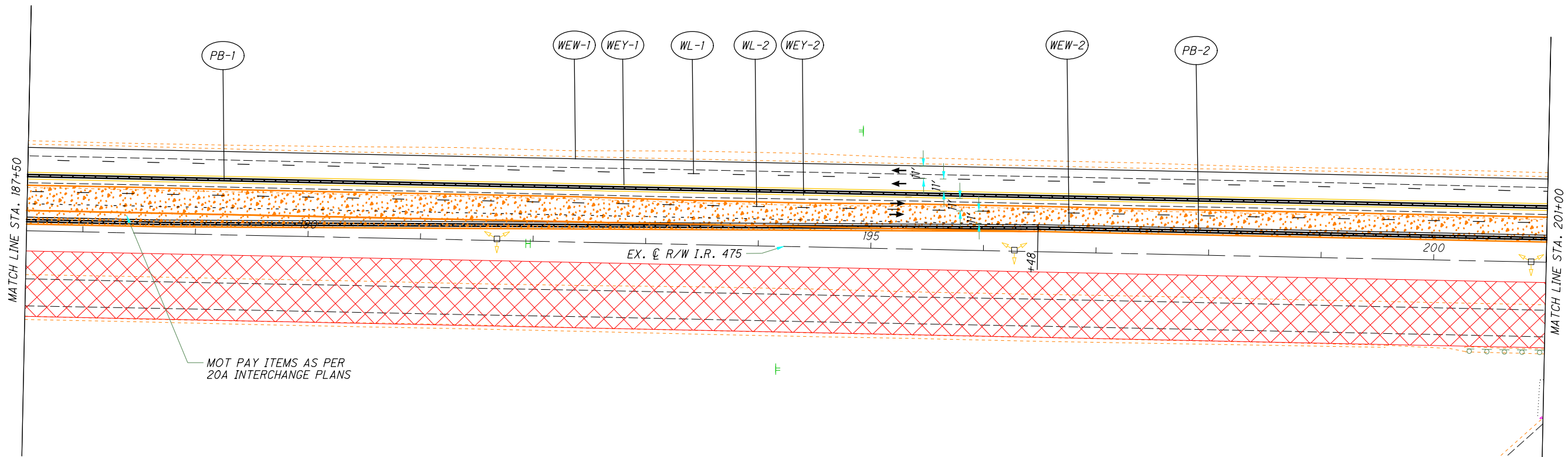
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**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 187+50 TO STA. 201+00**

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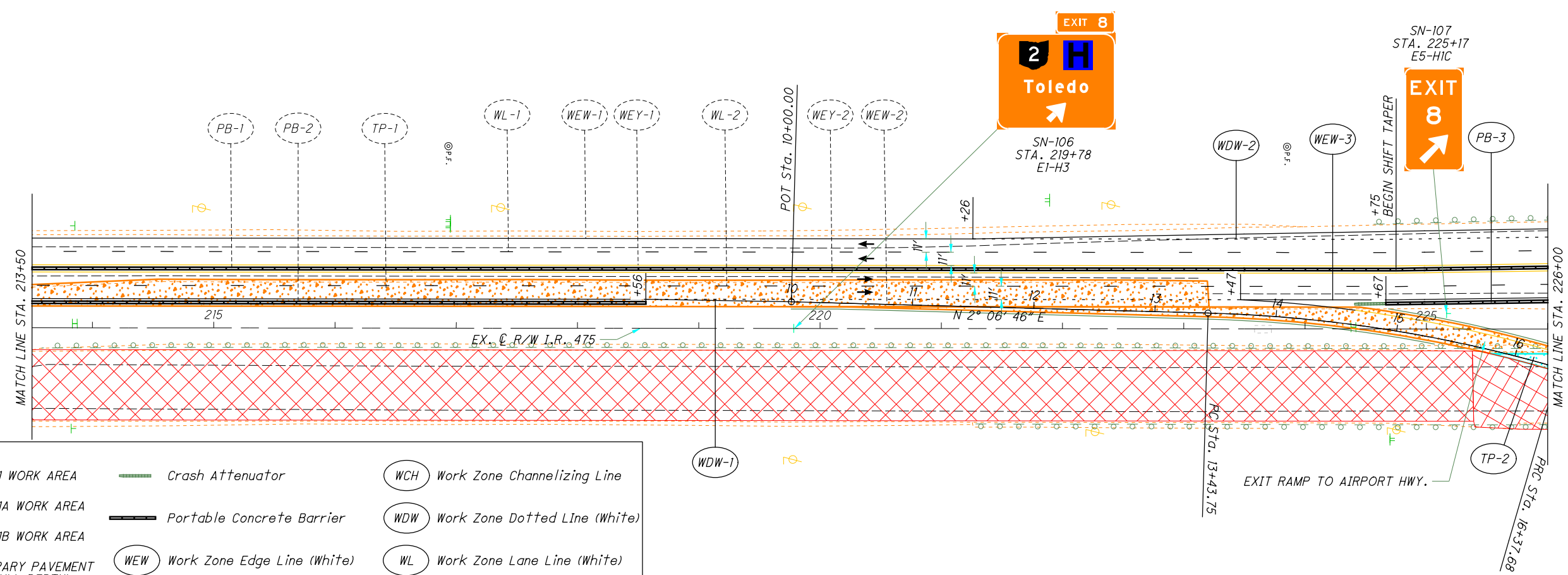
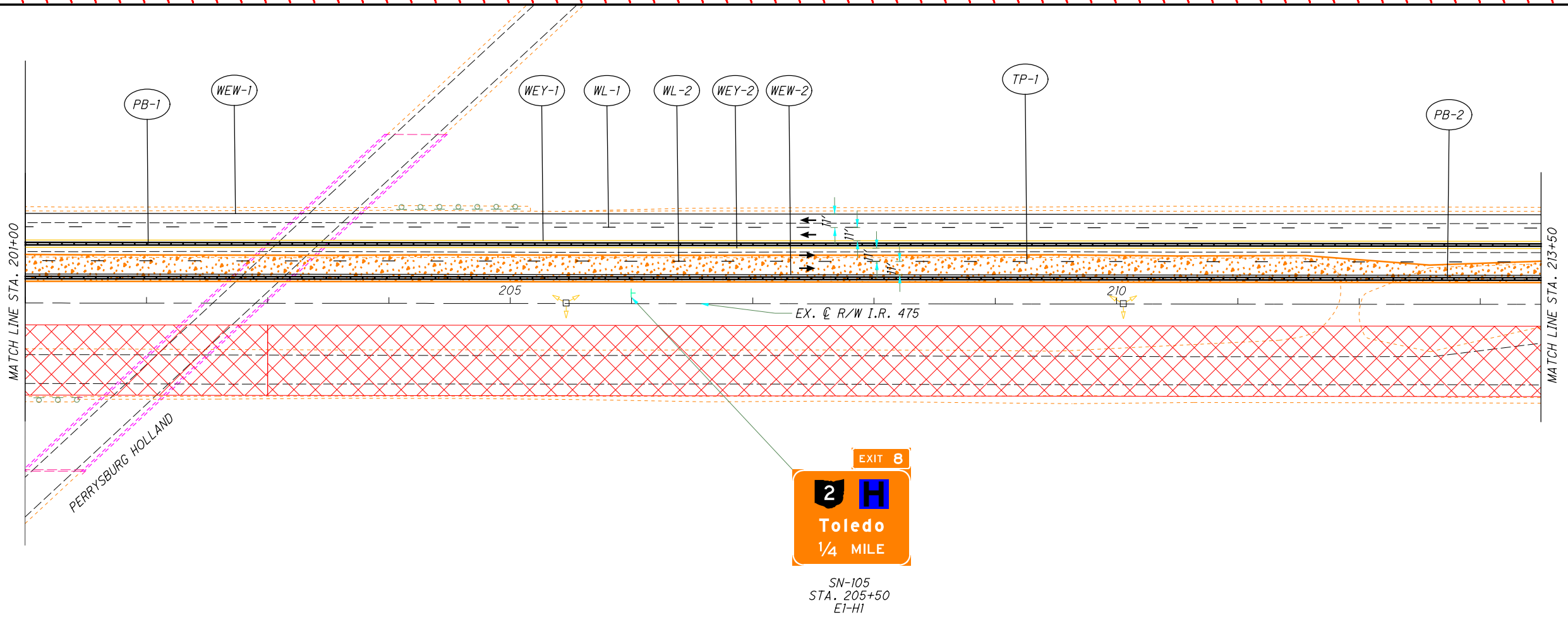


LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

0 50 100

 HORIZONTAL SCALE IN FEET

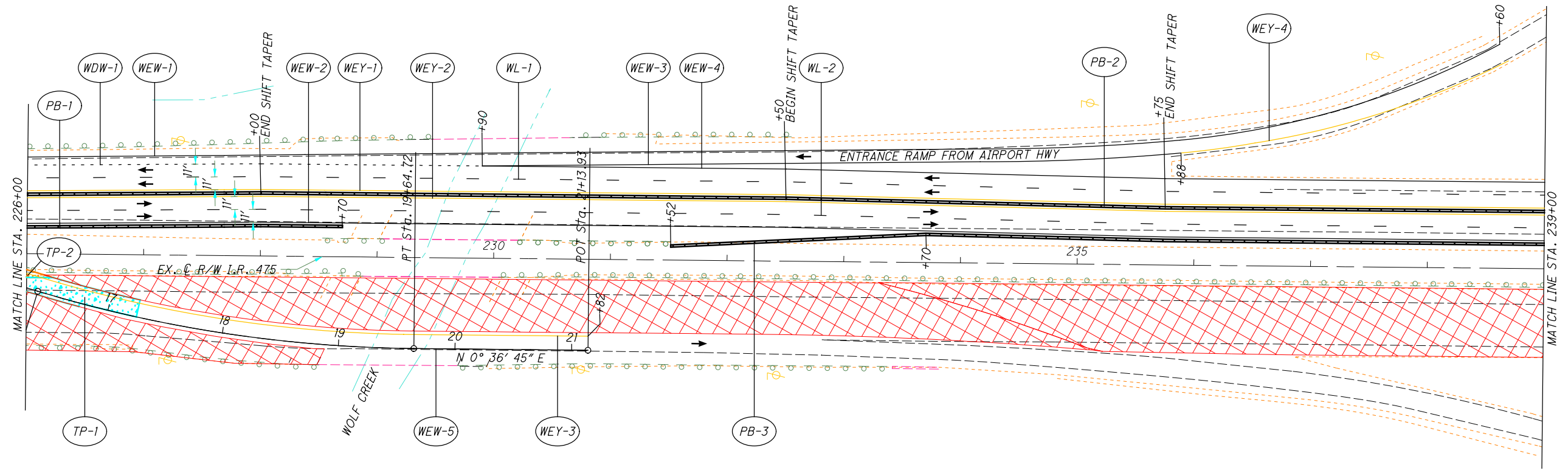
MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 201+00 TO STA. 226+00

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 SCALE IN FEET

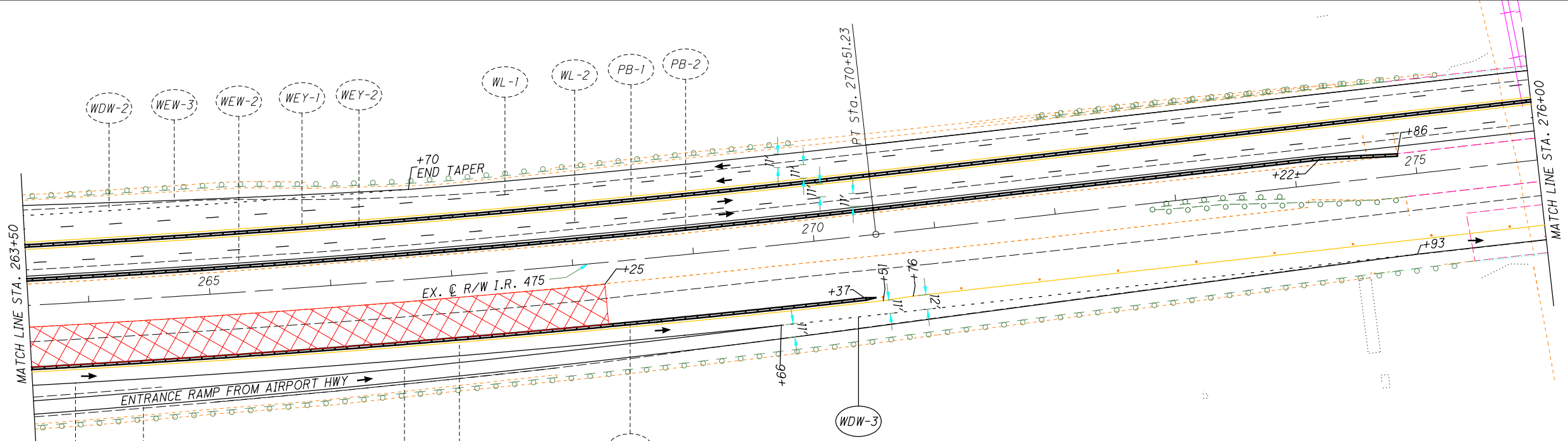
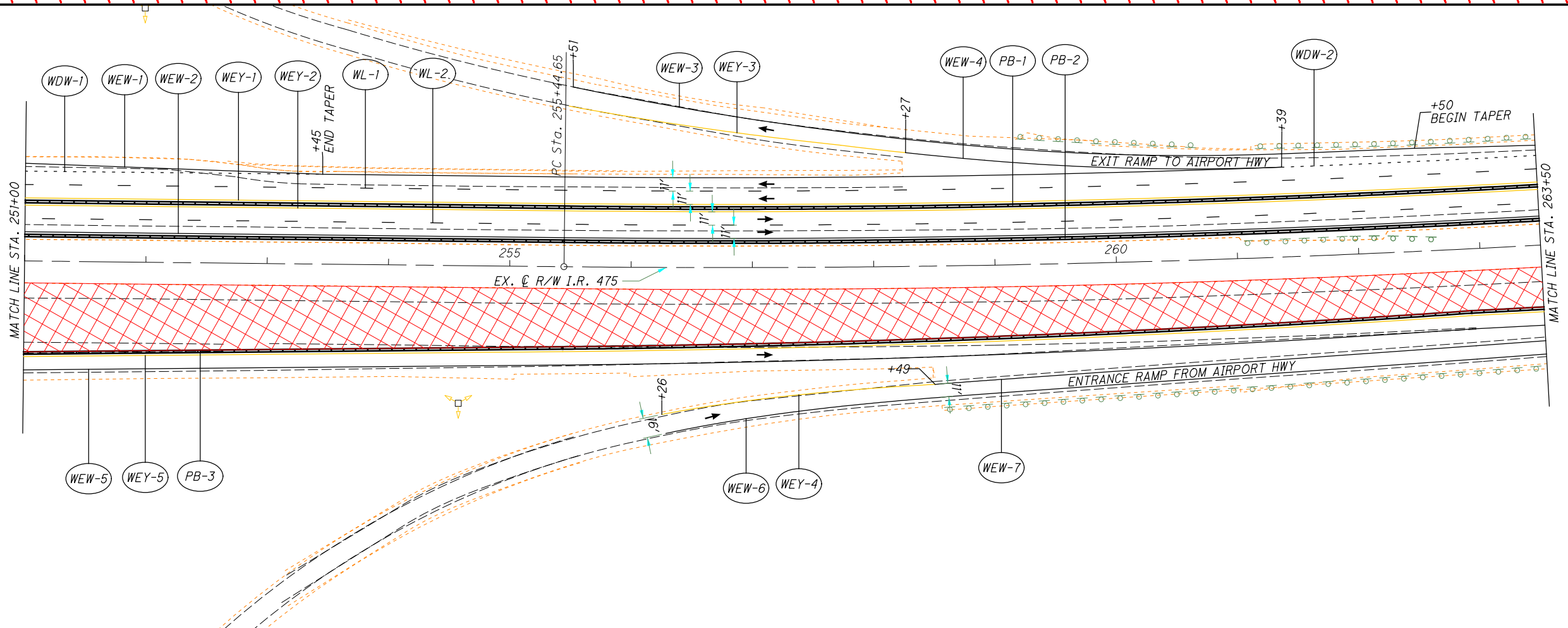
MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 226+00 TO STA. 238+50



LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

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LEGEND:

	Phase I WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase IA WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase IB WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

N

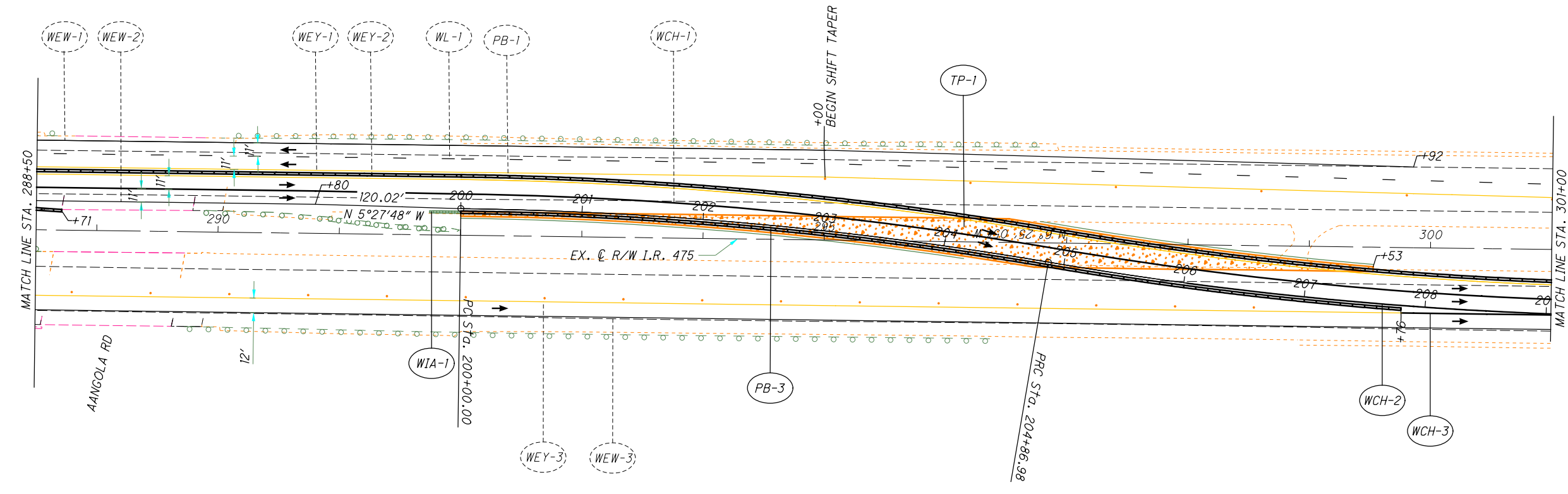
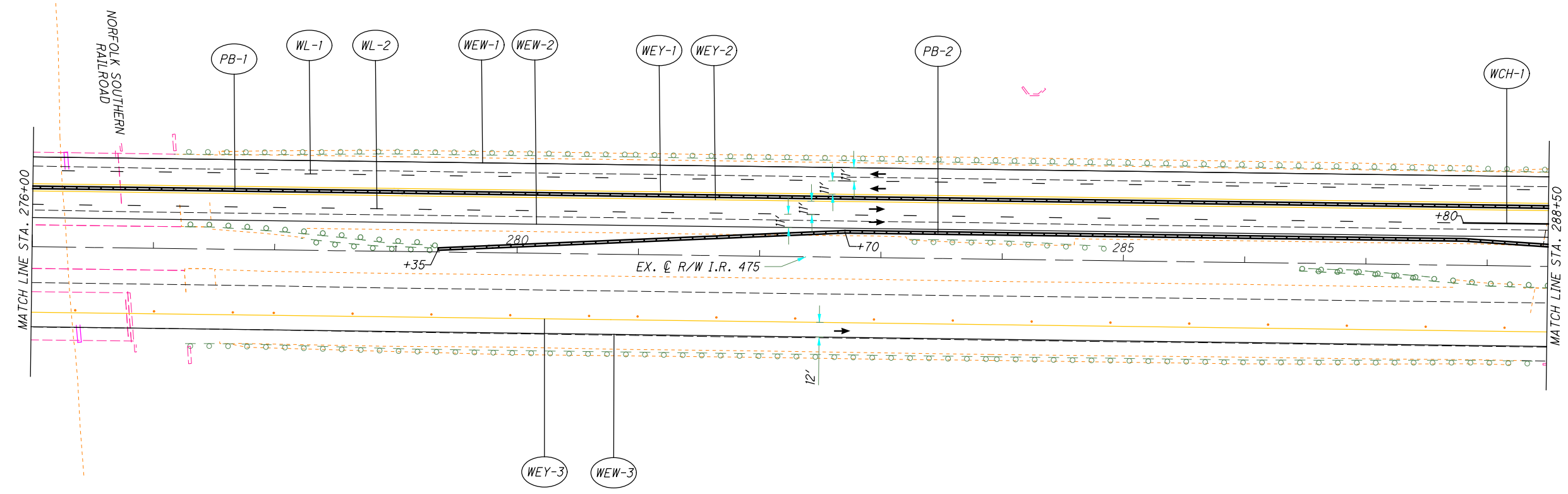
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HORIZONTAL SCALE IN FEET

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CHECKED
DEK

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 251+00 TO STA. 276+00

LUC-475-0.09

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LEGEND:

- | | | | | | |
|--|-------------------------------------|--|------------------------------|--|-------------------------------|
| | Phase I WORK AREA | | Crash Attenuator | | Work Zone Channelizing Line |
| | Phase IA WORK AREA | | Portable Concrete Barrier | | Work Zone Dotted Line (White) |
| | Phase IB WORK AREA | | Work Zone Edge Line (White) | | Work Zone Lane Line (White) |
| | TEMPORARY PAVEMENT (FULL DEPTH) | | Work Zone Edge Line (Yellow) | | Drums |
| | TEMPORARY PAVEMENT (VARIABLE DEPTH) | | | | |



**MAINTENANCE OF TRAFFIC - MAINLINE
 PHASE 1 - STA. 276+00 TO STA. 301+00**

LUC-475-0.09

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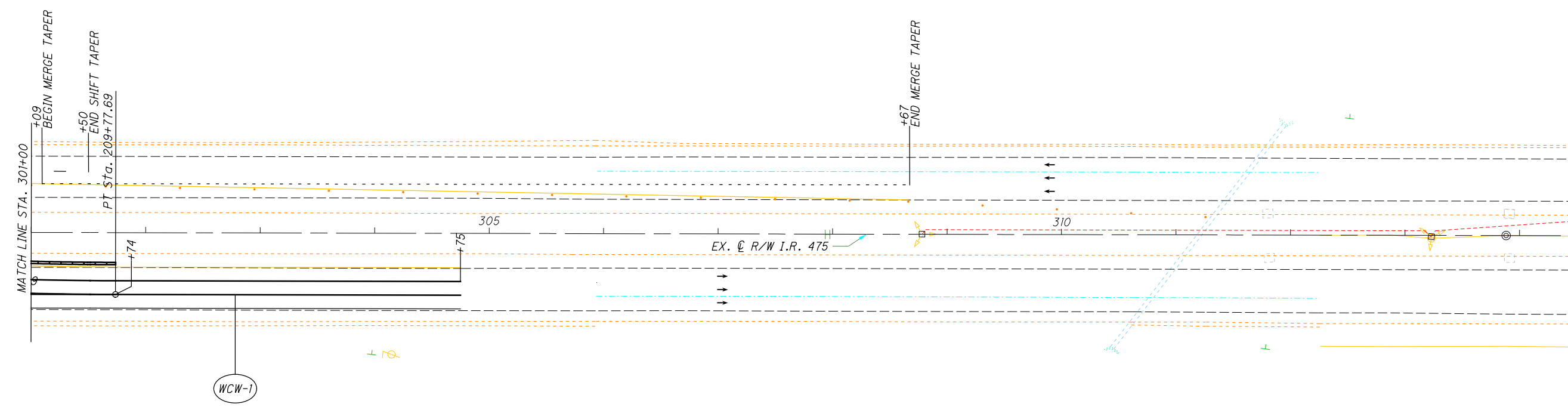


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MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1 - STA. 301+00 TO STA. 309+50

LUC-475-0.09

86
 855



LEGEND:

Phase 1 WORK AREA	Crash Attenuator	WCH Work Zone Channelizing Line
Phase 1A WORK AREA	Portable Concrete Barrier	WDW Work Zone Dotted Line (White)
Phase 1B WORK AREA	WEW Work Zone Edge Line (White)	WL Work Zone Lane Line (White)
TEMPORARY PAVEMENT (FULL DEPTH)	WEY Work Zone Edge Line (Yellow)	Drums
TEMPORARY PAVEMENT (VARIABLE DEPTH)		

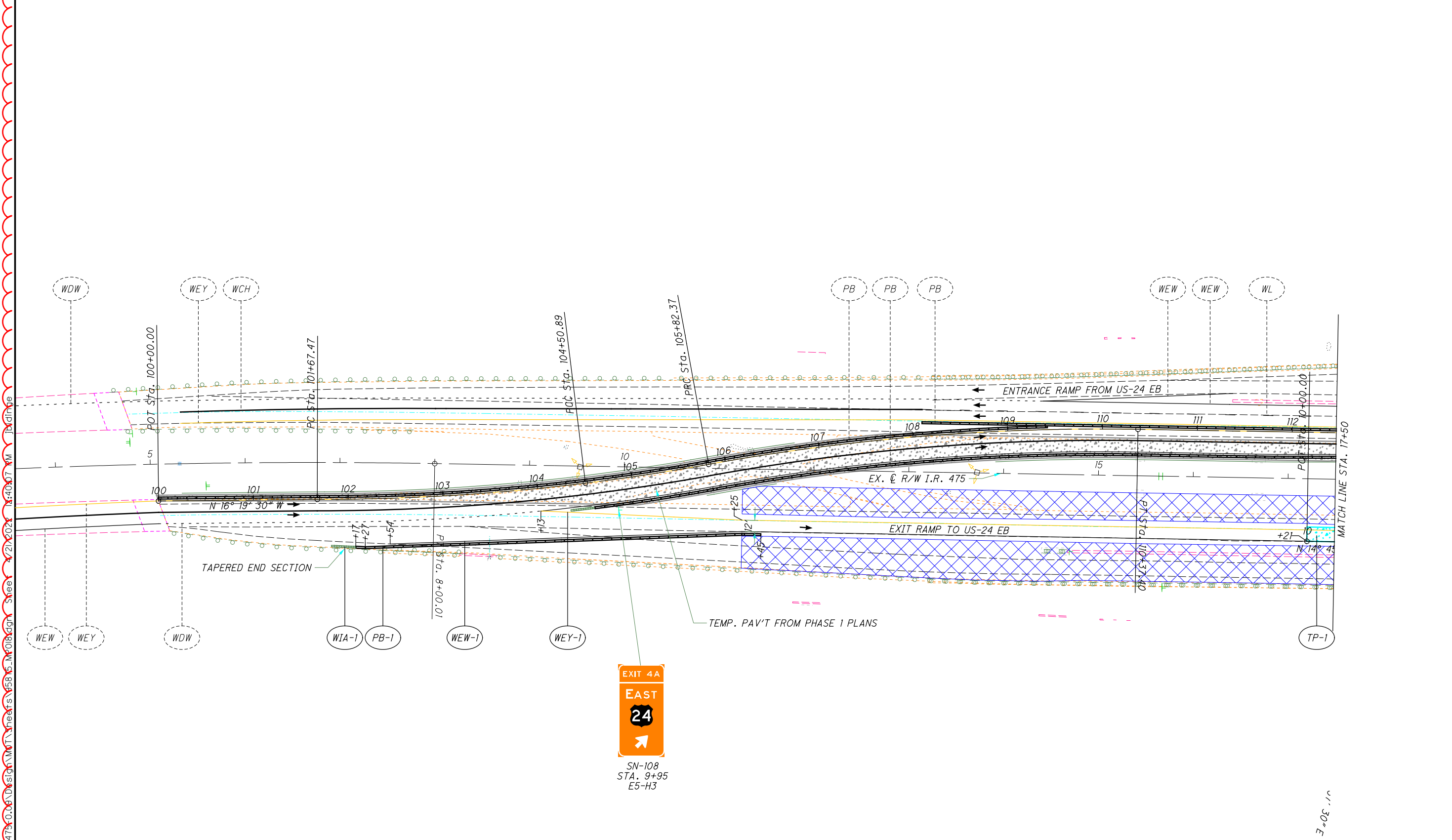


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MK
CHECKED
DEK

**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1A - STA. 5+00 TO STA. 17+50**

LUC-475-0.09

87
855



SN-108
STA. 9+95
E5-H3

LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

11' 30" E

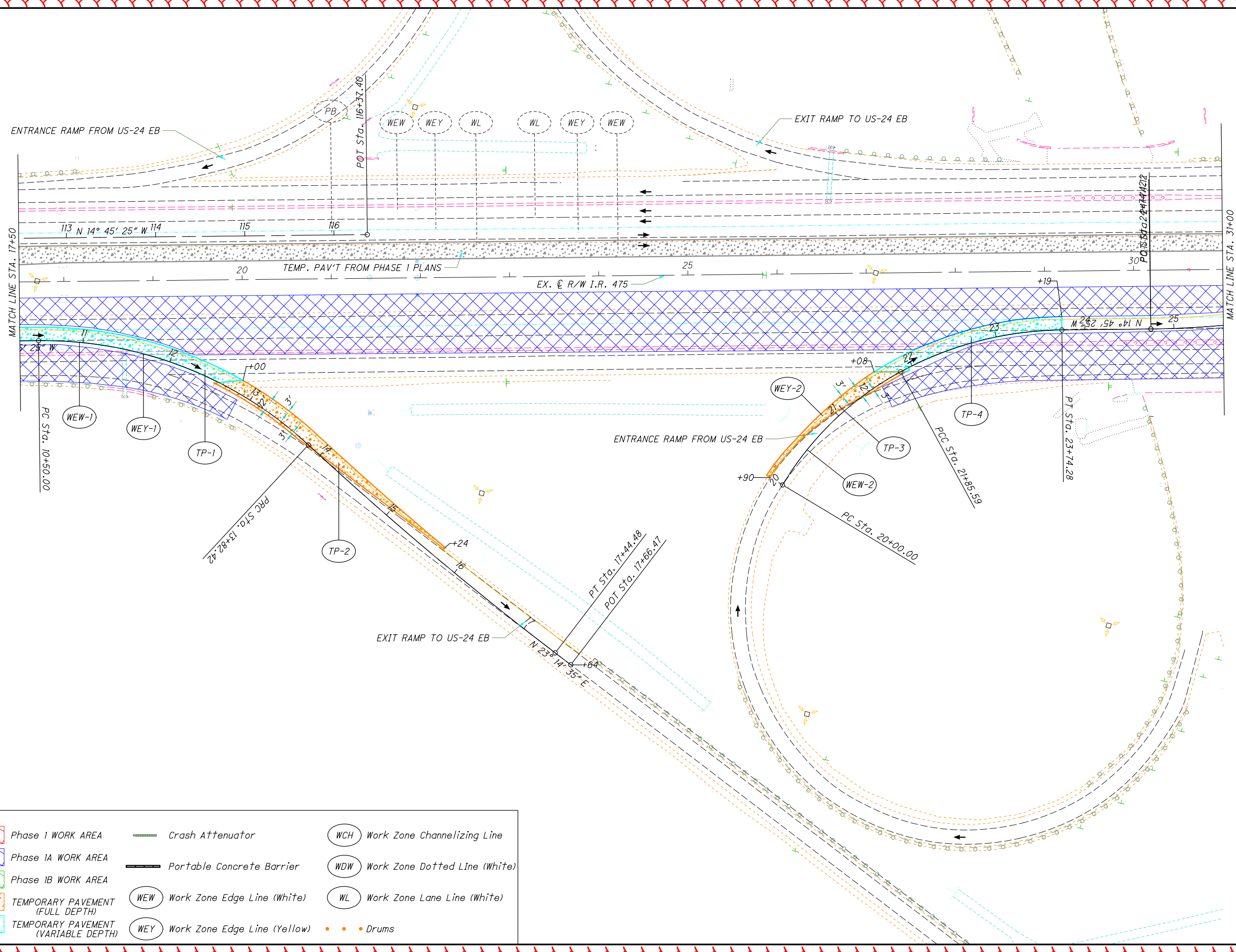


CALCULATED MK
CHECKED DEK

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1A - STA. 17+50 TO STA. 30+91.88

LUC-475-0.09

88
855



LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

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ENTRANCE RAMP FROM US-24 WB
TEMP. PAV'T FROM PHASE 1 PLANS

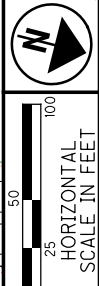
EXIT RAMP TO US-24 WB

MATCH LINE STA. 31+00

EX. ϕ R/W I.R. 475

TEMP. PAV'T FROM PHASE 1 PLANS

EXIT RAMP TO US-24 WB
(CLOSED FOR PHASE 1A)

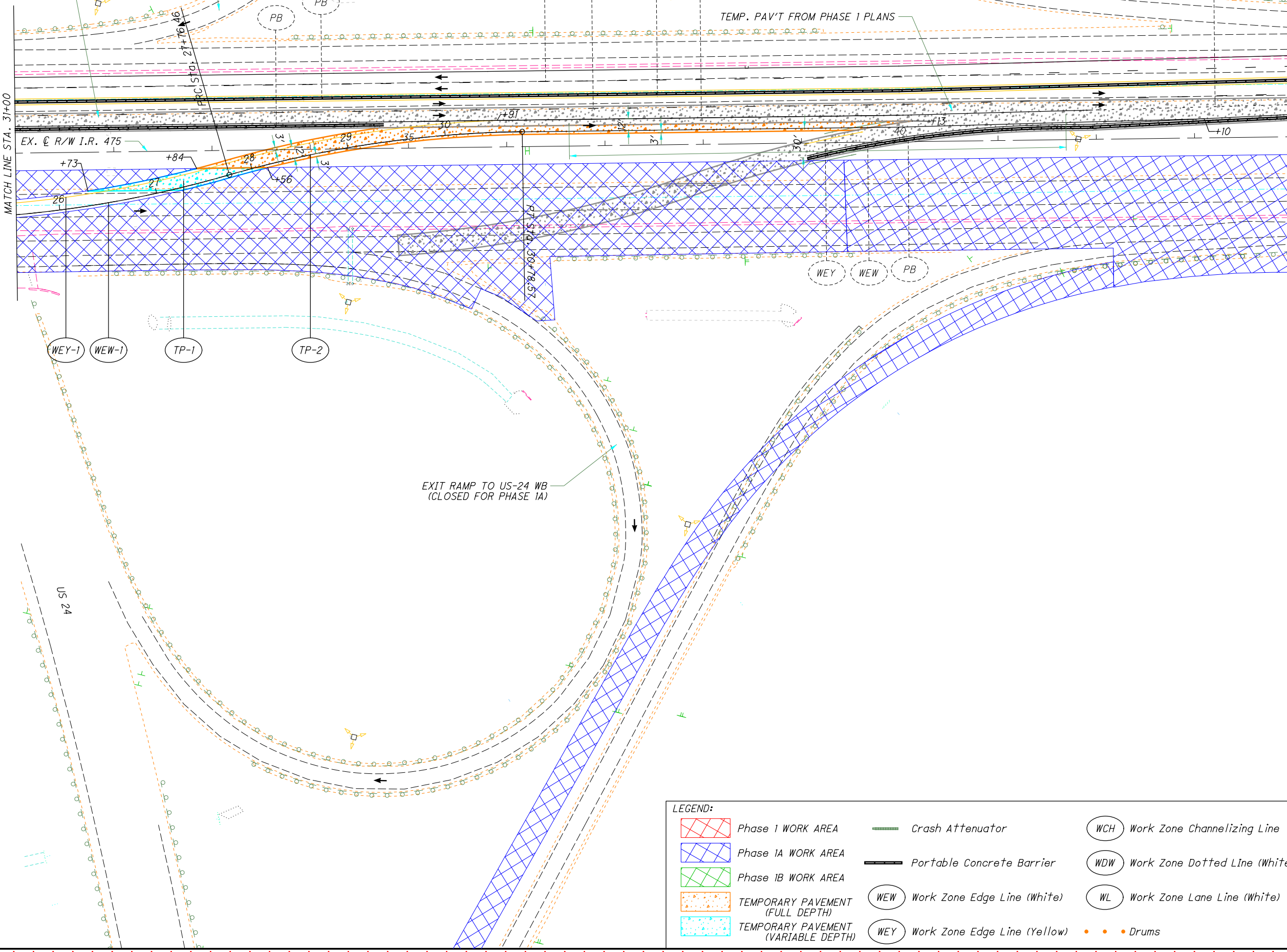


CALCULATED MK
CHECKED DEK

**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1A - STA. 31+91.88 TO STA. 44+00**

LUC-475-0.09


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LEGEND:

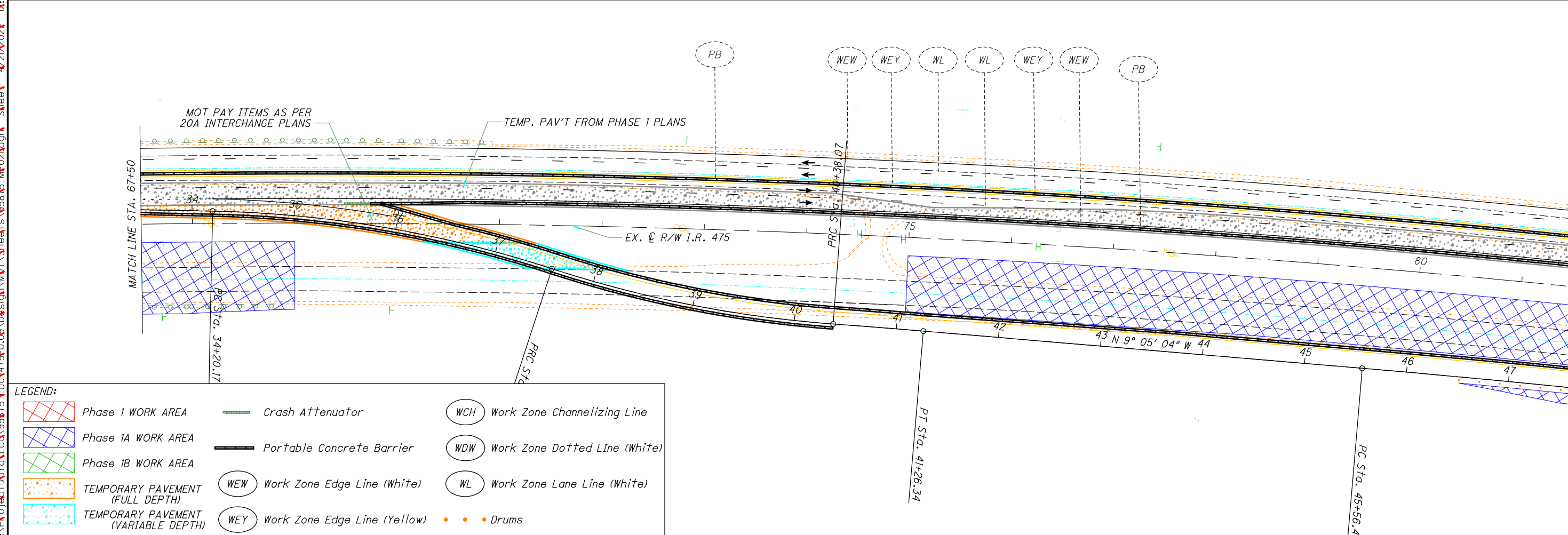
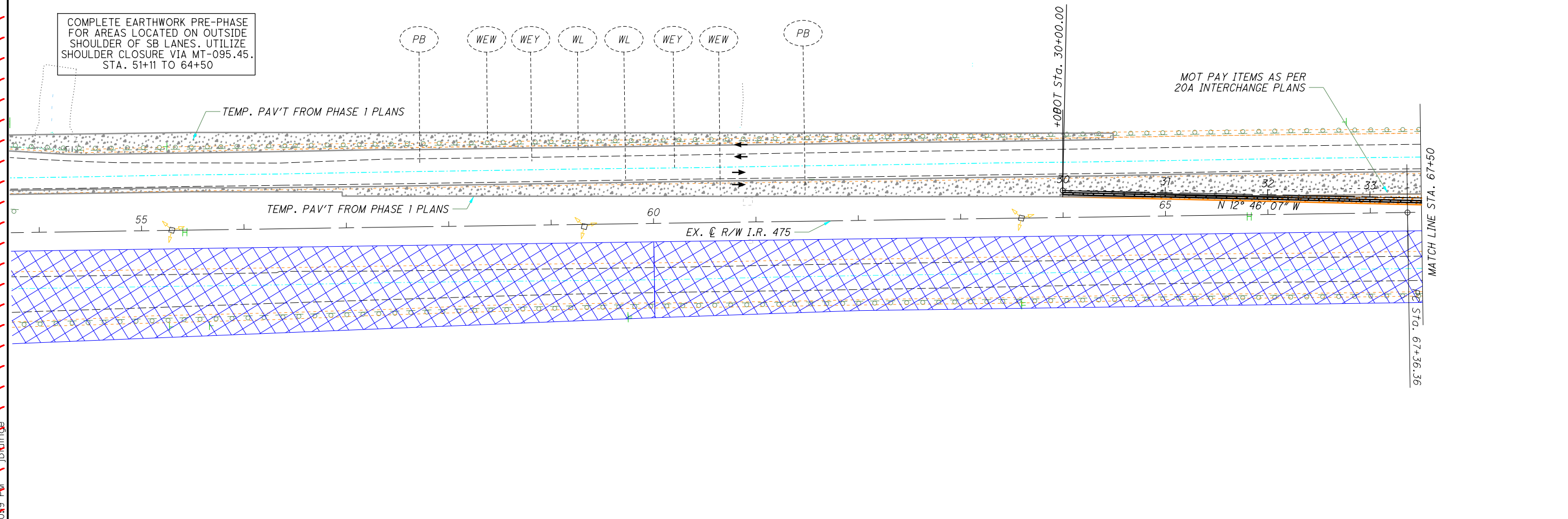
	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

COMPLETE EARTHWORK PRE-PHASE FOR AREAS LOCATED ON OUTSIDE SHOULDER OF SB LANES. UTILIZE SHOULDER CLOSURE VIA MT-095.45. STA. 51+11 TO 64+50





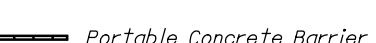












 CALCULATED MK
 CHECKED DEK



LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1A - STA. 53+72 TO STA. 81+46

LUC-475-0.09

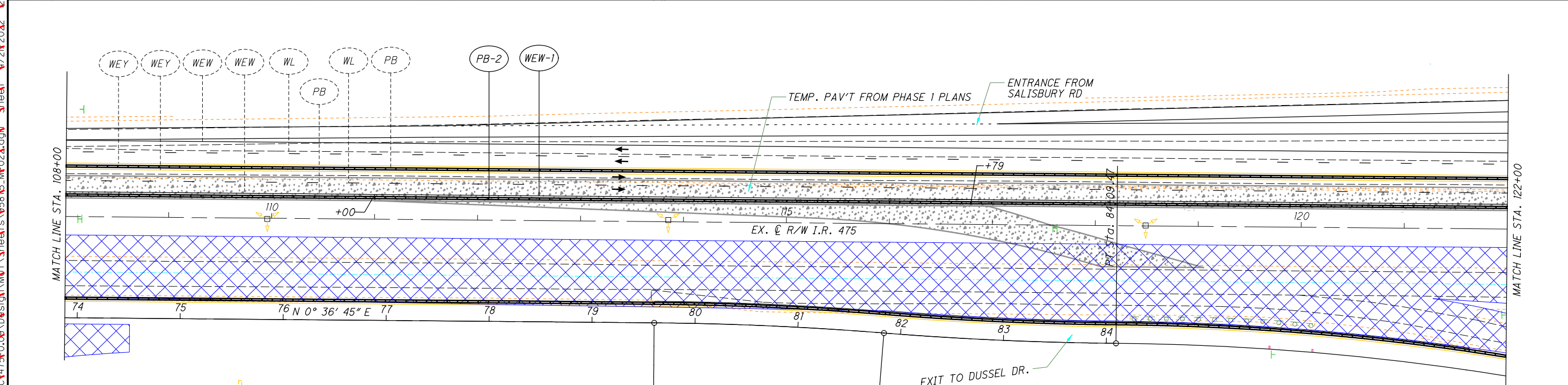
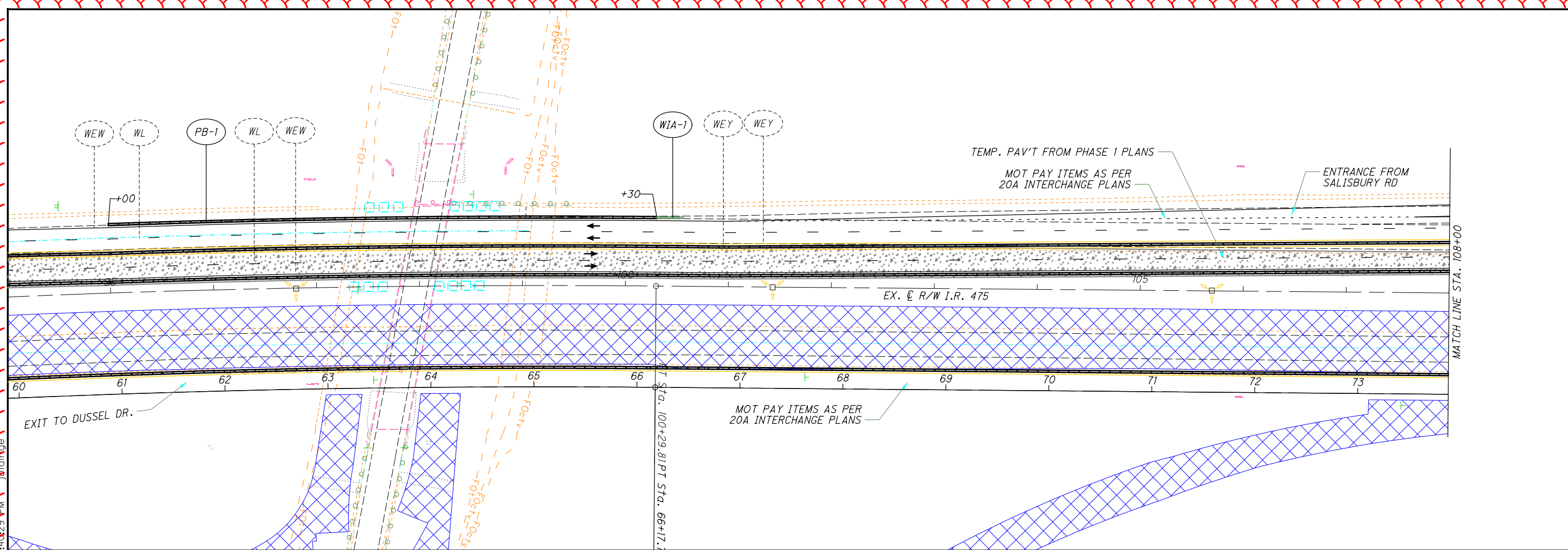
90
 855

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MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1A - STA. 94+00 TO STA. 122+00

