

MICROFILMED
JUL 08 1979
REPRODUCTION

CURVE DATA- SR 11

$\Delta = 41^\circ - 45' - 18''$ RT, $L_c = 3,340.4'$
 $D_c = 1^\circ - 15' - 00''$ T = 1,748.27'
 $R = 4,583.66'$ E = 322.09'

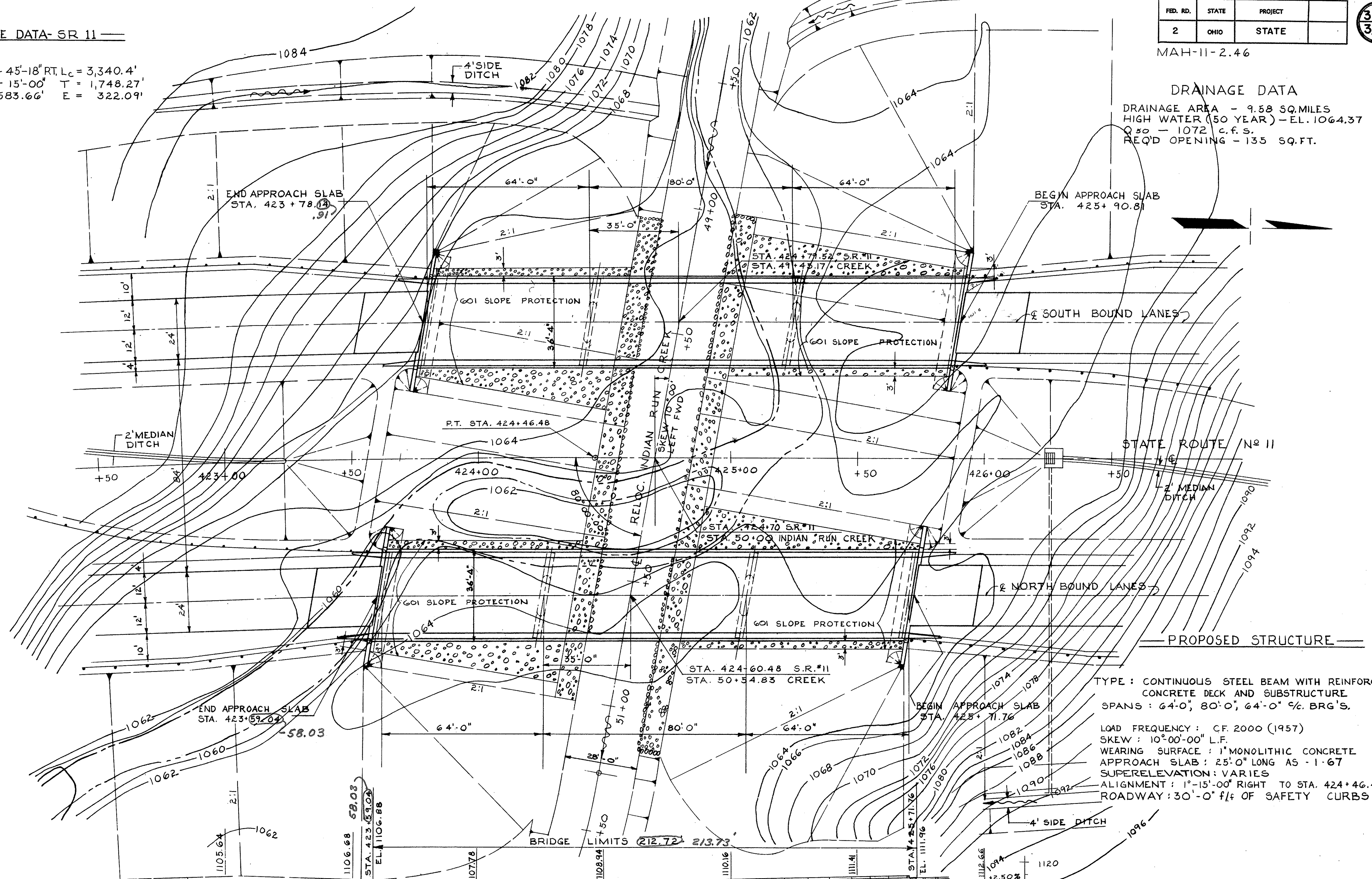
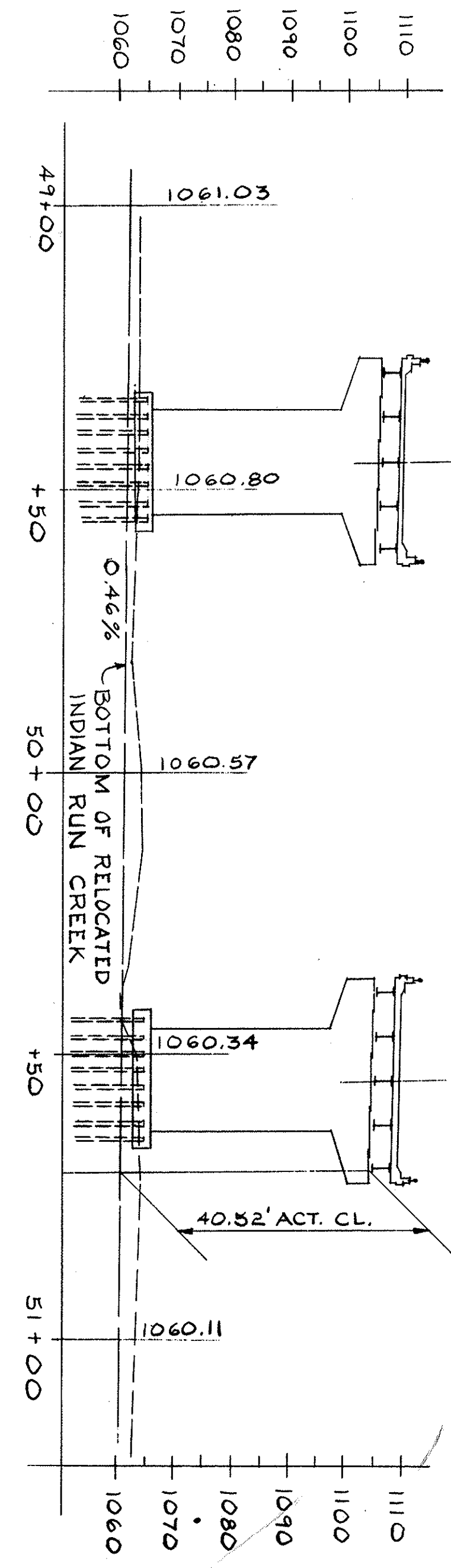
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| FED. RD. | STATE | PROJECT |
| 2 | OHIO | STATE |

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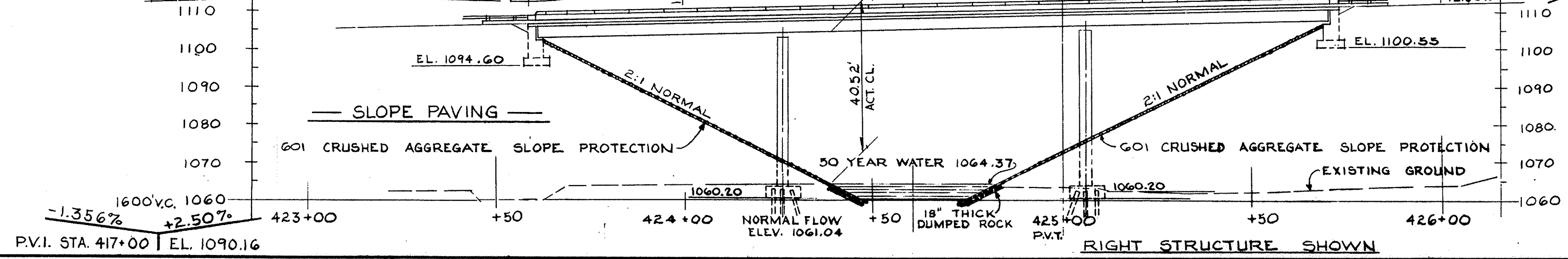
DRAINAGE DATA

DRAINAGE AREA - 9.58 SQ. MILES
HIGH WATER (50 YEAR) - EL. 1064.37
 $Q_{50} = 1072$ c.f.s.
REQ'D OPENING - 135 SQ. FT.

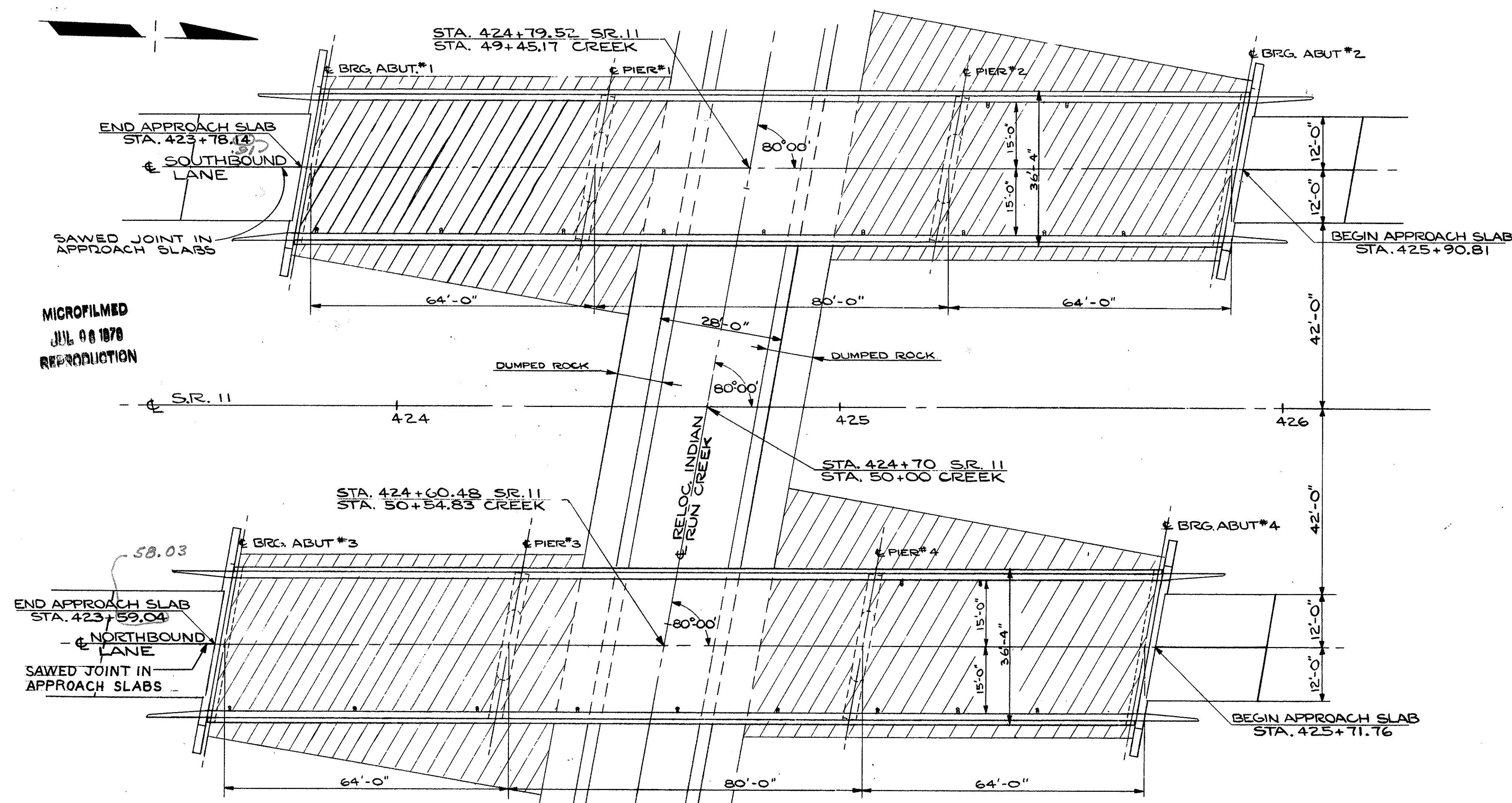


TYPE: CONTINUOUS STEEL BEAM WITH REINFORCED CONCRETE DECK AND SUBSTRUCTURE
SPANS: 64'-0", 80'-0", 64'-0" % BRG'S.
LOAD FREQUENCY: CF. 2000 (1957)
SKEW: 10°-00'-00" L.F.
WEARING SURFACE: 1" MONOLITHIC CONCRETE
APPROACH SLAB: 25'-0" LONG AS -1.67
SUPERELEVATION: VARIES
ALIGNMENT: 1°-15'-00" RIGHT TO STA. 424+46.48
ROADWAY: 30'-0" f/f OF SAFETY CURBS

— PILES —
12" DIA. CAST-IN-PLACE REIN. CONCRETE
AVERAGE ESTIMATED PAY-LENGTH
PIERS — 29'-0"



| | | | |
|--|---------|------------------|---------|
| STATE OF OHIO DEPARTMENT OF HIGHWAYS BUREAU OF BRIDGES | | | |
| AKRON, OHIO | | YOUNGSTOWN, OHIO | |
| GLAUS, PYLE & SCHOMER | | | |
| SITE PLAN | | | |
| BRIDGE N° MAH-II-0802 L & R | | | |
| OVER RELOCATED INDIAN RUN CREEK | | | |
| MAHONING CO. S.R. N°11 | | | |
| STA. 424+70 | | | |
| DESIGNED | DRAWN | TRACED | CHECKED |
| JTM | N.S. | L.E.V. | D.S. |
| REVIEWED | DATE | REVISED | |
| W.K.O. | 7-21-64 | 8-5-68 | |



MICROFILMED
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ESTIMATED QUANTITIES - TWO BRIDGES

| ITEM | TOTAL | UNIT | DESCRIPTION | SUPER STRUCT. | ABUT'S | PIERS | GEN. |
|------|---------|---------|---|-----------------|--------|----------------------|-------|
| 503 | 1,036 | CU.YDS. | UNCLASSIFIED EXCAVATION | | 362 | 674 | |
| 503 | LUMP | SUM | COFFERDAMS, CRIBS, AND SHEETING | | | | LUMP |
| 511 | 503 | CU.YDS. | CLASS "C" CONCRETE, SUPERSTRUCTURE | 503 | | | |
| 511 | 174 | CU.YDS. | CLASS "E" CONCRETE, ABUTMENT ABOVE FOOTINGS | | 174 | | |
| 511 | 322 | CU.YDS. | CLASS "C" CONCRETE, PIERS ABOVE FOOTINGS | | | 322 | |
| 511 | 222 | CU.YDS. | CLASS "E" CONCRETE, ABUTMENT & PIER FOOTINGS | | 73 | 149 | |
| 509 | 189,104 | LBS. | REINFORCING STEEL | 134,764 | 12,418 | 41,922 | |
| 513 | 448,080 | LBS. | STRUCTURAL STEEL | 448,080 | | | |
| 832 | 448,080 | LBS. | FIELD PAINTING OF STRUCTURAL STEEL | 448,080 | | | |
| 517 | 851.91 | LN. FT. | RAILING TYPE I | 851.91 | | | |
| 505 | LUMP | SUM | FIRST TEST PILE | | | | LUMP |
| 507 | 2436 | LN. FT. | 12" DIA. CAST-IN-PLACE REIN. CONCRETE PILES | | | 2436 | |
| 518 | 22 | EACH | SCUPPERS INCLUDING SUPPORTS | 22 | | | |
| 518 | 60 | CU.YDS. | POROUS BACKFILL | | 60 | | |
| 518 | 126 | LN. FT. | 6" NON-PERFORATED CORR. METAL PIPE | | 126 | | |
| 518 | 128 | LN. FT. | 6" PERFORATED CORR'D METAL PIPE, INCL. SPECIALS | | 128 | | |
| 601 | 1,941 | SQ.YDS. | CRUSHED AGGREGATE SLOPE PROTECTION | | | | 1,941 |
| 808 | 453 | UNITS | WATER-REDUCING, SET RETARDING ADMIXTURE | 453 | | | |
| 825 | 1845 | SQ.YDS. | CONCRETE SURFACE TREATMENT | 1815 | 30 | | |
| 828 | 136 | LN. FT. | JOINT SEALER | | 136 | | |
| | | | | BY DM: 11-29-67 | | CHECKED JWC 11-30-67 | |

PLAN

GENERAL NOTES

DESIGN SPECIFICATIONS: THIS STRUCTURE CONFORMS TO THE REQUIREMENTS OF "DESIGN SPECIFICATIONS FOR HIGHWAY STRUCTURES" OF THE STATE OF OHIO DEPARTMENT OF HIGHWAYS, DATED 9-1-57, TOGETHER WITH CURRENT REVISIONS THEREOF.

DESIGN DATA:

DESIGN LOADING - CF 2000

CONCRETE CLASS C - BASIC UNIT STRESS 1,333 P.S.I.
CONCRETE CLASS E - BASIC UNIT STRESS 1,133 P.S.I.

STRUCTURAL STEEL - ASTM A36 - BASIC UNIT STRESS 20,000 P.S.I.
REINFORCING STEEL - ASTM A15, A16, A140, DEFORMED, INTERMEDIATE OR HARD GRADE. BASIC UNIT STRESS OF 20,000 P.S.I. EXCEPT SPIRAL REINFORCEMENT MAY BE PLAIN, STRUCTURAL GRADE WITH BASIC UNIT STRESS OF 18,000 P.S.I.

REFERENCE SHALL BE MADE TO STANDARD DRAWINGS:

| | |
|-----------|------------------|
| AS-1-67 | REVISED: 1-11-68 |
| BR-1-65 | REVISED 11-24-65 |
| *FSB-1-62 | REVISED 1-15-63 |
| SD-1-65 | DATED 11-8-65 |

AND TO SUPPLEMENTAL SPECIFICATIONS:

| | | | |
|-----|----------------|-----|---------------|
| 808 | DATED 1-13-67 | 832 | DATED 5-25-67 |
| 825 | DATED 12-19-67 | 931 | DATED 5-25-67 |
| 828 | DATED 1-1-67 | 811 | DATED 1-1-67 |

EMBANKMENT PROCEDURE: THE EMBANKMENT SHALL BE PLACED AND COMPACTED UP TO THE FINISHED SPILL-THRU SLOPE AND TO THE LEVEL OF THE SUBGRADE FOR A DISTANCE OF 200 FEET BACK OF THE ABUTMENTS. NO WORK WILL BE PERMITTED ON THE PIERS UNTIL THE EMBANKMENT IS COMPLETED. AFTER A WAITING PERIOD OF 60 DAYS THE EXCAVATION SHALL BE MADE FOR THE ABUTMENTS.

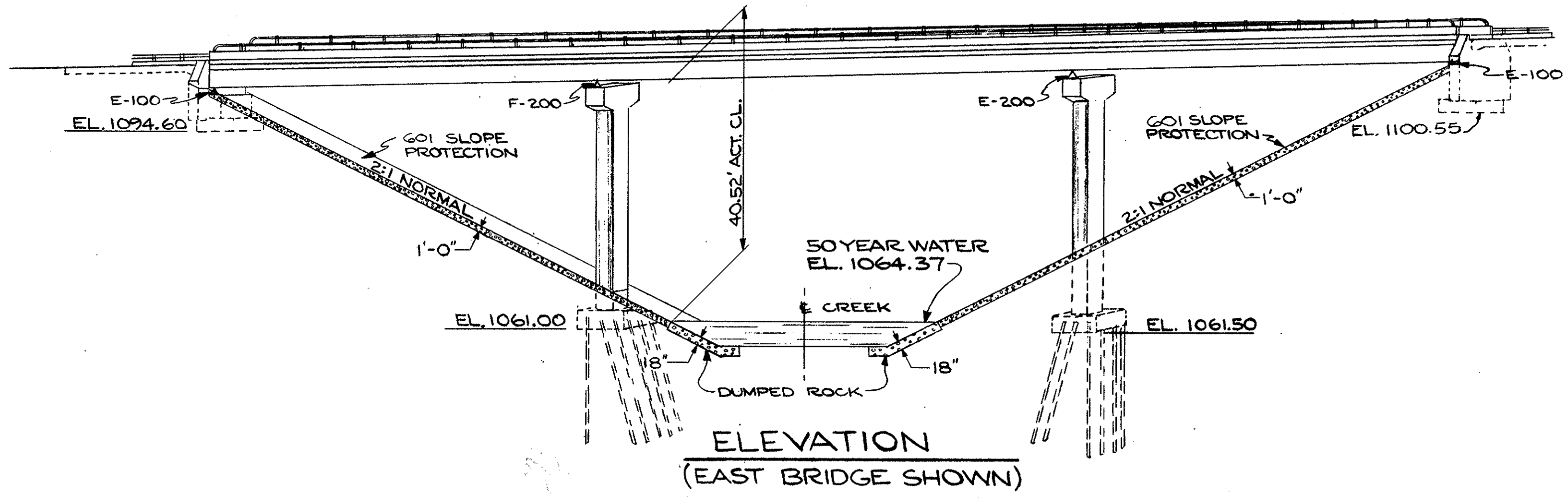
EXCAVATION QUANTITY INCLUDES THE REMOVAL OF FILL MATERIAL REQUIRED FOR CONSTRUCTION OF THE ABUTMENTS AND PIERS.

ABUTMENT FOUNDATIONS ARE DESIGNED FOR A MINIMUM BEARING PRESSURE OF ONE TON PER SQ. FT. PIER PILES SHALL BE DRIVEN TO A MINIMUM BEARING CAPACITY OF 45 TONS PER PILE.

THE CRUSHED AGGREGATE SLOPE PROTECTION SHALL EXTEND FROM THE FACE OF THE ABUTMENT DOWN TO THE TOE OF THE SLOPE AND EXTEND IN WIDTH TO 3 FEET BEYOND THE OUTER EDGE OF THE SUPERSTRUCTURE ON ONE SIDE, AND AT THE ACUTE CORNERS OF THE OTHER SIDE, THE OUTSIDE EDGE OF THE SLOPE PROTECTION SHALL INTERSECT THE PROJECTED FACE OF THE ABUTMENT 3 FEET BEYOND THE OUTER EDGE OF THE SUPERSTRUCTURE AND SHALL EXTEND DOWN THE SLOPE, NORMAL TO THE FACE OF THE ABUTMENT, TO THE TOE OF THE SLOPE.

MACHINE FINISH: THE CONCRETE BRIDGE DECK SHALL BE FINISHED BY THE USE OF A FINISHING MACHINE.

SEE SHEET 276 FOR "PAINTING OF STRUCTURAL STEEL," "SECONDARY WELD," "WELDED ATTACHMENTS" AND "STANDARD DRAWING," FSB-1-62, BEARING NOTES.



ELEVATION
(EAST BRIDGE SHOWN)

STATE OF OHIO
DEPARTMENT OF HIGHWAYS
BUREAU OF BRIDGES

AKRON, OHIO YOUNGSTOWN, OHIO

GLAUS, PYLE & SCHOMER

GEN. PLAN & ELEV.
BRIDGE N^o MAH-11-0802 L & R
OVER RELOCATED INDIAN RUN CREEK
MAHONING CO. S.R. N^o 11
STA. 424+70

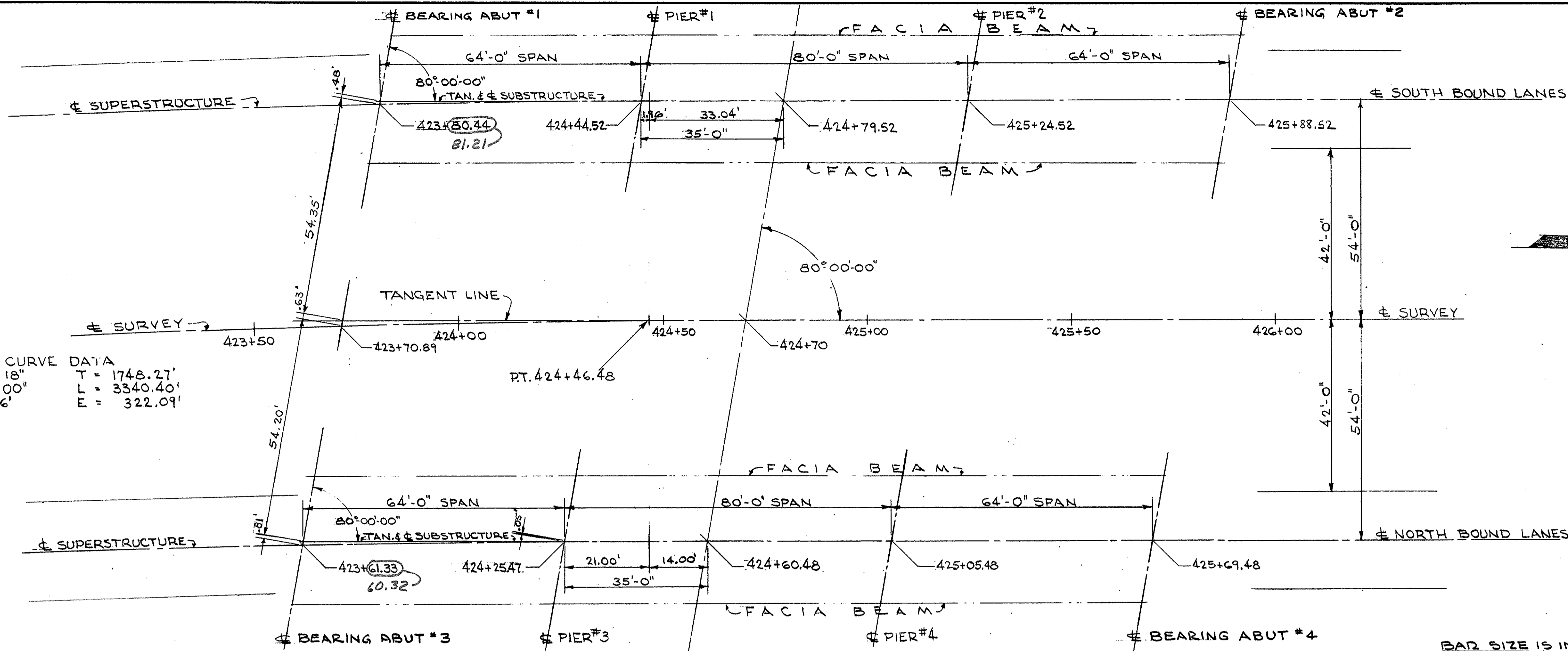
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| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| DJ | RGN | | DJ | WKO | 4-14-67 | 8-3-68 |

MICROFILMED
JUL 06 1979
REPRODUCTION

| | | |
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| FED. RD. | STATE | PROJECT |
| 2 | OHIO | STATE |

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GEOMETRIC PLAN

NOTES:
 BAR SIZE IS INDICATED IN THE BAR MARK. THE FIRST DIGIT WHERE THREE DIGITS ARE USED, AND THE FIRST TWO WHERE FOUR ARE USED, INDICATE THE BAR SIZE NUMBER. FOR EXAMPLE, A 700 IS A #7 SIZE BAR AND A 1014 IS A #10 SIZE.