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 91
 855

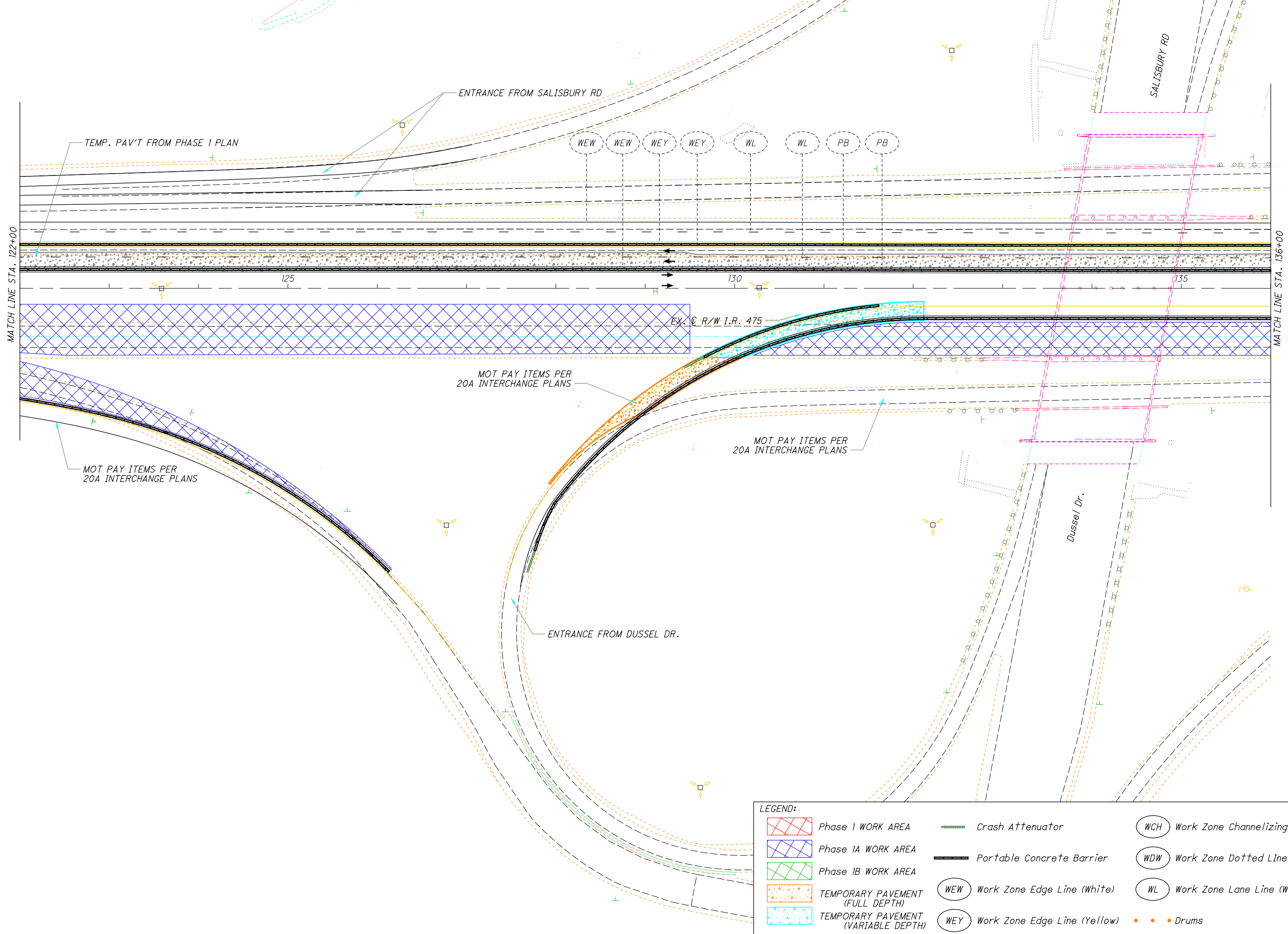


LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

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CHECKED
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0 50 100
HORIZONTAL
SCALE IN FEET

**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1A - STA. 122+00 TO STA. 136+00**

LUC-475-0.09

LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

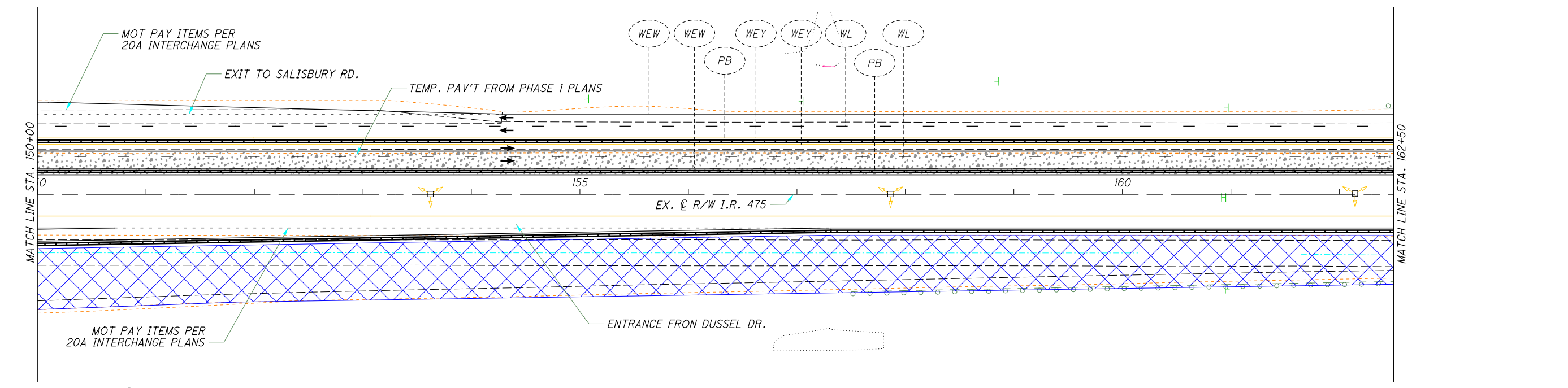
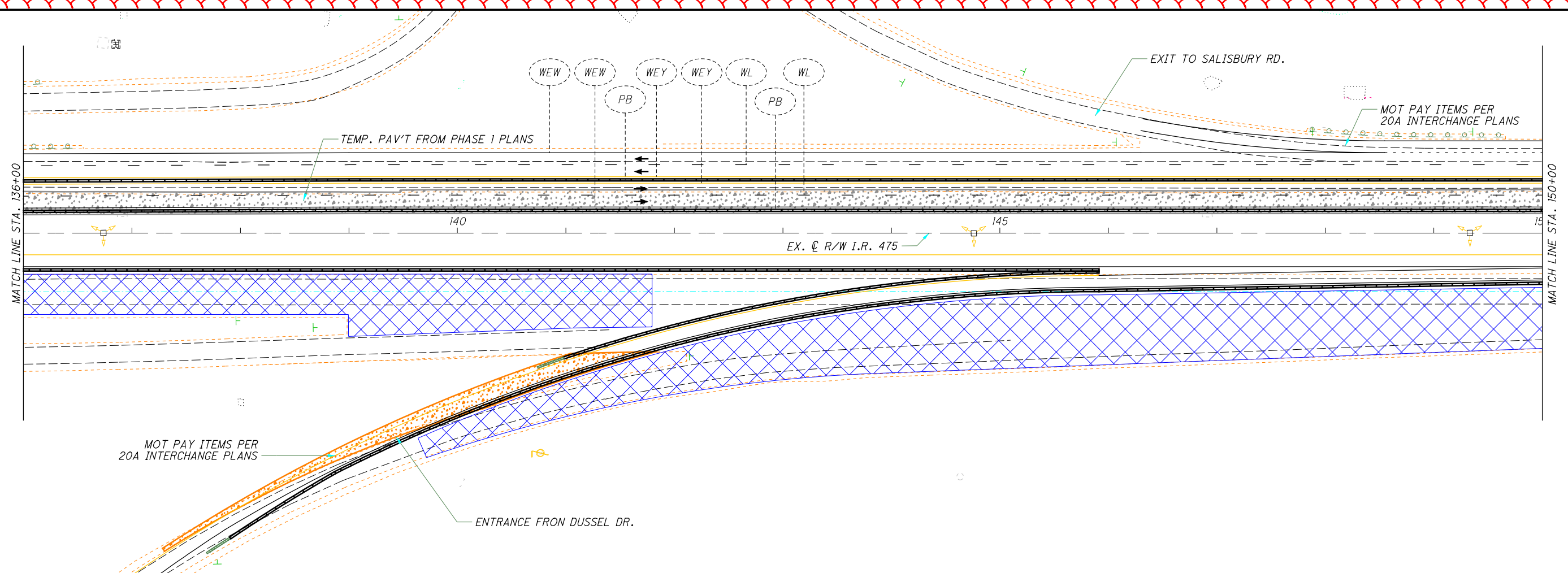


CALCULATED
MK
CHECKED
DEK

**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1A - STA. 136+00 TO STA. 162+50**

LUC-475-0.09

93
855

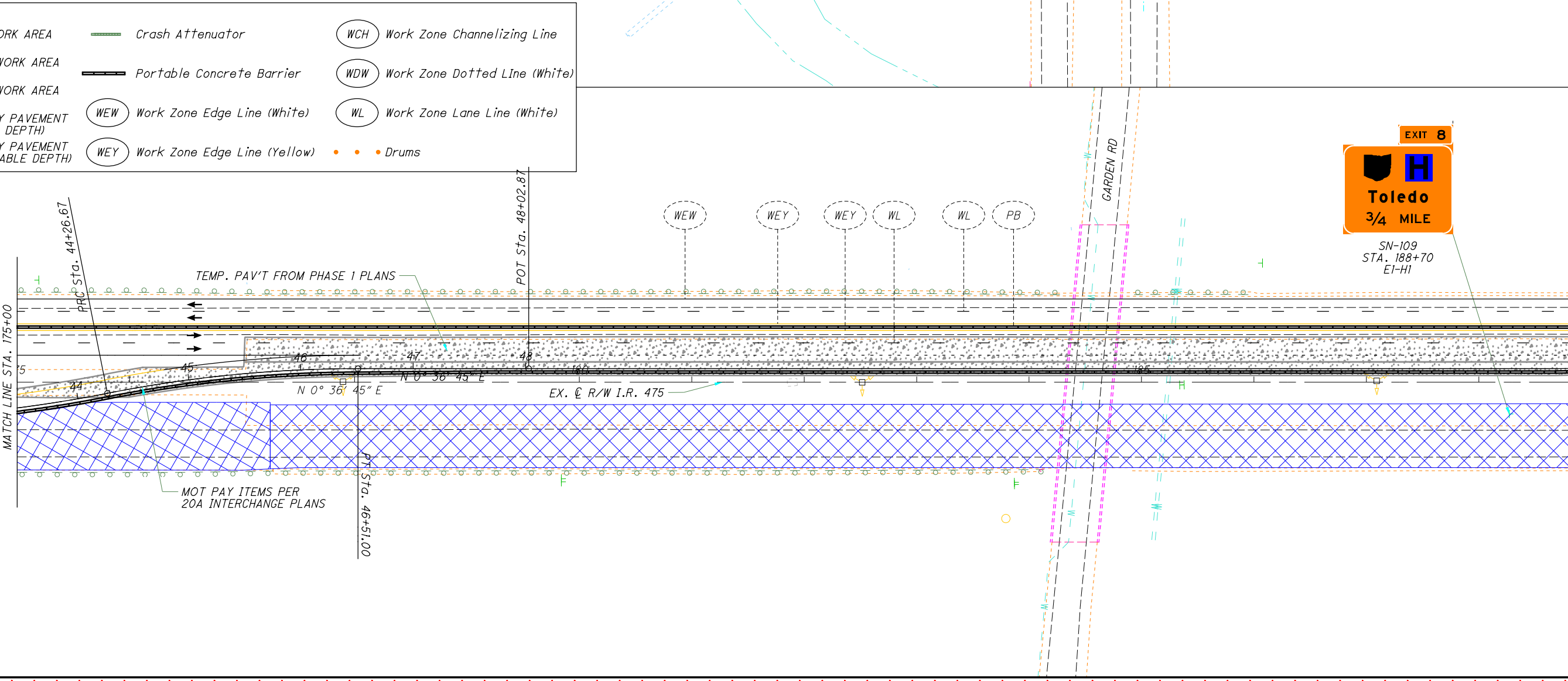
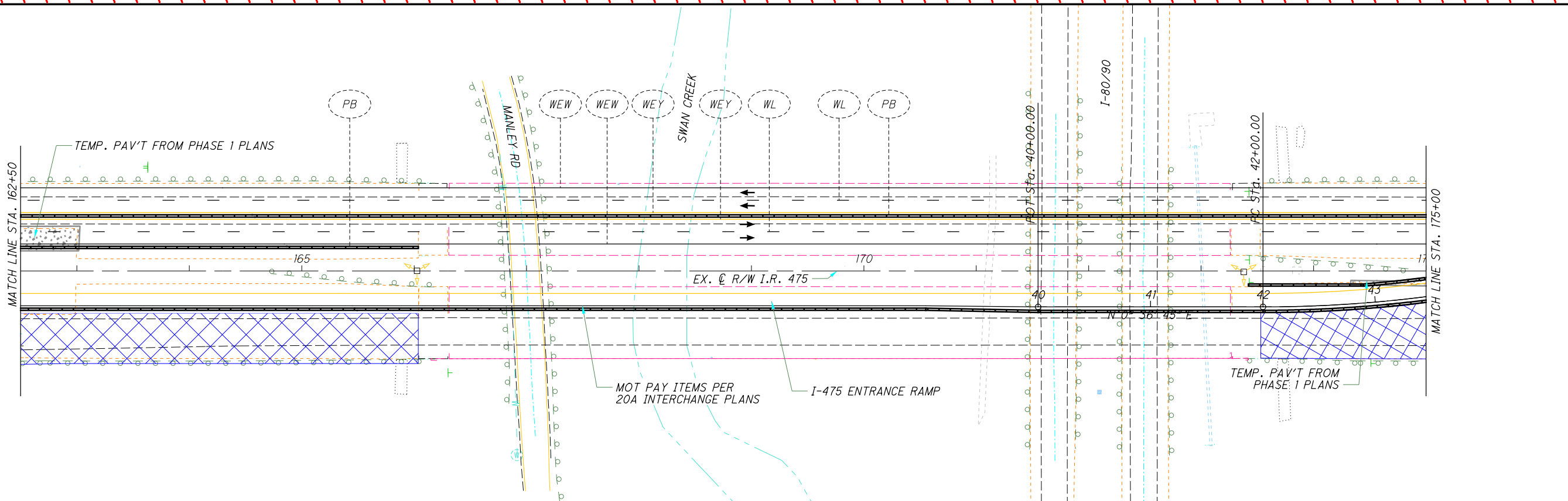


LEGEND:

Phase 1 WORK AREA	Crash Attenuator	Work Zone Channelizing Line
Phase 1A WORK AREA	Portable Concrete Barrier	Work Zone Dotted Line (White)
Phase 1B WORK AREA	Work Zone Edge Line (White)	Work Zone Lane Line (White)
TEMPORARY PAVEMENT (FULL DEPTH)	Work Zone Edge Line (Yellow)	Drums
TEMPORARY PAVEMENT (VARIABLE DEPTH)		

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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

CALCULATED MK
CHECKED DEK

HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1A - STA. 162+50 TO STA. 188+80

LUC-475-0.09

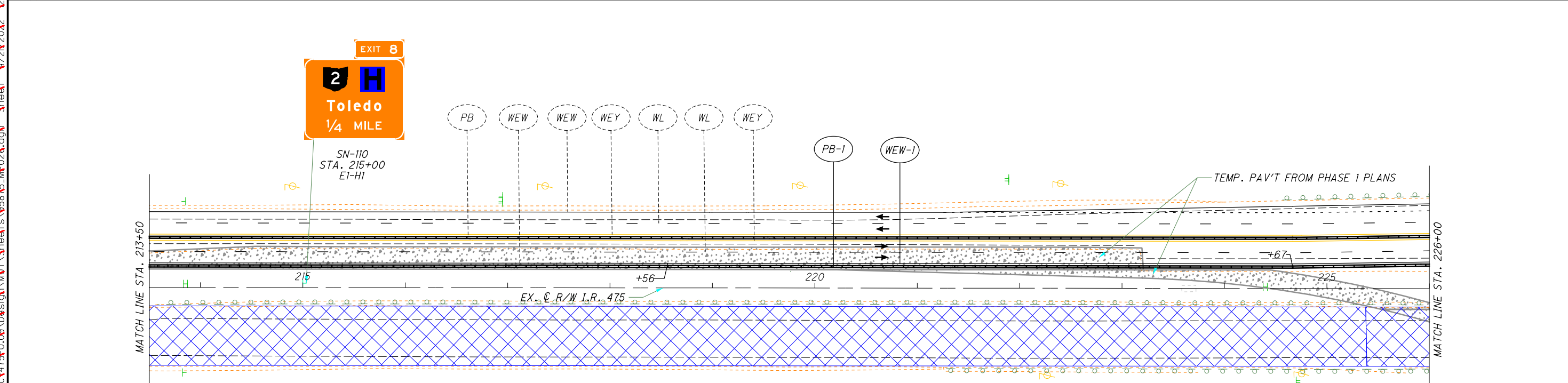
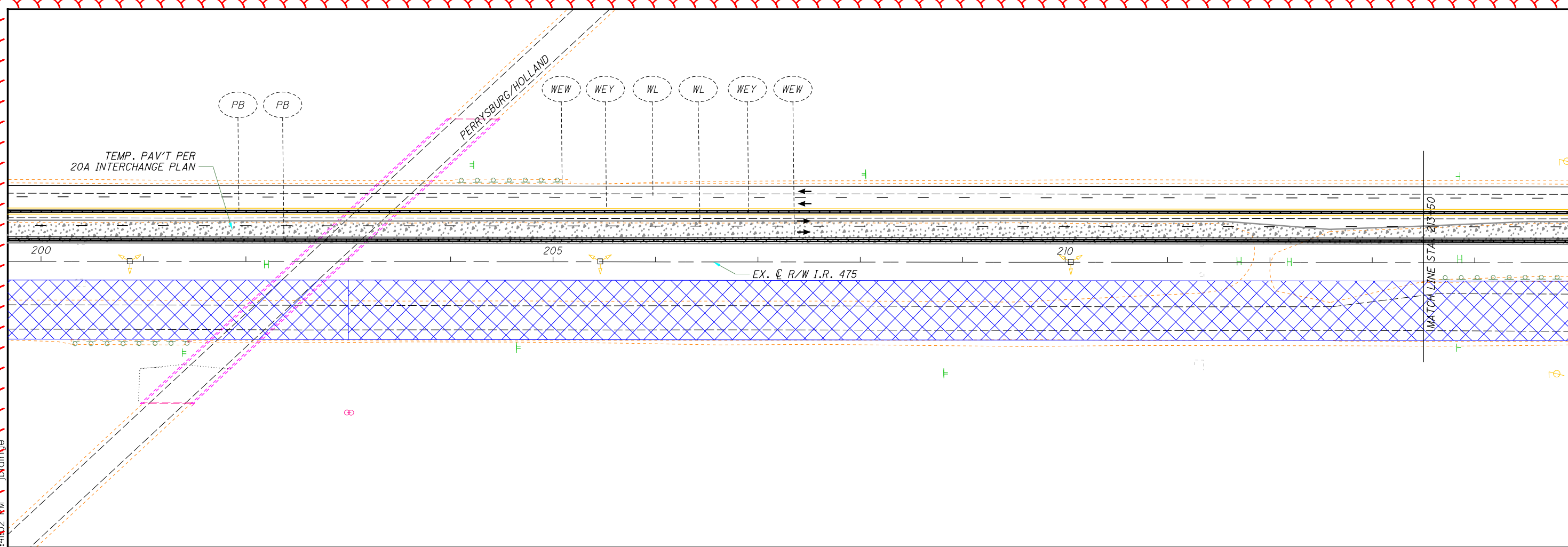


CALCULATED
MK
CHECKED
DEK

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1A - STA. 199+67 TO STA. 226+00

LUC-475-0.09

95
855



LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

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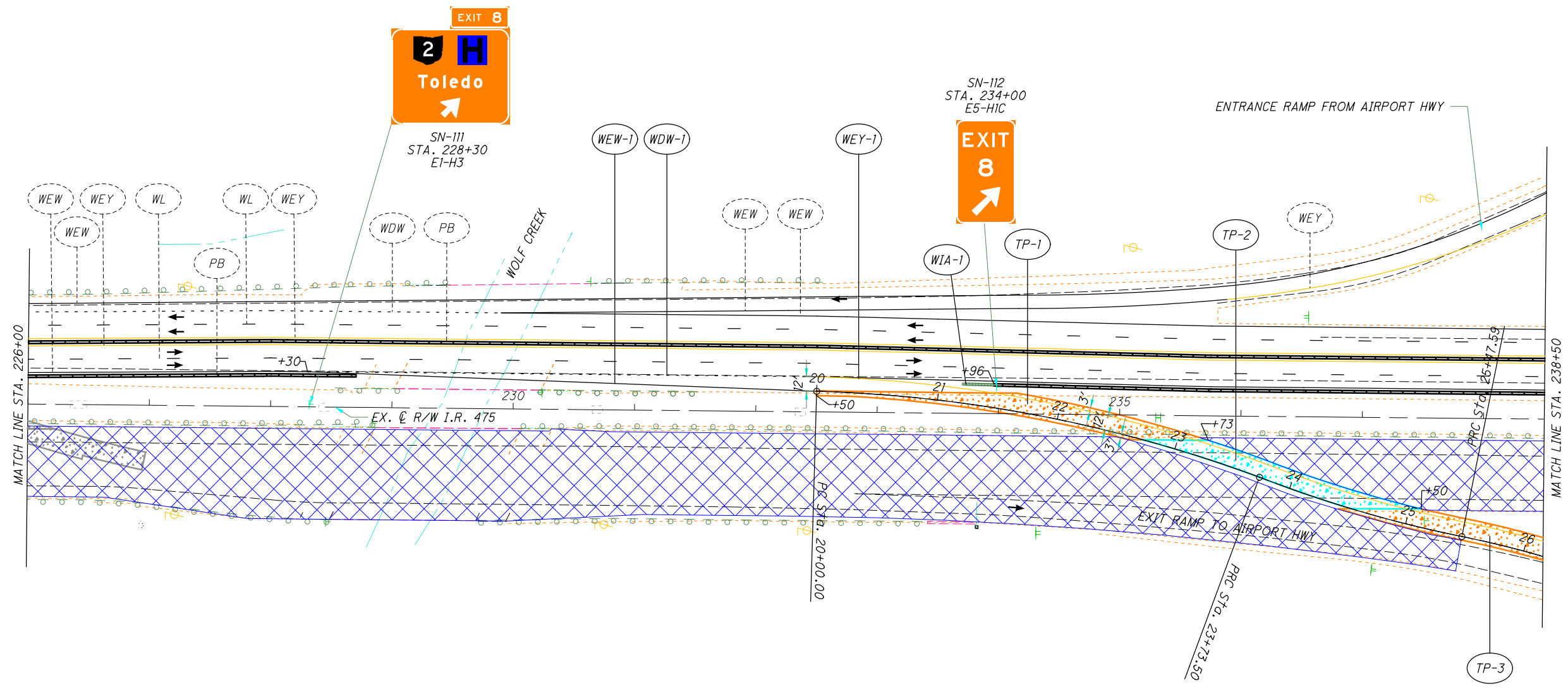


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**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1A - STA. 226+00 TO STA. 238+50**

LUC-475-0.09

96
855



LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

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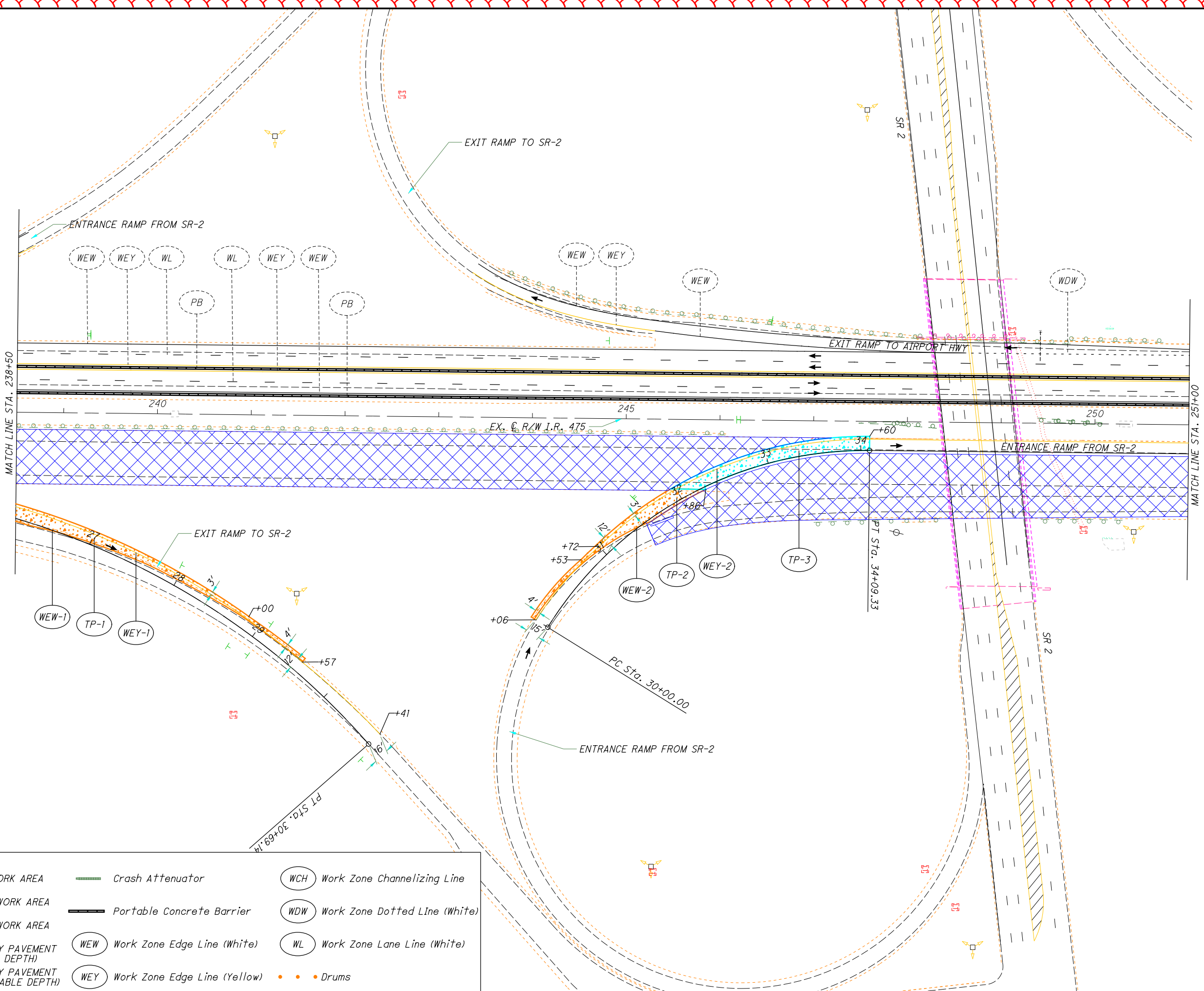


CALCULATED	MK
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**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1A - STA. 238+50 TO 251+00**

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97
855

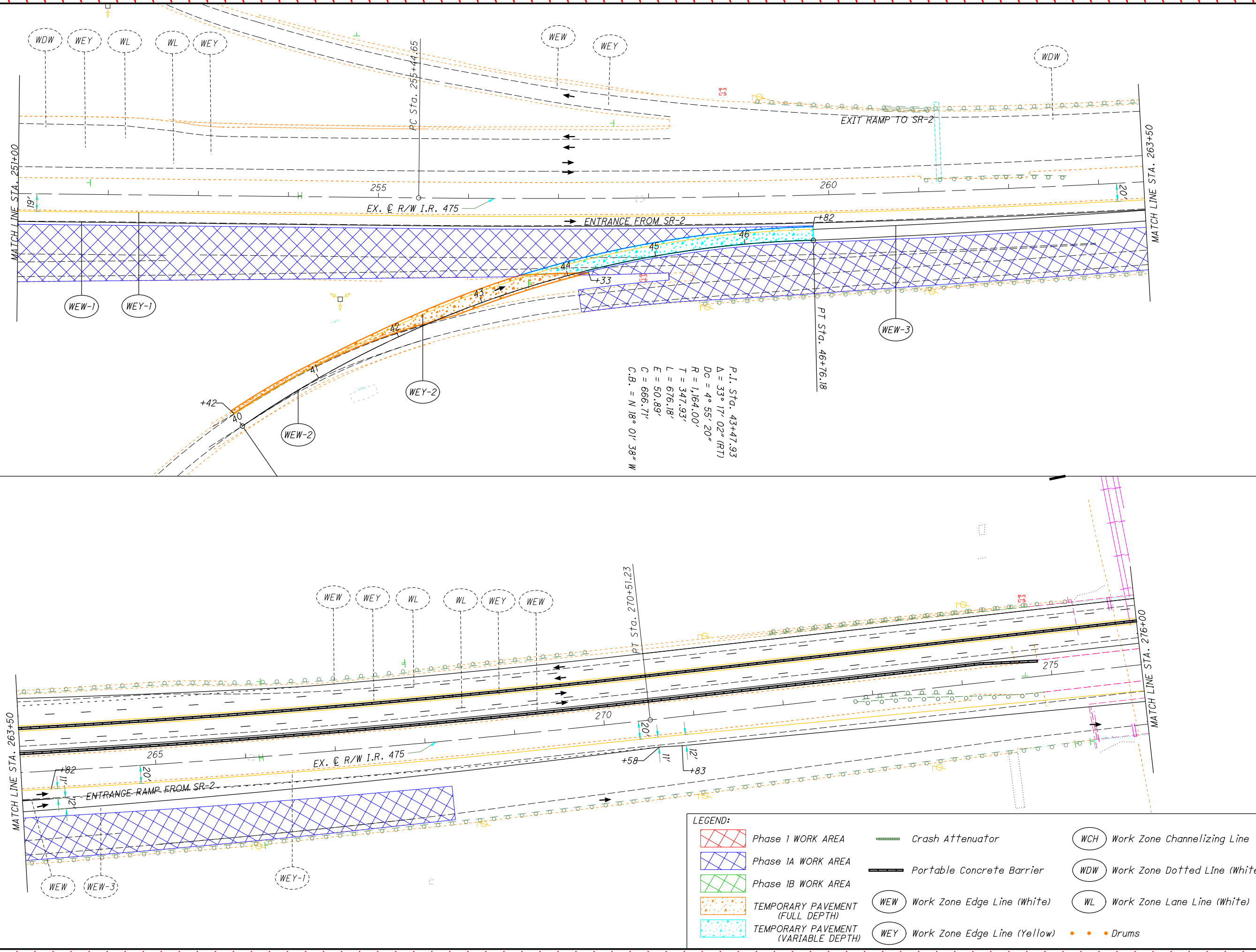


LEGEND:

Phase 1 WORK AREA	Crash Attenuator	WCH Work Zone Channelizing Line
Phase 1A WORK AREA	Portable Concrete Barrier	WDW Work Zone Dotted Line (White)
Phase 1B WORK AREA	WEW Work Zone Edge Line (White)	WL Work Zone Lane Line (White)
TEMPORARY PAVEMENT (FULL DEPTH)	WEY Work Zone Edge Line (Yellow)	Drums
TEMPORARY PAVEMENT (VARIABLE DEPTH)		





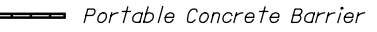








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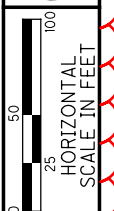
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P.I. Sta. 43+47.93
 $\Delta = 33^\circ 17' 02''$ (RT)
 $DC = 4^\circ 55' 20''$
 $R = 1,164.00'$
 $T = 347.93'$
 $L = 676.18'$
 $E = 50.89'$
 $C = 666.71'$
 $C.B. = N 18^\circ 01' 38'' W$

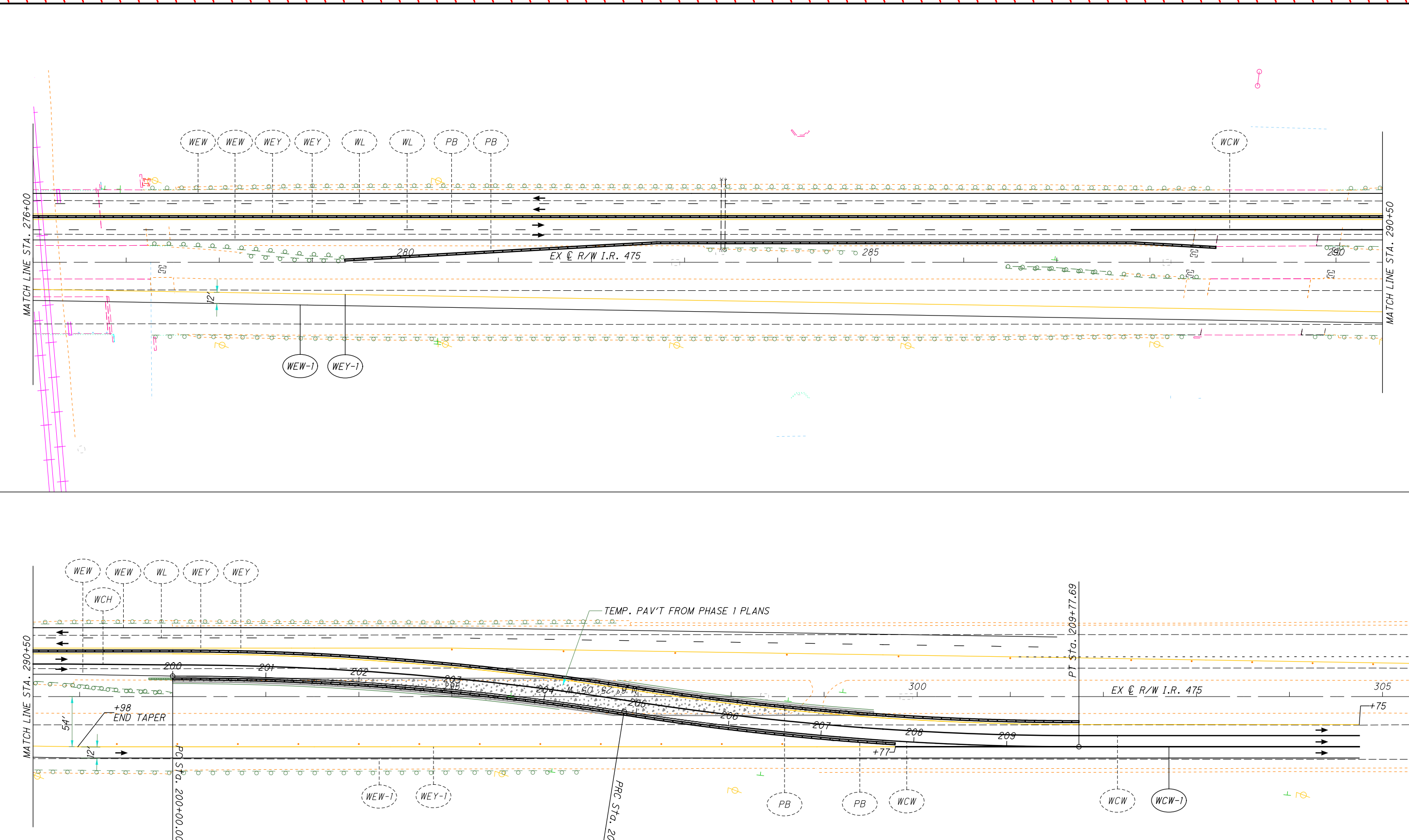
LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				



 HORIZONTAL SCALE IN FEET
 CALCULATED MK CHECKED DEK

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1A - STA. 251+00 TO STA. 276+00

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HORIZONTAL SCALE IN FEET

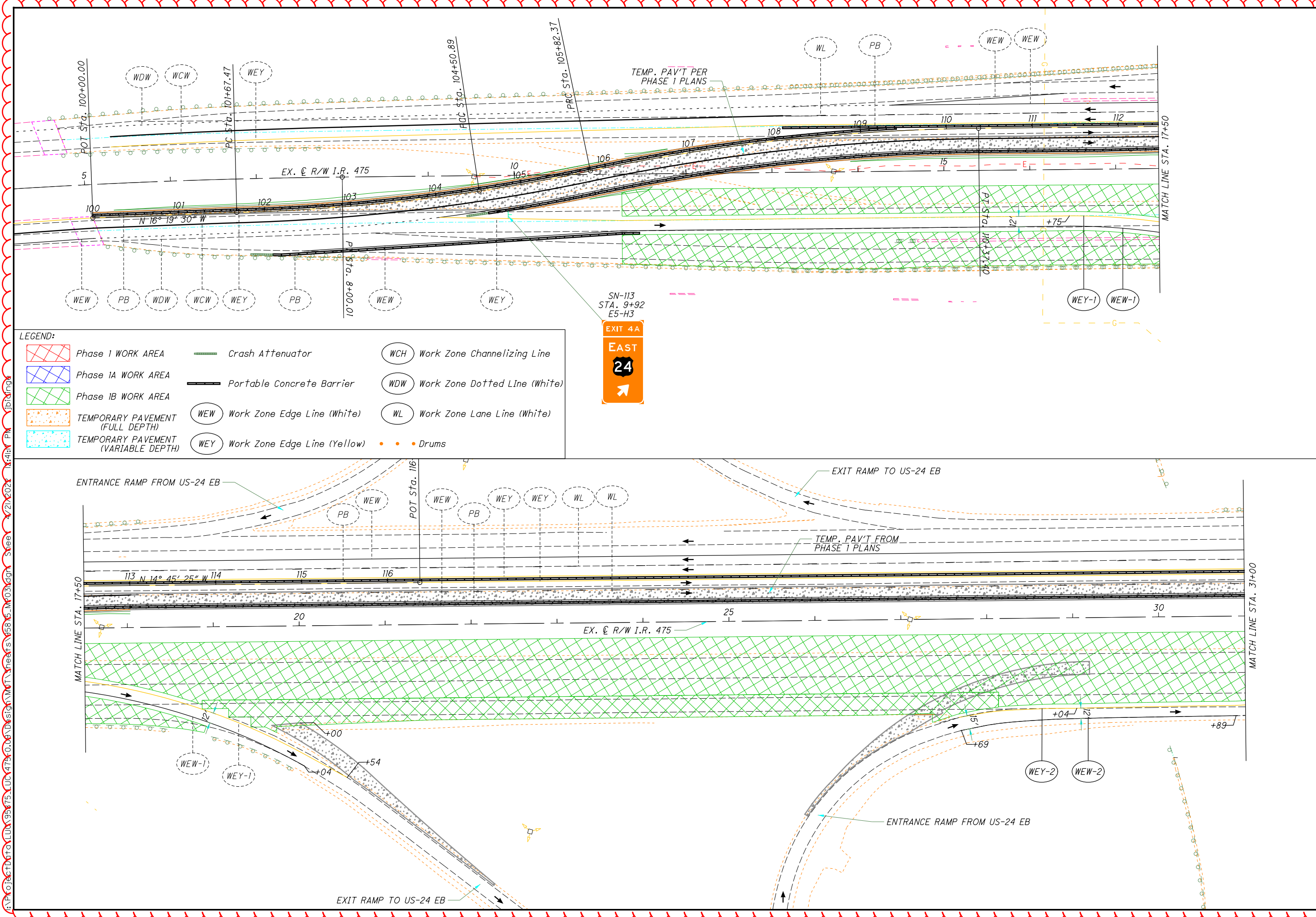
CALCULATED	MK
CHECKED	DEK

MAINTENANCE OF TRAFFIC - MAINLINE
 PHASE 1A - STA. 276+00 TO STA. 305+00

LUC-475-0.09

99

855



LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

CALCULATED MK
CHECKED DEK

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HORIZONTAL SCALE IN FEET

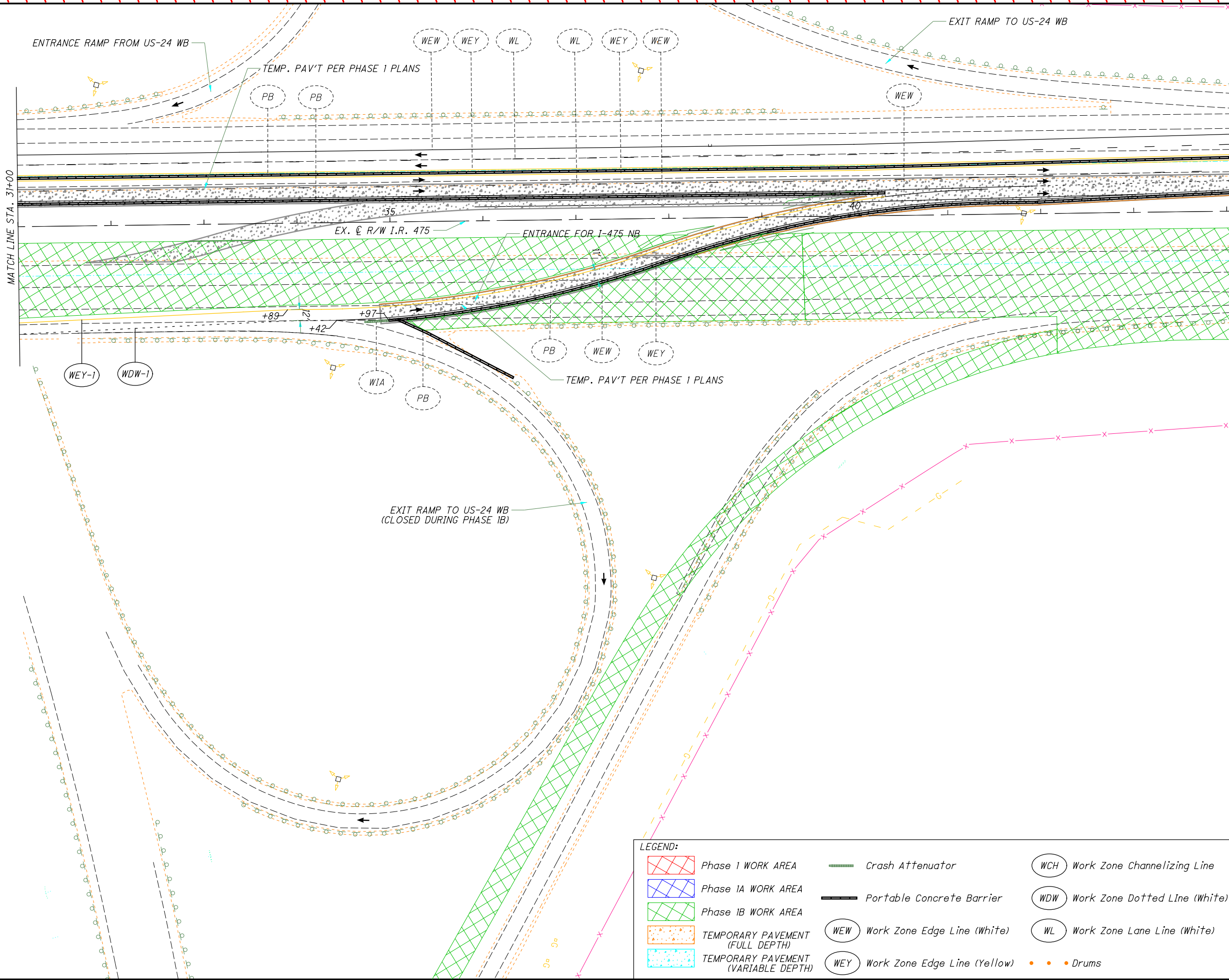
MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1B - STA. 5+00 TO STA. 31+00

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855

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













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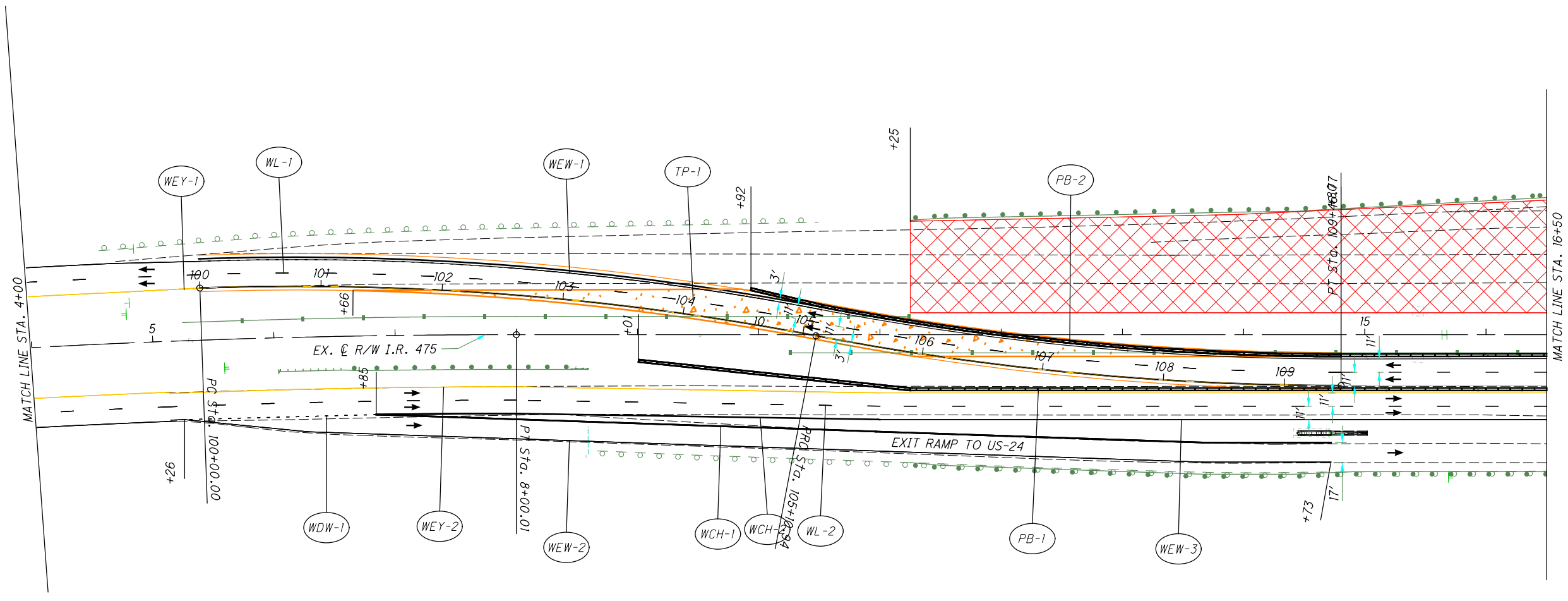
CALCULATED MK
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MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 1B - STA. 31+00 TO STA. 44+00

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 101
 855





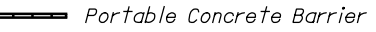








LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		WCH Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		WDW Work Zone Dotted Line (White)
	Phase 1B WORK AREA		WEW Work Zone Edge Line (White)		WL Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		WEY Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		WCH Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		WDW Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		WL Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