REINFORCING BAR SCHEDULE

| MARK NUMBER | LENGTH | LBS. | SHAPE |
|-----------------------------------|--------|---------|---------|
| ABUTMENTS #1, 2, 3 & 4 | | | |
| A 501 | 4 | 8'-6" | 35 ST |
| A 502 | 58 | 8'-6" | 514 BT |
| A 503 | 24 | 33'-4" | 834 ST |
| A 504 | 44 | 26'-2" | 1201 ST |
| A 505 | 20 | 6'-7" | 617 BT |
| A 506 | 6 | 8'-10" | 55 ST |
| A 507 | 6 | 8'-4" | 52 ST |
| A 508 | 16 | 24'-0" | 401 ST |
| A 509 | 8 | 21'-10" | 182 ST |
| A 510 | 4 | 13'-11" | 58 BT |
| A 511 | 28 | 9'-9" | 284 ST |
| A 512 | 8 | 8'-10" | 74 ST |
| A 513 | 8 | 8'-1" | 68 ST |
| A 514 | 10 | 7'-2" | 75 ST |
| A 515 | 22 | 6'-4" | 147 ST |
| A 516 | 16 | 6'-5" | 107 BT |
| A 517 | 4 | 9'-2" | 38 ST |
| A 518 | 12 | 11'-0" | 138 BT |
| A 519 | 8 | 8'-10" | 74 BT |
| A 520 | 10 | 9'-2" | 96 BT |
| A 521 | 10 | 9'-6" | 99 BT |
| A 522 | 8 | 9'-6" | 79 ST |
| A 523 | 8 | 27'-5" | 229 ST |
| A 524 | 32 | 9'-8" | 323 ST |
| A 525 | 6 | 9'-10" | 62 BT |
| A 526 | 8 | 8'-9" | 73 ST |
| A 527 | 8 | 7'-11" | 66 ST |
| A 528 | 2 | 10'-6" | 22 ST |
| A 529 | 10 | 36'-10" | 384 ST |
| A 530 | 4 | 13'-1" | 55 BT |

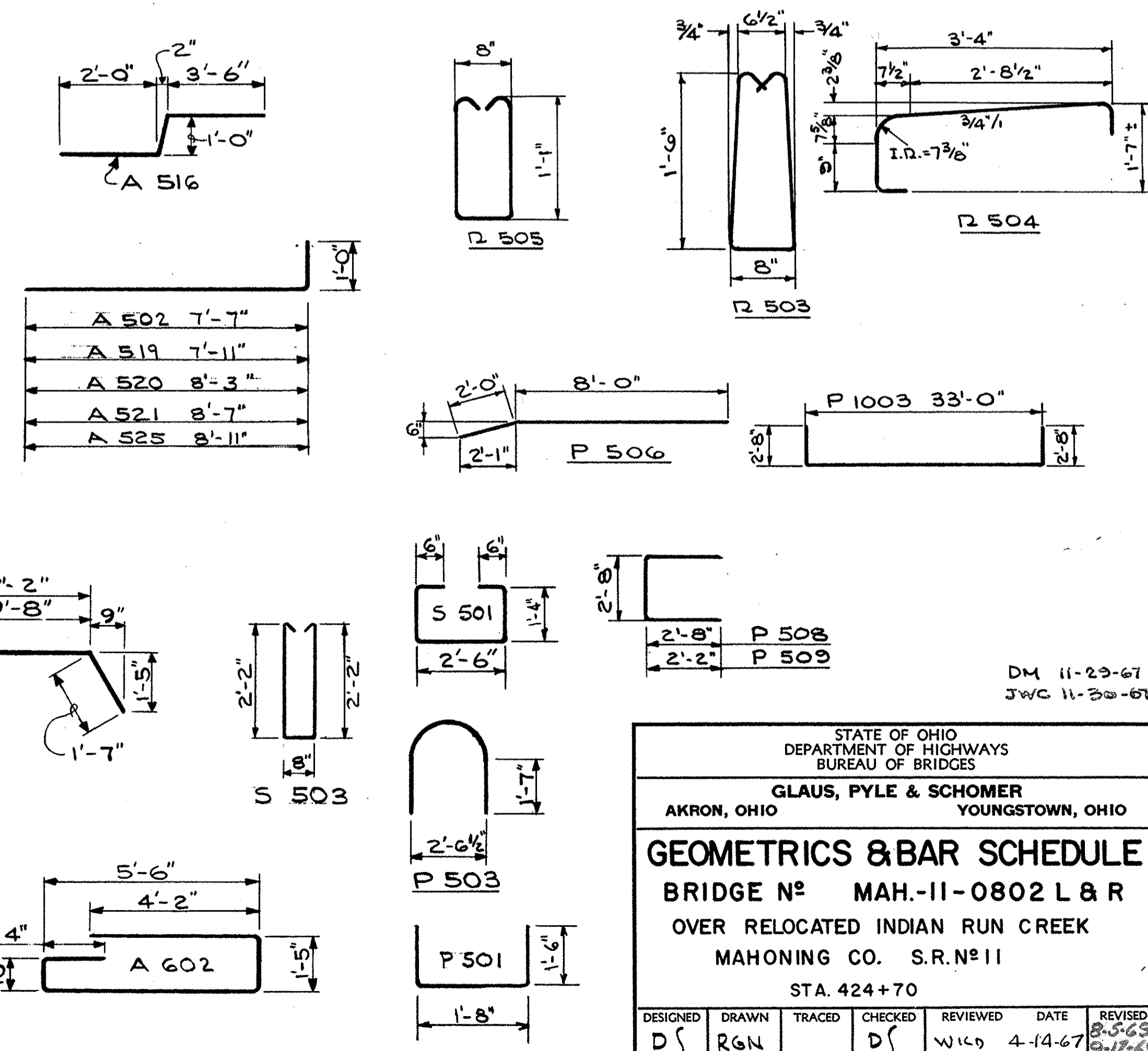
| MARK NUMBER | LENGTH | LBS. | SHAPE |
|-----------------------------------|--------|---------|---------|
| ABUTMENTS #1, 2, 3 & 4 | | | |
| A 531 | 8 | 7'-0" | 58 ST |
| A 532 | 8 | 15'-10" | 132 ST |
| A 533 | 64 | 2'-11" | 195 ST |
| A 601 | 62 | 12'-11" | 1203 BT |
| A 602 | 96 | 12'-10" | 1850 BT |
| A 603 | 26 | 12'-3" | 478 BT |
| A 604 | 8 | 15'-8" | 188 BT |
| A 605 | 10 | 16'-0" | 240 BT |
| A 606 | 10 | 16'-4" | 245 BT |
| A 607 | 12 | 13'-11" | 251 BT |
| A 608 | 8 | 18'-8" | 200 BT |
| A 609 | 16 | 18'-5" | 451 ST |
| A 610 | 16 | 20'-2" | 485 ST |
| ABUTMENT TOTAL | | | 12,418 |

| REINFORCEMENT | LENGTH | LBS. | SHAPE |
|---------------|--------|-------|-------|
| RE 501 | 2 | 6'-6" | ST |
| RE 601 | 5 | 7'-0" | ST |
| RE 701 | 3 | 7'-3" | ST |
| RE 801 | 1 | 7'-6" | ST |
| RE 1001 | 1 | 8'-2" | ST |

| MARK NUMBER | LENGTH | LBS. | SHAPE |
|--|--------|---------|---------|
| RAILING - THESE REINFORCING BARS WILL BE INCLUDED AS PART OF ITEM 517 FOR PAYMENT | | | |
| R 501 | 32 | 10'-0" | 334 ST |
| R 502 | 176 | 16'-10" | 3089 ST |
| R 503 | 24 | 4'-2" | 104 BT |
| R 504 | 16 | 5'-4" | 80 BT |
| R 505 | 16 | 3'-5" | 57 BT |
| R 506 | 16 | 3'-0" | 50 ST |

| MARK NUMBER | LENGTH | LBS. | SHAPE |
|-------------------------------|--------|-----------------|----------------|
| PIERS #1, 2, 3 & 4 | | | |
| P 501 | 384 | 4'-6" | 1802 BT |
| P 502 | 140 | 15'-0" | 2190 ST |
| P 503 | 140 | 6'-1" | 888 BT |
| P 504 | 8 | 18'-0" | 150 ST |
| P 505 | 8 | 33'-0" | 275 ST |
| P 506 | 16 | 10'-0" | 167 BT |
| P 507 | 40 | 12'-8" | 529 BT |
| P 508 | 48 | 8'-0" | 400 BT |
| P 509 | 80 | 6'-8" | 556 BT |
| P 701 | 24 | 19'-2" (15'-8") | 910 (729) ST |
| P 702 | 24 | 21'-6" (20'-8") | 1055 (1018) ST |
| P 703 | 24 | 18'-6" (17'-7") | 908 (862) ST |
| P 704 | 24 | 21'-2" (20'-8") | 1038 (965) ST |
| P 801 | 148 | 13'-8" | 5,402 BT |
| P 802 | 40 | 25'-8" | 2,742 BT |
| P 803 | 152 | 12'-10" | 5,207 ST |
| P 1001 | 152 | 7'-3" | 4,742 BT |
| P 1002 | 152 | 12'-0" | 7,413 ST |
| P 1003 | 8 | 37'-11" | 1,305 BT |
| P 1004 | 32 | 33'-0" | 4,544 ST |
| PIER TOTAL | | | 41,922 |
| | | | 92,253 |

| MARK NUMBER | LENGTH | LBS. | SHAPE |
|-----------------------------|--------|--------|-----------|
| SUPERSTRUCTURE | | | |
| S 501 | 560 | 5'-10" | 3,404 BT |
| S 502 | 560 | 3'-6" | 2,044 BT |
| S 503 | 584 | 5'-7" | 3,399 BT |
| S 701 | 628 | 35'-8" | 45,787 ST |
| S 601 | 628 | 35'-8" | 33,646 ST |
| S 602 | 866 | 30'-0" | 39,743 ST |
| S 603 | 92 | 32'-0" | 4,422 ST |
| S 604 | 126 | 12'-3" | 2,318 ST |
| SUPERSTRUCTURE TOTAL | | | 134,764 |
| TOTAL ABUTMENT | | | 12,418 |
| TOTAL SUPERSTRUCTURE | | | 134,764 |
| TOTAL PIERS | | | 41,922 |
| GRAND TOTAL | | | 189,935 |



STATE OF OHIO
DEPARTMENT OF HIGHWAYS
BUREAU OF BRIDGES

AKRON, OHIO YOUNGSTOWN, OHIO

GLAUS, PYLE & SCHOMER

GEOMETRICS & BAR SCHEDULE
 BRIDGE NO. MAH-11-0802 L & R
 OVER RELOCATED INDIAN RUN CREEK
 MAHONING CO. S.R. NO. 11
 STA. 424+70

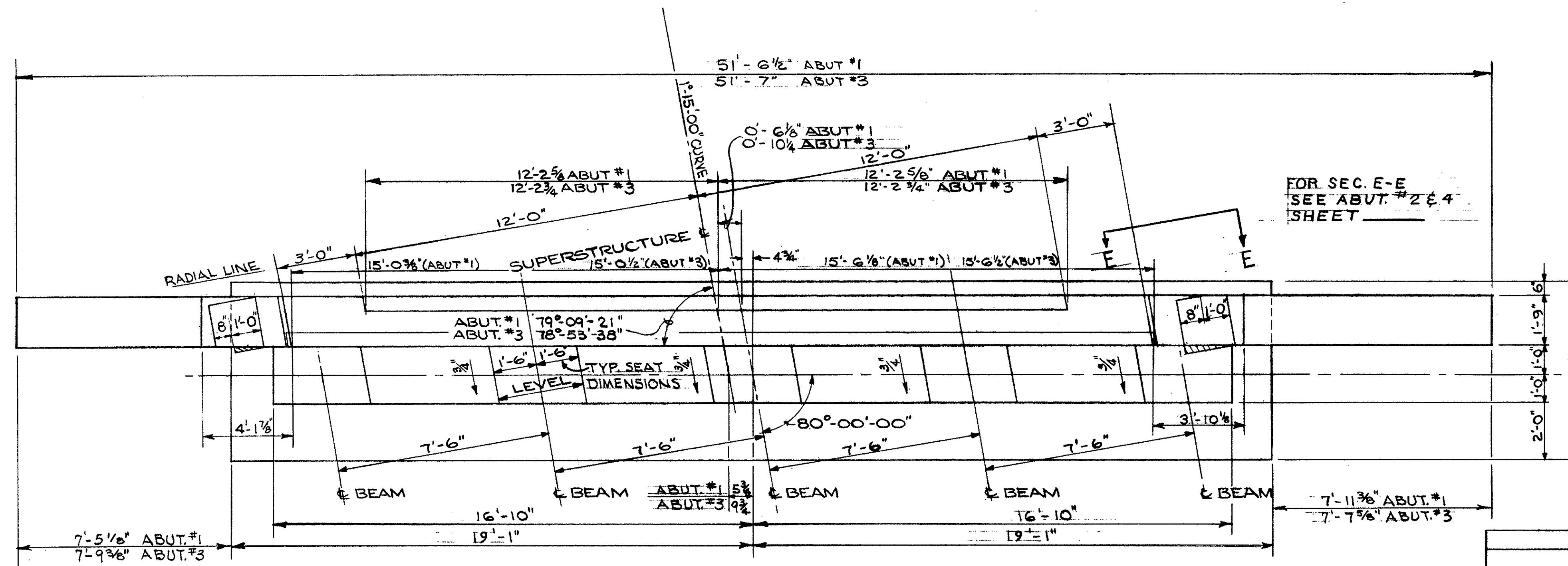
DESIGNED: DJS DRAWN: RGN TRACED: CHECKED: DJS REVIEWED: WLC DATE: 4-14-67 REVISED: 8-5-69 9-17-69

MICROFILMED
JUL. 06 1979
REPRODUCTION

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| FED. RD. | STATE | PROJECT |
| 2 | OHIO | STATE |

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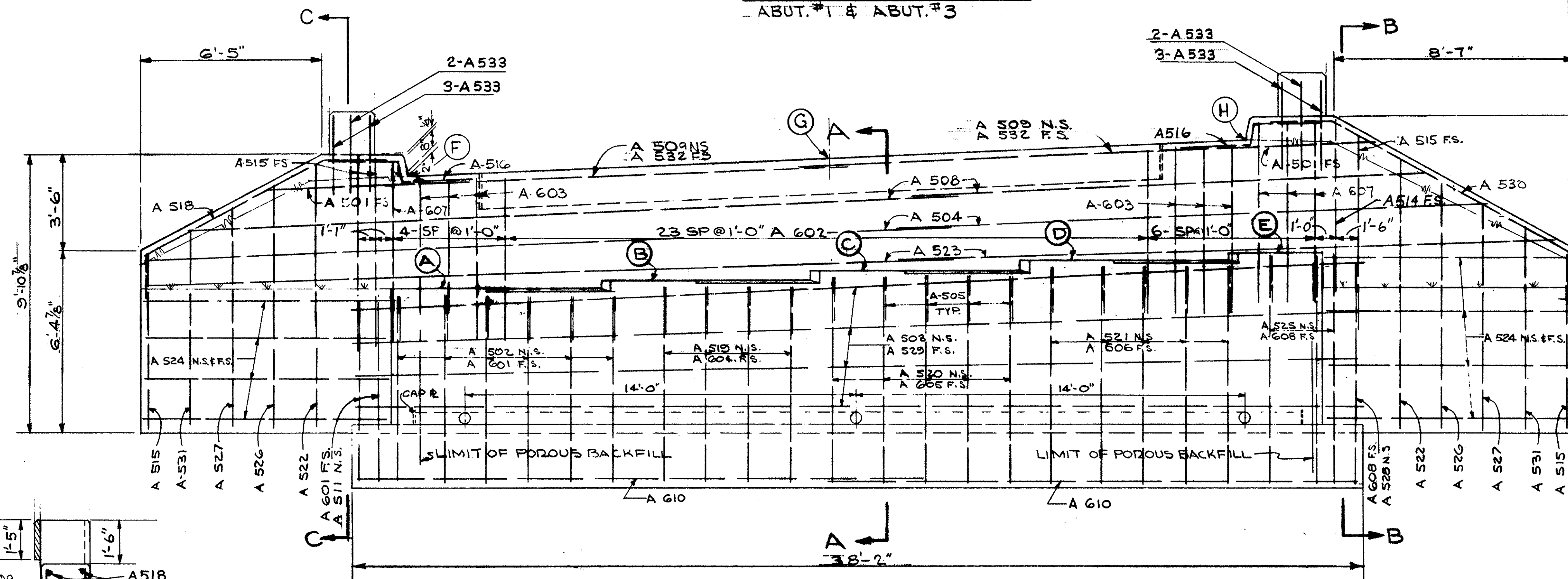
FOR SEC. E-E
SEE ABUT. #2 & 4
SHEET

NOTES:
SEE ABUT. #2 & 4 NOTES.

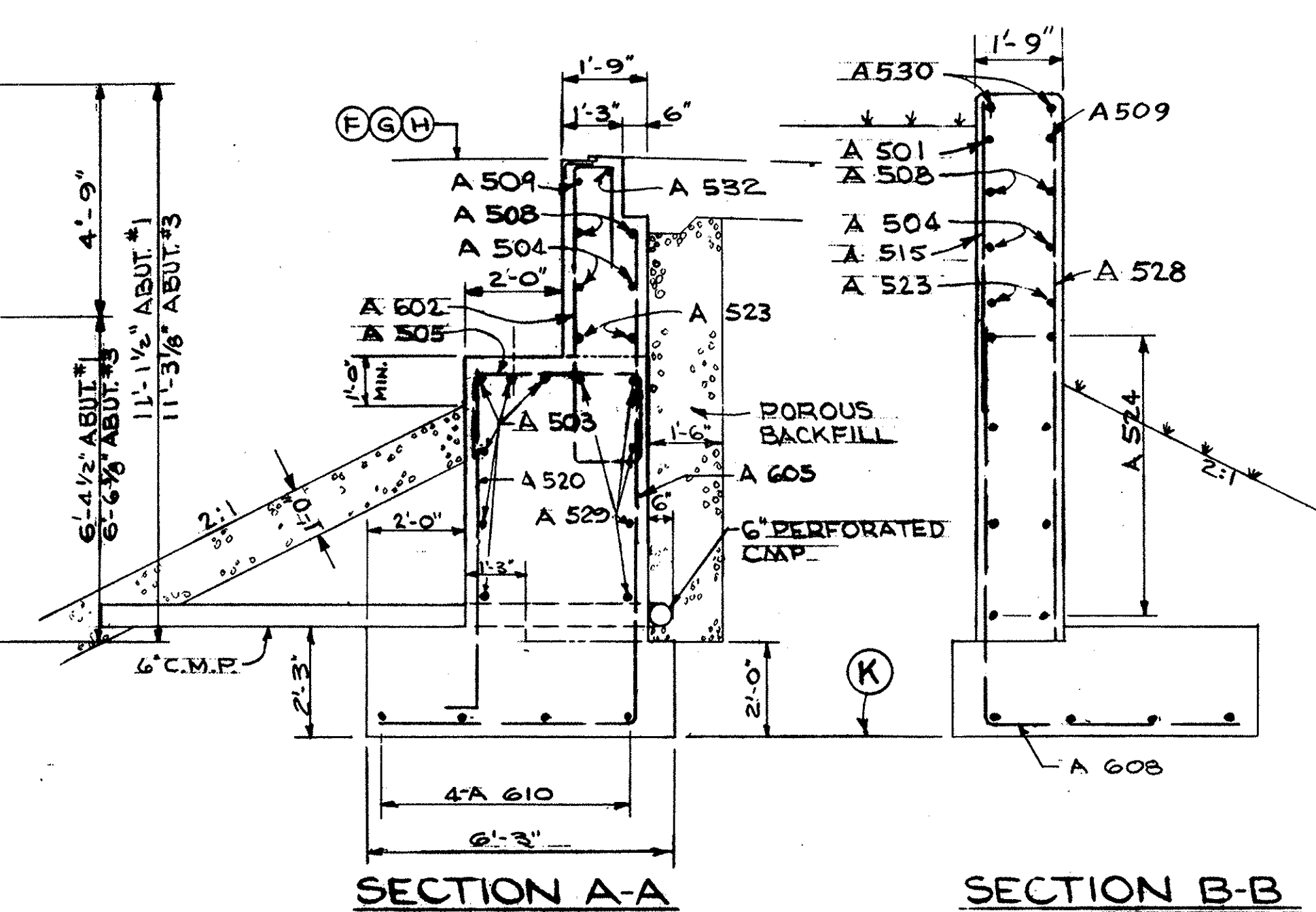
PLAN VIEW
ABUT. #1 & ABUT. #3

1104.01

| | A.07 | B.39 | C.70 | D | E.31 | F.06 | G | H | K |
|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|
| ABUT #1 | 1103.05 | 1103.38 | 1103.67 | 1103.97 | 1104.27 | 1107.08 | 1107.70 | 1108.30 | 1096.02 |
| ABUT #3 | 1101.63 | 1101.87 | 1102.81 | 1102.64 | 1102.98 | 1105.66 | 1106.84 | 1107.01 | 1094.60 |
| | .66 | .99 | .32 | .66 | .96 | .62 | .29 | 1106.96 | |

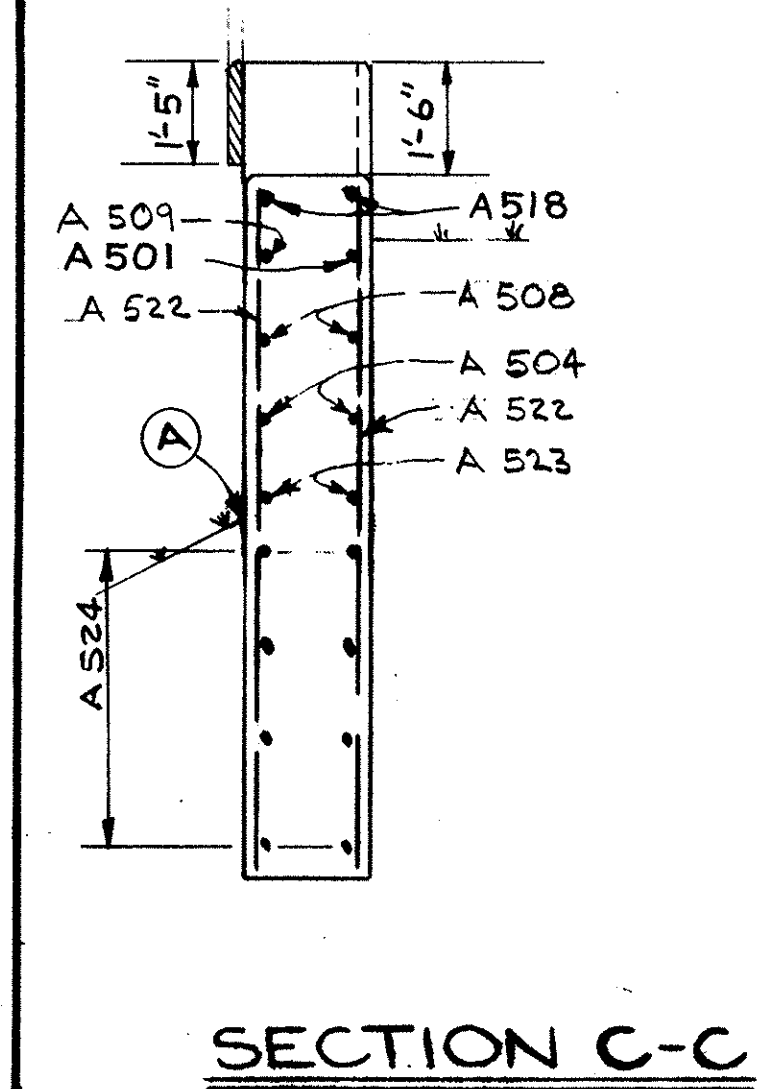


ELEVATION
ABUT. #1 & ABUT. #3



SECTION A-A

SECTION B-B



SECTION C-C

STATE OF OHIO
DEPARTMENT OF HIGHWAYS
BUREAU OF BRIDGES

AKRON, OHIO

GLAUS, PYLE & SCHOMER
YOUNGSTOWN, OHIO

ABUTMENT DETAILS
BRIDGE N^o MAH-11-0802 L & R
OVER RELOCATED INDIAN RUN CREEK
MAHONING CO. S.R.N^o 11
STA. 424+70

| | | | | | | |
|----------|-------|--------|---------|----------|---------|---------|
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| DS | RGN | | DS | WKB | 7-21-64 | 8-5-69 |

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SEE SHEET 313
FOR APPROACH GUARD RAIL
AND CURB DETAILS

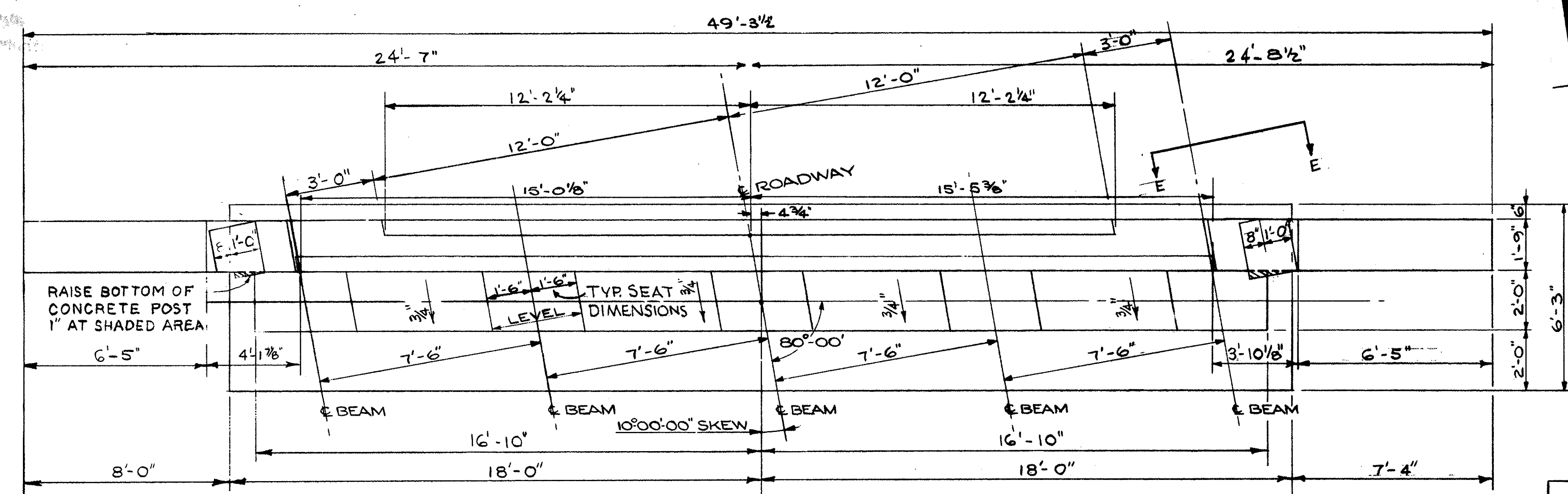
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ALL REINFORCING STEEL SHALL HAVE A 3" COVER IN BOTTOM OF FOOTING.
ALL OTHER REINFORCING STEEL SHALL HAVE A 2" MINIMUM COVER EXCEPT
WHERE OTHERWISE NOTED.

THE CONCRETE IN THE ABUTMENT BACKWALL SHALL NOT BE PLACED UNTIL STEEL
WORK IS ERECTED, BUT SHALL BE PLACED BEFORE THE DECK SLAB IS PLACED.

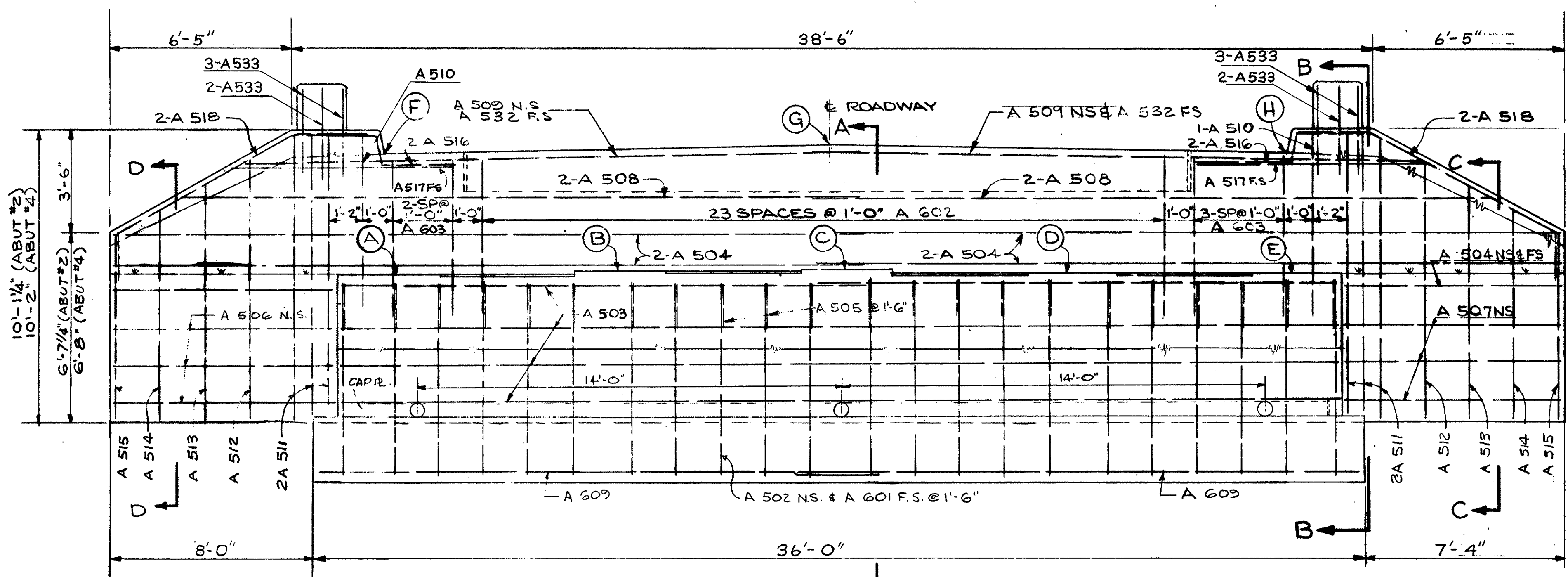
POROUS BACKFILL SHALL EXTEND UPWARD TO THE APPROACH SLAB AND TO
THE SURFACE OF THE EARTH SHOULDERS, AND OUTWARD AS SHOWN.