CALCULATED
MK
CHECKED
DEK

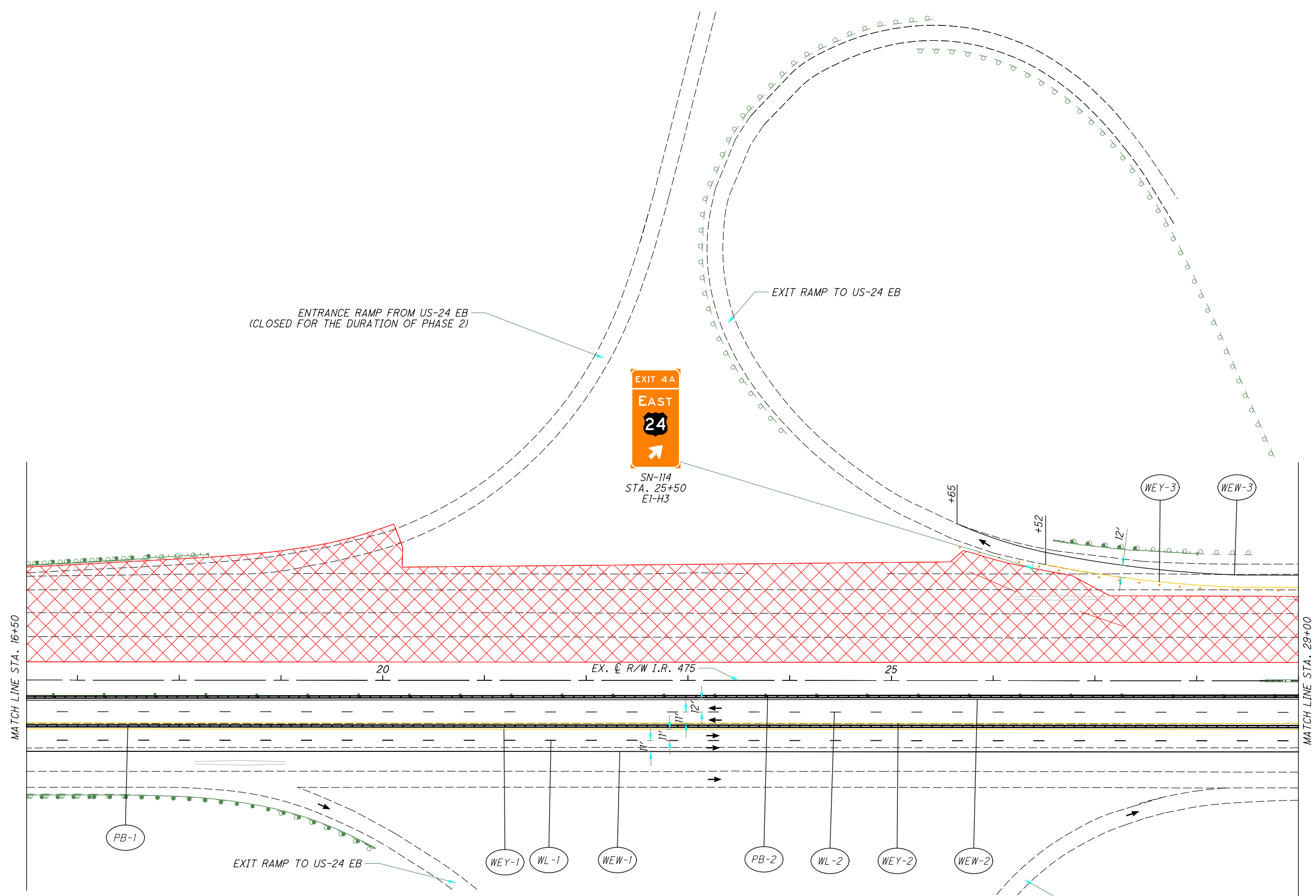



HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 4+00 TO STA. 16+50

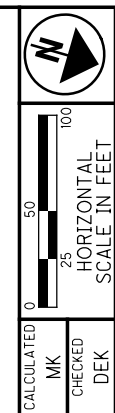
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LEGEND:

Phase 1 WORK AREA	Crash Attenuator	WCH Work Zone Channelizing Line
Phase 1A WORK AREA	Portable Concrete Barrier	WDW Work Zone Dotted Line (White)
Phase 1B WORK AREA	WEW Work Zone Edge Line (White)	WL Work Zone Lane Line (White)
TEMPORARY PAVEMENT (FULL DEPTH)	WEY Work Zone Edge Line (Yellow)	Drums
TEMPORARY PAVEMENT (VARIABLE DEPTH)		



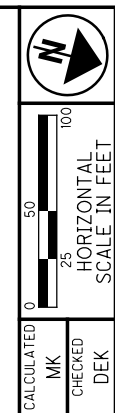
MAINTENANCE OF TRAFFIC - MAINLINE
 PHASE 2 - STA. 16+50 TO STA. 29+00

LUC-475-0.09

103
855

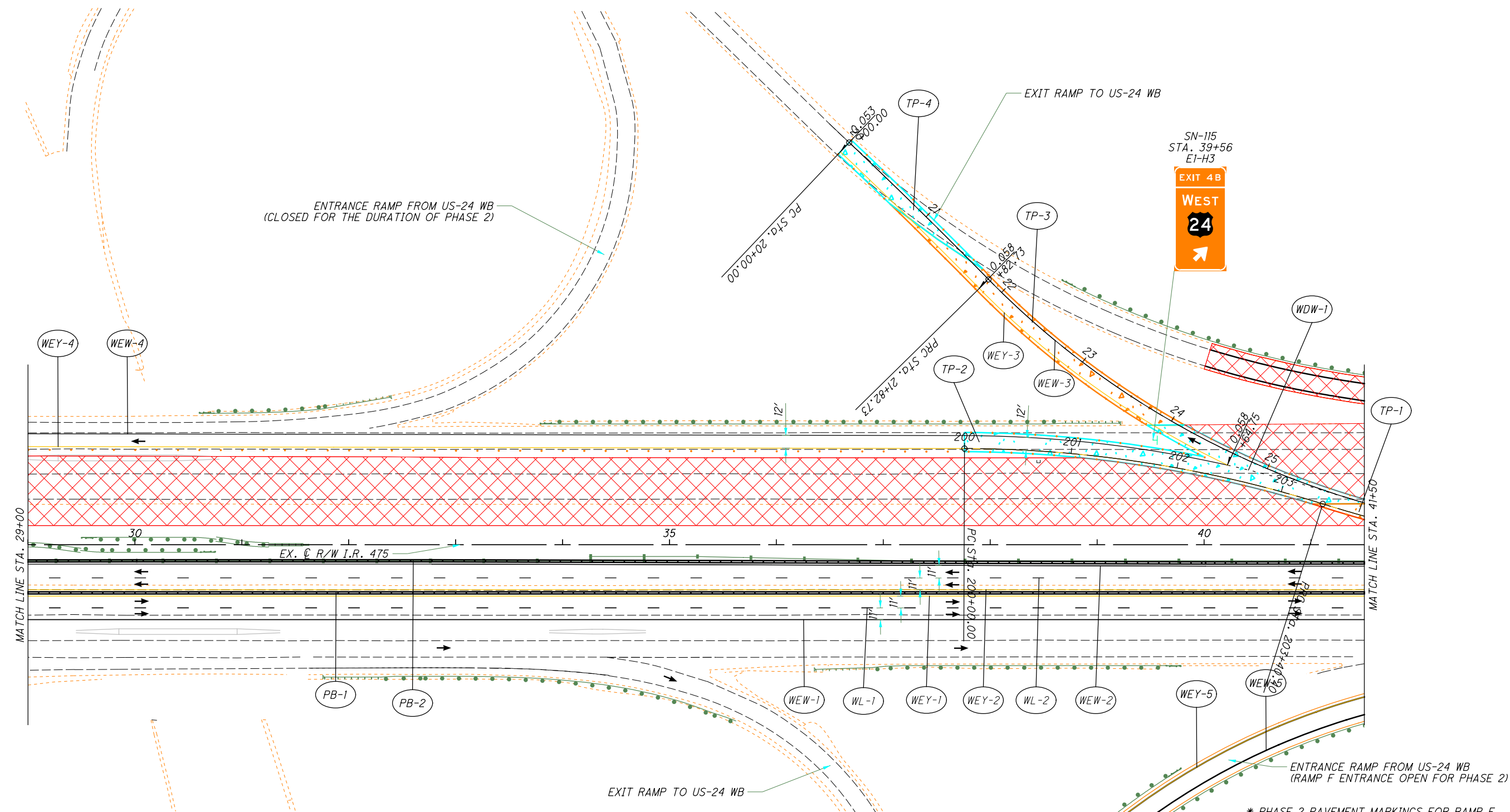
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MK
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DEK

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MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 29+00 TO STA 41+50

LUC-475-0.09

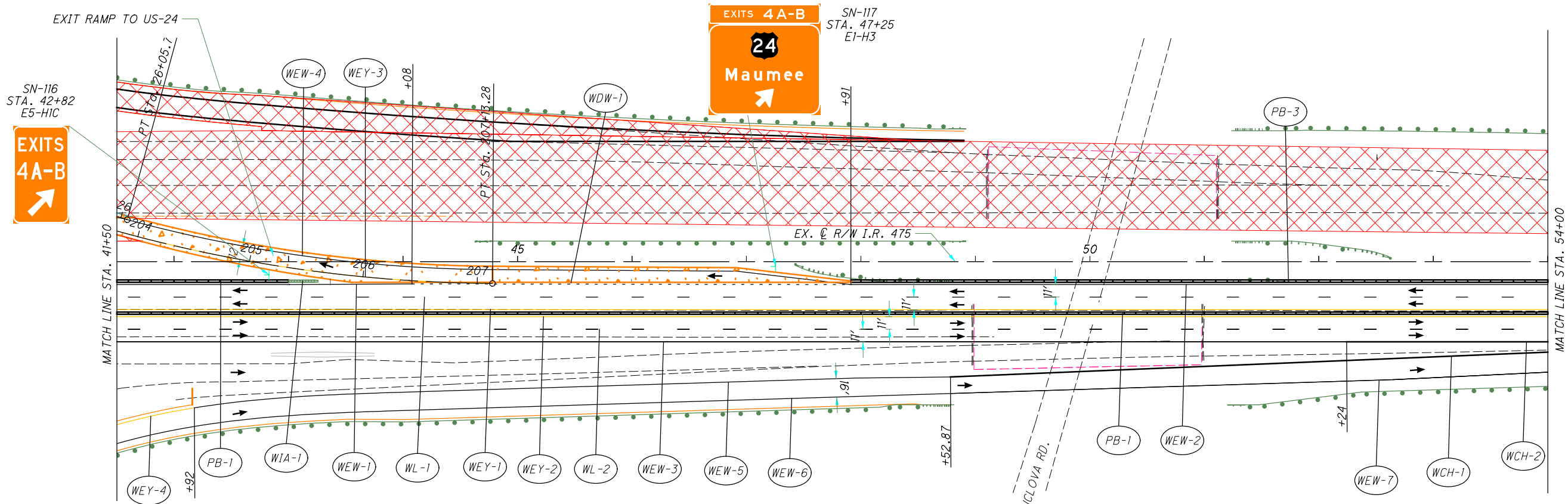


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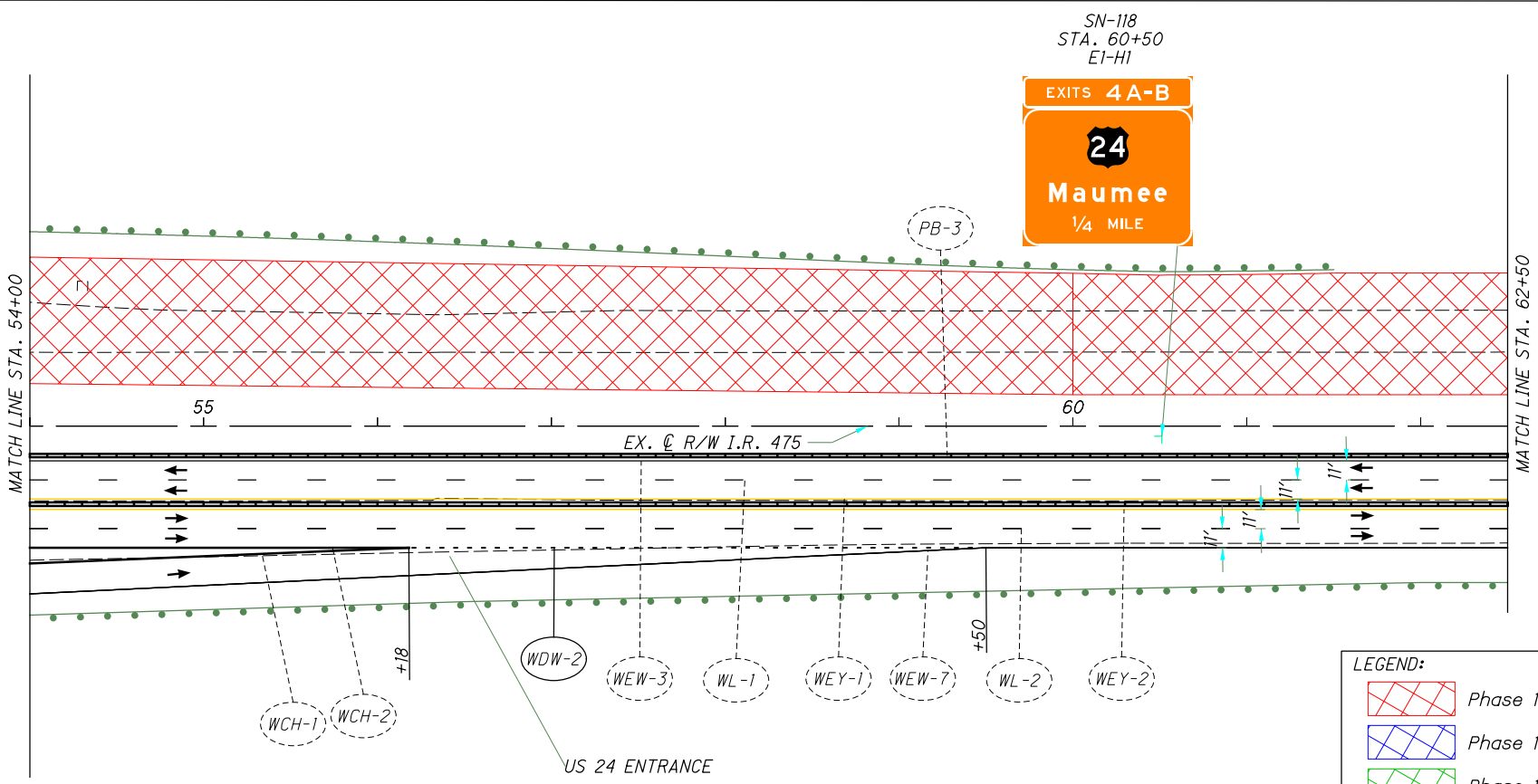
	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

* PHASE 2 PAVEMENT MARKINGS FOR RAMP F ARE FROM STATION 26+43.22 TO 41+50 (SHOWN ON SHEETS 321-324)

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* PHASE 2 PAVEMENT MARKINGS FOR RAMP F
ARE FROM STATION 41+50 TO 48+52.87
(SHOWN ON SHEETS 324-325)



LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

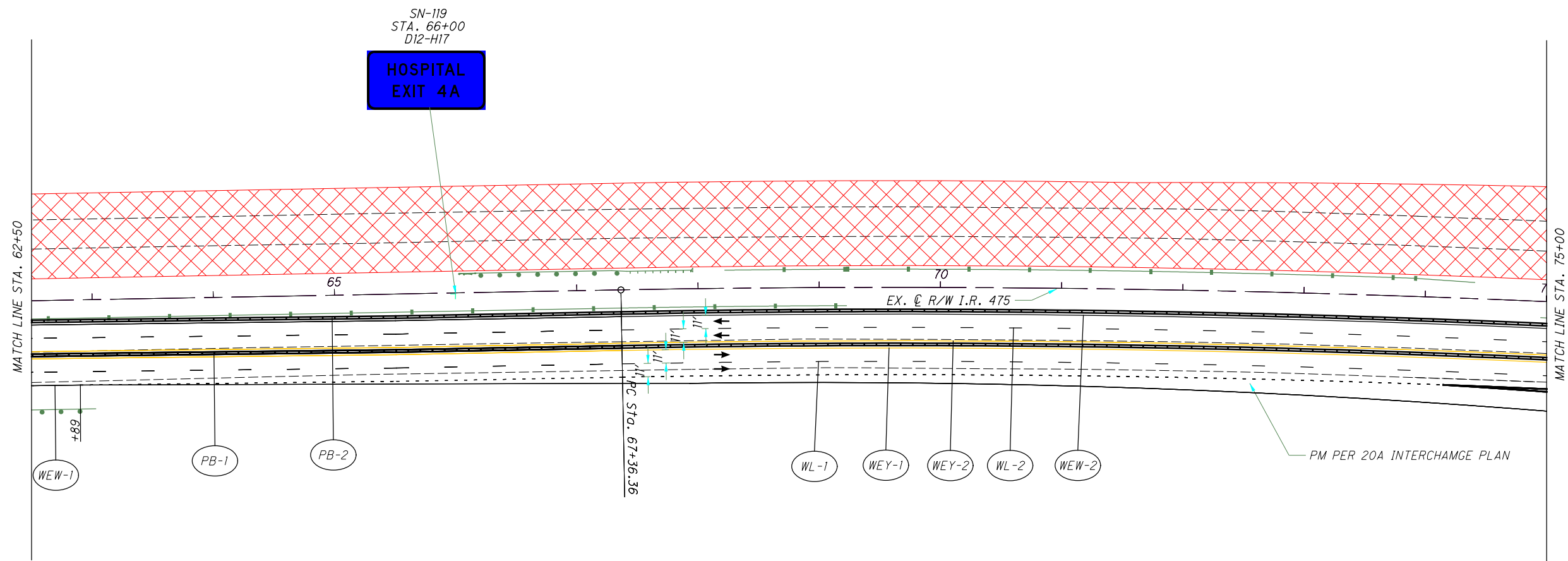


MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 41+50 TO STA. 62+50





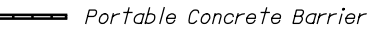








LUC-475-0.09

105
855

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LEGEND:

 Phase 1 WORK AREA	 Crash Attenuator	 WCH Work Zone Channelizing Line
 Phase 1A WORK AREA	 Portable Concrete Barrier	 WDW Work Zone Dotted Line (White)
 Phase 1B WORK AREA	 WEW Work Zone Edge Line (White)	 WL Work Zone Lane Line (White)
 TEMPORARY PAVEMENT (FULL DEPTH)	 WEY Work Zone Edge Line (Yellow)	 Drums
 TEMPORARY PAVEMENT (VARIABLE DEPTH)		



MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 62+50 TO STA. 75+00

LUC-475-0.09

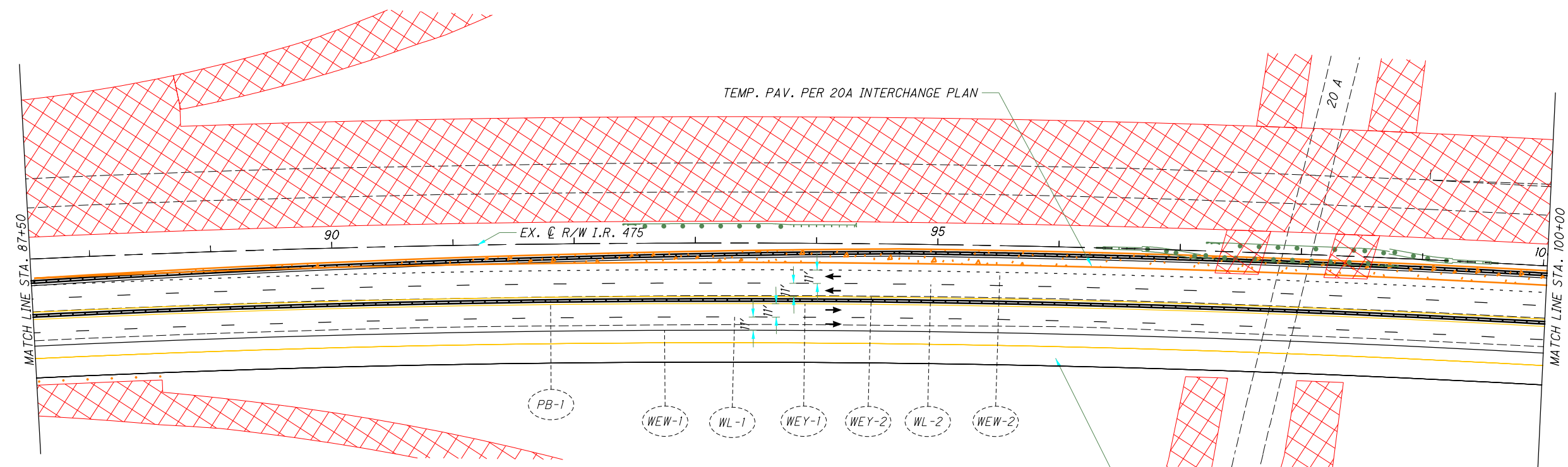
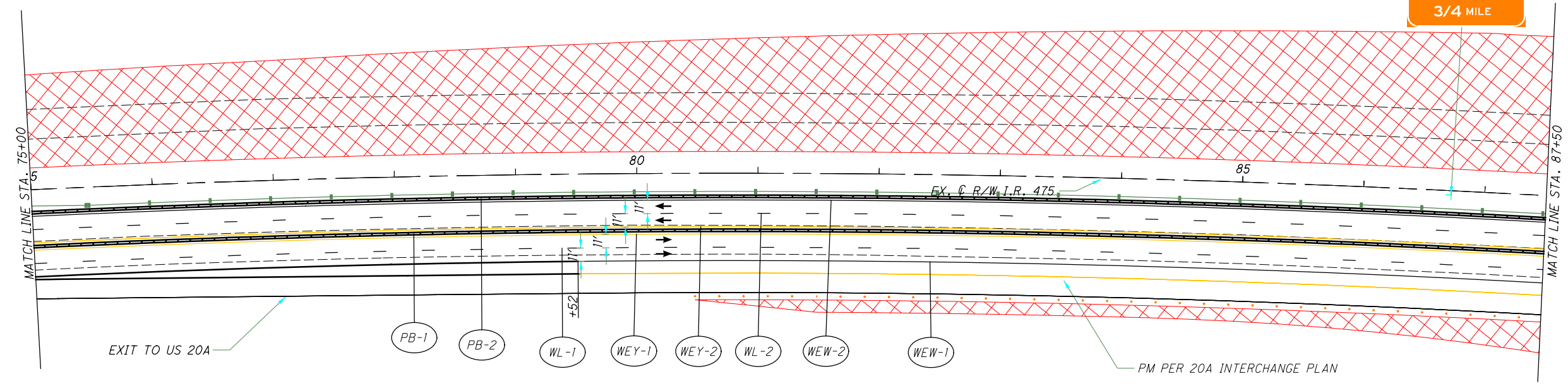
106
855

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SN-120
STA. 86+70
E1-H1
EXITS 4A-B
24
Maumee
3/4 MILE

CALCULATED MK CHECKED DEK

HORIZONTAL SCALE IN FEET



LEGEND:

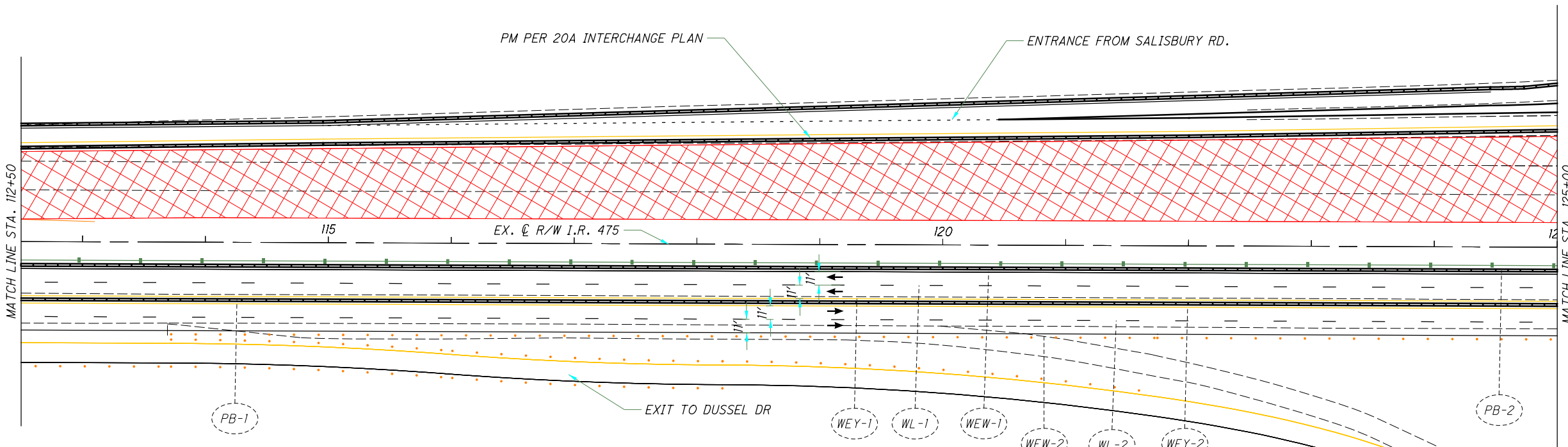
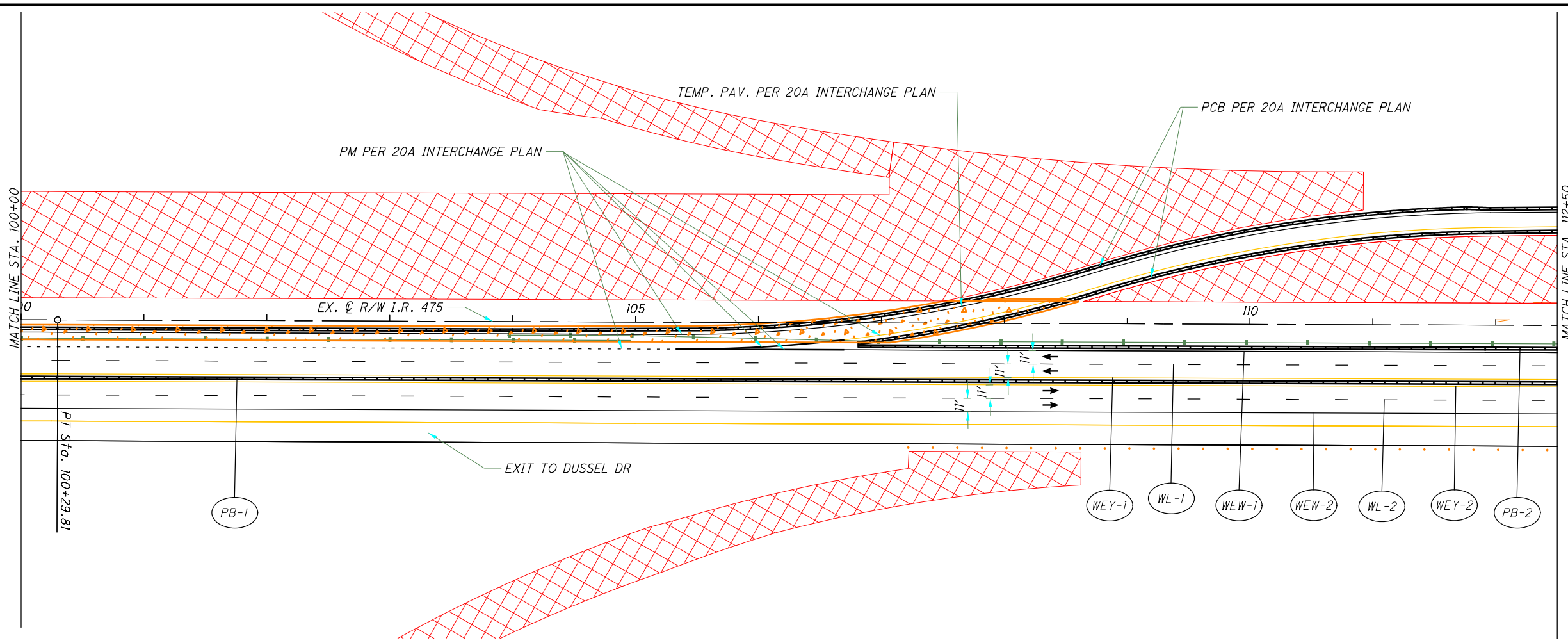
	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 75+00 TO STA. 100+00














LUC-475-0.09

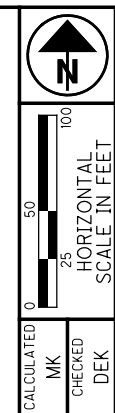
107
855

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LEGEND:

 Phase 1 WORK AREA	 Crash Attenuator	 WCH Work Zone Channelizing Line
 Phase 1A WORK AREA	 Portable Concrete Barrier	 WDW Work Zone Dotted Line (White)
 Phase 1B WORK AREA	 WEW Work Zone Edge Line (White)	 WL Work Zone Lane Line (White)
 TEMPORARY PAVEMENT (FULL DEPTH)	 WEY Work Zone Edge Line (Yellow)	 Drums
 TEMPORARY PAVEMENT (VARIABLE DEPTH)		

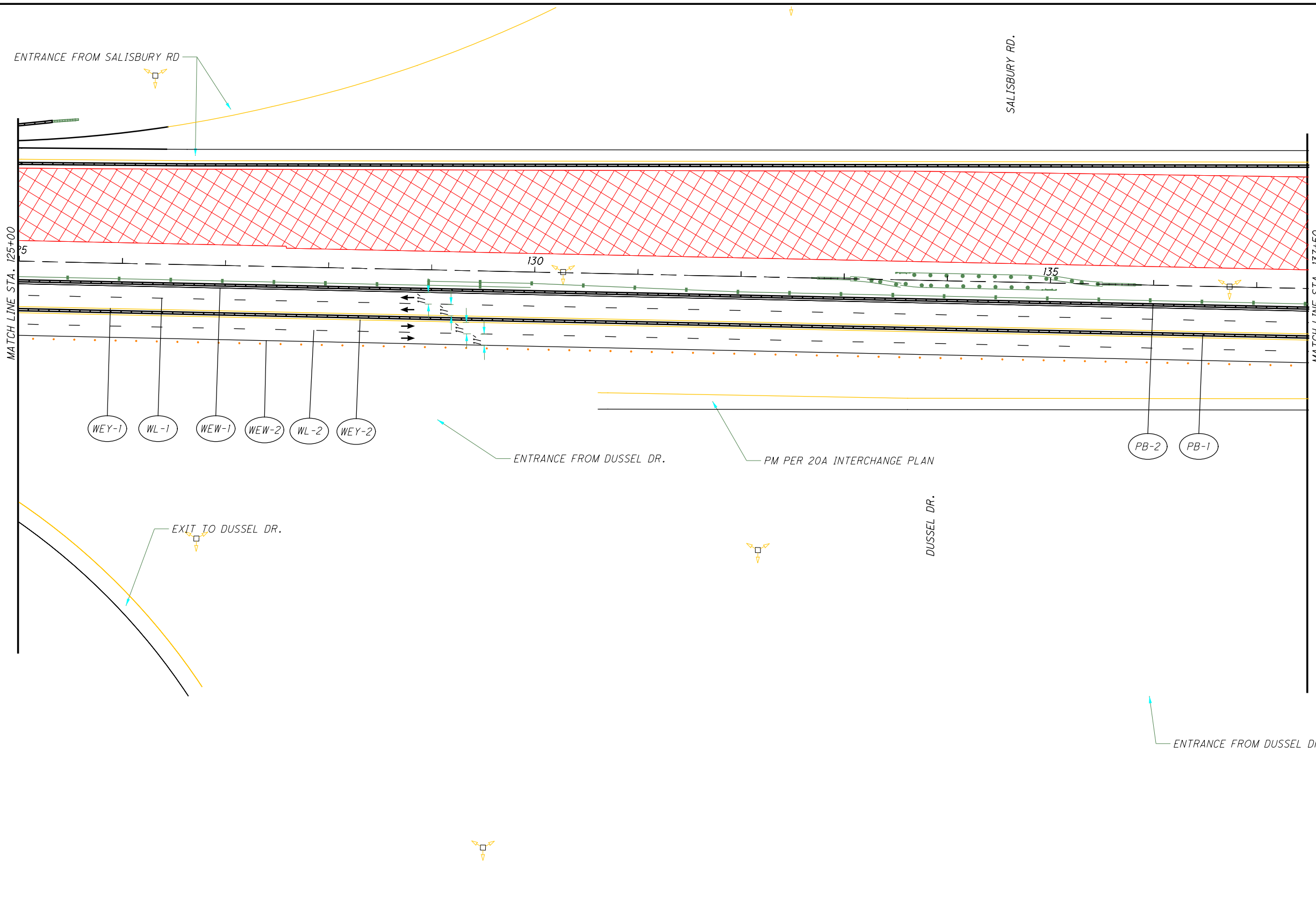


MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 100+00 TO 125+00

LUC-475-0.09

108
855

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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

CALCULATED
MK
CHECKED
DEK

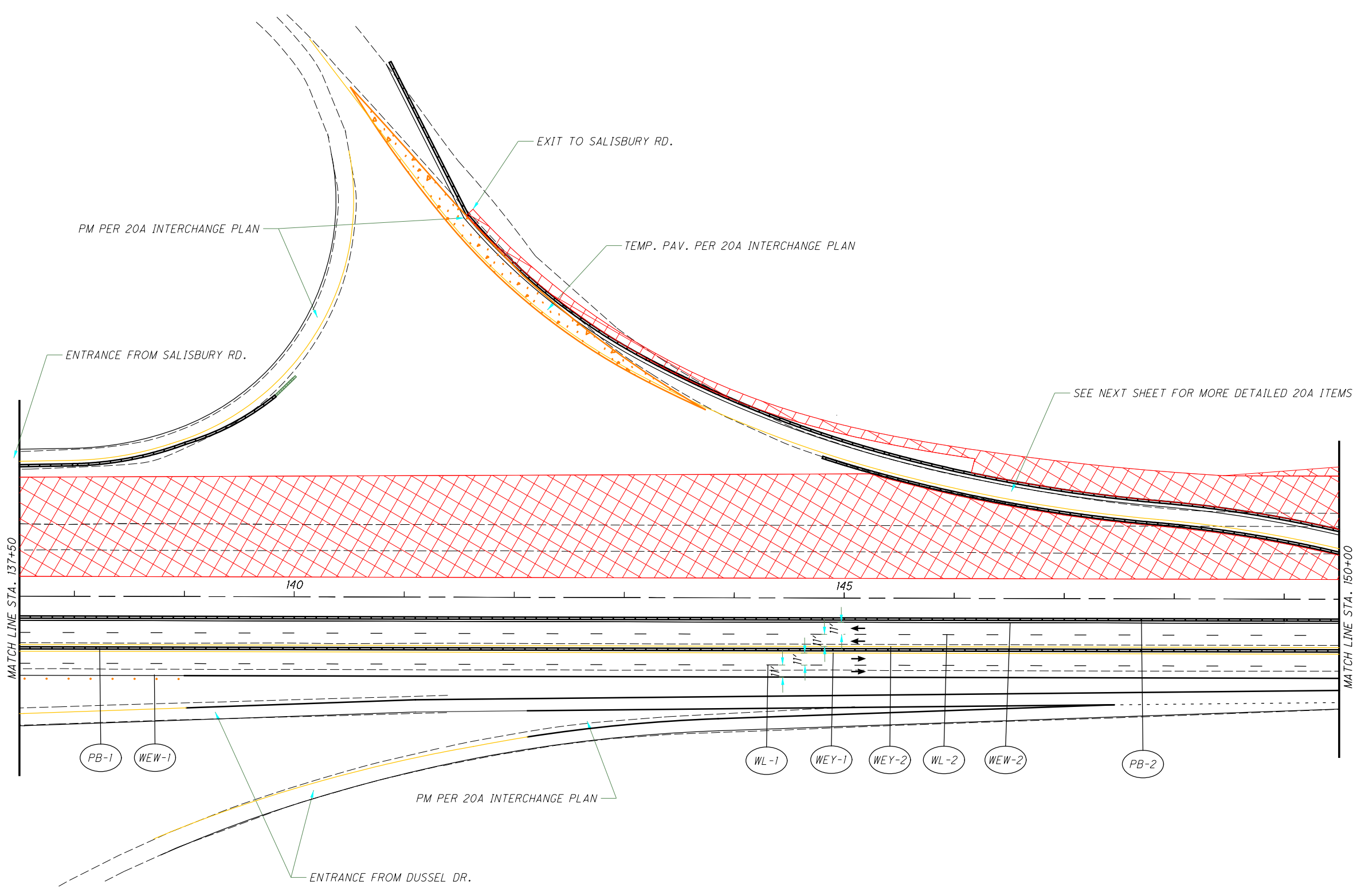
0 50 100
HORIZONTAL
SCALE IN FEET

**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 137+50 TO 150+00**

LUC-475-0.09

109
855

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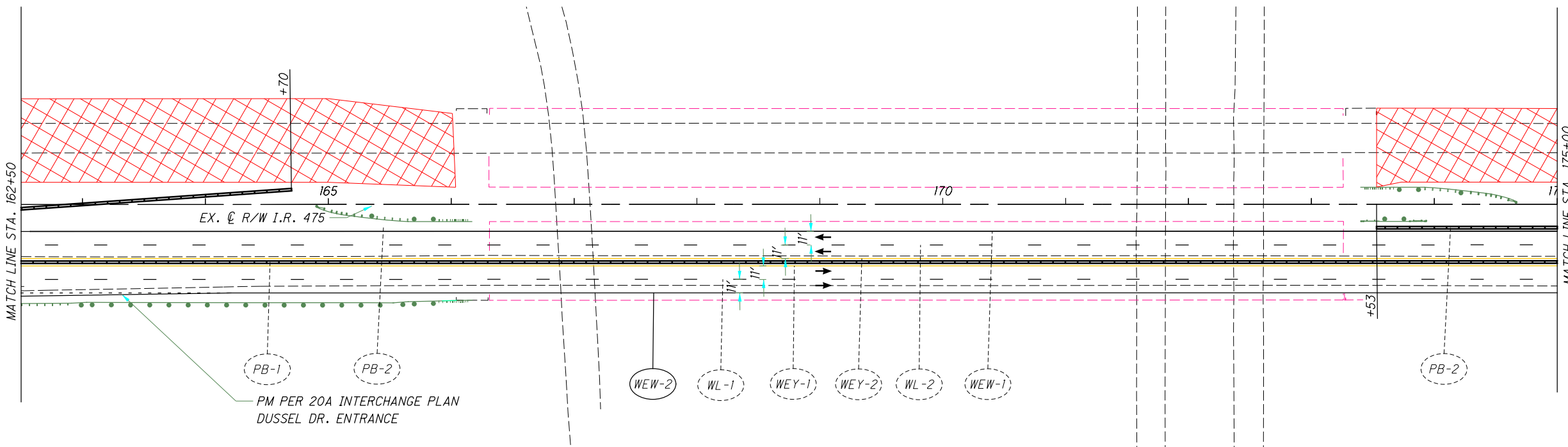
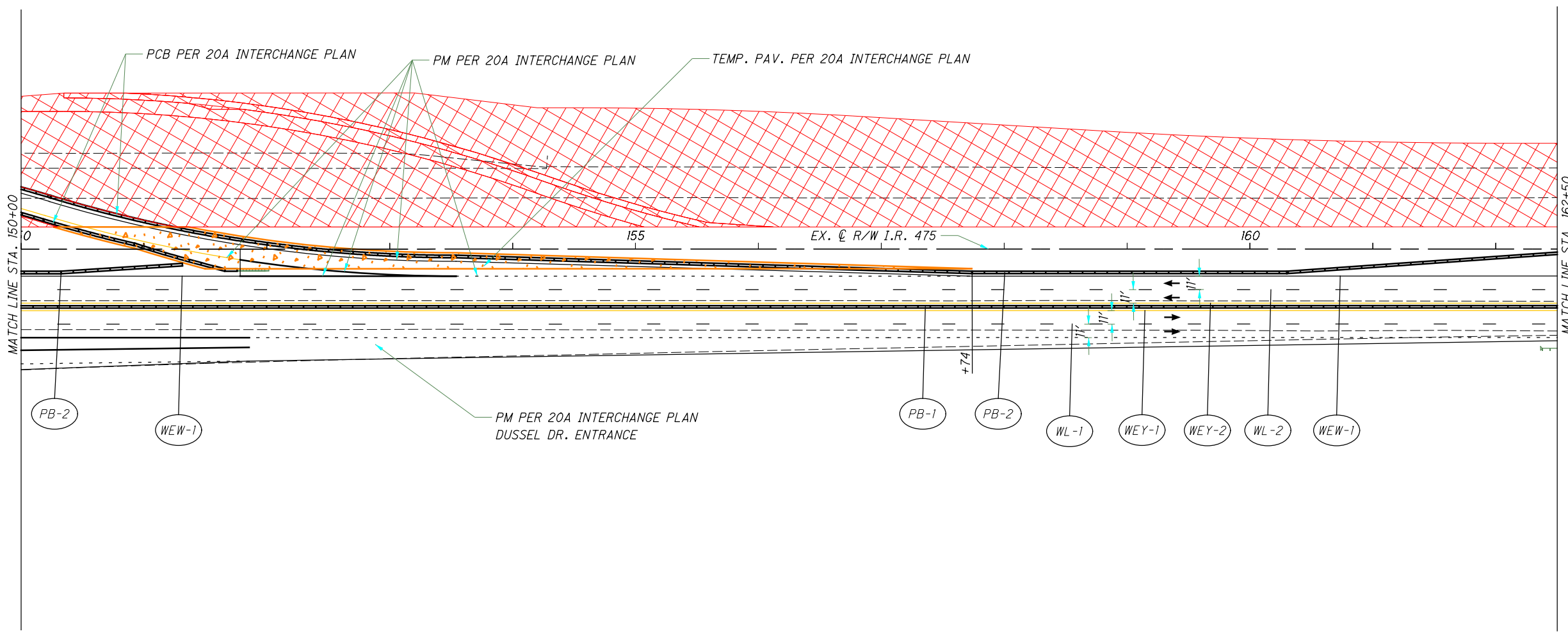
	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

CALCULATED
MK
CHECKED
DEK

0 50 100
HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 137+50 TO 150+00

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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

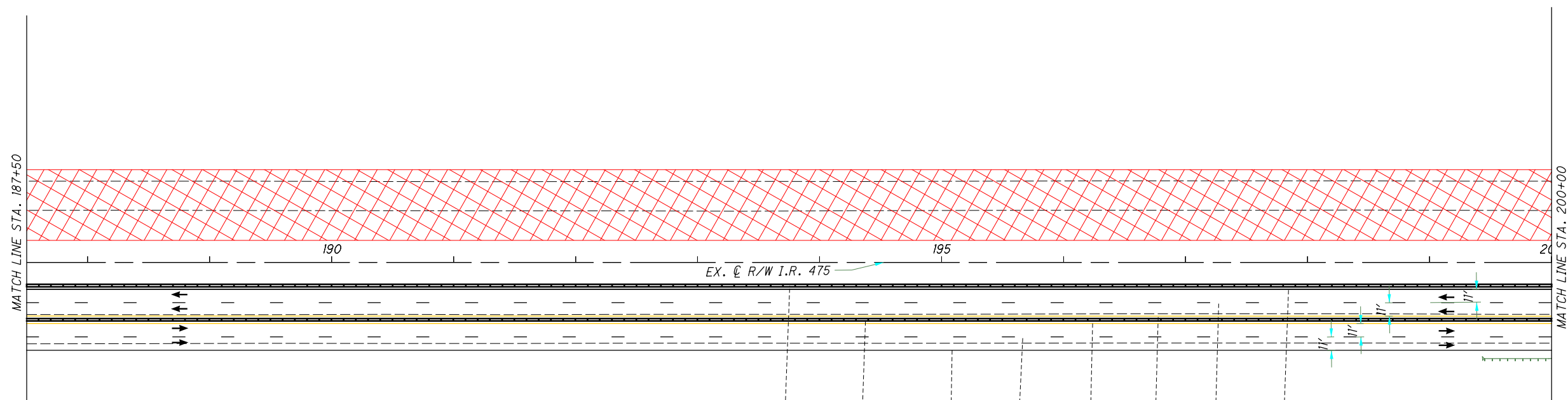
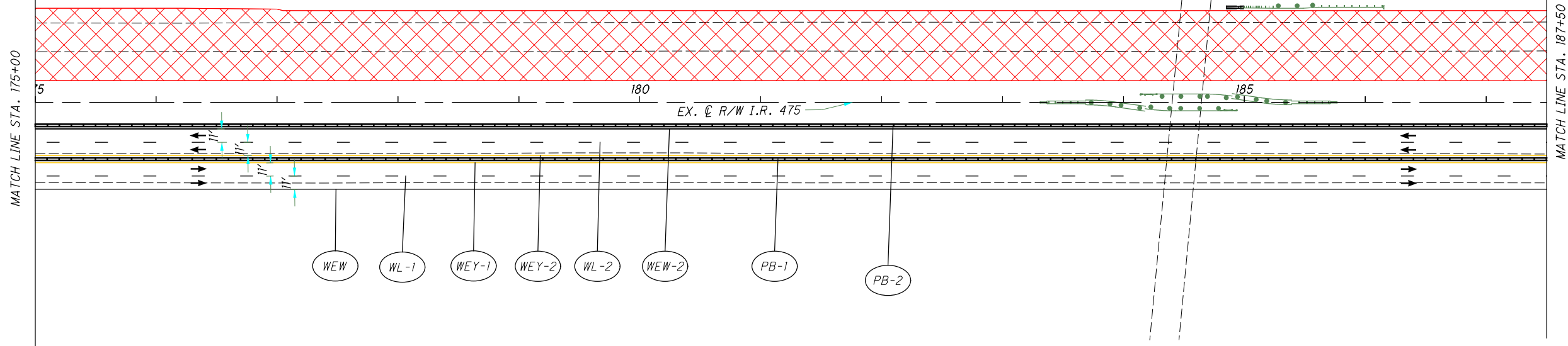


MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 150+00 TO 175+00

LUC-475-0.09

111
855

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LEGEND:

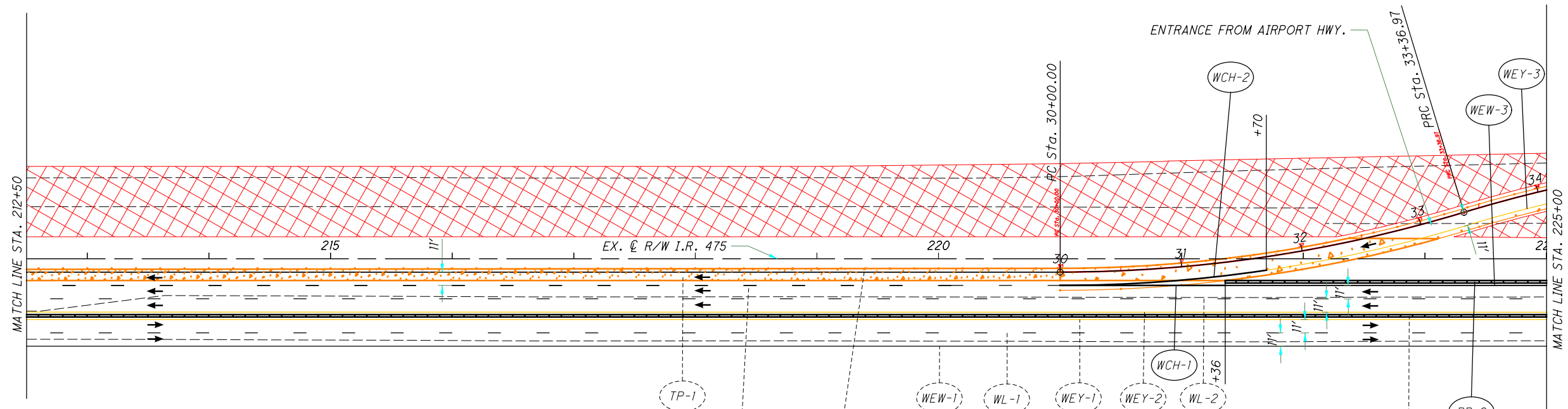
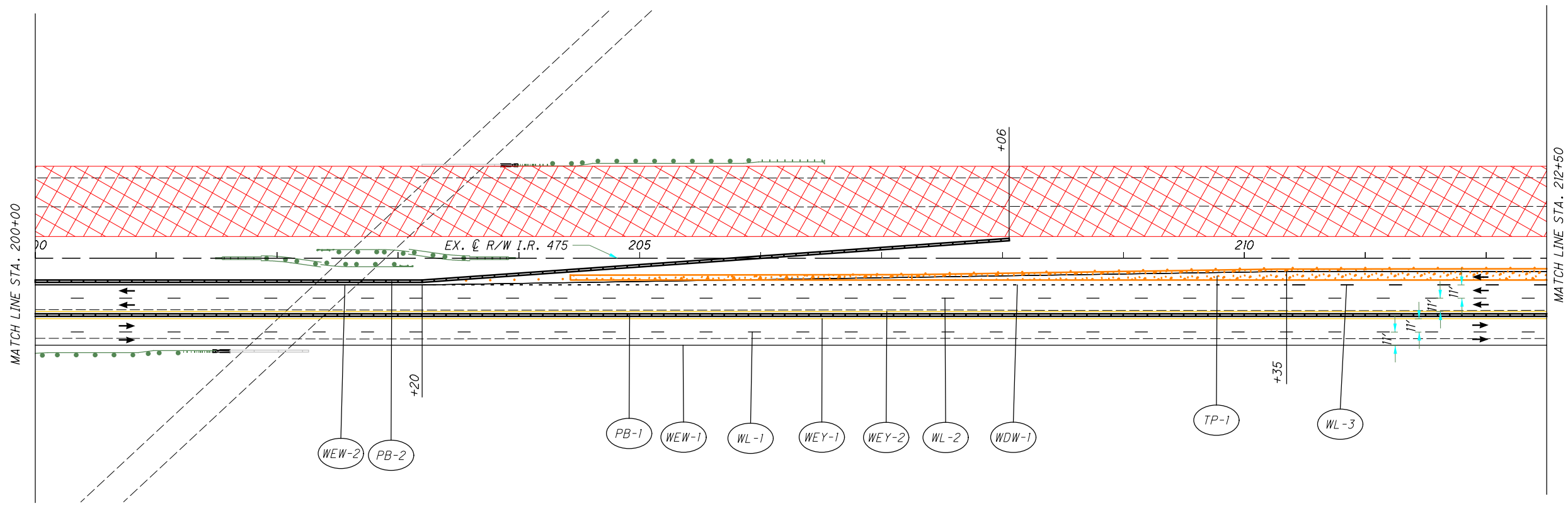
	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				



MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 175+00 TO STA. 200+00

LUC-475-0.09
 112
 855

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LEGEND:

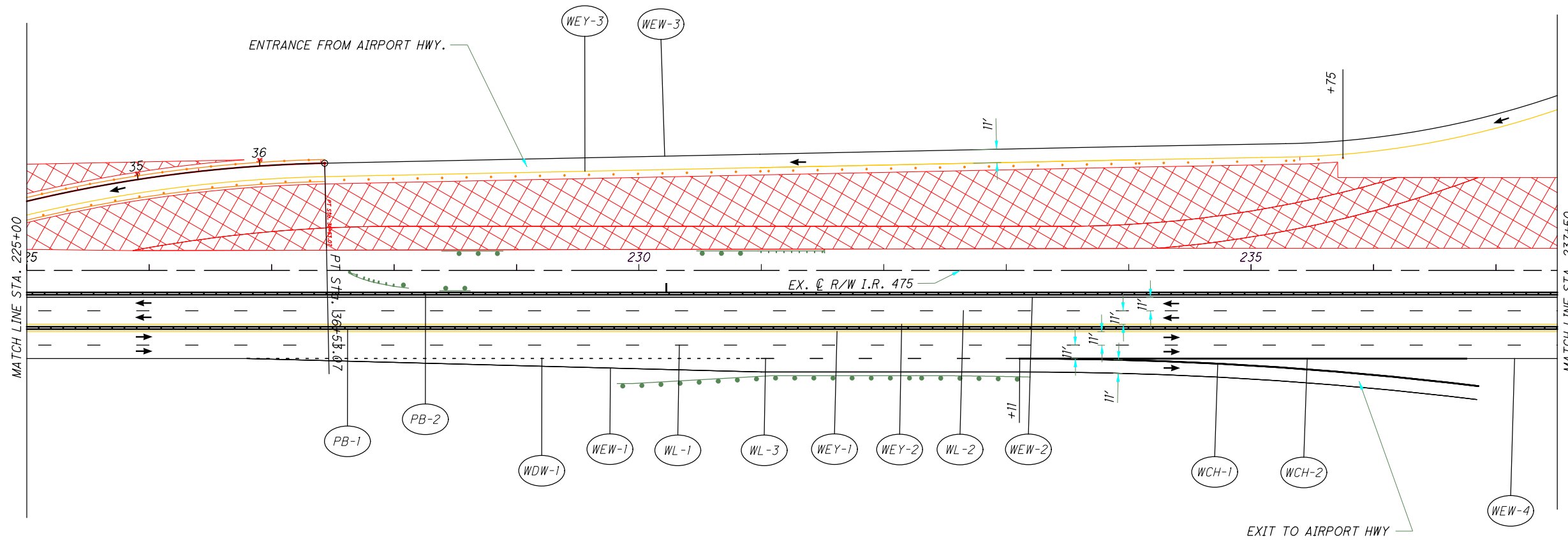
	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				



MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 200+00 TO STA. 225+00

LUC-475-0.09
 113
 855

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LEGEND:

Phase 1 WORK AREA	Crash Attenuator	WCH Work Zone Channelizing Line
Phase 1A WORK AREA	Portable Concrete Barrier	WDW Work Zone Dotted Line (White)
Phase 1B WORK AREA	WEW Work Zone Edge Line (White)	WL Work Zone Lane Line (White)
TEMPORARY PAVEMENT (FULL DEPTH)	WEY Work Zone Edge Line (Yellow)	Drums
TEMPORARY PAVEMENT (VARIABLE DEPTH)		

CALCULATED
MK
CHECKED
DEK

0 50 100
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 225+00 TO STA. 237+50

LUC-475-0.09

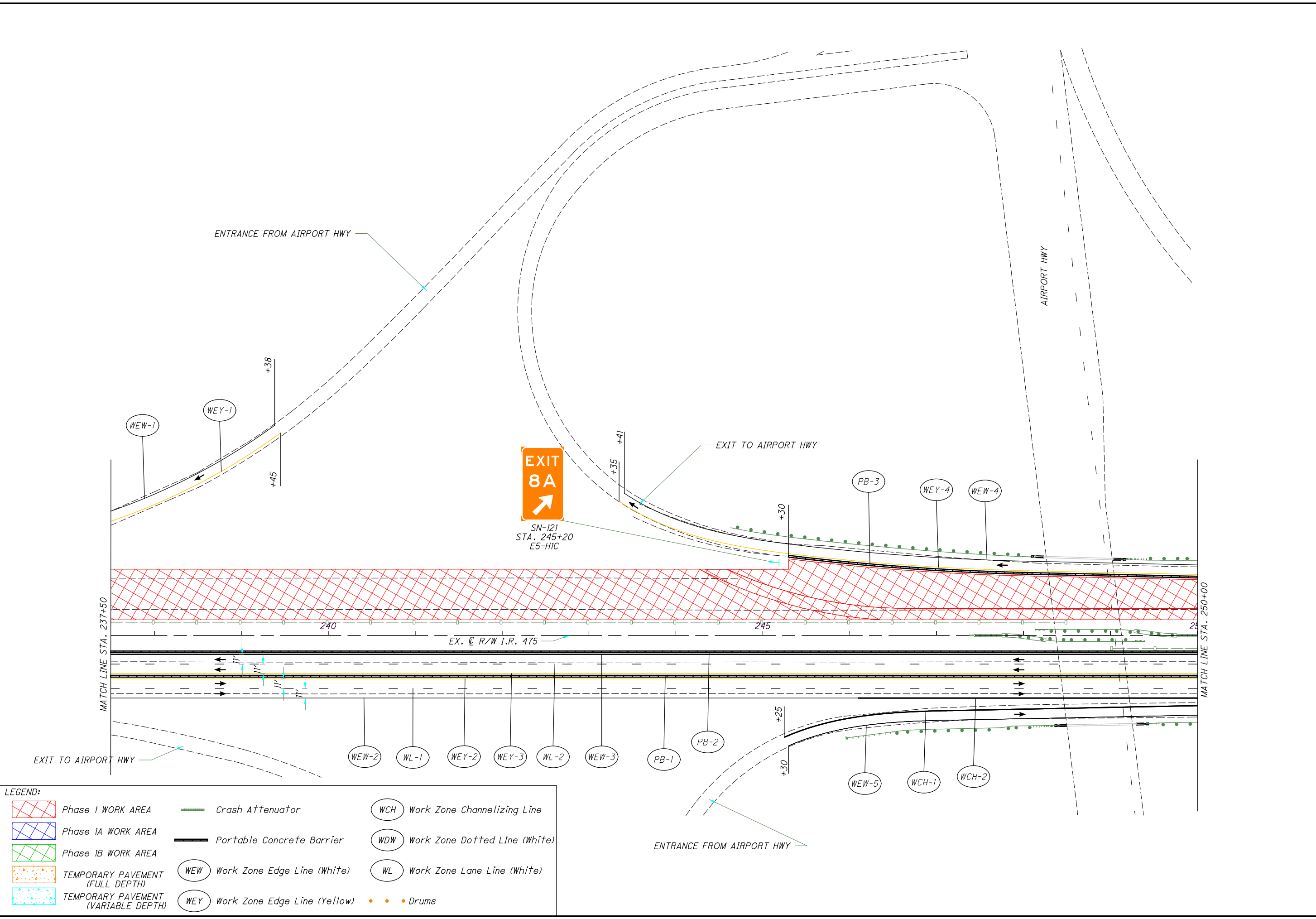
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CALCULATED
MK
CHECKED
DEK

0 50 100
25
HORIZONTAL
SCALE IN FEET

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 237+50 TO STA. 250+00

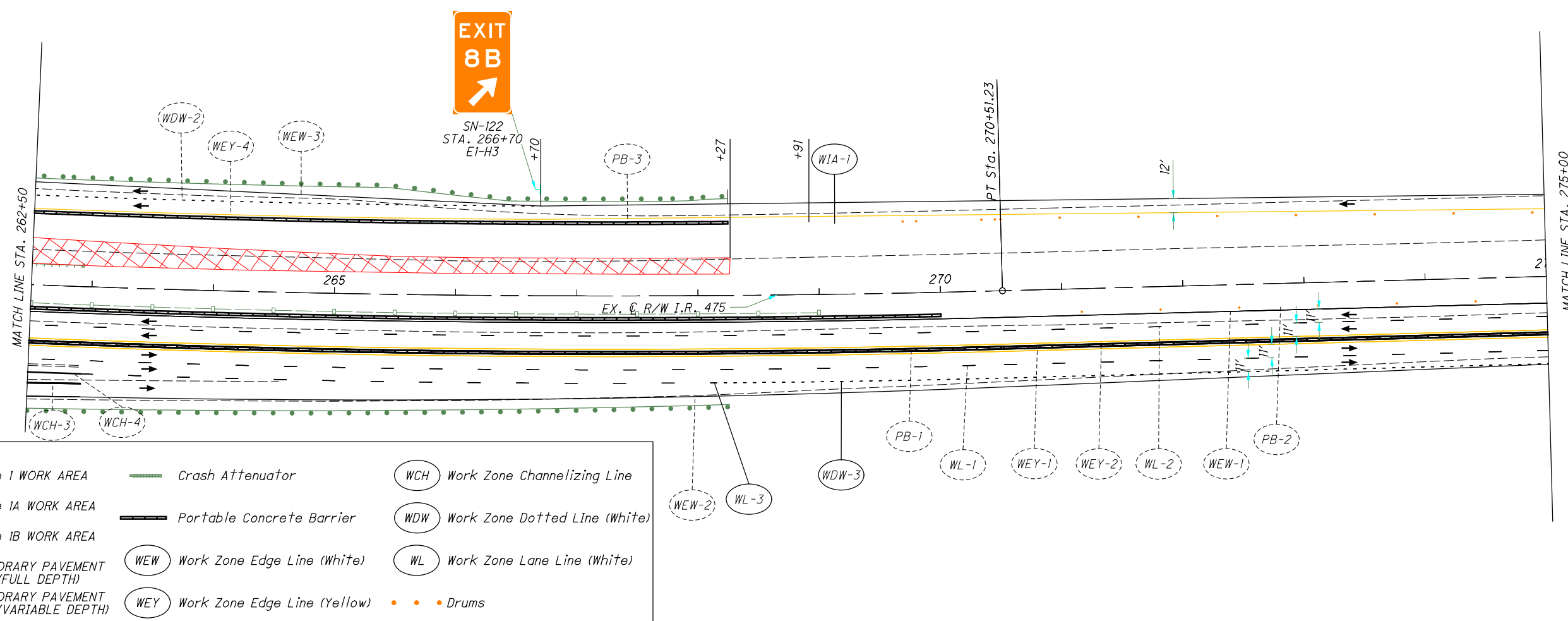
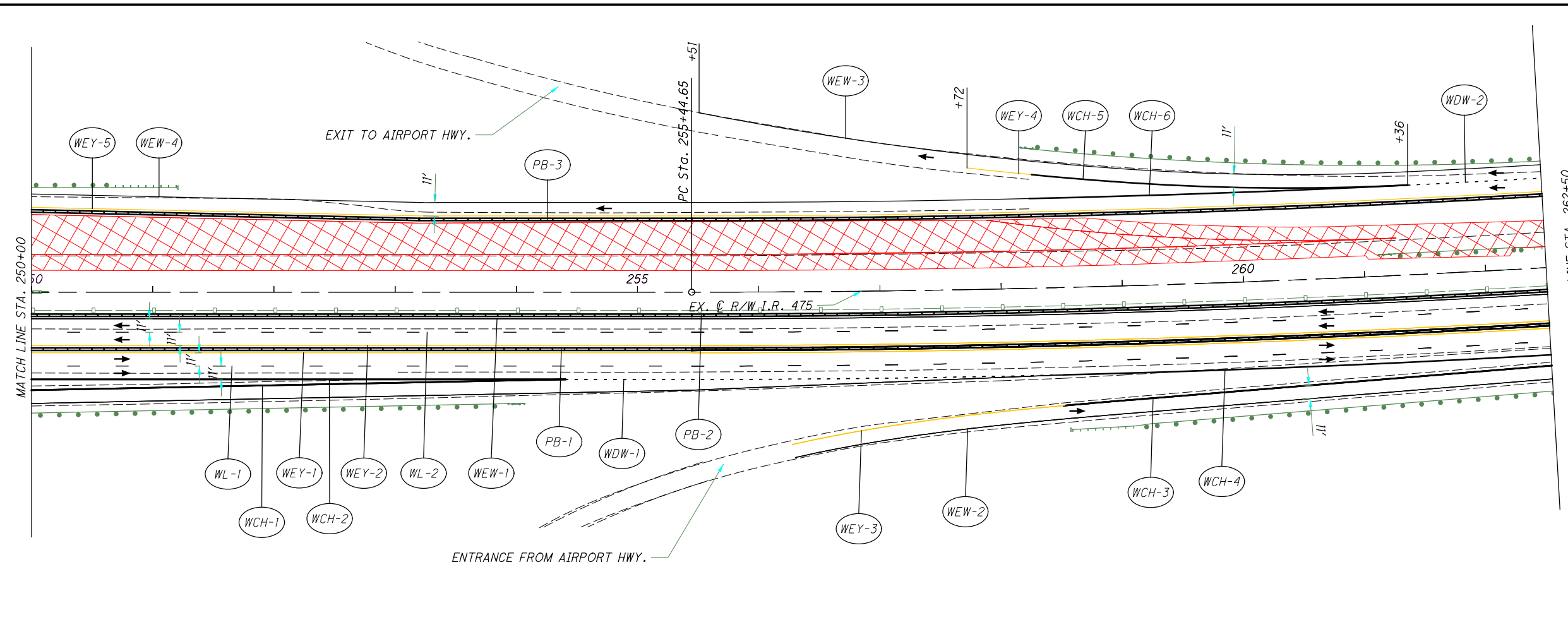
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LEGEND:

Phase 1 WORK AREA	Crash Attenuator	WCH Work Zone Channelizing Line
Phase 1A WORK AREA	Portable Concrete Barrier	WDW Work Zone Dotted Line (White)
Phase 1B WORK AREA	WEW Work Zone Edge Line (White)	WL Work Zone Lane Line (White)
TEMPORARY PAVEMENT (FULL DEPTH)	WEY Work Zone Edge Line (Yellow)	Drums
TEMPORARY PAVEMENT (VARIABLE DEPTH)		

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LEGEND:

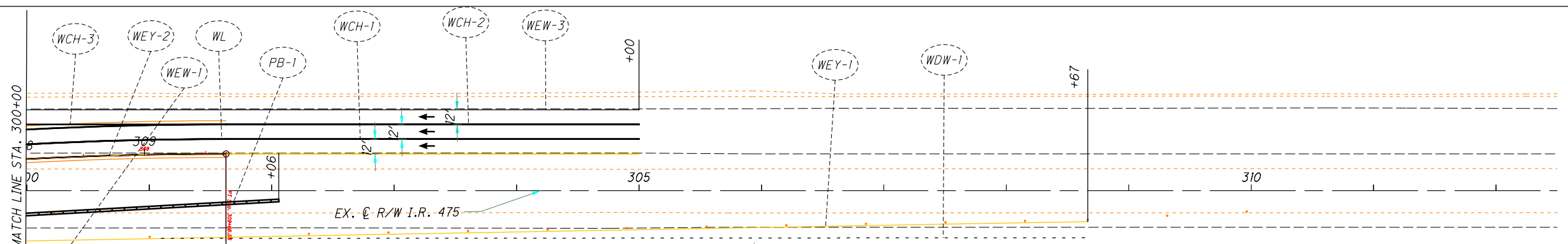
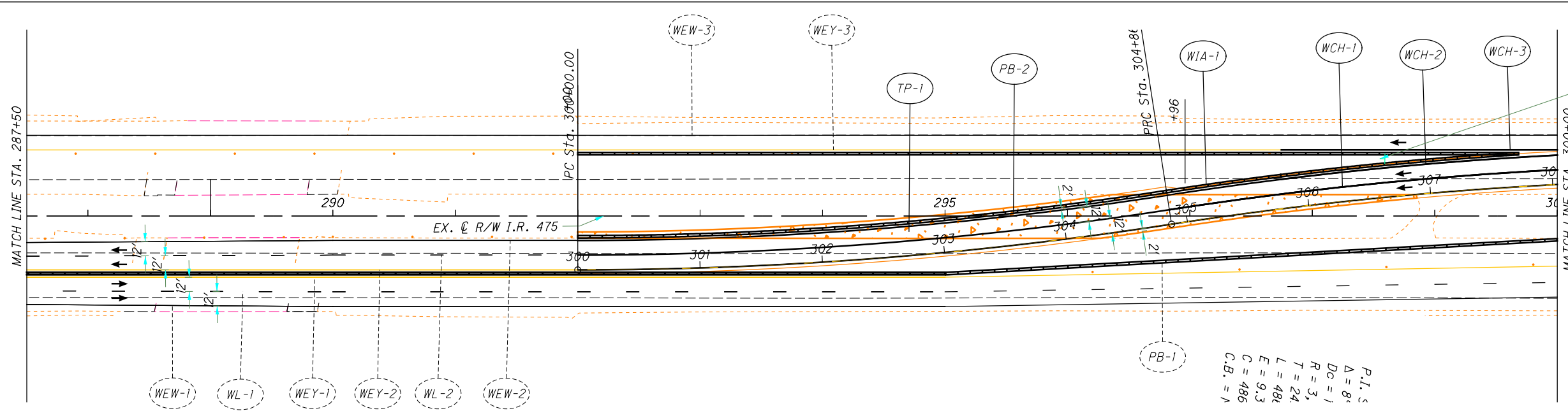
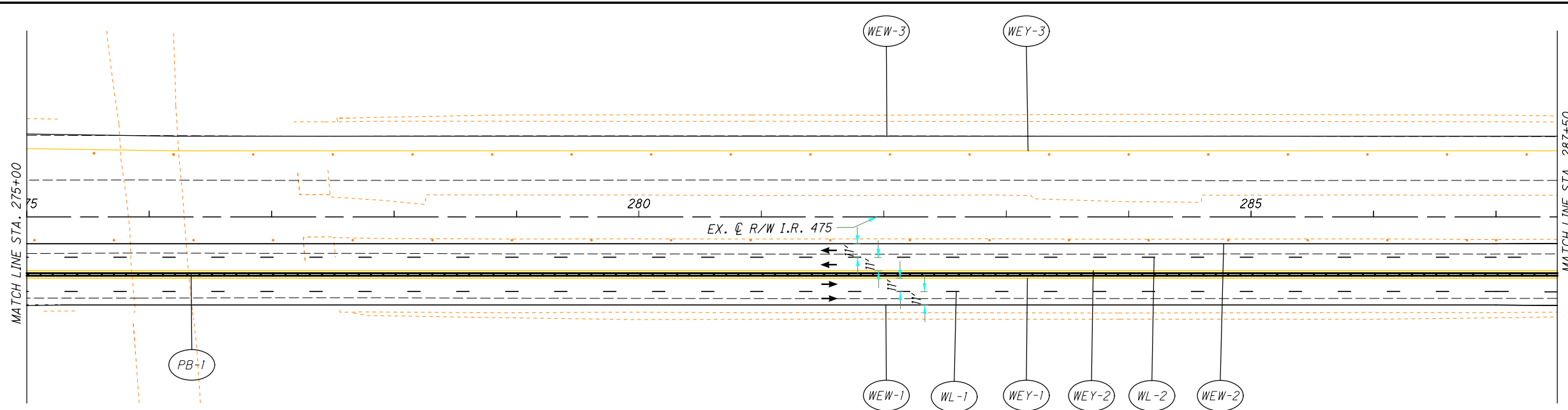
	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				



MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 250+00 TO STA. 275+00

LUC-475-0.09
 116
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SN-123
STA. 298+60
E5-HIC

P.I. S
Δ = 8'
Dc = 1
R = 3,
T = 24,
L = 48'
E = 9.3
C = 485
C.B. = 1

P.I. Sta.
Δ = 8° 47'
Dc = 1' 45'
R = 3,126.
T = 240.21'
L = 479.60'
E = 9.22'
C = 479.13'
C.B. = N 10°

LEGEND:

	Phase I WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase IA WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase IB WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				



MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2 - STA. 275+00 TO STA. 305+00

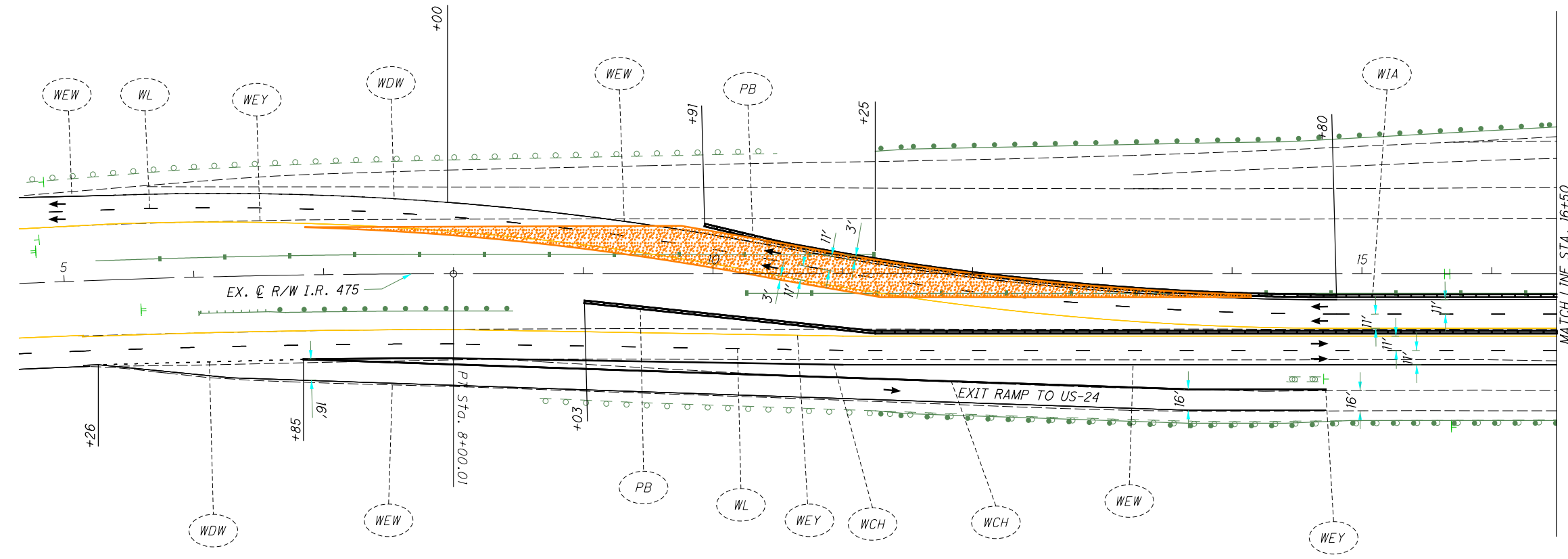
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CALCULATED
MK
CHECKED
DEK

0 50 100
25
HORIZONTAL
SCALE IN FEET

**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2A - STA. 4+00 TO STA. 16+50**

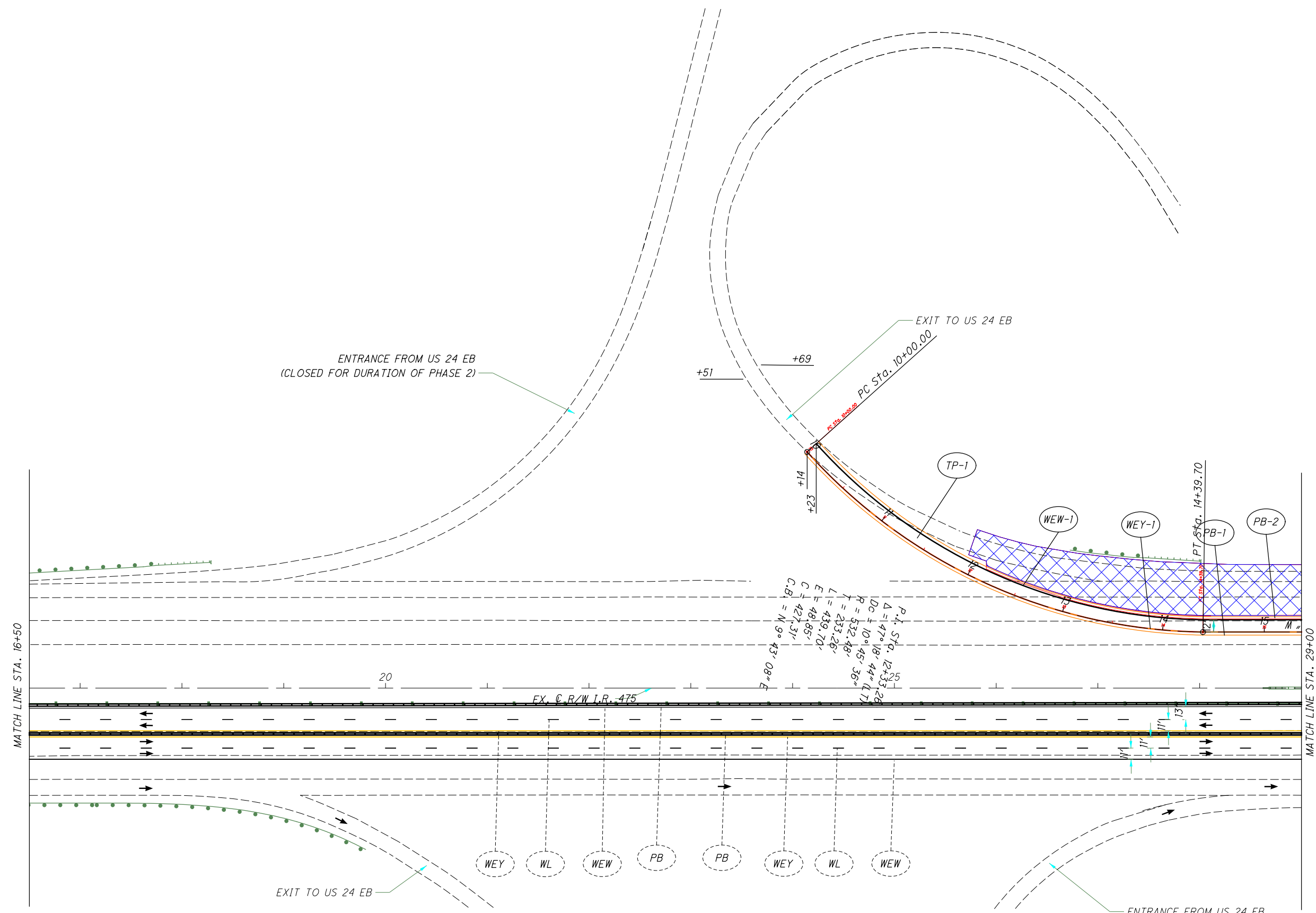
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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

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LEGEND:

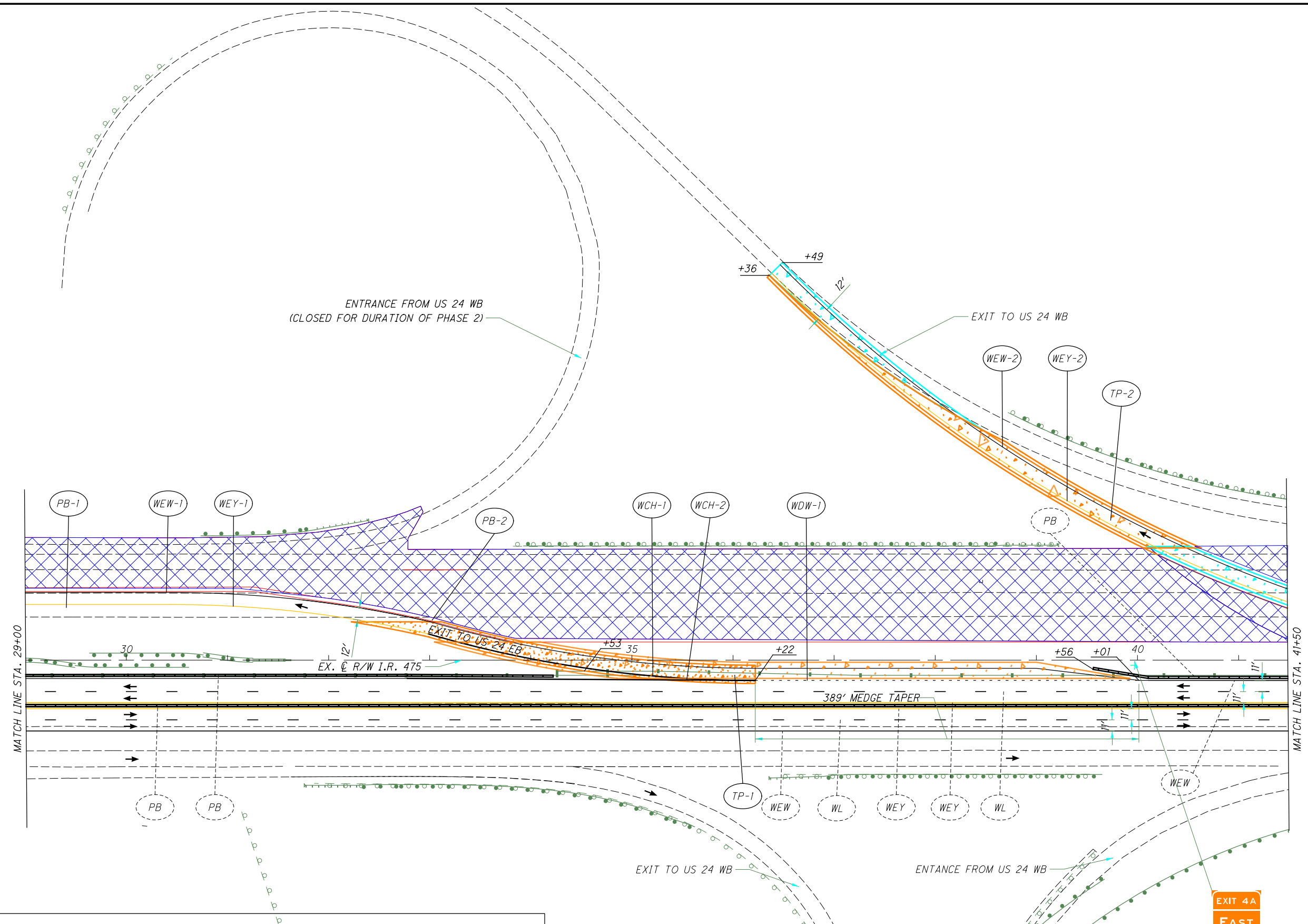
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	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				



**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2A - STA. 16+50 TO STA. 29+00**

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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				



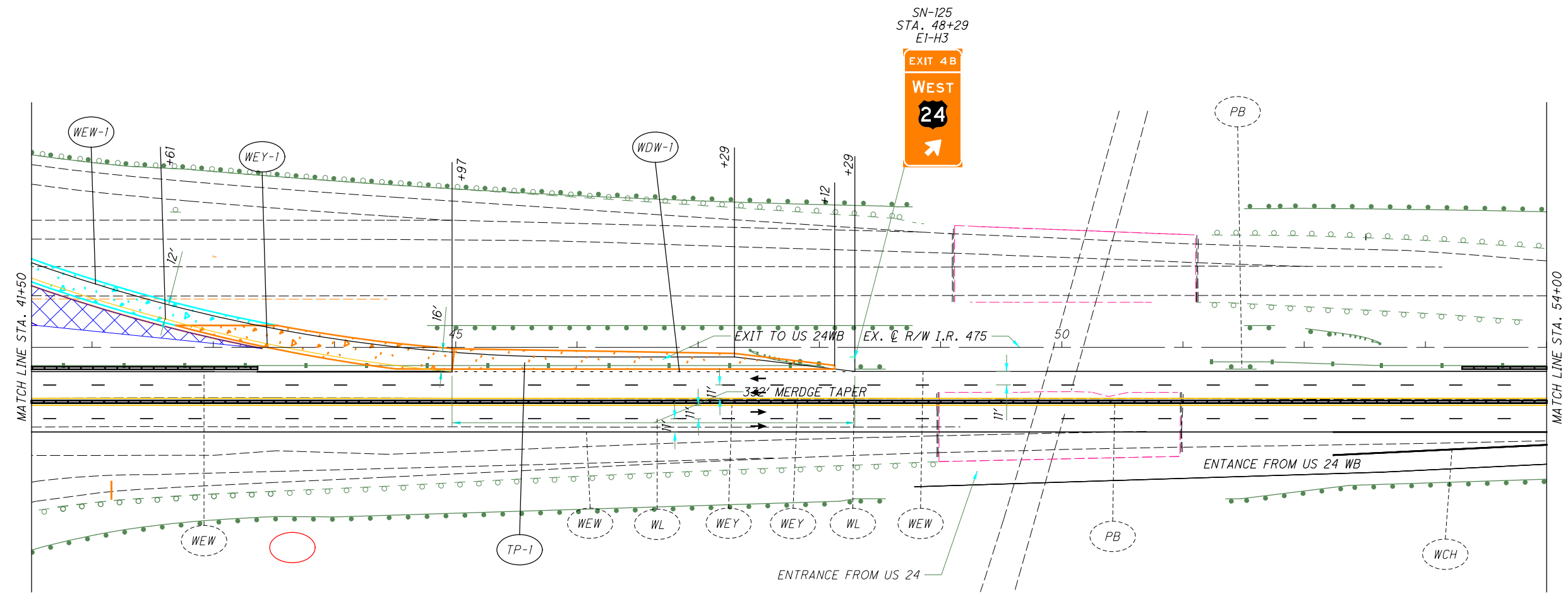
MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2A - STA. 29+00 TO STA. 41+50

LUC-475-0.09

120
855

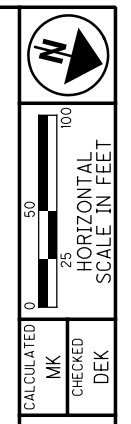


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LEGEND:

Phase 1 WORK AREA	Crash Attenuator	WCH Work Zone Channelizing Line
Phase 1A WORK AREA	Portable Concrete Barrier	WDW Work Zone Dotted Line (White)
Phase 1B WORK AREA	WEW Work Zone Edge Line (White)	WL Work Zone Lane Line (White)
TEMPORARY PAVEMENT (FULL DEPTH)	WEY Work Zone Edge Line (Yellow)	Drums
TEMPORARY PAVEMENT (VARIABLE DEPTH)		



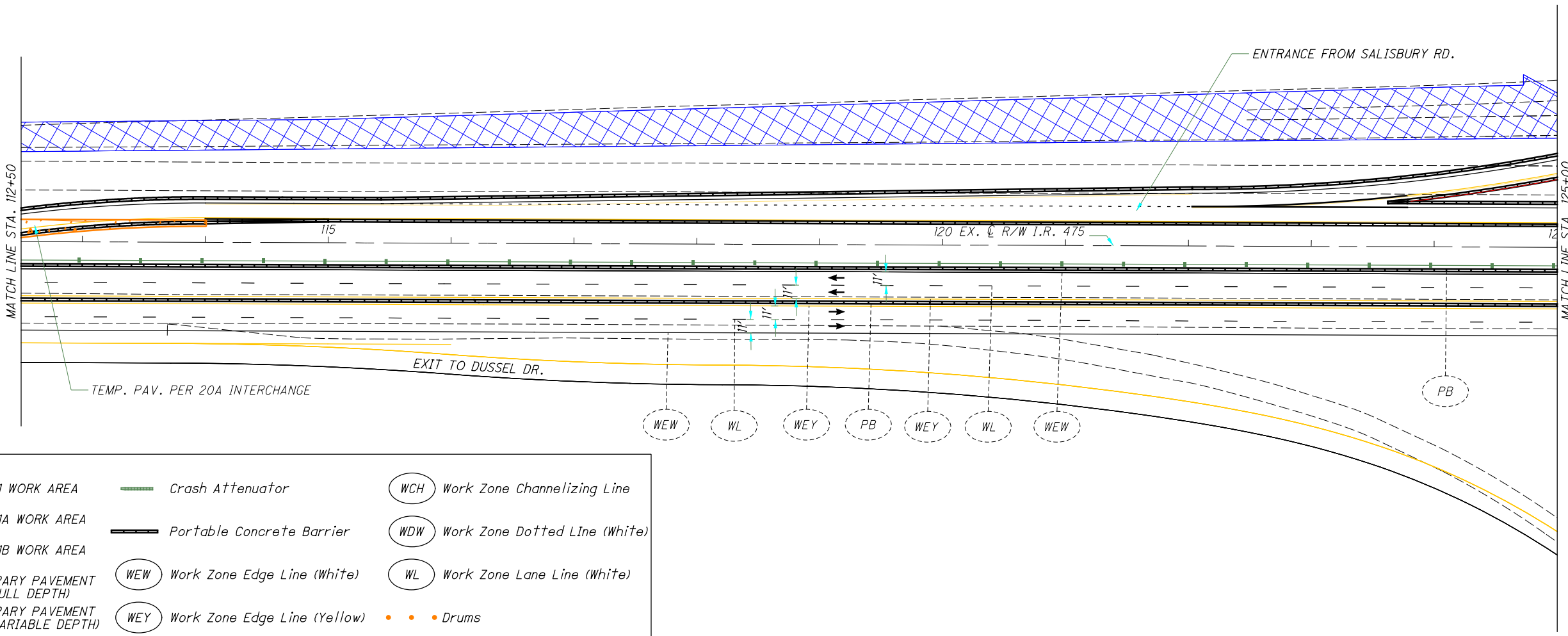
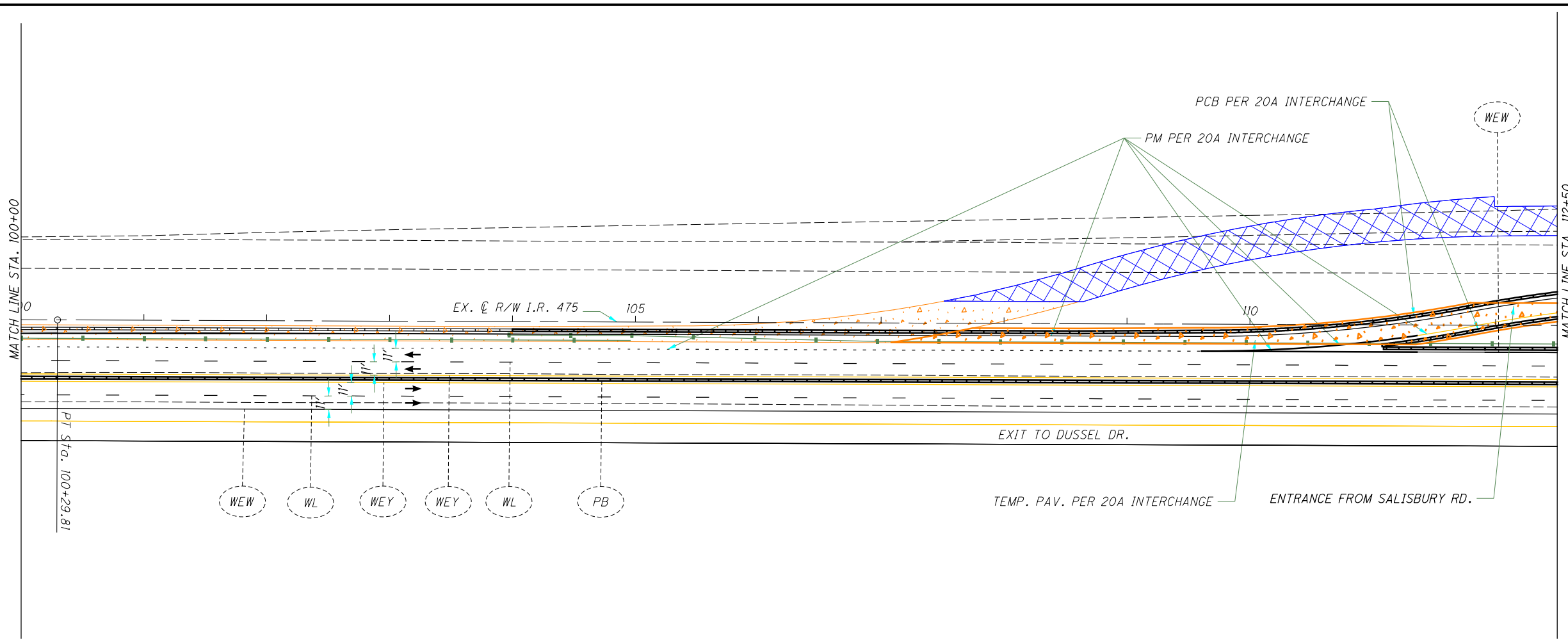
MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2A - STA. 41+50 TO STA. 49+00

LUC-475-0.09

121
855

CALCULATED
MK
CHECKED
DEK

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LEGEND:

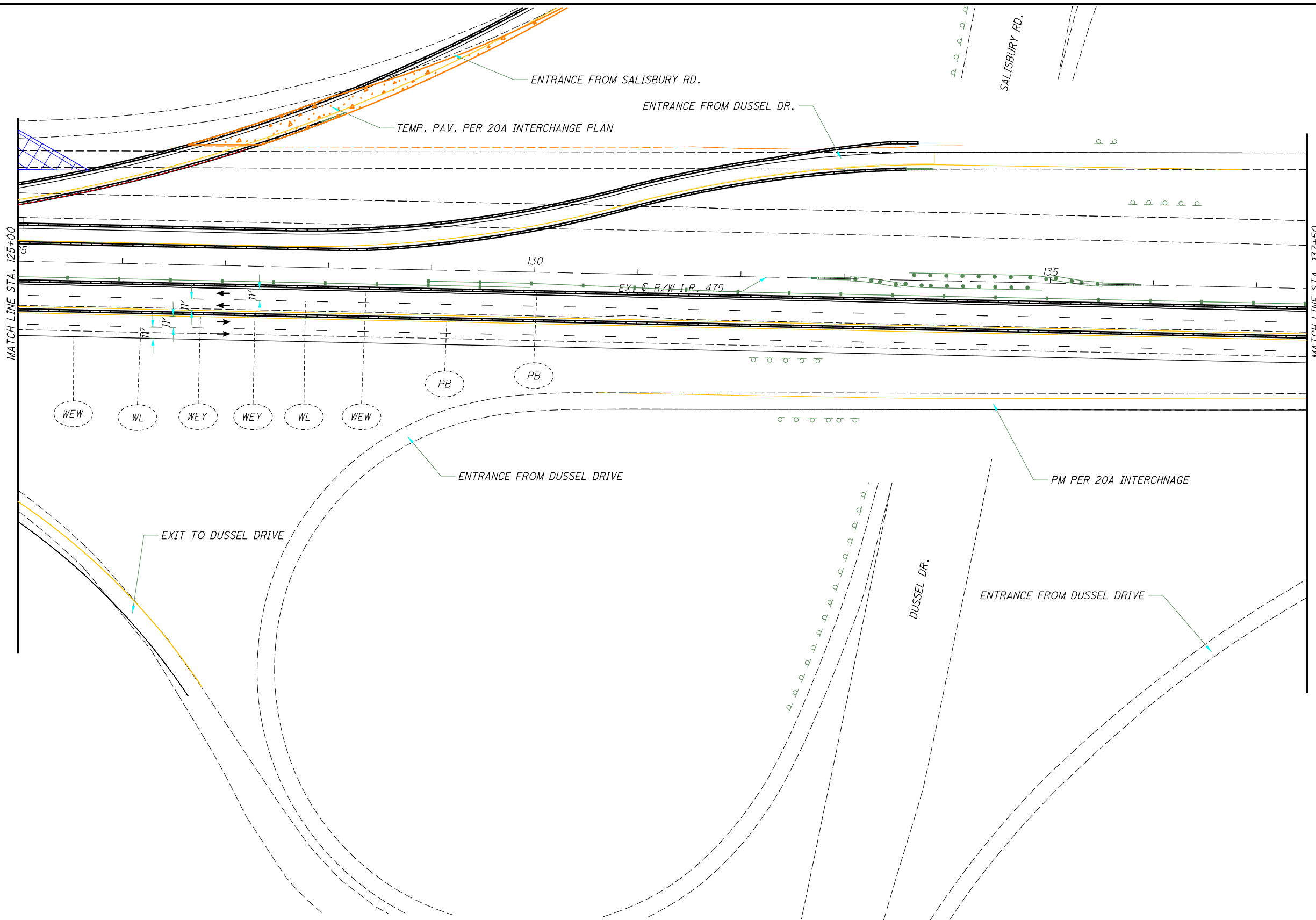
Phase 1 WORK AREA	Crash Attenuator	WCH Work Zone Channelizing Line
Phase 1A WORK AREA	Portable Concrete Barrier	WDW Work Zone Dotted Line (White)
Phase 1B WORK AREA	WEW Work Zone Edge Line (White)	WL Work Zone Lane Line (White)
TEMPORARY PAVEMENT (FULL DEPTH)	WEY Work Zone Edge Line (Yellow)	PB
TEMPORARY PAVEMENT (VARIABLE DEPTH)	Drums	



CALCULATED MK
 CHECKED DEK
MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2A - STA. 100+00 TO STA. 125+00

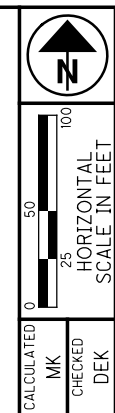
LUC-475-0.09
 122
 855

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LEGEND:

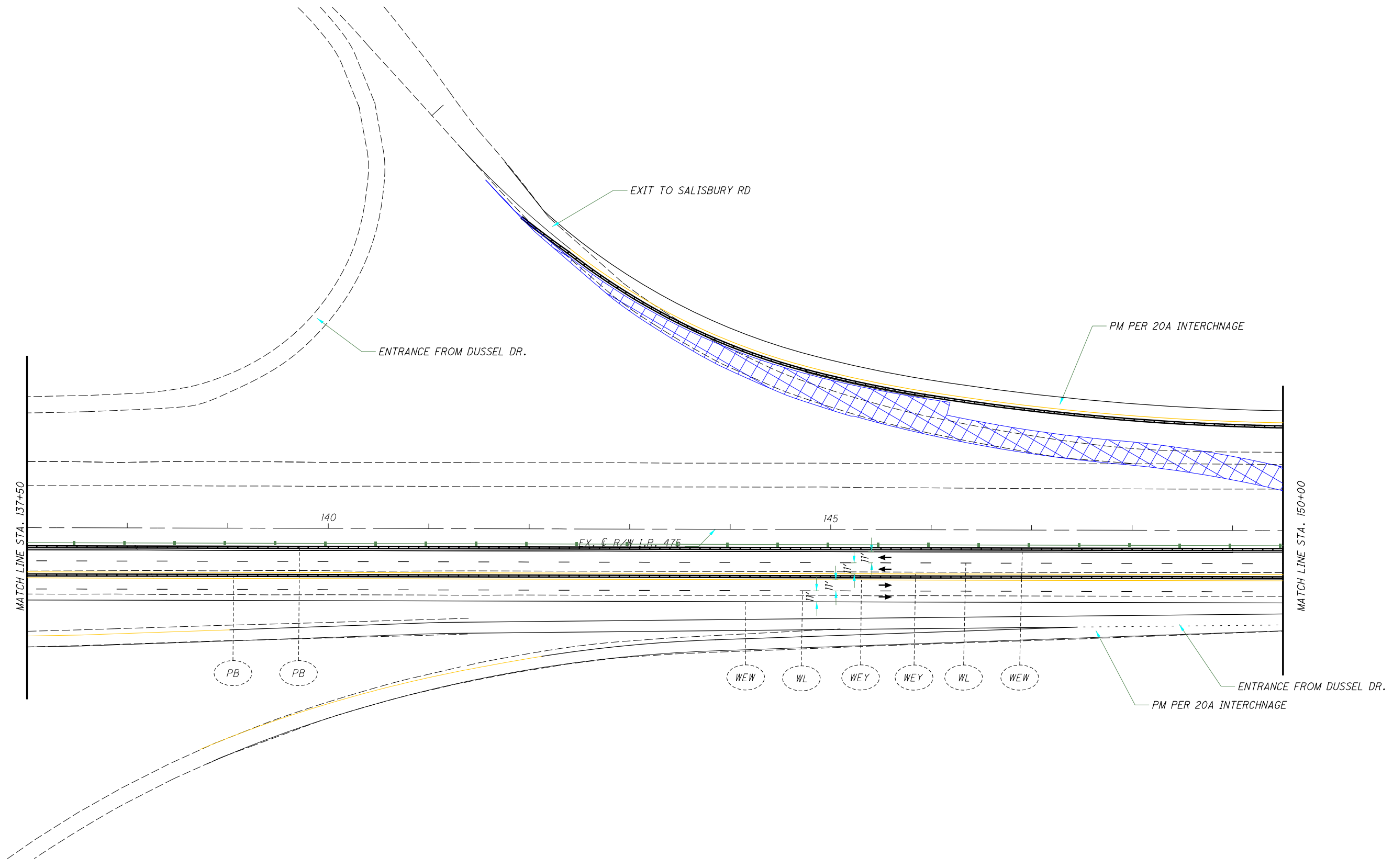
Phase 1 WORK AREA	Crash Attenuator	WCH Work Zone Channelizing Line
Phase 1A WORK AREA	Portable Concrete Barrier	WDW Work Zone Dotted Line (White)
Phase 1B WORK AREA	WEW Work Zone Edge Line (White)	WL Work Zone Lane Line (White)
TEMPORARY PAVEMENT (FULL DEPTH)	WEY Work Zone Edge Line (Yellow)	Drums
TEMPORARY PAVEMENT (VARIABLE DEPTH)		



CALCULATED MK CHECKED DEK
MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2A - STA. 125+00 TO STA. 137+50

LUC-475-0.09
 123
 855

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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

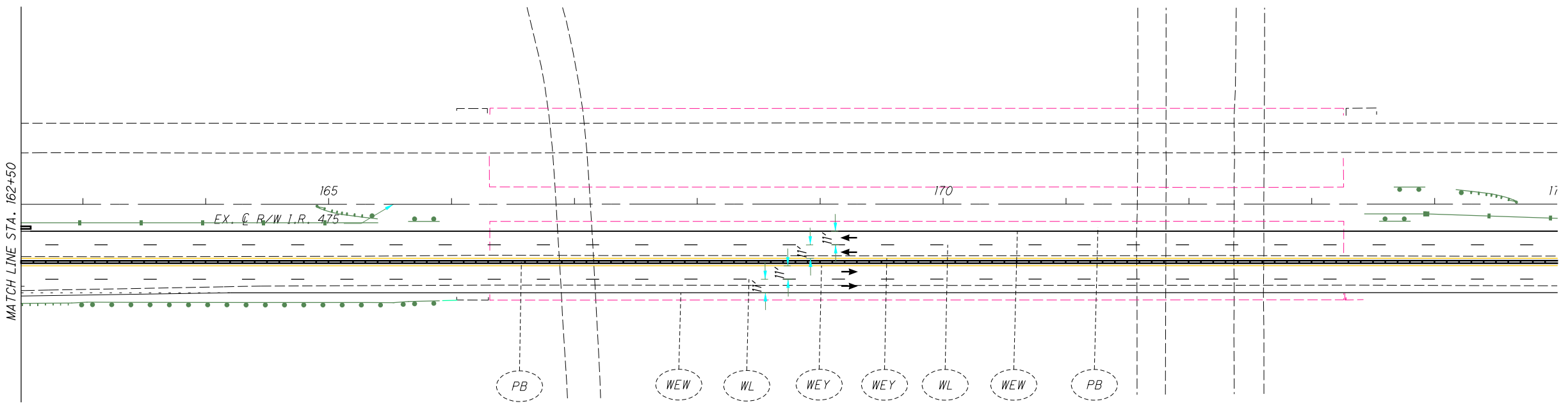
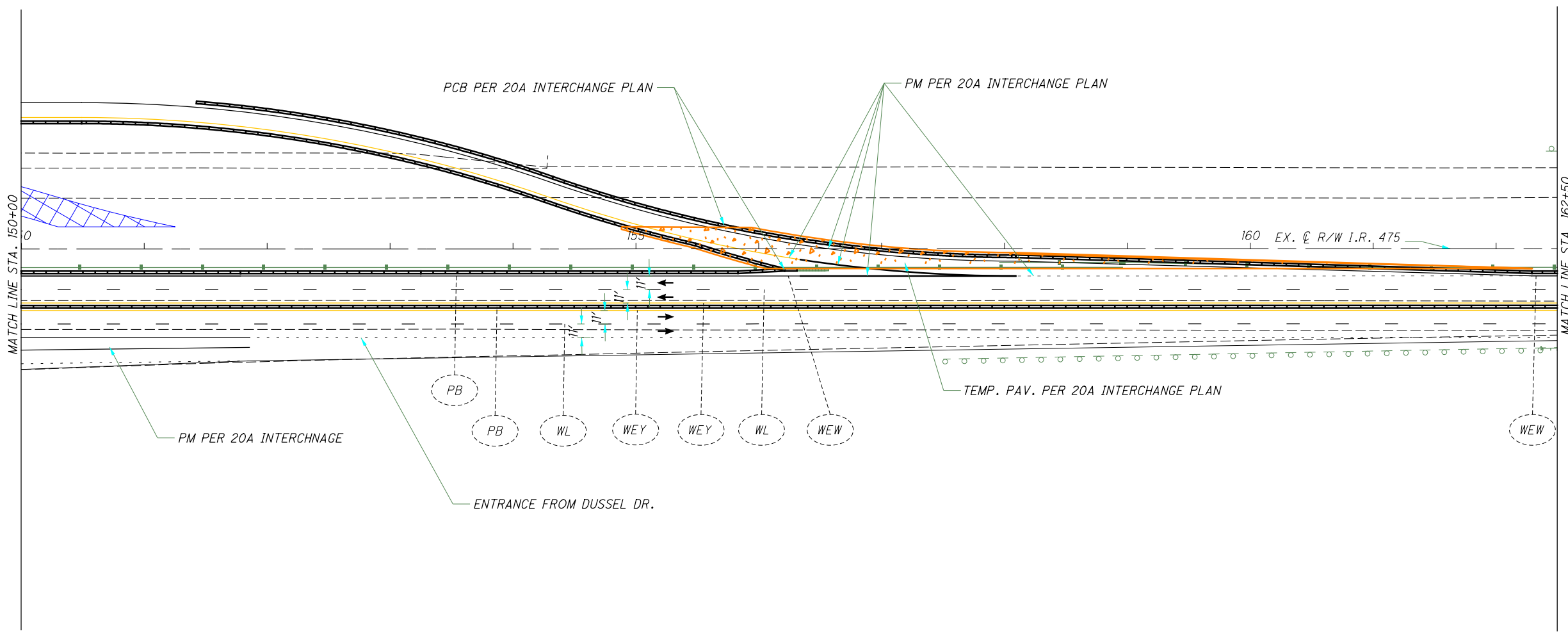
CALCULATED
MK
CHECKED
DEK

0 50 100
HORIZONTAL SCALE IN FEET

**MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2A - STA. 137+50 TO STA. 150+00**

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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

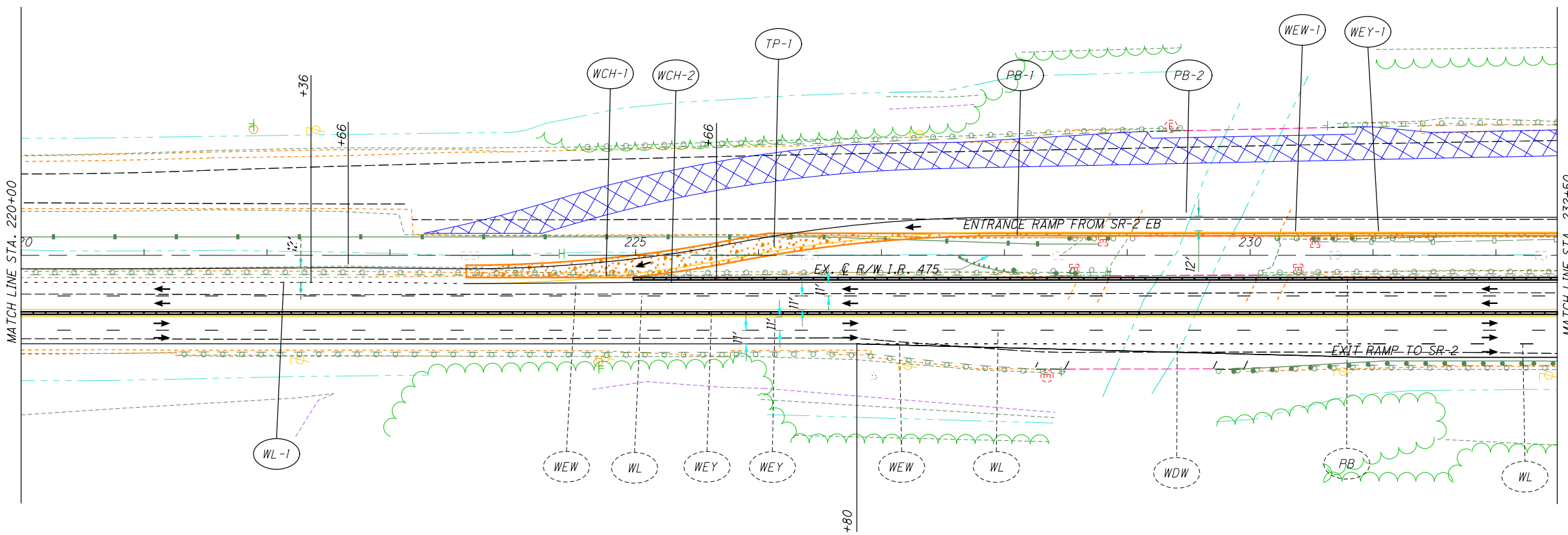


MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2A - STA. 150+00 TO STA. 175+00

LUC-475-0.09

125
855

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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				



MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2A - STA. 220+00 TO STA. 232+50


LUC-475-0.09

126
855

CALCULATED	IMK	CHECKED	DEK
------------	-----	---------	-----

I:\ProjectData\LUC-475-0.09\Design\M0T\Sheets\95875_MP058.dgn Sheet 4/21/2022 12:24:33 PM akaseml

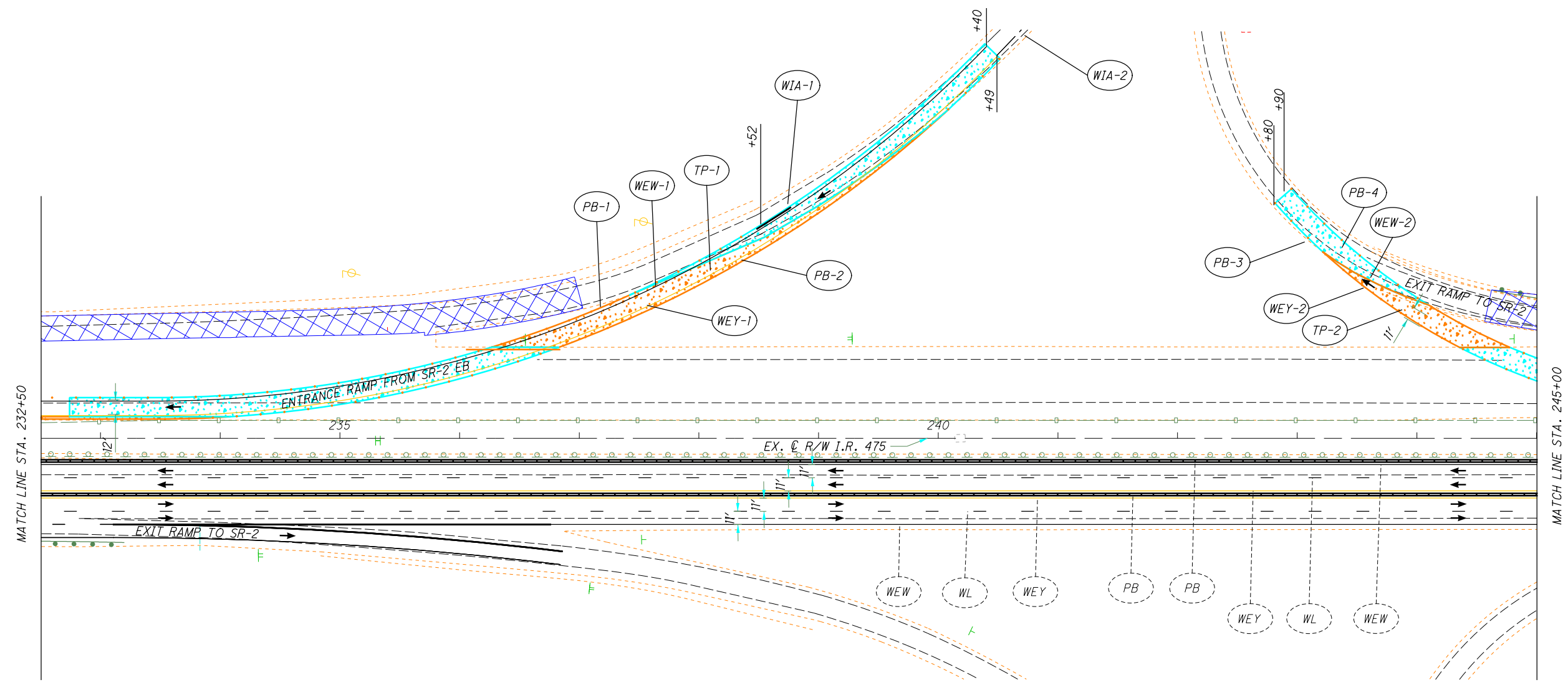









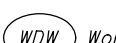




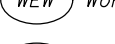


 CALCULATED: _____
 MK: _____
 CHECKED: _____
 DEK: _____

MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2A - STA. 232+50 TO STA. 245+00

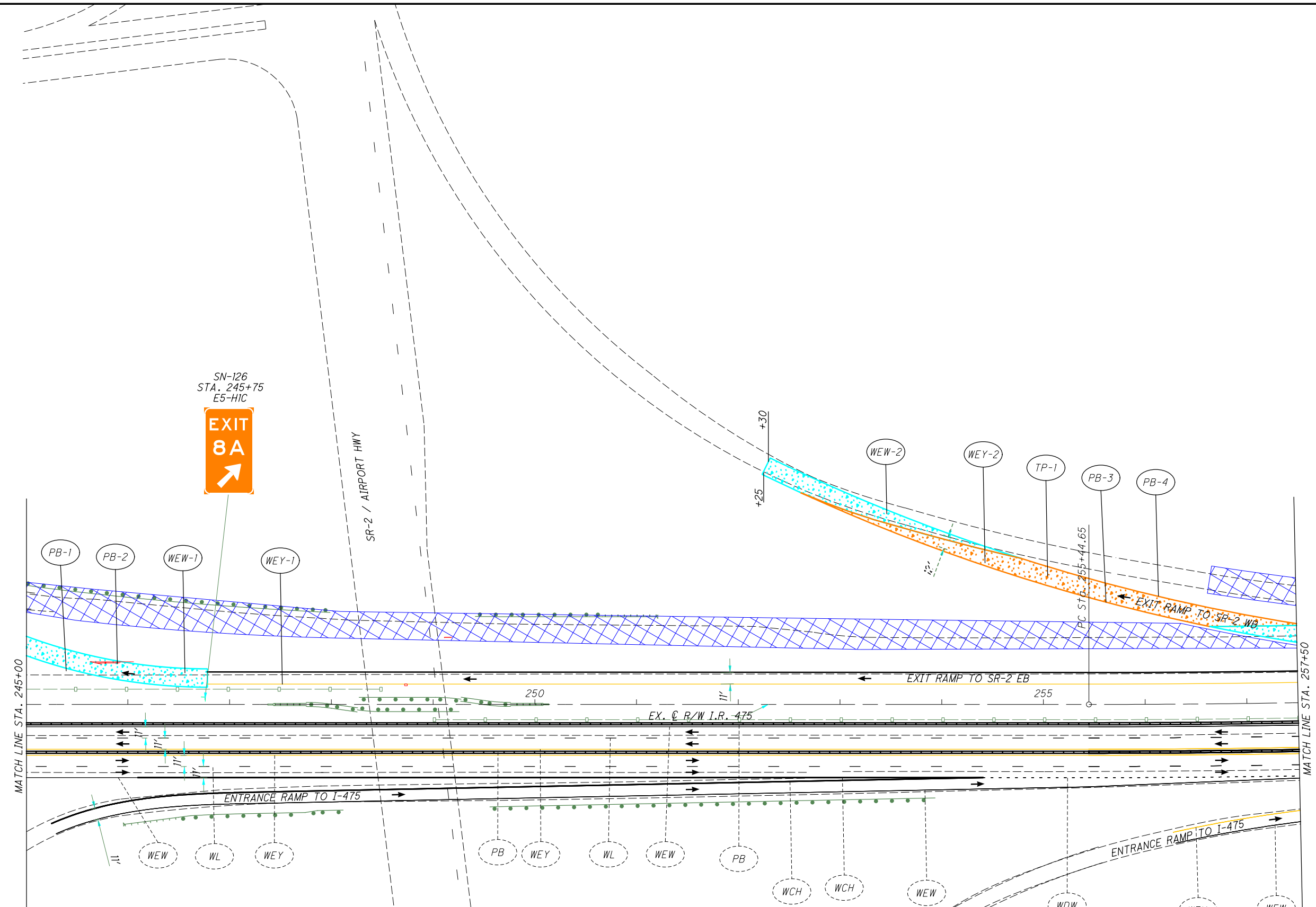
LUC-475-0.09
 127
 855







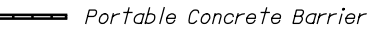








LEGEND:

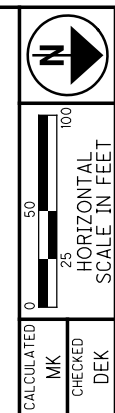
	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				

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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		WCH Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		WDW Work Zone Dotted Line (White)
	Phase 1B WORK AREA		WEW Work Zone Edge Line (White)		WL Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		WEY Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				



MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2A - STA. 245+00 TO STA. 257+50

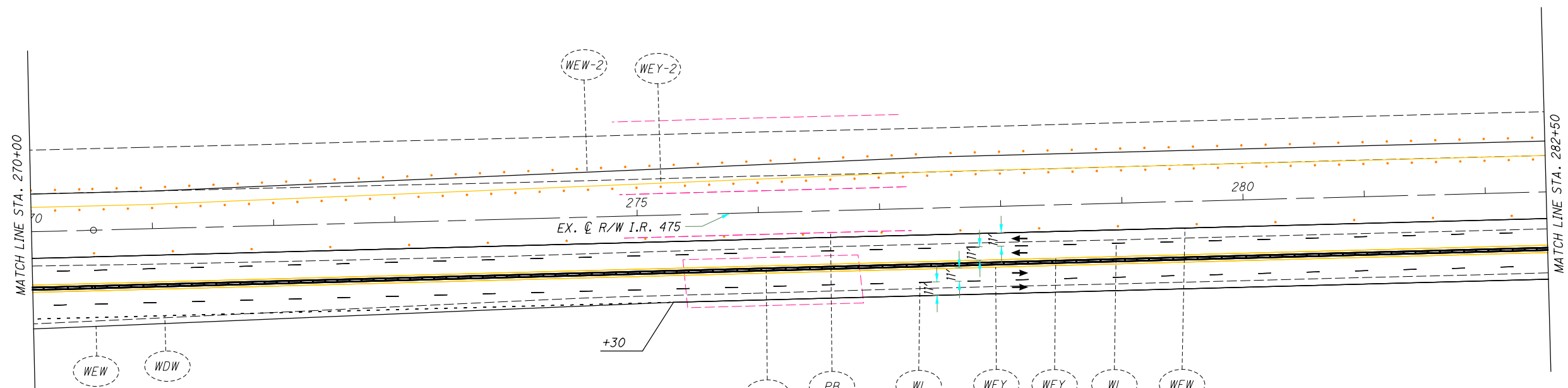
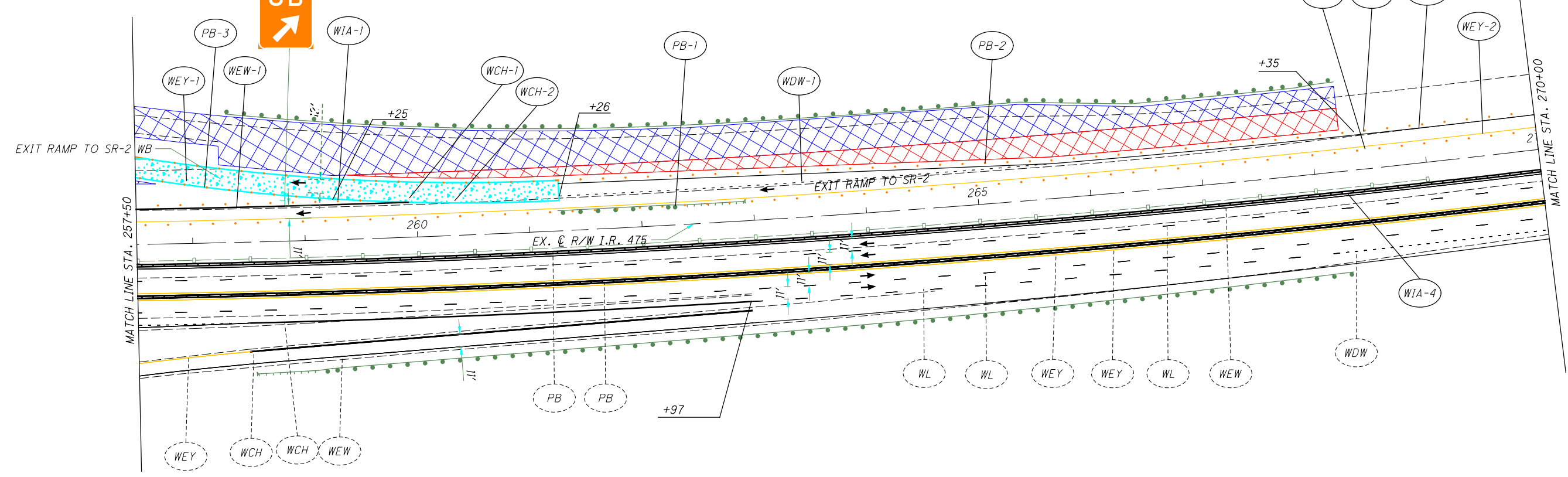
LUC-475-0.09

128
855

CALCULATED	
MK	
CHECKED	
DEK	

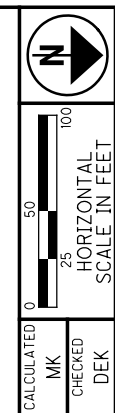
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SN-127
STA. 258+80
E1-H3



LEGEND:

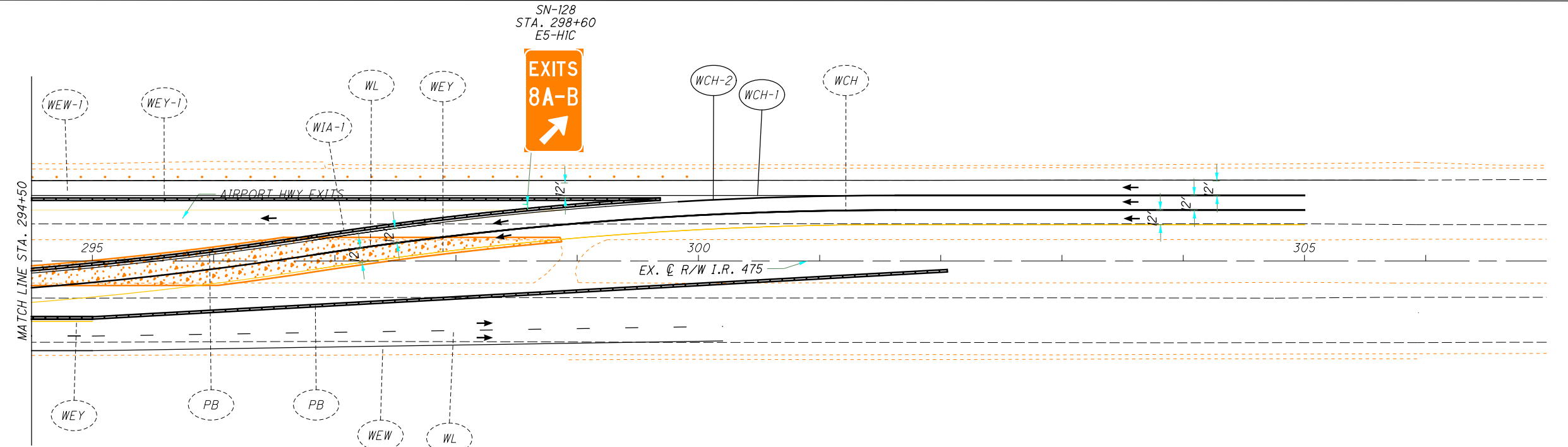
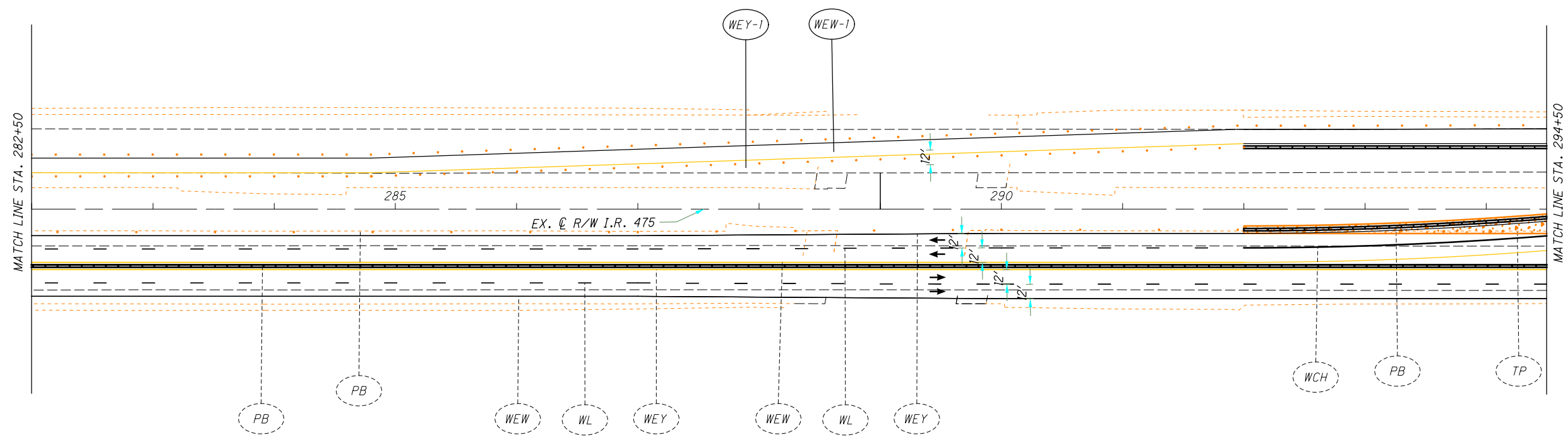
	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				



MAINTENANCE OF TRAFFIC - MAINLINE
PHASE 2A - STA. 257+50 TO STA. 282+50

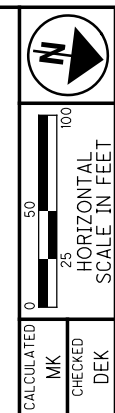
LUC-475-0.09
129
855

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LEGEND:

	Phase 1 WORK AREA		Crash Attenuator		Work Zone Channelizing Line
	Phase 1A WORK AREA		Portable Concrete Barrier		Work Zone Dotted Line (White)
	Phase 1B WORK AREA		Work Zone Edge Line (White)		Work Zone Lane Line (White)
	TEMPORARY PAVEMENT (FULL DEPTH)		Work Zone Edge Line (Yellow)		Drums
	TEMPORARY PAVEMENT (VARIABLE DEPTH)				



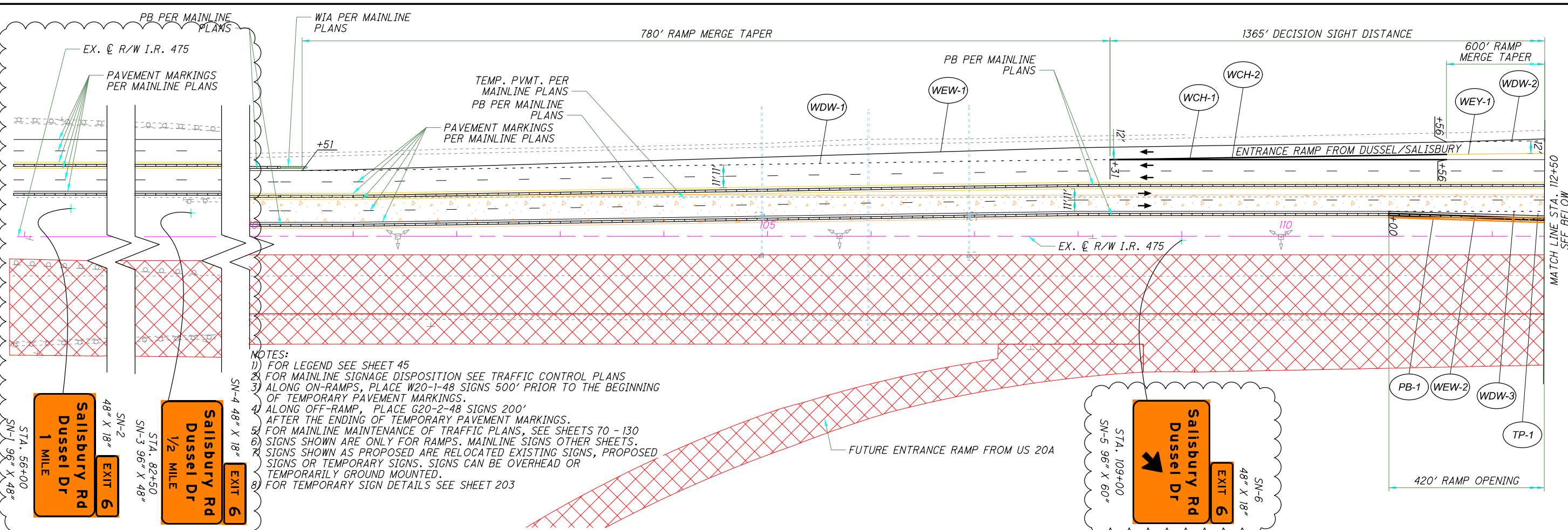
MAINTENANCE OF TRAFFIC - MAINLINE
 PHASE 2A - STA. 282+00 TO STA. 305+00

LUC-475-0.09

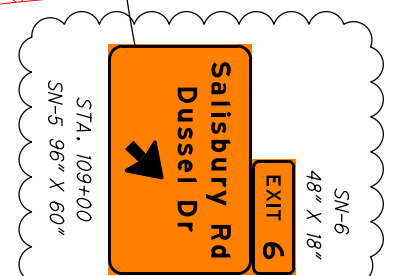
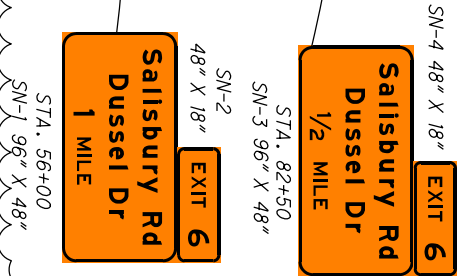
130
855

CALCULATED
MK
CHECKED
DEK

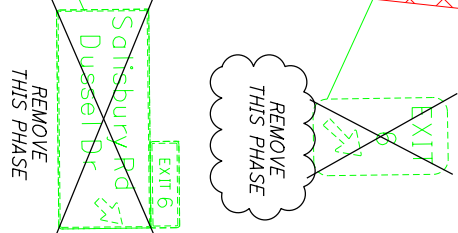
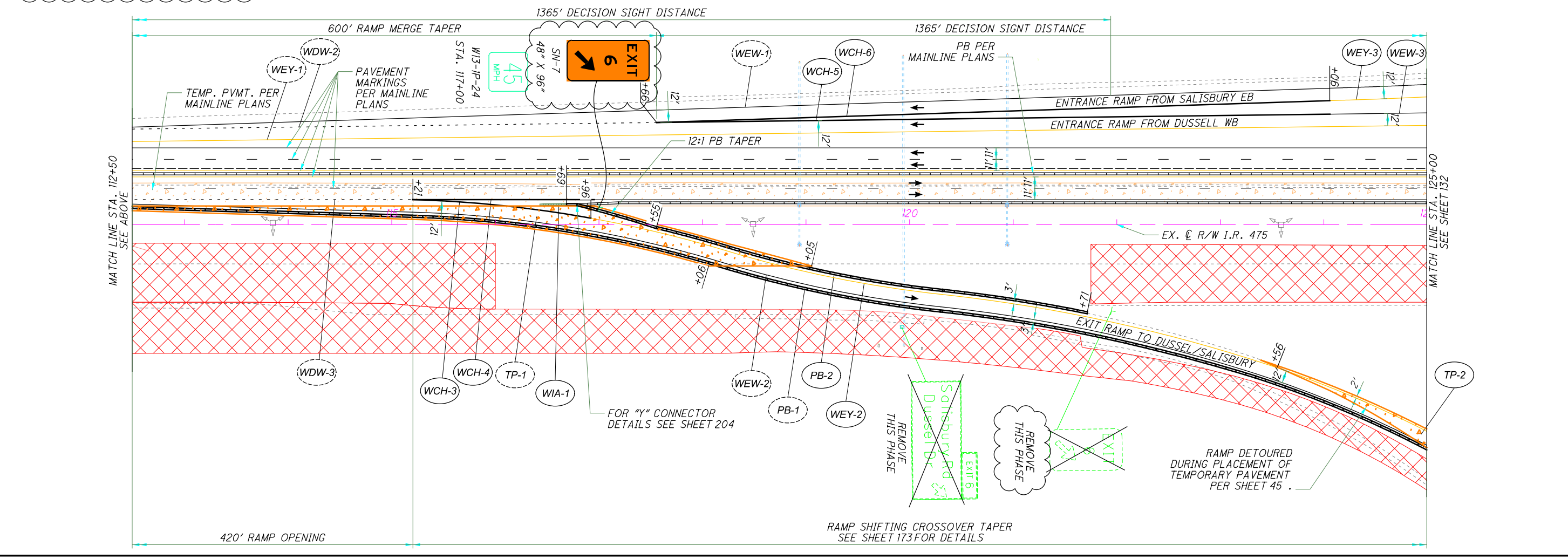
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- NOTES:
- 1) FOR LEGEND SEE SHEET 45
 - 2) FOR MAINLINE SIGNAGE DISPOSITION SEE TRAFFIC CONTROL PLANS
 - 3) ALONG ON-RAMPS, PLACE W20-1-48 SIGNS 500' PRIOR TO THE BEGINNING OF TEMPORARY PAVEMENT MARKINGS.
 - 4) ALONG OFF-RAMP, PLACE G20-2-48 SIGNS 200' AFTER THE ENDING OF TEMPORARY PAVEMENT MARKINGS.
 - 5) FOR MAINLINE MAINTENANCE OF TRAFFIC PLANS, SEE SHEETS 70 - 130
 - 6) SIGNS SHOWN ARE ONLY FOR RAMPS. MAINLINE SIGNS OTHER SHEETS.
 - 7) SIGNS SHOWN AS PROPOSED ARE RELOCATED EXISTING SIGNS, PROPOSED SIGNS OR TEMPORARY SIGNS. SIGNS CAN BE OVERHEAD OR TEMPORARILY GROUND MOUNTED.
 - 8) FOR TEMPORARY SIGN DETAILS SEE SHEET 203



CALCULATED TB CHECKED BRO
MAINTENANCE OF TRAFFIC - RAMP DETAILS
I-475 - PHASE 1 - STA. 100+00 TO STA. 125+00



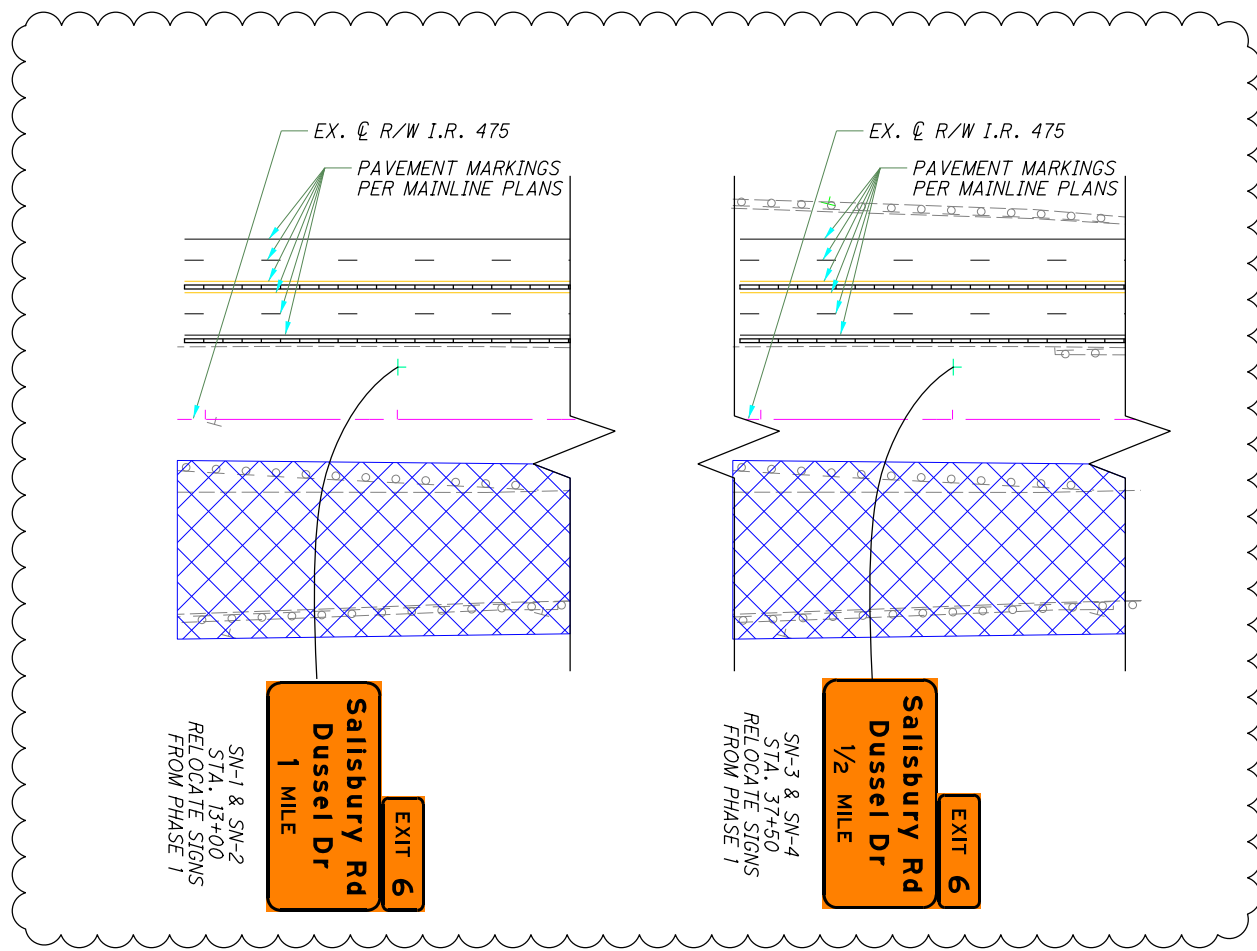
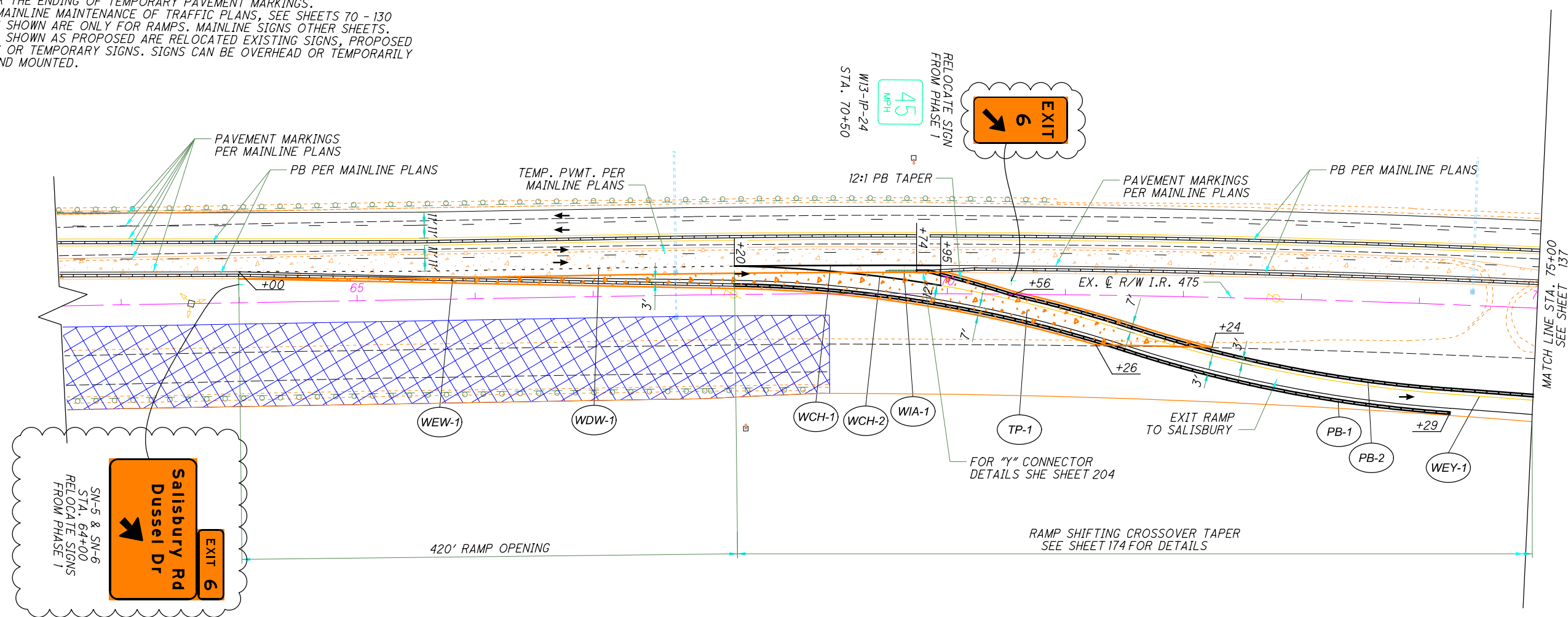
RAMP DETOURED DURING PLACEMENT OF TEMPORARY PAVEMENT PER SHEET 45.

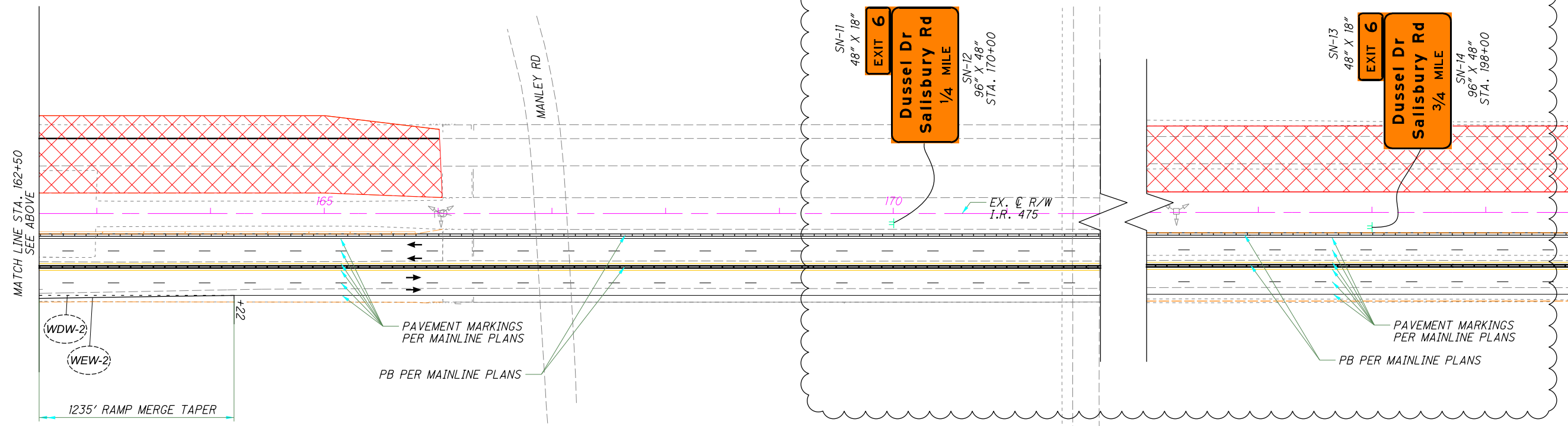
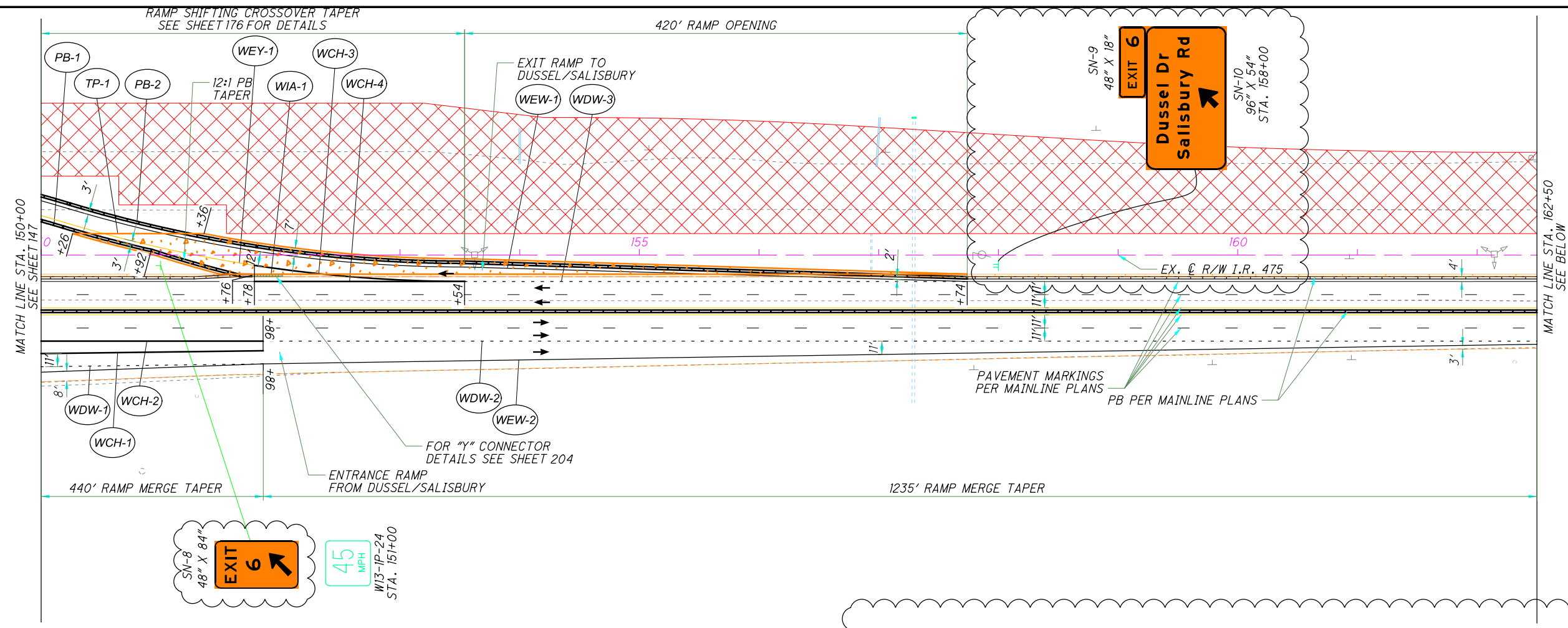
RAMP SHIFTING CROSSOVER TAPER SEE SHEET 173 FOR DETAILS

LUC-475-01.85
 131
 855

NOTES:

- 1) FOR LEGEND SEE SHEET 45
- 2) FOR MAINLINE SIGNAGE DISPOSITION SEE TRAFFIC CONTROL PLANS
- 3) ALONG ON-RAMPS, PLACE W20-1-48 SIGNS 500' PRIOR TO THE BEGINNING OF TEMPORARY PAVEMENT MARKINGS.
- 4) ALONG OFF-RAMP, PLACE G20-2-48 SIGNS 200' AFTER THE ENDING OF TEMPORARY PAVEMENT MARKINGS.
- 5) FOR MAINLINE MAINTENANCE OF TRAFFIC PLANS, SEE SHEETS 70 - 130
- 6) SIGNS SHOWN ARE ONLY FOR RAMPS. MAINLINE SIGNS OTHER SHEETS.
- 7) SIGNS SHOWN AS PROPOSED ARE RELOCATED EXISTING SIGNS, PROPOSED SIGNS OR TEMPORARY SIGNS. SIGNS CAN BE OVERHEAD OR TEMPORARILY GROUND MOUNTED.