EXCAVATION THEREFORE IN EXCESS OF THAT
REQUIRED FOR CONSTRUCTION OF THE ABUTMENTS, SHALL BE CONSIDERED
AS PAID FOR IN THE BID PRICE PER CUBIC YARD PAID FOR POROUS BACKFILL.

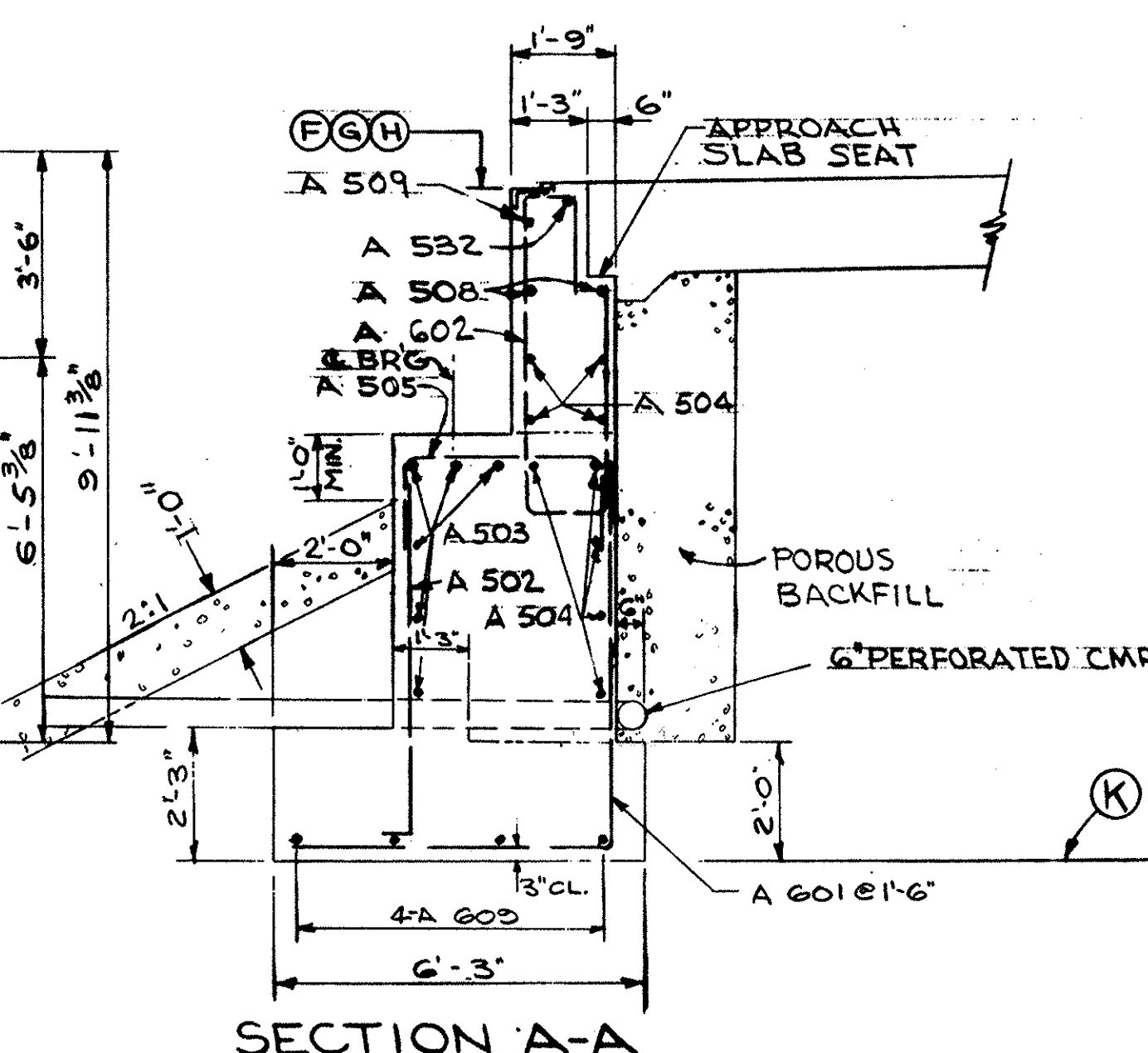


PLAN VIEW
ABUT #2 & ABUT #4

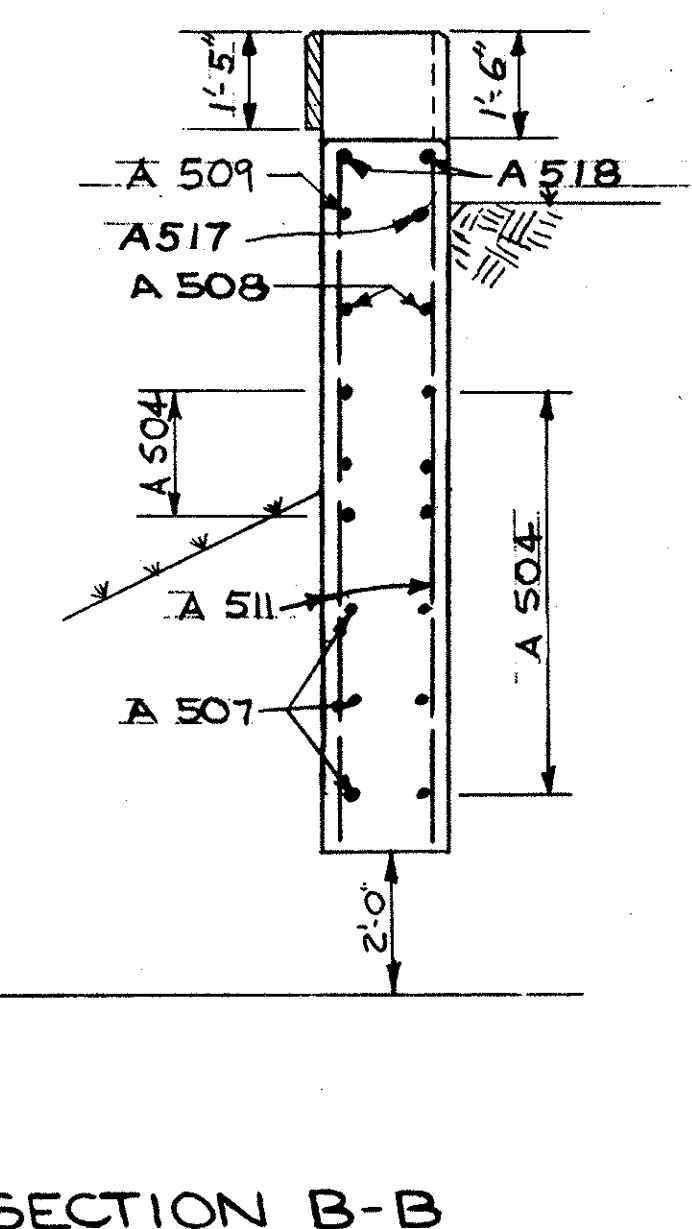
| | A | B | C | D | E | F | G | H | K |
|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|
| ABUT #2 | 1108.28 | 1108.32 | 1108.43 | 1108.28 | 1108.13 | 1112.35 | 1112.50 | 1112.20 | 1101.10 |
| ABUT #4 | 1107.80 | 1107.84 | 1107.89 | 1107.73 | 1107.58 | 1111.87 | 1111.95 | 1111.65 | 1100.55 |



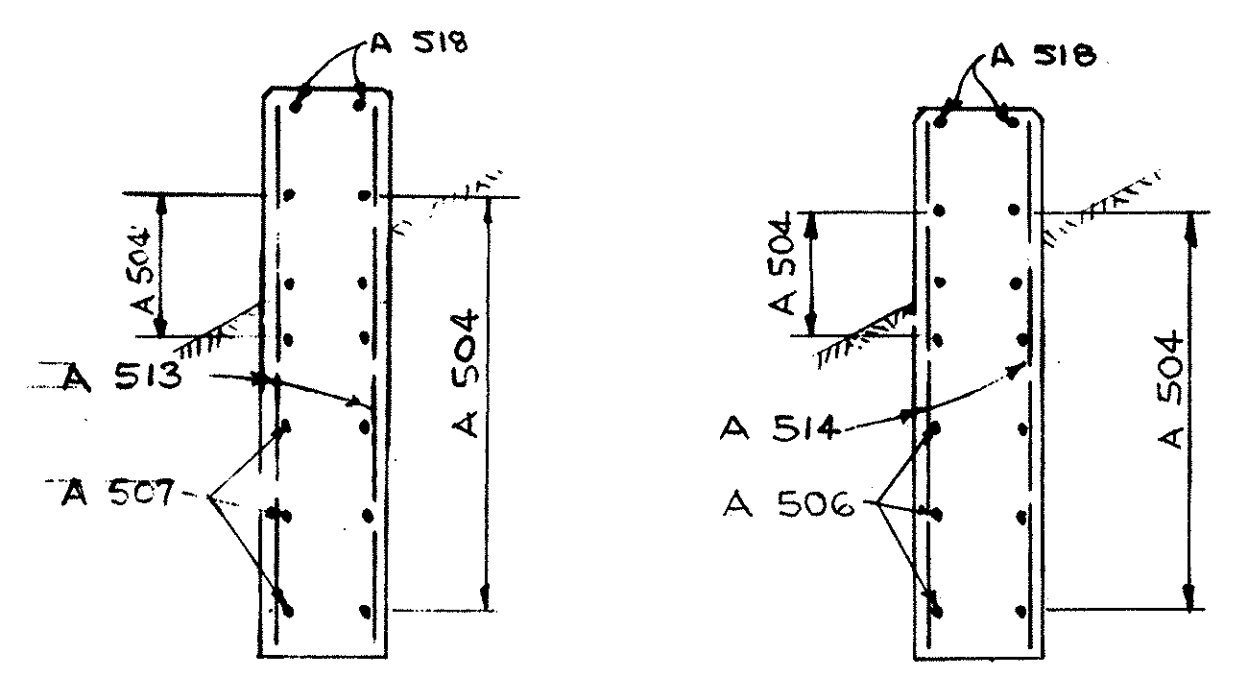
ELEVATION
ABUT #2 & ABUT #4



SECTION A-A

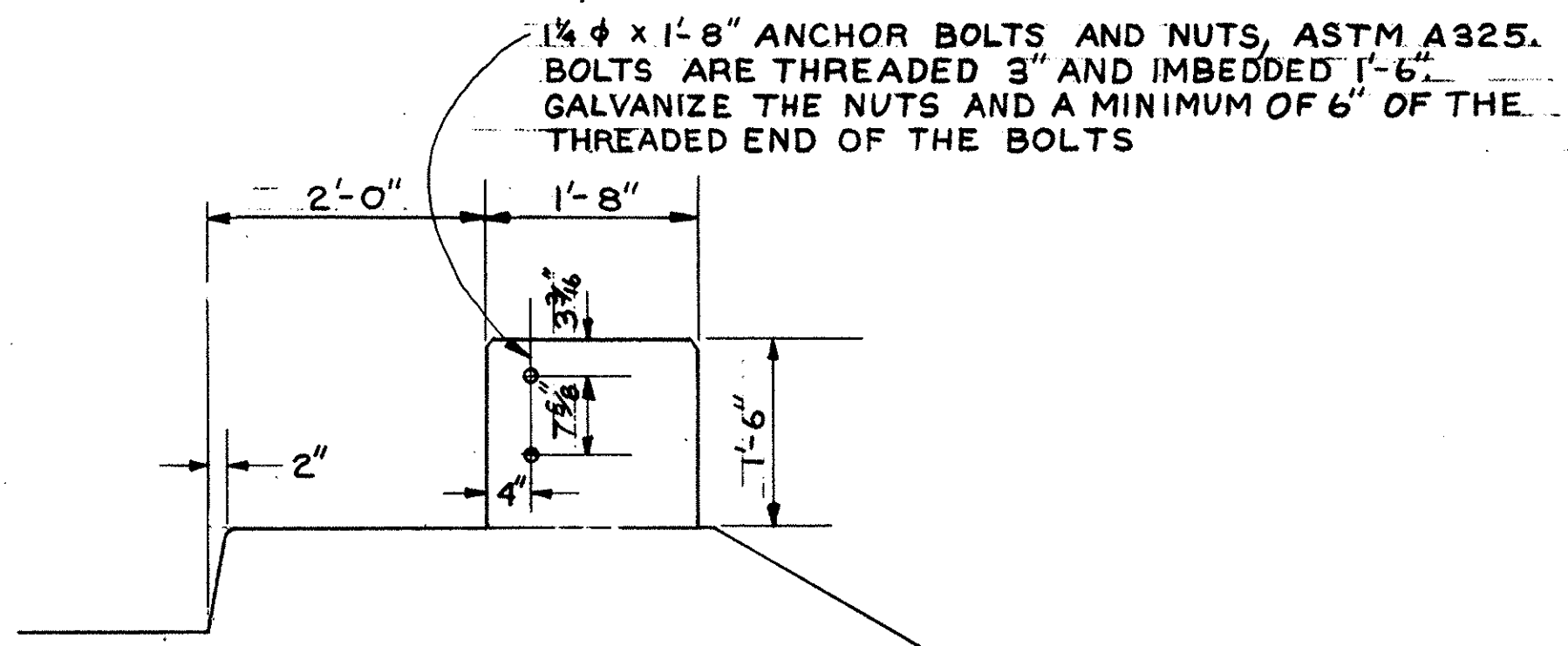


SECTION B-B



SECTION C-C

SECTION D-D



SECTION E-E

ANCHORS FOR ATTACHMENT
OF APPROACH GUARD RAIL
TO 1'-9" x 1'-8" CONCRETE POST

STATE OF OHIO
DEPARTMENT OF HIGHWAYS
BUREAU OF BRIDGES

AKRON, OHIO

GLAUS, PYLE & SCHOMER
YOUNGSTOWN, OHIO

ABUTMENT DETAILS
BRIDGE N° MAH-11-0802 L & R
OVER RELOCATED INDIAN RUN CREEK
MAHONING CO. S.R.N°11
STA. 424+70

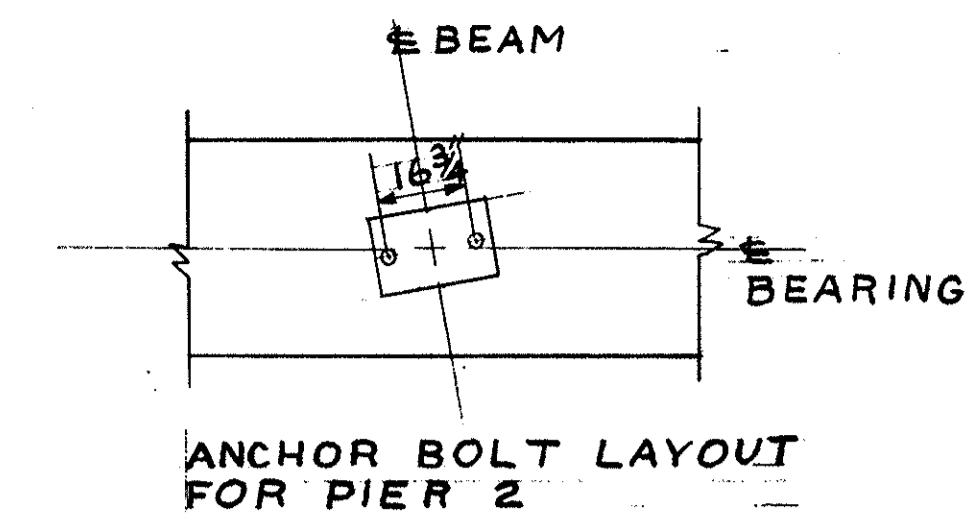
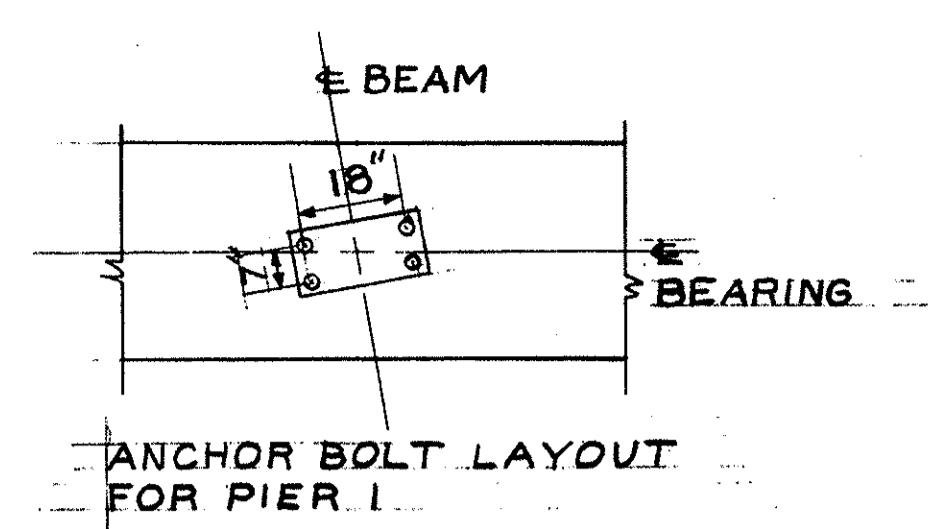
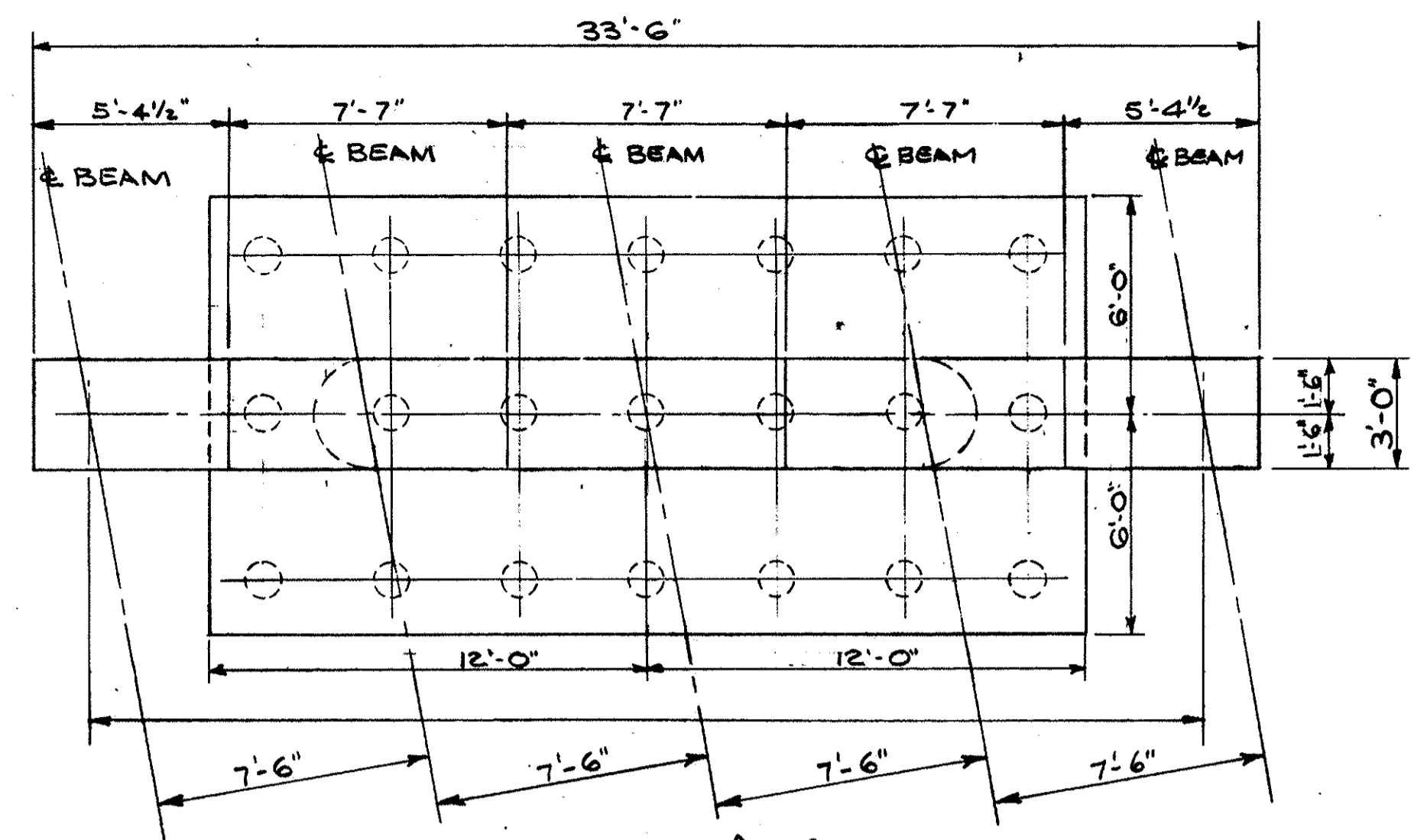
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| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| DS | RGN | | DS | WKS | 4-14-67 | |

MICROFILMED
JUL 29 1978
REPRODUCTION

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| FED. RD. | STATE | PROJECT |
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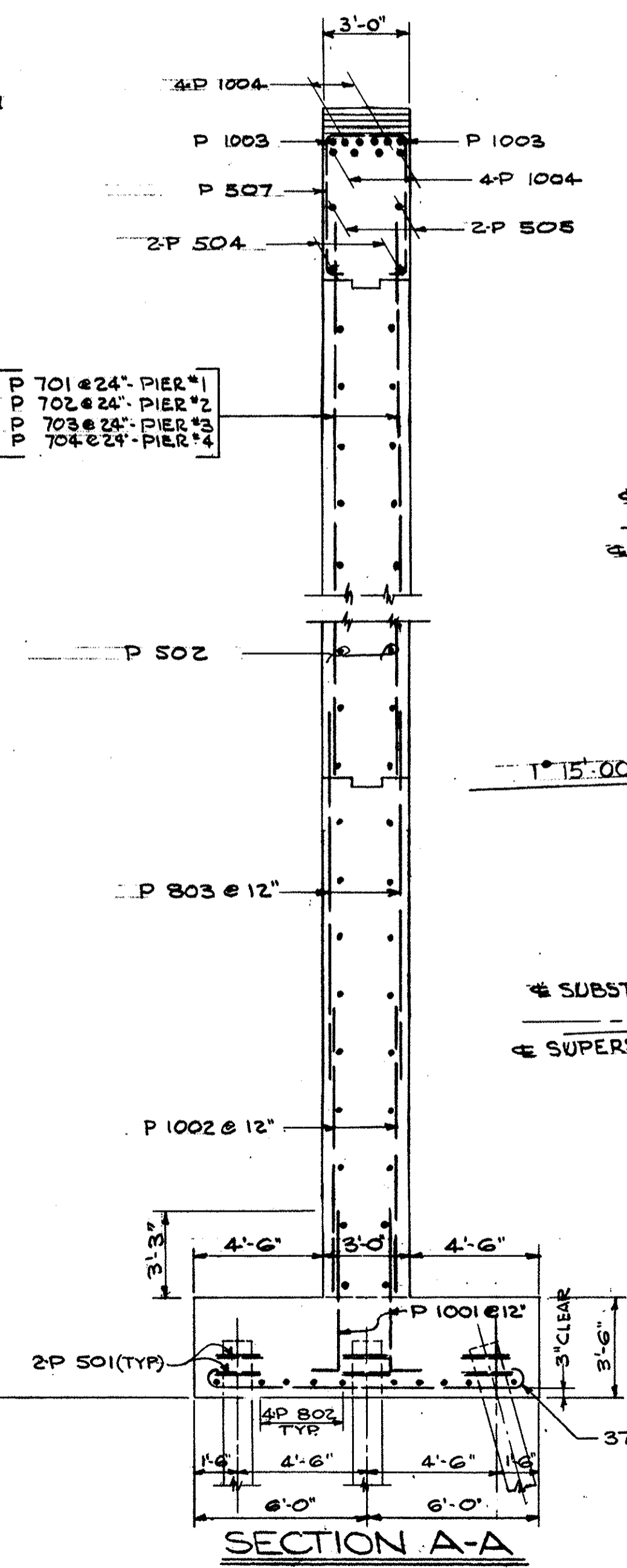
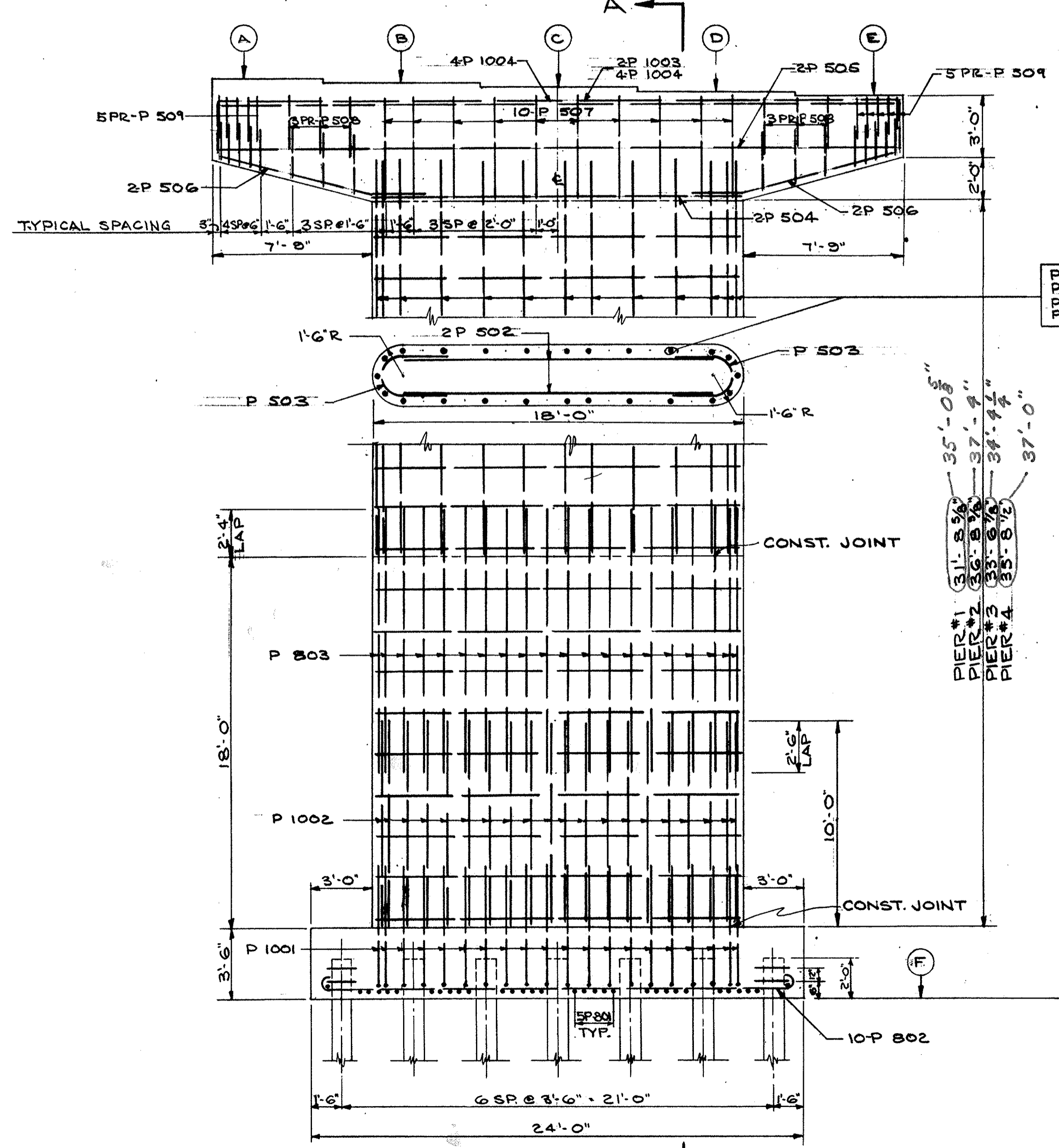