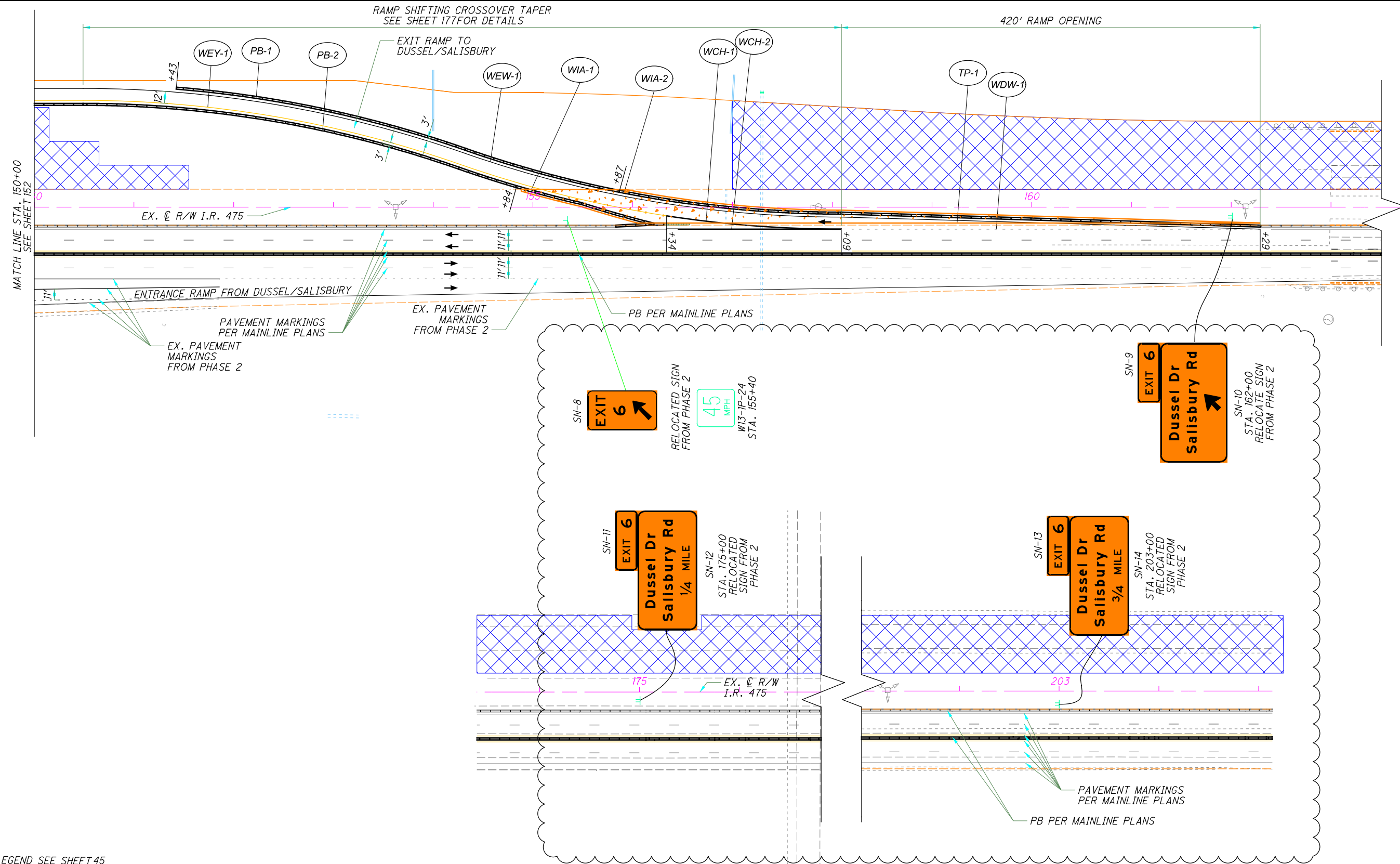
- NOTES:
- 1) FOR LEGEND SEE SHEET 45
 - 2) FOR MAINLINE SIGNAGE DISPOSITION SEE TRAFFIC CONTROL PLANS
 - 3) ALONG ON-RAMPS, PLACE W20-1-48 SIGNS 500' PRIOR TO THE BEGINNING OF TEMPORARY PAVEMENT MARKINGS.
 - 4) ALONG OFF-RAMP, PLACE G20-2-48 SIGNS 200' AFTER THE ENDING OF TEMPORARY PAVEMENT MARKINGS.
 - 5) FOR MAINLINE MAINTENANCE OF TRAFFIC PLANS, SEE SHEETS 70 - 130
 - 6) SIGNS SHOWN ARE ONLY FOR RAMPS. MAINLINE SIGNS OTHER SHEETS.
 - 7) SIGNS SHOWN AS PROPOSED ARE RELOCATED EXISTING SIGNS, PROPOSED SIGNS OR TEMPORARY SIGNS. SIGNS CAN BE OVERHEAD OR TEMPORARILY GROUND MOUNTED.
 - 8) FOR TEMPORARY SIGN DETAIL SEE SHEET 203

CALCULATED TB
CHECKED BRO

0 50 100
HORIZONTAL SCALE IN FEET

N

MAINTENANCE OF TRAFFIC - RAMP DETAILS
I-475 - PHASE 2 - STA. 150+00 TO STA. 175+00



- NOTES:
- 1) FOR LEGEND SEE SHEET 45
 - 2) FOR MAINLINE SIGNAGE DISPOSITION SEE TRAFFIC CONTROL PLANS
 - 3) ALONG ON-RAMPS, PLACE W20-1-48 SIGNS 500' PRIOR TO THE BEGINNING OF TEMPORARY PAVEMENT MARKINGS.
 - 4) ALONG OFF-RAMP, PLACE G20-2-48 SIGNS 200' AFTER THE ENDING OF TEMPORARY PAVEMENT MARKINGS.
 - 5) FOR MAINLINE MAINTENANCE OF TRAFFIC PLANS, SEE SHEETS 70 - 130
 - 6) SIGNS SHOWN ARE ONLY FOR RAMPS. MAINLINE SIGNS OTHER SHEETS.
 - 7) SIGNS SHOWN AS PROPOSED ARE RELOCATED EXISTING SIGNS, PROPOSED SIGNS OR TEMPORARY SIGNS. SIGNS CAN BE OVERHEAD OR TEMPORARILY GROUND MOUNTED.
 - 8) FOR TEMPORARY SIGN DETAILS SEE SHEET 203

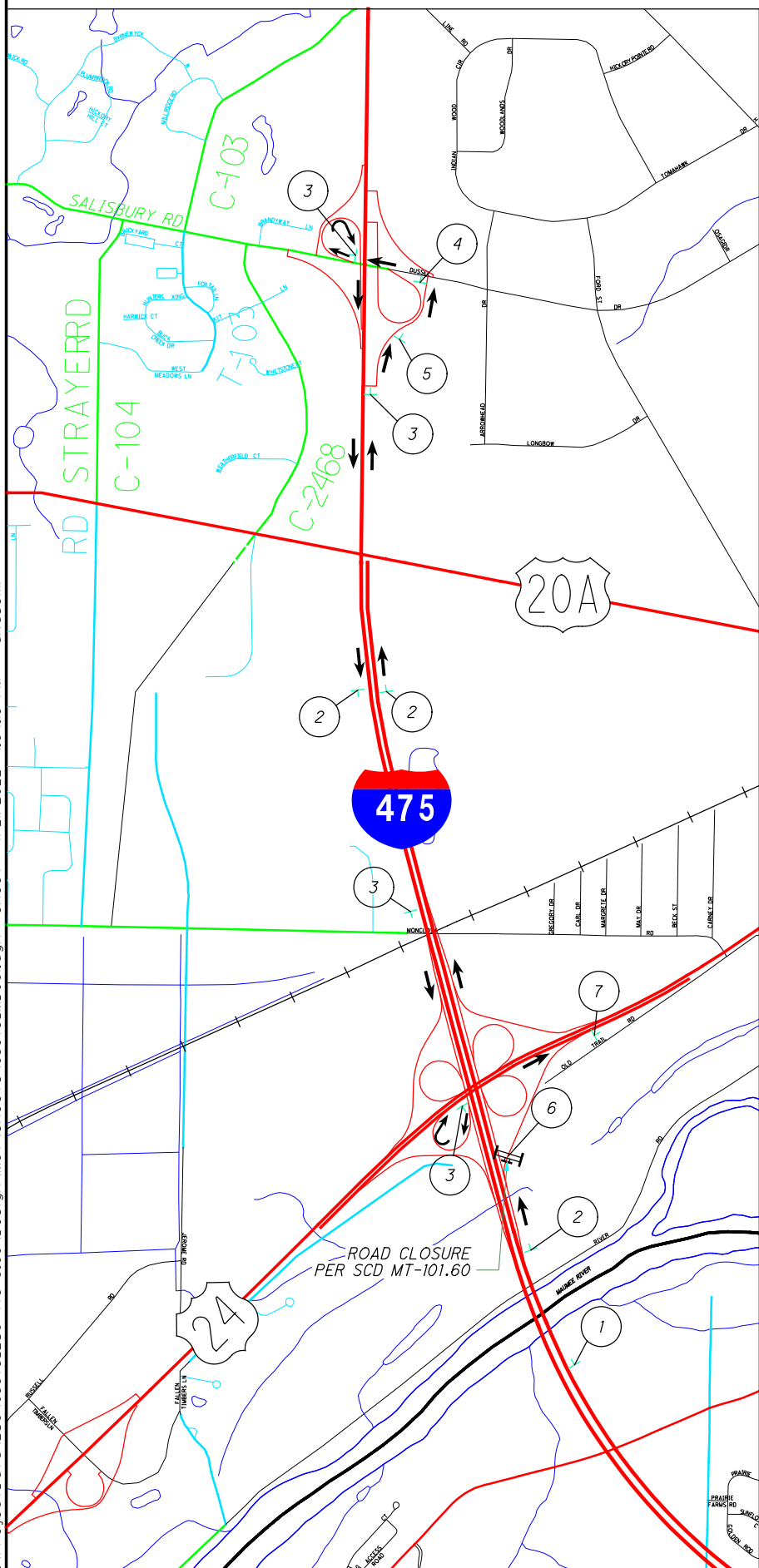
CALCULATED TB
 CHECKED BRO

0 50 100
 HORIZONTAL SCALE IN FEET

MAINTENANCE OF TRAFFIC - RAMP DETAILS
I-475 - PHASE 2A - STA. 150+00 TO STA. 175+00

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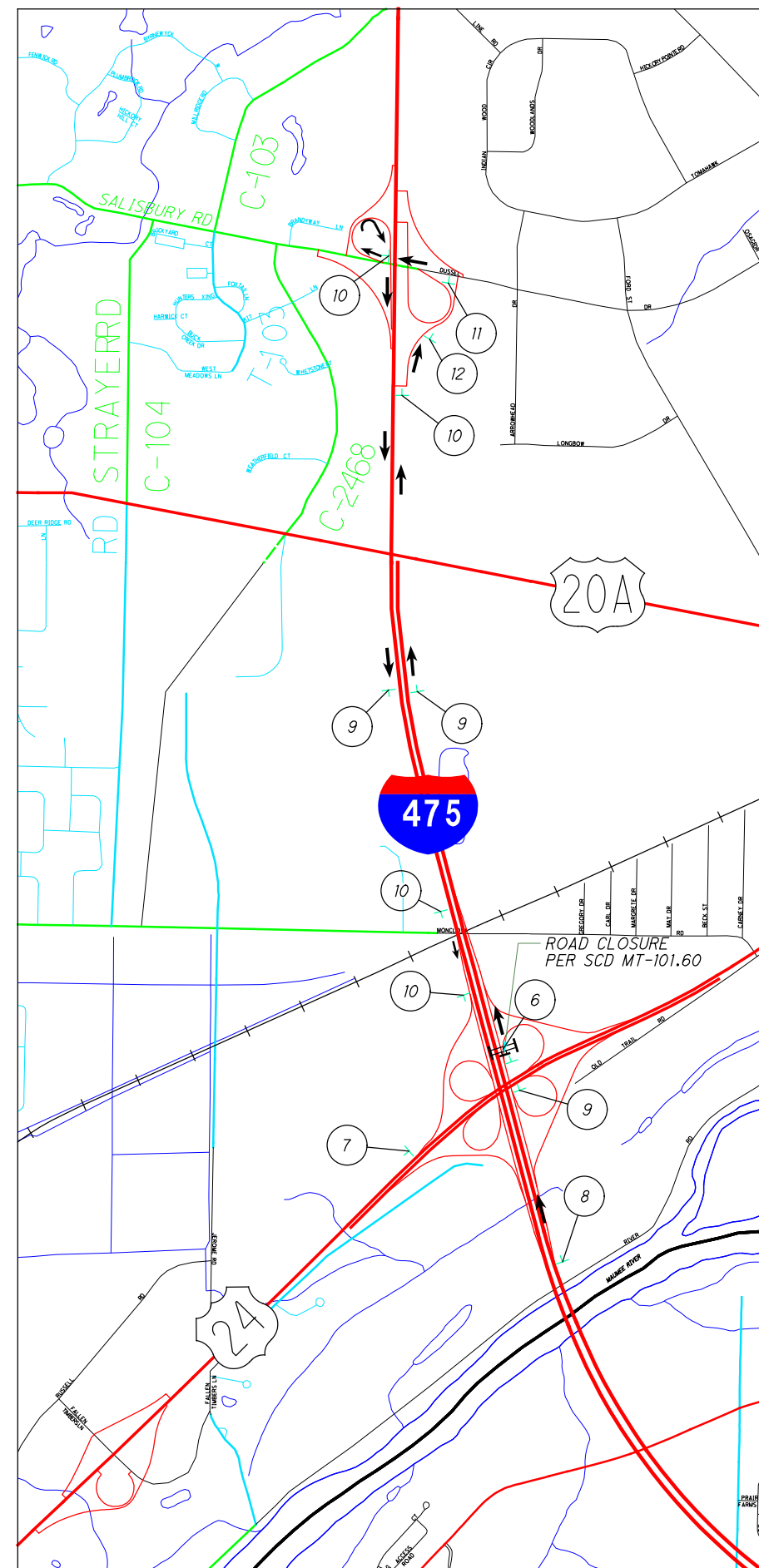
I-475 NORTHBOUND EXIT RAMP TO EASTBOUND US-24 CLOSURE



1	2	3	4	5	6	7
EAST M3-2-30	DETOUR M4-8-30	DETOUR M4-8-30	DETOUR M4-8-30	DETOUR M4-8-30	RAMP CLOSED R11-2-48	END DETOUR M4-8a-24
24 M1-4-30-2	24 M3-2-30	24 M3-2-30	24 M3-2-30	24 M3-2-30		
DETOUR AHEAD W20-2-48	24 M1-4-30-2	24 M1-4-30-2	24 M1-4-30-2	24 M1-4-30-2		
	↑ M6-3-30	↗ M6-2R-30	← M6-1L-30	↖ M5-1L-30		
			CENTER LANE M5-5-24	CENTER LANE M5-5-24		
8	9	10	11	12		
WEST M3-4-30	DETOUR M4-8-30	DETOUR M4-8-30	DETOUR M4-8-30	DETOUR M4-8-30		
24 M1-4-30-2	24 M3-4-30	24 M3-4-30	24 M3-4-30	24 M3-4-30		
DETOUR AHEAD W20-2-48	24 M1-4-30-2	24 M1-4-30-2	24 M1-4-30-2	24 M1-4-30-2		
	↑ M6-3-30	↗ M6-2R-30	← M6-1L-30	↖ M5-1L-30		
			CENTER LANE M5-5-24	CENTER LANE M5-5-24		

NOTE:
 1. OVERLAYS SHALL BE INSTALLED ON ALL EXIT GUIDE SIGNS WHEN THE EXIT RAMP WILL BE CLOSED FOR A MINIMUM OF 24 HOURS AS SPECIFIED IN MT-98.29. ALL COSTS FOR THE PRODUCTION, INSTALLATION, AND SUBSEQUENT REMOVAL OF THE OVERLAYS SHALL BE INCLUDED IN THE SUM BID FOR ITEM 614, MAINTAINING TRAFFIC.
 2. OVERHEAD AND GROUND MOUNTED LANE USE SIGNS SHALL BE COVERED WHEN THE ENTRANCE RAMP IS CLOSED FOR A MINIMUM OF 24 HOURS.

I-475 NORTHBOUND EXIT RAMP TO WESTBOUND US-24 CLOSURE

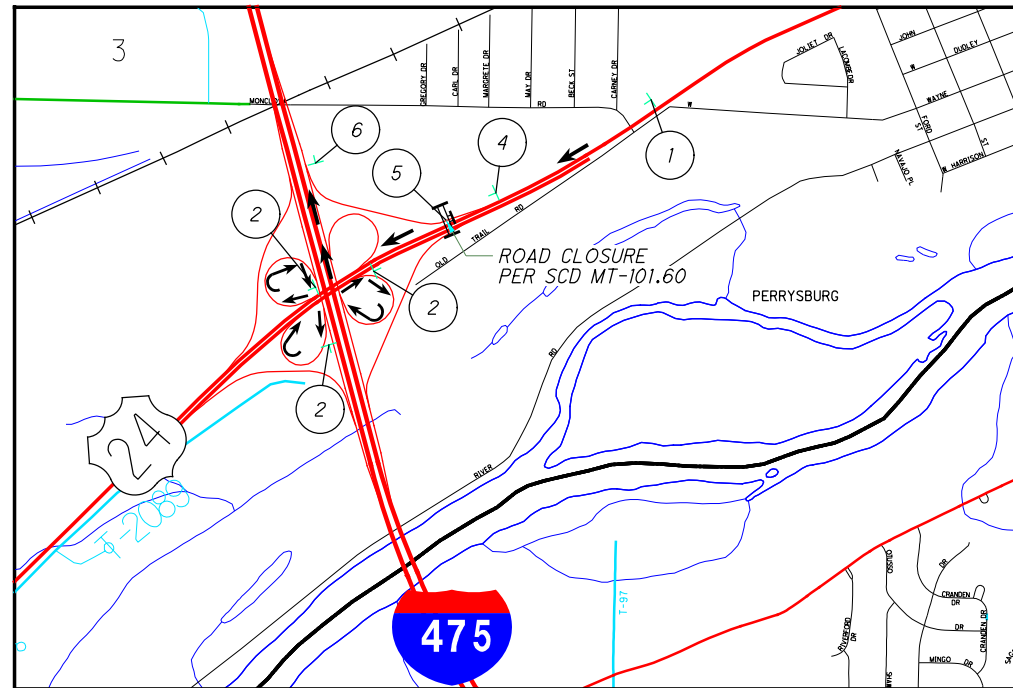


0 1000 2000
 500
 HORIZONTAL
 SCALE IN FEET
 CALCULATED
 NME
 CHECKED
 JMM

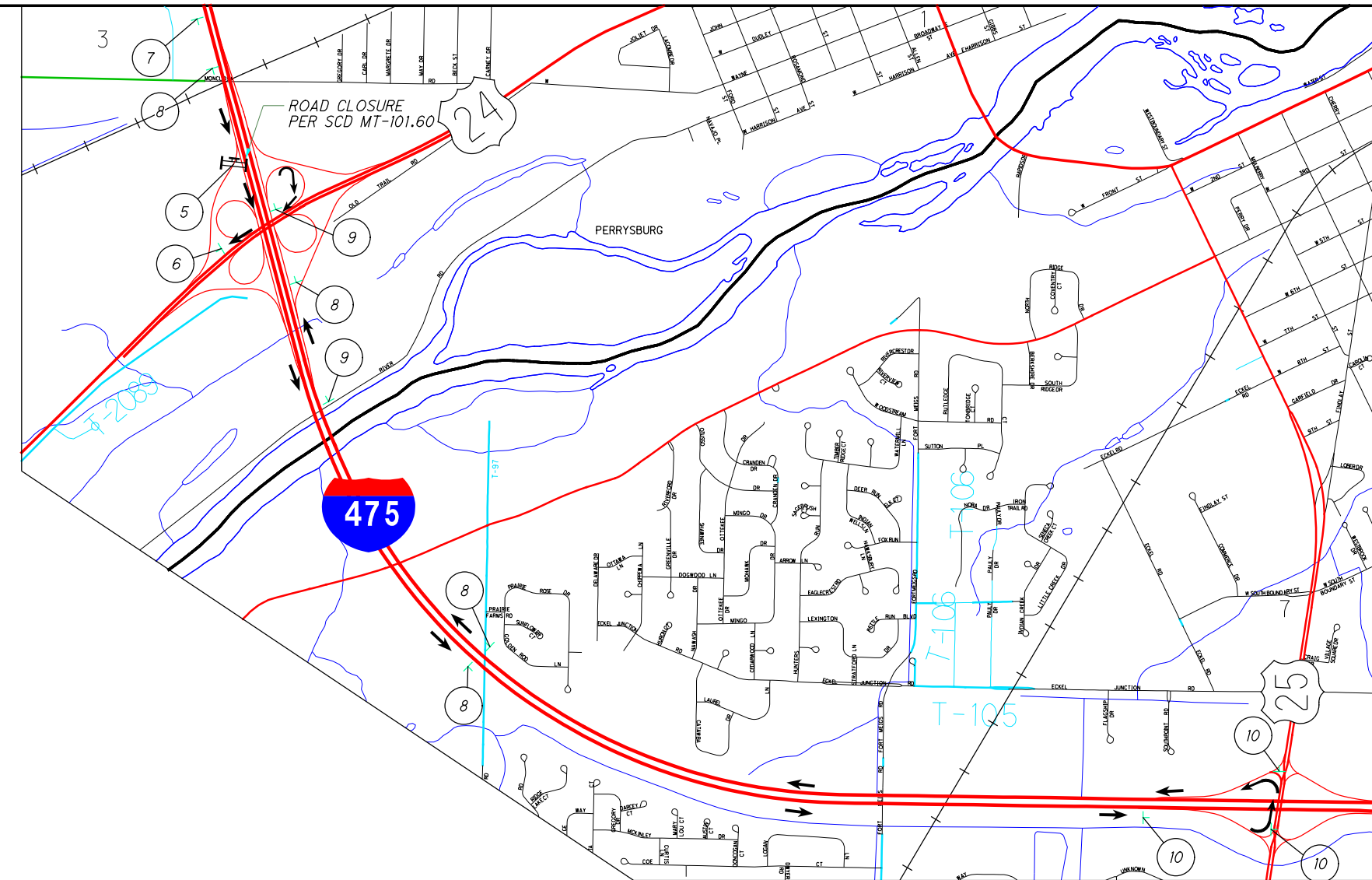
**DETOUR PLAN
 I-475 / US-24 INTERCHANGE**

LUC-475-0.09
 178
 855

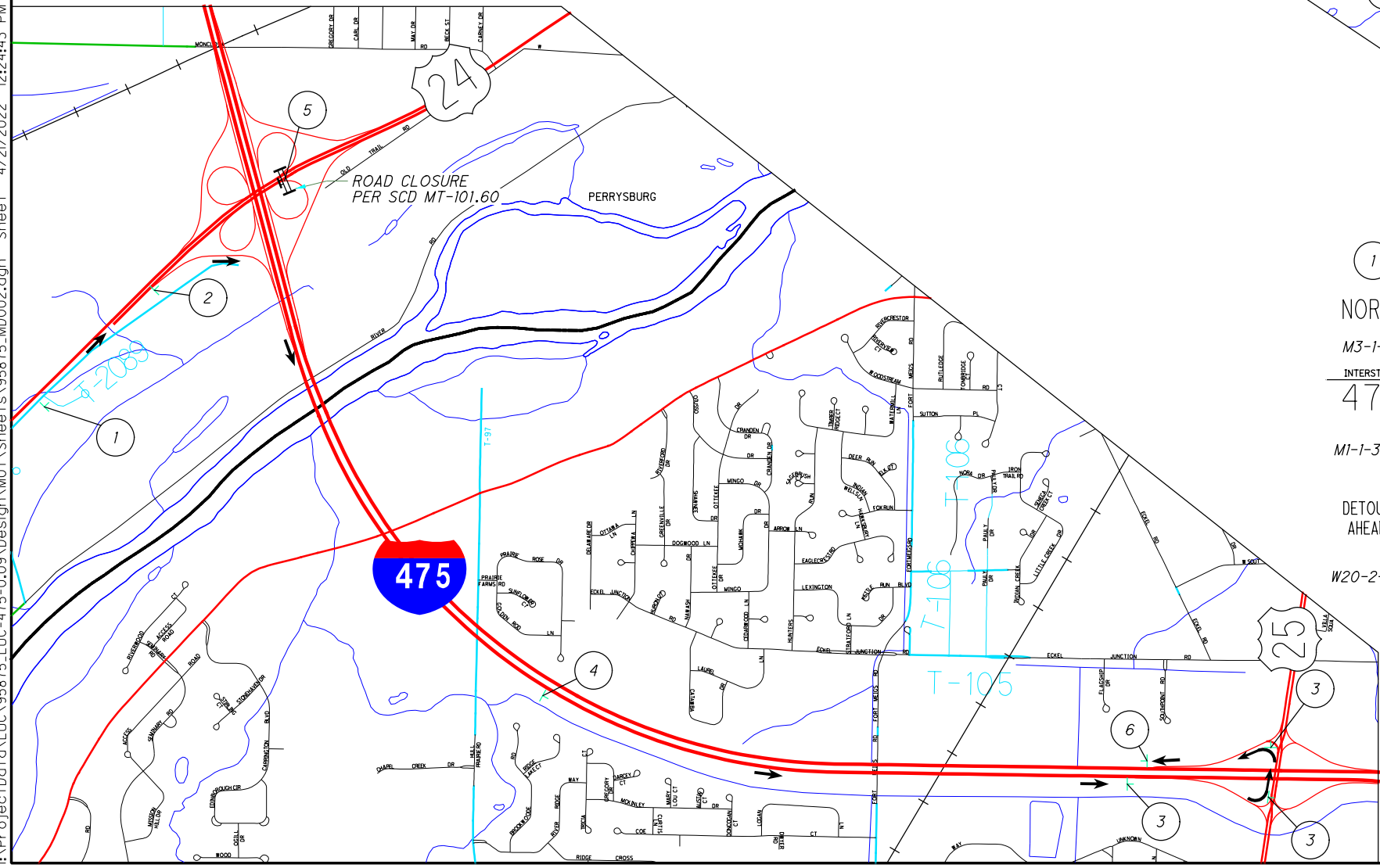
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I-475 NORTHBOUND ENTRANCE RAMP FROM WESTBOUND US-24 CLOSURE



I-475 SOUTHBOUND EXIT RAMP TO WESTBOUND US-24 CLOSURE



I-475 NORTHBOUND ENTRANCE RAMP FROM EASTBOUND US-24 CLOSURE

1	2	3	4	5	7	8	9	10
NORTH	DETOUR	DETOUR	DETOUR	RAMP CLOSED	WEST	DETOUR	DETOUR	DETOUR
M3-1-30	M4-8-30	M4-8-30	M4-8-30	R11-2-48	M3-4-30	M4-8-30	M4-8-30	M4-8-30
INTERSTATE	NORTH	NORTH	NORTH		24	WEST	WEST	WEST
475	M3-1-30	M3-1-30	M3-1-30		M1-4-30-2	M3-4-30	M3-4-30	M3-4-30
M1-1-30-3	INTERSTATE	INTERSTATE	INTERSTATE	6		24	24	24
DETOUR AHEAD	475	475	475	END	DETOUR AHEAD	M1-4-30-2	M1-4-30-2	M1-4-30-2
W20-2-48	M1-1-30-3	M1-1-30-3	M1-1-30-3	DETOUR AHEAD	W20-2-48	M6-3-30	M6-2R-30	M6-2L-30
	↑	↑	↑					
	M6-2R-30	M6-2L-30	M6-3-30					

NOTE:
 1. OVERLAYS SHALL BE INSTALLED ON ALL EXIT GUIDE SIGNS WHEN THE EXIT RAMP WILL BE CLOSED FOR A MINIMUM OF 24 HOURS AS SPECIFIED IN MT-98.29. ALL COSTS FOR THE PRODUCTION, INSTALLATION, AND SUBSEQUENT REMOVAL OF THE OVERLAYS SHALL BE INCLUDED IN THE SUM BID FOR ITEM 614, MAINTAINING TRAFFIC.
 2. OVERHEAD AND GROUND MOUNTED LANE USE SIGNS SHALL BE COVERED WHEN THE ENTRANCE RAMP IS CLOSED FOR A MINIMUM OF 24 HOURS.

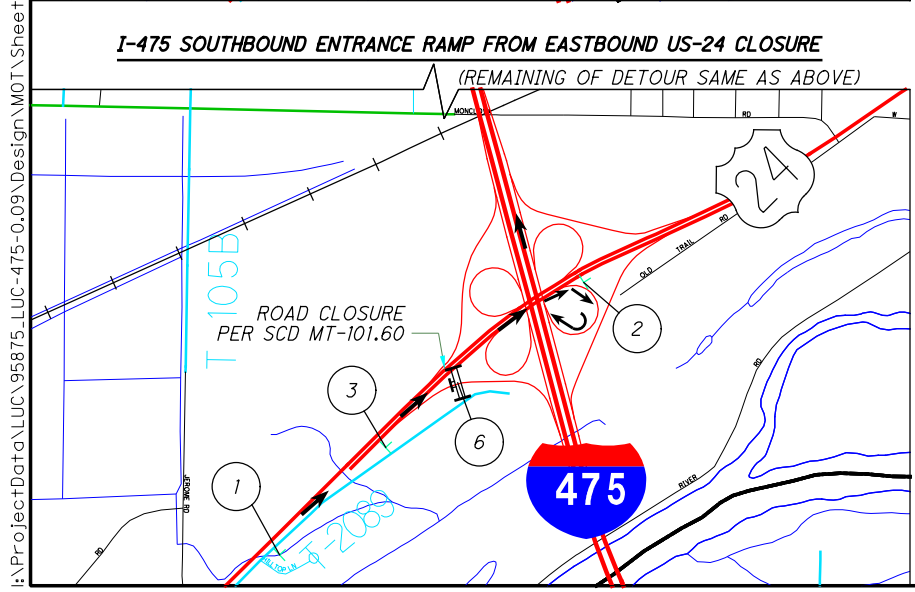
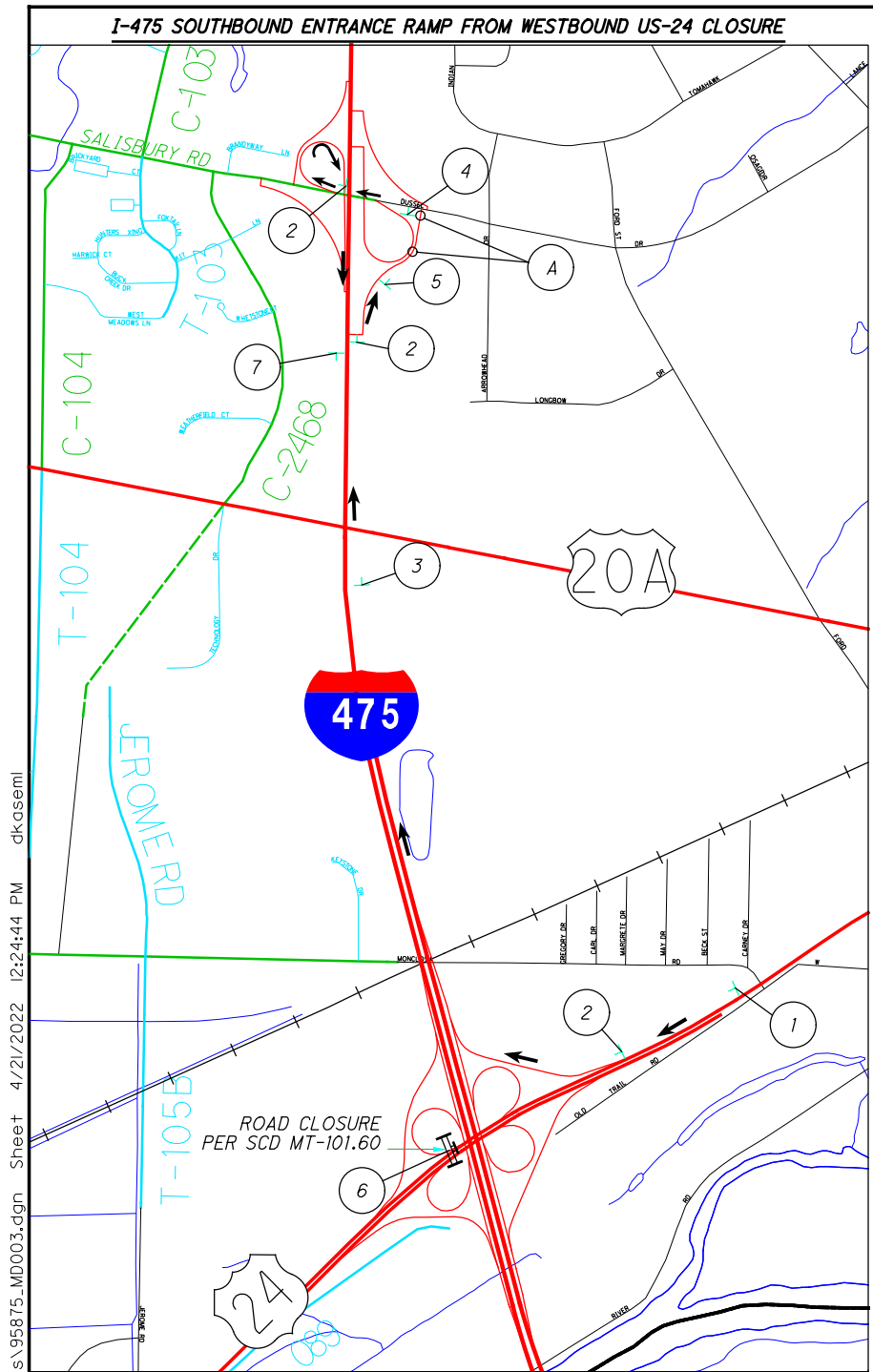
DETOUR PLAN
I-475 / US-24 INTERCHANGE

LUC-475-0.09

179
855

CALCULATED NME CHECKED JMM

0 1000 2000
 500
 HORIZONTAL SCALE IN FEET



1	2	3	4	5	6
SOUTH	DETOUR	DETOUR	DETOUR	DETOUR	RAMP CLOSED
M3-4-30	M4-8-30	M4-8-30	M4-8-30	M4-8-30	R11-2-48
INTERSTATE	SOUTH	SOUTH	SOUTH	SOUTH	
475	M3-4-30	M3-4-30	M3-4-30	M3-4-30	
MI-1-30-3	INTERSTATE	INTERSTATE	INTERSTATE	INTERSTATE	
	475	475	475	475	
DETOUR AHEAD	MI-1-30-3	MI-1-30-3	MI-1-30-3	MI-1-30-3	7
W20-2-48	↗	↑	←	↖	END
	M6-2R-30	M6-3-30	M6-1L-30	M5-1L-30	DETOUR
			CENTER LANE	CENTER LANE	M4-8a-24
			M5-5-24	M5-5-24	

8	9	10	11	12
EAST	DETOUR	DETOUR	DETOUR	DETOUR
M3-2-30	M4-8-30	M4-8-30	M4-8-30	M4-8-30
24	EAST	EAST	EAST	EAST
MI-4-30-2	M3-2-30	M3-2-30	M3-2-30	M3-2-30
	24	24	24	24
DETOUR AHEAD	MI-4-30-2	MI-4-30-2	MI-4-30-2	MI-4-30-2
W20-2-48	↑	↗	←	↖
	M6-3-30	M6-2R-30	M6-1L-30	M5-1L-30

DETOUR PAVEMENT MARKINGS:
 DURING THE CLOSURE OF THE I-475 SOUTHBOUND RAMP FROM US-24 WESTBOUND AND EASTBOUND, TWO SETS OF PAVEMENT MARKINGS SHOWN BELOW SHALL BE INSTALLED IN THE CENTER LANE OF I-475 NORTHBOUND TO DUSSEL DR EXIT RAMP. THE MARKINGS SHOULD BE INSTALLED APPROXIMATELY 75' AND 475' FROM THE STOP LINE AT DUSSEL DR.

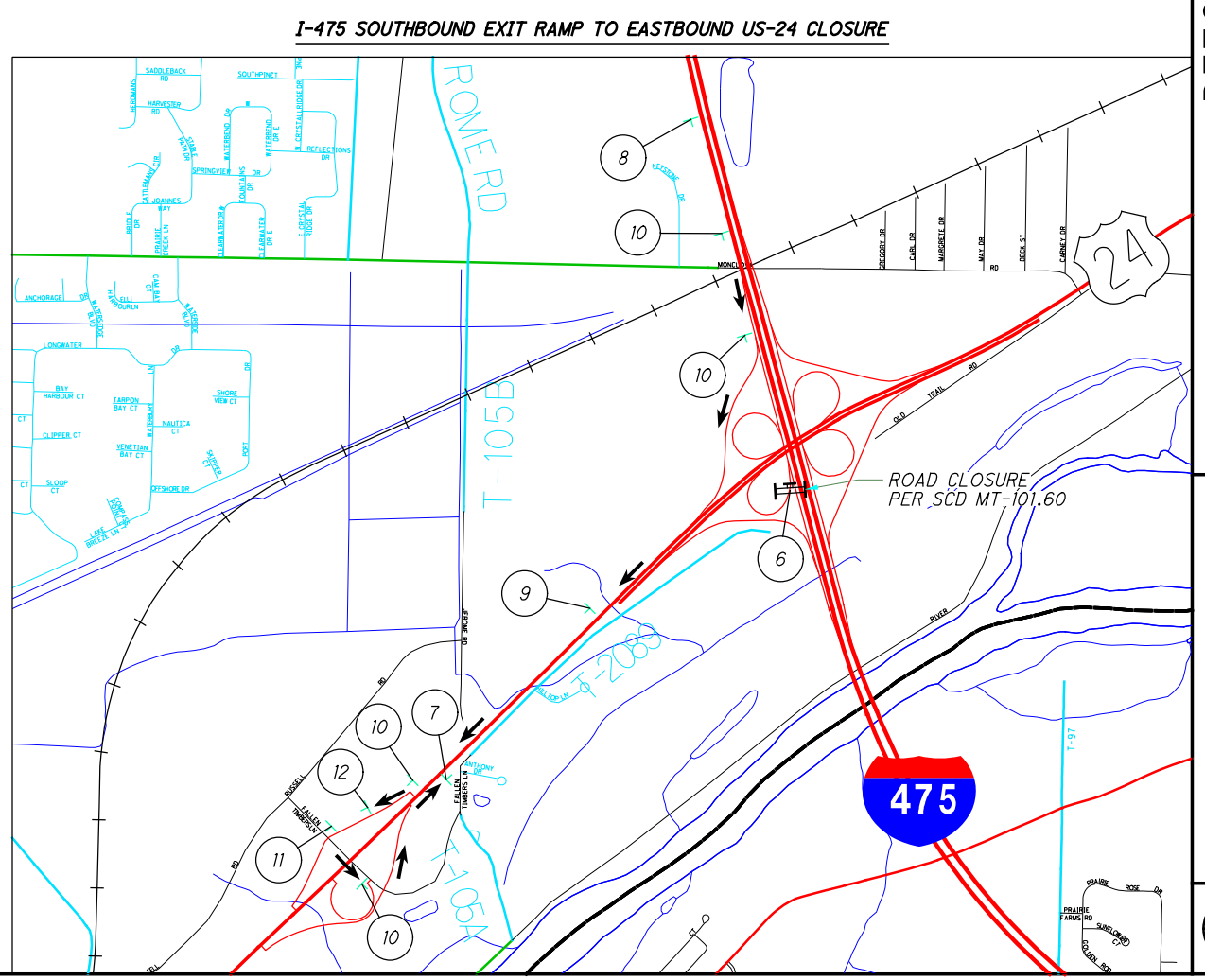
SOUTH
 INTERSTATE
 A: 475

TO

PAY ITEMS FOR THE INSTALLATION OF THESE ITEMS HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614E98200, WORK ZONE PAVEMENT MARKING, MISC.: ROUTE SHIELDS 2 EACH
 ITEM 614E31000, WORK ZONE ON PAVEMENT, 72", CLASS I 4 EACH

THE REMOVAL OF THESE PAVEMENT MARKINGS ONCE THE CLOSURE IS COMPLETED SHALL BE INCLUDED IN ITEM 614, MAINTAINING TRAFFIC LUMP SUM.



NOTE:
 1. OVERLAYS SHALL BE INSTALLED ON ALL EXIT GUIDE SIGNS WHEN THE EXIT RAMP WILL BE CLOSED FOR A MINIMUM OF 24 HOURS AS SPECIFIED IN MT-98.29. ALL COSTS FOR THE PRODUCTION, INSTALLATION, AND SUBSEQUENT REMOVAL OF THE OVERLAYS SHALL BE INCLUDED IN THE SUM BID FOR ITEM 614, MAINTAINING TRAFFIC.
 2. OVERHEAD AND GROUND MOUNTED LANE USE SIGNS SHALL BE COVERED WHEN THE ENTRANCE RAMP IS CLOSED FOR A MINIMUM OF 24 HOURS.

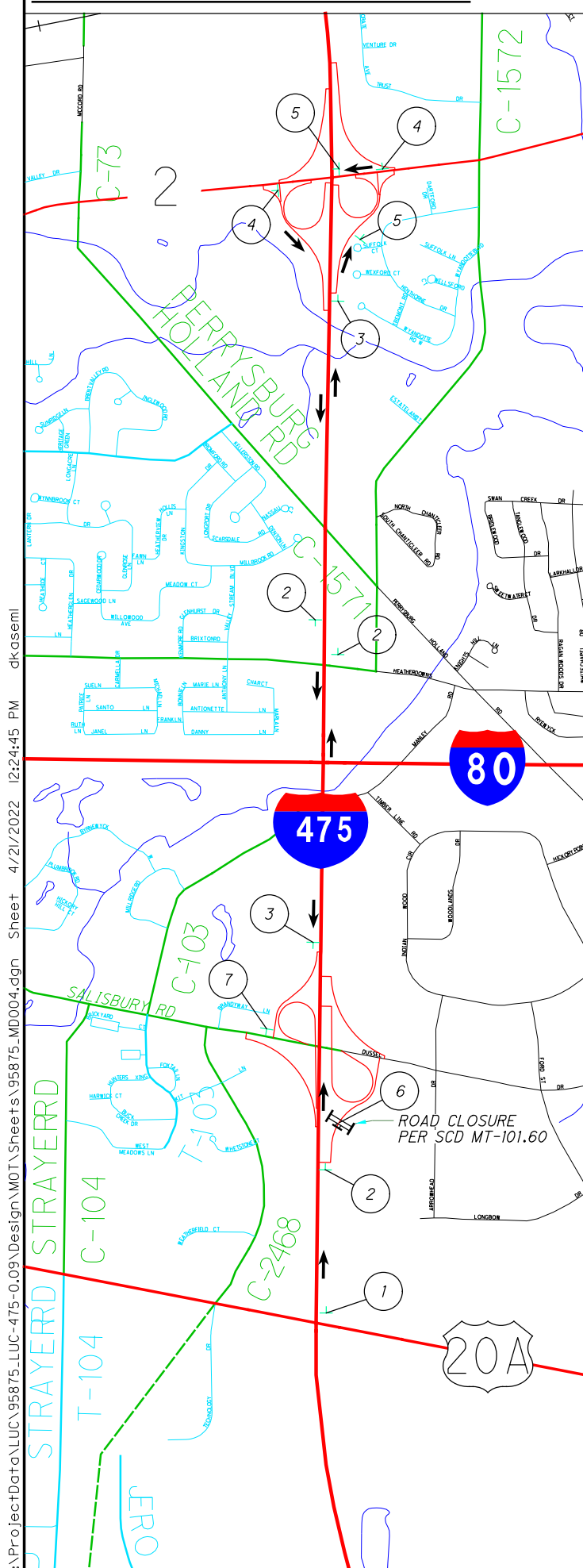
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DETOUR PLAN
I-475 / US-24 INTERCHANGE

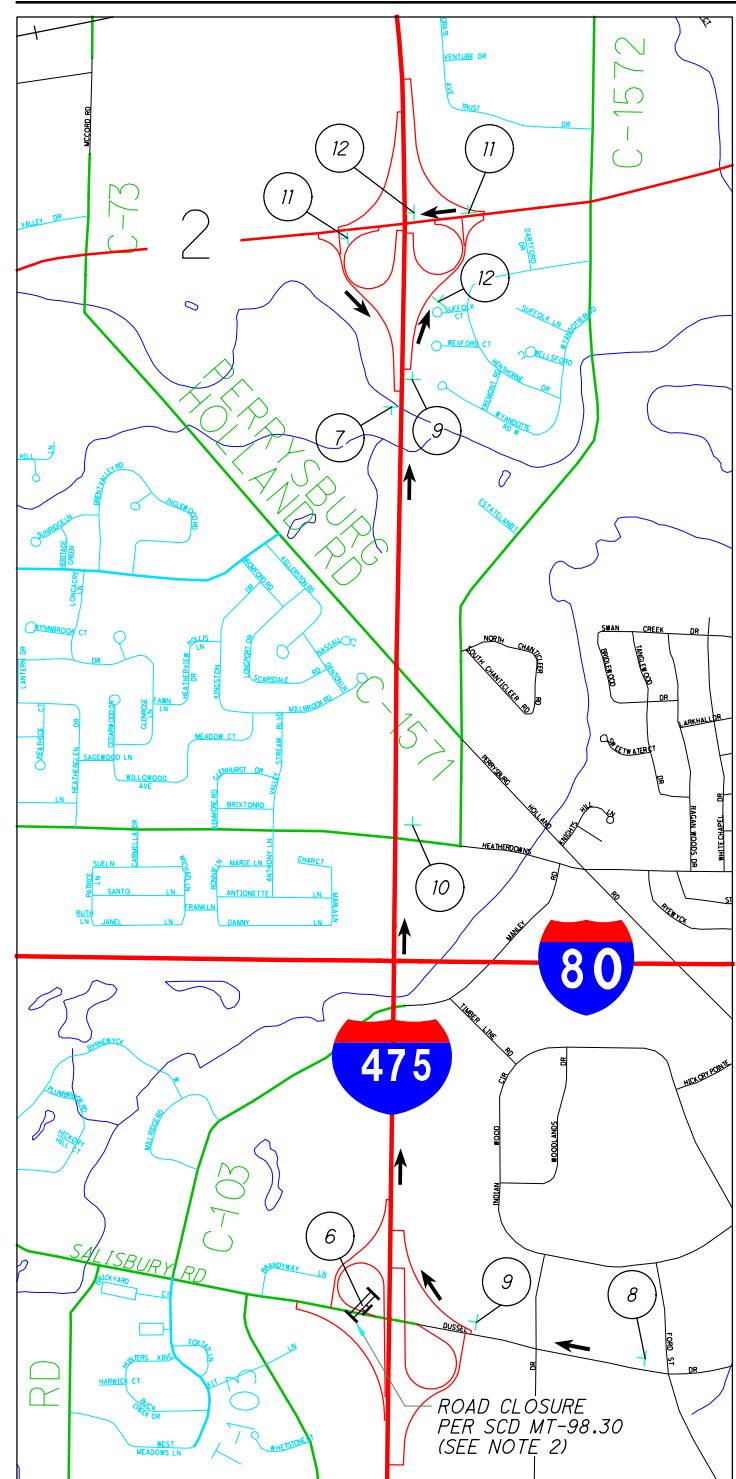
LUC-475-0.09

180
855

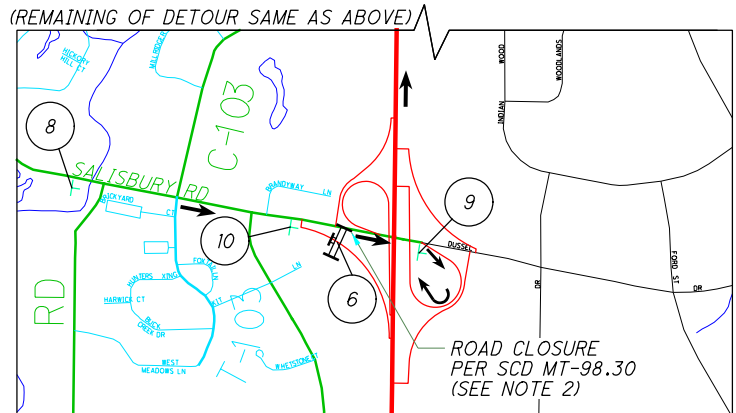
NORTHBOUND I-475 EXIT RAMP TO DUSSEL DR CLOSURE



SOUTHBOUND I-475 ENTRANCE RAMP FROM WESTBOUND DUSSEL DR CLOSURE



SOUTHBOUND I-475 ENTRANCE RAMP FROM EASTBOUND DUSSEL DR CLOSURE



1 Dussel Dr D3-1-24	2 DETOUR M4-8-30 Dussel Dr D3-1-24 Salisbury Rd D3-1-24	3 DETOUR M4-8-30 Dussel Dr D3-1-24 Salisbury Rd D3-1-24	4 DETOUR M4-8-30 Dussel Dr D3-1-24 Salisbury Rd D3-1-24
DETOUR AHEAD W20-2-48	↑ M6-3-30	↗ M6-2R-30	← M6-1L-30
5 DETOUR M4-8-30 Dussel Dr D3-1-24 Salisbury Rd D3-1-24	6 RAMP CLOSED R11-2-48	7 END DETOUR M4-8a-24	
← M5-1L-30			
8 SOUTH M3-4-30 INTERSTATE 475 MI-1-30-3	9 DETOUR M4-8-30 SOUTH M3-4-30 INTERSTATE 475	10 DETOUR M4-8-30 SOUTH M3-4-30 INTERSTATE 475	11 DETOUR M4-8-30 SOUTH M3-4-30 INTERSTATE 475
DETOUR AHEAD W20-2-48	↗ M6-2R-30	↑ M6-3-30	← M6-1L-30
			↖ M5-1L-30

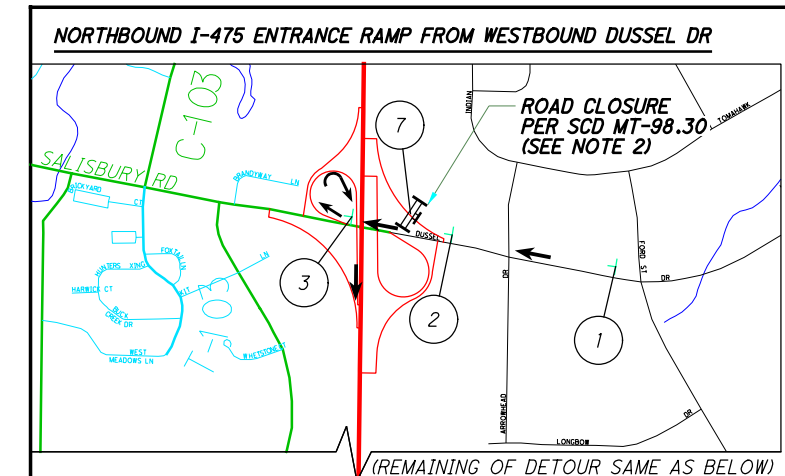
NOTE:
 1. OVERLAYS SHALL BE INSTALLED ON ALL EXIT GUIDE SIGNS WHEN THE EXIT RAMP WILL BE CLOSED FOR A MINIMUM OF 24 HOURS AS SPECIFIED IN MT-98.29. ALL COSTS FOR THE PRODUCTION, INSTALLATION, AND SUBSEQUENT REMOVAL OF THE OVERLAYS SHALL BE INCLUDED IN THE SUM BID FOR ITEM 614, MAINTAINING TRAFFIC.
 2. OVERHEAD AND GROUND MOUNTED LANE USE SIGNS SHALL BE COVERED WHEN THE ENTRANCE RAMP IS CLOSED FOR A MINIMUM OF 24 HOURS.

I:\ProjectData\LUC-475-0.09\Design\M01\Sheets\95875.MD004.dgn Sheet 4/21/2022 12:24:45 PM akasem

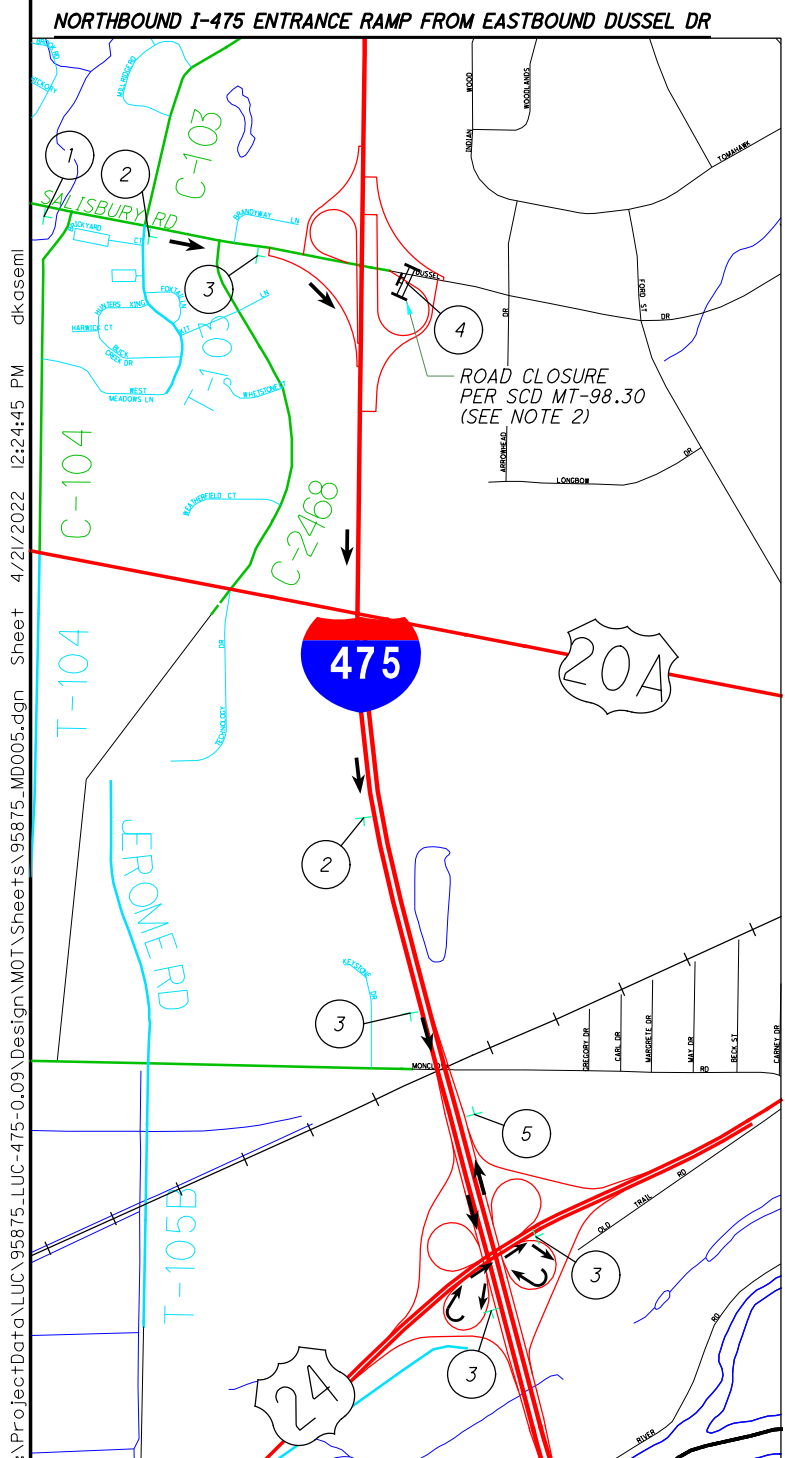
DETOUR PLAN
I-475 & DUSSEL / SALISBURY INTERCHANGE

LUC-475-0.09

181
855



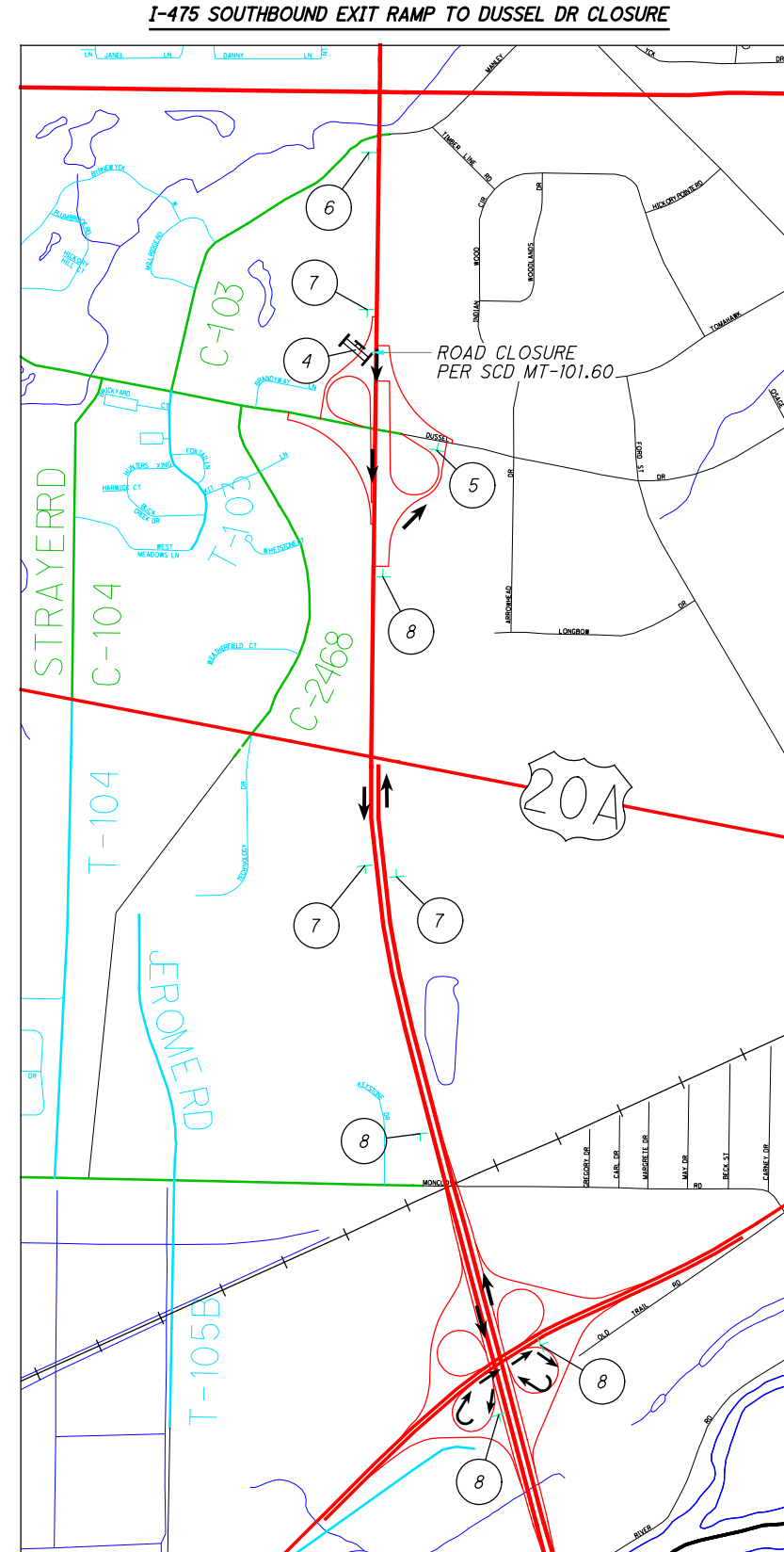
(REMAINING OF DETOUR SAME AS BELOW)



1	2	3	4	5
NORTH	DETOUR	DETOUR	RAMP	END
M3-1-30	M4-8-30	M4-8-30	CLOSED	DETOUR
INTERSTATE	NORTH	NORTH	R11-2-48	M4-8a-24
475	M3-1-30	M3-1-30		
M1-1-30-3	INTERSTATE	INTERSTATE		
	475	475		
DETOUR	M1-1-30-3	M1-1-30-3		
AHEAD	↑	↗		
W20-2-48	M6-3-30	M6-2R-30		

6	7	8	9
Dussel Dr	DETOUR	DETOUR	DETOUR
D3-1-24	M4-8-30	M4-8-30	M4-8-30
Salisbury Rd	Dussel Dr	Dussel Dr	Dussel Dr
D3-1-24	D3-1-24	D3-1-24	D3-1-24
DETOUR	Salisbury Rd	Salisbury Rd	Salisbury Rd
AHEAD	D3-1-24	D3-1-24	D3-1-24
W20-2-48	↑	↗	←
	M6-3-30	M6-2R-30	M5-1L-30

NOTE:
 1. OVERLAYS SHALL BE INSTALLED ON ALL EXIT GUIDE SIGNS WHEN THE EXIT RAMP WILL BE CLOSED FOR A MINIMUM OF 24 HOURS AS SPECIFIED IN MT-98.29. ALL COSTS FOR THE PRODUCTION, INSTALLATION, AND SUBSEQUENT REMOVAL OF THE OVERLAYS SHALL BE INCLUDED IN THE SUM BID FOR ITEM 614, MAINTAINING TRAFFIC.
 2. OVERHEAD AND GROUND MOUNTED LANE USE SIGNS SHALL BE COVERED WHEN THE ENTRANCE RAMP IS CLOSED FOR A MINIMUM OF 24 HOURS.



CALCULATED NME CHECKED JMM

0 1000 2000

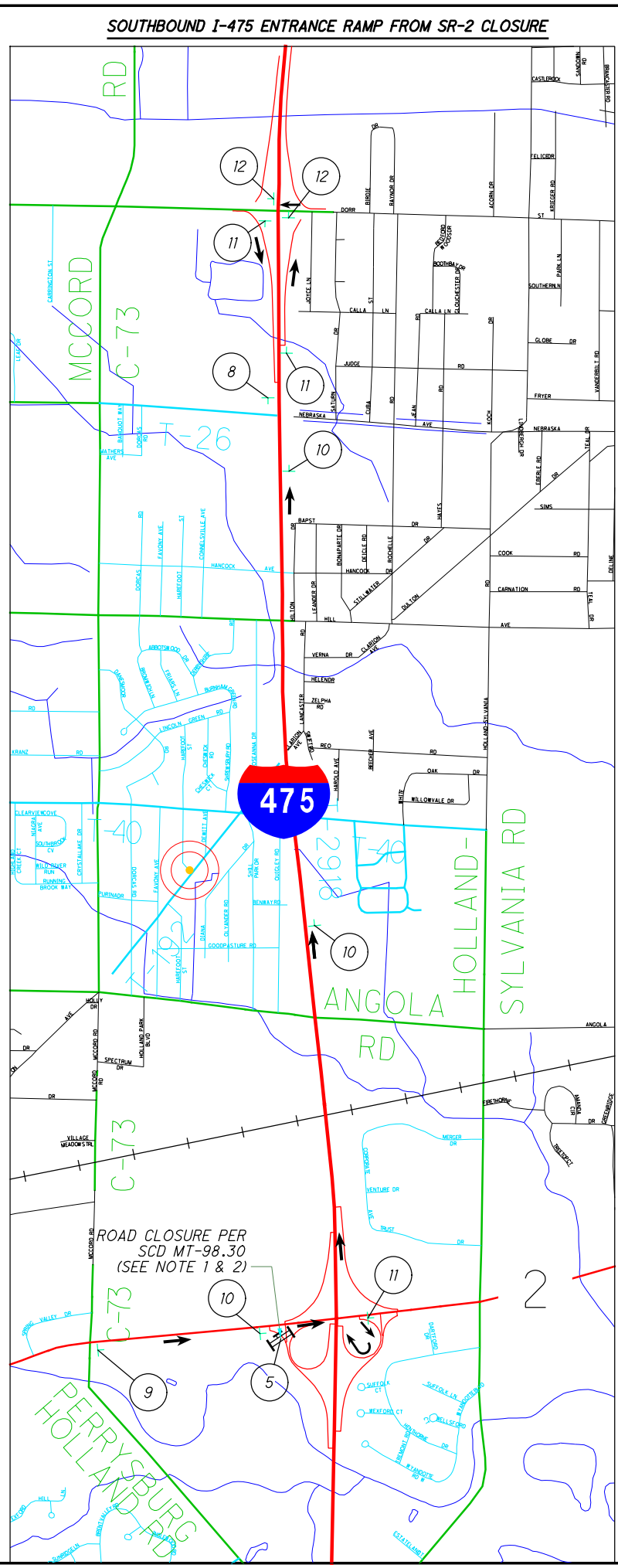
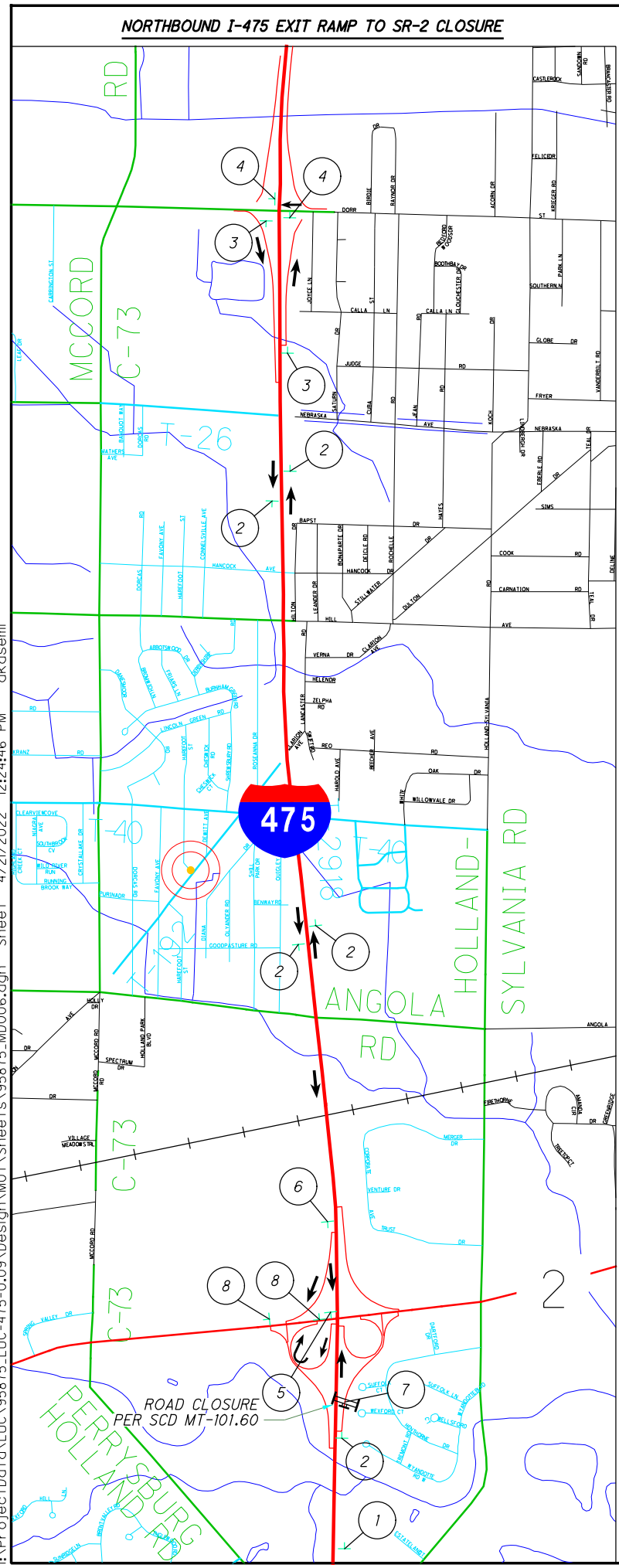
500

HORIZONTAL SCALE IN FEET

DETOUR PLAN
 I-475 & DUSSEL/SALISBURY INTERCHANGE

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I:\ProjectData\LUC-475-0.09\Design\M0T\Sheets\95875.MD006.dgn Sheet 4/2/2022 12:24:46 PM ck-ase.mtl



1	2	3	4	5	6
MI-5-30-2	DETOUR M4-8-30	DETOUR M4-8-30	DETOUR M4-8-30	DETOUR M4-8-30	DETOUR M4-8-30
DETOUR AHEAD	2	2	2	EAST M3-2-30	WEST M3-4-30
W20-2-48	MI-5-30-2 ↑	MI-5-30-2 ↗	MI-5-30-2 ↖	2 MI-5-30-2 ↗	2 MI-5-30-2 ↗
7	8				
RAMP CLOSED R11-2-48	END DETOUR M4-8a-24				
9	10	11	12		
SOUTH M3-4-30 INTERSTATE 475 MI-1-30-3	DETOUR M4-8-30 SOUTH M3-4-30 INTERSTATE 475	DETOUR M4-8-30 SOUTH M3-4-30 INTERSTATE 475	DETOUR M4-8-30 SOUTH M3-4-30 INTERSTATE 475		
DETOUR AHEAD W20-2-48	MI-1-30-3 ↑	MI-1-30-3 ↗	MI-1-30-3 ↖		

NOTE:
 1. OVERLAYS SHALL BE INSTALLED ON ALL EXIT GUIDE SIGNS WHEN THE EXIT RAMP WILL BE CLOSED FOR A MINIMUM OF 24 HOURS AS SPECIFIED IN MT-98.29. ALL COSTS FOR THE PRODUCTION, INSTALLATION, AND SUBSEQUENT REMOVAL OF THE OVERLAYS SHALL BE INCLUDED IN THE SUM BID FOR ITEM 614, MAINTAINING TRAFFIC.
 2. OVERHEAD AND GROUND MOUNTED LANE USE SIGNS SHALL BE COVERED WHEN THE ENTRANCE RAMP IS CLOSED FOR A MINIMUM OF 24 HOURS.

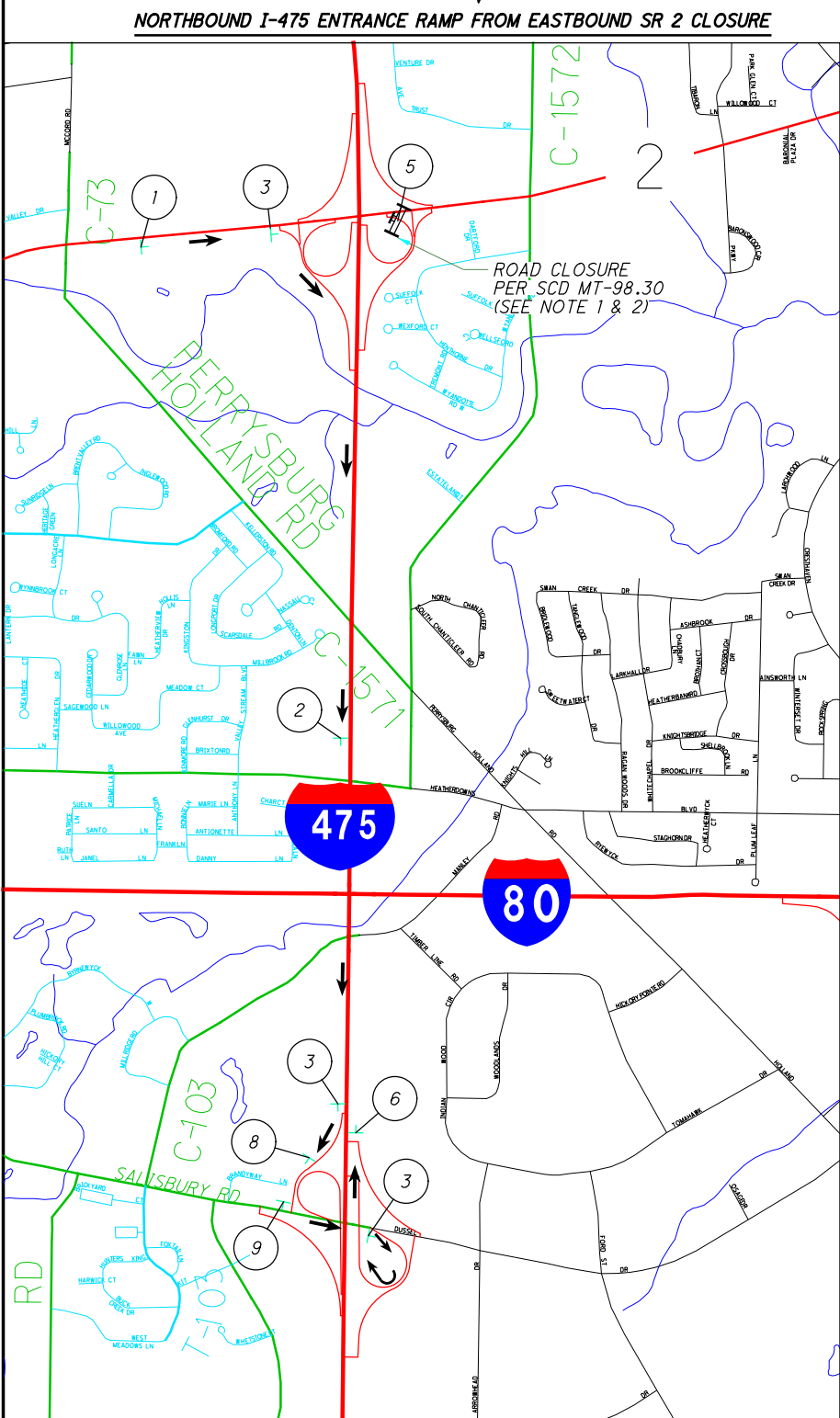
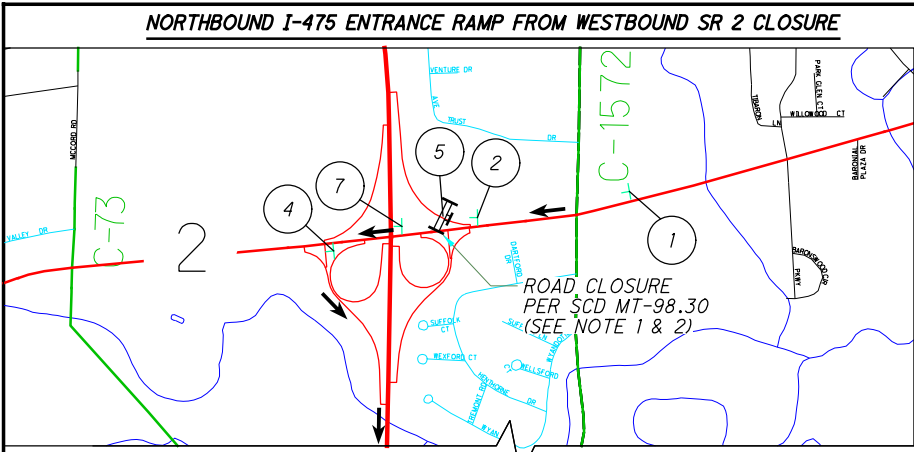
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 NME
 CHECKED JMM

DETOUR PLAN
I-475 / SR-2 INTERCHANGE

LUC-475-0.09

183
855

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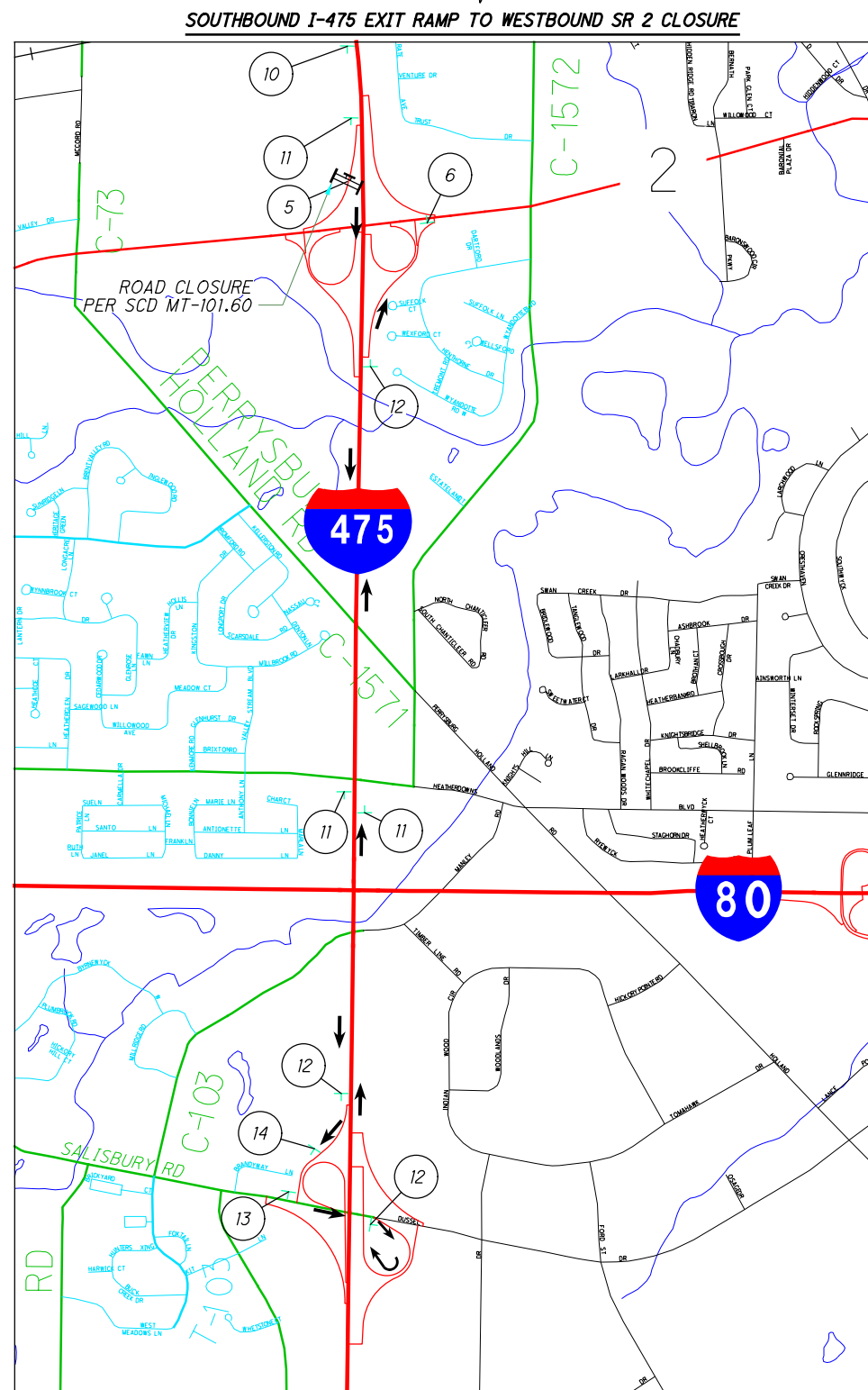
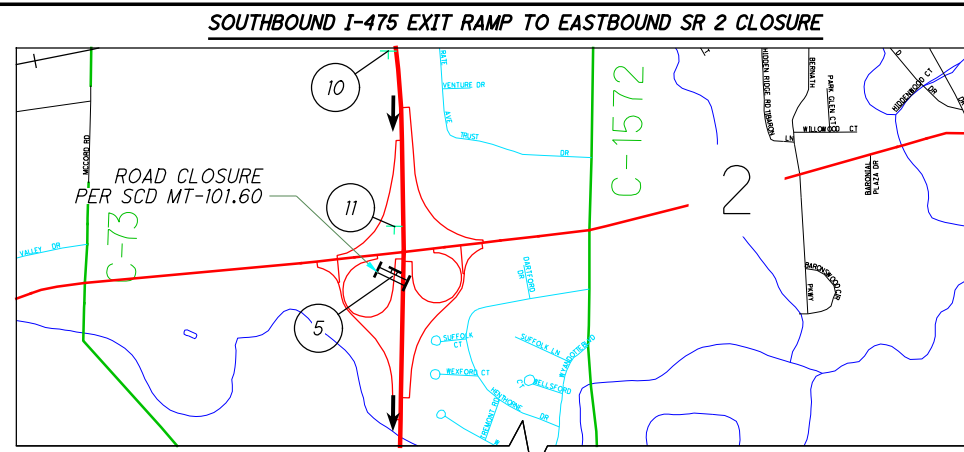


1	2	3	4	5	6
NORTH	DETOUR	DETOUR	DETOUR	RAMP CLOSED	END DETOUR
M3-1-30	M4-8-30	M4-8-30	M4-8-30	R11-2-48	M4-8a-24
INTERSTATE	INTERSTATE	INTERSTATE	INTERSTATE		
475	475	475	475		
MI-1-30-3	MI-1-30-3	MI-1-30-3	MI-1-30-3		
DETOUR AHEAD	↑	↗	←		
W20-2-48	M6-3-30	M6-2R-30	M6-1L-30		

7	8	9
DETOUR	DETOUR	DETOUR
M4-8-30	M4-8-30	M4-8-30
NORTH	NORTH	NORTH
M3-1-30	M3-1-30	M3-1-30
INTERSTATE	INTERSTATE	INTERSTATE
475	475	475
MI-1-30-3	MI-1-30-3	MI-1-30-3
←	←	←
M5-1L-30	M5-1L-30	M6-1L-30
	CENTER LANE	CENTER LANE
	M5-5-24	M5-5-24

10	11	12	13	14
2	DETOUR	DETOUR	DETOUR	DETOUR
MI-5-30-2	M4-8-30	M4-8-30	M4-8-30	M4-8-30
DETOUR AHEAD	2	2	2	2
W20-2-48	MI-5-30-2	MI-5-30-2	MI-5-30-2	MI-5-30-2
	↑	↗	←	↖
	M6-3-30	M6-2R-30	M6-1L-30	M5-1L-30
			CENTER LANE	CENTER LANE
			M5-5-24	M5-5-24

NOTE:
 1. OVERLAYS SHALL BE INSTALLED ON ALL EXIT GUIDE SIGNS WHEN THE EXIT RAMP WILL BE CLOSED FOR A MINIMUM OF 24 HOURS AS SPECIFIED IN MT-98.29. ALL COSTS FOR THE PRODUCTION, INSTALLATION, AND SUBSEQUENT REMOVAL OF THE OVERLAYS SHALL BE INCLUDED IN THE SUM BID FOR ITEM 614, MAINTAINING TRAFFIC.
 2. OVERHEAD AND GROUND MOUNTED LANE USE SIGNS SHALL BE COVERED WHEN THE ENTRANCE RAMP IS CLOSED FOR A MINIMUM OF 24 HOURS.



CALCULATED
 NME
 CHECKED
 JMM

0 1000 2000
 HORIZONTAL SCALE IN FEET

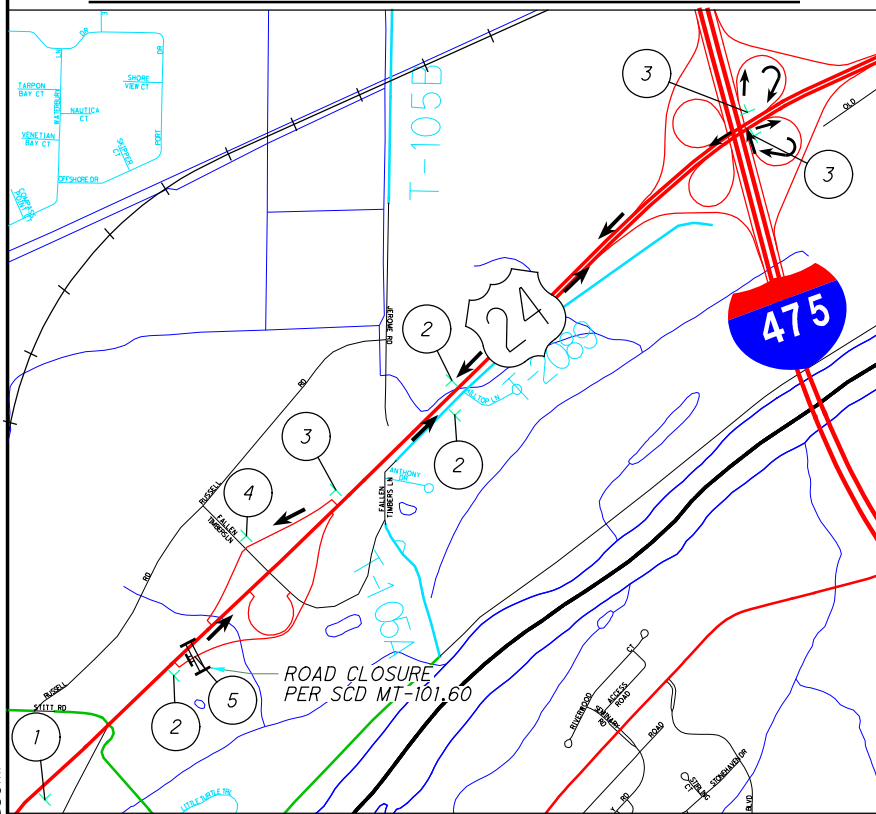
DETOUR PLAN
 I-475 / SR 2 INTERCHANGE

LUC-475-0.09

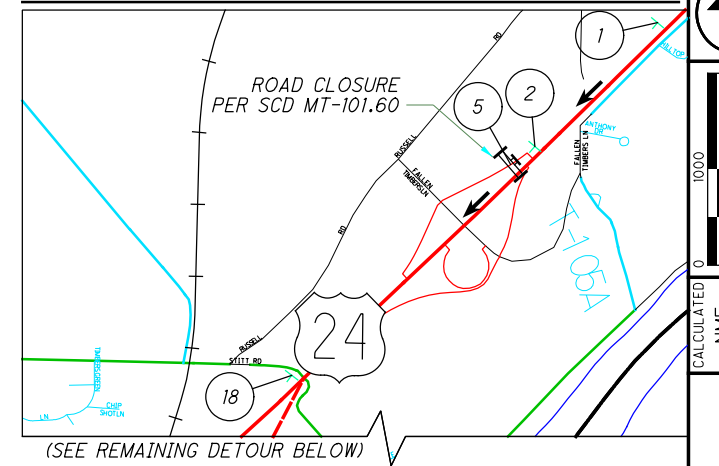
184
855

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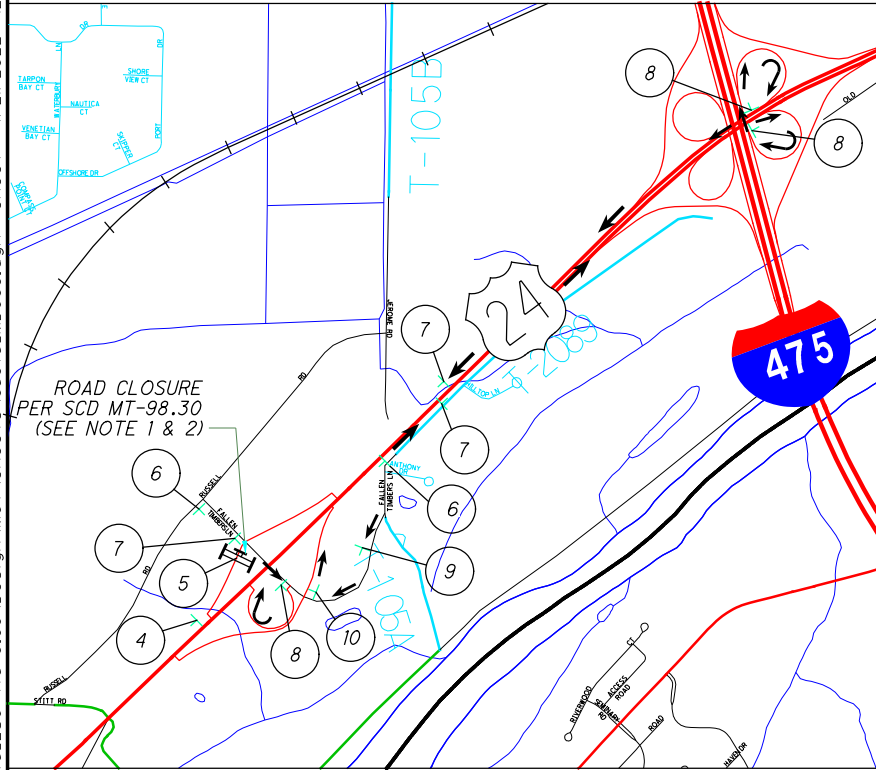
EASTBOUND US-24 ENTRANCE RAMP TO FALLEN TIMBERS LANE CLOSURE



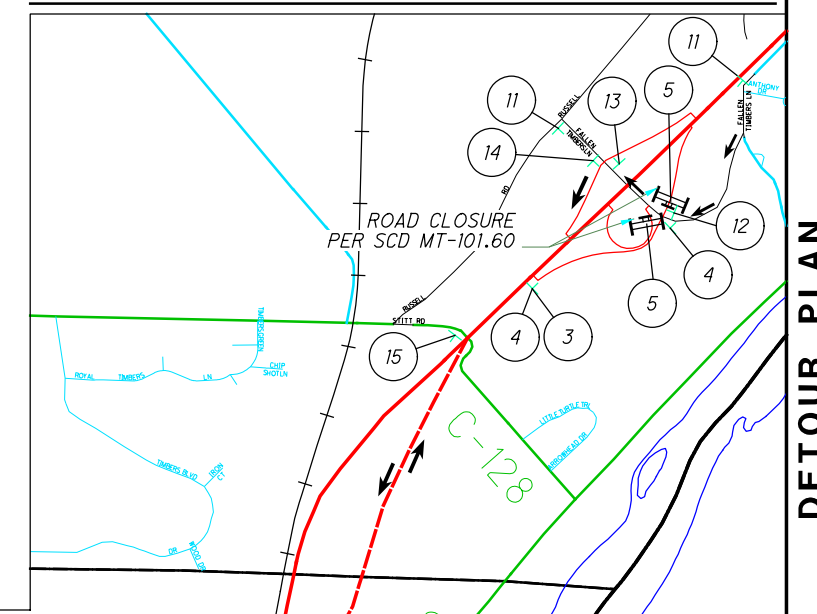
WESTBOUND US-24 EXIT RAMP TO FALLEN TIMBERS LANE CLOSURE



WESTBOUND US-24 ENTRANCE RAMP FROM FALLEN TIMBERS LANE CLOSURE



EASTBOUND US-24 ENTRANCE RAMPS FROM FALLEN TIMBERS LANE CLOSURE

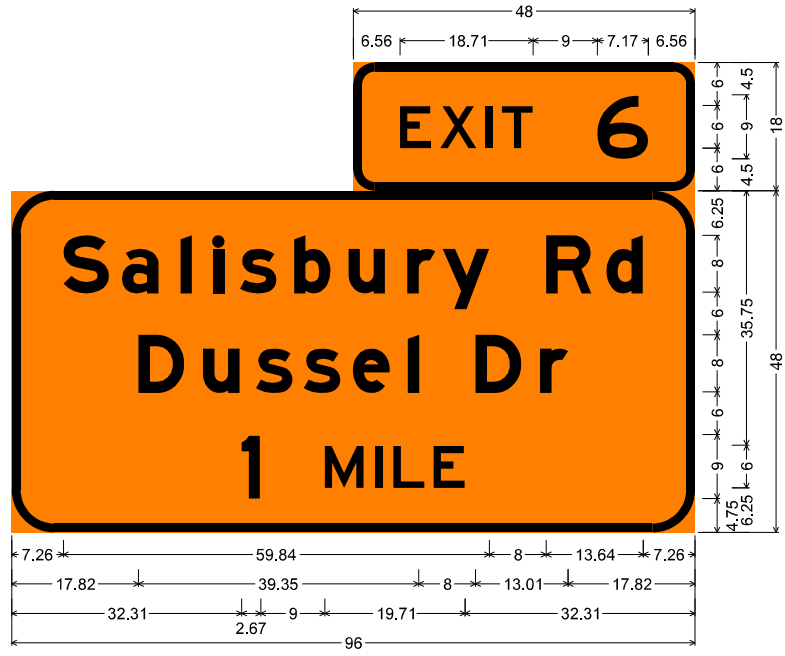


1 FALLEN TIMBERS D3-H6-48 DETOUR AHEAD W20-2-48	2 DETOUR M4-8-30 FALLEN TIMBERS D3-H6-48 ↑ M6-3-30	3 DETOUR M4-8-30 FALLEN TIMBERS D3-H6-48 ↗ M6-2R-30	4 END DETOUR M4-8a-24 5 RAMP CLOSED R11-2-48	6 WEST M3-4-30 MI-4-30-2 DETOUR AHEAD W20-2-48	7 DETOUR M4-8-30 WEST M3-4-30 MI-4-30-2 ↑ M6-3-30	8 DETOUR M4-8-30 WEST M3-4-30 MI-4-30-2 ↗ M6-2R-30	9 DETOUR M4-8-30 WEST M3-4-30 MI-4-30-2 ↘ M5-1R-30	
10 DETOUR M4-8-30 WEST M3-4-30 MI-4-30-2 → M6-1R-30	11 EAST M3-2-30 MI-4-30-2 DETOUR AHEAD W20-2-48	12 DETOUR M4-8-30 EAST M3-2-30 MI-4-30-2 ↑ M6-3-30	13 DETOUR M4-8-30 EAST M3-2-30 MI-4-30-2 ← M6-1L-30	14 DETOUR M4-8-30 EAST M3-2-30 MI-4-30-2 → M6-1R-30	15 DETOUR M4-8-30 EAST M3-2-30 MI-4-30-2 ↖ M6-2L-30	16 DETOUR M4-8-30 EAST M3-2-30 MI-4-30-2 ↗ M6-2R-30	17 DETOUR M4-8-30 EAST M3-2-30 MI-4-30-2 ↘ M5-1L-30	
18 DETOUR M4-8-30 FALLEN TIMBERS D3-H6-48 ↖ M6-2L-30	19 DETOUR M4-8-30 FALLEN TIMBERS D3-H6-48 ↙ M5-1L-30	20 DETOUR M4-8-30 FALLEN TIMBERS D3-H6-48 ← M6-1L-30						