"F" BASE ELEVATIONS

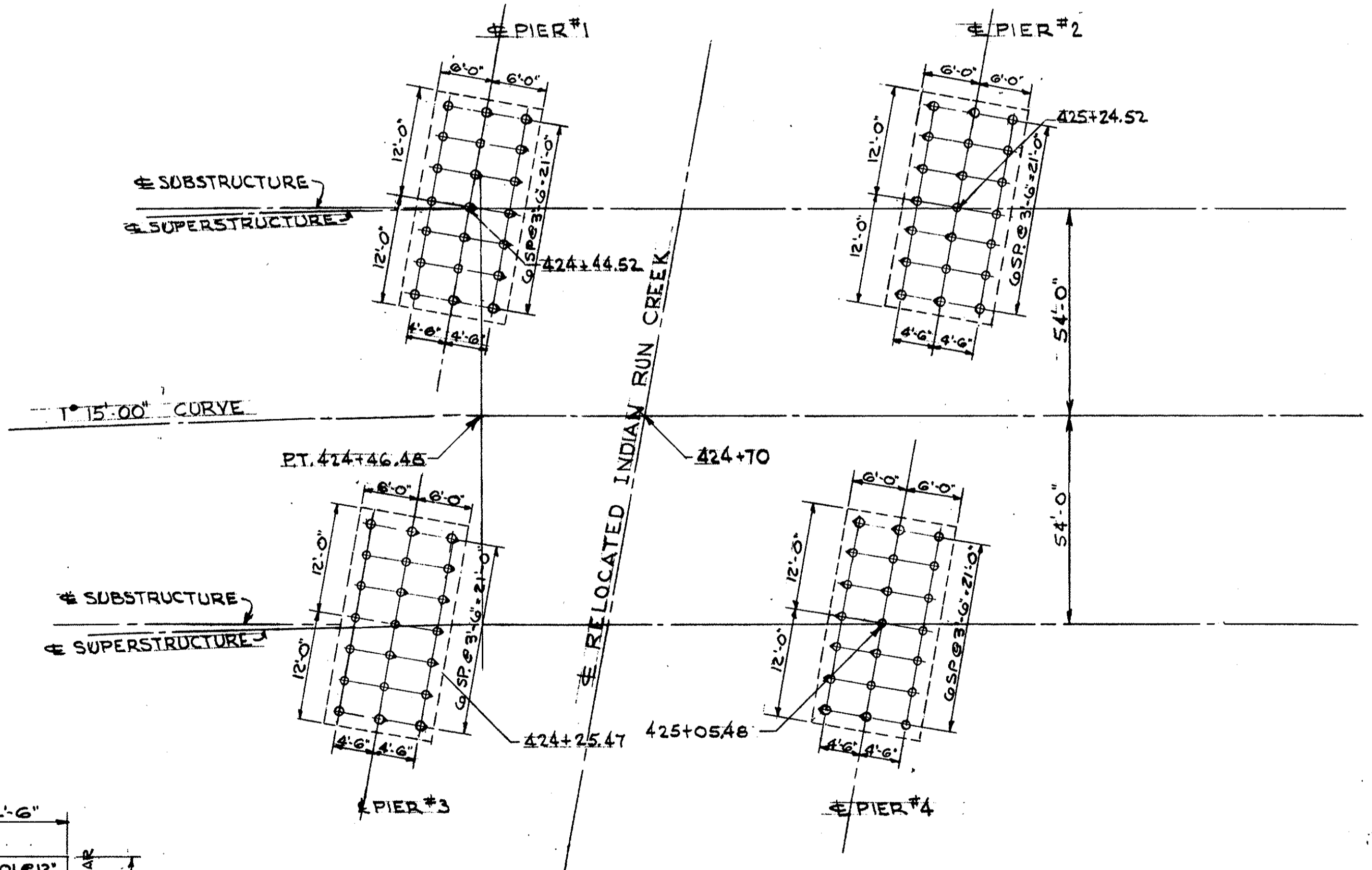
| | |
|---------|---------|
| PIER #1 | 1060.66 |
| PIER #2 | 1060.66 |
| PIER #3 | 1060.20 |
| PIER #4 | 1060.20 |

PIER ELEVATION TABLE

| LOCATION | "A" | "B" | "C" | "D" | "E" |
|----------|---------|---------|---------|---------|---------|
| PIER #1 | 1105.06 | 1104.85 | 1104.63 | 1104.42 | 1104.21 |
| PIER #2 | 1106.85 | 1106.82 | 1106.79 | 1106.64 | 1106.49 |
| PIER #3 | 1104.05 | 1103.80 | 1103.56 | 1103.30 | 1103.07 |
| PIER #4 | 1106.22 | 1106.11 | 1106.00 | 1105.85 | 1105.70 |



REINFORCING STEEL SHALL BE 2" CLEAR FROM FACE OF CONCRETE UNLESS OTHERWISE NOTED.



PILE & FOUNDATION PLAN
○ - INDICATES BATTERED PILE

STATE OF OHIO
DEPARTMENT OF HIGHWAYS
BUREAU OF BRIDGES

AKRON, OHIO

GLAUS, PYLE & SCHOMER
YOUNGSTOWN, OHIO

PIER DETAILS & PILE PLAN
BRIDGE N^o MAH-11-0802 L & R
OVER RELOCATED INDIAN RUN CREEK
MAHONING CO. S.R. N^o 11
STA. 424+70

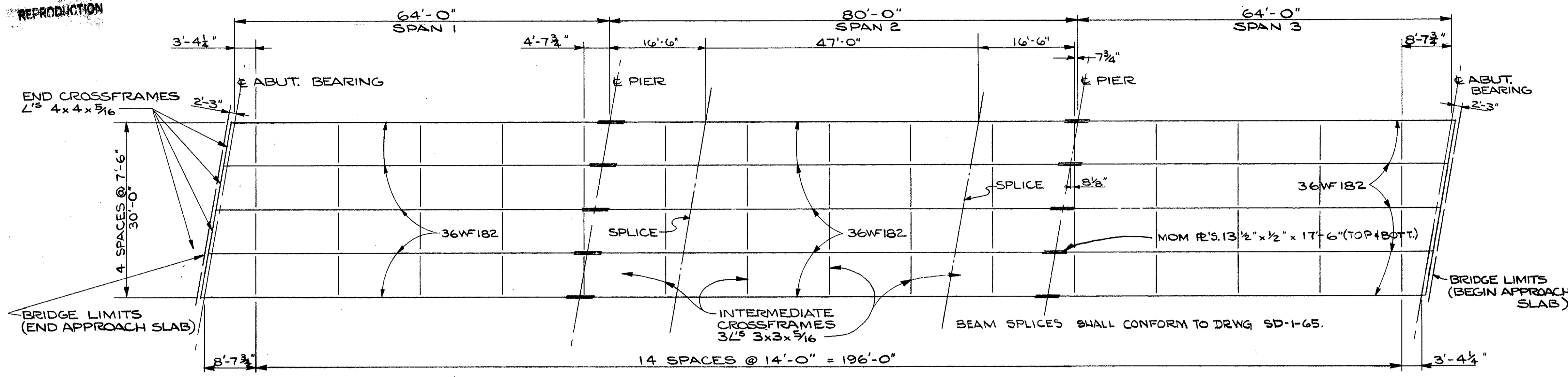
| | | | | | | | | | | | | | |
|----------|------|-------|--------|--------|------|---------|------|----------|--------|------|---------|---------|--------|
| DESIGNED | D.S. | DRAWN | R.G.H. | TRACED | D.S. | CHECKED | D.S. | REVIEWED | W.V.D. | DATE | 4-14-67 | REVISED | 8-5-69 |
|----------|------|-------|--------|--------|------|---------|------|----------|--------|------|---------|---------|--------|

MICROFILMED
JUL 09 1979
REPRODUCTION

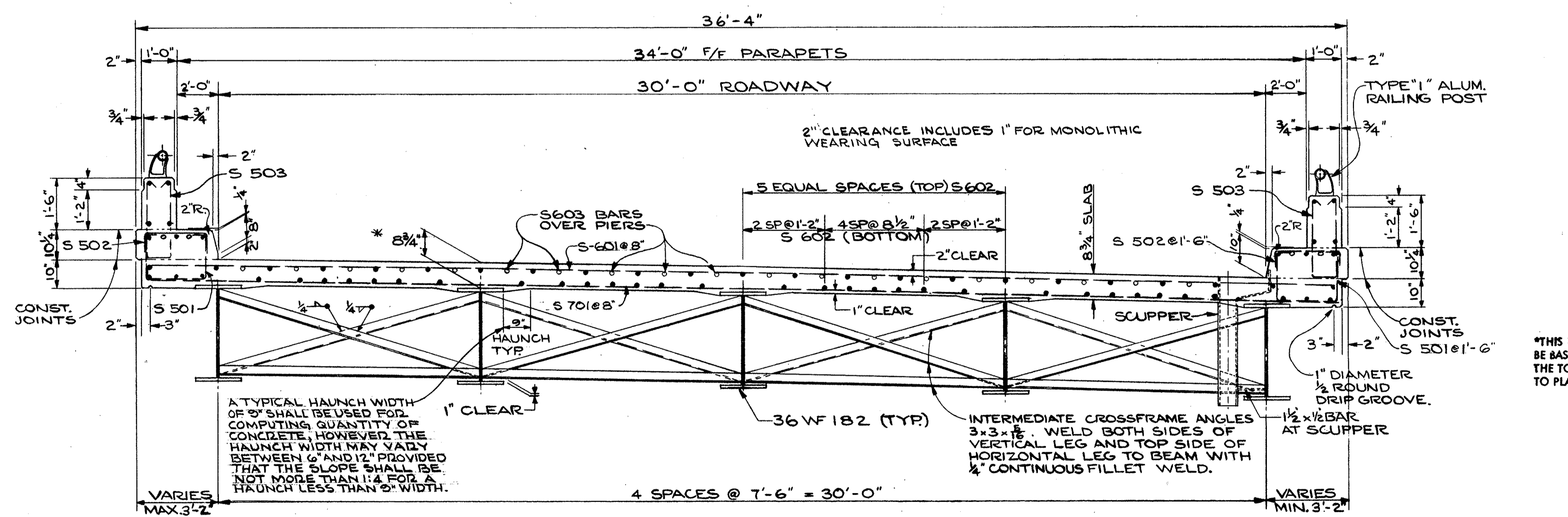
| | | |
|----------|-------|---------|
| FED. RD. | STATE | PROJECT |
| 2 | OHIO | STATE |

313
341

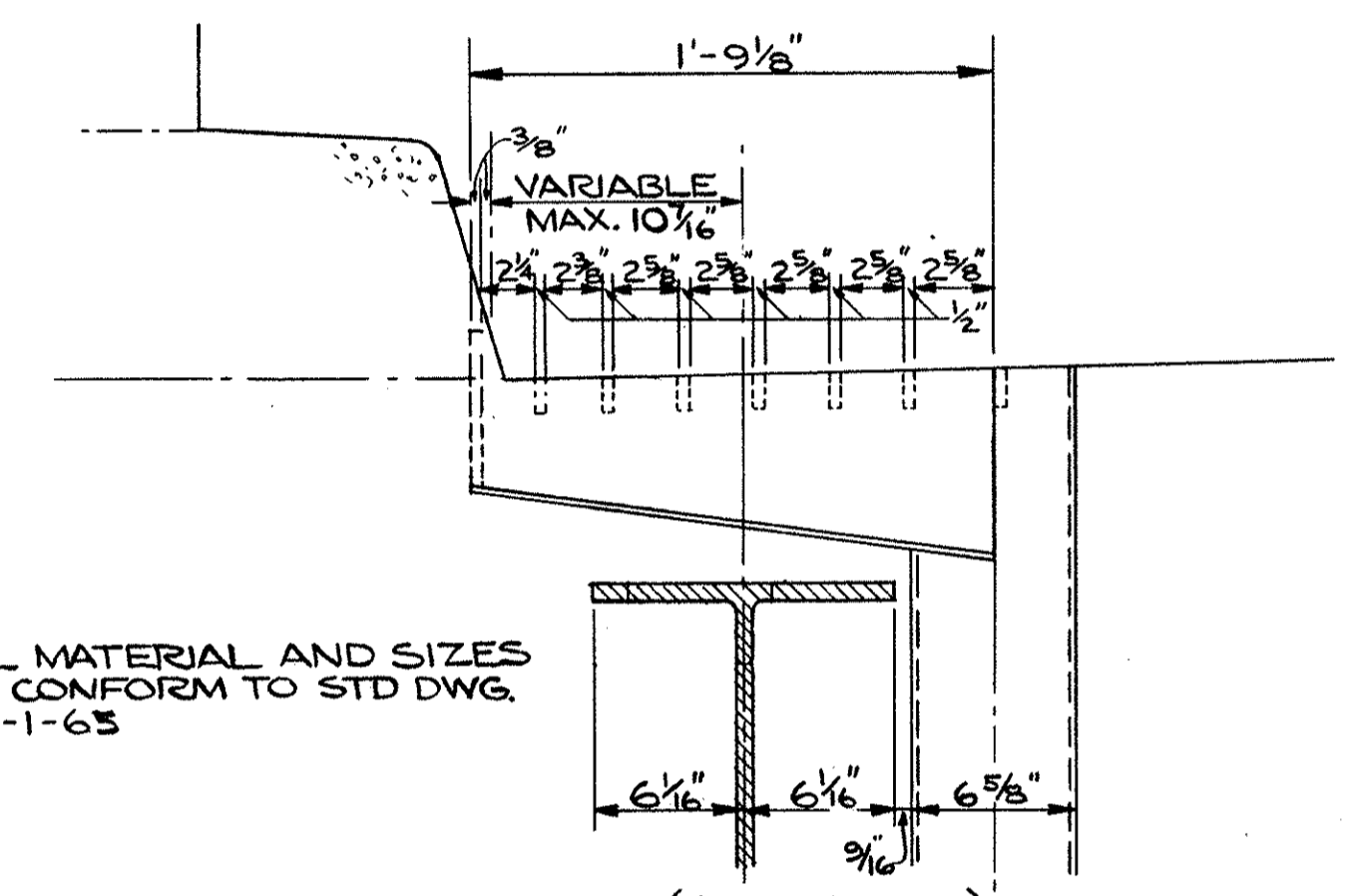
MAH-11-2.46



FRAMING PLAN



TRANSVERSE SECTION

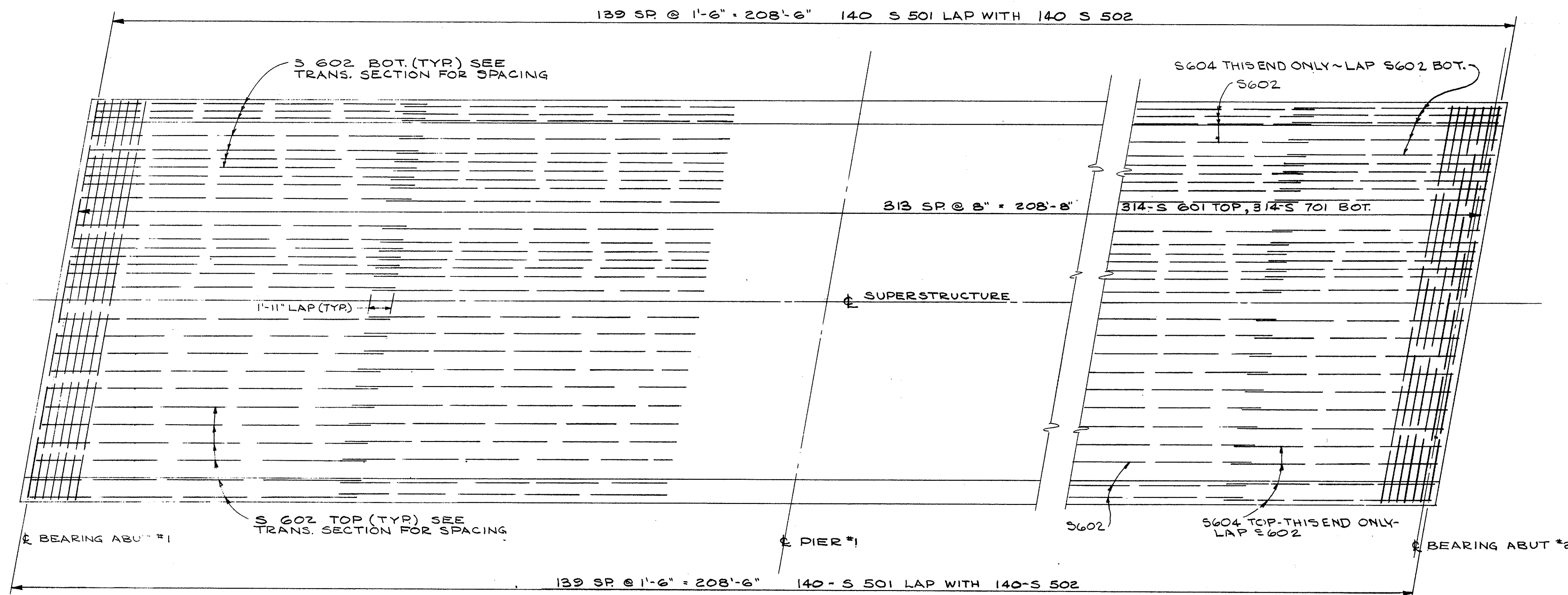


SCUPPER (SPECIAL)

ALL MATERIAL AND SIZES TO CONFORM TO STD DWG. SD-1-65

*THIS IS THE NOMINAL DIMENSION. THE QUANTITY OF DECK CONCRETE TO BE PAID FOR SHALL BE BASED ON THIS DIMENSION EVEN THOUGH DEVIATION FROM IT MAY BE NECESSARY BECAUSE THE TOP FLANGE OF THE BEAM MAY NOT HAVE THE EXACT CAMBER OR CONFORMATION REQUIRED TO PLACE IT PARALLEL TO THE FINISHED GRADE.

| | | | | | | |
|--|-------|--------|---|----------|---------|---------|
| STATE OF OHIO DEPARTMENT OF HIGHWAYS BUREAU OF BRIDGES | | | | | | |
| AKRON, OHIO | | | GLAUS, PYLE & SCHOMER YOUNGSTOWN, OHIO | | | |
| FRAMING PLAN | | | | | | |
| BRIDGE N° MAH-11-0802 L & R | | | | | | |
| OVER RELOCATED INDIAN RUN CREEK | | | | | | |
| MAHONING CO. S.R. N° 11 | | | | | | |
| STA. 424+70 | | | | | | |
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| DJ | RGN | | DJ | WKO | 4-14-67 | |

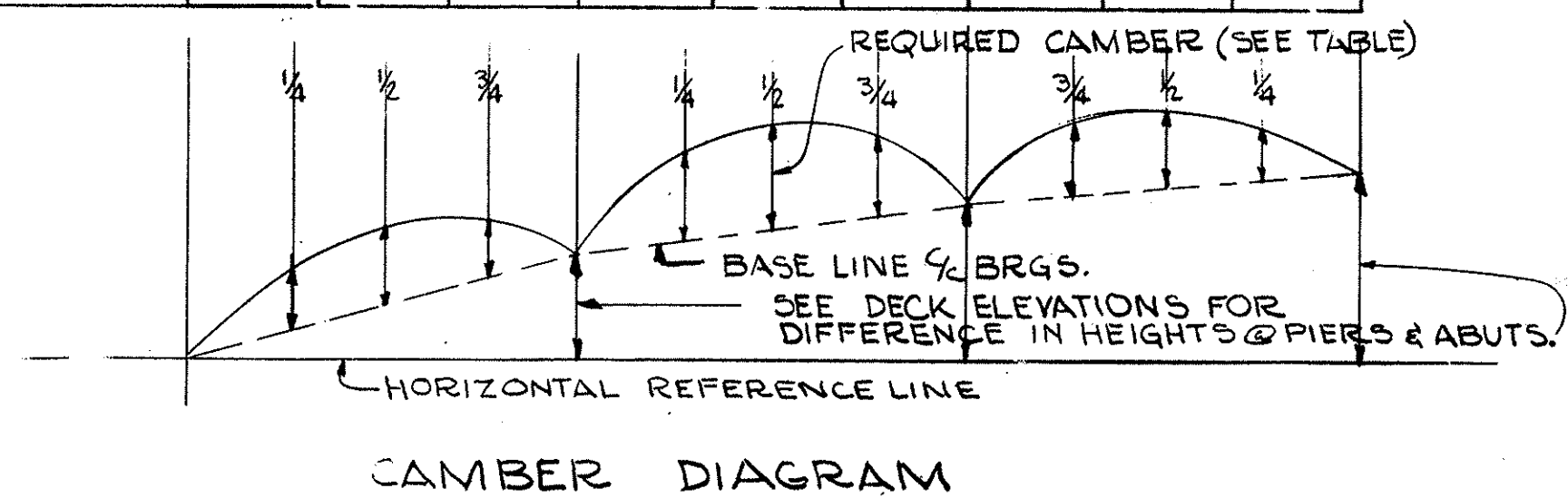


SLAB REINFORCING PLAN

DEFLECTION AND CAMBER

NORTH & SOUTH BOUND STRUCTURES

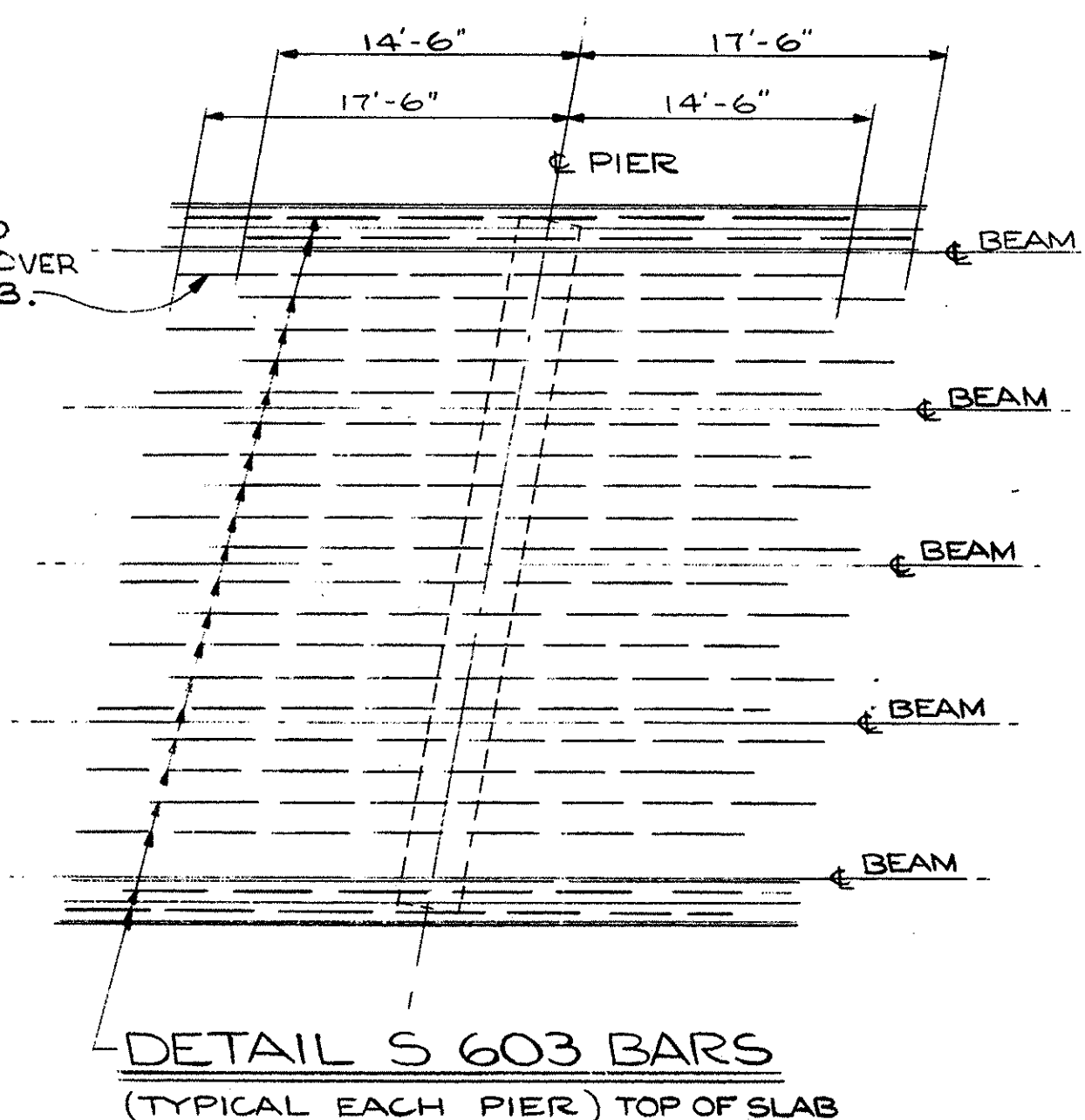
| BEAM LOCATION | SPAN 1 | | | SPAN 2 | | | SPAN 3 | | |
|---|---------|---------|---------|---------|---------|---------|---------|---------|---------|
| | 1/4 PT. | 1/2 PT. | 3/4 PT. | 1/4 PT. | 1/2 PT. | 3/4 PT. | 1/4 PT. | 1/2 PT. | 1/4 PT. |
| DEFLECTION DUE TO WEIGHT OF STEEL. | .07" | .08" | .04" | .06" | .11" | .06" | .04" | .08" | .07" |
| DEFLECTION DUE TO REMAINING DEAD LOAD. | .31" | .37" | .17" | .28" | .49" | .28" | .17" | .37" | .31" |
| ADJUSTMENT REQUIRED FOR VERTICAL CURVE. | -.12 | -.16 | -.12 | -.24 | -.31 | -.24 | 0 | 0 | 0 |
| REQUIRED SHOP CAMBER | .26" | .29" | .09" | .10" | .29" | .10" | .21" | .45" | .38" |



DECK ELEVATIONS AT CURB LINE

| LEFT BRIDGE | | | | | RIGHT BRIDGE | | | | |
|-------------|---------|---------|---------|---------|--------------|---------|---------|---------|---------|
| ABUT-1 | PIER-1 | PIER-2 | ABUT-2 | PIER-3 | PIER-4 | ABUT-3 | PIER-3 | PIER-4 | ABUT-4 |
| 1107.62 | 1108.73 | 1109.57 | 1112.45 | 1108.42 | 1110.71 | 1107.62 | 1108.73 | 1110.71 | 1112.45 |
| 1107.59 | 1107.96 | 1108.34 | 1112.30 | 1108.21 | 1110.51 | 1107.47 | 1108.21 | 1110.51 | 1112.30 |
| 1108.16 | 1108.01 | 1108.25 | 1112.13 | 1108.72 | 1111.08 | 1107.72 | 1108.72 | 1111.08 | 1112.13 |
| 1108.57 | 1109.57 | 1110.55 | 1111.78 | 1109.18 | 1111.43 | 1108.10 | 1109.18 | 1111.43 | 1111.78 |
| 1109.30 | 1110.21 | 1111.02 | 1112.02 | 1109.69 | 1111.51 | 1108.49 | 1109.69 | 1111.51 | 1112.02 |
| 1110.17 | 1111.02 | 1111.78 | 1112.45 | 1110.17 | 1111.02 | 1109.57 | 1110.17 | 1111.02 | 1112.45 |
| 1111.02 | 1111.43 | 1111.78 | 1112.13 | 1110.51 | 1111.02 | 1108.21 | 1110.51 | 1111.02 | 1112.13 |
| 1111.43 | 1111.78 | 1112.02 | 1112.30 | 1110.21 | 1111.43 | 1107.96 | 1110.21 | 1111.43 | 1112.30 |
| 1111.78 | 1112.02 | 1112.13 | 1112.45 | 1110.02 | 1111.78 | 1107.62 | 1110.02 | 1111.78 | 1112.45 |
| 1112.02 | 1112.13 | 1112.30 | 1112.45 | 1109.69 | 1111.51 | 1107.47 | 1109.69 | 1111.51 | 1112.30 |
| 1112.13 | 1112.30 | 1112.45 | 1112.45 | 1109.30 | 1111.02 | 1107.21 | 1109.30 | 1111.02 | 1112.30 |
| 1112.30 | 1112.45 | 1112.45 | 1112.45 | 1108.96 | 1110.51 | 1107.02 | 1108.96 | 1110.51 | 1112.30 |
| 1112.45 | 1112.45 | 1112.45 | 1112.45 | 1108.69 | 1110.21 | 1106.72 | 1108.69 | 1110.21 | 1112.30 |

NOTE:
THIS BAR USED
ON HIGH SIDE C/R
PIER #1 & PIER #2.



THE ELEVATIONS SHOWN AT THE FACE OF CURBS ARE THOSE WHICH ARE REQUIRED BEFORE THE CONCRETE DECK IS PLACED. PROPER ALLOWANCE HAS BEEN MADE FOR THE DEAD LOAD DEFLECTIONS CAUSED BY THE WEIGHT OF THE CONCRETE.

STATE OF OHIO
DEPARTMENT OF HIGHWAYS
BUREAU OF BRIDGES

AKRON, OHIO YOUNGSTOWN, OHIO

GLAUS, PYLE & SCHOMER

SLAB DETAILS
BRIDGE N° MAH-11-0802 L & R
OVER RELOCATED INDIAN RUN CREEK
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STA. 424+70

| | | | | | | |
|----------|-------|--------|---------|----------|---------|---------|
| DESIGNED | DRAWN | TRACED | CHECKED | REVIEWED | DATE | REVISED |
| DS | RGN | | DS | WIC | 4-14-67 | 2-3-68 |