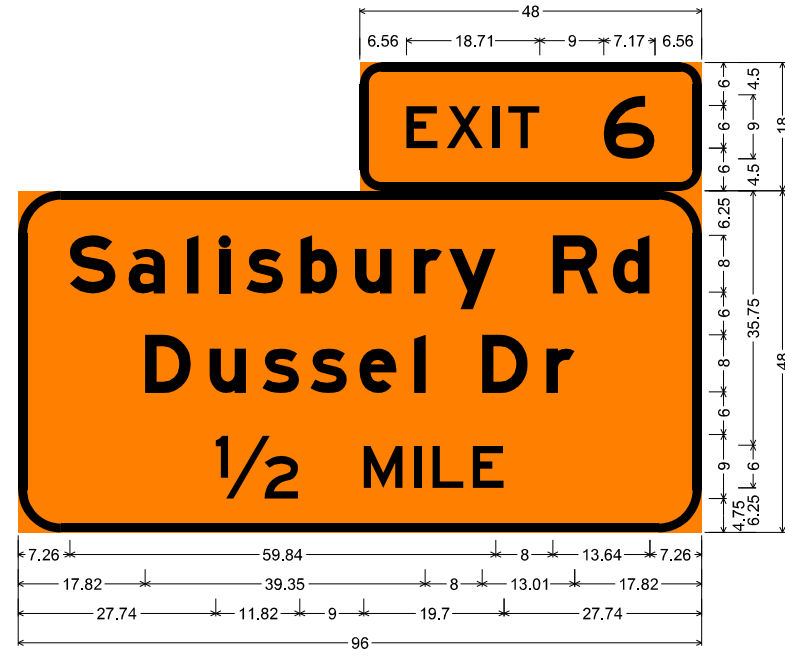
NOTE:
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CALCULATED NME CHECKED JMM
 HORIZONTAL SCALE IN FEET
 0 500 1000 2000
 185 / 855
DETOUR PLAN
US-24 / FALLEN TIMBERS
LUC-475-0.09

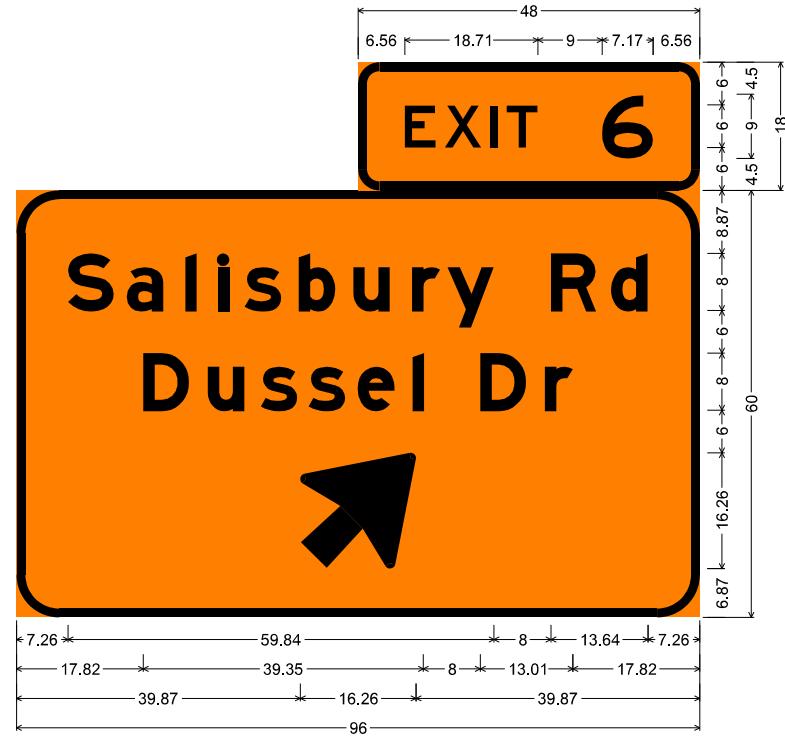
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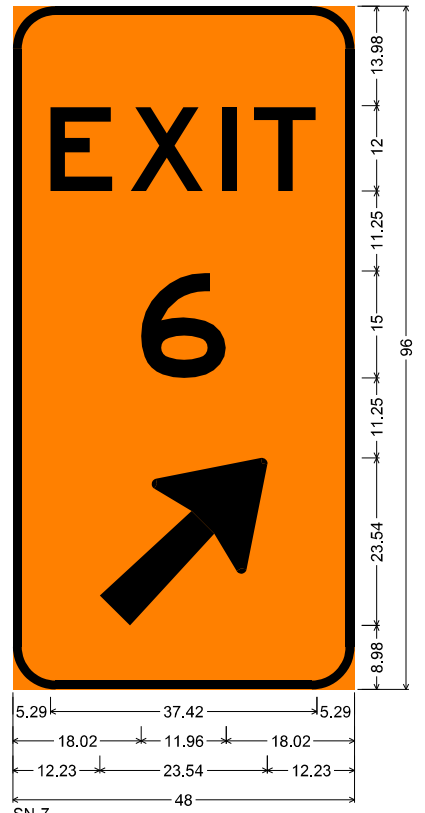
SN-1, SN-2
3.00" Radius, 1.25" Border, Black on Orange;
"EXIT 6", E;
6.00" Radius, 1.25" Border, Black on Orange;
"Salisbury Rd", E Mod; "Dussel Dr", E Mod; "1 MILE", E;



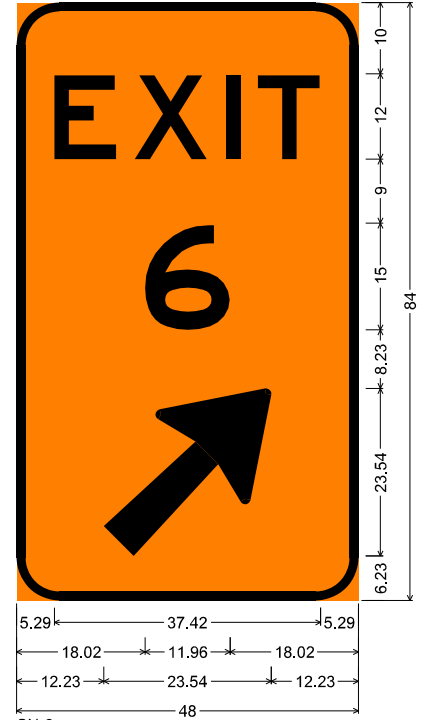
SN-3, SN-4
3.00" Radius, 1.25" Border, Black on Orange;
"EXIT 6", E;
6.00" Radius, 1.25" Border, Black on Orange;
"Salisbury Rd", E Mod; "Dussel Dr", E Mod; "1/2 MILE", E;



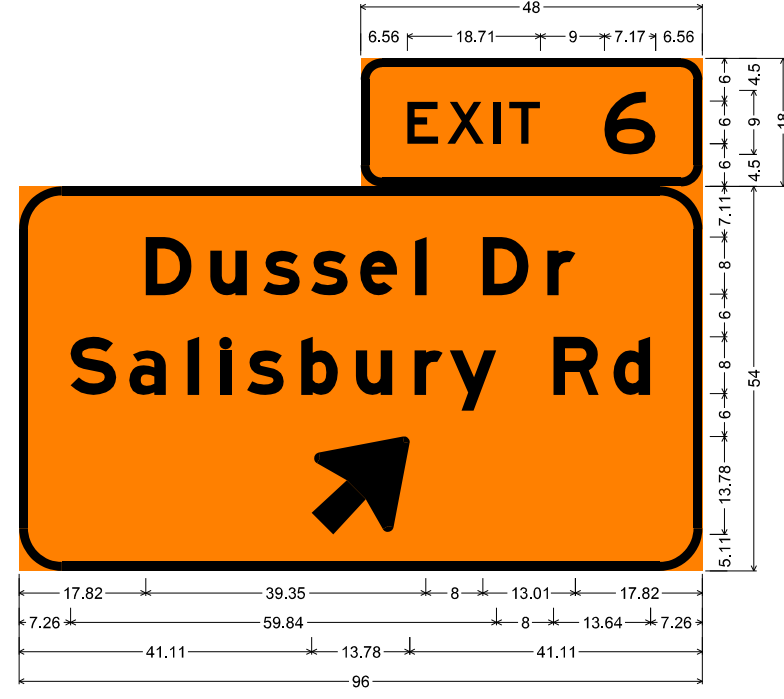
SN-5, SN-6
3.00" Radius, 1.25" Border, Black on Orange;
"EXIT 6", E;
6.00" Radius, 1.25" Border, Black on Orange;
"Salisbury Rd", E Mod; "Dussel Dr", E Mod; Arrow B-3 - 20.00" 45°;



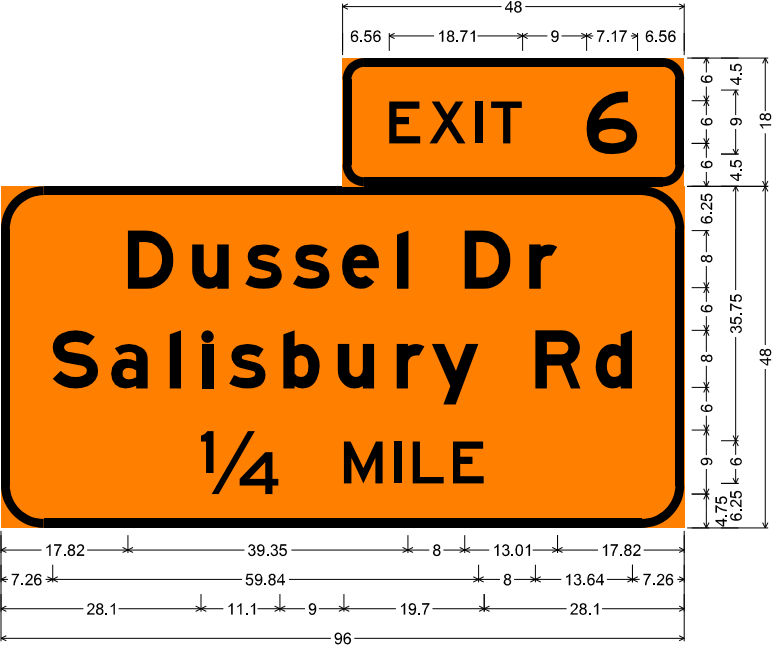
SN-7
6.00" Radius, 1.25" Border, Black on Orange;
"EXIT", E; "6", E;
Arrow A-2 - 30.00" 45°;



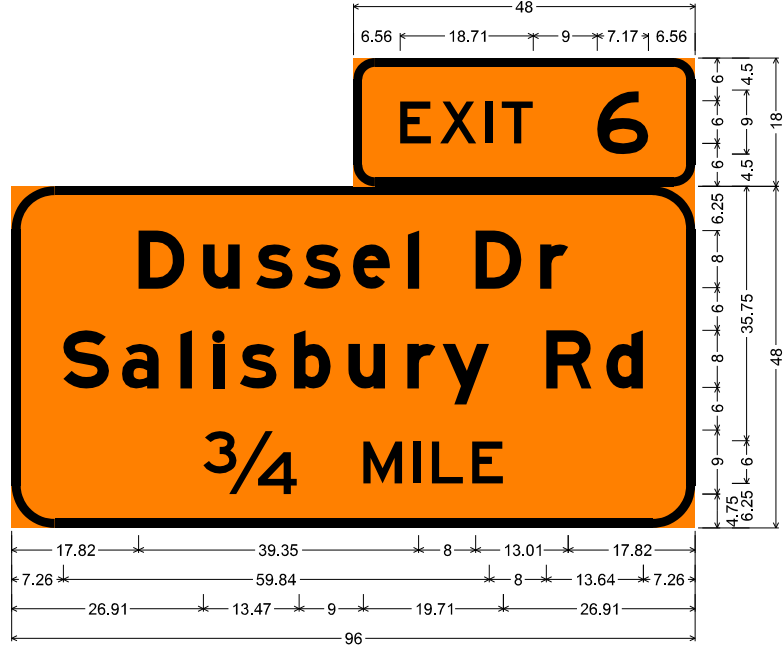
SN-8
6.00" Radius, 1.25" Border, Black on Orange;
"EXIT", E; "6", E;
Arrow A-2 - 30.00" 45°;



SN-9, SN-10
3.00" Radius, 1.25" Border, Black on Orange;
"EXIT 6", E;
6.00" Radius, 1.25" Border, Black on Orange;
"Dussel Dr", E Mod; "Salisbury Rd", E Mod; Arrow B-4 - 17.00" 45°;



SN-11, SN-12
3.00" Radius, 1.25" Border, Black on Orange;
"EXIT 6", E;
6.00" Radius, 1.25" Border, Black on Orange;
"Dussel Dr", E Mod; "Salisbury Rd", E Mod; "1/4 MILE", E;

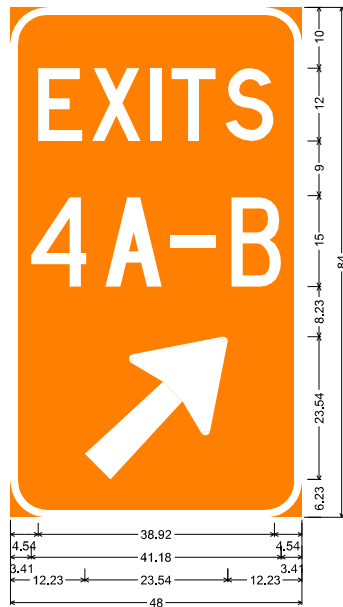


SN-13, SN-14
3.00" Radius, 1.25" Border, Black on Orange;
"EXIT 6", E;
6.00" Radius, 1.25" Border, Black on Orange;
"Dussel Dr", E Mod; "Salisbury Rd", E Mod; "3/4 MILE", E;

CALCULATED
CAJ
CHECKED
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MAINTENANCE OF TRAFFIC SIGNING DETAILS

LUC-475-01.85



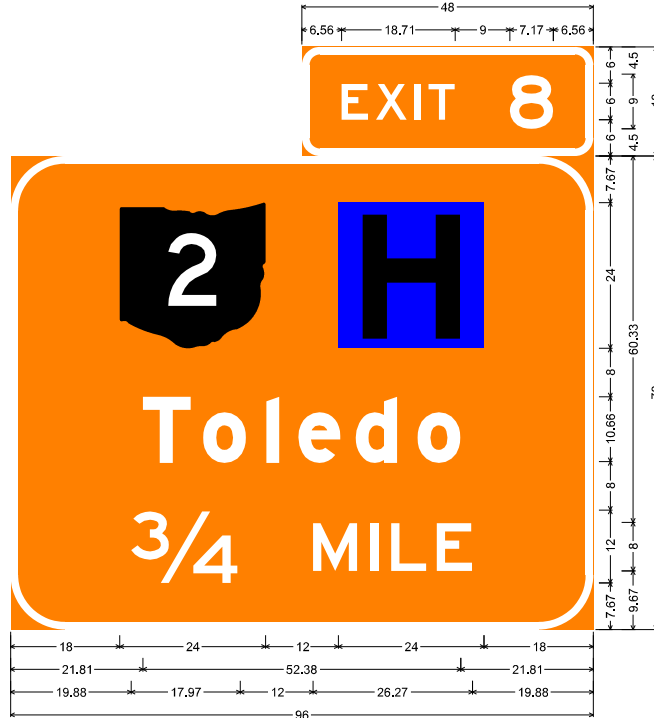
SN-101, SN-116
6.00" Radius, 1.25" Border, Black on Orange;
[EXITS] D 70% spacing;
[4A-B] C 80% spacing;
Arrow A-2 - 30.00" 45°;



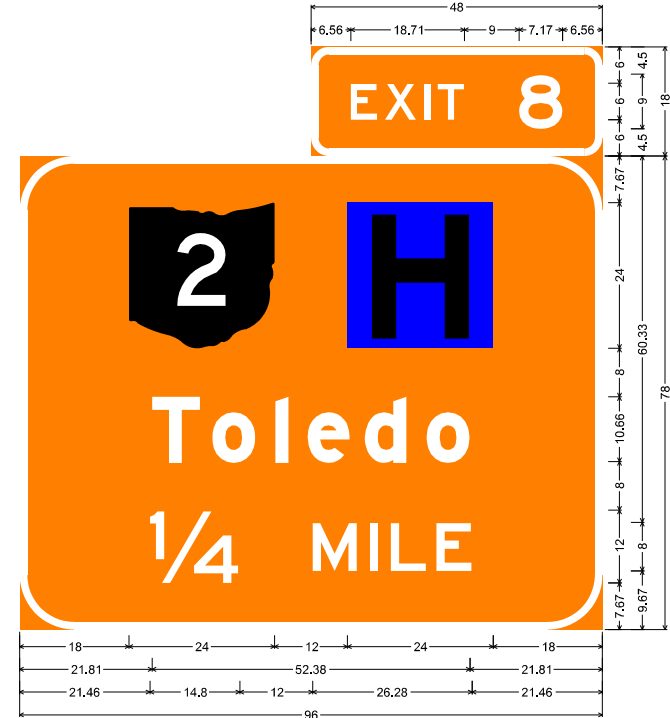
SN-102, SN-108, SN-113, SN-114, SN-124
6.00" Radius, 1.25" Border, Black on Orange;
[EXIT 4A] E: [E]; [AST] E: US 24 M1-4;
Arrow B-4 - 17.00" 45°;



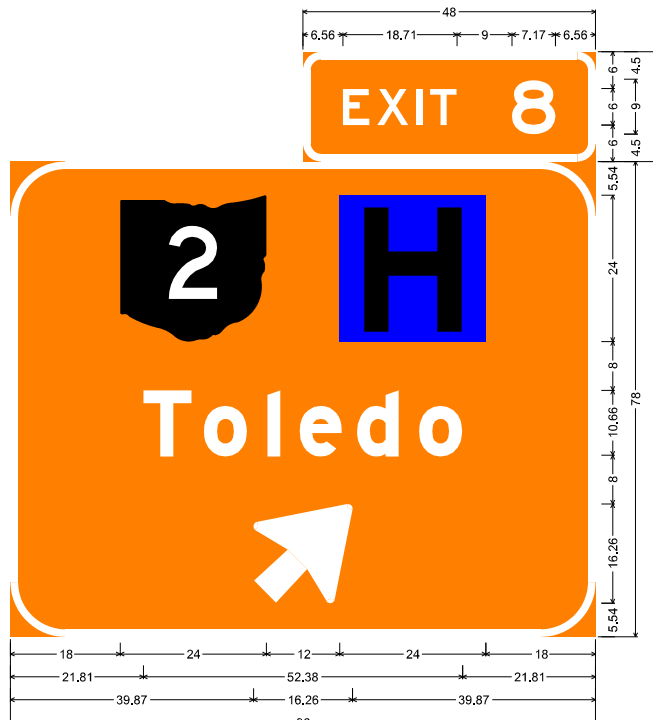
SN-103, SN-115, SN-125
6.00" Radius, 1.25" Border, Black on Orange;
[EXIT 4B] E: [W] E; [E] E; US 24 M1-4;
Arrow B-4 - 17.00" 45°;



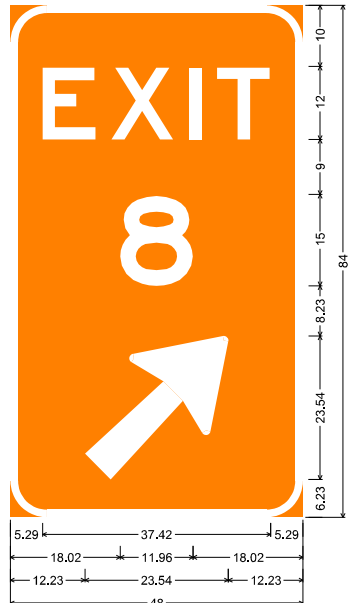
SN-104, SN-109
3.00" Radius, 1.25" Border, Black on Orange;
"EXIT 8", E;
9.00" Radius, 1.25" Border, Black on Orange;
State Highway 2 M1-H5-24-2; Rectangle Blue;
"Toledo", E Mod: "3/4 MILE", E;



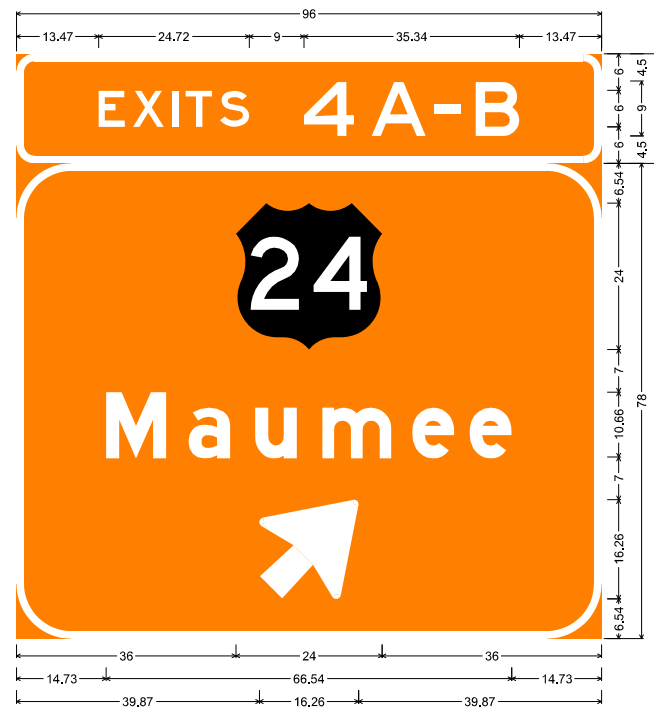
SN-105, SN-110
3.00" Radius, 1.25" Border, Black on Orange;
"EXIT 8", E;
9.00" Radius, 1.25" Border, Black on Orange;
State Highway 2 M1-H5-24-2; Rectangle Blue;
"Toledo", E Mod: "1/4 MILE", E;



SN-106, SN-111
3.00" Radius, 1.25" Border, Black on Orange;
"EXIT 8", E;
9.00" Radius, 1.25" Border, Black on Orange;
State Highway 2 M1-H5-24-2; Rectangle Blue;
"Toledo", E Mod; Arrow B-3 - 20.00" 45°;



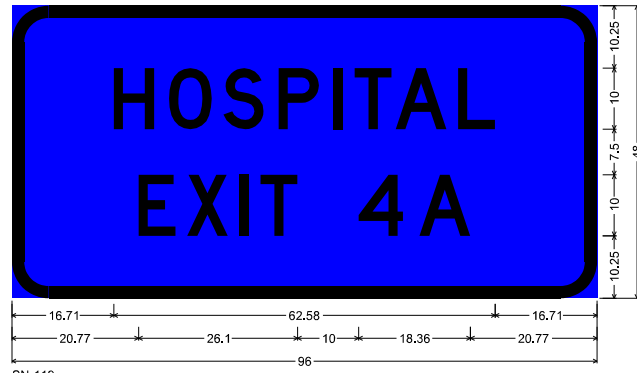
SN-107, SN-112
6.00" Radius, 1.25" Border, Black on Orange;
"EXIT", E: "8", E; Arrow A-2 - 30.00" 45°;



SN-117
3.00" Radius, 1.25" Border, Black on Orange;
"EXITS 4A-B", E;
9.00" Radius, 1.25" Border, Black on Orange;
US 24 M1-4; "Maumee", E Mod; Arrow B-3 - 20.00" 45°;



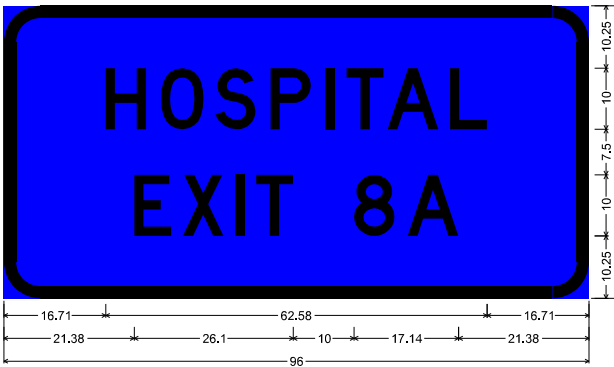
SN-118
3.00" Radius, 1.25" Border, Black on Orange;
"EXITS 4A-B", E;
9.00" Radius, 1.25" Border, Black on Orange;
US 24 M1-4; "Maumee", E Mod: "1/4 MILE", E;



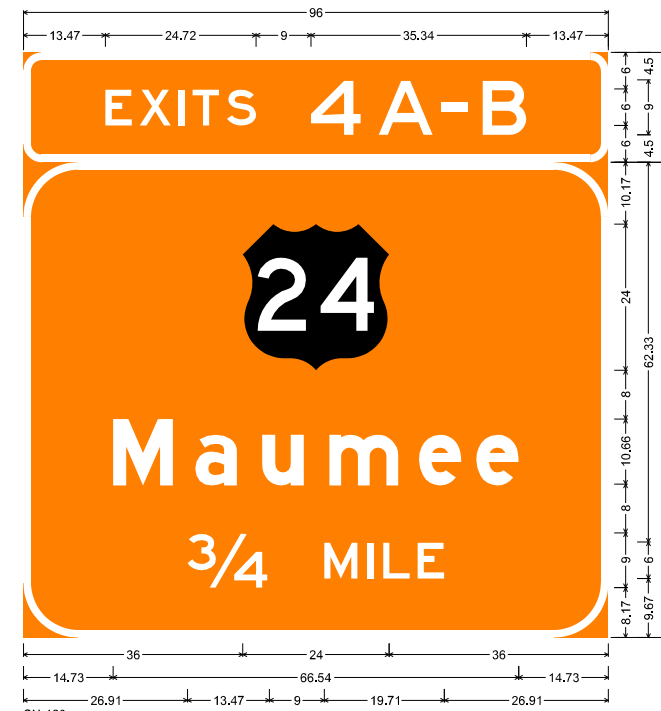
SN-119
6.00" Radius, 2.00" Border, White on Blue;
"HOSPITAL", D; "EXIT 4A", D;



SN-129 LOCATED APPROX. AT SLM 5.87
No border, White on Orange;
"1/2 MILE" Black, E;

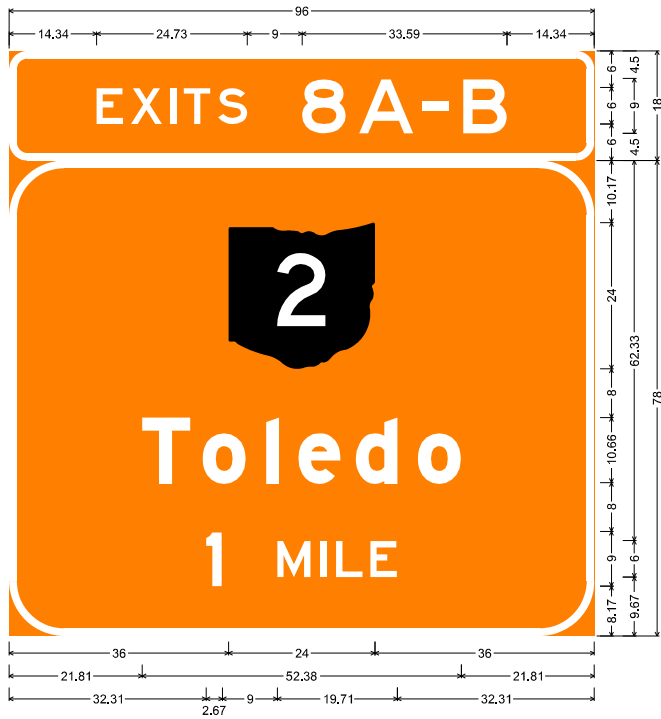


SN-130 LOCATED APPROX. AT SLM 6.10
6.00" Radius, 2.00" Border, White on Blue;
"HOSPITAL", D; "EXIT 8A", D;



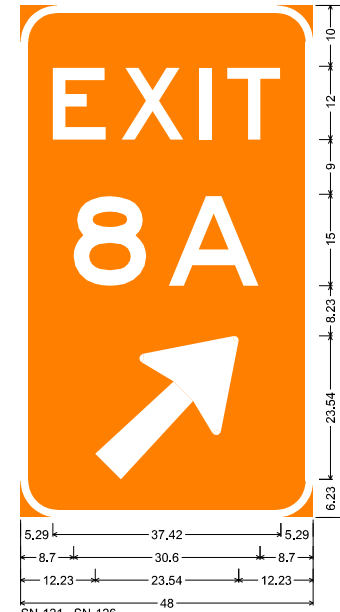
SN-120
3.00" Radius, 1.25" Border, Black on Orange;
"EXITS 4A-B", E;

9.00" Radius, 1.25" Border, Black on Orange;
US 24 M1-4; "Maumee", E Mod; "3/4 MILE", E;

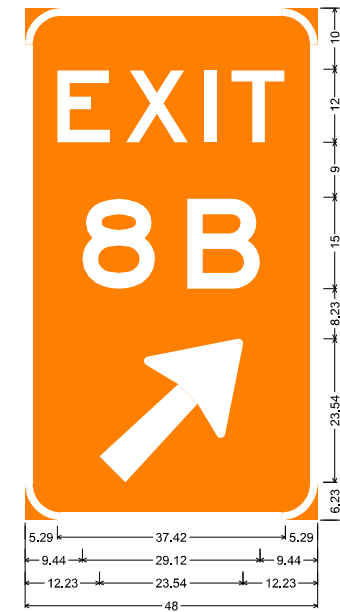


SN-131 LOCATED APPROX. AT SLM 6.56
3.00" Radius, 1.25" Border, Black on Orange;
"EXITS 8A-B", E;

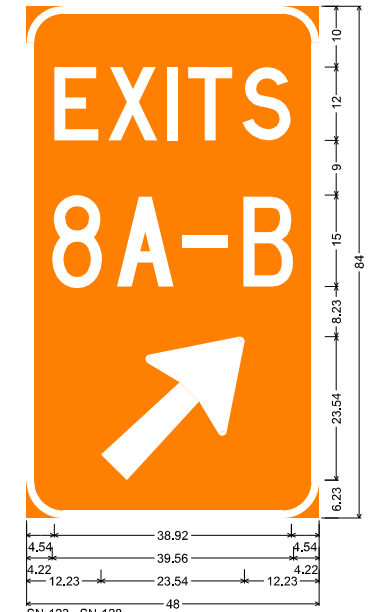
9.00" Radius, 1.25" Border, Black on Orange;
State Highway 2 M1-H5-24-2; "Toledo", E Mod; "1 MILE", E;



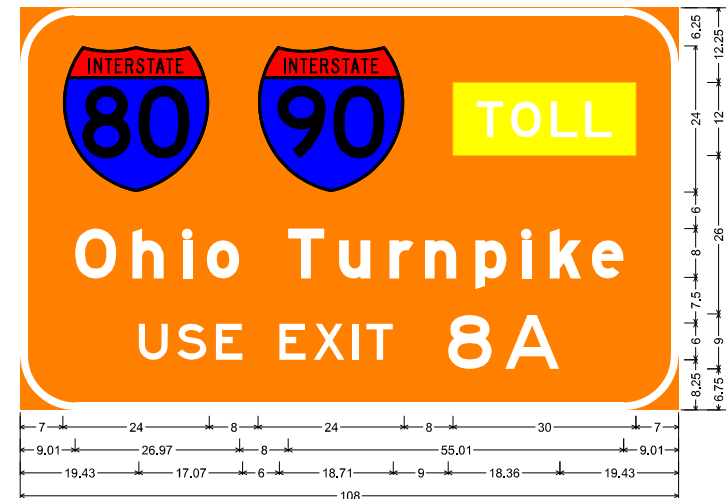
SN-121, SN-126
6.00" Radius, 1.25" Border, Black on Orange;
"EXIT", E; "8A", E; Arrow A-2 - 30.00" 45";



SN-122, SN-127
6.00" Radius, 1.25" Border, Black on Orange;
"EXIT", E; "8B", E; Arrow A-2 - 30.00" 45";



SN-123, SN-128
6.00" Radius, 1.25" Border, Black on Orange;
"EXITS", D 70% spacing;
"8A-B", C 80% spacing;
Arrow A-2 - 30.00" 45";



SN-132 LOCATED APPROX. AT SLM 6.71
9.00" Radius, 1.25" Border, Black on Orange;
Interstate 80 M1-1; Interstate 90 M1-1; Rectangle Yellow;
"Ohio Turnpike", E Mod; "USE EXIT 8A", E;

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REF NO.	SHEET NO.	STATION TO STATION		203	203	203	203	659
				EXCAVATION CY	EMBANKMENT CY	GRANULAR MATERIAL, TYPE B CY	GRANULAR MATERIAL, TYPE D CY	SEEDING AND MULCHING SY
		IR 475						
	331	12+00.00	TO 13+00.00	827	510			2118
	332	13+50.00		811	286			1412
	333	15+00.00		408	93			1040
	334	16+50.00		570	349			1760
	335	18+00.00		554	198			1290
	336	19+50.00		553	185			1001
	337	21+00.00		516	101			841
	338	22+50.00		762	41			811
	339	24+00.00		938	43			734
	340	25+50.00		649	40			806
	341	27+00.00		926	122			897
	342	28+50.00		1118	30			912
	343	30+00.00		1098	25			654
	344	31+50.00		1101	29			740
	345	33+00.00		1012	93			826
	346	34+50.00		1067	45			824
	347	36+00.00		1045	22			809
	348	37+50.00		960	25			718
	349	39+00.00		699	73			1648
	350	40+50.00		368	450			2988
	351	42+00.00		188	1122			2484
	352	43+50.00		124	1701			2309
	353	45+00.00		59	2397			2381
	354	46+50.00		6	2821			2326
	355	48+00.00		0	2199			1556
	356	51+00.00		3012	10218	118	343	1718
	357	52+50.00		6591	18962	222	654	3657
	358	54+00.00		6502	18333	226	659	3680
	359	55+50.00		6581	16685	235	688	3727
	360	57+00.00		6369	13728	220	643	3803
	361	58+50.00		6175	11049	57	167	3707
	362	60+00.00		5860	8818			3551
	363	61+50.00		3500	4563			2229
	364	63+00.00		1603	1772			1310
	365	64+50.00		524	79			606
	366	66+00.00		631	24			606
	367	67+36.36		453	14			402
	368	68+50.00		665	41			606
	369	70+00.00		670	46			606
	370	71+50.00		728	30			606
	371	73+00.00		691	44			606
	372	74+50.00		644	21			606
	373	76+00.00		746	32			606
	374	77+50.00		705	29			606
	375	79+00.00		686	42			606
	376	80+50.00		777	42			606
	377	82+00.00		775	39			606
	378	83+50.00		796	36			606
	379	85+00.00		780	51			606
	380	86+50.00		769	49			606
	381	88+00.00		812	35			606
	382	89+50.00		815	36			606
	383	91+00.00		818	37			606
	384	92+50.00		851	29			606
TOTALS CARRIED TO SHEET 3 OF 3				76888	117884	1078	3154	73183

REF NO.	SHEET NO.	STATION TO STATION		203	203	203	203	659
				EXCAVATION CY	EMBANKMENT CY	GRANULAR MATERIAL, TYPE B CY	GRANULAR MATERIAL, TYPE D CY	SEEDING AND MULCHING SY
		IR 475						
	385	94+00.00	TO 95+00.00	887	24			606
	386	95+50.00		875	25			606
	387	97+00.00		960	11			606
	388	98+50.00		933	17			606
	389	100+00.00		561	16			404
	390	101+00.00		695	38			606
	391	102+50.00		609	41			606
	392	104+00.00		625	30			606
	393	105+50.00		626	9			606
	394	107+00.00		619	26			606
	395	108+50.00		583	39			606
	396	110+00.00		573	44			606
	397	111+50.00		560	54			606
	398	113+00.00		568	47			606
	399	114+50.00		556	38			606
	400	116+00.00		549	38			606
	401	117+50.00		552	31			606
	402	119+00.00		625	11			606
	403	120+50.00		616	16			606
	404	122+00.00		601	20			606
	405	123+50.00		620	19			606
	406	125+00.00		612	28			606
	407	126+50.00		601	41			606
	408	128+00.00		648	31			606
	409	129+50.00		675	19			606
	410	131+00.00		690	14			606
	411	132+50.00		650	8			606
	412	134+00.00		609	3			606
	413	135+50.00		699	7			606
	414	137+00.00		663	15			606
	415	138+50.00		622	15			606
	416	140+00.00		687	9			606
	417	141+50.00		714	147			703
	418	143+00.00		665	354			729
	419	144+50.00		656	300			711
	420	146+00.00		588	277			688
	421	147+50.00		605	236			701
	422	149+00.00		716	199			712
	423	150+50.00		763	175			674
	424	152+00.00		701	157			656
	425	153+50.00		656	141			667
	426	155+00.00		635	126			669
	427	156+50.00		610	118			660
	428	158+00.00		760	82			675
	429	159+50.00		770	69			692
	430	161+00.00		799	55			677
	431	162+50.00		564	75			675
	432	164+00.00		216	72			702
	433	165+50.00		152	28			443
	434	174+00.00		146	44			1483
	435	175+50.00		221	63			1429
	436	177+00.00		445	35			1541
	437	178+50.00		468	179			1335
	438	180+00.00		519	173			786
TOTALS CARRIED TO SHEET 3 OF 3				33318	3889			37198

CALCULATED JRB
 CHECKED XXX
EARTHWORK SUBSUMMARY SHEET 1 OF 3
LUC-475-0.09
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REF NO.	SHEET NO.	STATION TO STATION				EXCAVATION CY	EMBANKMENT CY	GRANULAR MATERIAL, TYPE B CY	GRANULAR MATERIAL, TYPE D CY	SEEDING AND MULCHING SY
		IR 475	TO							
439	181+50.00	182+50.00			601	143			713	
440	183+00.00	184+00.00			815	20			730	
441	184+50.00	185+50.00			923	7			839	
442	186+00.00	187+00.00			984	4			799	
443	187+50.00	188+50.00			964	2			740	
444	189+00.00	190+00.00			873	5			757	
445	190+50.00	191+50.00			777	11			763	
446	192+00.00	193+00.00			682	28			782	
447	193+50.00	194+50.00			618	45			771	
448	195+00.00	196+00.00			610	64			799	
449	196+50.00	197+50.00			626	45			802	
450	198+00.00	199+00.00			647	31			829	
451	199+50.00	200+50.00			643	34			848	
452	201+00.00	202+00.00			850	5			898	
453	202+50.00	203+50.00			855	7			1093	
454	204+00.00	205+00.00			835	13			1042	
455	205+50.00	206+50.00			781	17			939	
456	207+00.00	208+00.00			688	35			1035	
457	208+50.00	209+50.00			666	39			1014	
458	210+00.00	211+00.00			618	57			1211	
459	211+50.00	212+50.00			655	26			1182	
460	213+00.00	214+00.00			425	53			1238	
461	214+50.00	215+50.00			236	160			1128	
462	216+00.00	217+00.00			271	169			1096	
463	217+50.00	218+50.00			237	188			1013	
464	219+00.00	220+00.00			222	201			1027	
465	220+50.00	221+50.00			205	196			952	
466	222+00.00	223+00.00			239	166			795	
467	223+50.00	224+50.00			226	153			377	
468	225+00.00	226+00.00			144	209			675	
469	226+50.00	227+50.00			69	326			810	
470	228+00.00	229+00.00			76	482			703	
471	230+00.00	231+00.00			7	214			132	
472	231+50.00	232+50.00			162	285			663	
473	233+00.00	234+00.00			187	205			405	
474	234+50.00	235+50.00			177	204			389	
475	236+00.00	237+00.00			402	306			537	
476	237+50.00	238+50.00			246	368			440	
477	239+00.00	240+00.00			162	288			289	
478	240+50.00	241+50.00			167	208			298	
479	242+00.00	243+00.00			169	208			211	
480	243+50.00	244+50.00			185	298			276	
481	245+00.00	246+00.00			289	315			800	
482	246+50.00	247+50.00			282	191			457	
483	248+00.00	249+00.00			221	190			313	
484	249+50.00	250+50.00			216	201			358	
485	251+00.00	252+00.00			233	188			376	
486	252+50.00	253+50.00			292	227			876	
487	254+00.00	255+00.00			720	199			1066	
488	255+44.65	256+00.00			397	192			809	
489	256+50.00	257+50.00			327	382			630	
490	258+00.00	259+00.00			448	207			550	
491	259+50.00	260+50.00			326	196			209	
492	261+00.00	262+00.00			285	199			191	
TOTALS CARRIED TO SHEET 3 OF 3					23961	8212			38675	

REF NO.	SHEET NO.	STATION TO STATION				EXCAVATION CY	EMBANKMENT CY	GRANULAR MATERIAL, TYPE B CY	GRANULAR MATERIAL, TYPE D CY	SEEDING AND MULCHING SY
		IR 475	TO							
493	262+50.00	263+50.00			272	193			195	
494	264+00.00	265+00.00			255	188			177	
495	265+50.00	266+50.00			220	182			197	
496	267+00.00	268+00.00			131	123			245	
498	26+43.23	27+50.00			49	35			423	
499	28+00.00	29+00.00			208	253			111	
500	29+50.00	30+50.00			435	973			1781	
501	31+00.00	32+00.00			555	1331			2127	
502	32+50.00	34+00.00			448	1909			2721	
503	34+50.00	35+54.90			264	667			1364	
504	36+00.00	37+50.00			516	1375			1981	
505	37+54.90	39+00.00			1008	1582	26	75	1382	
506	39+50.00	41+00.00			3564	7654	168	499	3040	
507	41+50.00	42+50.00			2729	5957	120	348	1708	
508	43+00.00	44+00.59			2948	6051	119	344	1517	
509	44+50.00	46+00.00			3994	9849	175	517	2094	
510	46+50.00	48+00.00			4421	11549	164	479	1994	
511	48+50.00	48+78.16			1802	4735	64	188	783	
512	39+00.00	40+00.00			12	3			14	
513	40+50.00	41+00.00			33	15			60	
514	41+50.00	42+50.00			9	147			125	
515	42+76.65	43+50.00				212			108	
516	44+00.00	45+00.00				379			183	
517	45+50.00	46+50.00				363			208	
518	47+00.00	48+00.00				329			231	
519	48+50.00					102			81	
628	1100+00.00	1101+50.00			444	491			270	
629	1102+00.00	1103+58.00			750	864			446	
630	1104+00.00	1106+00.00			594	659			361	
631	1106+50.00	1110+00.00			431	506			315	
632	1110+30.00	1111+40.07			461	513			301	
633	2100+00.00	2102+50.00			511	531			359	
634	2103+00.00	2105+50.00			71	13			116	
635	2106+00.00	2109+50.00			88	9			86	
636	2110+30.00	2111+00.00			35	4			36	
					8	0			20	
633	2100+00.00	2102+50.00			507	609			399	
634	2103+00.00	2105+50.00			468	529			353	
635	2106+00.00	2109+50.00			543	671			403	
636	2110+30.00	2111+00.00			812	855			497	
					263	246			220	
					94	4			68	
					56	5			56	
					3	0			7	
TOTALS CARRIED TO SHEET 3 OF 3					30012	62665	836	2450	29163	

CALCULATED JRB
 CHECKED XXX
EARTHWORK SUBSUMMARY SHEET 2 OF 3
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REF NO.	SHEET NO.	STATION TO STATION		203	203	203	203	659
				EXCAVATION	EMBANKMENT	GRANULAR MATERIAL, TYPE B	GRANULAR MATERIAL, TYPE D	SEEDING AND MULCHING
				CY	CY	CY	CY	SY
NOISE WALL 4A								
641	4101+00.00		4103+50.00	7	0			30
				27	0			66
642	4104+00.00		4105+81.00	20	5			43
				24	2			70
643	4106+00.00		4107+25.00	4	1			36
				0	0			0
NOISE WALL 5A								
644	5100+00.00		5103+50.00	25	0			65
				44	0			115
645	5104+00.00		5107+50.00	52	0			219
				57	0			266
646	5108+00.00		5111+50.00	56	0			355
				53	0			288
647	5112+00.00		5115+50.00	46	0			233
				47	0			146
648	5116+00.00		5118+50.00	46	0			128
				0	0			0
NOISE WALL 5B								
649	5200+50.00		5204+00.00	24	9			96
				44	8			75
650	5204+50.00		5208+00.00	39	13			90
				177	144			113
651	5208+50.00		5212+00.00	281	275			258
				896	1133			507
NOISE WALL 5C								
652	5301+00.00		5304+07.00	34	5			139
				24	0			47
653	5304+50.00		5307+91.00	26	0			59
				29	4			108
654	5308+00.00		5310+50.00	7	0			21
				29	0			109
655	5311+00.00		5312+25.00	15	2			62
				0	0			0
NOISE WALL 6A								
656	6101+00.00		6104+50.00	50	0			190
				80	0			303
657	6105+00.00		6108+50.00	34	46			322
				55	15			141
658	6109+00.00		6109+50.00	14	2			38
				0	0			0
NOISE WALL 6B								
659	6201+00.00		6203+84.00	18	7			64
				9	15			99
660	6204+00.00		6206+00.00	28	4			66
				17	0			48
661	6206+50.00		6209+00.00	27	0			40
				4	0			0
TOTALS CARRIED TO SHEET 3 OF 3				2469	1690			5055

REF NO.	SHEET NO.	STATION TO STATION		203	203	203	203	659
				EXCAVATION	EMBANKMENT	GRANULAR MATERIAL, TYPE B	GRANULAR MATERIAL, TYPE D	SEEDING AND MULCHING
				CY	CY	CY	CY	SY
NOISE WALL 6C								
662	6300+00.00	TO	6303+50.00	22	1			10
				75	8			67
663	6304+00.00		6307+15.00	558	641			424
				707	869			515
664	6307+50.00		6308+99.31	273	308			267
				22	23			30
NOISE WALL 6D								
665	6400+00.00		6402+50.00	49	41			38
				63	3			43
666	6403+00.00		6405+00.00	20	7			45
				28	13			78
NOISE WALL 6E								
667	6500+00.00		6503+00.00	31	0			43
				57	0			188
668	6503+50.00		6505+00.00	29	0			204
				0	0			0
EARTHWORK TOTALS THIS SHEET				1934	1914			1952
EARTHWORK TOTALS FROM 1 OF 3				76888	117884	1078	3154	73183
EARTHWORK TOTALS FROM 1 OF 3				33318	3889			37198
EARTHWORK TOTALS FROM 2 OF 3				23961	8212			38675
EARTHWORK TOTALS FROM 2 OF 3				30012	62665	836	2450	29163
EARTHWORK TOTALS FROM 3 OF 3				2469	1690			5055
TOTALS CARRIED TO GENERAL SUMMARY				168582	196254	1914	5604	185226

CALCULATED JRB
 CHECKED XXX
EARTHWORK SUBSUMMARY SHEET 3 OF 3
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